

**BRITAIN'S No.1  
MODEL RAILWAY MAGAZINE**

JULY 2026 £6.95

AT THE HEART OF THE HOBBY

VOLUME 77 No 909

# RAILWAY MODELLER

## Tunnel vision

**A signature Middleton Railway scene - modelled in OO**

**WIN**  
**A Cavalex**  
**Class 56**  
**WORTH £199!**  
**DETAILS INSIDE**



### Also inside...

**A layout built in a shipping container - in Cyprus!**  
**75 years of preservation at the Talylyn Railway**  
**Creating a cathedral in card - a masterpiece in N**

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# RAILWAY MODELLER

## Contents

July 2026 vol 77 No.909

### 502 **Lydbrook Dean** **Railway of the Month**

With this portable exhibition layout, Alexander Warren has captured the rustic atmosphere of the freight-only branch lines of the Forest of Dean in the later British Railways steam era.

### 510 **Modelling Class 121 and 122** **'bubble cars'**

Will Thompson showcases his collection of first generation single-car units in EM, created by enhancing, detailing, and in some cases repainting, proprietary models from Bachmann and Dapol.

### 515 **Preservation pioneer – the** **story of a Welsh steam survivor**

75 years ago, the Talylyn Railway in mid-Wales led the global preservation movement. To mark this anniversary, Simon Artymiuk charts the history of this remarkable prototype, which has been inspiring narrow gauge modellers for generations.

### 520 **Weobley** **Plan of the Month**

Darren Ray took inspiration from a classic 1970s C J Freezer track plan to create this demountable L-shaped layout in OO, which utilises a fiddle yard design attributed to Peter Denny.

### 526 **Culthwaite**

A desire for an N gauge exhibition layout with frequent train movements, and a lifelong love of the Settle & Carlisle line, led Philip Mason to build this model combining aspects of two locations on the famous route – Culgaith and Armathwaite.

### 532 **Whittingly Hospital** **Micro Marvel**

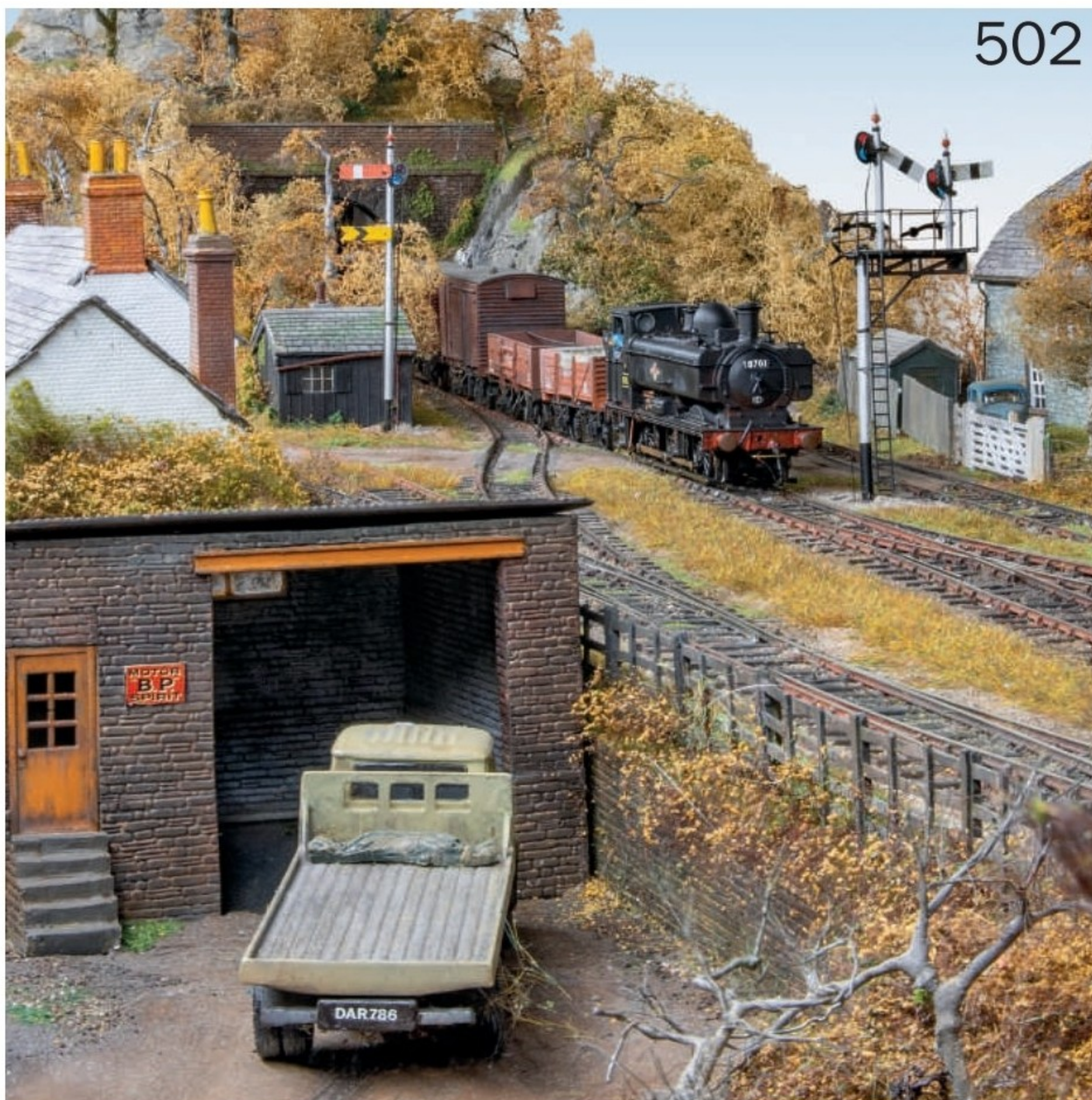
This OO gauge cameo built by Michael Campbell, which was one of the finalists in the Peco RAIL200 Model Layout Competition, was inspired by the private railways built in the late 19th and early 20th centuries to serve mental hospitals.

### 536 **A card cathedral in N** **Scale Drawings**

Michael Scott utilised his years of railway modelling experience to create a mammoth replica of Bradford Cathedral in 1:148 scale, which will be on public display inside the real building for two months from 13 July.

### 542 **Brent – change for Kingsbridge** **Through the Railway Room Door**

This article could be titled 'Through the Shipping Container Door', for that is what Jason Schofield has used to house his OO gauge Great Western Railway layout – in Cyprus!



548



510



532



542



## RAILWAY MODELLING *Explored*

Projects, hints and tips for beginners from Railway Modeller readers and staff

### 555 **Strathearn Parkway**

Dr Roger I Cartwright explains how a visit to a model exhibition near his home in Scotland inspired him to build an OO gauge exhibition layout, based on the local rail scene in the early 2000s.

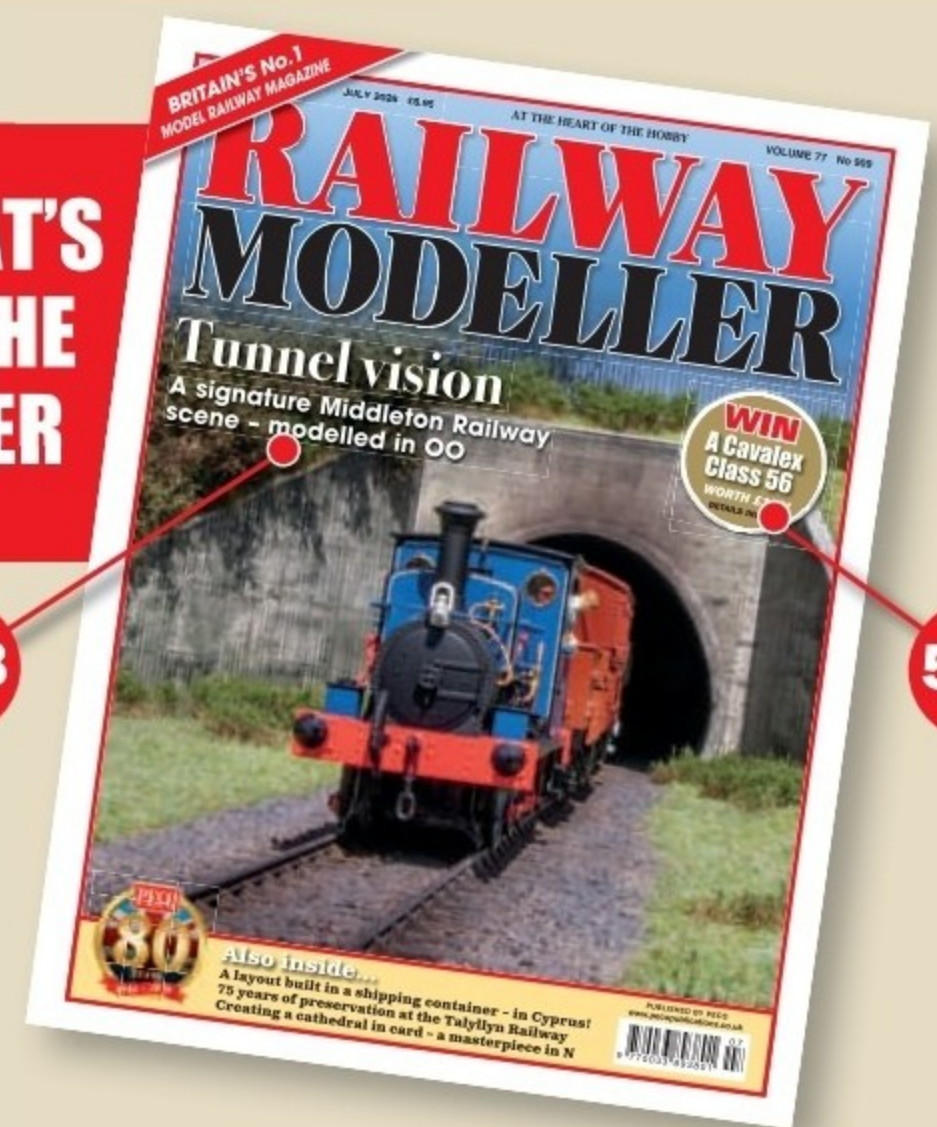
### 558 **Tunnel vision**

With the arrival of the new Manning Wardle L Class 0-6-0ST from Rapido Trains UK (see Reviews) Callum Willcox created a Middleton Railway-inspired scene for this month's cover photo.



555

WHAT'S  
ON THE  
COVER



558

571

*Cover photo:* The new Rapido Trains UK Manning Wardle 0-6-0ST (reviewed on page 564) on the tunnel diorama detailed on page 558.

### 548 **Building a Collett '9700' 0-6-0PT in O**

John Cockcroft describes how he used a conversion kit produced by Minerva Model Railways to construct one of the small fleet of Great Western pannier tanks that were equipped with condensing apparatus.

### 561 **Comment**

Opinions and observations across the field of railway modelling. This month, Paul Jones – who served as the exhibition manager for the Warley Model Railway Club for 40 shows – considers the benefits and the potential pitfalls of organising such events.

### 562 **Readers' Letters**

### 564 **New Product Reviews**

Leading the reviews this month is the Manning Wardle L Class 0-6-0 saddle tank in OO gauge from Rapido Trains UK. We also look at the 'Railway 200' Class 801/2 Azuma and the RailRoad 'Drax 50' Class 66 in OO from Hornby; PXA steel carriers and JXA scrap wagons in OO, produced exclusively for Rails of Sheffield by Cavalex Models; and Bulleid Pallet Vans in OO from Irish Railway Models.

### 570 **Book Reviews**

### 571 **Win a Cavalex 'Peco' Class 56**

RAILWAY MODELLER is giving readers the chance to win a Limited Edition Cavalex OO gauge model of the 'Peco' Class 56 diesel, worth £199.00.

### 572 **News**

South Eastern Railway 0 Class 0-6-0 in OO from Rapido Trains UK; new tooling Class 20s in N lead Bachmann summer announcements; exclusive models of preserved 'Black Five' No.45407 for Rails of Sheffield and Hornby Club; The Model Centre commissions bogie box wagons in TT:120; decorated sample of Locomotion Models 'NRM 50' Class 91 revealed; Brassmasters releases new kit for LNER J19/2 0-6-0 in 4mm scale; previews of Coventry, Exeter and RailEX North East 2026 exhibitions; the latest news from Peco, including new bullhead curved points in OO, and the presentation of the plates from the Class 56 named after the firm; and much more...

### 577 **Societies & Clubs**

The biggest and best guide to the exhibition scene.

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SEE PAGE 64a FOR DETAILS

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Latest Releases



OO Gauge SER O Class - Due In The Future



825001 - SER O Class 0-6-0 315 SER Cudworth Green.....£178.45



825002 - SER O Class 0-6-0 378 SER Lined Black With Red Lining.....£178.45



825003 - SER O Class 0-6-0 80 SER Lined Black Red and Yellow Lining.....£178.45



825004 - SER O Class 0-6-0 1 SECR Wainwright Green.....£178.45



825005 - SER O Class 0-6-0 381 SECR Wainwright Green.....£178.45



825006 - SER O Class 0-6-0 171 SECR Plain Green.....£178.45



825007 - SER O Class 0-6-0 142 SECR Grey.....£178.45



825008 - SER O Class 0-6-0 A8 SR Lined Black (Egyptian Lettering).....£178.45



825009 - SER O Class 0-6-0 436 SR Plain Black (Egyptian Lettering).....£178.45

Sound fitted versions of each model available.....£271.95 each

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Bachmann 35-777SFX Class 69 69002 'Bob Tiller CM&E' GBRf BR Large Loge Blue DCC Sound Deluxe.....£275.00



Rapido 958009 LNER J52 0-6-0 68832 BR Black Late Crest.....£115.00



Hornby R30274 - LMS Stanier Black 5 4-6-0 5047 LMS Black.....£185.00



Oxford Rail OR76J26001 - LNER J26 0-6-0 1057 LNER Black.....£103.99



Bachmann 32-679A - Class 45 D25 BR Economy Green Small Yellow Panels Split Headcode.....£99.99



OO Gauge LMS 4Fs - In Stock



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31-884ASF - 4F 0-6-0 43982 BR Black Late Crest with Fowler Tender And Yellow Cab Side Stripe DCC Sound Fitted.....£237.96



31-885 - MR 3835 4F 0-6-0 3850 LMS Black MR Numerals Johnson-Deeley Tender.....£144.46

31-885SF - MR 3835 4F 0-6-0 3850 LMS Black MR Numerals Johnson-Deeley Tender DCC Sound Fitted.....£237.96



31-886 - MR 3835 4F 0-6-0 43892 BR Black British Railways Fowler Tender.....£144.46

31-886SF - MR 3835 4F 0-6-0 43892 BR Black British Railways Fowler Tender DCC Sound Fitted.....£237.96

OO Gauge LNER J72s - In Stock



31-065 - NER E 0-6-0 Tank 495 NER Lined Green.....£144.46

31-065SF - NER E 0-6-0 Tank 495 NER Lined Green And Maroon DCC Sound Fitted.....£237.96



31-066 - NER E J71 0-6-0 Tank 317 LNER Lined Black.....£135.96

31-066SF - NER E J71 0-6-0 Tank 317 LNER Lined Black DCC Sound Fitted.....£229.46



31-069 - NER E J71 0-6-0 Tank 68260 BR Black Early Emblem.....£135.96

31-069SF - NER E J71 0-6-0 Tank 68260 BR Black Early Emblem DCC Sound Fitted.....£229.46

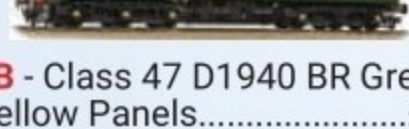
OO Gauge Class 47s - In Stock



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35-410ASF - Class 47 D1922 BR Green Small Yellow Panels DCC Sound Fitted.....£301.71

35-410ASFX - Class 47 D1922 BR Green Small Yellow Panels DCC Sound Fitted Deluxe.....£335.71



35-410B - Class 47 D1940 BR Green Small Yellow Panels.....£203.96

35-410BSF - Class 47 D1940 BR Green Small Yellow Panels DCC Sound Fitted.....£301.71

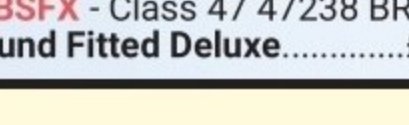
35-410BSFX - Class 47 D1940 BR Green Small Yellow Panels DCC Sound Fitted Deluxe.....£335.71



35-411A - Class 47 47226 BR Blue.....£203.96

35-411ASF - Class 47 47226 BR Blue DCC Sound Fitted.....£301.71

35-411ASFX - Class 47 47226 BR Blue DCC Sound Fitted Deluxe.....£335.71



35-411B - Class 47 47238 BR Blue.....£203.96

35-411BSF - Class 47 47238 BR Blue DCC Sound Fitted.....£301.71

35-411BSFX - Class 47 47238 BR Blue DCC Sound Fitted Deluxe.....£335.71



OO Gauge Manning Wardle L Class - In Stock



960002 - Manning Wardle L Class 0-6-OST 'Arthur' APCM Stone Green.....£127.45



960004 - Manning Wardle L Class 0-6-OST 'Sir Berkeley' Dark Green As Preserved 2010s Cranford Style.....£127.45



960005 - Manning Wardle L Class 0-6-OST No.20 'Logan and Hemmingway' Green.....£127.45



960006 - Manning Wardle L Class 0-6-OST 'Forward' NCB Green.....£127.45



960008 - Manning Wardle L Class 0-6-OST Plain Red.....£127.45



960009 - Manning Wardle L Class 0-6-OST SECR 331 'What If' Livery.....£127.45



960010 - Manning Wardle L Class 0-6-OST 481 'Hawthorn' Blue 'What If' Livery.....£127.45

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OO Gauge Railroad Class 37s - In Stock



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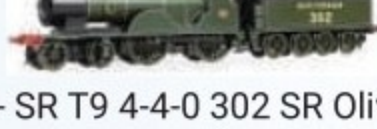
R30544TXS - Class 37 37422 'Pegasus' HNRc Swietelsky TXS Sound Fitted.....£134.99



R30545 - Class 37 37501 'Teesside Steelmaster' ROG Light Blue.....£85.49

R30545TXS - Class 37 37501 'Teesside Steelmaster' ROG Light Blue TXS Sound Fitted.....£134.99

OO Gauge SR T9 - In Stock



R30410 - SR T9 4-4-0 302 SR Olive Green.....£206.99



OO Gauge LSWR T3 - Last versions now in Stock



E85019 - LSWR Adams T3 561 LSWR Adams Green.....£169.96



E85023 - LSWR Adams T3 564 LSWR Urie Green.....£169.96



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SL89 - Large Radius Left Hand Point.....£20.85
SL90 - Double Slip.....£57.05
SL91 - Small Radius Right Hand Point.....£16.90
SL92 - Small Radius Left Hand Point.....£16.90
SL93 - Short Crossing.....£15.00
SL94 - Long Crossing.....£17.85
SL95 - Medium Radius Right Hand Point.....£18.45
SL96 - Medium Radius Left Hand Point.....£18.45
SL98 - Large Radius Y Point.....£19.40
SL99 - 3 Way Point.....£45.80

OO Gauge Code 100 Electrofrog Points

- SLE86 - Curved Right Hand Point.....£22.00
SLE87 - Curved Left Hand Point.....£22.00
SLE88 - Large Radius Right Hand Point.....£21.25
SLE89 - Large Radius Left Hand Point.....£21.25
SLE91 - Small Radius Right Hand Point.....£16.90
SLE92 - Small Radius Left Hand Point.....£16.90
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SLE96 - Medium Radius Left Hand Point.....£19.00
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- SL-100 - Code 100 Flexible Track Wooden Sleeper.....£6.40 A Length
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SLE189 - Large Radius Left Hand Point.....£22.20
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SLE196 - Medium Radius Left Hand Point.....£20.50
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- SL-100F - Code 75 Flexible Track Wooden Sleeper.....£6.80 A Length
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SLU1196 - Medium Radius Left Hand Point.....£32.55
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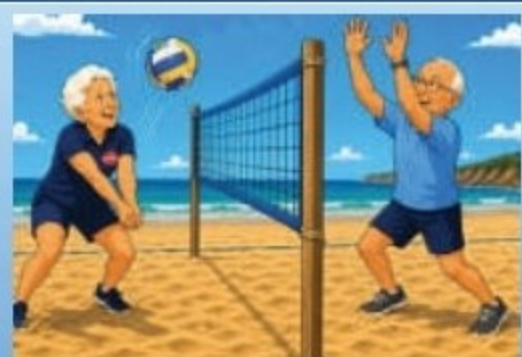
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PECO

N Gauge Code 55 Finescale  
Electrofrog Points

Table listing PECO N Gauge Code 55 Finescale Electrofrog Points with item names and prices.



Throughout the year we attended  
various exhibitions around the  
country. We are always happy to bring  
special orders to any of the shows we  
attend.

No exhibitions left this month.  
See next issue for forthcoming  
events

METCALFE

N Gauge Card Kits

Table listing METCALFE N Gauge Card Kits with item names and prices.

Due Soon/Latest Releases



N Gauge Class 56s - Due June/  
July

Table listing Dapol N Gauge Class 56s models with item names and prices.

New Announcements Available To Pre Order  
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TT Gauge Class 31s - Due April  
2027

Table listing Hornby TT Gauge Class 31s models with item names and prices.

TT Gauge LMS Black 5s - Due  
Early 2027

Table listing Hornby TT Gauge LMS Black 5s models with item names and prices.

TT Gauge HST Power Cars - Due  
September 2026

Table listing Hornby TT Gauge HST Power Cars models with item names and prices.

TT Gauge Class 37s - Due  
Throughout 2026

Table listing Hornby TT Gauge Class 37s models with item names and prices.

TT Gauge Class 50s - Due July  
2026

Table listing Hornby TT Gauge Class 50s models with item names and prices.

TT Gauge Mk1 Coaches - Due  
September 2026

Table listing Hornby TT Gauge Mk1 Coaches models with item names and prices.

TT Gauge Seacow Ballast  
Wagons - Due Early 2027

Table listing Hornby TT Gauge Seacow Ballast Wagons models with item names and prices.

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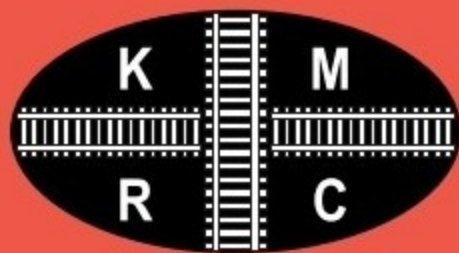
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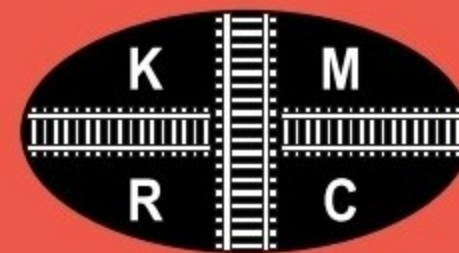
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SMT5 Double Pole Double Throw  
(On-Off-On).....£2.00  
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E85025SF Adams Class T3 4-4-0 558 SR Lined Olive Green 'E' Prefix.....DCC Sound £289.99

E85030SF Adams Class T3 4-4-0 No. 563 LSWR Drummond - as preserved.....DCC Sound £289.99

## LATEST ARRIVALS / OO

BACHMANN OO



31-995A LMS 1-Co-Co-1 Diesel No. 10001 BR Lined Green (Late Crest).....£178.46  
31-995ASF LMS 1-Co-Co-1 Diesel No. 10001 BR Lined Green (Late Crest).....DCC Sound £288.96



31-999A LMS 1-Co-Co-1 Diesel No. 10000 BR Black (Early Emblem).....£178.46  
31-999ASF LMS 1-Co-Co-1 Diesel No. 10000 BR Black (Early Emblem).....DCC Sound £288.96  
37-275H BR 27 Ton Steel Tippler Wagon No. B387939 BR Grey 'SAND'.....£19.51  
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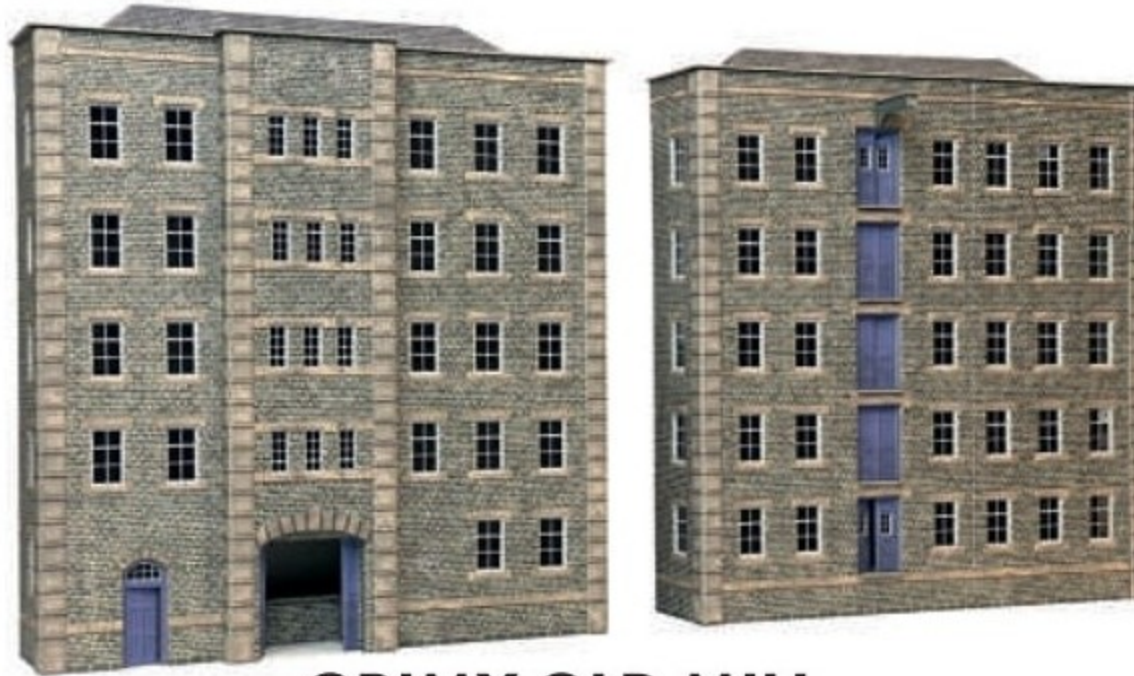
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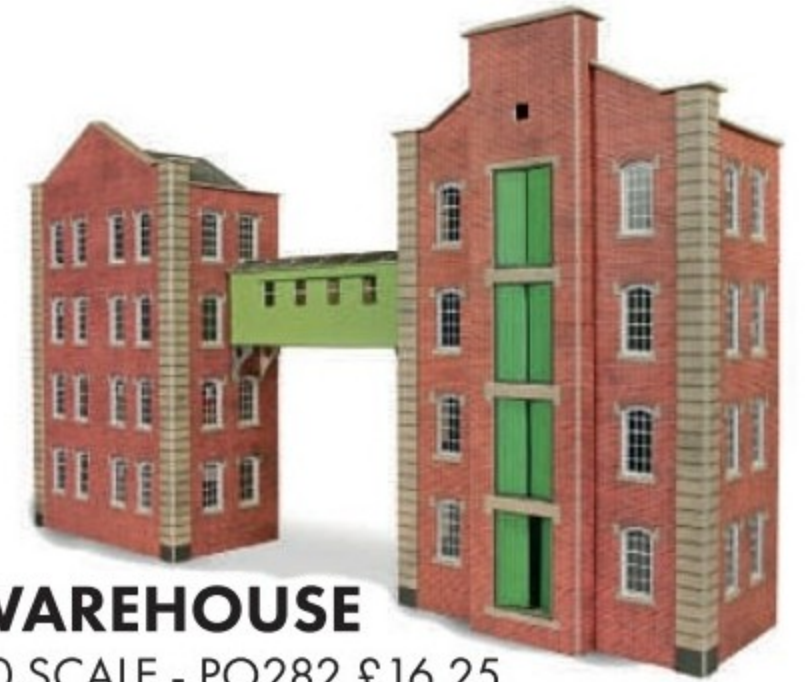
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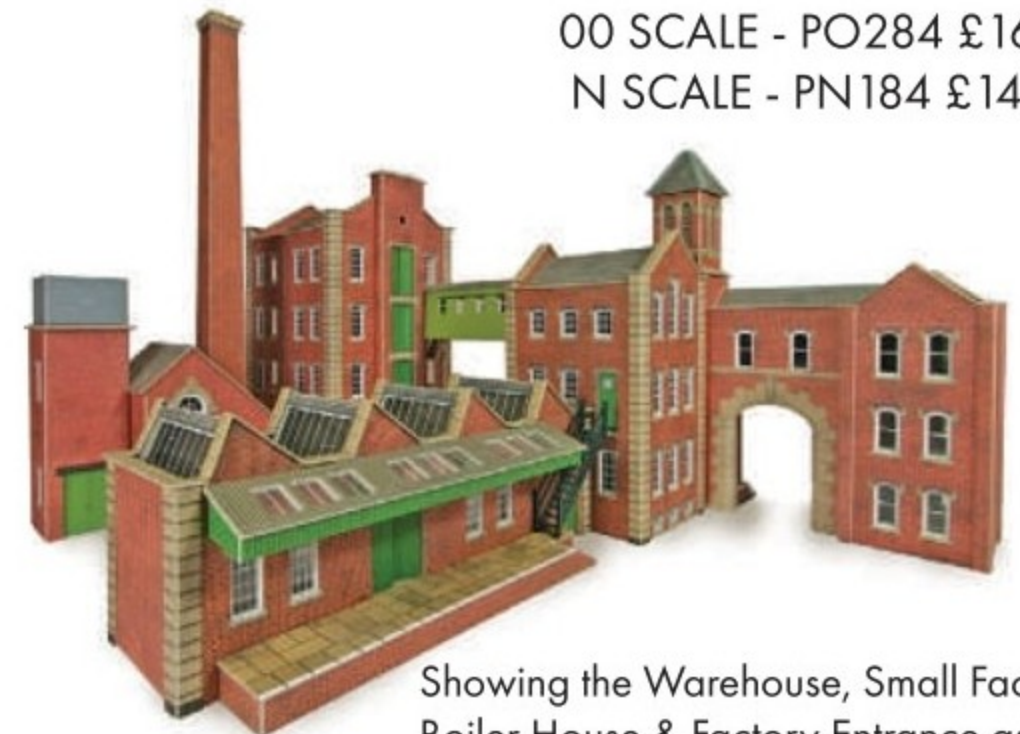
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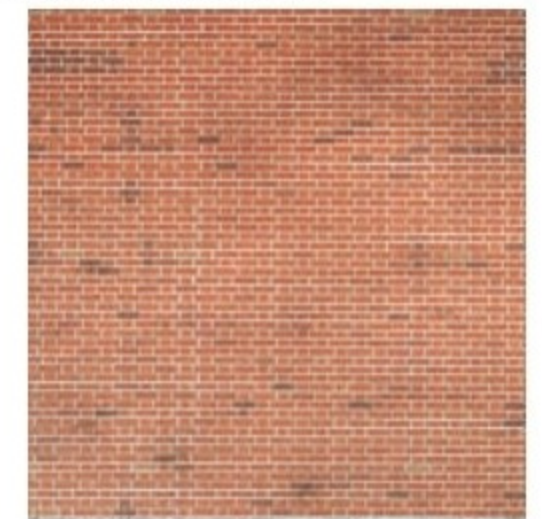
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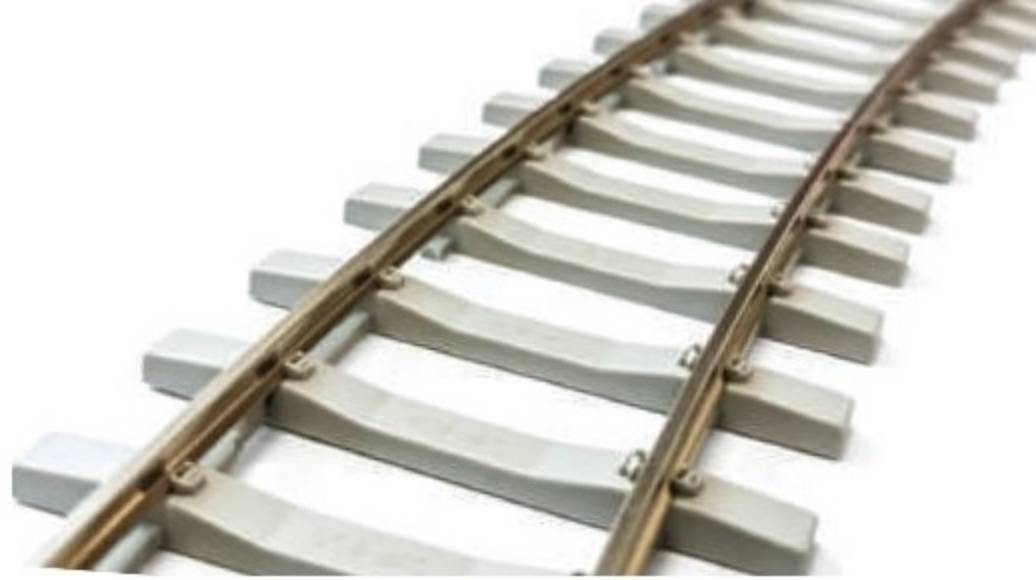


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Originally built as D6960 in January 1965, later renumbered 37260, it became notable for its Scottish service after allocation to Inverness in 1982. It hauled a wide variety of trains in the Highlands, including passenger workings on the Inverness to Kyle of Lochalsh and Far North lines, beginning regular services on the Kyle route from August 1983. It would also haul test trains and perform engineering duties, too. The name *Radio Highland* was applied at Dingwall station on July 7, 1984, to mark the launch of Radio Electronic

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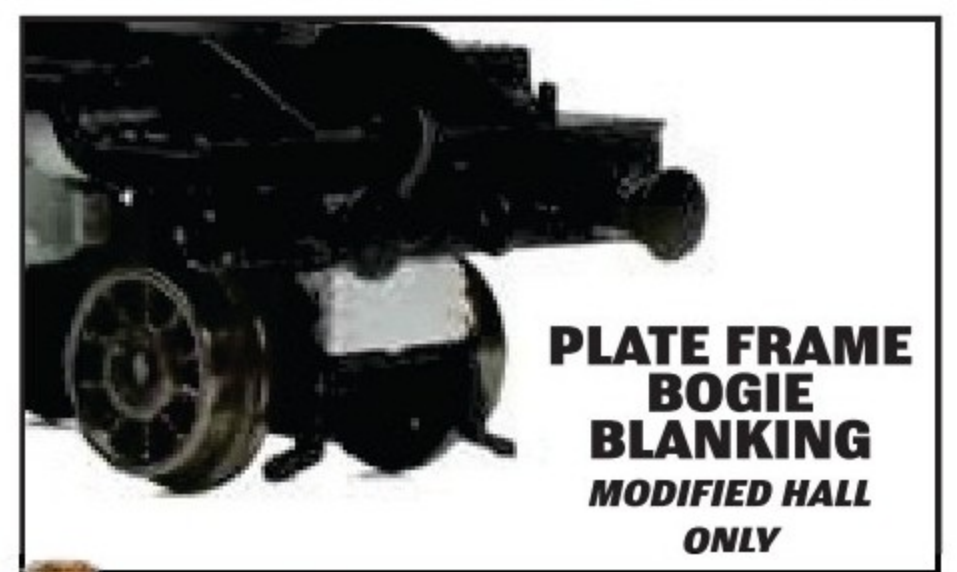
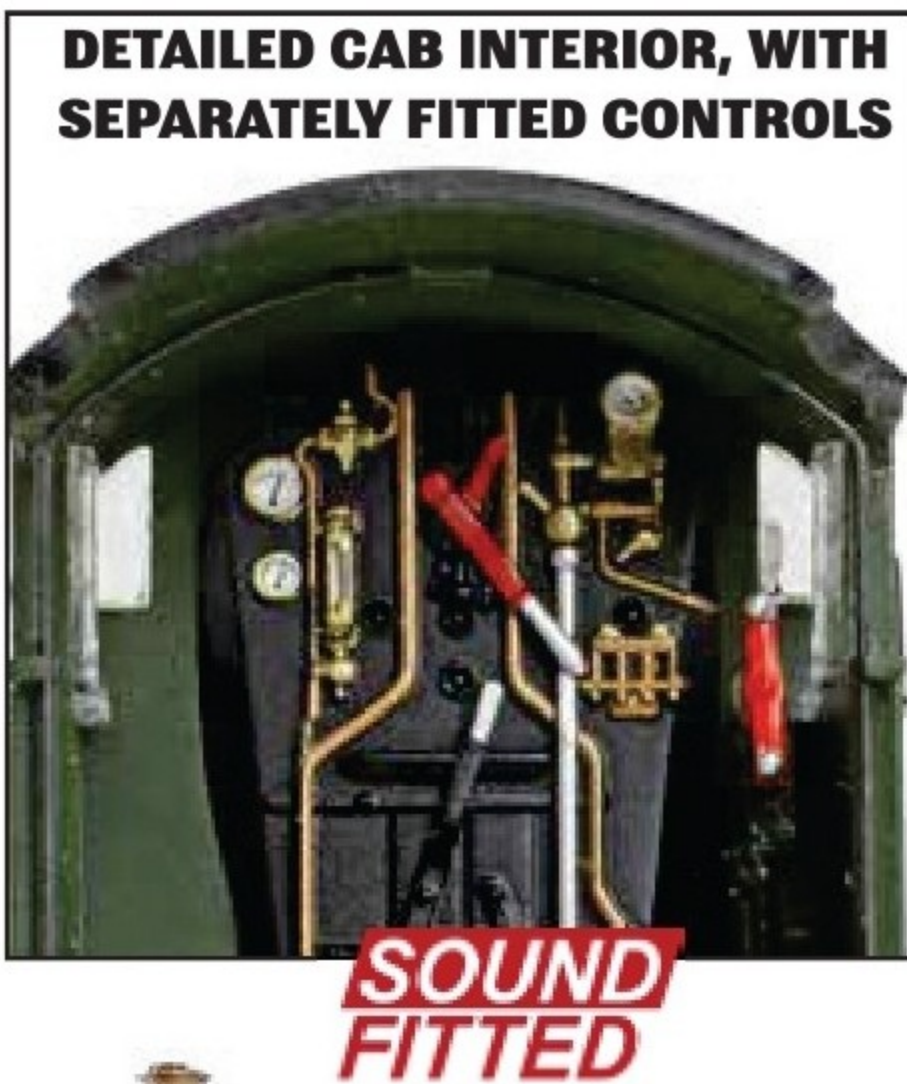


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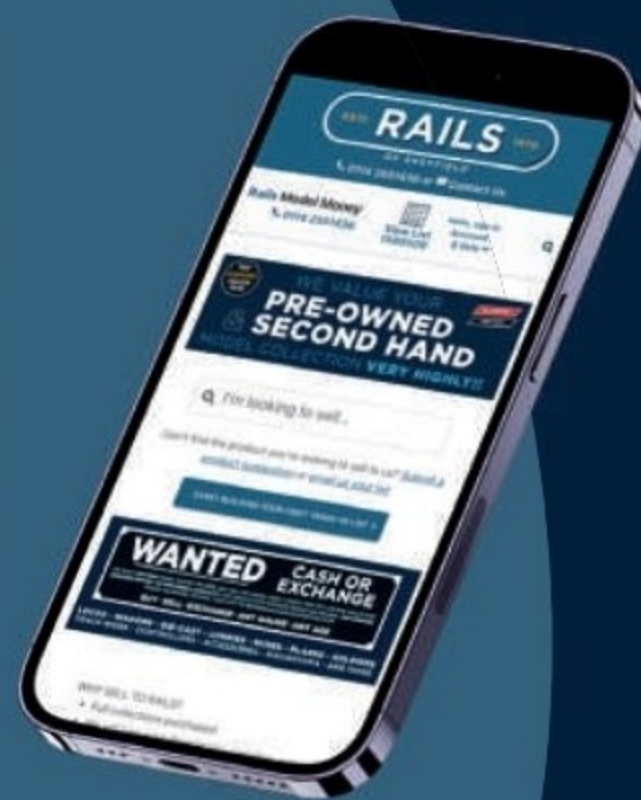
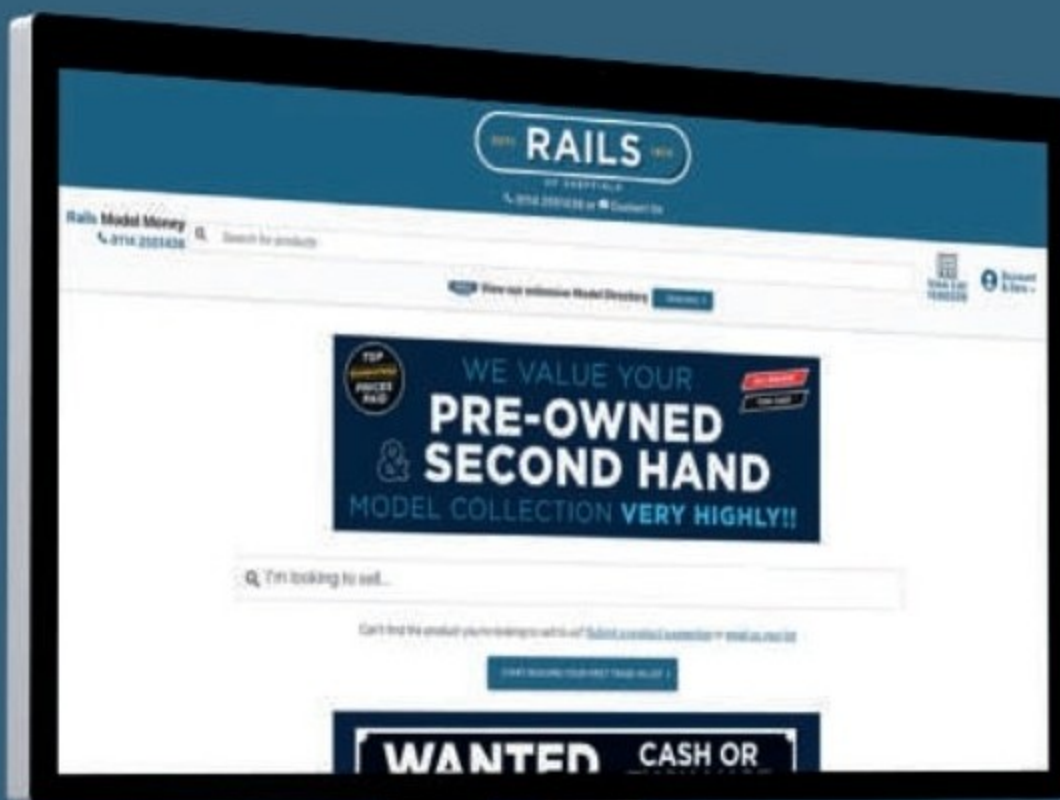
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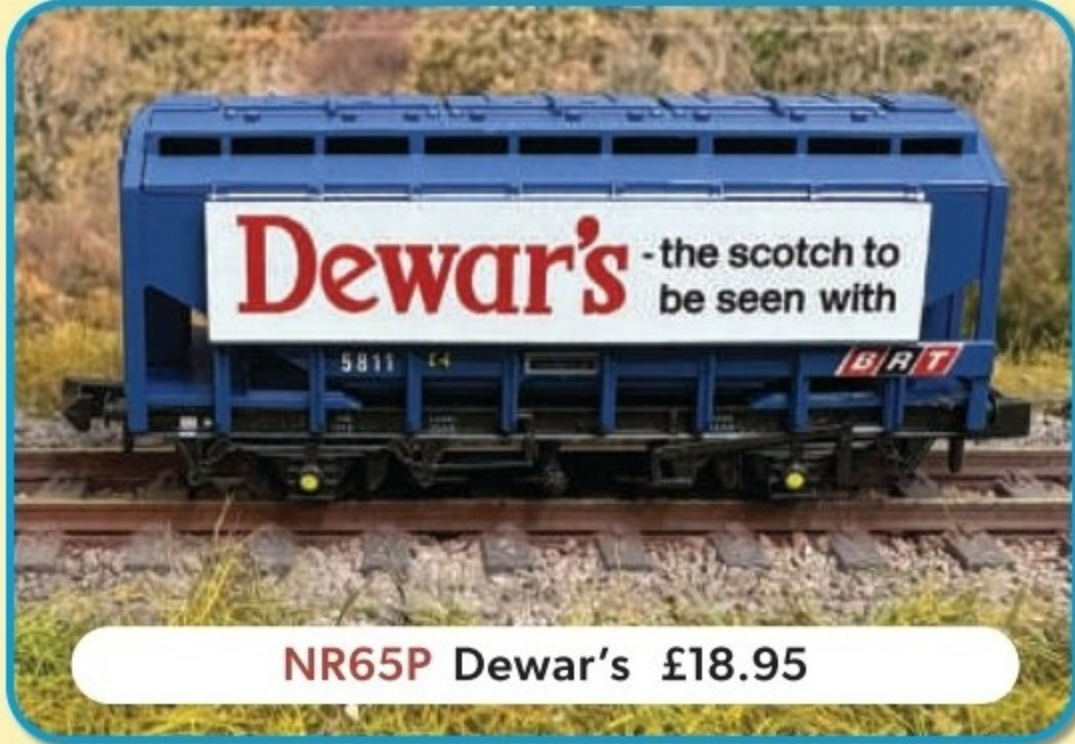
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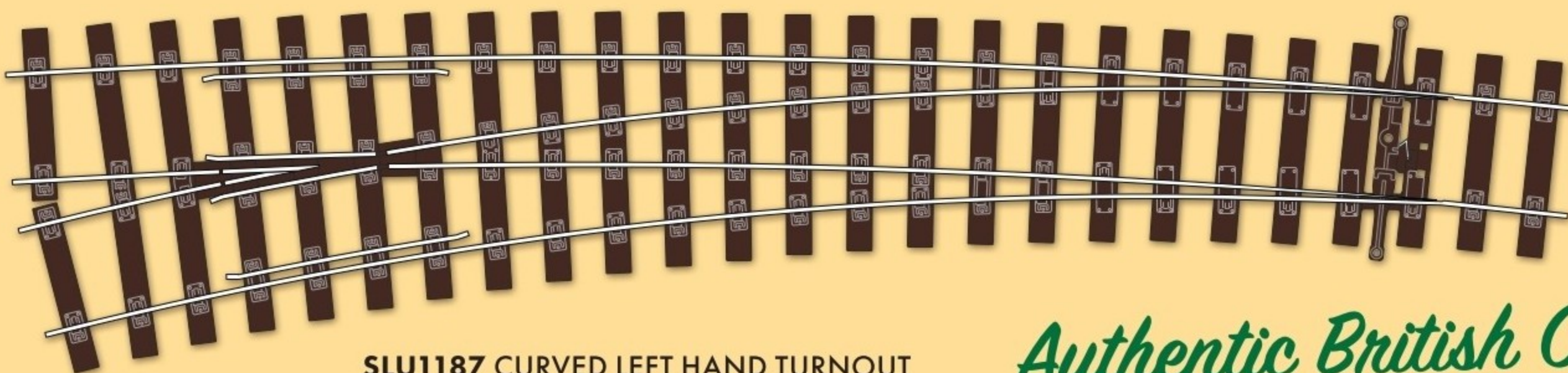
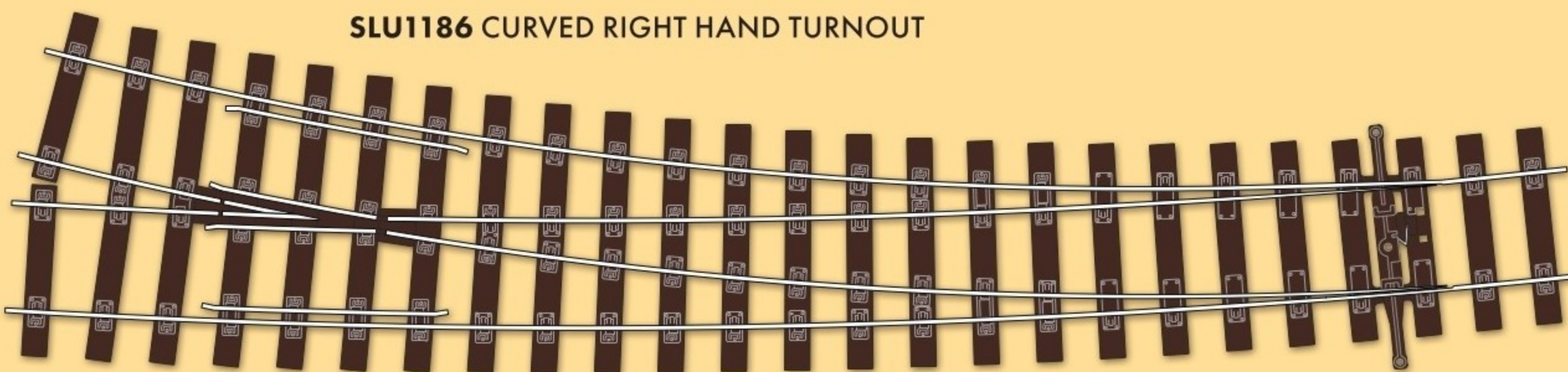
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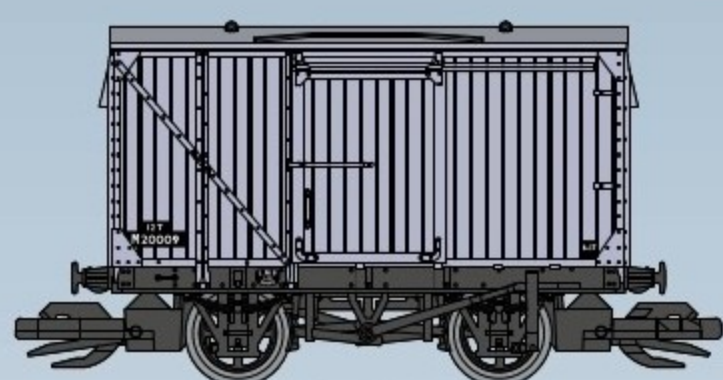
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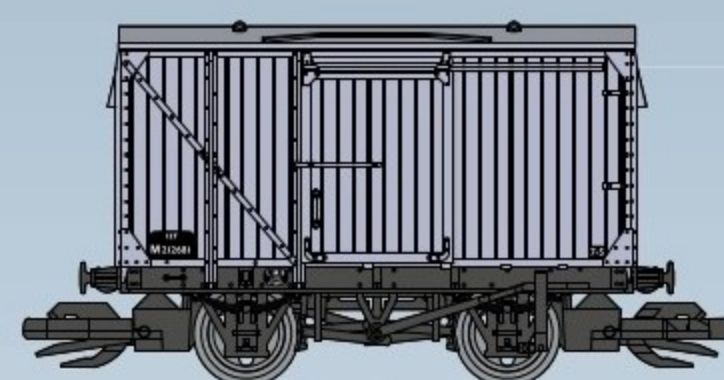
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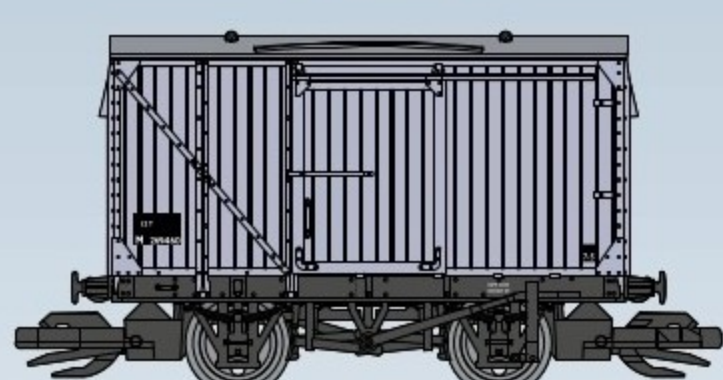
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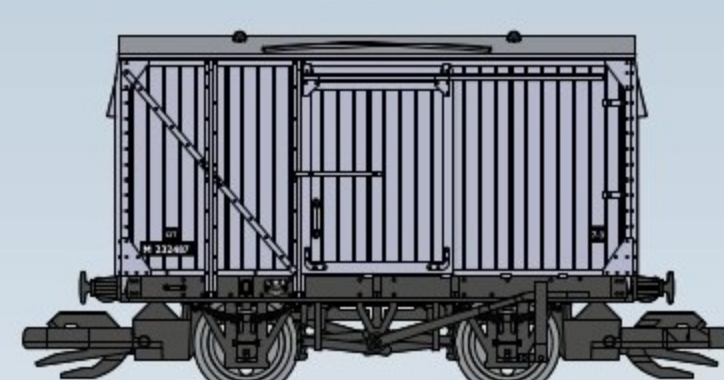
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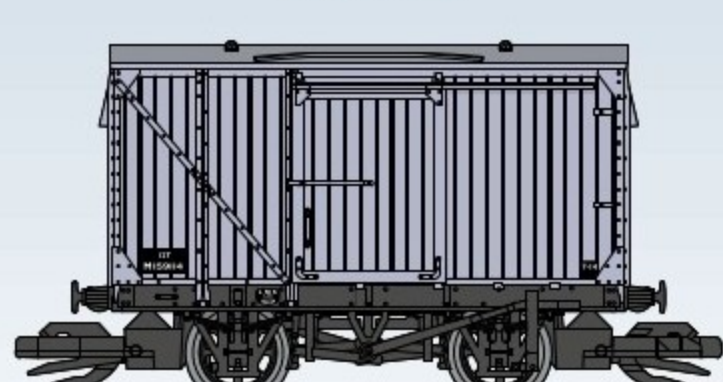
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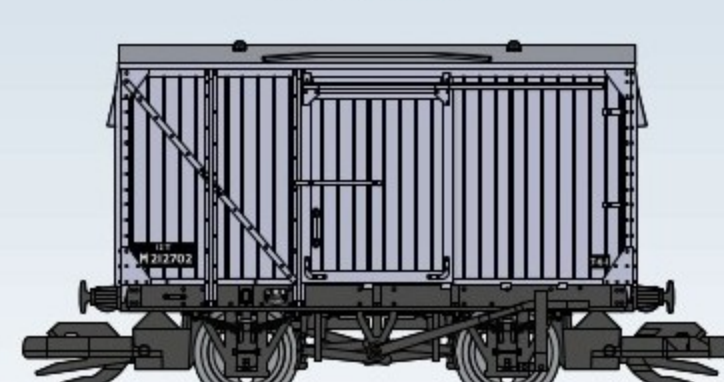
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# Welcome

## In search of the 'X factor'

**A**tmosphere: a special, intangible quality that many modellers strive to achieve with their layouts – but one that is also difficult to define and often proves altogether elusive.

For those in pursuit of this 'X factor', modelling a railway subject to precise scale dimensions, where everything replicates the prototype with painstaking levels of accuracy, is not a guaranteed recipe for success. Far from it, in fact: I have seen many layouts over the years that have excelled in terms of their technical accomplishment, but the finished works lacked that certain *je ne sais quoi*.

I can, however, bring to mind many other layouts that have managed, very successfully, to convey an unmistakable sense of atmosphere. But pinpointing the specific attributes or ingredients that contribute to this quality being achieved is far from easy. Is it having a cohesive overall feel, with a consistent level of finish and a controlled colour palette? Perhaps, or at least in part. But atmosphere is the product of more than careful observation and modelling with an artist's touch. It's also about capturing the *soul* of a place – the very essence of what it feels like to experience the real location.

For our Railway of the Month in this issue we present a layout that, for me, quite simply oozes with atmosphere – *Lydbrook Dean*. Its builder, Alexander Warren, has managed to capture the unique feel of its Forest of Dean setting, creating an enclosed and leafy landscape through which black panniers pant and wind their way along a rural freight byway. It doesn't take much to imagine you are really there, surrounded by the sounds of trees rustling gently in the breeze, accompanied by occasional locomotive whistles echoing through the forest and the squealing of wheel flanges on seldom-used rails. The fact that the model is not based on a specific Forest of Dean prototype makes what Alexander has achieved all the more remarkable.

### Gresley 150 to be marked at AIMREC on *The Gresley Beat*

Following on from last month's RM, which celebrated 150 years since the birth of Sir Nigel Gresley, Ashford International Model Railway Education Centre has confirmed its own programme of commemorative events. Taking place at its recently opened Bethersden exhibition building on Saturdays throughout July (4, 11, 18 & 25), the OO gauge layout *The Gresley Beat* will be in operation, with examples of Gresley motive power carrying special 'Gresley 150' headboards. The layout, which was the brainchild of AIMREC leading light Cliff Parsons, was featured in the December 2013 RM and went on to win that year's RM Cup competition. The July dates will be the first occasions that the layout has been run in full exhibition mode since becoming part of the AIMREC collection. For further details about AIMREC, go to:

[www.aimrec.co.uk](http://www.aimrec.co.uk)



**A Hawksworth '1600' 0-6-0PT  
arrives at Lydbrook Dean.**



**Craig Tiley** Editor

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**And for all enthusiasts modelling overseas railways.**

## CONTINENTAL MODELLER

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# Railway of the Month

Photography by Craig Tiley



# Lydbrook Dean

With this portable exhibition layout, **ALEXANDER WARREN** has captured the rustic atmosphere of the freight-only branch lines of the Forest of Dean in the later British Railways steam era.

**T**his layout was conceived to be transported to exhibitions in the family car. At the time I had built, and was operating, a model of Yelverton station set in the late 1950s. I wanted something a bit smaller and faster to erect, and to simplify stock handling. A freight-only branch in the Forest of Dean operated exclusively by pannier tanks fitted the bill. The layout currently has four grubby black panniers – all slightly different. Let no one tell you Great Western Railway engines are all the same. Pannier tanks are an absolute minefield! Riveted tanks, non-riveted tanks, top feed or the absence of top feed (often on the same

engine at different points in its career), as well as two types of cab – and that's just for starters...

## Construction

The layout is 8' long and folds in the middle. The folded length of 4' fits on the back seat of the family car. I wanted to keep the height of the folded layout below the height of the front seat backs. This meant that there was a restriction on the height of the boards themselves. I gained a bit of space here by using Cobalt point motors instead of my usual Tortoise motors, which allowed me to reduce the height of the layout framing by about 1cm.



**Left**  
Hawksworth '1600'  
0-6-0PT No.1623 (a  
Rapido Trains UK model)  
shunts Grampus and  
Dogfish ballast wagons in  
Whitehill Quarry.

**Right**  
Another pannier tank –  
Collett '8750' No.3737 –  
passes the diminutive  
signal box controlling  
Lydbrook Dean yard.



There are legs at each end which plug into the baseboard and are retained by a single bolt. The bottom half of each leg slides into the top half to allow them to fit into the car boot.

I am a believer in the high level display of layouts, with wings, a proscenium and integral lighting – ideas first discussed in the writings of Iain Rice, I believe. The wings and proscenium ‘frame’ the layout and are made from ply. The wings plug into the layout and the proscenium sits on top of them as well as folding in the middle for transportation. L-shaped struts, which bolt into the rear of the layout, support the rear lighting gantry. The centre strut projects forward to provide a support for the centre of the proscenium as well. The lighting itself uses LED tube lights purchased from B&Q. An important reason for having integral lighting is that the exhibitor can control the light a layout is displayed in without having to rely on the lighting in an exhibition hall, which may be inadequate.

The boards are constructed from 78mm x 18mm softwood frames with 6mm ply tops. When opened out,

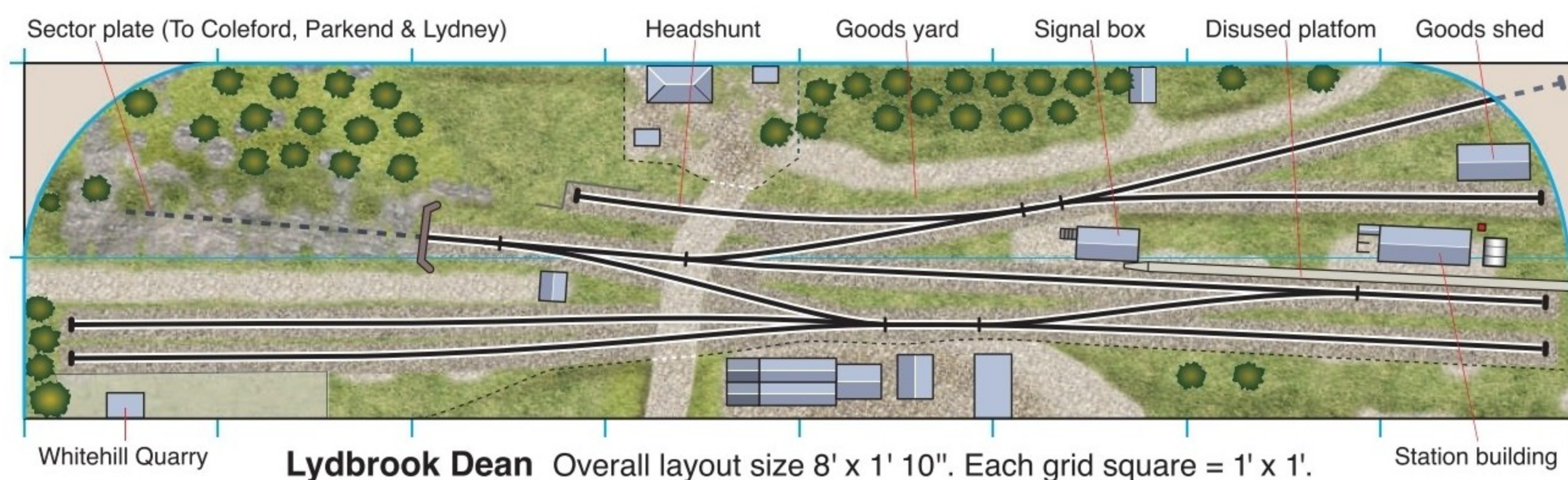
the boards are held together with a pair of M8 bolts at the centre and back of the layout, and an M6 bolt at the front. Two hinge blocks are fitted at the joint to give clearance between the folded boards. The one at the back is fixed in place, and the one at the front is removable. When the boards are opened out, the block is removed, and the resultant hole is covered by a removable piece of scenery and a lean-to building at the end of the pub, which simply slots in place. The fixed block at the rear is covered by the crossing keeper’s cottage.

### Track, signalling and stock

The track plan owes its origin to Iain Rice’s plan *Elan*, which I have used a number of times in different forms. Essentially, a truncated station is fed from a sector plate, which provides a kick-back to a single cassette behind the scenery.

With the exception of the three-way point, the track is OO gauge Peco code 75 bullhead, laid on DCC Concepts Legacy underlay. The three-way point was hand-built

**Above**  
Class 14 diesels, such as D9553 seen here, replaced the panniers on the Forest of Dean goods-only branches for the final few years before most of these lines closed altogether.





**Right**  
A pannier tank arrives at Lydbrook Dean with a short train of coal wagons. The signals were built from Model Signal Engineering parts.

using copper-clad sleepers about 25 years ago. When it was installed on *Lydbrook Dean*, I added cosmetic chairs, using C&L chairs. This improved the appearance no end, and gave a better match to the Peco track.

The track was pinned in place initially to check the basic alignment. When I was satisfied with this, the track was glued down using PVA and tested, before painting and ballasting. Any mistakes at this stage can be corrected by wetting the track with water, which will soften the PVA, allowing the track to be lifted. Once

satisfactory, the track can be painted. There is no quick way of doing this, but the results are extremely satisfying. I prime the track, having first masked off the tiebars and blades on the points. The track is then painted with Railmatch Sleeper Grime, and the rails and chairs are painted with Mig Old Rust. The chairs and rails are then dry-brushed with Mig Dark Rust.

The points are operated by Cobalt motors. The signals are built from Model Signal Engineering (Wizard Models) components and are servo operated.

## Block hinge arrangement

1



**Above**  
The two scenic boards are transported as a single unit, a pair of central block hinges enabling the boards to fold together.

2



**Above**  
With the boards folded out, the front block hinge is removed, whilst the rear one remains in place.

3



**Above**  
The crossing keeper's cottage is placed over the rear hinge to hide it from view.

4



**Above**  
A small drop-in scenic module, and a lean-to building on the pub, are added to fill the hole at the front.

Motive power consists of Bachmann and Rapido panniers, and the freight stock is a mixture of Bachmann, Hornby, Rapido and kit-built. As the line is freight-only, there is no passenger stock, except for a '6400' and auto-coach which may occasionally appear on specials.

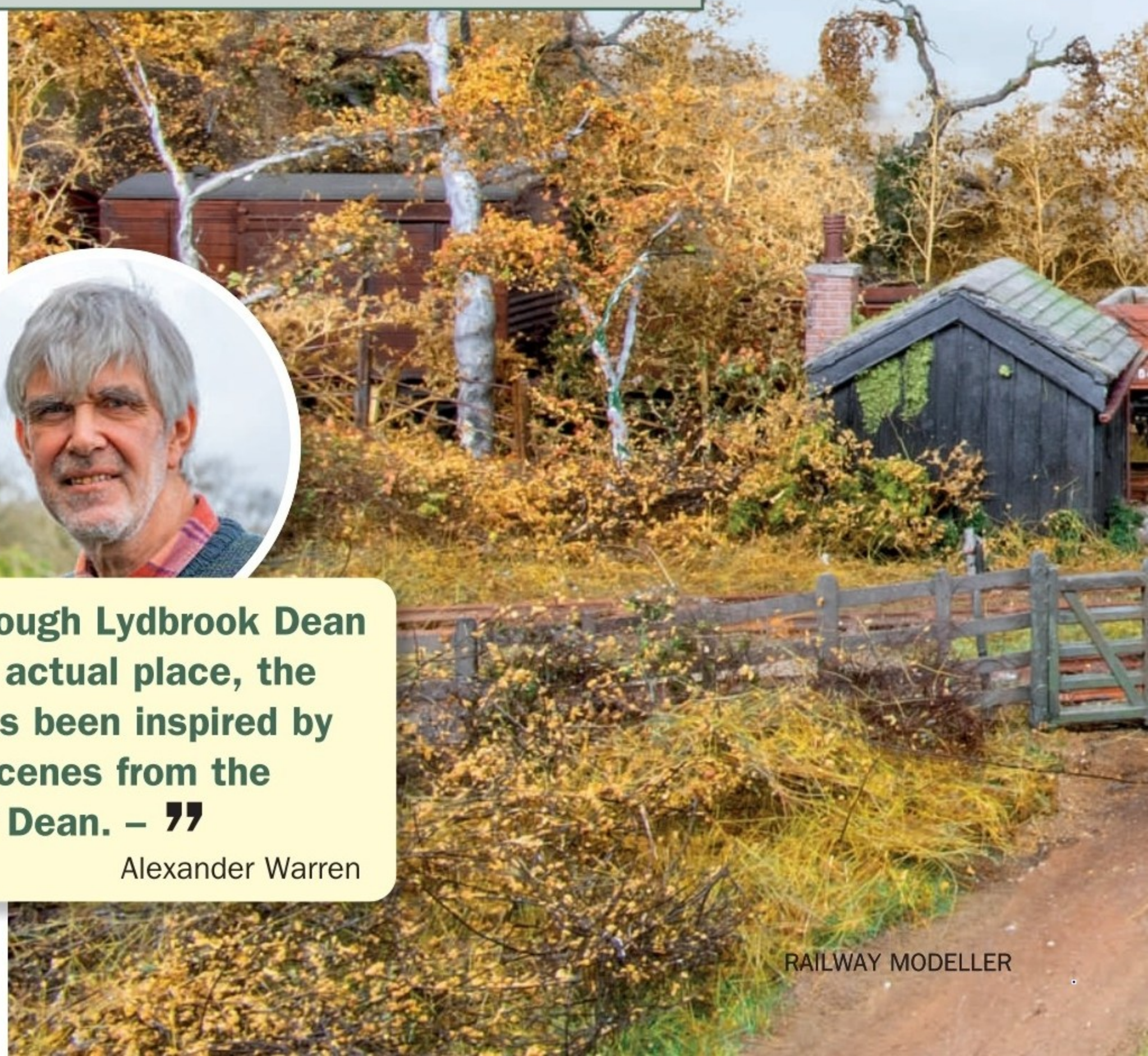
### The quarry

Many visitors to the layout have asked how I made the rocks in the quarry. I started with a profile piece of 5mm ply, fixed in place on the board. Sheets of polyurethane foam were then glued to the profile board with PVA to give the effect of a rock face carved away in slabs as the quarry was worked. This was then covered with various mixes of Woodland Scenics Lightweight Hydrocal Plaster and ordinary plaster of Paris. The advantage of the Hydrocal is its light weight, which is an important consideration for a portable layout. In places, rocks cast from Woodland Scenics rock moulds were pressed into the wet plaster. When all this was dry, the face was carved with various modelling tools, dental probes



“ – Although Lydbrook Dean is not an actual place, the layout has been inspired by various scenes from the Forest of Dean. – ”

Alexander Warren



**Right**  
The disused Lydbrook Dean station building slumbers quietly, decades after hosting its last passengers.



and old darning needles to blend the rocks together and highlight some of the individual rocks. Obvious runs and curves were removed at this stage. I then went over the whole face with a wire brush, using horizontal strokes to introduce the effect of strata.

The rock fall was made by first building a small mound at the bottom. 'Rocks' of various sizes were allowed to roll down the area of the fall and collect at the bottom. I started with the larger rocks, followed by some cat litter, granite ballast and finally some Tremendus Scenics Earth Powder. The larger rocks can be made by putting the dried unused plaster

from a plaster mix into a plastic bag, and hitting it with a hammer while on the garage floor. This breaks the plaster into rock-like pieces. The whole was then sprayed with Scenic Cement from Woodland Scenics and allowed to dry.

The rocks were coloured by a series of acrylic washes, applied with a sponge. There is no need to let the paint dry between washes. Initially I used Woodland Scenics earth colours. The technique is called

**Below**  
'1600' No.1631 passes the crossing keeper's cottage, which hides the rear baseboard hinge.





**Left**  
No.3737 shunts a rake of Dogfish hoppers into Whitehill Quarry for loading with ballast. The George pub, like other buildings on the layout, is based on a real example in the Forest of Dean.

**Right**  
Specially printed transfers were used to letter the lorries with the name of the fictional Whitehill Quarry.

'leopard spotting'. The first washes were yellow ochre over about a third of the area, and burnt umber over another third. The remaining third was left unpainted. The whole rock face was then given several washes of different greys. Once it was dry, some of the deep fissures were given a further coat of dark grey – almost black. The rocks were allowed to dry, and then dry-brushed with some lighter colours such as light grey and yellow ochre. Lichen was represented in places by stippling with white and yellow ochre. The rocks were further highlighted by brushing with weathering powders. Wet patches were created using Woodland Scenics Realistic Water, which was poured down the rock faces to find its own way to the bottom.

### Forest fact and fiction

Although Lydbrook Dean is not an actual place, the layout has been inspired by various scenes from the Forest of Dean. The goods shed is from Coleford, the pub is at Fetter Hill between Parkend and Coleford, and the crossing keeper's cottage is at Soudley. The quarry was inspired by the real Whitecliff Quarry, though I wasn't able to go to the Forest while building the model, so research was done at Kit Hill in Cornwall! The quarry at Lydbrook Dean is called Whitehill. I was able to get some transfers made up with the name, and this appears on a couple of the lorries in the quarry.

There are only a few figures on the layout. I may add a few more over time, but I want to give the impression of

**Right**  
With few figures and overgrown track, the layout captures the feel of a remote and run-down outpost of the railway network.



a slightly remote place, and too many figures would detract from that. I avoid figures in 'action' poses, so you won't find a fireman swinging a shovel or a passenger running for a train.

I have tried to get a flavour of the area upon which the model is based, and have resisted the temptation to add details simply because they are items that are of a

Great Western design. For example, I haven't felt the need to add a locomotive shed, often considered a typical GWR branch line feature. My previous layout, *Yelverton*, was a model of a prototype, and I found the experience quite liberating. I just put in what was there, nothing more, nothing less. I have taken a similar approach with *Lydbrook Dean*.



# Modelling Class 121 & 122 'bubble cars'

**WILL THOMPSON** showcases his collection of first generation single-car units in EM, created by enhancing, detailing, and in some cases repainting, proprietary models from Bachmann and Dapol.



Model photography by the author

Who doesn't like a humble 'bubble car'? Being so small and useful, they appear on many a layout, in particular those based on branch lines. They managed to survive a long time in traffic, with the Class 121s being the final first generation Diesel Multiple Units in regular passenger service, lasting until 2017 with Chiltern Railways. Their 57 years of service resulted in many liveries being applied, making them a fun subject for modelling.

They weren't confined to branch lines, and saw regular use on the main line, as well as being particularly useful to bolster existing services or replace a faulty power car on other first generation units. This was noticeable in the late 1980s and early 1990s, with many colourful and varied hybrid combinations being observed. Also, many examples ended their days in Departmental, route learning or Sandite use, providing more variation and liveries to be modelled.

## The prototypes

The Class 121 units were built by Pressed Steel, and were essentially the same as a Class 117 Driving Motor Brake Second,

but with an extra cab resulting in a smaller guard's area. The first units were powered by two AEC (BUT) 150hp engines, but these were later replaced with Leyland bus engines. Initially, all units had roof-mounted four-digit headcode boxes, which were plated over when this system of train identification was abandoned by BR. These boxes, however, meant the exhausts needed to be altered into their distinctive 'antler' style rather than being straight. The class was included in a refurbishment programme, with the main external difference being a grille appearing under the guard's window.

They have been popular with preservationists, with 10 units still in existence today. A few even returned to passenger use on the national network, as Nos.55020 and 55034 were purchased from preservationists by Chiltern Railways for use on the Princes Risborough branch, and No.55032 by Arriva Trains Wales for the Cardiff Bay line.

The Class 122 is a very similar unit to the Class 121, but was built by Gloucester Railway Carriage & Wagon Co. with a few notable external differences. The main ones were a Derby cab profile, less noticeable window

▲ Class 121 No.55026 (left) and Class 122 No.55016: the two are distinguished easily by the different headcode or destination boxes.

frames, shell roof vents, different handrails for the guard's compartment, and no handrails around the cab front. The Derby cab profile resulted in different positions for the destination displays, these being on the roof rather than in the middle cab window as on the Class 121. Two-character route indicator displays were mounted below the middle cab windows, though in later years these indicators were plated over. This arrangement allowed more conventional straight exhausts, although the first design included a small exhaust box above the destination box, which was eventually dispensed with. The Class 122s contained blue asbestos, so the units weren't intended to be refurbished and were expected to be withdrawn early, although they did survive in passenger service until the early 1990s.

## Models in 00 gauge

In the past, Lima produced a Class 121, which isn't a bad model but has many dimensions which are incorrect. The model was subsequently taken on by Hornby, which improved the

mechanism but not a lot else, though the result is still a fun model to own. The newer versions from Bachmann and Dapol are a big improvement: both capture the prototype well, and so form an excellent basis for modelling.

Only Dapol has produced a Class 122, which shares the same body as its Class 121 but with a different roof. The removable roof is a useful feature when modelling first generation units. This approach results in a few short cuts, as the Class 122 shouldn't have prominent window frames (which I've ignored), and the refurbished Class 121 units only have printed grilles under the guard's windows, whereas Bachmann has included these in its moulds.

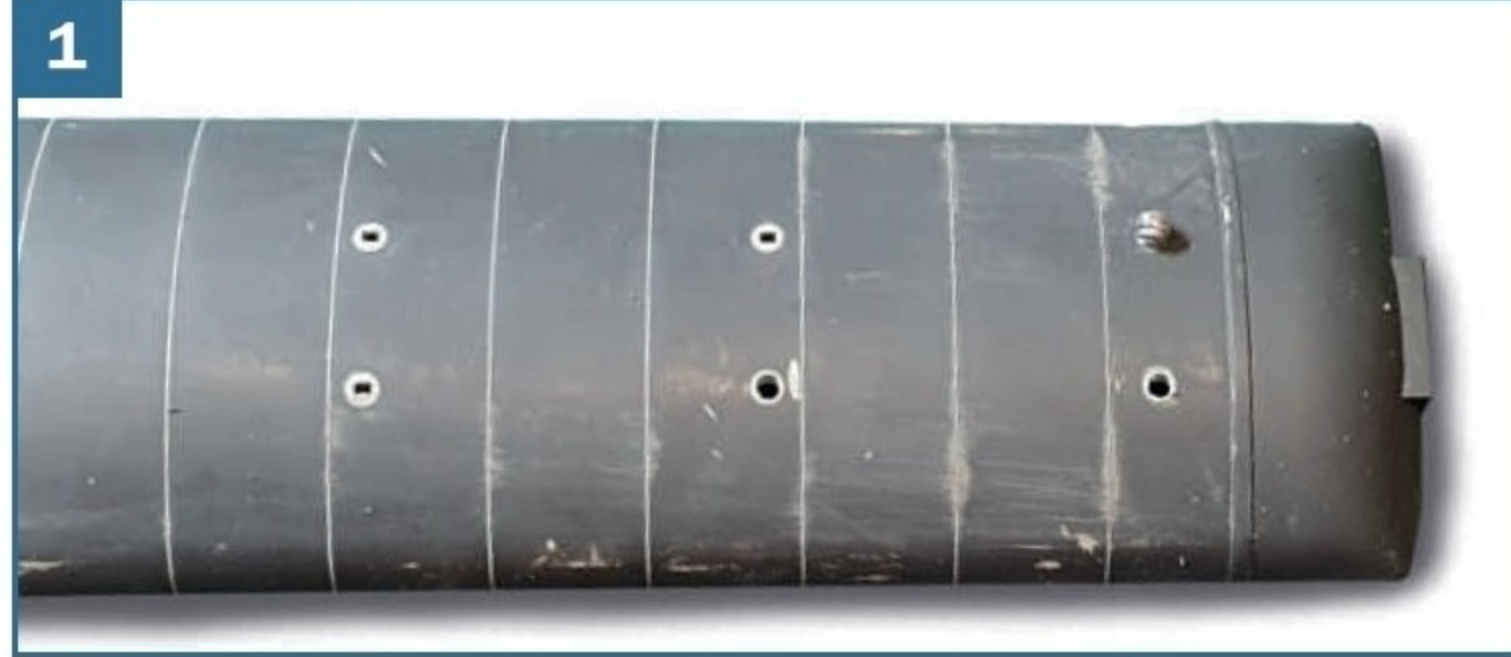
It's perhaps worth mentioning that I've had reliability issues with the Dapol chassis. In all cases, the problem has been the same, with the outcome being a model that either is extremely slow or doesn't move. The problem is that the bearing pivot which fits into the flywheel is made of plastic and, due to the powerful motor,

either splits or loses its grip on the metal transmission shaft. Replacing these, or burring the transmission shaft and then gluing the bearing pivot on, solves this. In the end, I decided to go with the Bachmann model for the Class 121 and use a Bachmann chassis under a Dapol Class 122 body. The fact that the Bachmann chassis is much easier to convert to EM gauge made my decision easier.

All of the chassis have been detailed, using the same approach outlined in my previous article on Classes 116 to 118 (RAILWAY MODELLER May 2025). One of the benefits of standardisation is that the approach undertaken here is the same. Other than the Regional Railways Class 122, all of the other models that were renumbered have had their transfers drawn in Inkscape and then printed as dry rub-on transfers for me by 4D Models.

### The Class 121 body

The Bachmann model doesn't need many changes, with the main improvement being to the cab windows by thinning the



window frames to give the impression that the cab fronts are thinner than the plastic shell, and finally fitting them with Shawplan Laserglaze.

### The Class 122 body

The Dapol model benefits from a few easy improvements, and it helps to remove the roof which is just clipped in, although it needs a little persuasion to part with the body as the internal clips are difficult to remove. With the roof removed, the over-prominent roof welds are reduced using a flat blade knife, and then tidied with wet and dry paper. I also change the roof vents to MJT shell whitemetal castings, as the originals look a bit small (photo 1). The roof is

then primed using Halfords grey primer to check it all looks OK, and then painted. I've started to brush-paint acrylics on with a sideways motion using a large flat brush to provide a bit of texture, which is then weathered using an airbrush. The roof destinations are printed onto normal paper using my computer, and these are then sandwiched between two pieces of clear acrylic shaped to fit. The Bachmann top circuit board has space for soldering an SMD warm white LED resistor, which illuminates the destinations nicely.



Parts of the Dapol body interior must be removed to fit the Bachmann chassis with lighting.

In order to accommodate a Bachmann chassis with interior lighting, I removed quite a bit of the interior support structure of the Dapol Class 122 body, but as I ultimately glued the roof on, it remains sturdy (photo 2). I spent a while trying to think of a clever way to attach the body to the chassis, but in the end opted for simple sticky Black Tack™ wedged between the body and chassis. As the Bachmann chassis is so light, I've not experienced any problems or shifting of the bodysells, so I'm keeping this low-tech solution.

## Class 122 No.55000 (122 100) – 1990s Regional Railways



No.55000 ended its BR service in Cornwall (allocated to Laira in Plymouth) and was often used on the St Ives branch. While allocated to Laira, the unit (alongside No.55012) was painted into a rather smart Regional Railways livery to help encourage more travellers. The Class 122s arrived in 1987 to cover for the poor-performing Pacers until the then-new second generation DMUs became available. This took longer than expected, and so No.55000 managed to last at Laira until the end of 1993, and happily has been preserved.

The model purchased had a broken exhaust, but as the prototype ended up with an exhaust straighter than the standard, it needed changing



anyway. I fabricated this from 0.9mm brass tube, then soldered resistor legs to give an impression of the brackets. Once filed flat, I think they're passable (inset).

I find the yellow chosen by Dapol on these models to have a lemon shade, and so this has been resprayed using Phoenix Precision warning yellow post-1985 enamel paint. The window frames were then painted black, which I find quite challenging! After completing my general Class 122 updates, dry rub-on transfers from DRD Transfers (Will's Workbench) were applied. This included new Regional Railways transfers, as the Dapol version seems too low on the body side for No.55000. I prefer dry rub-on transfers for numerals, as no carrier film is present, and I tend to get mixed results with waterslide transfers. The body was then sprayed with satin varnish and weathered.





Inspiration for my pair of 1970s 'bubbles' came from researching the Looe branch for a possible project set in that decade. Both of these units had stints on the branch at the time, and as they were in overall blue, I felt I had a chance at repainting models into this livery!

I used BR green examples for both projects, due to needing the indicator headcodes, and the Class 121 to be a non-refurbished example. The bodies were stripped in IPA, which is very effective if they are left to soak in it overnight, as the paint simply brushes off in the morning.

In order to modernise the Dapol Class 122, new exhausts were needed, which were acquired as spares and, prior to fitting, were opened carefully to suggest a pipe. I felt the indicator openings looked a bit on the small side, so these were removed, then opened out as well as thinned to fit more closely to the body. Extra SMD LEDs and resistors have been soldered to the Bachmann circuit boards, to illuminate the route indicators on the cab front as well as the roof-mounted ones. The route indicator blinds themselves were drawn in

Inkscape and then printed onto standard white paper. I like to model headcodes off-centre, as I feel this implies a little bit of motion, and it is reflective of many of the prototype photos I see. I weathered them with watered-down brown acrylics (much like using watercolours) to tone down the white characters. No.55026 sustained some damage to the cab corners, which I've replicated; strangely, in the same photo, it's missing a destination box in one of

the centre windows, so I couldn't resist removing one!

Both units were primed with Tamiya flat white rattle can primer, which I find excellent, although it's worth practising on something that matters less first. I spray yellows first, and after many attempts, I now tend to mix my own shades from various manufacturers of pre- and post-1985 yellows. This results in each model being slightly different, and in this case, No.55026 is more

orange than No.55016 to reflect the reference photos I was using. Admittedly, matching the colours in photos is fraught with challenges, which are beyond the scope of this article and my technical ability! The BR blue is by Rainbow Railways, which matches the latest shade from Bachmann. Once my transfers were applied, an overall coat of Railmatch satin varnish mixed with some gloss was applied, then a little weathering to finish.



### Class 121 No.55033 – BR blue & grey

This unit had an accident in 1980, when it ran through the stop blocks at Severn Beach station. The resulting damage required replacement of the roof headcode box with a spare Derby-style cab roof dome from a withdrawn vehicle. The unit therefore became unique, having different cab roof domes but retaining Class 121 exhausts and handrails. It was also refurbished around the same time, and was released back into traffic in BR blue & grey livery, being based at Cardiff. While at Cardiff, No.55033 received red bufferbeams and Welsh Valleys insignia, which were only partly removed when it was transferred to Laira in late 1985. It is in this



condition that I have modelled the unit, using a BR blue & grey Bachmann Class 121.

The only tricky element to the project is to remove the roof headcode box using a saw, and then graft on a Dapol Class 122 roof dome without damaging the exhausts. This was done in the

same way as my Class 116 builds (see RM May 2025). Any gaps were filled using Tamiya white filler, then filed using wet and dry fine sandpaper with water. Once finished, the roof was primed using Halfords grey primer and painted using a weathered black colour, which is pretty dark grey.

Having remnants of an older livery meant I removed all of the existing numerals using T-Cut applied with a cotton bud, then drew all of the insignia in Inkscape. It's pleasing to model something unique, despite the unit only lasting nine months or so in my preferred modelling area.



▲ The real No.55033 draws to a stand at Stapleton Road while working the 12:22 Severn Beach-Bristol Temple Meads service on 29 October 1980. Photo: John Chalcraft/Rail Photoprints



### Class 121 No.55020 – GWR 150 chocolate & cream

I think the livery application on the Kernow/Bachmann GWR 150 specials is excellent, and judging by many of the photos I've seen of No.55020, it often ran with either a Class 121 Driving Trailer Second or a Diesel Parcels Unit. As I was looking for a unit to run with my Class 121 DTS conversion, this release was ideal.

Bachmann has chosen to use smooth roofs and not include roof welds, which looks fine on those with dark roof colours. However, with such a light coloured roof that weathered easily, I've attempted to give an impression of the welds. This has been achieved by firstly priming the roof, then carefully masking the outlines of each roof weld (inset), and then spraying a dark grey paint fairly thickly. Once

the masking tape was removed, the outlines of the welds were allowed to dry, and the final greyish colour was sprayed over the top. I then weathered the roof with a combination of weathered black and some brown, which was



painted thinly, panel by panel, using a large brush in a sideways motion. Hopefully this highlights the welds! When all this had dried, a final blast with the airbrush, of roof dirt for some shading, and exhaust dirt, finished the weathering.

## Class 121 No.55024 – Network SouthEast revised blue



This rather nice and colourful 'bubble' didn't have many changes, other than repairing the exhausts using a splint (a piece of wire) and adding more transfers from the Railtech range for NSE branding, route symbols and destination blinds. When released from overhaul, the

unit had silver buffers, which quickly became dirty. I've replicated this by using thinned Humbrol Metalcote silver painted around the edges on a matt black base. I find this unit to be a nice colourful contrast to the others.

## Class 122 No.55002 & Class 121 No.55025 – BR blue & grey



My final two 'bubbles' are in standard corporate BR blue & grey livery. They have had all of the detailing and modifications mentioned in the article, and are as standard as possible. However, these two tend to have the most running of all of the 'bubbles'! In the photo (right), No.55002 has incorrect buffers, but I have obtained replacements from Lanarkshire Supplies and these will be fitted in due course.



▲ The Regional Railways-liveried Class 122 No.55000 poses for the camera in a station setting.

### Bubbling with inspiration

I hope this article provides some inspiration for a few fun 'bubble' projects. There are plenty more to do, as I haven't covered Departmental or route learning examples, or the final 'bubbles' with their many modern upgrades, and of course Bachmann has released some revised liveries... the early NSE guise is too tempting! Can you have too many 'bubbles'?

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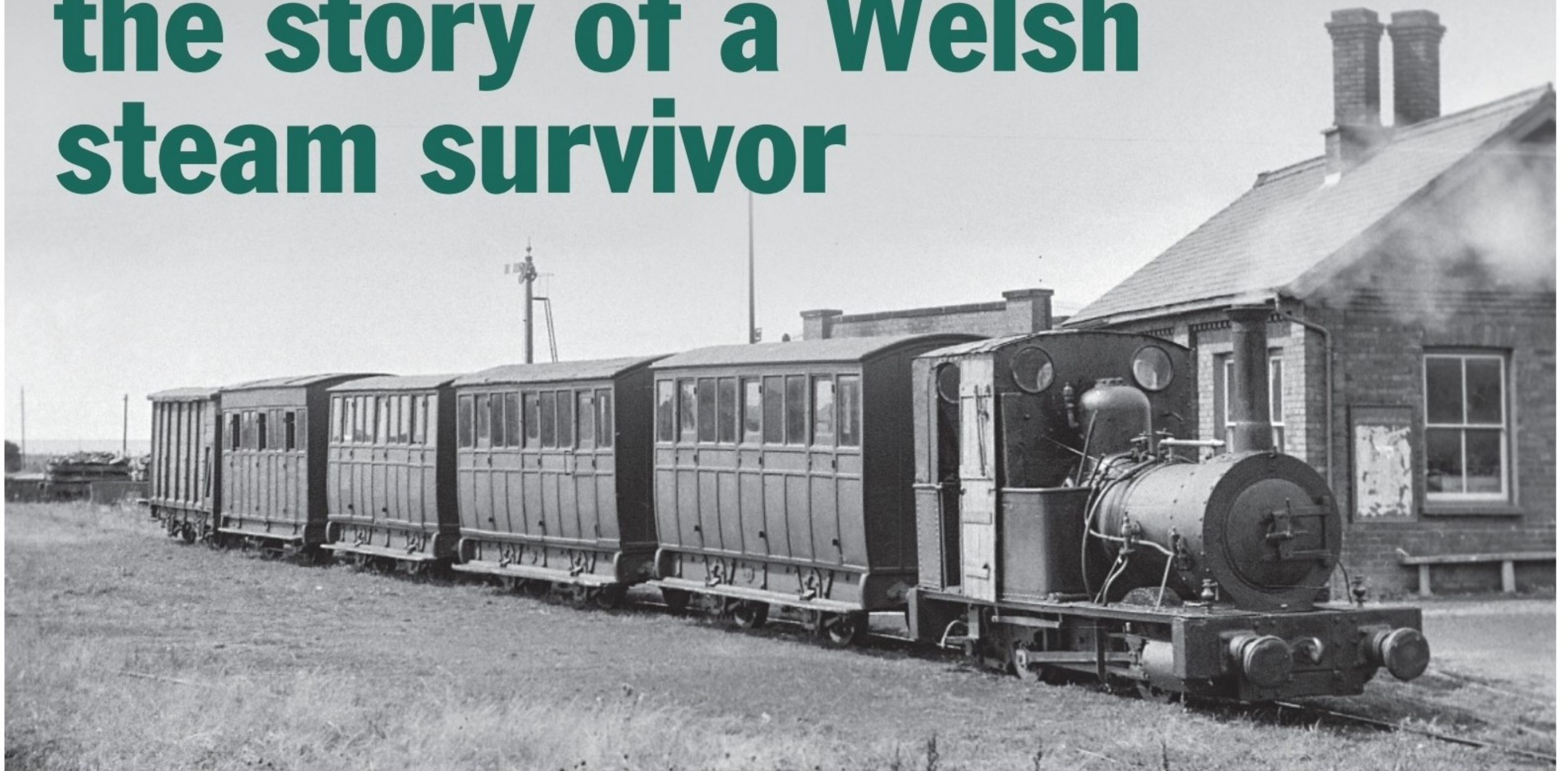
[www.railcar.co.uk](http://www.railcar.co.uk) (Warning: this site can be addictive!)

*DMUs in Colour* by Robert Dalton (RCTS)

*Diesel Dawn 4: Diesel Multiple Units – A Pictorial Observation* by Robert Carroll (Classic Magazines)

## Spotlight on the Talyllyn Railway

# Preservation pioneer – the story of a Welsh steam survivor



75 years ago, the Talyllyn Railway in mid Wales led the global preservation movement. To mark this anniversary, **SIMON ARTYMIUK** charts the history of this remarkable prototype, which has been inspiring narrow gauge modellers for generations.

This summer the Talyllyn Railway is celebrating its 75th anniversary of becoming the first railway in the world to be run by a preservation society – a pioneering development that has been followed by hundreds of other heritage railways across Britain and overseas.

It is incredible even that the line survived to 1951 for, as railway historian James Boyd wrote: “During... 80-odd years, it enjoyed the scantiest maintenance and paucity of expenditure.”

The line’s early history was linked to a slate quarry established in the 1840s at Bryn Eglwys, in the mountains above Abergynolwyn in the valley of the Afon Fathew. The slate was taken originally to the port of Aberdovey (now Aberdyfi) by packhorse and cart.

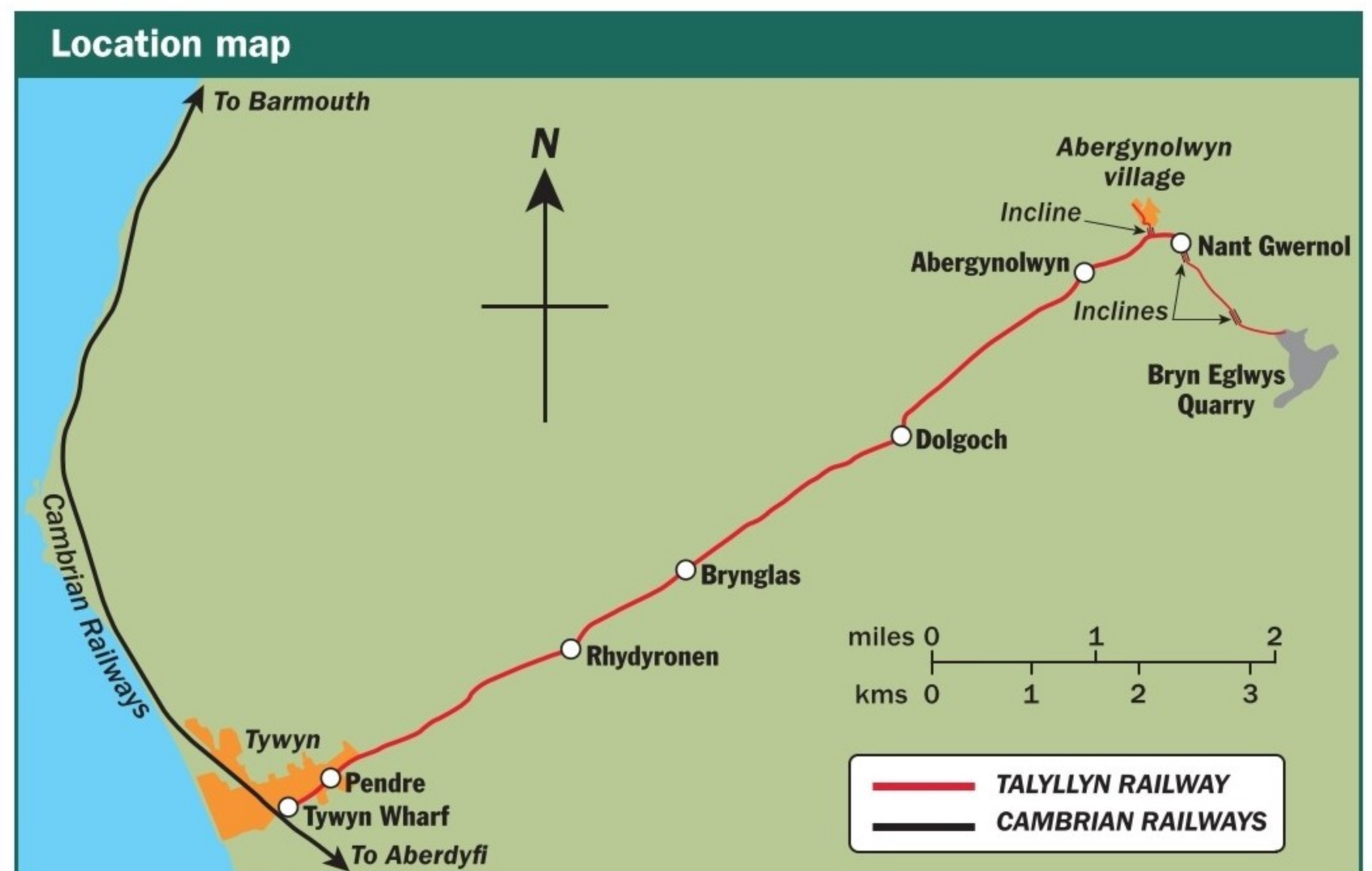
However, that changed due to the American Civil War of 1861-65 when a Union army blockade of ports in the southern States cut off the supply of raw cotton to the cloth industry of Lancashire. The crisis caused widespread unemployment and hunger.

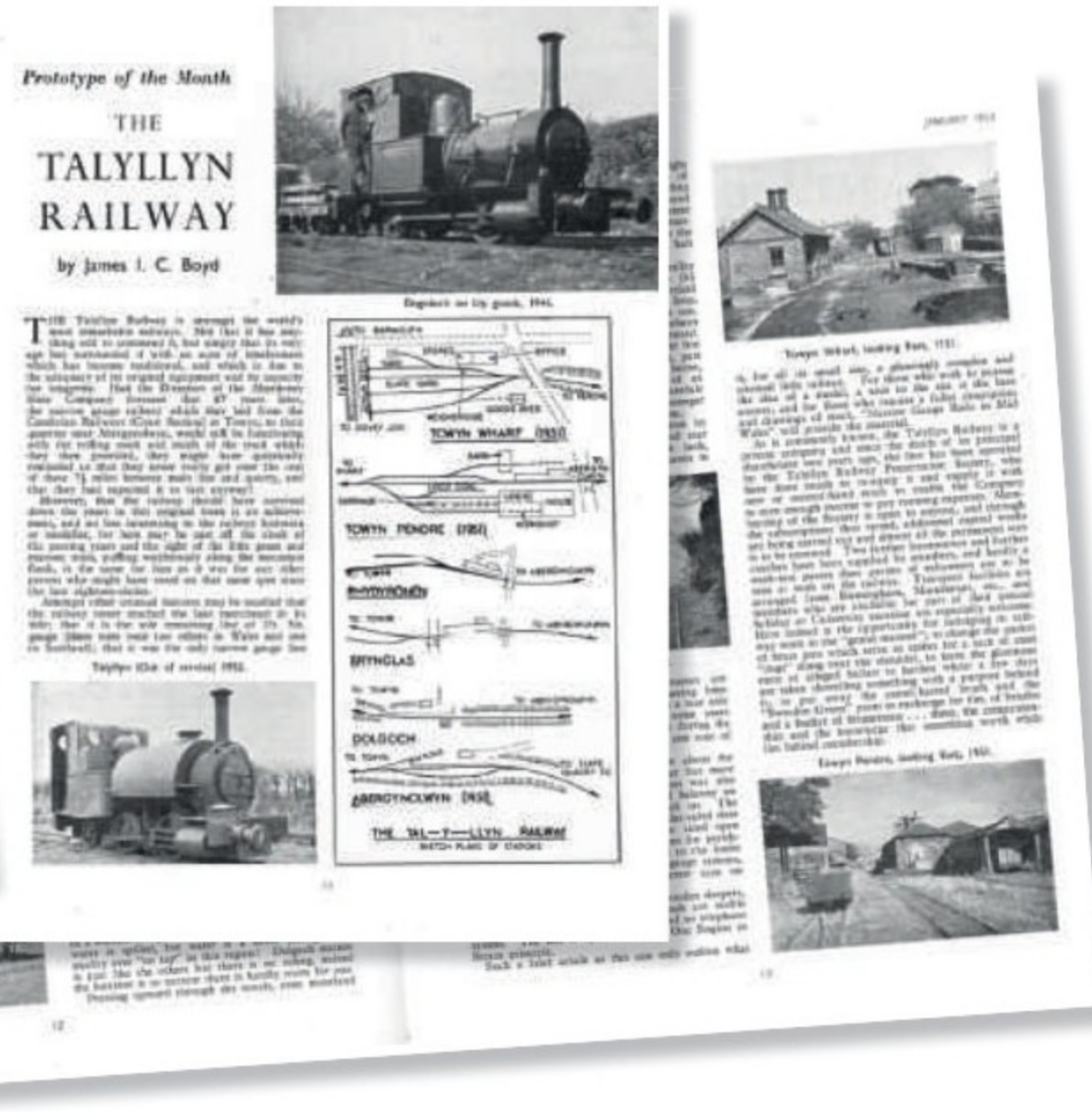
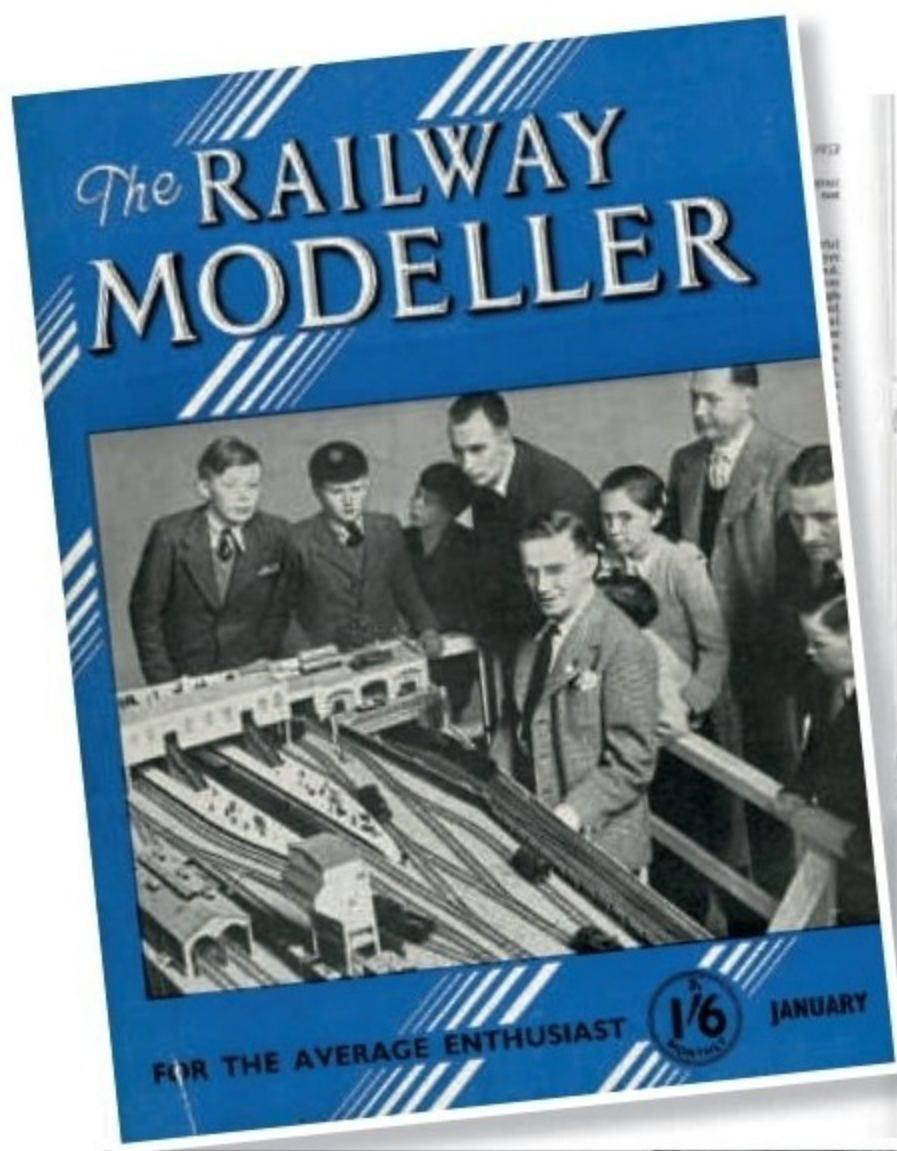
In Manchester, the Sedgewick Mill of Messrs McConnel & Kennedy had become the largest cotton spinners in the country. In

1861, William McConnel became sole proprietor and, around the same time, he purchased an estate near Dolgellau. It seems it was this that inspired him to diversify into the Welsh roofing slate industry by buying Bryn Eglwys, with friends and family

▲ TR No.2 at Towyn Wharf (now Tywyn) on 30 August 1950, not long before the line was preserved. Photo: *The Transport Treasury*

members as co-directors. He also invested in new equipment for the quarry and new housing for workers in Abergynolwyn.





▲ A survey of the line and its rolling stock by James I C Boyd was published in the January 1953 edition of *RAILWAY MODELLER*, soon after the railway's revival by preservationists.

◀ TR No.2 in c.1860s condition with open cab at Abergynolwyn, as modelled in 5.5mm scale by Tim Tincknell (see RM April 2015).

James Swinton Spooner – brother of Ffestiniog Railway chief engineer Charles Easton Spooner – was engaged to survey the route for a railway down the Fathew valley. In 1863, the FR had introduced steam locomotives to handle slate traffic on its 1' 11½" gauge, so the Talyllyn was planned to use steam from the outset. Originally it was to have run all the way to the quays at Aberdyfi, but, by a lucky chance, the Aberystwyth & Welsh Coast Railway

TR No.1 is seen with a short slate train on Tim Tinknell's 5.5mm scale layout.



(absorbed into the Cambrian Railways in 1865) had begun building its line northwards from Aberdyfi, so the 2' 3" gauge line instead ended at a wharf next to the standard gauge at Towyn (now Tywyn).

The Talyllyn Railway Company, incorporated in July 1865, was the first British narrow gauge line to be officially sanctioned to carry passengers from the outset. Construction presented few challenges apart from the 52' high brick viaduct over the Dolgoch gorge (a model of which is illustrated on p518), and the cable-worked inclines needed to connect the line's

mineral extension up to the quarry, and down to supply Abergynolwyn village.

Two locomotives of very different designs were ordered, both from the Cumberland builder Fletcher Jennings & Co. of Lowca, near Whitehaven. No.1 *Talyllyn* was a saddle tank, originally with a short 0-4-0 wheelbase, delivered in 1864. The loco very soon had a stabilising rear axle fitted, making it an 0-4-2ST.

No.2 *Dolgoch*, however, was a long-wheelbase 0-4-0 with both rear water tank and well tank between the frames, and was delivered in 1866. For passenger traffic there were three four-wheel, three-compartment carriages and a guard's van, supplied by Brown Marshalls of Birmingham. In 1867 a fourth passenger carriage of more basic design was supplied by Lancaster Wagon Co.

### Revisions for passenger services

When Captain Henry Tyler of the Board of Trade came to inspect the railway, he was displeased by the narrow apertures under the line's overbridges, as coach doors could not be opened wide enough for passengers to escape in an emergency. He therefore recommended that doors should open on one side only and the track be slewed to one side under the bridges. Platforms at the railway's stations are therefore all on the same side.

TR passenger services began in late 1866, with Pendre station – location for the loco and carriage sheds and workshop facilities on the edge of Tywyn – rather than the Wharf as the main starting point and Abergynolwyn as the upper terminus. In all the line was seven and a half miles long, with an average gradient on the upper section of 1 in 60, taking it to 270' above sea level and a point still 330' below the first quarry level. Three double track cable-operated inclines were therefore needed to reach the workings.

The quarry and railway were never a great success and there were no takers when an attempt was made to sell them by auction in 1879. In 1881, William McConnell bought the two concerns from his fellow directors for £1,800. Investment in new underground slate levels then followed, as well as a new slate mill being constructed.

For a while the railway operated summer horse-drawn carriage trips taking passengers on to the scenic Talyllyn lake, and photographs from the era of the 1899-1902 Boer War in South Africa show the loco *Dolgoch* bearing the temporary name *Pretoria* following the capture of that city by the British in 1900. However, when in 1902 William McConnell died and his son W H McConnell took over, the leases on the land occupied by the quarry were running out and in 1909, the workforce was told the quarry would be closing.

But in 1911, to keep the quarry workforce employed, the newly elected local MP, Henry Haydn Jones, bought the whole undertaking for £5,500. Despite a resurgence in business after the First World War, sales of slate subsequently dropped and to

**No.4 Edward Thomas was one of two former Corris Railway locomotives purchased by the fledgling TR in the 1950s.**

economise on opening new levels, the quarry resorted to cutting away at rock pillars separating existing chambers. At times quarrymen's hours were cut to a three-day working week. On Boxing Day 1946, the years of 'pillaring' finally took their toll when a quarry chamber collapsed, bringing about abandonment of the workings at Bryn Eglwys.

By this stage, on the railway only loco No.2 *Dolgoch* was running. The track was rusting and completely grass-grown, but Sir Henry Haydn Jones said he would continue to run a summer-only passenger service, for use by holidaymakers, a couple of days a week as long as he lived. He died in July 1950, and at season's end in October, it seemed the writing was on the wall.

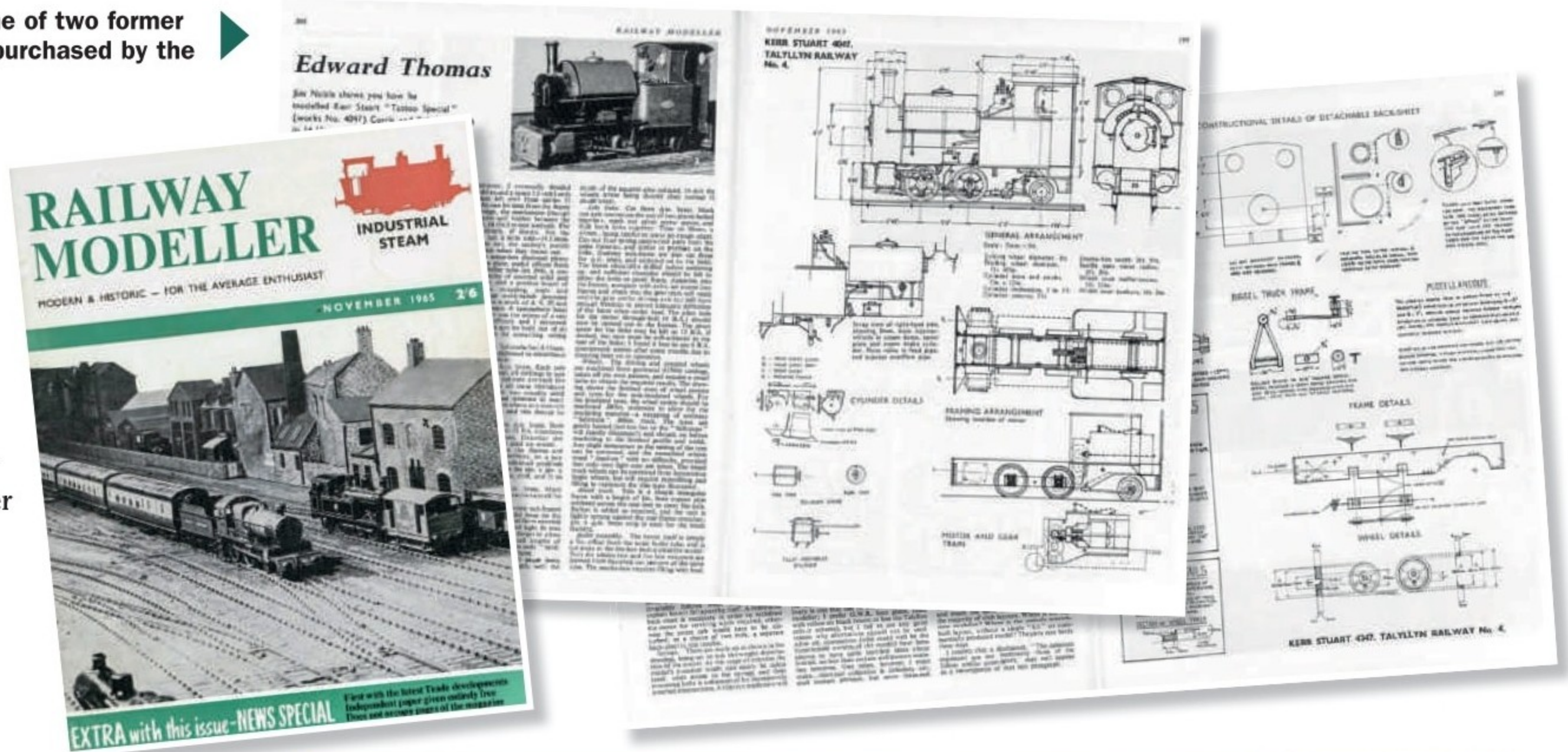
**Enter L T C Rolt**

But then another Talyllyn Railway saviour appeared in the shape of engineer, transport historian and author Lionel Thomas Caswell (L T C) Rolt, who wrote a letter to a Birmingham newspaper calling a public meeting to save the line. As a result, the Talyllyn Railway Preservation Society (TRPS) was formed – Rolt having had previous experience in conservation matters after co-founding the Inland Waterways Association with Robert Aickman and Charles Hadfield in 1946, in a bid to save the nation's canals.

Lady Haydn, Sir Haydn's widow, agreed to hand over the railway in return for the right to appoint some of the directors. The handover took place in February 1951 and the idea was that TRPS members would supply funds through membership fees, plus labour through volunteering, to keep the little railway running.

Initially *Dolgoch* remained the only serviceable loco but by yet another lucky chance, the station master at Machynlleth had safeguarded the two remaining Corris Railway locomotives after that line's closure by British Railways in 1948. The TR was therefore able to buy the Falcon 0-4-2ST No.3, dating from 1878, and the 1921-built Kerr Stuart 0-4-2ST No.4 for £25.00 each, along with a brake van and several goods wagons. There remained, however, the issue of improving the TR track.

At this date, RAILWAY MODELLER was only two years old, but the May-June 1951 issue carried an appeal on behalf of the new TRPS: "...when last year the owner and manager of the Talyllyn Railway died it seemed likely that this, the last narrow gauge passenger carrying railway, would follow the rest to the scrap yard and oblivion. However, the efforts



of a small band of enthusiasts and the generosity of the executors have reprieved the line... It is confidently hoped to re-open this Whitsun... Well, there it is, something like a model railway, three steam locomotives, eight miles of track, rolling stock and scenery; and it's all yours – or part of it is – for a pound a year."

**Public services restored**

The TRPS ran its first public service of five return trips from Tywyn to the intermediate

▲ A 21st century rendition of Tywyn Wharf, modelled in 5.5mm scale by George Williamson (RM August 2023). Photo: George Williamson

station at Rhydyronen on 14 May 1952, and services were extended on to Abergynolwyn on 4 June, with two trains running daily, Monday to Friday. The Hunslet Engine Co. in Leeds had meanwhile offered to overhaul

There have, over the years, been numerous trade offerings for TR models. This GEM body kit for TR No.6 *Douglas* dates from the 1970s.

**ANOTHER GEM 009 LOCOMOTIVE BODY KIT 4/mm scale 9/mm gauge**  
**TALLYLLYN RAILWAY No. 6 "DOUGLAS"**

The success of our first 009 kit "Dolgoch" proved to us the demand for prototypical kits in this scale. "Douglas" was built by Andrew Barclay of Kilmarnock in 1918. We are sure that it will be as popular as its older sister.

The body kit consists of metal cast parts, wire, split pins, etc. It can be assembled by glueing or by using GEM Metal Kit Solder. Illustrated instructions ease the building of this characterful model. Arnold 0-4-0 chassis is recommended.

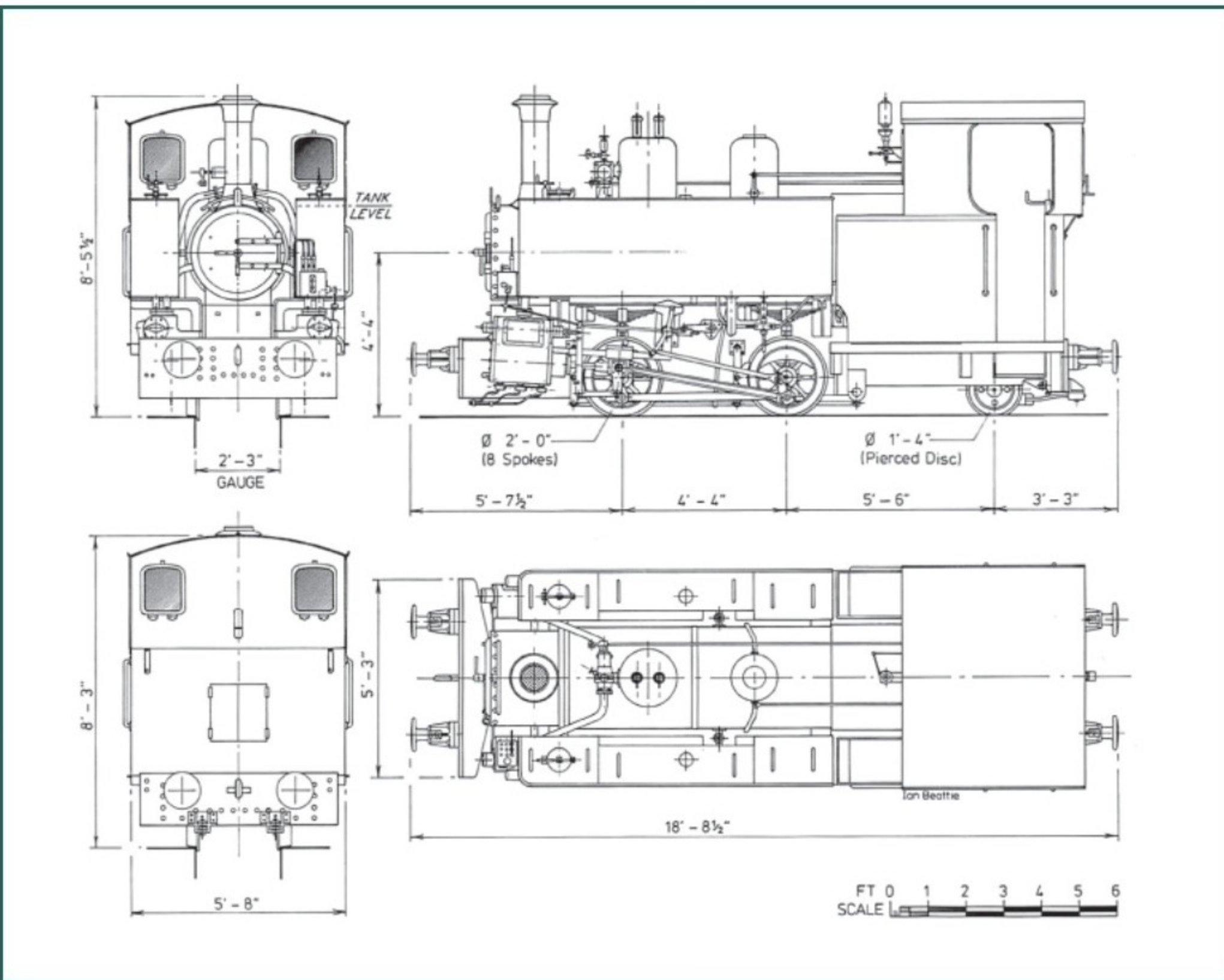
"Douglas" Loco body kit: £1.99. Arnold 0-4-0 chassis: £5.44. Nameplates: 20p.

Assembling a GEM kit is a fascinating hobby in itself. You can use one of the modern adhesives or solder the various parts together using our GEM Metal Kit Solder which melts at a low temperature. A 15p packet should be sufficient to complete your kit.

Send 5p for our OO fully illustrated brochure and up to date price list of loco and rolling stock kits, track and lineside accessories. Our new TT Catalogue — 16 pages — devoted to 3mm. scale, 12mm. gauge is a MUST for every TT modeller. Price 10p post free.

MODEL SHOPS IN MOST AREAS ARE GEM STOCKISTS. If you have any difficulty in obtaining locally we shall be pleased to supply you direct by post.

**George E. Mellor, GEM MODEL RAILWAYS, RHOS RD., RHOS-ON-SEA, NORTH WALES**



▲ A drawing (reproduced here at 4mm scale) by Ian Beattie of the new-build locomotive completed in 1991 as TR No.7 *Tom Rolt*. (See Scale Drawings – RM September 1991.)

classic encapsulating those pioneer preservation days, it was released in 1965.

### Preservation links

In August 1954, RM published a review of the new *Talyllyn Railway Guide*, noting that "... The line has been largely relaid... while an inter-station telephone, a passing loop at Brynglas, the division of the line into two sections, and the introduction of train staffs have enabled a more intensive service to be put on."

Interestingly the same page highlighted developments with the newly formed Festiniog Railway Society, the world's second oldest railway preservation endeavour. The two societies have retained close links ever since. The preservation society model was also mirrored at an early stage in Australia, where closure plans, following a landslide, for what became the 2' 6" gauge Puffing Billy Railway, in Victoria's Dandenong Ranges near Melbourne, were curtailed thanks to volunteer involvement in operating services in 1955. Today the Puffing Billy and the Talyllyn are twinned.

Corris loco No.4, subsequently named *Edward Thomas* after a stalwart of the old company, so soon there were two operational locos.

In 1953 the Territorial Army helped improve the track as part of a training exercise, meaning No.3, subsequently named *Sir Haydn*, could run with fewer derailments, and in the same year Abelson (Engineers) Ltd donated the ex-Airservice Construction Corps and RAF railway Andrew Barclay 0-4-0WT loco No.6, soon named *Douglas*.

The January 1953 RAILWAY MODELLER published a 'Prototype of the Month' feature on the TR by James Boyd, who wrote: "...its very age has surrounded it with an aura of timelessness... for here may be cast off the cloak of passing years and the sight of the little green and maroon train, puffing vociferously along the mountain flank, is the same... as... [in] the late eighteen-sixties".

He adds regarding TRPS membership: "Transport facilities are arranged from Birmingham, Manchester, etc, and members who are available for part of their annual holiday or University vacation are especially welcome."

By now the TRPS had acquired some Penrhyn Quarry Railway open workers' carriages and regauged them from 1' 10 3/4" to help increase capacity on passenger trains, and volunteers also built new bodies to put on two bogie underframes that the TRPS had acquired – making them the line's first ever bogie carriages. Second-hand rail was acquired from industrial lines and mounted on ex-BR sleepers sawn in half.

### A story for the silver screen

1953 saw Tom Rolt publishing his *Railway Adventure* account of running the TR in



▲ A 7mm scale diorama of Dolgoch viaduct, constructed by Bill Ferguson using foamboard covered with brickpaper. Photo: Bill Ferguson

1951-52. A number of the same incidents in the book – such as locomotive tanks having to be filled with buckets of water from a local stream and passengers being asked to help push carriages – inspired scenes in the Ealing comedy film *The Titfield Thunderbolt*, also released in 1953. The film's writer, T E B Clarke, had visited the TR in 1951 to gather plot material, and it is said that Tom Rolt was disappointed the film-makers chose to relocate the setting to the redundant ex-GWR Limpley Stoke to Camerton branch near Bath. However, it illustrated how the preservation model could in time be extended to standard gauge lines.

Later in 1953, the American film-maker Kit Davidson also charted the remarkable happenings on the Talyllyn Railway in the short documentary film now known as *Railway with a Heart of Gold*, featuring many genuine volunteers. Now regarded as a

### The Awdry connection

In May 1957, the BBC made a live outside TV broadcast from the TR, which was shown over two nights and had high-profile presenters Wynford Vaughan-Thomas and Huw Wheldon as its commentators. It featured interviews with volunteers, including Rev. Wilbert Awdry, who had become a volunteer guard in 1952. The broadcast resulted in passenger numbers doubling within two years while also placing a strain on the existing loco and carriage fleet. Services had to be increased and more carriages built.

As well as the original locos, *Dolgoch* and *Talyllyn*, being rebuilt, *Edward Thomas* ran for a few years with a Giesl ejector chimney designed to increase steaming efficiency, a development highlighted in a column in the May 1959 issue of RM.

The railway also acquired and restored two former Glyn Valley Tramway coaches as First class carriages, and the body of a former Corris Railway saloon was recovered from a field in Gobowen, fully restored and returned to service in 1961.

The mid-1960s saw extensive improvements at Tywyn Wharf station, celebrations for the centenary of the original engines and the roofing over of the old gunpowder store at Wharf to create the Narrow Gauge Railway Museum. The July 1965 RM featured a 'Talyllyn Centenary' feature comprising photos and line drawings of the line's historic structures.

In 1969, the old waiting shelter at Abergynolwyn was replaced with a new station building incorporating a booking office and refreshment room – though the

stone from the old structure was used to face this new building to ensure it was 'in keeping'.

### On to Nant Gwernol

The mineral extension had long been recognised to have scenic potential as a passenger line extension. Construction began in 1970, involving the widening of cuttings, reduction of gradients and easing of curves. Volunteers on the project were nicknamed 'Gwerns' and excavated material was used to widen the mountainside shelf just west of Abergynolwyn station to create an extended platform and new passing loop. Two trains can now use the platform at once, solving the complication of coach doors being on only one side. The new terminus was named after the gorge, Nant Gwernol. Although the station as finally built featured a traditional style of waiting shelter, there were proposed designs drawn up by Portmeirion architect Clough Williams-Ellis for a Swiss chalet-style structure built on stone pillars over the platform and loop. These drawings inspired a 'Nant Gwernol – as it might have been' OO9 modelling feature by Michael Farr, published in the February 2022 issue of RM.

Opening of the new extension took place 50 years ago on May 22, 1976, with the inaugural train hauled by *Dolgoch* and with Wynford Vaughan-Thomas as guest of honour. 1980 saw the opening of a new footbridge across the ravine to access a footpath to Abergynolwyn.

### New-build steam

Back in 1969, as a long-term project to create a new, more powerful locomotive, the TR had purchased a 3' gauge former Bord na Móna (Irish Turf Board) steam loco – one of three little-used Andrew Barclay 0-4-0WTs built in 1949 (see feature in the June RM issue). Given the unofficial, punning name 'Irish Pete', the original idea was to regauge it, but eventually a new locomotive was built incorporating the Irish engine's boiler, driving wheels, cylinders and other parts, while new frames were manufactured at Tywyn Pendre. The new loco emerged as an 0-4-2T in 1991 and was named *Tom Rolt*.



Recent years have seen the rapid growth of ready-to-run OO9, with models including versions of TR No.1 manufactured by Bachmann Trains (US). Photo: Bachmann Europe plc

### Notable events

With the TRPS 50th anniversary nearing, in 2001 the society launched a Golden Jubilee appeal to raise funds to improve the buildings at Wharf station. The new two-storey station building and museum complex was opened by the now King and Queen in July 2005. As well as narrow gauge and industrial railway items, the museum also houses an exhibition of memorabilia from the life of the Rev. Wilbert Awdry, including his study.

In recent years the Awdry connection has seen the TR hosting special commemorative events – and this year a Rev. W Awdry Little Engines Weekend, with a strong model railways element, including operation of Wilbert Awdry's own *Ffarquhar Branch* OO gauge layout (see RM December 1959) and revelations about a new discovery made among his surviving loco models, is due to take place on Friday 17, Saturday 18 and Sunday 19 July – see the Societies & Clubs pages in this issue.

### Investment for the future

In November 2000, RAILWAY MODELLER included a feature by Nigel Adams marking the 50th anniversary of the TRPS, with its achievements and models of the line highlighted. More recently, in the September 2020 RM, a Talking Points feature by the same author, entitled 'How times have changed' told of the impact on the line of the Covid-19 pandemic and measures taken to weather the impact on travel and tourism.

Fortunately that crisis has now passed and recently the Talyllyn Railway received three new bogie carriages commissioned from the Ffestiniog Railway Boston Lodge carriage works, two of them funded by the Welsh Government Coastal Communities Fund and incorporating disabled-accessible

and First class observation saloon sections. This year the railway has been awarded a £3.6m National Lottery grant towards its Heritage Fund Project, with the objectives of establishing new engineering facilities at Pendre, enhancing community engagement and refurbishing the original works for heritage displays in connection with The Slate Landscape of Northwest Wales World Heritage Site.

With its fascinating history, the charm of its locos and rolling stock and its scenic setting, this long-lived rail survivor continues to provide great inspiration for narrow gauge modellers – as well as being a line whose example has been followed by the now over 1,000 heritage railways around the globe.

A video of the Founders Day launch of the 75th anniversary celebrations, highlighting the impact of the TR on the world of rail preservation, can be watched at:

[www.youtube.com/watch?v=Uus2pVmJlyA](https://www.youtube.com/watch?v=Uus2pVmJlyA)

On selected Tuesdays in June, July and September, the TR is operating special 'Railway Adventurer' trips with a guide telling the story of how the railway became the first to be preserved, and including a tour of Pendre Works, the Tywyn NG Museum and a cream tea. For further details on the railway visit: [www.talyllyn.co.uk](http://www.talyllyn.co.uk)

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[www.talyllyn.co.uk/about/the-awdry-connection](http://www.talyllyn.co.uk/about/the-awdry-connection)  
[www.talyllyn.co.uk/heritage-fund](http://www.talyllyn.co.uk/heritage-fund)



The Talyllyn Railway Preservation Society held celebrations to mark 75 years of preservation, with volunteers pictured here at Tywyn just before the first train on 15 May. Photo: Owen Hayward

# Weobley

Photography by Ian Manderson

**DARREN RAY** took inspiration from a classic 1970s C J Freezer track plan to create this demountable L-shaped layout in OO, which utilises a fiddle yard design attributed to Peter Denny.



**T**his layout is the result of many happy months studying the track plans of Cyril J Freezer.

My initial attempt at a layout (*Handcross*, RM January 2019) had been based on the Reverend Awdry's *Ffarquhar Branch* layout from the 1950s, having the same track plan but different scenic treatment. I enjoyed operating the layout for a while, but I soon came to realise that it was neither one thing nor the other: it was too cumbersome to be a portable layout (comprising two 6' x 2' boards bolted side by side), and on the other hand it didn't really fit anywhere in the house well enough to be set up permanently.

So, I decided that a small L-shaped portable layout might make for a suitable

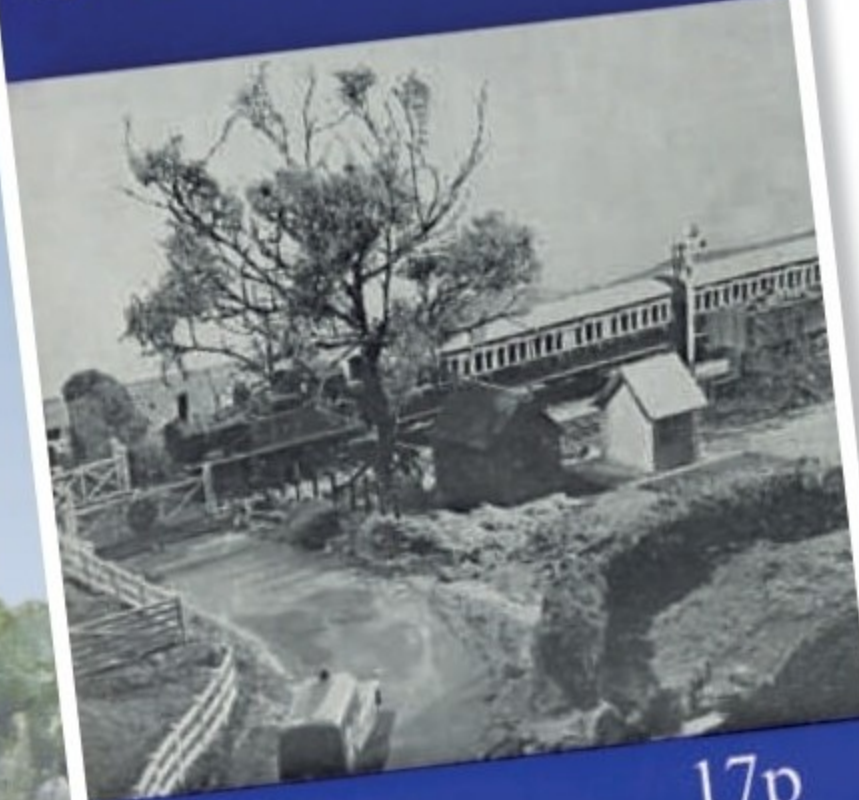
next project, one that could be set up in a corner of a room without getting in the way too much, but which could be stored even more out of the way if required. The plan I chose to broadly copy this time was C J Freezer's S35 from his *60 Plans for Small Railways*, based on one that he apparently built to fit in an under-stairs cupboard. It was the usual fiddle yard to branch line terminus arrangement, and seemed to have enough going on to keep me interested. I was particularly keen to try out the Peter Denny removable fiddle yard, which Mr Freezer described as "...probably the greatest aid to correct operation yet invented." How could I resist? It potentially allows for easy turning of the trains if you lift

it up and turn it around, although in practice I find it more straightforward to move the locomotives from one end of the train to the other by hand. It also makes for a handy storage box in between running sessions. I suppose cassettes are even more convenient if you have a large amount of rolling stock, but I do not foresee that I will amass much more than three trains' worth.

## Construction

At the station end, there is a very unrealistic sector plate (operated by a forefinger from under the baseboard), which takes up less space than a point and therefore allows for longer trains at the platform. I believe something similar was used in the real world

# 60 plans for small railways



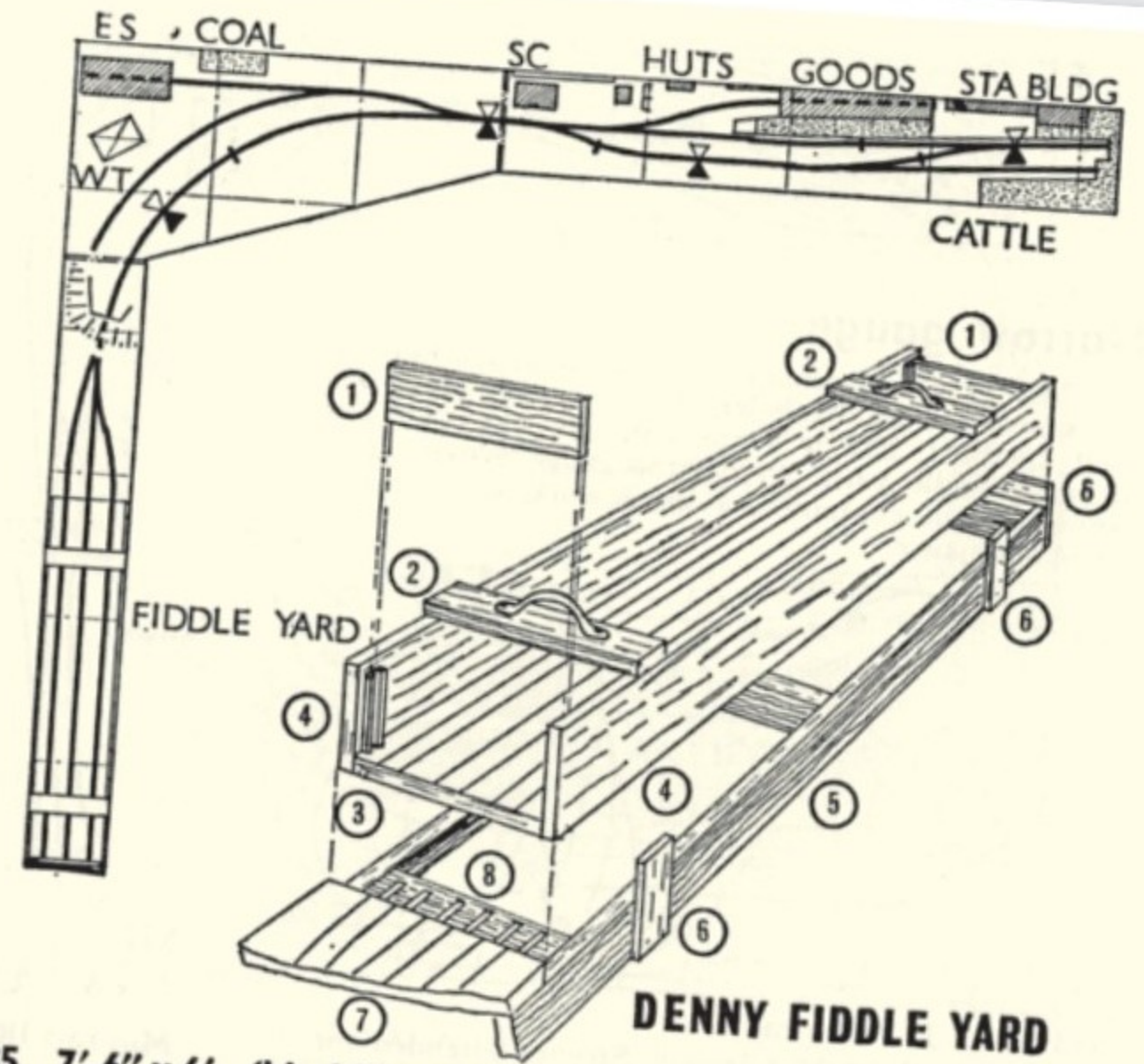
3rd Edition  
a Peco Publication

17p

▲ The track plan (right) was taken from the Peco booklet *60 plans for small railways*.

◀ The station building is based on the structure at Kingsland, on the Leominster & Kington Railway in Herefordshire.

A sector plate serves to switch the locomotive onto the run-round loop, saving space compared to a point.



S35 7'-6" x 6' 'L' Min rad 22"

## Fiddle yards

- |                      |                        |
|----------------------|------------------------|
| 1. Gate              | 5. Sub-frame           |
| 2. Lifting handles   | 6. Locating pegs       |
| 3. Fiddle yard base  | 7. Approach tracks     |
| 4. Fiddle yard sides | 8. Electrical contacts |

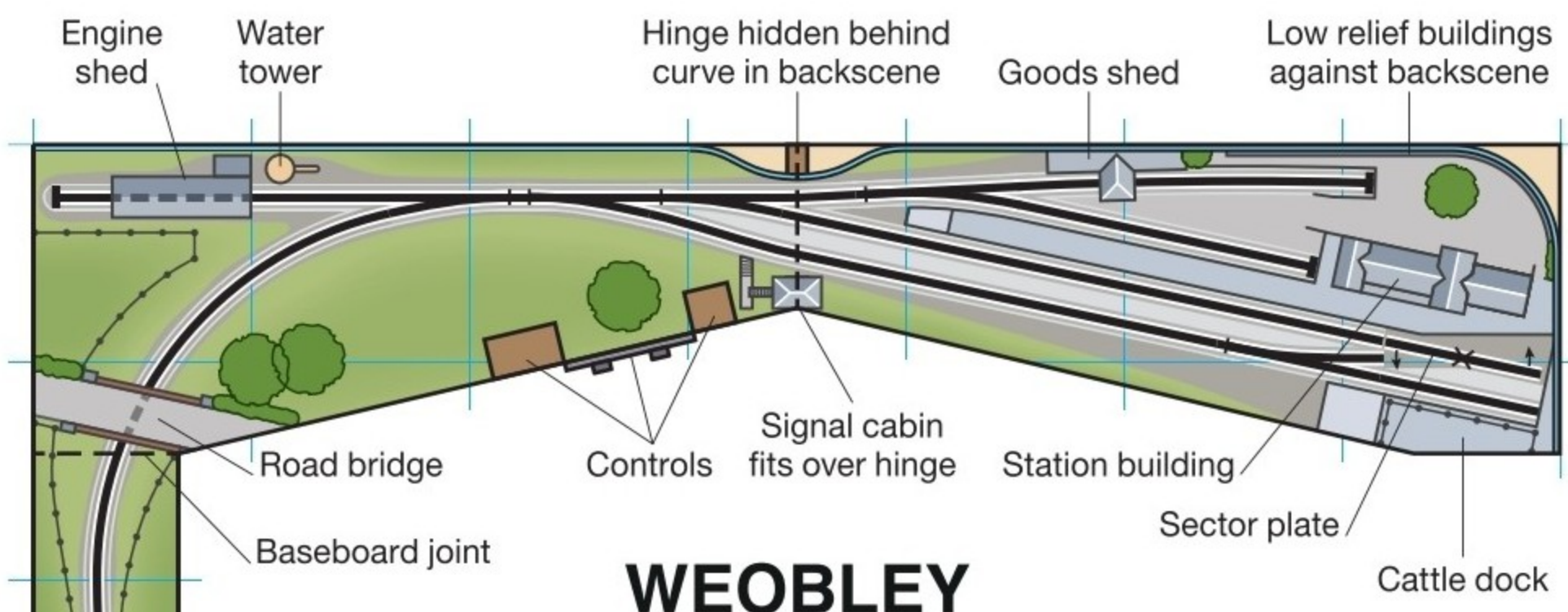
The fiddle yard is probably the greatest aid to correct operation yet invented. It consists of a number of storage sidings which are deemed to have no real existence. In these, one holds trains, swaps stock around and gets the locomotive and brake vans of freight trains to the other end of the train.

My own preference is for simple ladder yards where all stock is shifted by hand. This is not, I realise, to everyone's taste and, therefore, some prefer to provide run-round facilities, or locomotive lie-by sidings, both of which require additional space. A slightly more elegant method was devised by Peter Denny. Here the fiddle yard is arranged as an inverted box which either lifts off, or pivots in the fashion of a turntable. Drop down wooden gates prevent stock falling off the ends during the turn. It must be admitted that this system is very convenient and effective.

It has further advantages. On a portable system, it allows one to pick up and store the bulk of the stock rapidly. It is also possible to provide several identical magazines, enabling a much wider range of trains to be operated at choice.

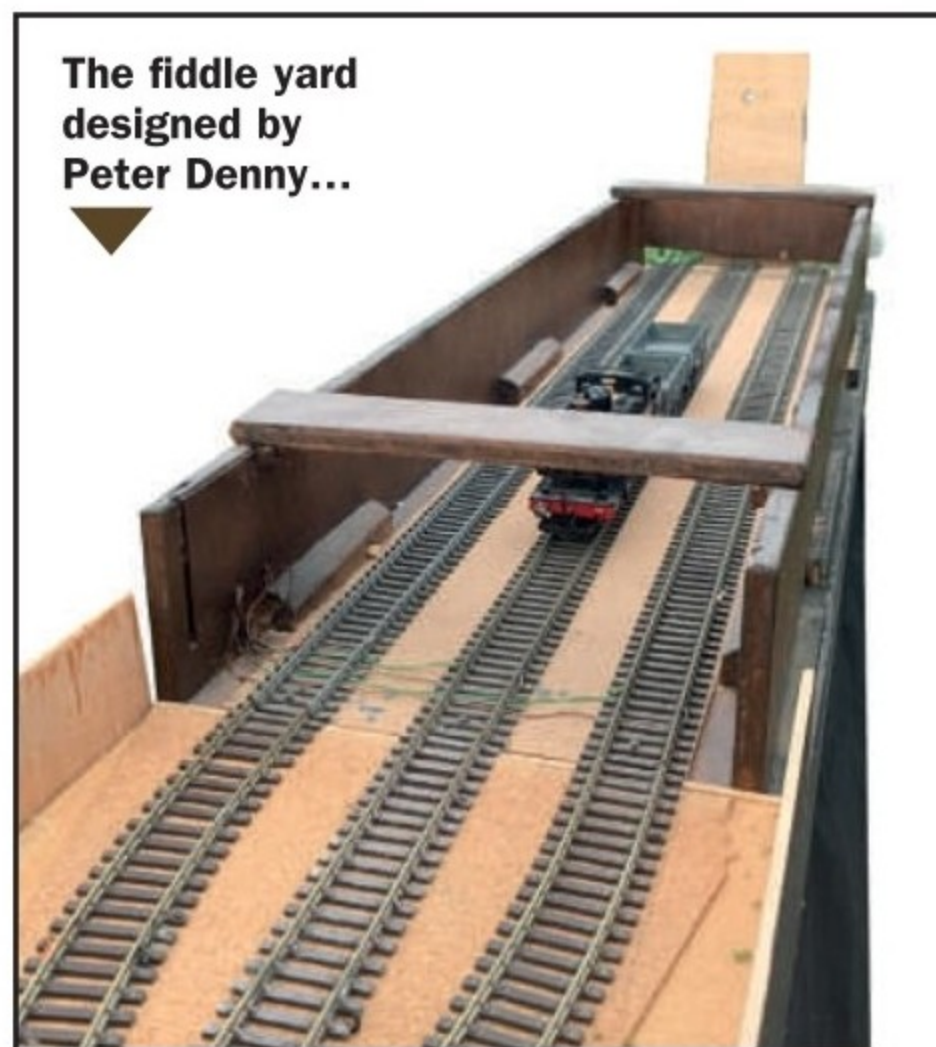
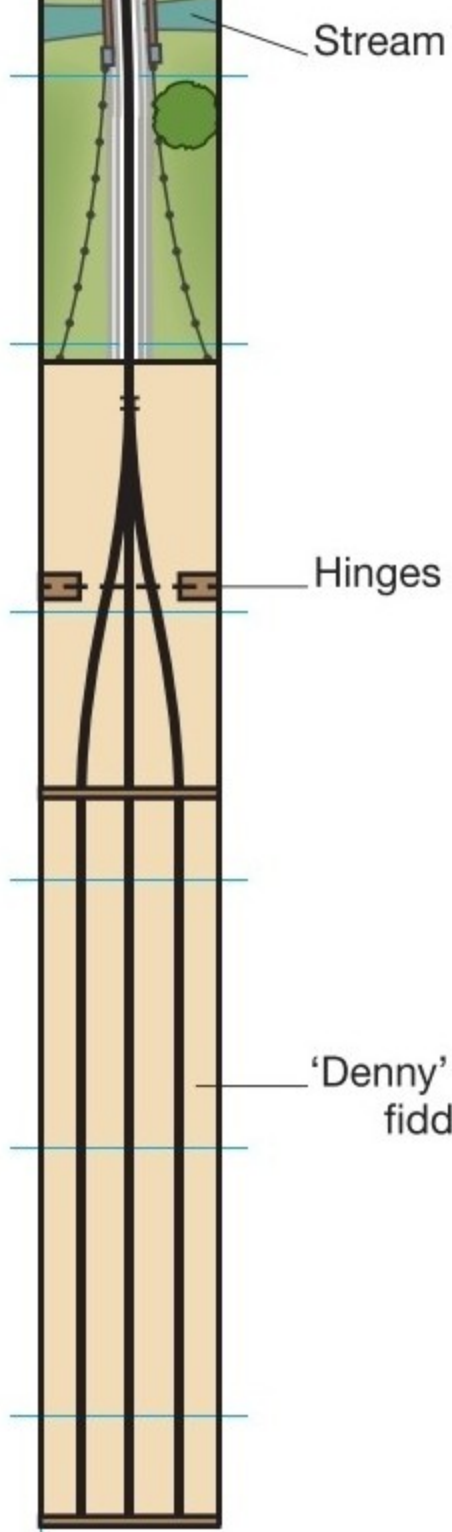
The Denny style fiddle yard is shown in S35, which is otherwise a relatively simple layout.





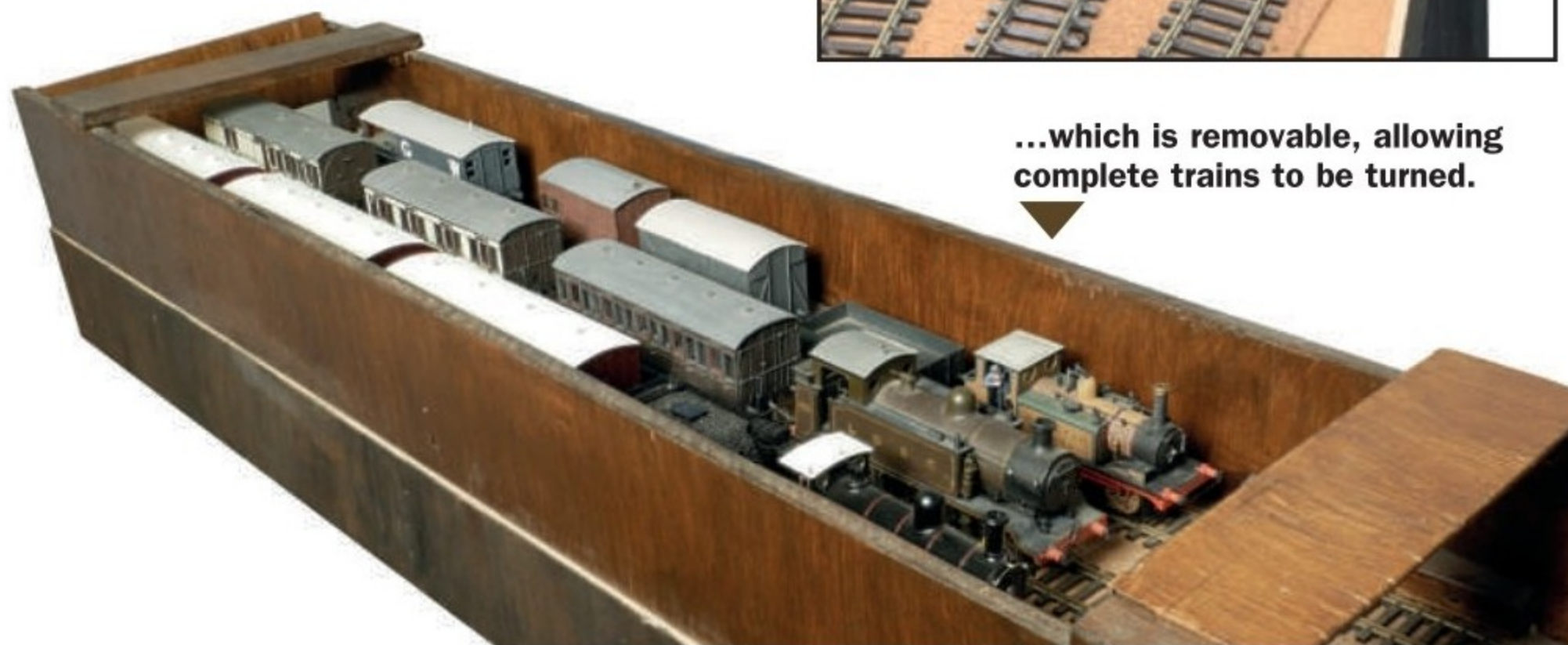
## WEOBLEY

Overall dimensions including fiddle yard: 7' 0" x 8' 5".  
Each grid square = 1ft x 1ft.



The fiddle yard designed by Peter Denny...

...which is removable, allowing complete trains to be turned.



A view of the station; note the signal box covering the hinge.

The goods shed is also based on the structure at Kingsland; it was scratch-built from card, with mortar joints scribed into the walls.



in a couple of locations, but I doubt very much they looked as crude as mine.

I was also inspired by an article which I found in the January 1982 issue of *Model Railways*, about a layout by J M Stevens called *Fittleton* which folded up very neatly in two directions, reducing an 11' 3" long layout into a 3' 9" long box. This principle did not seem to lend itself to my L-shaped plan, but I thought that if I split the whole thing into two sections and then hinged each section in the middle, forming two folded units each 3' 6" long, that would be quick enough to erect and small enough to store easily.

The construction of the boards is based quite closely on J M Stevens's drawings: essentially a 69mm x 18mm wooden frame with low density fibreboard for the surface and a 3mm plywood lining all the way round. The cross members of the frame are smaller section timber, as room had to be made for the 44mm x 18mm legs and braces which fold up into the body, held in place with magnets when folded and hinged struts when standing.

The hinges that join the two halves of the boards are raised 40mm above the surface on the fiddle yard board and 70mm on the station board, where a signal box slots over the front hinge when erected and the rear hinge is hidden behind a bulge in the backscene. Much fun has been had trying to arrange everything so that it fits together during folding. The ridge tiles on the station building got a bit scrunched by a fence on the other side, so the top rail had to be removed. I still need to repair the roof. The trees don't quite fit, but at least they can squash down slightly without too much

Each section of the layout is hinged in the middle, folding to lengths of 3' 6"...



...and forming compact boxes for transportation and storage.



damage. The whole layout is not too wobbly when erected, but trying to put it up and down single-handed is an awkward exercise. I know that one day I shall forget to remove the signal box from the hinge before dismantling and it will surely be ripped apart; I need to introduce some sort of locking mechanism before that happens.

#### Track, structures and scenery

Trackwork is code 100, with buff-coloured ballast. Point motors are Peco, operated from a couple of small switch panels – although two of the points have to be worked manually at the moment, as

somebody did not plan their layout very well and there are bits of structural timber in the way underneath. Similarly, the signal still needs to have an operating mechanism fitted, although as I can operate it well enough by reaching under the baseboard, this job will probably never get done. In fact, I suspect that the signal should be a double one in this situation. Perhaps another reader can advise?

The buildings are all made from card and paper. The station building and the goods

The locomotive shed is based on the example at Pontillas in Herefordshire.





engine inside through the windows. We live and learn!

The backscene comprises cut-out paintings of some of the houses in Hereford Road, Weobley, set away from the backscene slightly to try to give a sense of depth.

The trees and hedges are rubberised horsehair coated in some Woodland Scenics foliage. The grass was formed from the leftover scraps of hanging basket liner from my last layout, painted green. It may look passable from a distance, but I would like to redo it one day.

### Links to the past

After many years of experimenting with Sprat & Winkle couplings, but not being quite brave enough to put them on all my rolling stock (in case I changed my mind in favour of reverting to the standard tension-locks, or gave in to my modelling pen friend Antony's suggestions that I fit Kadees) I have finally decided in favour of Sprat & Winkles. These were used by the Reverend Awdry on his *Ffarquhar* layout, which inspired my previous layout, so it is nice to be able to continue to refer back to his railway in some way. I have taken the approach of fitting hooks to both ends of my wagons and coaches, as this means that my locomotives do not need a hook – a bar glued to the buffers will suffice. I do not think there is enough space behind the bufferbeams on my locos to fit the hook anyway.

### London, Brighton & Western

The eagle-eyed reader will notice that there is a geographical anomaly regarding the rolling stock. The Weobley branch line (I have yet to think up a more exciting name for it) looks as though it is currently being served primarily by locomotives and coaches borrowed from the London, Brighton & South Coast Railway – namely a 'Terrier' and a Class E2, both Hornby, along with a few kit-built coaches. I am happy that they are from the right period, as I have tried to set my layout roughly in Edwardian times, but geographically they have strayed

shed are based on those at the old Kingsland station in Herefordshire, which I can see out of my office window at my place of work and which I have always been keen to model. The station building still partly exists, but the goods shed needed to be re-created with the use of photos. Kingsland was not a terminus, so I decided to move them a few miles to the south-west and base my station in the village of Weobley, hitherto unconnected to the railway network.

Most of the structures have brickpapers for the walls, except for the stone goods shed, for which I scribed the mortar joints into the card walls and painted the individual stones with watercolours. Slate roofs were made in the Pendon manner from thin paper (from an envelope) with two thin cuts made between each slate and the resulting sliver removed, then the whole roof was painted in watercolours.

The engine shed is based on the one at Pontrilas station in the south of the county. I was running out of patience at this point, and thought I would just paint the windows on the wall rather than going to the effort of making them see-through. But I rather regret that now, as it would be nice to glimpse the

▲ Pending the arrival of a GWR '517' 0-4-2T from Dapol, the line is operated by Hornby LBSCR 0-6-0 tank engines – a Billinton E2...

...and a Stroudley 'Terrier', pictured shunting a GWR van alongside the goods shed. ▼





▲ The 'Terrier' crosses the small stream on the section of the line between the station and the fiddle yard.

a long way from home, so I am slowly trying to replace them with more appropriate London & North Western Railway and Great Western Railway rolling stock. I do have an Improved Precedent Class loco from

Locomotion Models and three Hornby LNWR six-wheel coaches, and have placed an order for a Dapol GWR '517' Class.

I have found from experience that I do not have the patience and skills to build my own locomotives, which is a shame as I would like to explore that part of the hobby. My sole attempt at locomotive kit-building resulted in a static model. I'm sure I fitted the motor correctly, but the locomotive just refused to move. I am tempted to refer back to the Rev. Awdry again and brick it up in a tunnel!



▲ The layout is set in the Edwardian era, with these passengers on the platform displaying the fashions of the day.

▼ The LNWR Improved Precedent 2-4-0 was produced by Bachmann for Locomotion Models, and the LNWR coaches are by Hornby.

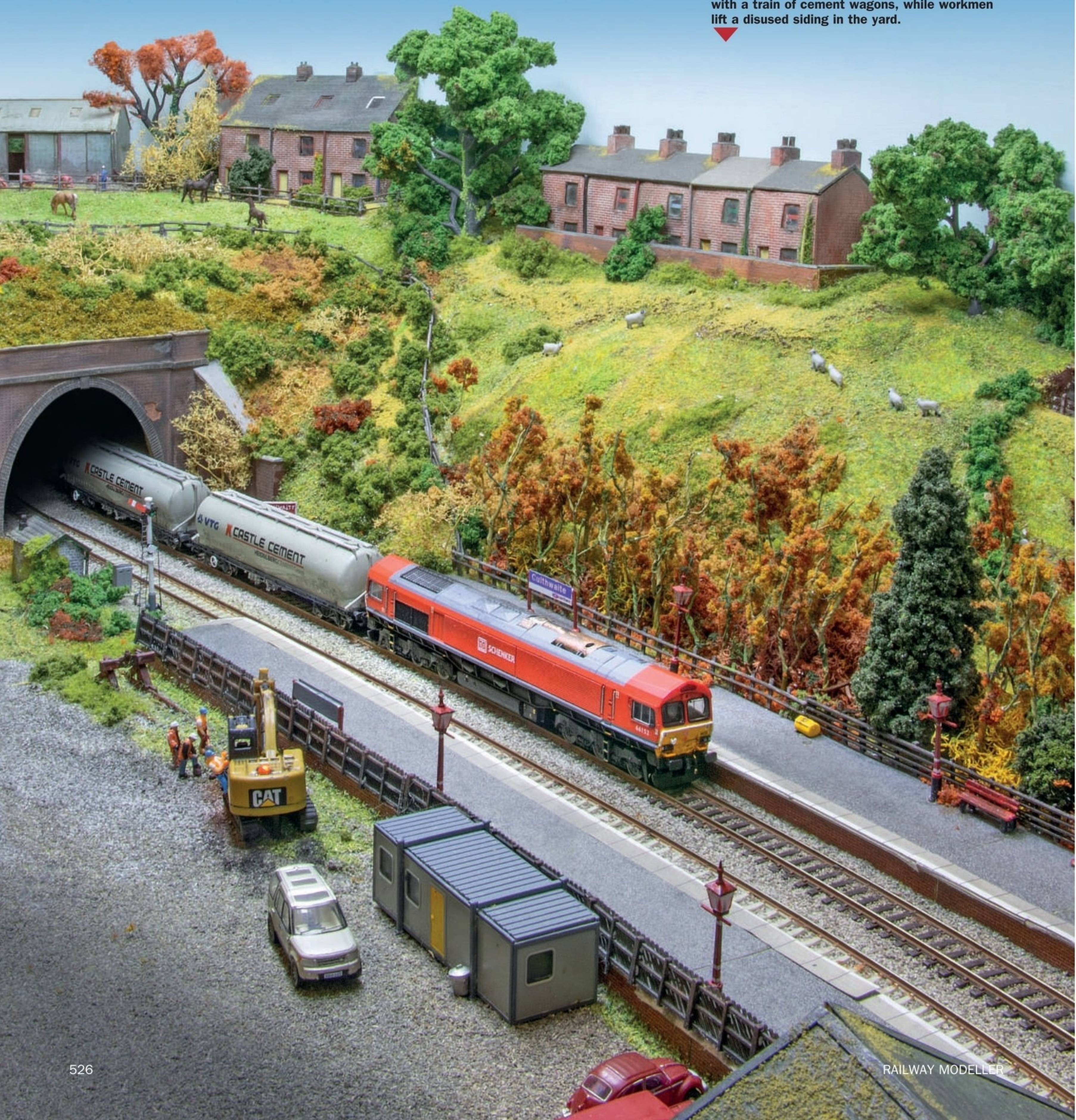


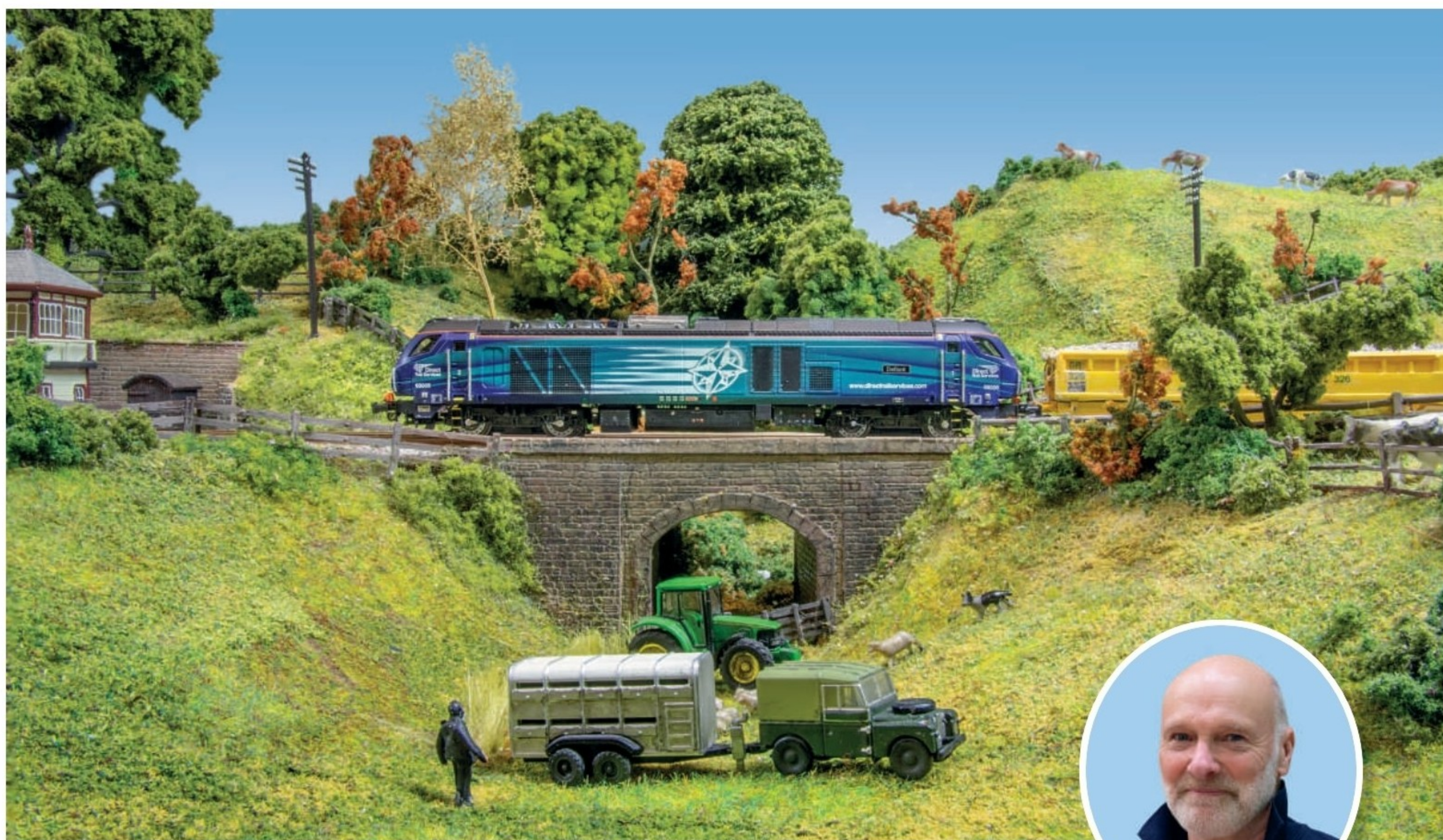
# Culthwaite

A desire for an N gauge exhibition layout with frequent train movements, and a lifelong love of the Settle & Carlisle line, led **PHILIP MASON** to build this model combining aspects of two locations on the famous route – Culgaith and Armathwaite.

Photography by Craig Tiley

DB Schenker 66 152 emerges from the tunnel with a train of cement wagons, while workmen lift a disused siding in the yard.





The kernel for this layout was sown at a Macclesfield Model Railway Group exhibition a few years ago. A gentleman saw me in my club T-shirt and wanted to provide some feedback: “It’s been a great show and there are some lovely models, but on some of them, not a lot happens”. I explained that, obviously, the exhibition manager could only invite what was out there, and I agreed there had been a trend towards smaller end-to-end layouts.

But it got me thinking. One striking layout that is always popular is by our friend and member of sister club Alsager Railway Association, Rich Brady. The layout is *Weaver Hill* and is a four-track main line with plenty of action. It’s dead simple: no station, no shunting, no junctions, nothing to slow the action; just four tracks running through the landscape, and a fiddle yard. Excited children would stand four deep waiting to see if ‘the Pendolino will overtake the Freightliner’.

After 50 or so years in the hobby, I’d amassed a reasonable amount of N gauge rolling stock, all post-Nationalisation, all suitable for the North of England. Nothing Great Western, nothing foreign. Most of this had never seen the light of day other than on my own loft layout. Macclesfield Model Railway Group is a marvellous institution, and even though I moved away a long time ago, I’m still a member 30 years on, but, to date, it has never had an N gauge layout on which to run my stock. When I first joined in the 1980s, the club N gauge layout was Continental, and I had to sneak on my Class 20s and ‘Merry-Go-Round’ trains when nobody was looking. The next project was, wait for it, Great Western and following that

▲ A local farmer and his Border Collie round up their sheep, while Direct Rail Services 68 005 heads a Network Rail ballast train.

was a model of Macclesfield itself (so we were getting nearer) but, unfortunately, the layout was to be set in the London Midland & Scottish Railway period.

I couldn’t wait forever for a club N gauge layout, so what could I build myself that would provide interest and movement for the fee-paying public at exhibitions? Rich’s *Weaver Hill* is OO gauge and isn’t small, but just a double track main line in N gauge, maybe 12’ long, could work.

### Choosing Culgaith

My mind then moved on to possible locations. I’ve had a lifelong love affair with the Settle & Carlisle, started by a gift of the

“ – Culgaith is a gem of a location crying out to be modelled – ”

Philip Mason

1961 *Locospotters’ Annual* which contained an article entitled ‘Stay up late for a night ride on the Condor, the fastest freight on British Railways’. I made my long-suffering mother read this to me as a bedtime story countless times. Later, at grammar school, I won a book prize and it had to be David Jenkinson’s *Rails in the Fells*. In 1975 (the centenary year of the S&C), aged 15,

▲ Above the tunnel is a farm, replacing the bungalow at the real Settle & Carlisle location of Culgaith.



I walked with a pal from Hellifield to Appleby and completed the route to Carlisle later on a push-bike. So it had to be the Settle & Carlisle, but where? Many of the obvious locations had already been done by other modellers.

Making trips to the Lake District from Yorkshire can mean using the A66 route. With a young family, there's often a need for a stop on the way, and that's how we stumbled over Culgaith. For many years, there was a garden centre and café next to the railway, just a couple of miles off the A66. Ideal for the family, it was also a way to give Dad his train fix.

Culgaith is a gem of a location crying out to be modelled. It's got a tunnel, the (still closed) station, a level crossing with a signal box and, not too far away to the south, another scenic break in the form of a beautiful high occupation bridge. The seed was firmly planted and beginning to sprout.

The era chosen for the layout was a 'no brainer'. Never before had the line seen such a variety and frequency of trains as in the 2010-15 period. All the major freight companies were using the route, with their plethora of liveries, and the fuel-hungry power stations of the Aire Valley and

**A pair of Direct Rail Services Class 37s rumble through the station, top-and-tailing a Rail Head Treatment Train.**

Fiddler's Ferry demanded feeding by frequent trains of imported and opencast coal from Scotland. Passengers were served by Class 156 and 158 units, and railtours made weekly appearances.

### Small & Compact

To make a round-and-round layout with double track requires a certain minimum space. That's governed by the radii of the curves and the length of trains in the fiddle yard. I had some Setrack and points discarded by my son, and I found that a reasonable selection of stock would run round this – anything else could be confined to the outer track. Usefully, a board to accommodate it would fit through my loft hatch for storage. The length was governed by the available car – at that time a Volkswagen Up – so a baseboard size of 2' 2" by 3' 9" was chosen. Initially meant to be three boards long, the layout somehow only ever got two and has stayed like that. Although the length of trains is limited to around seven bogie vehicles, this has the advantage that they take less time for a circuit (50% of which is behind the backscene) which therefore provides more variety and interest for the viewer.

Many compromises have been made compared with the prototype. I had made station buildings from drawings in



An empty train of ballast hoppers heads south behind Direct Rail Services 66 421.



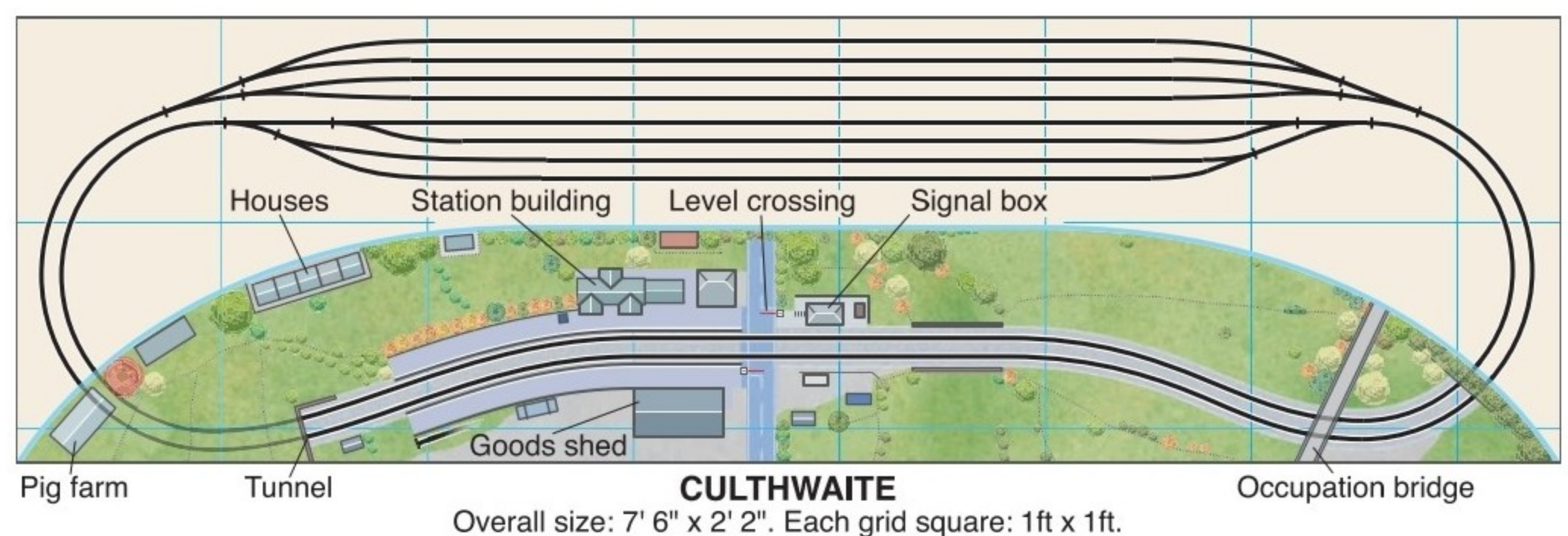
Mr Jenkinson's book when I was a teenager, but these had been stored after leaving home, together with some other random houses I'd built. Students of the line will know that Culgaith station opened a few years after the line and the buildings there were to a different pattern. Also, there was no goods shed but a large dairy building (which latterly housed the café). Then, to make the station fit the tracks, which needed to be curved away from the baseboard edge for aesthetic reasons, the platforms would have to be curved. Suddenly, my proposed layout wasn't looking too much like Culgaith any more.

Fortunately, not too far down the line, there is a station with a goods shed and curved platforms – Armathwaite. The fictitious 'portmanteau' location of 'Culthwaite' was born! It has the tunnel, crossing and signal box from Culgaith, and

the curved platforms, goods shed and station building from Armathwaite. The random houses have been used as part of the backscene, which bears no resemblance to either place. Indeed, where I have a pig farm above the tunnel there is, in reality, a rather smart bungalow. I am waiting for its owners to complain!

### Construction

One question that comes up many times at exhibitions is: "How long did it take you to build?" There are two answers to this. Firstly, "about six months" but was that full-time or an hour a week? I decided to log all the time for the construction itself and my second answer is "100 hours exactly". That



doesn't include the buildings and signals, nor, of course, countless hours of 'thinking time' but does include carpentry, electrics, scenery and track, etc.

The signal box is a Hornby Lyddle End product, and the crossing keeper's house is a kit from Knightwing, not unlike the bungalow provided at Culgaith. The signals are scratch-built. Convincing colour lights are easy enough using LEDs and a bit of brass tube. Working semaphores are rather more difficult. My preferred method is to use an old-fashioned 12V relay to provide the actuating power. It's easy to solder a brass rod to the moving part once the plastic cover has been removed. I make a baseplate from a piece of copper-coated paxolin (circuit board) and the signal post and ladder can be soldered to that. The tricky part is converting side to side movement of the relay into up and down movement for the signal arm. This needs a crank of some sort. I've used ladders and signal arms from MSE/Wizard Models, but I find their cranks and detail parts beyond my skill set. The

**A Colas Rail Class 70 on the log train meets a GB Railfreight Class 66 on the Wembley to Irvine china clay tanks.**

**The layout is based on Culgaith, but the station building and goods shed are from another S&C station, Armathwaite.**

result is a compromise, but one that works for me.

The geography and many small details come from Culgaith itself where possible. I've pored over countless photographs of the real location, and attempted to model the large tree behind the signal box, and the tunnel mouth, as accurately as possible. The former has since been removed or substantially reduced, no doubt at the request of Network Rail because a tree fall there would have obliterated the box and occupant.

### Operations and stock

As mentioned, the 2010-15 period saw an incredible variety of traffic, and an attempt has been made to reflect most of this on the model. During the chosen era, Northern Rail used a purple livery and, happily, suitable units have been produced by Graham Farish and Dapol. For some time the little passengers had to endure the dubious pleasures of a Class 142 unit all the way to



Carlisle, but the roster has since been supplemented by a Class 156 with Ribbleshead Viaduct branding, one of the few items I've actually splashed out on specially for the layout.

The core of the freight timetable is provided by coal trains from the main operators, GB Railfreight, English Welsh & Scottish, and Freightliner. One of the sets is loaded with black acoustic foam, which looks quite convincing. Other regular trains on the S&C to this day include the logs, now in Colas Rail hands, and the 6K05 engineering train worked by Direct Rail Services; both of these are much loved by photographers because they head south around the middle of the day. Log wagons have been made from N Gauge Society kits, with one cut-down Dapol Cargowagon as per the real thing, and the log loads cast in resin from masters made from silver birch twigs. A set of autoballasters for 6K05 has also been made from Society kits, with several empty spoil wagons of differing types on the back.

On the Down line, noteworthy trains in recent years have included the Wembley to Irvine china clay tanks and the Mountsorrel-Carlisle ballast train. While wagons for the former are available from Dapol, the MRAs for the latter have been scratch-built, again utilising resin castings for repeated parts. Also scratch-built, running on Lima container chassis, is the Rail Head Treatment Train. Transfers for many scratch-building projects such as these are made by Railtec, but I have tried inkjet decal sheets for some with surprisingly good results.

All trains have been weathered to a lesser or greater degree, and front and rear details



and tail lamps have been added, with couplings removed. They virtually fill the eight-road fiddle yard where each siding can take one long train or two short ones.

### Baseboards, control and lighting

The layout was built on conventional lines as outlined by the late, great Peter Denny. I've used frames of 2" x 1" 'skip wood' (i.e. anything I can lay my hands on) topped with Sundeala. For the section with the embankment and cattle creep, the Sundeala has gone under the baseboard frame. Scenery is also pretty standard, being expanded polystyrene covered with builders' plaster.

Very firmly in the dc camp, I use Gaugemaster units for train control, running off elderly Duettes, and a mixture of Hammant & Morgan and Peco point motors. To me, Digital Command Control slows everything down and doesn't add anything other than sound, which isn't good in a noisy exhibition hall anyway.

In an idle moment, I watched a video somebody had made of a show we attended, noting how many trains ran in the time seen. *Culthwaite* ran over twice as many trains as the show average and nearly double those of the next best layout – full vindication for the concept of a minimum space continuous run dc layout.

One last thing is lighting. Following club member Dave Wheelton's model, I've used an inverted uPVC gutter to hold a couple of 2' LED strips. We've often commented that we should win a trophy for 'best layout lighting in show'!

**Cars, a bus and a VW camper van wait at the level crossing. The Midland Railway signal box is a Hornby Lyddle End model.** ▶

### Time and money well spent

The layout has proved to be very successful and receives many favourable comments, one being 'punches above its weight'. For a minimum space layout that took 100 hours and an additional outlay of just £100 (six Streamline points, the crossing keeper's house and some Woodland Scenics products – everything else had been squirrelled away for many years) I think that's almost certainly true!

Thanks go to all the members of Macclesfield MRG from whom I've learnt so much over the years, and to operators including Tony Selby, Ian Willis, Jim Barry,

Ian Evans and Dave Bates. It needs a team of at least three to keep the Settle & Carlisle this busy for a full weekend.

### See it at Thirsk and Leeds shows

*Culthwaite* is booked to attend the Thirsk Model Railway Show at Thirsk Town Hall, Westgate, Thirsk YO7 1QR on 26 July (see Societies & Clubs for more details) and the Leeds exhibition on 24 & 25 October.





# MICRO MARVELS

THE SMALL LAYOUT GALLERY

## Whittingly Hospital



Modelling and words: **MICHAEL CAMPBELL** Photography: **CRAIG TILEY**

I must confess to being a serial micro layout builder, partly due to lack of space and varied interests, but also because I enjoy a project I can complete to a good standard in a reasonable time. I also like

unusual railways, normally modelling narrow gauge in OO9, or occasionally O-14, although I have collected a few of the superb industrial locomotives that have become available in recent years in OO gauge.

The layout was inspired by the Whittingham County Mental Hospital Railway, a private line from Grimsargh on the Preston & Longridge Railway. Opened in 1889, it closed in 1957.





Having not built a layout in OO since my teens (other than train set-style layouts for my son some years ago) a micro layout was deemed to be an ideal project. That's when the Peco RAIL200 Model Layout Competition was launched...

### Prototype inspiration

Inspiration was taken from former hospital railway systems. In the late 19th and early 20th centuries, hospitals were constructed to house patients suffering with mental illnesses who, at that time, might otherwise have been destitute. Often located in rural surroundings, several had railway spurs or branches from the nearest main line railway station to bring in building materials, and then to serve the large institutions with coal and other deliveries – some even ran passenger services for staff and visitors. The best-known systems were Whittingham in Lancashire and Hellingly in Sussex, which explains the name of this layout. These had short trains, compact facilities, and rural settings, all of which make for an attractive model.

I was drawn to the Modular category, as I thought it would be something rather special to see a large number of RAIL200 entries linked up, as indeed they were at the National Festival of Railway Modelling in November last year [Ed. See *RM January 2026*]. But what could I fit onto the small baseboard that captured some of the key features of these hospital railways? Whittingham featured a cramped yard with

sidings, a weighbridge, an engine shed, a spur to a boiler house, and a platform with a surprisingly grand overall roof. I came up with a plan that included a boiler house, weighbridge, stores siding, an engine shed, and a platform long enough for a couple of six-wheel coaches, with a somewhat less grand overall roof. Although the through modular track serving the platform is disconnected from the yard, the plan is to re-lay this with a point to form a crossover with the three-way point (now that the competition is over) to make a layout that should be interesting to operate.

### Building the model

By the time I started construction, there were less than five months left to the competition deadline, and with lots of bespoke brick

▲ A Hornby Peckett Class W4 0-4-0 saddle tank is stabled outside the single-road engine shed. This building was scratch-built using parts from a Wills Craftsman two-road shed, and arch-topped windows from the same range.

buildings to construct, I needed a short cut. The Wills range includes arch-topped windows (ref.SS71) that were ideal; I actually started with the Craftsman kit for a two-road engine shed (ref.CK12) plus some extra sheets with and without windows, making my own buildings to suit but using key dimensions and useful parts (like the doors)

▼ An overhead view of the layout, built within the Peco RAIL200 Competition space limit of just 29½" x 12" (750mm x 305mm). The main running line, left isolated for the competition, will be connected to the three-way point.





▲ **Locomotive No.1 on the Whittingham County Mental Hospital Railway was London, Brighton & South Coast Railway Stroudley D1 0-4-2T No.2357. When withdrawn in 1955, it was the last of its class in service. The coaches are converted London & North Western Railway brake vans. Photo: Mike Morant collection**

from the kit. The weighbridge is a Ratio kit (ref.524), the boiler house chimney came from a Ratio kit too, while the station overall roof is adapted from the venerable Dapol kit. I did just have time to add some lighting with lamps on the platform and on the outside of some of the buildings, and with LEDs under the platform canopy, inside the weighbridge hut and engine shed. The lights can be powered from a transformer, or from six AA batteries.

I felt a backscene was necessary, not just to frame the scene and remove external distractions, but also to help protect the layout in transit, although this had to fit within the competition dimensions, including the 140mm maximum height (to fit in the shipping box). 6mm MDF was cut and fitted on top of the board, within the width and length restrictions, and painted sky blue. The through track sits on a 3mm deep strip of wood (supplied in the kit to ensure compatibility when the Modular entries were joined together), so the rest of the board was brought up to the same level with 3mm foamboard. The yard track is Peco code 75, installed with appropriate insulating joiners and wire droppers ready for wiring, and holes ready for point motor rods, although it is not yet wired for operation. The track was ballasted with granite chippings, and with DAS clay and sand in the yard area painted to represent ash, and suitably weathered.

It's difficult to get away from a flat board look in a small layout, but I added some rising ground to the rear, while at the front of the layout I dropped part of the board by about 2mm to create a pond. The pond was made with Deluxe Materials Aqua Magic over the smoothed and painted ply base.

Like the Whittingham County Mental Hospital Railway, pictured above on 1 May 1954, the layout features a pond. This was made by slightly cutting down the front of the supplied competition baseboard.

Greenery used conventional methods, with static grass, and trees used Woodland Scenics armatures, hopefully giving the impression of parkland surrounding the hospital. A few details and figures from suppliers including Dart Castings, Monty's Models and Dapol complete the scene.

### What's next?

I was delighted that *Whittingly Hospital* was shortlisted to appear at the NEC alongside other Modular entries. It was great to see





▲ The station overall roof – based on the structure on the Whittingham railway – was adapted from a Dapol kit, while the weighbridge, its hut and the boiler house chimney are all from Ratio kits.

such a variety of ideas connected with trains passing through, and to see the interest that

the competition generated. But what next? The plan was always that this should become an operational layout. As well as the extra point, fitting point motors, a control panel, and wiring up, it will need a fiddle yard at the right-hand end to complete a run-round loop and provide a destination.

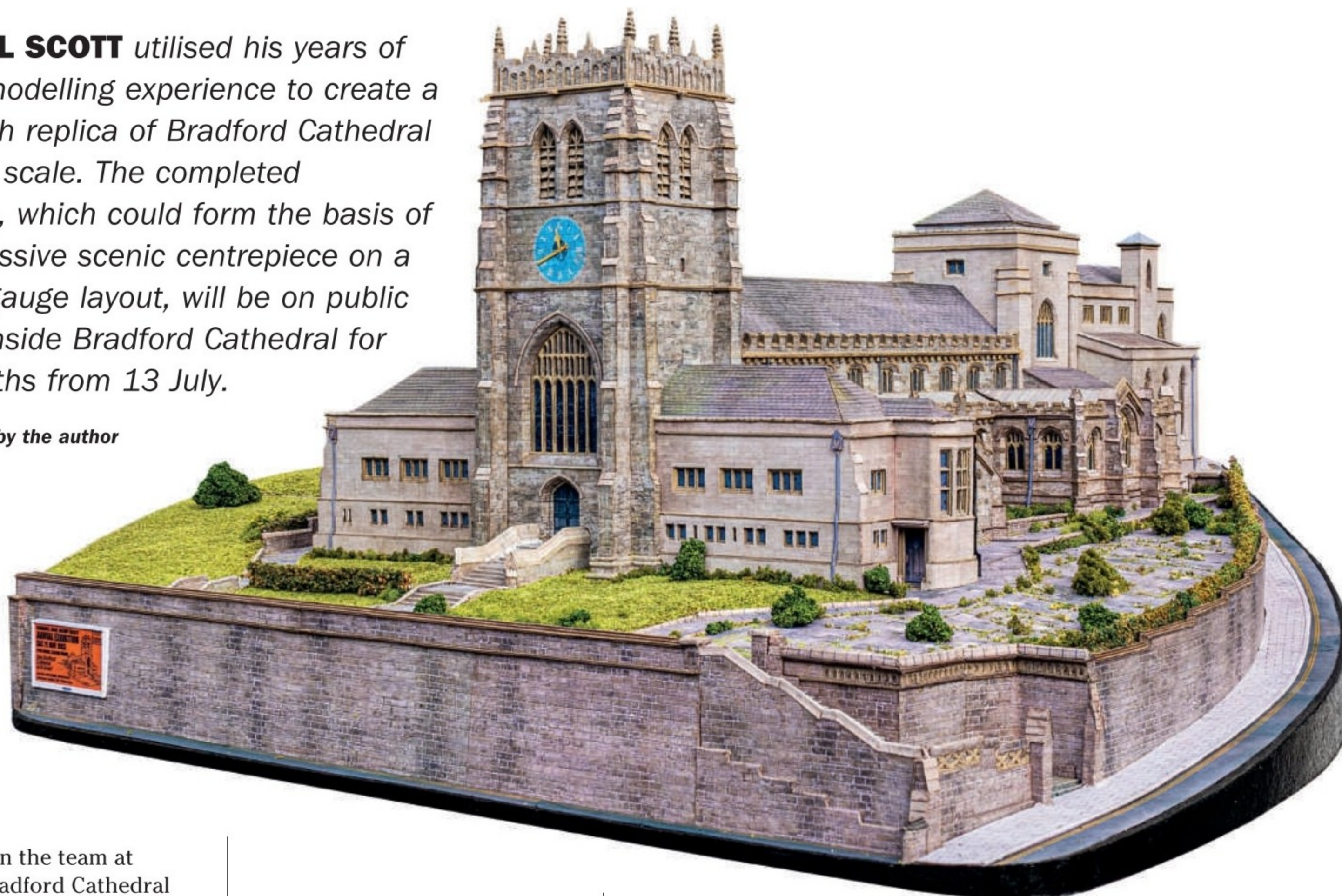
I plan to make a deeper backscene and add lighting to create a cameo-style layout. I also need to experiment with automatic couplings for shunting, and prepare and weather stock, to allow full operation. So, there is still plenty to do to finish this enjoyable excursion into OO.



# A card cathedral in N

**MICHAEL SCOTT** utilised his years of railway modelling experience to create a mammoth replica of Bradford Cathedral in 1:148 scale. The completed structure, which could form the basis of an impressive scenic centrepiece on a large N gauge layout, will be on public display inside Bradford Cathedral for two months from 13 July.

Photography by the author



When the team at Bradford Cathedral contacted me to ask if I'd like to make a part of the cathedral – for use on my N gauge layout *Chandwell* – as part of the Bradford 2025 City of Culture celebrations, I was quick to say yes. For many years, I have poked gentle fun at the city where I live through the silly stories of Chandwell ([www.chandwell.wordpress.com](http://www.chandwell.wordpress.com)), and I was keen to do something that would allow me to cast the city in a more positive light.

As far as cathedrals go, it's a small one, and didn't seem too complicated to model. I agreed to make the whole thing, estimating no more than nine months' work. In reality, it became a project that grew steadily and consumed me for nearly two years...

More than 20 months later, I was finished. Sitting on a base that is 64cm x 54cm, the building is almost 49cm front to back (over 72 scale metres), and is by far the most ambitious model I have attempted to date.

Although there's not a train to be seen, this is very much a

*Chandwell* model, and is built using traditional card modelling techniques available to everyone. It's covered in texture papers downloaded from the Scalesscenes website, the rest of it being card.

## Where to start?

Large and complex buildings demand a different mindset to single-structure projects – one that prioritises planning, subdivision, and patience. Some parts of the building date from 1458, whereas others were completed in the 1960s, leading to a variety of styles from the Gothic to the brutalist. There are hundreds of windows, many with elaborate stone tracery. The north side is different to the south. And the whole building nestles inside a hill – the west door being 25mm (a scale 3.7 metres) lower than the north-east door on Stott Hill.

I was lucky to have access to some of Sir Edward Maufe's architectural drawings of the 1950s extension, but I was as blind as anyone else when it

▲ The completed 1:148 scale model of Bradford Cathedral, showing the full footprint and the steep change in ground level from west to east.

came to the dimensions of the earlier parts of the building. Before anything else could be done, understanding the building's overall layout and footprint was essential.

A combination of pacing around the building, Google Maps, and the occasional planning application on the Bradford Council Planning Portal helped me to work out how the building was put together.

It became clear that this project would work far better when approached not as one huge building, but as a collection of smaller, self-contained structures. I've completed many buildings on my layout, so this immediately felt like familiar ground.

It took a couple of months of observation, measuring, pondering, and trial and error to get a working plan completed. The building comprises 23

individual elements, each of which could be approached as a single model (Fig.1). My plan was to complete one part before moving on to the next.

An early consideration was whether to model the 'weeping chancel'. Many old churches have a chancel that slants slightly, usually towards the north. The cathedral in Bradford has a 1.5° slant in the eastern section. Subtle though it is, I decided to model this.

## Considering the hillside

If the landscape shapes the building, it's far easier to design for that from the very beginning: the hillside needed a clear strategy from the outset. This meant deciding early on how the model would ultimately be presented. It was to be a self-contained model on a base, with the aim to properly show the steep hill upon which the cathedral sits. This meant

**Fig.1**

Overall building plan at 1:296 scale, showing the cathedral divided into 23 self-contained elements, each treated as an individual model, and the 1.5° weeping chancel.

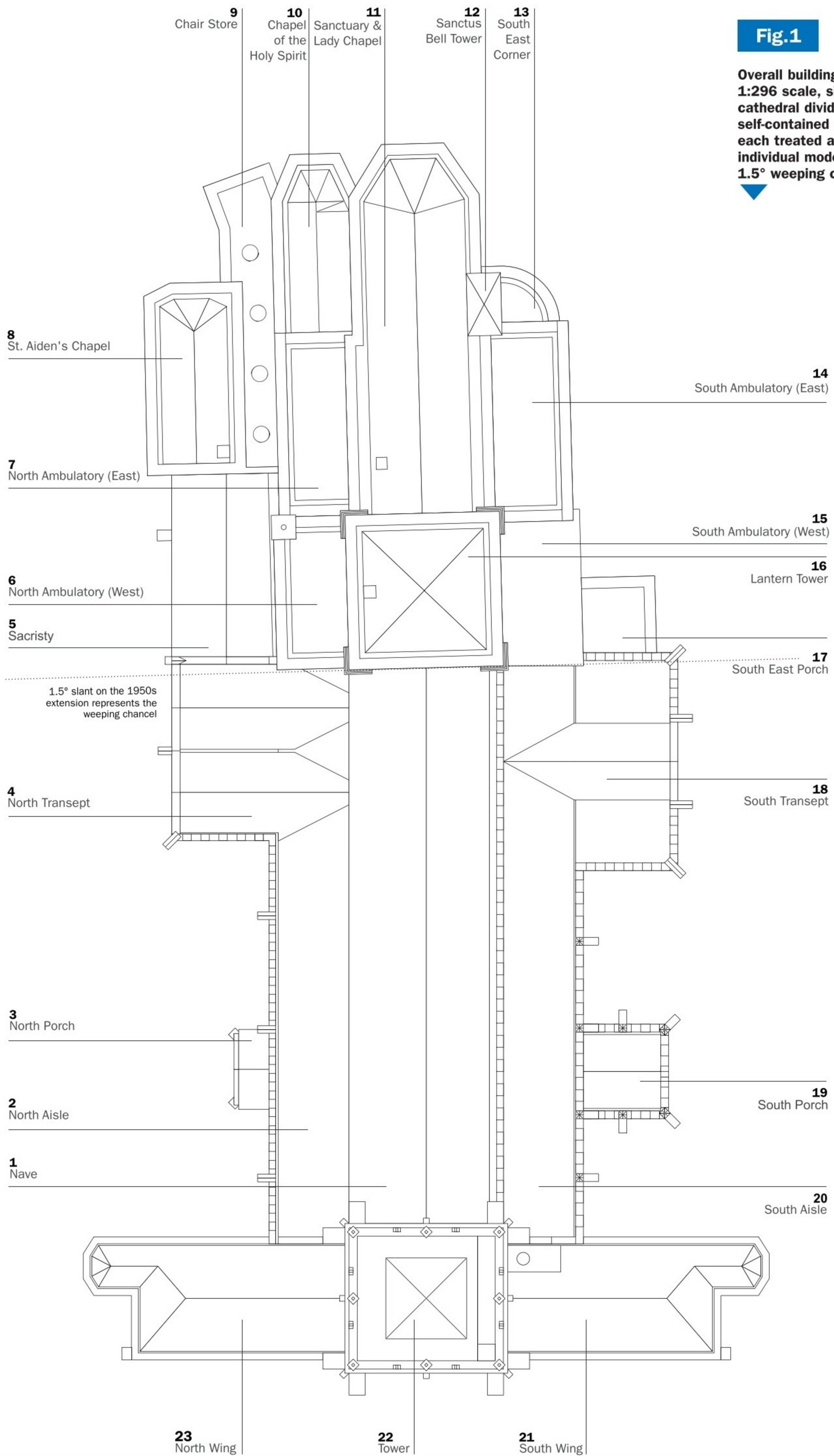
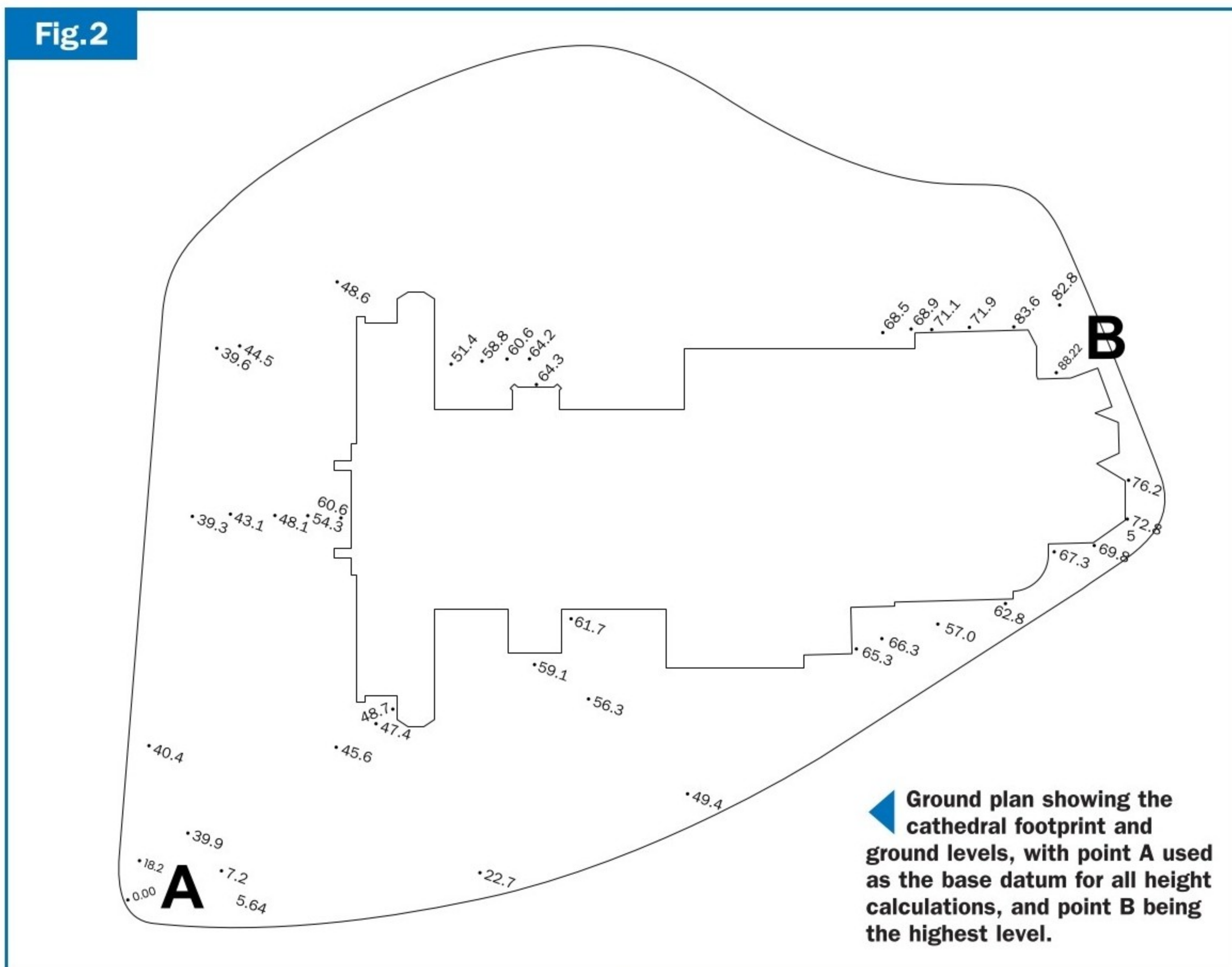


Fig.2



having to model part of Church Bank, which, at the western end of the cathedral, is 7.5 metres lower than the western door. In 1:148 scale, that's 51mm.

It is much easier to build ground around a model than it is to try to arrange the model onto uneven ground, so the model was built from the outset at the correct height relative to

the lowest datum – point A on Fig.2, which shows the overall ground plan. The model's base follows the landscape features around the cathedral – Church Bank and Stott Hill in the south and east, the retaining wall along the west, and one of the paths in the north.

I found and downloaded a tree protection document from the

council's planning portal, and it had a full ground elevation survey attached to it. Using point A (Fig.2) as reference, it was easy to take these heights above sea level and turn them into millimetre heights on my plan. From that point on, everything had a known reference. The land therefore rises from 0mm at point A to

88mm at point B – a scale increase in height of over 13 metres.

To handle the change in elevation, each of the 23 parts was therefore drawn with a foundation. I included references to the baseboard and other useful levels so that I could be sure that it would all fit together properly as the model developed. Fig.3 shows three elements with the foundations included, making it clear how each part relates back to the baseboard datum.

Building every wall and buttress with its foundation included – a method applicable to any sloping-site structure – ensured that everything would align correctly when it came to assembling the parts.

Approaching a hillside building in this way gives opportunities to easily model the subterranean parts. The north elevation has almost one storey of the building which sits below ground, separated from the hill by a narrow passageway. This can be seen in Fig.4 – the North Elevation.

### Making a start

The nave made the logical starting point, as everything else connects to it. The east end angles were critical in achieving the 1.5° slant for the weeping chancel. The north wall needed to be just 2.7mm longer than the south one.

It was interesting that both sides are different – the south having three-pane windows and crenelations, and the north having narrower two-pane windows and no crenelations.

The differences can be seen in Fig.5, and the transition in the roof caused by the skew is clearly visible, as are the differences in windows, crenelations, downspouts, and overall wall height caused by each aisle having a different roof level.

Much of the time I spent on this model was in careful observation of the real building; I don't think all of this detail would be apparent from a single visit, or one set of photographs.





The view from Stott Hill showing the significantly higher ground level at the eastern end of the cathedral.

Taking care at this stage pays dividends later on, as mistakes are easier to put right here than after the model is complete.

A good example of where taking more care would have been useful is the crenellations – compare those on the south side of the nave (Fig.5) to those on the south aisle (Fig.6). The ones on the nave are taller. I was many months into the build before I realised that I'd made the ones on the nave too tall. It's a subtle difference and probably one most observers wouldn't spot, but it was a useful reminder that small proportional errors can creep in early and stay with you.

Viewers of my YouTube channel (search for 'Chandwell') will be familiar with several other errors along the way, most of which, fortunately, I was able to put right.

Mistakes aside, the whole model followed the same process – each of the 23 parts was drawn roughly at the very start, and then fleshed out in detail, just in time for work to start on that part.

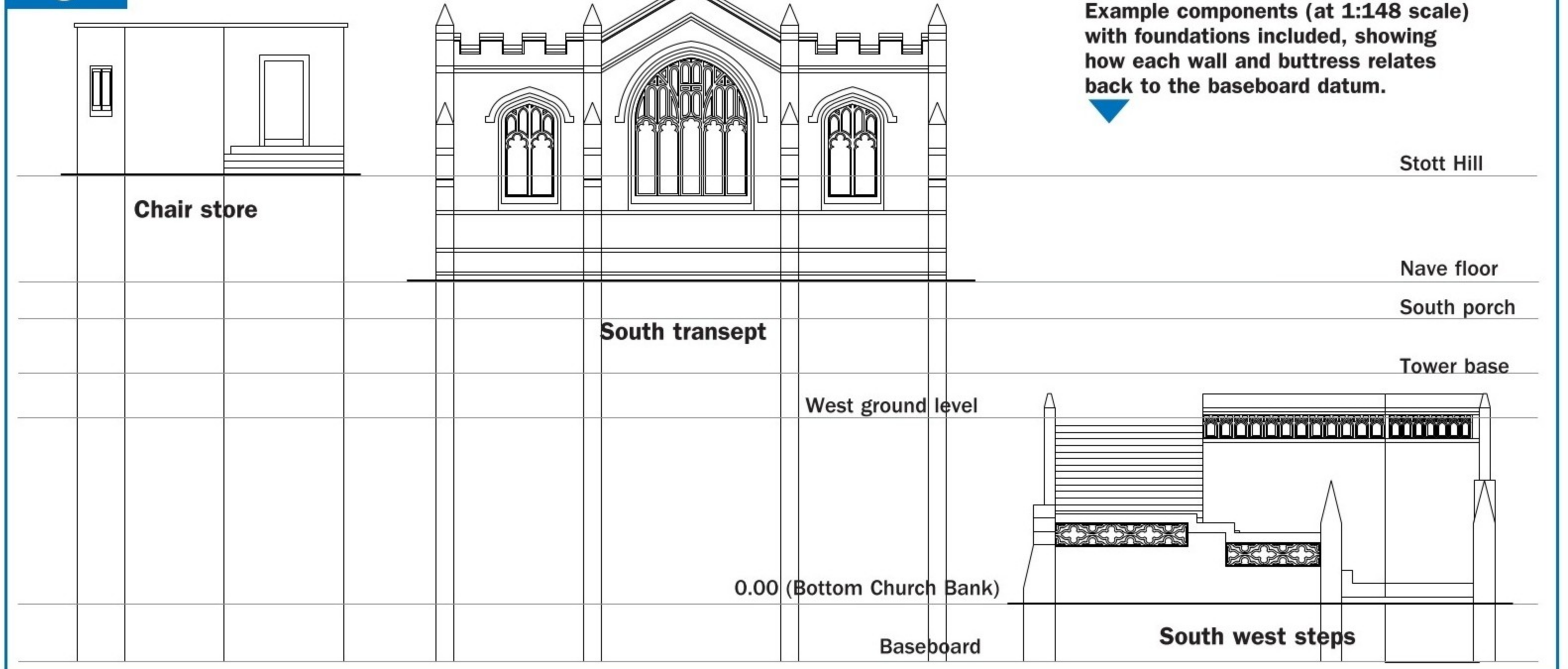
**Guesstimating**

Precise measurements were limited. There are not many plans, and the only measurements I could get were at ground level. Like many



The part-complete model assembled on its foundations, with full-height walls and buttresses used to confirm alignment across the sloping site.

**Fig.3**



modellers, I relied on 'brick counting' and a fair amount of informed guesswork.

A good example of this is the arrangement of the buttresses, dentils, gutters, string courses, and crenelations on the south aisle (Fig.6).

The length of the aisle and the width of the porch were known. I knew the number of crenelations, and the number of dentils. But it was not a simple case of dividing one by the other. It took a lot of jiggling around and studying of reference photos to understand how it all fits.

Looking at Fig.6, it can be seen how the crenelations next to the buttresses (B) are narrower than the full ones (A). The distance between the dentils (C) is inconsistent when they approach the buttresses (D).

The height of the porch (E) was guessed as best I could, trying to keep the drawing looking like the real thing, taking into account the fact that I was looking at it from the ground. The height of the hopper head (F), judged against the window beside it, helped me to work out the porch roof shape. The positions of the windows could then be calculated based on counting dentils (G). The tops of

**Fig.4**

The North elevation at 1:296 scale, showing the below-ground storey at the eastern end and overall fall of land east (left) to west (right).

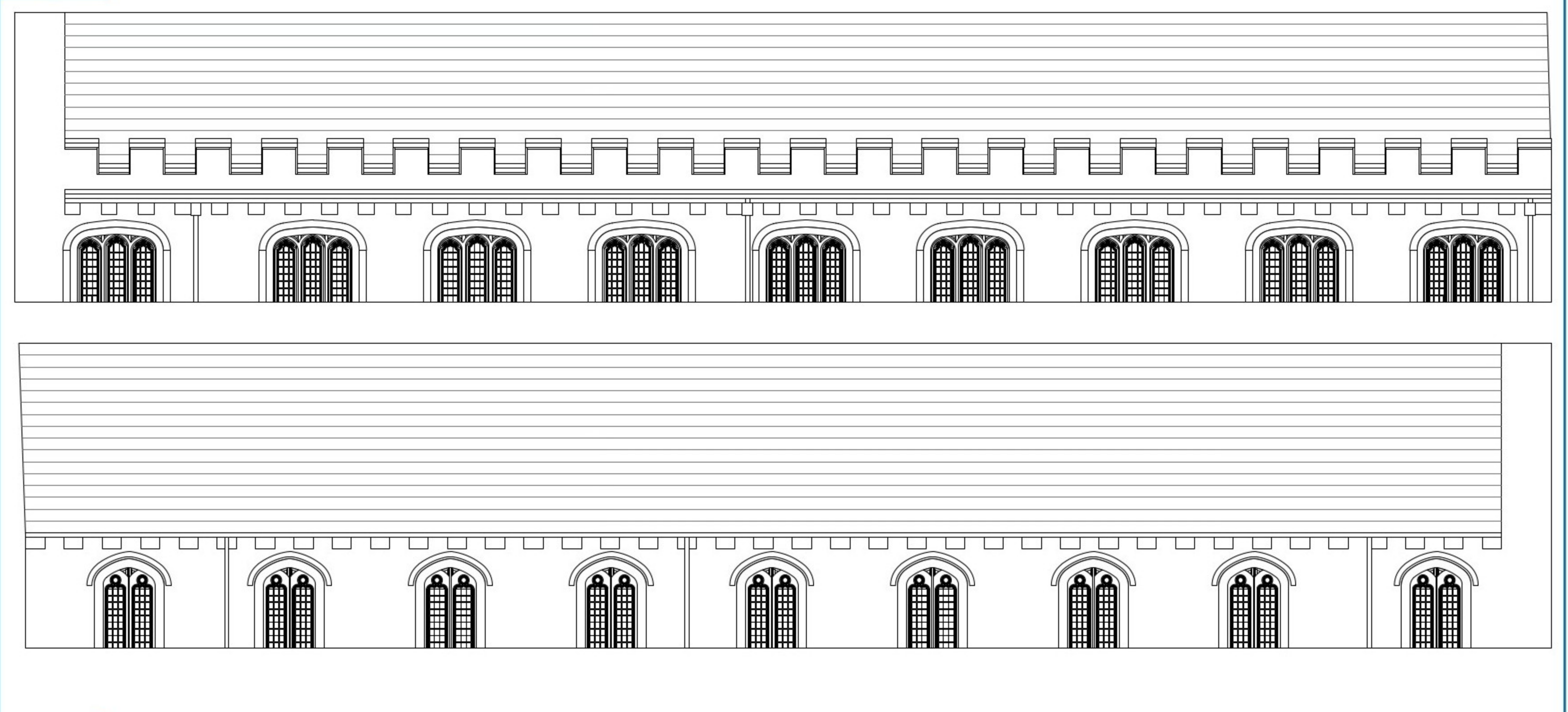


the buttresses (H) were just guessed outright!  
 This kind of informed guesswork is familiar to most scratch-builders, and the key is combining several imperfect clues rather than relying on just one.

**Keeping momentum**  
 Concentrating on a single model for almost two years was sometimes a challenge, particularly alongside day-to-day work and family life. Breaking the cathedral down into self-contained sections

proved invaluable, not just in managing the work but in keeping momentum going, as each completed element brought its own sense of progress.  
 Every part of the building came with at least one new

**Fig.5**



The nave elevations at 1:148 scale, comparing the north (bottom) and south (top) sides and highlighting differences in window arrangement, crenelations, wall height, and angled roof.



challenge, and each solution fed back into improving the overall model. Techniques developed for one section, such as the wall capping on the 1950s extensions, could then be repeated elsewhere, saving time and helping maintain consistency.

The project became an intensive learning exercise in what can be achieved with card and paper, ranging from the practicalities of handling a model over half a metre long, to the small details, such as forming rainwater goods with decorative motifs.

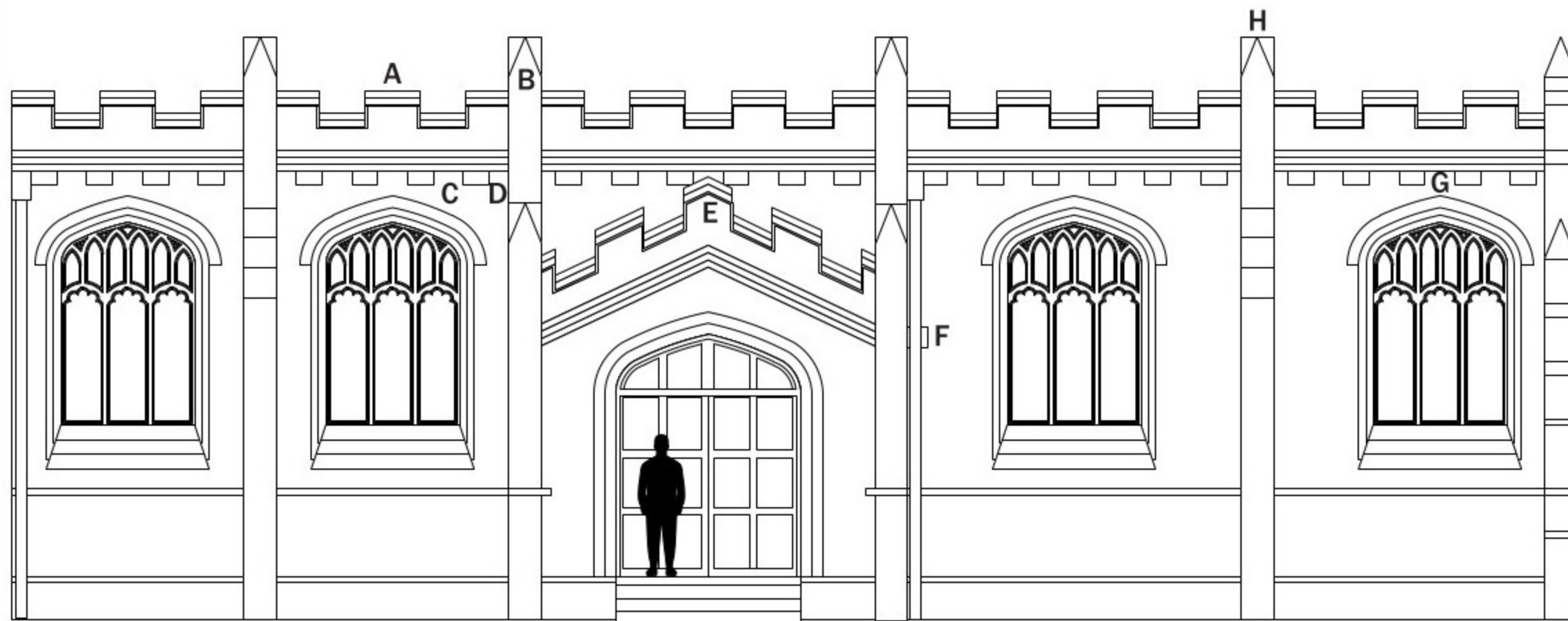
This article has only scratched the surface of a two-year build, but I hope that it encourages others to tackle large or intimidating structures, whether a cathedral or something more modest.

**See it at Bradford Cathedral**

For further details about the display of the model at Bradford Cathedral, which will be for a period of two months from 13 July 2026, go to: [www.bradfordcathedral.org.uk](http://www.bradfordcathedral.org.uk)

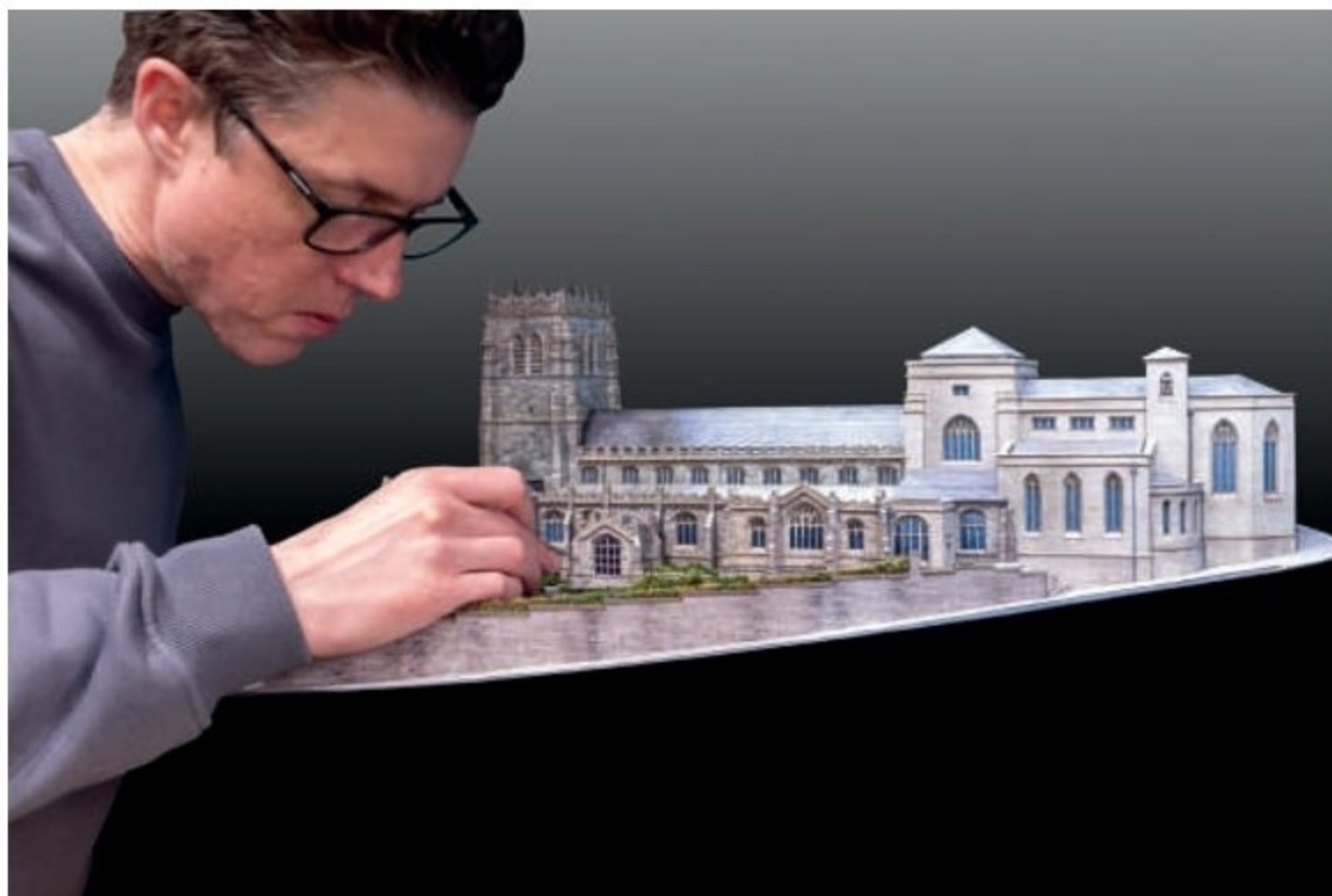
A general view of the completed model. Furthest from the camera is the east end extension, designed by architect Sir Edward Maufe. Approval for the extension was given in the early 1950s, and construction took place over the following decade. The new section was dedicated in 1963 by Archbishop Coggan.

**Fig.6**



45-degree buttresses not drawn

▲ The South aisle and South porch at 1:148 scale, showing the relationship between buttresses, dentils, crenelations, and window spacing.



Through the Railway Room Door

# Brent – change for Kingsbridge

This article could be titled 'Through the Shipping Container Door', for that is what **JASON SCHOFIELD** has used to house his OO gauge Great Western Railway layout – in Cyprus!

Photography by the author



**A**s a child, I spent many happy days with my grandparents. My granddad had a model railway in the spare bedroom, where I would happily empty coal trucks in amongst the ballast. He grew up in Plymouth, and was a Great Western Railway enthusiast through and through. He spent his days trainspotting in Devonport and Totnes, which later became the two stations on his layout.

When he passed away in 1998, his railway passed to me. I had no idea how to model a railway, but I've always been handy with wood, and so set to work on supports in the loft. Sadly, a house move prompted dismantling, and with a second move in 2006, I hadn't even unpacked the rolling stock. This time we were moving to Cyprus.

Wind forward to winter 2020/2021, and I had the idea of using a shipping container as

a railway room. This meant if we ever moved, it could be transported easily without having to interrupt progress. Excellent! So, on 1 February 2021, a 20' x 8' container was lowered onto the driveway.

## Converting the container

The main issue with a container in Cyprus is the sun. I constructed a secondary roof above the container to prevent direct sun.



▲ The church is one of several card models built by the author's late grandfather, which have been reused on the layout.

The container sits up against the house on one side, and has an 8' fence on the other side and at the closed end. The secondary roof extends above the doors into the container. So, the whole room sits in the shade.

Containers have vents in opposite corners to allow air flow. These should not be blocked, as they let hot air out and maintain circulation, which prevents any mould building up. This does mean that the air

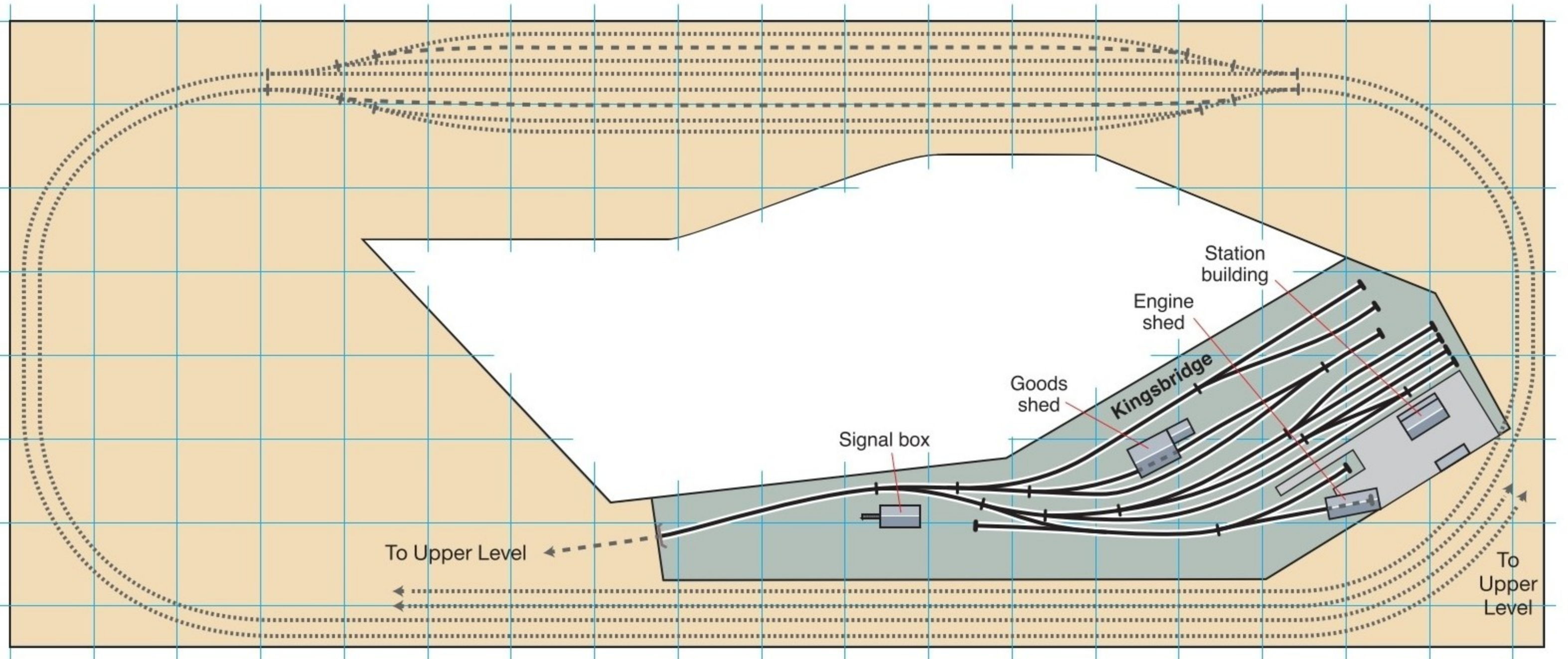
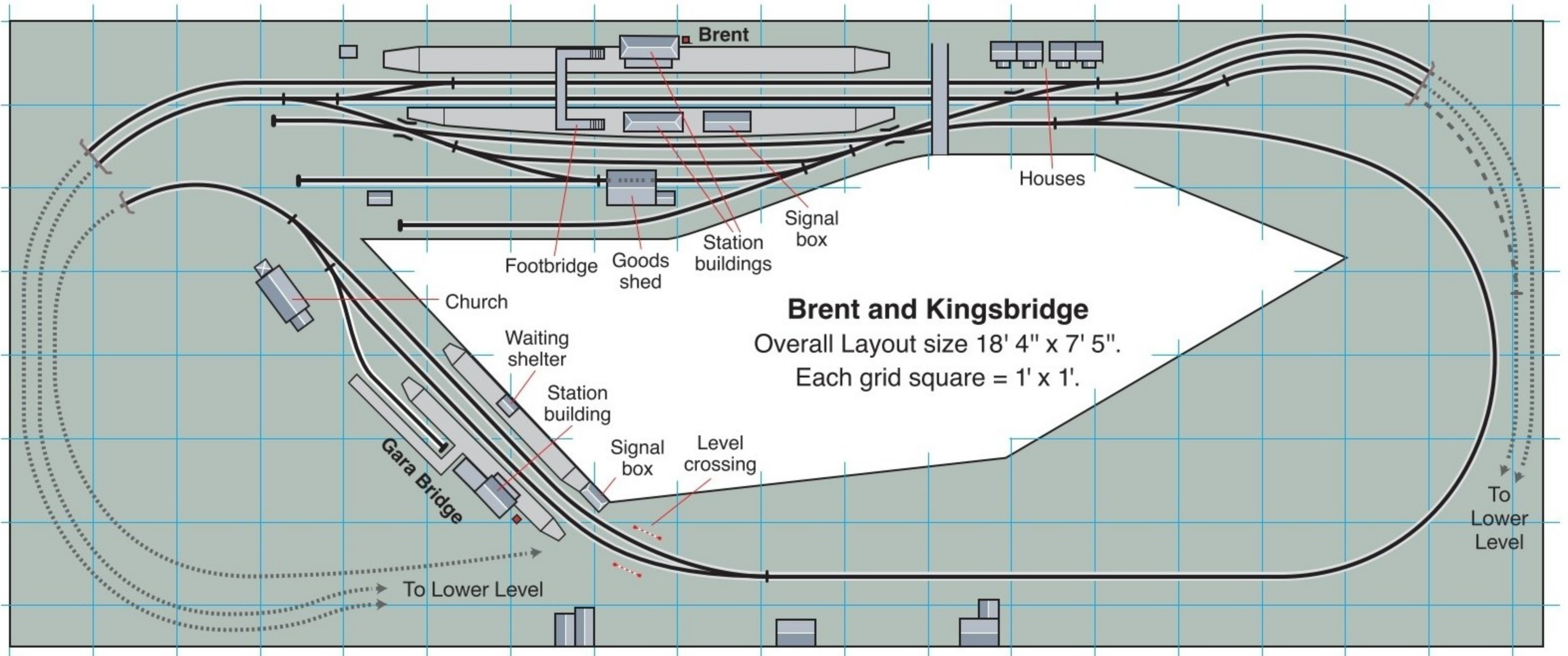
◀ Collett King 4-6-0 No.6002 *King William IV* (a Hornby model) heads a Paddington-bound express through Brent.

temperature inside mimics that outside. I was tempted by air conditioning, but after much reading decided against it. Lowering the temperature presents a risk of condensation. Hot air holds more moisture than cold air and this has to go somewhere when you cool the air down. Wood is pretty absorbent and so would have happily soaked up moisture as the air cooled. This would lead to serious problems. Yes, it's hot in the summer, but no more so than outside or indeed inside the house where a normal railway room might reside.

Cyprus is hot enough that I had to research the expansion coefficient of the

▲ A Class 117 Diesel Multiple Unit departs from the intermediate branch station of Gara Bridge. The signal box is a Superquick card kit.





1 The interior of the shipping container prior to work commencing.



2 A substantial timber framework was constructed.



3 The fiddle yard prior to Brent station being added on top.



4 The branch terminus of Kingsbridge taking shape.



▲ **BR Standard 4MT 4-6-0 No.75027 (Bachmann) hauls an Up goods train.**

track to avoid any unwanted S-bends appearing. Winter temperatures can be close to zero occasionally, but summer can see 45°C. I have left plenty of gaps in off-scene areas to allow for expansion.

The only air quality I control is humidity. Smart plugs and sensors enable an automated routine to switch on a dehumidifier if humidity reaches 60% and turn it off once it dips below 50%. This keeps everything dry enough and comfortable.

**A view of most of the completed layout: Kingsbridge is on the left and Brent on the right.**

The container is made of Corten steel. It's not easy to fix items to it. I created a skeleton of 2" x 2" N-shaped timber ribs throughout. Each rib is only fixed to the wooden floor and to the next rib. The top rib is pinned to the ceiling by its two uprights and screwed to them. This gave a solid structure. Once electrics and lighting had been fitted, I was ready to start on baseboard construction.

### Planning and construction

Before I surged ahead, I had to decide where to model. I came across Brent by accident while looking for a GWR location with a main line and a branch. When I realised that the next station up the line was Totnes and a

few stops to the west was Devonport, my mind was made up. It had to be Brent.

Anyrail software was used to plan the layout. It is a double loop which drops, so that after a full lap around the room it is beneath where it started. This lower area is the eight-road fiddle yard. It sits beneath Brent station and feeds four tracks to the Up main line and four to the Down main line. The branch curves off and stops at Gara Bridge halfway along, then winds around and under itself before ending at Kingsbridge.

I also added a layer in the Anyrail software to plan where each piece of timber would go, so that I would avoid having timber beneath a point once track was laid.





▲ Collett '6100' Class large Prairie No.6129 works a branch train. The backscenes are by iD Backscenes.

Construction is 'belt and braces' with 2" x 1" and 2" x 2" used in the main, with some 3" x 2" and even 4" x 2" for unsupported overhangs. I wanted to keep the underneath clear of supporting woodwork so as to utilise the space for other things, particularly in the event of a house move.

### Track, electrics and wiring

The layout uses dc wiring, as I was restricted by my granddad's ageing locomotives. These would have been difficult and expensive to convert to Digital Command Control.

▼ The large Prairie arrives at Gara Bridge, with a milk tank and horsebox on the rear of its train. The pub on the hill is another Superquick kit.

I have used Peco code 100 track and Electrofrog points. I used Gaugemaster GM500 self-latching relays to change frog polarity and carry power through the point. It was at this point that I realised I had to learn how to solder. At first, my granddad's ageing soldering was far better than mine. It was pin-prick small and neat. Thankfully, mine has improved as I've gone along.

Controllers are from Gaugemaster, and supply each main line track via a bus main running beneath, with droppers on each section. Areas beyond points are fed dependent on their direction and the GM500 controlling them.

Brent and Kingsbridge station yards can be controlled independently using the cab control method. A flick of a switch leaves the

branch operated by one controller, and the yard can be shunted using another.

The control panels were made using 3mm ply. A plan was printed, laminated and then placed on top. Holes were then drilled through for each toggle switch and LED. PC ATX power leads were used to provide a secure 24-pin plug and socket arrangement. Four of these are required on the main panel to allow it to be unplugged and removed easily for maintenance. The Kingsbridge panel uses two of these. They were also used on the three sections making up Brent station. These are removable should I ever need access to the fiddle yard beneath.

### Buildings and structures

I wanted the main buildings at each station to look like the prototypes. I must thank Andy Pearce at Rail Model in Plymouth. He took the black & white photos of the relevant buildings, and from those produced laser-cut MDF kits for me to paint and assemble. My wife painted the tiny window frames and doors.

Fencing and smaller structures are from Scale Model Scenery, in addition to the platform papers. I also tried to utilise as many of my granddad's card models as possible. The church was his and bears some scars, but since he is the reason I've done this, I'm happy to accommodate them.

### Scenery and backscenes

The scenery was the thing that I dreaded. I am a technical, precise, black and white sort of person. I am not artistic. So, attempting something with very few rules was scary.

I had for my guidance an excellent book by Ken Williams and Dermot Reynolds – *The Kingsbridge Branch*. It gave me lots of



invaluable and interesting information and pictures. It provided the inspiration for many scenic features as well as the buildings mentioned above.

After a laborious start with wire mesh and plaster bandages, I moved on to polystyrene. This was fast, and enabled me to complete sectional pieces in the house during a hot summer in 2024. The scenic material is from Woodland Scenics, and after watching many videos, I actually enjoyed the freedom of creating the landscape.

In sync with the landscape, I fixed scenic backboards in place to give me a boundary for each polystyrene section. Rightly or wrongly, I finished up by adding iD backscenes to the backboards. I'm not sure whether this would have been easier if done first, but here we are. It worked, just!

After adding some blending between landscape sections and up against the backscene, I added copious amounts of trees to try to create the feel of the branch line.

### Future improvements

I am currently exploring Kadee magnetic couplers, as I would like to carry out some realistic shunting in each station yard. I currently use some clear acetate, arched to create an uncoupler. This allows me to uncouple engines on the branch line and run them round to travel back. It's free and it



works okay, but I know it can be improved. I'm sure I'll find plenty more to keep me busy!

### Acknowledgements

In addition to those already mentioned, I must thank those close friends who regularly come to their second homes in Cyprus and carried over all sorts of supplies for me. Thanks also to my wife who gave me praise and encouragement whenever it was needed

▲ These semi-detached houses are more Superquick kits.

– which was often. Finally, thanks to my granddad for leaving me with his unexpectedly rewarding hobby and the inspiration for making *Brent*. He read RAILWAY MODELLER since its release in 1949 and would never have imagined that his locos would one day be in it. I wish he were able to read this edition.



▲ A Hawksworth '9400' Class pannier tank (a Bachmann model) shunts wagons into the goods shed at Brent.



An 'ugly duckling' in 7mm

# Building a Collett '9700' 0-6-0PT in O

**JOHN COCKCROFT**

*describes how he used a conversion kit produced by Minerva Model Railways to construct one of the small fleet of Great Western pannier tanks that were equipped with condensing apparatus.*

*Model photography by the author*

Over the last few years, I have written a number of articles in RAILWAY MODELLER about building O gauge locomotive kits. I have tried to introduce some additional element to each article, starting with a simple tank locomotive and progressing to a tender locomotive with outside cylinders – the latest article chronicling, in two parts, the building of a Great Western Railway Star Class 4-6-0 (RM November & December 2025).

There are other possibilities still to discuss, such as locomotives with outside valve gear or outside frames. But here I am going back to basics, to describe a locomotive kit that



▲ The author's completed '9700', finished as No.9704.

can be assembled relatively easily – because it is, in essence, an adaptation of a ready-to-run model.

Years ago, in the 1960s and 1970s, there were quite a lot of 4mm scale kits on the market that were basic body kits designed to fit on proprietary chassis. Wills Finecast made a range of prototypes that fitted onto Hornby Dublo mechanisms, notably the Southern R1 0-6-0 tank chassis. They were an excellent way to develop skills for building something different and gain confidence to tackle more complex jobs. However, this approach is less common nowadays, which is why I was delighted when I first saw the

'9700' conversion kit produced by Minerva Model Railways.

Minerva has, in recent years, developed a very good range of ready-to-run locomotive and rolling stock models made in China. These include the Great Western Railway '5700'/'8750' 0-6-0 pannier tank [Ed. See *Reviews*, RM November 2017]. There were hundreds of these locomotives built, and they are a must for almost any layout based on the GWR. However, there was one small group of variants that were not feasible for Minerva to produce as part of its R-T-R range, so instead the manufacturer released a

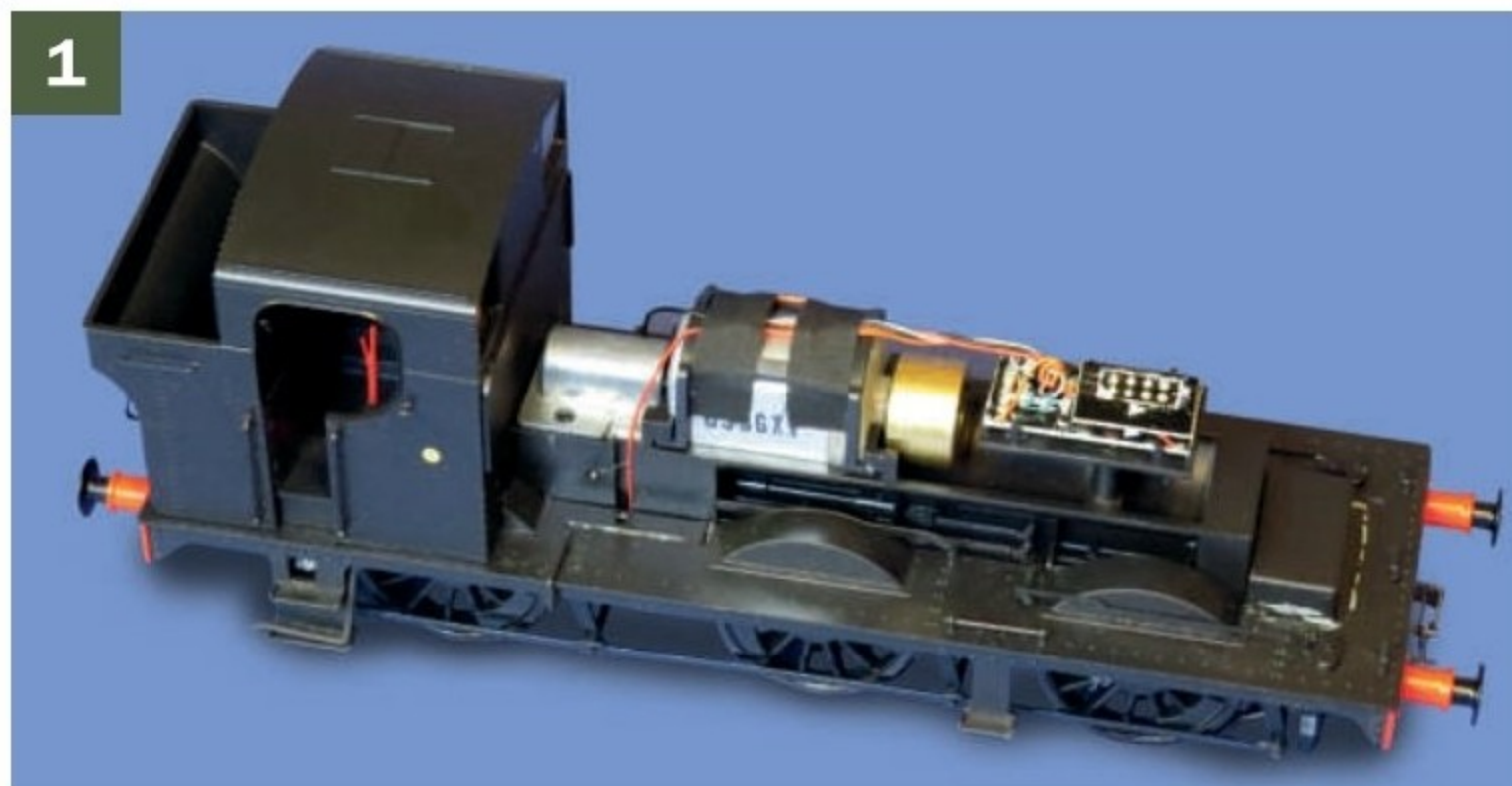
conversion kit based on its existing '8750' model.

## Ugly ducklings

The '9700' sub-class consisted of only 11 locomotives. These were completed from 1933 onwards to take over operation of meat van trains to Smithfield Market in London, which was accessed via the Hammersmith & City Line, and they were also used for Empty Coaching Stock duties at Paddington. The '9700' was a condensing version of the '8750', enabling their use on Underground lines. To fit the condensing pipes to the smokebox, the pannier tanks



▲ The standard version of the Minerva Model Railways '8750' pannier tank, as supplied ready-to-run. Photo: RM Archive



The kit comes with the required parts of the Minerva '8750' model included (chassis, footplate, cab and bunker). All you have to do with the chassis is test that it runs smoothly – which mine did!

were cut back, but to increase water capacity, the tanks extended down to the footplate at the cab end, thus giving them the look of hybrid pannier/side tank locomotives. However, the steam from the condensing gear heated the water in the tanks and made conventional coned injectors unreliable. So, double-acting Weir feed pumps were

installed by the right-hand side of the smokebox.

The result was most charmingly ugly, with an outline that looked more Continental than British! I don't have any particular interest in London railways, but the opportunity to model one of these engines proved irresistible, and it would be an ideal resident on a micro-

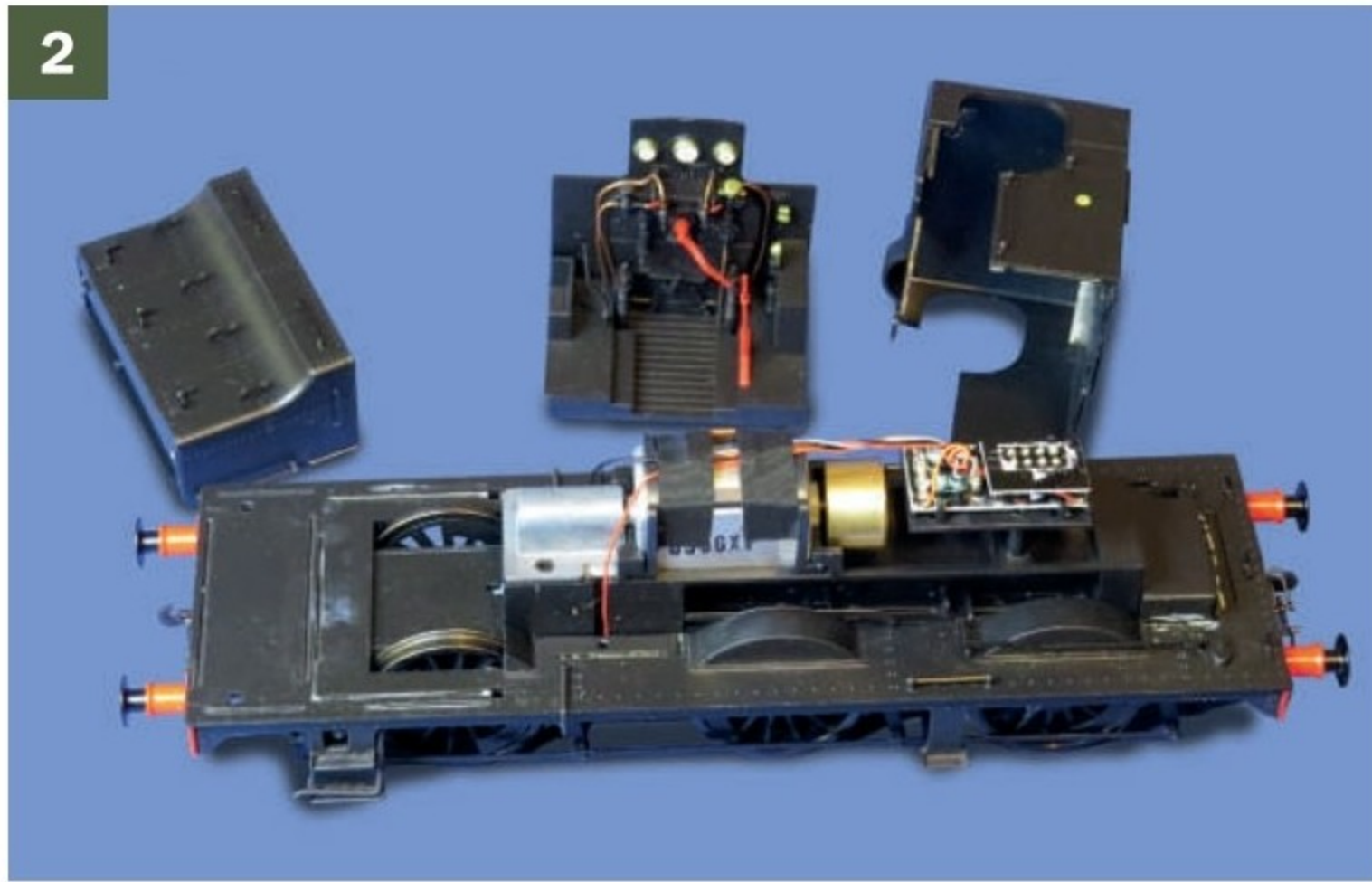
layout – so I bought one of the Minerva kits...

**Kit contents**

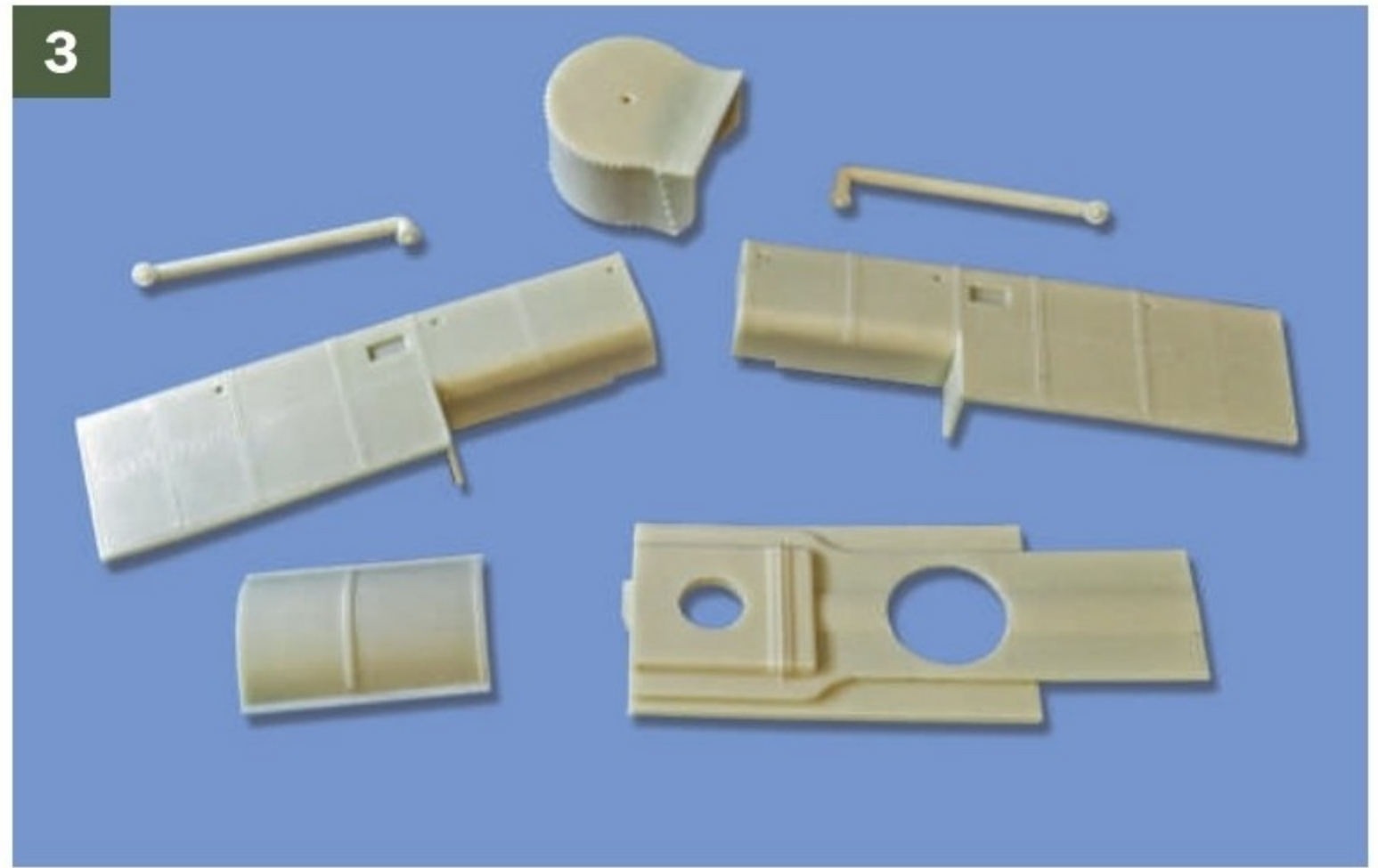
The kit comprises a ready-to-run chassis, cab assembly and footplate using parts from the '8750' model (supplied assembled and painted), together with unpainted resin castings for the tank sides,

boiler/firebox top and smokebox, plus other detail castings in resin, lost wax and whitemetal. Other small parts provided include lifting rings, steps and handrails.

The instructions are extensive, and include lots of reasonably clear photos, together with a text that is best read before starting work.



**2** It appears that, to produce the kit, Minerva dismantles a complete '8750' model, and removes the pannier tanks and some other superfluous details not needed for a '9700'. The cab can be detached from the bunker quite easily, which is convenient as it gives access to the cab interior.



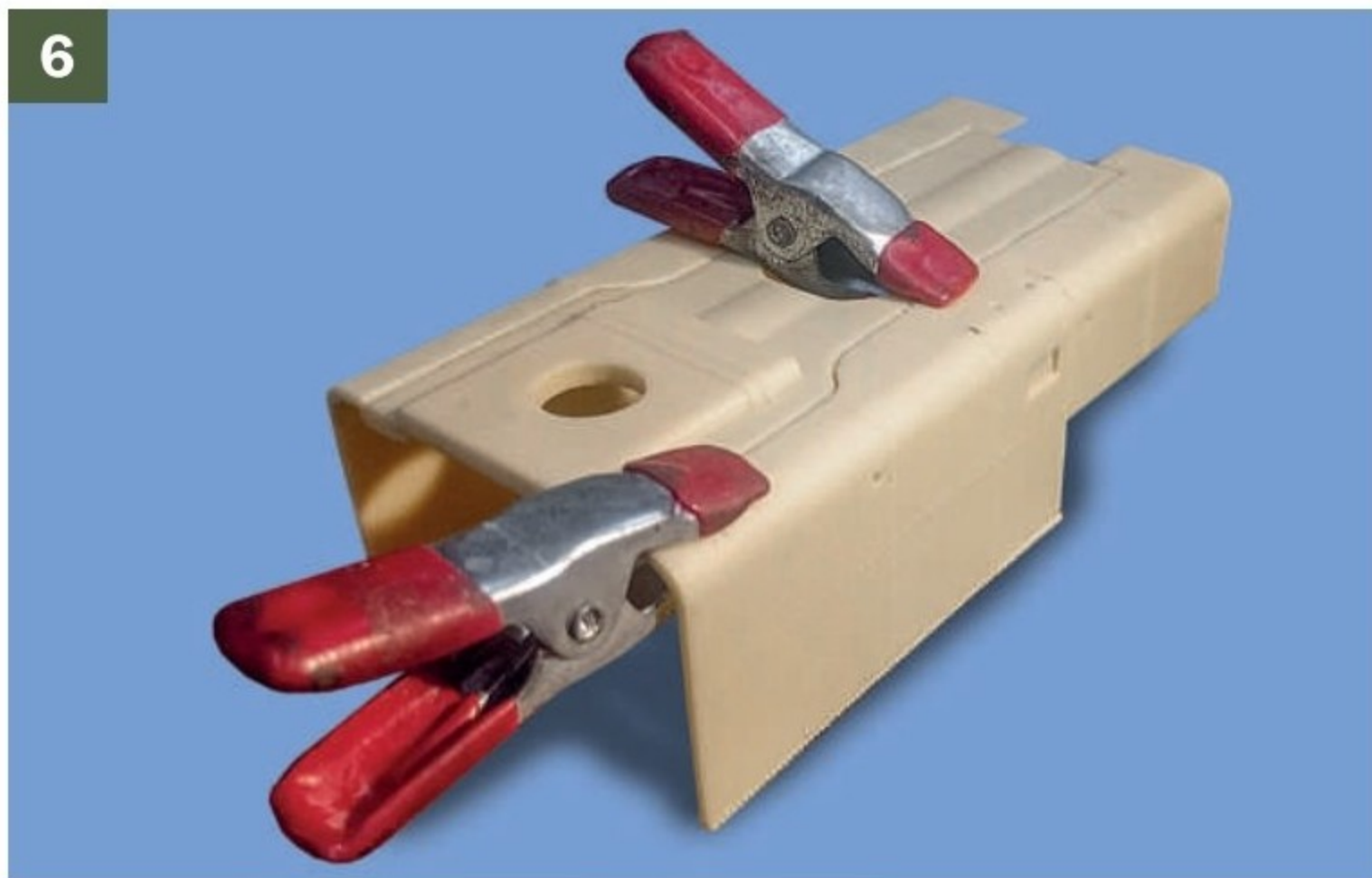
**3** These are the substitute tanks and boiler (although the boiler underside casting is not required). As can be seen, the resin parts are very simple but well detailed. The resin has a habit of bending slightly, but will respond well to gentle manipulation once dipped in hot (but not boiling) water.



**4** The kit includes lost wax castings for fittings that are unique to the '9700', including the Weir pump. The castings come on a number of sprues, and have to be detached and cleaned up. I use a fine piercing saw to cut through the sprues near the casting, so there is less filing required. Here, I have cleaned up those I need – the sprue on the left carries spares.



**5** As I noted above, Minerva removes some details from the original model but includes all those that are needed on the '9700', including the chimney, dome and safety valve cover. There are lots of other small parts in a plastic bag, together with etched footsteps, sandbox operating rods, and a cast whitemetal smokebox door.

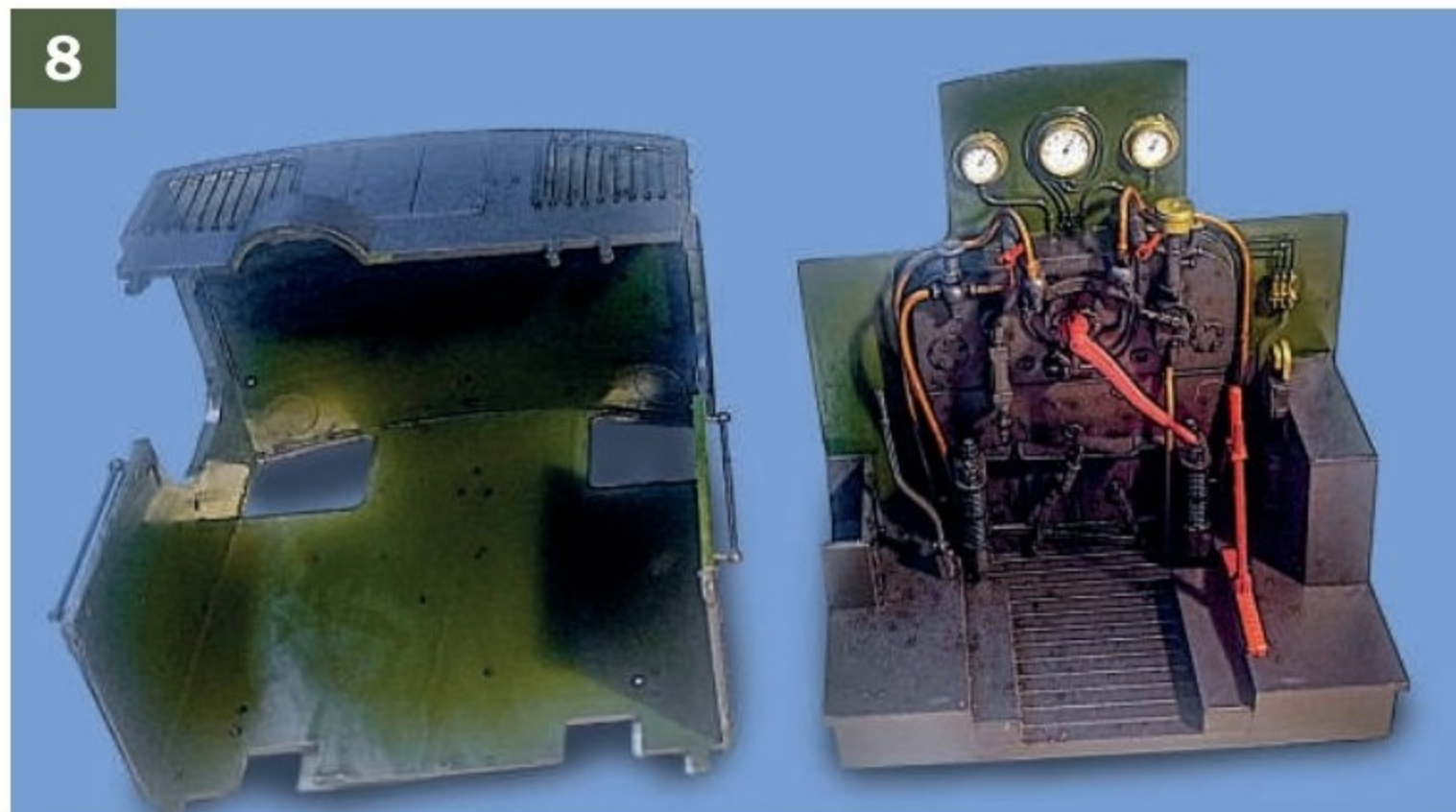


**6**

After cleaning up any minor blemishes on the edges of the resin parts, and giving the castings a good clean with Cif cream cleaner, I began assembly. There are only four major parts to the body, and assembly is straightforward. I used small crocodile clips, as recommended in the instructions, and glued the parts together with two-part epoxy resin. I was very careful to get the top and sides at right angles, and only put glue on the joining surfaces at the back parts of the side castings. Only do one join at a time and, while the two-part glue goes off, keep a close eye on the tightness of the join, and that the side and top are at right angles. Leave the basic structure for 24 hours for the glue to set, and check that it sits flat on the loco footplate.



**7** The last major element of the loco body is the smokebox, which comes as a single resin casting. I realised that the model would be easier to assemble if the footplate were detached from the chassis. This was achieved by releasing four small screws under the footplate (as illustrated in the instructions for the original Minerva '5700'/'8750' model, which are included in the kit). The smokebox casting required a little filing to be a snug fit on the footplate. There was a shallow recess in the cast metal footplate to accommodate the smokebox, and therefore I used a fine file to ensure that the smokebox fitted into the recess. I also had to shape the back of the smokebox underside to make a snug and level fit against the front splashers of the model. Finally, I dry-fitted the cab front and replacement pannier tanks to the footplate, and tested and adjusted the smokebox until it sat on the footplate vertically and slotted tidily into the recess at the front of the tank and boiler unit. Note that the instructions mention that there will be a small gap between the back of the smokebox and the boiler top – but that will be covered by a cast tank support later. The smokebox is glued only to the front of the tanks when you are happy with the fit. The whole thing can then be lifted off the footplate – I found it much easier to carry out most of the detailing work with the new tank and boiler unit detached from the rest of the model.



**8** While the boiler and tank unit was setting, I considered the cab and bunker parts of the original Minerva model. Though I thought the loco would look good in BR black, all my goods stock is pre-World War II, so I decided to finish the model in circa 1930s condition in Great Western green. The wrap-over cab on the Minerva model has no separate roof part, so I knew that getting into the cab wouldn't be straightforward when the model was complete. Therefore, I hand-painted the cab interior GWR green while it was still in bits. (I would later pick out more brass details and weather the interior prior to final assembly.) I then realised that the cab glazing had to be removed so I could spray the outside of the cab and bunker, which then enabled me to refit the glazing in the window apertures while I still had access!



**9** The last resin cast items are the condensing pipes, and there are no indications of where to locate them on the tank tops. The instructions give a pretty good idea about how to fit them accurately – though the curved edges make it difficult to find a centre. However, I concluded that the pipes would look OK if I drilled a hole 5.5mm from the inside joint with the boiler, as illustrated.



**10** Here, the condensing pipes have been fitted. The kit includes two lost wax cast packing pads to go between the condensing pipes and the smokebox. The instructions say that the condensing pipes should be parallel to the tops of the tanks when looking from the side, and parallel to the sides of the tanks when looking from above. So, I started by glueing the packing pads to the front end of each pipe (using cyanoacrylate) and testing how it all went together with the pipes dry-fitted. Next, I glued the rear end of a pipe to the tank top and positioned the pipe carefully as described above. When I was happy with the position, I applied cyanoacrylate to the front end to secure it to the smokebox.



**11** The cast whitemetal smokebox door is located by scribing a vertical line where the hinge goes, and then drilling both the door and the smokebox front. I used the drill to position the door centrally, as pictured.



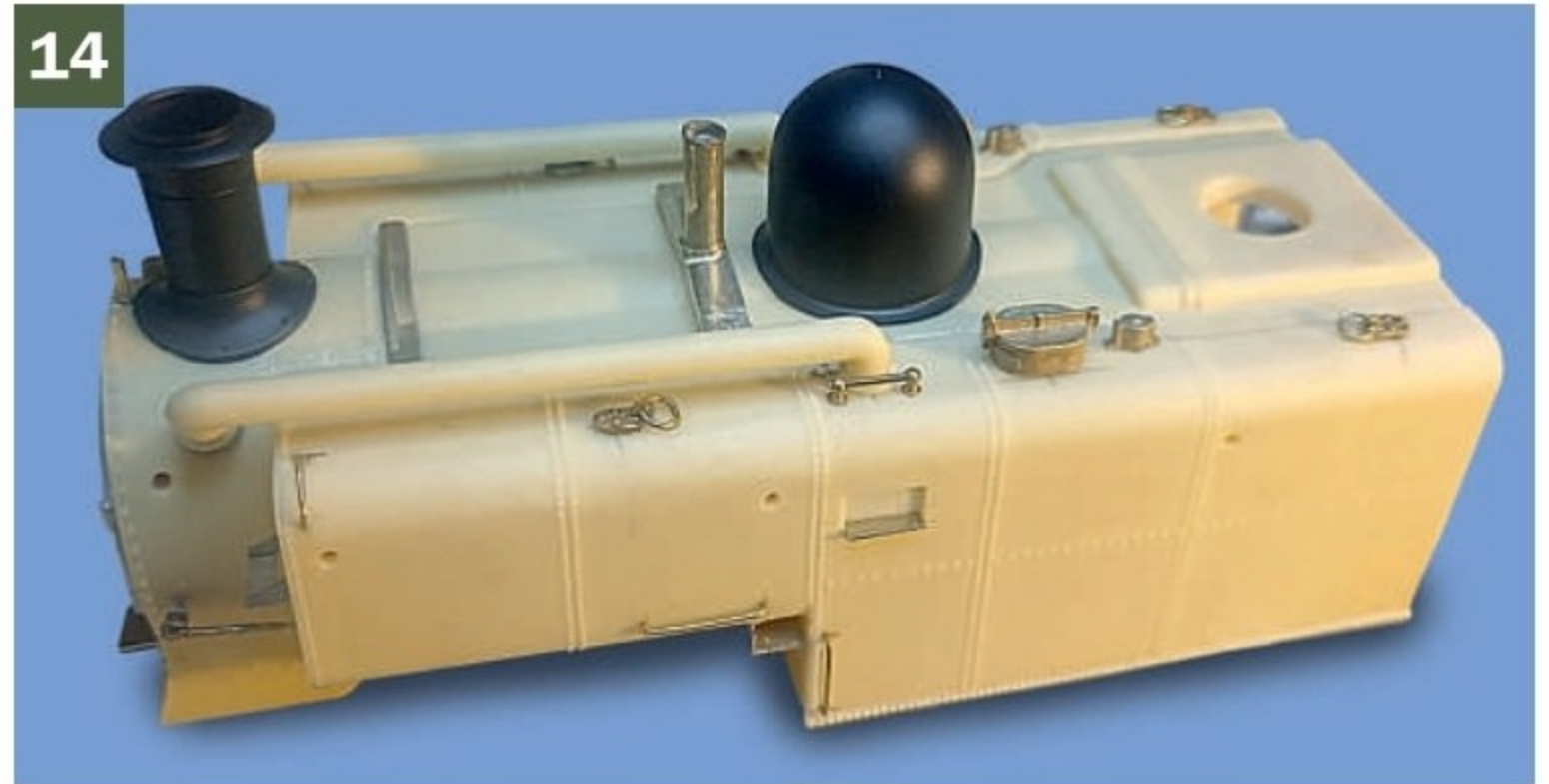
**12** A number of detail parts go on top of the boiler/tank assembly, and need to be filed and fettled to achieve a good fit. I used cyanoacrylate to attach them. The chimney-like casting in front of the dome is a vent to discharge the pressure in the tanks when the condensing gear is in use. I drilled each end of the casting, and used it to mark and drill matching holes in the tank top – this was to insert short lengths of wire because the casting needs to be fixed firmly. I did the same for the tank filler lid bases. I use a small vertical drill press that enables accurate and rapid drilling whilst saving on broken drill bits.

13



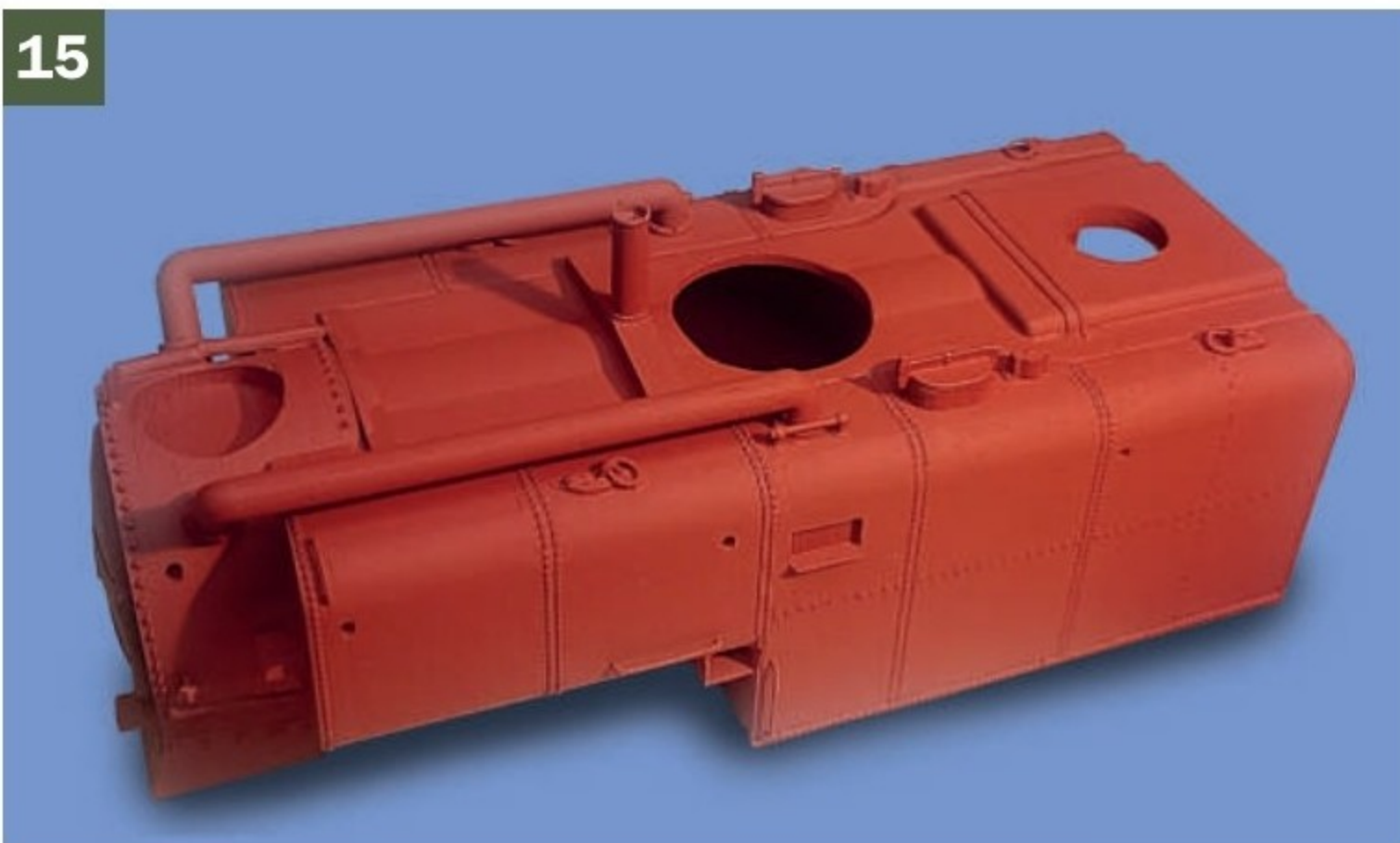
There are quite a lot of steps and handrails to fit on the tanks, and a couple of big brackets at the sides of the smokebox. Prototype photos show a rod and lever on the left side of the smokebox, which I surmise was the operating rod for the condensing gear. I cobbled up a representation using 0.7mm wire and some scrap etches from my store.

14



Here, you can see the detail on the tops of the tanks. I cheated with the lifting rings, because I had a set of lost wax brackets in my spares box. However, the etched plates, split pins and rings supplied in the kit are fine. I left the handrails on the tank sides until after the engine was assembled, because they extend to the cab sides and would be vulnerable if fitted earlier.

15



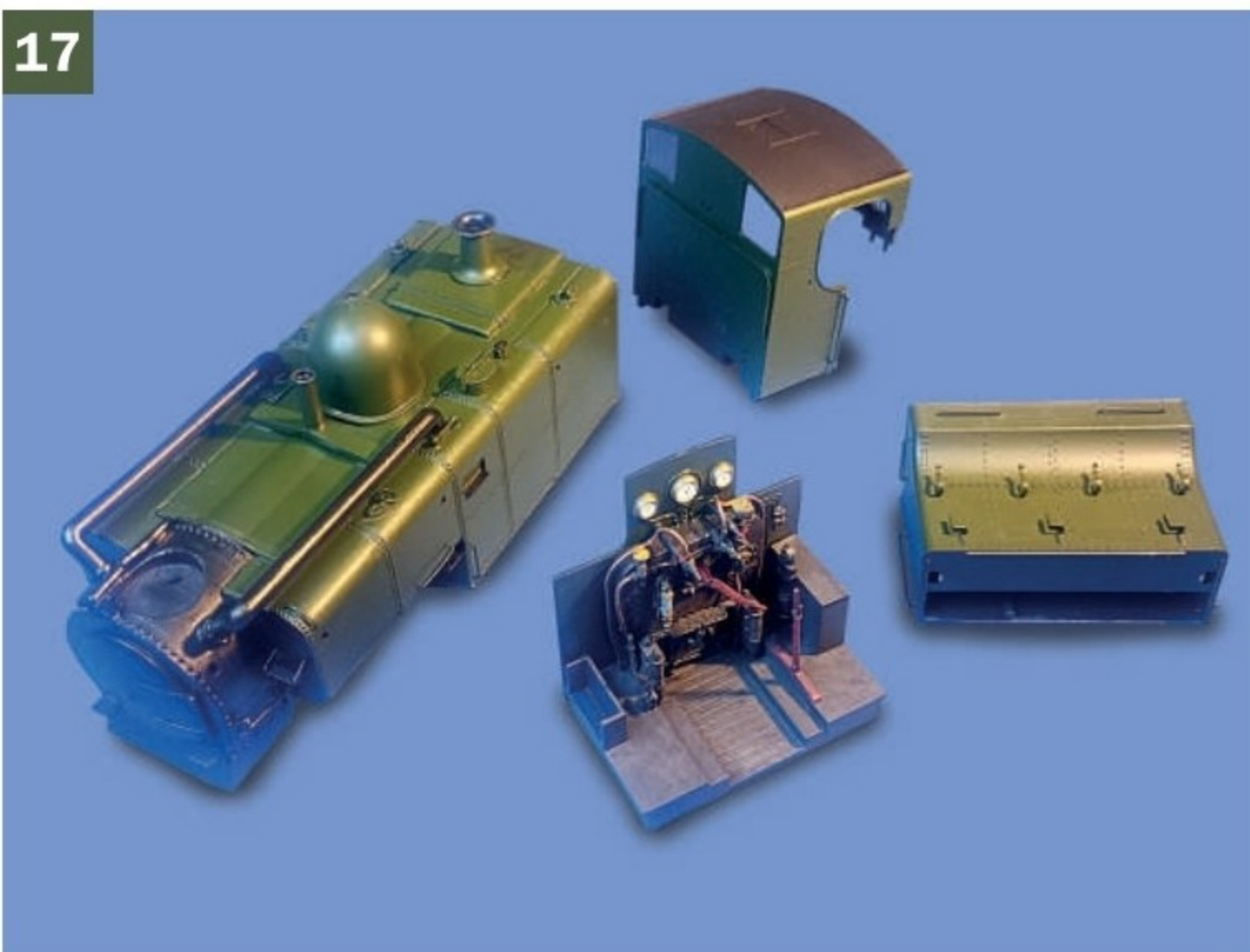
I approached this build by making up units separately, to facilitate both the construction and the painting. At this stage, I decided to paint the tank unit, and started with a coat of Halfords red primer. I tend to prefer red primer to grey, because it is easier to cover it with darker top coats.

16



The next coat was Halfords matt black. Had I decided to paint the locomotive BR black, this stage would have been the end of spraying until a possible coat of varnish. However, as I was going to paint the loco GWR green, I next had to mask parts of the tanks, cab and bunker units, as illustrated.

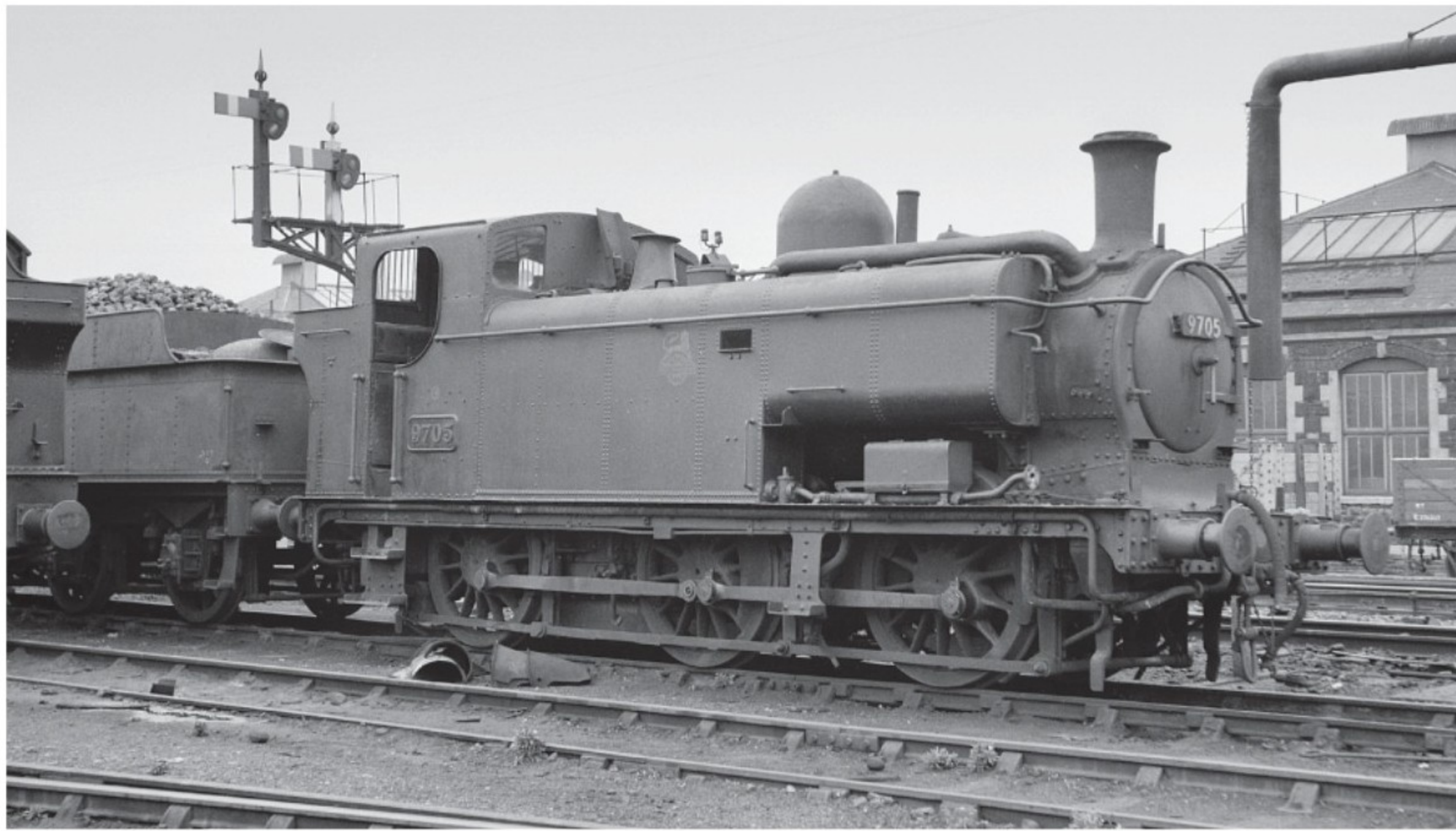
17



After spraying the model with Precision Paints GWR 1906-28 Green, I undertook some tidying-up of black detail – prototype photos show the condensing pipes were black. I also finished touching in some of the cab controls.

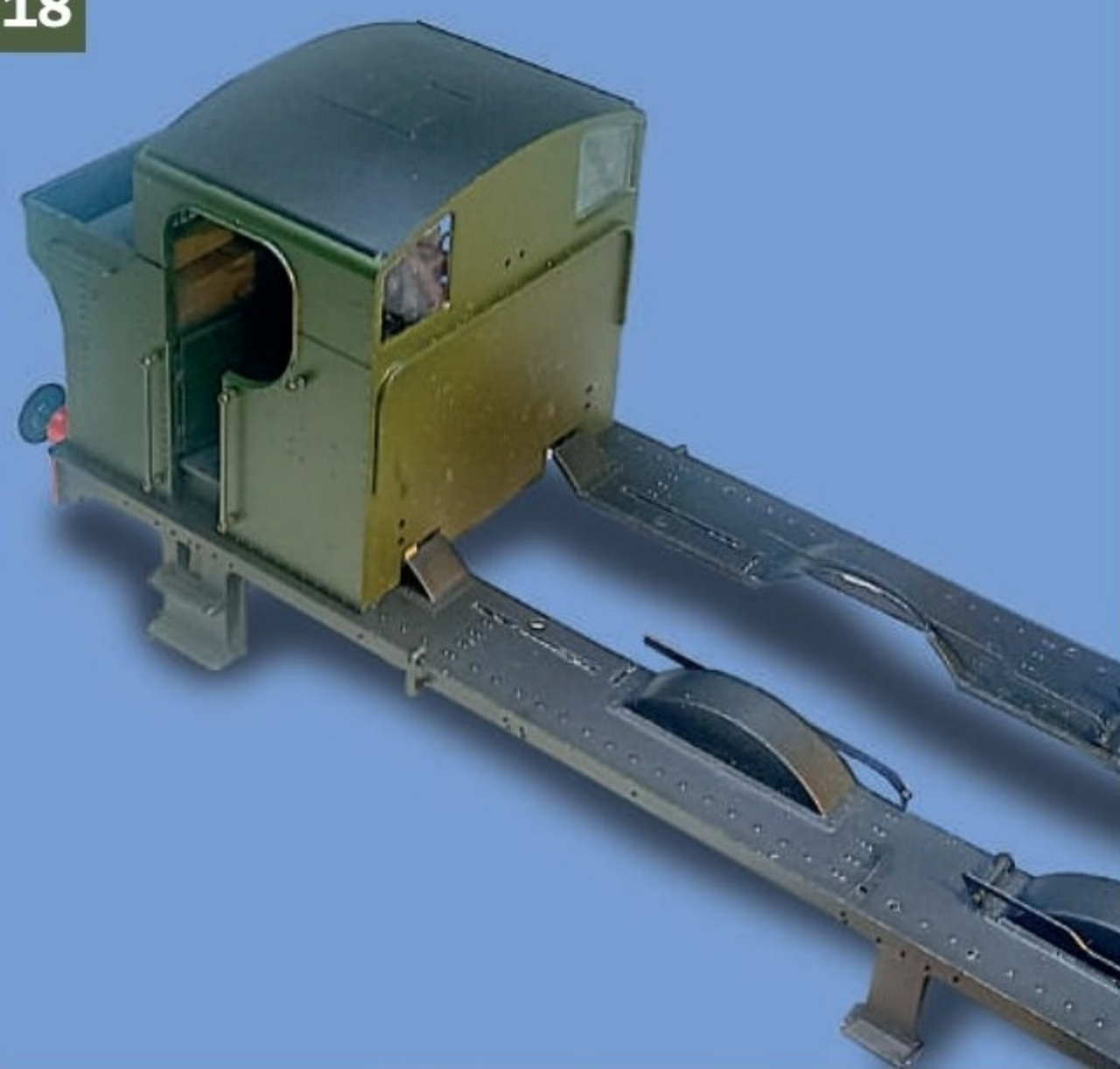


▲ The GWR green livery with full 'GREAT WESTERN' lettering represents No.9704 as built at Swindon Works in October 1933. After a working life of 30 years, it was withdrawn at Old Oak Common in 1963.



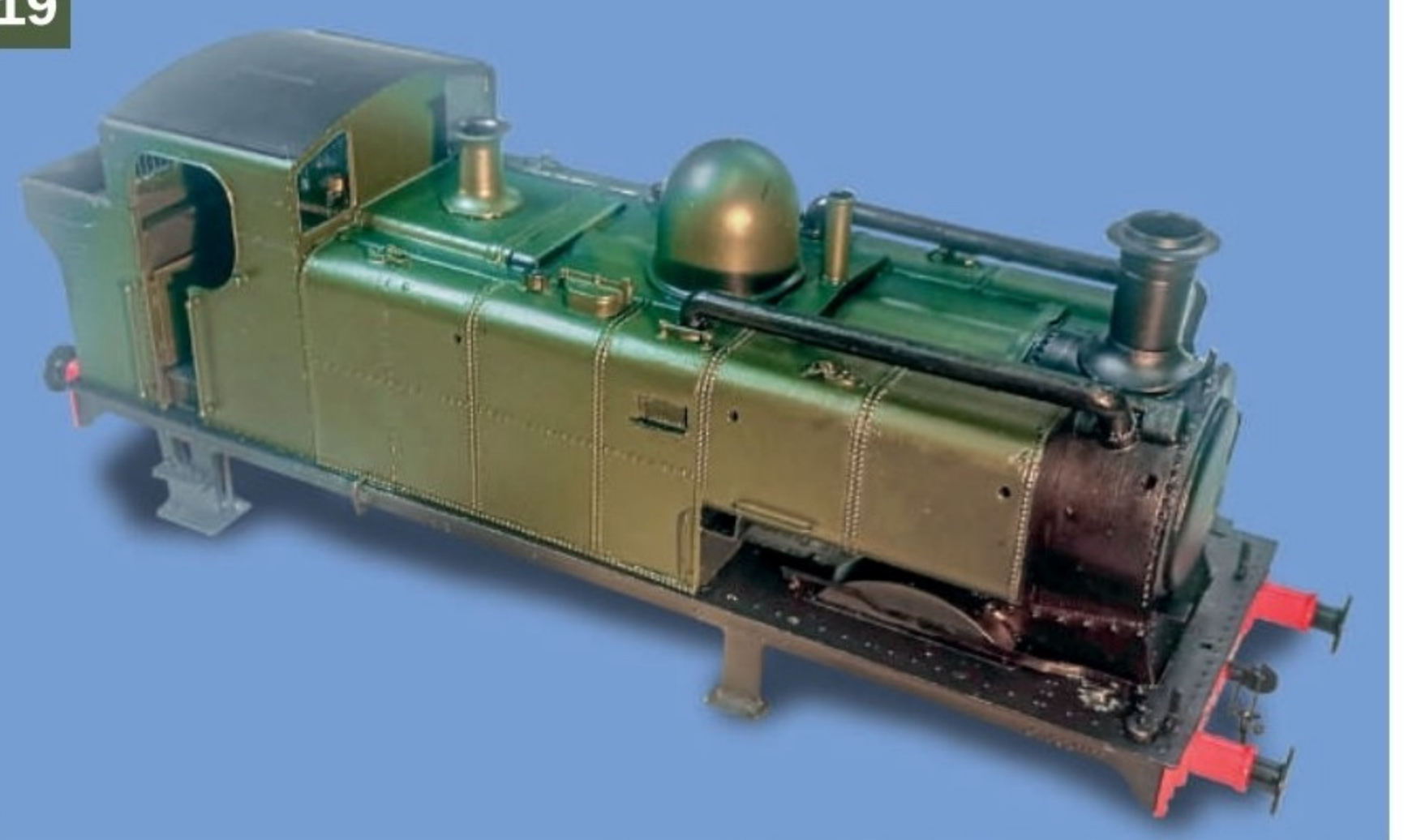
◀ No.9705 at Swindon on 5 November 1961. The locomotive is recorded as having been withdrawn the previous month, and would be scrapped in January 1962; note that the Weir pump has already been removed.  
*Photo: Hugh Ballantyne/ Rail Photoprints*

18



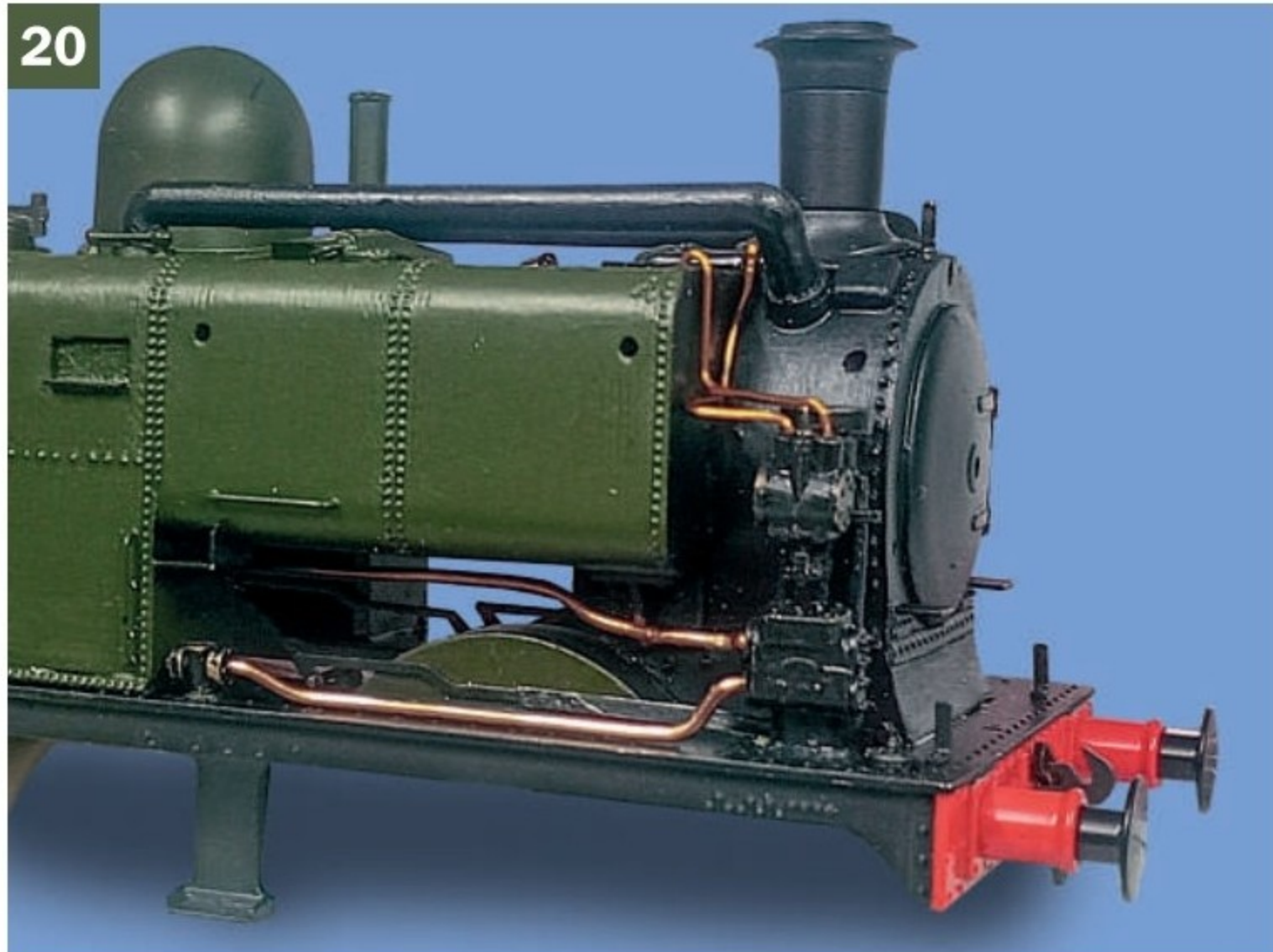
I then first assembled the cab and bunker – the locating tags had become damaged, so I used cyanoacrylate to join the three elements of the cab together permanently. I left the glue to harden for 24 hours before fixing the tanks.

19



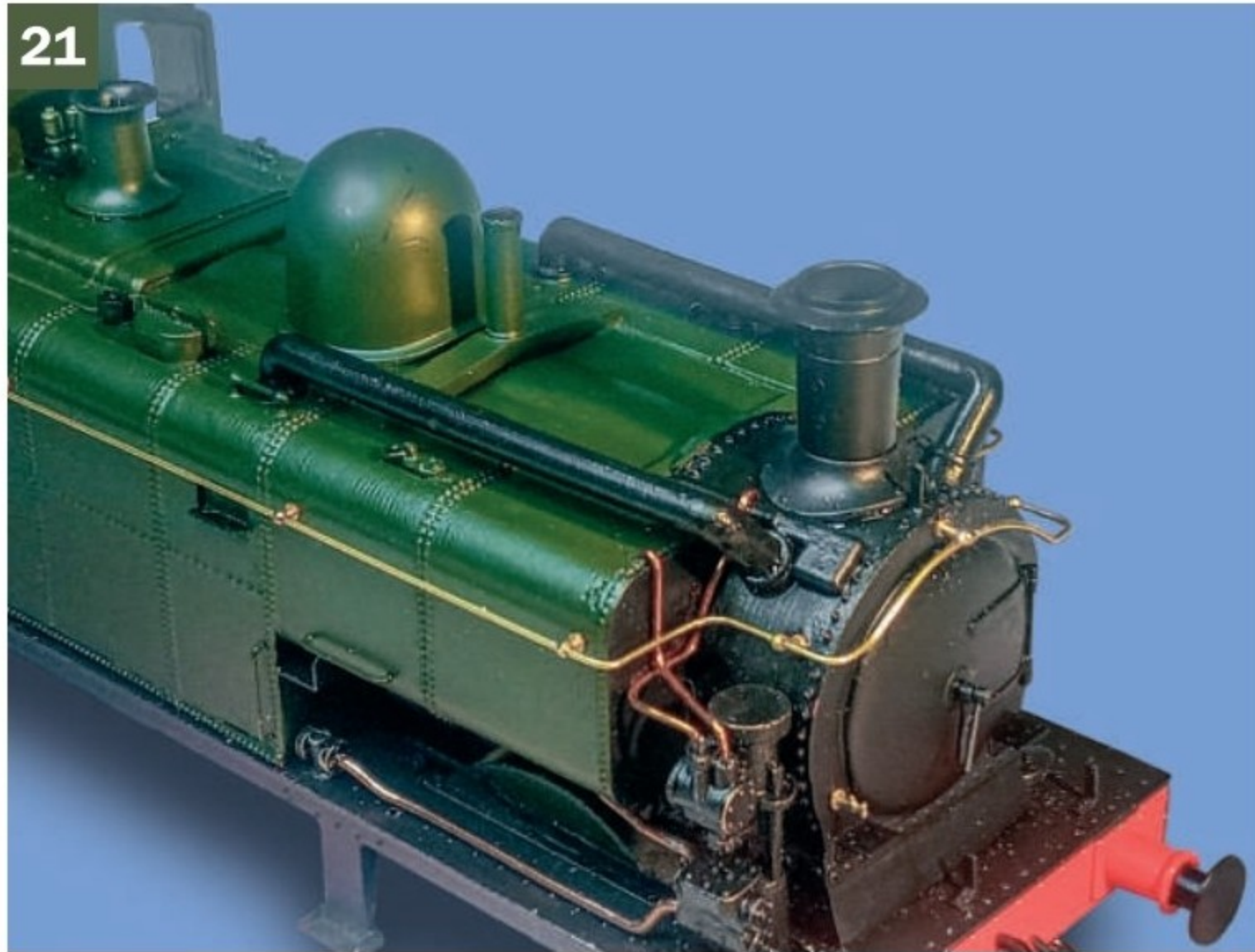
Before fitting the tank/boiler unit, I glued a brass strip along the inside bottom edge of the right-hand tank, because it was a bit warped. Then, to attach the unit to the model, I used two-part epoxy to join the cab spectacle plate to the top of the firebox, and the smokebox to the front footplate. I used a weight to hold the tanks to the footplate while the epoxy set. When all was firm, I ran a fillet of cyanoacrylate around the bottom edges of the tanks to locate them fully. I subsequently touched in any blemishes in the green and black paintwork.

20



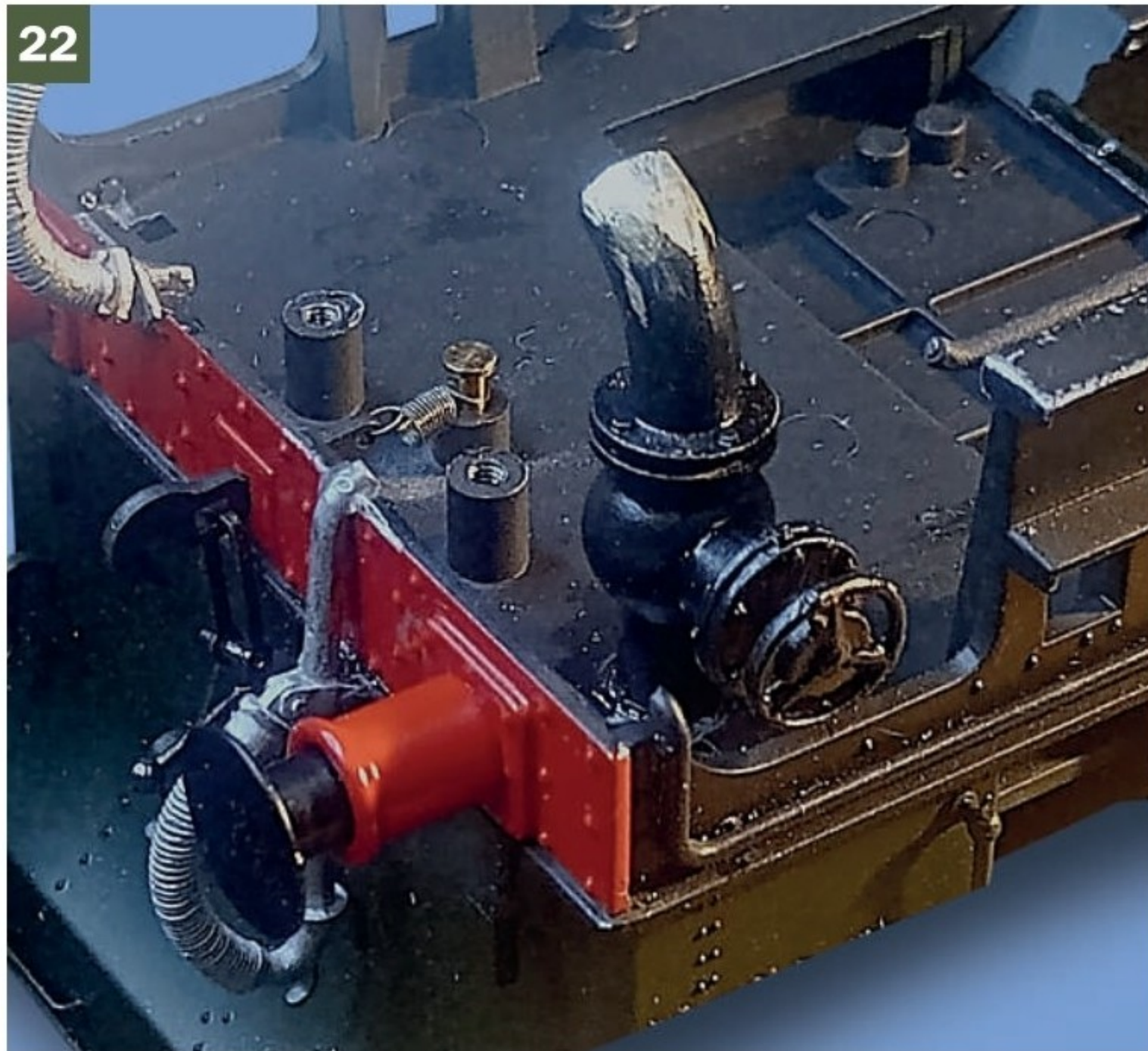
Photos of '9700' locomotives in action are not all that common. What photos exist are often dark and nearly always taken from the front three-quarter angle, making it difficult to work out the details of the Weir pumps. These were used widely in industry, and were direct-acting non-rotary devices with a steam cylinder at the top and a water pump at the bottom. The instructions only mention one pipe from the Weir pump bottom chest along the footplate to a gland casting, and one other from the steam chest to the top of the pannier tank. Both steam and water need two pipes – and the castings provide for this – but I couldn't get a clear view of where these other pipes went! The top pipes are easier to see, but it was not until I found a photo listed by the Historical Model Railway Society (ref.AAD 105 – a right-hand side view of No.9702 in GWR condition) that I was able to see the second pump pipe bending up over the splasher to vanish somewhere under the boiler. Illustrated here is my interpretation of the arrangement, created using copper wire, which I painted black after taking the photo.

21



The instructions make a suggestion for how best to fit the rather convoluted handrail arrangement around the boiler and tanks. For handrails that curve over the front of a smokebox, I always find it best to fit them in two halves, with the join located inside the centre handrail knob on the smokebox front. I start by forming the correct radius curve on each side of the smokebox, but leave the wire overlength. I then form the bends and twists of each side independently – which in the case of the '9700' involves three main bends to route the wire along the smokebox and around the front end of the tank. With the four handrail knobs pre-fitted on each tank side, I then slid the overlength straight section of wire along the tank, through the knobs. Next, I attached the knob on the side of the smokebox, and then fitted the rail through the centre knob on the smokebox front. Once I was happy with the shape of the handrail, I marked and trimmed it with snips so that it only half-fitted into the centre knob. I repeated the process for the other side. When both sides looked OK, I secured the wires into the knobs with cyanoacrylate, together with a spot of 145° solder to fix the join inside the front handrail knob. I used short handrail knobs on the tank sides, and medium length versions on the smokebox.

22



There were a few items that I wanted to deal with below the footplate. The simplest was the large discharge valve and pipe at the rear of the left-hand side, which must have been used to get rid of the hot and dirty water in the tanks after a grubby session in the tunnels under London! A lost wax casting is supplied with the kit, and was fitted in accordance with the instructions.

The other additions were replacement train heating and vacuum pipes on the bufferbeams. I thought the plastic versions supplied with the model were too thin and spindly, so I used some cast whitemetal versions from my spares box instead. They are not very robust, but none of them are in particularly vulnerable positions, so hopefully they are not likely to be knocked off.



▲ A final application of weathering has highlighted the detail on the tops of the pannier tanks.



◀ No.9710 a long way from its usual stamping ground, at Cardiff Canton shed on 25 November 1961 – following an overhaul undertaken, unusually for this variant of pannier tank, at Caerphilly Works. This may explain the non-standard positioning of the BR late crest, compared to the early emblem on No.9705 pictured opposite. Photo: Rail-Online

23



The numberplates for 9704 were obtained from Severn Mill Nameplates, which, following the passing of Chris Watford, is now under new ownership. The transfers are from the HMRS range. After a final masking of the cab windows, I stuffed tissue inside the cab, and sprayed the whole body with satin varnish from an airbrush. This gives a more even finish and makes weathering more consistent.

### Supplier information

Minerva Model Railways  
[www.minervamodelrailways.co.uk](http://www.minervamodelrailways.co.uk)

24

I gave my model a pretty good dose of weathering – but I think the workaday state of it seems to suit the functional life that these interesting engines led. If you fancy something different and want to get into kit-building, this Minerva model is a good place to start.



No.9703 about to leave Paddington with empty stock for Old Oak Common in July 1959. On the right is Modified Hall 4-6-0 No.6962 Soughton Hall.  
 Photo: C R L Coles/Rail-Online





# RAILWAY MODELLING

## Explored



Projects, hints and tips for beginners from *Railway Modeller* readers and staff

**THIS MONTH** • A Scottish park & ride station in OO • Constructing a modern tunnel

## Strathearn Parkway

**DR ROGER I CARTWRIGHT** explains how a visit to a model exhibition near his home in Scotland inspired him to build this compact OO gauge exhibition layout. See it at the Perth show on 27 & 28 June.

Photography by the author

I have had previous experience of exhibiting model railway layouts, dating from my time working in Kent as a primary school headteacher in the 1980s. However, a subsequent move to academia in Oxford, and then to retirement in Scotland, followed by my taking up of new roles as an author and as a guest speaker on cruise ships, meant I only had a permanent layout of my own at home.

However, following a visit to the Perth exhibition in 2025, and after talking to the operators of the superb layouts on show, I started thinking about how I could try to emulate them and build a small exhibition layout.

By that time, two strokes and cancer had led me to take the decision to give up the cruise ship work and to concentrate on my other role as a radio presenter. I therefore had the time and the motivation to build such a layout, but what would be the scenario? Well, why not a fictional but perhaps plausible 'Strathearn Parkway' station, set in the modern era around the



▲ The Glasgow-bound trains running from Strathearn Parkway, with its basic modern facilities serving the town of Crieff from its outskirts, are now in the hands of Class 170 Turbostars.

▲ A member of train staff heads for the on-platform portable cabin to take a break between turns, while DB-operated 66 012 enters the station freight yard.

Millennium, when passenger trains carried ScotRail 'Swoosh' livery and freight services were operated by EWS?

### Some history – real and imagined

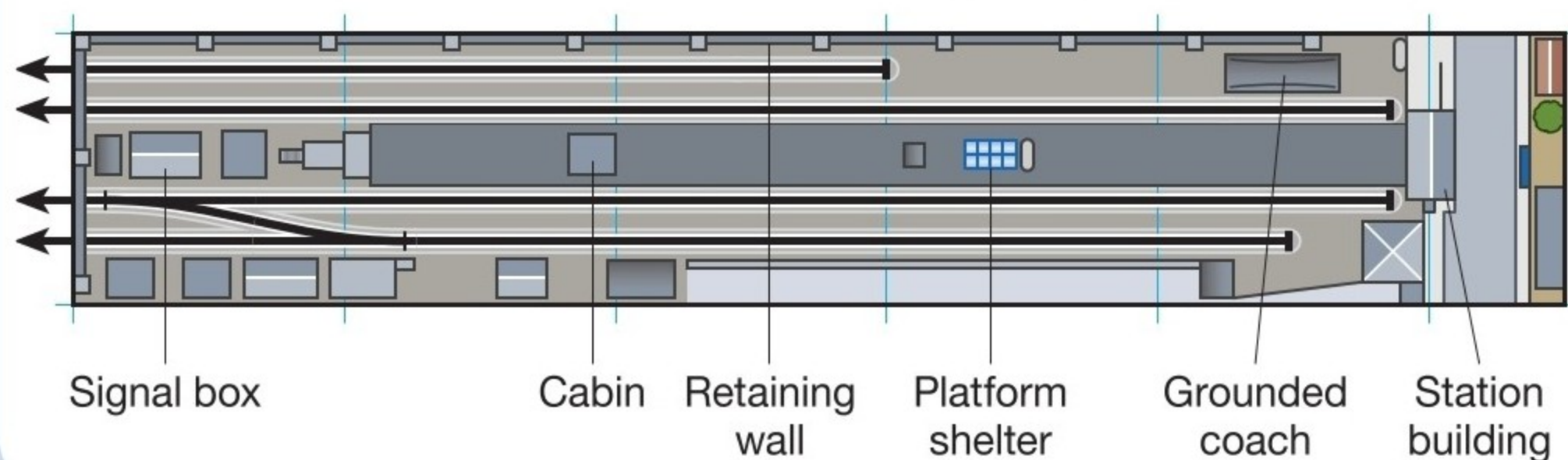
The semi-fictional back story to my layout imagines a 'what could have been' or 'what

should have been' scenario that Strathearn Parkway serves the town of Crieff in Perthshire.

The real railway history is that Crieff was reached from Gleneagles in 1856 and from Perth in 1866, with an extension built westwards to Comrie in 1893 (complete with a new station in Crieff). In 1905, the line was fully extended to Balquhidder, and so a connection was made with the Callander & Oban line at what was originally called Lochearnhead station. These were eventually Caledonian Railway and later LMS operations, and Lochearnhead was renamed Balquhidder in 1904-05.

## STRATHEARN PARKWAY

Overall dimensions: 5' 6" x 1' 0". Each grid square = 1ft x 1ft.



**A steam-hauled 'Santa Special' run by the Perthshire Railway Heritage Trust, with BR Standard Class 2MT No.78000 pulling preserved Caledonian Railway coaches, enters the station.**

The passenger service from Crieff to Perth, and also the Comrie to Balquhiddy line, closed as early as 1951, and the Comrie-Crieff-Gleneagles line also succumbed in 1965. However, in recent years, Crieff has expanded greatly, so just suppose that this expansion took place earlier, and that the lines from Crieff to Perth and also to Gleneagles were merely cut back to the south and east of the town. This would allow land to be released for housing, and a new, simplified station would open, called Strathearn Parkway, with train services to Edinburgh via Perth. This imagines that the current Perth to Edinburgh service is extended to Crieff, with stops at Madderty and Methven Parkway before Perth,



and that there is a two-hourly service to Glasgow via Muthill, Gleneagles and Stirling. In this scenario, as Strathearn is a Parkway station, there is plenty of car parking, although this is in fact left to the imagination due to lack of space! Of course, if the line from Crieff to Locheearnhead had remained

open, it would be a superb tourist attraction today, as Crieff has become popular with visitors for such attractions as the Caithness Glass Visitor Centre and Glenturret Distillery, while the Locheearnhead area is noted for its mountain and lakeside scenery.

### Baseboards, track and points

I realised early on that construction of the layout would have to be simple, due to my being to a degree disabled, especially as regards mobility and hand issues. This has also meant that the fiddly extras on rolling stock have had to be left off, but that has not detracted from the overall effect, or at least in my opinion that is the case.

In order to fit in my car, the scenic section consists of a 5' main board with a 6" additional section comprising the station building (a Metcalfe Models card kit), the road and bus stop, two disabled parking spaces, and the start of a housing development building site to reflect the growth of the town.

The rest of the 10' length is a fiddle yard, reached through a low-relief Metcalfe bridge connected to a retaining wall (also from that company) leading down to the station. Baseboard construction is simply 2" by 1" with a ply top.

### Track and electrics

Track is Peco code 100, much of which I already had to hand, so the only track I had to buy was of the concrete sleeper variety for use on the visible sections next to the two platforms. After laying, the track was ballasted in the traditional manner using diluted PVA glue.

Electrics are as simple as possible, using Gaugemaster and Peco products. Only the two points on the scenic board have motors – all the fiddle yard points are hand-operated.

Switches are mounted just behind the plain backscene so that the levers are visible for operating from the front when the layout is set up at home, and from behind at exhibitions. The tracks cross the boards using sliding fishplates for electrical continuity.

### Adding in the details

About half of the buildings and vehicles were already in my bits box when I started building the layout, although a Stagecoach bus had to be purchased to represent the No.15 service from Perth to Crieff, Comrie and beyond, together with the builder's excavator and some fencing.

Colour light platform-end signals and buffer stop signals were also purchased to add a little extra to entertain visitors at exhibitions.

### Operating the trains

In the station, Platform 1 is for the Crieff-Perth-Edinburgh service, calling at Madderty and Methven Parkway. This is normally



**The No.15 bus, here calling at the bus stop with shelter in Strathearn Parkway station forecourt, is now the only public transport connection to Comrie since closure of the Crieff to Comrie line in 1965.**

operated by a two-car Class 158 Diesel Multiple Unit, whilst trains using Platform 2 serve Glasgow via Gleneagles with either a Strathclyde-liveried Class 101 (just about still in service by the date of the layout) or a three-car Class 170 Turbostar DMU doing the honours.

There is also a stretch of rarely used track by the retaining wall that holds some disused freight stock.

There is a small freight terminal off stage, handling a weekly fuel oil train (in reality there is a fuel company in Crieff, so it would be a possibility), some timber and the products of the oldest distillery in Scotland, Glenturret, and the Stuar Crystal factory (now sadly closed and demolished). These services are dealt with by either an EWS-liveried Class 37 or 66.

To add a different element, I have postulated that the old Methven Junction to Methven branch is still in existence and is now home to the Perthshire Heritage Railway Trust (PHRT). The Scottish Model Engineering Trust, home of the Perth Model Railway Club, is situated just outside Methven, so this is most appropriate.

My fictional scenario is that as there is no ScotRail service to Perth on a Sunday, the main line from the renamed Methven Parkway to Crieff is used by the PHRT to run a heritage service, with locomotive classes that might have been seen on such a line, like a BR Standard 2-6-0, a 4F, a 'Black Five', a Caledonian 812 Class 0-6-0 and a Park Royal railbus. Due to the lack of run-round facilities, the trains are top-and-tailed with a larger loco and one of the industrial steam or diesel locos owned or lent to the trust. Among the rolling stock owned by the PHRT are two vintage Caledonian Railway coaches.

### Enjoyable and therapeutic

At home the layout is on a 10' long bench, and it is then raised on 30cm folding legs, sourced online, to elevate it to a decent height for myself and for exhibition visitors. Building *Strathearn Parkway* has been both



▲ A Class 158 two-car Express Sprinter DMU, in ScotRail 'Swoosh' livery, departs Strathearn Parkway Platform 1 with a service to Perth and Edinburgh via Madderty and Methven Parkway, while in Platform 2 a three-car Class 170 Turbostar DMU waits with a Glasgow via Gleneagles service.



◀ The freight terminal alongside Strathearn Parkway station still handles some timber traffic, hauled here by an EWS maroon & gold-liveried Class 37, as well as serving the local distillery and glass factory in Crieff and handling a weekly fuel oil train.

enjoyable and therapeutic, and I hope it demonstrates, at the very least, that mild disability is no bar to modelling. The layout's back story scenario may be a fantasy, but as I have written a number of non-fiction and text books, this has given me a creative outlook – I have researched the history of Strathearn's railways, and this is my 'novel' based on that, not written but modelled.

*Strathearn Parkway* is available for exhibitions, and the only requirement is some help in setting up and the hope that visitors will enjoy and be inspired by it.

### Acknowledgements

Special thanks go to Rails of Sheffield (my usual models and accessories supplier) for the fast despatching of parcels, and to my long-suffering postie for delivering them – he was thanked by being given a first viewing of the finished layout!

### Historical background sources

*The Railways of Upper Strathearn*  
by Bernard Byrom  
Published by The Oakwood Press  
ISBN: 978 0853616221

*Branch Lines of Strathearn*  
by John Young  
Published by Lightmoor Press  
ISBN: 978 1899889884

*Angus Railway Group Steam Album – Volume 3: Perthshire*  
Published by Angus Railway Group

*An Illustrated History of Tayside's Railways: Dundee and Perth*  
by W A C Smith & Paul Anderson  
Published by Irwell Press  
ISBN: 978 1871608731



▲ It must be a Sunday at Strathearn Parkway as, in this view, the Perthshire Heritage Railway Trust preserved Park Royal railbus is occupying Platform 1, while on Platform 2, a single-car Class 153 Super Sprinter is sufficient to handle today's Glasgow service.

## Tunnel vision

With the arrival of the new Manning Wardle L Class 0-6-0ST from Rapido Trains UK (see review on p564), **CALLUM WILLCOX** set about creating a Middleton Railway-inspired scene in 4mm scale – this forming the backdrop for this month's cover photo.

A tunnel is a staple feature of many a model railway, be it as a point of scenic interest, or for more practical reasons – such as disguising a transition to an off-scene area or fiddle yard.

To coincide with our review of the new Manning Wardle L Class 0-6-0ST in OO from Rapido Trains UK (for which our example is presented as preserved Works No.1601 *Matthew Murray*), I elected to create a tunnel diorama in 4mm, taking inspiration from the

preservation-era Moor Road Tunnel in Middleton, South Leeds – which carries the M621 (formerly M1 South-East Urban) motorway over the Middleton Railway. Constructed in 1971, the tunnel uses relatively modern construction techniques, being lined inside with corrugated metal, whilst the portals are of yellow brick with concrete wing walls.

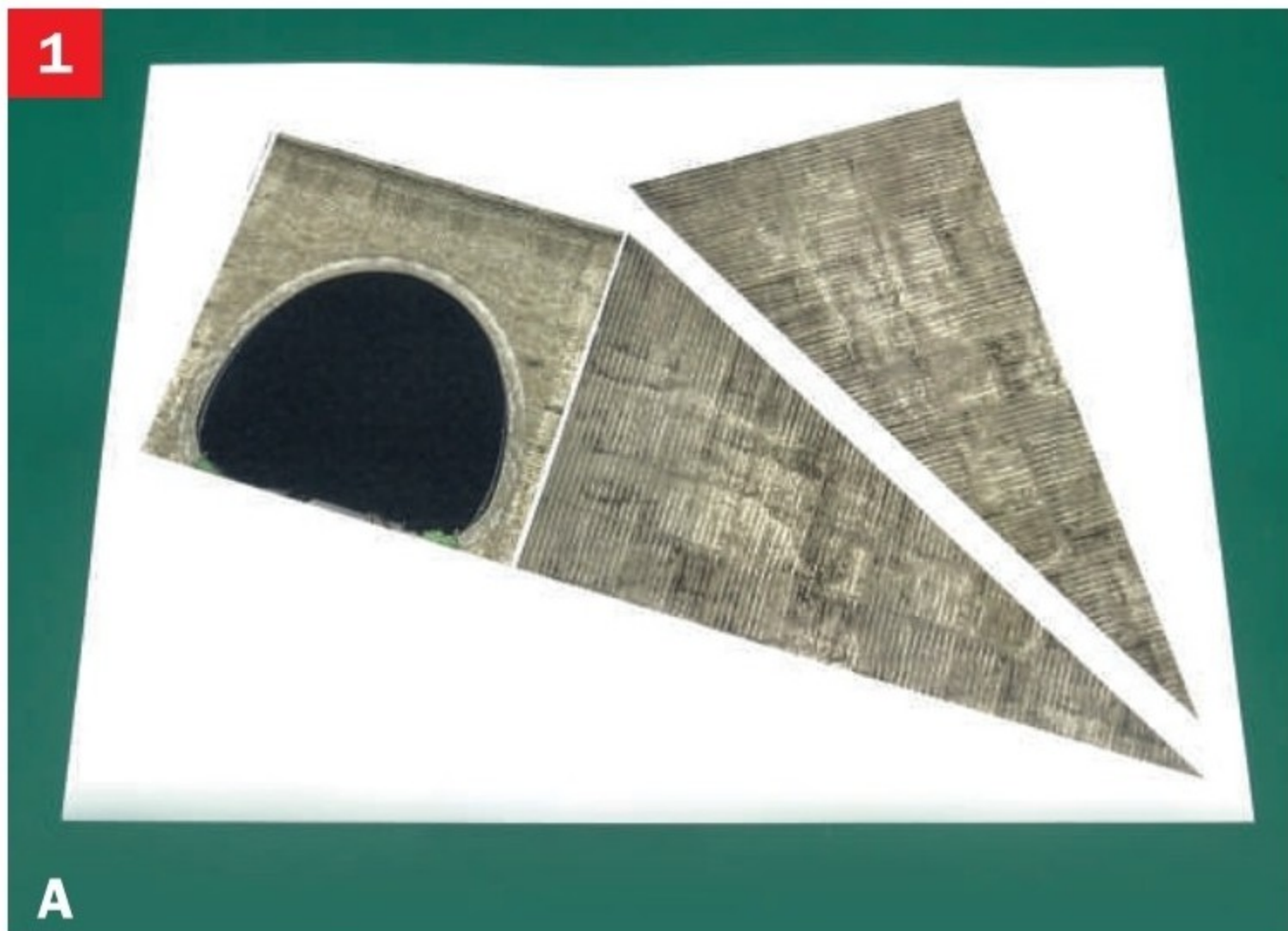
In this step-by-step article, I cover the basic aspects of constructing the tunnel using

▲ North Eastern Railway H Class 0-4-0T No.1310 emerges from the M621 tunnel on 20 May 2021. Photo: Declan Hargreaves

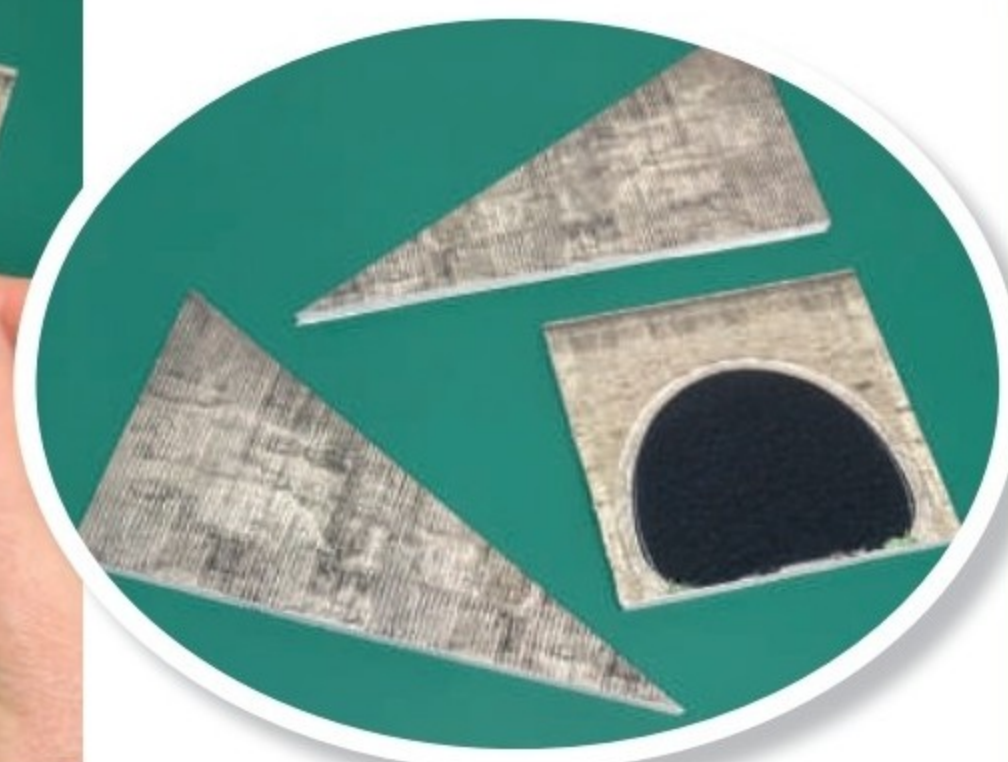
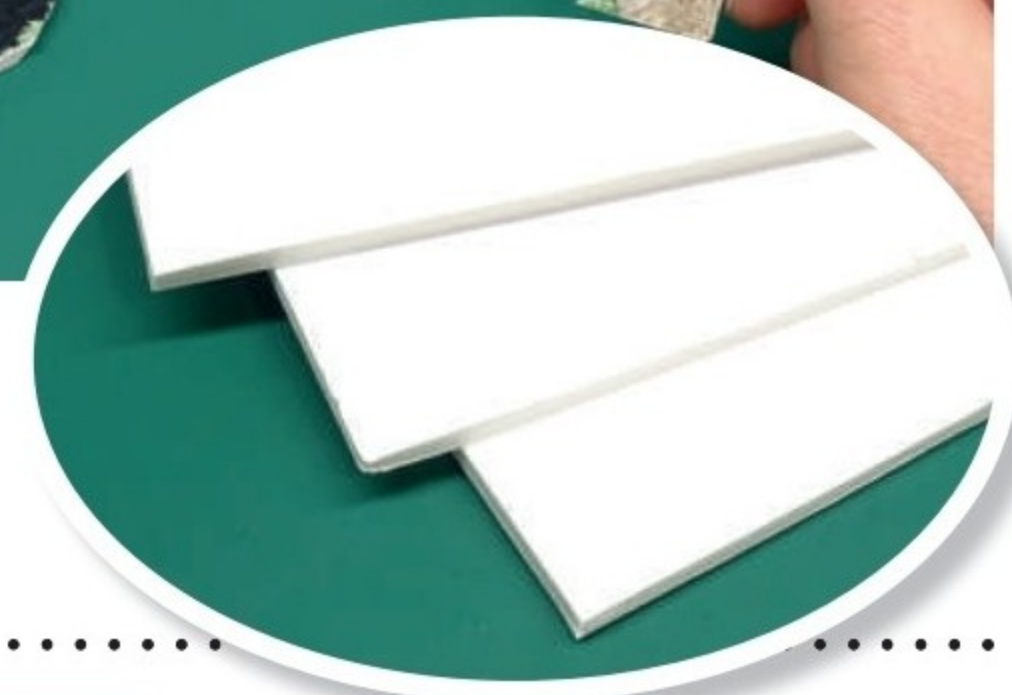
custom prints of the prototype (mounted onto foamboard), including how to incorporate it into a scene that is evocative of the real location.

▼ The Rapido Trains UK model of No.1310 (reviewed in RM January 2025), hauling wagon types also found at the Middleton Railway.

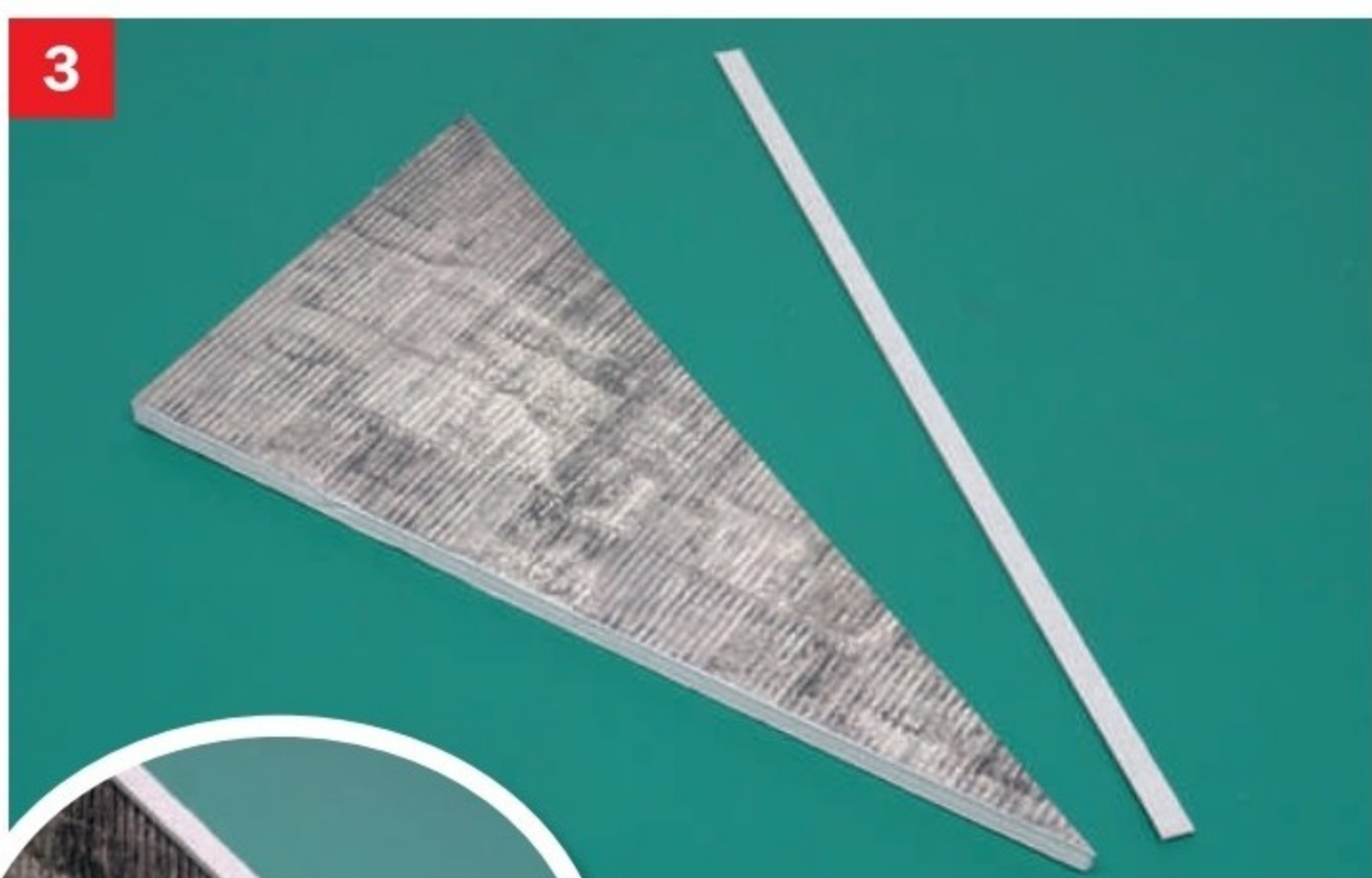




Printed versions of the tunnel portal and wing walls (at the south end) were created using photo-editing software, by adapting and scaling images of the prototype. Once I was happy with the results, the parts were printed onto thin matt card (A), before being mounted onto 5mm foamboard using double-sided tape (B).



Using a steel rule as a guide, each component was cut from the sheet using a sharp craft knife – this process being relatively straightforward due to the easy-to-cut nature of the foamboard. Slightly more challenging, however, was creating the tunnel opening, this being done freehand by following the curve of the mouth very carefully with the blade (B).



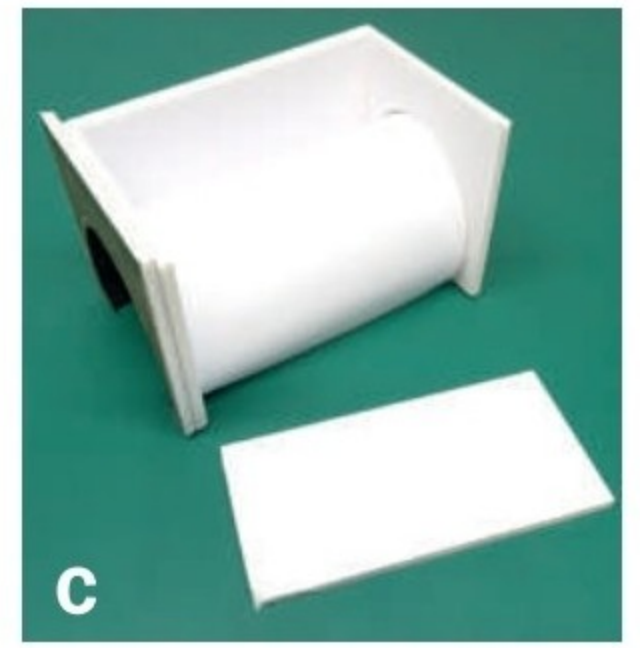
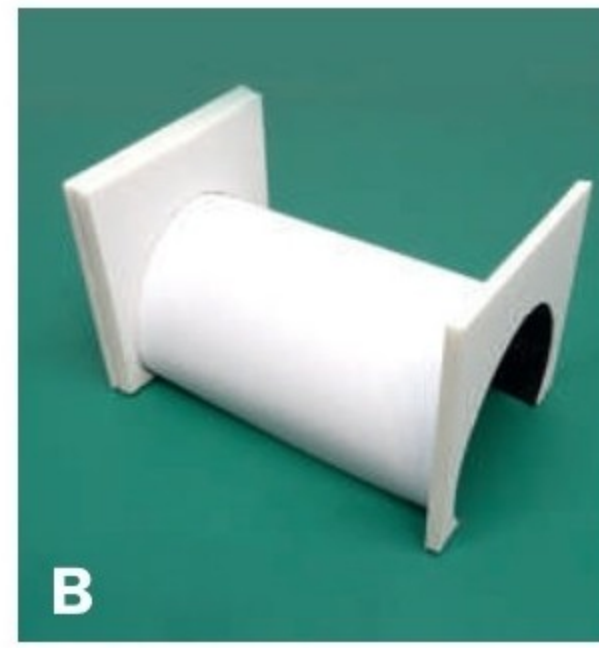
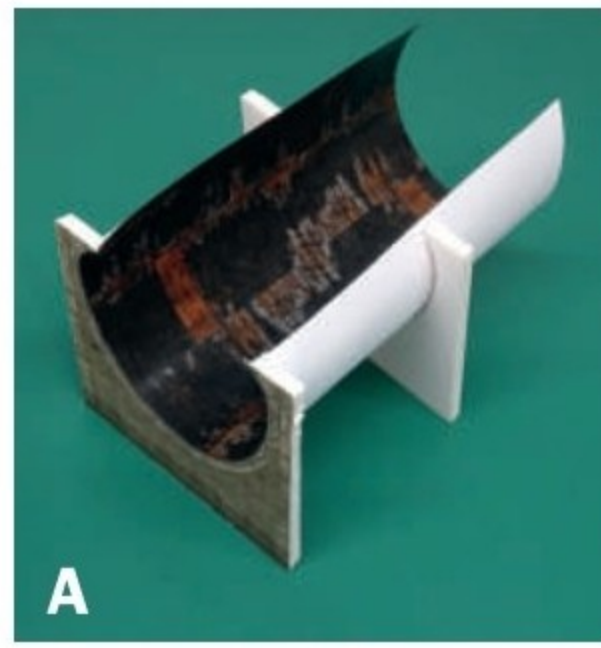
Strips of 1mm card – measuring 6mm in width – were used to replicate the concrete coping. These were trimmed to length against the tunnel components, before being put aside, ready for mounting at a later stage.

4 In a similar fashion to how the exterior parts were created, a representation of the corrugated metal tunnel lining was adapted from a texture sheet supplied with a Scale Model Scenery kit, this being scanned, scaled and printed onto thin card (inset). After being trimmed roughly to size, this was then shaped around a rolling pin, before being test-fitted to the inside of the tunnel mouth, and trimmed to size for a flush fit.



## Assembly of the tunnel

**5** Assembly began with the fitting of the tunnel lining. This was fixed directly to the inside of the tunnel mouth using contact adhesive, starting from the base of the mouth at one side and working gradually around to the other – ensuring the lining was flush along the entirety of the edge; I found the best method was to create an additional tunnel mouth former, this being used as a clamp to stop the lining unravelling whilst glueing it in place (A). With a dummy portal at the other end of the tunnel fixed in position (B), a box-like superstructure (using additional sections of foamboard) was created around the bore (C); this protected the tunnel lining, whilst also providing the model with additional strength.



## Setting the scene



With the positioning of the tunnel established on the diorama, surrounding terrain was formed using Celotex sheets (a material widely used in the building industry that is lightweight and easy to shape).



For ease of access and to avoid any unnecessary water damage to the printed card detail, I elected to remove the tunnel prior to ballasting – with the position and width of the bore having been marked prior to removal (*inset*). Fine grade Peco Weathered Grey ballast was used for the ballasting. Once dispensed as required, the granules were first moistened with IPA (isopropyl alcohol), before being fixed in place with the usual solution of PVA mixed 50/50 with water.



With the ballast cured and the tunnel now fixed in position, DAS modelling clay was next applied to the landforms to create a hard and smooth outer shell (A) – note how the tunnel portal and wing walls had been covered and masked prior to this. Once the clay had hardened, AK Interactive ref.AK8035 Splat Effects Dirt textured acrylic was applied for ground cover, ready for the addition of static grass fibres and other scenic materials (B). With the paint dry and the masks removed, the coping sections were glued into position (C).



The scenic treatment was completed with the addition of static grass fibres and other materials to simulate the grass cover and foliage. Using photographs of the real location for reference, I determined which fibres would be best suited for a spring/summer scene, with various colour blends and lengths applied in layers to create natural variations (A). Shrubs and other vegetation are also prominent features of the scene, with various off-the-shelf scenic products utilised, produced by the likes of the Dutch manufacturer, Lars op 't Hof Scenery (B), whilst trees and bushes were created from seafoam using the method described in the July 2023 issue (*inset*). All model photography by the author

**Paul Jones**



## Clubs and exhibitions

“**W**hy do model railway clubs organise model railway exhibitions? I have always thought that there are three principal reasons. Firstly, to raise much needed funds for the organising club so that it can continue for the next 12 months. Secondly, to show the work of that club’s members with layouts and demonstrations. Thirdly, to promote the creative hobby of model railways as a worthwhile pastime, with the hope of creating an interest in our hobby and therefore attracting new members into the club.

These were certainly the objectives of Warley Model Railway Club when it held its first exhibition in October 1967. This was during the heyday of the formation of model railway clubs across the country, resulting in a significant increase in model railway exhibitions, many of which, fortunately, continue to this day.

My experience is that exhibitions tend to grow like Topsy. This is true of the Warley club exhibition and others I know of. There are obvious reasons for this – attendance exceeding hall capacity and requiring a larger venue, demand from club and other layout exhibitors to display at the exhibition, and trade exhibitor demand for space, to name just a few. In the case of Warley MRC, we started off in a local school, moved to a recreation centre and then eventually, as is well known, organised the NEC show from 1993 until 2023. These moves were brought about for the reasons stated above.

But what is the effect on a club of an ever-larger show? Clearly, for any exhibition, an enthusiastic organiser is required, together with a supporting team. Yes, I was that enthusiastic exhibition manager for 40 Warley shows, supported by a team of 8-10 fellow members, all of whom gave a lot of their time to the organisation of the event. The problem is that this organisational time means that there is less time to be involved in the other aspects of club life, including working on club model railway projects.

As a result, there is a danger of a club with an exhibition becoming an ‘exhibition with a club’, and in some respects, this is what happened at Warley MRC. In the three months before each year’s show, club activities were dominated by the organisational requirements for the event, often involving many club members. For many this was a very enjoyable time (me included!), but I am sure some members got involved out of a sense of duty to the club.

During the remainder of the year the exhibition team were kept busy, taking them away from the core activities of a model railway club. Over the weekend of the show, it was expected that members would give their time to set up, run, and take down the exhibition, with early starts and late finishes. The show could not have happened without that not inconsiderable commitment from members.

Some clubs have decided not to hold an exhibition, despite successful shows over several years. This is perhaps because of the challenge of finding a suitable venue (an increasingly difficult task) or volunteers not

coming forward to manage and run the event. I have been told that ceasing to hold an exhibition has been game-changing for some clubs, enabling them to focus on layout building projects and related activities. I assume that these clubs have been able to find other ways of financing their activities, but these are interesting observations. Clearly, an exhibition is not essential.

It is well known that Warley MRC decided not to organise the NEC show after the 2023 event, for reasons well publicised. We were intending to have a rest in 2024 and gather our thoughts, when we were invited to organise a show at the Statfold Barn Railway. This smaller show has significantly ‘de-scaled’ the club’s exhibition organising activities, enabling project groups to get on with the task of building layouts, with several new projects also under way. It has restored a balance in the club. In association with the Statfold Barn Railway team, only a

small group of enthusiastic members is involved in making the arrangements for the show, to the benefit of the club generally. Warley MRC has returned to being a ‘club with an exhibition’, which is much less stressful for all concerned!

Exhibitions remain an important activity for the hobby for many reasons, and

hopefully they will continue for many years to come. Some will be organised by clubs, some by charity organisers, and some on a commercial basis. All will continue to encourage and inspire railway modellers, and promote our hobby as a wonderful, creative and worthwhile pastime.

*...there is a danger of a club with an exhibition becoming an ‘exhibition with a club’, and in some respects, this is what happened at Warley MRC...*

*Paul Jones is a Charity Trustee of Warley MRC.*

The final Warley MRC show at the NEC, which took place on 25 & 26 November 2023. Photo: Craig Tiley



# Readers' Letters

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: [editorial@railwaymodeller.co.uk](mailto:editorial@railwaymodeller.co.uk)

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

## Access to all areas at Peco for the Branch Line Society

It is many years since I visited Pecorama, back when my daughters were children, and it was well remembered as a day out we all enjoyed. Now that my grandchildren are grown up and driving cars, it was definitely time for another visit! The Branch Line Society offered a welcome opportunity on 8 March 2026.

Arriving on a beautiful spring morning, I was given a warm and friendly welcome by Tina and Samantha, who directed me to *Orion* where they were serving tea and coffee. This was much appreciated by those of us who had made an early start and travelled some distance.

Once refreshed, and with latecomers accounted for, we were given a full briefing and asked to don orange 'visitor' jackets ready for the promised factory tour later, before first being directed to the 7¼" gauge Beer Heights Light Railway. Our train, immaculately presented, was ready and off we went, exploring the main line through the beautiful gardens with spring flowers on display.

We divided into groups for the factory visit. My group was conducted by Eve, whose knowledge was inexhaustible; she appeared to know every process in detail and was prepared to take the time to explain and to answer questions. We saw every stage of model railway track and stock construction and finishing, which was far more complex than any of us had imagined. We were all impressed by the skill and enthusiasm of the staff, their dexterity and attention to quality and detail. Everyone we spoke to was positive, welcoming, friendly and happy to explain.



What particularly held our attention was the innovative approaches to the incorporation of modern technology into the manufacturing processes. I would have happily done the factory tour again, and would doubtless have seen and learned things that I had missed the first time around, but it was time to return to *Orion*.

Tina and Samantha served us with an excellent lunch. There was ample food and then, with everybody fully satisfied, we returned to the miniature railway. This time we were in two groups, with the first group doing a tour of the remote parts of the railway, including from Beer Mine Junction to Wildway Down and the headshunt as far as we were able to get towards Beer Mine Bottom. The second group traversed all available roads into the carriage and engine sheds, including the turntable!

A change of trains ensued and the routes were repeated, thus everyone had the opportunity to travel on all the

available track. Branch Line Society members like to travel on every possible inch of railway track, right up to the buffers, and Peco cheerfully obliged. The train staff and crews, all professional, pleasant and helpful, made sure that this was achieved, resulting in some very happy Branch Line Society members.

We rounded off our day with a visit to the model railway exhibition, where all the layouts were operating for us, and to the shop where we were delighted to be offered a special discount. Thus our visit was concluded.

So, what did we gain from the day? Our visit was clearly well planned, organised, hosted and informative, far exceeding our expectations. Those who use a red pen to mark, on a track diagram, every possible piece of track they have travelled over, were a happy bunch indeed; our coverage of the railway was comprehensive. For the rest of us, some of whom have used

▲ **Members of the Branch Line Society gather alongside Pullman car *Orion* during their visit to Pecorama on 8 March. Photo: Samantha Maddox**

Peco products for many years, it was an enjoyable, interesting day which left us with a very positive view of Peco and Peco products, now so much more to us than a name moulded into the underside of railway sleepers.

Was it worth my trip from north-east of Carlisle? Definitely; worth every penny!

A very big thank you to Tina, Samantha, Eve and the other members of staff who looked after us so well and who made our visit so special.

**ADRIAN TAYLOR**

[Ed. For further details about Peco Factory Tours, Locomotive Driving Experiences on the Beer Heights Light Railway, and other special events, go online to: [www.pecorama.co.uk](http://www.pecorama.co.uk)]

## Memories of Peco in the 1960s

Thank you for the fascinating *Peco – 80 years* booklet that was included free with the May 2026 edition of RAILWAY MODELLER.

I was an administrator with the former Seaton Urban District Council from 1964 to 1966, and was heavily involved in the closure of the branch railway. The Council decided to apply for an Industrial Development Certificate from the Board of Trade for the station site, which I negotiated successfully.

We had hoped to persuade Peco to build its new premises on the station site, but the company decided to go to Beer!

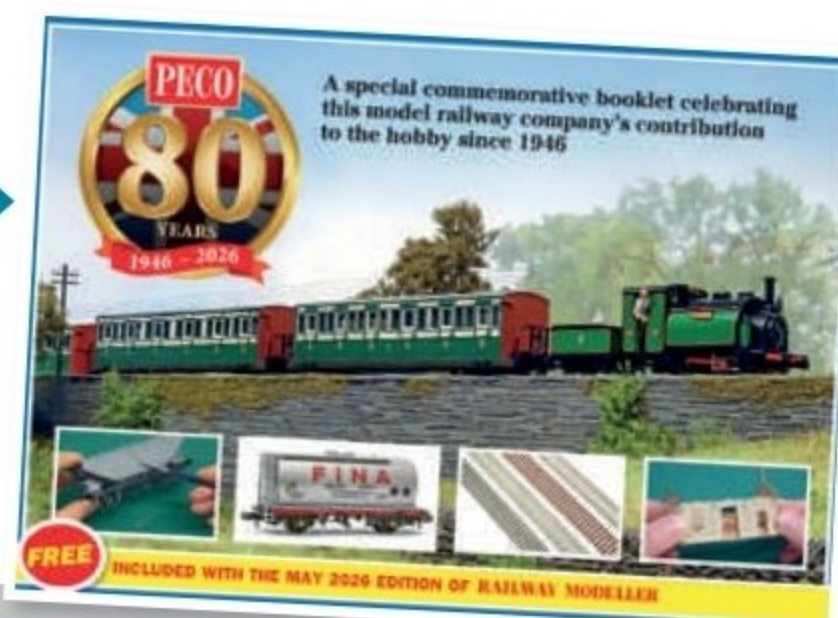
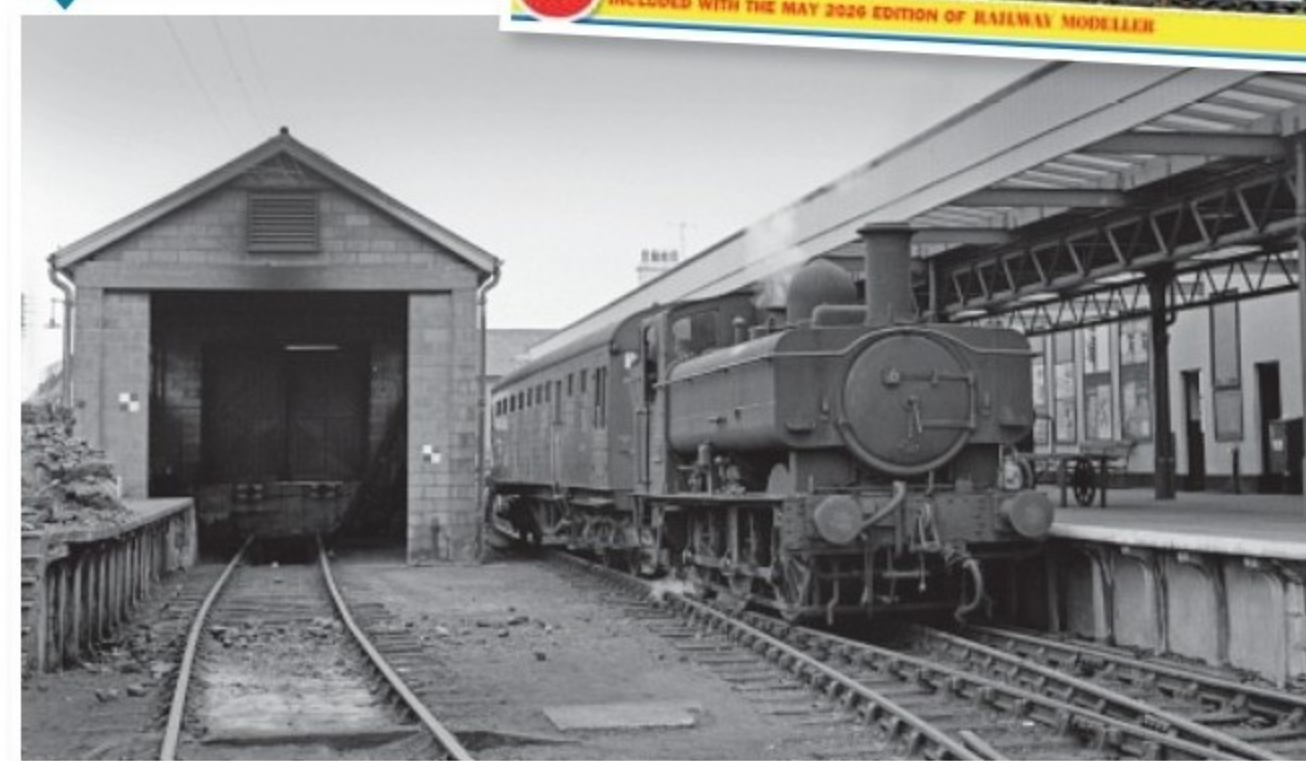
The booklet brought back very happy memories of my two years in Seaton working for the Council. Thank you again.

**RICHARD HAMPSON**

The cover of the 80th anniversary booklet.

**GWR 0-6-0 pannier tank No.6400 at Seaton on 18 July 1963.**

**Photo: Hugh Ballantyne/Rail Photoprints**



## Modelling and the benefits for mental health

Reading Ian Pickvance's letter following up on the article in the April RM by James Walker (about neurodiversity and the value of railway modelling) reminds me of another article in RM a few years ago. This discussed depression and how railway modelling can help. Hobbies such as gardening and various forms of artistic pursuit or sport can all help in control of these conditions. A further benefit of railway modelling is that, as it can be stopped and started at will, it doesn't create an additional 'must do' pressure or worry. Hobbies really can be a great benefit for all in their daily and often stressful lives today. Thanks to both authors for their input.

**DAVID BECK**

[Ed. We believe that the earlier article to which David refers is likely to be the Talking Points feature published in the April 2016 issue, which was penned by David Powell and titled 'The hobby's hidden health benefits'.]

## Class 303 EMU model appeal

I am looking for an OO gauge model of a Class 303 Electric Multiple Unit, known as the 'Blue Trains', introduced in 1960 on the north bank of the Clyde.

The model would be for display only, so does not need to be powered, or even usable, as long as it looks 'right'. Second-hand is fine and even a single driving end vehicle would suffice, but I would want it in original livery (not BR blue & grey, plain BR blue, or Strathclyde orange).

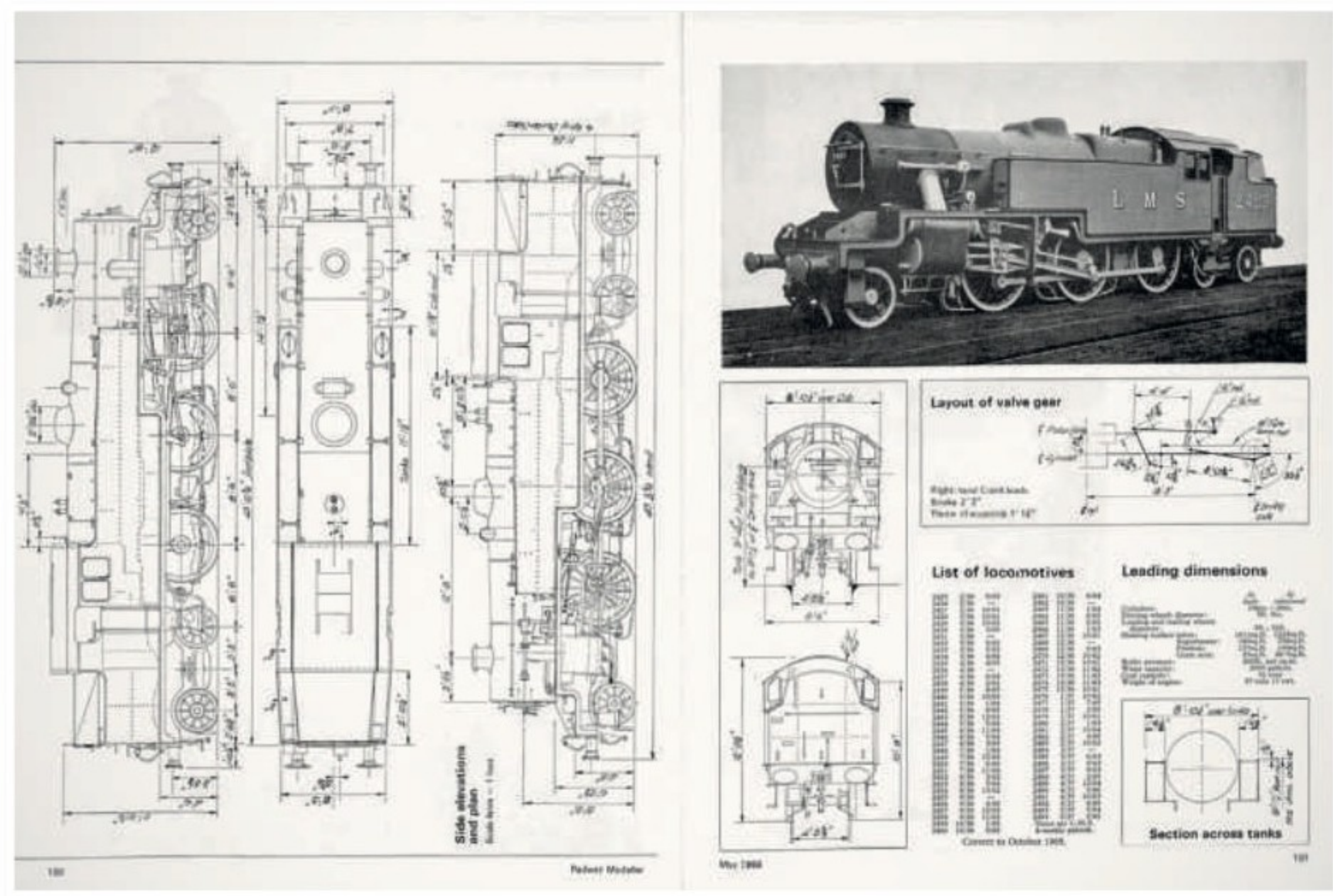
**JAMES DUNCAN**

[Ed. Kits have been produced in OO by DC Kits, Britannia Pacific Models and Modern Traction Kits.]



▲ One of the Class 303 units at Airdrie in its original blue livery. Photo: Dave Cobbe collection/Rail Photoprints

## Reference wanted for a Stanier 2-6-4T build in Gauge 1



I am currently planning to scratch-build a model of an LMS Stanier 2-6-4T to a scale of 10mm:ft (Gauge 1). Has there ever been an article published in RAILWAY MODELLER describing such a project?

**KEVIN R FLEET**

[Ed. 4mm scale drawings by Bill Ibbott of a Stanier 2-6-4T were published in the May 1966 edition. Whilst we don't believe that such a project has

been chronicled in RM, there was a series of articles by Jim Smith (April-June 2024) describing the scratch-building of a London & North Western Railway 'Watford Tank' in Gauge 1, from which many of the techniques and methods would be transferable to other locomotive designs.

All 909 issues of RAILWAY MODELLER are, of course, available to subscribers on the RM Digital Archive.]

## Further information sought on Gresley locomotive models

Thank you for the excellent Gresley 150 Scale Drawings article (RM June). It was great to be reminded of his many beautiful designs. Perhaps No.6701, his electric locomotive of 1941, should have been mentioned too?

The article gave a lot of helpful modelling suggestions, but do any readers know of OO gauge models of the Great Northern Railway H2 and H3 classes (LNER K1 and K2), or the LNER K4, or V4?

I would love to see a table of 'who makes what' model locomotives, across all the scales from N to O gauge.

**CHRIS TUCKER**

[Ed. The online LNER Encyclopedia is a great resource for prototype and model information on specific locomotives, including those absorbed from pre-Grouping companies: [www.lner.info](http://www.lner.info)]



▲ Gresley V4 2-6-2 No.61701 moves off Haymarket shed in July 1955. Photo: David Anderson/Rail Photoprints

## Support for a heritage model railway society?

Since the turn of the millennium, model trains have seen a vast improvement in the levels of detail and features as a result of advances in manufacturing technology. Alongside this, however, has been a consequent increase in the prices of new models.

Therefore, I am proposing the formation of a 'heritage model railway society'. This would not be another collectors' association – instead, the aim would be to promote the benefits of creating layouts using older models, as an inexpensive and simple way of enjoying the hobby.

Eligible models would include those manufactured by Airfix/GMR, Lima, Mainline (Palitoy) and Minitrix, along with pre-2000s Bachmann, Dapol, Graham Farish and Hornby (including Tri-ang), plus many others. Such models can be picked up relatively inexpensively – online, at model railway exhibitions and at most model shops. They are often straightforward to maintain with basic tools, and are supported by ready access to spares and detailing parts, should one need or wish to employ them.

The one drawback to this approach is the lack of pre-2000 models for certain routes and prototypes when compared to more recent ready-to-run offerings – the Southern Electric, in particular, is one area that has only received the attention of the major manufacturers comparatively recently. However, there are still many subjects that can be modelled easily with pre-2000s models, in periods up to and including the early Privatisation era.

Such a society would be more about the overall scene than absolute accuracy. All enquiries and expressions of interest should be sent, in the first instance, to:

**KEVIN RUSSELL**

Email: [kevinsrussell2@gmail.com](mailto:kevinsrussell2@gmail.com)

## Praise for Wessex TMD

I am writing to say how much I enjoyed the Through the Railway Room Door article in your June 2026 issue of RM, describing Wessex TMD, written by David Skuse.

It is always enjoyable reading about people's experiences and motivations for building their layouts, but I was particularly interested in the perspective of someone who also shares and understands the difficulty of modelling with an Essential Tremor, which I have had for more than 30 years.

Like David, I have had to find many varied and creative ways to overcome the limitations that I experience with this condition, not only from a railway modelling perspective, but also in life in general.

The article that David wrote was positive and encouraging, and shows that 'where there's a will there is always a way'. The results he has achieved, and his plans for future building projects, are to be applauded.

**CHRIS TWAMLEY**



▲ A Class 31 and Class 97 (formerly 24) take centre stage in this scene on Wessex TMD. Photo: Craig Tiley/RM Archive

# NEW PRODUCT REVIEWS

FROM **RAILWAY MODELLER** BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

## Manning Wardle L Class

New from Rapido Trains UK in OO

Announced in our October 2023 issue, the latest ready-to-run model in OO to be released by Rapido Trains UK covers the Manning Wardle L Class 0-6-0ST – built by the famous Leeds-based firm over a 45-year period between 1881 and 1926.

Favoured by civil engineering contractors and several light railways, the L Class could be tailored to the customer's requirements, with the Rapido model being no different. 10 versions form the initial launch range, with a comprehensive tooling suite catering for a multitude of different detail permutations, including: curved or diagonal-ended frames; single or double boss wheels; tapered, parallel or dumb buffers; open footplates with weatherboards or fully-enclosed cabs; as well as variations in smokebox doors, steps, handrails and pipework arrangements.

Supplied Digital Command Control ready or DCC sound-fitted, the liveries and identities being produced are: Logan and Hemingway No.20 (Works No.1191) in lined green; plain red; *Bombay* (Works No.1674) in lined red; *Forward* (Works No.1690) in green; *Arthur* (Works No.1601) in green; the same engine in its 2010s preserved guise – named *Matthew Murray* – in lined blue; and *Sir Berkeley* (Works No.1210) in both 2010s Cranford condition (plain green with red running plate) and as currently preserved at the Middleton Railway in lined green.



In addition to the standard models, Rapido has produced two 'what if' versions: one in South Eastern & Chatham Railway lined green – referring to the fact that this company did possess some Manning Wardle 0-6-0STs, albeit of different classes – while the other is in the popular Bluebell Railway-style lined blue colour scheme and bears the name *Hawthorn*; this ornate livery was previously applied to the firm's SECR O1 Class 0-6-0, with the name *Bramble*.

### Our sample in detail

Our sample (ref.960001) represents one of two class members preserved at the Middleton Railway – Works No.1601 *Matthew Murray* in lined blue. Outshopped in 1903, this example spent its early service career as a contractor's locomotive – working for P & W Anderson Ltd and used in the construction of the Kent Portland Cement Works at Stone, near Dartford. Following completion of this contract, it was sold to the cement company and

operated with the Associated Portland Cement Manufacturers, carrying the name *Arthur* (another Rapido model depicts it in this condition).

Initially preserved in 1967 at the Kent & East Sussex Railway, it moved to Peak Rail in 1985, and four years later to the Middleton Railway, where it later returned to traffic and resides to this day. It is now named after Leeds engineer Matthew Murray, who designed and built the first commercially viable steam locomotive for the Middleton Railway in 1812.

The model depicts No.1601 in its preserved form circa 2010. Features specific to this enclosed cab locomotive include curve-ended frames, single boss wheels, parallel buffer shanks, riveted smokebox door, drain and steam feed for the Roscoe lubricator mounted on the front of the saddle tank, plus additional saddle steps and grab handles for access to the water filler.

The application of the elaborate colour scheme is second to none: the rich blue paintwork exhibits a silky satin sheen, while the smokebox colour has a noticeably flatter appearance than the rest of the black paintwork. The white lining has been applied with great finesse throughout, providing a clean delineation with the black border surround. All of the lining is straight and even, with no visible blemishes or fuzziness. Etched versions of the printed name and builder's plates are provided, with these having been fitted for photography.





▲ The cab is fully detailed, with many parts fitted separately.



▲ Pipework and the various linkages on the side of the boiler and saddle tank are separate fittings.

Construction makes extensive use of die-cast metal. This includes, but is not limited to, the saddle tank and smokebox, boiler, running plate, cab, wheels, frames and chassis block – giving the model an impressive overall weight of 117g, despite its diminutive size. Other metal fittings to note include coupling rods, turned brass chimney, and buffers (the latter not sprung). Handrails and grab handles are formed from a mixture of fine metal wire and durable plastic, with the former utilised for the curved handrail over the smokebox – extending from the front of the saddle tank and encompassing the chimney.

Produced as separate plastic fittings, the vast, complex array of pipework and linkages which adorn the sides of the boiler and saddle tank have been captured beautifully. Most impressive are the commendably fine renditions of the lubrication pipework on the fireman's side of the running plate: moulded in fine plastic, these pipes faithfully mimic the natural kinks and bends of the prototype fittings. A portrayal of the inside motion (picked out in red) is present between the frames, with representations of the brake pull-rods running parallel below the running plate. Optional parts supplied for the modeller to fit include a set of vacuum pipes, dumb buffers and decorative three-link couplings – the top links of the latter clipping over the locating holes in the coupling hooks.



▲ Dumb buffers are included in the parts pack for the modeller to add if required.

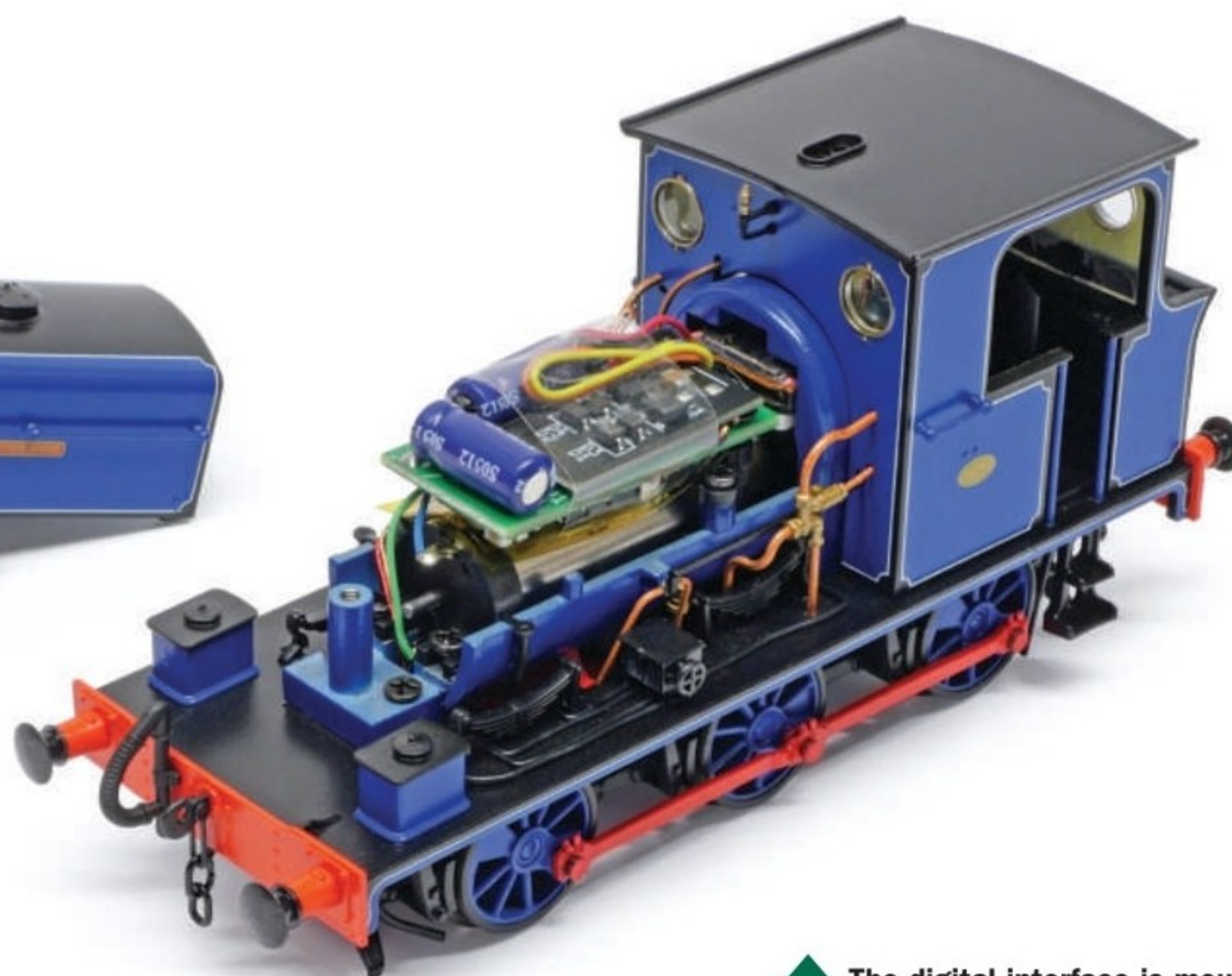


The firebox backhead displays a comprehensive array of copper pipework, brass handles and burnished controls, with the majority being separate fittings – these details no doubt being appreciated further on examples with open footplates. The glazed spectacle windows feature depictions of the brass frames, complete with central pivots. Unfortunately, no firebox glow is present.

Slimline tension-lock couplings, housed within dovetail-mounted NEM pockets, are fitted as standard – these being mounted between the frames, each with a degree of sideways movement. Whilst these are intended to be removable, we found them to be glued in place on our sample, resulting in their breakage when attempting to remove them for photography.

#### Mechanism and performance

The model is equipped with a flywheel-fitted coreless motor, housed within the casting of the boiler barrel, providing drive to the rear axle. Current is transmitted to the motor from all



▲ The digital interface is mounted above the motor.

wheels via a combination of plunger and wiper pick-ups: the former on the front and centre wheels, with the latter exclusive to the rear wheelset. All axles are seated in brass bearings.

A printed circuit board sits atop the motor, equipped with an E24 DCC interface and a factory-fitted 15mm x 11mm 'sugar cube' speaker sited within the firebox. All models, including our DCC ready sample, are also equipped with MoPower stay-alive capacitors, to smooth out any interruptions to the current collection when operating on DCC.

Access to the mechanism and decoder interface is achieved by removing the smokebox and saddle tank – these being held in place by a single screw located within the chimney. Once this is removed, the smokebox can be lifted carefully, before sliding the whole assembly forward, disengaging the locating lug under the front of the cab. Extreme care must be taken to avoid catching the cylinder drain cock linkage.

On test, the model proved to be a smooth and quiet performer through

the speed ranges, down to prototypical shunting speeds, and ran without hesitation through large radius Peco Streamline code 75 bullhead points.

#### Manning Wardle magic

This project has been something of a personal passion, with no secret made of the fact that Andrew Hardy, General Manager of Rapido Trains UK, passed his steam locomotive driving test on this very engine at the Middleton Railway! But these decidedly dainty and attractive little locomotives, in their colourful liveries, will no doubt appeal to many others.

#### Sample supplied by

RAPIDO TRAINS UK LTD,  
Unit 3, Clinton Business Centre,  
Staplehurst, Kent TN12 0QF  
[www.rapidotrains.co.uk](http://www.rapidotrains.co.uk)

#### PRICE

£149.95 DCC ready  
£259.95 DCC sound-fitted

# 00 Hornby 'Railway 200' Class 801/2 Azuma

Hornby has received the second of its two present-day high-speed Electric Multiple Units carrying commemorative liveries in connection with the Railway 200 celebrations, which marked the bicentenary of the opening of the Stockton & Darlington Railway in 2025. Plans for these models were revealed in our May 2025 issue, with the first – a four-car Southeastern Class 395 Hitachi Javelin EMU presented as set number 395 015 (ref.R30442) – featuring in our July 2025 issue.

Supplied Digital Command Control ready, this latest Railway 200-themed release reflects LNER Class 801/2 Azuma set number 801 207 (part of the AT300 series of Hitachi passenger trains), which carries large 'Darlington' graphics along the sides of the PDTRBF (Pantograph Driving Trailer Restaurant Buffet First) celebrating the town's connection with the birth of the modern railway, with the wording 'Driving innovation for 200 years'. The graphic was officially unveiled by LNER on 14 January 2025.

Presented in a stylish red card box with white Railway 200 branding, the five-car pack (ref.R30460) comprises PDTRBF car No.829207, Motor First (MF) No.828207, Motor Standard (MS)



cars Nos.825207 & 822207, and Pantograph Driving Trailer Standard (PDTs) car No.821207. Additional centre cars – which make up the full nine-car formation – are available separately in the form of a four-car coach pack (ref.R40493), comprising Motor Composite (MC) No.827207, Trailer Standard (TS) No.826207, Trailer Standard Restaurant Buffet

(TSRB) No.824207 and MS No.823207.

Construction and transmission arrangements are similar to other members of the Hitachi AT300 family produced by Hornby, which to date include the Class 800 (see reviews, RM May 2018), and more recently the Class 802/2, which was reviewed in our March 2025 issue. The mechanism

– which comprises a twin flywheel-fitted five-pole motor, providing power to both bogies – is concealed within the PDTRBF; the motor, for the most part, is obscured by the vinyl covering of this vehicle. All axles are driven, with current collected from all wheels – these running in metal bearings.

The unit is equipped with directional and interior lighting. Each car is



# 00 Hornby RailRoad 'Drax 50' Class 66

Hornby has gone for gold with its latest General Motors Class 66 Co-Co diesel-electric locomotive, depicting class celebrity GB Railfreight 66 301. Named *Drax Power Station 50* at the eponymous power station near Selby, North Yorkshire, on 18 August 2024, the locomotive sports a distinctive gold colour scheme, which pays tribute to the ongoing partnership between the freight operator and the renewable power pioneer, Drax Group; this commemorative livery was also applied to an IIA-D biomass hopper, with Hornby also producing a model of this vehicle (ref.R60322).

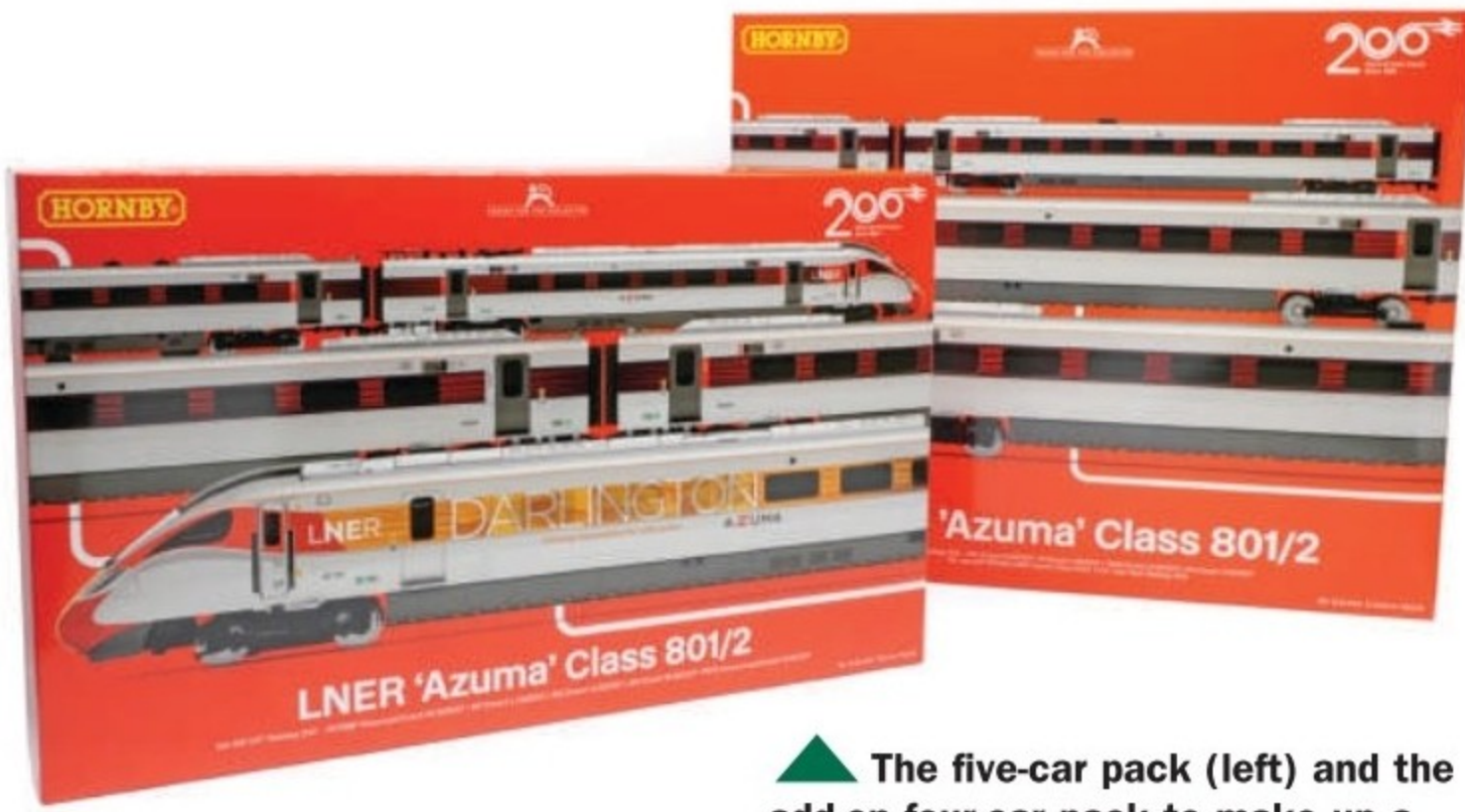
Initially released by Lima in 1999, this Class 66 tooling now forms part of the Hornby RailRoad range of budget models, featuring an upgraded mechanism and – most recently – a new 21-pin Digital Command Control



▲ Metal nameplates are provided to glue over the model's printed versions.

decoder interface. The model (ref.R30461) comes presented in a lavishly decorated box, which mirrors the gold & blue colours of the prototype. As with other recent releases under the RailRoad banner, the model features an enhanced level of decoration, faithfully capturing the bold metallic livery – with printed metal versions of the nameplates provided for the modeller to fit.

Current is collected from all wheels (with the exception of the centre wheelset on the motor bogie) via wiper pick-ups – though two of the driving wheels are equipped with traction tyres to aid adhesion. The bogies incorporate NEM coupling sockets, with slimline tension-lock couplings fitted as standard; these can be swapped for full-depth versions of the striker plates, which attach to mounts on the bogies.



▲ The five-car pack (left) and the add-on four-car pack to make up a full nine-coach set.

equipped with wiper pick-ups, which collect current directly from the wheels, powering roof-mounted LED strips; these illuminate when power is applied to the track under 12V dc operation or constantly when on digital control. A capacitor in the latter's circuitry provides continuity of supply in the event of momentary interruption. Two decoders are required for digital operation, with 21-pin interfaces situated within the underframe equipment modules on both the

PDTRBF and PDTS, these providing digital control of the directional marker and tail lights, in addition to motor functions (PDTRBF only). Each module is held in place by three screws, with ample space provided for a suitable speaker for those wishing to install sound.

Recommended for use on curves of no less than second radius, the connections between cars are by way of a clip-fit drawbar system, ensuring the correct orientation of vehicles. The



▲ The cars are connected via a clip-fit system.



▲ Alternative wheelsets with traction tyres are provided.

nose covers can be unclipped to reveal non-functional representations of the automatic Dellner multiple-working couplings. These mouldings – housed within NEM pockets – can be swapped for suitable couplings to allow two sets to be paired in tandem.

The full nine-car set made an impressive sight on the Pecorama loft layout. A pair of substitute wheelsets – each equipped with a single traction tyre – are intended to increase adhesion, but we found that even with these fitted, the full nine-car formation struggled on the challenging 1 in 48 gradients and 3' radius curves.

This commemorative model draws a neat conclusion to a year of railway celebrations, in a similar vein to the real bi-modal unit, which was posed at Darlington station for the arrival of replica Stockton & Darlington Railway 0-4-0 No.1 Locomotion during the

latter's historic three-day journey between Shildon and Stockton in September 2025 – retracing the route taken by the original on 27 September 1825.

### Samples supplied by

**HORNBY HOBBIES Ltd**  
Westwood, Margate, Kent  
CT9 4JX  
[www.hornby.com](http://www.hornby.com)

#### PRICES

Ref.R30460 – LNER Azuma Class 801/2 five-car train pack (Railway 200) – £549.99

Ref.R40493 – LNER Azuma Class 801/2 coach pack (Railway 200) – £289.99



▲ The model is presented in a gold & blue box, matching the colour scheme of the locomotive.

Decorative screw-link couplings and air hoses are also supplied as part of the covering accessory bag.

As with previous releases under the RailRoad brand, the model provides a suitable basis for those of lesser experience to try their hand at customising a ready-to-run model; an article documenting such improvements featured in our July 2024 issue.

### Sample supplied by

**HORNBY HOBBIES Ltd,**  
Westwood, Margate,  
Kent CT9 4JX  
[www.hornby.com](http://www.hornby.com)

#### PRICE

Ref.R30461 – £94.99 DCC ready

# 00 Cavalex PXA & JXA wagons (Rails Exclusive)



Rails of Sheffield has received stocks of its all-new ready-to-run PXA steel carriers and JXA bogie scrap wagons in OO, produced exclusively in conjunction with Cavalex Models.

The 54 101-tonne PXA wagons were built by Powell Duffryn in 1986 for steel coil traffic from South Wales steelworks at Port Talbot, Trostre, Llanwern and Ebbw Vale to Dee Marsh, Round Oak, Brierley Hill and Swindon. Other locations served by the wagons in later years included Wisbech and Carlisle (loaded with tinplate), Hamworthy (hired by Poole shipping agent John Carter for coil traffic to Swindon), Boston, King's Lynn, Blackburn and Goole, with the last example remaining on the Total Operations Processing System until January 2007.

The JXA bogie box wagons were converted in 1994 from 38 steel coil wagons, for scrap traffic from Snailwell

and Willesden to Sheerness steelworks. Being located in the area of freight operator Mainline, Class 58s were the usual motive power, but pairs of Class 37s also appeared on the Snailwell workings, and double-headed Class 33s on the Willesden services.

The sphere of operation of the JXAs widened in 1998 when the Sheerness plant was purchased by Allied Steel & Wire, and the scrap wagon fleet there was amalgamated with those at the firm's Cardiff Tidal works. However, withdrawals of the wagons began in the early 21st century, and the Sheerness works closed in 2012. A few JXAs remain in service as internal user wagons at Scunthorpe steelworks.

Offered in triple packs, the models reflect the various branding and markings received by both types of wagon over the course of their service careers, together with an additional



▲ The JXA bogie scrap wagon and its Rails-branded packaging.

# 00 Irish Railway Models Bulleid Pallet Vans



Irish Railway Models has received stocks of its Córas Iompair Éireann 12-ton Pallet Vans in OO.

499 of these 10' wheelbase wagons were built in 1964-65 to handle the growing trend for palletised freight. Utilising CIÉ Chief Mechanical Engineer Oliver Bulleid's patented triangulated chassis, the vans featured offset sliding doors for forklift loading, and were used to carry everything from palletised goods, as well as larger items such as tools and machinery. However, the vehicles were designed as a stopgap measure, and within 10

years, block trains of continuously braked bogie wagons began to displace them. A dwindling number soldiered on into the late 1970s and early 1980s,

when CIÉ finally ceased operating unfitted goods trains.

The models are supplied in triple packs, and for review we received a pair

▲ Our samples in CIÉ grey and bauxite liveries.

of samples: ref.IRM1290, containing vans Nos.26185, 26199 and 26204 in grey with CIÉ roundel; and ref.IRM1291, comprising Nos.26070, 26083 and 26123 in brown with CIÉ roundel. The IRM tooling caters for two door types (panelled and flush-sided) and two body end styles (corrugated and cross-braced), reflecting material shortages faced during their construction; all

PXA twin pack – comprising two vehicles sporting ‘John Carter Poole Ltd’ banners – and a single JXA featuring a yellow body band. Each pack or single model is also available with a bespoke weathered option, replicating the true look of these wagons in service.

Received and illustrated here are samples of PXA triple pack A (ref.PXA01 TP(A)) comprising vehicles Nos.3024/3037/3048 in as-built, circa 1988 condition, and JXA triple pack A (ref.JXA01 TP(A)) featuring Nos.3012/3042/3051 in as-converted, circa 1995 condition. Printed details – including branding, data panels and warning labels – are applied sharply throughout, and all are fully legible under magnification. Note the yellow bogies on the JXAs: this colour scheme was applied shortly before conversion to assist with in-service inspections.



▲ The JXAs feature distinctive yellow-painted bogies.



▲ The moulded PXA hood looks very convincing.

Construction takes the form of die-cast metal for the chassis, with injection-moulded plastic utilised for much of the upper works – adorned with many fine separate plastic and metal wire components. The taut appearance of the tarpaulin covers on the PXAs has been captured well, with the moulding convincingly mimicking the natural creases and ripples of the material, stretched over its ribbed supporting framework. The buffers are plastic, but sprung, with screw-link couplings and air hoses supplied as optional fittings.

The bogies are equipped with free-rolling metal wheelsets on pin-point axles, which can be swapped for P4 or EM versions using 26mm axles, with alternative brake shoe assemblies (with wider spacing) provided to accommodate these. Sprung self-

centring NEM pockets are mounted to the underside of the chassis, with slimline tension-lock couplings fitted as standard. All versions are now in stock and available to order from Rails of Sheffield; for further details, see the retailer’s website.

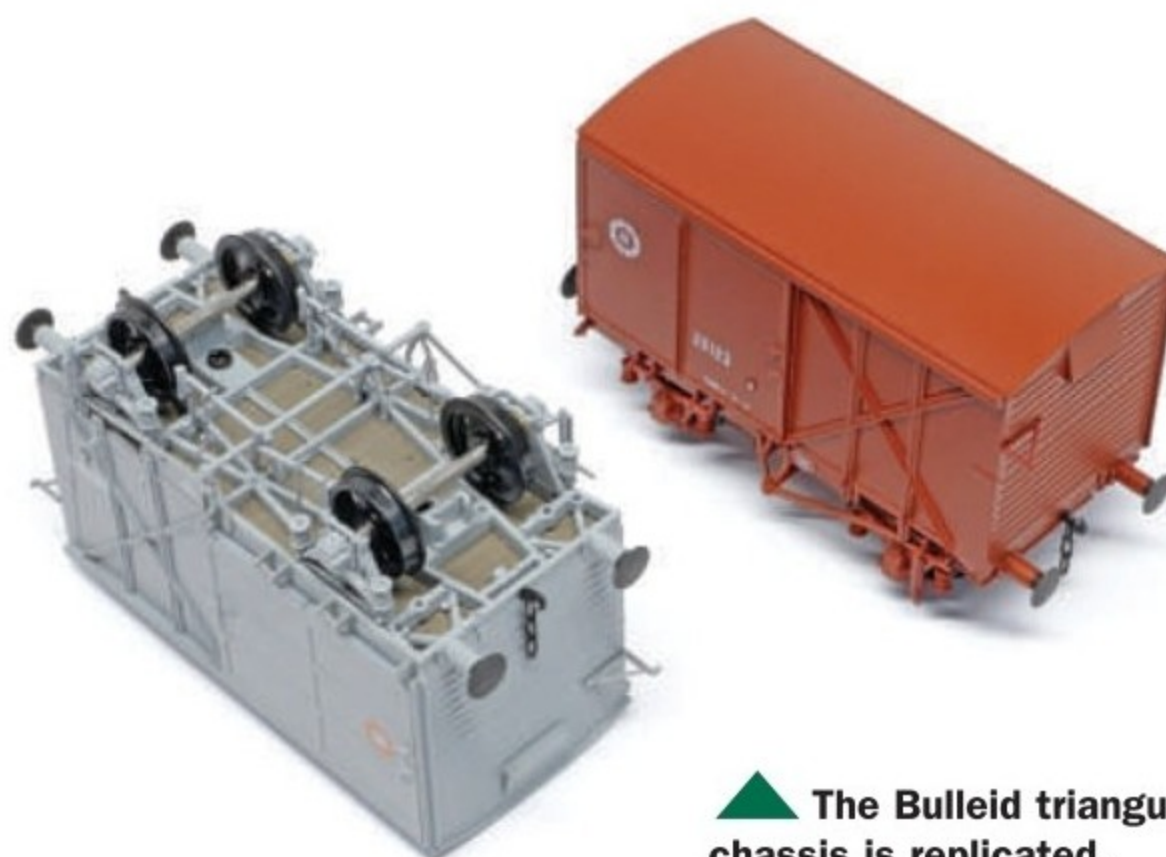
### Samples loaned by

**RAILS OF SHEFFIELD**  
21-29 Chesterfield Rd,  
Heeley, Sheffield S8 0RL  
Tel: 0114 255 1436  
[www.railsofsheffield.com](http://www.railsofsheffield.com)

**PRICES**  
Triple packs – £149.95 (pristine)  
£179.95 (weathered)  
Single JXA – £49.95

models across our sample suite feature corrugated ends, with those in grey sporting panelled doors, whilst brown examples are fitted with the flush-sided design.

Construction and specifications largely mirror those of the previous releases, with injection-moulded plastic utilised for the van bodies and characteristic triangulated frames (each decorated with numerous separately fitted plastic and fine metal wire parts), with the internal metal weight cleverly disguised as the planked wooden floor – this being fully decorated and visible on the underside of each vehicle. The axleboxes are equipped with extremely



▲ The Bulleid triangulated chassis is replicated.

free-rolling, chemically blackened metal wheelsets housed in brass bearings, whilst the buffer heads are turned metal and sprung.

The Pallet Van is the latest in a long line of models from the firm, based on the Bulleid patented triangulated four-wheel chassis. Previously released models include: the corrugated 12-ton open (see reviews, RM September 2024); LB and Permanent Way Department 12-ton flat wagon (RM, October 2024); fitted 12-ton H van and 12-ton grain wagon (both reviewed in RM, June 2025); and unfitted 12-ton H van (RM, January 2026).



### Samples supplied by

**IRISH RAILWAY MODELS Ltd,**  
Unit 8, The Hyde Building,  
The Park, Carrickmines,  
Co. Dublin D18 Y3F9

[www.irishrailwaymodels.com](http://www.irishrailwaymodels.com)  
[www accurascale.co.uk](http://www accurascale.co.uk)

**PRICE**  
£109.96/€126.95 per pack

### Narrow Gauge Enigma: The story of Peter Rampton and his Collection X

Stephen D Phillips

There are no doubt many modellers who find they have been guilty of acquiring far more locomotives and rolling stock, or kits and accessories, than they can possibly operate on their layouts – but imagine if that scenario applied to full-size items rescued from a range of countries around the world.

That is exactly what happened in the case of the late Peter Rampton, who almost by accident became a collector of historic narrow gauge locomotives. Despite Peter's involvement in the creation of the Brecon Mountain Railway and acquisition of the Vale of Rheidol Railway, his growing collection remained tightly packed into barns at a farm in Surrey – and became the legendary and much whispered about, but seldom viewed, Collection X.

The story begins with Peter's childhood years growing up in Surbiton, where he watched Southern Railway trains passing through the Art Deco station and ran a live steam Bassett-Lowke 0 gauge Mogul in the loft of the family home. Boarding school years spent at Malvern College in 1948-52 later broadened his horizons to the BR Western Region scene on the Worcester to Hereford line, which ran past the school.

The Rampton family's wealth came from the then prolific Freemans mail order catalogue business, of which Peter's father Eric was a joint Managing Director. In 1963 Freemans became a public company and a successful share issue enabled Peter's father to buy the first of three Surrey farms the family acquired in the 1960s – one of which eventually became home to Collection X. It was also in the late 1960s that Peter met the young trainee teacher Nigel Bowman – today the owner of the Launceston Steam Railway in Cornwall and who has appropriately written one of the forewords to this book.

At that time Nigel had almost completed the restoration of the former Penrhyn Quarry, 1883-built Hunslet 0-4-0ST *Lilian*, and Peter offered him use of one of his barn buildings at Gorebridge Farm for finishing the project. It was not long before Peter himself also began wanting to acquire narrow gauge steam locos to restore.

However, by that time the UK supply of 2' gauge engines was drying up – so Peter began using his contacts to find obsolete equipment abroad from countries including Spain and France. All of these items are detailed in the book with background information, photographs and line drawings in 7mm scale, as are nearly all items from the collection.

A visit Peter made to South Africa in 1969 opened up a new source of narrow gauge loco acquisitions.

We read that, during the mid-1970s, Peter looked seriously into acquiring a disused trackbed on which to run the equipment, with those eventually rejected including the Lynton & Barnstaple Railway and the Moretonhampstead, Bridport and Lyme Regis branches in Devon and Dorset, as well as the course of the former Elan Valley reservoirs construction railway in Mid Wales. In the end he collaborated with the late Tony Hills on acquiring the former South Wales standard gauge trackbed which was relaid in 1' 11½" gauge as the Brecon Mountain Railway.

The mid-1970s was also noteworthy for Peter's visit, accompanied by Nigel Bowman, to the Isle of Man Railway to view the many Victorian 3' gauge carriages that had been abandoned in the sheds at St John's following closure of the Peel and Ramsey lines. They were able to purchase four bogie carriages and two ex-Manx Northern Railway Cleminson six-wheelers, and arrange shipment of them to Surrey – very luckily as it turned out, as many of the vehicles left behind were destroyed in a subsequent shed fire.

Also added to the eclectic mix of items arriving at Gorebridge Farm were a Ransomes & Rapier Ltd steam mobile crane and a larger standard gauge crane

from the Longmoor Military Railway – both of which saw use lifting heavy wooden carriages or rusting locomotives off low-loaders.

There were further acquisitions from overseas, including ex-South African Railways Hanomag of Hanover-built NGG13 Garratt No.82.

In 1983 Peter Rampton finally realised his aim of setting up a self-supporting trust to look after his growing railway collection while also benefiting from tax concessions yielding funds for restoration projects. He named it the Phyllis Rampton Narrow Gauge Railway Trust after his late mother and invested money from his late parents' estate as part of the trust's assets. This subsequently facilitated the 1988 purchase by Peter and Tony Hills of the Vale of Rheidol Railway from British Rail. In the decades since there has been much investment in restoring stations and views lost to tree growth, as well as in conversion of the former GWR loco shed at Aberystwyth as a museum.

The 1990s saw 19 locos joining the Surrey collection, some from India. Among them, Peter was able to buy two Gwalior Light Railway Bagnall Pacific tender locos which have just recently been displayed in their unrestored state at the VoRR. Other purchases included the 2' 6" gauge Bagnall 0-6-2T *Conqueror*, built for Bowater's Paper Mill at Sittingbourne and later used at Whipsnade Zoo. It has just recently been donated to the Welshpool & Llanfair Railway.

What fate awaits other 2' 6" items in the collection has yet to be revealed. They include an Indian Railways Delta Class 2-6-2T, AK16, with a 'long tanks' appearance not unlike the VoRR 2-6-2Ts.

Peter also acquired various small industrial locos, including a Kerr Stuart Wren 0-4-0ST and a Ruston, Proctor & Co. Ltd 4w paraffin-mechanical shunter. Other smaller locos from further afield include Fowler 0-6-0Ts, 0-4-2Ts and 0-4-0Ts used on sugar cane lines in locations as far apart as Fiji and Mozambique.

Peter's search for perfection also extended to his efforts to renovate and enhance his Surrey farmhouses – and sections of the book feature the extraordinary carved stone fireplace he had sculpted based on one in the Great Hall at Edinburgh Castle, no less. He asked the sculptor to incorporate crests of railways whose rolling stock he had acquired. In the same vein, some fine colour plates show the magnificent stained glass window he commissioned with panels showing scenes and crests from four of these railways, including the Manx Northern and lines in Spain and Africa. Sadly, only three of the panels had been completed by the time of Peter's death in 2019, but the VoRR ensured the fourth was added posthumously in 2024.

All in all, this book tells the story of an extraordinary man and his remarkable collection, helping to reveal its full extent after years of speculation and obscurity. Although there are some text errors which have escaped the proofreaders, and some modellers wanting to use the detailed drawings may question the wisdom of printing some of the longer ones across page spreads with gutters, it is an impressive, absorbing and useful piece of research with many photographs printed in both colour and black & white. The high production values go a long way towards justifying the price tag.

Published by the Vale of Rheidol Railway  
www.rheidolrailway.co.uk

280mm x 220mm, 420pp  
**Hardback, £80.00**  
ISBN 978 0 95445 467 8

SA

### Railways Remembered – Southern England 1962-1972

Martin Jenkins and Kevin McCormack

Colour photo albums tend to fall into two specific camps, the first of these tending to cover a form of motive power, particularly steam and increasingly diesel. The second tends to be period-based, although one type of motive power will still tend to dominate.

This volume of photos by Blake Paterson takes a slightly different path. Not only is the time period not where it normally sits for such books, but the subject matter is also much looser. The period covered is what we tend to refer to as 'transition', though this does not pick the usual 1967 cut-off of the end of steam, but drifts gently into the early 1970s. It also takes a looser attitude toward motive power and there are easily as many, if not more, diesel and electric subjects as steam.

There is a single-page introduction, then it is immediately into the photos, which are all colour and full-page in this square-format book. Geographically, we travel east to west, starting on the Ashford line and ending in North Devon. Note that this is very

much Southern Region, as distinct from Southern England including the Western Region, except in passing. Most of the likely SR steam classes get a look in, but there are a large number of DEMUs and even a few EMUs featured, as well as the expected BRCW Type 3s (later Class 33s) and BR Standard steam locomotive classes. With the corporate image upgrade falling in the middle of the date range, a few of the diesels are in blue.

What stands out though are the settings chosen: these are in the main station-based and not open line. As modellers, we tend to model stations, but this can be full of assumptions. This set of photos, if studied carefully, will really get you into the raw detail and blow many of these assumptions out of the water. For example, lamps are not always the concrete quadrant type and everything seems to be painted in peeling cream and faded green – certainly not the pristine colours one might expect. Also, unit



stock excepting, the motive power ranges from unclean to absolutely filthy, there being a consistent theme of a railway in decline and abandonment. It would take a brave modeller to go the whole hog and represent this fully.

This is an excellent book, and while the subject may seem well-worn at first glance, the slightly different perspective makes it a great hunting ground for the serious SR

modeller who is determined to get things right.

CF

Crécy, 1a Ringway Trading Estate, Shadowmoss Rd,  
Manchester, M22 5LH  
www.crecy.co.uk

225mm x 227mm, 160pp  
**Hardback, £22.50**  
ISBN 97818003533497

# WIN A CAVALEX MODELS CLASS 56 IN OO

**WORTH  
£199.00!**

## RAILWAY MODELLER is giving readers the chance to win a Limited Edition 'Peco' Class 56 model in OO.

The ready-to-run OO gauge Class 56 locomotive is finished in Colas Rail Freight livery as 56 302 PECO – *The Railway Modeller* – 2016 70 years (ref.CM-56302-CR) and is supplied DCC ready with a 21-pin decoder socket. Features of this Limited Edition model include directional lighting, working fans and separate etched nameplates, whilst the mechanism comprises a centrally mounted five-pole motor that is equipped with twin flywheels and provides power to both bogies. For a full evaluation of the Cavalex Models Class 56, refer to the May 2024 edition of RAILWAY MODELLER, whilst a review of the Limited Edition version presented as 56 302 was published in the June 2026 RM.



Commissioned from Cavalex Models by the organisers of Model Rail Scotland and sold through Rainbow Railways, this model is priced £199.00 new (based on the retail price listed on the Rainbow Railways website: [www.rainbowrailways.co.uk](http://www.rainbowrailways.co.uk)).

**To have a chance to win, all you have to do is answer the following question and enter via the RAILWAY MODELLER website no later than 10 July 2026.**

### QUESTION

**At which works was the full-size locomotive (56 302) built?**

- A. Electroputere, Romania**
- B. Doncaster** C. Crewe

### HOW TO ENTER

Complete the competition entry form online at [www.peco-uk.com/pages/rm-competition](http://www.peco-uk.com/pages/rm-competition) (or scan the QR code) for your chance to win. See the website for full terms and conditions. Competition entries can be placed online from 11 June 2026 and the closing date is 10 July 2026. The winner will be contacted no later than 17 July 2026.

No purchase necessary. There is no cash alternative to the prize offered and the editor's decision is final. The competition is not open to employees of Peco or Cavalex, their family members, or anyone connected professionally to either company.



## SER O Class in OO from Rapido Trains UK

Rapido Trains UK has announced it is producing new models of South Eastern Railway O Class 0-6-0 tender locomotives in OO.

Designed in 1878 by James Stirling, early locomotives were built by Sharp, Stewart & Company, while subsequent examples of the 122-strong class were built until 1899 at the railway's own Ashford Works, the last five after the merger creating the South Eastern & Chatham Railway. In the period 1900-23, 28 O Class locos received replacement boilers with deeper fireboxes. While remaining predominantly goods engines, they were also used from time to time on passenger duties.

At the 1923 Grouping 31 O Class locos passed to the Southern Railway while a handful were sold to the East Kent Railway. The last was withdrawn by the SR in 1932.

The models feature die-cast metal and injection-moulded construction, firebox flicker with dynamic draw effect on Digital Command Control fitted examples, a factory-fitted speaker, a MoPower stay-alive, a 21-pin decoder socket, plunger pick-ups on the loco plus wheel bearing pick-ups on the tender, and sprung buffers. The tooling will cater for different tender fronts and toolboxes, and smooth or riveted smokeboxes. Optional etched numberplates will be included.

The livery versions comprise: SER No.315 in Cudworth green (ref.SKU

825001); SER No.378 in black with red lining (ref.SKU 825002); SER No.80 in black with red & yellow lining (SKU 825003); SECR No.1 & No.381 in Wainwright green (SKU 825004 & SKU 825005); SECR No.171 in plain green (SKU 825006); SECR No.142 in grey (SKU 825007); SR A8 in lined black with Egyptian lettering (SKU 825008); and SR No.436 in plain black with Egyptian lettering (SKU 825009).

DCC ready models are priced £205.95 and DCC sound-fitted versions £319.95. Currently in tooling, the models are available to order now.

### Horseboxes and carriage trucks

Rapido has also announced that it is expanding its Evolution range of OO gauge generic coaching stock with the



▲ O Class No.315 in SER green.

addition of horseboxes (27 versions available) and open carriage trucks (22 versions). Both models have an RRP of

£39.95 each. For more details, visit the manufacturer's website: [www.rapidotrains.co.uk](http://www.rapidotrains.co.uk)



▲ Evolution horsebox in LMS Crimson Lake.



▲ Open carriage truck in GNR livery.

## Bachmann announces Scottish Class 20s in N

The headline item in the Bachmann Summer Announcements was the unveiling in the Graham Farish N gauge range of new tooling BR Class 20 diesel locomotives.

The new models are of locos built for use in Scotland, with large cab side windows and lower recesses, and with or without tablet catchers. The tooling also allows for a later era when the cab sides were plated flush.

The chassis has been reconfigured to accommodate a speaker and Next18 DCC decoder interface, as the models are being offered optionally sound-fitted.

The livery versions are: D8032 with tablet catchers, in early BR all-over green with red bufferbeams and late

crest (ref.371-030A & 371-030ASF) and D8102, without tablet catchers, also in green and with BR roundels (ref.371-030B & 371-030BSF).

Appearing in BR blue with cab side recesses and yellow ends are 20 077 (ref.371-032B & 371-032BSF) and 20 098 (ref.371-032C & 371-032CSF). 20 090 (ref.371-039 & 371-039SF) and 20 104 (ref.371-039A & 371-039ASF), meanwhile, are in later Railfreight grey with yellow cabs and red footplate stripe, and cab sides plated flush.

The models are supplied with a selection of open and closed disc headcodes, and DCC ready versions have an RRP of £169.95 while sound-fitted models are priced £279.95.



▲ D8032 in BR green in N.

For further details about these models, together with other new items announced – including new livery versions of locomotives and rolling stock in OO and

N, and several additions to the Scenecraft ranges in both scales – see the firm's website:

[www.bachmann.co.uk](http://www.bachmann.co.uk)



▲ 20 090 in Railfreight grey in N.



▲ 20 077 in BR blue in N.

# Exclusive Stanier 'Black Five' for Rails and Hornby Club

Hornby has added two exclusive models to its OO gauge range, one being produced as a retailer exclusive for Rails of Sheffield and the other as a Hornby Club exclusive model.

Both depict London Midland & Scottish Railway Stanier Class 5MT 4-6-0 No.45407 *The Lancashire Fusilier*.

The models, ref.R30563 in the case of the DCC ready retailer exclusive version for Rails, and ref.R30563SS for the steam generator-fitted model being produced as a Hornby Club exclusive, represent the locomotive in its preserved guise,

as regularly operating on 'The Jacobite' steam-hauled services on Scotland's West Highland Extension, with *The Lancashire Fusilier* nameplates fitted in 2004.

The model features a die-cast chassis and running plate, a five-pole skew-wound motor, a detailed cab with fully decorated backhead with instrument dials, and illuminated flickering firebox. There is also an opening cab roof vent, adjustable cab doors and a quick release locomotive to tender connection.

The models also have sprung metal buffers, separately fitted lamps and coal



load, electrical pick-ups on the coupled wheels and tender wheels, and an accessory bag containing tablet catcher, vacuum pipes, lamps and couplings.

The exclusive model for Rails is priced £249.99, with delivery expected

in September 2026; and the Hornby Club exclusive model fitted with steam generator has an RRP of £329.99 and is expected to arrive in spring 2027.

[www.railsofshffield.com](http://www.railsofshffield.com)  
<https://uk.hornby.com>

## TMC commissions bogie box wagons in TT:120



The Model Centre has commissioned four new JNA bogie box wagons in TT:120, which are being manufactured for the retailer by Revolution Trains.

The new models are intended to help expand the existing range of modern TT:120 freight rolling stock by providing a selection of contemporary aggregate and infrastructure wagons finished in Cappagh-branded dark blue and plain VTG blue liveries.

The JNA and MMA box wagons are widely used across the modern railway network in mixed freight formations and are regularly seen being hauled by

locomotives operated by DB Cargo, GBRf, Colas Rail and Freightliner.

The three models carrying 'Cappagh' branding on Mid Ultramarine Blue livery – a lighter shade of blue than the GBRf variants previously announced in the main Revolution wagons range – are refs.TT-EAL-107A, TT-EAL-107B and TT-EAL-107C.

The fourth model, ref.TT-EAL-107D, however represents a debranded plain 'spot' JNA wagon in VTG blue, reflecting the appearance of these wagons as currently operating in service.

The wagons feature separately fitted details, fine printed decoration and wagon-specific variations including braking systems, underframe layouts and body details.

The price of the models is £44.95 per wagon, with delivery expected in the fourth quarter of 2026. TMC states that production quantities will be limited and based closely on the number of pre-orders. For further details about these models, refer to the retailer's website:

[www.themodelcentre.com](http://www.themodelcentre.com)

## Sample of Locomotion 'NRM 50' Class 91 arrives

Locomotion Models has received from Hornby a decorated sample of its OO gauge model of LNER Class 91 electric 91 150, the commissioning of which was announced in RM December 2025.

The model features the *National Railway Museum 50 Years 1975-2025* nameplate carried by the prototype, along with its colourful NRM 50 vinyls marking both the National Railway Museum golden anniversary and the long-standing partnership between rail operator LNER and the museum.

The livery, unveiled at the NRM a year ago this July, runs across both sides of the locomotive and features some of the best-known objects in the museum's collection, including locomotives *Mallard*, No.1 *Locomotion* and *Duchess of Hamilton*.

The model is available to pre-order on the retailer's website priced £244.99 for DCC ready versions (300 in total to be produced) and £299.99 for DCC sound-fitted models (200).

[www.locomotionmodels.com](http://www.locomotionmodels.com)



## Brassmasters releases new kit for LNER J19/2 0-6-0 in 4mm scale

Brassmasters has unveiled a new 4mm scale locomotive kit of the LNER J19/2 0-6-0 which can be built to OO, EM or P4 gauges, as well as announcing further forthcoming locos and the release of new wagon kits.

The J19/2 was the final development of the T77 class, designed by A J Hill and built by the Great Eastern Railway in 1912-1920.

These powerful freight locomotives could be found in all corners of the

ex-GER network and the Midland & Great Northern Joint Railway.

The kit features a resin printed boiler/round-top firebox and tender, representative inside motion and an etched compensated chassis. The cost of the kit is £145.00. Brassmasters adds that a version of the kit to build a J18 or J19/1 is in the final stages of development and will be available in a few months' time.

In addition, more of the resin-printed Meon Valley 4mm scale



wagon bodies have been introduced – kit ref.MVW4125 is for a Railway Clearing House 1907 Glass Houghton Colliery 15' five-plank open, and ref.MVW4126 is for a Stephenson Clarke 16' six-plank open. A 16' seven-plank

Stephenson Clarke wagon is due to follow shortly.

All these kits are priced £14.00, and all prices exclude post and packing. For further details:

[www.brassmasters.co.uk](http://www.brassmasters.co.uk)

# EXHIBITION FOCUS

## Covrail26 to benefit services charity Help for Heroes

The Covrail26 model railway show being held on Saturday 27 June at Sports Connexion, Ryton-on-Dunsmore, Coventry CV8 3FL from 10:30-16:30, will be helping to mark Armed Forces Day by not only raising funds for the Help for Heroes charity but also by offering free entry to veterans and serving armed forces personnel showing ID.

The event, which will feature 48 layouts in scales from G to N, is being staged by Coventry-based model shop Thunderbolt Models, run by Dale Hayes, James Humphreys, Fergus Leahy and Terry Rowe. James, who served in the British Army for 30 years, said: "All of us have strong links with the military, so it makes sense to us that we support Help for Heroes. We all feel that we've been

really lucky with our military careers and want to give something back."

Keeping up the military theme will be a couple of layouts that fit into shell cases excavated from First World War battlefields and an ammunition box, and a special section of 009 micro layouts having the names *Rear Echelon*, *Trenchard Ridge*, *Trenchard Support Trench* and *Trenchard the Fallen*.

Adding to the micro layouts among the show selection are two in N gauge named *Suitcase Layout 1* and *Suitcase Layout 2*. For a full layout list see the Societies & Clubs pages.

The show will also feature demonstrations and numerous trade and society stands. For further details visit:

[www.modelrailwayevents.com](http://www.modelrailwayevents.com)

## More than 30 layouts for RailEX North East 2026

Blythe & Tyne Model Railway Society will be staging its annual RailEX NE event on the weekend of 25 & 26 July.

Taking place at Parks Sports Centre, Howdon Road, North Shields NE29 6TL, more than 30 layouts are booked to attend, these covering a wide array of scales, gauges and eras. Amongst the models in action will be the superb N gauge model of Richmond (pictured) by Peter Brown, which was featured in the August 2024 RM. Supporting the layouts will be traders and several modelling demonstrations.

Opening times are Saturday 10:00-17:00 and Sunday 10:00-16:00, although advance ticket holders are eligible for entry from 09:30. Tickets are priced £10.00 for adults, £5.00 for under 16s (children aged under five are free) and £22.00 for families. Tickets can be booked in advance via the website (below).

The event is being held in conjunction with the nearby Stephenson Steam Railway, which will be holding its own model railway event on the same weekend. Accordingly, a free shuttle bus service will operate from North Shields town centre to RailEX NE and onwards to the Stephenson Railway Museum – see the event website for further details and timetable.

[www.railexne.co.uk](http://www.railexne.co.uk)

## Exeter show to host SWOONS and mark 80 years of S Scale Society

Saturday 11 & Sunday 12 July will see The Exe Model Railway Society in Devon holding its show at Matford Livestock Centre, Matford Park Road, Marsh Barton, Exeter EX2 8FD, with the event running from 10:00-17:00 on the Saturday and from 10:00-16:00 on the Sunday.

The event is due to feature more than 40 layouts and, in addition to the main selection listed in the Societies & Clubs pages of this magazine, will include demonstrations by Exeter Gauge O Group (EXGOG), Exeter N Gauge Railway Modellers and The Exmoor Weatherman.

This year marks the 80th anniversary of the S Scale Model Railway Society, S being 1/64th scale, which equates to 4.76mm to 1' or in Imperial 3/16" to 1'. Use of the scale was first recorded in 1898 and the S Scale Model Railway Society was founded in 1946. Therefore to help mark this anniversary, the Exeter show will include the S scale layout *Nancarrow Junction*.

In addition, there will be a special narrow gauge section staged by SWOONS (the South West 009 Society), including the 009 layouts



*Bleat* by Mike & Jenny Carter (pictured), *Bowmoors Cider* by Terry Bowden, *Foxcombe Colliery* by Francis Farr-Cox, *Tan y Bryn* by John Murrell and *Wild Goose & Riddon* by Geoff Bowyer; as well as the 7mm scale narrow gauge layouts *New Florence Mine Tramway* by Peter Hollins and *Shillingstone Light Railway* by Toby Hollins-Jones.

For further details, see the Societies & Clubs pages in this issue and visit the event website:

[www.exemrs.co.uk](http://www.exemrs.co.uk)



▲ A G5 0-4-4T shunts a rake of vans on the N gauge layout *Richmond*, which will be in action at RailEX North East. Photo: Peter Brown

## Settle & Carlisle-inspired chapel kit from Metcalfe Models

Metcalfe Models has added a new stone chapel kit to its Little Cardleywick range of card buildings kits, for which there is already a stone station building and a station shelter.

The new kit, which will be available to both OO and N gauge modellers, has been loosely based on the chapel at Garsdale Head on the Settle & Carlisle line, a building which was strategically placed at the meeting point of various dales and with a view of Dandry Mire Viaduct.

Now known as the Hawes Junction Chapel, the prototype was originally named the Mount

Zion Chapel and was opened in 1876, the same year in which the Midland Railway inaugurated its S&C route over the Pennines. Funded by the Middleham Primitive Methodist Circuit and constructed by the stonemason Groves & Woodiwiss who had built the adjacent station and cottages, it replaced an earlier timber school, reading room and chapel built by the railway contractors.

Since 1999 it has been looked after by The Friends of Hawes Junction Chapel and has been redecorated in Midland Railway colours.

The footprint of the OO model measures 126mm deep, including porch and steps, by 90mm wide for the square main building, and 36mm wide for the square side building. The price of this kit is £16.00.

The version for N gauge modellers measures 75mm deep, including porch and steps, by 55mm for the square main building, and 29mm wide for the square side building. This kit is priced £12.50.

For further details about all the models in the range, visit the Metcalfe Models website:

[www.metcalfemodels.com](http://www.metcalfemodels.com)



## Colas Class 56 nameplates presented to Peco

The 'Peco' Class 56 locomotive, Colas Rail Freight 56 302, which since June 2016 has carried the name *PECO The Railway Modeller 2016 70 Years*, has been sold to new owners.

With the locomotive's identity and livery removed following the sale (it was subsequently repainted into an unbranded white guise), the opportunity was taken for both of the cast nameplates that formerly adorned the locomotive to be presented to Peco.

Pictured here during the formal presentation of the plates on Wednesday 20 May 2026 is (left) Steve Haynes

(Head of Business Development, Publishing & Marketing at Peco) and Simon Ball (Freight Director – Services,



Colas Rail Freight) with the unbranded and re-liveried former 56 302 behind.

Cavalex Models recently delivered stocks of its OO gauge models of 56 302 with Peco name (see review in the June RM). The manufacturer has kindly provided us with a DCC ready version model as a competition prize – turn to p571 to find out how to enter and be in with a chance of winning this fantastic prize, which is worth £199.00.

Steve Haynes (left) receives the nameplates on behalf of Peco from Simon Ball (Freight Director, Colas).



## Peco delivers new bullhead curved points in OO

Peco has further expanded its range of OO gauge Streamline code 75 bullhead trackwork with the delivery to retailers of its new curved two-way points.

Comprising curved right-hand (ref.SLU1186) and curved left-hand (ref.SLU1187) versions, the points are equipped with Unifrog and match the geometry of the flat-bottom curved points already in the Streamline range. With a length of 258mm, the radii of the curved routes are 762mm (inner) and 1,524mm (outer).

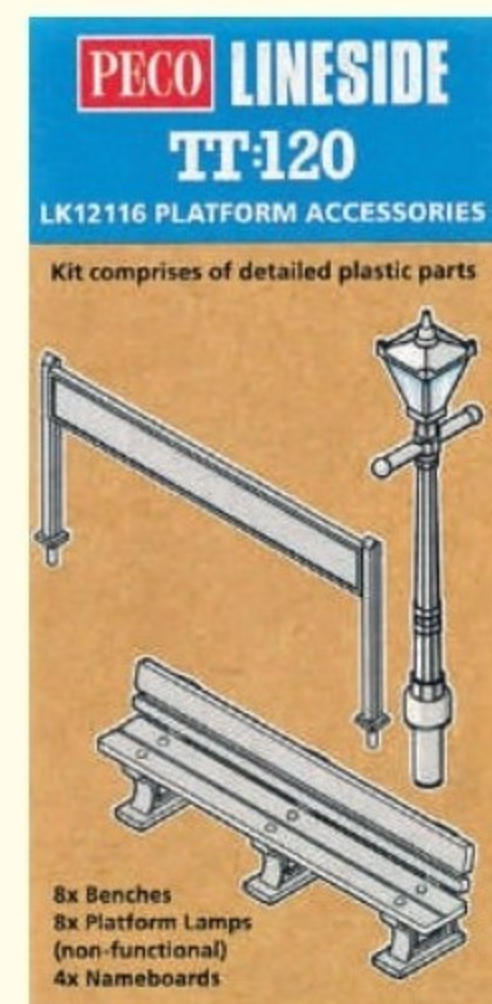


A full evaluation of these new products is planned for inclusion in the review pages of the August issue.

## New platform accessories in TT:120

New to the Peco Lineside range in TT:120 is a set of platform accessories, comprising eight benches, eight platform lamps (non-functional) and four nameboards. Comprising plastic moulded parts, a small amount of preparation and assembly is required, together with painting to suit.

Priced £9.95, the kit (ref.LK12116) is now obtainable from Peco stockists.



## New Peco Power-Lube Plus

New to the Peco Tools range is PT64 Power-Lube Plus Contact Oil, which is designed to help maintain electrical conductivity and also for the lubrication of model mechanisms.

Priced RRP £13.99, the specially formulated light oil is supplied in a 12ml pen-shaped applicator fitted with a 40mm fine-tipped nozzle.



## Peco N gauge bolsters price correction

There is a correction to be made to the price information given for the pair of new Peco N gauge wagons reviewed in the June issue of RAILWAY MODELLER.

The price of the Diagram 1/402 bolster wagons twin pack (ref.NR1151E), in which the vehicles are finished in NE grey livery representative of London & North Eastern Railway-built examples, is in fact £28.95, not the £22.50 quoted in the review.

The price information of £17.50 given for the other wagon reviewed, the BR Dia.1/004 pig iron wagon (ref.NR1100B), was however correct.

[www.peco-uk.com](http://www.peco-uk.com)

## See Peco at Buxton and Bluebell events

Peco will be attending two model railway events in June, where the model railway company will be showcasing new and forthcoming products that it is currently developing, together with promoting its range of publications including RAILWAY MODELLER and CONTINENTAL MODELLER magazines.

The first event is Railex Buxton on the weekend of 20 & 21 June, which is being held at Buxton Pavilion Gardens, St John's Road, Buxton, Derbyshire SK17 6BE. For further details about this exhibition, where there will be more than 30 layouts in action, see the diary listing in Societies & Clubs.

The second event is being held on the weekend of 27 & 28 June and is taking place at the Bluebell Railway in Sussex. With activities and displays spread across two of the heritage line's stations, combined with the opportunity to travel on the railway itself, the event seeks to offer a full and varied experience for visitors of all ages. Amongst the exhibits booked to attend are *Penworth* in N gauge (RM November 2025, pictured) and the *Ffarquhar Branch* by the Rev. Awdry (as first featured in RM December 1959). For further details, including opening times, admission prices and train times, visit:

[www.bluebell-railway.com](http://www.bluebell-railway.com)



# Coming next month

August 2026 –  
Out Thursday 9 July

Going outdoors in 00  
The Daws Heath Garden Railway



Little Ashton  
A Privatisation era depot in N



Nant Gwernol  
A tribute to the Talylyn in 009



plus all the  
regular features ...

**SAVE £1 ON NEXT MONTH'S RM!**  
Use the coupon printed on p34a  
of this issue to save £1 on the  
cover price of the August 2026  
edition of RAILWAY MODELLER, when  
purchased from a Peco stockist.

## Shop News

### New shop in Folkestone aims to be a meeting point for modellers



A new destination for modellers and hobbyists has opened in the heart of Folkestone in Kent, with Harbourcraft Model Shop welcoming customers at 29 Guildhall Street.

Seeking to be more than just a retail space, Harbourcraft is aiming to re-create the charm of the traditional model shop — a place where enthusiasts can not only shop but also connect. Visitors are invited to enjoy a tea or coffee, meet fellow modellers, and share ideas, techniques and inspiration

in a relaxed and friendly setting. The shop offers a wide range of model railway items, scenics, kits and accessories, catering to all experience levels. From locomotives and rolling stock to paints, tools, and materials suitable for scratch-building, Harbourcraft aims to provide everything needed to build and enhance layouts. Items for most gauges are available.

Harbourcraft also buys and sells pre-loved collections, giving customers the chance to discover

unique items or pass on treasured pieces to new homes.

With its focus on community, knowledge-sharing, and quality products, Harbourcraft is well on its way to becoming a hub for modelling enthusiasts across the Kent area and further afield.

Harbourcraft Model Shop is now open at 29 Guildhall Street, Folkestone, Kent CT20 1EB. For more information, call 07595 865452 or visit:

[www.harbourcraft.co.uk](http://www.harbourcraft.co.uk)

### Alan John Austin

*The team at RAILWAY MODELLER was sorry to hear news of the passing of EM gauge modeller and accessories manufacturer Alan Austin and would like to express our condolences to his widow and friends. We have received the following tribute from Paul Bason:*

Although many readers may have seen Alan's *Newton Gate* EM layout featuring many experimental methods of construction in RM November 2016, and a progress report illustrating his very ambitious *Low Dale and Calver* fine scale project in Plan of the Month RM January 2019, Alan was also known to many modellers for his extensive range of etched fine scale parts and accessories which he marketed as Ambis Engineering (Alan's Micro Based Information Service).

Alan was part of the team behind Ilford & West Essex Club's large EM layout that started out as *Sherdington* before evolving into *Eastwell*, a well-known show favourite for many years from the 1970s onwards.



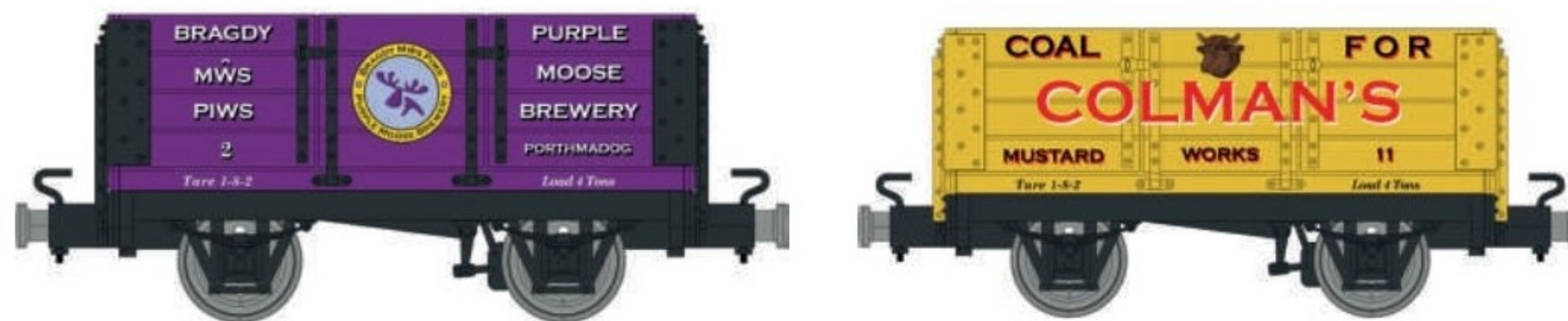
Alan later belonged to the Barking/East Ham clubs and was a founder member, newsletter writer/editor and exhibition organiser of the ELFS (East London Fine Scale group), an informal group of friends living as far afield as Hereford, Peterborough and Folkestone.

Having been diagnosed with Myeloma in 2009, Alan – a town planner by profession – underwent the then groundbreaking stem cell treatment in 2010 before moving from Ilford to Norfolk, where he had space to build a shed big enough for *Low Dale and Calver* and where he would fully immerse himself in the hobby as part of his recovery.

Much to Alan's annoyance, a string of medical setbacks in recent years prevented him from completing his dream layout, and he passed away peacefully in hospital in King's Lynn. He leaves a devoted wife, Maureen (Mo), who was always by his side during his illness and at shows too.

*Alan John Austin. 5 March 1948 – 18 March 2026*

## New 16mm scale ready-to-run wagons from Footplate



Footplate, in partnership with Dapol, has announced the launch of a brand-new range of 16mm narrow gauge ready-to-run rolling stock, developed and produced in the UK.

The range will debut with a series of Vale of Rheidol Railway-inspired open wagons, offered in five colour schemes, including plain bauxite, plain grey, Great Western dark grey and Private Owner Colman's Mustard and Purple Moose brewery liveries. Tooling for these wagons is already at an advanced stage, with the first items scheduled for release this summer.

Prices for the open wagons will start at £69.99, and the models will be available in both the 32mm common in 16mm scale modelling and 45mm gauge as used by G scale brands, ensuring compatibility with a wide range of garden railway set-ups.

Each wagon will feature insulated metal wheels and axles, along with a turned metal centre buffer and hook, complete with a spring-loaded mechanism for reliable operation.

Following the initial wagon release, the range will next include a flat wagon in a variety of liveries. As development

continues, other projected models include a round-top open wagon, a bolster wagon, a brake van, a box van, a passenger coach and additional items as the tooling suite progresses.

All of the models in the range will be available exclusively through Footplate, but the firms state that the partnership between Footplate and Dapol will also enable the latter to invest further in its UK-based toolroom capabilities.

For further details visit the retailer's website:

[www.footplate.co.uk](http://www.footplate.co.uk)

# Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling to a listed live event should contact the organiser first. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and notifications sent via email to [clubs@railwaymodeller.co.uk](mailto:clubs@railwaymodeller.co.uk) are required six weeks prior to publication date. Entries must include the organiser's email or website and/or a telephone number.

SEPTEMBER 2026 issue deadline: JULY 1st

## LIVE EVENTS

### Saturday 13 June

#### ASHFORD RAILEX ASHFORD, Kent

**Organiser:** Ashford Model Railway Club.  
**Venue:** The Swan Centre, Turner Close Newtown, Ashford, Kent TN24 0PQ.  
**Open:** 1000-1600  
**Admission:** adults £5.00, acpd U16s free.  
**Amenities:** free parking for all attendees, with additional local on-street parking. Venue is close to Ashford railway station. Refreshments available.  
**Features:** around 12 layouts in various gauges and scales. The Ashford club is presently undergoing a restructuring process, with a focus on enhancing its presence within the local community, so this year's show is being organised by a completely new exhibition management team. The venue's proximity to the renowned Ashford Railway Works in Newtown provides a fitting backdrop.  
**Contact:** 01233 647066.  
**Email:** [info@ashfordmrc.co.uk](mailto:info@ashfordmrc.co.uk)  
**Facebook:** [www.facebook.com/AshfordMRC](http://www.facebook.com/AshfordMRC)

### Saturday 13 June

#### BILLERICAY, Essex

**Organiser:** Billericay Model Railway Group.  
**Venue:** Hannakins Farm Community Centre, Rosebay Avenue, Billericay CM12 0XP.  
**Open:** 1000-1600  
**Admission:** adults £6.00, children £3.00, family tickets £14.00. Proceeds will go towards the model railway group's funds.  
**Amenities:** the Community Centre has free parking. Billericay railway station is close by and local buses also stop nearby. Light refreshments will be on sale throughout the day in the café.  
**Features:** 15 model railway exhibits in a range of scales, including: *Hursly Quay* (0); *Nonsuch* (0-16.5/NG7); *Bayou Scie* (On30); *Rixworth Green* (P4); *Abergwynfi*, *Billericay Quay* (club's own latest layout), *Jake's Yard*, *Shunting Puzzle*, *Test track* (00); *Farm Rise*, *Himalayan Adventure* (009); *Broken Creek* (HO); *Outwood Common*, *Wickford Southminster* (N). Traders offering a wide selection of pre-loved and nearly new models, accessories and spare parts.  
**Email:** [secretary@billericaymrg.org](mailto:secretary@billericaymrg.org)  
**Website:** [www.billericaymrg.org](http://www.billericaymrg.org)

### Saturday 13 June

#### BURTON-ON-TRENT, Staffordshire

**Organiser:** 7mm Narrow Gauge Association.  
**Venue:** Burton Town Hall, King Edward Place, Burton-upon-Trent DE14 2EB.  
**Open:** 1030-1630  
**Admission:** non-member adult £14.00, family £19.00; member adult £8.00, family £11.00. Accompanied U16s free.  
**Amenities:** venue has wheelchair access. Refreshments and bar available.  
**Features:** 7mm narrow gauge layouts including: 5.5mm Modular Group's *Afon Adit* (09); *Black Diamond Modular* (On30); *Dobbins Adit* (0-6.5); *Esgairgellig* (15.75mm gauge); *Glendale*; *Johannesdorf* (0e, 1:45); *Lostanbodge*, *Low Tide*, *Queens Wharf*, *St Oswalds* (0-16.5/NG7); *Megantic* (On2). Plus Association modelling goods and publications; demonstrations of tree construction using a 3D-printing pen and other landscape tips, scenic modelling and a variety of modelling skills; society stands, 12 specialist traders.

**Contact:** Arline Wilson using email below.  
**Email:** [exhibition@7mmnga.org.uk](mailto:exhibition@7mmnga.org.uk)  
**Website:** [www.7mmnga.org.uk/AGM.php](http://www.7mmnga.org.uk/AGM.php)

### Saturday 13 June

#### HEWORTH, York, North Yorkshire

**Event:** Joint Annual Open Day.  
**Organisers:** N Gauge Society Yorkshire Area Group & N Gauge Society WorldWide Group.  
**Venue:** Heworth Community Centre, 38 Applecroft Road, Heworth, York YO31 0HQ.  
**Open:** 1030-1600  
**Admission:** £5.00 per modeller or family group. U16s must be accompanied by an adult.  
**Amenities:** limited parking on site so visitors are requested to respect the centre's neighbours when parking in the local residential area. Local bus No.11 runs from York railway station to Ashley Park Bridge, just outside the Community Centre. Level access throughout. Refreshments available.  
**Features:** modular layouts from both N gauge groups featuring a great variety of styles and scenes from around the world. Extensive T-Trak and Freemo modular circuits featuring British, European, Japanese and North American scenes as well as fantasy scenes. Other layouts comprise *Mühlenfelder* and *Trifolou*. Plus club sales and trade stands from Mount Tabor Models, TrainTrax and Grandad's Trains. Event also a gathering for N Gauge Society members.  
**Contact:** John Brady, 07818 077663.  
**Email:** [wwg-membershipsec@outlook.com](mailto:wwg-membershipsec@outlook.com)  
**Website:** <https://yagngs.wixsite.com/yorkshire-area-group>  
[www.ngaugesociety.org/area-groups/worldwide-group](http://www.ngaugesociety.org/area-groups/worldwide-group)  
**Facebook:** [www.facebook.com/YorkshireNgaugSociety/](http://www.facebook.com/YorkshireNgaugSociety/)

### Saturday 13 June

#### HUCCLECOTE, Gloucester

**Organiser:** Hucclecote Methodist Church & Gloucester Model Railway Club.  
**Venue:** Hucclecote Methodist Church, Carisbrooke Road, Gloucester GL3 3QP.  
**Open:** 1000-1700  
**Admission:** adults £6.00, children free.  
**Amenities:** venue has a car park as well as there being free on-street parking nearby. Venue is also on the Stagecoach No.10 bus route from Lower Tuffley to Cheltenham via Gloucester. Venue has disabled access. Refreshments.  
**Features:** 14 working layouts in a variety of scales and gauges, plus traders, demonstrations and static displays.  
**Email:** [info@hucclecotemc.org.uk](mailto:info@hucclecotemc.org.uk)  
**Website:** [www.hmcmrs.org.uk](http://www.hmcmrs.org.uk)  
[www.gloucestermodelrailway.org](http://www.gloucestermodelrailway.org)  
**Facebook:** [www.facebook.com/groups/2112520075664900/](http://www.facebook.com/groups/2112520075664900/)

### Saturday 13 & Sunday 14 June

#### MERE, Wiltshire

**Organiser:** Mere & District Railway Modellers.  
**Venue:** Mere Lecture Halls, Salisbury Street, Mere BA12 6HA.  
**Open:** 1000-1600  
**Admission:** adults £7.00, acpd U16s free.  
**Amenities:** free parking in adjacent public car parks. Light refreshments available.  
**Features:** at least 12 layouts incl: *Crossways Wagon Works* (0 & 9mm); *Moorside*, *Wedmore* (0); *Cwm Caradoc* (EM); *Thomas Visits Bride Bay* (00); *Launceston*, *Somewhere in France* (009); *Summit Else*, *The Junction* (N); *Sylvasprings Watercress* (006). Club members' table, demonstration & trade stands.

**Email:** [paddy.newton@sky.com](mailto:paddy.newton@sky.com)  
**Website:** [www.mererailwaymodellers.co.uk](http://www.mererailwaymodellers.co.uk)

### Sunday 14 June

#### UFFCULME, Devon

**Organiser:** Culm Valley Model Railway Club.  
**Venue:** Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.  
**Open:** 1000-1600  
**Admission:** free but donations welcome.  
**Amenities:** garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.  
**Features:** 60 years of steam and diesel traction on *Tiverton Junction* (30' long 00 gauge model) plus *Soldor*, home to a little blue engine, driven by visitors. Club layouts.  
**Website:** [www.cvmrc.weebly.com/tiverton-junction.html](http://www.cvmrc.weebly.com/tiverton-junction.html)

### Tuesday 16 June

#### BEER, Devon

**Event:** Silver Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1200  
**Tour cost:** £40.00 per person.  
**Amenities:** free parking, with the tour starting point near the shop entrance. Note that visitors must be over the age of 10 and must wear closed toed shoes. This tour involves walking around the site, flights of stairs and standing for long periods of time.  
**Features:** after meeting a tour leader in the 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique two-hour behind-the-scenes experience as Peco opens the doors to its Devon factory. See where model railway track, wagons and accessories have been rolling off the production line for 80 years. See how raw materials are turned into finished products. You'll watch track components being assembled, learn about the machines used and see products being packaged, ready for shipping around the world.  
**Website:** [www.peco-uk.com](http://www.peco-uk.com)  
**Email:** [tina.bishop@peco.co](mailto:tina.bishop@peco.co)  
**Facebook:** [www.facebook.com/PecoModelRailways/](http://www.facebook.com/PecoModelRailways/)

### Tuesday 16 June

#### BEER, Devon

**Event:** Gold Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1500  
**Tour cost:** £65.00 per person or £120.00 for two people booking together.  
**Amenities:** free parking, with the tour starting point of the *Orion* Pullman car by the shop entrance. Visitors must be over the age of 10 and, as this is a working factory, they must wear closed toed shoes. The tour involves walking around the site, flights of stairs and standing for long periods.  
**Features:** after meeting a tour leader in the former 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique full-day behind-the-scenes experience as Peco opens the doors to its Devon factory. It begins with a two-hour tour, with participants seeing track components being assembled and learning about the machines used, then finished products being packaged ready for shipping to 35 countries around the world. Refreshments throughout the day, including a light lunch in *Orion*, followed by a 30-minute Q&A session with a Peco management team member. There will then be an opportunity to get 'hands-on' as tour members make their own souvenir limited edition 009 wagon. Finally, they can visit the Peco model railway exhibition and shop, with a 10% discount.  
**Website:** [www.peco-uk.com](http://www.peco-uk.com)  
**Email:** [tina.bishop@peco.co](mailto:tina.bishop@peco.co)  
**Facebook:** [www.facebook.com/PecoModelRailways/](http://www.facebook.com/PecoModelRailways/)

### Saturday 20 June

#### BANSTEAD, Surrey

**Organiser:** North Downs Model Railway Circle.  
**Venue:** 3rd Banstead Scout Group, Scout Ridge, Banstead, Surrey SM7 1RB.

**Open:** 1000-1700

**Admission:** adults £6.00, U16s £3.00, senior/disabled £5.00, families (2+2) £12.00. All profits to 3rd Banstead Scout Group.  
**Amenities:** free parking. Refreshments.  
**Features:** 21 layouts in several major scales plus traders.  
**Contact:** Stuart Robinson, 07758 458827.  
**Email:** [stuart.robinson27@ntlworld.com](mailto:stuart.robinson27@ntlworld.com)  
**Website:** [www.ndmrc.info/Home/Tadworth.htm](http://www.ndmrc.info/Home/Tadworth.htm)

### Saturday 20 June

#### EASTBOURNE, East Sussex

**Organiser:** Pevensey Bay Model Railway Club.  
**Venue:** Victoria Baptist Church Hall, Eldon Road, Eastbourne, East Sussex BN21 1UE.  
**Open:** 1000-1600  
**Admission:** adults £7.00, children (over 5) £1.00, family (2+2) £14.00.  
**Amenities:** the venue has a large on-site car park, wheelchair accessible entrance and toilets. Stagecoach bus service No.1A runs nearby on Eldon Road. Refreshments.  
**Features:** layouts in various gauges.  
**Email:** [PevenseyBayMRC@gmail.com](mailto:PevenseyBayMRC@gmail.com)  
**Website:** [www.pbmrc.club](http://www.pbmrc.club)

### Saturday 20 June

#### GAINSBOROUGH, Lincolnshire

**Organiser:** Gainsborough Model Rly Society.  
**Venue:** Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.  
**Open:** both days 1330-1730 (last admission 1630).  
**Admission:** adults £5.00, children £4.00, family (2+2) £15.00.  
**Amenities:** limited accessibility for disabled.  
**Features:** open day for a society which is this year celebrating its 80th anniversary, with its 0 gauge East Coast Main Line layout in operation. This offers a nostalgic opportunity to see how the LNER was run in the heyday of steam, with dozens of trains, formed from over 180 locomotives and over 400 items of rolling stock running on half a mile of track. Note there are also club nights open from 1930 on Mondays throughout the year.  
**Contact:** B Hodgkinson, 07943 182520.  
**Email:** [gmrcontact@gmail.com](mailto:gmrcontact@gmail.com)  
**Website:** [www.gainsboroughmodelrailway.co.uk](http://www.gainsboroughmodelrailway.co.uk)

### Saturday 20 June

#### NEWTOWNABBEY, Belfast, Northern Ireland

**Organiser:** Glenabbey Model Railway Club.  
**Venue:** Glenabbey Church, Ballycraig Road, Newtownabbey, Co. Antrim BT36 5ZZ.  
**Open:** 1000-1630  
**Admission:** adults £4.00, children £2.00, family ticket (2+2) £10.00.  
**Amenities:** venue features an accessible entrance and disabled toilets.  
**Features:** Glenabbey is celebrating 10 years of its model railway club with members' first exhibition. Layouts and other displays from all over Ireland.  
**Email:** [sjni@hotmail.co.uk](mailto:sjni@hotmail.co.uk)  
**Facebook:** [www.facebook.com/events/d41d8cd9/glenabbey-mrc-10th-anniversary-exhibition/26012959988327878/](http://www.facebook.com/events/d41d8cd9/glenabbey-mrc-10th-anniversary-exhibition/26012959988327878/)

### Saturday 20 June

#### PRESTATYN, Clwyd

**Organiser:** Rhyl & District Model Railway Club.  
**Venue:** Christ Church Parish Church and Community Hall, High Street, Prestatyn LL19 9AR.  
**Open:** 1030-1600  
**Admission:** adults £5.00, acpd U15s free. Note payment is by cash only and as well as raising funds for RDMRC, this year's event will support the work of Blood Bikes Wales.  
**Amenities:** up to 3 hours' free parking is available in Nant Hall Road Car Park, adjacent to the venue (see ticket machines for details), or up to 3 hours at Prestatyn Retail Park (5 minutes' walk away). Prestatyn railway station is within a 10-minute walk. Full disabled access.  
**Features:** at least 12 layouts in a variety of sizes, scales and gauges, plus demonstration and sales stands.  
**Website:** [www.rhyldistrictmodelrailwayclub.co.uk](http://www.rhyldistrictmodelrailwayclub.co.uk)  
**Facebook:** [www.facebook.com/p/Prestatyn-Model-Railway-Exhibition-61558151288517/](http://www.facebook.com/p/Prestatyn-Model-Railway-Exhibition-61558151288517/)

## Saturday 20 & Sunday 21 June

### RAILEX BUXTON 2026

#### BUXTON, Derbyshire

**Organiser:** Railex Buxton.

**Venue:** Buxton Pavilion Gardens, St John's Road, Buxton, Derbyshire SK17 6BE.

**Open:** Saturday 0930-1700  
Sunday 0930-1630

**Admission:** by variable entry periods/tickets priced from £7.50 (see website for details). Entry is valid for the day of purchase only and is non transferable. Children must remain under supervision at all times. Unaccompanied 13-18 year-olds can enter at the discretion of the door staff but no large groups are permitted. All over-13s and adults must wear a wristband supplied upon entry. Full colour A4 guidebook is available priced £2.50.

**Amenities:** on-site pay & display parking may be accessed from Burlington Road (£5.40 all day) or at nearby railway station and Palace Hotel. Disabled parking at venue is free. Buxton town centre has several car parks and free on-street parking. Buxton is well served by trains from Manchester and buses from towns bordering the Peak District. Note a few exhibits (around 5%) will be on a stage accessed by steps only. Unenclosed mobility scooters are permitted if used with care. Only registered therapy and guide dogs permitted. Do not take food or beverages into the exhibition areas. Venue also has an opera house, palm house, cafés, restaurant, shops, ice cream parlour, parks with boating lake, miniature railway and bandstand (in use on Sunday afternoons).

**Features:** 50% larger venue than previous years. Over 30 layouts including: *Farringdon Street* (7mm broad gauge); *Purgatory Peak* (On30); *Dent, Hampshire Hills, Leicester Junction, Minorities, New Street Goods, Pencadder, Rockport, Stumbleton Castle, Westmoor Junction* (OO); *Glenties* (OO3); *Cherry Willingham* (3mm/TT); *Chipping Norton* (2mm Finescale); *Carlisle, Evercreech Junction, Thelwall, Nowhere In Particular* (N). Demonstrators, trade & society stands.

**Email:** mbhenshaw1967@gmail.com

**Website:** www.railexbuxton.co.uk

**Facebook:** www.facebook.com/p/Railex-Buxton-100076146964942/

## Saturday 20 & Sunday 21 June

### EXBURY, Hythe, Hampshire

**Organisers:** Exbury Gardens & Steam Railway & New Forest Model Railway Society.

**Venue:** Exbury Gardens & Steam Railway, Summer Lane, Exbury, Southampton SO45 1AZ.

**Open:** both days 1000-1730

**Admission:** entry to exhibition free with membership or gardens admission: adults £18.00 or £19.80 with Gift Aid; children (3-15) £7.00 or £7.70 with Gift Aid; U3s free; family £47.50 or £52.25 with Gift Aid; Universal Credit/Benefit holder £1.00 but must show documentation listed on website. Dads enjoy half-price admission on Sunday. 12¼" gauge steam train rides £7.50 per person or £28.50 for family (2+2) – children must be accompanied on trains.

**Amenities:** Exbury Gardens & Steam Railway are approx. 20 mins from M27 Jct. 2 & 3 by following brown tourist signs on the A326. Very large free car park. Closest railway station is Brockenhurst, approx 10 miles' taxi ride from Exbury. Parking bays for blue badge holders are nearest to the visitor entrance and the steam railway has four carriages adapted for wheelchairs. Accessible toilet facilities. Dogs allowed if kept on a short lead. Catering available.

**Features:** model railways owned by members of the New Forest Model Railway Society, plus various working steam engines, all displayed within working railway shed. Enthusiasts will be on hand to give hints and tips on building model railways at home, with layouts sized from 30' long to those running on a windowsill. Visitors can also explore 200 acres of gardens, with visiting working miniature traction engines running on pathways, and ride on the 1½ mile, 12¼" gauge steam railway.

**Contact:** 023 8089 1203.

**Website:** www.exbury.co.uk

**Facebook:** www.facebook.com/exburygardens/

## Saturday 27 June

### COVRAIL26

#### RYTON-ON-DUNSMORE, Warwickshire

**Organiser:** Thunderbolt Models & Model Railway Events Ltd.

**Venue:** Sports Connexion, Leamington Road, Ryton-On-Dunsmore, Coventry CV8 3FL (What3words burglars.cherubs.truffles).

**Open:** 1030-1630

**Admission:** adults – early online booking £7.00 (via early bird booking button on website), on the day tickets £8.00; students aged 8-17 £2.00; veterans & serving armed forces personnel with ID free. Proceeds go to charity Help for Heroes.

**Amenities:** for road directions to venue see <https://api.warwickshire.gov.uk/documents/WCCC-764-191> Free parking but limited designated disabled parking. There is a bus stop outside on the Coventry to Rugby bus route (for details use QR code on website). Venue has good access & air conditioning. Support animals welcome. Food & refreshments.

**Features:** 48 layouts, including: *Roche Junction, Wuppertal* (G); *Cook End, Deeping Road MPD, Schnapsidee, Why O Y* (O); *Test Track* (O & OO); *Diorama* (O, OO & N); *Bryncrug Sidings* (O-16.5/NG7); *Snowy River Railroad* (On30); *Arley SVR, Bottom of the Barrel, Brunswick East, Carlisle Canal, Ebbols Lane TMD, Giffard Lane, Kimbolton, Lymington, Project New Street, Much Wittering, The Henley in Arden Line, Three Spires TMD* (OO); *Harry's Heritage Railway* (OO & OO9); *Mynydd Tatws, Quarry End, Smallbridge Junction* (OO9); *Rear Echelon, Trenchard Ridge, Trenchard Support Trench, Trenchard the Fallen* (OO9 micro layouts); *Big Pine, Central Alonzo, Westhampton* (HO); *Small Town USA* (HO3); *Daventry North, East Ansty, East Hamilton, Inglenook Sidings, Littleton Bridge, Not Littlefield, Oakgrove Central, Suitcase Layout 1, Suitcase Layout 2, Two Locks, Two Seasons, Winklesea Central, Ysuru no Michi* (N). Plus demonstrations and many trade & society stands.

**Contact:** Thunderbolt Models 07851 731629.

**Email:** eventsmanager@modelrailwayevents.com

**Website:** www.modelrailwayevents.com

**Facebook:** www.facebook.com/photo/?fbid=1427997996041528&set=pcb.913373181520455

## Saturday 27 June

### STEVENTON, Oxfordshire

**Organiser:** Oxfordshire Narrow Gauge Modellers.

**Venue:** Steventon Village Hall, The Green, Steventon OX13 6RR.

**Open:** 1000-1600

**Admission:** adults £7.00, acpd children free.

**Amenities:** venue is 5 minutes by car from the A34 and has free parking and step-free access. It is also 15 minutes by bus from Didcot Parkway railway station. Refreshments.

**Features:** 13 narrow gauge layouts plus 4 trade stands, incl. the OO9 Society sales stand and demonstrations. For details see website.

**Contact:** Tim Crockford via email address.

**Email:** ng.steventon@gmail.com

**Website:** www.oxngm.org.uk

**Facebook:** www.facebook.com/groups/379756891360987/

## Saturday 27 & Sunday 28 June

### PERTH, Perth & Kinross

**Organiser:** Perth Model Railway Group.

**Venue:** Dewars Centre, Glover Street, Perth PH2 0TH.

**Open:** Saturday 1000-1700  
Sunday 1000-1630

**Admission:** adults £10.00, children £3.00, adult weekend £12.00. Advance tickets available via website allowing 0930 entry.

**Amenities:** on-site parking. Venue is adjacent to Perth station and close to the bus station. Park & ride at Perth Broxden which is also a hub for buses connecting the major Scottish cities. Catering and bar.

**Features:** 32 layouts in many scales and gauges including club layout *Newton Ferrier* running for the first time in the pre-Nationalisation era featuring LMS and LNER stock. 15 trade stands, demonstrations and specialist societies.

**Contact:** 07588 754959.

**Email:** stanmoug@gmail.com

**Website:** www.smet.org.uk/show/

## Saturday 27 & Sunday 28 June

### PINCHBECK, Spalding, Lincolnshire

**Organiser:** Spalding Model Railway Club.

**Venue:** 1st floor, Pinchbeck Village Hall, Knight Street, Pinchbeck, Spalding, Lincolnshire PE11 3XA.

**Open:** 1000-1600 both days.

**Admission:** free.

**Amenities:** free parking. Light refreshments.

**Features:** club and members' layouts including *St Jude's Bay* (OO & OO9, RM June 2026. Club sales stand. Bring your own stock.

**Website:**

www.spaldingmodelrailwayclub.org.uk

## Sunday 28 June

### ELMSWELL, Suffolk

**Organiser:** Stowmarket Model Railway Club.

**Venue:** Blackbourne Community Centre, Elmswell, Suffolk IP30 9UH. (What3words: ///minerals.relieves.active).

**Open:** 1030-1600

**Admission:** £6.00 per person, acpd U12s free.

**Amenities:** free parking. Refreshments.

**Features:** 16-plus layouts as well as trade stands and information table.

**Email:** exhibitions@stowmarketmrc.org

**Website:** www.stowmarketmrc.org

**Facebook:** www.facebook.com/groups/stowmarketmodelrailwayclub/

## Sunday 28 June

### HEATH PARK, Cardiff

**Organiser:** Cardiff Model Engineering Society.

**Venue:** Heath Park Miniature Railway, King George V Drive East, Cardiff CF14 4AW.

**Open:** 1300-1700.

**Admission:** £2.80 (card payment only), children aged 3 & under free (must be accompanied on rides). No dogs except service dogs.

**Amenities:** refreshments, gift shop.

**Features:** miniature railways (steam and electric trams), model and garden railways.

**Website:** www.heathparkminiaturerailway.co.uk

## Sunday 28 June

### UFFCULME, Devon

**Organiser:** Culm Valley Model Railway Club.

**Venue:** Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.

**Open:** 1000-1600

**Admission:** free but donations welcome.

**Amenities:** garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.

**Features:** 60 years of steam and diesel traction on *Tiverton Junction* (30' long OO gauge model) plus *Soldor*, home to a little blue engine, driven by visitors. Club layouts.

**Website:** www.cvmrc.weebly.com/

tiverton-junction.html

## Saturday 4 July

### BETHERSDEN, Kent

**Organiser:** Ashford International Model Railway Education Centre.

**Venue:** Unit 2 Mill Farm Business Units, Fridd Lane, Bethersden, Kent TN26 3DB.

**Open:** 1030-1600

**Admission:** adults £10.00, under-14s £5.00, family (2+2 or more) £25.00. 20% discount for Gresley Society members.

**Amenities:** step-free access and accessible toilets. Shop and tea room.

**Features:** OO layout *The Gresley Beat* in operation to celebrate 150 years since the birth of Sir Nigel Gresley, with locomotives of his design carrying 'Gresley 150' headboards.

**Website:** www.aimrec.co.uk

## Saturday 4 July

### SCARBOROUGH, North Yorkshire

**Organiser:** Richard Pratt.

**Venue:** Newby & Scalby Hall, Scalby Road, Scarborough YO13 0RA.

**Open:** 1000-1600

**Admission:** adults £5.00, children £1.00, family £10.00.

**Amenities:** parking, disabled toilets.

**Features:** 10-plus layouts, trade support.

**Contact:** 07817 865538.

**Email:** eastcoastmodelshow@gmail.com

**Facebook:** www.facebook.com/profile.php?id=61583342095806

## Saturday 4 July

### TEWKESBURY, Gloucestershire

**Organiser:** Mr Matthew and Michael Wathen.

**Venue:** Tewkesbury Baptist Church, Station Street, Tewkesbury GL20 5DR.

**Open:** 1000-1700

**Admission:** adults £6.00, children £1.00. Note: cash only – event is benefiting The Tewkesbury Baptist Church.

**Amenities:** free limited parking on site, with Pay & Display sites within a 5-min walk from the venue. Disabled access. Refreshments.

**Features:** layouts including: *Boskett Metals Recycling, Somerset Lane* (O); *Tony's Forest* (On14); *Alstone Servicing Depot, Binegar, Kinlet Road Shed, Little Rocks, Melton Park Crossing, Penmaenbach, Portishead TMD* (OO); *Puffin Colliery* (OO/OO9); *Penorwic Slate Quarry* (OO9); plus OO9 work in progress layout by Steve Mann, TT:120 layout by Chris Cole and Lego Railway by Scott Bachelor. Miniature figure painting demonstration by Harvey Faulkner-Aston.

**Contact:** 07855 503709.

**Email:** matchwad@hotmail.com

**Facebook:** www.facebook.com/groups/778054087144055

## Saturday 4 July

### WEST DRAYTON, London

**Organiser:** Hillingdon Railway Modellers.

**Venue:** Yiewsley Baptist Church, 74 Colham Avenue, West Drayton, London UB7 8HF.

**Open:** 1100-1500

**Admission:** free.

**Amenities:** street parking (venue inside Low Emission Zone), 15 minutes' walk from West Drayton station on the Elizabeth Line. Tea, coffee and cake available.

**Features:** club open day featuring award winning exhibition layout *Grindley Brook* and two other layouts under construction.

**Website:**

www.hillingdonrailwaymodellers.co.uk

## Saturday 4 & Sunday 5 July

### HUNSLET, Leeds, West Yorkshire

**Organiser:** Middleton Railway Trust.

**Venue:** Middleton Railway, The Station, Moor Road, Hunslet, Leeds LS10 2JQ.

**Open:** both days 1000-1600 (steam passenger trains operating 1030-1530).

**Admission:** adults £9.00, children £4.00, family £22.00 (covers model exhibition plus unlimited train rides). All proceeds go to the Middleton Railway Trust.

**Amenities:** free parking on site or in nearby streets. Except for the footplate experience (see details below) all areas of the exhibition and the train ride are accessible to visitors with disabilities. Light refreshments are available in the shop/café. For family members who are non-modellers or who have exhausted the facilities at Moor Road station, Middleton Woods and Park are attractions at the top of the line, the Tulip Retail Park is opposite, and the White Rose Shopping Centre is a short distance away.

**Features:** 16 layouts in a variety of scales displayed amongst the real thing in the railway Engine House, including: *Bishopgate, Queensbury Depot* (O); *Leeds Trams, Oakdale, Stanley Ferry* (OO); *Bryn Oriog* (OO9). As well as other layouts in O-16.5, there will be Lego, vintage Tri-ang and Tri-ang Hornby set-ups operating, plus several trade stands. There will also be the opportunity to drive a full-size locomotive for a suggested donation of £5.00 (no need to book in advance – participants must be aged 18 to 76).

**Contact:** Exhibition Manager Ian Dobson on 01274 613687 (evenings only please).

**Email:** middletonmre@middletonrailway.org.uk

**Website:** www.middletonrailway.org.uk

**Facebook:** www.facebook.com/MiddletonRailwayOfficial/

## Saturday 4 & Sunday 5 July

### BeyondSTEAM – A CELEBRATION OF CONTEMPORARY RAILWAY MODELLING SWINDON, Wiltshire

**Organiser:** Beyond Exhibitions.

**Venue:** Great Western Hall, STEAM – The Museum of the GWR, Swindon SN2 2EY.

**Open:** 1000-1630 both days.

**Admission:** adults £10.00, acpd children free. Visitors entitled to 10% off entry to STEAM.

**Amenities:** by road, from M4 Jct.16 follow brown signs for 'Designer Outlet' and 'M' (for museum). Car parking is available at the Swindon Designer Outlet and it is recommended to use the North Car Park – postcode SN2 2AQ. If arriving by rail, from Swindon railway station, turn right and follow the pedestrian path until you come to the GWR tunnel on the right and follow the signpost for STEAM. Swindon Stagecoach bus Nos.1, 1A, 5, 10, 19, 22, 53, 54 & 55 stop close to STEAM. Venue has disabled access & a café.

**Features:** layouts set in the era 1968-2026, including: *Cardiff Canton*, *Charwelton into the 80s*, *Deadmans Lane*, *Ditchat Junction*, *Fareford East* (4mm scale); *Beijiao* (HO); *Llanwyddyn* (TT:120); *Bristol Road*, *Newvaddon Parkway* (2mm scale). Trade & society stands.

**Email:** chris@beyondsteam.com  
**Website:** www.beyondsteam.com  
**Facebook:** www.facebook.com/steammuseum/mentions/

### Sunday 5 July

#### WARLEY AT HOME – SUMMER OPEN DAY OLDBURY, West Midlands

**Organiser:** Warley Model Railway Club.  
**Venue:** The Clubrooms, Unit 1F Pearsall Drive, Oldbury, West Midlands B69 2RA.  
**Open:** 1000-1600  
**Admission:** free but donations welcome.  
**Amenities:** light refreshments available.  
**Features:** annual summer open day, featuring working layouts in many popular scales, including some members' layouts, layouts under construction, the club library, demonstrations, a members' bring and buy stand.  
**Email:** info@warleymrc.co.uk  
**Website:** www.warleymrc.co.uk  
**Facebook:** www.facebook.com/warleyatstatfold/

### Saturday 11 July

#### BETHERSDEN, Kent

**Organiser:** Ashford International Model Railway Education Centre.  
**Venue:** Unit 2 Mill Farm Business Units, Fridd Lane, Bethersden, Kent TN26 3DB.  
**Open:** 1030-1600  
**Admission:** adults £10.00, under-14s £5.00, family (2+2 or more) £25.00. 20% discount for Gresley Society members.  
**Amenities:** step-free access and accessible toilets. Shop and tea room.  
**Features:** OO layout *The Gresley Beat* in operation to celebrate 150 years since the birth of Sir Nigel Gresley, with locomotives of his design carrying 'Gresley 150' headboards.  
**Website:** www.aimrec.co.uk

### Saturday 11 July

#### CONWY, North Wales

**Organiser:** The North Wales Small Scale Model Railway Club.  
**Venue:** St Mary's Church Hall (also known as Neuadd Ni), Rose Hill St, Conwy LL32 8LD.  
**Open:** 1000-1600  
**Admission:** adults £5.00, acpd U16s free, family (2+2) £10.00.  
**Amenities:** light refreshments available.  
**Features:** exhibition of around 10 layouts.  
**Contact:** 07796 640131.  
**Email:** oakleygrange@hotmail.co.uk

### Saturday 11 July

#### CRUMPSALL, Manchester

**Organiser:** Heywood Model Railway Group.  
**Venue:** Crumpsall Labour Club, 98 Wilton Road, Crumpsall, Manchester M8 6PX.  
**Open:** 1000-1600  
**Admission:** £3.00 per adult, acpd children free. Donations welcome.  
**Amenities:** parking on local streets should not be problematical, but unfortunately disabled access to the group's new premises is very limited. Refreshments available.  
**Features:** club's first open day at its new premises following a relocation from Heywood announced in the May RM Societies & Clubs pages. In addition to the club test tracks in N,

009, 00 and 0 gauge, on which visitors can run their own locos, there are plans to invite guest layouts to complement members' own. Shop.  
**Contact:** Phil Evans by emailing: phil\_evans@sky.com  
**Website:** https://www.hmrg.net/

### Saturday 11 July

#### HILDENBOROUGH, Kent

**Organiser:** St John's Church, Hildenborough, in conjunction with Southwark & District Model Railway Club and Tonbridge MRC.  
**Venue:** St John's Church & Centre, 194 Tonbridge Road, Hildenborough, Tonbridge TN11 9HR.  
**Open:** 1000-1500  
**Admission:** free entry.  
**Amenities:** free off-site parking. Wheelchair access, refreshments.  
**Features:** layouts including: *Sewage Works* (G); 0 gauge tinsplate; *Grately*, *Solomon's Dock* (O-16.5); *Bulldog Quay* (EM); *Addleford Green* (OO); *Westhay* (OO9); *Smarden Maltings* (N). Meccano, demonstration stand & trade stands.  
**Contact:** Valerie Marsh, 07957 287866.  
**Email:** valeriemarsh1947@gmail.com

### Saturday 11 July

#### PRINCES RISBOROUGH, Buckinghamshire

**Organiser:** Risborough & District MRC.  
**Venue:** St Mary's Hall, Church Street, Princes Risborough HP27 0AF.  
**Open:** 1000-1600  
**Admission:** free.  
**Amenities:** disabled access. Light refreshments available.  
**Features:** club layouts, trade support and demonstrations.  
**Email:** secretary@rdmrc.org.uk  
**Website:** www.rdmrc.org.uk  
**Facebook:** www.facebook.com/profile.php?id=100068939964862

### Saturday 11 & Sunday 12 July

#### EXETER, Devon

**Organiser:** Exe Model Railway Society.  
**Venue:** Matford Livestock Centre, Matford Park Road, Marsh Barton, Exeter EX2 8FD.  
**Open:** Saturday 1000-1700  
Sunday 1000-1600  
**Admission:** adults £10.00, acpd children free.  
**Amenities:** venue is close to Matford Park & Ride, which is well signposted from nearby roads. The Livestock Centre itself has free parking and is a level venue with disabled access. Restaurant and café.  
**Features:** over 40 layouts, including: *Chard Junction*, *The John Pentney*, *Upton-Downton* (O); *Nancarrow Junction* (S); *Bristol Avon Bridge* (featured in November 2023 RM), *Coaley Junction*, *Ditchat Junction*, *Kingdoms Junction*, *Otterford*, *Shobrooke Road* (June 2024 RM), *West Harkenback*, *Whitehall Halt* (OO); *Dartmoor 009 Group Layout*, *Tan y Bwlch Station* (October 2025 RM), *The Rocks*, (OO9); *Bahnbetriebswerk Dugelshausen*, *Bitterfeld*, *Port Dominic & Nucor Steel* (HO); *Klein Albula* (HOM); *Wingrove Tramway* (HOe); *Great Western Brick Railway* (Lego). SWOONS display: *Bleat*, *Bowmoors Cider*, *Foxcombe Colliery*, *Tan-y-Bryn*, *Wild Goose & Riddon* (OO9), *New Florence Mine Tramway*, *Shillingstone Light Railway* (7mm NG). Demonstrations by Exeter O Gauge Group, Exeter N Gauge, The Exmoor Weatherman and EXGOG as well as up to 15 trade stands.  
**Contact:** 07775 765716.  
**Email:** train@exemrs.co.uk  
**Website:** www.exemrs.co.uk  
**Facebook:** www.facebook.com/exemrs

### Saturday 11 & Sunday 12 July

#### PENARTH, Vale of Glamorgan

**Organiser:** Barry & Penarth Model Railway Club.  
**Venue:** St Cyres School, Sully Road, Penarth, Vale of Glamorgan CF64 2TP.  
**Open:** Saturday 1000-1630  
Sunday 1000-1600  
**Admission:** adults £8.00, acpd U16s free.  
**Amenities:** ample free parking. Free heritage bus shuttle from Cogan railway station, served by Transport for Wales Cardiff to Barry trains. Venue has disabled access throughout. Refreshments available.  
**Features:** 15-plus layouts in various scales, plus around 10 traders.

**Email:** exhibitions@bpmrc.club  
**Website:** www.bpmrc.club  
**Facebook:** www.facebook.com/BarryandPenarthModelRailwayClub/

### Sunday 12 July

#### PETT, East Sussex

**Organiser:** East Sussex Gauge O Group.  
**Venue:** Village Hall, Pett, near Hastings TN35 4HB.  
**Open:** 1000-1600  
**Admission:** adults £4.00, acpd children free.  
**Amenities:** free parking, wheelchair access and refreshments.  
**Features:** open day with around five layouts, a continuous running track, modelling demonstration and pre-used items sales stand.  
**Email:** mikeh.reeves@tiscali.co.uk  
**Website:** www.gaugeoguild.com/clubs/clubDetails.aspx?id=1011

### Sunday 12 July

#### UFFCULME, Devon

**Organiser:** Culm Valley Model Railway Club.  
**Venue:** Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.  
**Open:** 1000-1600  
**Admission:** free but donations welcome.  
**Amenities:** garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.  
**Features:** 60 years of steam and diesel traction on *Tiverton Junction* (30' long OO gauge model) plus *Soldar*, home to a little blue engine, driven by visitors. Club layouts.  
**Website:** www.cvmrc.weebly.com/tiverton-junction.html

### Thursday 16 July

#### BEER, Devon

**Event:** Silver Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1200  
**Tour cost:** £40.00 per person.  
**Amenities:** free parking, with the tour starting point near the shop entrance. Note that visitors must be over the age of 10 and must wear closed toed shoes. This tour involves walking around the site, flights of stairs and standing for long periods of time.  
**Features:** after meeting a tour leader in the 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique two-hour behind-the-scenes experience as Peco opens the doors to its Devon factory. See where model railway track, wagons and accessories have been rolling off the production line for 80 years. See how raw materials are turned into finished products. Watch track components being assembled, learn about the machines used and see products being packaged for shipping around the world.  
**Website:** www.peco-uk.com  
**Email:** tina.bishop@peco.co  
**Facebook:** www.facebook.com/PecoModelRailways/

### Thursday 16 July

#### BEER, Devon

**Event:** Gold Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1500  
**Tour cost:** £65.00 per person or £120.00 for two people booking together.  
**Amenities:** free parking, with the tour starting point of the *Orion* Pullman car located by the shop entrance. Visitors must be over the age of 10 and, as this is a working factory, must wear closed toed shoes. This tour involves walking around the site, flights of stairs and standing for long periods.  
**Features:** after meeting a tour leader in the former 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique full-day behind-the-scenes experience as Peco opens the doors to its Devon factory. It begins with a two-hour tour, with participants seeing track components being assembled and learning about the machines used, then finished products being packaged ready for shipping to 35 countries around the world. Refreshments throughout the day, including a light lunch in *Orion*, fol-

lowed by a 30-minute Q&A session with a Peco management team member. There will then be an opportunity to get 'hands-on' as tour members make their own souvenir limited edition 009 wagon. Finally, they can visit the Peco model railway exhibition and shop, with a 10% discount.  
**Website:** www.peco-uk.com  
**Email:** tina.bishop@peco.co  
**Facebook:** www.facebook.com/PecoModelRailways/

### Friday 17, Saturday 18 & Sunday 19 July

#### REV W AWDRY'S LITTLE ENGINES WEEKEND TYWYN, Gwynedd

**Organiser:** Tallylyn Railway.  
**Venue:** Wharf Station, Tywyn, Gwynedd, LL36 9EY & Magic Lantern Cinema, 6a Corbet Square, Tywyn LL36 9DF.  
**Open:** check Tallylyn Railway event web pages for details on timetables, event timings and how to book.  
**Admission:** train ride rover tickets: adults £29.00, children £5.00, TR Preservation Society members free. Includes Fan Tent. The Exhibition entry (booking by 25-min time slot): adults £10.00, children £7.50. Evening Entertainment at Magic Lantern Cinema (Friday 17 & Saturday 18 July only): adults £25.00, children £15.00. Special extra fares apply to 'Fred & Rusty's Special Train' (Friday 17 July only), 'The Sudrian' (two departures each on Saturday 18 & Sunday 19 July) and 'Rusty's Line Inspection' (one departure each on Saturday 18 & Sunday 19 July) – see event web page for details.  
**Amenities:** Wharf Station is on the A493 from Machynlleth to Dolgellau and, if driving, car parking is available across the bridge over the main line railway (it is a council-owned car park so check charges). If travelling by train, Wharf station is approximately 300 yards' walk from Tywyn Cambrian Coast Line station, served by Transport for Wales trains from Shrewsbury via Machynlleth and Aberdyfi to Barmouth, Porthmadog and Pwllheli – leave the station via Platform 1 and car park, turn right and continue up the main road. For bus routes from Dolgellau, etc. see 'how to find us' section of the Tallylyn Railway website, which also has an accessibility section. Tywyn Wharf and Abergynolwyn TR stations both have cafés.  
**Features:** weekend event celebrating the life and legacy of Thomas the Tank Engine creator & Tallylyn Railway volunteer the Reverend Wilbert Awdry. Though on a smaller scale than previous Awdry Extravaganza events, it will provide a community-focused meeting place offering Railway Series stories fans young and old various activities to enjoy, including a Fan Tent at Tywyn Wharf station containing Awdry-themed model displays and a multi-gauge test track on which to exhibit and run model engines. Also at Wharf, with viewings by booked time slots, will be a Rev. Awdry Exhibition showcasing models, paperwork and artifacts relating to the long history of the author's works across the years. Wilbert Awdry's own OO gauge *Ffarquhar Branch* layout will also be on display and in full operation. On Friday and Saturday only, there will also be an Evening Entertainment at the Magic Lantern Cinema in Tywyn, including the first in-person reading of the 'Narrow Gauge Rails in Sodor' lecture and remastered versions of the Rev. Awdry's narrations of some stories, including 'Sir Handel – in live action!' on the big screen. On Friday only, organisers will also reveal a 'special something' which has been teased in a video featuring some of Wilbert Awdry's surviving models and which can be viewed at: https://www.youtube.com/watch?v=SQxWL-jmP6NE  
There will be an intensive service of trains including a special guided 'Sudrian' train hauled by *Sir Handel* and showcasing locations on the TR which inspired Railway Series stories. Also 'Rusty's Line Inspection', 'The Picnic' and 'Fred and Rusty's Special Train'.  
**Contact:** 01654 710472.  
**Email:** enquiries@tallylyn.co.uk  
**Website:** www.tallylyn.co.uk/events/little-engines-weekend  
**Facebook:** www.facebook.com/Tallylyn

### Saturday 18 July BETHERSDEN, Kent

**Organiser:** Ashford International Model Railway Education Centre.  
**Venue:** Unit 2 Mill Farm Business Units, Fridd Lane, Bethersden, Kent TN26 3DB.  
**Open:** 1030-1600  
**Admission:** adults £10.00, under-14s £5.00, family (2+2 or more) £25.00. 20% discount for Gresley Society members.  
**Amenities:** step-free access and accessible toilets. Shop and tea room.  
**Features:** OO layout *The Gresley Beat* in operation to celebrate 150 years since the birth of Sir Nigel Gresley, with locomotives of his design carrying 'Gresley 150' headboards.  
**Website:** www.aimrec.co.uk

### Saturday 18 July FILEY, North Yorkshire

**Organisers:** Christopher Martin & Scarborough Area Model Trains.  
**Venue:** St John's Parish Church, West Avenue, Filey YO14 9AU.  
**Open:** 1000-1630  
**Admission:** by donation (suggestion of £4.00 each – with Gift Aid if possible – as event is raising funds for the continued development of St John's Parish Centre). U14s can enter free of charge.  
**Amenities:** limited parking. Refreshments.  
**Features:** 10 layouts plus trade stands & model shop.  
**Contact:** 01723 870166.  
**Website:** www.fileyparish.org.uk/what-s-on

### Saturday 18 July NORTHAMPTON, Northamptonshire

**Organiser:** Northampton & District MRC.  
**Venue:** Roade Village Hall, Bailey Brook Lane, Roade, Northampton NN7 2LS.  
**Open:** 1000-1600  
**Admission:** adults £6.00, acpd children free.  
**Amenities:** venue has free parking and is on bus route Nos.X6 & 33 from Northampton to Milton Keynes. Disabled access.  
**Features:** model exhibition being held in a railway village location on the West Coast Main Line. Layouts in several gauges with all new to this particular show. Also trade support.  
**Contact:** Secretary Richard Deacon on 01604 890275 or use contact form on website.  
**Website:** https://ndmrc.org  
**Facebook:** www.facebook.com/groups/165670697446426/

### Saturday 18 & Sunday 19 July BEER HEIGHTS LIGHT RAILWAY GALA BEER, Devon

**Organiser:** Peco Model Railways.  
**Venue:** Pecorama, Underleys, Beer, Seaton, Devon EX12 3NA.  
**Open:** both days 1000-1700  
**Admission:** advance tickets: adults £27.50, U3s free, dogs welcome at £2.00 per pet. Card payments only – book via www.pecorama.co.uk  
**Amenities:** free parking, and ticket also includes access to the site's gardens with fine views over Beer village, Lyme Bay and the Jurassic Coast World Heritage Site, as well as an opportunity to play mini golf. Note site is steep in places. Model railway exhibition, model shop and former 'Golden Arrow' Pullman car *Orion* on site. Refreshments available.  
**Features:** for railway enthusiasts everywhere: with Pecorama temporarily closed for the 2026 tourism season, this is one of three gala open events where tickets include rides on the 7½" gauge BHLR, which with its mile-long, fully signalled route through the site's Mediterranean-style gardens is one of the finest lines of that gauge in the UK. Tickets also include entry to the garden walks, with their panoramic views, mini golf course, *Orion* Pullman car and Pecorama model railway exhibition with its many layouts in a variety of scales and gauges showing how model railways can be set up in home and garden settings. The event ticket also allows a 10% discount on purchases from the well-stocked on-site model shop.  
**Contact:** 01297 21542.  
**Website:** www.pecorama.co.uk/blogs/events/heights-light-railway-gala-may-half-term  
**Facebook:** www.facebook.com/pecorama/?locale=en\_GB

### Saturday 18 & Sunday 19 July GOATHLAND, North Yorkshire

**Organiser:** Whitby & District Model Railway Club.  
**Venue:** Goathland Village Hall, The Common, Goathland, Whitby YO22 5JS.  
**Open:** 1000-1600  
**Admission:** adults £5.00, acpd U16s free.  
**Amenities:** there is free parking at the venue. It is also a short walk from the Coastliner No.840 bus stop and the North Yorkshire Moors Railway station.  
**Features:** six layouts, in O, EM, HO and N gauges. Plus a display of collectable toys and modelling demonstrations. See club Facebook page for full details.  
**Email:** wdmrc@outlook.com  
**Facebook:** www.facebook.com/WhitbyMRC/

### Sunday 19 July GAINSBOROUGH, Lincolnshire

**Organiser:** Gainsborough Model Rly Society.  
**Venue:** Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.  
**Open:** both days 1330-1730 (last admission 1630).  
**Admission:** adults £5.00, children £4.00, family (2+2) £15.00.  
**Amenities:** limited accessibility for disabled.  
**Features:** open day for a society which is this year celebrating its 80th anniversary, with its O gauge East Coast Main Line layout in operation. This offers a nostalgic, historical and colourful opportunity to see how the LNER was run in the heyday of steam, with dozens of trains, formed from over 180 locomotives and over 400 items of rolling stock running on half a mile of track. Note there are also club nights open from 1930 on Mondays throughout the year.  
**Contact:** B Hodgkinson, 07943 182520.  
**Email:** gmrscontact@gmail.com  
**Website:** www.gainsboroughmodelrailway.co.uk

### Sunday 19 July HEATH PARK, Cardiff

**Organiser:** Cardiff Model Engineering Society.  
**Venue:** Heath Park Miniature Railway, King George V Drive East, Cardiff CF14 4AW.  
**Open:** 1300-1700.  
**Admission:** £2.80 (card payment only), children aged 3 & under free (must be accompanied on rides). No dogs except service dogs and no ball games allowed on site.  
**Amenities:** refreshments, gift shop.  
**Features:** miniature railways (steam and electric trams), model and garden railways.  
**Website:** www.heathparkminiaturerailway.co.uk

### Sunday 19 July HOVETON, Norfolk

**Organiser:** 32A Model Railway Club.  
**Venue:** Hoveton Village Hall, Stalham Road, Hoveton, Norfolk NR12 8DU.  
**Open:** 1000-1600  
**Admission:** adults £4.00, children 50p and family tickets £6.50.  
**Amenities:** there is ample parking outside the hall, and a children's play area is adjacent. Refreshments are available.  
**Features:** 32A summer club day, featuring an exhibition of 10 club members' and guests' layouts in O, OO and N, plus a club test track for visitors to take along their own locos or test a new purchase. Also modelling demonstrations and trade support.  
**Contact:** Exhibition Manager Alan Ball, 07717 502597.  
**Website:** www.32amrc.co.uk

### Saturday 25 July BETHERSDEN, Kent

**Organiser:** Ashford International Model Railway Education Centre.  
**Venue:** Unit 2 Mill Farm Business Units, Fridd Lane, Bethersden, Kent TN26 3DB.  
**Open:** 1030-1600  
**Admission:** adults £10.00, under-14s £5.00, family (2+2 or more) £25.00. 20% discount for Gresley Society members.  
**Amenities:** step-free access and accessible toilets. Shop and tea room.  
**Features:** OO layout *The Gresley Beat* in operation to celebrate 150 years since the birth of Sir Nigel Gresley, with locomotives of his design carrying 'Gresley 150' headboards.  
**Website:** www.aimrec.co.uk

### Saturday 25 July HITCHIN, Hertfordshire

**Organiser:** Letchworth Model Railway Society.  
**Venue:** The Priory School, Bedford Road, Hitchin SG5 2UR.  
**Open:** 1000-1630  
**Admission:** adults £5.00, acpd U16s free with 2 paying adults.  
**Amenities:** free parking. Refreshments.  
**Features:** over 18 layouts, including: *Cwm Bach*, *Docklands*, *French's Mill* (O); *North Level Railway* (O-16.5/NG7); *Top Yard* (EM); *Fenwick*, *Giffard Lane*, *Little Colne*, *Reddingham*, *Sandside Quarry* (OO); *Tidley Light Railway*, layout with name yet to be confirmed (OO9); *Rock Ridge* (HO); *Donnersbachkogel* (HO & HOe); *Fanniston*, *Foxvale* (N); plus Battle of Britain Society second-hand stall, Model Bus Federation demonstration, Talylyn Railway souvenirs and trade support.  
**Contact:** use contact form on website.  
**Website:** www.letchworthmrs.org.uk  
**Facebook:** www.facebook.com/LetchworthMRS/

### Saturday 25 July HOUNSLOW, West London

**Organiser:** Twickenham & District MRC.  
**Venue:** Kerswell Hall, 125 Wills Crescent, Hounslow TW3 2JF.  
**Open:** 1000-1600  
**Admission:** free but donations welcome.  
**Amenities:** on-site parking. Wheelchair-friendly venue. Refreshments available.  
**Features:** club open day with 11 layouts on show in scales from G to N – nine working and two under construction. Large selection of second-hand items for sale.  
**Email:** secretary.tdmrc@gmail.com  
**Website:** www.tdmrc.icu  
**Facebook:** www.facebook.com/TwickenhamMRC

### Saturday 25 & Sunday 26 July NEW MILTON, Hampshire

**Organiser:** South Coast Model Railway Club.  
**Venue:** The Arnewood School, Gore Road, New Milton, Hants BH25 6RS.  
**Open:** Saturday 1000-1700  
Sunday 1000-1600  
**Admission:** adults £8.00, acpd U16s free. Both card & cash payments will be accepted at the door and throughout the exhibition.  
**Amenities:** venue has plenty of free parking and is within walking distance from New Milton railway station. Disabled access. Cafeteria.  
**Features:** 17 layouts plus a number of traders.  
**Contact:** use email address below or contact form on website.  
**Email:** info@southcoastmodelrailwayclub.co.uk  
**Website:** www.southcoastmodelrailwayclub.co.uk

### Saturday 25 July – Sunday 9 August MAKING TRACKS AT THE BEACH ABERYSTWYTH, Ceredigion

**Organiser:** Vale of Rheidol Railway & Pete Waterman's Making Tracks team.  
**Venue:** Vale of Rheidol Railway, Park Avenue, Aberystwyth, Ceredigion SY23 1PG.  
**Open:** daily 1000-1600 (last admission time).  
**Admission:** adults £8.00, children £5.00. Visitors travelling on a Vale of Rheidol Railway train service during the exhibition period will receive free admission to the exhibition. Booking in advance recommended.  
**Amenities:** the VORR station is in the centre of Aberystwyth, near the Cambrian Coast Line station and with a dedicated steam railway car park off Park Avenue/Boulevard St Briec – sat-nav SY23 1PG. A gravelled path short-cut links the main line railway and bus station, served by Transport for Wales trains from Shrewsbury via Welshpool and Machynlleth, and Trawscymru and local buses, to the VORR station, or visitors may prefer to use the slightly longer pavement route along intervening streets. There is level access from the car park to the booking office & gift shop, and a level foot crossing from the station platform to the Engine Shed Museum exhibition venue. For train passengers, a wheelchair accessible 'summer carriage' (with access via a ramp) is scheduled to run on every train – space can be booked on the VORR booking website. Aberystwyth station has a café offering light refreshments and hot & cold drinks.

**Features:** following the success of last year's World Record Model Railway visit to Aberystwyth, Pete Waterman and his team from Making Tracks are returning to the 1' 11½" gauge Vale of Rheidol Railway Engine Shed Museum, located in the former GWR locomotive shed, with their 72' long *Watford Junction* model railway layout. Visitors will have the opportunity to drive the trains themselves. Aimed particularly at families, there will be a beach-themed treasure hunt involving visitors searching the layout for cleverly hidden seaside-inspired items tucked in among the layout tracks, buildings and scenery.  
**Contact:** 01970 625819.  
**Email:** info@vor.wales  
**Website:** www.vor.wales/blog/peter-waterman-and-making-tracks-return-to-the-seaside/  
**Facebook:** www.facebook.com/rheidolrailway/

### Sunday 26 July MODELS IN THE MUSEUM ALDRIDGE, Walsall, West Midlands

**Organiser:** Aldridge Transport Museum.  
**Venue:** Aldridge Transport Museum, Shenstone Drive, Northgate, Aldridge, Walsall WS9 8TP.  
**Open:** 1000-1600  
**Admission:** adults £6.00, children £2.00, family £15.00, U5s free entry. Includes entry to the museum.  
**Amenities:** free parking on site and at Park & Ride facilities. The nearest rail station is Walsall. Free bus service every half hour to and from the event departing Walsall (Hatherton Road) and Aldridge Community Centre (for Park & Ride) – see website for details. Alternatively, from the rail station you can take bus services Nos.7 or 35A to the junction of Northgate and Leighwood Avenue, from where it is a short walk north along Northgate to the museum.  
**Features:** model show with 20 layouts plus trade stands and museum shop.  
**Contact:** 01922 454761.  
**Email:** amrtm1@aol.co.uk  
**Website:** www.amrtm.org  
**Facebook:** www.facebook.com/AldridgeTransportMuseum/

### Sunday 26 July THIRSK, North Yorkshire

**Organiser:** Thirsk Model Railway Show Committee.  
**Venue:** Thirsk and Sowerby Town Hall, Westgate, Thirsk YO7 1QR.  
**Open:** 1000-1600  
**Admission:** adults £5.00, acpd children free, payable cash only on the door. Event is raising funds for the Yorkshire Air Ambulance.  
**Amenities:** venue has free parking, although space is limited. Alternative parking in nearby Market Place and local car parks, where parking is also free of charge. Unfortunately, part of the exhibition will be on the first floor and there is currently no wheelchair access to that part of the building. Refreshments.  
**Features:** 15 layouts, incl: *Americana*, *Amwell Basin*, *Back o't' shed*, *Corrieshalloch*, *Culthwaite*, *Doon Road*, *Harrachov cz*, *Izal Factory*, *Matlock Road*, *Project New Street*, *Ravensworth Road*, *St Agur*, *Sans Pareil*, *The Warren*, *Two Goods*, *West End Drift*. Plus demonstrations on such topics as brass wagon building and weathering rolling stock. New this year will be a display by members of Thirsk Model Railway Group of a new modular approach to layout building. Five traders.  
**Contact:** 07779 436652 or use contact form on show website.  
**Website:** www.expo-thirsk.co.uk

### Sunday 26 July UFFCULME, Devon

**Organiser:** Culm Valley Model Railway Club.  
**Venue:** Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.  
**Open:** 1000-1600  
**Admission:** free but donations welcome.  
**Amenities:** garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.  
**Features:** 60 years of steam and diesel traction on *Tiverton Junction* (30' long OO gauge model) plus *Soldor*, home to a little blue engine, driven by visitors. Club layouts.

**Website:** [www.cvmrc.weebly.com/tiverton-junction.html](http://www.cvmrc.weebly.com/tiverton-junction.html)

#### Thursday 30 July BEER, Devon

**Event:** Silver Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1200  
**Tour cost:** £40.00 per person.  
**Amenities:** free parking, with the tour starting point near the shop entrance. Visitors must be over the age of 10 and must wear closed toed shoes. Tour involves walking around the site, flights of stairs and standing for long periods.  
**Features:** after meeting a tour leader in the 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique two-hour behind-the-scenes experience as Peco opens the doors to its Devon factory. See where model railway track, wagons and accessories have been rolling off the production line for 80 years. See how raw materials are turned into finished products. Watch track components being assembled, learn about the machines used and see products being packaged, ready for shipping around the world.  
**Website:** [www.peco-uk.com](http://www.peco-uk.com)  
**Email:** [tina.bishop@peco.co](mailto:tina.bishop@peco.co)  
**Facebook:** [www.facebook.com/PecoModelRailways/](http://www.facebook.com/PecoModelRailways/)

#### Thursday 30 July BEER, Devon

**Event:** Gold Experience Peco factory tour.  
**Organiser:** Peco Model Railways.  
**Venue:** Peco, Underleys, Beer, Devon EX12 3NA.  
**Open:** 0915-1500  
**Tour cost:** £65.00 per person or £120.00 for two people booking together.  
**Amenities:** free parking, with the tour starting point of the *Orion* Pullman car located by the shop entrance. Visitors must be over the age of 10 and wear closed toed shoes. Tour involves walking around the site, flights of stairs and standing for long periods of time.  
**Features:** after meeting a tour leader in the former 'Golden Arrow' Pullman car *Orion* for refreshments and a safety disclaimer, join a unique full-day behind-the-scenes experience as Peco opens the doors to its Devon factory. It begins with a two-hour tour, with participants seeing track components being assembled and learning about the machines used, then finished products being packaged ready for shipping to 35 countries around the world. Refreshments throughout the day, including a light lunch in *Orion*, followed by a 30-minute Q&A session with a Peco management team member. There will then be an opportunity to get 'hands-on' as tour members make their own souvenir limited edition OO9 wagon. Finally, they can visit the Peco model railway exhibition and shop, with a 10% discount.  
**Website:** [www.peco-uk.com](http://www.peco-uk.com)  
**Email:** [tina.bishop@peco.co](mailto:tina.bishop@peco.co)  
**Facebook:** [www.facebook.com/PecoModelRailways/](http://www.facebook.com/PecoModelRailways/)

## MEETINGS

#### Thursday 11 June – 1500

**South East Essex Railway Society**  
*Club visit to the 7¼" gauge Barnards Farm Miniature Railway in Brentwood, Essex.*  
**Details from:** [www.seers-rail.org/programme.html](http://www.seers-rail.org/programme.html) or [seers.rail@gmail.com](mailto:seers.rail@gmail.com)

#### Wednesday 17 June – 1930

**The Grimsby & Cleethorpes Railway Group**  
*'Leeds to Llandudno 1958-2025' by Roger Hepworth.*  
**Venue:** The No.1, Station Approach, Cleethorpes DN35 8AX.  
**Entry:** small entrance fee.  
**Details from:** Stuart Blackburn, 07745 961538 or [sc.blackburn@ntlworld.com](mailto:sc.blackburn@ntlworld.com)

#### Thursday 18 June – 1930

**Marlow & District Railway Society**  
*'Irish Narrow Gauge' by David Pinniger.*  
**Venue:** Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or on Zoom.

**Entry:** £3.00 non-members, free for members.  
**Details from:** Martin Stoolman, 07954 849390.  
**Website:** [www.mdrs.org.uk](http://www.mdrs.org.uk)

#### Thursday 18 June – 1430

**South East Essex Railway Society**  
*Club visit to the Mangapps Railway Museum near Burnham-on-Crouch, Essex.*  
**Details from:** [www.seers-rail.org/programme.html](http://www.seers-rail.org/programme.html) or [seers.rail@gmail.com](mailto:seers.rail@gmail.com)

#### Thursday 25 June – 1000

**South East Essex Railway Society**  
*Club visit to the Colne Valley Railway near Halstead, Essex.*  
**Details from:** [www.seers-rail.org/programme.html](http://www.seers-rail.org/programme.html) or [seers.rail@gmail.com](mailto:seers.rail@gmail.com)

#### Thursday 2 July – 1815

**The Great Central Railway Society (Sheffield branch)**  
*A visit to Workshop X and a walk around Killamarsh.*  
**Venue:** meet at Forge Lane, Killamarsh, at 1815.  
**Entry:** £4.00 donation is requested.  
**Details from:** Bob Gellatly 07519 011213 or [gcrs@talktalk.net](mailto:gcrs@talktalk.net)  
**Website:** [www.gcrsociety.co.uk](http://www.gcrsociety.co.uk)

#### Monday 6 July – 1930

**Peak Rail Association (Sheffield Branch)**  
*'A Cinder Path Leads to the Shed' by John Hunt.*  
**Venue:** The Farm Road Sports & Social Club, Farm Road, Sheffield S2 2TP.  
**Entry:** £5.00 donation is requested.  
**Details from:** Dave Sharp 07487 685306 or [davidsharpsheffield@gmail.com](mailto:davidsharpsheffield@gmail.com)  
**Website:** [www.peakrail.co.uk/sheffieldbranch](http://www.peakrail.co.uk/sheffieldbranch)

#### Wednesday 8 July – 1600

**South East Essex Railway Society**  
*Visit to member's garden railway in OO gauge.*  
**Details from:** [www.seers-rail.org/programme.html](http://www.seers-rail.org/programme.html) or [seers.rail@gmail.com](mailto:seers.rail@gmail.com)

#### Thursday 16 July – 1930

**Marlow & District Railway Society**  
*'That Was The Year That Was – 1972' by Geoff Plumb.*  
**Venue:** Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or on Zoom.  
**Entry:** £3.00 non-members, free for members.  
**Details from:** Martin Stoolman, 07954 849390.  
**Website:** [www.mdrs.org.uk](http://www.mdrs.org.uk)

#### Thursday 16 July – 1500

**South East Essex Railway Society**  
*Visit to member's house and garden railway with models in gauges O, On30, P4, OO and HO.*  
**Details from:** [www.seers-rail.org/programme.html](http://www.seers-rail.org/programme.html) or [seers.rail@gmail.com](mailto:seers.rail@gmail.com)

## CLUB NEWS/NOTICES

**Bedfordshire 16mm Narrow Gauge Modellers** meet on the first Sunday of the month 1030-1600 at Eaton Bray Village Hall, near Dunstable. New members welcome. Two layouts for 16mm live steam and electric locos in 32mm and 45mm gauges. Go along, visit [www.bag16mm.org.uk](http://www.bag16mm.org.uk) or email [membership@bag16mm.org.uk](mailto:membership@bag16mm.org.uk)

**Berkshire N Gauge Society** meets at 1900 on Thursdays at West London Aero Club, White Waltham Airfield, Maidenhead SL6 3NJ. New members welcome. Email: [tornado.gr.1@hotmail.co.uk](mailto:tornado.gr.1@hotmail.co.uk) or [percy.richard1946@gmail.com](mailto:percy.richard1946@gmail.com)

**Bilton Grange Model Club** is a recently formed group in east Hull, with members interested in OO, Tri-ang TT and O gauge plus some in military modelling, aircraft, tanks, ships and Warhammer. The group meets every second Tuesday in the month from 1900-2100 at the Bilton Grange Community Association building opposite The Grange pub in Grange Road, east Hull. Call 07748 406806 or email [biltongrangemodelclub@yahoo.com](mailto:biltongrangemodelclub@yahoo.com)

**Bosvenegh Model Railway Group** is a fairly new group meeting every Friday evening from 1900-2200 at Withiel Village Hall, Withiel, near Bodmin, Cornwall. The group is building

an OO layout. New members welcome – get in touch by emailing [bosveneghmr@gmail.com](mailto:bosveneghmr@gmail.com) or visit [www.bmrg.org.uk](http://www.bmrg.org.uk)

**Bridgend Model Railway Group** meets weekly on Thursdays at 1900 upstairs at Laleston Gardener's Club, Cliff Cottage, Wind St, Laleston CF32 0HN. Group has two layouts in 7mm and one in 4mm. Visit [www.bridgendmodelrailwaygroup.co.uk](http://www.bridgendmodelrailwaygroup.co.uk) or email [bridgendmodelrailwaygroup@protonmail.com](mailto:bridgendmodelrailwaygroup@protonmail.com)

Group for anyone interested in railway modelling and/or railways meets in **Bubwith Leisure Centre bar, Main Street, Bubwith, Selby YO8 6LX** at 1930 on the last Monday of each month. Email: [steve.grantham1@btinternet.com](mailto:steve.grantham1@btinternet.com)

**Camborne Railway Modelling Group** in Cornwall meets every Wednesday 1700-2100 at Camborne Community Centre, 9 South Terrace, Camborne TR14 8SU and welcomes members of all abilities and ages (but U18s must be acpd by a responsible adult). Members can bring their own projects or help with the club layout. Charge of £3.00 a visit for room rent & refreshments. For details email [cambornerrailwaymodellinggroup@gmail.com](mailto:cambornerrailwaymodellinggroup@gmail.com) or call Wayne on 07469 944847.

**The Derby & District Area Group of the N Gauge Society** meets (normally) on the 4th Monday of each month (excl. December) between 1900-2100 at St Stephen's church hall, 311 Sinfyn Lane, Derby DE24 9GP. The Group runs a growing T-Trak modular layout and new members are welcome. Charge of £4 per visit to cover venue hire and refreshments. Email either Chris Prior ([cmprior@gmail.com](mailto:cmprior@gmail.com)) or Phil Atkinson ([familyatkinson@talktalk.net](mailto:familyatkinson@talktalk.net)). Facebook page (Derby & District N Gauge Area Group) can be checked for changes to dates.

**East Sussex Gauge O Group** – Interested in modelling O gauge or 7mm narrow gauge? This group's meetings at Pett Village Hall (just outside Hastings) are held on the 2nd Friday of each month from 1900-2000. Test track can run fine and coarse scale two or three-rail, dc or DCC rolling stock. See Gauge O Guild website or email [mikeh.reeves@tiscali.co.uk](mailto:mikeh.reeves@tiscali.co.uk)

**Haverfordwest Model Railway Club**, formerly Pembrokeshire Model Railway Club, meets on Wednesdays at 1915 at Haverfordwest Mosque, located at 1 Cherry Grove, Haverfordwest SA61 2NZ. For further details visit [www.facebook.com/groups/1448793075977538/](http://www.facebook.com/groups/1448793075977538/) or email: [wordpress@pmrc.welshbrook.com](mailto:wordpress@pmrc.welshbrook.com)

**Kingsway Model Railway Society** welcomes new members when it meets every Tuesday from 1900-2100 at the Kingsway Centre, 130 Kingsway, Wellingborough, Northants NN8 2HF. Visit website [www.kmrs.org.uk](http://www.kmrs.org.uk), visit Facebook Kingsway Model Railway Society or email [secretary@kmrs.org.uk](mailto:secretary@kmrs.org.uk)

**Launceston Model Railway Group** holds drop-in meetings on the 3rd Saturday of each month 1000-1400 at St Stephen's Church Hall, Roydon Road, Launceston PL15 8HL. All ages welcome but children must be acpd. O, OO and N layouts in dc & DCC. Call 07974 893172 or email Alan & Sally Barriball at: [furzepark@hotmail.com](mailto:furzepark@hotmail.com)

**Leyland Model Railway Club** meets at Hillthorpe Farm, Knoll Lane, Little Hoole PR4 4TB every Monday and Wednesday (except Mon bank holidays) from 1900-2200. Layouts in OO and N plus others being built. Call 07796 130295.

**Morecambe & Heysham Model Railway Club** meets at 9 Morecambe Street West, Morecambe LA4 5HE on Tues & Thurs from 1900-2100 and has open days on second Saturday of each month from 1000-1600, with last admission at 1530. Small admission charge but children free. Layouts in O, OO (one DCC), N & O-16.5. Venue has a disabled accessible toilet and a kitchen for open days. Gerry Blaikie, 01524 425523.

**New Forest Model Railway Society** meets on Tuesday evenings at Brockenhurst Village Hall, with layouts in N & OO, dc & DCC. For further details visit [www.nfmrs.org](http://www.nfmrs.org)

**Northampton and District Model Railway Club** now meets at Roade Village Hall, Bailey Brooks Lane, Roade, Northampton NN7 2LS on Fridays from 1900-2200. Club welcomes new members. For details visit [www.ndmrc.org](http://www.ndmrc.org)

New members are wanted by the new **North Wales Small Scale Model Railway Club**. If anyone is interested in modelling in the smaller scales from OO to T gauge, then go along to its meetings which take place in the Sea Scouts Hall, All Saints Avenue, Deganwy, Conwy LL31 9DZ on the first and third Monday of each month, 1900-2100, or telephone chairman Gareth Minords on 01248 6980547.

**Poole and District Model Railway Society** has opened a larger clubroom off Poole High Street. Includes OO gauge *Sterte Bay* (under construction), British/American N gauge layout and OO & N test track. Bring locomotives and/or stock to try out. Workshop and refreshment area. Tuesdays 1900-2200. See [www.pdmrs.com](http://www.pdmrs.com)

**Romiley Methodist Railway Modellers** meet from 1400 onwards every Wednesday in clubroom behind Romiley Methodist Church. Wide range of layouts in N to O gauge. See [rmm-club.com](http://rmm-club.com) or contact [rmmhonsec@yahoo.com](mailto:rmmhonsec@yahoo.com)

**Seaton Carew Model Railway Club** welcomes new members and enthusiasts, meeting on the third and fifth (where applicable) Saturday of every month at Salvation Army Hall, Park Road, Hartlepool TS26 9HT from 1200-1700 with OO and N gauge layouts plus OO, TT and N gauge test tracks. All welcome (U16s should be acpd by adult). See <https://seatoncarewmrc.byethost8.com> or email [contact.scmrc@gmail.com](mailto:contact.scmrc@gmail.com)

**Sidmouth Model Railway Group** welcomes enthusiasts to its meetings and has layouts (N, OO and O) in progress for all wishing to join an active group. Visit [www.sidmouthmrg.com](http://www.sidmouthmrg.com) or contact via email: [sidmouthmrg@gmail.com](mailto:sidmouthmrg@gmail.com)

**Stockton & District Model Railway Club** is a friendly group of all ages, skills and interests, and anyone is welcome to bring items to run on the O or OO layouts. Members meet every Monday & Wednesday from 1830 at Elmwood Community Centre, Stockton-on-Tees TS18 5EP. Contact John Madden on 01642 614253 or email [secretary.stocktonmrc@outlook.com](mailto:secretary.stocktonmrc@outlook.com)

**Tamar Valley Model Railway Club** meets at Bere Alston Bowling Club, Bedford Street, Bere Alston PL20 7DZ, on 3rd Wednesday of month, Jan to Nov, and on 2nd Wednesday in Dec, 1000-1600. Launceston & Cornwall N Gauge Enthusiasts attend. N, OO, OO 3-rail and O layouts. Members can buy tea, coffee & home-made cakes and cooked lunches. Les Whaley, 07972 333202.

**Tonbridge Model Railway Club** welcomes new members with meetings at Christ Church Centre, High Street, Tonbridge from 1900 each Thursday. Access by rear Church entrance (opp. petrol station). Parking in the main car park outside Sainsbury's, TN9 1SF. Layouts in O and O.16.5/O9 plus 4mm scale. Find club@TonbridgeMRC or email [tonbridgemrc@yahoo.co.uk](mailto:tonbridgemrc@yahoo.co.uk)

**West Midlands N Gauge Club** is in its 30th year and is seeking new members. The club meets on the 1st and 3rd Saturdays in the month at Tipton Green Methodist Church, Park Lane West, Tipton DY4 8LD, 1400-1700, including coffee and cake. Four working layouts and two new projects are under way. For details visit [www.wmngc.co.uk](http://www.wmngc.co.uk)

**Wisbech Model Railway Club**, formed in 2005 by Brian Baylis, meets on Thursdays at 1900-2100, now on the ground floor of Wisbech Social Club and Institute, 15 Hill Street, Wisbech PE13 1BA (through the front door to the left). New and returning members of all skill levels welcome; U16s must be acpd by a parent or guardian. Two N gauge club layouts and OO gauge layout, all in dc. Contact chairman Jack Daniels by email at [jack\\_daniel5@icloud.com](mailto:jack_daniel5@icloud.com) or layout manager Richard on 07702 644066.

# RAILWAY MODELLER Opportunity Yours

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### Trade

#### Retailers' Announcements

##### YORK – MONK BAR MODEL SHOP LTD.

2 Goodramgate (by Monk Bar). Large selection of new model railways, Scalextric, plastic kits, diecasts, etc. Tel: 01904 659 423.

##### COLWYN BAY, CLWYD MODELS.

Opening times: Thursday, Friday + Saturday 10-1pm, 2-5pm. Second hand model railway books, videos, layouts, diecast bought, sold and exchanged. 447, Abergele Road, Old Colwyn, Colwyn Bay LL29 9PR Tel: 01492 518 709, [keith@clwydmodels.co.uk](mailto:keith@clwydmodels.co.uk)

##### DINGWALL (ROSS-SHIRE)-SPORTS & MODEL

SHOP, 66 High Street, Dingwall, IV15 9RY. PLASTICS-METALS-SCENICS. OO and N scale, PECO, Hornby, Bachmann, Metcalfe, Wills, Ratio & Dapol. Tel: 01349 862 346.

##### SWANAGE MODEL RAILROADING CENTRE – N GAUGE TO G SCALE.

Good selection of Preowned models in N, OO and O gauge. Open Wednesday, Thursday and Saturday 9.30 to 2pm. Other times by appointment please, call John on 07956 973 072.

##### CANADA and USA.

Narrow Gauge Model Railway Supply Canada ([ngmrsc.ca](http://ngmrsc.ca)) for OO9, Standard Gauge Model Railway Supply Canada ([sgmrsc.ca](http://sgmrsc.ca)) for OO, and DCC Supply Canada ([dccsc.ca](http://dccsc.ca)) for EX-DCC kit.

##### NORTH NORFOLK – SCALE MODELS

CENTRE. Unit 2, Groveland, Thorpe Market Road, Roughton, Norfolk, NR11 8TB. PECO, Bachmann, Oxford, Tasma, new & second-hand model railways, kits, diecast, scenics, scenery workshops, commission sales. 01263 791119 [www.scalemodelscentre.co.uk](http://www.scalemodelscentre.co.uk)

### Trade Sales Products

**O GAUGE ETCHED KITS.** Locomotives, Carriages and Wagons. Connoisseur Models online catalogue, [www.jimmcgeown.com](http://www.jimmcgeown.com) or send 2nd class address label for free printed catalogue. 1 Newton Cottages, Nr Weobley, Herefordshire, HR4 8QX.

##### LOCOMOTECH MOTORIZING KITS FOR PECO AND SOUTH EASTERN FINECAST

**TURNTABLES.** Standard (3.5rpm) £29.99, Deluxe (2 rpm) £39.99, U.K. postage £3.90. 12V motor. Enclosed all-metal gearbox. Reviewed by Railway Modeller May 2016. More information and order online at [www.locomotech.co.uk](http://www.locomotech.co.uk) Telephone orders: 01903 871149 – afternoons only.

##### NEW WEBSITE: [www.zetlandmodelrailways.co.uk](http://www.zetlandmodelrailways.co.uk)

Good quality second-hand North American, Continental & British, "O", "HO", "OO", "N" & "Z" scales. Email [Bob@zetlandmodelrailways.co.uk](mailto:Bob@zetlandmodelrailways.co.uk)

##### OVER 1000 OO GAUGE LOCO, COACH AND WAGON KITS, many no longer in production.

Etched brass, white metal and plastic kits from over 20 Manufacturers and covering various Railway Companies. Also hundreds of items by Hornby, Bachmann, Dapol and others. Tel: 01444 413723 email: [andrew@wmcollectables.co.uk](mailto:andrew@wmcollectables.co.uk) Website: [www.wmcollectables.co.uk](http://www.wmcollectables.co.uk)

### Trade Sales Books

**RAILWAY MODELLER BACK NUMBERS** – for more information on which issues are available, please contact: *Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580.* Please see website for prices.

### Trade Services

##### DECALS AND 3D-PRINTED COMPONENTS FOR LOCOS, ROLLING STOCK, SIGNALS AND BUILDINGS.

Scales 2mm to 16mm. Plus fully custom decal and component (3D-printed and etched metal) design and production service. See [www.aardstorm-models.com](http://www.aardstorm-models.com).

##### KIT BUILDING SERVICE, LOCOS, ROLLING STOCK, QUALITY WORK AT REASONABLE PRICES.

Contact: JOHN NOWELL, 28 Victoria Road, Tuebrook, Liverpool, L13 8AW. Tel: 0151 259 5957, email: [johnnowell@blueyonder.co.uk](mailto:johnnowell@blueyonder.co.uk) [www.tuebrooktrains.co.uk](http://www.tuebrooktrains.co.uk)

### Wanted (Trade)

This section is reserved for the use of traders only.

**AIRFIX/FROG/REVELL** and other makes of unmade Second-hand plastic kits wanted. Aircraft especially wanted. Any quantity. KINGKIT, Unit 8, Cedar Court, Halesfield 17, Telford, TF7 4PF. Tel: 01952 586 457.

##### WANTED – TRAIN COLLECTIONS O, OO, N.

Always calling in areas SOMERSET, DEVON, CORNWALL, S-WALES, WILTS, HAMPSHIRE, most places in UK covered. Will dismantle layouts any size. Interested in British N gauge Farish, Minitrix, Peco, OO Bachmann, Hornby, Mainline, Lima, Wrenn, Tri-ang. Damaged, scrap locos, Peco points, track work, buildings, cars, people, books, diecast, EFE buses. Cash paid, friendly service offered. R.FOSTER 15 Rosebery Avenue, Yeovil, Somerset BA21 5LW. Tel 01935 424165 email [russelltrains@aol.com](mailto:russelltrains@aol.com)

##### MODEL RAILWAYS ALWAYS WANTED,

including Hornby, Lima, Bachmann, Dapol, Modern Image, OO, O, N gauge, kits and kit built. Send lists or give me a call. BARRY JONES, 28 Marine Crescent, Worthing, BN12 4JF. Tel/Fax: 01903 244 655.

**WE CARE A GREAT DEAL MORE!** We realise that your collection large or small is a prized possession, so we will always give our highest price, we will respond quickly to receiving your list and collect, dismantle and remove your items efficiently because we care. Whatever you have to sell from single items to large collections. Give us a call and prepare to be amazed! S&J MODELS. Tel: 01606 872 786 email: [sjmodels@tiscali.co.uk](mailto:sjmodels@tiscali.co.uk) website: [www.sandjmodelrailways.co.uk](http://www.sandjmodelrailways.co.uk)

**THINKING OF SELLING YOUR MODEL RAILWAY COLLECTION?** If so contact me for an immediate evaluation, distance no object, collections large or small. PAUL CULL, telephone: 01302 481 274 or mobile: 07767 356 890 or send list to: The Bungalow, Dentons Green Lane, Kirk Sandal, Doncaster, DN3 1JP, email: [pcull123@gmail.com](mailto:pcull123@gmail.com)

**HORNBY & BASSETT-LOWKE O GAUGE TRAINS,** wagons, coaches and accessories. Anything considered. Top prices paid for single items and/or complete collections. Tel: 01993 840 064 (Oxon) 0797 991 0760.

**ALWAYS WANTED BY TRI-ANGMAN –** collections, layouts, seeking, Tri-ang, Hornby, Dublo, Wrenn, Bachmann etc. Will collect across most of the UK. More than fair prices paid. Tel: 07966 333 605. email: [Laurence@tri-angman.co.uk](mailto:Laurence@tri-angman.co.uk) [www.tri-angman.co.uk](http://www.tri-angman.co.uk)

**MODEL RAILWAY COLLECTIONS URGENTLY REQUIRED – SMALL & LARGE,** any scale and any condition. British, European and American collections all required. Nationwide collections, distances no object. Contact us today for a quick and friendly service. Telephone: 01302 371 623, mobile: 07526 768 178, email: [anoraksanonymous@googlemail.com](mailto:anoraksanonymous@googlemail.com)

**WANTED WANTED MODEL RAILWAYS, ALL MAKES, ALL SIZES** from N Gauge, 00 scale to G Scale and larger, will travel all UK. We also dismantle layouts, over 35 year's experience. Send lists to DEREK BARNETT, 6 Vale Gardens, Penkridge, Staffs, ST19 5LQ. Tel: 01562 750 076 daytime, or text me 07721 333 521 anytime and I will ring you back, email [footplate@btconnect.com](mailto:footplate@btconnect.com)

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**WANTED – COLLECTIONS OF TRAINS FROM THESE AREAS:-** North Wales, Cheshire, Merseyside, Manchester, and Lancashire. Also Diecast. If you really want the best price then send me or email me your list. Tel 07511 899314 Email [gambiacat@tiscali.co.uk](mailto:gambiacat@tiscali.co.uk)

**BATH BRISTOL MELKSHAM TROWBRIDGE SHEPTON WANTED:** Model Railway & Model Car Collections. Railways: All Gauges & Makes. Hornby, Dublo, Triang, Bachmann, Wrenn etc. Mid & Large collections always needed. Metal & Plastic Kits. Live Steam. 45MM, Mamod etc. Model Cars: Dinky, Corgi, Matchbox EFE etc. I am a retired, personal, hobbyist/dealer. Contact Roger: telephone 01225 360 037, mobile: 07752 222 645 Email: [rogersmith200@hotmail.com](mailto:rogersmith200@hotmail.com)

**WANTED ALL ITEMS HORNBY DUBLO –** call or email: SIMON 07704704643 [info@simonshindler.co.uk](mailto:info@simonshindler.co.uk)

## Private

### Property for Sale

**Cut the expense of moving house by advertising your property in the RAILWAY MODELLER or CONTINENTAL MODELLER for just 15p a word its great value for money!!**

### Private Sales

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### Wanted Private

(This column is reserved for readers who are private Model Railway enthusiasts or collectors and require items purely for their own personal use).

**TRIX TWIN AND TRIX EXPRESS.** Avid collector. TTRCA No26, anything will be considered. Please phone anytime, will collect. Tel: 01736 754 200. Mobile: 07478 059 249.

**N OR OO GAUGE,** British outline Model Railways sought by collector, Wrenn, Hornby, Bachmann, Farish etc. Will travel to collect. Tel: 01702 461 214 after seven, 07399 73 44 94, daytime. Send lists to: [psdavis17@talktalk.net](mailto:psdavis17@talktalk.net)

**HORNBY O GAUGE** LMS Princess Elizabeth & Southern L1. Both 20 volt electric. Tel: 01993 840 064 (Oxon) or 0797 991 0760.

**WANTED TRIANG TT AND LONE STAR TREBLE O LECTRIC BY PRIVATE COLLECTOR.** Good price paid, will travel. Tel: 01980 862 387 or email: [hamptonrob1951@outlook.com](mailto:hamptonrob1951@outlook.com) (Wiltshire).

**WRENN 1966 TO 1992 –** items wanted by collector preferably boxed, single or complete collections. Please telephone: 0773 0957 800/01582 401 053 or visit my website [www.wrennspecialist.co.uk](http://www.wrennspecialist.co.uk)

**KIT ITEMS, WHITEMETAL, BRASS OO & O,** DJH, Wills, Millholme, Alan Gibson etc. Engines, coaches, wagons, unmade, part made, engines working or not. Kit spares – will travel. Tel: 01302 481 274, mobile: 07767 356 890, email: [pcull123@gmail.com](mailto:pcull123@gmail.com)

**O GAUGE WANTED – LOCOS, ROLLING STOCK, ACCESSORIES, TRACK/POINTS, SPARES AND BUILDINGS.** Large or small collections purchased. Tel 01302 481274, Mob. 07767 356 890. Email: [pcull123@gmail.com](mailto:pcull123@gmail.com)

# Swapmeets • Sales • Fairs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so. All advertisements include the organiser's town and telephone number. The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

## June 2026

**28 June (Sun) 10am-3pm**

**NEWTON ABBOT TRAIN & TOY FAIR**, Newton Abbot Racecourse, Devon TQ12 3AF. Admission £3. Superb venue, free parking, refreshments. 100+ tables. Organised by RAY HEARD.  
Tel: 01823 480 097,  
email: [rayheard66@btinternet.com](mailto:rayheard66@btinternet.com)

## July 2026

**4 July (Sat) 10am-2pm**

**BRENTWOOD**, The Brentwood Centre, Doddinghurst Road, Brentwood Essex CM15 9NN. Adults (16+) £4, accompanied children 15 and under free. J&J FAIRS.  
Tel: 01522 880 383 [www.j-jwebbtoyfairs.com](http://www.j-jwebbtoyfairs.com)  
[jandjfair@gmail.com](mailto:jandjfair@gmail.com)

**5 July (Sun) 10.30am-3pm**

**COVENTRY - 160 STALLS**, The Connexion, Ryton on Dunsmore, Coventry, CV8 3FL. Lots of stalls full of new and used model railways - many at bargain prices. Adults £3.50, Seniors £3, Children £1.  
Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**5 July (Sun) 10.30am-2.30pm**

**YEOVIL, SOMERSET (SWAPMEET)**. Yeovil Railway Centre, Yeovil Junction Station, BA22 9UU.  
Tel: 01935 421 107.

**12 July (Sun) 9.45am-1.15pm**

**DITTON**, Ditton Community Centre, Kilbarn Road, Ditton, Kent ME20 6AH.  
KRJ VINTAGE TOYFAIRS.  
Tel: 07977 646 386 & 07889 901 729.

**12 July (Sun) 10.30am-2.30pm**

**HULME HALL**, 23 Bolton Rd, Port Sunlight CH62 5DH.  
For bookings contact DAVID on 07967 681 080  
[jacko17356@icloud.com](mailto:jacko17356@icloud.com)

**12 July (Sun) 10am-2pm**

**IPSWICH**, Copdock Village Hall, London Road, Copdock, IP8 3JN. Light refreshments, good lighting.  
Telephone: PHIL COOPER for bookings on: 01787 372 559 or BILL BOURNE on: 01787 280 452.

**14 July (Tues) from 6pm**

**HOOK** - Community Centre, Ravenscroft. Hook. RG27 9NN. The only monthly fair in the South for a good old fashion mix of railway and diecast. Free parking. Excellent cafe.  
[stevedevizes@aol.com](mailto:stevedevizes@aol.com) 07732 330 305.

**19 July (Sun) 10.30am-3pm**

**DONCASTER RACECOURSE**, The Lazarus Exhibition Centre, Leger Way, Doncaster DN2 6BB. More than 350 stalls of toys, trains and models. Adults £6, Seniors £5.50, Children £2.  
Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**25 July (Sat) 10.00am to 4.00pm**

**BLUEBELL RAILWAY** - Horstead Keynes Station, RH17 7BD.  
Entry with a platform or travel ticket  
Stall bookings 01279 301 407.

**26 July (Sun) 10am-1pm**

**LOSTWITHIEL TOY FAIR**, Lostwithiel Community Centre PL22 0HE  
Admission £2 per Adult, U16 Free.  
Email: [auctionfindssouthwest@gmail.com](mailto:auctionfindssouthwest@gmail.com)

## East Midlands Swapmeets

For Trains & Railwayana Only

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**Saturday 27th June 2026**  
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## Opportunity Yours

RM Classified Advertisement Order Form — last date for receiving copy for October 2026 issue is 17th July. Please insert the following advertisement under the section headed: .....


**PLEASE NOTE: If you do wish to renew an existing advert, phone, write, fax or email within 3 months of the original advert being placed.**

**MINIMUM CHARGE £4.20 (£3.50 + 70p VAT) (per advert).**  
Prices below do not include VAT. Please add 20% VAT to total.  
**Rates per word:** Private 15p, Trade 35p, Swapmeets 65p.  
Box number £5.50 extra.

Tick if a box number is required

Advertisements without a box number must include your name, address or telephone number. Standard abbreviations (LMS, META, 0-6-0), normal hyphenations (O-gauge, 2-rail, pre-war) and prices count as one word. Telephone numbers, whether exchange or all figure, count as two words.

### CONDITIONS OF ACCEPTANCE

All copy is subject to our approval. We reserve the right to request amendments.  
Scale or gauge should be mentioned when referring to models. Swapmeets and other events must include a telephone number at which the organiser can be contacted.  
Advertisers are expected to acknowledge all enquiries that enclose an SAE (even if the item has already been sold).  
Advertisers are reminded of the Trade Descriptions Act and must avoid misrepresentation of goods offered. The Business Advertisements (Disclosure) Order 1977 requires all who sell goods in the course of a business to indicate this clearly in the advertisement.

**Please email your advert to: [classifieds@peco.co](mailto:classifieds@peco.co)**  
**Alternatively send this completed form with your contact details for payment to: Classifieds Department, Peco Publications, Beer, Seaton, Devon, EX12 3NA**

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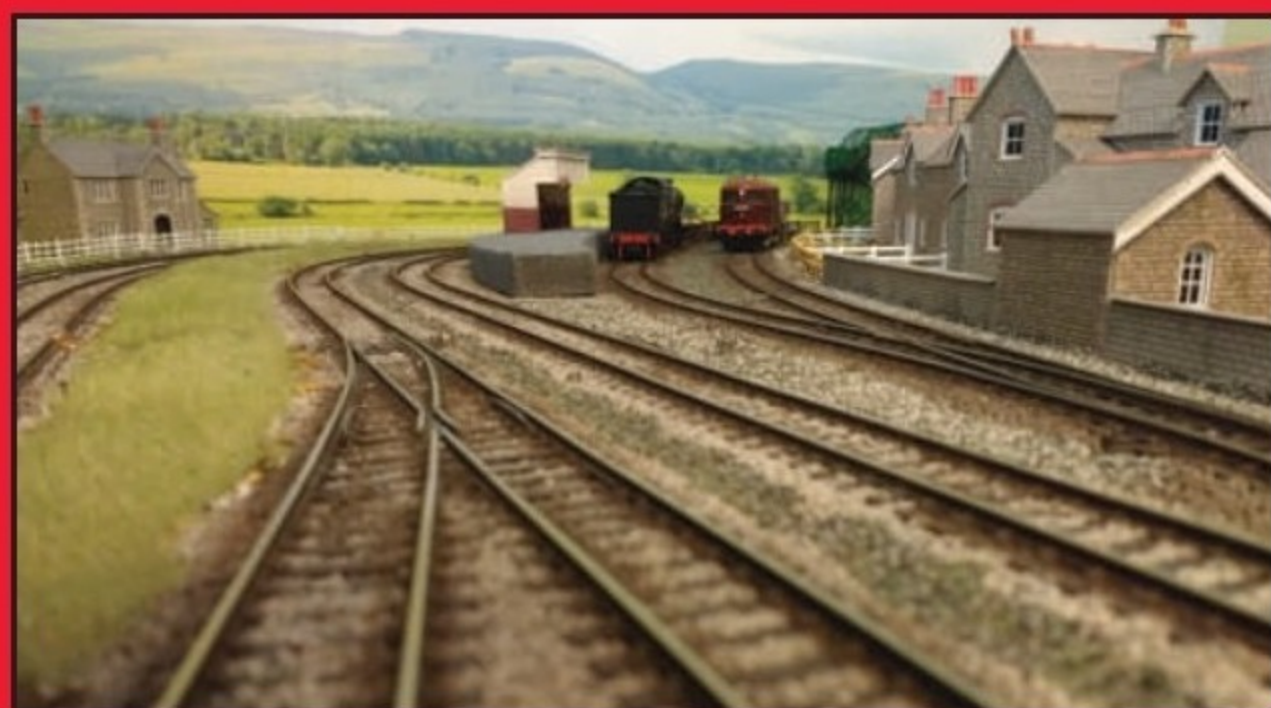
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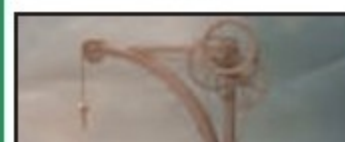
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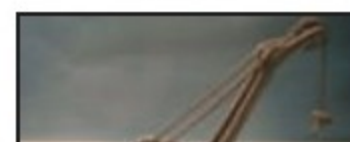
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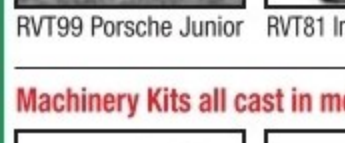
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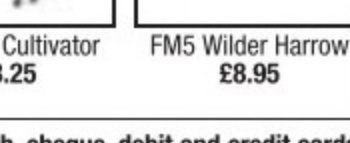
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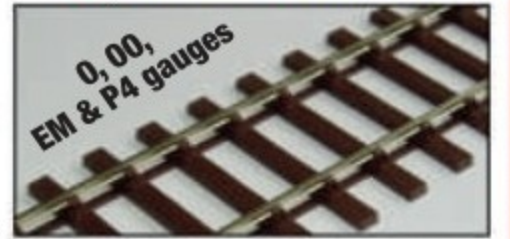
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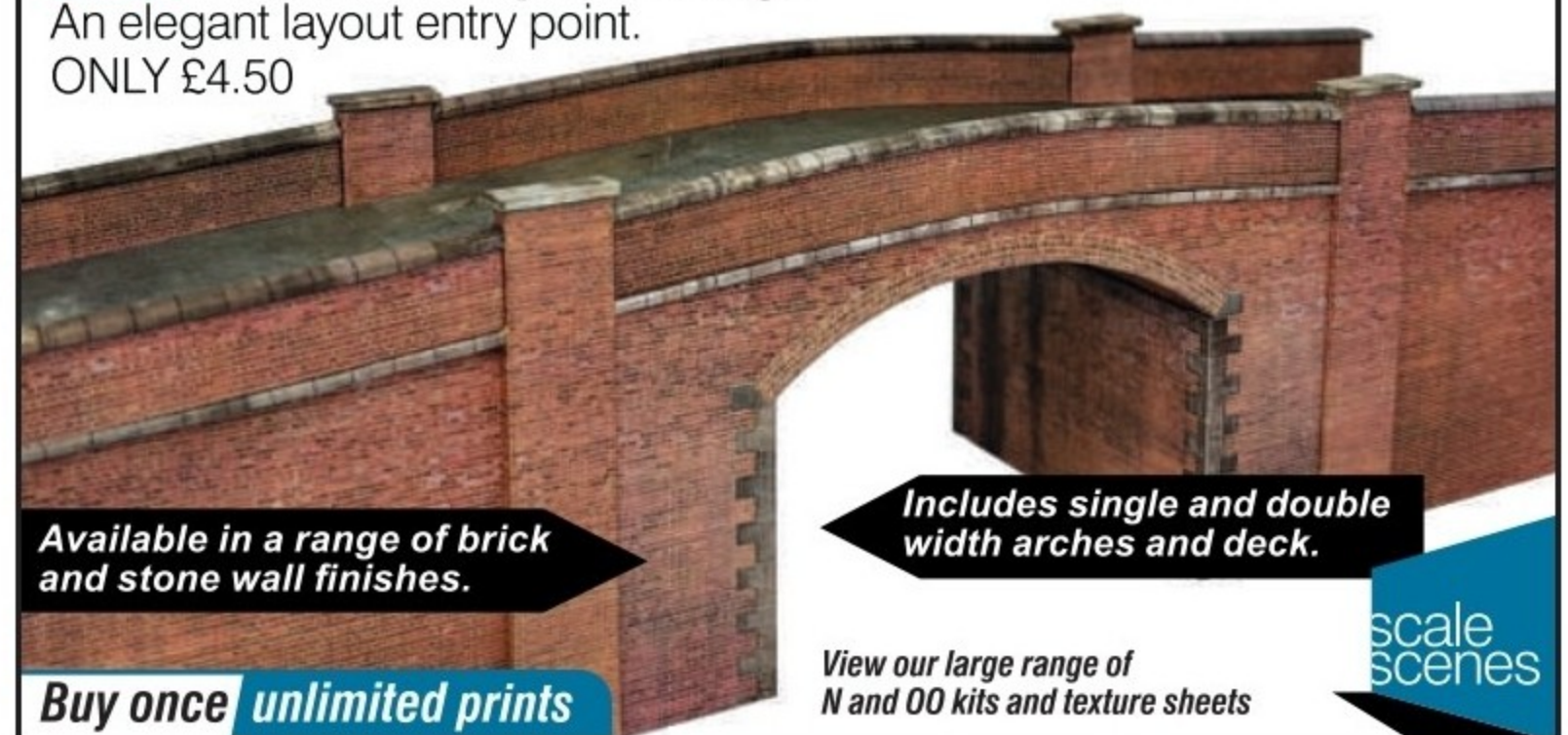
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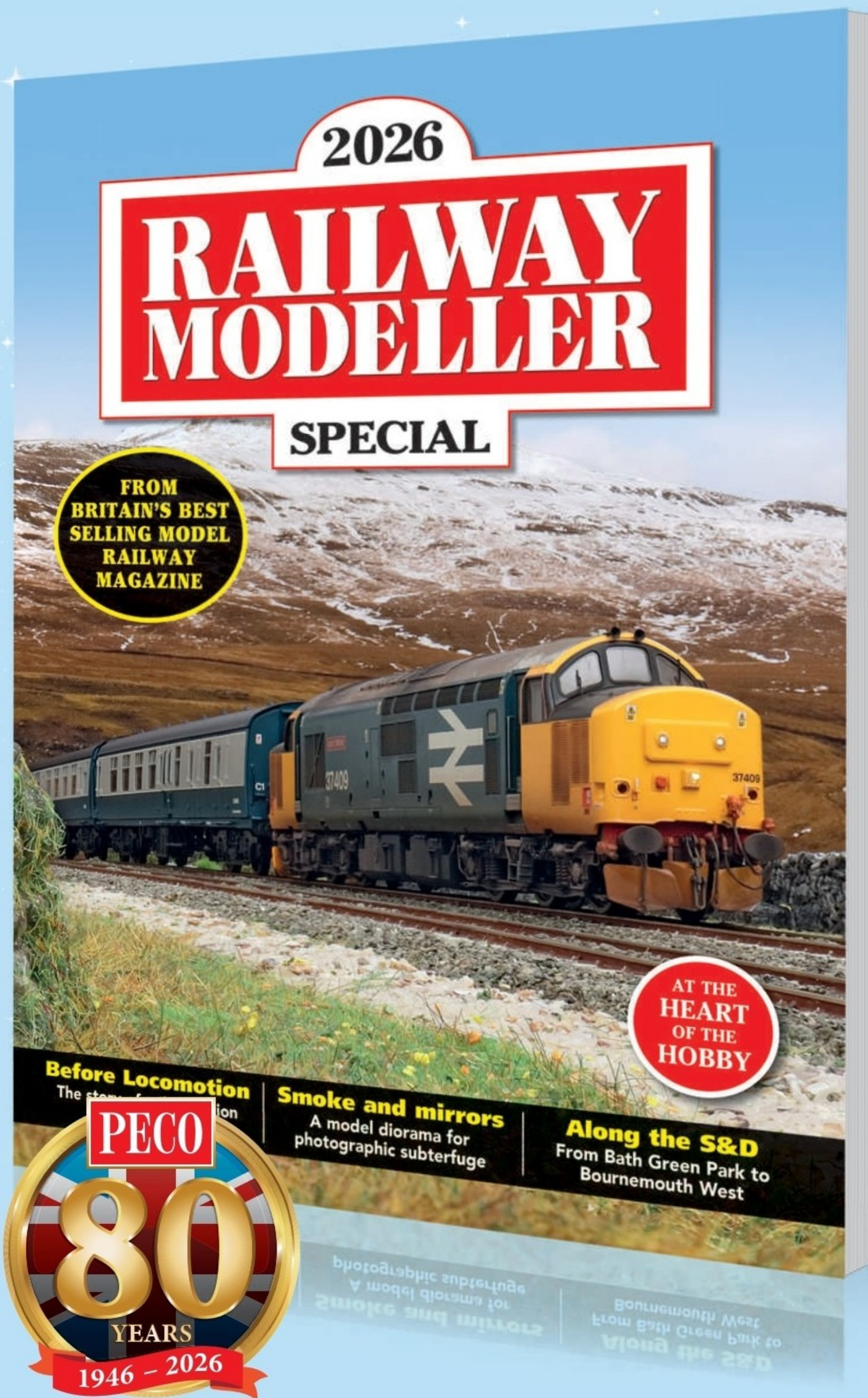


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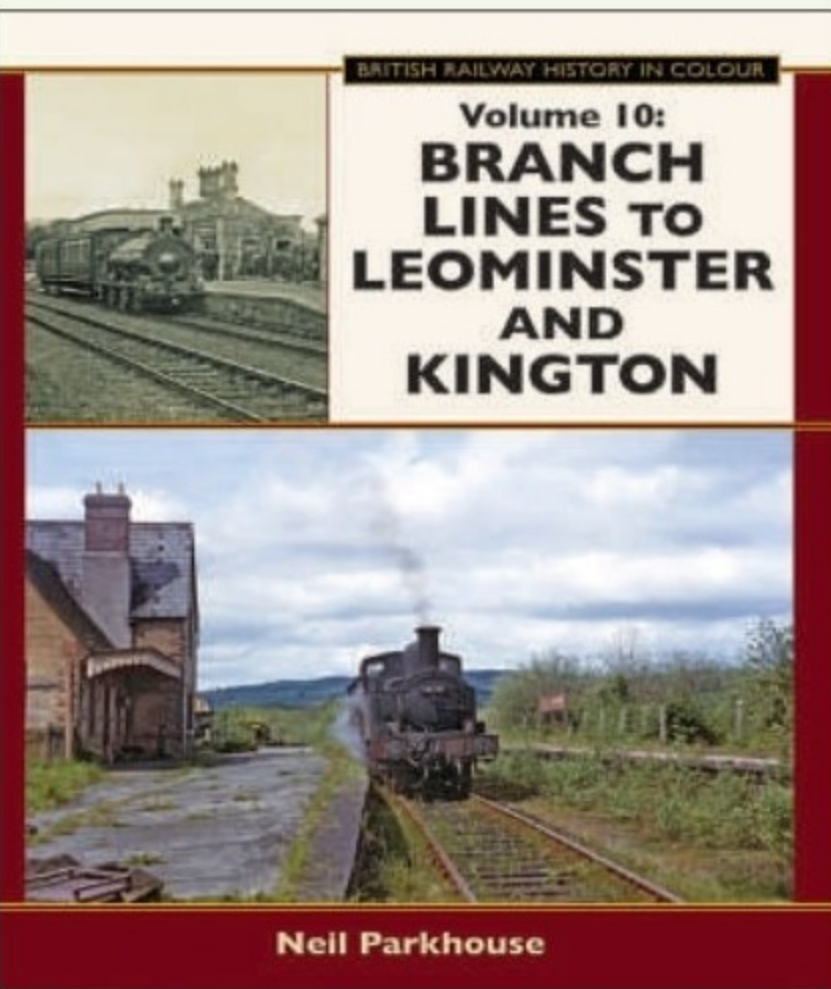


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## Volume 10: BRANCH LINES TO LEOMINSTER AND KINGTON Neil Parkhouse

- Worcester-Bromyard-Leominster • Leominster Station •
- Leominster-Kington Eardisley-Kington-New Radnor • Titley Junction-Presteign •

Leominster today is a two platform station on a double line of plain track but once it was the junction for branch lines heading east to Bromyard and Worcester, and west to Kington and New Radnor. Before reaching Kington, this latter branch passed through Titley Junction, where further branches from Eardisley, on the Hereford, Hay & Brecon line, and Presteign also came in. All of these branches are now long gone, with some having much longer working lives than others, but all once performed a vital function for the communities they served. The earliest, to Kington from Leominster, opened in 1857 and along with the branch to Presteign (which BR had more correctly renamed Presteigne), it was also the last to go when the freight service was withdrawn on 24th September 1964.

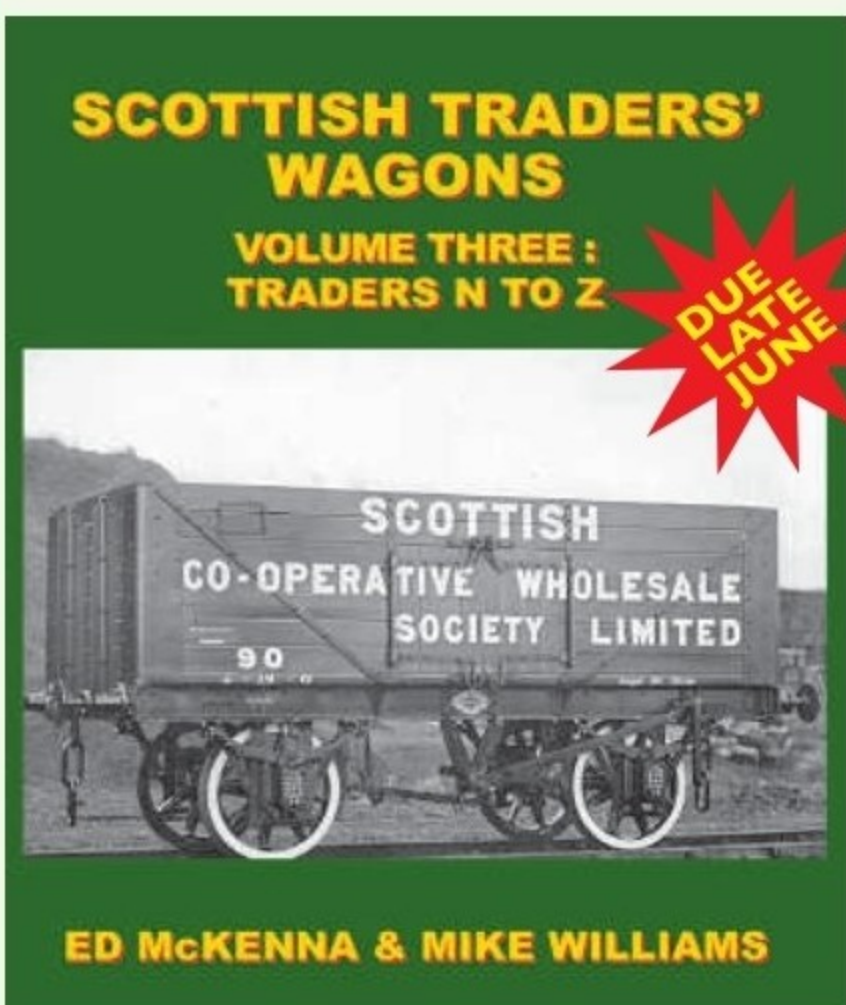
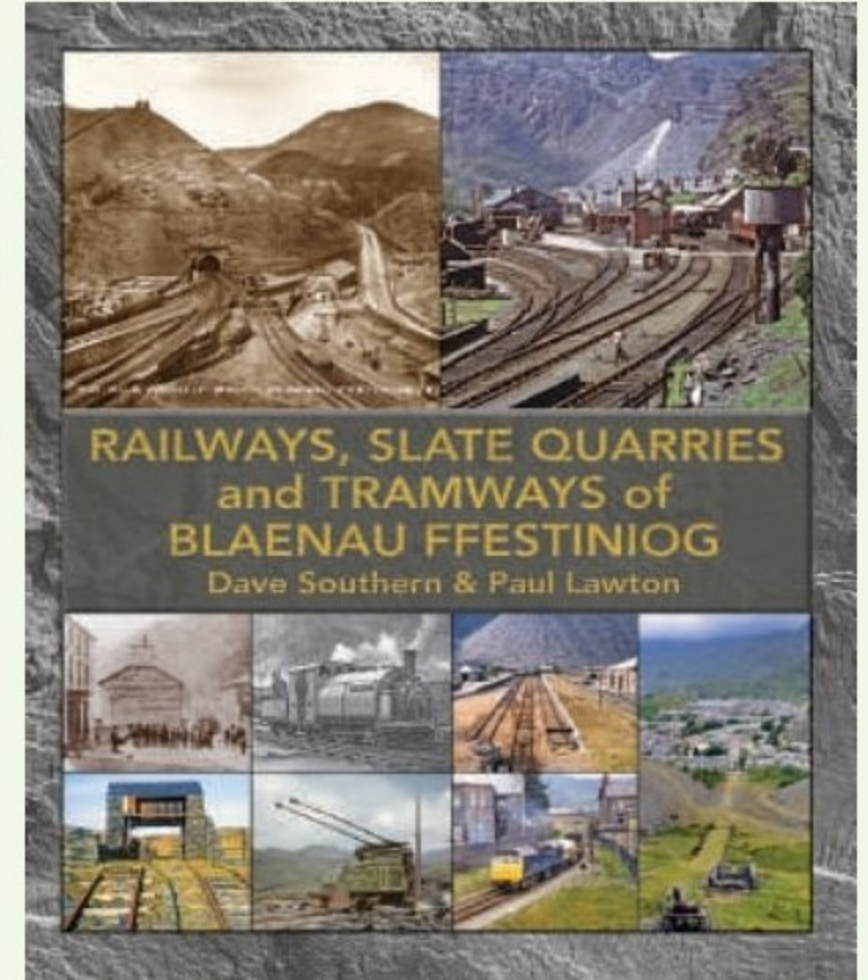
Travelling through some of Britain's most rural countryside in Worcestershire, Herefordshire and Radnorshire, we start our journey at Worcester Shrub Hill station, where we also pay a visit to the engine sheds. Heading then over the River Severn and through the western outskirts of the city, we take the line to Bromyard and Leominster at Bransford Road Junction. Work began on building the line in 1864 but mired in financial difficulties from the start, it was only finally completed in 1897. After only fifty-five years, that last section from Leominster then closed in September 1952, leaving the rest as a branch to Bromyard. We stop at Leominster to study the station in detail, which still retains its original buildings today but is otherwise a shadow of its former self, before heading off west. With two different companies involved in their construction, the Leominster & Kington Railway and the Eardisley & Kington Railway (which followed the course of the 3ft 6ins gauge horse-drawn Kington Railway of 1820), these lines are traversed by date of build and company who built them. Thus, after arriving at Kington, we then head to Eardisley to travel the obscure branch up to Titley Junction (opened in 1874) and then via Kington to New Radnor (opened in 1875) and finally Titley Junction to Presteign, also opened in 1875.

Passenger service withdrawals west of Leominster were early, all pre-Beeching, with Eardisley to Titley Junction going in 1940, to New Radnor and Presteign in 1951 and Leominster to Kington in 1955. In a departure from previous volumes in this series, we have therefore included a fine selection of early postcard views along with the usual array of colour slides, to show these stations in their heyday, as well as track plans courtesy of OS extracts and a fine selection of colour plans of the lines to Kington from GWR 2-chain surveys. So join us here as we travel through some glorious countryside, on ex-GWR diesel railcars and Class '122' units between Worcester and Bromyard, or in the brake van of a short freight hauled by a Class '14XX' 0-4-2 tank on its way to Presteign or passing the delightful Forge Crossing deep in the woods near Kington. Marvel at the sight of Dolyhir station when the tracks were still in place or enjoy watching the trains at Worcester when ex-GWR steam still just reigned supreme – all in glorious colour!

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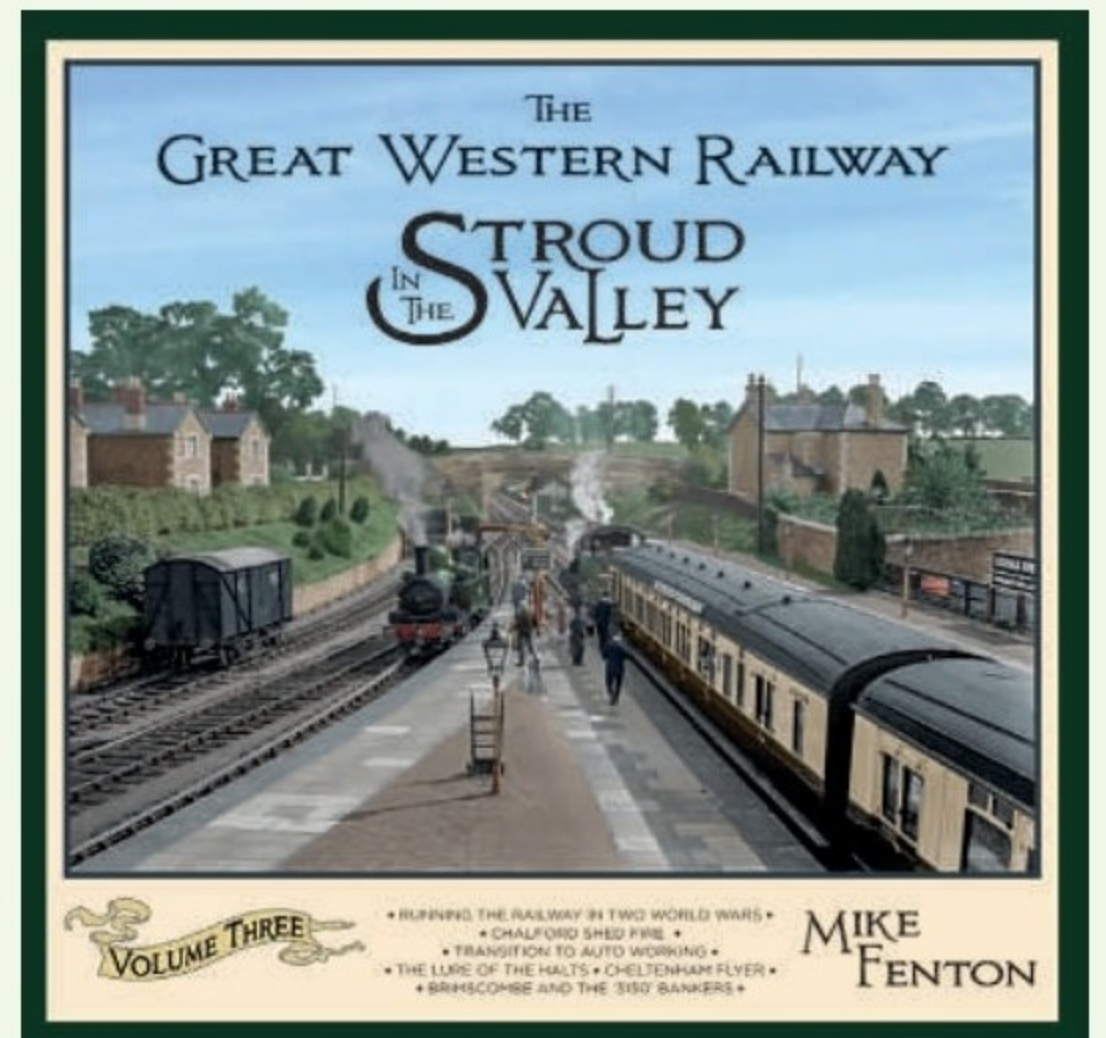
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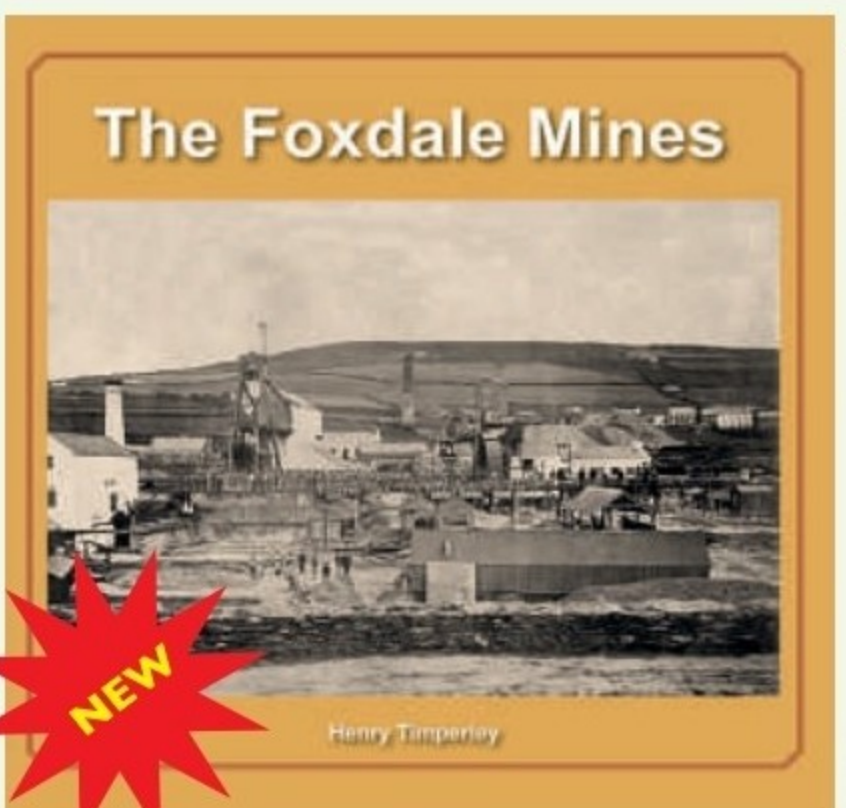
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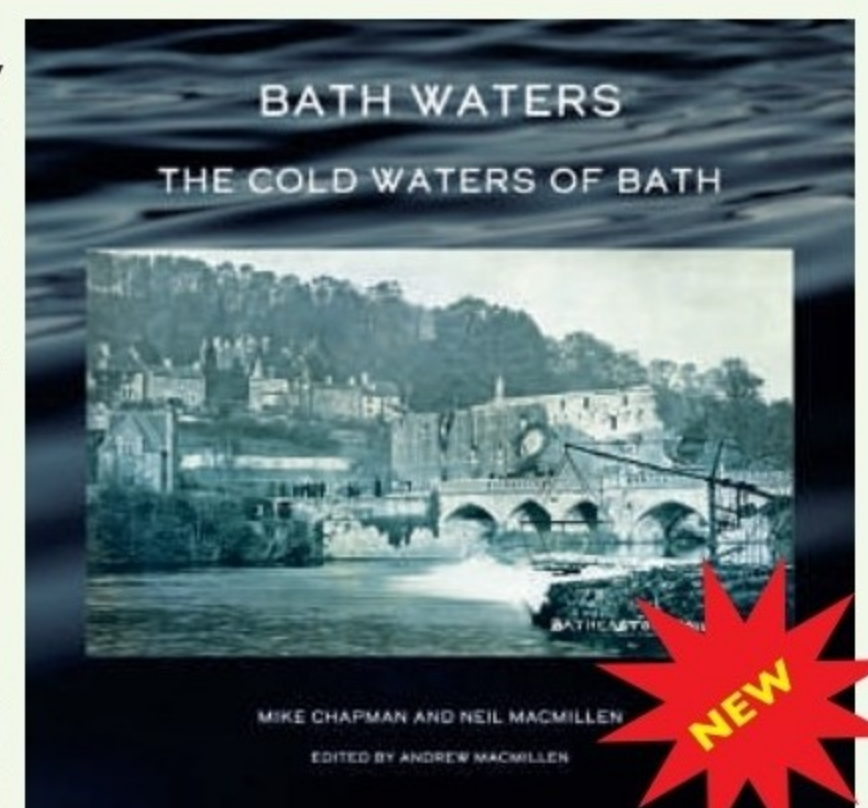
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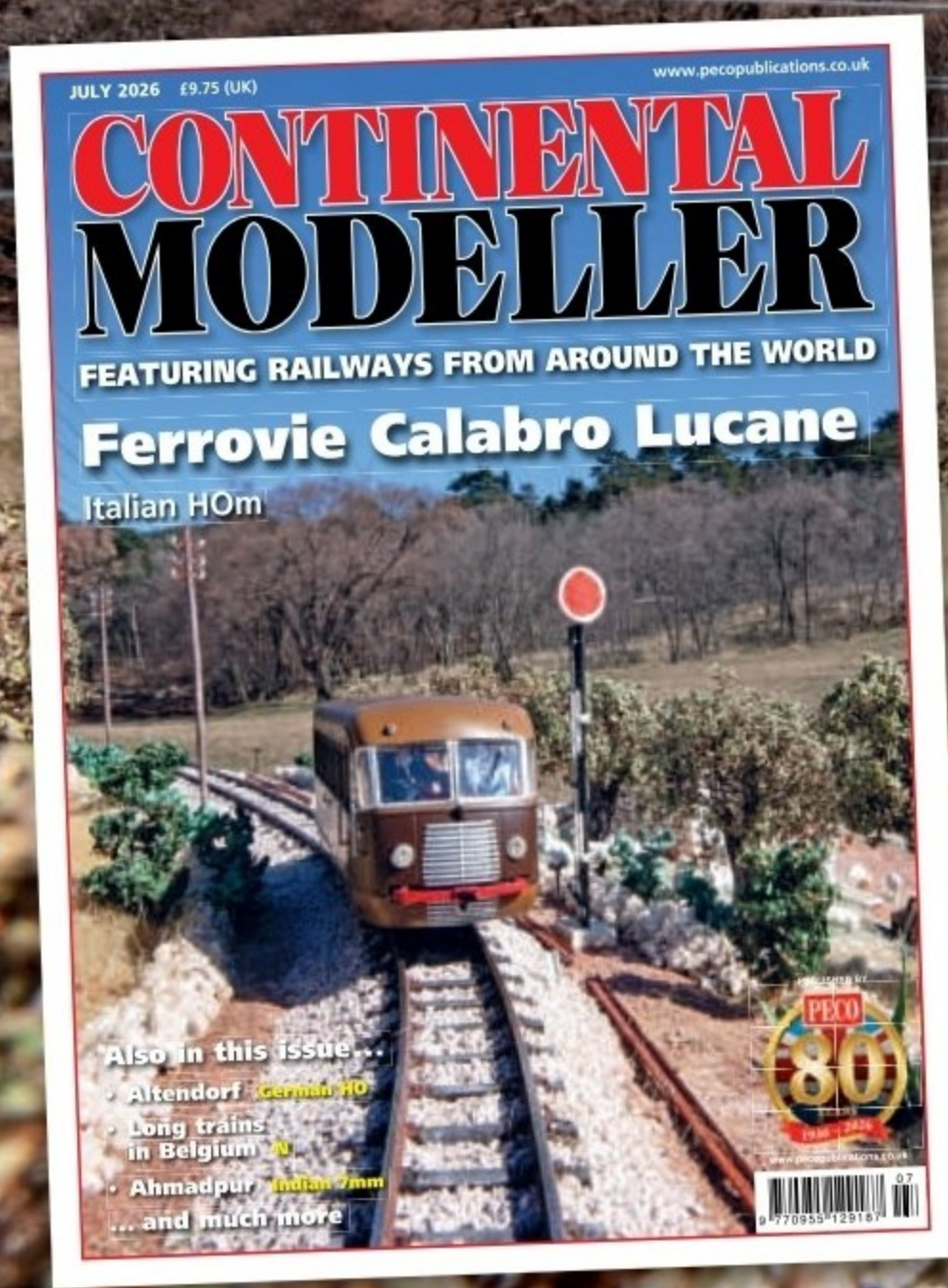
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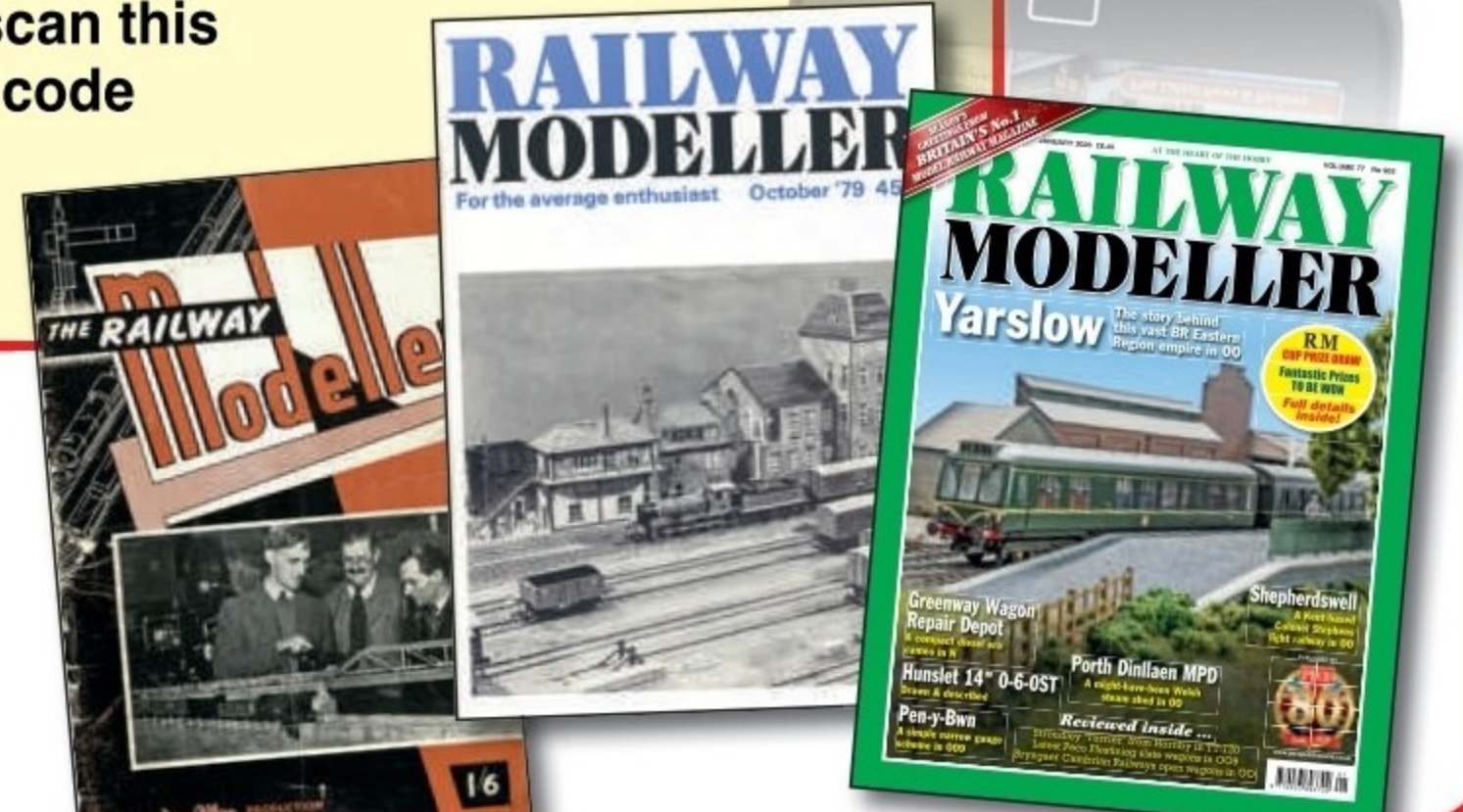
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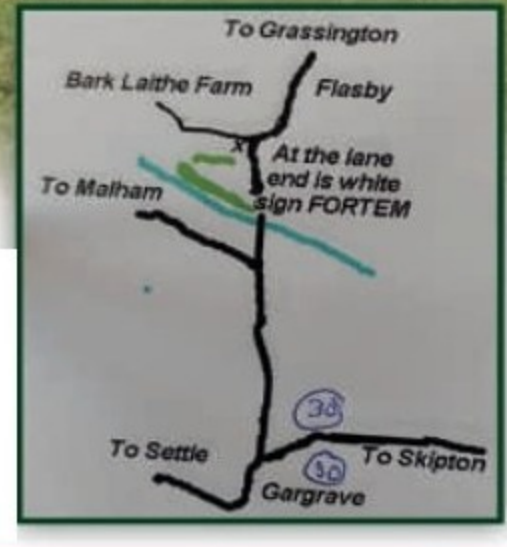


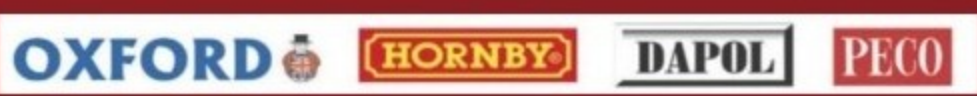
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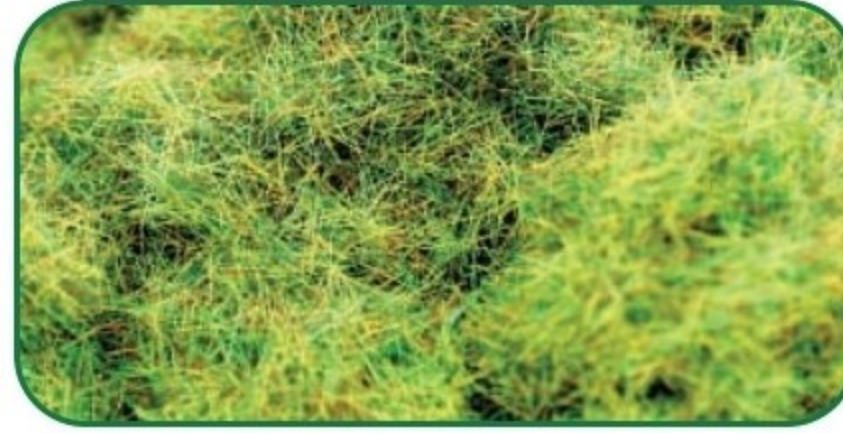
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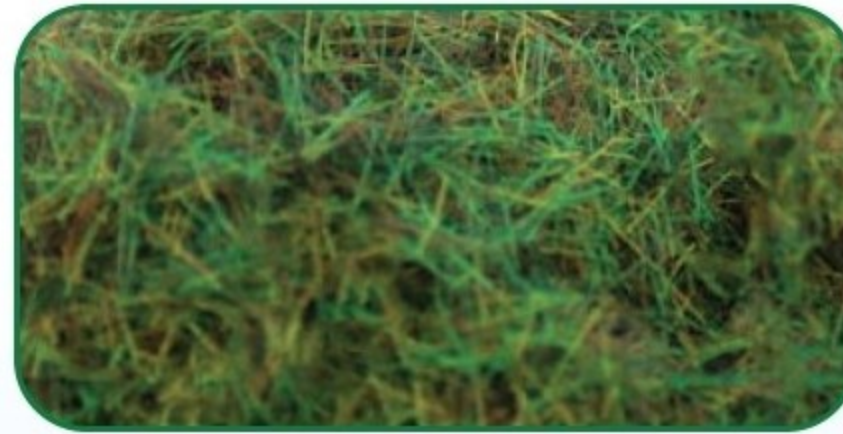
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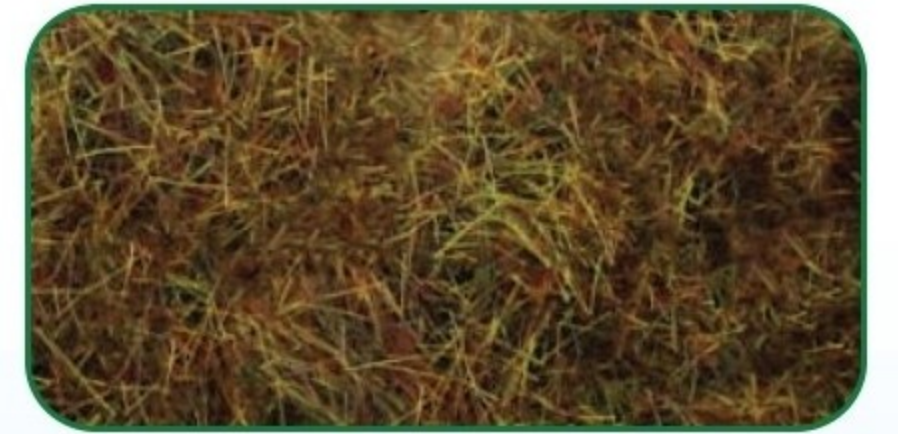
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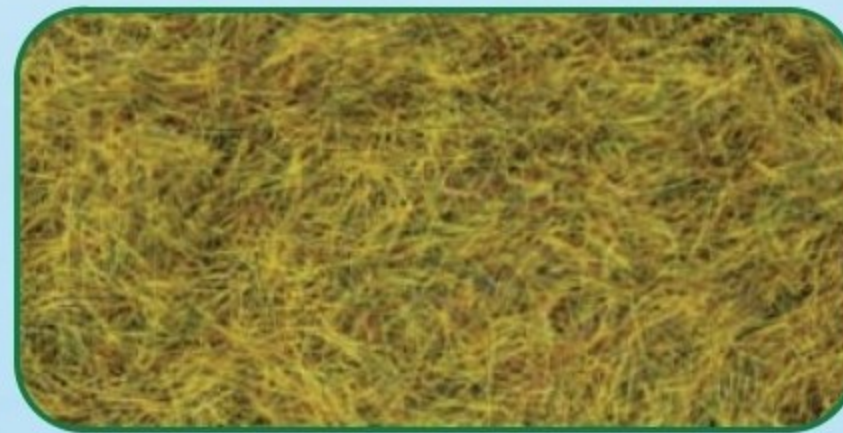
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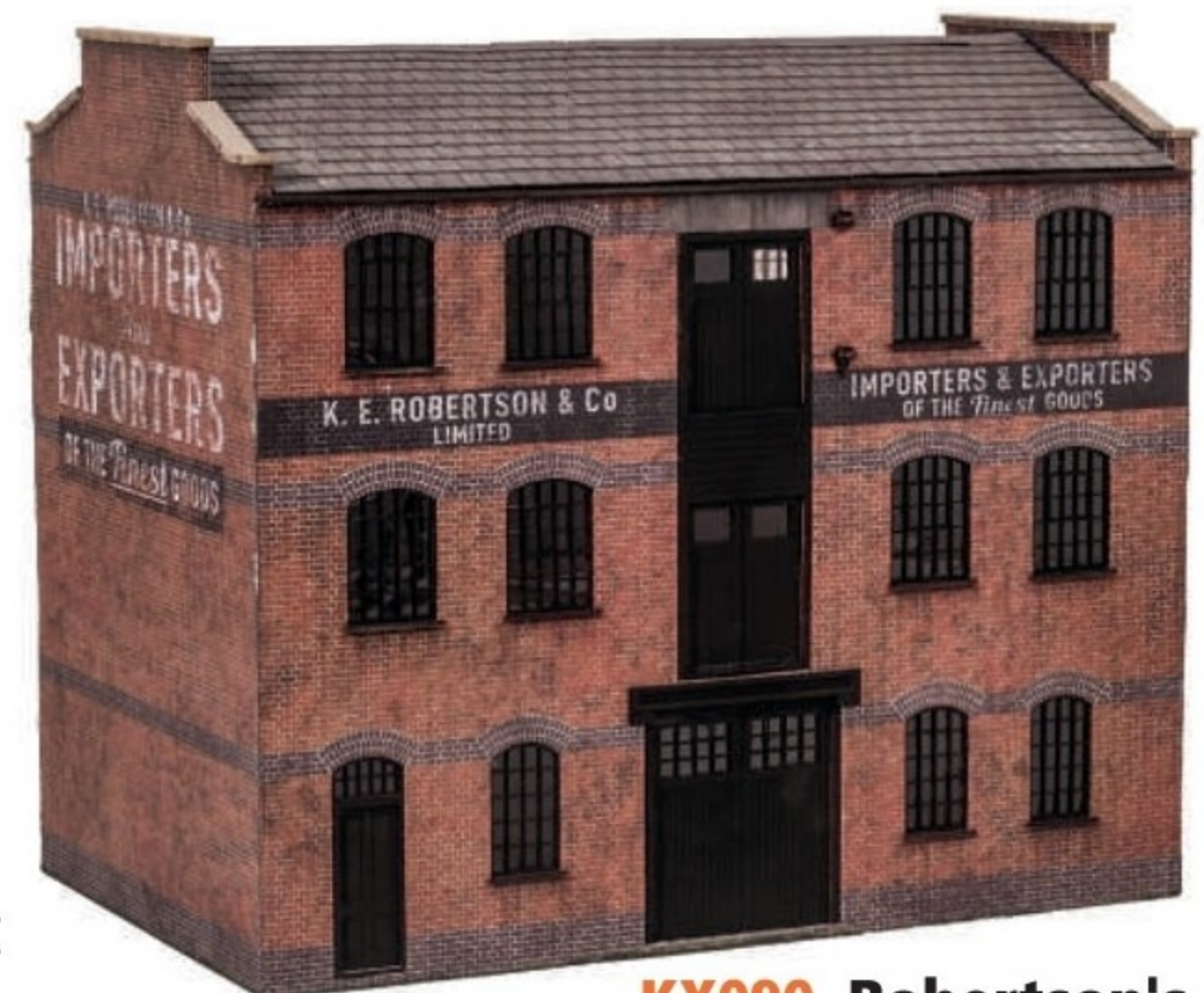
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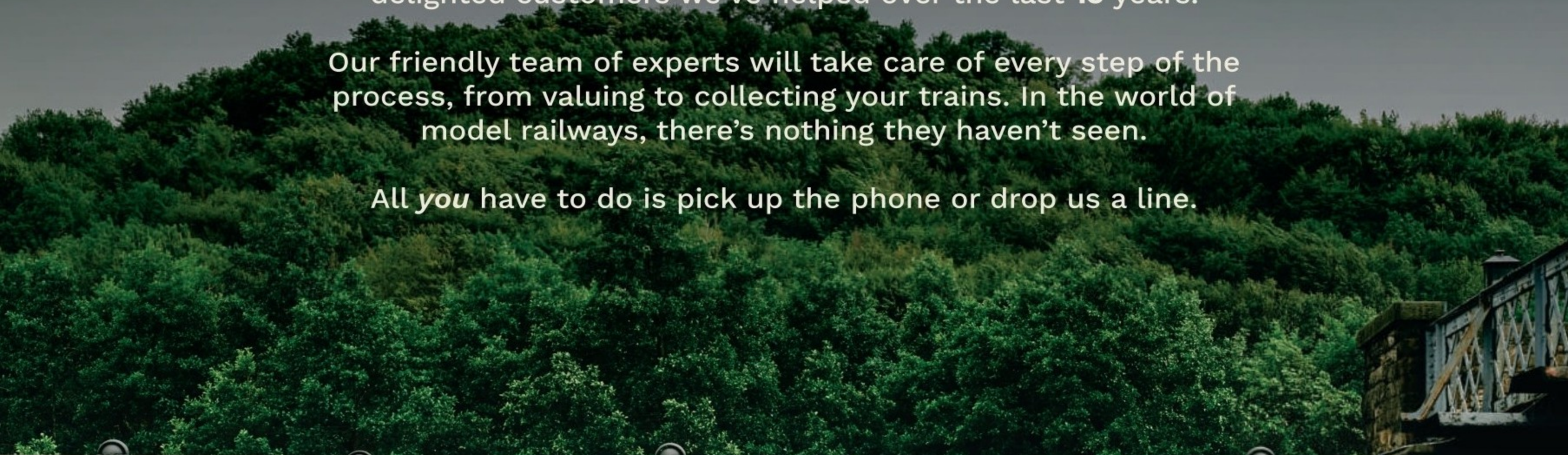
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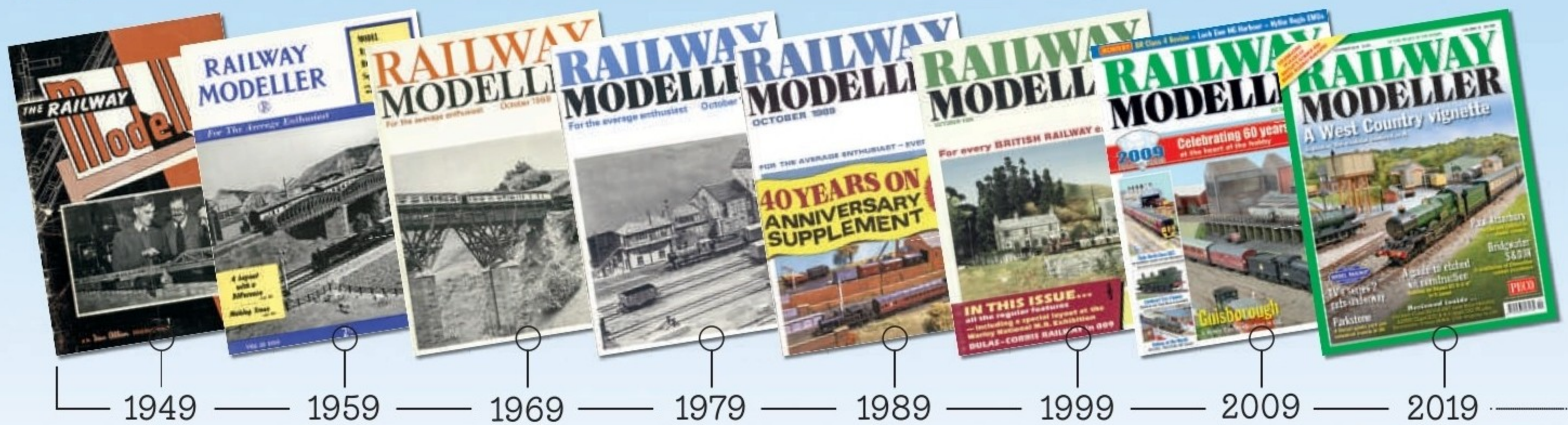
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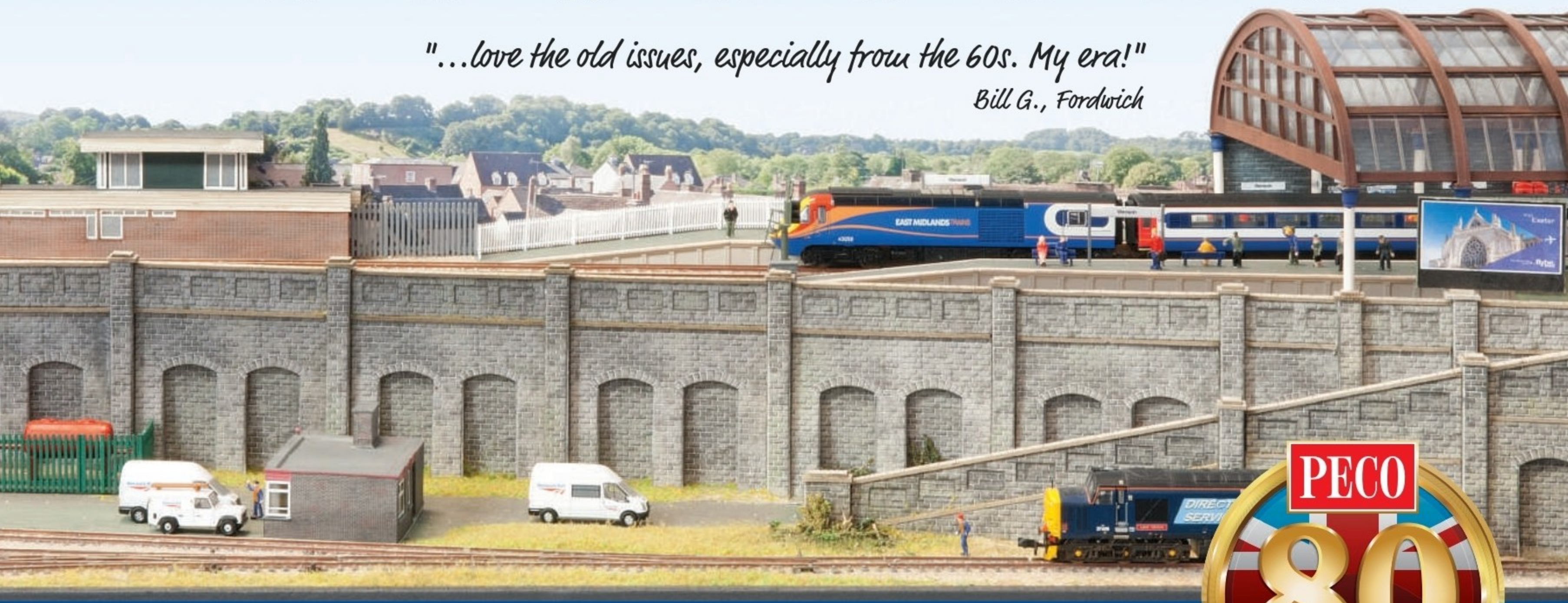


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## 'MS' Series Sound Decoders (16-bit audio)

### MS440 'Maxi'

21-pin MTC socket  
30 x 15 x 4mm

3W Audio 1.2A Motor 8 FOs  
Unlimited 'stay-alive'

### MS450 'Maxi'

8-pin & PluX-22  
30 x 15 x 4mm

3W Audio 1.2A Motor 10 FOs  
Unlimited 'stay-alive'

### MS481 'Mini'

6-pin, 8-pin & PluX-16  
19 x 11 x 3.1 mm

3W Audio 0.8A Motor 6 FOs  
1,000µF direct 'stay-alive'

### MS491 'Mini'

6-pin & 8-pin  
19 x 7.8 x 2.8 mm

1W Audio 0.7A Motor 5 FOs  
1,000µF direct 'stay-alive'

### MS500 'Micro'

6-pin & 8-pin  
14 x 10 x 2.6 mm

1W Audio 0.7A Motor 4 FOs  
1,000µF direct 'stay-alive'

### MS540 'Micro'

E24 (new 28-pin)  
19 x 8.7 x 2.8 mm

1W Audio 0.8A Motor 8 FOs  
1,000µF direct 'stay-alive'

### MS560 'Micro'

KATO EM13 (Azuma)  
27 x 14 x 2.6 mm

1W Audio 0.7A Motor 2 FOs

### MS581 'Mini'

Next-18 only  
25 x 10 x 4 mm

3W Audio 0.8A Motor 6 FOs

### MS591 'Micro'

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15 x 9.3 x 3.1 mm

1W Audio 0.7A Motor 6 FOs

### MS950 'O' Gauge

34-pin L-interface  
50 x 23 x 13 mm

2 x 3W Audio 4A Motor 11 FOs  
Integral 'stay-alive'

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2 x 10W Audio 6A Motor 15 FOs  
Integral 'stay-alive'

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for modellers  
by modellers

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### MN340 'Maxi'

21-pin MTC socket  
28.6 x 15.3 x 2.5mm

1.2A Motor rating  
8 Function outputs  
Unlimited 'stay-alive'

### MN330 'Maxi'

8-pin & PluX-22  
30 x 15.3 x 2.2mm

1.2A Motor rating  
10 Function outputs  
Unlimited 'stay-alive'

### MN300 'Mini'

6-pin & 8-pin  
17.6 x 10.5 x 3.1 mm

1.0A Motor rating  
6 Function outputs  
15,000µF direct  
'stay-alive' connection

### MN180 'Micro'

Next-18

13.3 x 9.5 x 2.6 mm  
0.7A Motor rating

4 Function outputs  
15,000µF direct  
'stay-alive' connection

### MN170 'Micro'

6-pin & 8-pin

12 x 8.6 x 2.3 mm  
0.7A Motor rating

6 Function outputs

### MN160 'Micro'

6-pin direct & Wires

13 x 7.5 x 1.6 mm  
0.5A Motor rating

4 Function outputs

### MN150 'Micro'

6-pin direct & Wires  
8.2 x 5.9 x 2.1 mm

0.5A Motor rating  
4 Function outputs

### MN140 'Micro'

E24 & PluX-12

13.5 x 8.7 x 2.3 mm  
0.5A Motor rating

8 Function outputs

### MN250 'low voltage'

Leads for hardwiring only

9.9 x 7.5 x 2.1 mm  
0.2A Motor rating

4 Function outputs  
Mini goldcap direct  
'stay-alive' connection

# INFINITY ∞

MODEL RAILWAY CONTROL [and beyond]



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These products are available from your local model shop, or, in case of difficulty, direct from ourselves.

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