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RAILWAY MODELLER

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900th Anniversary Issue

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RAILWAY MODELLER

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October 2025 vol 76 No.900

772 Bristol Templecombe Road 82G
Railway of the Month
Nigel Smith describes this extensive model of a British Railways steam era Motive Power Depot in 7mm scale, which has been developed over a period of 24 years by members of Warley Model Railway Club. It is set to be one of the main exhibits at the organisation's next annual show at Statfold on 11 & 12 October.

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Terry Small used traditional construction methods and 12V dc control for this shed-based terminus to fiddle yard system in OO.

830 More everyday items for building details

Following on from his article in the August issue, Adrian Cook explains how he has re-purposed various other readily available products and materials to enhance his 4mm scale card structure modelling projects.

WHAT'S ON THE COVER

Cover photo: *A special model for a special cover: an O gauge GWR Castle 4-6-0, hand-built by Stanley Beeson and purchased by Peco founder Sydney Pritchard, displaying a '900' reporting number. Our thanks to Fred Lewis for providing the number frame (assembled from a Malcolm Mitchell etch) for this purpose.*

818 Talking Points

Topical issues from the world of railway modelling. With RAILWAY MODELLER this month celebrating its 900th issue, Nigel Adams takes the opportunity to reflect on his own lengthy association with the hobby, which began 77 years ago…

820 Whatever happened to… Flintfield

The story of this classic Great Eastern Railway branch terminus in P4 goes back to the late 1980s, when Dutch modeller Vincent de Bode first embarked on its construction. Having previously been exhibited at only a small number of UK shows, the layout is now under new ownership and will be at this year's Uckfield Model Railway Exhibition on 18 & 19 October.

832 Readers' Letters

834 New Product Reviews

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842 Book Reviews

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All-new Stanier 8F 2-8-0 in OO gauge leads Bachmann autumn announcements; Peco reveals Ffestiniog Railway train packs in 009 with England engines; Ellis Clark Trains to produce Lancashire & Yorkshire Railway 'Pug' 0-4-0STs in OO and O gauge; Revolution Trains receives engineering samples of TT:120 MMA and JNA box wagons; previews of Uckfield, Warley at Statfold and Crewe exhibitions, and the O Gauge Get Together at the Severn Valley Railway; new run of Accurascale Class 50s; and much more…

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The biggest and best guide to the exhibition scene.

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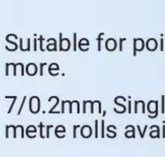

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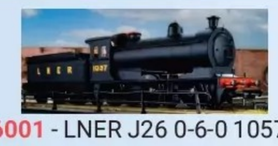

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35-802A - Class 30 D5862 BR Green Small Yellow Panels.....£182.71
35-802ASF - Class 30 D5862 BR Green Small Yellow Panels **DCC Sound Fitted**.....£276.21
35-802ASFX - Class 30 D5862 BR Green Small Yellow Panels **DCC Sound Fitted Deluxe**.....£318.71

35-803A - Class 31 5848 BR Blue.....£182.71
35-803ASF - Class 31 5848 BR Blue **DCC Sound Fitted**.....£276.21
35-803ASF - Class 31 5848 BR Blue DCC **Sound Fitted Deluxe**.....£318.71

35-804 - Class 31 31101 BR Blue.....£182.71
35-804SF - Class 31 31101 BR Blue **DCC Sound Fitted**.....£276.21
35-804SFX - Class 31 31101 BR Blue **DCC Sound Fitted Deluxe**.....£318.71

35-822 - Class 31 31188 BR Engineers Grey & Yellow.....£182.71
35-822SF - Class 31 31188 BR Engineers Grey & Yellow **DCC Sound Fitted**.....£276.21
35-822SFX - Class 31 31188 BR Engineers Grey & Yellow **DCC Sound Fitted Deluxe**.....£318.71

35-827 - Class 31 31423 *'Jerome K. Jerome'* BR InterCity Mainline.....£182.71
35-827SF - Class 31 31423 *'Jerome K. Jerome'* BR InterCity Mainline **DCC Sound Fitted**.....£276.21
35-827SFX - Class 31 31423 *'Jerome K. Jerome'* BR InterCity Mainline **DCC Sound Fitted Deluxe**.....£318.71

OO Gauge SR E4s- Due October

35-075A - LB&SCR E4 Tank 514 LBSC Lined Umber.....£140.21
35-075ASF - LB&SCR E4 Tank 514 LBSC Lined Umber **DCC Sound Fitted**.....£233.71

35-076B - LB&SCR E4 Tank 2505 SR Maunsell Green.....£140.21
35-076BSF - LB&SCR E4 Tank 2505 SR Maunsell Green **DCC Sound Fitted**.....£233.71

35-079A - LB&SCR E4 Tank 32473 BR Lined Black Early Emblem.....£140.21
35-079ASF - LB&SCR E4 Tank 32473 BR Lined Black Early Emblem **DCC Sound Fitted**.....£233.71

35-080 - LB&SCR E4 Tank 2487 SR Black Sunshine.....£140.21
35-080SF - LB&SCR E4 Tank 2487 SR Black Sunshine **DCC Sound Fitted**.....£233.71

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OO Gauge LMS 8Fs - Due Summer 2026

35-225 - LMS 8F 2-8-0 8036 LMS Black Riveted Tender.....£212.46
35-225SF - LMS 8F 2-8-0 8036 LMS Black Riveted Tender **DCC Sound Fitted**.....£305.96
35-225SFX - LMS 8F 2-8-0 8036 LMS Black Riveted Tender **DCC Sound Fitted Deluxe**.....£322.96

35-226 - LMS 8F 2-8-0 8233 LMS Black Riveted Tender.....£212.46
35-226SF - LMS 8F 2-8-0 8233 LMS Black Riveted Tender **DCC Sound Fitted**.....£305.96
35-226SFX - LMS 8F 2-8-0 8233 LMS Black Riveted Tender **DCC Sound Fitted Deluxe**.....£322.96

35-230 - LMS 8F 2-8-0 48305 BR Black Early Emblem Welded Tender.....£212.46
35-230SF - LMS 8F 2-8-0 48305 BR Black Early Emblem Welded Tender **DCC Sound Fitted**.....£305.46
35-230SFX - LMS 8F 2-8-0 48305 BR Black Early Emblem Welded Tender **DCC Sound Fitted Deluxe**.....£322.96

35-231 - LMS 8F 2-8-0 48703 BR Black Late Crest Welded Tender.....£212.46
35-231SF - LMS 8F 2-8-0 48703 BR Black Late Crest Welded Tender **DCC Sound Fitted**.....£305.46
35-231SFX - LMS 8F 2-8-0 48703 BR Black Late Crest Welded Tender **DCC Sound Fitted Deluxe**.....£322.96

35-235 - LMS 8F 2-8-0 48339 BR Black Late Crest Riveted Tender Weathered.....£220.96
35-235SF - LMS 8F 2-8-0 48339 BR Black Late Crest Riveted Tender Weathered **DCC Sound Fitted**.....£314.46
35-235SFX - LMS 8F 2-8-0 48339 BR Black Late Crest Riveted Tender Weathered **DCC Sound Fitted Deluxe**.....£331.46

OO Gauge Wickham Trolley - Due Early 2026

32-993A - Wickham Type 27 Trolley Car BR Departmental Yellow Wasp Stripes.....£84.96

32-995 - Wickham Type 27 Trolley Car WD9033 LMR Blue.....£84.96

OO Gauge 2 HAP - Due October

31-393 - Class 414 2 HAP 2 Car EMU 6079 BR Green Small Yellow Panels.....£254.96
31-393SF - Class 414 2 HAP 2 Car EMU 6079 BR Green Small Yellow Panels **DCC Sound Fitted**.....£348.46

EFE Rail
Great British Model Railways

OO Gauge NAAs - Due September

E86040 - NAA Propelling Control Vehicle 94331 Rail Express Systems Royal Mail.....£63.71

E86019 - NAA Propelling Control Vehicle 95301 EWS.....£63.71

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OO Gauge Card Kits

PO205 - Low Relief Pub & Shops.....£12.75
PO206 - Low Relief Cinema & Shops.....£10.20
PO210 - Paving Slabs.....£5.95
PO216 - Platform Kit.....£12.75
PO226 - Parish Church.....£16.15
PO228 - Coaching Inn.....£11.05
PO232 - Goods Shed.....£13.20
PO233 - Signal Box.....£9.80
PO235 - Stone Platform.....£12.75
PO236 - Footbridge.....£11.50
PO237 - Country Station.....£15.30
PO240 - Double Track Viaduct Brick.....£15.30
PO241 - Double Track Viaduct Stone.....£15.30
PO242 - Double Tunnel Entrance.....£10.20
PO243 - Single Tunnel Entrance.....£9.35
PO244 - Retaining Walls Red Brick.....£13.20
PO245 - Retaining walls Stone.....£13.20
PO246 - Railway Bridge Brick Double or Single Track.....£14.05
PO247 - Railway Bridge Brick Double or Single Track.....£14.05
PO248 - Tapered Retaining Walls Brick.....£13.20
PO249 - Tapered Retaining Walls Stone.....£13.20
PO250 - Manor Farm House.....£10.20
PO251 - Manor Farm Barn.....£9.80
PO252 - Manor Farm Buildings.....£11.05
PO253 - Village School.....£10.20
PO254 - Village Shop & Café.....£11.90
PO255 - Workers Cottages.....£12.75
PO257 - Grange Cottage.....£11.05
PO258 - Gardeners Cottage.....£9.35
PO259 - Crofters Cottage.....£8.50
PO261 - Red Brick Terraced Houses.....£11.05
PO262 - Stone Terraced Houses.....£11.05
PO263 - Red Brick Corner Shop.....£11.90
PO264 - Stone Corner Shop.....£11.90
PO267 - Town End Cottage.....£12.75
PO271 - Low Relief Bank and Shops.....£11.50
PO272 - Low Relief Shop Fronts Red Brick.....£12.75
PO275 - Low Relief Stone Terraced House Fronts.....£11.05
PO276 - Low Relief Brick Terraced Houses Backs.....£11.05
PO277 - Low Relief Stone Terraced Houses Backs.....£11.05
PO279 - Low Relief Department Store.....£13.60
PO281 - Service Station.....£13.60
PO282 - Full or Low Relief Warehouse.....£13.20
PO283 - Small Factory.....£13.20
PO284 - Boiler House with Chimney and Factory Entrance.....£13.20
PO285 - Industrial Unit.....£11.50
PO286 - Ramshackle Workshop.....£12.75
PO287 - Old Factory.....£15.30
PO288 - Brewery.....£16.15
PO289 - Fire Station.....£16.60
PO291 - Castle Gatehouse.....£17.00
PO292 - Watch Tower.....£5.95
PO293 - Castle Curtain Walls.....£10.20
PO294 - Castle Hall.....£16.60
PO295 - Castle Stonework Sheets.....£4.95
PO296 - Castle Bridge.....£9.35
PO300 - Brick Terraced Houses.....£14.90
PO301 - Stone Terraced Houses.....£14.90
PO302 - Low Relief Brick Terraced House Fronts.....£13.60
PO303 - Low Relief Stone Terraced House Fronts.....£13.60
PO304 - Low Relief Brick Terraced House Backs.....£16.15
PO305 - Low Relief Stone Terraced House Backs.....£16.15
PO306 - Low Relief Brick Shop Fronts.....£16.15
PO307 - Low Relief Stone Shop Fronts.....£16.15
PO308 - Brick Corner Shop Or Pub.....£16.15
PO309 - Stone Corner Shop Or Pub.....£16.15
PO313 - Track Engine Shed.....£21.25
PO320 - Mainline Station Booking Hall.....£21.25
PO321 - Parcels Office.....£19.55
PO322 - Island Platform Building.....£17.00
PO323 - Modern Platform Shelter.....£10.20
PO330 - GWR Signal Box.....£14.05
PO331 - Red Brick Single Track Engine Shed.....£14.05
PO332 - Stone Single Track Engine Shed.....£14.05
PO333 - Settle And Carlisle Railway Station.....£21.25
PO334 - Settle & Carlisle Station Shelter.....£9.80
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PO336 - S&C Station Goods Shed.....£21.25
PO337 - S&C Style Stone Engine Shed.....£21.25
PO340 - Platform Canopy.....£14.45
PO360 - Tower Block.....£14.45
PO362 - Municipal Building.....£15.30
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PO374 - Low Relief Shop Front.....£8.95
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PO400 - Platform Underpass.....£5.55
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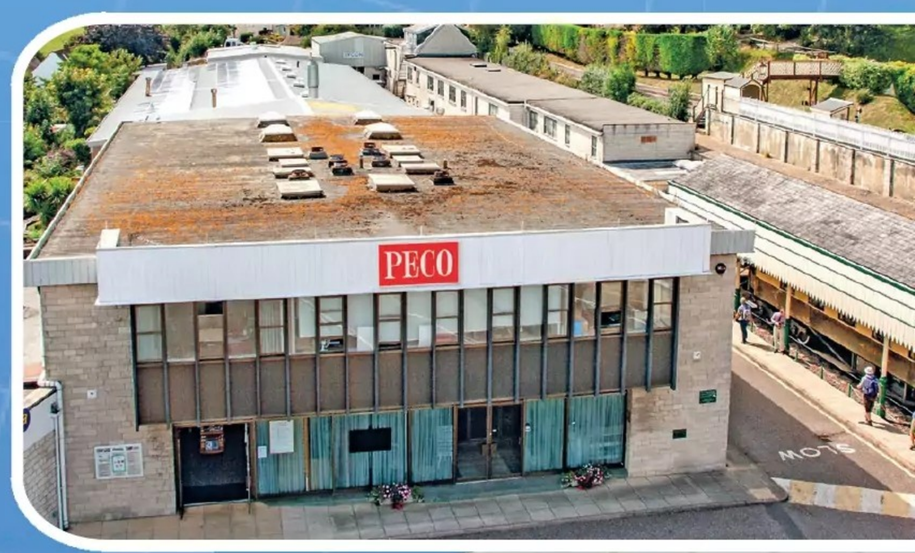

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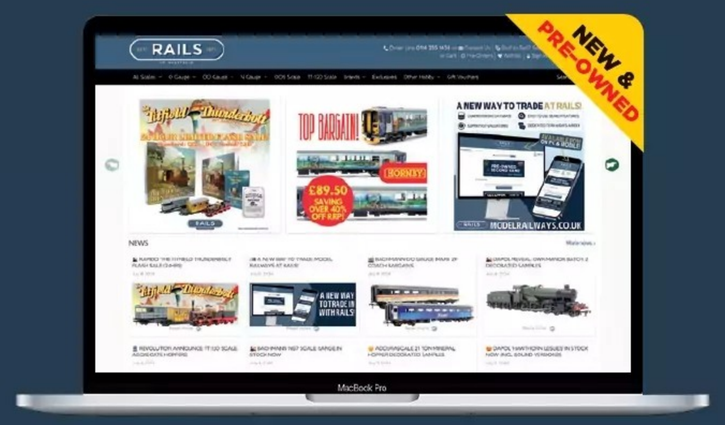

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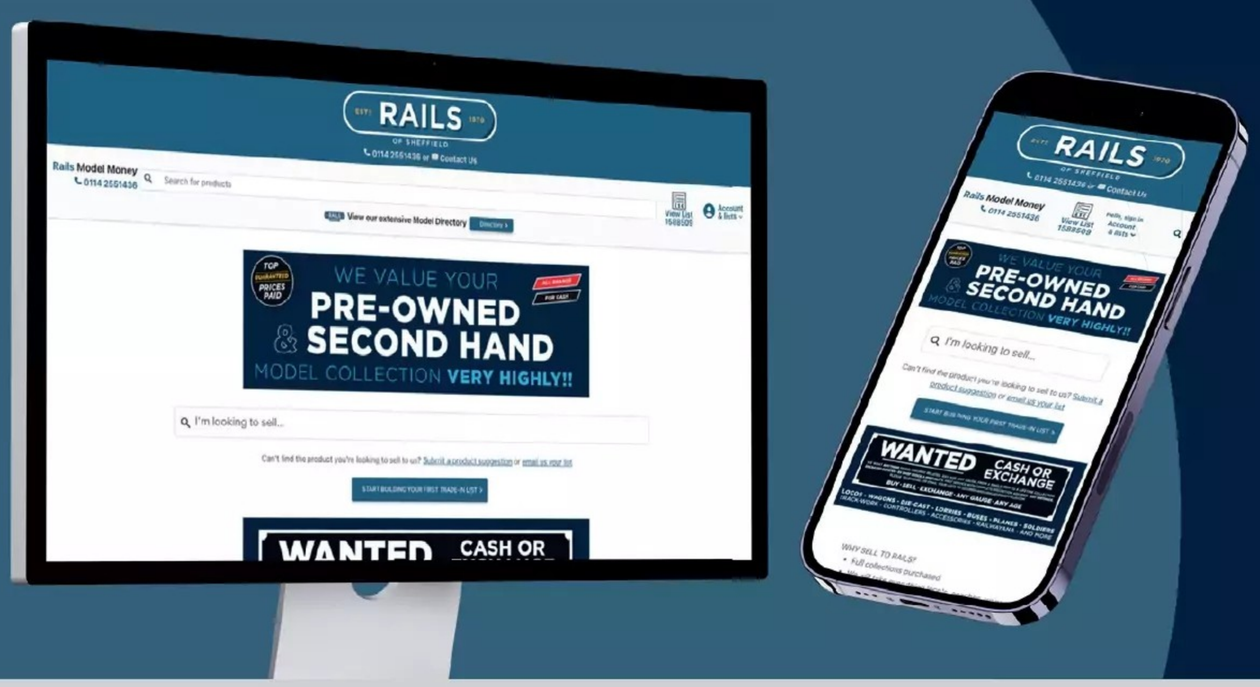

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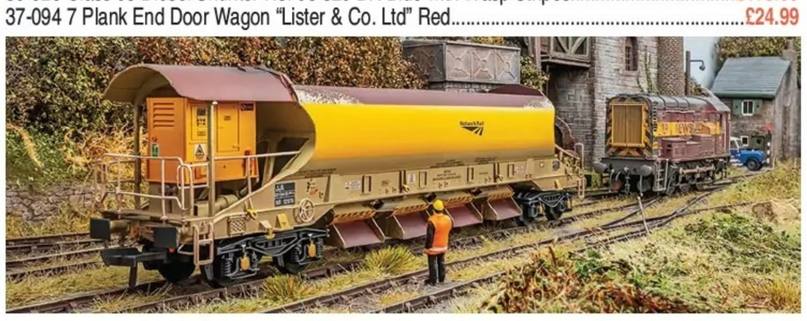

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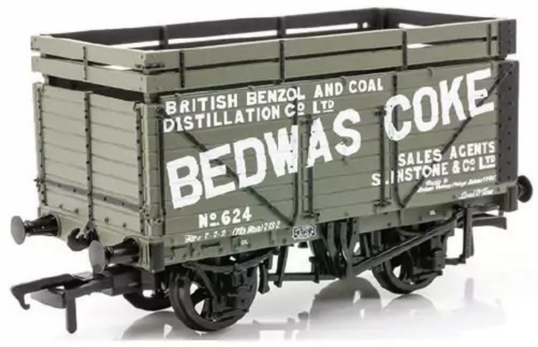

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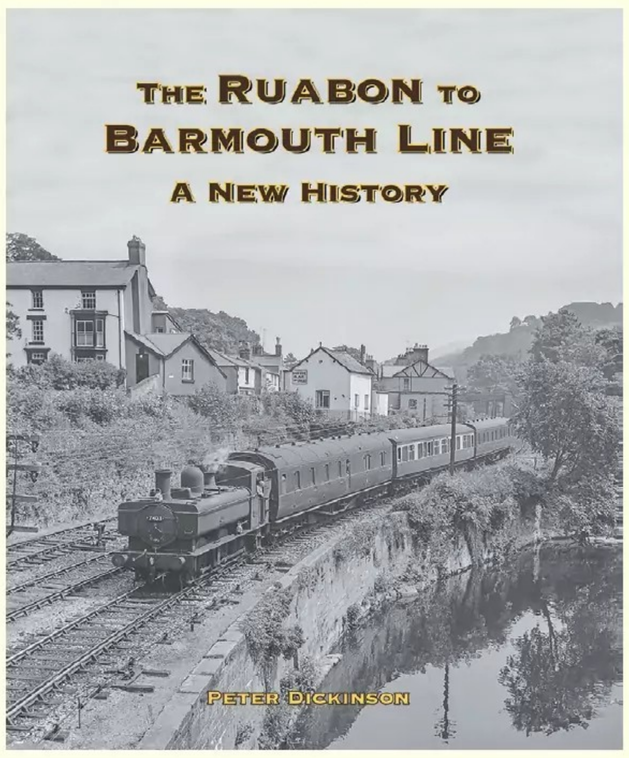

THE RUABON TO BARMOUTH LINE
A NEW HISTORY – PETER DICKINSON

For nearly 100 years, passengers could enjoy arguably one of the finest journeys in the British Isles from the comfort of their railway carriage. The Great Western Railway described the journey from Ruabon to Barmouth as 'a paradise for artists and fishermen and a country rich in mountain streams, wild woods and wide far views, unbeaten in any part of Wales'. The 54-mile route carried generations of holidaymakers to the beaches of the Cambrian Coast, as well as providing a vital lifeline to the rural communities it served. Branching away from the Chester to Shrewsbury route just south of Ruabon, the railway served the historic towns of Llangollen, Corwen, Bala and Dolgellau, before reaching the famous Barmouth Bridge and the holiday resort of Barmouth. It was a line of contrasts, from the industrial environs of Ruabon in the east, to the rugged coastline in the west. It was also busy with holidaymakers and tourists during the summer months, but painfully quiet during the winter. The story of the Ruabon to Barmouth line begins in the mid-19th century when the townsfolk of Llangollen longed for a connection to the rapidly-expanding railway network. The Vale of Llangollen Railway Company opened a 5-mile branch to passenger traffic in 1863, promoted by the Great Western Railway who leapt at the opportunity to tap deeper into North Wales. Over the next decade, the railway extended ever westwards until it reached the county town of Dolgellau and the metals of the rival Cambrian Railway. The uneasy relationship between the two railway companies continued into the 20th century and was only ended through the Cambrian's amalgamation in the 1920s. Two sections of the Ruabon to Barmouth line have reopened as heritage railways; meaning that the sylvan beauty of the Dee Valley can once again be enjoyed from the carriage window on the Llangollen Railway, and the lapping waters and rugged shoreline of Llyn Tegid on the Bala Lake Railway. The author, who has been closely associated with the preserved Llangollen Railway for many years, has produced a detailed account of the Ruabon to Barmouth line's history, looking at all aspects of its traffic and operation form the earliest days right up to closure. The stories of the preserved Llangollen Railway and Bala Lake Railway have been well documented, so no attempt has been made to take this story on beyond track lifting in 1969. The book is illustrated with a wealth of photographs, the majority of which have not previously been published, as well as many items of paperwork from the author's own archives. **DUE END OF SEPTEMBER**

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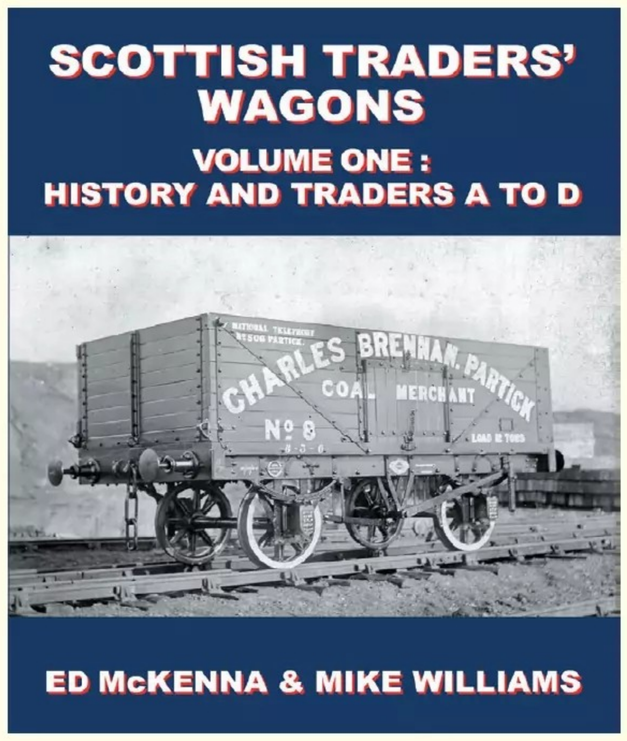
Over 50,000 traders' wagons were registered by the Scottish railway companies post-1889, and many more built prior to registration remained in traffic into the 20th Century. In this and two forthcoming volumes an attempt has been made to identify every Scottish user of traders' wagons in the period c1845 to 1948. This volume opens with general information about the Scottish traders, their often-fraught relationship with the railway companies and the legal and regulatory context under which they operated, which differed from elsewhere in the UK. The next chapter provides information about the builders who supplied the wagons, sources of finance, the geographical distribution of the wagons and their use. The wagons registered with each Scottish railway company are then discussed up to the Grouping in 1923 and through to nationalisation. The third chapter gives technical information on the wagons themselves from the earliest days through to the specifications published by the Railway Clearing House (RCH). It reveals that the Caledonian and North British issued their own 'approved designs' in lieu of the first specification and highlights some details which mark out a wagon as built for a Scottish trader. The remaining four chapters begin an alphabetical directory of traders, which will continue over the other two volumes. Coal merchants ranged from local one-man, and sometimes one-woman, bands to large concerns owning many wagons and trading from multiple locations. Wagon hire was a particular characteristic of this group, which meant that a wagon could carry up to three or sometimes more liveries during its working life. Coal and ironmasters owned collieries, iron ore deposits and blast furnaces, and they often exported coal. Their wagon fleets were larger; the Fife Coal Company, the biggest of them all, owned more than 7,000 wagons over the years as a result of business takeovers. The directory also deals with papermakers, textile manufacturers, oil and chemical traders, brewers and distillers, as well as municipal undertakings such as gasworks and quarries. Finally, a comprehensive list of primary source material is provided, in the hope that others may be stimulated to research this fascinating and very complicated subject. **DUE END OF SEPTEMBER**



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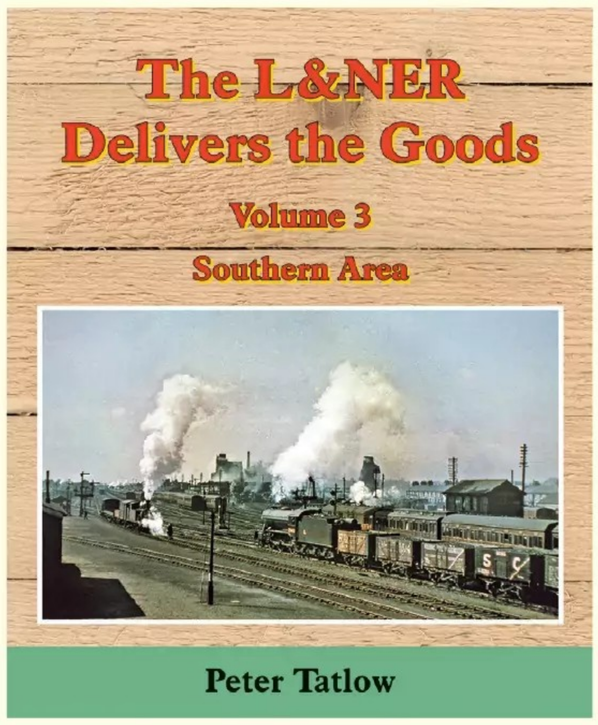

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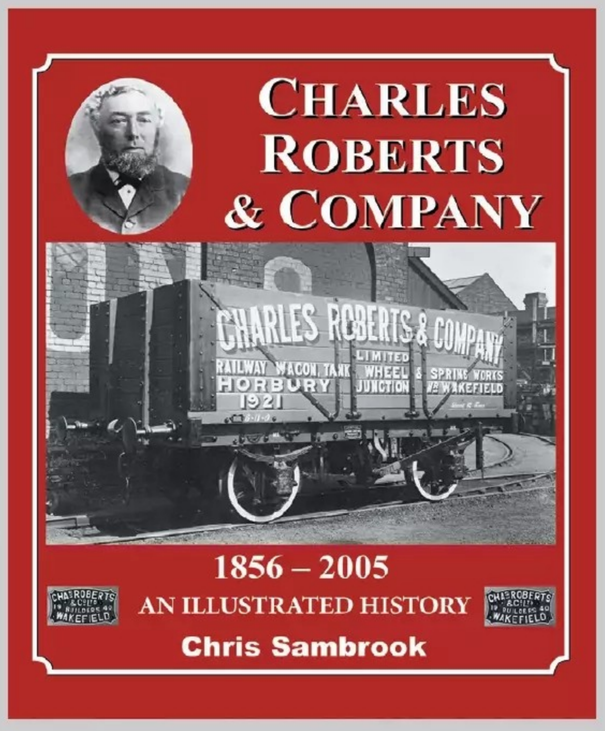

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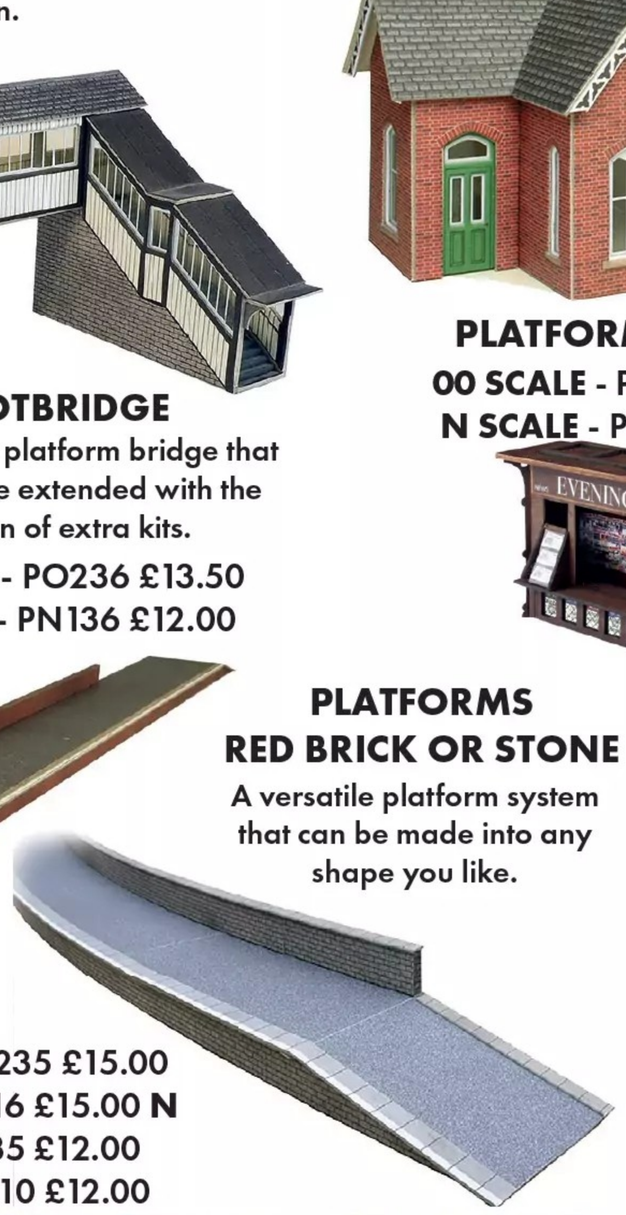

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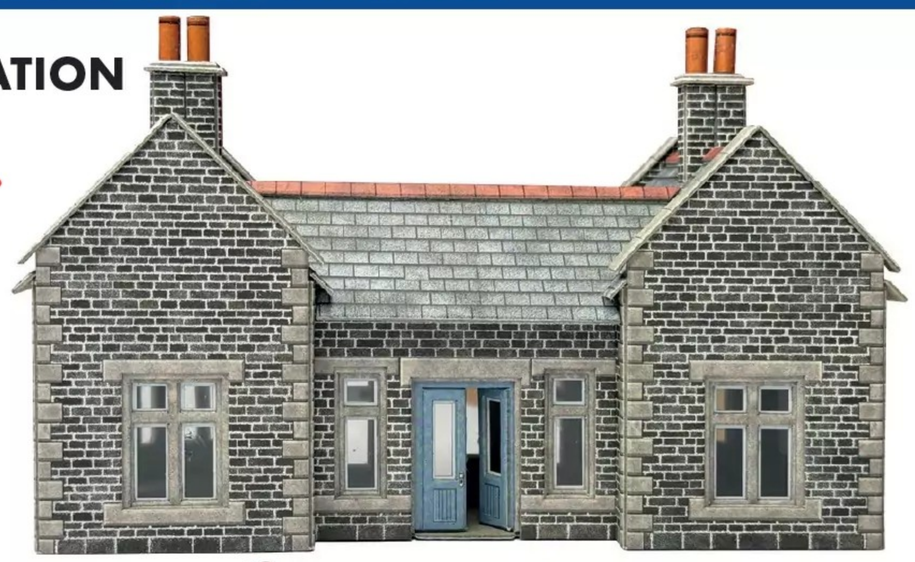

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Wessex Trains 31601 *The Mayor of Casterbridge* at Bradford on Avon with 2V74, the 11.00 Weymouth-Bristol Temple Meads on October 25, 2004. MARK FEW

Undoubtably the most daring and vibrant privatisation livery applied to a locomotive and rolling stock - here's a memorable train for 4mm:1ft scale / OO gauge that'll embolden your collection!

Cause a stir and brighten-up your model landscape with our exclusive miniatures of Class 31, 31601 *The Mayor of Casterbridge* and accompanying Mk. 2F coaches in the bright, hot pink colours of the Wessex Trains franchise.

With 'Pink Panther' levels of suaveness, these all-new exclusives for 'OO' benefit from Bachmann Europe's forthcoming, further revised, cutting-edge Class 31 tooling which now features a 'Bach-up' stay-alive system and all-wheel electrical pick-up.

Owing to a shortage of multiple units, from May 2004, Wessex Trains hired Fragonset Railways to provide 'top-and-tail' motive power to a set of Mk. 2 coaches for services under the Heart of Wessex Rail Partnership, between Bristol and Weymouth.

To coincide with the launch, Fragonset's 31601 and five Mk. 2 coaches were repainted into this stunning pink livery, whipping rail enthusiasts and the railway media into a frenzy, while the covers of print media and lineside photography instantly became more colourful.

In this dazzling livery, 31601 also saw use on evening specials running to Glastonbury Festival, many spot-hire services, Network Rail Overhead Line Electrification (OLE) trains from Finsbury Park - even running light-engine as far north as Inverness to Carlisle on route learning for Serco!

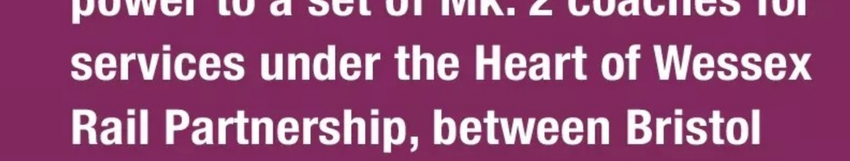
Find the brightest model releases produced for 'OO' on our website and pre-order your models today for 'early-bird' savings. Search for 'Wessex Trains' at TMC to explore our combination deals.

Be outlandish, be bold – go pink!

NEW FOR Q1 2026

31601 *The Mayor of Casterbridge*
(35-825Z) DCC ready
(35-825ZSF) DCC sound-fitted
(35-825ZSFX) DCC sound-fitted Deluxe

NEW FOR Q1 2026



Mk. 2F Tourist Second Open (TSO) 6117
(39-684X) standard
(39-684XDC) DCC onboard with lights

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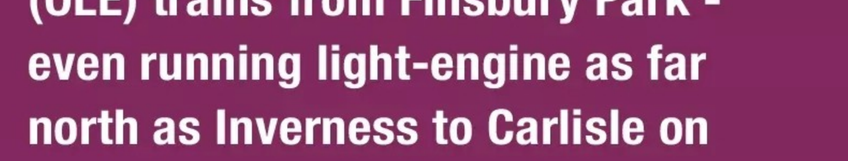
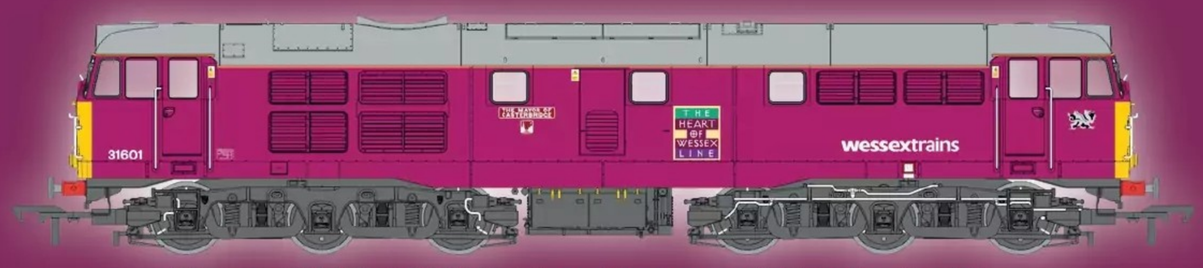


Mk. 2F Brake Second Open (BSO) 9525
(39-706Z) standard
(39-706ZDC) DCC onboard with lights

‘BEAUTY’ AND ‘THE BEAST’

Purchase 31601 *The Mayor of Casterbridge* in Wessex Trains pink livery and 31452 *Minotaur* in Fragonset Railways livery together in DCC ready, DCC sound-fitted or DCC sound-fitted ‘Deluxe’ guises (find all product codes below) and benefit from more advantageous prices on ‘early-bird’ pre-orders, all with free postage!

The pairing was a regular match for Wessex Trains services, with our Mk. 2F coaches – or even a Network Rail test train in latter years...



Class 31/6 31601 *The Mayor of Casterbridge*

Class 31/4 31452 *Minotaur*

31452 (35-825Y) + 31601 (35-825Z) DCC-ready duo: £340.00* (£199.95 RRP ea.) Save £59.90!
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(39-684ZDC) DCC onboard with lights

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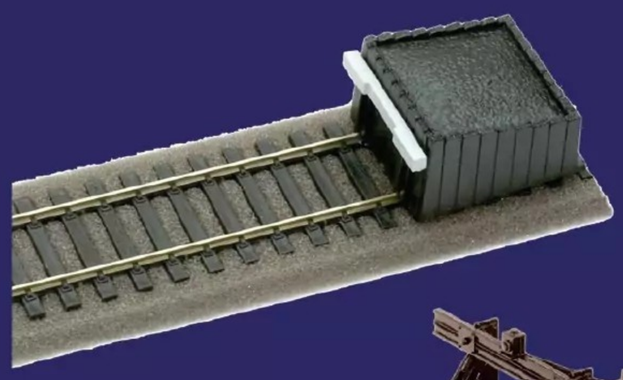

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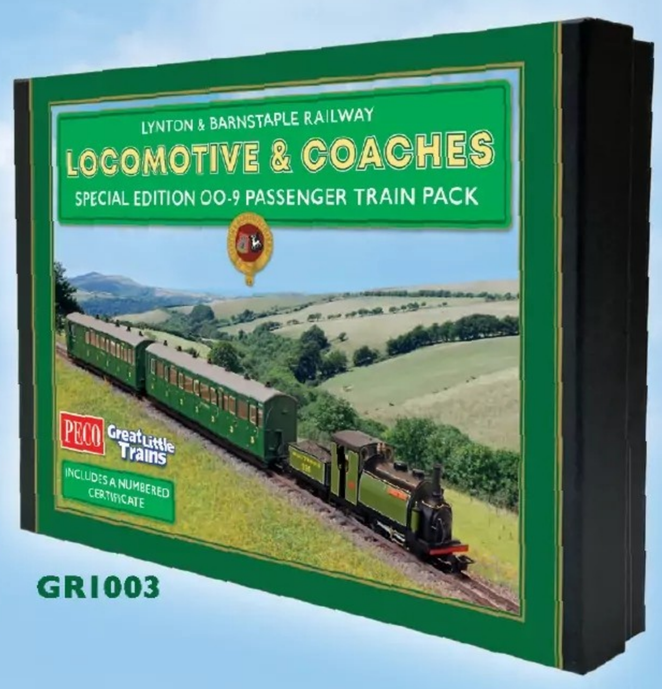

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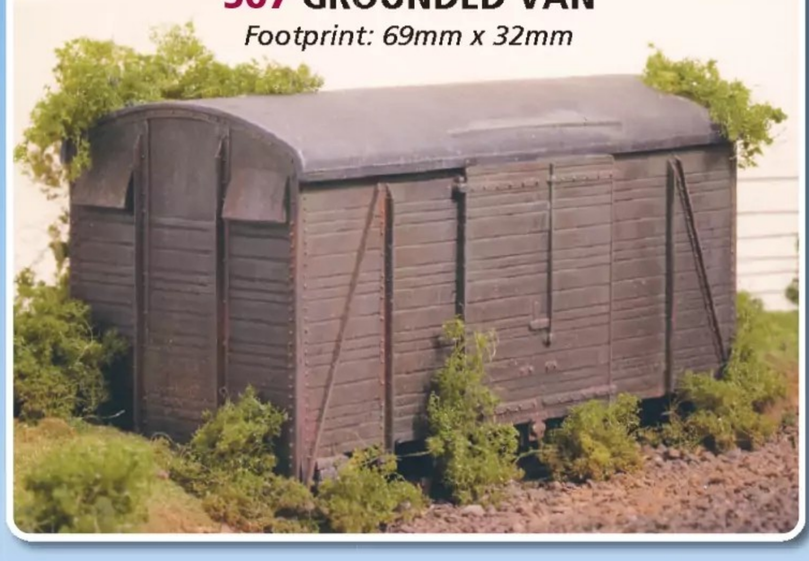

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diagrams 1/142 & 1/144

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diagram 100

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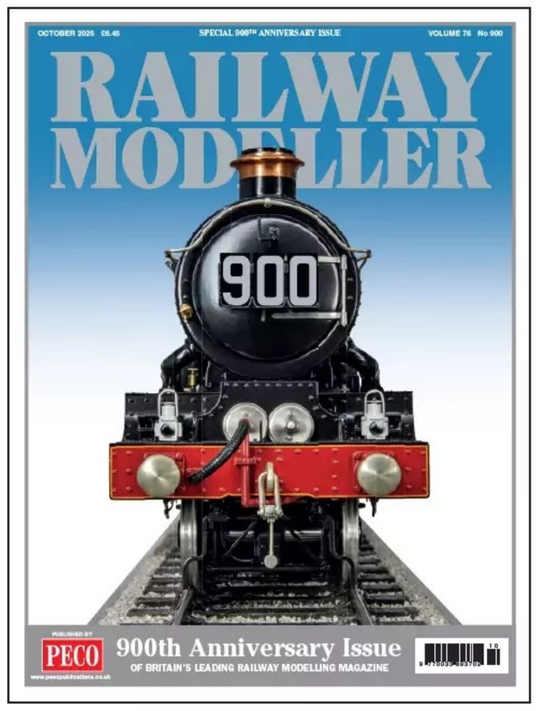
Welcome

Our 900th issue



Arriving at a notable milestone is usually cause for celebration and therefore the publication of this, the 900th edition of Railway Modeller, is not something we could let pass without a small amount of fanfare.

Reaching the publication's ninth 'century' led us, naturally, to look back at the eight that have come before, from issue 100 in February 1959, to issue 800 in June 2017. Toby Jennings takes us on a journey through each of these landmark issues with a special 'From the Archive' feature (p782). This retrospective serves to remind us how much the hobby has changed through the years spanned by these 'ton-up' issues, including technological advances such as 3D-printing, laser-cutting and digital control – all of which have opened up new opportunities for manufacturers and modellers alike. One particular area of the hobby that has seen significant change is the British ready-to-run arena: even in the eight or so years since our previous 'century' issue we have seen new players joining the fray, including the likes of Accurascale, Rapido Trains UK and Sonic Models – plus a new scale with TT:120.

I cannot help but wonder what the next 100 issues will record in terms of the continuing evolution of our hobby. Will we see Artificial Intelligence become part of railway modellers' tool kits, or radio control become a mainstream alternative to 12V dc and digital systems? We'll have to wait until RM 1000 to find out…

Whilst we have been careful to avoid indulging in too much self-congratulation, we have taken this 'historic' issue as our cue to present examples of railway modelling that each have their own history worth celebrating. We begin overleaf with *82G*, a huge 7mm shed scene that has been 24 years in the making; on p805 stalwart contributor Ian Nuttall builds an LMS Patriot from a kit that was first released nearly half a century ago; on p818 veteran modeller Nigel Adams reflects on his 77-year association with the hobby; and on p820 we find out whatever happened to *Flintfield*, a P4 project that dates from the late 1980s.

On behalf of everyone at Peco Publications, thank you to all of our valued readers, contributors and advertisers. Without your continued support we would not be celebrating this milestone in the magazine's history. Here's to the next 'century'…

Save 50p on next month's Railway Modeller

There has been a fantastic response from retailers to the new Railway Modeller promotion (launched in last month's issue), which is intended to encourage those readers who ordinarily go out to buy a printed copy of RM, to do so from their local Peco stockist. The benefit to you, our readers, is a 50p saving on the cover price (see coupon on the opposite page), whilst the benefit to model shops is an opportunity to increase footfall – it's a win-win situation! To find out if your local Peco retailer is one of those that is stocking Railway Modeller, go to:
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Craig Tiley Editor

You can keep up to date between issues by visiting our new website at www.peco-uk.com and our Facebook page. You can also follow us on X (formerly Twitter): @RailwayModeller

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And for all enthusiasts modelling overseas railways.
CONTINENTAL MODELLER
Published on the third Thursday of the preceding month.

Railway of the Month

Photography by Craig Tiley

On shed in 7mm scale

Bristol Templecombe Road – 82G

NIGEL SMITH describes this extensive model of a British Railways steam era Motive Power Depot, which has been developed over a period of 24 years by members of Warley Model Railway Club. It is set to be one of the main exhibits at the organisation's next annual show, which will be taking place at Statfold on 11 & 12 October.

Back in 2001, after a decade or so on the exhibition circuit with their 'Loco Clinic', a group of O gauge modellers in Warley MRC decided that they needed somewhere to exhibit and run their completed models. Being predominantly locomotive modellers, what better than a layout based on a Motive Power Depot?

Many designs were considered but, in the end, a track plan based on Bournemouth shed was selected. We used the basic track layout, modified it slightly to fit the space, and started work in April 2002. The idea was to create a realistic diorama that could be exhibited easily.

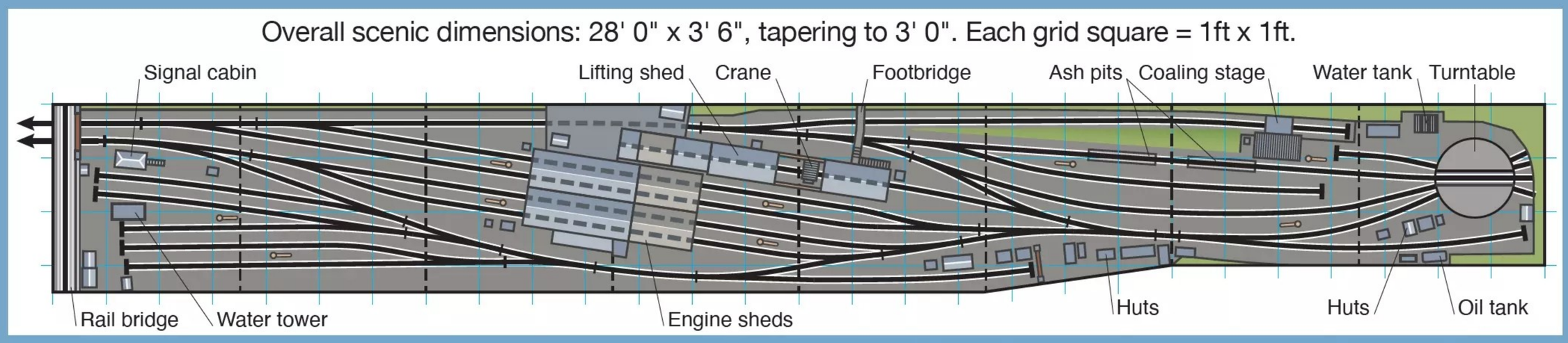
We came up with the idea of naming it after an appropriate shed code. 82G seemed to fit, so a layout was born.

Southern influence

Our modelling interests predominantly centre on Western, London Midland and Southern Region steam between Nationalisation and the early to mid 1960s,

Left
Moving off the turntable is Castle 4-6-0 No.5059 *Earl St Aldwyn*, a limited edition by Masterpiece Models, and weathered by Neil Podbery.

Right
Bulleid Q1 0-6-0 No.33016 was built and painted by Keith Blake from an MOK kit and weathered by Nigel Smith.



hence the preponderance of motive power in British Railways liveries.

At exhibitions, the scene transitions through the day, progressing slowly from pre-Nationalisation, through the early BR period, then incorporating the introduction of diesels into the BR portfolio, concluding with the decline and end of steam, and full dieselisation.

Those who remember the shed at one end of Bournemouth Central station will recognise the lifting shed and crane, which was easily viewable from the station platforms.

The original team comprised John Edwards, myself and Peter Silvester, later joined by Keith Blake and Andrew Sturmey.

Like many layouts, the project developed over time. At first there were five scenic boards, the end of the layout being in the middle of the loco shed. This was fine until the loco stud grew and a couple more people joined us. So we decided to extend the layout and add the other half of the shed and some new storage sidings. Three

Right
The layout's track plan is based on Bournemouth shed, pictured in the 1960s with (left to right) rebuilt Battle of Britain No.34060 *25 Squadron*, BR 4MT 2-6-4T No.80085 and West Country No.34006 *Bude*. An unidentified BR 4MT 2-6-0 is underneath the crane also modelled on the layout. *Photo: Mike Morant collection*

Below
Modified Hall No.7918 *Rhose Wood Hall* was built by Nigel Smith from an Acorn kit, with heavy modifications including a scratch-built boiler. Painted by Ian Rathbone, it was weathered by Neil Podbery.

new scenic boards were built for what is now the left-hand side of the layout, with track laid and buildings constructed.

The main shed buildings are completely scratch-built and based on standard four-road engine sheds at many locations. The brick walls are MDF, faced in various thicknesses of ply and then covered in plasticard. The main shed building has fully detailed roof trusses, smoke hoods, lighting and its own stationary steam engine. A full machine shop is installed, as is the locomotive roster on one of the walls.

The lifting shed and crane are constructed from brass sections. No drawings were available, so Pete drew up his own plans by scaling dimensions from photographs and researching the prototype. Likewise, the lifting crane was built from brass section. This is based on the one at Bournemouth, and hopefully we have captured the essence of a busy shed and repair facility in steam days.

The area of hardstanding outside the shed is brickwork scribed into filler, which was then painted and weathered. We wanted the whole of this scene to be instantly recognisable as Bournemouth.

Digital changeover

We exhibited the extended layout a number of times and it was well received. However, we wanted to gauge reaction if we changed the operation of the layout from 12V dc analogue to Digital Command Control with sound. At the York show some years ago, for the last hour of each day we set up a DCC system. The reaction from visitors was marked, and so a decision was made that we should go fully digital and equip the locos with sound.

The Gauge O Guild was very supportive when we made the transition from analogue to DCC. We were later invited to take the layout to Guildex (the Guild's flagship annual model railway exhibition), to show it for the first time in its full DCC format.

The chosen operating system was one marketed by Digitrax. We chose this as many friends already had it on their layouts and were familiar with it. In the main it's proven reliable, although there have been instances at two shows where we have experienced problems with interference from other exhibits.

Left
USA 0-6-0T No.30069 is an old Acorn/Harris kit, built and painted by Keith Blake and weathered by Nigel Smith.

Right
Many large engine sheds in steam days had their own stationary boilers, providing a hot water supply for locomotive boiler washouts.

Below
Two footplatemen arrive for their turns, one of whom is lucky enough to afford a second-hand but well-kept Austin A30.

Expanded coaling facilities

The layout was exhibited in this format for a number of years. However, with the shed having been enlarged, it seemed the coaling facilities were primitive for a depot of this size. John and Pete did some research and decided that the former coal stage at Slough would be ideal, particularly as there was a kit available from Intentio.

Pete and John constructed the coaling stage and, with them both being structural engineers, modifications were made to the kit to enhance its appearance.

While the coaling stage was being constructed, thoughts turned to the track. The rear access road to the turntable needed to be changed to an incline, to

provide the coal delivery line. This proved a bit of a headache, as the existing rear wall in this location was fixed and we didn't want to destroy large sections of the original layout. Eventually, a new incline was installed and the coal stage building was tested in its desired location – it all fitted!

Scenic changes and details

Next followed a period of changing the wiring, painting and fettling the new track, together with building and revising the scenics. We also removed the previous coaling facilities and carried out various repairs to the yard area.

While undertaking this work, we decided to improve the signalling. New signals were constructed and working signalling installed. The wiring is still being completed, but the aim is to have this finished in time for the Warley at Statfold event in October (see panel).

We are often asked about the track. Plain track is Peco bullhead flexible track with hand-built C&L points. It was all built *in situ* to fit the space available. Ballast is largely kiln-dried sand, as used on driveways. This was then painted and blended using various colours.

From the outset the layout was designed for display at exhibitions. The boards are manageable sizes and bolted together in pairs for transit. (The scenic section comprises eight boards, each measuring 1070mm long and between 915 and 1070mm deep – see plan.) It's still surprising that the whole layout fills a Luton type van!

As with all larger layouts, we keep finding things we can improve and change, such as the water cranes, which were recently replaced with new items.

One thing we have decided to do in future is extend the old coal truck road to the turntable. While mentioning the turntable, we are often asked about its operation. The turntable is a Metalsmith kit, with added

Below
Beattie 2-4-0WT No.30585 is a Roxey Models kit, made and painted by Keith Blake. Battle of Britain No.34057 *Biggin Hill* is a Martin Finney kit, owned by Roger Scanlon, painted and weathered by Paul Moore. Maunsell S15 No.30827 is another of Roger's models, a Gladiator kit weathered by David Amias.

details. Underneath is a special gearbox made by ABC Gears. The deck can be turned very slowly and is lined up by eye, which is easy enough as the slow control is outstanding. The only thing we have to watch for is when someone sends a locomotive to the turntable which is a bit too large, such as a Princess Royal Pacific or LMS Beyer-Garratt!

Putting on a show

The current team comprises myself, Peter, John and Keith, with Roger Scanlon joining in more recent times. In addition, Ian Rathbone, Aaron Brown, Dan Haines, Fred Lewis, Peter Stamper, Kevin Wilson, Brian Malyon and Chris Field have all contributed to the project.

The whole purpose of the layout is to display a collection of locomotives owned by the team. We are able to amass an interesting display from three of the Big Four companies – it is only the LNER for which we have relatively few examples available for exhibitions.

82G is a fairly large layout. Ideally it needs eight operators at shows, which does limit where it can be exhibited. However, we aim to put on a good show and it remains available for exhibitions going forward.

See it at Statfold

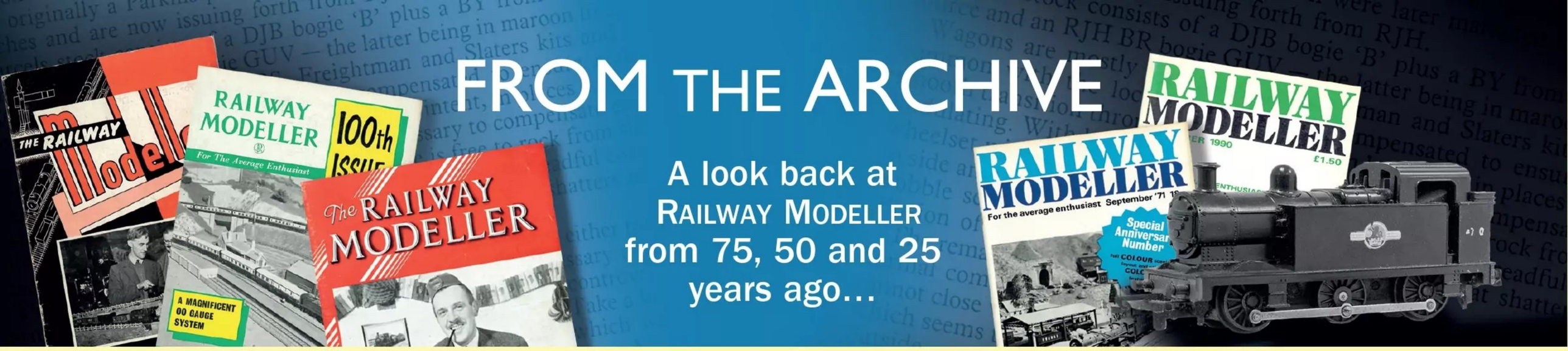
Warley Model Railway Club has once again joined forces with Statfold Narrow Gauge Museum Trust to stage a model railway exhibition at the Statfold Country Park near Tamworth over the weekend of 11 & 12 October 2025. *82G* will be one of 33 layouts in action – see Societies & Clubs for further details, or go to: www.thewarleyshow.co.uk

Top left
Patriot No.45513 is a Gladiator kit, built and painted by Ian Rathbone with a 'shiny dirty' weathered finish, as if cleaned with an oily rag.

Above
The Great Western coaling stage is based on the example at Slough.

Left
Rebuilt Merchant Navy No.35014 *Nederland Line* and Schools Class No.30926 *Repton* are both built from David Andrews kits.

Right
GWR '2800' No.2881 is a Martin Finney kit, painted by John Edwards and weathered by Fred Lewis.



FROM THE ARCHIVE

A look back at RAILWAY MODELLER from 75, 50 and 25 years ago…

In a special instalment of this feature for the 900th issue, **TOBY JENNINGS** *takes an extended journey into the archives to look back at every other edition that marked a 'century'… or did they?*

During the restoration of its Great Western Railway Hall 4-6-0 in 1998, Tyseley Locomotive Works discovered, from the stamped numbers on its components, that it was not No.4983 *Albert Hall*, as had been thought ever since it was rescued from Barry scrapyard, but No.4965 *Rood Ashton Hall* – the two locomotives having exchanged identities in 1962 when they were in Swindon Works together. Later the same year, while rebuilding its Class A5 0-4-0 into original tender engine condition from its later saddle tank guise, the Furness Railway Trust similarly found that the engine was not No.18, as had been previously believed, but No. 20. Most recently, it has been suggested that ex-Barry Battle of Britain Class No.34073 *249 Squadron* may in fact be No.34054 *Lord Beaverbrook* – which, if true, would have entailed swapping not just the name and number plates, but the 8' 6" wide cab of No.34054 for the 9' wide design on No.34073…

What has all this got to do with RAILWAY MODELLER? Well, the process of compiling this article has revealed a similar discrepancy that has been perpetuated for even longer: ever since 1959, the number on the cover of the magazine has not corresponded to the actual number of issues that have been published.

The lost issue

In the summer of that year, a printers' strike meant that the August edition – which would have been issue No.106 – simply never appeared, although some of the articles that had been prepared for it were incorporated into an enlarged September issue. However, that September magazine bore the issue number 107 – and so, when the milestone of the 200th issue was reached in 1967, the June edition was numbered 200 when, in fact, the July 1967 RM was the 200th to go on sale.

Ever since, the celebratory 'century' issues have thus been technically one month early: the October 1975 edition was numbered 300 when the November issue was the 300th to appear; the February 1984 issue celebrated 400 editions when the March issue was the 400th to hit the shelves; and so on right up until the issue you hold in your hands or have on your screen, which may be numbered 900 but is only the 899th to be actually published.

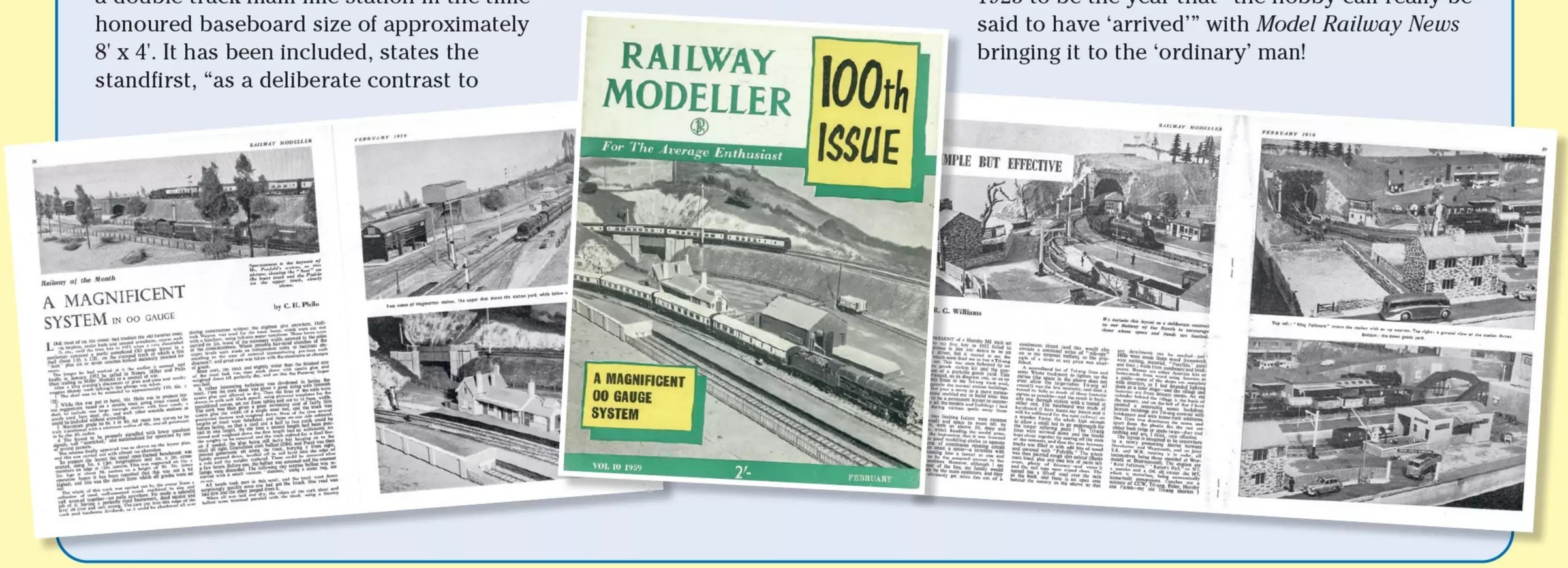
We're sure that, as far as the RM staff at the time were concerned, they'd still done the hard work of putting together that August 1959 magazine (or at least significant chunks of it) and so they would have considered that the issue number still correctly reflected those produced. We've taken the same view, so the milestone issues on these pages remain the ones numbered as such – but if you want to see the 200th, 300th and so on to appear, you can find the following months' issues in the RM Digital Archive, which is available to subscribers – details for which can be found on page 62a.

No.100 – February 1959

The 'magnificent OO gauge system' trailed on the cover of the 100th issue of RM – which is the Railway of the Month inside – is indeed the kind of layout of which many modellers dream: two double track main lines on different levels (giving the impression of two converging routes) in a shed measuring 32' x 11' 6", enough for long sweeping curves giving a real sense of a main line in the countryside.

Similarly, the second layout featured is titled 'Simple but effective' and is just that: a beginner's first permanent layout, with a double track main line station in the time-honoured baseboard size of approximately 8' x 4'. It has been included, states the standfirst, "as a deliberate contrast to Railway of the Month, to encourage those whose space and funds are limited" and, in thereby showing what a newcomer can achieve, it's the sort of layout that we would today give pride of place in Railway Modelling Explored.

But perhaps the most interesting article is the special one entitled 'The Start of It All' by P D Hancock. By his reckoning, this year marks not just the 200th anniversary of the Stockton & Darlington Railway, but would be the centenary of the hobby: while model railway clubs had already appeared, he considers 1925 to be the year that "the hobby can really be said to have 'arrived'" with *Model Railway News* bringing it to the 'ordinary' man!

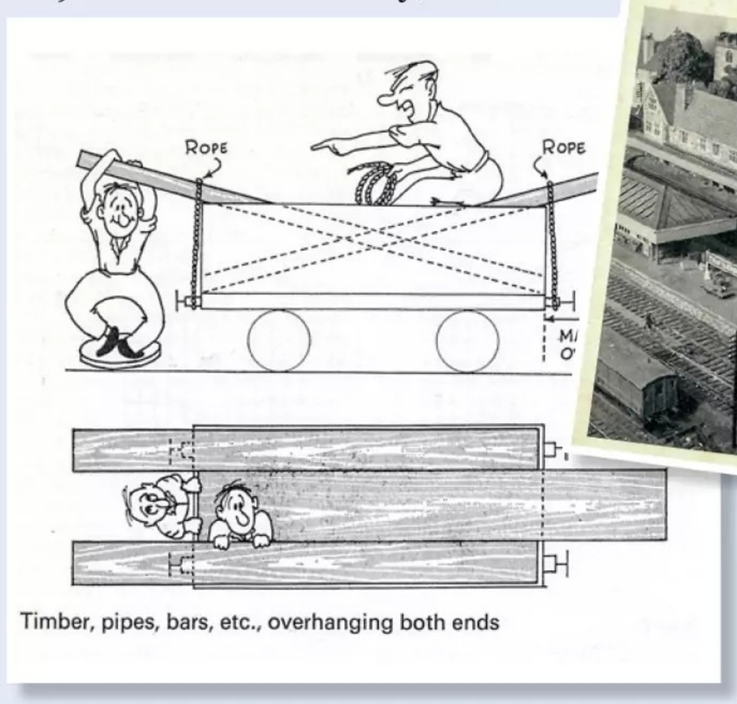


No.200 – June 1967

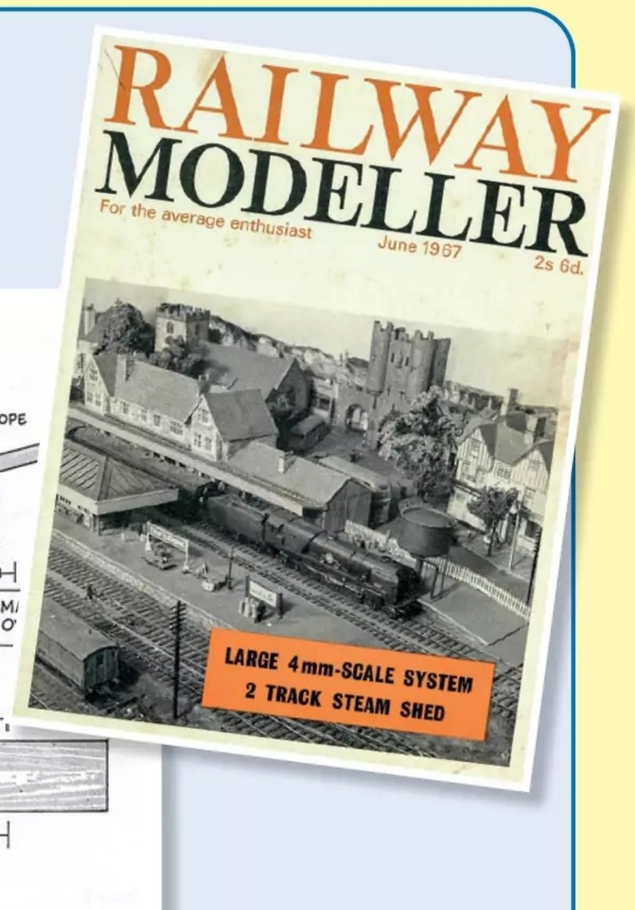
The Railway of the Month in issue 200 is another large 4mm scale main line system, even more extensive than that in the 100th edition, and all the more impressive for the fact that all its track – including some very complicated pointwork – is hand-built. This was, of course, still very much the era when stock, track and train sets from Tri-ang Hornby (as it then was) dominated the hobby, and modellers wanting a 'proper' layout still had to do some 'proper' modelling and scratch-building.

Following articles include a very useful guide to official British Railways methods of securing over-length loads in wagons (still not modelled all that often, even now); a triple bill of scale drawings depicting Great Central Railway Robinson 4-4-2Ts, Furness Railway G5 0-6-0Ts and London & South Western Railway G6 0-6-0Ts; and a steam shed layout based on Plymouth Laira as Plan of the Month.

To mark the 200th issue, Editor Cyril J Freezer adds some thoughts on how the hobby had developed in the nearly 20 years since RM had begun, many of which are still relevant today in their own way – but perhaps most of all his concluding comment: "Railway modelling is fun. Or, at least, it ought to be, because if it is not, it is meaningless. There are enough serious things in all our lives without adding to them."



Timber, pipes, bars, etc., overhanging both ends

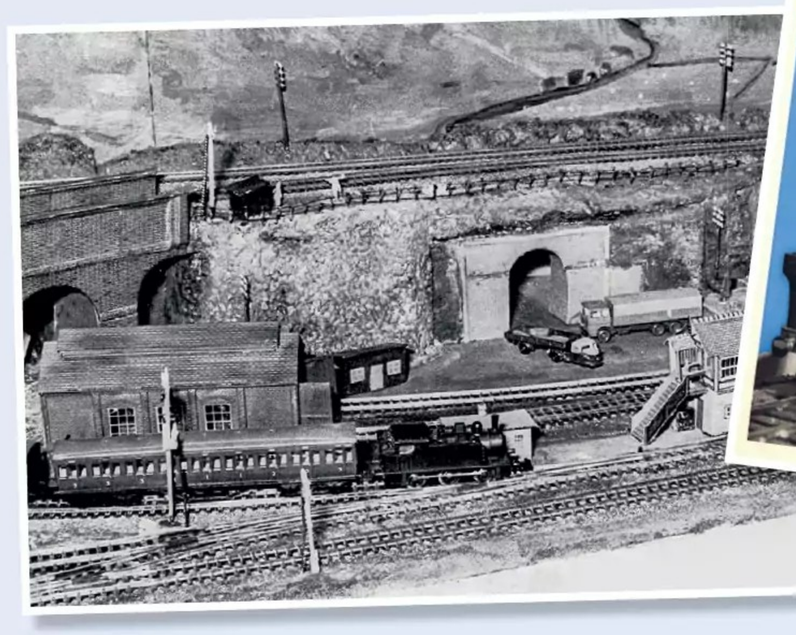


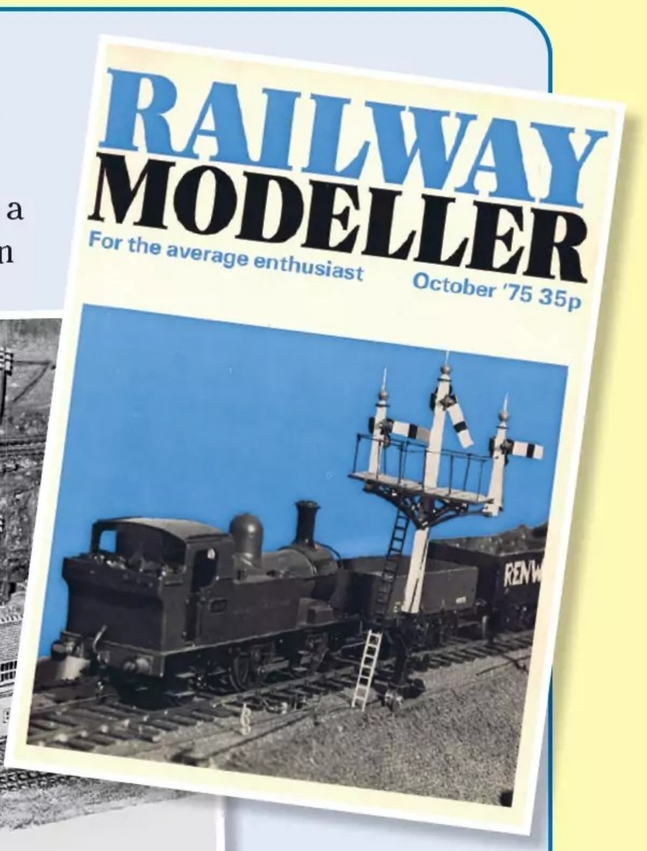
No.300 – October 1975

For this milestone issue, we provided not one but two Railways of the Month, the first being *Shelford* – an appropriately titled N gauge layout on a shelf (pictured). With N still relatively new to the hobby at this time, it showed the possibilities that the smaller scale offers, with a realistic-looking main line station and a continuous run in a space of just 8' x 14". The other ROM is a 7mm scale model of a popular Great Western branch line terminus – Kingsbridge in South Devon.

There are also two mining-themed models – Littler's Pit, described as a 'coal mine in a corner' of a layout, and a 'mine up a mountain' by Allan Downes, complete with an amusing piece on how he built the said mountain. Another trio of Scale Drawings cover an Avonside 0-6-0ST, a Taff Vale Railway goods wagon, and an experimental brake van built from concrete by the London & North Eastern Railway.

To show that good ideas for layouts have a habit of resurfacing, the Plan of the Month – *Ikenfield*, with a single track line and interchange facilities with an industrial route – seems a remarkably similar concept to our much more recent POM in the May 2023 issue (*An industrial branch line*), suggested to represent lines in the North East or the South Wales valleys.





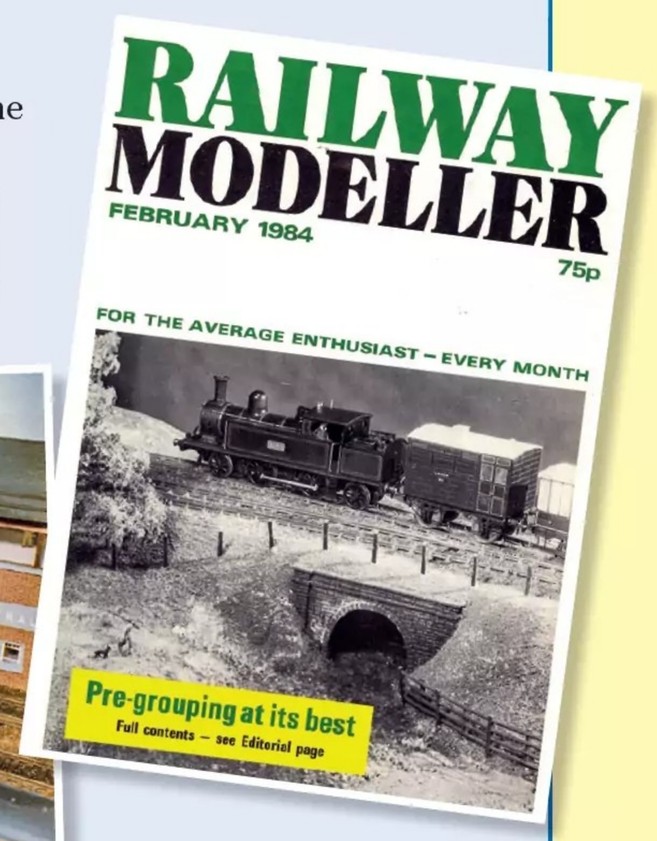
No.400 – February 1984

The Railway of the Month here is described on the cover as 'Pre-Grouping at its best': *Bevleys*, a delightful London & North Western Railway branch terminus in 4mm scale by Dave and Bev Lowery. Later in the issue, the article *Historical modelling – a simple approach* discusses some relatively easy ways to model pre-Grouping rolling stock, by modifying proprietary kits or ready-to-run models.

The next layout featured is Robert S Telford's first venture into OO9, followed later by a layout plan representing the preserved West Lancashire Light Railway – a subject that might be worth revisiting, with the growth and popularity of OO9 today?

Main line action with big engines is provided by *Farnwood Central* (pictured), a very impressive four track OO layout by J C Honeysett, recreating his memories of the Southern Region in the Hampshire/Surrey area in British Railways steam days.

The Plan of the Month strikes a local chord for us, with a scheme for an extensive layout representing Exmouth Junction and surrounding Exeter area stations including Pinhoe, Whimple and part of the Exmouth branch, set in the (then) present day. While these lines had already been rationalised a great deal since their heyday, it's remarkable in 2025 to read how much freight this section of the South Western Main Line still carried in 1984.



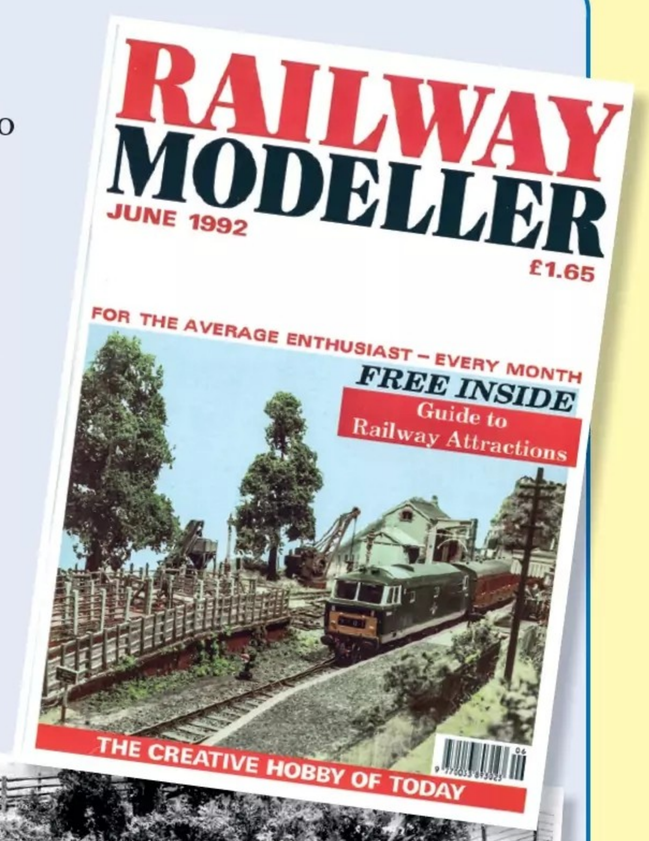
No.500 – June 1992

The editorial in issue 500 makes the prediction: "We would hazard a guess that by issue 1000 there will be precious little that would not be modellable almost 'off the shelf'…" Well, it now won't be that much longer before we find out, and it surely won't be literally fulfilled in terms of buildings and infrastructure, but as far as locomotives and rolling stock are concerned, we really are now a long way towards it. Who would have believed in 1992, for instance, that Hornby would one day see a market for the locomotive that is the subject of the Scale Drawings in this issue: A2/3 Pacific No.500 *Edward Thompson*, selected for its number, but a member of a relatively small, rather ungainly-looking class that was thoroughly disliked by many LNER enthusiasts?

In similar vein are the pieces in News regarding the launch of G scale kits for Ffestiniog Railway England 0-4-0ST+T *Prince* and Double Fairlie *Merddin Emrys*, plus an O-16.5 kit for a Fairlie – seen at the time, no doubt, as welcome developments, but who would have predicted how OO9 modelling would be taking off in the 2020s, thanks at least in part to ready-to-run Englands and Fairlies from Peco/Kato and Bachmann respectively?

The News pages also announce the release of the Peco plastic Country Station kit in OO, still a staple item in the range today (ref.LK12/LK13).

Country stations, in the form of Great Western branch line layouts, were still very much to the fore in 1992, though the example featured as Railway of the Month in this issue was a somewhat unusual example of the breed: *Priston* (pictured, below right), a 4mm scale model of a 'might-have-been' alternative to the Camerton branch, famous as the location for *The Titfield Thunderbolt*.



NEWS SPECIAL

The stop press of the model railway world

The latest first

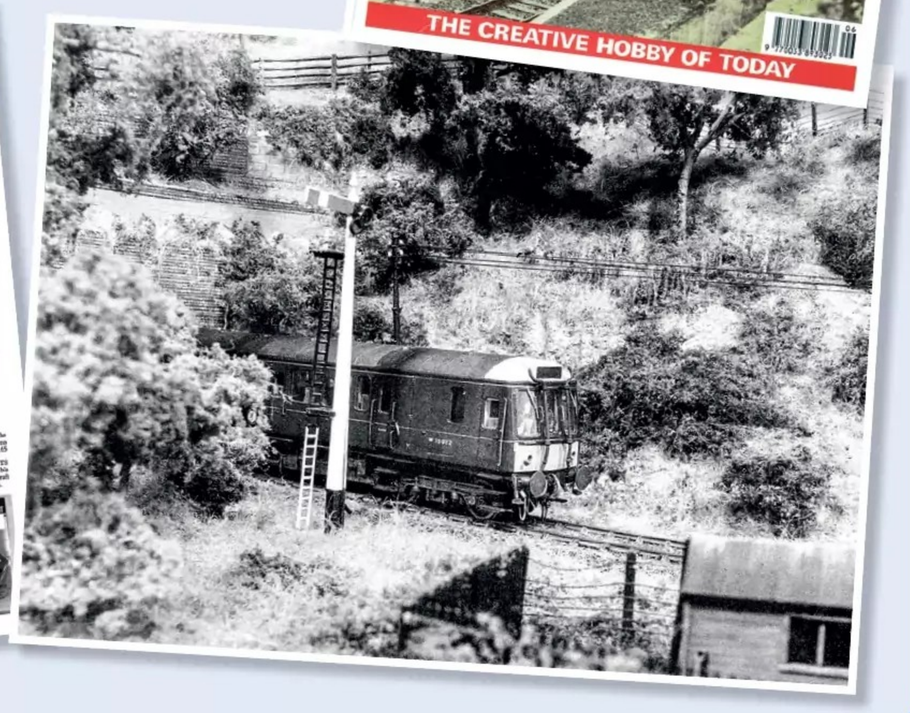
PECO slips in Code 75

Index to reports June 1992

Traditional station buildings for 4mm scale

County Donegal railcar

Locos for G scale

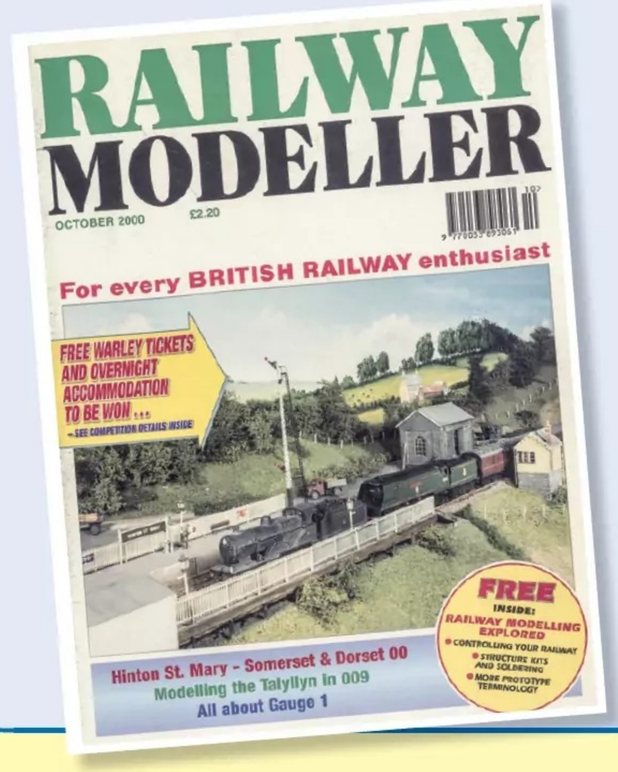


No.600 – October 2000

Do you have difficulty deciding on the scale, let alone the subject, of your next layout? Do you find yourself swayed by narrow gauge one week, industrial railways the next, but hanker after a main line layout by the end of the month? Then perhaps *Hammeston Wharf* in this issue (pictured, below right) would provide some inspiration: telling how Macclesfield Model Railway Group combined members' interests of standard gauge, narrow gauge, light railways and trams all on one relatively small 7mm scale layout, yet without making it look overly crowded, and indeed making it look so good that it's Railway of the Month.

This issue contains a bumper crop of equally varied layouts: *Hinton St Mary*, a fictional Somerset & Dorset layout in OO combining elements from several of that famous line's stations; a plan for an OO9 model of the preserved Talyllyn Railway in the 1950s; Ryde Esplanade on the Isle of Wight, modelled in N with its then stock of 1938 tube trains; and the Ashover Light Railway in OO9.

The lead review item is appropriately significant for such a milestone issue: the Hornby OO gauge rebuilt Merchant Navy (pictured above), the model that marked the company's shift from its old, relatively crude tender-drive designs to the locomotive-drive, super-detailed products which we take for granted from Hornby and other firms today.

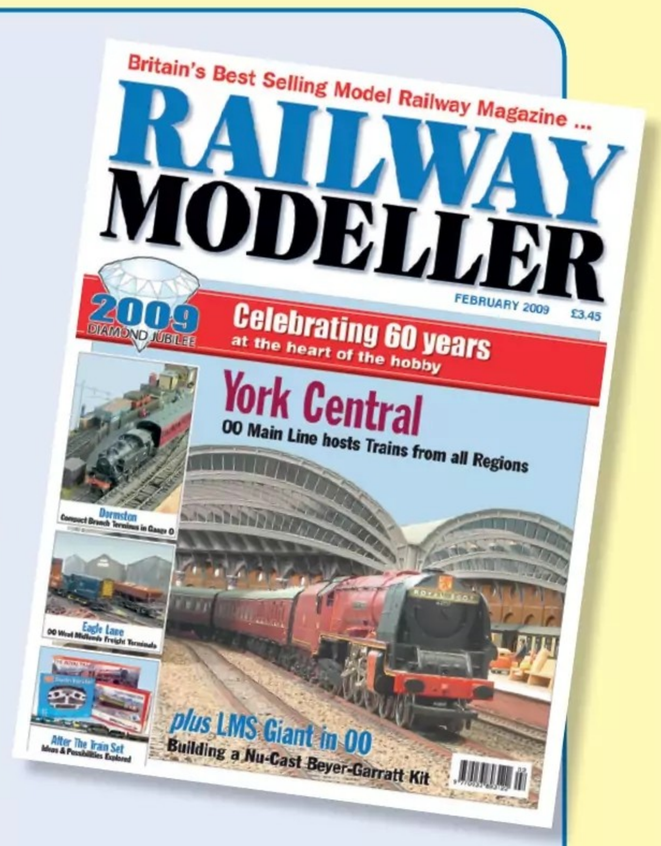


No.700 – February 2009

Much like issue 500, the editorial in this issue proved prophetic, predicting that more ready-to-run pre-Nationalisation models were "just around the corner" – and indeed, several of the 4-4-0 classes named as possibilities have appeared since, namely the Claud Hamiltons, Great Central Directors, South Eastern & Chatham D Class and Great Western 'Dukedogs'. This comment was prompted by the review of the Hornby T9 4-4-0 in OO (pictured), while another prototype of which the first R-T-R model was announced in this issue was the BR Clan Pacific, also in OO by Hornby.

The Railway of the Month is another that combines several interests: a still recognisable, but slightly fictionalised, version of York where locomotives from all the Big Four companies appear daily. Also featured, or rather revisited, is *Littleton Parkway* (pictured) – a layout that appeared in the March 1990 issue and was then set in steam days, but had now become a modern layout set in the then-present Privatised era.

Tim Rayner presents a plan for Mossley Hill station in Liverpool, which was also previously featured as a plan in the December 1962 issue – and very nearly appeared a third time! Not long after joining RM, I was inspired by a new Oakwood Press book on that route to draw up a layout plan for it, only to discover from the RM Digital Archive that it had already been done on these two occasions. If we've all seen that modelling potential decades apart, surely it would make a good layout: has anyone actually built it?



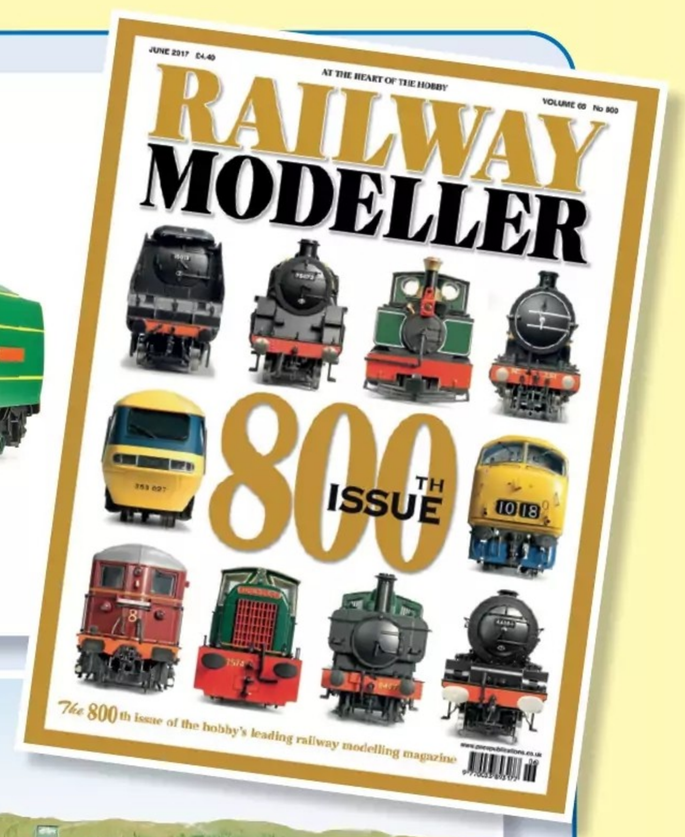
No.800 – June 2017

Given away free with the 800th edition to mark this milestone was a booklet tracing the history of RAILWAY MODELLER, but unlike this article, providing an overview of how it developed through each decade in turn. One interesting comment homed in on the "ever more vibrant cover designs to stand out" meaning that "long gone now is the single, dignified photograph of a contributor's layout" – but we have since reverted to the latter in the form of the special subscribers' covers.

Railway of the Month is a museum quality P4 model of Sidmouth by Richard Harper (pictured); I use the words 'museum quality' quite deliberately, for this layout has now ended up on permanent display in the town's museum, as reported in the March 2023 issue.

Among many other excellent layouts in this issue, another that stands out is *Tan y Llyn*, with its superb OO9 renditions of rock formations and water effects to portray the Aberglaslyn Pass on the Welsh Highland Railway.

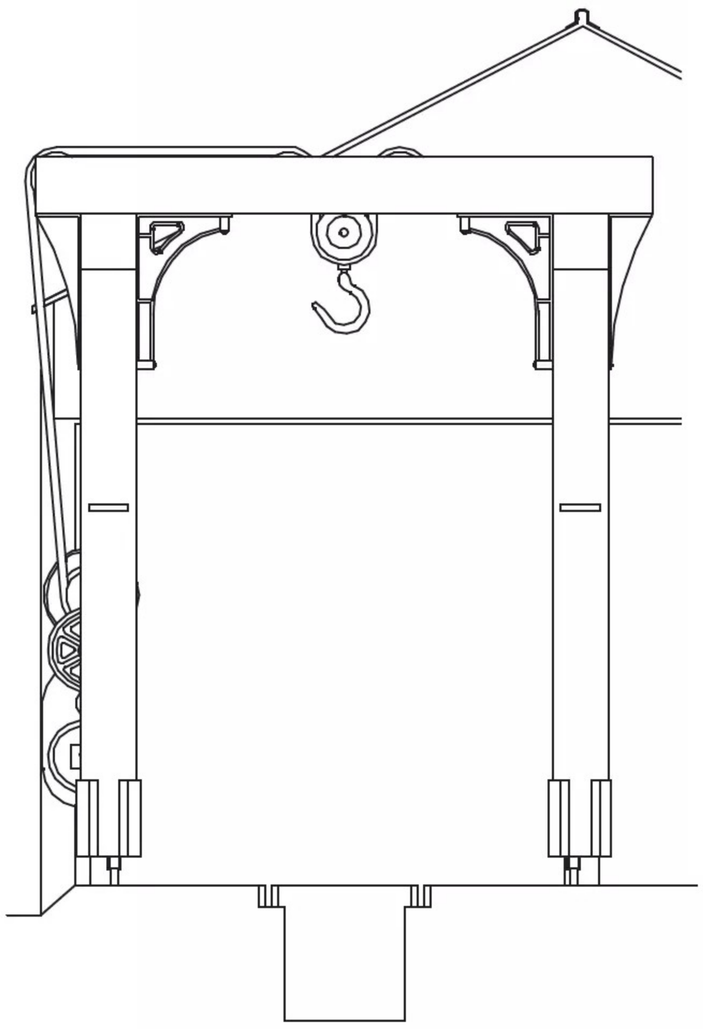
Moving on from issue 600, another new Hornby Merchant Navy is reviewed – this time the original air-smoothed version (pictured) – along with a Bachmann Somerset & Dorset 7F 2-8-0, another prototype of which a ready-to-run model would have seemed a pipe dream not so long before.



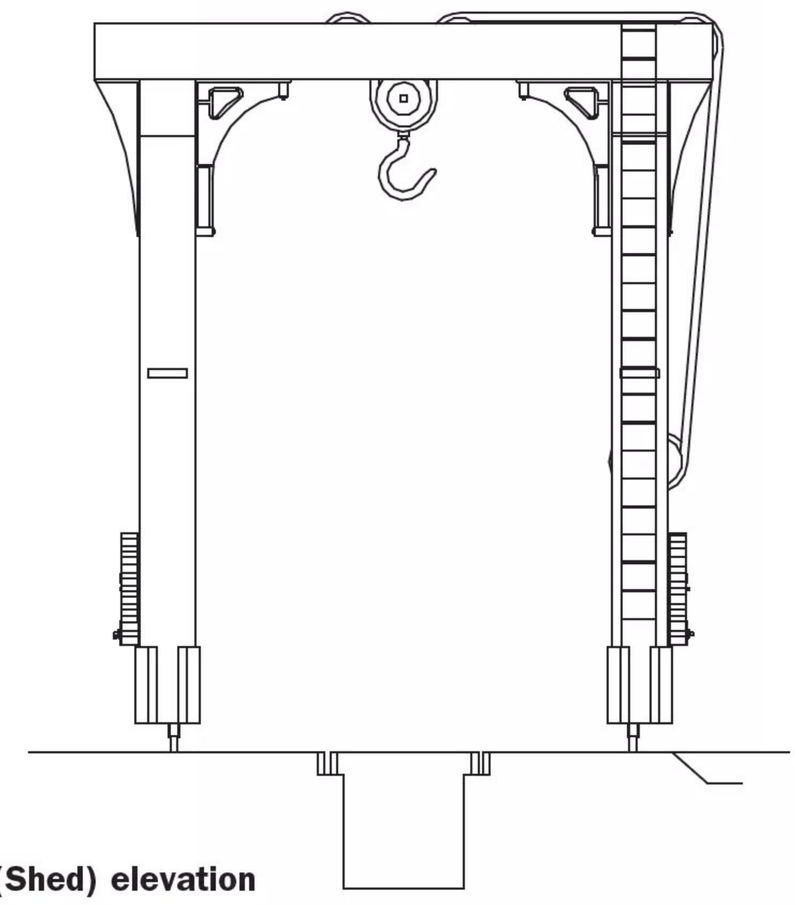
A locomotive hoist

JOHN TREAYS *provides drawings for this substantial piece of locomotive maintenance equipment, which served for many years at Wadebridge shed in Cornwall.*

Photography as credited



East elevation



West (Shed) elevation

▲ Beattie well tank No.30587 stands outside Wadebridge shed with the locomotive hoist alongside. The trio of veteran 2-4-0WTs (Nos.30585-7) were withdrawn in 1962; the polished buffer heads and 'SPL' (special) route disc evident here suggest that this scene was recorded during the latter stages of the locomotive's career. *Photo: Colour-Rail*

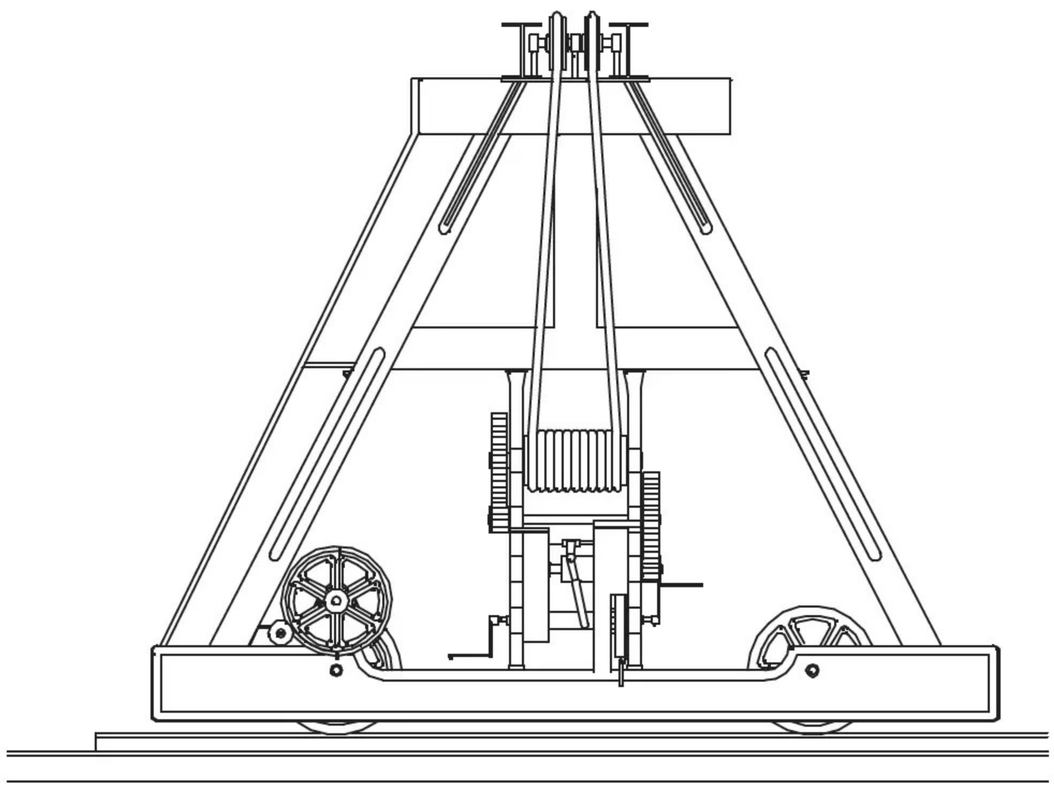
This locomotive hoist was a prominent feature of the London & South Western Railway shed at Wadebridge up until its closure in 1964. The provision of a hoist at this Cornish outpost was necessary to ensure that the staff could remain self-sufficient and carry out locomotive maintenance – the principal engineering base being located some considerable distance away at Exmouth Junction, Exeter.

Similar to other examples that could be found at locations elsewhere on the LSWR network, such as Basingstoke, the hoist was hand-operated and used to facilitate the removal of bogies and wheelsets from locomotives. The hoist was mobile with wheels on each side, running on short lengths of rail, which enabled it to accommodate locomotives of different lengths. (These rails appear on an official drawing of the site dated 1907.)

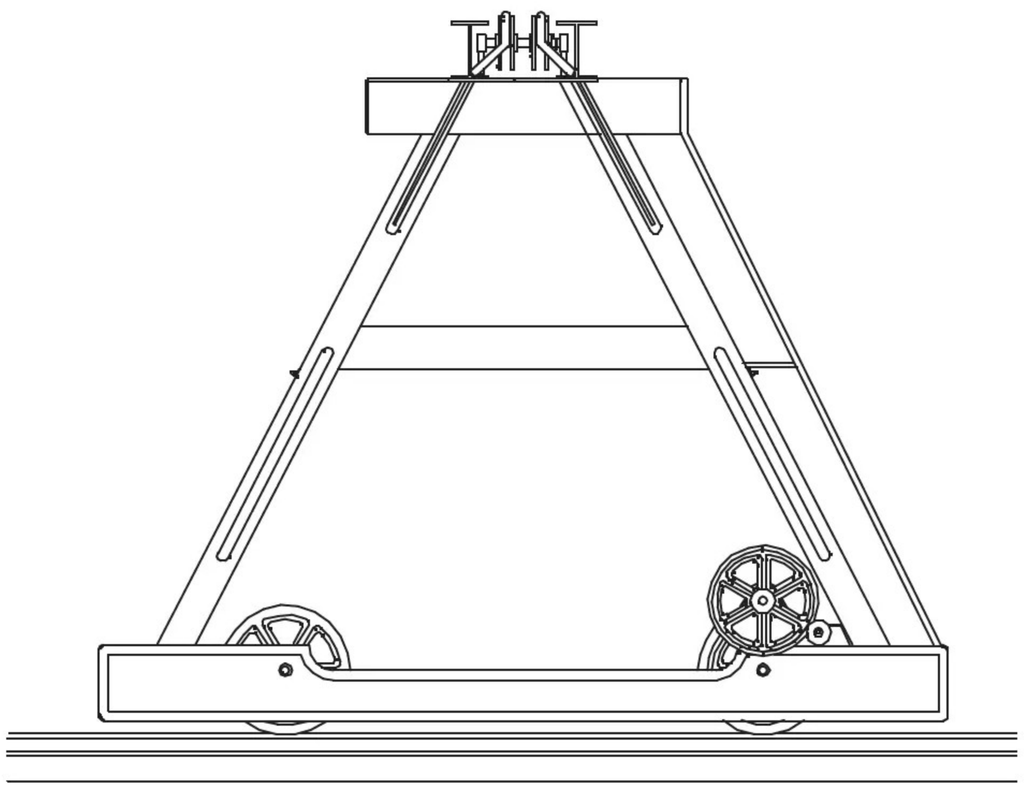
The accompanying drawings are reproduced at 3mm:ft scale and were prepared by scaling measurements from various archive photographs. Published details about the hoist are few and far between (can any readers provide further information?) but photos are plentiful in published books and online – Wadebridge shed having been popular with photographers in the early 1960s owing to its resident trio of veteran Beattie 2-4-0WTs.

No.30587 again, this time several years earlier, on 29 May 1950. This photo affords a clear side view of the locomotive hoist, showing the hand-operated winding mechanism.
Photo: Rail-Online

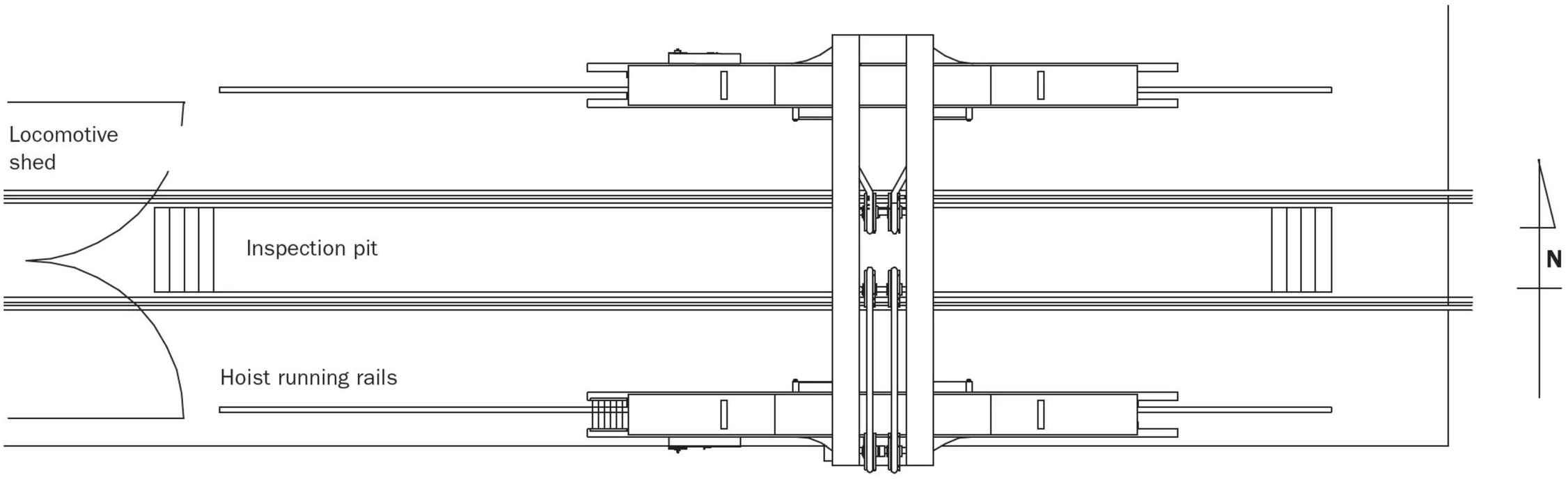
Wadebridge shed locomotive hoist – drawings published at 3mm:ft scale



Side elevation (South)



Side elevation (North)



Plan

The Harboro Stone Company

Modelling, words and photography: **DAVE HALL**

This isn't the first layout that I have built portraying the quarrying and stone products industry: some readers may recall *White Peak Limestone & Tarmacadam Ltd*, which featured in the January 2020 edition of RM. At the end of that article, I said that I would let RM readers know how I got on with the alterations that I had planned for the layout, and so here I am.

The truth is that I never did make the alterations and the layout never did appear at any further exhibitions. I sold it to my friend Scott Martin, and went on to build two more layouts before returning to the same geographical area and industry with a new layout.

Fact and fiction

As far as I know, no firm named The Harboro Stone Company ever existed, but looking at an Ordnance Survey map for a location where such an industrial railway could have been sited revealed a place called Harboro Rocks, alongside the route of the former Cromford & High Peak Railway, between Middleton Top and Brassington. In fact, there is still to this day an active quarry very nearby, so that real place became the location for this fictional company and its railway.

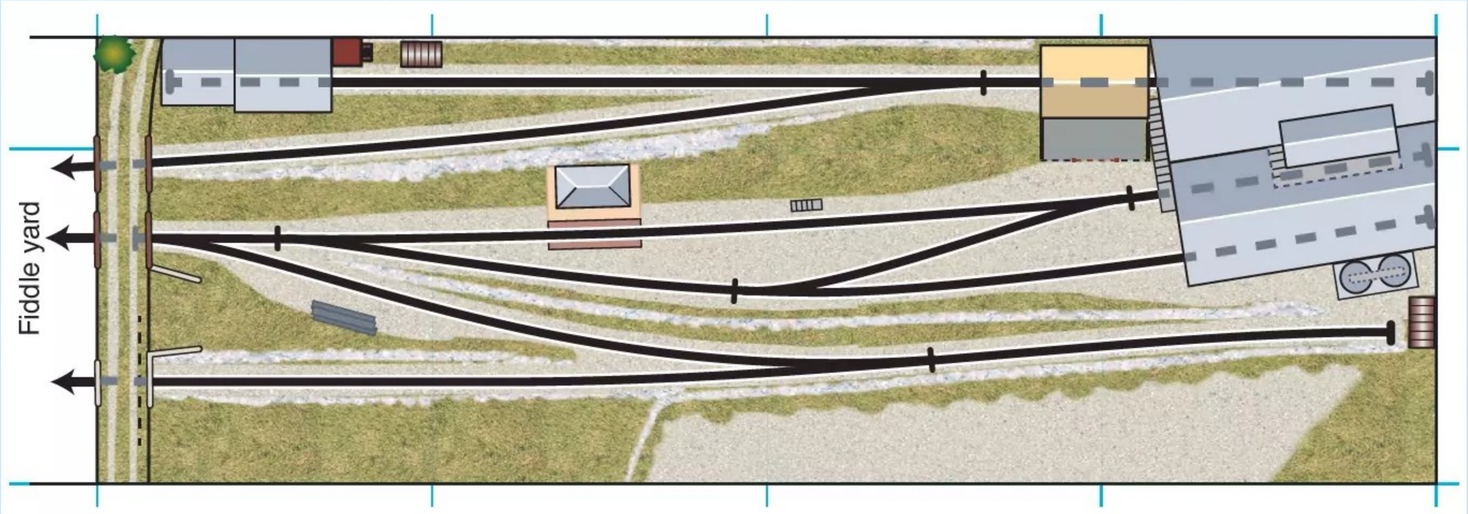
The time period of the 1960s was chosen to make use of rolling stock that I already had, and to allow the use of both diesel and steam locomotives. A particular feature of the layout is the very tight loading gauge on the line to the quarry and the need for such a restriction, in the fictional history of the railway, is that it was originally constructed as a narrow gauge feeder line. The actual reason for this is to justify the use of several modified ready-to-run locomotives, details of which are provided in the captions.

Baseboard and fiddle yard

The outer casing of the board was made using 9mm plywood, with a 5mm top. The top was cut using an electric jigsaw to follow

A Peckett 0-4-0ST emerges from the tight confines of the former narrow gauge tunnel that leads to a storage cassette.

The Ruston 48DS four-wheel diesel-mechanical is a Hornby ready-to-run model that has received a new roof, along with suitable weathering.



The Harboro Stone Company Size of scenic section 4' x 1' 4". Each grid square = 1' x 1'.

the trackbeds and allow them to be dropped or elevated from the datum height. From this basic frame and trackbeds I infilled the open areas with papier mâché, made from old newspapers and PVA glue.

The accompanying fiddle yard is also made from 9mm ply and attaches to the baseboard by use of tee nuts, M8 hex-headed screws and wing nuts. It is fitted with a three-road traverser that is mounted on a couple of ball-bearing drawer runners.

As I use Digital Command Control, there is no need to isolate the tracks from one another, so power is fed via copper tape to both drawer runners and to each rail, which means all tracks are live at all times, even when in motion. There are no wires to snag or get in the way, and sound is maintained to the sound-fitted locomotives at all times.

A fourth track, the one linked by the low-height tunnel, uses a cassette to store the train.

Track and electrics

The straight track on the scenic section is Peco bullhead with code 75 Streamline points of the smallest length available. *[Ed. Small radius points are now available in the bullhead range – see the review in the July 2025 issue of Railway Modeller.]* It is all ballasted using sand and PVA glue.

Since building *White Peak* I moved over to DCC operation, so the electrics are very simple on *Harboro Stone*. There is no need for section switches to isolate sidings or microswitches to change the polarity of point frogs. It is really only a matter of putting power to the rails and wiring each frog to a frog juicer unit. For the DCC bus I used copper tape, stuck to the underside of the baseboard and running the length of it to

The Peckett arrives in the yard with empty iron ore tipplers, while the Ruston 48DS shunts a Prestwin wagon.

The Motor Rail diesel is a Planet Industrials kit on a Hornby 48DS chassis. It only just fits through the tunnel but needed no alteration from its as-built state.

allow droppers from the tracks to be soldered at any place they were required.

I have also moved away from using electrically-operated point switching. With a layout that is only 4' long, there is no need for it as all points are within easy reach, so they are each operated by a length of sub-baseboard piano wire, with a turned brass knob protruding through the baseboard fascia. This method saves a lot of time and expense.

Scenics

As with *White Peak*, I used Woodland Scenics rubber moulds to produce the rocky outcrops, but on *Harboro Stone* not all of them were made in the normal way, i.e. pouring in a plaster-type material. Instead, I pushed into the mould rolled-out lengths of DAS air-drying clay to give a lighter, thinner and more flexible piece to work with. This was especially useful in lining the cutting walls. All of the ballast, trackwork and rock faces were painted by brush and airbrush in both Woodland Scenics Concrete and Tamiya acrylic Deck Tan paints.

The greenery consists of the usual static grass and one solitary seamoss tree.

The model is presented as a cameo-type layout and so has a backscene at the rear and ends. The board is integral to the baseboard and consists of 3.6mm plywood, reinforced at the corners with narrow softwood battens. The backscene was made by going to the site and taking photographs of the actual scenery in the vicinity of the imagined location of the line. These photographs were manipulated with photo-editing software to remove power lines and wind turbines before being sent to iD Backscenes to be printed.

Structures

All of the buildings have been scratch-built to fit their location.

The largest building is the main processing plant, where the limestone would be crushed to powder. The carcass for this was made from picture mounting board and is clad with corrugated aluminium sheet. Previously I used Wills plastic sheeting for corrugated iron structures, but the relatively

The Motor Rail 14-ton four-wheel diesel-mechanical shunts a pair of weathered Bachmann Diagram 1/272 Presflo wagons.

small size of the sheets and the problem of blending them together to cover large areas would have limited the size of the building. The corrugated aluminium was purchased as a large sheet and had to be cut into individual sheets of 10 x 40mm. Each piece also had to be embossed with indentations to represent the bolts that hold it onto the frame. The small unloading building, next to the main one, was built from Wills

The black Peckett is a Hornby model with the cab and boiler fittings cut down, and the rear cab sheet removed.

corrugated asbestos plastic sheet, so was a very quick and easy build.

The walls of the main part of the engine shed were built from individual pieces of DAS clay, representing a rough stone construction. Sausages of clay were rolled by hand, then flattened and allowed to harden and dry. The flattened sausages were broken up to make individual stones that were glued together using PVA. The roof of

Large lumps of stone for sea defence or civil engineering work are transported on a TMC/ Bachmann BR Plate wagon.

The two Diagram 1/210 Covhop wagons under the main processing plant are weathered but otherwise standard Bachmann items.

the shed is mounting card with paper 'slates'. The extension to the shed was made using the same materials as the main processing building. The engine shed is rather small and the entrance to it has a very limited clearance. This is deliberate and supposes that it was originally built to house narrow gauge engines.

The last building is the weigh cabin. The walls and roof were made as per the engine shed but the front area, with its windows, was converted from a Wills conservatory kit. The weighbridge itself is simply a rectangle of tread plate-patterned plasticard.

The other structures are the bridges and a tunnel that form the view blocker to the fiddle yard. These have also been constructed using individual blocks of clay on a mount board former. The tunnel entrance was also made using DAS, with a Wills plastic brick arch set into it. The plate girder bridge spans were scratch-built using plasticard and rivet transfers.

Traffic

The raw material inward is, of course, limestone and, like on *White Peak*, this is quarried off-scene, but rather than being brought to the plant by conveyor belt, it is transported by rail, in the company's own internal use wagons.

The majority of the stone is assumed to be processed into powdered limestone products to be used in a variety of processes and products, including as a filler, as a fire suppressant in coal mines, a soil improver in agriculture and an ingredient in chemical manufacture. Such a diversity of uses allows the assumption that the various customers have different handling facilities, and so provides for a range of rolling stock types to be used. These are Presflos and Prestwins, Covhops, sheeted 22-ton hoppers and sheeted 16-ton minerals. The quarry site itself is assumed to have a primary crusher to reduce large lumps of stone to a size suitable for use as road stone, and this leaves in British Railways 26- and 27-ton tippler wagons.

In addition to the processed and crushed stone, there are large lumps, carried on BR Plate wagons. Such lumps were used in sea defences and other civil engineering, and add variety to the rolling stock and enhance the operational interest of the layout.

The Andrew Barclay 14-inch cylinder 0-4-0ST is a Hattons R-T-R model that has had the footplate, boiler and tank lowered.

Shuffling wagons around

Although *Harboro Stone* is a shunting layout, it isn't a shunting puzzle. I have never been keen on the Inglenook and other types of layout, where wagons are seemingly moved around with no real purpose. All movements on the layout have a purpose and, although it is obviously not run to a timetable, there is a sequence to be followed in order to get all the wagons to their correct places to be loaded or unloaded and keep the line from becoming blocked or the fiddle yard choked.

Incoming wagons have to be put over the weighbridge, where in real life the weigh man would note down the tare (empty) weight of each wagon. They are then moved to where they need to go to be loaded: in the case of road stone and large lumps, this is to the quarry, via the tunnel line. For everything else, this is to one of the roads under the main building.

After loading, all wagons must be weighed again, where they would receive their labels with the weight and destination, before being despatched to the exchange sidings (fiddle yard) to await collection for onward movement by British Railways.

The exceptions to the rule of being weighed are the internal use tipplers: these run directly from quarry to plant without the need for weighing, either empty or full.

Despite its size, it is a very interesting layout to operate, and requires two engines to move the 17 wagons of eight differing types through all of the moves to leave everything at the point at which it started.

The original idea was to have a working loader, as on *White Peak*, but for a number of practical reasons this was abandoned and so the powdered limestone idea came about because a covered hopper, or sheeted wagon, looks the same whether empty or full. The tipplers, however, do have exposed loads, but as they are loaded off-scene, I use fixed loads, made from card and grades of model railway ballast that is glued on. Each load has a small magnet embedded into it so as to make removal (using a magnet on the shunting pole) easier.

Forthcoming exhibitions

It is planned that *Harboro Stone* will attend a number of model railway exhibitions in the future, although currently its next booked appearance isn't until Wakefield in 2026.

Claremont

PETE LATHAM *describes this small and self-contained N gauge layout, which depicts a BR freight yard and utilises older examples of ready-to-run models. The layout plan provides the basis of an ideal entry-level project for those who are both short of space and working to a budget.*

Photography by Craig Tiley

“ – you do not necessarily need a big space for a layout – ” Pete Latham

37 894 in BR Coal sector livery departs with a rake of loaded steel carriers. An assortment of wagons occupy the sidings in the background.

When my previous N gauge layout (*Mayfield Sidings* – RM November 2018) had chalked up three years touring the exhibition circuit, I felt that the time had come for a rebuild.

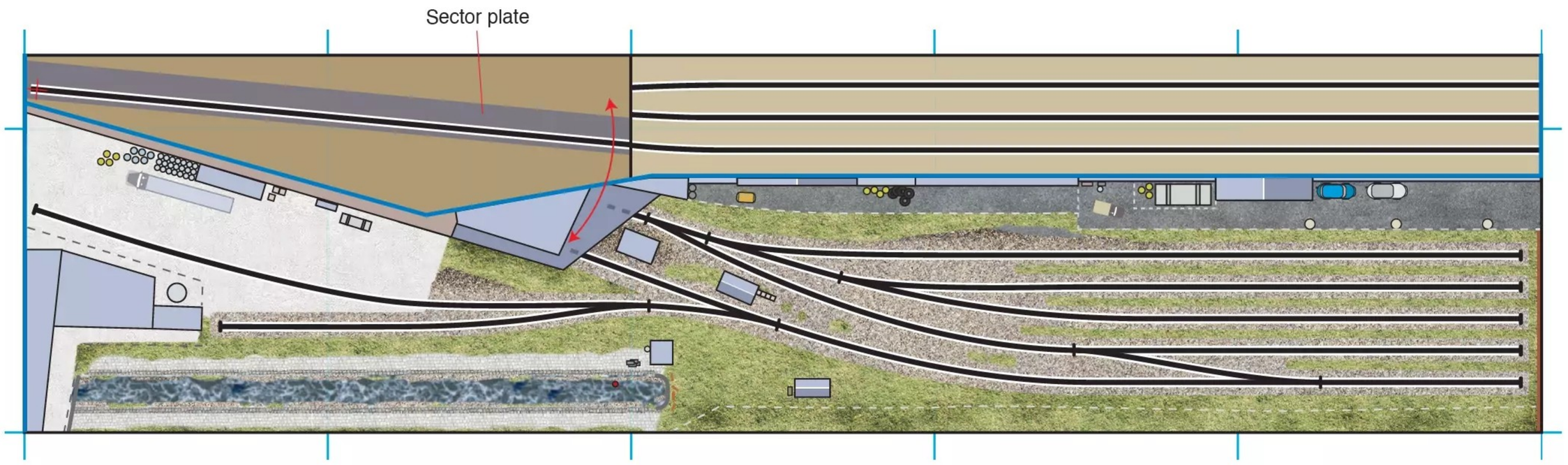
Looking through my layout reference file, I came across an article on the 4mm scale (EM gauge) layout *Canada Road*, built by the late Peter Johnson. I first saw this layout at Cheadle Hulme School in about 1980. It was at that time too modern for me, as it consisted of BR blue diesels, but it did work well. (The layout was eventually succeeded by *Canada Street*, which was described in the November 2017, January and April 2018 issues of RM.)

I decided to use a very similar track plan, which necessitated the adapting of the footprint of the *Mayfield Sidings* baseboard from 4' x 1' 6" to 5' x 1' 3". (I now realise rebuilding a layout a number of times adds more timber and therefore more weight!) The boards used a timber frame of 4" x ½" softwood laths, with a 6mm plywood top and backscene, all screwed and glued together.

Construction

The rebuilt layout still uses the 21" sector plate and a lot of the buildings from *Mayfield*. Others have been added, including an HO factory frontage. Most buildings have been enhanced with items from suppliers including Unit Models and Rhuddlan Models, plus my trusted scrap box.

All the trackwork is Peco Streamline code 55, using mainly the small radius points. This was then ballasted with products from the Greenscene



Claremont Overall layout size 5' x 1' 3". Each grid square = 1' x 1'.

and Woodland Scenics ranges. Once completed, all the track was given a quick spray of Humbrol Dark Earth matt paint, before quickly wiping the tops of the rails and point blades clean.

The wiring for the layout is very simple – two feeds, one on the sector plate and one at the end of the headshunt. The fiddle yard is identical to how it was configured for *Mayfield Sidings*, such that power is transferred to each of the three storage lines via square brass section sliding rods.

A Class 56 draws a trio of wagons with permanent way materials into the yard. The exit to the off-scene sector plate can be seen behind.

Train control uses an analogue 12V dc handheld unit – an All Components HH5/N N gauge speed controller. This provides smooth slow speed control, which is essential for a shunting layout like this.

Points are operated mechanically by wire-in-tube under the baseboard top – the wires pass through plastic tubes fabricated from cotton buds and held in a small piece of electric 'chocolate block'. The handles to operate the wire rods are positioned along the rear edge of the fiddle yard.

Most locomotives are pre-2000 ready-to-run models – they seem to operate slowly and well with this feedback controller.

The varied array of freight stock comprises kits and ready-to-run items.

A single-road sector plate connects the scenic section with three storage sidings that run along the back of the layout.

Space saving

The layout has now been shown at a number of exhibitions (including this year's Model Rail Scotland) and proves that you do not necessarily need a big space for a layout.

And finally, what of the name? *Claremont* came from the road on which Cheadle Hulme School is situated, where I first saw *Canada Road* all those years ago…

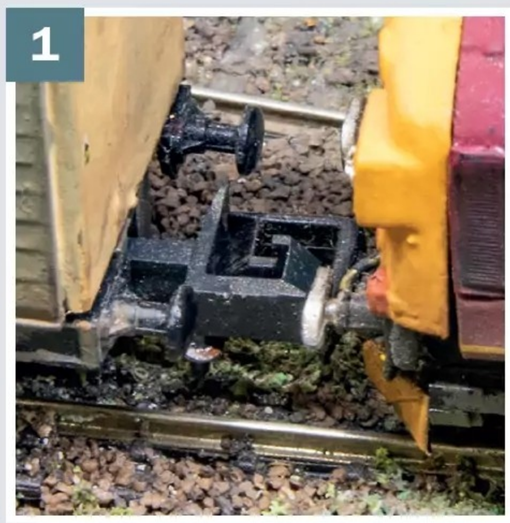
Automatic uncoupling in N

To provide a reliable means of hands-free automatic uncoupling, I installed 10 electromagnets (a combination of PK Mag and SEEP products). The locations of these are marked with a white dot on the operator's side of the track.

Every wagon is fitted with a Peco uncoupling arm (ref.NR103). Graham Farish wagons have the couplings removed and a Peco coupling (ref.NR102) fitted. I have used this system for many years and it works well. (It even works on coaches by simply turning the bogie through 180° and glueing on a Peco coupling.)

Other layouts on which I have used this system include: *Clayton East* (RM March 1998), *Bradford Road 1981* (RM May 2005), *Deep Lane* (RM November 2008) and *Imperial Yard* (RM February 2012).

An example of a wagon equipped with a Peco coupling and uncoupling arm.



Vehicles can be uncoupled by first bringing the uncoupling arm to a stop over an electromagnet.

When the electromagnet is energised, the coupling equipped with the uncoupling arm lifts up.

This allows the vehicles to be separated, the coupling then returning to its normal position.

See it at Stockport show

Claremont is booked to appear at the Hazel Grove Model Railway Exhibition on **25 & 26 October**, which will be taking place at Woodford (War Memorial) Community Centre, Chester Road, Woodford, Stockport SK7 1PS. See Societies & Clubs for more details, or go to: **www.hgdmrs.org.uk**

A Class 31 passes the abandoned signal box. Many of the structures on the layout were reused from previous projects.

Like most of the locomotives on the layout, this Class 47 is from an older generation of ready-to-run tooling, but still gives sterling service.

Irish broad gauge in 7mm

Northport Quay

DAVE HOLMAN *presents his latest essay portraying the railways of the Emerald Isle, which demonstrates how a layout in a large scale, with operational interest, can be achieved with a scenic footprint of just 7' 6" x 1' 6".*

Photography by Andrew Burnham

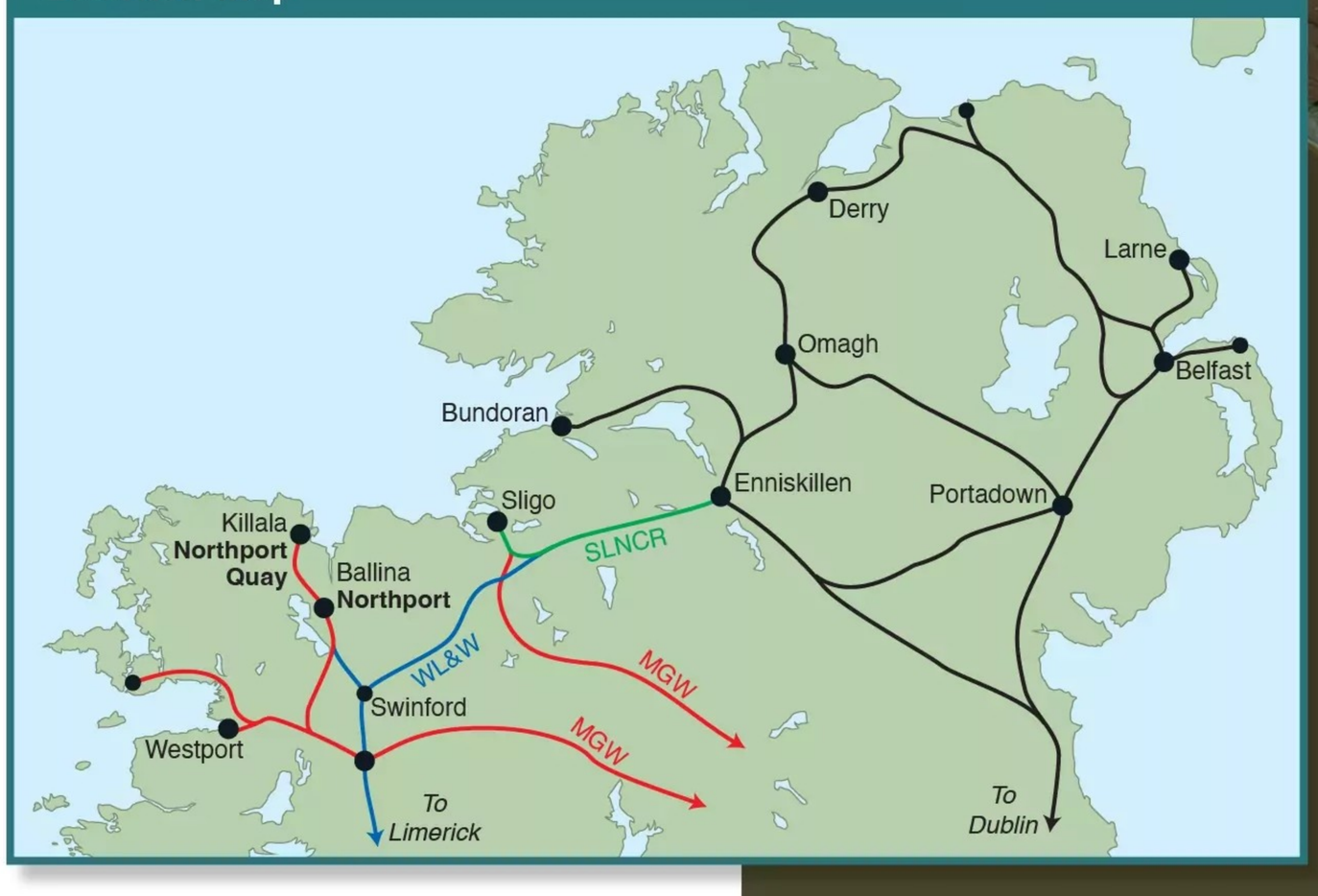
This is a cameo layout. It tries to depict a small terminus station, but with everything pared down to the bare minimum.

Trains are short (two six-wheel coaches and a van, or equivalent), but the layout does have a range of interesting features to engage the viewer, with enough operational interest too. The setting is unusual in that it depicts north-west Ireland and is 7mm scale, so the track gauge is 36.75mm, for Irish 5' 3".

Several Irish branch lines had harbour extensions – Bantry and Westport to name but two and it is the latter that inspired the layout name, with Northport deemed to be where the actual town of Ballina is located, and the quay being a few miles further up on the coast. The accompanying map shows the imagined locations – in particular the links to Sligo, which are important because the latter town was originally served by no fewer than four independent railway companies.

The Midland Great Western came in from Dublin, the Sligo Leitrim & Northern Counties from Enniskillen, while the Waterford, Limerick & Western came up from the south and was eventually taken over by the Great Southern & Western. Such

Location map



▲ The Great Southern & Western Railway passenger train consists of two Alphagraphix etched brass six-wheel coaches, and a Class 101 (later J15) 0-6-0 from a TMD kit, commissioned but never built by the late Richard Chown of *Castle Rackrent* fame. An Alphagraphix GSWR F6 2-4-2T is also available for this duty.

information is important, as it has enabled me to indulge in building a wide variety of trains to run on the layout, covering two distinct periods – the early 1900s and the mid-1950s.

Design concept

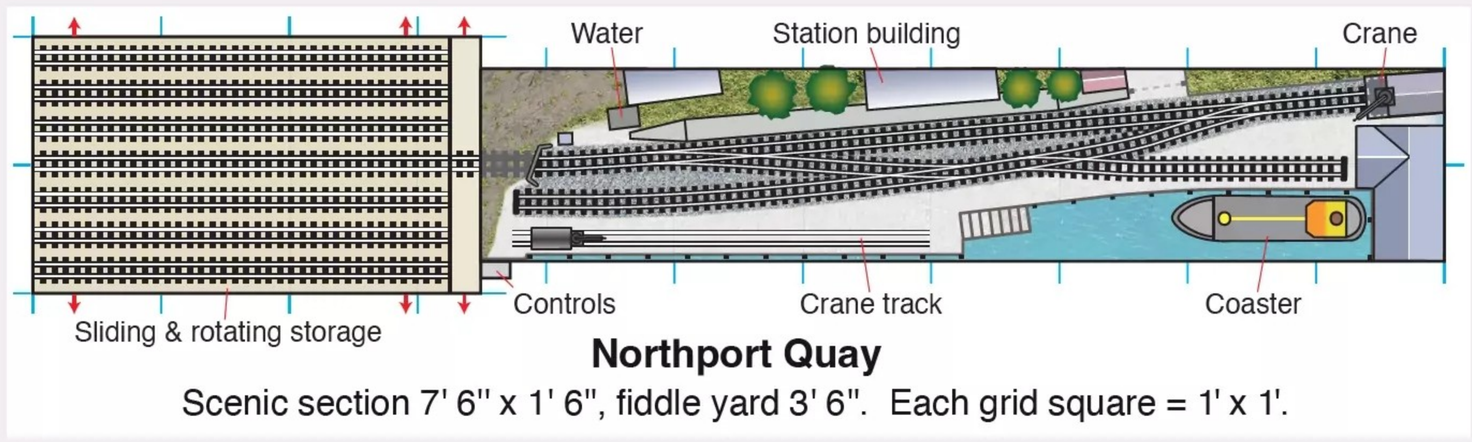
I've been modelling the Irish scene for a number of years and my previous layout, *Arigna Town*, appeared in RM April 2015. It went to 30-odd exhibitions, including the Ulster Folk & Transport Museum at Cultra near Belfast. Eventually, though, I decided I wanted something smaller than its 16' x 2' and, after much scribbling, came up with a plan that is just 7' 6" x 1' 6" on two baseboards, plus a fiddle yard 3' 6" long.

Overall, it is just a loop and two sidings, but also includes a quayside with a mobile crane (on its own separate track), plus a coaster. Trains come on scene via a short tunnel through a rocky headland and can potentially exit the other end behind a large warehouse onto an off-scene pier.

Construction

Baseboards are 6mm ply, with the integral backscene helping with structural integrity. I used code 100, flat-bottomed rail on copper-clad sleepers for the track, in order to create the lightweight look of many Irish branch lines. Ballast is Woodland Scenics fine ash, softened with judicious use of weathering powders and talc.

The four points are operated from just two DPDT (Double Pole Double Throw) switches as a pair of crossovers, via Tortoise point motors. The latter are mounted on their sides on home-made brackets, meaning that the baseboard frames only need to be 70mm deep, rather than 100mm if the motors were mounted the usual way.



Northport Quay

Scenic section 7' 6" x 1' 6", fiddle yard 3' 6". Each grid square = 1' x 1'.

A general view of the scenic section of the layout, with the Midland Great Western mail train in the platform.

Control is analogue, via Gaugemaster handheld controllers and a small mimic panel, let into the front frame, with the two point switches and several push buttons for the uncoupling magnets. The starter signal is worked by wire-in-tube from the panel.

The fiddle yard is unusual in that it both slides and rotates. The design was first described by Robin Fielding in *Model Railway Journal* No.199 and *Northport Quay* now uses my Mk.II version (as the first was decidedly heavy and had to be split into two parts to be carried!). Essentially, it is made up of a plywood frame, which holds a section that can be slid either side of centre via the type of two-way drawer runners used on kitchen island units. On top of this, fixed to a lazy Susan is the track table, containing five roads just over 3' long. When the sliding section is pulled to one side, the track table can be turned 180°, thereby obviating the need to handle stock. Home-made bolts both align the track and feed power to the line in use, while hinged gates (aluminium strip), prevent stock running off the ends when the table is turned.

Presentation

At home, the three boards sit on longitudinal beams, which are screwed to kitchen cupboard base units. For exhibition use, the beams sit on two home-made trestles, which give a track height of about 4' 3", for a reasonably natural viewing angle, with the

This railbus is a converted road bus built for the Sligo Leitrim & Northern Counties Railway in the Great Northern workshops at Dundalk.

The Midland Great Western D16 4-4-0 is almost entirely scratch-built and powered by an ABC motor gearbox. The two coaches are Alphagraphix kits.

operator able to do most of the work perched on a bar stool. The scenic part of the layout is enclosed by a shallow side frame at each end, upon which a plywood pelmet is simply attached using flush mount plates. A gallows bracket (similarly attached at the rear), provides support in the middle. The latter carries basic wiring for the lighting: one strip of LEDs running the length of the layout towards the rear, with another fixed just inside the pelmet to light the front edge. Two additional fascias are attached to the front as information boards about the layout.

Front operation is aided by the use of Alex Jackson-type couplings and electromagnets. They are simple to make, a bit harder to set up and certainly need care to keep that way, but they are unobtrusive and very effective when working properly.

The mobile crane is scratch-built, using Wills corrugated iron sheet on a nickel silver chassis with a High Level motor gearbox.

The CIÉ G Class four-wheel diesel was built from a set of Worsley Works etches, while the wagons are plasticard bodies on whitemetal underframes.

Two people can set the layout up in about half an hour, with dismantling taking less than half that. The record for being back in the car after a show is just 15 minutes – though we were right by the door. Much as I love seeing larger layouts, there is much to be said for smaller ones with minimal setting-up times.

Scenic features

Scenically, there is a short platform for passengers and mails. There is a simple, half-relief station building (based on Westport Quay), plus a corrugated iron goods store and a small water tank. The tunnel mouth and rock face make much use of DAS clay, scribed and sculpted to shape and then blended into a painted backscene, which depicts a hilly coastline stretching away into the distance. A couple of scrubby trees hide the gable ends of the station building, while a larger tree, painted on the backscene, tries to hide the baseboard join.

Beyond the platform, a low wall suggests the start of a stone pier, reaching out into the harbour, with the headshunt extending along this behind a large, rendered stone-built warehouse based on one at Rathmelton, near Letterkenny. The front edges of the baseboards depict a harbour wall, with the right-hand side inset to pose a waterline model of a small coaster. More DAS was used on both horizontal and vertical surfaces to create stone walls and the setts enclosing the inset track.

The coaster started life as the Langley kit of a Clyde puffer. It is doubtful if these little craft ever ventured out into the wild Atlantic coast of this part of Ireland. Instead, I took inspiration from *VIC 96*, a fine little coaster which currently resides in Chatham Marina. The 'VIC' name comes from their main use as 'Victualling Inshore Crafts', servicing larger navy craft offshore. Despite being built after World War II, their puffer heritage is clearly evident. However, the high forecastle and funnel set behind the wheelhouse give them (to my mind) a more balanced and pleasing appearance. My model, called *Acla*, the Gaelic name for Achill Island, uses the Langley hull with a raised forecastle and rearranged rear superstructure. About 20 scale feet shorter than *VIC 96*, it nevertheless fits the scene nicely.

Two cranes and a lot of dockside clutter complete the scenic features. The first crane is a static model intended to hide where the track exits, stage right, onto the pier. It is the old Airfix/Dapol kit, with a new cabin, more suited to 7mm scale, but otherwise made up as per the instructions. The other crane is mobile and runs up and down its own piece of track along the left-hand side of the quay. I made a simple four-wheel chassis from nickel silver, which contains a High Level motor gearbox. On top of this sits a corrugated iron body (Wills sheet), with a cosmetic steam boiler powering a winch, plus a jib made from plastic strip. Folk sometimes ask if the crane actually

Hunslet 0-4-2T *Lark*, built from an Alphagraphix kit, shunts a short goods train onto the quay.

***Hazelwood*, one of the distinctive Sligo Leitrim & Northern Counties Railway 0-6-4Ts, was built from a Northstar kit – which is now available in the Studio Scale Models range.**

operates. One day, maybe, though to make that worthwhile I will need to build another ship for it to transfer goods to and from wagons on the quayside, and that isn't going to happen any time soon!

Irish charm

So, there we are with *Northport Quay*. Small and simple, but fun to build and with just the right amount of operational interest at exhibitions, enabling us to talk to visitors while still keeping trains running.

Modelling broad gauge is not as difficult as it may seem, especially in 7mm scale, where Slater's wagon and coach wheels have axles long enough for 36.75mm gauge. They produce a broad gauge locomotive axle too, while fittings like buffers, W-irons, brake gear and couplings are the same as used this side of the water. While nothing much is available straight out of the box, there is a surprising range of kits available from the likes of Studio Scale Models, Worsley Works and, of course, Alphagraphix. Irish railways have a charm all of their own and for anyone looking to do something different and requiring a bit more effort than doing ready-to-run, then the extra challenge can be very rewarding.

DAS clay was used to make the quayside, scribed to represent the stone walls and setts around the inset rails.

The coaster *Acla* is a Langley Clyde puffer kit, modified with a raised forecastle and alterations to the rear superstructure.

See it at Aldershot

Northport Quay **will next be exhibited at this year's Aldershot model railway exhibition, which is to be held on the weekend of 11 & 12 October.**

See Societies & Clubs for more details, or go to: **www.farnhammrc.org.uk**

Part 3 – Coach roofs and interior details

Constructing BR Mk.I coaches in Gauge 1

New Zealand-based modeller **JOHN BOYSON** *concludes his account of how he used 3D-printing methods to scratch-build a rake of BR Mk.I coaching stock in 1:32 scale.*

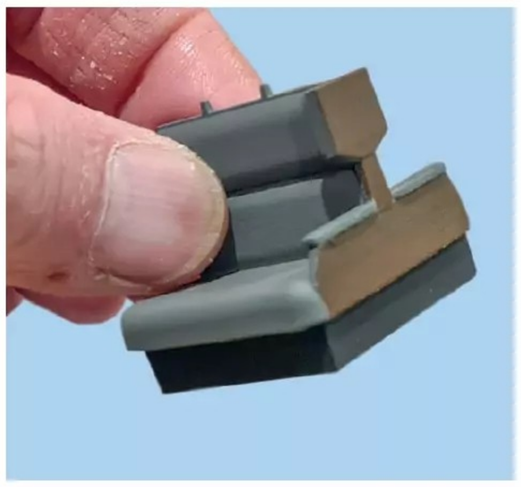
Model photography by the author

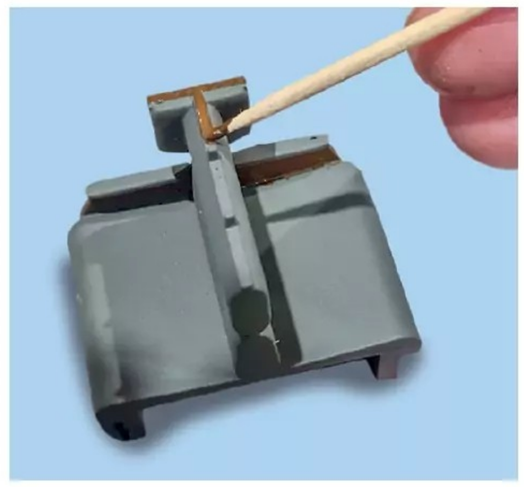
In part two of this series (last month), construction of my rake of coaches had reached the stage where the chassis, together with the bodysides and ends, were largely complete. The remaining areas outstanding were the roofs and interior detailing.

Interiors

Internal partition panels were cut out using the Tinkercad artwork once again and a Cricut cutting machine. The 'Maker' version of the Cricut has the capability of cutting thin wood section, as well as paper and card. Solid panels were cut from 1mm birch ply (I have a large 1.8 x 0.9m sheet of this material in stock which provides for all my modelling needs in this material for a long time). Glazed panels were formed from two identical pieces of paper, again cut out using the Cricut machine. These were painted brown on both sides, before being glued to an oversize piece of clear polycarbonate as used for the external windows. Once the glue had set, the edge of the panel (as denoted by the paper) was scored all around and the excess was snapped off.

Seats were 3D-printed and painted.



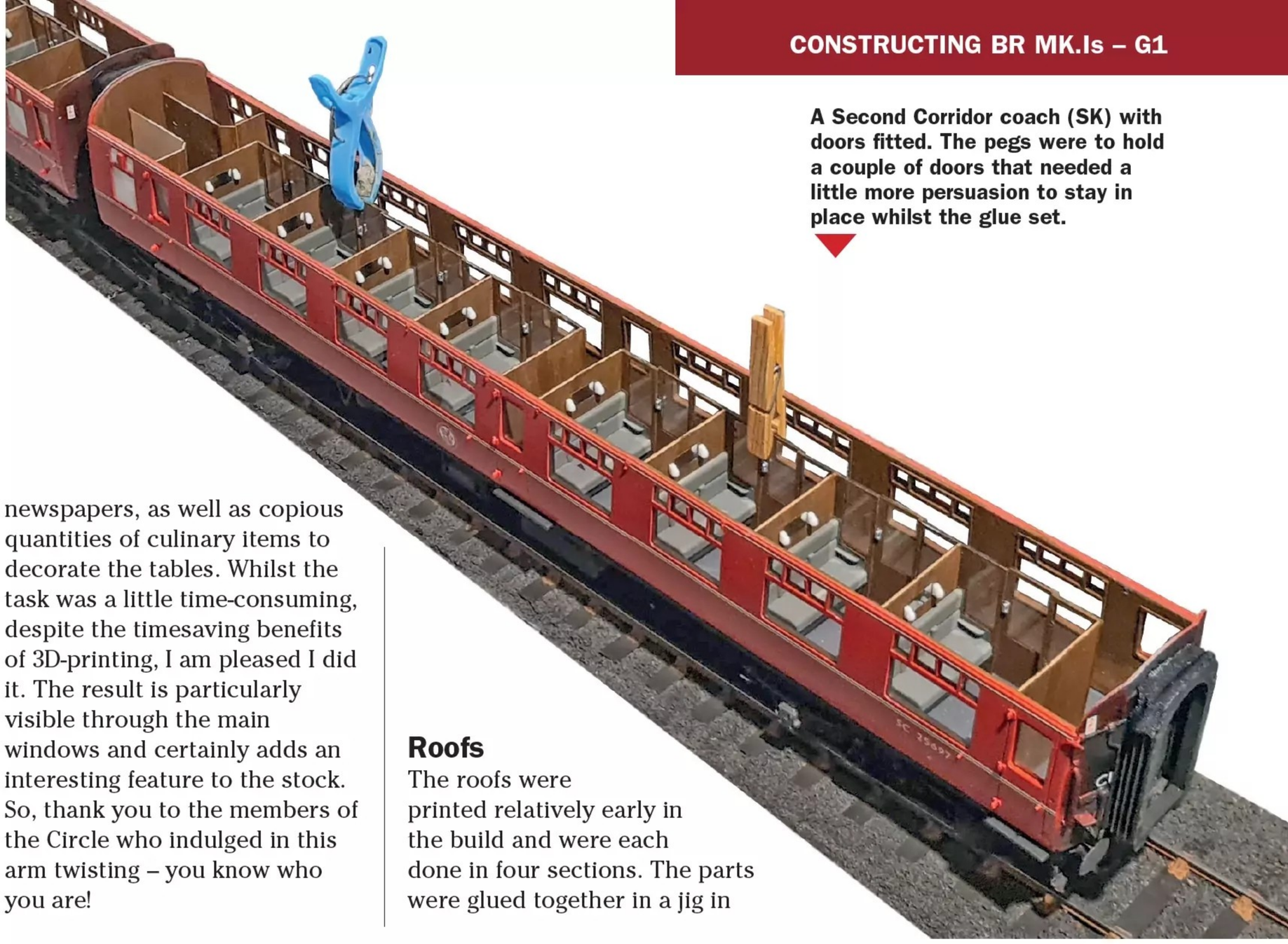


Painting the brown wooden areas of the open carriage seats. A cocktail stick is a good applicator for the areas more difficult to get at, without spreading paint where it isn't wanted. I know the plain grey finish of the seats is not prototypical; however, at this point I called a halt to this level of detail, since these are not particularly visible behind windows, tables and passengers, etc.

Some of the compartment seats and partition sub-assemblies.

Sub-assemblies of the various compartments were created before building up the various units within the carriages.

I wanted to populate the carriages as well, which created a dilemma initially. Model figures, particularly in the larger scales, generally don't come cheap. I worked out that for the eight carriages I would need



A Second Corridor coach (SK) with doors fitted. The pegs were to hold a couple of doors that needed a little more persuasion to stay in place whilst the glue set.

about 200-300 of them. Artwork masters, likewise for 3D-printing, tend to be somewhat pricey as well. However, when I trawled the internet, I came across 1:32 scale sitting people in bulk for a reasonable price. So, four sets of 50 were ordered and they duly arrived. However, once they were unpacked, I found a number of issues, including limited variation between the different figures (they all had black hair for instance), and they sat a little high on the seats when placed *in situ*.

Fortunately, I was also gifted a number of 1:32 figures by a modelling friend to increase the variety and stock.

A mass and brutal pruning operation was undertaken with a bench grinder to remove the excess material. Then they were repainted to generate a greater variety of clothes and hair colours. Once treated, they were glued in place in random groups in the various bays and compartments.

Once the carriages had been populated, tables complete with legs were printed, painted and fitted to the open stock bays.

At this point, a little gentle coercion took place. Certain members of The Gauge One 3D-Printing Circle suggested that, as well as populating the carriages, I should also give the passengers some occupation and refreshment. As if I hadn't enough to do! Having had the challenge set, I could hardly refuse, and the result was a number of bags and newspapers, as well as copious quantities of culinary items to decorate the tables. Whilst the task was a little time-consuming, despite the timesaving benefits of 3D-printing, I am pleased I did it. The result is particularly visible through the main windows and certainly adds an interesting feature to the stock. So, thank you to the members of the Circle who indulged in this arm twisting – you know who you are!

Roofs

The roofs were printed relatively early in the build and were each done in four sections. The parts were glued together in a jig in the same manner as the sides (see part two). Having been used to keep the sides and ends in place whilst these were glued to the underframes, the roofs were set aside until the interiors were completed, some 12 months later.

However, on returning to them after this time, I found that they had distorted somewhat. This is one of the inherent weaknesses of 3D-printed items.

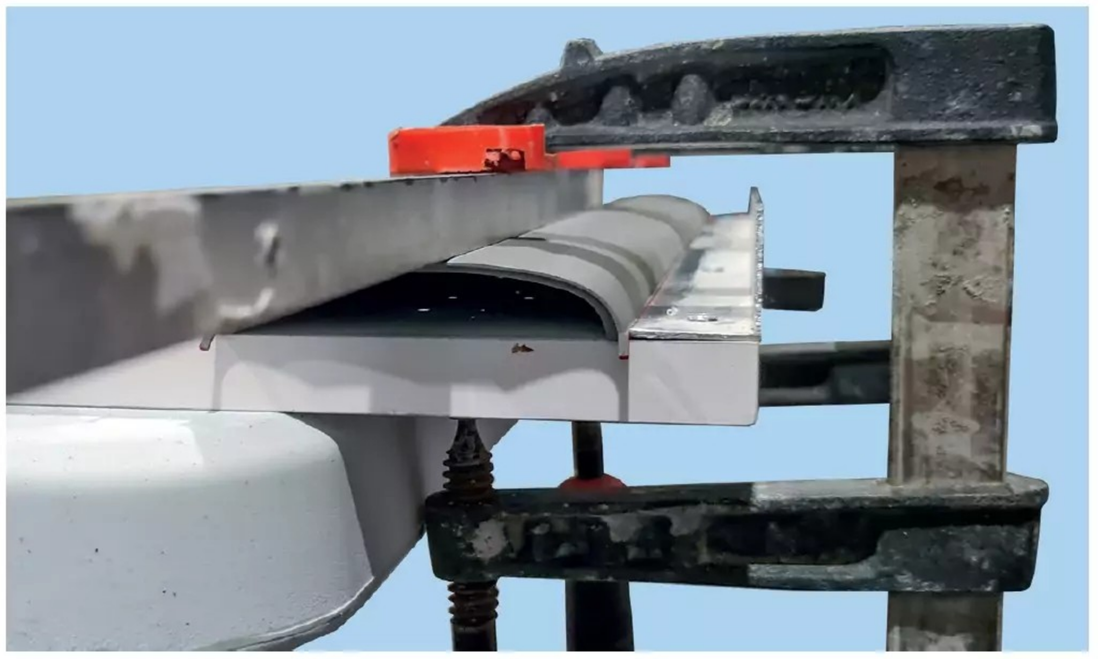
The buffet car (RMB) complete with attendant. Note that the shelves have yet to be stocked with bottles. The attendant started life as a mechanic who, with some judicious painting, has changed careers!

A BRCW Type 2 Bo-Bo diesel hauls the completed rake of BR Mk.I coaches around the outdoor section of the author's developing Gauge 1 model railway, which is set in the West Highlands of Scotland in the 1960s.

The remedy was to put them back into the jig used to glue the sections together and clamp them down. This somewhat rough treatment encouraged them to revert to their correct profile. Unfortunately, this was not a permanent fix and so, following sanding and painting, they were returned to the jig for further encouragement. In the meantime, to hold them in line permanently, false ceilings were fitted to the tops of the carriage sides. This ensured that these areas also stayed true and provided a rigid box structure on which to mount the roofs.

Aluminium channel was clamped down on the crown of

An example of the roof deformation that occurred after 12 months storage.



Above: looking along the jig (made from a 600mm melamine shelf and two aluminium angles screwed down to the shelf). The crown is straight and the sides touch the jig edge along the whole length. Above right: the straightened roof released from the jig after two days, and ready to be glued in place on the coach. The good thing here is the resilience of the ABS resin, which can withstand this sort of treatment.

the roof to apply an even pressure along its whole length. This compressed the raised section and, in turn, spread the narrowed areas at the base out to the edges of the jig. Careful, even application of pressure across the three clamps that I used was key here. The Goldilocks principle applies: just enough in the right place and not too much!

With everything prepared, each roof was released from the jig after two days and glued immediately onto its parent carriage before it had time to lose its shape once again. This somewhat severe treatment worked, and the roof and sides have stayed true ever since.

The final job on the roofs was to fit various ventilators and water supply pipework. Again, all these features were 3D-printed.

Finishing

The final job was to add the finishing touches. These included the door and grab handles, and the handrails. As these are somewhat fragile and exposed, I used cast brass fittings from Walsall Model Supplies for the handles, which are much stronger than 3D-printed parts would have been. The handrails were formed from wire.

On the left: the artwork for a standard British Railways Mk.I carriage toilet header tank supply pipe. Above: the installed kitchen supply pipe for the buffet car, which is one of the longest roof pipes found on this stock. This was split into three sections to fit into the printer. (These were connected with holes and spigots within the pipes to form seamless joints beside the holding brackets.)

Conclusion

The project has taken two years to complete, and a lot of learning and fun has been had along the way. It has also saved money, since a commercial off-the-shelf eight-coach rake would have cost about £2,500 or £5,000, depending on the manufacturer (there are two who offer these carriages in this scale). My rake set me back about £2,000.

As far as Computer Aided Design work is concerned, I started off at ground zero. Hence Tinkercad proved to be invaluable. It may be that, in the future, I might move on to more sophisticated programs as my knowledge develops. However, I am content with where I am at for the moment.

For myself, I regard 3D-printing as a means to an end (to improve my modelling standards) and not an end in itself. That said, it is a game-changer, which will definitely help improve the hobby of model railways, particularly in the larger scales. Thus, this is a powerful modelling tool amongst my armoury to take me further down the path I have chosen.

Most importantly, the whole process is fun and can be indulged at whatever level you wish. As long as it is enjoyable, that is all that matters.

The finished Brake Second Open, showing the handrails and handles fitted.

More information

The Gauge One Model Railway Association
www.g1mra.com

The Gauge One 3D Circle
https://gaugeone3dcircle.groups.io/g/home

Project of the Month

A Patriot for Dentdale

...or the trials and tribulations of a locomotive kit-builder. Modelling and words by **IAN NUTTALL**

Model photography by the author

No.5514 *Holyhead* poses for its official 'ex-works' photo immediately following its repaint in August 1947, although, as explained, it should now be a rebuilt Patriot; *mea culpa*. The location is the Up lay-by in Dentdale.

In the mid-1920s the Claughtons ruled the roost on the West Coast Main Line, but, with the arrival of the Royal Scots in 1927, they were downgraded and sought pastures new. Some appeared on the Settle & Carlisle (the setting for my OO gauge layout, *Dentdale* – see RM March 2018), although photographic evidence is rather thin.

Locomotive superintendent Henry Fowler appreciated they were becoming long in the tooth, and also heavy on coal, which upgrading to a larger boiler didn't solve. Two were taken out of service and rebuilt; rumour has it that precious few original parts were used to create the first two Patriots in 1930, numbered 5971/2, which became Nos.5500 *Patriot* and 5501 *St Dunstan's* in the 1934 renumbering scheme. These were the only two to operate without smoke deflectors... and this was just in their early years.

Whilst not as powerful as the Royal Scots, they were a huge success, so 40 more were ordered (30 built at Crewe, 10 at Derby), emerging in 1932-3. A further 10 appeared out of Crewe Works in 1934. All were designated 5XP in the power classification system, proved versatile, and were well liked.

Chosen model

I have two Millholme Models LMS Patriot kits amongst my hoard, both acquired relatively recently via a well-known online auction site.

The one I selected had the bonus of Romford coupled wheels, thus saving me around £40, but which particular Patriot should be the subject of my latest build? I must put my hand up and confess to having a soft spot for the class, and am privileged to possess my mentor Ron Smith's scratch-built No.5504 *Royal Signals*, given to me by Ron's widow Dorothy. Over the years I have built two others, Nos.5535 and 5538, so my options were more limited as they didn't appear on the S&C with anywhere near the same frequency as the Jubilees, albeit several Patriots were shedded at either Leeds Holbeck or Carlisle.

As my three were in Crimson Lake livery, I was keen to explore the possibility of one in the 1946 post-war lined black, although it was not absolutely essential.

You would have thought that, with access to a vast library of published reference material (see further reading panel), I couldn't go wrong, yet I did... to a degree. After studying all the shed allocations for every member of the class, I settled on No.5514, which was un-named originally. It became *Holyhead* in 1938, lost this from 1942-7, and regained it in the summer before Nationalisation. I found a photo of it at Bell Busk, the southern end of the S&C, in 1935, and all references concur that it was shedded at Leeds from 1932-5, plus Carlisle from 1939-41, and also in 1949; it was an S&C engine... and etched plates for *Holyhead* were in my kit... clearly it was meant to be. Also, for the statistically minded,

No.5514 at Camden MPD on 26 March 1938, prior to receiving its nameplates. The locomotive carries LMS Crimson Lake livery. *Photo: Rail-Online*

Holyhead recorded the highest mileage for any original Patriot during its lifetime, an impressive 1,507,446.

Another reason for selecting it was that, in the book *Claughton & Patriot 4-6-0s*, it was stated that it received the 1946 lined black livery in August 1947, but then I made my big mistake: I misread its rebuilding date… Oops. Hence my model is, to a degree, a figment of my imagination, as its rebuilding occurred just before its relivery… which makes sense.

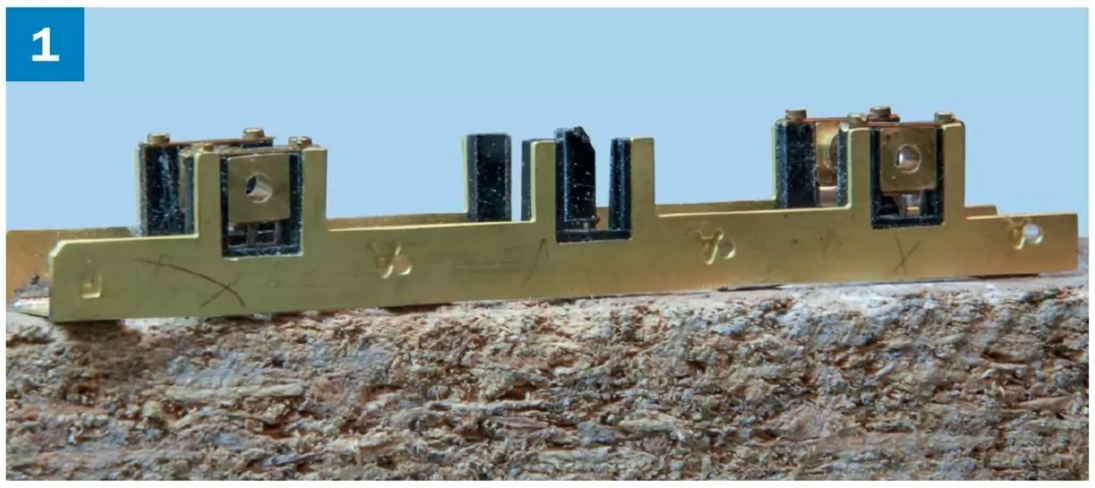
Excitement must have got the better of me. I suppose I could have just said nothing, and hoped to get away with it, but that would not be fair on fellow modellers who would have been misled, and could have perpetuated my error. So, if you want to replicate No.5514 *Holyhead* in its original form, simply paint it in LMS Crimson Lake.

The kit

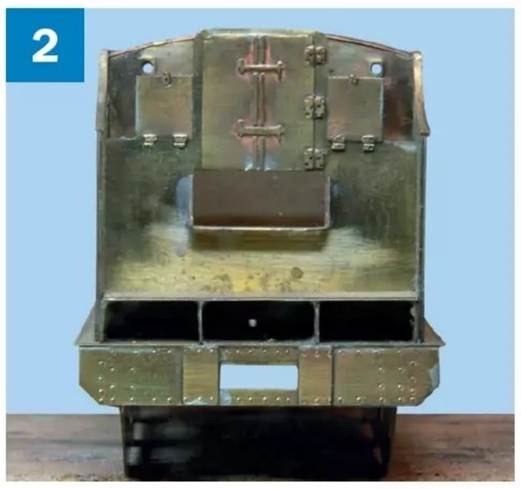
I recall Millholme Models in the early 1970s being based in an old school in the Nottinghamshire countryside. During that time I visited the manufacturer's base with my good friend, and partner in crime, Roger Ellis. We were impressed with the range at the time – they provided options not available elsewhere. That was 50 years ago, so it was not surprising that I found the kit rather 'old school', with relatively chunky castings, and barely a nod to finer details… brake gear, cab fittings, lamp brackets, injectors, cylinder drain pipes, etc.

To drag the kit into the present day, I drew up a shopping list. Firstly, as I had a partially-built body to hand, I used a Pendon etched Fowler 3,500-gallon tender. I do believe it was the test build conducted by the, oh so talented, and much missed, Tony Reynalds, to check that all parts fitted sweetly. They certainly did on the basic body. I added a Maygib sprung tender chassis that had been occupying shelf space for years. A smooth-running tender ensued.

The rest of my 'shopping list' were detail parts as follows: chimney and firebox backhead (247 Developments); LMS Crewe-built and tender plates (Modelmaster); lost wax brass

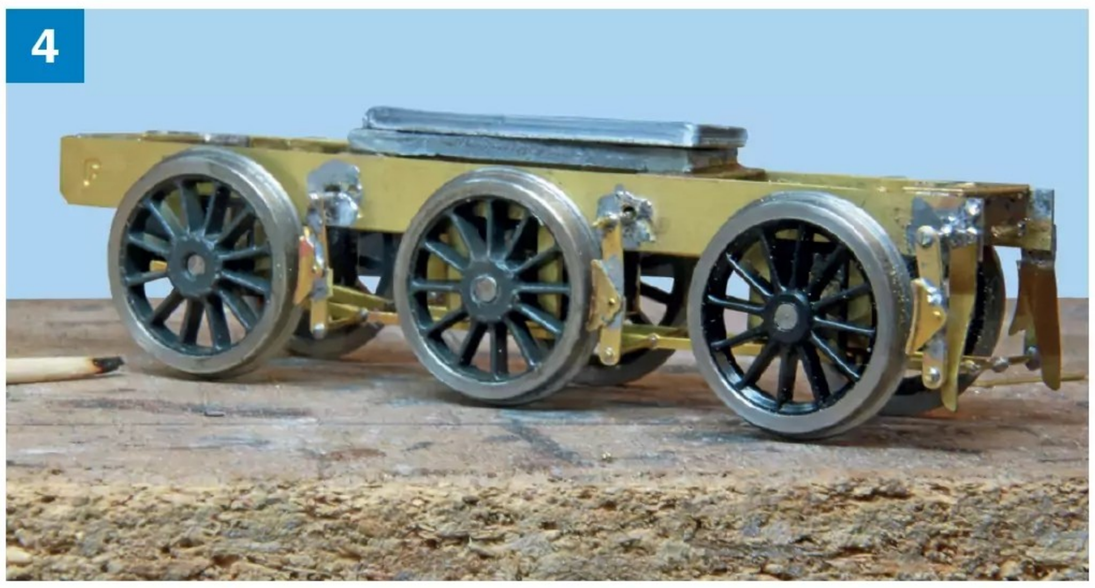


Construction begins with the Maygib sprung chassis, which was chosen to match the quality of my Pendon etched Fowler 3,500-gallon tender. Neat plastic guides sit in the etched cut-outs. Brass hornblocks, retained by a thin brass strip and 12BA cheesehead bolts, slide easily in the mouldings. The minute springs can just be seen below the hornblocks. It is a system that creates a very smooth running chassis for very little effort, and no great degree of skill.

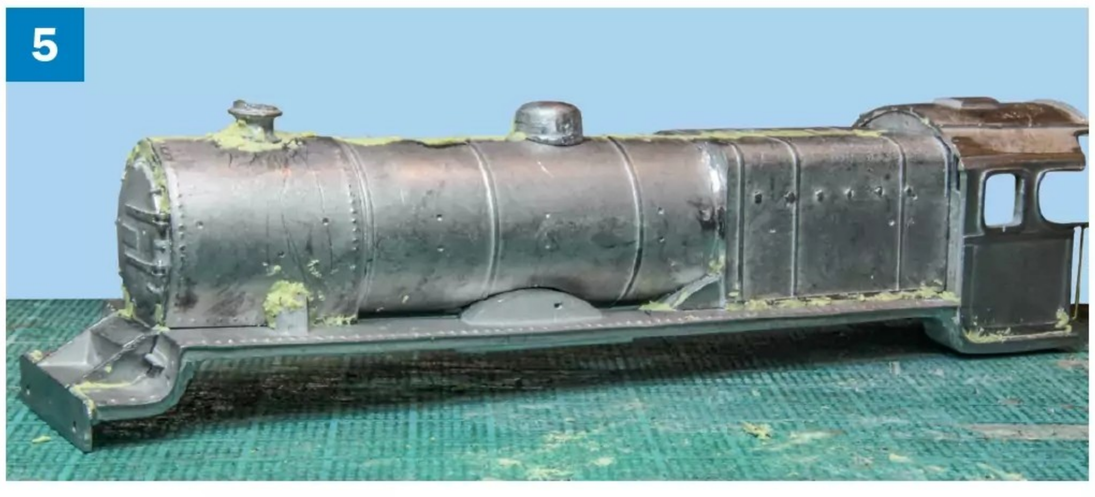


Whilst you won't see much of this when it is in service, it is a chance to appreciate the level of detail on the etchings. And remember, Pendon offered this kit 40-plus years ago; food for thought. I often wonder why Pendon had it in their range, as such a tender would never be seen anywhere near their layouts?

Beautifully turned fittings grace the tender rear, whilst I have beefed up the coal rail mounting brackets to make them better able to take the occasional knock.



With the fine Alan Gibson wheels in place, plus the brake gear and guard irons, the sprung chassis can be pushed through pointwork to test I have set the back-to-backs accurately on the wheels (14.5-14.8mm is my permitted range), and allowed sufficient sideplay to tackle my sharpest curves (3' radius). I like to weight my stock, yet not too much in this case or the springs won't be able to flex.



cast steam heat and vacuum pipes (Brassmasters); tender vents and tender dome (Jackson-Evans); tender water filler (Westward); sprung LMS buffers (Slater's); etched loco steps (Comet ref.LS33); etched brass smoke deflectors (Jackson-Evans… the packet was dated 1981); loco bogie and tender wheels, and loco brakes (Alan Gibson); screw couplings (Smith's ref.LP8); Mashima 1424 motor; and a Branchlines 53:1 Multibox gearbox. Some of the aforementioned suppliers are no longer trading but, fortunately, most items I had in stock already.

Construction begins

I put the chassis together first, adding the Romford 26mm coupled wheels so that I had a basic 0-6-0 to test for a fit inside the body. The latter I assembled with low melt solder and soon appreciated that clearances were tight, in some cases very tight, even for 16.5mm gauge.

Much scraping and fettling ensued. I had to resort to removing the tops of two splashers and replacing them with thin plasticard later in construction, all due to castings which have rather too much meat on them. It is fair to say that this took far more time than anticipated and I am sure that modellers have given up at this stage in the past.

I thought I ought to conduct a trial with the motor and gearbox. Somehow I couldn't achieve the mesh I wanted: it was simply all too tight. This was down to me, as I have found the Mashima/Multibox combination a reliable, smooth performer many times in the past. I had to accept defeat and substituted a High Level RoadRunner+ at 60:1 ratio, using a Mashima 1420 on its side as the fixing holes in the gearbox dictate this.

Next, I looked at the loco brakes, which were conspicuous by their absence in the original kit: they weren't a 'given' in the

One inevitable aspect of whitemetal kits is the need to fill any gaps between castings. In a perfect world, there wouldn't be any, but with shrinkage etc. that is rarely the case. Miliput is applied reasonably generously and then filed/scraped back once it has set. I just accept it as one of the routine stages of building a kit.

▲ **A chance to appreciate some of the front end enhancements. I prefer sprung buffers (Slater's in this case) to cast ones, purely a personal choice. The vacuum pipe is lost wax cast brass (Brassmasters) as it is far more resistant to knocks than a whitemetal equivalent. The front loco steps are from Comet, whilst the lamp irons are from scrap etch, and the slot in the bufferbeam has been cleaned out to accept the screw coupling. All these extras help to complete the overall portrayal of a Patriot.**

early 1970s. Again, clearances were minimal so I played safe and used Alan Gibson 4M104 Midland-style brakes which look right and are plastic, so totally eliminate the risk of shorting. They were fixed to the loco frame via stubs of 0.88mm plastic Evergreen rod into pre-drilled holes.

Two steps forward...

Road trials without valve gear were encouraging – running was smooth and stable, and the speed range was ideal, whilst the haulage power offered real promise and potential. Then I hit a stumbling block. Quite simply, I didn't find the Millholme etched valve gear fit for purpose, as there was nowhere to anchor the valve rod and expansion link. Perhaps a part was missing? I had, after all, bought it second-hand, yet the instructions didn't show, or mention, one. I put the valve gear together, yet it whirled in an uncontrolled, unrestricted manner under power – anything but prototypical. I tried fitting a Comet substitute set of valve gear, but there were too many discrepancies, so I bit the bullet, sidelined the original chassis, and put together a Comet one in its place. This was more expense, admittedly, but it did lift the working bits into the modern era.

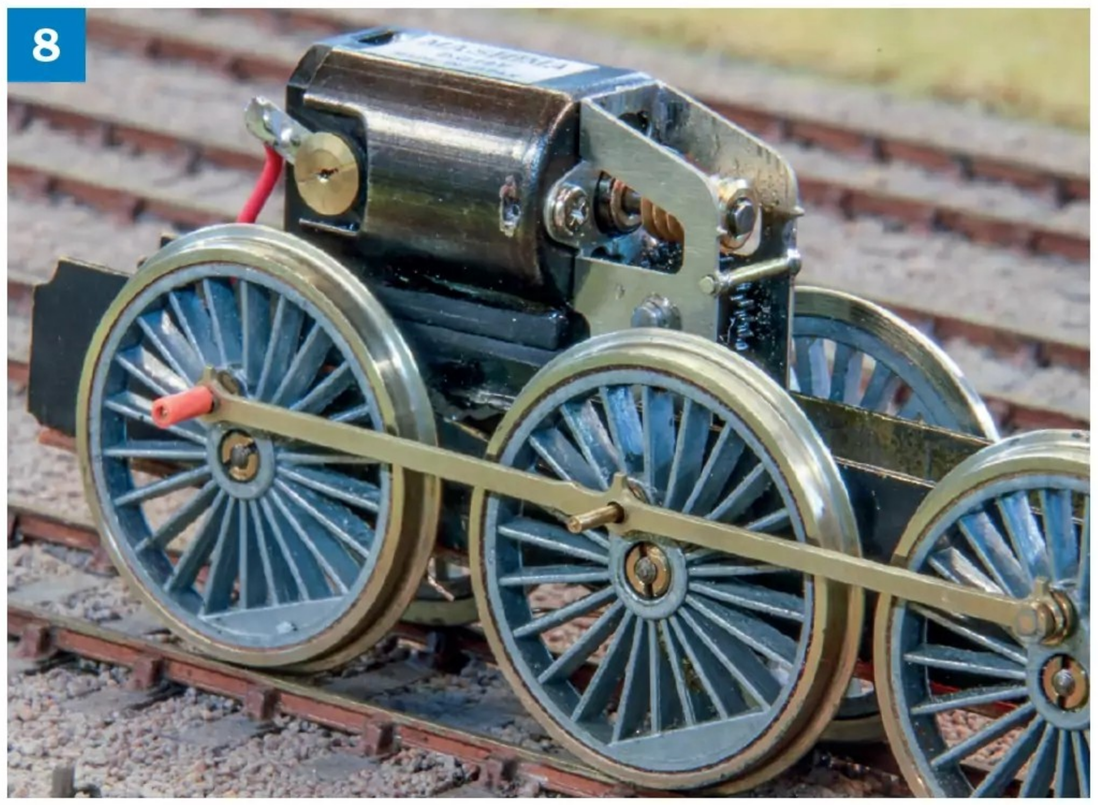
▲ **After a session on my rolling road, it was time to put my Patriot through its paces on *Dentdale*.**

The Comet offering is not designed specifically to fit the Millholme Models kit but, with some filing to the etched frames, it slotted into place with the cylinders flush against the underside of the footplate. I was able to reuse the motor/gearbox melange, plus the Romford coupled and Alan Gibson bogie wheels. As my body fixing nuts were already *in situ*, I had to ensure that two of the holes in the frame spacers aligned to facilitate fixing with brass bolts. Before soldering in the final chassis spacers, I conducted a trial placing of the motor/gearbox, which was still attached to the substantial plastic block (this helping to seat the motor on the Millholme chassis). Fortuitously, the block was an interference fit between the etched Comet frames, so the unit held itself *in situ* while I sought its optimum position. Once this was found, I could solder a frame spacer directly below the plasticard block, which was drilled and tapped to accept a fixing bolt.

My order of events for conducting road trials is as follows:

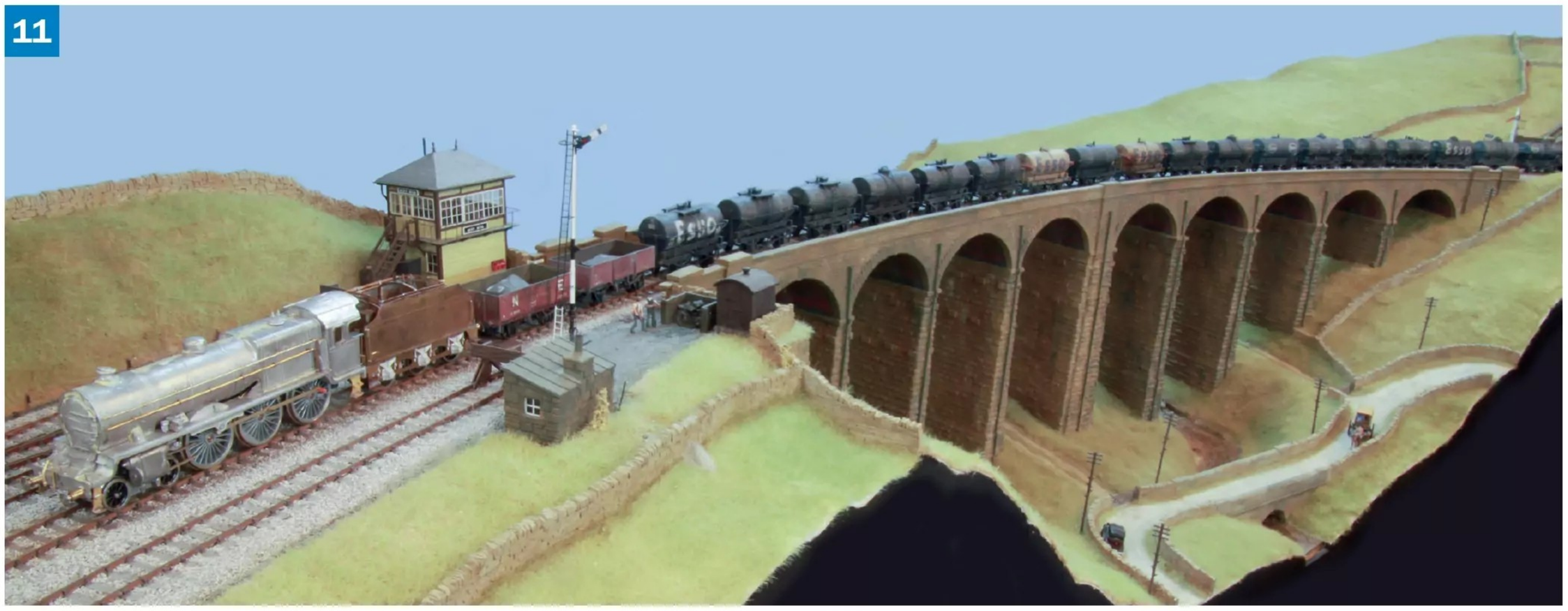
i) Ream out each soldered brass axle bearing so that the coupled wheels rotate unhindered.

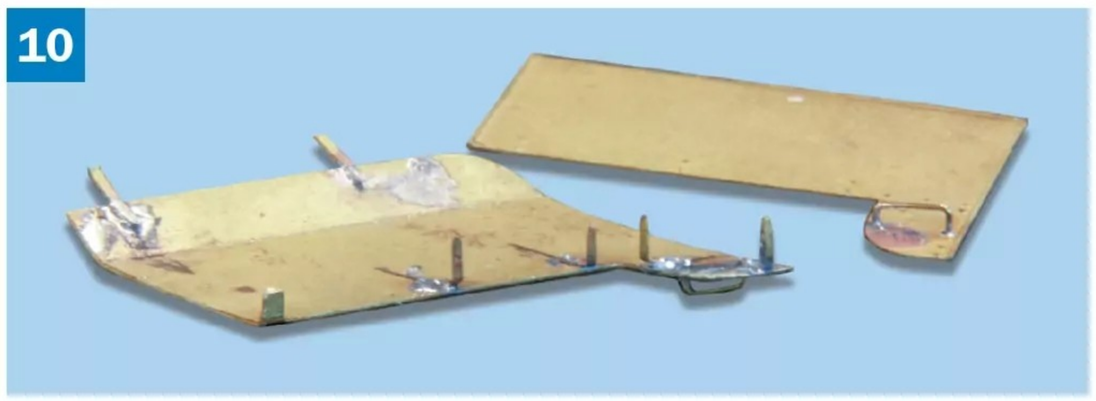
ii) Fit the coupled wheels to check for squareness on a plate glass surface; my chassis needed a little tweak/twist to achieve this, which is possible before the extra spacers are in place.



▲ **A chance to view my Mashima 1420/High Level 60:1 RoadRunner+ combination at close quarters. Note that the front crankpin has a washer soldered to it to retain the coupling rod, with the excess cut off and filed flush: this is because clearances between it and the slidebar are minimal. I have used a temporary fixing of connecting wire sleeving on the rear driver crankpin, making it much easier to take off the wheels during construction should the need arise.**

▲ **This kit does not come with a firebox backplate, cab floor or fallplate between the loco and tender. The first is a casting available from 247 Developments, whilst the etched planked floor is a surplus panel from a Midland Railway signal box kit, and the hinged fallplate is hand-built... its construction is described in the main text and Fig.2. I've used this approach on many of my locos.**





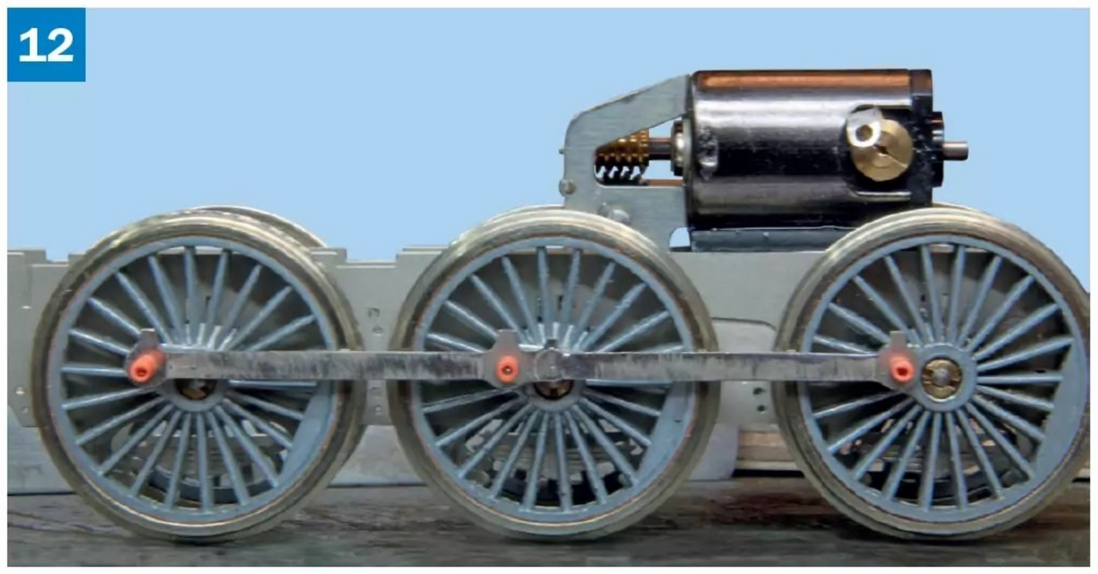
▲ **The etched smoke deflectors are a significant upgrade on the cast ones supplied, although you do have to attach a fair number of small fixing brackets. The one on the nearest corner is an extra one of mine to help the smokebox sit securely on the Millholme Models footplate.**

iii) Push the chassis through various bits of pointwork and round your tightest curve (3' radius in my case) so you know you have sufficient sideplay.

iv) Solder up the double-thickness coupling rods. Comet claims that the Patriots sported both plain and fluted rods. This statement is true, but it is heavily weighted in favour of plain. In fact, having gone through all my Patriot reference books, I can only find the first two, Nos.5500 and 5501, with fluted rods. Hence my model of No.5514 has plain coupling rods.

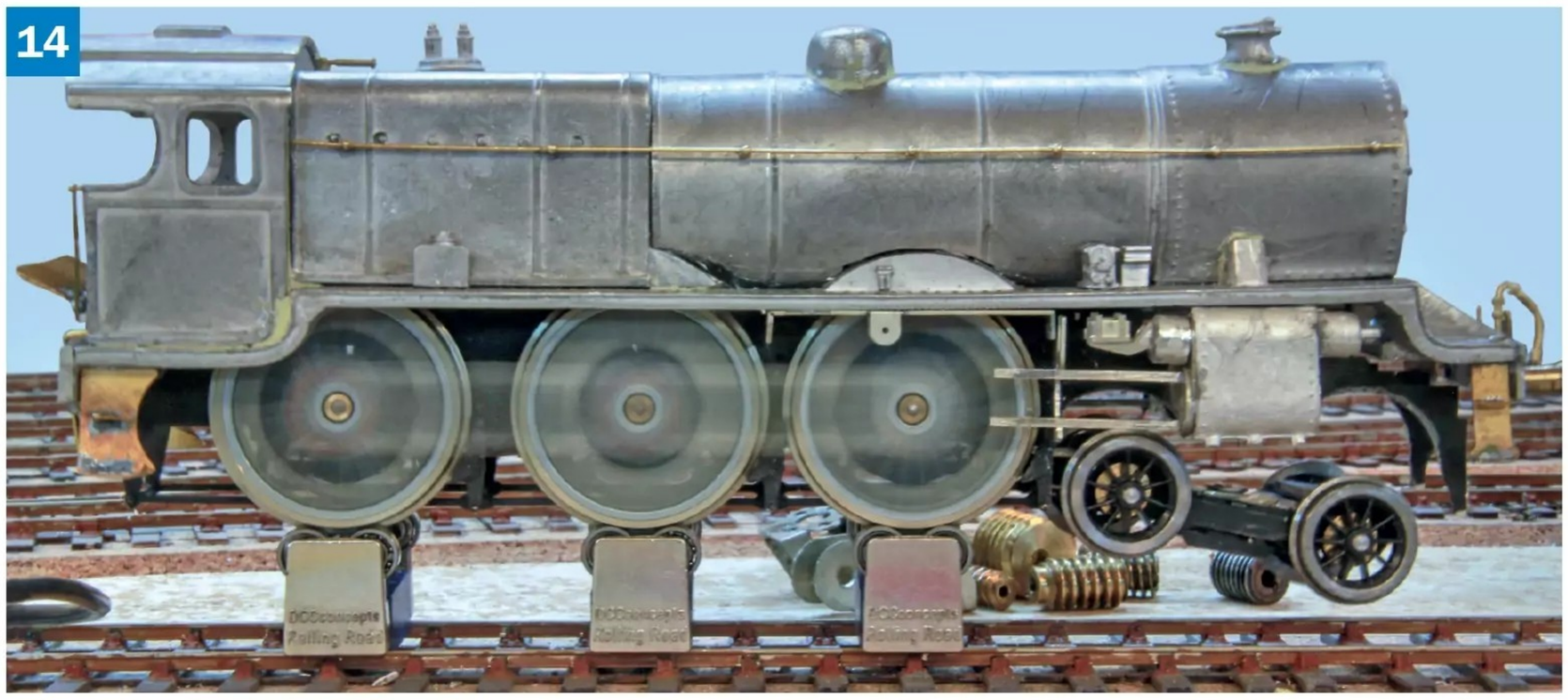
v) Drill out each crankpin hole to 1mm and ease out slightly with a reamer. Fit temporarily by

▲ **With 23 Esso tank wagons, in various guises, two barrier wagons, and a brake van in tow (which will be described in a future article) my Patriot is given a stern test up my 1 in 100 gradient over Dent Head Viaduct, although, in reality, the 'Long Drag' was level over this short section. I needed to gain more headroom for clearances for my storage loops... needs must. The loco came through the trial well.**



▲ **After hitting the proverbial buffers with my Millholme etched valve gear, I changed tack and substituted a Comet etched chassis, plus the associated cylinders and valve gear. Life is invariably more straightforward when everything matches. I was able to reuse the wheels, motor and gearbox.**

▲ **The Comet chassis has received its brake gear and a couple of coats of Expo Tools Matt Black Primer, which is so much easier to do before fixing the wheels *in situ*... I wonder how I know? For comparison, what is to be No.5514 sits alongside my renamed, detailed Bachmann model of sister loco No.5535 *Sir Herbert Walker K.C.B.*, very much a stalwart of the Settle-Carlisle line.**



The relatively slow shutter speed makes the wheels' speed seem considerably faster than it actually is, but this makes for an interesting shot. Rolling roads can be set up on any length of track. This is right at my baseboard edge, and at eye level when I am sitting down, hence I can view things at close quarters, and assess the situation accurately.

using short sleeves of connecting wire insulation pushed onto the crankpins.

vi) Add the motor and gearbox.

vii) Test on a rolling road with temporary jump leads.

viii) Once happy… a bit more reaming may be required… glue copper-clad paxolin strips underneath the chassis to accept phosphor bronze wire (0.35mm) or 1mm strip as pick-ups.

ix) Test under power on the rolling road, checking that each individual wheel picks up power, then repeat on the track.

Seeing it move under 'its own steam' is a major step forward – a feeling that progress is being made.

The Comet instructions suggest a dry run with the etched motion bracket support to check the body sits correctly, with no gap between the cylinders and footplate, plus the buffers are at the correct height. This was even more sagacious advice in my case as the chassis and body were not specifically designed to match, despite being the same class of loco. I had to remove the cast motion bracket from the footplate as an etched one was now provided as part of the Comet chassis.

The lost wax brass cast crossheads seem a good idea, but need fettling in two ways. First, the recess to accept the connecting rod was partly filled with surplus brass casting. I spent a nervous time with a 2.5mm bit in my 12V mini-drill milling away the excess… I'm not sure how else I could have overcome the problem.

There is no doubt that the smoke deflectors provide added character to the look of the Patriot; the one or two locos which ran without them in their very early LMS days didn't seem such handsome engines. And, in my opinion, neither did those with the straight smoke deflectors, as opposed to the cranked ones featured here. On the further deflector you may spot the tiny bracket which connects the deflector to the drop in the footplate. Also in evidence are my home-produced cylinder drain pipes, formed from 0.4mm brass wire soldered together and fixed into 0.45mm holes drilled into the bottom of the cylinder casting.

Secondly, the channels for the slidebars need filing deeper to achieve an easy sliding fit; not ideal on either count.

For connecting the valve gear parts, Comet provides fine rivets, although I opt to use pins (either 0.45 or 0.56mm diameter from Ambis Engineering), not forgetting to push a thin piece of paper onto each pin to separate the two moving parts whilst soldering. This cuts out the risk of soldering a joint up solid. The paper can then be torn out, with the surplus pin snipped off and the remainder filed flush. It makes a neat, workable joint.

A series of shorts

I found I had a mysterious, intermittent short-circuit once the body was on: this only occurred whilst traversing left-hand curves. Investigation showed that the curve was pushing the rear insulated coupled wheel across, making it nudge the inside of the firebox. More paring of whitemetal ensued, but I moved the chassis fixing point 0.5mm across and lined the inside of the firebox with insulating tape. Problem solved.

Feeling I'd overcome a potentially major hurdle, I assembled the bogie. Initially, I struggled to make it sit right. First, it wasn't square (resoldering solved this) and then I tweaked the angle of the swing link that connects the bogie to the chassis, to ensure weight was shared equally on the two axles. Clearances are tight, despite the fine flanges on the Alan Gibson wheels. Hence I filed the bogie cut-outs back a bit, and also the backs of the front loco brakes.

It was infuriating to find there was still an occasional short, caused by the rear axle lifting slightly when in motion. I added two small rectangles of lead

By clamping a piece of card behind the cab window, I can push-fit the Perspex cab glazing and it is supported whilst the glue sets… and I am not on commission from Deluxe Materials, merely a satisfied customer. The extra handrail below the cab window is also apparent, for which I used 0.4mm brass wire.

The vacuum ejector is a relatively complex fitting and, whilst I haven't been able to include all elements, I do believe I have replicated most. It is a significant improvement on the basic box supplied with the kit. As with similar situations, I have used the relatively flexible brass mesh from wine bottles for the fine piping.

flashing to the bogie to aid road holding and help it have more presence on the track. I tweaked the swing link a final time and crossed my fingers. Job done.

I am beginning to think I am reporting a catalogue of challenges. All kits throw up issues, but there is no doubt that this has created far more than average; although it ups the sense of achievement when it's finished.

Fixing the return cranks to the centre driver crankpin proved troublesome as I struggled to achieve a sound solder joint. Inspection showed that the crankpin had partially pushed the paper washer up the hole in the return crank. Trying to solder to paper is not a great idea. Reseating the paper washer overcame this particular hiccup.

Feeling more confident, I added the tender… shorting out occurred, again only on a left-hand curve. I knew where to look immediately and, by inserting a 1mm thick washer on each of the two outer tender axles, their play was restricted and the short eliminated.

Finer details

Although you can buy cylinder drain pipes in either whitemetal (Comet) or lost wax cast brass (Brassmasters) I fashioned mine from soldered 0.4mm brass wire using photographs in my reference books as a guide.

A vertical steam pipe goes up from the footplate on the left-hand side just before the smokebox. The joint to the ejector pipe is just represented by a whitemetal box. In reality it was far more complex, so I did my best with spare parts in my collection of 'I'm sure it will be useful one day' bits, and some fine wire teased to match the photographs I had, and then soldered in place. (Page 91 in *Claughton & Patriot 4-6-0s* proved very helpful.)

Painting

I have already discussed the whys and wherefores of my choice of 1946 lined black livery, so I could opt for my preferred approach of Expo Matt Black Primer, in aerosol form, both as a base and a top coat.

HMRS Sheets 2A and 3 cater for the lining and insignia, with a couple of notable exceptions. Lining Sheet 3 does not include the arcs of 1946 maroon edged with a single yellow band to fit the two upper curved edges of the Fowler tender. There were some of the correct profile, but they were black with a yellow band. I had no real alternative but to use these and paint in the maroon band with a 10/0 brush… gently does it.

Similarly, the curve from the footplate down to the bufferbeam should be maroon with twin yellow edges. The sheet only provided one with a single yellow edge, which I fitted. Cutting just the yellow band from a spare transfer allowed me to include the yellow edging line. Fiddly? Most definitely.

I was unsure whether the two lined bands at the front and rear ends of the boiler had one or two yellow lines flanking the maroon panel. Photographs in *Claughton & Patriot 4-6-0s* were inconclusive, but p62 of *A detailed history of The LMS Patriot 4-6-0s* showed both an original and rebuilt Patriot in 1946 livery. Both had double yellow bands, so I could proceed. Once fitted, all transfers were sealed with a coat of matt varnish – Tamiya ref.XF-86 in this case.

My valued friend Colin Brown, a fellow S&C devotee, invariably shows an interest in my current projects, so at this stage I sent him a few photos of progress. His knowledge is deep and he loves delving. He was a tad concerned that my cab lining didn't seem quite right, as he was familiar with a rectangular block of lining below the cab window on the rebuilt versions, and questioned whether the original Patriots were similar. This forced me to recheck and I do believe my version is fine, with one caveat. One photo (p82 of *Claughton & Patriot 4-6-0s*) showed it continued round the cab cut-out, but it was the only evidence. Should this be proved to be the rule, as opposed to the exception, I will alter mine accordingly.

However, in the same reference mentioned above, I spotted an extra maroon band with a double yellow edge applied to the very rear of the firebox, immediately in front of the cab. Other photos do not give clear evidence one way or the other. Was this the norm? If this is so, and I am alerted, I will add the missing band and weather it lightly to match the state of the loco. This does emphasise just how challenging it can be to portray a loco in a particular period accurately…

Missing parts

I was congratulating myself for seeing light at the end of what had been a very long tunnel, when I checked my efforts against photos. All was not there… no sandboxes, no sand pipes, no reversing lever, no handrail below the cab window, no smokebox door handrail, no smokebox door lamp iron, no glazing in the cab windows, no cab doors, no fallplate… oops, a veritable catalogue of obvious omissions… a senior moment? The reason I overlooked them was, to me, obvious: apart from the reversing lever, there had been no mention of them in the instructions, nor any bits provided. I set out to put matters right.

Sandbox fillers I found in my spares box and I made the sandboxes from plasticard blocks, filed to shape. The rear ones are footplate-mounted, whilst the others are fixed to the chassis. All have 0.6mm brass wire as sand pipes, glued with both superglue and five-minute epoxy (belt and braces for parts that could be knocked easily).

The cab glazing is 30thou Perspex, cut fractionally large with a jeweller's piercing saw, then filed back gradually to be an interference fit and secured with a sliver of Deluxe Materials

Despite owning four reference tomes on Patriots, and countless other books on LMS locos, I struggled to locate a photo clearly showing the layout of the lining on the tender rear. This is my interpretation; I hope I have it right. The LMS registration plate and the oval one denoting its 3,500-gallon capacity stand out, hence I feel it was worth the expense investing in them from Modelmaster.

Apart from its coal load and judicious weathering, the tender is complete and fit for purpose. Sadly this Pendon kit is no longer available; it does produce an accurate, quality model.

Glue 'n' Glaze, which dries clear. By clamping card temporarily behind the cab window opening, wedged in with a cocktail stick, it meant the Perspex had support whilst the glue dried. I fashioned the reversing lever (the original had gone AWOL) out of 20thou plasticard… not too arduous a task.

The cab doors and fallplate from cab to tender are both scratch-built, and both are designed to hinge freely to provide flexibility. The accompanying diagrams (Figs.1 and 2) should, hopefully, make my approach clear; they cost next to nothing, which is a bonus.

To finish matters off, I painted a couple of appropriate 3D-printed loco crew: one doing 'not a lot', the other struggling with a shovelful of coal. I chose to fit loco lamps, centre and right, to depict a parcels service, or a train carrying perishable goods and formed of coaching stock… both suitable for my Patriot.

I try to weather all my models, although my Patriot warranted a relatively light touch as it received its repaint only months before the end of my era of modelling. My usual palette of Humbrol Nos.26, 33, 34 and 70 was employed, albeit in diluted, muted tones. Some light brushing with rust/black weathering powders proved beneficial.

No.5514 was now fit for purpose and ready to earn its keep on *Dentdale*. I won't deny that it occupied more modelling time than intended – I estimate around 150 hours. Hence it is not a project for the faint-hearted, and, would you believe, I have another stored away for the future… but not just yet.

Postscript

I had just finished typing up this article when I obtained a copy of issue No.2 of *Midland Times*, which includes a significant article on the 'Resort Named Patriots of the LMS'.

This confirmed most of the facts I had collated on No.5514, plus a couple of unseen photos, one of which showed the loco with a nameplate backing plate, yet no nameplate… hence probably in mid-1938, just before the naming ceremony. Such snippets help to create the overall picture and history.

Fig.1

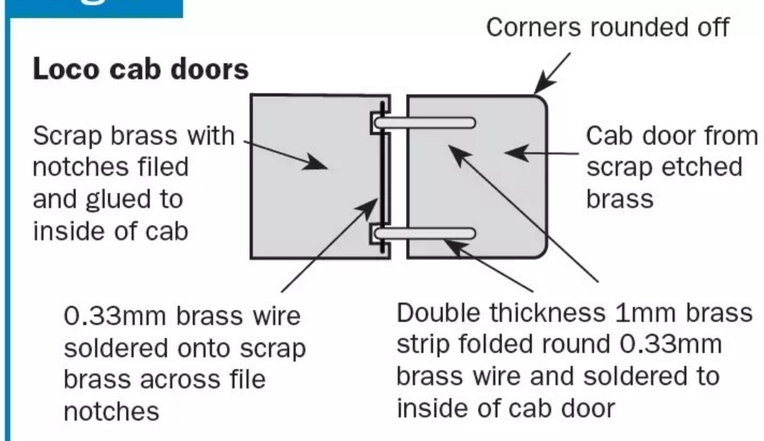
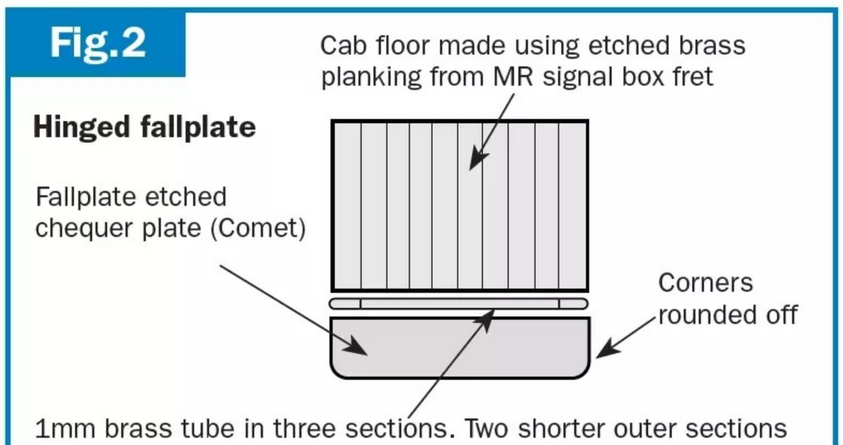
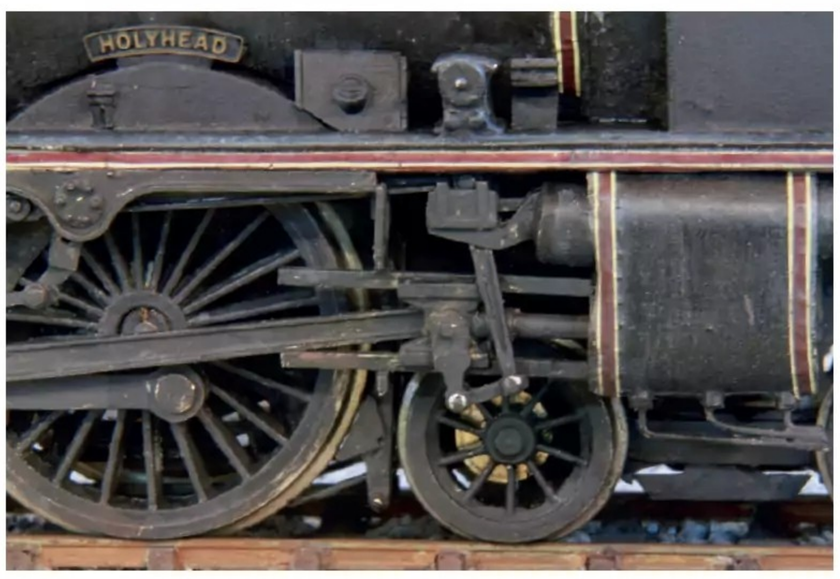


Fig.2





Because I could only give a light degree of weathering at most, I found the task quite testing, purely because I had to be so restrained. I have tried to portray the impression of a loco that has been in service for only a couple of months since its repaint. Hence it is no longer pristine, but no rust patches are evident.

Close-ups can be cruel, but this does give a detailed view of the valve gear and the sandbox filler with its bottom corner angled off as per the prototype, and it illustrates the limited room for the front loco brakes between the leading coupled and bogie wheels. The fine (0.45mm) pins I use to connect the valve gear parts are also apparent.

It should be clear how both the cab doors and fallplate hinge, whilst the driver is definitely at ease. I had to chop 3mm off his legs, which I don't think is too obvious, as he was too tall for the cab.

Further reading

The Power of the Patriots
by J S Whiteley and G W Morrison
(OPC 1997)

Claughtons & Patriot 4-6-0s
by G Toms and R J Essery
(Wild Swan 2006)

A detailed history of The LMS Patriot 4-6-0s – including 5551 The Unknown Warrior
by John Jennison
(RCTS 2018)

British Steam: Patriots – Creating the New National Memorial Locomotive
by Keith Langston
(Pen & Sword 2011)

Welsh narrow gauge in 4mm

Tan y Bwlch

A different take on an oft-modelled Ffestiniog Railway location, by seasoned OO9 modeller **RICHARD HOLDER**.

Layout photography by the author, except where credited

Double Fairlies passing: *Merddin Emrys* is in the Down platform with the Victorian carriage set, whilst *David Lloyd George* waits in the Up platform with a rake of slate wagons.

Regular readers of RAILWAY MODELLER may remember seeing articles about my previous OO9 exhibition layouts. *The Clydach Railway* was my first attempt and the story was published in the November 2010 edition. The layout attended 42 exhibitions between 2010 and 2022, but has now been retired from the show circuit.

My version of the Wantage Tramway came next, appearing in the September 2013 issue, and so far has attended 46 exhibitions, with bookings well into the future. I built *Launceston Steam Railway* during 2015 and 2016, and it first appeared in public in 2017. It featured in RM in September 2017. So far, it has attended 48 shows. My little model named *Woodbury Wharf* appeared in RM in August 2019, and was my most recent creation before I started on this, my latest project.

Why Tan y Bwlch station?

When Bachmann introduced its ready-to-run Double Fairlie in OO9 a few years ago (see review, RM January 2022), I could not resist buying one. A friend bought a sound-fitted one and, although he was using it on an analogue layout, the 'chuff' really did add something. Soon afterwards, I saw one being operated using Digital Command Control and it was very impressive, DCC taking things to another level. So, after many years using 12V dc to operate my OO9 models, most of which were kit-built, I toyed with the idea of using DCC for my next layout project.

I soon bought a sound-fitted Double Fairlie and also set about converting my first Bachmann Double Fairlie with digital sound. Then, of course, came the Bachmann 'Main Line' Hunslets (RM January 2023) and the Bachmann Quarry Hunslets (RM July 2024), not forgetting the Peco/Kato Small and Large Englands – all very finely detailed models and all associated with the Ffestiniog Railway in North Wales.

The Double Fairlies have always been intrinsically linked in the UK with the FR and, although we can now see these delightful models running on many and varied OO9 layouts, I thought that if I was going to build a new exhibition layout, it had to be based on the Ffestiniog…

Tan y Bwlch station has long been a popular choice for OO9 modellers so, at first, I discounted this location. I really wanted to model an area that was new to the

exhibition circuit. However, once I looked at other options on the FR, I soon realised that the operational and scenic potential of other stations on the line was limited. I did not want to model the terminus stations at Blaenau Ffestiniog or Porthmadog, as I wanted through running. So, back to square one. How could I make my model of Tan y Bwlch different from those that have gone before?

After some thought, I decided to model Tan y Bwlch station as viewed from the opposite side of the tracks to the car park, i.e. looking down on the platform from behind Station House, looking towards the car park.

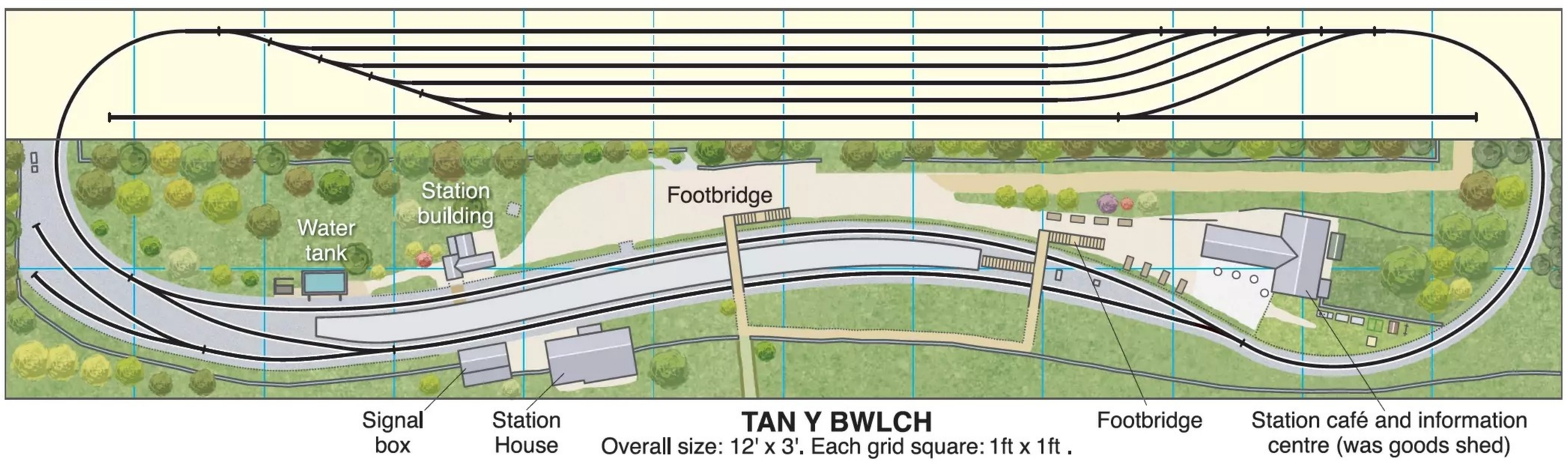
▲ **Quarry Hunslet 0-4-0ST *Britomart* arrives with a demonstration goods train.**

Baseboards and operation

I opted for the same baseboard kits that I had used for my Launceston model. This enabled me to reuse the same bolt-on legs, the baseboard curtain, and lighting rig. This has saved both time and money. The baseboard kits, obtained from Model Railway Solutions of Poole, are cut accurately and are easy to assemble. The three scenic boards each measure 4' x 2', and the three fiddle yard boards each measure 4' x 1'. The whole layout measures 12' in length and 3' in depth. When crated up, the whole layout will just fit into the back of my car.

The operating position is at the rear. I am very new to DCC, so have only taken tiny steps. The trains are controlled using an NCE PowerCab. I was recommended this system as one that would suit a newcomer to digital operation. Points, colour-light signals and automatic uncoupling are all operated using methods that I have employed

◀ **New-build Lynton & Barnstaple Manning Wardle 2-6-2T *Lyd* enters the station with the 'Woodland Wanderer' from Porthmadog.**



TAN Y BWLCH
Overall size: 12' x 3'. Each grid square: 1ft x 1ft .

Large England *Welsh Pony* prepares to depart with the Victorian carriage set.

successfully for many years. Greenwich couplings are used on the ends of rakes of coaches, and SEEP electromagnets, designed for use with N gauge, are used for automatic uncoupling. I have also experimented with magnetic couplings, some from Porterhouse Models and others from GLR Bespoke Services, where automatic uncoupling is not required. These are simple to fit to coaches and wagons, especially those with NEM sockets.

On the whole the trials have been encouraging, except for the longer coaches where the couplings' limited ability to articulate was causing the coaches to derail, especially in the fiddle yard, where 12" radius points were used. I did try the pivoted couplings available from Porterhouse Models, and they did work well, but had limited strength when I tried them on long, heavy rakes of bogie coaches. I also wanted to have the coaches reasonably close-coupled, but these couplings did not provide the closeness I was after. However, friends came to the rescue. David Cooke designed a suitable magnetic coupling using CAD software and Stuart Taylor printed them in resin for me. David still had to construct each pair of couplings by fixing the tiny magnets and the metal pivots. So far, tests of these newly designed couplings have proved to be very successful.

Rolling stock

I wanted the opportunity to run a wide range of coaches and freight wagons that represented the historic heritage of the line. The Ffestiniog Railway runs special events during the year, including what is called the 'Bygones Weekend' at the beginning of October. The event gives the FR opportunities to run a variety of coaching and freight vehicles, including carriages from Victorian times, the 1930s, and the 1960s as well as demonstrations of freight and gravity slate trains. So, the 'Bygones' event became the focus for my model.

Peco has produced various versions of the 'Bowsider' coaches and the four-wheel 'Bug Boxes', and Bachmann released its 'Tin Car' coaches at the 2024 'Bygones Weekend' at Porthmadog.

Peco has also produced a model from its Lynton & Barnstaple bogie coach tooling as FR carriage No.14, known as 'The Snapper Bar'. The Peco model captures the exterior of the coach really well, but the interior is the same as used for the Peco L&B stock. Fellow OO9 modeller Michael Farr kindly sent me an interior and other details that he had created to provide a more accurate representation of the current interior of carriage No.14. Michael also sent me a great deal of very useful information about Tan y Bwlch station, including some scale plans of Station House (more anon).

I have supplemented the Peco and Bachmann carriage models with a number of other FR model coaches and wagons using 3D-printed kits from Fourdees and Nigel Brooks, together with brass etches from Worsley Works, and plastic kits from Dundas Models.

Buildings and structures

Tan y Bwlch station has a large 1980s stone-clad water tank with two water pipes with 6" pull-valves to feed both ends of a Double Fairlie in two minutes. The original, smaller, tank, of which the stone plinth stands nearby, had a single, smaller pipe and took

No.190 *Lyd* ready to return to Porthmadog, with the 1960s carriage set being used for the 'Woodland Wanderer' service.

▲ *Britomart* trundles through the station with a demonstration goods, as a packed passenger train waits to continue to Blaenau Ffestiniog.

five minutes to fill the Fairlies. The fitting of larger valves here knocked three minutes off the journey time! Whilst searching online for photos of the water tower, I discovered a delightful 3D print of the structure from an online retailer called MS Models. Clearly, this was going to give me a good start, and would save me time compared to scratch-building the tower. I modelled the remains of the base of the original water tower using Wills slate walling sheets.

Very soon afterwards, MS Models released a beautifully detailed model of the small station building. I quickly ordered one of these too and emailed Mark, the proprietor of MS Models, to ask if he had any plans to release any more of the Tan y Bwlch buildings. He replied very promptly saying that he hoped to model all the structures eventually, but had no idea how long it would be before they would be available. Once I had added the small details provided to each of these complete buildings, I painted them ready to add to the layout.

I next set about constructing a model of the 'signal box'. The box was built for a signalling installation that was cancelled, but was subsequently used to house the relays for the current signalling. I used Wills building sheets and detail packs to build my model. My friend and regular co-operator at exhibitions, David Cooke, 3D-printed the windows for me, which saved a huge amount of time. David also printed the windows for the station house, which I also built using Wills plastic sheets. The roof tiles of the station house were a challenge, as they appear to be of a half-hexagon design, and I could not find any online suppliers that produced anything like them in 4mm scale. In the end, I downloaded a sheet of hexagonal graph paper from the internet, reduced it to the correct scale on a copier, printed the hexagonal grid on thin grey card, then sliced the card up using a guillotine, and stuck the resulting strips onto the roof by overlapping each strip to the one below. The decorative ridge tiles were obtained online from Smart Models.

Since I built my model of the signal box, the small station building, and the station house, Peco has released a laser-cut kit of the small station building, MS Models has now released the signal box and Station House as 3D prints, and Bachmann has now produced a number of colour variations of Station House as part of its Scenecraft range. However, the latter are not models of the building as it stands today – these represent the structure when it was composed of two

The station café and part of the children's play area. ▶

small semi-detached cottages. These have subsequently been combined as one 'dwelling', with a small extension on the rear.

MS Models has also created a model of the original goods shed that stood at Tan y Bwlch. Nowadays, the extended and altered building houses the station café and information centre. Once again, David 3D-printed the windows, and I constructed the model using Wills slate walling building sheets, and various doors that closely match the doors on the current building. Some of these are plastic, some brass, and one is a laser-cut product. Self-adhesive weathered roof slates from Scale Model Scenery were used on the roof of this model.

There are two footbridges at the station. Bachmann has produced a model inspired by the wooden structure that stands at the station today. However, this is not a true likeness, as the real bridge has just one set of steps from the car park side that takes passengers across the two tracks and the island platform, to give access to the footpath (formerly an ancient Roman road). The original footbridge was erected in 1873, but removed in 1933. The replica that exists today was installed in 2012. I built my model using parts from the venerable Dapol/Airfix plastic footbridge kits, which are still readily and cheaply available.

The southern bridge was completed in 1971 (adapted from an ex-British Railways bridge) to provide access to the island platform when that was put in. It had to be at the end of the platform as otherwise there would have been insufficient platform width to pass beside the stairs. It also serves the footpath and was refurbished in 2012, and again in 2020. I have built a model bridge to stand at this position, once again using parts from the Dapol/Airfix kits and other plastic and metal parts that I had in stock. Eventually I hope to replace this structure with a more accurate representation of the real footbridge.

A lady spends a quiet few minutes in thought whilst examining the names on the war memorial in the station car park.

Double Fairlie *Merddin Emrys* prepares to return to Porthmadog with the Colonel Stephens carriage set.

Scenery

The basic landform was built using Celotex insulation sheets, available in a number of sizes from builders' merchants. The material is easy to cut and shape with a saw or sharp knife, and can be fixed in place using PVA glue. I usually cover this base with Woodland Scenics fine turf scatter, also fixed in place using PVA. I find that this provides a sound base before adding the static grass.

I used WWScenics autumn 4mm static grass, layered where necessary. I generally add small flowers and weeds to the final surface to give a more natural and colourful finish. I reused trees from some of my earlier layouts. Most of these were built using Woodland Scenics plastic armatures, covered with a wide variety of colours and textures of 'foliage'. I have supplemented these with more trees built the same way, and others that have been bought online from the Model Tree Shop, Primo Models, K&M Trees, and the 4D Model Shop.

A lot of the scale figures have also been recycled from earlier layouts, but I have also made use of quite a few new models from Modelu. These include the varied characters on the gravity slate train, some photographers, and a few figures dressed in period costume to mimic the visitors and volunteers who arrive at the 'Bygones' weekend suitably dressed for the occasion! There is also a model of yours truly sitting on one of the platform benches.

I commissioned Stuart Taylor (ST Models) to 3D-print the platform benches for me, using photos and measurements that I provided. Stuart is also a member of the OO9 Society and editor of *OO9 News*, the monthly journal of the society. He regularly used to help me exhibit my layouts when we both lived in Oxfordshire. Stuart produces a wide range of 3D-printed items for railway modellers, particularly those interested in narrow gauge.

The dry-stone walls are the same as I have used on previous layouts, and often spark off conversations when I am exhibiting. They are

FR diesel *Harlech Castle* waits in the station with the permanent way tool vans.

plaster castings bought from Ten Commandments that, with careful painting, can produce some pleasing effects. They need to be 'broken' to follow undulating ground, but gaps can be filled easily using household filler and textured before painting.

The cliff faces were created by applying a mixture of household filler, PVA, and black paint. Scrunched-up kitchen foil was pressed into the mixture and allowed to dry, before peeling it off and painting the dry mixture with various shades of grey and brown.

The road vehicles in the car park are mostly Oxford Diecast models that I have managed to pick up at various exhibitions and online.

The island platform is foamboard, cut to shape and glued in place with PVA. Edging is formed by numerous narrow strips of plastic, cut into suitable lengths to represent concrete kerbing. It is all painted using emulsion matt paint from DIY 'tester' pots.

The children's play area near the café is created using a kit available from Gaugemaster, with a few additional scratch-built items. The children playing there are figures purchased from Pete Goss, the well-known and gifted modeller.

Track

All the track is from Peco. I used the flexible code 80 Streamline 'Main Line' sleeper track, available in 914mm lengths (ref.SL-404). 'Main Line' 18" radius Electrofrog points were used on the scenic boards, and small (12") radius Electrofrog points in the fiddle yard. Slow-action MTB MP1 point motors operate the points on the scenic side of the layout. These were purchased originally from DCC Train Automation and include a built-in switch for providing power to the point frog. They are operated using a 12V dc supply and switches on a mimic board control panel.

Operation of the fiddle yard points is achieved by using SEEP PM1 point motors available from Gaugemaster. These also have a built-in switch to change the polarity of the point frogs. The fiddle yard points are also operated by switches on the control panel, with power provided by a CDU (Capacitor Discharge Unit).

Signals

Originally, the Ffestiniog Railway used some rather unusual signals that were circular red discs or rectangles that rotated. They also used more familiar semaphore signal designs. However, in more recent times, certainly at Tan y Bwlch, colour light signals are in use. I have represented these using Eckon kits. These are the simple red/green platform starter signals, which have been recycled from previous layouts. Calling-on signals have been added to the posts of these signals. These were obtained from Absolute Aspects, this manufacturer also producing the Limit of Shunt signals. At Tan y Bwlch these restrict the distance from the station along the main line that locomotives are allowed to travel when running around their train – for instance, when a train runs from Porthmadog to Tan y Bwlch and returns to Porthmadog (the 'Woodland Wanderer'), instead of continuing the journey to Blaenau Ffestiniog.

The gravity slate train rattles through Tan y Bwlch on its way down the valley to Porthmadog. A second motorised slate wagon is under construction which, it is hoped, will allow the train to operate more effectively.

The gravity train

Hauling a long rake of slate wagons up the line, using one of the Double Fairlie locomotives, is not difficult to model. Running the slate train back down the line under 'gravity' has proved more challenging. Phil Farr-Cox, a member of my local South West Area Group of the 009 Society (SWOONS), is the proprietor of WrenShed 009 Workshop. He agreed to construct a whitemetal motorised slate wagon fitted with a DCC decoder. This worked well, but was not powerful enough to haul the whole slate train. Phil is now building a second motorised wagon to run in tandem with the original model. We hope that this combination will have sufficient haulage capacity to manage the whole train and better represent the downward journey through Tan y Bwlch. Some of the slate wagons are Peco products, some are from Bachmann, but the majority are 3D-printed items from Nigel Brooks 3D Models.

Scenic details

I like to add as much detail as I can to my exhibition layouts. Although I aim to keep trains running almost continually, if there is a brief pause in train movements, there are always little scenes for the public to explore. I have included the war memorial that has been built at Tan y Bwlch, and the gap in the wall where there is an information board giving visitors details of local walks. Near the small waiting room building there is a small, square fenced area that surrounds a section of track that demonstrates the early use of stone sleepers. This can also be seen on the model.

Acknowledgements

I am so grateful for all the encouragement and help that I receive from members of the 009 Society, especially those who are members of SWOONS, who meet each month in Taunton. I must also give a special mention to David Cooke, who has helped me so much in recent years, not only at exhibitions, but also with practical help when working at home tracklaying and wiring etc. I have already mentioned his help with the 3D-printing of various items.

Finally, thanks to my wife Linda, who is very encouraging and puts up with me working on the dining room table when it is too cold or damp in the garage, and to RAILWAY MODELLER for continuing to inspire and promote this wonderful hobby.

The 009 Society

The 009 Society promotes the modelling of small scale narrow gauge railways. To find out more about the organisation, including how to join as a member and forthcoming events, go to: **www.009society.com**

See it at Statfold

Tan y Bwlch will be one of 33 layouts in action at this year's Warley at Statfold model railway exhibition, which is being staged by the Warley Model Railway Club and will be taking place at Statfold Country Park near Tamworth over the weekend of **11 & 12 October** 2025. See Societies & Clubs for further details, or go to: **www.thewarleyshow.co.uk**

Railways in the blood – an enduring passion

With RAILWAY MODELLER this month celebrating its 900th issue, Nigel Adams takes the opportunity to reflect on his own lengthy association with the hobby, which began 77 years ago…

I really didn't stand a chance. Both my grandfathers worked in the Great Western Railway works at Swindon; my father served his apprenticeship there as a fitter and turner, and worked there until 1938 when the GWR sacked over 1,000 workers on the 'last in, first out' basis; and my great-uncle was a guard working out of Weymouth until he retired in 1953. So railways are 'in my blood'.

In 1983, I became a traffic volunteer on the Talyllyn Railway, subsequently holding the roles of Guard, Station Master, Blockman (Signalman), Platform Inspector, Controller, Signalling Inspector, Duty Officer and Controller Assessor. In the end, it became a family affair, as my wife became a Station Mistress, whilst my late elder son, Justin, became a Guard, Station Master, Platform Inspector and a Controller, and also worked with the 'outdoor gang', as did my younger son for a while. I stopped regular active volunteering in 2024, having done it for 41 years!

▲ ***Small MPD*** **is exactly that, modelled in O gauge with a space-saving traverser.** ***Photo: Martin Hewitt/author's collection***

An introduction to railways – real and model

My first train set was a Trix Twin Railway one, which I was given for Christmas in 1948. My father had already laid the track on a sheet of hardboard, and so began 77 years of railway modelling.

I didn't do much in my teens and early twenties, but I was still very interested in railways and went trainspotting a lot. In fact, two teachers at my grammar school said that they would be the 'responsible adults' if we organised the trainspotting trips to London stations from Oxford, where I grew up. My friends and I were allowed by our parents to go to Bletchley, Swindon, Didcot and the London stations in groups of three or four.

We also used to go to Swindon for the Wednesday afternoon conducted tours of the locomotive works. You queued up outside and then guides took parties of about 10 round the works. When I was about 12, my paternal grandparents moved to live in Weymouth, so that became the destination for our two-week family summer holidays. At that time, my great-uncle was a guard on the Portland & Easton line and the Abbotsbury line. When he was on the early turn, I used to go with him on his first trip of the day and was occasionally allowed to travel on the footplate! I still remember quite vividly that the loco crew used to fry their breakfast on the shovel during the layover at the end of the branch, which I thought was very odd.

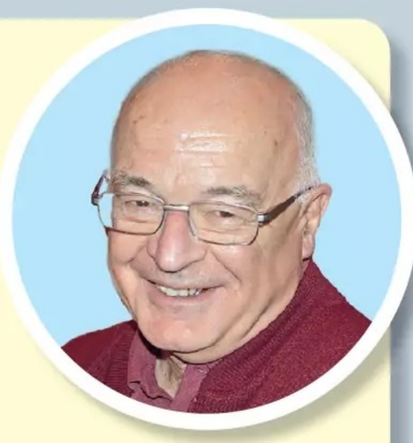
A hobby for life

Soon after getting married in 1964, railway modelling became my hobby, and has been ever since. I was a founder member of the Oxford, Leamington and (now defunct) Tywyn model railway clubs. Although I have been involved in big club layouts and enjoyed operating them, I have always built small layouts because, for me, there is something very satisfying about completing and exhibiting them. I started exhibiting in 1966 and retired 'gracefully' after the pandemic – as I was then over 80!

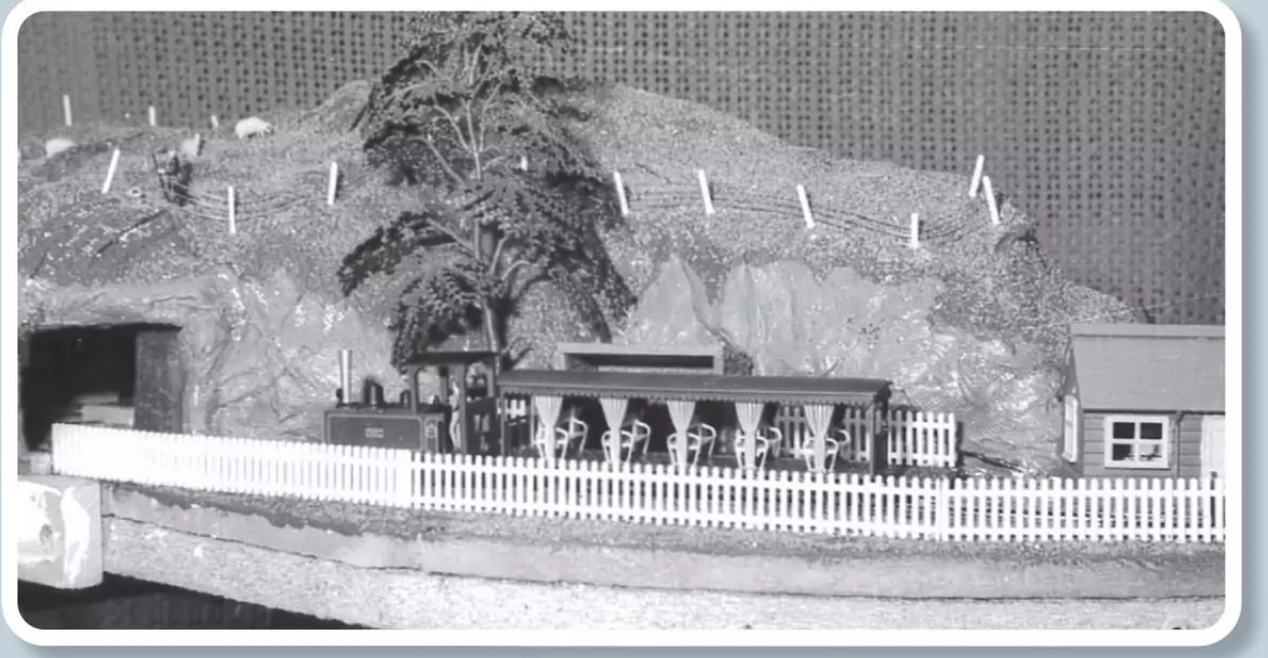
Small is beautiful

Since 1964, I have built and completed 46 layouts in OO, OO9, O-16.5 and O. I also built two layouts in Gn15 (G scale on 16.5mm gauge track to represent 15" gauge), but decided that was not for me. I am a big fan of micro layouts and I am currently building another O

"– I have always built small layouts because, for me, there is something very satisfying about completing and exhibiting them –"



Nigel Adams



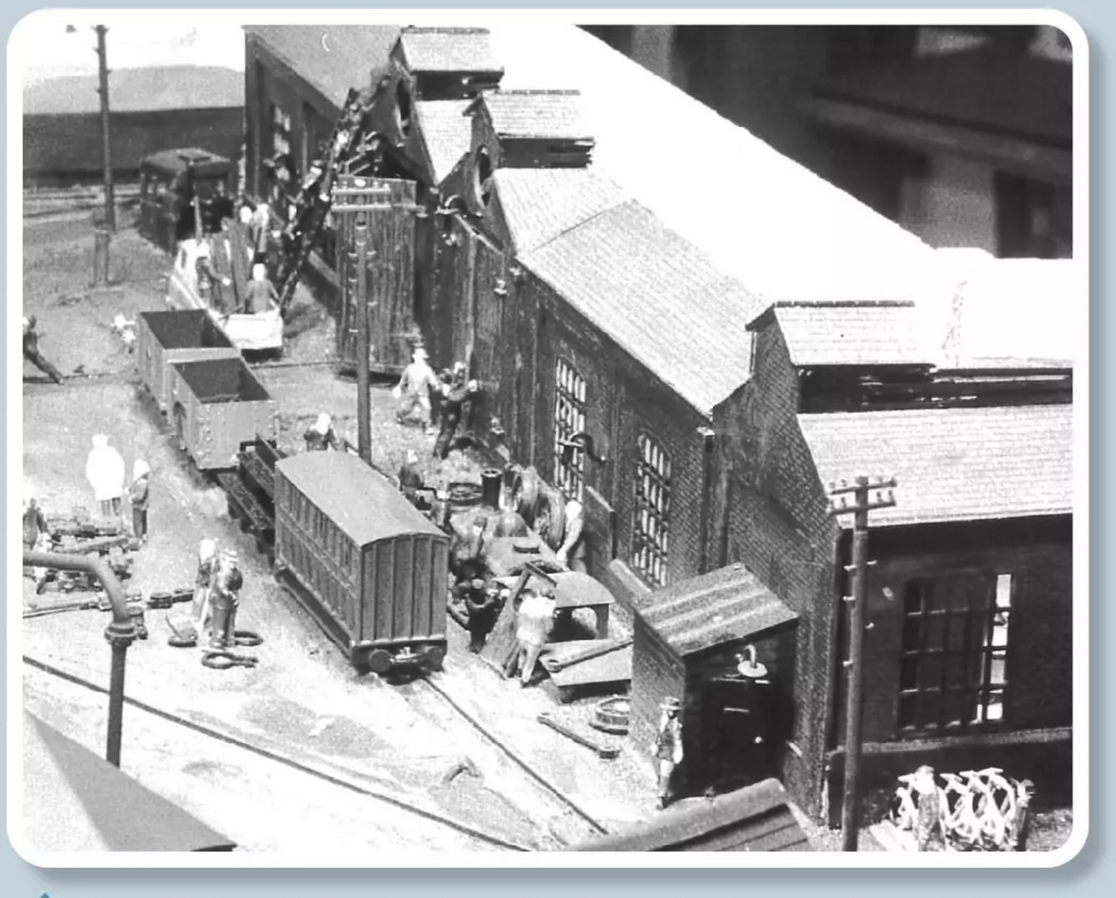
▲ **The first example of a layout constructed by the author that uses an ironing board as a base – *Pen-y-Bord* in 009.** ***Photo: author's collection***

gauge one on an ironing board. I have used ironing boards as baseboards on a number of occasions and can highly recommend them. They are easily stored and transported, and have built-in legs, which is very advantageous when erecting the layout either at home or at exhibitions. Whilst I enjoyed operating big layouts such as *Radley* (by Oxford MRC) and the late John Pomroy's *Winton* layout, I have never had the space, or the inclination, to embark on a large project myself. For me, 'small is beautiful'.

The room where it happens

My current railway modelling activities take place in a 12' x 8' garden shed and that is where my layouts are kept. I have a permanent layout down each side and my workbench is at the far end, whilst my exhibition micro layouts are stored on shelving that runs down each side, under the two main layouts. The fact that the bench is at the far end of the shed maximises the amount of shelf storage space.

As readers and visitors to exhibitions may remember, I am a big fan of layouts depicting loco depots, and I have built a number of these models over the years. They have the advantages of being reasonably small and self-contained, easily stored and easily transported to and from exhibitions, and there is no need for any coupling or uncoupling of rolling stock. They can also be built without using any points at all, because you can 'feed' the tracks by using a hidden sector plate or cassettes. Alternatively, you can feed the tracks using a traverser, as I have done on a number of occasions. If you do that, the 'fiddle yard' is a single track, either hidden from view behind the backscene, or by having a loco shed on the track nearest the backscene, which is then, in effect, the 'fiddle yard'/hidden siding.



An early layout showing the author's continuing penchant for building locomotive sheds, this one a narrow gauge model in 009. *Photo: Brian Higgins/author's collection*

The Shed Mk.2* in O gauge, with Wainwright P Class 0-6-0T No.31027 standing on the very small turntable. *Photo: author's collection

For security reasons, I do not keep my locos and rolling stock in the shed. They are kept in stock boxes (as described in the September 2024 RAILWAY MODELLER).

I have two other layouts in the shed on which I can enjoy some 'leisurely shunting moves' if I wish, but they are never taken to exhibitions.

Our hobby can be enjoyed in many different ways, but I always refer to the 'Rules of the Layout', which I saw displayed on a layout at an exhibition years ago, and unashamedly copied and used on my own:

1. This is *my* layout.
2. Questions and comments are always welcomed.
3. If you disagree with anything you see on the layout, please refer to rule No.1!

Encouraging the next generation

Over the years, I have always derived great pleasure from talking to the public at exhibitions. I remember exhibiting my *Small MPD* layout at the Warley MRC show at the NEC some years ago and a boy, aged about eight, kept coming back with his dad to look at it. He was so interested that I asked him if he wanted to operate it under my supervision, and he did. He ran the layout really well for about 30 minutes. As he and his dad left the last time, he said: "This is the best layout here!".

As my exhibiting days have now ended, I hope that I have 'done my bit' to ensure that our hobby continues to gain new followers. I have thoroughly enjoyed my 77 years of railway modelling.

Remembering Justin Adams (1970-2023)

It was very sad when my elder son, Justin, died two years ago, as he was a first class modeller who built a number of exhibition layouts – the last one being *Wyken Yard*, which is now owned by Coventry & Warwickshire Gauge O Group, of which Justin was a founder member – as I was. The group has renamed the layout *Justin's Yard* and so his memory lives on.

Justin's Yard can be seen during the group's open days (the next one is planned to take place on Saturday 25 October 2025) and at some exhibitions in the Midlands. For more details see Societies & Clubs or go to: **www.covgog.co.uk**

Originally known as *Wyken Yard*, this O gauge layout by the late Justin Adams is now named *Justin's Yard* in his memory. *Photo: courtesy COVGOG*

A c.1983 photo of the author and his late son Justin operating an O-16.5 layout. *Photo: RM Archive*

Whatever happened to ...

Flintfield

The story of this classic Great Eastern Railway branch terminus in P4 goes back to the late 1980s, when Dutch modeller **VINCENT DE BODE** *first embarked on its construction. Having previously been exhibited at only a small number of UK shows, the layout is now under new ownership and will be at this year's Uckfield Model Railway Exhibition on 18 & 19 October.*

Layout photography by Gerard Tombroek

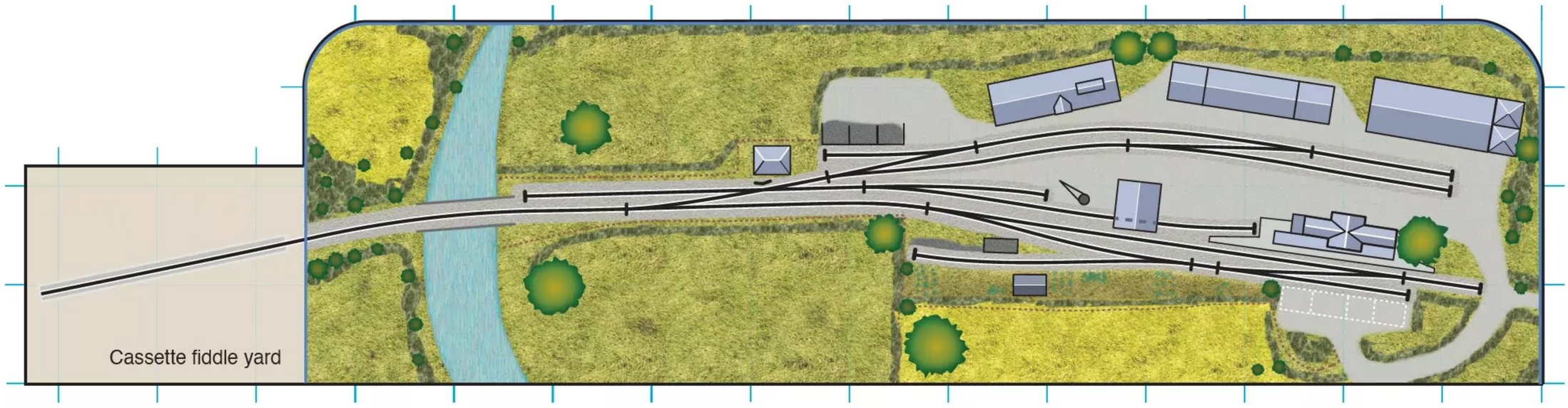
A long time ago, in 1987 to be precise, I decided to take up railway modelling again and visited the 'Rail 87' exhibition in Den Bosch. What was exhibited there was quite different from the Trix set from my teenage years. The stars of the exhibition were the British entries – *Bodmin*, *Exebridge Quay*, *Laxfield* and *Petherick*. I kept coming back to these, especially *Petherick*, a layout that had more than just trains – it was a landscape with a train running through it!

Once home, I decided to build something British in 4mm scale. The internet didn't exist yet in my world, so I joined The Scalefour Society, subscribed to *Model Railway Journal* and obtained a copy of *Landscape Modelling* by Barry Norman.

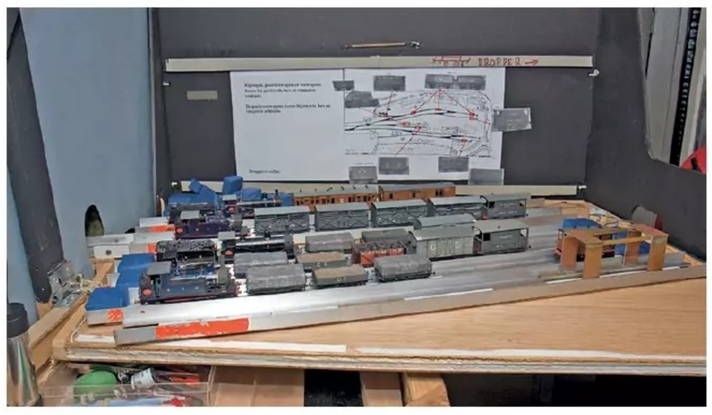
Great Eastern Railway Class T26 2-4-0 No.433 crosses the typical GER trestle bridge, which was made from balsa wood.

I also read articles by the late Iain Rice, which were very inspirational. He also produced his own kits under the name of Riceworks, from which I ordered a Great Eastern Railway Class E22 0-6-0T. It took me three months, but with some hand tools, a calliper and a soldering iron I succeeded in turning this kit into a running locomotive.

A summer holiday to Camelford brought me close to the home of Iain. Following a phone call, I arranged a visit and my engine was allowed to take a test run on his P4 layout *Woolverstone*. To my delight, the locomotive, with its 0.3mm high flanges,



Flintfield Overall layout size 4600mm x 1100mm. Each grid square = 300mm x 300mm.



The cassette fiddle yard – note the locomotive lifting module on the right.

The cable on the goods yard crane is a stiff steel wire, so it doesn't swing all the time; a visitor gave me that tip.

stayed on the track and ran satisfactorily. Now that I had proven to myself that I could build P4 stock, it was time to start thinking about a layout. I wanted to create a model that was as realistic as possible but had scope for shunting.

Going back in time

Through the Dutch Scalefour Society area group I met Dave Doe, an Englishman with a great deal of knowledge of British railways, who lived in the Netherlands. After seeing my Class E22 in its LNER black livery, Dave convinced me to repaint it and model the period around 1900, the heyday of the railways, in this case the GER.

However, modelling such an early era meant undertaking some more research. Much information was available from the Great Eastern Railway Society, so membership was the obvious way forward.

Prototype inspiration

It was also Dave's idea to take the track plan of the town of Eye as a starting point for what was to become *Flintfield*. The attraction of Eye was mainly the storage buildings for malt and barley behind the station building, which had their own rail connection.

In reality, one steam locomotive did the work at Eye. To bring more life to the whole, Dave suggested making some changes to the track plan, such that it became possible to shunt two trains at the same time independently of each other (this was before Digital Command Control existed). Therefore, while shunting was taking place in the goods yard, a second operator could handle a passenger train in the station.

Making a start

A model railway should, in my opinion, meet the following criteria:

1. Everything should be modelled to the same standard.

A general view of the station area, as the T26 prepares to depart with a passenger train.

Trains departing from the goods yard are given a green flag by the motorised signalman.

2. The colours should be soft and not screaming for attention.

3. Visible seams in the model railway and background should be avoided.

4. The attention of those viewing the layout should be focused on the model railway as much as possible and should not be disturbed by the environment.

5. The trains must run smoothly without a hitch.

6. Coupling and uncoupling must be done hands-free, without giant hands emerging from the sky.

Points 1 and 2 are obvious, but what about point 3, non-visible seams? When I started building *Flintfield*, I was aware that seams between the different parts of many model railways and in backgrounds are generally accepted. The challenge was to avoid those seams because, to me, they spoil the illusion of reality.

In Iain Rice's book on layout design, I read something about 'puzzle scenery'. Instead of the usual rectangular boxes, a model railway, according to this principle, consists of irregular parts, the seams of which run along natural boundaries of the scenery. That idea appealed to me and I wanted to apply it to *Flintfield*. However, that was easier said than done. How on earth do you keep the puzzle pieces together? Moreover, there were additional requirements: the construction had to be light and easy to transport, so the puzzle pieces had to be manageable.

To have something light and sturdy, two rectangular frames of 200 × 50cm were made. The girders consisted of box beams with sides of 80 × 30mm. These beams consisted of 3mm plywood sides with a 35 × 12mm spruce batten at the top and bottom, which were nailed and glued together. It seems like a complex construction method, but from experience with building wooden multi-hull sailing boats, I knew that it would result in a very stiff and lightweight construction. Plywood triangles at the corners kept the base frame straight.

M15 2-4-2T No.780, nicknamed a 'Gobbler' because of its coai consumption.

The river bed was painted with acrylics and covered with around 20 coats of yacht varnish.

The two base frames were connected to each other with four hinges (two above and two below), creating a base frame of 400 × 50cm. This frame is a kind of backbone onto which everything else can then be attached. On top of the beams I glued cross slats of 10 × 30mm, with a space of 30cm. On that came the trackbed, which consists of 4mm plywood and a layer of 3mm cork. That turned out to be a bit weak and, where necessary, the trackbed was reinforced at the bottom with vertical strips of 4mm plywood. (In hindsight, It may have been

The model of Eye station building was built from Howard embossed card, and was quite a puzzle with lots of chimneys. When it was almost finished, I found a photo of the other side, revealing an extra chimney...

better to omit the cork and use 8mm plywood.)

P4 trackwork

Fortunately, flexible track for P4 (18.83mm gauge) was already available. That did not apply to the pointwork, however, which I built from C&L components using bullhead rail from the Scalefour Society stores. (These stores also provided the templates and various gauges that were required.)

Once the rails were in place, it was time for the wiring and point operation. After experimenting with old telephone relays, I designed my own system, based on simple toggle switches and old-fashioned operation with rods. In principle, the system comes down to a control panel (positioned at the front of the layout) with wooden blocks that move horizontally, connected with iron push/pull rods to the point tiebars and signals.

Smooth running

With the trackwork complete, I was able to conduct thorough test running. Point five of my layout criteria stated that the trains should run smoothly, and I thought this aspect could be made better.

I knew that trains moving naturally was possible to achieve – I had seen this at a show on a 2mm model of *Chipping Norton*. How this was achieved was described in the 2mm Association handbook, so I sought guidance from this publication.

Essentially, the locomotives were made as heavy as possible, with all wheels used for current collection. The wheels must, of course, make contact with the rails, and therefore the locomotive chassis use compensation (the rolling stock is also compensated).

Last but not least, the locomotives received high reduction gearboxes, making them more controllable.

Hands-free uncoupling

Aside from not wishing have a 'giant hand from the sky', it was also not practical to reach three-link couplings from the back of the layout because of the high background used for its theatrical presentation. It was not possible from the front either: the track is at eye level and there is quite a bit of landscape between the front and the track.

The choice fell on the Alex Jackson method of delayed-action auto-coupling. The advantages are that these couplings are inconspicuous, reliable (if made with the necessary care) and inexpensive. They also seldom disconnect spontaneously due to irregularities in the track, and the bufferbeam can remain completely intact. However, disadvantages are that the

Wagons have compensated chassis, and so stay on the track even with their tiny scale flanges.

Trains can be operated simultaneously in the station and goods yard.

couplings are more sensitive to rough handling than most factory-made couplings, you have to make them yourself, and the coupling does not act as a buffer (when propelling uncoupled vehicles).

The uncoupling is achieved by pulling down one of the two couplings with an electromagnet. For this purpose, a 'hanger' of soft metal (paper clip) hangs around or on the shaft of the coupling. The electromagnets are installed in suitable places between the tracks. A clump of grass or something else indicates the place, so that the operator knows where the magnet is.

Cassette storage

On *Flintfield* I use cassettes, long enough for a whole train. The cassettes can be changed on the 'fiddle table' and they are not turned around. Using a locomotive lifting module, the locomotive is placed on the other end of the train, with brake vans marshalled by hand.

The cassettes are made by glueing two aluminium angle profiles with contact adhesive onto a strip of plywood. The angle profiles lie with their flat side inwards at a distance that corresponds to the track width.

A scenic puzzle

At this stage, everything more or less worked and now I could continue with the scenery, building more rolling stock in the meantime.

It had to be determined what the landscape would look like and where the seams in the scenery should be. It was obvious that there would be a seam between the landscape and the trackbed, which gave me some concern initially, but it turned out that the longitudinal seams are not noticeable at all – only the transverse seams (in the viewing direction) are visible. I tried to make them coincide as much as possible

The layout scenery is formed of 'puzzle pieces' to avoid unsightly seams.

with natural lines, such as hedges, track equipment or a building.

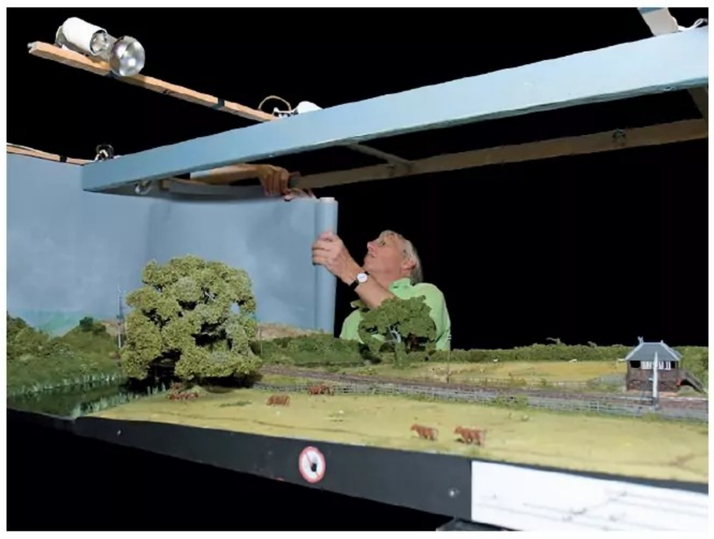
The landscape panels themselves were made to be detachable, with each supported by a framework of battens, whilst weighing little. The panels with the meadows were made of chicken wire over the battens and covered with textile and Alabastine. (If doing this nowadays, I would use layers of paper and diluted PVA.)

For extra depth, the transition between the landscape and the backscene had to be invisible. That was no problem for the cornfield in the left corner, because by letting it run down a bit it was not visible. However, it was more difficult with the flat meadow and behind the buildings but, by placing hedges 10 to 15cm in front of the backscene, the join was obscured.

The hedges themselves were made of rubber-impregnated horsehair that was sprayed with glue and coated with various colours of coarse turf from Woodland Scenics. Behind the buildings, the hedges are higher and wilder.

Grass cover was created using surgical lint, with Woodland Scenics coarse turf used in places to introduce some natural variation. Of course, at the time, there were no static fibres, which really are much more convenient to use!

The 'Gobbler' coasts towards the station with a cattle van and passenger train in tow.



The lighting gantry and backscene – the latter being unrolled and attached to the layout.

The river

The river was modelled using a 4mm sheet of plywood, on which the colour of the water was painted with acrylics. The water was made by applying many layers (around 20) of polyurethane yacht varnish. After a few layers of varnish, the current of the water was suggested with a few brushstrokes of black paint. Each coat was applied within a few days so that sanding was not necessary. While it was drying, I stood the river upright so that no dust would collect on it.

The reeds along the riverbanks were made from bristles of a large paintbrush.

Dave provided me with an original drawing of a trestle bridge typical of the GER. As a builder of (model) gliders, I thought that something like this could be built using balsa wood. The two main beams are 8 × 3mm and the cross beams are 3 × 3mm. The trestles also consist of beams of 3 × 3mm, with diagonals of 3 × 1.5mm.

Layout lighting

To keep the attention of an audience on the model railway, good lighting and a decent background are vital, this forming a kind of mini theatre to shut out the real world.

My first engine, a Riceworks Class E22 0-6-0T, shunting a cattle train.

Because of how the layout is assembled, it was not possible to attach the lighting to the front or back. The solution was found by mounting vertical battens at both ends of the main layout frame, upon which a wooden box beam for the lighting was placed. This box beam was constructed in the same way as the box beams for the layout sub-base. To facilitate transport, the lighting bar is hinged in the middle, with a long overlapping piece at the hinge preventing the beam from sagging in the middle.

Six transverse battens were fastened with wing nuts to the lighting beam. There are two sockets per batten, with a 60W spotlight at the front and a 40W behind (LED lighting was not available when this was constructed.) The lamps all point in the same direction, at an angle of about 20° (to the horizon) on the layout. This gives the pleasant effect of a summer afternoon, with shadows all falling in the same direction.

To shield *Flintfield* off from the surroundings, like a stage, I created a black proscenium arch to help maintain the viewer's focus on the miniature world. It is made from 1cm polystyrene foam, with black thin card glued on both sides with PVA.
A black curtain at the front under the layout hides the legs and other stuff stored there.

G15 Class 0-4-0T tram locomotive No.128 crosses the river with a cattle train.

The front meadow was made from surgical lint: static fibres were not being used in the hobby at that time.

The layout set up at a past exhibition, showing the proscenium arch style of presentation and high display level.

A seamless backdrop

For *Flintfield* I wanted a neutral – seamless – backscene that wouldn't distract from the model. Made using roller blind material (which doesn't crease when rolled for transport), the backscene is about 60cm high. In the corners, it is curved to a radius of about 15cm. With a soft blue colour as a starting point, Len de Vries took on the task of adding the clouds and trees for me – he's really an artist at that.

Making it portable

Meanwhile, another problem had to be solved – transport. How could the six landscape panels, the river, some large trees, buildings and signals and the frames be transported? There was already the idea that everything could be made to fit in a friend's Volkswagen van. For the sub-frames, this meant building a crate at least 2m long. Based on that length, two crates were made from 4mm plywood: one for the sub-frames and lighting beam, and another for the landscape panels and other scenery. The benefit of these boxes is that the layout can also be stored dust-free between exhibitions.

Front to back

After a few exhibitions, it was found that having two operators and the public all in front of the layout could sometimes become a bit crowded. With the rail level 132cm above the floor and the backscene 60cm on top of that, operation from the rear in the usual way was not possible. This was where the transport boxes came in handy. During exhibitions, the boxes were set up at the rear and so served as a platform to stand on for the goods yard operator, who could now control the trains at the rear of the layout from this position – without being seen by the public in front.

This required a second control panel at the back, for which the point sliders are directly connected to those for the front control panel. Therefore, if a point is now set at the front by pulling the slide forward (away from the model railway), the slide at the rear will move backwards (towards the model railway). This way, the station operator at the front can see what is happening at the rear, and vice-versa. It is a simple form of point control – almost a statement against digital systems – that has functioned without problems or wear in the more than 600 hours that *Flintfield* has been in operation at shows.

And finally...

This is the short story of a layout with a long history. After 30 or so years of great fun and having operated *Flintfield* with many friends, visiting exhibitions and being awarded prizes, the time had come to say goodbye, with OntraXS 2024 being my final show with the layout.

Finally, I would like to take this opportunity to thank all my friends and helpers. In addition, of course, thanks to the Scalefour and GER societies, the modelling press, the public, and for the help received at the many shows over the years.

See it at Uckfield

Flintfield will next be exhibited at this year's Uckfield Model Railway Exhibition on **18 & 19 October**. See Societies & Clubs for more details, or go to:
www.uckfieldmrc.co.uk

Under new ownership

Words by Adrian Colenutt

In October 2023, I became aware that Vincent was looking for a new home for *Flintfield*. A conversation took place, which resulted in a trip in March 2024 to the OntraXS show held in the Spoorwegmuseum, Utrecht, Netherlands, where the layout was booked to make its final appearance under Vincent's ownership. The team from the UK got involved in the setting up and operating of *Flintfield* over the three days of the show, which along with the excellent hospitality provided by Vincent and his wife, Jenny, contributed to a most enjoyable weekend. At the end of the show, the layout was dismantled and came back to the UK with us.

A programme of refurbishment is being undertaken, including small repairs to the layout's scenery and buildings, tidying up its presentation and fascia, and overhauling the electronics.

A major change is the method of locomotive control, with an NCE Powercab Digital Command Control system being installed. All the locomotives are now fitted with YouChoos Zimo sound decoders, 'sugar cube' speakers and Tantalum capacitors providing stay-alive functionality. Vincent's mechanical method of operating points and signals, however, remains firmly in place.

Although a regular attendee at Scalefour Society events over the past 30 years, the layout has been to very few general exhibitions in the UK, and those were mostly back in the late 1990s and early 2000s. The plan is very much that *Flintfield*, with Vincent's blessing, will once again appear on the UK exhibition circuit, hopefully attending a couple of shows each year. Invitations are welcomed, with the team contactable via the layout's website:
www.flintfield.org.uk

Plymouth General

TERRY SMALL *used traditional construction methods and 12V dc control for this shed-based terminus to fiddle yard system in OO.*

Layout photography by Craig Tiley

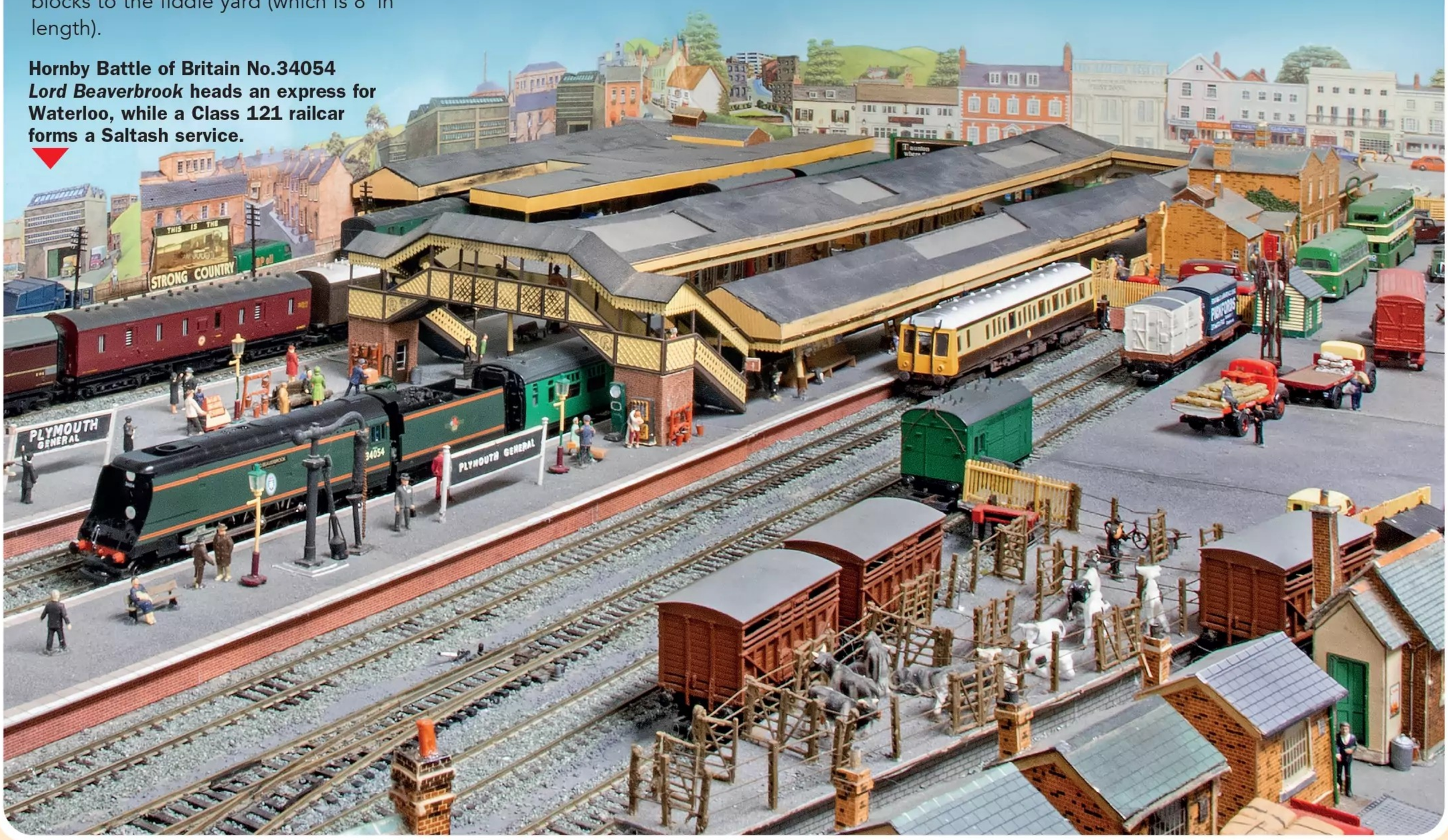
I wanted to build a layout incorporating both the Southern and Western Regions of British Railways during the 1958-1960 period. I decided to model a West Country terminus and sought inspiration from Plymouth Friary, but changed the name to Plymouth General.

The layout is housed around three walls of a 20' x 10' shed in my garden, this giving me a run of about 40' from the station stop blocks to the fiddle yard (which is 8' in length).

Construction

The layout was developed with the help of my good friend Graham Hooper. The baseboards comprise a 2" x 1" timber frame with Sundeala board tops, whilst the extensive track plan utilises Peco Streamline code 100 trackwork with Electrofrog points. The layout is wired for 12V dc analogue cab control.

The layout has two control panels. The one for the main lines has two Gaugemaster 100 single track panel-mounted controllers, with 24 two-way centre-off section switches and 22 point switches. The locomotive shed control panel utilises a Gaugemaster D controller and has 25 on/off section switches, plus three point switches for the shed exit and crossover at Lipson Road. All the points



Hornby Battle of Britain No.34054 *Lord Beaverbrook* heads an express for Waterloo, while a Class 121 railcar forms a Saltash service.

A Bachmann Maunsell N Class 2-6-0 pulls out of the large goods yard with a mixed freight, as pannier tanks carry out shunting duties.

in the shed and goods yard areas are hand-operated.

The buildings are a combination of scratch-built models, or card kits from suppliers such as Superquick or Metcalfe. The viaduct is a converted Metcalfe kit and the backscenes are mostly Peco.

The grassy banks were constructed using Sculptamold paste with various scatters.

A tour of the layout

Trains leave the nine-road fiddle yard through a road bridge to enter Lipson Road, a small country station.

Hornby Bulleid Q1 0-6-0 No.33037 passes over the level crossing with a goods train. Buildings are a mixture of card kits and scratch-built structures.

After passing the entrance to the loco sheds, the platforms are reached, which are staggered due to the shed entrance. The line passes the back of the loco sheds and then crosses the viaduct over Mount Gould Road, followed by the level crossing over Friary Road. A milk depot is passed on the left and the goods yard on the right, before entering the terminus, which has four passenger platforms and two parcels bays.

The station goods yard has sidings for a coal yard, grain store and cement traffic. There is also a dedicated road for brake vans, together with roads for a goods shed, cattle dock and loading bay.

There are two loco sheds; the 'old' shed (a Superquick kit) and the 'new' four-road shed (two Metcalfe kits combined). The shed area is on a higher level than the main line and includes a siding with stores and a lamp hut situated by the 'new' shed, and a coaling stage.

Locomotives run over the disposal pits and past the coal stage to use the turntable. Next to the turntable are the fitters' sidings, complete with hoist. Once turned, locomotives either enter or pass the 'old' shed, to be stabled for their next duty.

The trains

Motive power for the layout consists of 56 steam locomotives and one single car diesel railcar. Rolling stock wise, there are 56

The author at the controls on the Plymouth General station side of the layout. The engine shed is opposite, with the fiddle yard out of shot to the left.

services include auto trains and Diesel Multiple Units to Saltash, and loco-hauled 'B' sets to Tavistock and Launceston, whilst there are local services to Truro and express trains to Paddington, Birmingham and Penzance, and a sleeper service to Liverpool.

Goods trains include mixed goods, milk, cement, permanent way and ballast. There is a daily pick-up goods for Okehampton, plus trip workings to Devonport Docks and Ernesettle sidings.

Ideally, the layout is operated by two people, but it can be operated by one person if necessary.

Thanks

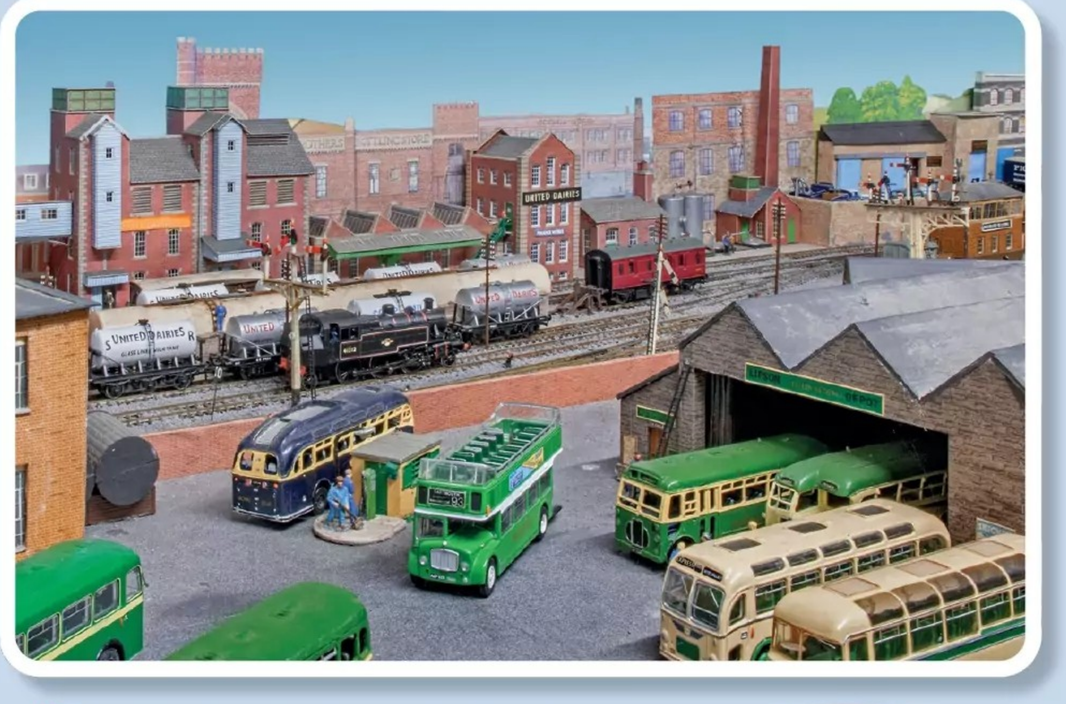
There are no plans to extend or alter the layout. My thanks to Graham Hooper for his help with the building and wiring, and to the late Alan (Gus) Garret and his wife Sue for the many items of rolling stock supplied to me. Final thanks must go to my long-suffering wife, who saw very little of me during the layout's construction!

coaches, 39 parcels vans and 187 goods wagons. On my layout I have supposed that all sides of Laira triangle are used for train services (although the triangle itself is off the layout).

There are Southern Region services from Plymouth General for Okehampton, Exeter Central and Waterloo. Western Region local

A large Prairie pushes wagons of coal up to the coaling stage, ready to replenish the tender of a rebuilt Bulleid West Country.

The Class 121 in Great Western 150 livery crosses the viaduct, which was made from a converted Metcalfe kit.



A large bus depot reflects Terry's other transport interest, following a career driving buses and working as a guard on BR (see below).

A career in transport

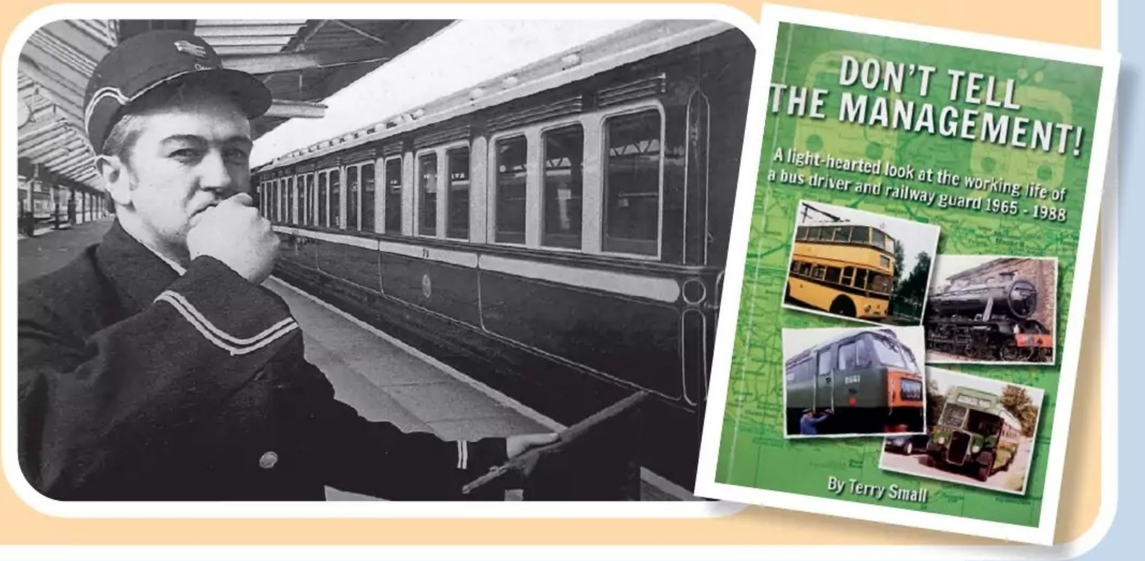
I grew up in the small Dorset town of Shaftesbury, where the nearest railway station was five miles away at Semley. Therefore, my main transport interests were the many bus operators that terminated at or passed through the town.

I knew from a young age that I was destined to become a bus driver. Over the next 49 years I worked on both buses and trains.

During my time as a railway guard in the 1970s and 1980s, based at Eastleigh on the Southern Region, I was lucky enough to work with diesel and preserved steam locomotives.

One notable working was on the old decommissioned Royal Train in 1979, which went from Eastleigh to Brockenhurst and then on to Weymouth. The train was on hire to Lord Montagu of Beaulieu and I then worked the return trip to Eastleigh.

After my retirement, I was persuaded to write a book about my experiences on both modes of transport, so I chose to write about all the comical things that happened before Health & Safety put a stop to having fun at work. It was titled *Don't Tell the Management!*



Terry Small on duty as the guard of the 'Beaulieu Belle' special on 17 September 1979; this picture was taken prior to leaving Weymouth on the return to Eastleigh. *Photo: Southern Newspapers Ltd*

More everyday items for creating building details

Following on from his article in the August issue, **ADRIAN COOK** *explains how he has re-purposed various other readily available products and materials to enhance his 4mm scale card structure modelling projects.*

Photography by the author

When I was very young, I built the Superquick low-relief café and supermarket with great pride. Also, back in the day, I visited Pendon Museum and the quality of building in those thatched cottages left a lasting impression. My attempts to model the buildings of south Manchester in low relief, for the three-metre-long High Street on my exhibition layout *Rockport*, has allowed me to relive these memories.

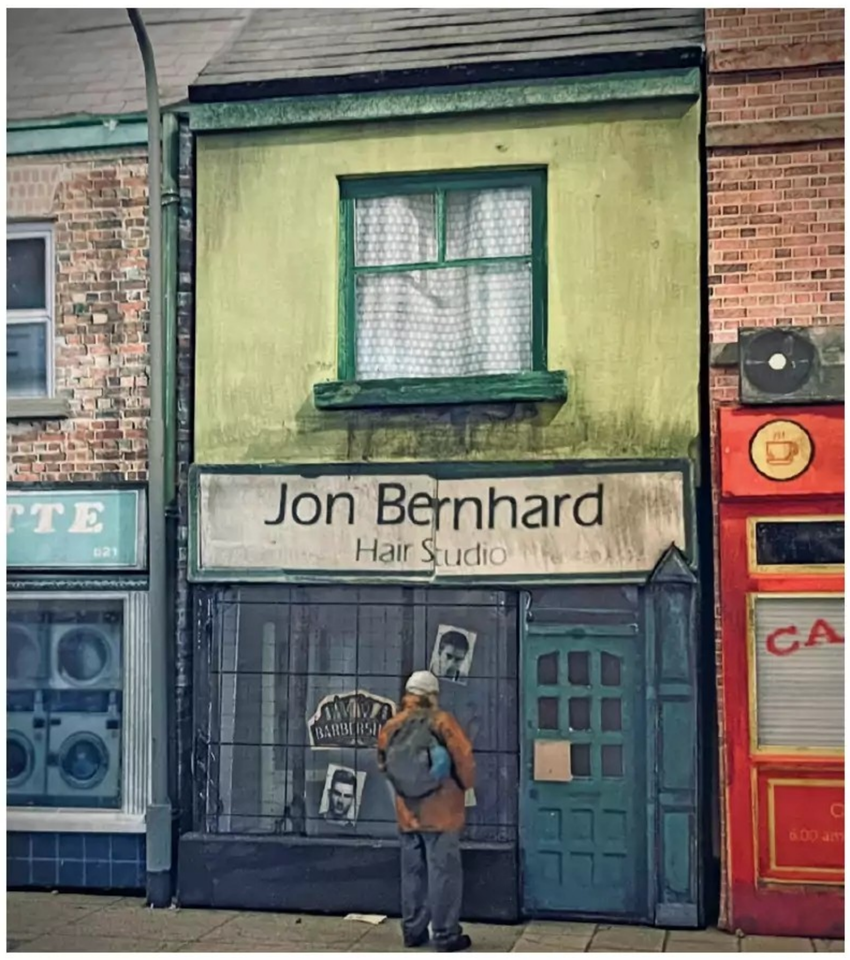
In my previous article, I presented a selection of everyday items that I have incorporated into my card structure modelling projects. That initial selection comprised doll's house picture frames, elastic bands, insect wire, nail art transfers, costume jewellery and hairbrush pins. These are all items that you wouldn't immediately associate with railway modelling, but they are easy to source and, in most cases, cost very little.

I hope the ideas that I have shared across these two articles are of use to other modellers. I've certainly been pleased with the way they have enhanced my own card buildings.

Urban realism, achieved using traditional card modelling techniques combined with modern computer-based methods.

Using everyday items

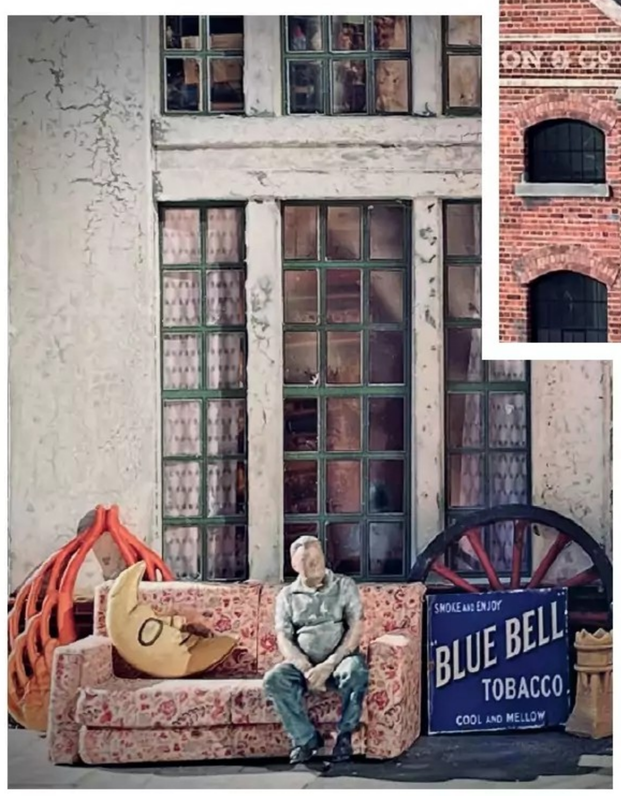
Graph paper



I had the idea of photocopying graph paper (reduced by 70%) onto acetate sheets. The print side is matt in finish, so can be placed across doors and windows with the matt side out as security grilles. I've seen others try to model this with wire or plastic mesh, and it can look overscale and chunky, which can be avoided using this method as the wires are much finer.

Children's craft glue

This is something I have used extensively. This is just a weathering technique, and the only thing presented in these articles for which I wouldn't claim credit. Where I have painted stonework and wish to show some peeling or lost paint, I have used this method. The feature is painted in a stone grey. Once this is dry,



I paint it with craft glue. Before the glue fully dries (about five to seven minutes) I then cover this with a generous layer of paint. As this begins to dry, it is stretched by the glue drying underneath, and begins to crack. You can also, at this stage, dab it off with a tissue. This is a highly unpredictable method, but gives the modelmaking process a much-needed edge and element of jeopardy!

Rawlplugs

I've only used these once so far, when I needed a curved top to the pilasters on my pet shop and draper's building. These seemed perfect once sliced off and cut in half. It is, however, worth mentioning at this point that some nylon-style plastics don't take kindly to any type of glue or paint. Rawlplugs worked, but I have had a few of my attempted improvisations halted by this. Also, at this point, I should explain how I make pilasters (the vertical stone or wooden pillars that are used on many shop fronts). I look for square-on photographs on the internet. Using the on-screen cutting tool, I cut them out and open them in Inkscape (a form of free computer drawing software). These are scaled to the size I require and coloured, either by adding a translucent colour overlay, or by printing them and later adding a colour wash in watercolour. I believe the 2D photograph is enough to provide convincing details of panels or finger grooves. However, I like to add an extra layer of paper for the foot of the column, and then add my doll's house or rawlplug column head to maintain a 3D effect.

Sticky labels

My layout has a carpet shop, but these could just as easily adorn a skip or be tossed over a wall onto a railway embankment. I simply took some carpet photographs from the internet, scaled them in Inkscape and printed them onto sticky label paper. Once printed, they are rolled up tightly and put on display, or on my carpet delivery van. Sticky label printing is my key method for making windows. When I first began card building five years ago, I came across a video of Michael Scott's N gauge layout *Chandwell* showing how to do this, and realised that, if I could make windows to the very size, shape and pattern of the prototype, I could model any building I wanted. I have taken this method a little further on occasions by printing a photo of a window frame onto sticky label paper, then removing the glass panes to create an even more convincing outcome.

Barbecue skewers

I have used barbecue skewers split lengthways to make rounded brick wall coving. There's a bit of frustration involved here, as the hard vascular fibres running through the skewers make them tricky to cut lengthwise and achieve a perfect 'D' profile. Once this is done, however, I then print Scalescenes brick coving onto sticky label paper and wrap it around the half-skewer, to make the rounded effect seen on many terraced yard rear walls.

Readers' Letters

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: **editorial@railwaymodeller.co.uk**

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

'Rooters' in the family

LBSCR Stroudley 'Rooter' 0-6-0T No.60 *Ewell* at New Cross shed c.1890. *Photo: Rail-Online*

I really enjoyed the Scale Drawings feature on the Stroudley 'Terrier' 0-6-0Ts in the September edition, particularly as my late grandfather, Charles (Tiny) Carr, started his London Brighton & South Coast Railway career as an engine cleaner in 1873 at New Cross shed.

He cleaned 'Rooters' (as LBSCR enginemen referred to them) and other locomotives that ran on the East/South London and other lines. He later progressed to the role of porter at the then Old Kent Road & Hatcham station, finishing his LBSCR career as lamp foreman at London Bridge terminus.

I was born in 1947 and grew up under the South London Line's arches off the Old Kent Road. By this time, 'Rooters' had mainly gone from the electrified route, but I did see the daily Brighton-bound steam-hauled set (not headed by a 'Rooter'). Incidentally, the passage of heavy steam locomotives had damaged the foundations of our 1860s terraced house, eventually causing the whole terrace to be demolished in 1961!

DEREK CARR

James Street – a small retort

I am writing to say how much I enjoyed the article, *James Street extended – part 3*, in the September edition of RAILWAY MODELLER.

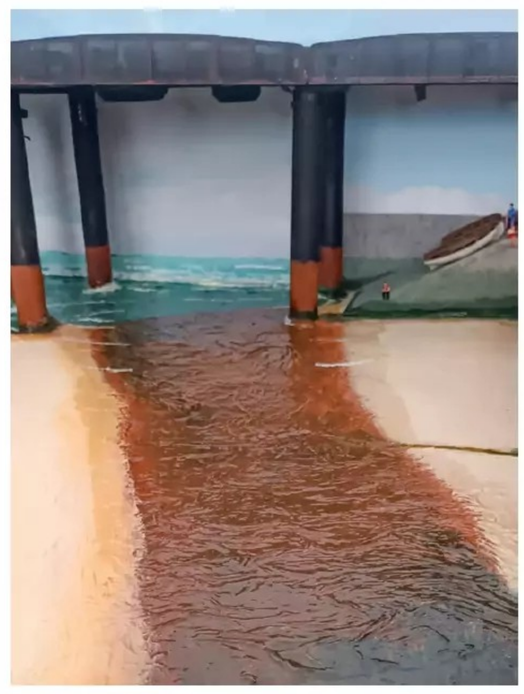
I was surprised to see a model of the vertical retort house of Launceston gasworks. However, it is located in Launceston, Tasmania – not in New Zealand as stated in the article.

It is believed to be one of the last remaining vertical retort houses in the world. It has been stripped of the relevant gas-making equipment and has been converted to a restaurant.

Here is a photo of my own 4mm model of it, on my under-construction layout.

ROBIN ADDERLEY

Water modelling success



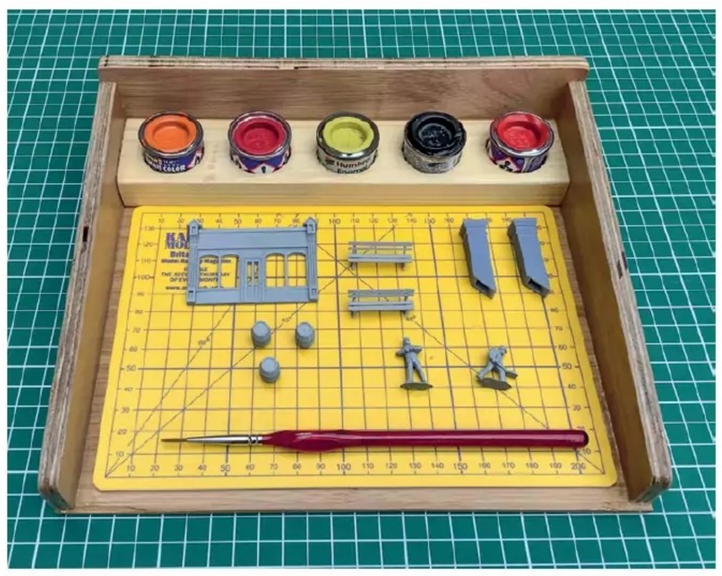
In the July edition of RAILWAY MODELLER there was an article (titled *Barrihandie harbour*) about modelling water using kitchen roll soaked in PVA. What a brilliant idea!

I tried this method out on my own harbour model. When the PVA had dried I painted the base of the river with acrylic paints and then, when this was dry, I applied two separate coats of yacht varnish.

The accompanying photo shows the result. Thank you to Alisdair Macdonald for sharing his method.

GARY DIXON

Using the free RAILWAY MODELLER cutting mat to make a work station



The free cutting mat (supplied with printed copies of the August edition of RAILWAY MODELLER) was a welcome addition to my workshop. With it I made a very simple, and inexpensive, work station from scrap plywood.

With the advent of 3D printing, we are able to purchase a myriad of small detailing pieces, mostly unpainted, for our layouts, but the chances of damage or loss of these parts can be annoying, as well as expensive, with some figures priced over £5 each!

This small work station is ideal for working on small parts, and also for the safe storage of unfinished parts. The small size of the work station means that, even in the smallest of homes, it should be possible to find a small corner to keep it in, so that completion of your painting can resume quickly.

I have used 9mm plywood, but 6mm would easily suffice due to the nature of the work for which it will be used. I was fortunate enough to have a small piece of spare 44 x 22mm softwood, so I was able to make the stand for the Humbrol tinlets, using a 30mm forstner bit at my local Men's Shed. A small offcut of plywood would work just as well, if placed across the width of the work station to keep the tinlets in order.

The parts were all glued together, and when complete, were given a light coat of varnish. On the underside, I attached four self-adhesive felt pads to ensure the work station doesn't scratch the table top.

PHIL KEMPSEY

Coupling fix on Minerva USA 0-6-0T

Further to the review in the September RM of the USA 0-6-0T in O gauge from Minerva Model Railways, I recently saw one of these fine models at a model railway exhibition. I had an interesting chat with the owner who, though impressed with his purchase, commented that the model as supplied could not physically couple to his rolling stock. This was because, with the buffer heads touching, the coupling links couldn't reach the hooks.

The issue was solved by packing out the rear of the sprung hooks on the locomotive with a piece of plastic, which had the effect of pushing the hooks forward, so enabling stock to couple. (The rolling stock in question was not at fault as this had always coupled with other locomotives without a problem.)

DAVE HANMER

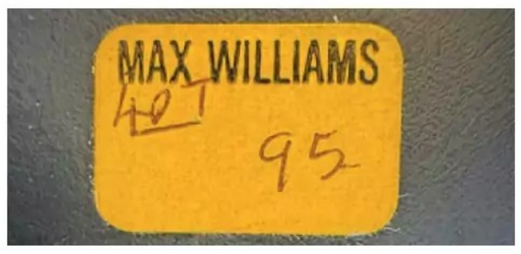
Memories of Max Williams' model shop

Reading the editorial in the September RM, and the mention of Max Williams, brought back happy memories of when I first started work for Bristol Omnibus Ltd, in 1970, which had its new office block just down from Max's shop on Church Road.

I used to visit Max's shop most lunch times when it was 'closed'. I put closed in quotation marks because he never actually closed the shop for lunch, instead just putting the closed notice in the window; those in the know just came in as they knew it was a quiet time! Another frequent visitor during lunch times was David Weeks of the Yatton Junction Model Railway Group, who also worked nearby. Max never had lunch when we were there – he just got his pipe out and started to puff away, whilst we chatted away putting the world to rights. His wife (unfortunately I cannot remember her name) would be in the back room and would call him in when his food was ready. We would then leave as his son, John, would appear from Max's bicycle shop across the road.

One year I was invited to join Max on his stand at one of the Bristol shows held then at the Victoria Rooms, but I cannot remember what year that was.

As Craig said in his editorial, the shop was an Aladdin's cave, not just selling model railways but all sorts of toys. When entering I always looked at the small glass cabinet to the left, as this contained any second-hand items.



Behind the counter at the back of the shop was a large wooden cabinet with lots of small drawers, which contained all the spare parts that Max used and sold for repairing locos, etc.

Other lunch time visitors included Tony Reynalds (also of Yatton Junction Model Railway Group at that time) and Guy Williams.

Happy days indeed. Attached is a photo of a price tag on some plasticard which I still have.

MIKE CORP

Helland Wharf in N

I was very interested in the article in the September RM on *Helland Wharf* by Mike Walker. I happen to have modelled this famous and very picturesque part of the old Wenford Bridge branch myself. However, I have modelled it in N gauge and in a box file!

In fact, I have managed to incorporate three scenes from Cornish china clay branches in the box. The scenes are Helland Wharf, Ponts Mill (that was originally off the Newquay branch) and the arched bridge at Moorswater (on the Liskeard to Looe branch).

Although not truly accurate, I hope they show a representation of the branches in question. It was fun to build and is fun to operate. I have named the model *Wents Mill*, part of the supposed Wents Mill branch.

GARETH M MINORDS

[Ed. Unfortunately, the details for some of the forthcoming exhibitions listed at the end of Mike Walker's article on Helland Wharf *were incorrect. The references to Slough, Newbury and High Wycombe were actually show appearances that took place in 2024. However, the references to appearances in Cambridge on 13 September, Reading on 27 September and Maidenhead on 10 January 2026 were correct. In addition, the layout is booked to attend the Salisbury & South Wilts Railway Society show at the Thruxton Centre on 16 & 17 May 2026. Our apologies to Mike for the error, and to our readers for any confusion caused.]*

'Coffee Pots' of a different kind

I enjoyed the articles about the Great Eastern Railway '209' Class 'Coffee Pot' 0-4-0STs in the August issue. Being a 7mm modeller, seeing the drawings presented in this scale was even better.

It is worth noting that Springside Models offers a complete 7mm kit, but it is of an early version, so some modifications would need to be carried out to conform with the condition shown in the drawing (and as described in the constructional article).

A recent running session I attended at an O gauge garden railway saw a trio of 'Coffee Pots' of a different kind in operation – Bulleid Q1 0-6-0s, which assumed the same nickname in service. They are shown in the accompanying photo on the layout *Peacehaven*, which is an imagined third-rail electric railway set in the south of England, and is owned by a member of the Exeter Gauge O Group.

The three Q1s were each built from a different kit, these being marketed by Modern Outline Kits, Ace Products and J&M Hughes. I model the preservation era and therefore the model of No.33026 is named *The Royal Artilleryman* after my father, using etched plates from Fox Transfers (as used on the Peak diesel locomotive).

PETER LEWIS

Restoring a Hornby Dublo N2

The photo of the Hornby Dublo three-rail Gresley N2 0-6-2T on p531 of the July issue reminded me how, for Christmas in 1959, I received one as part of a Hornby Dublo Goods Set.

My father screwed the track supplied to a sheet of green-painted plywood and it wasn't long before this was expanded with points and additional rolling stock. We soon discovered Airfix plastic model railway kits and added a station and water tower, etc.

Although I switched to two-rail in the mid-1960s, I kept much of my Hornby Dublo rolling stock. Many years and a couple of house moves later (including one across the Atlantic), I discovered that my N2 no longer ran. I decided to convert it to two-rail using Romford wheels and cleaned up, detailed and repainted it at the same time. The accompanying photo shows the result at the head of a short goods train.

I have been getting RM since 1967 and it's always a good read – keep it up!

VICTOR CROASDALE

Snippets...

Support local model shops

Congratulations on introducing a discount on the cover price of RAILWAY MODELLER if bought from a Peco stockist. Sadly, with the demise of Hatton's and local model shops in Formby and Burscough, I would have to travel very far indeed to pick up an issue. These days, I try to source my modelling requirements at the model shop in Chester. This is handy if, as recently, I was visiting Chester to see Pete Waterman's *Making Tracks 200* event, but this entails making two long train journeys each way. I always try to support my local traders but, sadly, many people take the easy way out and opt for mail order. Once it's gone, it's gone!

ROGER WALSH

Buckhorn slip

There is one minor error with the track plan that accompanied the article on my layout (*Buckhorn Weston*) in the September RM. This concerns the slip points at the right-hand (Down) end of Buckhorn station, which is in fact a single slip, rather than the double slip that is shown. The single slip allows Down traffic from the branch to cross onto the Down through line, should the normal branch road be occupied. A double slip would allow traffic from the Down through line to access the Up local line, which would not be a good idea!

MAURICE LOCKWOOD

Track cleaning on ac or dc?

I wonder if any research has been undertaken with regards to dc and ac supplies to OO gauge track and the accumulation of dust and dirt? My own layout, *Robertstown* (RM March 2015) is a loft layout. With the summer we have been having, I have not been running trains, partly as the loft becomes something of an oven in very hot weather! My layout can be dual dc (analogue) and DCC controlled. So, when I restart running trains in the autumn, I wonder whether I should apply dc or ac first to run the track cleaning vehicles?

OWEN EDIS

Mistaken identity?

In response to the *Stockton & Darlington* article in the September RM, I do not think the photo on p735 shows Willie Whitelaw as stated. He may well have accompanied HRH Prince Philip, but the gent described as Whitelaw looks very much like the late Lord Stockton, better known as Harold Macmillan, who was MP for Stockton until 1945.

CHRIS PHILLIMORE

FROM RAILWAY MODELLER BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

Churchward '4400' 2-6-2T

New from Rapido Trains UK in 00

When thinking of Great Western Railway branch lines, one of the most quintessential locomotives that springs to mind is the small Prairie. While the most numerous class, the Churchward '4500', has been produced previously in ready-to-run form, the pioneers of this series of 2-6-2 tank engines – the Churchward '4400' Class – had yet to be tackled… until now. A model in 00 gauge, from brand-new tooling, has been released by Rapido Trains UK, having been announced in our January 2023 issue.

Supplied Digital Command Control ready or DCC sound-fitted, the initial release comprises: Nos.4400 & 4408, both in green with 'GREAT WESTERN' lettering; No.4402 in green with GWR 'shirtbutton' roundel; No.4406 in green with 1940s GWR lettering; No.4404 in green with 'BRITISH RAILWAYS' lettering in Egyptian font; No.4409 in BR lined black with 'BRITISH RAILWAYS' in sans serif font; No.4406 in BR lined black with early emblems; and No.4401 in BR plain black with early emblems.

Our sample in detail

The Rapido tooling focuses on the class in post-1920s condition with straight running plates, superheated boilers with extended smokeboxes, and lengthened bunkers. Our sample represents class pioneer No.4400 (originally No.115), the sole example to be constructed at Swindon Works in 1904. According to *The Prairie Papers No.3* by Ian Sixsmith (published by Irwell Press, ISBN 978 1 906919 870) the locomotive was initially allocated to Blaenavon from 1 April 1905, spending much of its early service career in South Wales. It received its new number, 4400, in December 1912 whilst based at Tondu, and superheating was fitted to the boiler in March 1921. Transferred to Cornwall and St Blazey shed on 30 November 1924, it spent the next 11 years in the West Country, until being moved to the West Midlands and Wellington shed on 2 May 1936. Having passed into British Railways ownership at Nationalisation in 1948, it remained in traffic until withdrawn at Wellington on 4 April 1951, having covered 970,206 miles during its 46 years of service.

The model depicts the locomotive in circa 1928-1935 condition: No.4400 retained its numberplates in the original position on the tank sides until at least 1928, and received outside steam pipes in October 1935. (The photograph opposite shows the engine in the condition modelled, during the early 1930s.) In addition to the inside steam pipes and extended bunker, features specific to No.4400 during the period and replicated on the model include: original straight-sided chimney with copper cap; polished tall safety valve cover; tapered buffers; extra rivet lines on the side tanks, with straight filler lid stands; and the cab minus side shutters, with peaked rain strips.

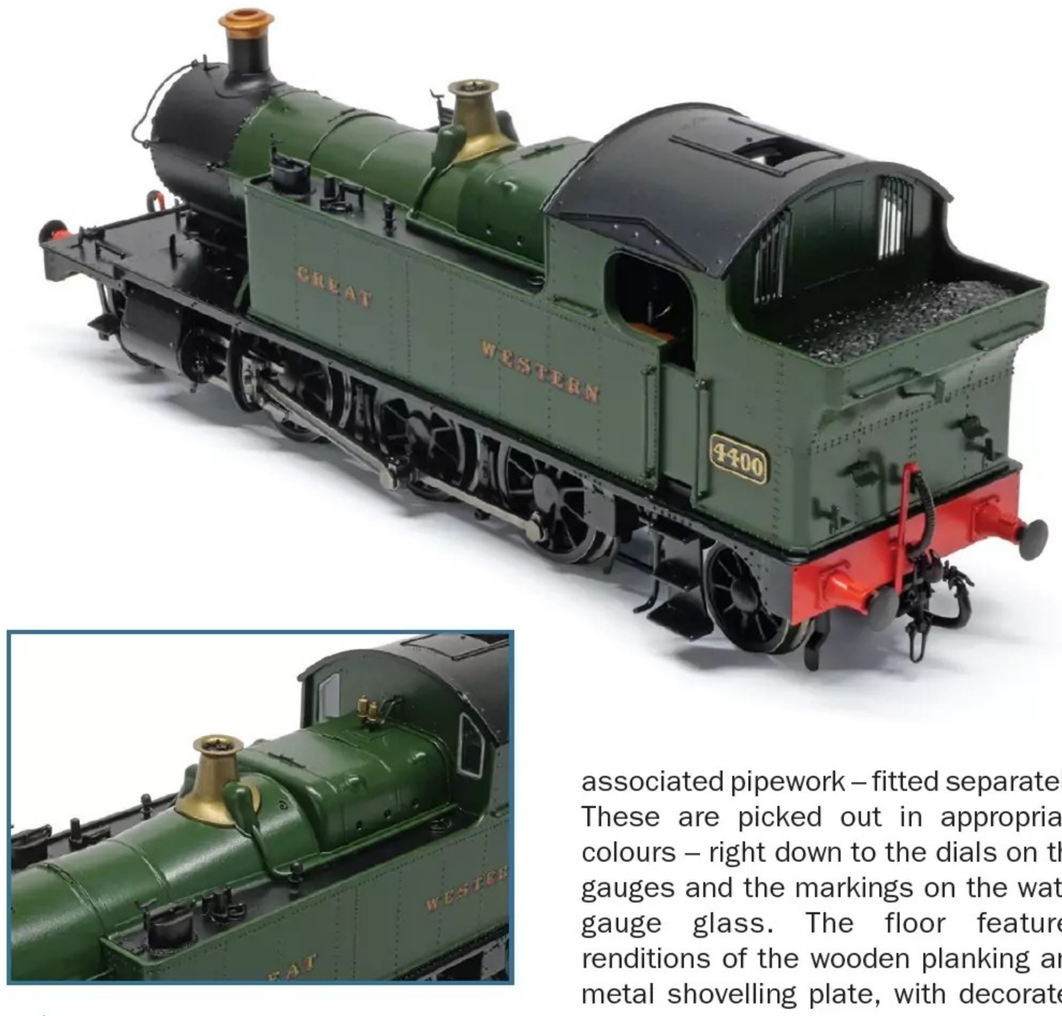
The Great Western green paintwork exhibits a very convincing metallic finish, while the smokebox area has a noticeably flatter appearance than the rest of the black paintwork. The only minor detraction on our sample was an 'orange peel' effect in the application on some parts – this being particularly noticeable on the right-hand cylinder. Another point of interest is the decoration of the safety valve bonnet and other brass fittings such as the whistles – these parts having a very realistic scale appearance and dull brass colour, one of the best renditions we've seen to date. Crisp printing of the gold 'GREAT WESTERN' lettering, complete with red & black shading, adorns the side tanks, with the running number on the front bufferbeam finished in yellow with black shading. Etched brass versions of the printed numberplates are supplied separately as part of the accessory pack, and have been fitted for photography.

The model tips the scales at 256g, with die-cast metal utilised for the chassis block, running plate, cylinders, boiler barrel and wheels, while injection-moulded plastic accounts for much of the upper works, including the smokebox, cab and firebox. The side tanks are made of plastic, but lined with die-cast metal. Handrails are formed from a mixture of metal wire and durable fine plastic, with lamp brackets and bunker steps produced as etched metal parts. Buffers are turned metal and sprung, while brake rigging, front steps and cylinder drain pipes are all pre-fitted.

Separate details supplied for the modeller to fit include a pair of screw-link couplings, and front vacuum and steam-heat pipes – with all appropriate details having been fitted here for photography. It should be noted that the fitting of these details requires the removal of the factory-fitted NEM slimline tension-lock coupling(s). A set of spare whistles are also included, should these parts become damaged or lost.

The appearance of these characterful engines has been captured beautifully. All the available key dimensions are precisely at or very close to scale – most notably the distinctive 4' 1½" coupled wheels, which scale out at 16.5mm, with those on the model coming in at a near-perfect 16.53mm! Castings for the coupling rods incorporate very fine depictions of the oiling points, while the fluted connecting rods exhibit a convincing sense of weight. The smokebox bristles with finely rendered rivet and

▲ The firebox glow illuminating the cab.



▲ The safety valve bonnet has a very convincing dull brass finish.

bolt detail, enhanced further with separately fitted plastic details, including commendably fine depictions of the steam lance cock, snifting valves, and smokebox door dart.

Despite being largely enclosed, the cab has certainly not been overlooked, with the majority of the backhead details – including controls and associated pipework – fitted separately. These are picked out in appropriate colours – right down to the dials on the gauges and the markings on the water gauge glass. The floor features renditions of the wooden planking and metal shovelling plate, with decorated depictions of the tip-up wooden seats on the front of the bunker. All this footplate detail is complemented by the firebox glow, which operates when power is applied under analogue control; it is understood that this feature is synchronised with the sound functions on factory-fitted DCC sound examples.

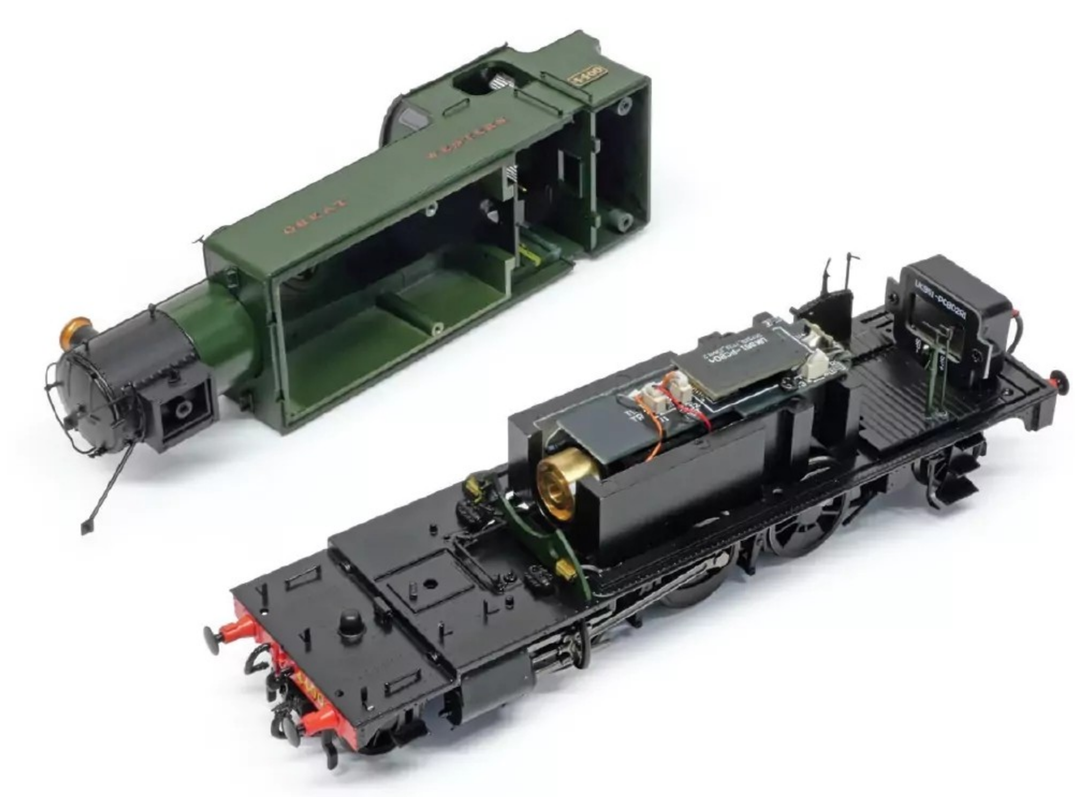
Mechanism and performance

The model is equipped with a flywheel-fitted motor, mounted amidships within the tank casting, and providing drive to the rear coupled axle. Current is collected via wiper pick-ups from all coupled wheels, all of which feature brass bearings. The pony and trailing trucks have slotted pivots and operate on a sprung cam system – an arrangement which allows the model to negotiate second radius curves.

Provision is included for digital operation, in the form of a Next18 DCC interface, located atop the motor assembly. This is complemented by a factory-fitted 15mm x 11mm 'sugar cube' speaker, situated in the bunker.

The mechanism and decoder interface is accessed by removing the bodyshell, held in place by five screws: two under the bunker, two under the side tanks (one either side), and one located deep within the chassis casting between the cylinders. Once these are removed, the body can be lifted carefully away from the running plate – ensuring that the smokebox struts (which are also part of this assembly) are disengaged from their housings in the front of the running plate.

On test the Prairie proved to be a smooth and quiet runner through the speed ranges. In haulage tests, it managed a remarkable 12 Pullman cars around the Pecorama loft layout, with 1 in 48 gradients and 3' radius curves – far exceeding the limit of four GWR bogie coaches, or 120 tons, for the class on the 1 in 40 gradients of the Princetown branch.



Prairie precursor

Rapido is to be highly commended for bringing to market this missing link in the small Prairie story, one which is sure to be the catalyst for numerous Great Western rural branch line layouts – further complemented by the firm's two-coach Diagram E140 'B' sets (see reviews, RM April 2025) and its plethora of GWR goods vehicles.

Like the prototype, could the '4400' form the basis for future releases, such as the Churchward '4500' and Collett '4575' variants – something that Rapido did allude to in its initial announcement – or even the '4400s' in their earlier Edwardian form? Now that would be something to savour!

PROTOTYPE VIEW GWR '4400' 2-6-2T

▲ No.4400 in the condition represented by our Rapido sample, at Swindon shed in the 1930s. *Photo: Rail-Online*

Prototype information

The first '4400' – originally numbered 115 but later renumbered as 4400 – was the first small Prairie to be built, at Swindon Works in 1904. After a lengthy trial period, 10 more were constructed at Wolverhampton Works in 1905/6, numbered 3101-10 (later 4401-10). Like No.115, these locomotives featured 4' 1½" driving wheels, saturated boilers with inside steam pipes and short smokeboxes (without struts), and small coal bunkers which matched the height of the side tanks. Though they proved more than capable machines, the small coupled wheels turned out to be a hindrance when higher speeds were required, and so the '4500' class was introduced in 1906, with 4' 7½" coupled wheels.

The locomotives were subjected to numerous improvements over the course of their service careers, including (to name but a few) the addition of superheating to the boilers, smokeboxes extended by 6" with supporting struts, and some examples receiving outside steam pipes. One of the most drastic changes was the extension of the frames and running plate by 9" at the rear, to accommodate longer bunkers with higher coal capacity; all bar No.4410 received this modification.

The class became synonymous with several steeply graded and tightly curved GWR branches – most famously the Yelverton to Princetown line across Dartmoor – with their sure-footedness and quick acceleration making them popular amongst crews. All would pass into British Railways ownership following Nationalisation in 1948, but withdrawals began soon after, starting with No.4402 in December 1949. The last survivor – No.4410 – was condemned in September 1955, and all were scrapped.

Sample supplied by

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PRICES
£172.95 DCC ready
£284.95 DCC sound-fitted

British Rail Class 60 from Cavalex Models

After an impressive debut with the British Rail Class 56 (see reviews, RM May 2024), Cavalex Models has delivered its second 00 gauge ready-to-run locomotive project, depicting Britain's last truly home-produced diesel-electric – the Brush Type 5/BR Class 60 – plans for which were revealed in our April 2023 issue.

Based on original works drawings and extensive surveys of multiple machines at Toton, the Essex-based manufacturer has developed a comprehensive tooling suite, with provision to accurately cover all 100 locomotives from their introduction into traffic (including class pioneer 60 001 with its numerous unique detail differences) and their subsequent in-service modifications – including those overhauled as part of the 'Super 60s' programme for current main line operation.

Supplied Digital Command Control ready or DCC sound-fitted, no fewer than 11 versions form the general release, comprising BR Sectorisation grey guises (including Petroleum, Construction, Coal and Metals sector branding), along with a 'shadow Privatisation' example in Loadhaul black & orange, and a variety of post-Privatisation colour schemes – including English Welsh & Scottish Railway maroon & gold, DB Cargo red, Colas Rail yellow & orange, DC Rail Cappagh Group of Companies blue and GB Railfreight blue & orange.

Five retailer exclusives have also been produced: two in triple grey with Transrail branding for Ultimate Model Railways; Rails of Sheffield exclusives 60 062 *Sonia* with DB 'Steel on Steel' side vinyls portraying a glowing furnace, and 60 081 *Isambard Kingdom Brunel* in Great Western Railway green; and 60 098 *Charles Francis Brush* in Railfreight grey with Construction Sector branding, forming part of the Locomotion Models/Rails of Sheffield 'Railway Icons' range, and complete with replica Brush Traction works plate.

A brief history

With British Rail needing locomotives to supplement its Class 56 and 58 fleets in the growing heavy haul sector, a competitive tender for 100 new Type 5 heavy freight locomotives was announced on 10 August 1987, with the contract formally awarded to Brush Traction of Loughborough on 17 May 1988. Design work and bogie fabrication was carried out by Brush, with the fabrication of the bodyshells subcontracted to Procor of Horbury, the power units sourced from Mirrlees in Stockport and the cooler units from Serck, with the assembly of the locomotives being completed in the revamped erecting shops at Brush Falcon Works, Loughborough. The first of the class – 60 001 *Steadfast* – was handed over to BR on time on 30 June 1989, with delivery of all 100 locomotives completed by 1991.

Their introduction was not smooth, however, with the early deliveries encountering a number of serious issues – most involving computer software, with some noted to have bogie and structural defects. Eventually, these problems were ironed out, with the fleet settling down to provide more reliable service.

In preparation for Privatisation during 1994, BR's trainload freight business was broken up into three 'shadow' private companies – Loadhaul, Mainline and Transrail – in 1994, with the Class 60 fleet spread between these entities. Following the purchase of all three companies by the American railroad Wisconsin Central and its formation of English Welsh & Scottish Railway in 1996, all passed into EWS ownership. However, like most former BR locomotives, their tenure with the company was short-lived, with many soon being placed into storage in favour of the state-of-the-art General Motors Class 66s – though the remainder were still in active service right up to the acquisition of EWS by DB Schenker in 2008.

An overhaul and upgrade programme was initiated by DB during 2011, improving the locomotives' reliability and range, with these being dubbed 'Super 60s'. Since then, over 20 locomotives have been through the programme for use by DB Cargo, along with further class members purchased by Colas Rail in 2014, while some mothballed examples were later overhauled for use by DC Rail. To date, many remain out of use, with some recently acquired by DC Rail and Land Recovery for potential future reinstatement into service.

Magnetic bufferbeams allow easy swapping of the fully detailed versions for those with slots for the tension-lock couplings.

Our samples in detail

We received a pair of samples for review. The first (ref.CM-60027-TGP) is presented as 60 027 *Joseph Banks* in BR Railfreight triple grey with Petroleum sector branding. The real machine entered service on 2 February 1991, being allocated to Immingham (IM) from new. Following Privatisation in 1996, it was absorbed into the EWS fleet (now DB Cargo), being placed into storage during 2009. It is currently one of a large number of 60s languishing at Toton Old Bank yard.

The second model (ref.CM-60061-TGCL-SOUND) is finished as 60 061 *Alexander Graham Bell* in BR Railfreight triple grey with Coal sector branding (this sample also being equipped with DCC sound). Entering service on 6 June 1991 and allocated to Toton (TO), it would become one of 17 Class 60s to be operated by the 'shadow' private

company Transrail in the build-up to Privatisation – with the locomotive gaining the operator's branding and 'T' logo, though retaining the triple grey colour scheme. As with the rest of the fleet, it became the property of EWS in 1996, continuing in on-off service until withdrawn from traffic at Toton in August 2009 – still sporting its Transrail livery. It has since been purchased by freight operator DC Rail, returning to its place of birth – Brush Works, Loughborough – for storage.

Like the Class 56s, the models are presented in a sleek, dense foam-lined card box, finished in the firm's eye-catching orange & black colour scheme. Inside is a lavish owner's manual that provides historical notes on the real machines, as well as general information on the model – including features, accessory fitting, lighting and functions, and much more. (An additional functions list is also provided.) The models themselves are supplied mounted to a plastic cradle, which is attached securely to the underside of the chassis by two screws.

Upon releasing the locomotives from their packaging, the first aspect that strikes you is the quality of the finish and the fidelity to the prototype. Application of the BR Sectorisation triple grey livery is second to none: all lines are straight with clean delineation between colours, complemented by fine renditions of the orange cantrail stripe which neatly follows the circumference of the roofline. The striking sector logos are applied crisply, along with legible data labels and Brush Traction works plates.

This level of finely printed detail extends to the dials on the fuel tank gauges, and the plethora of small markings and warning labels which adorn the lower body and hydraulic dampers on the bogies. Nameplates and stainless steel BR double arrows are provided as separate metal etches, each supplied with double-sided self-adhesive backing and a handy jig (with separate instructions) to ensure correct placement.

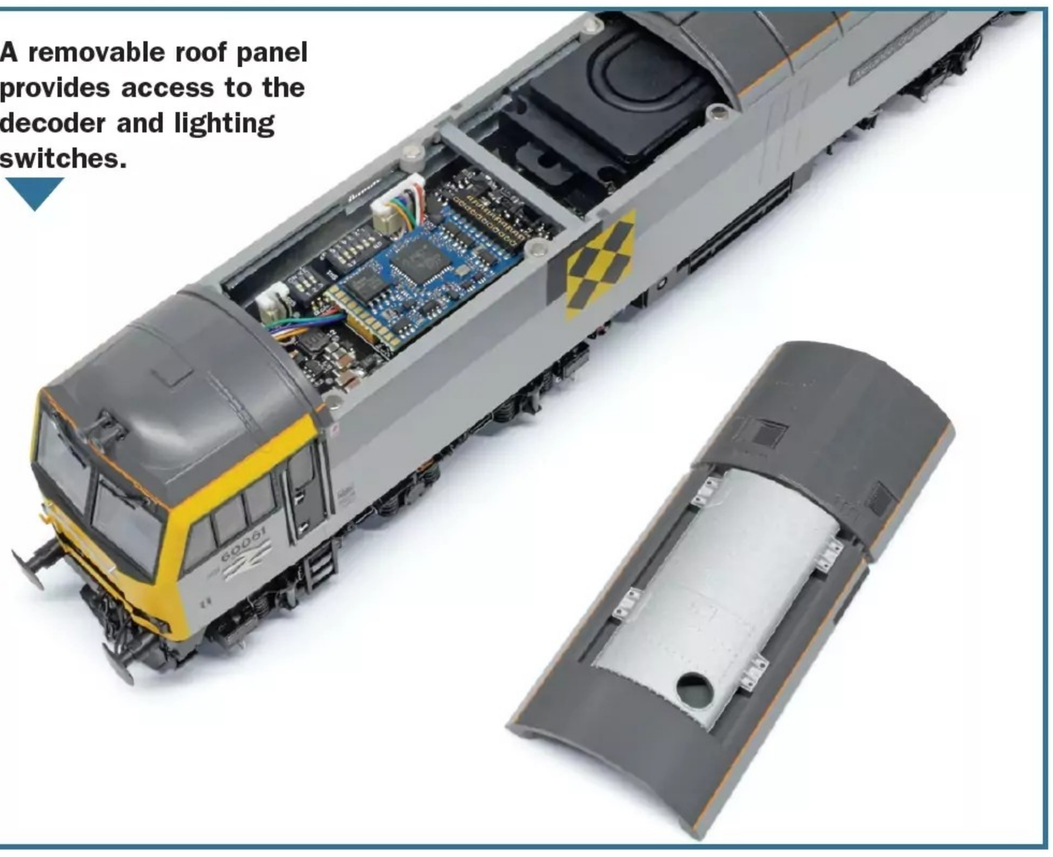
Features specific to our samples, to name but a few, include: three-pane driver's windows; NRN pods; scavenger vents (inboard of the exhaust at the No.2 end to help reduce overheating); two-piece fabricated lamp irons; and original horns (without snow shields). The only minor detail difference – such is the level of accuracy – concerns the otherwise standard original BMAC light clusters, with 60 027 being one of several early locos to have its marker and headlights sealed with white silicon sealant, a detail which has not been overlooked on the model!

By far one of the most distinctive features of these Brush heavyweights is the open nature of the cooler group area at the No.1 end, and Cavalex has gone to great lengths to represent this key aspect of the Class 60 design. The commendably fine etched bodyside grilles (which have been manufactured using a two-stage etching process to replicate the over/under lapping wire mesh) afford not only a clear view of the prototype-specific cooling system (produced from several separate mouldings, with the radiator on the secondman's side even sporting its own individual etched mesh), but also the ability to see right through the locomotive and out the other side – just like the real thing! The drive shaft from the motor (which has to pass through this area) has been skilfully concealed, with the adjoining bogie gear tower enclosed out of view within the vestibule between the cab and engine room bulkhead. This area is further enhanced by the prototypical placement of the engine room lighting, which clearly illuminates the internal machinery, whilst casting a near-perfect glow through the side mesh.

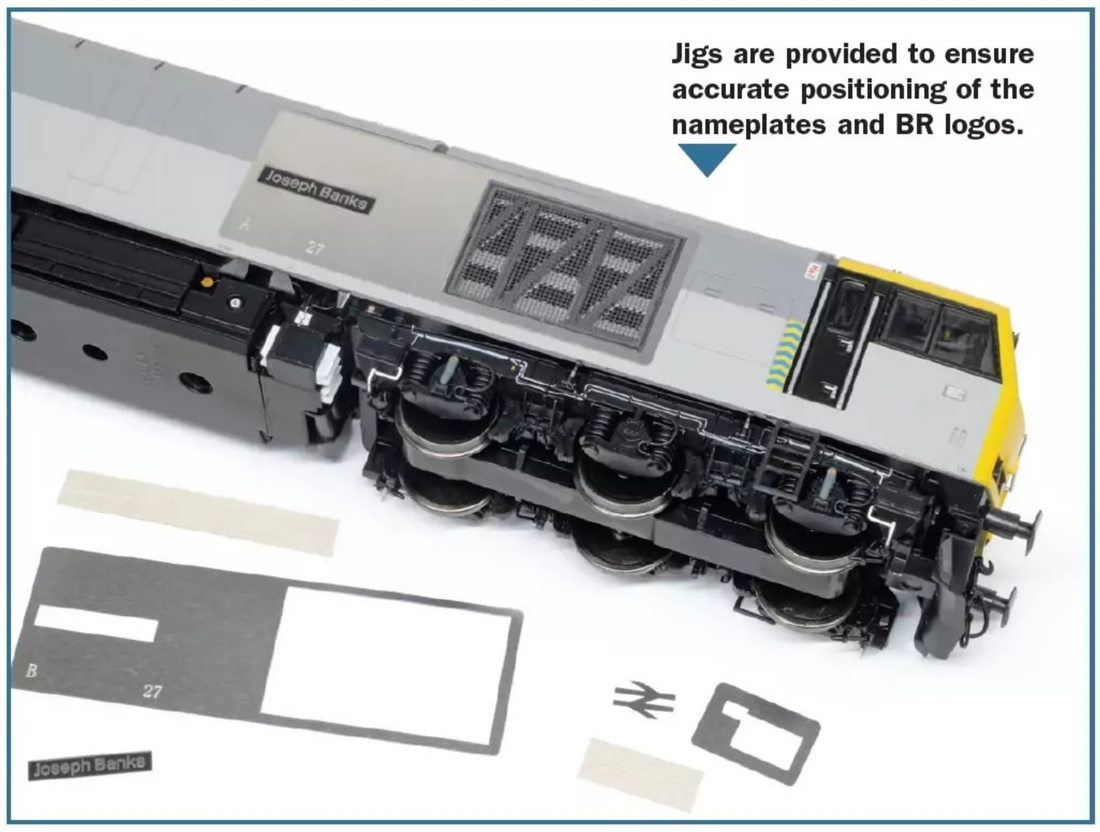
The bogies – which take the form of plastic mouldings – exhibit a very impressive level of depth and detail, with full-relief representations of the suspension springs, hydraulic dampers and axlebox assemblies, the latter embellished with the maker's stamp 'SKF'. The abundance of pipework found on the real machines has been depicted faithfully, with these intricate and complex pipe runs formed as part of the moulding process, crisply capturing the various levels and layers to great effect – all picked out neatly in appropriate colours. The cab steps, earthing cables and bogie retention brackets are produced as separate fittings, with the former – true to the prototype – tapering towards the bogie at the bottom.

The models feature innovative magnetic bufferbeams – dubbed Adaptable Magnetic Detail – which give the modeller the option to swap the as-supplied fully furnished renditions of the bufferbeams (complete with

A heavy cast metal chassis block houses a centrally mounted motor driving both bogies.



A removable roof panel provides access to the decoder and lighting switches.



Jigs are provided to ensure accurate positioning of the nameplates and BR logos.

Our samples depict 60 027 with Petroleum sector logos (left) and 60 061 with Coal sector branding.

pivoting screw-link couplings, and full depictions of the air/brake hoses and snowploughs), for partially decorated versions with apertures providing access to the self-centring NEM pockets. (Two slimline tension-lock couplings are provided in one of the accessory bags.) The hold of these magnetic bufferbeams is surprisingly strong, allowing for trains to be hauled using installed screw-link couplings. Buffer heads are plastic, but sprung.

Cab glazing is bordered by extremely fine renditions of the frames (complete with screw head detail) and gives a tantalising view of the period-specific cab interiors. Control desks are embellished with finely printed renditions of the dials and gauges, with a number of the controls picked out individually in the appropriate colours, giving the appearance of separately fitted parts. Note the structural framework just visible in the front corners of the quarterlights: this distinctive feature is also formed as part of the cab interior assembly. Two optional blinds are supplied as separate accessories, and the seats (complete with armrests) are removable, allowing for the easy installation of era-specific crew figures – available from the likes of Modelu (see page 841). Access is achieved by removing the bodyshell, held in place by four securing clips – one located to the rear of each bogie.

Ultimate driver experience

The chassis comprises a substantial metal casting, housed within which is a centrally mounted twin flywheel-fitted five-pole motor providing drive to the outer axles on each bogie. Power is collected from all wheels – all of which feature brass bearings – and the bogies have been designed with provision for regauging, with replacement wheelsets from Cavalex: EM/18.2mm gauge (ref.CAV-WS60-EM) or P4/18.83mm gauge (ref.CAV-WS60-P4).

Tipping the scales at 687g, on test our analogue sample was capable of hauling 24 Pullman cars with ease around the Pecorama loft layout, with its 1 in 48 gradients and 3' radius curves.

Factory sound-fitted models are equipped with a 21-pin ESU LokSound V5 sound decoder, which can be accessed via a removable roof section held in place with magnets behind the cab at the No.2 end. The decoder comes pre-loaded with a bespoke sound project incorporating no fewer than 31 control, lighting and sound functions; produced with the assistance of a former Class 60 driver, the project is complemented by a written step-by-step guide to the day-to-day prototypical operation of the locomotives, which gives further insight into how best to optimise the operation of the model for the most immersive driving experience.

Sounds are emitted through an ESU Passive Radiator Speaker, which combines a 15 x 11mm 'sugar cube' speaker with a 24 x 55 x 8.6mm acoustic chamber featuring a built-in passive radiator. (It should be noted that the latter is only installed in factory sound-fitted examples.) As with the firm's Class 56 project, this powerful speaker system makes for a very impressive combination, capturing the characteristic throaty roar of these 3,100hp machines. Built-in Hall sensors automatically activate flange squeal when entering curves under a certain radius (this sound can be isolated using function F17), whilst other notable functions include a driver lock feature that enables the engine speed to be increased or decreased whilst the model is maintaining a set speed, and heavy train mode, which replicates the locomotive working under load and increases inertia.

In addition to the aforementioned engine room lighting, the model comes pre-programmed with an extensive list of lighting configurations for prototypical operation, including yard, shunting and stabling modes, as well as the usual day/night functions and independently controllable tail lights. A new lighting feature not seen previously on any 4mm scale UK model is the Hazard Warning Lights function: when activated, this causes both headlights to flash, in order to warn drivers of passing trains or members of staff on the lineside of a serious problem or hazard. For 12V dc users, a limited range of lighting functions can be controlled via the smaller bank of three switches parallel with the decoder socket.

'Super 60s'

The Class 60 is an impressive sequel to the firm's Class 56 model, taking what was already a solid ethos (with regard to research, design and the quality of the end product) and taking it another leap forward. Simple, yet innovative features such as the inclusion of jigs for correct alignment of the etched plates, and the firm's ground-breaking Adaptable Magnetic Detail bufferbeams, are game-changers which we hope to see included on future releases.

On this note, the firm has recently revealed the subject of its next OO gauge ready-to-run locomotive project, which focuses on another classic item of Brush traction – the ubiquitous Type 4/BR Class 47 (see news, RM September 2025).

Samples loaned by

CAVALEX MODELS
www.cavalexmodels.com

PRICES
Ref.CM-60027-TGP – DCC ready – £194.95
Ref.CM-60061-TGCL-SOUND – DCC sound-fitted – £294.95

ICI bogie limestone hoppers by Accurascale

00

▲ **Twin packs of wagons in final 1992-1997 condition (top) and 1973-1992 guise.**

Back in our May 2018 issue, we appraised examples of the first ready-to-run models of the celebrated Imperial Chemical Industries bogie limestone hoppers in 00. Originally produced by Oxford Rail exclusively for the former Widnes-based retailer Hattons, the tooling for these popular models has since been secured by Accurascale, with the wagons joining the firm's growing 'Moving Britain' range.

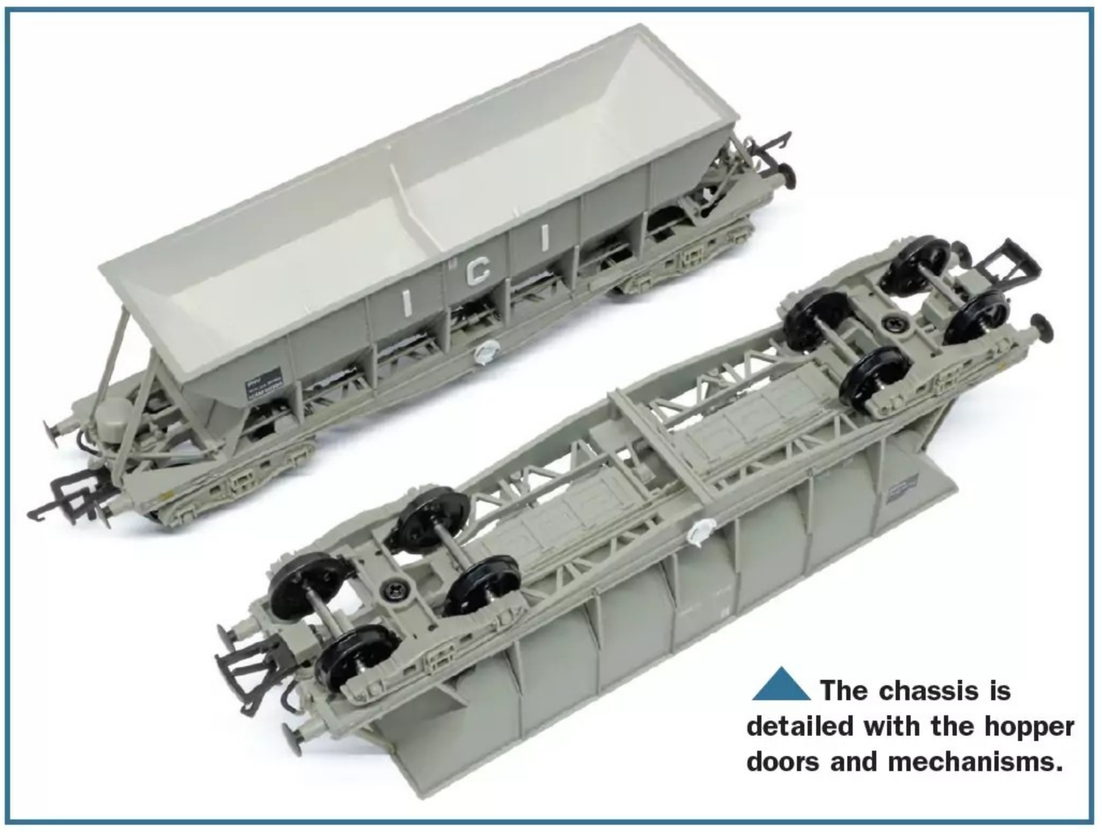
Ordered by ICI to transport crushed limestone from Tunstead Quarry in Derbyshire to the soda ash plants at Northwich in Cheshire, a total of 152 of these wagons were constructed by Charles Roberts of Horbury in three batches between 1936 and 1953. Although all were similar, differences between the batches became widespread over time, with the diamond-frame bogies on the first batch of 84 vehicles being replaced with the plate-frame type as fitted to later builds, and oil axleboxes subsequently replaced with roller bearings, amongst other subtle changes.

The hoppers would go on to have long and illustrious careers, spanning over 60 years, becoming synonymous with the Derbyshire Peak District railway scene. Motive power was provided by a wide variety of steam and diesel traction, most notably Stanier 8F 2-8-0s and BR Class 37s. It is estimated that some of the wagons accumulated mileages of between 3 and 5 million miles during their working lives!

The last of the hoppers were withdrawn in 1997, though this was not down to their age or condition, but due to the lack of suitable vacuum-fitted motive power being readily available to haul them. Upon scrapping, it was noted that many still had much life in them, no doubt due to their solid construction. Six have been preserved.

15 twin packs, spanning three eras (1950s-1973, 1973-1992 and 1992-1997) have been produced, allowing modellers to replicate a sizeable block train formation of 10 individually numbered vehicles in their chosen period.

For review we received samples of each of the eras outlined above. The first of these (ref.ACC3066) comprises Nos.3268 and 3285 in 1950s-1973 condition – No.3268 being part of the initial batch dating from 1936, correctly featuring original diamond-frame bogies and smaller ICI lettering on the body sides, with No.3285 (produced as part of the second batch in 1945) riding on plate-frame bogies with oil axleboxes, and wearing larger ICI lettering. The second pack (ref.ACC3071) contains ICIM19061 and ICIM19094 in 1973-1992 condition, both sporting plate-frame bogies with roller bearings and PHV TOPS (Total Operations Processing System) codes. The third pack (ref.ACC3074) consists of Nos.19017 and 19025 in their final 1992-1997 guise, once again on plate-frame bogies with roller bearings, though with the ICI lettering removed.



▲ **The chassis is detailed with the hopper doors and mechanisms.**

▲ **Original diamond-frame (left) and later plate-frame bogies.**

Details and specifications are very similar to the initial Hattons/Oxford Rail release, with construction taking the form of injection-moulded plastic for the hopper bodies, frames and bogies. Tipping the scales at 45-46g, the models are adorned with numerous fine separate plastic components (forming detailed depictions of the vacuum braking systems and hopper door mechanisms), with etched metal fittings utilised for the vacuum cylinder supports. Buffer heads are plastic and not sprung. The bogies are equipped with chemically blackened pin-point metal wheelsets of the three-hole design, and are extremely free-rolling.

NEM slimline tension-lock couplings are fitted as standard – mounted in self-centring dovetail pockets attached to the frames. Each wagon is supplied with a pair of alternative three-link couplings, provided in a small poly bag; however, we found these too small to fit over the drawhooks, and they should be screw-links or instanters as per the prototype.

Printing on all models is first class, right down to the ornate Charles Roberts & Co. Ltd cast builder's plates (present on the steam-era examples) and TOPS panels applied to those in post-1973 condition. Worthy of note is the fine printing on the oil axleboxes, which reads 'C.ROBERTS & CO. LTD, WAKEFIELD'. True to the prototypes, the ICI letters have been produced as separate fittings.

The models are available direct from the manufacturer's website, priced £79.94 per twin pack. With plenty of suitable diesel traction currently on offer, this month's announcement by Bachmann of an all-new Stanier 8F could not have been more timely!

Samples supplied by

ACCURASCALE Ltd,
60 Windsor Avenue,
London SW19 2RR.
www.accurascale.co.uk

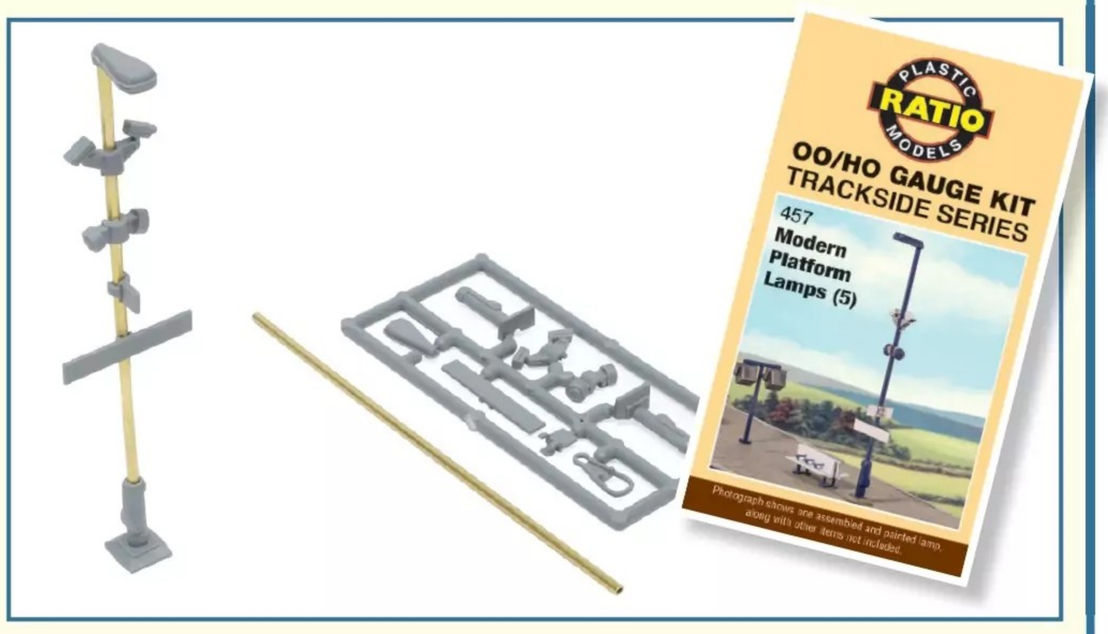
PRICE
See text

New products from the PECO group of companies

Modern platform lamps from Ratio in OO & N

Ratio Plastic Models (part of the Peco group of companies) has added to its extensive range of accessories and kits, with modern platform lamps in both 4mm/OO (ref.457) & 2mm/N (ref.282).

Produced mainly in plastic, with easy-to-assemble parts and clear instructions, the 4mm scale kits have the added advantage of pre-cut lengths of brass tubing for the main post – facilitating the option of adding lighting, though the modeller will need to source the necessary fittings separately. Each sprue includes optional attachments, allowing for varying levels of customisation depending on personal preference, such as CCTV cameras, loudspeakers and backings for station signage.



Priced £12.95, the OO kits include enough parts to produce five complete lamps, with 10 able to be assembled from the N gauge version, priced £10.95.

Naturally, the purchaser will need to paint and assemble the models, with contact adhesive recommended for bonding. Unpainted assembled examples for both scales are illustrated here.

Samples supplied by

PECO
Underleys, Beer, Seaton, Devon
EX12 3NA
www.peco-uk.com

PRICES
See text

21-ton mineral wagons from Hornby

TT:120

Following our review of the TT:120 vacuum-fitted BR 21-ton mineral wagon (Total Operations Processing System code MDV) in our August 2024 issue, Hornby has received stocks of its unfitted counterpart – the Dia.1/107 (TOPS code MDO) – in the same scale.

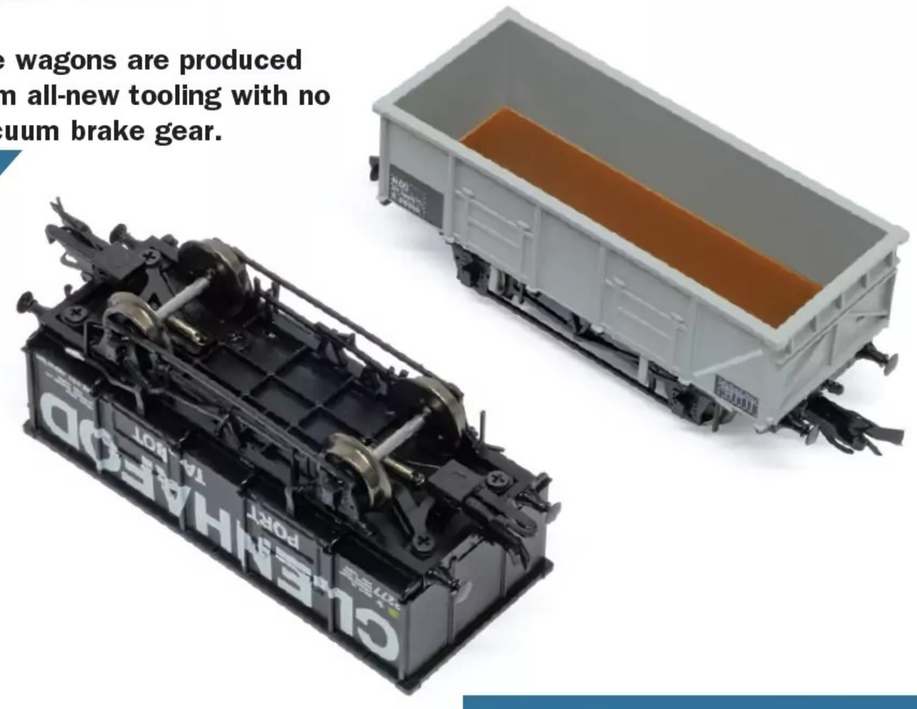
Two examples form this latest release: the first (ref.TT6016) is presented as No.B316500 in BR grey with MDO TOPS code and data panels. Upon comparing the model to images of the real vehicle taken by prolific wagon photographer Paul Bartlett, we noticed a few discrepancies, mainly down to the prototype being a 1972 Shildon rebodied wagon (of which 2,600 were built on redundant hopper chassis between 1971-73) with roller bearing axleboxes: the tooling for the model caters for original bodies of welded construction with oil axleboxes.

The second wagon (ref.TT6017) is presented as No.2277 in the bold black livery of Glenhafod Collieries Ltd, Port Talbot, with unshaded white lettering. This choice of prototype is even more perplexing, with the real No.2277 not being part of the Dia.1/107 family at all, but a 20-ton all-steel mineral wagon of riveted construction built by Charles Roberts & Co. Ltd, Wakefield, in 1933! Though an 'off-the-wall' choice, the colour scheme certainly suits the wagon well – the same identity having also previously been applied to a generic steel-sided wagon in the firm's OO range back in 2002.

Discrepancies aside, both examples capture the liveries of their chosen prototypes very well. Markings on our BR example replicate those seen in the Paul Bartlett image – with hastily applied MDO stencilled lettering and crudely painted black data panel backings, which partially cover the diagonal end door stripe on one side. The rendition of the Glenhafod livery is particularly sharp, and is closely representative of that applied to the prototype vehicle as illustrated in *Private Owner Wagons Volume One* by Bill Hudson, extending to renditions of the Charles Roberts & Co. Ltd cast builder's plates on the solebars.

Despite being outwardly similar in appearance to their vacuum-fitted counterparts, both the wagon body and chassis have been produced from all-new tooling – the most notable differences (aside from the lack of the vacuum-braking system) being side doors without top flaps, and single door springs. The headstocks sport separately fitted depictions of the vacuum pipe plugs, despite the wagons being unfitted! Tiebars and four-shoe Morton brake gear exhibit a commendable level of delicacy, though surprisingly, the latter does not align with the wheel treads.

The wagons are produced from all-new tooling with no vacuum brake gear.



Construction mirrors that of the previously released MDVs, taking the form of an injection-moulded plastic body and chassis with metal wheels, giving an overall weight of 9g. The buffer heads are plastic and not sprung. NEM European-style couplings are fitted as standard, housed within sprung self-centring sockets attached to the underside of the frames. The models roll freely on chemically blackened three-hole wheelsets with pin-point axles, though spoked wheels may have been more fitting for the Glenhafod example.

Despite the discrepancies with the identities, the model as a whole is a sound one and sure to please many.

Samples supplied by

HORNBY HOBBIES Ltd
Westwood, Margate, Kent
CT9 4JX
www.hornby.com

PRICE
£22.99 each

Fundraising wagon for the Mid-Suffolk Light Railway

00

The Middy Trading Company, the fundraising arm of the Mid-Suffolk Light Railway, has commissioned from Dapol the latest in a line of exclusive limited edition wagons in 00. The model – limited to 156 pieces – is the company's 24th limited edition special commission, with profits going towards the running costs of the MSLR.

The livery depicts a five-plank Great Eastern Railway wagon with the running number 28681, which has been restored by the MSLR – although the use of standard Dapol tooling means

that it is not an exact rendition of the prototype. Equipped with metal spoked wheelsets, a removable coal load and slimline tension-lock couplings mounted in NEM pockets, it is finished in the company's overall grey colour scheme, with GE lettering and the running number in white.

The models are available in a pristine or weathered finish, and can be ordered through the MSLR shop (priced £17.00 each) or by post (priced £21.00 including postage and packing, or £38.00 for two) with cheques made payable to 'Middy Trading Co Ltd'. 'Pristine' or 'weathered' must be specified when ordering. Wagons can also be ordered online from the MSLR shop website (see panel).

Sample loaned by

MIDDY TRADING COMPANY Ltd
5 Melton Meadow Road,
Woodbridge,
Suffolk IP12 1SB
www.mslr.org.uk/shop

PRICES
See text

New figures from Modelu

4mm

Modelu continues to expand its extensive range of 3D-printed domestic and wild animal figures, which cover scales from 1:148 (N gauge) through to 1:32 (Gauge 1), examples of which are illustrated here in 4mm scale.

First up, we have a selection of livestock packs, in the forms of sheep (ref.31201-076), lambs (ref.31202-076) and pigs (ref.31240-076). Each pack comprises six figures, set in a range of poses (such as standing, sitting and eating) – with the sheep priced £7.95 per pack, the lambs £5.49, and the pigs £11.95.

There is also a Piglets & Sow pack (ref.31241-076) which comprises 10 piglets and a sow, also priced £7.95.

Joining the range of domestic pet figures is the West Highland Terrier pack (ref.31018-076), with two terriers in sitting and standing poses, priced £4.95. There are also several new additions to the firm's range of avian

figures, which now includes Seagulls (ref.31400-076), Crows (ref.31402-076), Pigeons (ref.31403-076), Ducks (ref.31404-076) and Pheasants (ref.31406-076). Priced £4.95 per pack, each comprises a selection of each bird in a range of poses – with wing, feather and feet detail second to none!

Heading to the races, the Thoroughbred Horses in Transit pack (ref.31300-076) and Race Horse Handlers (ref.1643-076) are ideal for a stable or racecourse setting – priced £11.95 each.

Finally, we have a pair of British Rail Class 60 crew packs, comprising a driver and secondman – available in a choice of late BR, circa 1990s (ref.10234-076) or modern (ref.10235-076) attire – priced £7.95 per pack. Produced in a bespoke manner for the new 00 gauge Class 60 models from Cavalex (see review, page 836), each figure is supplied posed in a seated position, complete with crew chair – the latter being a removable detail on the model, allowing for ease of installation.

All figures and accessories are supplied unpainted in a grey-coloured material and require painting.

The full range of Modelu 3D-printed figures and accessories can be seen on the firm's website, together with details of how to order. A custom scanning service is also available by prior arrangement.

Samples supplied by

MODELU
Office & Workshop,
40 Hounds Road,
Chipping Sodbury BS37 6EE
www.modelu3d.co.uk

PRICES
See text

Bob Symes figures from Buggleskelly Station

Multi Scale

Tom Marshall, of Buggleskelly Station, has added a new 3D-printed figure to his range of film and TV personalities and characters from literature, with the latest depicting TV presenter, inventor and railway enthusiast Bob Symes (Robert Symes-Schutzmann). He is fondly remembered by many enthusiasts for his inspirational TV shows on railways and railway modelling, including *Model World* and *Making Tracks*, and videos like *How to Build a Model Railway*, and *A Lineside Look at Model Railways*.

Designed from scratch, the models are based on images of Bob from the 1970s and 1980s, complete with his trademark bushy beard and whiskers.

Produced in a choice of two poses (one enthusiastically talking, the other holding a BBC microphone), the figures are available in scales from US N (1:160) up to 1:12 (doll's house scale). Unpainted models start at £5.00 in scales from US N (1:160) to 00 (1:76), with hand-painted figures priced at £22.00 each, whilst those in larger scales such as 0 (1:43) start from £9.00 unpainted or £26.00 hand-painted – examples of the latter being illustrated here. (Please note that each figure is painted to order and this may take a few weeks to fulfil.)

In addition, Buggleskelly Station is making a limited-time offer for RAILWAY MODELLER readers with a 10% discount on all figures using code 'RM10WYT', with others in the range including the Eric Morecambe and Ernie Wise duo, the Stan Laurel and Oliver Hardy double act, the cast of the classic 1937 Will Hay film *Oh, Mr Porter!* and the Railway Director from the Reverend Wilbert Awdry's Railway Series.

For further details and to explore the full range, see the retailer's website.

Samples loaned by

BUGGLESKELLY STATION
www.buggleskellystation.com

PRICES
See text

Book Reviews

Milk on the Rails

Matthew Pinto

Milk tanks. Hands up who hasn't at some point owned one of the United Dairies (or similar) tank wagons from Tri-ang or Hornby? There surely can't be many of the readership who missed these classic models.

Most of the time, these would have been marshalled into a pick-up goods and towed around the layout without much thought to whether this was correct or not. Like most things in life, the truth is a long way from the assumption and, in this particular case, anyone who asks the question will almost certainly want to pick up a copy of this book.

As a commodity, milk was really much more suited to road transport, given that the quantities can be small and most farms were not rail-served. Milk also has a short life span – especially in the warmer months – so needs speed to convey it. These factors were only outweighed by the need for milk in the larger conurbations, particularly London.

The book's initial chapters not only explain these factors, but present a deep dive into how this traffic could be organised. The development of the various vehicles used is examined fully from chapter four, and it is possibly this which will be of most interest to RM readers, particularly the under-modelled road/rail combination vehicles, which on the face of it seem far more logical than transferring the product from one tank to another.

Routes taken in each region are explored; again, this is London-centric. Train formations, marshalling and loading are covered thoroughly, and often overlooked by modellers is the almost exclusive use of cascaded brake coaches rather than brake vans outside of the pure branch line scenario – the former being rated to work within the NPCS (Non-Passenger Coaching Stock) bracket and at the express speeds required.

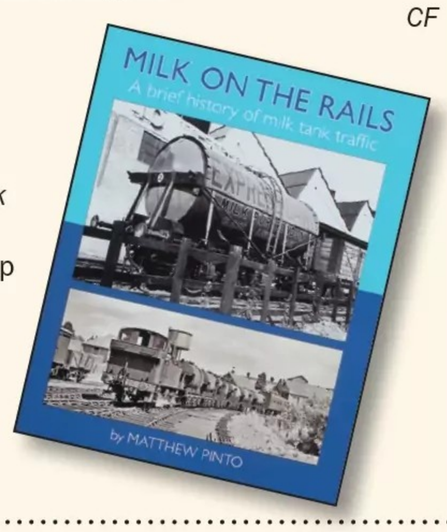
The various small and quite enormous transfer facilities and creameries in each region are covered in a lengthy section, before the book wraps up with appendices on vehicles. As would be expected from this publisher, the quality of presentation is second to none, on gloss paper with copious (mainly monochrome) photos, reproduced to pin-sharp quality. The text is detailed, but easy to absorb and every possible avenue is explored.

Put simply, this is a fine piece of work and is one of those books that has universal appeal.

CF

Wild Swan Books Ltd
4 Tollbridge Studios,
Tollbridge Road,
Bath BA1 7DE
www.wildswanbooks.co.uk

274mm x 215mm, 186pp
Softback, £28.95
ISBN 9781912038572



The Last Days of Second Generation EMUs on the Main Line

Adam Head

For those of a certain age, it may come as a surprise to learn that the 1950s Modernisation Plan diesel & electric units have been replaced. More shocking still is that such a length of time has passed that these replacements too are now time-expired and are in the process of withdrawal – or indeed, in many cases, have already been scrapped. We are now in the third age of multiple units!

All levity aside, many of these second generation units have been with us since the 1970s and 1980s, and not only have they outrun the first batch, but such is the nostalgia around some of them that a select few are now being preserved! This compact volume seeks to provide an overview of these newer units and give them a rousing send-off.

The general presentation is 15 chapters, with two or three photos per page following a half-page potted history of each class (or sub-class). Each photo is accompanied by a medium-length caption, which expands on the initial half-page of information, gives detail for the individual photo, and details routes and other operating information, including end of service dates where applicable.

Classes are covered in numerical order: 313/4/5/7/9, 321/322, 332, 360, 365, 442, 455, 456, 465/6 and 507/8. The oddball in the pack is the Class 483 ex-London Underground 1938 stock, these units ending their 'main line' service on the Isle of Wight. Towards the end of some chapters, the new third generation replacements are also pictured and detailed, which provides contrast and also demonstrates how things have progressed since the 1970s.

This is a clear and colourful volume, which will not only fill some information gaps for the more knowledgeable reader, but almost falls into a 'spotter' framework and so will equally appeal to the newcomer to the subject as an excellent-value primer. For those who can remember the Met-Camms and slamming doors… well, it's just going to make you feel old.

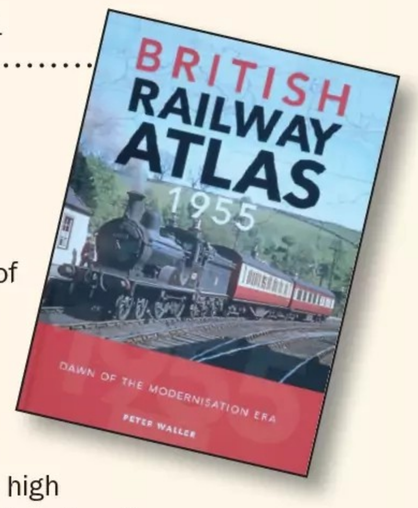
CF

Amberley Publishing
The Hill, Merrywalks, Stroud, Gloucestershire GL5 4EP
www.amberley-books.com

234mm x 165mm, 96pp
Softback, £15.99
ISBN 9781398117631

British Railways Atlas 1955

Dawn of the modernisation era Peter Waller



There cannot be too many serious railway modellers who do not have a rail atlas somewhere in the collection, as they are ideal for getting to grips with that geographical 'what if?' part of the layout planning process. Given that the most popular modelling era is still probably the post-war to steam/diesel transition period, this particular volume could be your ideal companion.

It is, in fact, not a new book, but an updated version of a 2000 edition produced by Ian Allan Books. Here, newly reproduced under the Crécy imprint, it has been physically enlarged to match the other rail atlases in the publisher's catalogue.

Like the others, it pinpoints a particular date – in this case 1955, equidistant between Nationalisation and the more drastic mileage reductions felt after the Beeching cuts.

The format is similar to that of a road atlas, with a sheet format starting in the far South West and rolling upward through 39 sheets to Caithness.

The reproduction is very clear, and some of the more compressed areas, such as South Wales and Greater London, are reproduced in a smaller scale to provide the detail needed on separate sheets.

The map shows the Eastern, London Midland, North Eastern, Scottish, Southern and Western regions in individual colours for clarity, further split into solid and broken lines for routes open to passengers and closed or goods-only. New to this edition are 32 pages of black & white photos.

These cover a wide variety of locomotive subjects with a common thread of their date, 1955. This is the overriding qualification and the photos, while they are all reproduced to a very high quality, show no logic to their order of presentation, either geographically or in their subject. I'm not sure if this section adds anything to what is a good, clear set of maps, as the modeller will naturally seek out specific prototype photographic material from other sources. Nice though it is, it feels like a filler. The book concludes with a clear alphabetical 30-page station index with map references.

The overall quality is second to none, and this would be an excellent, good value addition to any railway library, especially for those modellers working within the Nationalisation to BR blue period.

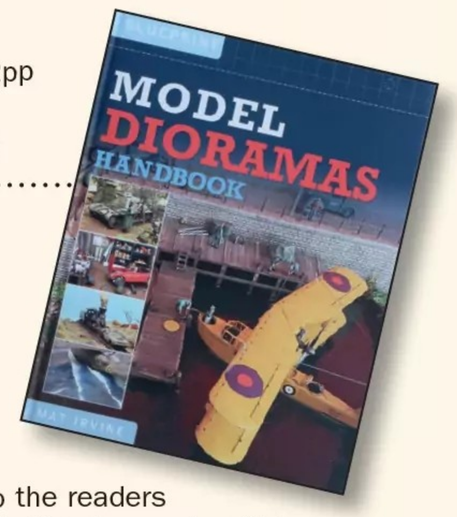
CF

Crécy
1a Ringway Trading Estate, Shadowmoss Rd,
Manchester M22 5LH
www.crecy.co.uk

304mm x 217mm, 112pp
Hardback, £20.00
ISBN 9781800353213

Model Dioramas Handbook

Mat Irvine



Do we build layouts or dioramas? Discuss… The line between the two has always been slightly blurred, and as houses have become more compact, and layouts have become generally smaller as a result, what would have been considered a diorama only a few years ago is now a micro layout. While this book is published by a company which is known for railway titles, it is not aimed specifically at the railway modeller; in fact, this section of the modelling fraternity hardly gets a look in. Does this make this a pointless review? Absolutely not, as there is much in here to inspire.

After a short introduction, there is a chapter defining what a diorama is, and the historical roots of placing models on fixed scenic bases. Although the introduction says that tools and construction will not be discussed, there follows a long chapter on various tool selections and what they may be needed for, before moving on to kits and aftermarket scenic accessories. Bases are considered – that is, the various sheet materials that could be used. This may seem blindingly obvious, but there are some slightly left-field and interesting suggestions offered. Displaying the diorama – how, why and where – is covered, before looking at buildings, either to be used as stand-alone subjects, or as a secondary scenic background or surround. The final chapters cover construction and a lengthy discourse of the application within film sets – with, of course, an honorary mention for the ground-breaking work of *Thunderbirds*.

At first glance, this is little more than a coffee table book, and may be seen as not having any application to the readers of this magazine. However, there is a whole raft of inspirational nuggets within the pages – inspiration that can very easily be transferred to a model railway layout, either wholesale, or by taking the concept of a small detailed scene and incorporating it into a large layout. Conversely, it doesn't require too much of a jump to take one or more of the ideas included, enlarge slightly, and shoehorn a railway into it – thus creating a micro layout, which is where we came in.

CF

Crécy,
1a Ringway Trading Estate,
Shadowmoss Rd,
Manchester M22 5LH www.crecy.co.uk

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NEWS FROM RAILWAY MODELLER

OCTOBER 2025

Stanier 8F 2-8-0 in OO leads latest Bachmann announcements

Engineering prototype of the Bachmann 8F, with straight reverser reach rod and Fowler 3,500-gallon tender.

As part of its Autumn announcements, Bachmann has revealed that it is developing an OO gauge model of the London Midland & Scottish Railway Stanier Class 8F 2-8-0, one of Britain's most numerous and successful steam locomotives, with news of the new model coinciding with the 90th anniversary of the first prototype example emerging from Crewe Works.

In all, 852 examples of the class were built, many by various builders for other Big Four companies and military use during the Second World War, but the BR fleet peaked at 666 in the 1957-60 period and 150 were still in service in 1968, the last year of BR steam. Eight are preserved in Britain, with two of those repatriated from Turkey.

The Bachmann OO gauge model draws inspiration from the N gauge version in the company's Graham Farish range. The new model has a die-cast metal boiler and running plate, and a coreless motor which provides drive through a die-cast metal gearbox to the third set of driving wheels. Electrical collection comes from all eight driving wheels, and tender pick-ups are fitted.

Details include separate metal wire handrails, turned brass safety valves and metal top feed pipes. The whistle, sandboxes, lamp irons and smokebox darts are all added individually, along with the lubricators and their pipework, steam pipes and the smokebox saddle. Sprung metal buffers are fitted to the bufferbeam, which will be smooth or riveted to suit individual locomotives.

The smokebox door is an area of detail differences between prototypes, so this is a separate part on the model to allow various numberplate and lamp bracket configurations to be depicted. Further additions come in the form of the reverser reach rod, which can be straight or curved to match the prototype, and Automatic Warning System (AWS) equipment where required. Below the boiler is further detailing representing the locomotive's frames and internal fittings atop the chassis.

The cab will feature accurate rivet pattern detailing, flush-glazed windows and individual components such as cab roof ventilators and controls, dials, gauges and valves on the backhead, and a tender fallplate reproduced in metal and hinged to the cab floor. The tender, attached to the loco with an adjustable length drawbar, will come in the form of riveted or welded Stanier 4,000-gallon types, or the Fowler 3,500-gallon design.

Digital Command Control provision comes in the shape of a Plux22 decoder socket within the boiler, and DCC users can benefit from a pre-installed Bach-Up Stay Alive system. Firebox lighting will be included, which operates on both analogue and DCC. Speakers are fitted within both the locomotive and tender.

Sound Fitted and Sound Fitted Deluxe models will come with a Zimo MS450P22 DCC sound decoder pre-fitted, and this feature can be used on both analogue and DCC control.

Five versions have been announced, comprising No.8036 in LMS black with riveted tender; No.8233 in LMS black with riveted tender; No.48305 in BR black with early emblem and welded tender; No.48703 in BR black with late crest and welded tender; and No.48339 in weathered BR black with late crest and riveted tender.

The manufacturer states that tooling for the new 8F is now complete (engineering prototypes are illustrated here) and livery samples are awaited. The expected arrival date of the models is Summer 2026. Prices for standard models are expected to start at £249.95, rising to £359.95 for Sound Fitted examples and £379.95 for Sound Fitted Deluxe versions.

A view into the cab, showing separate fittings and the metal fallplate.

8F with curved reach rod, Stanier 4,000-gallon tender and snowplough.

Report continues overleaf...

Continued from previous page...

E Class 0-6-0T in NER green.

Wickham trolley in Longmoor Military Railway blue.

Livery samples for LNER J71

Also revealed are livery samples of the North Eastern Railway E Class 0-6-0Ts (later London & North Eastern Railway J71 Class) announced in the News section of the September RAILWAY MODELLER. The liveries include NER No.495 in green with maroon and white lining; LNER No.317 in black lined in red with shaded ochre lettering; and BR No.68260 in unlined black with early emblem. The black-liveried models have an RRP of £159.95 for standard versions and £269.95 for Sound Fitted locos, while the NER green-liveried model is priced £169.95 for the standard version and £279.95 for Sound Fitted.

Loco sound upgrades in OO

Continuing with steam outline models in OO, the London Brighton & South Coast Railway E4 Class 0-6-2T will return to the range having been upgraded for sound, with the four versions optionally sound fitted comprising LB&SCR No.514 in lined umber; Southern Railway No.2505 in lined Maunsell green; SR No.2487 in plain black with 'SOUTHERN' on the tanks in 'sunshine' lettering; and BR No.32473 in lined black with early emblem. Standard models have an RRP of £164.95 and Sound Fitted models are priced £274.95.

Moving into the field of diesels, the Class 30/31 models unveiled in 2024 (see review, RM September that year) now have the Bach-Up Stay Alive system fitted as standard along with all-wheel electrical pick-up. The six models now available include some new liveries not previously offered and all come with the option of Sound Fitted or Sound Fitted Deluxe versions, with the latter having auto-release couplings providing automated uncoupling at the press of a single DCC function, along with motorised roof fans and tinted driver's and secondman's windscreen glazing. The livery versions are Class 30s D5574 or D5862 in BR green with side stripes and small yellow end panels; Class 31/1s No.5848 or 31 101 in BR blue with full yellow ends; Class 31/1 31 188 in BR 'Dutch' grey & yellow; and Class 31/4 31 423 *Jerome K Jerome* in BR InterCity (Mainline) 'Swallow' livery. Prices are £214.95 for standard models, £324.95 for Sound Fitted models and £374.95 for Sound Fitted Deluxe versions.

The BR Southern Region 2-HAP Electric Multiple Unit is also joining the Autumn 2025 range with two new livery options – Southern Region green with yellow end panels and BR blue with full yellow ends – and, again for the first time, this model will be offered with a Sound Fitted option. Standard models are priced £299.95 and Sound Fitted versions £409.95.

Other rolling stock in OO

At the other end of the size scale is the diminutive Wickham trolley, with two new versions available including a model, in dark blue livery, of Longmoor Military Railway WD9033, which found fame in the 1960s comedy film *The Great St Trinian's Train Robbery*. The other version is in BR Departmental yellow with end wasp stripes. Both are portrayed hauling a small ballast trailer and are priced at £99.95.

As for new OO rolling stock, there is a hopper theme as new versions of the bulk grain wagon (priced £29.95-£31.95) and JGA, HHA and HKA bogie hoppers (priced £49.95-£64.95) will all be available.

In the EFE Rail range, a new batch of the NAA Propelling Control Vehicles has been announced, with these models now in the Res/Royal Mail and EWS liveries, priced £74.95.

N gauge announcements

Returning to the Graham Farish N gauge range will be the Great Western Railway Castle Class 4-6-0, with three

E4 0-6-2T in Southern Railway Maunsell lined green.

Class 31/1 No.5848 in BR blue.

Bulk grain wagon in weathered BRT blue.

2-HAP EMU in Southern Region green.

NAA PCV in EWS livery.

NER Dia.P7 wooden coal hopper.

Castle No.5055 in post-war GWR green.

Mountsorrel three-plank wagon.

Class 150/2 DMU in Transport for Wales livery.

Carflat wagon in BR Motorail blue.

liveries available with or without sound. No.5055 *Earl of Eldon* will carry post-war GWR lined green livery; No.4082 *Windsor Castle* will appear in lined green with GWR 'shirtbutton' monogram on the tender; and No.5015 *Kingswear Castle* in BR lined green with early emblem. Prices are £179.95 for standard versions and £289.95 for Sound Fitted models.

In N gauge diesel outline, the Class 150/2 Diesel Multiple Unit will be arriving in two new liveries – GWR green and Transport for Wales red & grey – with standard models priced £239.95 and Sound Fitted models priced £359.95.

In addition to nine livery variants being revealed for the NER Diagram P7 wooden coal hopper wagons announced in the September RM – ranging from NER grey to National Coal Board black and BSC Ltd light green, all priced at £29.95 – there are four new livery examples of the three-plank open wagons, which now boast NEM coupling pockets for the first time and are priced £21.95 each (illustrated is an example in Mountsorrel Granite grey); and also new versions of the five-plank china clay wagons in GWR grey with no hoods as individual wagons priced £22.95, and in triple packs liveried in BR bauxite (early) with black flat covers, and BR bauxite (Total Operations Processing System) with peaked blue hoods, with each pack priced £69.95.

In addition, BR bogie Carflats will be joining the Graham Farish range for the first time, with two wagons each available in BR bauxite or BR blue Motorail liveries at an RRP of £36.95.

Scenecraft additions in 4mm

New additions to the Scenecraft range of resin buildings for modellers in OO include a concrete footbridge with blue or red lighting columns (RRP £59.95); a depot mess room and toilet block with blue or red doors and window frames (£49.95); a GWR standard station building with awning in either chocolate & cream or blue & white (£99.95); and a rail yard crew room with red or green framing (£29.95).

There are also platform newspaper kiosks in blue or brown (£34.95); carriage siding wooden raised walkways (£32.95); a thatched cottage with either honey-coloured or grey stone walls (£44.95); and canal coal barges in four different colour schemes, ranging from blue to green and red at £49.95 each.

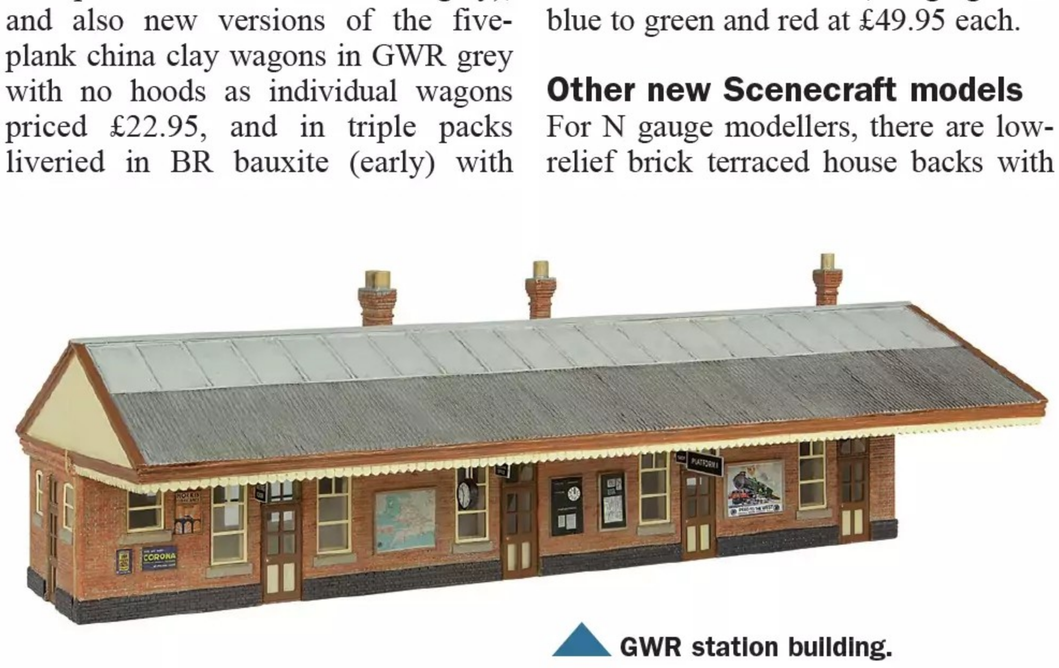
Other new Scenecraft models

For N gauge modellers, there are low-relief brick terraced house backs with either white or blue doors on the left or right of a lean-to extension (£24.95 each); a sectional lineside hut with either red or blue door (£9.95); and a model of a station yard crane from Highley on the Severn Valley Railway (£27.95).

Finally, for modellers in O-16.5/NG7, there is a corrugated iron locomotive shed priced at £89.95, and a green narrow gauge metal water tank on a cross-frame base, retailing at £49.95 – though the latter might also suit modellers of standard gauge light railways in 7mm scale, were the frame to be mounted on a platform.

www.bachmann.co.uk

NG7 corrugated iron engine shed.



GWR station building.

NG7 water tank.

In brief

25 layouts for Folkestone show

On Saturday 4 & Sunday 5 October, the Folkestone Hythe & District Model Railway Club will hold an exhibition of over 25 layouts at Leas Cliff Hall, Folkestone CT20 2DZ.

The exhibition will be open from 10:00-17:00 on both days and the price of admission is adults £8.00, children aged 5-15 £4.00 and family tickets £22.00. The venue is near bus routes, Folkestone railway station and the M20, with the main access to the exhibition being via Sandgate Road car park.

For those with limited mobility, parking on site is limited, including in the disabled driver bays, but there is lift access to most areas and wheelchair users gain access via the lift adjacent to the top floor Leas Shelter coffee shop. Refreshments are available at the venue's bars and Channel Suite bar and café. For further details visit: **www.folkestone-mrc.com**

New premises for Bosvenegh Model Railway Group

Following 12 months spent refurbishing and automating the historic model layout at the Bodmin Railway (formerly the Bodmin & Wenford Railway) in Cornwall, the members of the Bosvenegh Model Railway Group (BMRG) have announced that the group has now relocated to new premises near St Austell.

Hot on the heels of that good news, the group has recently received an award of over £3,000 in grants to help it to fund a new layout.

The new premises will not only allow the group to press on with plans for a new layout but will also put it in a position to recruit new members.

BMRG meets every Friday evening at 19:00, with a warm welcome awaiting both new recruits to the hobby and those with more experience. The members say it is also a perfect time to get in on the 'ground floor' to create a new layout from scratch. For all those interested in joining the group, call Steve Willis on 01579 362505 or email him at wilnatron53@gmail.com to receive further information.

Birmingham show

On Saturday 25 October, Birmingham Model Railway Club will be holding its annual exhibition at Arden Hall, Water Orton Road, Castle Bromwich, Birmingham B36 9PB. Open from 10:00-16:00, entry will cost £6.00 for adults (one child can enter free with each adult). There will be 18 layouts and around 11 trade stands. Refreshments will also be available. For further details, telephone the organiser on 07766 923361.

The latest news from the PECO group of companies

Peco reveals Ffestiniog Railway 009 England engine train packs

FR *Palmerston* WHR Press Train Pack.

Following on from the recent success of its Lynton & Barnstaple Railway-themed 009 train packs, featuring the 'what if' Southern Railway-liveried Peco/Kato model of a George England 0-4-0ST+T named *Exmoor Pony* with either carriages or wagons in livery variations exclusive to these packs, Peco has announced it will also be producing two England engine train packs in late 19th-early 20th century Ffestiniog and Welsh Highland Railways liveries.

One of the packs reflects the fact that, following the original opening of the Welsh Highland Railway through to Porthmadog in 1923, the elderly FR Small and Large England locomotives, as well as FR carriages such as the 1870s 'Bowsiders', were regularly pressed into service on the northern line to supplement the WHR fleet.

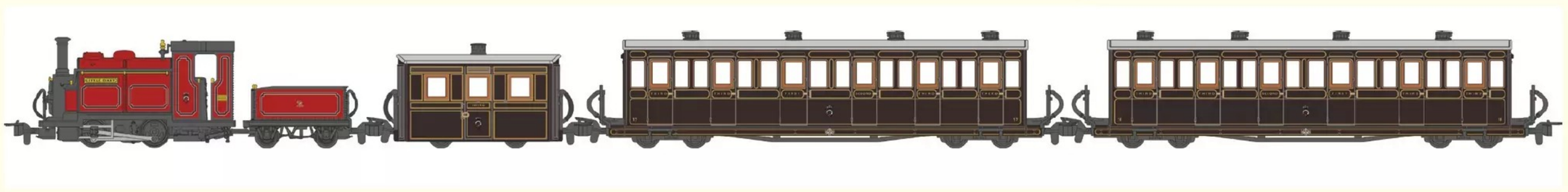
The first pack (ref.GR1005) is the 'FR *Palmerston* WHR Press Train Pack' and comprises the Peco/Kato model of the 1864-vintage Small England loco *Palmerston* in lined maroon (the prototype was actually used by the contractors constructing the WHR, and survives in service today) together with long 'Bowsider' carriage No.19 and short 'Bowsider' No.18. Both carriages are in an Edwardian red livery exclusive to this train pack. The RRP of this complete pack is £245.00.

The second new offering (ref.GR1006) is the 'FR *Little Giant* Letter Service Train Pack', reflecting the fact that the FR held authorisation to run a letter service from 1895, and comprises a Peco/Kato model of the 1867-vintage Large England locomotive *Little Giant* (a prototype withdrawn in 1924) together with FR Third class 'Bug Box' four-wheel coach No.4 plus two short 'Bowsider' carriages, Nos.17 & 18. All the carriages are in Victorian all-over purple-brown livery lined in gold, and the RRP of this pack is £285.00. It is envisaged that these train packs will appear in attractive FR-themed presentation packaging in a similar style to the L&BR sets.

The latter, though selling fast, may still be available from some retailers. The ref.GR1003 L&BR 009 Passenger Train Pack (with SR green-liveried Large England *Exmoor Pony* plus two L&BR bogie carriages in an exclusive SR 'sunshine' green livery), and ref.GR1004 L&BR Goods Train Pack (featuring *Exmoor Pony* with goods wagons and a brake van in SR liveries exclusive to this pack) have an RRP of £245.00.

www.peco-uk.com

FR *Little Giant* Letter Service Train Pack.



Ffestiniog Railway shop commissions exclusive bogie wagon from Peco

The Ffestiniog & Welsh Highland Railways shop at Porthmadog Harbour Station in Gwynedd, North Wales, has announced it has commissioned from Peco an exclusive 009 bogie wagon.

The model has one of the manufacturer's Lynton & Barnstaple Railway wagons as its basis, but incorporates details based closely on a historic vehicle that ran on the FR itself from the railway's Victorian heyday until just after the First World War.

Two large, high-sided bogie waggons (as spelt in FR heritage parlance), Nos.88 and 105, were built at the FR Boston Lodge Works in 1874 as 12-ton ballast wagons but were later used for coal. However, one at least bore the side markings 'Permanent Way Ballast Truck', and it is in this form, and in oxide red livery, that the exclusive model (ref.GRC019) appears as FR Bogie Ballast Truck No.88.

Although both prototype wagons were dismantled around 1918, an 1887 photograph by Bleasdale of No.105 shows markings with a tare weight of '4-5-2' and a capacity legend 'To carry 12 tons' that have been incorporated in the model's livery.

The F&WHR advises that the model will be on sale in time for the railways' Bygones Weekend on 3-5 October.

It is anticipated that the model will have a price of £39.95. Enquiries for further details of post and packing rates, etc., can be made by phone or in writing:

F&WHR, The Gift Shop, Harbour Station,
Porthmadog, Gwynedd LL49 9NF
Tel: 01766 516 034
www.festrail.co.uk/shop/

Final call for entries to Peco RAIL200 Model Layout Competition

The closing date for the Peco RAIL200 Model Layout Competition is fast approaching, with 30 September being the date by which entrants need to submit photos of their finished models.

Then, during early October, the models will be evaluated by a panel of judges, from which a list of finalists will be drawn, with the judges looking for models that demonstrate creativity and ingenuity.

For each category ('Modular' and 'Standalone'), a winner will be declared for dioramas by entrants aged under 16, and for those aged 16 and over – so four winners in all. These winners will be announced in the December issue of RAILWAY MODELLER, on sale from 13 November.

The four winners will each receive a £500 voucher (to be redeemed at approved Peco stockists); a 12-month subscription to RAILWAY MODELLER with full access to the RM Digital Archive; tickets to Pecorama; and a special commemorative trophy. There will also be four runner-up prizes, each comprising a £100 voucher (redeemable at approved Peco stockists); a 12-month subscription to RAILWAY MODELLER with full access to the RM Digital Archive; tickets to Pecorama; and a special commemorative certificate.

Arrangements will be made for up to 50 selected finalists (including the winning entries) to be displayed at the National Festival of Railway Modelling, which will be taking place at the NEC in Birmingham, on the weekend of 22 & 23 November. (One complimentary ticket to the event will be given to the entrant of each of the selected finalists.)

PECO RAIL200 MODEL LAYOUT COMPETITION

It is planned that the 'Modular' finalists will be joined together for the event, with a special train running from one end to the other to provide a fitting finale to the contest. For further details about the competition, go to:

www.peco-uk.com

Ellis Clark announces Lancashire & Yorkshire ‘Pug’ models in OO and O

Ellis Clark Trains has announced that the next addition to its Clark Railworks range will be models of the Lancashire & Yorkshire Railway Class 21 ‘Pug’ 0-4-0ST in both OO and O.

Both the OO and O gauge model versions will feature mostly die-cast construction, providing them with useful weight to assist with smooth running. The cabs and their backheads will be fully detailed and the O gauge models will also feature fully working inside motion. In addition, a coreless motor will be married to a gearbox designed to assist slow running when shunting, and for the benefit of P4 and EM finescale modellers, there will be sufficient space in the chassis design for regauging.

Digital Command Control sound-fitted models will feature on-board stay-alives, a bespoke sound file and pre-fitted speakers, plus models in both 4mm and 7mm scales will have controllable firebox lighting, with those in the larger scale also incorporating an atmospheric ashpan glow. Other details in 7mm scale include an opening smokebox door with full interior detail, tank and cab variations, removable cab roof and slidebar covers, sprung buffers where appropriate, etched plates, separately fitted etched details and an additional full detailing pack.

In 4mm scale, details will include sprung buffers where appropriate, a semi-open die-cast chassis, tank and cab variations, detailed backhead and cab, removable NEM coupling and pocket, etched plates, separately fitted etched details and a full detailing pack.

There will be 11 livery variations, beginning with No.155 in L&YR black lined in red; then No.11247 in London Midland & Scottish Railway unlined black with large number painted in straw-coloured numerals on the saddle tank sides and with red ‘LMS’ panels on the cab sides; and next the former LMS No.11224 in the blue livery it carried after it was sold to ICI in 1934 for use at West Bank Power Station, Widnes, where it carried the name *Weston*.

A Computer Aided Design render showing the O and OO gauge versions.

Next there are three livery versions charting the career of the former L&YR No.19 after it was sold into private ownership by the LMS in 1931: it first appears in green livery as *Bassett*, the name under which it worked for construction company John Mowlem & Co. on a project to extend Southampton Docks; next in lined red livery as *Prince,* the name it carried while operating for United Glass Bottle Manufacturers (South London); and finally with its original identity of No.19 restored, when preserved at the Ribble Steam Railway in Preston in L&YR plain black.

There are then four models in variations of British Railways black with smokebox numberplates: No.51234 with early emblem on its tank sides; No.51241 with no emblem or crest; and Nos.51218 & 51229 with late crest on the tank sides. In the case of No.51218, this is the form in which the prototype is currently preserved on the Keighley & Worth Valley Railway (KWVR), where it was the heritage line’s first locomotive. It also took part in the 1975 cavalcade at Shildon, County Durham, marking the 150th anniversary of the opening of the Stockton & Darlington Railway.

There is also a model of No.51218 in early 2005-15 KWVR preservation guise as No.68 in L&Y black, lined in red.

DCC ready versions in OO will be priced £150.00 and those in O gauge £325.00. The manufacturer states that prices have still to be finalised for the DCC sound-fitted versions in both gauges, but adds that delivery of the O gauge models is expected in summer 2026, while the 4mm scale models are expected in late 2026 or early 2027.

www.ellisclarktrains.co.uk

Revolution reveals samples of its first models in TT:120

Revolution Trains has received engineering prototype samples of its first models in TT:120, portraying the MMA and JNA box wagons now seen widely across the British network, with around 1,200 examples currently in service.

The versions received are the 11-rib body with no side doors and body-mounted parking brake wheels (to be produced in Wascosa/Network Rail yellow, as well as Touax maroon with yellow lettering offered exclusively by The Model Centre); the 11-rib version with side doors and bogie-mounted parking brake wheels (to be finished in GB Railfreight steel blue); and the nine-rib version with no side doors and body-mounted brake wheels (to be finished in Ermewa/Tarmac grey).

In addition, there are to be nine-rib versions with side doors in DB red, and 11-rib versions with no side doors and bogie-mounted brake wheels in Mendip Rail silver. All models will be supplied with removable loads and each livery will be available with five different running numbers. The wagons have an RRP of £44.95 each and delivery is expected in spring 2026.

Tooling samples of the box wagons in TT:120.

Tooling samples of the K-type Pullmans in N.

K-type Pullman samples in N

Revolution Trains has also received first samples of its forthcoming range of five different 1928-built K-type steel-bodied Pullman cars in N gauge. The initial images show the 1928 version First and Third class parlour cars, and a refurbished First class kitchen, as operated by VSOE for a number of years and now owned by Belmond British Pullman.

Revolution will also be offering Kitchen Third and Parlour Brake Third cars to allow prototypical rakes to be assembled.

The models will feature detailed interiors with working table lamps and saloon lights, and NEM couplers in kinematic close-coupling pockets. Bogie details from the prototypes will be replicated: for example, the original cars ride on standard Pullman bogies, while the refurbished car is fitted prototypically with Gresley heavyweight bogies and an altered underframe including reinforcing trusses.

The models are available to pre-order in two-, three- and four-car packs. Roof colours, crests and other livery details will reflect the intended era of each pack, with full details (including prices) to be found on the Revolution website. Delivery of the models is expected in spring 2026.

For further details go to:

www.revolutiontrains.com

www.themodelcentre.com

EXHIBITION FOCUS

Uckfield MRC gears up for 40th show

The P4 layout *Flintfield* (see p820). *Photo: Gerard Tombroek*

This year's annual Uckfield Model Railway Exhibition will take place on Saturday 18 & Sunday 19 October, at the Uckfield Civic Centre, Civic Approach, Uckfield, East Sussex TN22 1AE, and will be open from 10:00–17:00 on both days.

This year's event is the club's 40th Model Railway Exhibition and will see an eclectic mix of 20 layouts in no fewer than 11 different scale and gauge combinations. The show will also incorporate Scalefour Southeast, a Scalefour Society regional event, featuring the society publicity stand and the Scalefour Stores, along with associated P4 layouts and demonstration stands. Also the 2mm Scale Association Roadshow will be making an appearance with several 2mm finescale layouts and the Association information stand.

Celebrating the 40th exhibition milestone, the layout list includes some circuit favourites along with several debut appearances. With locomotive control recently changed to DCC with sound by new owner Adrian Colenutt, Vincent de Bode's *Flintfield* (featured on p820-826 of this RM issue) portrays a Great Eastern Railway terminus in about 1910.

Having been refurbished and extended, and now in the ownership of Rex Davidson and Stephen Williams, Gordon Gravett's 7mm scale layout *Ditchling Green* (featured in RM March 1992 and representing a small Sussex BR Southern Region station) will be making an appearance. Meanwhile, Ed Purcell will be bringing along his 1:148 scale layout *Todmorden Midland* (RM October 2016), and William Lloyd will be making his exhibition debut with the atmospheric *Rye Sands* in OO, portraying a South Eastern & Chatham Railway outpost on the south-east coast. Another layout making its debut will be *Roman Road*, again in OO, constructed by Oly Turner and featuring BR (SE) overhead line equipment. Other layouts will be in 0-16.5, S, P4, EM, OO, 009, HO, 3mm finescale and 2mm finescale.

The show will also feature 22 trade stands, including some from specialist manufacturers. In addition, there will be demonstration stands helping everyone from beginners to more experienced modellers to hone their skills. For details see the Societies & Clubs pages in this issue, contact exhibition manager Adrian Colenutt on 01903 745759 or visit:
www.uckfieldmrc.co.uk

Warley at Statfold to herald opening of permanent mezzanine display

Warley Model Railway Club has again joined forces with the Statfold Narrow Gauge Museum Trust to stage its 2025 exhibition at the Statfold Country Park, near Tamworth B79 0BU, over the weekend of 11 & 12 October.

Tagged as 'the model railway show with a difference', the event will host 33 layouts spread across three halls, including several which have featured in RAILWAY MODELLER, including *82G* in 0 (see p772) and *Tan y Bwlch* in 009 (see p812). For the full layout list see the Societies & Clubs pages.

There will also be over 40 mainstream and specialist trade exhibitors, plus a 'tech zone', and in the spirit of the former Warley National Shows at the Birmingham NEC, the Club has arranged for a centrepiece full-size locomotive to be on display between the three halls – this year it is a vintage Hunslet standard gauge 0-4-0ST *Hodbarrow*.

Added to this is the fact that Statfold Country Park is also home to one of the world's largest museums dedicated to narrow gauge railways, and has two operational live steam narrow gauge lines which operate around the site, one 2' gauge and the other 12¼". This year the attraction has also opened The National Fairground Museum, featuring classic fairground rides (for which there is an additional charge for rides), and stalls have been established on the site.

Furthermore, a new permanent model railway interactive display is in the process of being installed in conjunction with Warley Model Railway Club, on a newly created mezzanine area within the Statfold museum, and its Grand Opening will take place on the Saturday morning of the show at 11:00.

The show will be open from 10:00-17:00 on both days and pre-booked ticket prices are adults £17.50, children/seniors £15.00, carers £10.00 and family £57.50. At the gate prices, meanwhile, are adults £19.50, children/seniors £17.00, carers (pre-notified) £12.00 and family £65.50. There is free car parking and a free shuttle bus will operate from Tamworth rail station to Statfold Country Park from 09:30-16:30.

For further details see the Societies & Clubs pages in this issue or visit:
www.thewarleyshow.co.uk
www.statfold.com

Northbridge* in 00. *Photo: Steve Flint

Modern Image O Gauge Group Show in Crewe to aid Prostate Cancer UK

The Modern Image O Gauge Group (MIOG) will once again be holding its annual exhibition at Crewe Heritage Centre, Vernon Way, Crewe, Cheshire CW1 2DB, on Saturday 25 & Sunday 26 October, with the event open from 10:00-16:00 on the Saturday and 10:00-15:30 on the Sunday.

The cost of admission is £10.00 for adults and under-16s who are accompanied by an adult can enter free of charge, while there will be a charge of £5.00 for any young person aged 12-15 entering unaccompanied. Ticket prices include entry to all the Crewe Heritage Centre attractions, and all profits will go to the charity Prostate Cancer UK in memory of MIOG member Colin Burgess, who passed away in June 2024.

Eight O gauge layouts are confirmed for the event, whilst manufacturers Ellis Clark Trains, Heljan and Footplate Models will again be in attendance, along with around 20 traders and a number of railway societies. It is hoped that main line operator Locomotive Services Ltd may be able to arrange for a full-size loco to be on display, subject to availability.

With only limited disabled parking on the site (although pay & display car parks are available nearby) a free shuttle bus service will be operated from Crewe station. For further details visit:
www.modernimageogauge.co.uk/exhibition-2025.html

Hardwick Bridge* – one of eight O gauge layouts attending. *Photo: Chris Warner

O Gauge Get Together set to take place at Highley Engine House

The Severn Valley Railway will host its annual O Gauge Get Together, established four years ago by some of its volunteers, over the weekend of Saturday 18 & Sunday 19 October, the venue being The Engine House at Highley station.

The event will be open on Saturday from 10:45-16:45 and on Sunday from 10:45-16:00, and admission will be included in the price of either Freedom of the Line or short journey tickets (see SVR website for details). Layouts will include: *Brixham, Brook Street, Coldharbour Lane, Exeter St George, Halesfield TMD, Netherwood Sidings, Norman Colliery, Sherton Abbas* and *Worcester South Quay*.

There will be trade support from Buzz Models, Connoisseur Models, Dapol, Ellis Clark Trains, Finney7, Joe Lock Model Railways, Moore View Models, Ragstone Models & Yellow Shed Designs and displays provided by Bridgnorth Station SVR, Gauge O Guild, Model Electronics Railway Group and several loco support groups.

In addition, there will once again be a limited edition wagon commissioned from Dapol and based on a local prototype. This year it is a five-plank wagon bearing the branding 'Pearson & Co of Kidderminster', a coal and builders' merchants once based on the site of today's SVR Kidderminster station. The model will be available solely at the show (at the time of going to press the price had yet to be finalised) and in a limited run of 50.

As there is very limited parking at Highley station, arriving by SVR service train is recommended, with entry to The Engine House included as part of the train fare. All trains feature wheelchair accommodation and The Engine House is fully accessible. Catering facilities are available at the venue. For details visit the Severn Valley Railway website:
www.svr.co.uk/event/o-gauge-get-together/
www.facebook.com/SVRGaugeOshow/

Draft artwork for the limited edition wagon.

Sherton Abbas* by Dave Stone. *Photo: Craig Tiley

More Accurascale Class 50s

Accurascale has revealed the livery line-up for its new production run of BR Class 50 locomotives in OO (see review in RM August issue). In addition to an Accurascale Exclusives release featuring 50 007 *Hercules* (which carried the number and name 50 034 *Furious* on one side) and 50 049 *Defiance*, both in GBRf livery, there will be four locomotives covering the British Rail era.

This includes 50 029 *Renown* in the revised Network SouthEast livery, retaining the so-called 'toothpaste' red, white & blue guise but with the BR double arrow replaced by West of England route branding, and 50 019 *Ramillies* in the distinctive 'Laira Blue' livery worn by some class members towards the end of their careers. This livery comprised Rail grey roof and black window surrounds in addition to BR all-over blue, plus NSE-sized numbers were applied to the driver's cabside, with double arrow logos at the other end, and the nameplates were retained in a high position.

Appearing in BR standard blue will be 50 035 *Ark Royal*, one of just three Class 50s to gain plated headcode panels with sealed beam marker lights in the late 1970s. In January 1978, this locomotive became the first of the newly named 'Warship' class to receive nameplates, and the double arrow logos were relocated to accommodate the large ship's crests above the nameplates.

The final livery version is 50 046 *Ajax* in BR large logo blue livery with yellow cab sides and ends and grey roof. Although this loco was scrapped in 1992 its cabs survive, one with an owner in Cornwall and the other at the Plym Valley Railway in Devon.

Accurascale states that after feedback from customers, it has revised its packaging for the latest Class 50s and has attended to issues with self-coloured plastic parts. Prices are £189.99 for DCC ready models and £299.99 for DCC sound-fitted versions.

BR large logo blue and GBRf livery samples for the next run of Accurascale Class 50 models in OO.

DB Cargo names Class 66 loco after Accurascale

Rail freight operator DB Cargo UK used its Family Day, held on July 19 at the Midland Railway Centre at Butterley, Derbyshire, to name Class 66 locomotive 66 091 *Accurascale* in honour of the model railway manufacturer.

The two organisations raised £10,000 for Martin House Children's Hospice after Accurascale released a limited-edition model of the DB Cargo locomotive 66 190 bearing the charity's name.

Participating at the ceremony were DB Chief Transformation and Digitalisation Officer Mike Gray and celebrity music guru and rail enthusiast Pete Waterman, who was representing Accurascale as his Making Tracks model railway events have been used to launch new models.

Accurascale has confirmed that a model of 66 091, bearing its own name, will head up its next run of Class 66 models, which are due to be released in the autumn. For more details, see the manufacturer's website:
www.accurascale.com

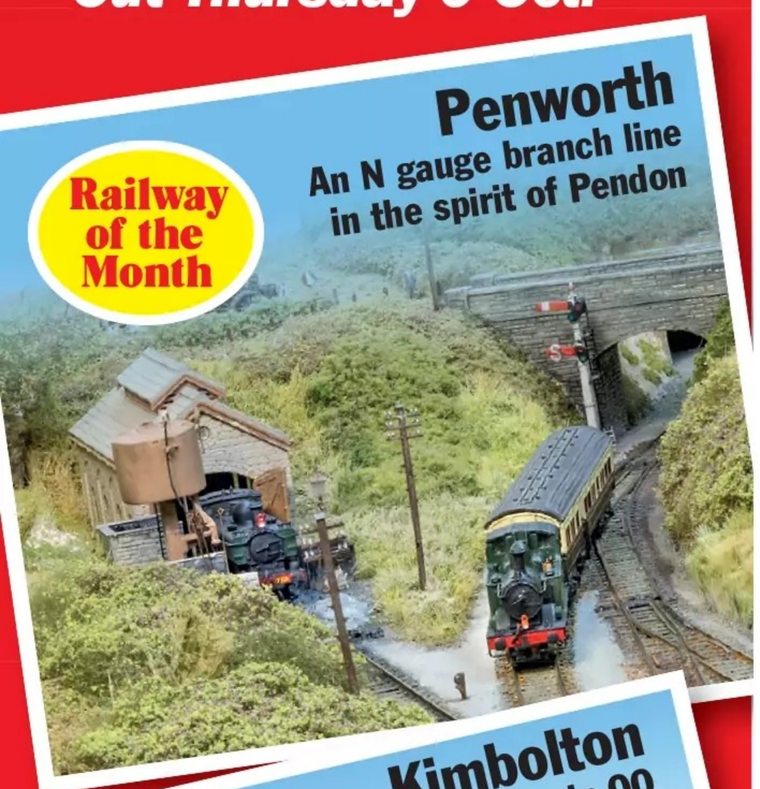
Accurascale and DB representatives, together with Pete Waterman (fourth from right) after the naming.

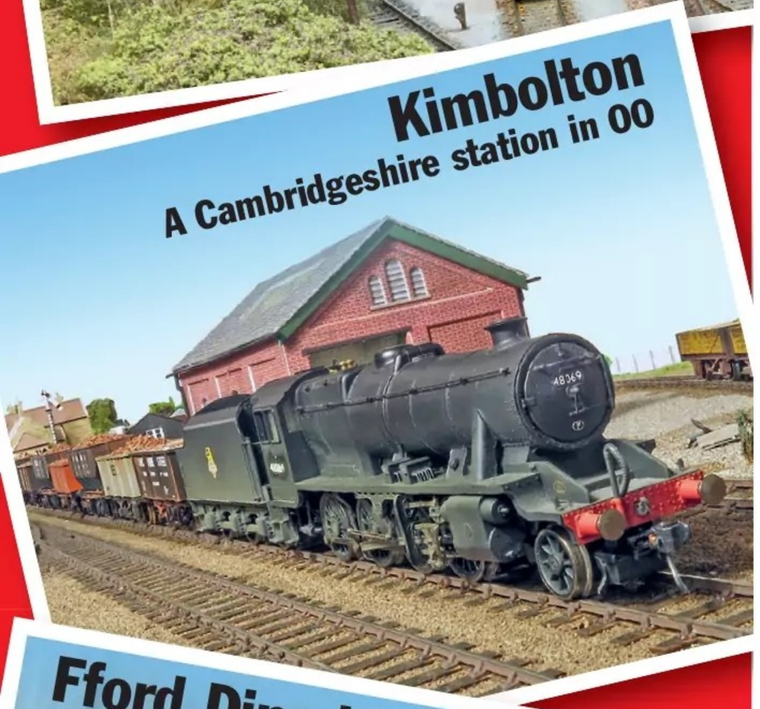
A sample of newly named 66 091 *Accurascale* in model form, which will be released in the autumn.

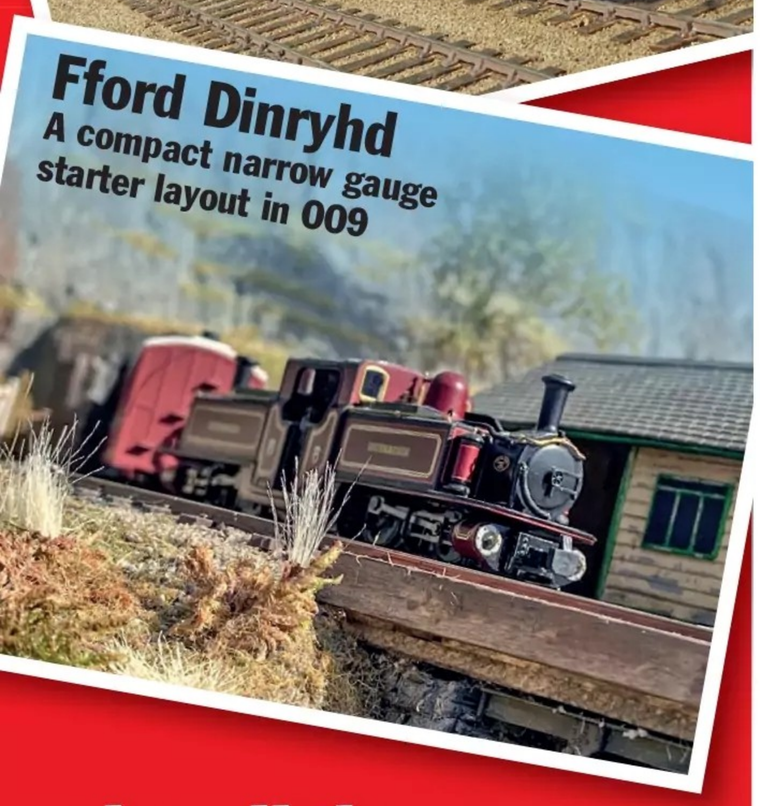
Coming next month

November 2025

Out Thursday 9 Oct.







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Use the coupon printed on p36a of this issue to save 50p on the cover price of the November 2025 edition of RAILWAY MODELLER, when purchased from a Peco stockist.

Barrihandie scoops two awards at Perth show

Barrihandie, the Scottish OO gauge layout built by members of the Wirral & North Wales Model Railway Group, picked up two awards following its appearance at the Perth Model Railway Exhibition on 28 & 29 June 2025. The first was 'The Harburn Hobbies Shield' for best in show, as voted by the public, with the second being 'The Ten Commandments Trophy' for architectural modelling.

Barrihandie was featured as Railway of the Month in the June RM, with a follow-up article on the harbour in the July issue.

Stan Moug (Perth & District Model Railway Group Exhibition Coordinator) presents 'The Harburn Hobbies Shield' to Alisdair Macdonald (left) with Ray Reed alongside. *Both photos: Perth & District MRG*

Alisdair Macdonald received 'The Ten Commandments Trophy' from the firm's proprietor, Dave Young. Also pictured (from left to right) are W&NWMRG members Graham Heald, Ray Reed, John Brayford and Clive Gardner.

Whitstable club unveils latest limited edition wagon model in OO ahead of October show

Whitstable-based East Kent Model Railway Society has unveiled its latest limited edition model wagon, this time bearing the name of a Herne Bay coal merchant.

Dapol has produced 120 OO gauge examples of the seven-plank wagon bearing the black & white livery, dating from the mid-1920s, of Thomas Searle King, which began dealing in coal in 1915 from premises in Herne Bay High Street. The firm still trades today from a coal order office in nearby Richmond Street. Each model will come in a Dapol box with an individually numbered certificate.

Wagons can be purchased either by sending a cheque, made out to EKMRS Whitstable, to Chairman Nick Evans at 27 Douglas Avenue, Whitstable, Kent CT5 1RT, or by modellers emailing their email and phone details to ekmrswhitstable@gmail.com so that details can be sent back regarding making an electronic payment. The price per wagon is £23.00, which includes post and packing.

Two wagons may be purchased for £45.00, also inc. p&p. EKMRS will be holding its next exhibition in Whitstable on Saturday 25 October, with more dates being planned for 2026, details of which will be released in due course.

David Philip Ellis, 25 May 1944 –13 July 2025

The team at RAILWAY MODELLER was sorry to receive news that David Ellis, well-known in the hobby from his South Eastern Finecast business, passed away on 13 July. Jerry Colebrooke, who worked with him for 15 years, has written the following obituary:

Dave was born in Crawley in 1944. He took an interest in trains and aeroplanes from an early age as the Brighton main line and fledgling Gatwick Airport were close to his family home. The love of planes led to a job working for an air freight carrier as an engineer on the planes they used, and later he gained a pilot's licence and a part share in a light aircraft.

Dave enjoyed modelmaking, both aircraft and railway modelling, and he helped form a radio-controlled flying club in East Grinstead.

Changes in employment led him to be a sales rep, first for Eveready Battery Company, then for a toy model kit importer. Then, deciding that he wanted to work for himself, he opened a model shop in East Grinstead, South Eastern Models, in 1978 and it specialised in supplying model railway kits and accessories.

Always looking to do something to improve the turnover of the shop, Dave then came up with the idea of Flushglaze. These window packets became best sellers, with thousands sold over the years, something which was always a great surprise to Dave. The Builder plastic sheets and turntable kits then followed, again with strong sales.

The plastic products were never big enough to be a full-time business in their own right, but it was proving difficult to manufacture them and run the shop as well. The Wills Finecast whitemetal range became available and was subsequently bought, and then the shop was sold off, with Dave then creating South Eastern Finecast and moving into industrial premises in Forest Row, East Sussex. He subsequently added many new kits to the range, both cars and railway items, and he also upgraded the existing locomotive kits.

Dave and Brian Osborne (of Branchlines) then went into partnership as Nu-Cast Partners and bought the K's Nu-Cast range of loco kits.

However, around this time ill health meant that Dave sold off the South Eastern Finecast business and concentrated on reintroducing the Nu-Cast range, including the final re-issue of his favourite loco, the London Brighton & South Coast Railway K Class, this being because he had seen the prototypes passing his house as a child.

Dave finally retired due to ill health in 2024.

He is survived by his two children, Elizabeth and Christopher, three grandchildren and two great-grandchildren, all of whom he adored.

Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling to a listed live event should contact the organiser first. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and notifications sent via email to **clubs@railwaymodeller.co.uk** are required six weeks prior to publication date. Entries must include the organiser's email or website and/or a telephone number.

DECEMBER 2025 issue deadline: OCTOBER 1st

LIVE EVENTS

Saturday 13 September
BEER, Devon
Event: Lynton & Barnstaple 009 Exhibition.
Organiser: Pecorama.
Venue: Gallery Lecture Theatre, Pecorama, Underleys, Beer, Devon EX12 3NA.
Open: 1000-1600
Admission: incl. in Pecorama special 50th anniversary entry prices: adults £7.25, concessions (ages 3-17 & 65+) £6.25, family (x4) £24.95, family (x5) £29.95, under 2s free, dogs £1.00. Quote RM50 for discount. Extra Beer Heights Light Railway train rides £4.00.
Amenities: parking, refreshments, disabled access (hilly, unsuitable for 3-wheel scooters).
Features: marking 90 years since closure of the 1' 11½" gauge Lynton & Barnstaple Railway, the Dartmoor 009 Railway Group will be operating layouts depicting stations on the line. Tickets also include main Pecorama model exhibition, a ride on the Beer Heights Light Railway – with special Diesel Day and home engines *Jimmy* and *Ben* joined by visiting diesels – plus gardens.
Website: www.pecorama.co.uk

Saturday 13 September
RAILEX EAST
CAMBRIDGE, Cambridgeshire
Organiser: 31A Cambridge Model Railway Club.
Venue: Cambridge Regional College, Kings Hedges Road, Cambridge CB4 2QT.
Open: 1000-1600 (last admission 1530)
Admission: adults £7.00, acpd children £3.00 (all children must be acpd and where 2 or more children with at least 1 adult, 1 child enters free).
Amenities: large free parking area. Full access for disabled visitors. Refreshments.
Features: layouts incl. *Fen End Pit* (16mm scale); *Dunwich Tramway, Hobbs Warren* (O); *Appleton Dene* (O-16.5); *Whitley* (EM); *Helland Wharf, Morfrail, Providence Colliery, Rails Through Histon* (OO); *Tidley* (009); *Donnersbachkogel* (HO/HOe Austrian). Trade and society stands.
Email: Neil Chapman, club secretary, via secretary@cambridge31a-mrc.org.uk
Website: www.cambridge31a-mrc.org.uk

Saturday 13 September
HEYWOOD, Lancashire
Organiser: Heywood Model Railway Group.
Venue: Unit 4, River Street, Heywood, Lancashire OL10 4AB.
Open: 1000-1600
Admission: £3.00 per adult, acpd children free. Donations welcome.
Amenities: free parking. Wheelchair access. Refreshments.
Features: open day for club test tracks in N, 009, OO and O gauge. Run own locos. Shop.
Contact: Phil Evans by emailing: phil_evans@sky.com
Website: https://www.hmrg.net/

Saturday 13 September
OLNEY, Milton Keynes
Organiser: Olney Model Railway Club.
Venue: The Olney Centre, High Street, Olney MK46 4EF.
Open: 1030-1600
Admission: adults £5.00, acpd U15s free.
Amenities: free parking in easy walking distance. Disabled access. Refreshments.
Features: new venue and layouts incl. *Billingham Street, Cwm Bach* (O); *Tonys Forest* (O-14); *Victoria* (EM); *Granddad's Train Set* (Hornby Dublo 3-rail); *Bucklebury Lane, Daisyfield Yard, Kyle of Tongue, Southdale* (OO); *Quarry End* (009); *Sorrento Park* (TT:120 USA); *Liddle Town, Three Bridges* (N). Trade stands.
Contact: Malcolm Haynes, 07752 683619.
Email: malcolm@olneymrc.org.uk
Website: www.olneymrc.org.uk/2025-exhibition

Saturday 13 September
SOUTH CROYDON, South London
Organiser: Christopher Dean & 7th/12th Croydon Scout Group.
Venue: St Paul's United Reformed Church, Croham Park Avenue, South Croydon CR2 7HF (opposite Old Whitgiftian Rugby Ground).
Open: 1000-1600
Admission: adults £6.00, children £3.00, families (2+2) £15.00. All proceeds to be divided between local charities.
Amenities: street parking. Bus & Tramlink services nearby. Disabled access. Hot & cold food.
Features: layouts in O, On30, EM, OO, HO & N plus second-hand stall and trade support.
Contact: 07956 520959.
Email: tinkerscorner@aol.com
Website: www.stpaulsurc.org.uk
Facebook: www.facebook.com/p/Croydon-Model-Railway-Society-100084407896822/

Saturday 13 September
STOCKPORT, Greater Manchester
Organiser: Romiley Methodist Railway Modellers.
Venue: Romiley Methodist Church, 77 Stockport Road, Romiley, Stockport SK6 3AA.
Open: 1030-1630
Admission: by donation for Action for Children.
Amenities: parking for those with limited mobility only. Rail station & bus stops 400m away.
Features: several hands-on/drive-it-yourself layouts, alongside traditional layouts & trader.
Contact: Mark Howarth, Exhib Secretary by email.
Email: pickupgoods@hotmail.com
Website: www.rmrmclub.com

Saturday 13 & Sunday 14 September
FAVERSHAM, Kent
Organiser: Faversham Model Railway Club sponsored by Doug's Model Trains & Tools.
Venue: The Abbey School, London Road, Faversham, Kent ME13 8RZ.
Open: Saturday 1000-1630
Sunday 1000-1600
Admission: adults £7.00, children (5-16) £3.00, U5s free (cash preferred).
Amenities: free on-site parking. Nearest rail station is Faversham – walk under subway and continue for 10 mins or take taxi. Refreshments.
Features: layouts incl. *Stodmarsh, Wapping Wharf* (O); *Batou Scie.* (On30); *Essex Brick, Millfield, Shunting Puzzle, Smallbox Colliery, Swaleford, The Ladykillers, Thomas Themed* (OO); *Halden Lane, Scallywag Corner, That Dam Railway* (009); *Stevens Point Service* (HO); *Wilmington* (TT); *Burnham-on-Sea, Flatlands, Headcorn Halt, Holt Street, Melton Mowbray, Oaklands, Shillingstone, Staffordshire Potteries* (N). Trade stands.
Website: www.favershammrc.org.uk/2025.html
Facebook: www.facebook.com/FavershamModelRailwayClub/

Saturday 13 & Sunday 14 September
INTERNATIONAL N GAUGE SHOW
LEAMINGTON SPA, Warwickshire
Organiser: Meridienne Exhibitions Ltd.
Venue: Warwickshire Event Centre, Nr Leamington Spa – on the junction of the A425/B4455, visitor car park CV31 1FE.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £14.50, senior citizens (65-plus) £13.00, children aged 5-14 inclusive £6.00. Tickets (valid for one day) are available via website www.ngaugeshow.co.uk – tickets on the day from the ticket office.
Amenities: visitor entrance off A425 Southam Road. Follow signs to 'Visitor Entrance' and use postcode CV31 1FE. (The exhibitor entry is off the B4455 Fosse Way.) Free visitor car park plus area for Blue Badge holders. Saturday only Exhibition Link Bus runs every 30 mins from Leamington Spa rail station – see website. Disabled facilities. Restaurant & coffee shop.
Features: N gauge layouts incl. *BNSF Metra Chicagoland, Braemuir Harbour, Brocolitia, Castle Rise, Chaddesley Corbett, Chrilvinton Town, Denby Road, Donge Farm, East Anstey, Edrom, El Cremallera, Fontaine Quarry, Haversham Central, Hawes Junction, Heathbury, Low Fell, Maun Valley Junction, Mereley, Muhlenfelder, Nazareth Portland, Rothley, Sellraintal, Singen, Two Seasons, Udford Wood Halt, Vintage N Gauge, Westwick*. Trade & society stands.
Contact: 01926 614101.
Email: info@meridienneexhibitions.co.uk
Website: www.meridienneexhibitions.co.uk/events/the-international-n-gauge-show/clubs-and-layouts
Facebook: www.facebook.com/MeridienneEx/

Saturday 13 & Sunday 14 September
LEIGH FESTIVAL OF MODEL RAILWAYS & TRANSPORT MODELLING 2025
LEIGH, Greater Manchester
Organiser: Leigh Model Railway Society.
Venue: Leigh Sports Village, Leigh Leisure Centre, Sale Way, Leigh, Greater Manchester WN7 4JY.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £8.00, U15s have free entry with every paying adult.
Amenities: free parking. Free vintage bus service from Leigh bus station to venue and back. Disabled access. Café and raffle.
Features: 30-plus layouts incl. *The Big Thomas One, Libomy Alp* (G); *Newchapel Junction, Dalry Road* (O); *Bryncrug Sidings* (O-16.5/NG7); *Ashdan Junction, Bodmin Generally, Bridge Road, Bridgewater, Bucks Lane, Georgemas Junction, Goodley Lane 2, Kings Road TMD, Kids Drive It* (OO); *Fryup, Fallgate, Mynydd Tatws Newydd* (009); *Bryford County Terminal USA, Bad Kissen* (HO); *Auswiss* (HO & HOm); TT:120 layout to be announced; *Nine Mills, Enngage Junction, Llwyn Grug* (N); plus planes, cars, trams, boats, Scalextric & Lego, traders.
Email: lmrs.secretary@gmail.com
Website: www.leighmodelrailwaysociety.wordpress.com/exhibition-2023-2/
Facebook: www.facebook.com/LeighMRS/

Saturday 13 & Sunday 14 September
NEWTON AYCLIFFE, County Durham
Organiser: Darlington MRC in association with Railway 200, S&DR 200 & Accurascale.
Venue: Oakleaf Sports Complex, School Aycliffe Ln, Newton Aycliffe DL5 6QZ.
Open: both days 1000-1600
Admission: adults £8.00, U16s free.
Amenities: free parking. Around 0.7 miles from Newton Aycliffe rail station. Good access for people with disabilities. Wide range of meals and drinks in its lounge bar.
Features: marking 200 years since Stockton & Darlington Railway opening on 27 September 1825, with layouts based on lines in the UK and elsewhere incl Austria, Switzerland and Thailand. Historic pictures on display. Trade support.
Contact: info@DarlingtonMRC.co.uk
Website: www.railway200.co.uk/activity/darlington-model-railway-club-exhibition-2025
Facebook: www.facebook.com/p/Darlington-Model-Railway-Club-100041630905166

Saturday 13 & Sunday 14 September
SWANSEA EXHIBITION 2025
SWANSEA, South Wales
Organiser: Swansea Rly Modellers Group sponsored by Oxford Diecast, Ray Heard & WW Scenics.
Venue: LC Swansea, Oystermouth Road, Swansea SA1 3ST.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £10.00, acpd children £1.00 (note dogs are not allowed in venue).
Amenities: adjacent public car parks. Swansea rail and bus stations are half a mile away. Full disabled access and toilets. Costa coffee café.
Features: layouts incl. *St Bryan's, Thornley WRD* (O); *Mettoy Trains* (vintage O, made in Swansea); *Hope under Dinmore, Llawryglyn* (EM); *Brief Encounter, Artist's Valley, Museum of Transport, Farbine Road, Dilwyn, Upper Bank, SRMG Juniors* (OO); *Penllynbach* (009); *Beijiao, Bremen Trams, Norge* (HO); *Heybridge Wharf* (3mm); *Hells Glen, Newvaddon Parkway, Ashfield, Moors View, Cwmfelin Parkway* (N). Traders & society stands.
Contact: Martin Evans, 07884 183395.
Website: www.srmg.org.uk
Facebook: www.facebook.com/SRMGTrains/

Sunday 14 September
HEATH PARK, Cardiff
Organiser: Cardiff Model Engineering Society.
Venue: Heath Park Miniature Railway, King George V Drive East, Cardiff CF14 4AW.
Open: 1300-1700.
Admission: £2.80 (card payment only), children aged 3 & under free (must be accompanied on rides). No dogs except service dogs.
Amenities: refreshments, gift shop.
Features: miniature railways (steam and electric trams), model and garden railways.
Website: www.heathparkminiaturerailway.co.uk

Sunday 14 September
OXRAIL 2025
OXFORD, Oxfordshire
Organiser: Oxford & District MRC.
Venue: Edward Feild School, Bicester Road, Kidlington, Oxford OX5 2LG.
Open: 1030-1600
Admission: Adults £8.00, acpd U16s free.
Amenities: free car parking and disabled access to all areas. Café available.
Features: layouts including *Burford, Tetbury, Denton Sidings*. Club stall, trade support and Rail200 memorabilia stand.
Email: secretary@oxfordmrc.org.uk
Website: www.oxfordmrc.org.uk
Facebook: www.facebook.com/oxfordMRC/

Sunday 14 September
UFFCULME, Devon
Organiser: Culm Valley Model Railway Club.
Venue: Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.
Open: 1000-1600
Admission: Free but donations welcome.
Amenities: garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.
Features: *Tiverton Junction* (30' long OO gauge model) and *Soldor*, home to a little blue engine, driven by visitors. Club layouts.
Website: www.cvmrc.weebly.com/tiverton-junction.html

Friday 19 & Saturday 20 September
MAIDSTONE, Kent
Event: Rapido Railex.
Organiser: Rapido Trains UK.
Venue: The Market Hall, Maidstone, Kent ME16 8LW.
Open: 1000-1630 both days
Admission: pre-booked adult tickets £7.50, on the door £10.00, acpd children free.
Amenities: 5 minutes walk from Maidstone station, ample parking, wheelchair-friendly.
Features: *Overlord* (OO), *Shepherdswell EKR, Bryn Stwffwl* (009), *Leysdown* (P4), *Redbridge Wharf* (OO), *Whiteoak Light Railway* (O-16.5), *Bryn y Brenin* (OO & 009), *Bristol Harbour* (OO), *Bryford County Terminal* (HO), *Clyre Valley* (009), *Folly Lane* (OO), *Bowaters* (O-16.5), *Dinsdale 1820s* (OO), *Croxley Hall Depot* (OO), society & trade stands.
Website: www.rapidotrains.co.uk/rapido-railex/

Saturday 20 September
IPSWICH, Suffolk
Event: Club Open Day.
Organiser: Ipswich Railway Modellers Association.
Venue: Ipswich Rly Modellers Assoc., Welton House, 40a Norfolk Road, Ipswich IP4 2HB.

Open: 1000-1500
Admission: free.
Amenities: Parking on site is limited to disabled badge holders only. Refreshments.
Features: view IRMA layouts in N, OO and O, plus three new layouts under development. Learn about membership. Pre-owned items stall.
Email: ipswichrma@gmail.com
Website: www.irma.org.uk
Facebook: www.facebook.com/IpswichRailwayModellersAssociation/

Saturday 20 September
KETTERING, Northamptonshire
Organiser: Kettering & District MRS supported by Mawson Models & Keith's Models.
Venue: All Saints Church, William Street, Kettering NN16 9RR.
Open: 1000-1600
Admission: adults £6.00, concessions £5.00, children £3.00, U10s free, family (2+2) £15.00.
Amenities: free parking. Disabled access. Refreshments available.
Features: 15 layouts plus trade stalls.
Website: www.kdmrs.webador.co.uk
Facebook: www.facebook.com/ketteringanddistrictmodelrailwayclub/

Saturday 20 September
LICHFIELD, Staffordshire
Organiser: Trent Valley Model Railway Society.
Venue: Life Church, Lichfield, Netherstowe, Lichfield, Staffordshire WS13 6TS.
Open: 1000-1600
Admission: adults £6.00, acpd children £2.00, family £12.00.
Amenities: parking and disabled access. Heritage bus rides covering both stations (timetable on website). Refreshments.
Features: 16 layouts plus traders and models from Sutton Coldfield Model Engineers.
Email: trentvalleymodelrailwaysociety@gmail.com
Website: www.TVMRS.org.uk
Facebook: www.facebook.com/tvmrs/

Saturday 20 September
WILTON RAILEX 2025
SALISBURY, Wiltshire
Organiser: Salisbury & South Wilts Rly Society.
Venue: Michael Herbert Hall, South Street, Wilton, Salisbury SP2 0JS.
Open: 1000-1600
Admission: adults £5.00, children £2.00 (age 5-16), accompanied U5s free.
Amenities: local free parking. Full disabled access to all areas and toilets. Catering.
Features: 12-plus club and guest layouts plus trade support. Popular garden railway, a short walk away through Wilton, will also be returning.
Contact: David Graney, 07768 448369.
Website: www.sandswrs.co.uk
Facebook: www.facebook.com/SSWRailSoc/

Saturday 20 & Sunday 21 September
BALA, Gwynedd
Organiser: Bala Lake Railway.
Venue: Ysgol Godre'r Berwyn (Bala School), Ffrydan Rd, Bala, Gwynedd LL23 7RU.
Open: both days 1000-1600
Admission: adults £7.00, concessions £6.00, children £2.00 (one child can enter free with each adult), Bala Lake Railway members £3.00.
Amenities: limited free parking. Free shuttle bus will connect with Bala Lake Railway. Hot & cold food and drinks from school cafeteria.
Features: layouts including: *Gaddhill, Indian Hill* [live steam] (SM32); *Gants Hill* (1:35 scale narrow gauge); *Cog Road, Wood Street* (O); *Henmore Creamery, Rhiw Goch* (0-16.5); *Morfa Mawr* (EM); *Bottom of the Barrel, Glanrafon, Kilburn, Penhelig, Phoenix Road* (OO); *Tan-y-Coed, Teifi Gorge, Silff Ffenestr* (009); *Amlwch, Boston & Maine, Llanuwchllyn, Millhouses* (N). Trade stands.
Contact: 01678 540666.
Website: www.bala-lake-railway.co.uk/events
Facebook: www.facebook.com/BalaLakeRailway/

Saturday 20 & Sunday 21 September
42ND ANNUAL EXHIBITION
BLACKPOOL, Lancashire
Organiser: Blackpool & North Fylde MRC.
Venue: Blackpool 6th Form, Blackpool Old Road, Blackpool, Lancs FY3 7LR.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £8.00, concessions £7.00, acpd U16s free.
Amenities: on-site parking and easy disabled access. Refreshments available.
Features: layouts in G to N plus demos, club sales stall and trade support.
Website: https://livewyre.wixsite.com/bnfmrc/annual-exhibition
Facebook: www.facebook.com/p/Blackpool-North-Fylde-MRC-100066398330690/

Saturday 20 & Sunday 21 September
COVENTRY, West Midlands
Event: Heritage Open Weekend.
Organiser: Coventry Model Railway Club.
Venue: Unit 3, Coventry Canal Basin Warehouse, Leicester Row, Coventry CV1 4LH.
Open: both days 1000-1600
Admission: free.
Amenities: access to club room is via Canal Basin entrance and is then up three flights of narrow stairs so unfortunately there is no wheelchair access. Refreshments available.
Features: coinciding with Heritage Open Weekend in Coventry, six layouts on show in OO, N & 009, plus exhibits for children.
Email: secretary@coventrymodelrailwayclub.org.uk
Website: www.coventrymodelrailwayclub.org.uk
Facebook: www.facebook.com/coventrymrc/

Saturday 20 & Sunday 21 September
SCALEFORUM 2025
HIGH WYCOMBE, Buckinghamshire
Organiser: Scalefour Society.
Venue: Cressex Community School, Holmers Lane, High Wycombe HP12 4QA.
Open: Saturday 1030-1730
Sunday 1000-1600
Admission: adults £12.00, acpd children free.
Amenities: parking, disabled access, refreshments.
Features: 14 layouts including: *Bramsden, Brettell Road, Cheddar, Coombe Wharf, Flintcombe, Forge Mill Sidings, Hedley Hope, Hepton Wharf, High House Colliery, Mostyn, Nottingham Goods, Rolvenden, Ruyton Road, Seaton* (P4); *Ewe* (OO). Plus demonstrations and trade support focusing on 4mm finescale.
Email: enquiries@scalefour.org
Website: www.scalefour.org
Facebook: www.facebook.com/ScalefourSociety/

Saturday 20 & Sunday 21 September
TROON, South Ayrshire
Organiser: Ayr Model Railway Group.
Venue: Walker and Concert Hall, South Beach, Troon KA10 6EF.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £7.00, children £4.00, U5s free, family ticket (2+2) £16.00.
Amenities: street parking nearby (some metered). Troon railway station is half a mile away. Bus service from Ayr stops outside. Teas, coffees & food on-site supporting local RNLI.
Features: 16 layouts in G, O, OO and N, including layouts of special interest to children, plus demonstrations and trade & society stands.
Email: ayrmodelrailwaygroup@btinternet.com
Website: www.ayrmrg.co.uk
Facebook: www.facebook.com/ayrmrgsandgate/

Saturday 20 & Sunday 21 September
TUNBRIDGE WELLS, Kent
Organiser: Spa Valley Railway.
Venue: Tunbridge Wells West Station, Nevill Terrace, Tunbridge Wells TN2 5QY.
Open: both days 1000-1700
Admission: adults £16.00, children £8.00, seniors £15.00 (includes train travel).
Amenities: car parking nearby & wheelchair access. Exhibits at all stations & unlimited train rides. Light refreshments available.
Features: 30 layouts, incl. *England's Dark Satanic Mills, Tunbrick Wells, Upperton, The Paystow Branch, Brook Street, Ensthorpe, Harland Castle, Daleside, Travelling to Wedded Bliss, Brunswick, Oakbourne, Quarry Falls, Merrybrook Parkway, Oxley Heath, Horsebridge Wharf, Kinlochewe, Scarlington, Dublingham Goods Station, The LB&L Line, Out West, White Meadow Quarry, Roast Green Road Depot, Hornby Dublo, Larger and Lime Works, Lockbourne Ohio, West Green MPD, Moor Boxes.* Plus trade support.
Contact: Richard Arding, 07507 182763.
Email: richard.arding@spavalleyrailway.co.uk
Website: www.spavalleyrailway.co.uk
Facebook: www.facebook.com/SpaValleyRailway/

Saturday 20 & Sunday 21 September
WIMBORNE, Dorset
Organiser: Wessex Association of MRCs.
Venue: The Hamworthy Club, Magna Road, Wimborne BH21 3AL.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £7.00, acpd U16s free.
Amenities: free car park, disabled access (except for squash court area). Restaurant & bar.
Features: 14-plus layouts in 0, 00, 009 and N gauges, incl. GWR steam, narrow gauge, diesels & Agatha Christie. Demos & trade stands.
Email: chairman@wamrc.com
Website: https://wamrc.co.uk

Sunday 21 September
WISBECH, Cambridgeshire
Organiser: Wisbech Model Railway Club.
Venue: Parson Drove Village Hall, 63 Main Road, Parson Drove, Wisbech PE13 4LA.
Open: 1000-1600
Admission: adults £4.00, acpd U16s £2.00, acpd U5s free.
Amenities: free parking, disabled access. Refreshments and toilets on site.
Features: layouts incl. *Castle Hill, Emneth, Burma Railway, Edale, Lower Tyldesley and Weston Road*. Plus demonstrations, traders and a prize draw including a donation from Dapol.
Contact: 07798 767890.
Email: wisbechmrc1@gmail.com
Facebook: https://www.facebook.com/groups/480494708795233

Saturday 27 September
DARTMOUTH RAILWAY 200 EVENT
DARTMOUTH, South Devon
Organiser: Dartmouth Team Steam.
Venue: Guildhall, Victoria Road, Dartmouth, Devon TQ6 9RY & Dartmouth Visitor Centre, Mayors Avenue, Dartmouth TQ6 9YY.
Open: 1000-1600
Admission: free but donations welcome.
Amenities: car parks (fees apply) around Dartmouth or travel by Dartmouth Steam Railway Paignton-Kingswear then ferry across Dart. Buses from Totnes, Kingsbridge, etc. Guildhall fully accessible, as is most of Visitor Centre.
Features: marking actual 200th anniversary of opening of the Stockton & Darlington Railway with model layouts, exhibits, films, oldest surviving Newcomen atmospheric steam engine (invented 1712 by Thomas Newcomen, born in Dartmouth in 1663), ride-on train, children's activities, railway history and more. Layouts incl. South Devon MRS's *Bradley Bottom* and a *Brixham Station* layout. Dartmouth Museum will showcase its Hulse models depicting stationary steam engines from 1712-1806, and Newton Abbot Library its model railway books collection.
Contact: 01803 834224.
Email: visitorcentre@exploredartmouth.co.uk
Website: www.exploredartmouth.co.uk/event/railway-200-celebrating-steam-in-dartmouth/

Saturday 27 September
EARLEY, Reading, Berkshire
Organiser: Loddon Vale Model Railway Club.
Venue: St Peter's Church Hall, Church Lane, Earley, Reading RG6 1EY.
Open: 1000-1630
Admission: adults £6.00, children £2.00, family (2+2) £13.00.
Amenities: limited parking plus limited disabled access (show is over two floors and unfortunately there is no disabled access to the upper floor). Refreshments available.
Features: 11 layouts plus trade stands.
Contact: Chris Marsh, 07972 115816.
Email: ex.man.lvmrc@gmail.com
Website: www.lvmrc.co.uk
Facebook: https://www.facebook.com/Loddonvalemrc/?locale=en_GB

Saturday 27 & Sunday 28 September
DORKING, Surrey
Event: Autumn Exhibition 2025.
Organisers: Dorking & Horsham MRCs jointly.
Venue: The Ashcombe School, Ashcombe Road, Dorking RH4 1LY.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £5.00, children £3.00, family (2 plus up to 3) £10.00.
Amenities: free on-site parking plus wheelchair access. Rail and bus stations are 5 mins walk away. Refreshments available.
Features: selection of layouts, interactive layout for children plus trade stalls.
Email: exhibitions@horshammrc.org.uk
Websites: www.dadmrc.co.uk www.horshammrc.org.uk

Saturday 27 & Sunday 28 September
LYDNEY, Gloucestershire
Event: Lydney Model Railway Show.
Organiser: Forest of Dean Model Railway Club.
Venue: The Dean Academy, Lydney, Gloucestershire GL15 5DZ.
Open: Saturday 1100-1700
Sunday 1000-1600
Admission: adults £5.00, children £3.00, family (2+2) £12.00.
Amenities: free parking and the venue is fully accessible. Venue is a 15-minute walk from Lydney railway station. On-site café.
Features: 20-plus layouts in various scales from G to N plus traders selling new and second-hand model items, accessories and books.
Contact: 01594 835935.
Website: www.fodmrc.org.uk

Saturday 27 & Sunday 28 September
RAILWAY 200 AUTUMN GALA
MARTINHOE CROSS, North Devon
Organiser: Lynton & Barnstaple Railway.
Venue: Woody Bay Station, Martinhoe Cross, Parracombe, Devon EX31 4RA.
Open: see railway website for updates.
Admission: see railway website for details.
Amenities: free parking at Killington Lane & disabled parking at Woody Bay station. Café & barbecue at Woody Bay. Restored L&BR Chelfham station, 4½ miles from Barnstaple (see RM August issue) also open to visitors.
Features: marking 90 years since closure of the original L&BR, on 29 September 1935. Intensive train service on relaid section of the L&BR with visiting guest locos *Chaloner*, *Peter Pan* and from Westonzoyland. *Woody Gate* 16mm scale garden railway plus 7¼" gauge ride-on railway. Model layouts in marquee in adjoining grounds of Moorlands Hotel plus sales stands.
Contact: 01598 763487.
Email: enquiries@lynton-rail.co.uk
Website: www.lynton-rail.co.uk
Facebook: www.facebook.com/Lynton.Barnstaple/

Saturday 27 & Sunday 28 September
STAFFORD RAILWAY CIRCLE ANNUAL MODEL RAILWAY EXHIBITION 2025
STAFFORD, Staffordshire
Organiser: Stafford Railway Circle Ltd.
Venue: Bingley Hall, Stafford Showground, Weston Road, Stafford ST18 0BD.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adult advance tickets £16.00, on the door tickets £18.00; children aged 5-16 £5.00; family tickets (2 adults + up to 3 children) in advance £32.00, on the door £36.00.
Amenities: County Showground is 3 miles east of the town on A518. Extended free parking (satnav ST18 0BD). Free bus service from Stafford station. Disabled access and catering.
Features: layouts incl. *Allerby, Blandings Parva, Blowers Green Wagon Works, Kensington Addison Road, Kingsbury, Old Elm Park, Peasevern Yard, Ullapool, Woodhill Goods* (O); *Frecclesham, Weedram Distillery* (7mm scale); *Pwllheli, Underhill Road and Yard* (P4); *Lochty, Roundtree Sidings, Shwt, Westcliff* (EM); *Albert Street Parcels, Bampton, Barrihandie, Brief Encounter Monochrome, Crinan, Doe Hill, East Quay, Fairwood Junction, Gracetown Bank, Helston, Hogsmeade, Loch Syd, Tellindalloch in*

Autumn, Temple Bridge, Titfield The Next Chapter, Whiteacres, Witham (OO); *Bond Lane* (OO9); *Bear Creek, Eight Dollar Canyon, Marmagne* (HO); *Ballyconnel Road, Bluish Moor, Exebridge, Foxbury* (3mm scale); *Doe Lea, Middle Elmton, St Andrews and Guard Bridge, Smalldale Quarry* (N); *Ashwood, Caddenford, Clun Castle, Trivago*. Traders & miniature train rides.
Contact: Terry Robinson, 07825 917857.
Email: tcr.robinson@icloud.com
Website: www.staffordrailwaycircle.org.uk
Facebook: www.facebook.com/StaffordRailwayCircleLTD/

Saturday 27 & Sunday 28 September
STICKNEY, Lincolnshire
Organiser: Boston Model Railway Society.
Venue: Stickney Village Hall, Hall Lane, Stickney PE22 8BG.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £4.00, children £2.00, family (2+3 maximum) £10.00.
Amenities: venue is 7 miles north of Boston on A16. Free car parking and good disabled access. Hot and cold drinks and food.
Features: layouts incl. *Shagbats* (O); *Byway MPD, Gracetown Bank, Humberside TMD, End of the Line* (OO); *St Judes Bay* (OO/OO9); *Holcombe Beach* (TT:120); *Belle Isle, Kitdale View, North Colton, Wits End* (N); trade stands incl. 3D Printing for Charity (profits split between Lincs & Notts Air Ambulance & LIVES). Raffle & tombola.
Email: Christoper Ellerby via christopherellerby@hotmail.co.uk
Website: www.bostonmrs.50webs.org/Exhibitions.html
Facebook: www.facebook.com/BostonMRS/

Sunday 28 September
UFFCULME, Devon
Organiser: Culm Valley Model Railway Club.
Venue: Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.
Open: 1000-1600
Admission: Free but donations welcome.
Amenities: garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.
Features: *Tiverton Junction* (30' long OO gauge model) and *Soldor*, home to a little blue engine, driven by visitors. Club layouts.
Website: www.cvmrc.weebly.com/tiverton-junction.html

Saturday 4 October
BEER, Devon
Event: Drive a Model Train.
Organiser: Pecorama.
Venue: Gallery Lecture Theatre, Pecorama, Underleys, Beer, Devon EX12 3NA.
Open: 1000-1600
Admission: included in Pecorama special 50th anniversary entry prices: adults £7.25, concessions (ages 3-17 & 65+) £6.25, family (x4) £24.95, family (x5) £29.95, under 2s free, dogs £1.00. Quote RM50 for discount. Additional Beer Heights Light Railway train rides £4.00.
Amenities: parking, refreshments, disabled access (hilly, unsuitable for 3-wheel scooters).
Features: event when visitors can operate the Swiss RhB-themed DCC layout *The Andeer Line* (HOm) – instructions given. Tickets also include Pecorama model railway exhibition, a ride on the Beer Heights Light Railway and gardens.
Website: www.pecorama.co.uk

Saturday 4 October
HILLINGDON, West London
Event: Test Track Open Day.
Organiser: Hillingdon Railway Modellers.
Venue: Yiewsley Baptist Church Hall (side entrance), 74 Colham Avenue, Yiewsley, Hillingdon, London UB7 8HF.
Open: 1000-1500
Admission: £6.00 per person.
Amenities: nearest rail station is West Drayton (on Elizabeth Line), 12 minutes' walk away. Local bus Nos. 222, 350, U1, U3 & U5 serve the area. Refreshments available all day.
Features: chance to bring and run O, OO or N gauge locos & trains on club's three test tracks – dc and DCC operation available, but sorry no live steam. Chance to meet fellow modellers and try out models. Expert help regarding problem models. New members welcome.
Contact: Peter, 020 8368 4090.
Website: www.hillingdonrailwaymodellers.co.uk
Facebook: www.facebook.com/p/Hillingdon-Railway-Modellers- 100064718916905/

Saturday 4 October
ROWLEY REGIS, West Midlands
Event: Cradley Heath MRC 12th Model Railway Exhibition.
Organiser: Cradley Heath Model Railway Club.
Venue: Rowley Learning Campus, Curral Road, Rowley Regis, West Midlands B65 9AN.
Open: 1000-1630
Admission: adults £8.00, children (5-15) £3.00, U5s free, family (2+2) 18.00.
Amenities: venue has free parking, can be reached by a main bus route and is 0.9 mile from Rowley Regis railway station. Venue is fully accessible. Light refreshments.
Features: over 20 layouts in various scales. Also trade & society stands including the OO9 Society, 7mm NG Association and MERG. Plus 7¼" gauge miniature train rides.
Contact: 07861 640971.
Email: chmrc@hotmail.co.uk
Website: www.cradleyheathmrc.co.uk
Facebook: www.facebook.com/cradleyheathmrc

Saturday 4 & Sunday 5 October
CHESTER, Cheshire
Event: Chester Model Railway Exhibition
Organiser: Chester Model Railway Club.
Venue: Saltney Community Centre, Sandy Lane, Saltney, Chester CH4 8UB.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £5.00, acpd U16s free.
Amenities: free car parking. On bus routes 4, 11, T8 & 16. Disabled access. Refreshments.
Features: 8 layouts, trade stand & club display.
Contact: Russell Davies, 07717 583660.
Website: www.chestermodelrailwayclub.com
Facebook: www.facebook.com/chestermrc/

Saturday 4 & Sunday 5 October
MICKLEOVER MODEL RAILWAY EXHIBITION
MICKLEOVER, Derby
Organiser: Mickleover Model Railway Group.
Venue: Mickleover Community Centre, Uttoxeter Road, Mickleover, Derby DE3 0DA.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £6.00, U16s free.
Amenities: limited free parking but also nearby car parks and on-road parking. Trent Barton Bus *The Mickleover* 'Green' service to Ladybank Estate is most direct route to the venue from Derby bus station, but 'Blue' service is the most direct when returning to the bus station. Disabled access. Refreshments available.
Features: five club layouts: *Warner Street* (O); *Duffield, Stoney Lane, Thomas and Friends* (OO); *Derwent Valley* (N). Plus 10 visiting layouts, incl. *Lockbourne Ohio* (S scale/1:64) from the USA. Also demonstrations and trade stands.
Email: mickleoverexhibition@gmail.com
Website: https://mmrg.org.uk
Facebook: www.facebook.com/MickleoverModelRailwayGroup/

Saturday 4 & Sunday 5 October
SHILDON EXHIBITION 2025
SHILDON, County Durham
Organiser: Shildon Model Railway Club.
Venue: Locomotion Railway Museum, 1 Dale Road, Shildon DL4 2RE.
Open: Saturday 1000-1700
Sunday 1000-1700
Admission: free to all – ticket booking is not required, only advisable.
Amenities: free parking. Access from Shildon rail station, served by Northern Rail services from Darlington and Bishop Auckland plus Arriva bus Nos.5 & 5A from the same towns. Disabled friendly venue. Café facilities available all day.
Features: 25-plus layouts incl. *G Scale Layout* (45mm gauge); *Amblemere, Brafferton, Ideford Town, Queensbury Depot, Wendys Halt* (O); *Ravensworth Road* (EM); *Castledale Fabrications, Cottlestone, Gallows Close, Renewable Railways, Ribblehead Viaduct, Scalby, Thomas, Town Square, Vintage Showcase* (OO); *Mynydd Tatws* (OO9); *Megsdorf* (HO); *Eaglescliffe, Mugga Ghyll Junction* (N). 20 trade stands with railwayana.
Contact: Robert Arrand, 07564 301487.
Website: www.shildonmodelrailwayclub.co.uk
Facebook: www.facebook.com/people/Shildon-MRC/61566876388574/?_rdr

Sunday 5 October
BURY ST EDMUNDS, Suffolk
Event: East Anglian O Gauge Celebration.
Organisers: King's Lynn O Gauge Group, Norfolk Mardlers, North East Essex O Gauge Group & Waveney Valley Model Railway Group.
Venue: British Sugar Sports & Social Club, Hollow Road, Bury St Edmunds IP32 7AZ.
Open: 1000-1600
Admission: £4.00 for all. Proceeds go to Cancer Research UK (CRUK) or a similar charity.
Amenities: free parking and disabled access. Refreshments available.
Features: layouts incl. *Ashwell Moor, Bishop's Lane Brewery, Blewbury, Croften Maltings, Cromer Beach Engine Shed, GWR Engine Shed, Willowbank Marsh.* Plus traders, displays, demos and Bring & Buy (donations welcome).
Contact: 01553 810673.
Email: pearman.books@idnet.com

Sunday 5 October
GAINSBOROUGH, Lincolnshire
Organiser: Gainsborough MRS.
Venue: Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.
Open: 1330-1730 (last entry 1630).
Admission: adults £5.00, children £4.00, family (2+2) £15.00.
Amenities: limited accessibility for disabled.
Features: open day at O gauge East Coast Main Line layout. See how LNER was run in the heyday of steam, with dozens of trains, formed from over 180 locomotives and over 400 items of rolling stock running on half a mile of track.
Contact: B Hodgkinson, 07943 182520.
Email: gmrscontact@gmail.com
Website: www.gainsboroughmodelrailway.co.uk

Sunday 5 October
NORTH BRISTOL, Bristol
Organiser: Sodbury Vale Model Railway Club.
Venue: Lyde Green Community Centre, Willowherb Road, Lyde Green, Bristol BS16 7GW.
Open: 1000-1600
Admission: adults £6.00, acpd U16s free.
Amenities: venue is on a main bus route and close to Lyde Green Park and Ride. Disabled parking and access. Refreshments available.
Features: layouts, trade support, interactive demonstrations and 'how to' displays, plus local model club & society stands.
Email: sodburyvalemrc@gmail.com
Website: sodburyvalemrc.co.uk
Facebook: www.facebook.com/SodburyValeMRC/

Saturday 11 October
ABBOTSKERSWELL, Devon
Event: South Devon MRS Open Day.
Organiser: South Devon Model Railway Society.
Venue: Church House, Slade Lane, Abbotskerswell, Devon TQ12 5NZ.
Open: 1000-1600
Admission: adults £2.00, children 50p, family £5.00. Proceeds to Church House Trustees.
Amenities: parking to be confirmed. Refreshments at Orchard Coffee & Crafts.
Features: O, OO & N gauge layouts.
Email: southdevonmrs@hotmail.com
Website: www.sdmrs.org.uk

Saturday 11 October
WATFORD, Hertfordshire
Event: Autumn Model Railway Exhibition
Organiser: South-West Herts MRS.
Venue: St Peters Church Hall, Bushey Mill Lane, Watford WD24 7TF.
Open: 1000-1600
Admission: adults £4.00, up to two acpd U16s can enter free per adult.
Amenities: close to M1 Jct. 5 with A41. On-site parking. Wheelchair access. Refreshments.
Features: club layouts incl. brand new *Fudge Town* plus *Silverbury* (DCC), *Tedburn St Mary* and *Thomas Town* children's layout (all OO); *Dudley Frey* (N). Plus club members' layouts.
Contact: Michael Roualle, 07377 188329.
Email: Exhibition Manager Philip Chown via pc.SWHMRStr41n@gmail.com
Website: www.southwesthertsmrs.org.uk
Facebook: www.facebook.com/SouthWestHertsMRS/

Saturday 11 & Sunday 12 October
FARNHAM & DISTRICT 50TH EXHIBITION
ALDERSHOT, Hampshire
Organiser: Farnham & District MRC.
Venue: Alderwood Leisure Centre, Tongham Road, Aldershot, Hants GU12 4AS.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adults £12.00, children £3.00, family £26.00, including show guide.
Amenities: free parking. Wheelchair access. Catering from 0930 each day.
Features: 40 layouts incl. *Northport Quay* (7mm scale Irish broad gauge – featured in this RM issue p796-801); *The Yard* (O/0-16.5); *Worton Court* (0-16.5); *Plumpton Green* (P4); *Cadhay Sidings, Llawryglyn* (EM); *Bigbury-on-Sea, Fence Houses NCB, Hazelbury Junction, Much Murkle, Tellindalloch, 200 Years of Railways display* (OO); *Clyre Valley Railway, Chwarel Tegid* (OO9); *Neuburg Project* (HOm); *Ulvaryd Strand* (HOe); *North Quay, Port Jubilee, Rydes Vale, Tucking Mill & William Smith's Wharf* (2mm Finescale); *McDermot's Machinery* (N Finescale); *Ashfield, Brinklow, Charity Layout, Haxton Castle, Royal Albert Bridge, Smalldale Quarry* (N). Traders.
Email: exhibition@farnhammrc.org.uk
Website: www.farnhammrc.org.uk

Saturday 11 & Sunday 12 October
BARROW-IN-FURNESS, Cumbria
Event: 62nd Model Railway & Transport Exhibition.
Organiser: Furness Model Railway Club.
Venue: Chetwynde School, Rating Lane, Barrow-in-Furness, Cumbria LA13 0NY.
Open: both days 1000-1700
Admission: adults £8.00, children £4.00 (card payments accepted).
Amenities: see website for details.
Features: new venue with layouts incl. *Chop It Up Lumber* (1/24th); *Arnside, Castle View, Coppell, Poulton-le-Fylde Junction, Sodor* (OO); *Saint Agur* (HO); *Cwm Hyfryd* (Nn3); *Nazareth Portland, Rycroft Arena* (N); *Tapton* (Z) plus demonstrations by Scottish Modellers, several traders, model boat and model engineering displays, Train Sim, miniature train rides & exhibits.
Contact: Exhibition Manager, 07764 273581.
Website: www.furnessmrc.com
Facebook: www.facebook.com/furnessmodelrailwayclub.fmrc

Saturday 11 & Sunday 12 October
BOURNEMOUTH, Dorset
Event: Christchurch Model Railway Exhibition.
Organiser: South Western Model Railway Club.
Venue: Bournemouth School for Girls, Castle Gate Close, Castle Lane West, Bournemouth BH8 9UJ.
Open: both days 1000-1600
Admission: adults £7.00, acpd U16s free.
Amenities: wheelchair-friendly venue.
Features: layouts incl. *Wedmore, Thiswaye* (O); *Upbeck St Mary* (EM); *Bosuns Wharf, Rodwell Lodge, Watershoot Bay* (OO); *Quarry Bank* (OO9); *Burnham-on-Sea* (2mm Finescale); *Hells Glen, Stafford Potteries* (N). Demo & trade stands.
Contact: Tim Drew, 07711 891118.
Email: tjdrew52@gmail.com
Facebook: www.facebook.com/southwesternmrc/

Saturday 11 & Sunday 12 October
KIRKBY IN ASHFIELD, Nottinghamshire
Organiser: Elizabethan Railway Society.
Venue: The Summit Centre, Lowmoor Road, Kirkby in Ashfield NG17 7LL.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: £5.00 (cash only). Acpd U16s free.
Amenities: free parking. Disabled access. Refreshments available.
Features: 10 layouts plus trade stands.
Contact: 07904 422812.
Email: ers-showmanager@talktalk.net
Website: www.elizabethanrailwaysociety.com
Facebook: www.facebook.com/groups/2103031423251526/?locale=en_GB

Saturday 11 & Sunday 12 October

WARLEY AT STATFOLD
TAMWORTH, Staffordshire

Organisers: Warley Model Railway Club & Statfold Country Park.
Venue: Statfold Country Park, Ashby Road, Tamworth, Staffs B79 0BU.
Open: both days 1000-1700
Admission: pre-booked tickets purchased online – adults £17.50, child/senior £15.00, carer £10.00, family £57.50. At the gate prices – adults £19.50, child/senior £17.00, carers (pre-notified) £12.00, family £65.50.
Amenities: site is off the B5493 north of Tamworth with easy access from M42 Jct. 11. Free car parking. Free shuttle bus will operate from Tamworth rail station to Statfold Country Park from 0930-1630. Venue has full disabled access. Choice of refreshment facilities.
Features: layouts incl. *82G, Stodmarsh, The Henley-in-Arden Line* (O); *Gileston Light Railway* (0-16.5); *Backan Ford Tramway* (On30); *Mount Woodville Works* (P4); *Blicsdale, Bottom of the Barrel, Burton Leicester Junction, Children's Layout, Fairwood Junction, Funville, Helston, Minories, Northbridge, Layout with No Name, Trinity Dock Street Bridge, Welby Lane RTC, Wickham Market, Wardwood* (OO); *Upsan Down Tramway* (12mm gauge); *Tan y Bwlch* (009); *Kleine Albula* (HOm); *Gare Sainte Marie, Kaninchenbau* (HOe); *Nové Mesto na Nedostatku* (TT:120); *Oberdorf II* (Nm); *York* (2mm Finescale); *Alston, Burdock, Hawes Junction, Hapless Cement Works, Macclesfield Hibel Road* (N). Tech Zone demos, trade & society stands.
Contact: 01827 830389.
Email: query@warley-mrc.org.uk
Website: www.thewarleyshow.co.uk
Facebook: www.facebook.com/warleyatstatfold/

Saturday 11 & Sunday 12 October

62ND WAKEFIELD MODEL RAILWAY SHOW
WAKEFIELD, West Yorkshire

Organiser: Wakefield Railway Modellers' Society.
Venue: Queen Elizabeth Grammar School, Sports Hall, Northgate, Wakefield WF1 3QX.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adults £10.00, 2 children aged 12-17 can enter free with each paying adult. Unaccompanied young people (12-17) £2.00.
Amenities: car park. Wheelchair access. Catering.
Features: layouts incl. *Nafferton, Prime Cereals, White Rat Sidings* (O); *Boston and Frodsham, Chatsworth Road, Queens Road Sidings* (P4); *Hartley Bank Screens, Hudson Street* (EM); *Edenbridge, Inchyra Paper, Roydburn Mills, South Millfields, Stanley Ferry, Thomas* children's layout, *Trevago ECC* (OO); *Chicago 31st Street Yard, Gusslingen, Lacota Grain* (HO); *Kayeton* (N).
Contact: Exhibition Manager Stephen Foster.
Email: committee@wakefieldrms.org
Website: www.wakefieldrms.org
Facebook: www.facebook.com/wakefieldrms/

Sunday 12 October

CLEVELAND, Yorkshire

Event: Autumn Open Day.
Organiser: Cleveland Model Railway Club.
Venue: Unit 2, The B-Hive, Skelton Industrial Estate, Skelton, Cleveland TS12 2LQ.
Open: 1000-1600
Admission: adults £1.00, children 50p.
Amenities: free parking. Nearest bus stop on Arriva route No.5: 'Hollybush, Skelton'. Disabled access (except for mezzanine). Refreshments.
Features: see the club's layouts in a relaxed atmosphere and chat with group members.
Email: Clevelandmrc.publicity@gmail.com
Facebook: www.facebook.com/groups/clevelandmrc

Sunday 12 October

HEATH PARK, Cardiff

Organiser: Cardiff Model Engineering Society.
Venue: Heath Park Miniature Railway, King George V Drive East, Cardiff CF14 4AW.
Open: 1300-1700.
Admission: £2.80 (card payment only), U4s free (must be acpd on rides). Service dogs only.
Amenities: refreshments, gift shop.
Features: miniature railways (steam and electric trams), model and garden railways.
Website: www.heathparkminiaturerailway.co.uk

Sunday 12 October

UFFCULME, Devon

Organiser: Culm Valley Model Railway Club.
Venue: Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.
Open: 1000-1600
Admission: Free but donations welcome.
Amenities: garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.
Features: *Tiverton Junction* (30' long OO gauge model) and *Soldor*, home to a little blue engine, driven by visitors. Club layouts.
Website: www.cvmrc.weebly.com/tiverton-junction.html

Thursday 16 – Sunday 19 October

THE MIDLANDS MODEL ENGINEERING EXHIBITION
LEAMINGTON SPA, Warwickshire

Organiser: Meridienne Exhibitions Ltd.
Venue: Warwickshire Event Centre, near Leamington Spa – on the junction of the A425/B4455, visitor car park CV31 1FE.
Open: Thursday-Saturday 1000-1630
Sunday 1000-1600
Admission: adults £13.50, seniors (age 65-plus) £12.50, children (5-14) £5.00, U5s free. Tickets are valid for any one day and can be purchased in advance via website.
Amenities: free parking, shuttle bus from Leamington Spa railway station on Thursday only (charges apply). Restaurant & coffee shop.
Features: 35 clubs and societies displaying exhibits covering a range of modelling skills. Plus 39 model engineering specialist trade suppliers. Society of Model & Experimental Engineers will give demonstrations covering lathe work, milling, Computer Aided Design, simple CNC machining and 3D printing. Competition and display entries.
Email: info@meridienneexhibitions.co.uk
Website: www.midlandsmodelengineering.co.uk
Facebook: www.facebook.com/MeridienneEx/

Friday 17 – Sunday 19 October

MODEL DISPLAY & WEST SOMERSET RAILWAY AUTUMN STEAM WEEKEND
BISHOPS LYDEARD, Taunton, Somerset

Organiser: Taunton Model Railway Group in conjunction with West Somerset Railway.
Venue: Bishops Lydeard Station, Platform 1, West Somerset Railway, Taunton TA4 3RU.
Open: 1000-1600
Admission: small charge (note children must be accompanied).
Amenities: free parking. Refreshments.
Features: two finescale OO layouts, *Bath Green Park* and *Tamerig New* plus sales table.
Email: secretary.tmrg@hotmail.com
Website: www.tauntonmodelrailwaygroup.co.uk
Facebook: www.facebook.com/tauntonmodelrailwaygroup

Saturday 18 October

CHELMSFORD, Essex

Organiser: Chelmsford & District MRC.
Venue: St John Payne School, Patching Hall Lane, Chelmsford CM1 4BS.
Open: 1000-1600
Admission: adults £7.00, juniors (12-17 inclusive) £2.00.
Amenities: free car park. Disabled access. Bus route Nos.C1 and 170 from Chelmsford bus station to Patching Hall Lane. Refreshments.
Features: 21 layouts in O, On30, OO, HO and gauges plus traders and club stand.
Contact: use form on the club website.
Website: www.cdmrc.co.uk

Saturday 18 October

BECKENHAM MODEL RAILWAY SHOW 2025
FOREST HILL, South East London

Organiser: Beckenham & West Wickham Model Railway Club sponsored by Harrison Brown Models, Heritage in Motion and Invicta Model Rail.
Venue: Forest Hill Boys School, Dacres Road, Forest Hill, London SE23 2XN.
Open: 1000-1630
Admission: adults £10.00, acpd U16s free (facilities for card payments available). Save £2.00 when purchasing tickets in advance via link on club website.
Amenities: free on-street parking. 15 mins' walk from Forest Hill and Sydenham rail stations. Bus No.75 from Lewisham and Catford stations to Sydenham, Penge East and West Croydon stations stops at Dacres Road. Fully accessible to hand-powered wheelchairs. Café and raffle.
Features: new, larger venue with layouts incl. *Batcombe, Croxley Hall Depot, East Glasson Port Authority, East Highland Railway, Fareford East, Horton Common, Joganji Rover, London Road Sidings, Medway Quay, New Oak Lane, Newport Street, Nudge Nudge, Plugstreet, Rawdon West, San Telmo, Small N Working, Smithfield, Southwick, Steamtown, Templegate Wagon Works, The West Exchange Siding, Tramlink.* Demos and trade & society stands.
Email: bwwmrc@me.com
Website: www.bwwmrc.co.uk
Facebook: www.facebook.com/Beckenhamandwestwickhammrc/

Saturday 18 October

ICKENHAM, West London

Event: Ickenham Model Railway Show.
Organiser: Ickenham & District Society of Model Engineers.
Venue: Ickenham Village Hall, 33 Swakeleys Road UB10 8DG.
Open: 1000-1630
Admission: adults £3.00, children £1.00.
Amenities: disabled access. Refreshments.
Features: seven layouts on display.
Contact: use contact form on the website.
Website: www.idsme.co.uk
Facebook: www.facebook.com/IckenhamSME/

Saturday 18 October

ANNUAL WINCHESTER AMERICAN & CONTINENTAL O GAUGE EXHIBITION
WINCHESTER, Hampshire

Organiser: Bevis King (who is taking over for the first time in 2025 as Alan Marlow is stepping down after five years of running the show).
Venue: Kings School, Romsey Road, Winchester, Hants SO22 5PN.
Open: 1000-1600
Admission: adults £8.00, children £2.00, family £12.00.
Amenities: cooked breakfasts.
Features: overseas layouts in scales 1:43 to 1:50 including 2-rail and 3-rail O gauge standard gauge and 7mm scale narrow gauges. Plus traders and new R-T-R Emporium offering new and boxed second-hand 7mm scale R-T-R models. Visitors can bring their own stock to run on the four-line test track (2-rail only).
Contact: Bevis King, 07889 461946.
Email: brwk@bevteccom.co.uk
Website: www.alan49.x10.mx/index1w.htm

Saturday 18 & Sunday 19 October

CAISTOR, Lincolnshire

Event: Caistor MRC Show.
Organiser: Caistor Model Railway Club.
Venue: Caistor Town Hall, High Street, Caistor, Lincolnshire LN7 6TJ.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adults £5.00, U16s £3.00.
Amenities: refreshments available plus tombola in aid of Caistor Food Bank.
Features: 12 layouts plus trade stands.
Contact: Graham Parkin, 07733 414866.
Facebook: www.facebook.com/groups/464928214440733/?locale=en_GB

Saturday 18 & Sunday 19 October

DUNDEE MODEL RAILWAY EXHIBITION 2025
DUNDEE, Eastern Central Lowlands

Organiser: Dundee Model Railway Club.
Venue: The Ardler Community Complex, Turnberry Avenue, Dundee DD2 3TP.
Open: Saturday 1000-1630
Sunday 1000-1630
Admission: adults £6.00, children/concessions £4.00, family (2+2) £14.00, accompanied U5s free.
Amenities: free car park in East Scotscraig Lane. Venue is 2.7 miles from both Invergowrie and Dundee rail stations. Blue Badge parking bays within car park and route to the entrance is accessible to a wheelchair user with assistance. Accessible toilet. Café Ardler will be open.
Features: model displays in various scales.
Contact: 07497 919817.
Email: dundeemrc@outlook.com
Website: www.dundeemrc.uk
Facebook: www.facebook.com/DundeeModelRailwayClub/

Saturday 18 & Sunday 19 October

GREENOCK, Inverclyde

Organiser: Greenock & District MRC.
Venue: Lyle Kirk (Union Street Building), 31 Union Street, Greenock PA16 0TG.
Open: Saturday 1000-1700
Sunday 1200-1700
Admission: adults £6.00, children/concesssions £5.00, family (2+2) £20.00.
Amenities: buffet with snacks, teas & coffees.
Features: layouts in various scales, including 009/OO layout *Minffordd* making its debut.
Email: info@greenockmrc.co.uk
Website: www.greenockmrc.co.uk
Facebook: www.facebook.com/p/Greenock-District-Model-Railway-Club-100064730287091/

Saturday 18 & Sunday 19 October

40TH UCKFIELD MODEL RAILWAY EXHIBITION & SCALEFOUR SOUTHEAST
UCKFIELD, East Sussex

Organiser: Uckfield Model Railway Club.
Venue: Uckfield Civic Centre, Civic Approach, Uckfield, East Sussex TN22 1AE.
Open: both days 1000-1700
Admission: adults £10.00, acpd children free (Note: up to two children aged 5 to 15 inclusive will be admitted free if accompanied by a paying adult. Unaccompanied & additional children £5.00. Acpd U5s free). Admission on the door only – cash or card. A5 Exhibition Guide £1.00. Only assistance dogs are permitted in venue.
Amenities: free parking in adj. High Street car park and at rail station courtesy of APCOA and Network Rail. 5-minute walk from rail and bus stations. Easy wheelchair access. Periscopes offered for a £5 returnable deposit. Refreshments.
Features: layouts incl. *Ditchling Green* (O & 0-16.5); *St Oswalds* (0-16.5); *East Lynn Quay* (S); *Flintfield* (featured in this RM issue), *Fryers Lane, Laxfield, Pwllheli* (P4); *Janes' Creek* (EM); *Roman Road, Rye Sands, Sheep Dip, Shepherdswell EKR* (OO); *Devil's Bridge* (009); *Broken Creek* (HO USA); *Les Caves du Roy* (HO Lebanon); *Linfield* (3mm Finescale); *Callaton, Ley Hill, Mrs Wilberforce's Yard* (2mm Finescale) *Todmorden Mendip* (N). Trade support.
Contact: Adrian Colenutt, 01903 745759.
Email: enquiries@uckfieldmrc.co.uk
Website: www.uckfieldmrc.co.uk
Facebook: www.facebook.com/UckfieldModelRailwayClub

Sunday 19 October

POOL, Redruth, Cornwall

Event: Cornwall Model Railway Exhibition.
Organiser: Cornwall O Gauge Group.
Venue: Carn Brea Leisure Centre, Station Road, Pool, Redruth, Cornwall TR15 3QS.
Open: 1000-1600
Admission: adults £7.00, family £10.00.
Amenities: free parking, wheelchair access, refreshments.
Features: 15 layouts, demos, trade stands.
Email: secretary@cornwallOgauge.org.uk
Website: www.cornwallOgauge.org.uk/site/

Sunday 19 October

POOLE, Dorset

Organiser: Poole & District Model Railway Society sponsored by Model Railway Solutions.
Venue: Corfe Hills School, Higher Blandford Road, Broadstone, Poole BH18 9BG.
Open: 1000-1630
Admission: adults £8.00, acpd U16s free (cash or card taken on the door).
Amenities: free car park. Wheelchair friendly venue. Light refreshments available.
Features: layouts incl. *Harlyn Pier* (O); *Alstone Quay, Gretton & Wenlock Rly* (0-16.5); *Cwm Caradoc, Express Daisy Sidings, Portskerra* (EM); *Beattock Summit, Children's Layout, Fence Houses, Millburn, Piddle Regis* (OO); *Red Hook Bay* (HO USA); *Shaftesbury Parkway* (N); *Lightening Ridge* (Z USA). Plus demos and trade & society stands.
Contact: Exhibition Managers Steve Cope or Derek Orchard via email address below.
Email: pdmrs.exhibition@yahoo.com
Website: www.pdmrs.com

Saturday 25 October
BRADFORD, West Yorkshire
Event: Eccleshill Tinplate Vintage and Live Steam Model Railways Fair.
Organiser: Eccleshill Mechanics Institute.
Venue: Eccleshill Mechanics Institute, 93B Stone Hall Road, Town Centre, Bradford BD2 2EW.
Open: 1000-1600
Admission: charity event, with all proceeds going to the Eccleshill charity.
Amenities: free car park nearby. Commercial kitchen selling food and drinks all day.
Features: live steam railway models; tinplate Gauge 1 electric layout; Bassett-Lowke Northern electric O Gauge layout. Plus sales tables with vintage Bassett-Lowke, Leeds Model Company, Bonds and other vintage toys, or new O gauge electric locomotives and rolling stock.
Contact: to book sales table, call John, 07973 410342. Table £15.00 incl. sandwich & drink.
Email: john@jelmechanical.co.uk
Facebook: www.facebook.com/story.php?story_fbid=913207091009825&id=100069616260044

Saturday 25 October
COVENTRY, West Midlands
Event: COVGOG Open Day.
Organiser: Coventry & Warks O Gauge Modellers.
Venue: The Church of the Holy Cross, St Austell Road, Wyken, Coventry CV2 5AE.
Open: 1000-1500
Admission: free.
Amenities: refreshments available.
Features: club's full test track will be up and running, including a scenic section based on Water Orton station, so bring along stock to run. Also two layouts, demonstration on 3D printing, a bring and buy table, a display of members' models and two traders.
Contact: 02476 313221.
Email: clifton.p1@sky.com
Website: www.covgog.co.uk

Saturday 25 October
EXETER GARDEN RAILWAY SHOW
EXETER, Devon
Organiser: South Devon Garden Railway Group.
Venue: Matford Centre, Matford Park Road, Marsh Barton Industrial Estate, Exeter EX2 8FD.
Open: 1030-1630
Admission: adults £12.00, acpd U14s free.
Amenities: free parking & level access. Refreshments available.
Features: live steam & electric layouts incl. *Blackgang Amalgamated Railway*, Gauge 3 Society (Gauge 3); Devon Area G Scale, *Hambleden Valley, Whiteleaf Rack Railway* (G scale); Central Somerset & Two Moors Group, *Dreckly, Ffordd y Deryn*, Hampshire Area Group – 16mm Association, *Lyme Bay, Mwch Grumblyn, Phurcombe Hall & Phurcombe Hall Static* (16mm scale); *Morton Road* (Gauge 1); plus *Kinder Bahn 2* vintage Playmobil fun for children as well as Newton Abbot MES & Tiverton MES displays of miniature live steam locos, etc., and Westland & Yeovil MES display of locos in gauges 5" to 32mm. Trade stands.
Contact: 01548 550077.
Email: show.coordinator@sdgrg.org.uk
Website: www.exetergardenrailwayshow.com

Saturday 25 October
NARROW GAUGE NOW
MANSFIELD, Nottinghamshire
Organiser: East Midlands NG Modellers Group.
Venue: Portland College, Nottingham Road, Harlow Wood, Mansfield NG18 4TJ.
Open: 1000-1600
Admission: adults £10.00 (cash only), acpd children free.
Amenities: free car parking. Disabled friendly venue. Refreshments.
Features: 30-plus exhibits incl. *Melin Llechi* (SM32); *Penrhyn Quarries* (10mm scale on 16.5mm gauge); *Chesterfield Road, Whiteoak Light Railway* (0-16.5); *Jubilee Works* (0-14); *Bunkers Lane, Derwent Road* (09); *5.5mm Modular Group* (12mm gauge); *Bardsea Quay* (009/EM); *Angst-Lesspork, Blackmoor, Millthorpe, Nant Gwernol* (009). Demos, layout design clinic, trade stands.
Email: narrowgaugenow@hotmail.com
Website: www.narrowgaugenow.uk
Facebook: www.facebook.com/groups/3052081245086394

Saturday 25 October
WHITSTABLE, Kent
Event: Whitstable Model Railway Exhibition.
Organiser: East Kent Model Railway Society (Whitstable).
Venue: Swalecliffe & Chestfield Community Centre, 19 St John's Road, Swalecliffe, Whitstable, Kent CT5 2QU.
Open: 1000-1600
Admission: adults £5.00, acpd U16s free.
Amenities: free parking on roads. Venue is a short walk from Chestfield & Swalecliffe station and Stagecoach East Kent buses stop outside.
Features: layouts incl. Paul Edwards' *Swanage*; Stuart Addison's *London Underground and Trams Layout*; and Martin Barnard's *Broomfield Pond*, plus a kit-building demo & trade stands.
Contact: Nick Evans, 07743 704547.
Email: nickevanscomms@sky.com
Facebook: www.facebook.com/EKMRSWhitstable/

Saturday 25 & Sunday October 26
ABERDEEN, North East Scotland
Organiser: Aberdeen Model Railway Club.
Event: Aberdeen MRC 2025 Exhibition.
Venue: Curl Aberdeen, Eday Walk, Aberdeen, Aberdeenshire AB15 6LN.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £8.00, children £4.00, family £20.00.
Amenities: free parking. Buses pass venue along the Lang Stracht from the city centre.
Features: layouts in a variety of scales and gauges plus traders & society stands.
Contact: 07768 266444.
Email: Info@aberdeenmrc.co.uk
Facebook: www.facebook.com/AberdeenModelRailwayClub/

Saturday 25 & Sunday 26 October
FESTIVAL OF STEAM & PETE WATERMAN'S RECORD-BREAKING *MAKING TRACKS*
ABERYSTWYTH, Ceredigion
Organiser: Vale of Rheidol Railway.
Venue: Vale of Rheidol Railway, Park Avenue, Aberystwyth, Ceredigion SY23 1PG.
Open: check railway website for details.
Admission: for Festival of Steam train fares visit the VORR website, where it is also possible to book hourly time slots between 1000 & 1600, on dates from Saturday 25 October through to Sunday 2 November, for viewing *Making Tracks* layout at an additional cost of adults £10.00, children (3-15) £5.00.
Amenities: railway car park (charges apply) is located off Park Avenue/Boulevard St Brieuc. A path connects the VORR station with Cambrian Coast Line & bus stations. For accessibility arrangements see website. Café at Aberystwyth station.
Features: marking Railway 200, VORR will showcase its steam locos and museum. Plus Pete Waterman's Guinness World Record-breaking 208'-long portable *Making Tracks* layout will go on display until Sunday 2 November with rolling stock from the steam age to modern era, and under supervision the public can use tablets to control trains. As evening falls, the line will also adopt a Hallowe'en atmosphere. A narrow gauge display will showcase Welsh railway history, and suppliers of models and scenic materials will be on hand to share insights.
Contact: 01970 625819.
Email: info@vor.wales
Website: https://vor.wales
Facebook: www.facebook.com/rheidolrailway/

Saturday 25 & Sunday 26 October
CHELTENHAM, Gloucestershire
Event: Autumn Model Railway Exhibition.
Organiser: Cheltenham GW Modellers Group.
Venue: St Margaret's Hall, Coniston Road, Off Windermere Road, Hatherley, Cheltenham, Glos GL51 3NU.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adults £6.00, children £2.00, family (2+2) £12.00. Note: cash only. Event is raising funds for Cheltenham Cobalt Unit.
Amenities: bus route D runs from the town centre and railway station to Windermere Road. Disabled access. Refreshments.
Features: layouts incl. *Snowdon NWNGR* (8mm scale/1:38); *Hanbig Sidings* (O); *Bond Lane, Portskerra* (EM); *Aimford End, Bristol St Philips, Honeycombe Leaze, Melton Park Crossing, St Luke's Road, Thomas drive yourself layout* (OO); *Bruckless, Co. Donegal* (OOn3); *Morley* (009); *Binegar* (N). Model road vehicles. Trade stands.
Contact: Mike Walker, 01242 517788.
Website: www.cheltmodrail.org.uk
Facebook: www.facebook.com/p/Cheltenham-Great-Western-Modellers-Group-100067525716491/

Saturday 25 & Sunday 26 October
LEEDS MODEL RAILWAY SOCIETY ANNUAL EXHIBITION
LEEDS, West Yorkshire
Organiser: Leeds Model Railway Society.
Venue: The Grammar School at Leeds, Alwoodley Gates, Harrogate Road, Leeds LS17 9NA.
Open: Saturday 1000-1700
Sunday 1000-1630
Admission: adults £10.00, acpd U16s free.
Amenities: ample free parking. Lunches and refreshments available.
Features: 20-plus layouts incl. *Chesters Yard, Arcadia Pennsylvania, Schnapsidee* (O); *Halifax King Cross* (P4); *High Peak Stone, Hartley Bank Screens, Hudson Street* (EM); *Wentworth Junction, Oakdale, Taylor Made Waste, Pennhelen Cove, South Millfields, Boomerang Lane, Two Goods, TGB Metals, Somers Spur, Remember Tri-ang* (OO); *Pine River* (HO); *Bluish TT* (TT:120); *Chipping Norton, Modbury* (2mm Finescale).
Contact: John Aldrick, 07771 691820.
Website: www.leedsmrs.org
Facebook: www.facebook.com/Leedsmodelrailwaysociety

Saturday 25 & Sunday 26 October
STOCKPORT, Greater Manchester
Organiser: Hazel Grove & District MRS.
Venue: Woodford (War Memorial) Community Centre, Chester Rd, Woodford, Stockport SK7 1PS.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £7.00, acpd children free to a maximum of two, otherwise children £3.00.
Amenities: free parking. Courtesy bus from Hazel Grove station (see website). Service bus Stagecoach 42B runs from Manchester through Parrs Wood, Cheadle and Bramhall to event. Refreshments available.
Features: club & visiting layouts. Trade stands.
Email: garylittlewood@hotmail.com
Website: www.hgdmrs.org.uk
Facebook: www.facebook.com/HazelGroveRailwayModellers/

Saturday 25 & Sunday 26 October
RAIL-EX TAUNTON 2025
TAUNTON, Somerset
Organiser: Somerset Railway Modellers Club.
Venue: Taunton School, Staplegrove Road, Taunton TA2 6AD.
Open: Saturday 1000-1700
Sunday 1000-1600
Admission: adults £15.00, concessions £13.00, U16s free.
Amenities: free on-site parking, level access & restaurant catering.
Features: 35 layouts incl. *Bradstock Loco, Castlebanks 77, Holmeshurst, Love Lane Stabling Point* (O); *Harton Gill* (P4); *Carterton, Llandyfai, Overlord, Rosebury Goods, Rush Green Colliery, West Harkenback* (OO); *Beat the Drum & The Shed, Feòrag* (009); *Central Alonzo, Klapping* (HO); *Kleine Albula* (HOm); *Cwm Bach & Llados Halt, Kings Park, Penworth, Westwick* (N). Trade stands.
Contact: Tony Ingalls, club secretary by email.
Email: ingall@btinternet.com
Website: www.railextaunton.uk
Facebook: www.facebook.com/RAILEXTAUNTON

Sunday 26 October
KIDDERMINSTER, Worcestershire
Organiser: Wyre Forest Model Railway Club.
Venue: Club Rooms, First Floor, 101 Comberton Hill, Kidderminster DY10 1QH.
Open: 1100-1600
Admission: free, but donations appreciated.
Amenities: light refreshments available.
Features: club open day with models including a junior layout and new O gauge layout. DCC and dc test tracks in N/OO available for visitors to use plus second-hand stall.
Contact: David Rook, 01902 843760.
Website: www.wyreforestmrc.com

Sunday 26 October
REEPHAM, Norfolk
Event: Whitwell & Reepham Railway O Gauge Day.
Organiser: Whitwell & Reepham Railway.
Venue: The Sidings Function Room, Whitwell & Reepham Station, Norwich, Norfolk NR10 4GA.
Open: 1030-1630
Admission: adults £3.00, U16s free.
Amenities: disabled access. Food & drink available from the regular weekend Sidings menu.
Features: 7mm scale layouts plus trade support and Norfolk Mardlers demonstrating kit-building skills. Brake van rides on standard gauge railway. 7¼" gauge Top Field Light Railway will hold Driver for a Fiver experiences.
Email: robertewles79@gmail.com
Website: https://whitwellstation.com
Facebook: www.facebook.com/WhitwellAndReephamRailway/

Sunday 26 October
UFFCULME, Devon
Organiser: Culm Valley Model Railway Club.
Venue: Old Well Garden Centre, Waterloo Cross, Uffculme, Cullompton EX15 3ES.
Open: 1000-1600
Admission: Free but donations welcome.
Amenities: garden centre with parking located close to Junction 27 of M5 just off A38. 'Railway Room' is adjacent to top car park.
Features: *Tiverton Junction* (30' long OO gauge model) and *Soldor*, home to a little blue engine, driven by visitors. Club layouts.
Website: www.cvmrc.weebly.com/tiverton-junction.html

Sunday 26 October
WIMBORNE, Dorset
Event: Annual O Gauge Model Railway Exhibition.
Organiser: Central Southern Gauge O Group.
Venue: Allendale Centre, Hanham Road, Wimborne Minster, Dorset BH21 1AS.
Open: 1000-1600
Admission: adults £7.00, acpd children free.
Amenities: pay & display parking. Wheelchair access. Restaurant.
Features: finescale O gauge and 7mm scale narrow gauge layouts, plus other 7mm scale displays and fine and coarse scale test tracks with dc & DCC operation. Trade stands.
Email: csgog@outlook.com
Website: www.csg0g.org

MEETINGS

Thursday 11 September – 1930
South East Essex Railway Society
Test-Track night – N, TT, 00, O and 1 – help to set up from 1900 welcome as always.
Venue: Friends' Meeting House, 18 Dundonald Drive, Leigh on Sea, Essex SS9 1NB.
Details from: www.seers-rail.org/programme.html or seers.rail@gmail.com

Friday 12 September – 1900
Stephenson Locomotive Society
'All change at Grantham: photographs from the 1960s' presentation by John Clayson.
Venue: The Conference Room, 1st floor (access via Black Swan Court Yard), Newcastle Arts Centre, 67 Westgate Road, Newcastle upon Tyne NE1 1SG.
Details from: www.slsnewcastle.org.uk

Tuesday 16 September – 1400
Stephenson Locomotive Society
'Northern Rails East Of The Pennines' presentation by Steve Armitage.
Venue: Langdon Square Community Centre, Coulby Newham TS8 0TF.
Details from: 01429 270591.

Tuesday 16 September – 1930
The Leicester Railway Society
'Doncaster to Hull via Goole' by Stephen Gay.
Venue: The Braunstone Civic Centre, 209 Kingsway, Leicester LE3 2PP.

Entry: £5.00.
Contact: Brian Holland, 0778 570 7573
Email: bryanholland60@aol.com
Website: www.leicesterrailwaysociety.com

Wednesday 17 September – 1930
The Grimsby & Cleethorpes Railway Group
'The Old Dalby Test Track' and 'Railways from both sides of the fence' by Jason Cross.
Venue: The No.1, Station Approach, Cleethorpes DN35 8AX.
Entry: £3.00.
Contact: Stuart Blackbourn, 01472 505853 or 0774 596 1538
Email: sc.blackbourn@ntlworld.com

Thursday 18 September – 1930
Lutterworth Railway Society
LRS Model Running Evening.
Venue: The Pavilion, Recreation Ground, Coventry Rd, Lutterworth LE17 4RB.
Entry: members £2.00, non-members £5.00.
Contact: Alan Bilton, 01455 207996 or mobile 07771756013.
Website: www.lutterworthrailwaysociety.co.uk

Thursday 18 September – 1930
Marlow & District Railway Society
Rail Industry Update by Mark Hopwood (GWR).
Venue: Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or on Zoom.
Entry: £3.00 non-members, free for members.
Contact: Martin Stoolman, 07954 849390.
Website: www.mdrs.org.uk

Thursday 25 September – 2000
South East Essex Railway Society
'150 years of the London Underground' presented by Adrian Wright.
Venue: Friends' Meeting House, 18 Dundonald Drive, Leigh on Sea SS9 1NB.
Details from: www.seers-rail.org/programme.html or seers.rail@gmail.com

Thursday 2 October – 1930
Lutterworth Railway Society
'Scenic Britain by Train' by Stephen Gay.
Venue: The Pavilion, Recreation Ground, Coventry Rd, Lutterworth LE17 4RB.
Entry: members £2.00, non-members £5.00.
Contact: Alan Bilton, 01455 207996 or mobile 07771756013.
Website: www.lutterworthrailwaysociety.co.uk

Thursday 2 October – 1930
The Great Central Railway Society (Sheffield)
'Midland Wanderings' by Mike Eggenton.
Venue: The Farm Road Sports & Social Club, Farm Road, Sheffield S2 2TP.
Entry: £4.00.
Contact: Bob Gellatly, 07519 011213 or gcrs@talktalk.net
Website: www.gcrsociety.co.uk

Friday 3 October – 1900
Stephenson Locomotive Society
'The G5 Project' presentation by Dr Mike Wood.
Venue: Nunthorpe Methodist Church, Connaught Road, Nunthorpe, Middlesbrough TS7 0BP.
Details from: 01429 270591.

Monday 6 October – 1930
Peak Rail Association (Sheffield Branch)
'Railway Rainbow' & 'Fifty Shades' by Gavin Lake.
Venue: The Farm Road Sports & Social Club, Farm Road, Sheffield S2 2TP.
Entry: £5.00.
Contact: Dave Sharp, 07487 685306 or davidsharpsheffield@gmail.com

Wednesday 8 October – 1915
Stephenson Locomotive Society
'Coastal Railways' by Steven Williams.
Venue: Abbott Hall Social Centre, Dowker Lane, Kendal LA9 4DN.
Details from: martyn.hill4472@hotmail.com

Thursday 9 October – 1930
South East Essex Railway Society
Test-Track night – N, TT, 00, 0 and 1 – help to set up from 1900 welcome as always.
Venue: Friends' Meeting House, 18 Dundonald Drive, Leigh on Sea, Essex SS9 1NB.
Details from: www.seers-rail.org/programme.html or seers.rail@gmail.com

Friday 10 October – 1900
Stephenson Locomotive Society
'The Northern Belle – Cruising in style by the LNER' by Sam Woods & Richard Barber.
Venue: The Conference Room, 1st floor (access via Black Swan Court Yard), Newcastle Arts Centre, 67 Westgate Road, Newcastle upon Tyne NE1 1SG.
Details from: www.slsnewcastle.org.uk

Monday 13 October – 1930
Lincoln Railway Society
'Railways in a Cornish Landscape – Part 1' by Stephen Gay.
Venue: St Hugh's Church, Harewood Crescent, North Hykeham, Lincoln LN6 8JG.
Entry: £4.00. Non-members welcome.
Contact: Publicity Member, 01522 698085 or email grahamlightfoot1948@gmail.com

Wednesday 15 October – 1930
The Grimsby & Cleethorpes Railway Group
'Scottish Railways through the Years' by Mike Fish.
Venue: The No.1, Station Approach, Cleethorpes DN35 8AX.
Entry: £3.00.
Contact: Stuart Blackbourn, 07745 961538 or sc.blackbourn@ntlworld.com
Website: www.gcrsociety.co.uk

Thursday 16 October – 1930
Lutterworth Railway Society
'Florida Key West Extension' by Tom Ingall.
Venue: The Pavilion, Recreation Ground, Coventry Rd, Lutterworth LE17 4RB.
Entry: members £2.00, non-members £5.00.
Contact: Alan Bilton, 01455 207996 or mobile 07771756013.
Website: www.lutterworthrailwaysociety.co.uk

Thursday 16 October – 1930
Marlow & District Railway Society
'The Great Northern in Ulster 1957-1965' by Leslie McCallister.
Venue: Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or on Zoom.
Entry: £3.00 non-members, free for members.
Contact: Martin Stoolman, 07954 849390.
Website: www.mdrs.org.uk

Tuesday 21 October – 1400
Stephenson Locomotive Society
'Railways Over The Stainmore Pass' presentation by E Chris Rowley.
Venue: Langdon Square Community Centre, Coulby Newham TS8 0TF.
Details from: 01429 270591.

Tuesday 21 October – 1930
The Leicester Railway Society
'Another Look at North Eastern Railways' by John Brigstock and Michael Chapman.
Venue: The Braunstone Civic Centre, 209 Kingsway, Leicester LE3 2PP.
Entry: £5.00.
Contact: Brian Holland, 0778 570 7573 or bryanholland60@aol.com
Website: www.leicesterrailwaysociety.com

Thursday 23 October – 2000
South East Essex Railway Society
'The story of HMS Hood, the Royal Navy battlecruiser' presentation by member Eddie Hardiman followed by a railway quiz.
Venue: Friends' Meeting House, 18 Dundonald Drive, Leigh on Sea SS9 1NB.
Website: www.seers-rail.org/programme.html or seers.rail@gmail.com

Tuesday 28 October – 1930
South East Essex Railway Society
Test-Track night – N, TT, 00, 0 and 1 – help to set up from 1900 welcome as always.
Venue: Friends' Meeting House, 18 Dundonald Drive, Leigh on Sea, Essex SS9 1NB.
Details from: www.seers-rail.org/programme.html or seers.rail@gmail.com

CLUB NEWS/ NOTICES

Bedfordshire 16mm Narrow Gauge Modellers meet on the first Sunday of the month 1030-1600 at Eaton Bray Village Hall, near Dunstable. New members welcome. Two layouts for 16mm live steam and electric locos in 32mm and 45mm gauges. Go along, visit www.bag16mm.org.uk or email membership@bag16mm.org.uk

Berkshire N Gauge Society meets at 1900 on Thursdays at West London Aero Club, White Waltham Airfield, Maidenhead SL6 3NJ. New members welcome. Email: tornado.gr.1@hotmail.co.uk or percy.richard1946@gmail.com

Bilton Grange Model Club is a recently formed group in east Hull, with members interested in 00, Tri-ang TT and 0 gauge plus some in military modelling, aircraft, tanks, ships and Warhammer. The group meets every second Tuesday in the month from 1900-2100 at the Bilton Grange Community Association building opposite The Grange pub in Grange Road, east Hull. Call 07748 406806 or email biltongrangemodelclubb@yahoo.com

Bridgend Model Railway Group meets weekly on Thursdays at 1900 upstairs at the Laleston Gardener's Club, Cliff Cottage, Wind St, Laleston CF32 0HN. Group has two layouts in 7mm and one in 4mm. Visit www.bridgendmodelrailwaygroup.co.uk or email bridgendmodelrailwaygroup@protonmail.com

Group for anyone interested in railway modelling and/or railways meets in **Bubwith Leisure Centre bar, Main Street, Bubwith, Selby YO8 6LX** at 1930 on the last Monday of each month. Email: steve.grantham1@btinternet.com

Camborne Railway Modelling Group is a new club in Cornwall that meets every Wednesday from 1700-2100 at Camborne Community Centre, 9 South Terrace, Camborne TR14 8SU and welcomes members of all abilities and ages (but U18s must be acpd by a responsible adult). Members can bring their own projects or help with the club layout. There is a charge of £3.00 a visit for room rent and light refreshments. Email cambornerailwaymodellinggroup@gmail.com or telephone Wayne on 07469 944847.

East Sussex Gauge 0 Group – Interested in modelling 0 gauge or 7mm narrow gauge? This group's meetings at Pett Village Hall (just outside Hastings) are held on the 2nd Friday of each month from 1900-2000. The test track can run both fine and coarse scale two or three-rail, dc or DCC rolling stock. See Gauge 0 Guild website or email mikeh.reeves@tiscali.co.uk

Haverfordwest Model Railway Club, formerly Pembrokeshire Model Railway Club, meets weekly on Wednesdays at 1915 upstairs at Haverfordwest Menshed, at The Old Print Works, Castle Square, Haverfordwest SA61 2AA. Attend meetings or join. Visit www.facebook.com/groups/1448793075977538/ or email: wordpress@pmrc.welshbrook.com

Kingsway Model Railway Society welcomes new members when it meets every Tuesday from 1900-2100 at the Kingsway Centre, 130 Kingsway, Wellingborough, Northants NN8 2HF. Visit website www.kmrs.org.uk, visit Facebook Kingsway Model Railway Society or send an email to secretary@kmrs.org.uk

Launceston Model Railway Group holds drop-in meetings on the 3rd Saturday of each month 1000-1400 at St Stephen's Church Hall, Roydon Road, Launceston PL15 8HL. All ages welcome but children must be acpd. 0, 00 and N layouts in dc & DCC. Call 07974 893172 or email Alan & Sally Barriball at: furzepark@hotmail.com

Leyland Model Railway Club meets at Hillthorpe Farm, Knoll Lane, Little Hoole PR4 4TB every Monday and Wednesday (except Mon bank holidays) from 1900-2200. Layouts in 00 and N plus others being built. Call 07796 130295.

Morecambe & Heysham Model Railway Club meets at 9 Morecambe Street West, Morecambe LA4 5HE on Tues and Thurs from 1900-2100 and open days on second Saturday of each month from 1000-1600, with last admission at 1530. Small admission charge but children free. Layouts in 0, 00 (one DCC), N & 0-16.5. Venue has a disabled accessible toilet and a kitchen for open days. Gerry Blaikie, 01524 425523.

New Forest Model Railway Society will be offering 3 months' free membership to those visiting the 50th anniversary exhibition at Brockenhurst Village Hall SO42 7RY on Sunday 30 November, and also to readers of RAILWAY MODELLER. Offer will run until the end of Feb 2026, with no obligation to join. Members meet Tues evenings at Brockenhurst Village Hall, with layouts in N & 00, dc & DCC. Visit www.nfmrs.org

In mid-March, **Northampton and District Model Railway Club** moved to a new venue. Members now meet at Roade Village Hall, Bailey Brooks Lane, Roade, Northampton NN7 2LS on Friday evenings from 1900-2200. Club welcomes new members. For details visit www.ndmrc.org

North Wales Small Scale Model Railway Club (formerly N Gauge Society North Wales Group Conwy) meets at the Sea Cadets Hall, Deganwy, Conwy, on the first and third Monday of each month at 1830-1900. Call 07796 640131 or email oakleygrange@hotmail.co.uk

Poole and District Model Railway Society has opened a larger clubroom off Poole High Street. Includes 00 gauge *Sterte Bay* (under construction), British/American N gauge layout and 00 & N test track. Bring locomotives and/or stock to try out. Workshop and refreshment area. Tuesdays 1900-2200. See www.pdmrs.com

Romiley Methodist Railway Modellers meet from 1400 onwards every Wednesday in clubroom behind Romiley Methodist Church. Wide range of layouts in N to 0 gauge. See rmrmclub.com or contact rmrmhonsec@yahoo.com

Seaton Carew Model Railway Club welcomes new members and enthusiasts, meeting on the third and fifth (where applicable) Saturday of every month at Salvation Army Hall, Park Road, Hartlepool TS26 9HT from 1200-1700 with 00 and N gauge layouts plus 00, TT and N gauge test tracks. All welcome (U16s should be acpd by adult). See https://seatoncarewmrc.byethost8.com or email contact.scmrc@gmail.com

Sidmouth Model Railway Group welcomes enthusiasts to its meetings and has layouts (N, 00 and 0) in progress for all wishing to be part of an active group. Visit www.sidmouthmrg.com or email: sidmouthmrg@gmail.com

Tamar Valley Model Railway Club meets at Bere Alston Bowling Club, Bedford Street, Bere Alston PL20 7DZ, on 3rd Wednesday of month, Jan to Nov, and on 2nd Wednesday in Dec, 1000-1600. Launceston & Cornwall N Gauge Enthusiasts attend. N, 00, 00 3-rail and 0 layouts. Members can buy tea, coffee & home-made cakes and cooked lunches. Les Whaley, 07972 333202.

Tonbridge Model Railway Club welcomes new members with meetings at the Christ Church Centre, High Street, Tonbridge from 1900 each Thursday. Access is by the rear Church entrance (opp. petrol station). Parking is in the main car park outside Sainsburys, TN9 1SF. Club has layouts in 0 and 0.16.5/09 plus 4mm scale. Next exhibition 21 February 2026. Find club @TonbridgeMRC or email tonbridgemrc@yahoo.co.uk

West Midlands N Gauge Club is in its 30th year and is seeking new members. The club meets on the 1st and 3rd Saturdays in the month at Tipton Green Methodist Church, Park Lane West, Tipton DY4 8LD, 1400-1700, including coffee and cake. There are four working layouts and two new projects are under way. For photos and further details visit www.wmngc.co.uk

Wisbech Model Railway Club, formed in 2005 by Brian Baylis, is marking 20 years of modelling history. Members meet on Thursdays at 1900-2100, now relocated to the ground floor of Wisbech Social Club and Institute, 15 Hill Street, Wisbech PE13 1BA (through the front door to the left). New and returning members of all skill levels welcome; U16s must be acpd by a parent or guardian. Two N gauge club layouts and 00 gauge layout, all in dc. Contact chairman Jack Daniels at jack_daniel5@icloud.com or layout manager Richard on 07702 644066.

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Trade

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HULL – EAST YORKSHIRE'S ONLY SPECIALIST model railway shop. Model Railways bought, sold and exchanged. *53A MODELS, 430 Hessle Road, Hull.* (Closed Mondays). *www.53amodels.co.uk. Tel: 01482 227 777.*

COLWYN BAY, CLWYD MODELS. Opening times: Thursday, Friday + Saturday 10-1pm, 2-5pm. Second hand model railway books, videos, layouts, diecast bought, sold and exchanged. *447, Abergele Road, Old Colwyn, Colwyn Bay LL29 9PR Tel: 01492 518 709, keith@clwydmodels.co.uk*

PLUS DAUGHTERS OF BASILDON ESSEX. N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. *PLUS DAUGHTERS, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon, Essex, SS13 1EU. Tel: 01268 726 211 www.plusdaughters.co.uk*

DINGWALL (ROSS-SHIRE)-SPORTS & MODEL SHOP, 66 High Street, Dingwall, IV15 9RY. PLASTICS-METALS-SCENICS. OO and N scale, PECO, Hornby, Bachmann, Metcalfe, Wills, Ratio & Dapol. *Tel: 01349 862 346.*

SWANAGE MODEL RAILROADING CENTRE – N GAUGE TO G SCALE. Good selection of Preowned models in N, OO and O gauge. Open Wednesday, Thursday and Saturday 9.30 to 2pm. *Other times by appointment please, call John on 07956 973 072.*

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NEW WEBSITE: www.zetlandmodelrailways.co.uk Good quality second-hand North American, Continental & British, "O", "HO", "OO", "N" & "Z" scales. *Email Bob@zetlandmodelrailways.co.uk*

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Trade Sales Books

RAILWAY MODELLER BACK NUMBERS – for more information on which issues are available, please contact: *Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580*. Please see website for prices.

Trade Services

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Wanted (Trade)

This section is reserved for the use of traders only.

MODEL RAILWAYS WANTED – cash waiting for British & American N & OO/HO scales plus Continental HO model railway equipment. British and American 0-Scale also considered along with interesting items of railwayana. (Closed Mondays). *53A Models, 430 Hessle Road, Hull. 01482 227 777. www.53amodels.co.uk*

AIRFIX/FROG/REVELL and other makes of unmade Second-hand plastic kits wanted. Aircraft especially wanted. Any quantity. *KINGKIT, Unit 8, Cedar Court, Halesfield 17, Telford, TF7 4PF. Tel: 01952 586 457.*

WANTED – TRAIN COLLECTIONS O, OO, N. Always calling in areas SOMERSET, DEVON, CORNWALL, S-WALES, WILTS, HAMPSHIRE, most places in UK covered. Will dismantle layouts any size. Interested in British N gauge Farish, Minitrix, Peco, OO Bachmann, Hornby, Mainline, Lima, Wrenn, Tri-ang. Damaged, scrap locos, Peco points, track work, buildings, cars, people, books, diecast, EFE buses. Cash paid, friendly service offered. *R.FOSTER 15 Rosebery Avenue, Yeovil, Somerset BA21 5LW. Tel 01935 424165 email russelltrains@aol.com*

MODEL RAILWAYS ALWAYS WANTED, including Hornby, Lima, Bachmann, Dapol, Modern Image, OO, O, N gauge, kits and kit built. Send lists or give me a call. *BARRY JONES, 28 Marine Crescent, Worthing, BN12 4JF. Tel/Fax: 01903 244 655.*

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WANTED WANTED MODEL RAILWAYS, ALL MAKES, ALL SIZES from N Gauge, 00 scale to G Scale and larger, will travel all UK. We also dismantle layouts, over 35 year's experience. *Send lists to DEREK BARNETT, 6 Vale Gardens, Penkridge, Staffs, ST19 5LQ. Tel: 01562 750 076 daytime, or text me 07721 333 521 anytime and I will ring you back, email footplate@btconnect.com*

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Email: rogersmith200@hotmail.com

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The following announcements are included in good faith from information provided by the organisers.
As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so.
All advertisements include the organiser's town and telephone number.
The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

September 2025

21 September (Sun) 10am-2pm
POTTERS BAR TOY & TRAIN FAIR, Elm Court Youth and Community Centre, Mutton Lane, EN6 3BP. *Telephone Colin on 01279 301 407 or Joe 07866 641 215.*

27 September (Sat) 10am-2pm
BRENTWOOD, The Brentwood Centre, Doddinghurst Road, Brentwood Essex CM15 9NN. Adults (16+) £4, Seniors £3.50, accompanied children free. Organised by J&J FAIRS.
Tel: 01522 880 383 www.j-jwebbtoyfairs.com jandjfairs@gmail.com

27 September (Sat) 10am-2pm
DERBY (TRAINS ONLY) SWAPMEET, Our Lady of Lourdes Parish Centre, 36 Uttoxeter Road, Mickleover, Derby DE3 9GE. Admission £2, free car parking, refreshments available. *Contact: PETER STANTON.*
Tel: 07798 858 532,
email: peterstanton1000@gmail.com

27 September (Sat) 10.30am-2pm
LUDLOW RACECOURSE Bromfield, Ludlow, Shropshire SY8 2BT, (off A49), admission £2. TONY OAKES FAIRS.
Tel: 01270 652 773 & 07825 631 323.

October 2025

4 October (Sat) 10.30am-3.30pm
CREWE TOY & TRAIN COLLECTORS FAIR, Hopes and Beams, Broad Street, Crewe, CW1 3UD. 6ft tables £25. Enquiries; 93 Merlin Way, Crewe, CW1 3YP. *Tel: 01270 505781 (before 8pm).*

4 October (Sat) 9am-2.30pm
MAIDSTONE, at Lockmeadow Market Hall, Barker Road, Maidstone, Kent ME16 8HR. Admission £3.
Tel: 01732 840 787 or 01622 298 159.

4 October (Sat) 10am-2.30pm
THEYDON BOIS TRANSPORT BAZAAR VILLAGE HALL, THEYDON BOIS, COPPICE ROW, ESSEX CM16 7ER. Five minutes from Theydon Bois Station, Bus & Rail books, photos, DVD's, vinyl, maps, timetables, tickets, memorabilia. Admission £3 u16 free with adult. No Dogs.
Tel: 07949 356 706 e-mail: ttbazaars@btinternet.com

5 October (Sun) 10.30am-3pm
COVENTRY – 160 STALLS, The Connexion, Ryton on Dunsmore, Coventry, CV8 3FL. Lots of stalls full of new and used model railways – many at bargain prices. Adults £3.50, Seniors £3, Children £1.
Tel: 01604 846 688 www.bpfairs.com

5 October (Sun) 10am-3pm
IVANHOE MODEL RAILWAY SOCIETY Diecast, toy, and train fair.
The Mountsorrel Memorial Centre, 105 Leicester Road, Mountsorrel, Loughborough,Leicestershire, LE12 7DB.
50+ tables. Accessible venue. Light refreshments and free parking available. Admission: Adults £2, U16 Free.
www.ivanhoe-mrs.co.uk

5 October (Sun) 10am-2.30pm
MERTHYR TYDFIL TOY AND TRAIN FAIR, Merthyr Tydfil Leisure Centre, Merthyr Tydfil Leisure Village, Wales CF48 1UT. Loads of onsite parking, onsite food, wheelchair friendly.
Email: pauldean020658@gmail.com telephone: 07807 038 161.

9 October (Thurs) 7pm-9pm
THEYDON BOIS, Theydon Bois Village Hall, Coppice Row (B172), Theydon Bois, near Epping, Essex CM16 7ER. Close to M25 J26 and M11 J5. Admission £1. Contact JOE LOCK.
Tel: 07866 641 215.

11 October (Sat) 10.30am-2.30pm
OSWESTRY – The Pavilion, Oswestry Showground, Park Hall, Whittington Rd SY11 4AS. JACKSON & JACKSON EVENTS.
For bookings contact DAVID on 07967 681 080 jacko17356@icloud.com

12 October (Sun) 10.30am-3pm
DONCASTER RACECOURSE, The Lazarus Exhibition Centre, Leger Way, Doncaster DN2 6BB. More than 350 stalls of toys, trains and models. Adults £6, Seniors £5.50, Children £1.
Tel: 01604 846 688 www.bpfairs.com

12 October (Sun) 10am-2pm
IPSWICH, Copdock Village Hall, London Road, Copdock, IP8 3JN. Light refreshments, good lighting.
Telephone: PHIL COOPER for bookings on: 01787 372 559 or BILL BOURNE on: 01787 280 452.

12 October (Sun) 9.45am-1.15pm
RAYLEIGH ESSEX, The Sweyne, Park School, Sir Walter Raleigh Drive, off London Road, SS6 9BZ. Adults £2.50, under 16 free, refreshments on site. KRJ VINTAGE TOYFAIRS.
Tel: 07977 646 386 & 07889 901 729

14 October (Tue) 6.30pm-8.30pm
HOOK TOY & TRAIN FAIR. Community Centre, Ravenscroft Rd, Hook RG27 9NN. Free parking, café, easy access from M3 & Hook railway station.
Admission £1.
stevedevizes@aol.com 07732 330 305.

19 October (Sun) 10.30am-3pm
STAFFORD – MORE THAN 300 STALLS. The Prestwood Centre, Stafford County Showground, Weston Road, Stafford, ST18 0BD. Adults £5, Seniors £4.50, Children £1. There are many stalls selling OO and O gauge trains.
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WICKHAM – Community Centre, Mill Lane, PO17 5AL. Relocation of the Eastleigh fair to this popular centre. Just off the M27 and handy for both Portsmouth and Southampton. Light refreshments and free parking.
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19 October (Sun) 11am-2pm
WINDSOR TOY & TRAIN FAIR, New Windsor Community Centre, Hanover Way, Windsor, SL4 5NW.
motorail3@virginmedia.com Telephone: 01279 301407

26 October (Sun) 9.45am-1.15pm
BROMLEY – Bromley High School, Blackbrook Ln, BR1 2TW. KRJ VINTAGE TOYFAIRS.
Tel: 07977 646 386 & 07889 901 729

28 October (Tues) 5pm-8pm
GARSTANG TOYFAIR – Catterall Memorial Hall PR3 0HR (at junction of A6 and A586). Refreshments, £1 entry. EMANUEL FAIRS.
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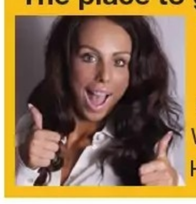

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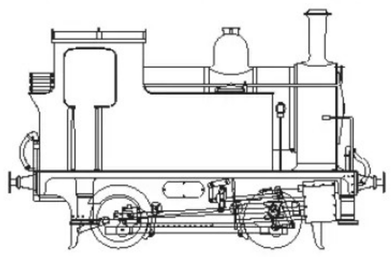

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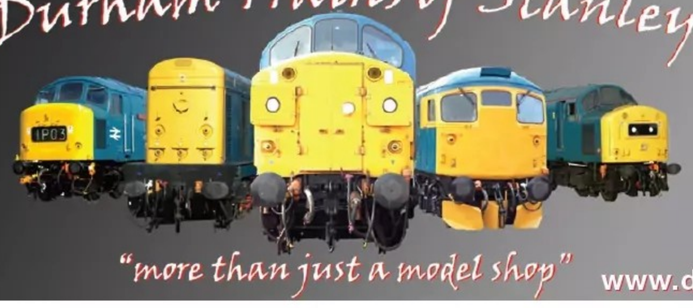

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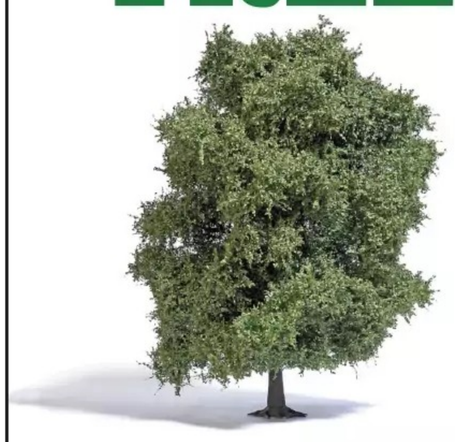

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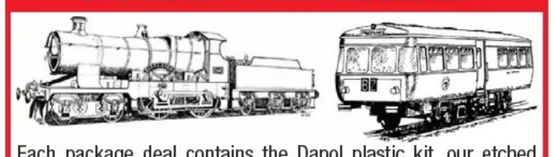

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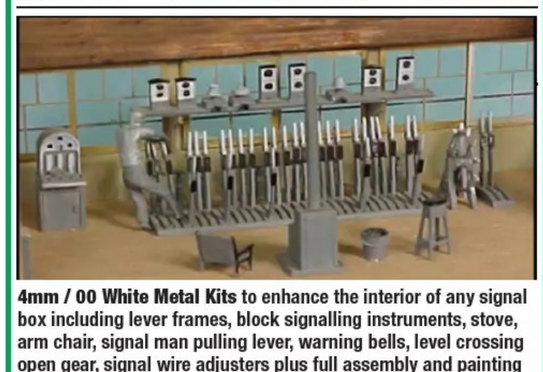

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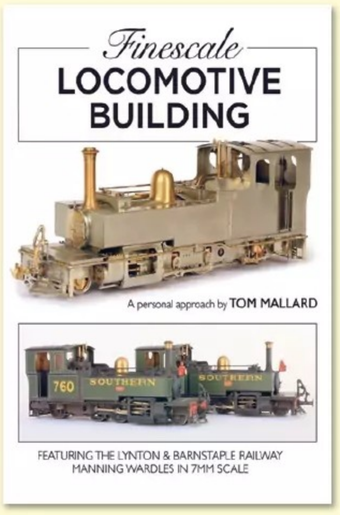

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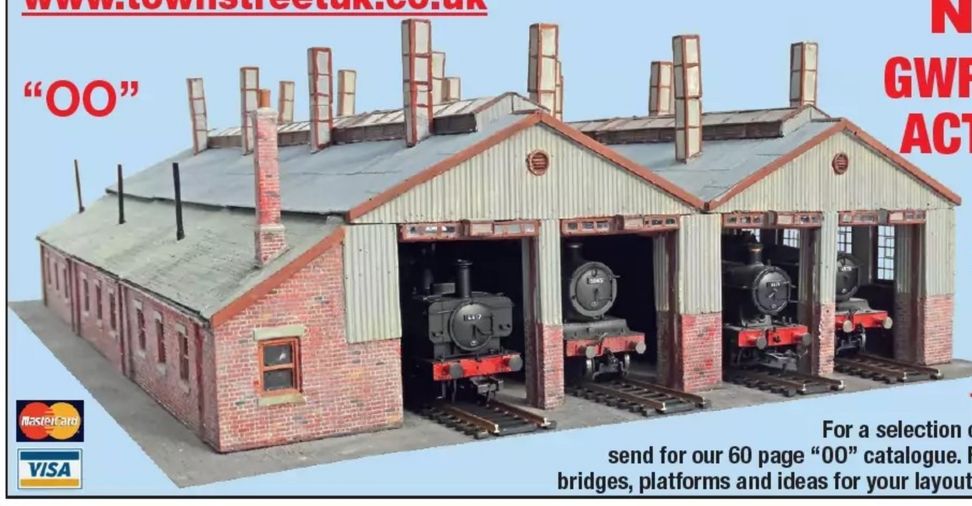

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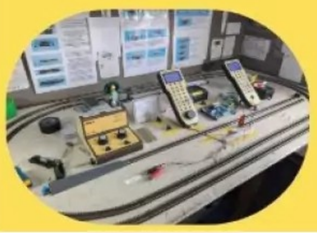

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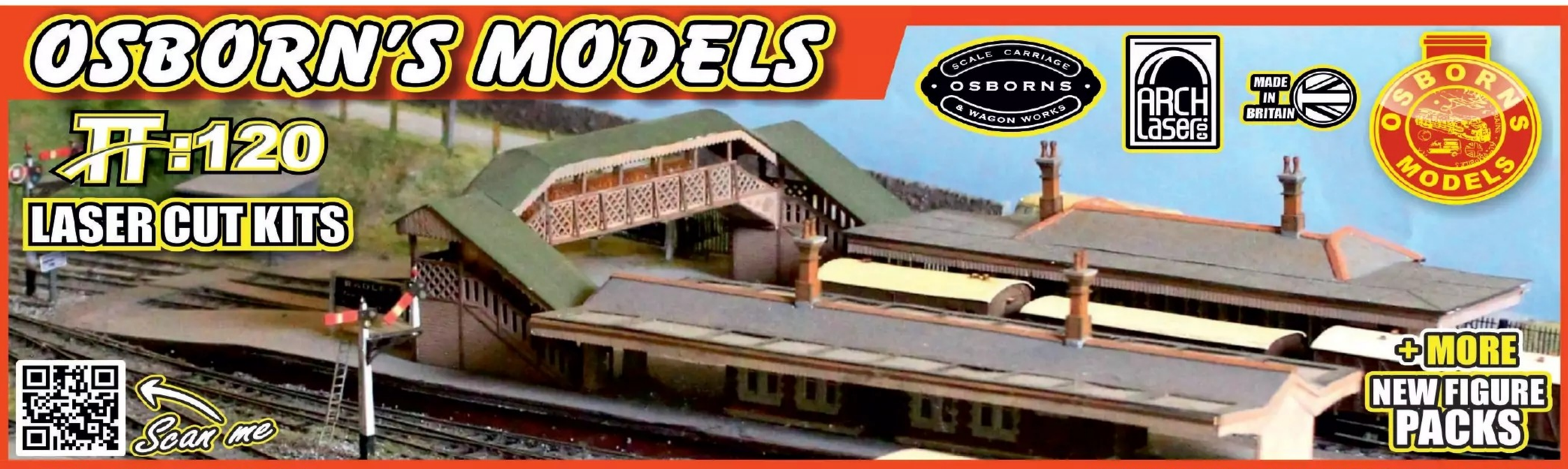

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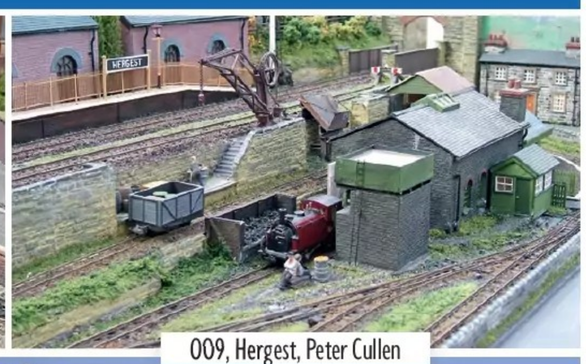

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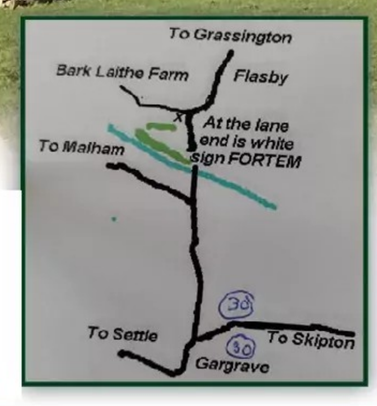

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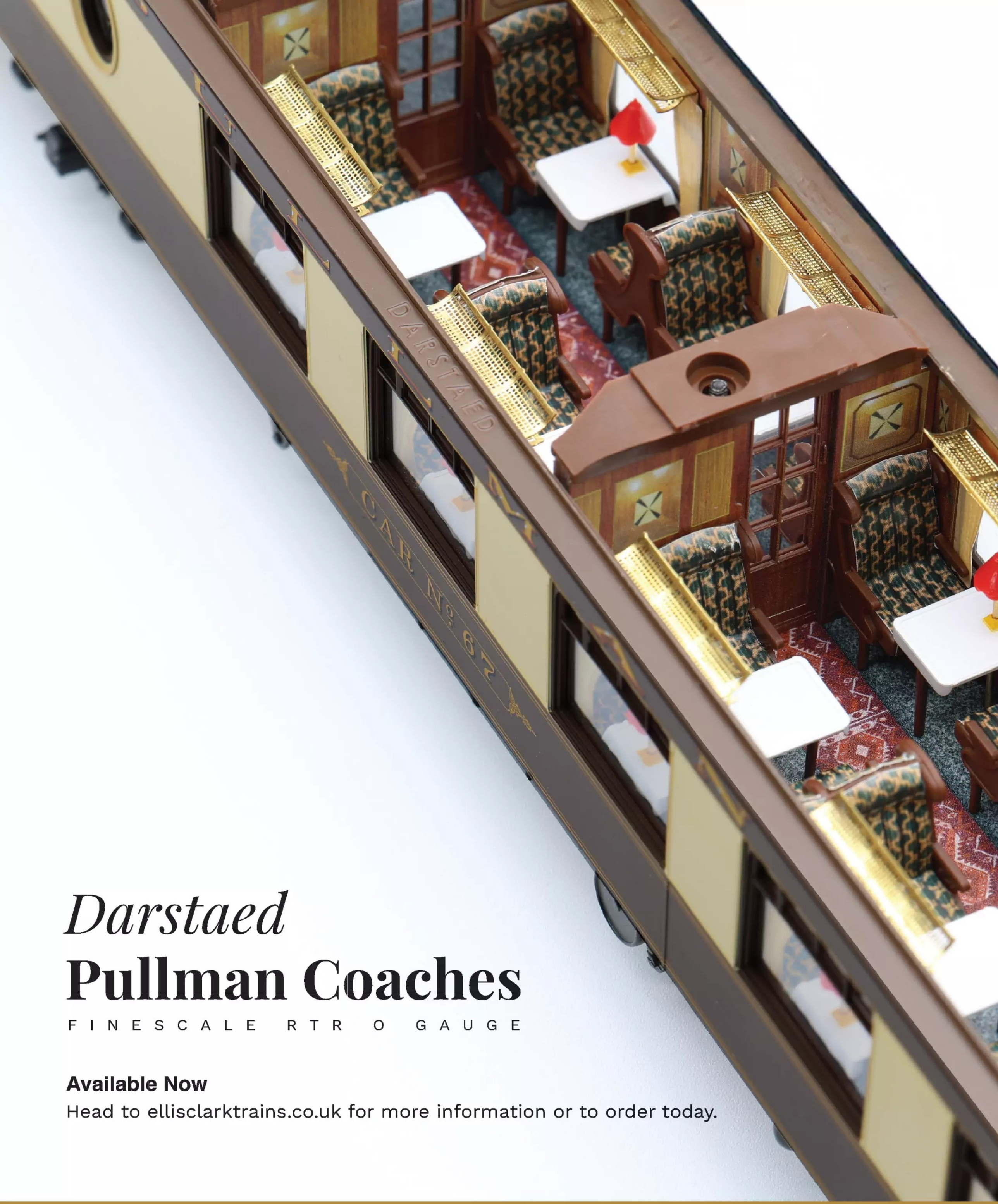

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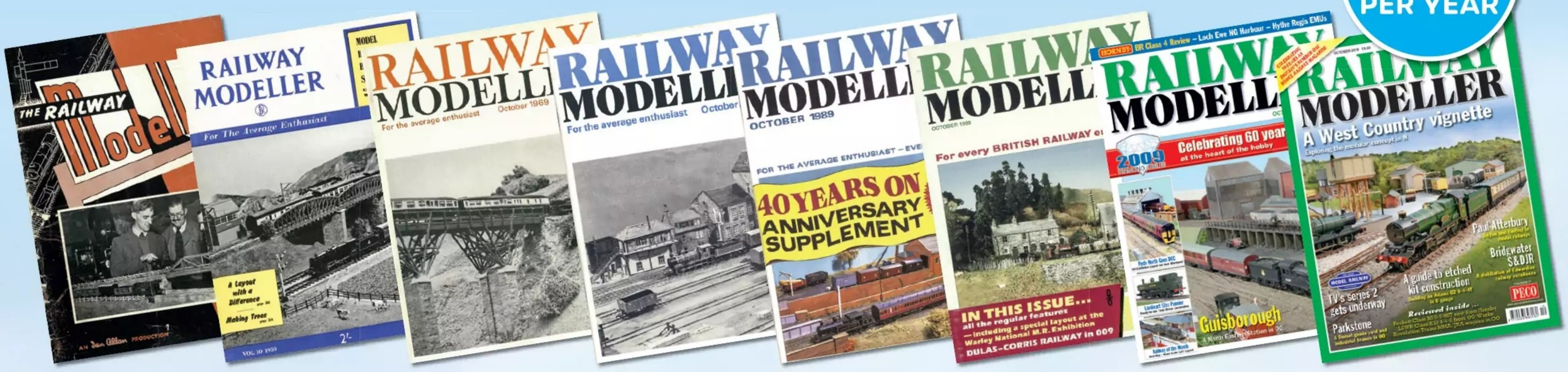

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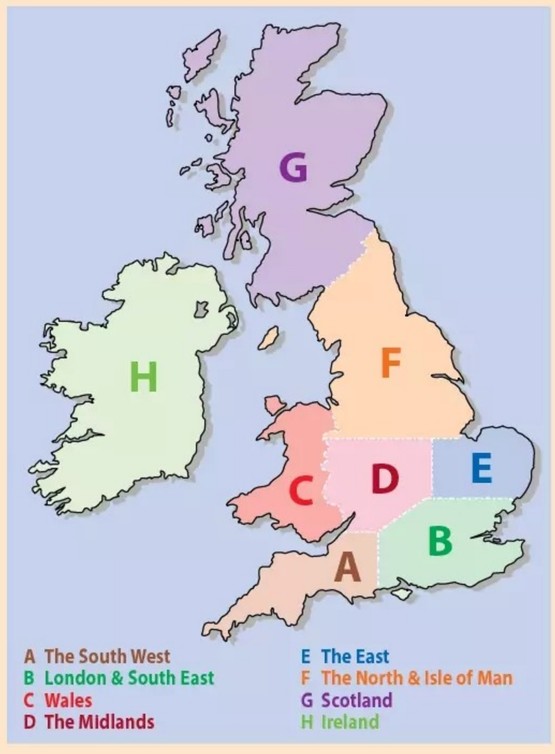

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