

**BRITAIN'S No.1
MODEL RAILWAY MAGAZINE**

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AT THE HEART OF THE HOBBY

VOLUME 73 No 862

RAILWAY MODELLER

**FREE
BOOKLET
WITH THIS
ISSUE**



North West steam in the garden

The realisation of
a childhood dream
in O gauge



North Ballachulish

A might-have-been Scottish
Highlands branch in 4mm

Rhapsody in blue!

Recreating experimental
liveries in N gauge

Spotlight on the Lancashire & Yorkshire

An introduction to this
pre-Grouping company

A station building in TT:120

Constructing a Peco kit in this
brand new scale for British outline

Reviewed inside ...

Peco bullhead crossing and slips for OO
New Graham Farish Class 319 EMU in N
TEA bogie wagons new from Cavalex Models in OO

Port Joyce

A BR steam and diesel scene
housed in a converted loft

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RAILWAY MODELLER

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North West steam in the garden Railway of the Month

Having dreamt for many years of one day recreating his childhood memories in O gauge, Peter Holland built this large twin-level system with outdoor running.

540 Talking Points

Topical issues from the world of railway modelling. This month, Chris Ford talks with Daniel Wells of Fourdees Ltd to find out about the story behind the firm and its ready-to-run 009 items.

542 North Ballachulish

Walking holidays inspired Andy Cooper to switch to modelling the Scottish Highlands and build a might-have-been Callander and Oban branch in 4mm scale.

548 Lancashire & Yorkshire Railway spotlight

To mark this year's centenary of the Lancashire & Yorkshire Railway's merger with the LNWR, Toby Jennings looks at the prospects for modelling the L&Y.

551 The Lancashire & Yorkshire 'Dreadnought' 4-6-0s

Scale Drawings

This month we present one of the company's most imposing locomotive designs, drawn and described by the late Ian Beattie.

554 Summerseat

Plan of the Month

Chris Ford outlines the history of this passing station on the former East Lancashire Railway route north of Manchester.

555 A postscript to the Thames- Forth Express

Project of the Month

Ian Nuttall used a Ratio Midland Railway clerestory coach kit as the basis of a MR 45' Full Brake to replace a less accurate model on his 4mm scale Settle & Carlisle-themed layout.

558 Southampton East goes West!

Geoff Green describes the latest extension to his 1980s/1990s Network SouthEast-inspired N gauge system.

564 Rhapsody in blue

Project of the Month

Attracted to the striking British Railways experimental blue livery schemes, Sacha Case set about creating a collection of examples in miniature by repainting ready-to-run models.

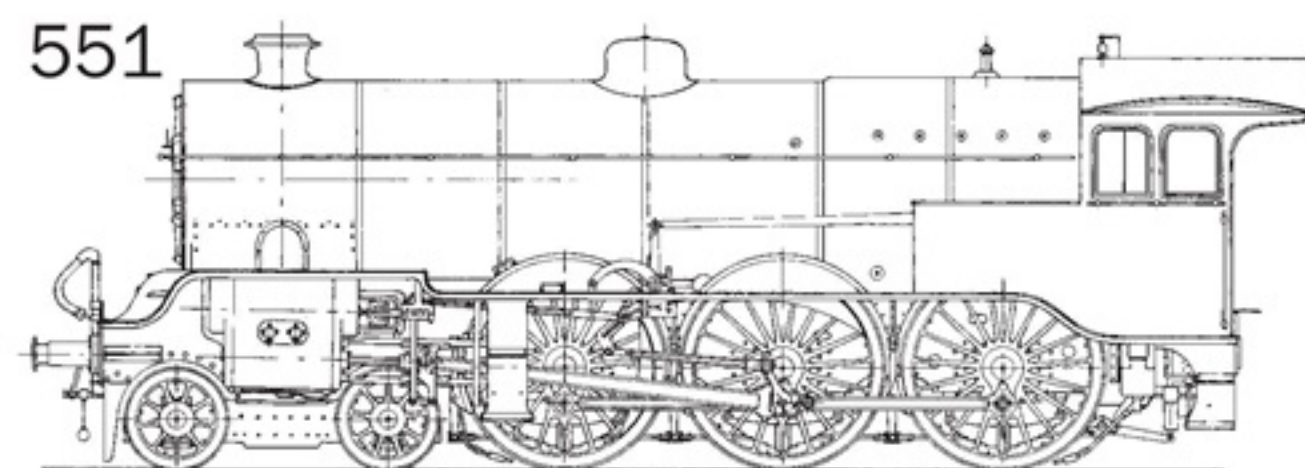
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RAILWAY MODELLING *Explored*

Projects, hints and tips for beginners from Railway Modeller readers and staff

581 **Basingstoke Juniors: Stratton Park**

Junior members of the Basingstoke and North Hampshire Model Railway Society tell how they each created a section of a modular OO gauge exhibition layout.

584 **TT:120 Country Station Building**

Callum Willcox constructs the new TT:120 scale Country Station Building kit from Peco, based on the surviving GWR example at West Bay.



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WHAT'S ON THE COVER



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Cover photography: Neil Davis

568 **Raven Hill**

After years of modelling in OO9, Graham Watling decided to make the move to 7mm scale. The result is this Yorkshire-based exhibition piece on 9mm gauge track.

572 **Small scene, large scale!**

Keith Apperley set himself the challenge of building a small static model in 16mm scale. This characterful diorama is the result.

574 **A tribute to Toller**

Myles Munsey created a small OO gauge model of this wayside station to rekindle happy memories of the former Bridport branch in Dorset.

576 **Port Joyce**

John Margerum was at the point of moving to a larger property, but a loft conversion presented some 27' of modelling space; just right for a move to O gauge!

587 **Comment**

Opinions and observations across the field of railway modelling. This month, Josh Williams suggests we should return to the root of why we began in the hobby in the first place.

588 **Readers' Letters**

590 **New Product Reviews**

The big news is the eagerly awaited Class 319 EMU in N gauge from Graham Farish (by Bachmann). We also take a look at the new Peco OO Bullhead slips and plain crossing, and the MGR hoppers in OO from Accurascale. OO9 is not forgotten with a review of the new Snailbeach District Railway hoppers from Peco; and much more...

596 **Book & DVD Reviews**

598 **News**

Heljan is first to announce a ready-to-run British outline locomotive in the new TT:120 scale, while other manufacturers are offering buildings and accessories. The Warley National Model Railway Exhibition is confirmed as going ahead this year; Accurascale is to produce a range of BR Mk.I suburban coaches in OO gauge; Rapido Trains UK expands its range of Southern wagons in OO and N; and much more...

604 **Societies & Clubs**

The biggest and best guide to the exhibition scene.

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SS57 - Vari Girder Plate Girder Panel.....£5.65	
SS58 - Garden Sheds Timber Type.....£5.65	
SS59 - Brick Tunnel Mouth & Walls.....£8.10	
SS60 - Station Platform Shelter Timber.....£8.50	
SS61 - Station Platform Sections.....£5.65	
SS62 - Station Platform Ramps (pair).....£5.65	
SS63 - Goods Yard Store Timber.....£8.50	

WILLS-KITS

OO Gauge Builders Sheets

Each pack contains 4 sheets 130mm x 75mm of injection moulded styrene approximately 2mm thick, making them rigid enough to be self supporting.

SSMP199 - Kit Builders Corner Fillets.....£2.65	SSMP235 - Gabion Cage Walling.....£3.80
SSMP200 - Coarse Stone.....£3.80	SSMP236 - Herringbone Block Paving.....£3.80
SSMP201 - Wood Planking.....£3.80	
SSMP202 - Dressed Stonework.....£3.80	
SSMP203 - Slate.....£3.80	
SSMP204 - Granite Setts.....£3.80	
SSMP205 - Cobblestone Walling.....£3.80	
SSMP206 - Pantiles.....£3.80	
SSMP207 - Rounded Tiles.....£3.80	
SSMP208 - York Stone Paving.....£3.80	
SSMP209 - Wane Edge Boarding.....£3.80	
SSMP210 - Crazy Paving.....£3.80	
SSMP211 - Plain Tiles.....£3.80	
SSMP212 - Brickwork Plain Bond.....£3.80	

UK Mainland Standard Postage Rate £4.00 per shipment

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Postcodes IM, BT, PH, PA, PO30-PO41, KW, AB, KA & IV will incur extra Postage - please enquire

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Selection Of The Latest Releases

All of the latest releases are available to view in the New In section on our website



N Gauge 769/319 Units



372-850 - Class 769 4-Car BiMU 769008 Transport for Wales.....£297.46



372-875 - Class 319 4 Car EMU 319004 Network SouthEast.....£297.46



372-876 - Class 319 4 Car EMU 319382 Thameslink.....£297.46



372-877 - Class 319 4 Car EMU 319362 Northern Powerhouse Northern Rail.....£297.46

N Gauge Class 08 Shunter



371-004B - Class 08 08721 'Starlet' BR Red Star Express Parcels.....£123.21

371-004BSF - Class 08 08721 Starlet BR Red Star Express Parcels DCC Sound Fitted.....£208.21



371-007A - Class 08 08953 BR Engineers Grey.....£123.21

371-007ASF - Class 08 08953 BR Engineers Grey DCC Sound Fitted.....£208.21



371-010 - Class 08 08441 RSS Railway Support Services.....£123.21



371-011 - Class 08 08417 Network Rail Yellow.....£123.21

371-011SF - Class 08 08417 Network Rail Yellow DCC Sound Fitted.....£208.21



371-012 - Class 08 08919 Rail Express Systems.....£123.21

371-012SF - Class 08 08919 Rail Express Systems DCC Sound Fitted.....£208.21

N Gauge Class 101 DMU



371-506 - Class 101 2-Car DMU BR Blue & Grey.....£178.46

371-506SF - Class 101 2-Car DMU BR Blue & Grey DCC Sound Fitted.....£263.46



371-508 - Class 101 2 Car DMU BR Green Speed Whiskers.....£178.46

371-508SF - Class 101 2 Car DMU BR Green Speed Whiskers DCC Sound Fitted.....£263.46



N Gauge Class 37s



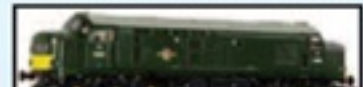
371-164 - Class 37/4 37428 'David Lloyd George' BR Railfreight.....£131.71



371-172 - Class 37/5 37669 WCRC Maroon.....£131.71



371-173 - Class 37/5 37521 Colas Rail Freight.....£131.71



371-453A - Class 37/0 Centre Headcode D6984 BR Green Small Yellow.....£131.71



371-465A - Class 37/0 Centre Headcode 37284 BR Blue.....£131.71



371-466A - Class 37/0 Split Headcode 37046 BR Engineers Grey & Yellow.....£131.71



OO Gauge Mk4 Coaches



R40146 - BR Mk4 Standard/Kitchen GNER Coach H.....£40.49

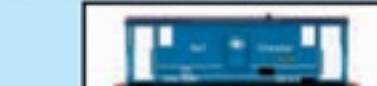
R40166B - BR Mk4 Standard GNER Coach D.....£40.49

R40164 - BR Mk4 Open First (Accessible Toilet) GNER Coach L.....£40.49

OO Gauge Wagons



R60170 - Lowmac with Coca-Cola Bottle Load.....£22.49



R60088 - LMS 20 Ton Brake Van S&T Blue Chester.....£26.54



OO Gauge Wagons



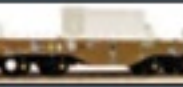
38-273 - BR 22T on 'Presflo' Cement Wagon 'Blue Circle Cement' Yellow.....£42.46

38-272A - BR 22 Ton 'Presflo' Cement Wagon BR Bauxite TOPS 'Rugby Cement'.....£42.46



38-049 - MTA Open Wagon EWS (Ex-Loadhaul) Weathered With Load.....£32.36

38-052A - MTA Open Wagon EWS With Load.....£29.71



38-345B - BR FNA Nuclear Flask Wagon Flat Floor With Flask.....£50.96



OO Gauge Class 08 Shunter



32-114B - Class 08 13052 BR Black Early Emblem.....£127.46

OO Gauge L&Y Class 5 Tanks



31-168A - L&YR Class 5 Tank 10730 LMS Crimson Lake LMS.....£123.21



31-171 - L&YR Class 5 Tank 1042 L&YR Lined Black.....£123.21

OO Gauge GW 64xx Pannier Tanks



31-635B - GWR 64XX 0-6-0 Pannier Tank 6414 GW Green GWR.....£118.96

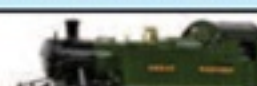


31-639 - GWR 64XX 0-6-0 Pannier Tank 6421 BR Lined Green Early Emblem.....£118.96

OO Gauge GW Prairie Tanks



32-132 - GWR 45XX Prairie Tank 4571 BR Lined Green Early Emblem.....£140.21



32-131A - GWR 4575 Prairie Tank 5526 GW Green Great Western.....£140.21

OO Gauge BR Standard 3MT Tank



31-976B - BR Standard 3MT Tank 82041 BR Lined Green Late Crest.....£140.21



31-982 - BR Standard 3MT Tank 82018 BR Lined Black Late Crest Weathered.....£152.96



New OO Gauge Code 75 Bullhead Points Now Available



SL-U1180 - OO Gauge Code 75 Bullhead Rail Single Slip Unifrog.....£59.50

SL-U1194 - OO Gauge Code 75 Bullhead Rail Crossing Unifrog.....£25.50

SL-U1190 - OO Gauge Code 75 Bullhead Rail Double Slip.....£63.75



Flexi Loco Lift

PT-60 - Flexi Loco Lift.....£9.95

PT-61 - Flexi Loco Lift Double Pack.....£16.65



Controllers

COMBI - Single Track.....£38.00

D - Twin Track Controller.....£105.00

DS - Twin Track Controller With Simulation.....£164.00

Q - Four Track Controller.....£188.00

W - Single Track Walkabout.....£33.99

SEEP Point Motors

PM1 - Point Motor with Switch.....£5.50

PM2 - Point Motor.....£3.60

PM4 - Self Latching Point Motor.....£5.90

Ballast



Coarse Grey Ballast.....£3.00

(3mm average size)

Fine Grey Ballast.....£3.00

(1mm average size)

Fine Salmon Ballast.....£3.00

(1mm average size)

All are supplied in 550g (approx.) bag.

Ballast Spreaders



N Gauge Ballast Spreader.....£10.99

OO Gauge Ballast Spreader.....£8.99

(Please note only fine ballast to be used with the ballast spreader)

Cork Sheetting

1/32" Thick - £4.00 each

2 for £7.00

1/16" Thick - £6.35 each

2 for £11.50

1/8" Thick - £8.35 each

2 for £16.00

Each roll measures 36" x 24" (3ft x 2ft)

Stock Boxes



All stock boxes available in 5 colours, Red, Green, Blue, Black & Brown.

Large - Suitable for large tender locos & diesels.....£3.65

Length 330mm, Depth 48mm, Width 62mm

4 of any colour large stock box for £13.00

Medium - Suitable for larger tank engines and medium sized diesels (class 20s, 25s etc).....£3.15

Length 220mm, Depth 48mm, Width 62mm

Small - Suitable for small steam locos (pannier tanks etc.) and small diesels (08 shunters etc.).....£3.15

Length 157mm, Depth 48mm, Width 62mm

4 of any colour medium or small stock box for £10.00

Sundeala Board

600mm x 1200mm (4'x 2')

(approximate measurements)

Single sheet £12.99 Shop Only

6 Sheets £72.00 + £9.30 postage



Xuron

Track

Cutter

£15.99

Electrics



Toggle Switches

SMT1 Single Pole Single Throw, (On-Off).....£1.05

4 x SMT1.....£4.00

SMT2 Single Pole Double Throw (On-On).....£1.20

4 x SMT2.....£4.50

SMT3 Single Pole Double Throw, Centre Off, (On-Off-On).....£1.20

3 x SMT3.....£3.00

SMT4 Double Pole Double Throw (On-On).....£1.35

4 x SMT4.....£4.80

SMT5 Double Pole Double Throw (On-Off-On).....£1.45

4 x SMT5.....£5.20

SMT6 Point Toggle Switches.....£1.45

6 x SMT6.....£7.80

Push to Make Switches Red, Black.....50p

Each

6 of any colour.....£2.70

Grain of Wheat Bulbs

12v Bulbs

Available in Clear, Red, Yellow, Green or Amber Packs of 10.....£4.50

Layout Wire

Suitable for point motors, power feeds & more.

7/0.2mm, Single Core, Multi Strand 100 metre rolls available in 7 colours.

Red/Black/Blue/Green/Yellow/Brown/White.....£9.00 a roll

.....Or any two 100 metre reels for.....£16.99

Same colours available in

10 metre rolls.....£2.00 each

20 metre rolls.....£3.00 each



AC11 - Pin Flow Applicator.....£11.95

AC13 - Track Magic.....£9.95

AC22 - Strip Magic.....£10.25

AC26 - Track Magic Refill.....£26.50

AD29 - Tacky Wax.....£5.50

AD55 - Glue N Glaze.....£7.50

AD57 - Roket Card Glue.....£6.95

AD60 - Tissue Paste.....£6.95

AD70 - Plastic Kit Glue.....£3.25

AD75 - Ballast Bond.....£6.65

AD77 - Plastic Magic.....£6.50

AD84 - Ballast Bond Refill.....£18.95

AD85 - Rocket Oderless Cyano.....£3.25

BD64 - Aqua Magic (250ml).....£17.75

BD44 - Perfect Plastic Putty.....£7.50

BD65 - Aqua Magic (125ml).....£10.50

BD68 - Eze Epoxy Putty.....£7.75

Mod Roc

Mod-Roc (Plaster Of Paris Bandage).....£3.60

Each roll is 15cm x 270cm approx.)

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Sold in packs of 10

2074351 - Large.....£5.00

(Suitable for GW King, Class 20, Class 26,

Class 27, Class 31, Class 37, Class 40, Class

47, Class 59, Class 60, Class 66, Warship,

Western, Class 86 & Class 87 Electrics)

2074352 - Small.....£5.00

(Suitable for GW Railcar, HST Power Cars,

Class 101 DMU, Class 50, Class 73, Class 117

DMU, Class 156 DMU, Deltic)

'O' CLASS 37/4



SRP **£599.00**

Weathered versions SRP: £619.00

COMING SOON

- | | |
|------|--|
| 3741 | BR Large Logo Blue Unnumbered |
| 3745 | Trainload Freight 3T grey Unbranded/Unnumbered |
| 3748 | Colas Railfreight 37421 |
| 3753 | Railfreight Petroleum 37418 Pectinidae |
| 3754 | BR Large Logo 37426 Y Lein Fach Vale of Rheidol |
| 3755 | BR Large Logo 37404 Ben Cruachan |
| 3756 | Railfreight Construction 37425 Sir Robert McAlpine/Concrete Bob <i>LIGHTLY WEATHERED</i> |
| 3757 | Railfreight Distribution 37413 Loch Eil Outward Bound |

'O' BR CLASS 50

AVAILABLE NOW



- | | |
|------|--|
| 4021 | BR Large Logo Blue Class 50 (grey roof) |
| 4029 | BR Large Logo Blue Class 50 (black roof) |
| 4030 | BR Large Logo Blue 50036 Victorious - <i>WEATHERED</i> |
| 4034 | HANSON & HALL grey 50008 Thunderer |
| 4037 | BR Large Logo Blue 50046 Ajax (black roof) |
| 4038 | NSE light blue 50032 Courageous - <i>WEATHERED</i> |
| 4039 | Revised Network SouthEast (dark blue) 50033 Glorious |

SRP **£699.00**

Weathered versions SRP: £719.00

'OO' CLASS 86/4

AVAILABLE NOW



- | | |
|------|---|
| 8640 | InterCity SOLD OUT ve 86404 |
| 8641 | Railfreight Distribution grey 86634 University of London. |
| 8642 | Res red/grey 86416 |
| 8643 | BR retro blue SYP 86426/E3195 |
| 8644 | Railfreight Distribution European grey 86622 |
| 8645 | Caledonian Sleeper blue 86401 Mons Meg |
| 8646 | Freightliner green/yellow 86609 |

SRP **£219.95**

'O' BR CLASS 56

AVAILABLE NOW

- | | |
|------|--|
| 5600 | BR SOLD OUT s 56 (unnumbered) |
| 5601 | BR SOLD OUT o Blue Class 56 (unnumbered) |
| 5602 | Railfreight Large Logo grey Class 56 (unnumbered) |
| 5603 | Railfreight Red Stripe grey Class 56 (unnumbered) |
| 5604 | Trainload Freight Sector grey Class 56 (unbranded) |
| 5605 | Railfreight Coal Sector grey 56101 Mutual Improvement - <i>WEATHERED</i> |
| 5606 | Railfreight Construction Sector grey 56110 Croft |
| 5607 | Loadhaul black/orange Class 56 (unnumbered) |
| 5608 | BR SOLD OUT o Blue 56099 |
| 5609 | BR SOLD OUT 1 |



SRP **£699.00**

Weathered versions SRP: £729.00

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OO Gauge Steam Locos



L&MR 'Lion' 0-4-2

Stage: Engineering Sample Due: 2023
Price: from £152.96 Digital: NEXT-18
hattons.co.uk/rapidolion



Hawthorn Leslie 0-4-0ST

Stage: CADs Due: Q3 2023
Price: from £99.45 Digital: NEXT-18
hattons.co.uk/dapolhl040



Hunslet 16" 0-6-0ST

Stage: Engineering Sample Due: TBC 2022
Price: from £110.46 Digital: NEXT-18
hattons.co.uk/rapidohunslet



Class 78xx 'Manor' 4-6-0

Stage: In Production Due: Jun 2022
Price: from £140.72 Digital: NEXT-18
hattons.co.uk/dapolmanor



Class 78xx Manor 4-6-0

Stage: Engineering Sample Due: Q3 2022
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascalemanor



HR Class I 'Jones Goods' 4-6-0

Stage: CADs Due: TBC 2023
Price: from £186.96 Digital: 21-pin
hattons.co.uk/rapidojonesgoods

OO Diesels & Electrics



Class 31

Stage: Engineering Samples Due: Q2 2023
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascale31



Class 37/0, 37/4 & 37/6

Stage: Decorated Samples Due: Q3/4 2022
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascale37



Class 47

Stage: EP & Livery Artwork Due: Q4 2022
Price: from £186.95 Digital: 21-pin
hattons.co.uk/heljanoo47



Class 59

Stage: Painted Samples Due: Late 2022
Price: from £136.31 Digital: 21-pin
hattons.co.uk/dapol59



BR NBL Prototype 10800

Stage: CADs Due: 2023
Price: £199.71 Digital: 21-pin
hattons.co.uk/heljan10800

OO Gauge Multiple Units



Class 104 DMU

Stage: CADs Due: Q1 2023
Price: from £296.65 Digital: 21-pin
hattons.co.uk/heljan104

OO Coaches & Wagons



Genesis 4 & 6 Wheel Coaches

Stage: Deco Samples Due: 2022/23
Price: from £30 Digital: 18-pin
hattons.co.uk/genesis



BR Mk2B Coaches

Stage: Deco Samples Due: Q4 2022
Price: £59.95 each Digital: Yes
hattons.co.uk/accurascalemk2b



GWR 'Toplight City' Coaches

Stage: Deco Bodyshells Due: 2023
Price: £51 each Digital: 6-pin
hattons.co.uk/dapoltoplightcity



Siphon G Vans

Stage: Early Samples Due: Q1 2023
Price: £54.95 Digital: N/A
hattons.co.uk/acccsiphong



SECR 10t Goods Van

Stage: Deco Samples Due: 2023
Price: £28.01 Digital: N/A
hattons.co.uk/rapidosecrvan



VIX Ferry Vans

Stage: Engineering Samples Due: 2022
Price: £39.06 each Digital: N/A
hattons.co.uk/rapidovix



Chaldron 4-wheel Wagons

Stage: Deco Samples Due: Q3 2022
Price: £44.99 (3-pack) Digital: N/A
hattons.co.uk/accurascalechaldron



GWR 10t Iron Mink Vans

Stage: Engineering Samples Due: Q4 2022
Price: £28.01 each Digital: N/A
hattons.co.uk/rapidoironmink



45t OAA Wagons

Stage: CADs Due: Q1 2023
Price: £33.96 each Digital: N/A
hattons.co.uk/rapidooaa

N Gauge Steam Locos



LSWR Class M7 0-4-4T

Stage: Prototype Due: Q3 2022
Price: £83.2 Digital: Yes - PCB swap
hattons.co.uk/dapolm7

N Gauge Diesel Locos



Class 28 Co-Bo

Stage: Early sample Due: TBC 2022
Price: from £101.96 Digital: NEXT-18
hattons.co.uk/rapidocobo



Class 59

Stage: Engineering Samples Due: Q4 2022
Price: from £136 Digital: NEXT-18
hattons.co.uk/dapoln59

N Gauge Wagons



JHA 102t Hopper Wagons

Stage: Engineering Samples Due: Q3/4 2022
Price: £33.96 Digital: N/A
hattons.co.uk/dapolnjha

O Gauge Steam Locos



Class J94 Austerity 0-6-0ST

Stage: CADs Due: 2023
Price: from £212.50 Digital: 21-pin
hattons.co.uk/dapolj94



Class B4 0-4-0T

Stage: Deco Samples Due: Q3 2022
Price: from £195.50 Digital: 21-pin
hattons.co.uk/dapololsrb4

O Gauge Diesels/ Electrics



Class 26

Stage: Deco Samples Due: Q2 2022
Price: from £509.15 Digital: ESU XL
hattons.co.uk/heljano26



Class 37/0 Re-Tool

Stage: CADs Due: 2023
Price: £534.65 Digital: ESU XL
hattons.co.uk/heljano37



Class 55 'Deltic'

Stage: Early sample Due: 2023
Price: from £594.15 Digital: ESU XL
hattons.co.uk/heljandeltic



Class 66

Stage: Engineering Sample Due: Q3 2022
Price: from £285.92 Digital: 21-pin
hattons.co.uk/dapolo66



Class 73

Stage: Engineering Sample Due: Mid-2022
Price: from £636.65 Digital: ESU XL
hattons.co.uk/heljan73

O Diesel Multiple Units



GWR Streamlined Railcar

Stage: Painted Samples Due: Q2 2022
Price: from £297.50 Digital: 21-pin
hattons.co.uk/dapolorailcar



Class 117 2/3-car DMU

Stage: Early samples Due: 2022
Price: from £551.65 Digital: 21-pin
hattons.co.uk/heljan117

O Coaches & Wagons



Stroudley 4 Wheel Coaches

Stage: Deco Samples Due: Q2 2022
Price: from £96.77 Digital: Yes TBC
hattons.co.uk/dapolstroudley



Mk2 & Mk2A Coaches

Stage: Prototypes Due: Q3 2022
Price: from £237.15 Digital: N/A
hattons.co.uk/heljanmk2



Mk1 Covered Carriage Trucks

Stage: Early sample Due: 2022
Price: from £152.15 Digital: N/A
hattons.co.uk/heljanmkt



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00 Gauge (1:76 Scale)

Dapol - Wagons

NEW



4F-027-022 ICA 'Silver Bullet' bogie tank wagon in NACCO - 37807898055-4 - weathered NEW... £36.51

Hattons Originals - Steam locos



H4-AB14-006 Andrew Barclay 0-4-0ST 14 'Little Barford' in Action Lane Power Station blue wisp stripes (RRP £99) BARGAIN... £84



H4-P-013 SECR P Class 0-6-0T 5027 in ROD green (RRP £99) BARGAIN... £84

Heljan - Steam locos



266220 Beyer Garratt 2-6-0 0-6-2 47993 in BR black early emblem and revolving coal bunker - heavily weathered... £221

Hornby - Steam locos



R3541 Class Q6 Raven 0-8-0 2265 in LNER black pre-war lettering (RRP £194.99) BARGAIN... £114

NEW



R3843 Rebuilt Class W1 Hush-Hush 4-6-4 10000 in LNER garter blue NEW... £218

Diesel locos

NEW



R3900 Class 08 08645 'St Piran' in GWR Cornish Flag NEW... £159.50

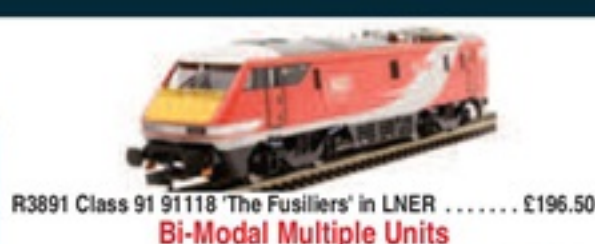


R30044 Class 37 97302 'Ffestiniog & Welsh Highland Railways' in Network Rail yellow - Railroad plus range... £87.50

Electric locos



R3893 Class 91 91117 'Cancer Research UK' in GNER... £196.50



R3891 Class 91 91118 'The Fusiliers' in LNER... £196.50

Bi-Modal Multiple Units



R3762 Class 800 IEP five car pack 800201 in LNER 'Azuma'... £430.50

Train sets - analogue



R1167 'Flying Scotsman' starter train set 4472 'Flying Scotsman' loco in LNER green and three Gresley teak coaches... £157

Coaches



R4493B 63' Hawksworth first corridor W8119W in BR crimson and cream (RRP £60.49) BARGAIN... £33



R4684A Collett 57' corridor third in BR crimson and cream - W4910W (RRP £52.75) BARGAIN... £33



R4683A Collett corridor composite (Right-Hand) 6527 in GWR chocolate and cream (RRP £58.49) BARGAIN... £33



R4188D ex-LMS Period II 12-wheel dining car M236M in BR crimson and cream (RRP £60.49) BARGAIN... £23



R4406B Hawksworth 63' corridor brake third in BR crimson and cream - W2251W (RRP £60.49) BARGAIN... £33



R40031 Maunsell composite dining saloon S7841S in BR southern region green... £54.50



R4816A Maunsell restaurant kitchen and dining car 7865 in SR olive green (RRP £58.49) BARGAIN... £29



R4843 Period III 50' passenger brake 31010 in LMS crimson lake (RRP £60.49) BARGAIN... £25

Northcord Model Company - Buses

NEW



UK6526 ADL Enviro400 MMC 'National Express West Midlands' NEW... £51

009 Narrow Gauge

Heljan - Steam locos



9962 Lynton & Barnstaple 2-6-2T 30190 'Lyd' in BR lined black early emblem (RRP £219.95) BARGAIN... £154

Peco Products - Wagons

NEW



GR-340UB Snailbeach District Railway 4-wheel hopper wagon in plain brown NEW... £20

NEW



GR-340UG Snailbeach District Railway 4-wheel hopper wagon in plain grey NEW... £20

N Gauge

Dapol - Diesel locos



2D-002-006 Class 50 50046 'Ajax' in BR large logo blue black roof... £131.92

DMUs



2D-021-004 Class 156 'Super Sprinter' 156509 in Strathclyde Passenger Transport SPT orange and black... £127.52

O Gauge (1:43 Scale)

Dapol - Wagons

NEW



7F-200-012 20 ton standard brake van in BR bauxite and yellow - B954768 NEW... £71.40

NEW



7F-200-009 20 ton standard brake van in BR grey - B951841 NEW... £71.40

NEW



7F-200-015 20 ton standard CAR brake van in Railfreight red and grey - B954561 NEW... £71.40

NEW



7F-200-014 20 ton standard ZTO brake van in Civil Engineers 'Dutch' grey and yellow - DB951767 NEW... £71.40

Hattons Originals - Steam locos



H7-A3-001 Class A3 4-6-2 2750 'Papyrus' in LNER Grass green unstreamlined corridor tender - 'Record Breaker' (RRP £750) BARGAIN... £579



H7-A3-007 Class A3 4-6-2 60103 'Flying Scotsman' in BR green late crest and unstreamlined corridor tender (RRP £750) BARGAIN... £579



H7-A4-006 Class A4 4-6-2 60007 'Sir Nigel Gresley' in BR express blue unstreamlined corridor tender - 'Record Breaker' (RRP £750) BARGAIN... £579

Coaches



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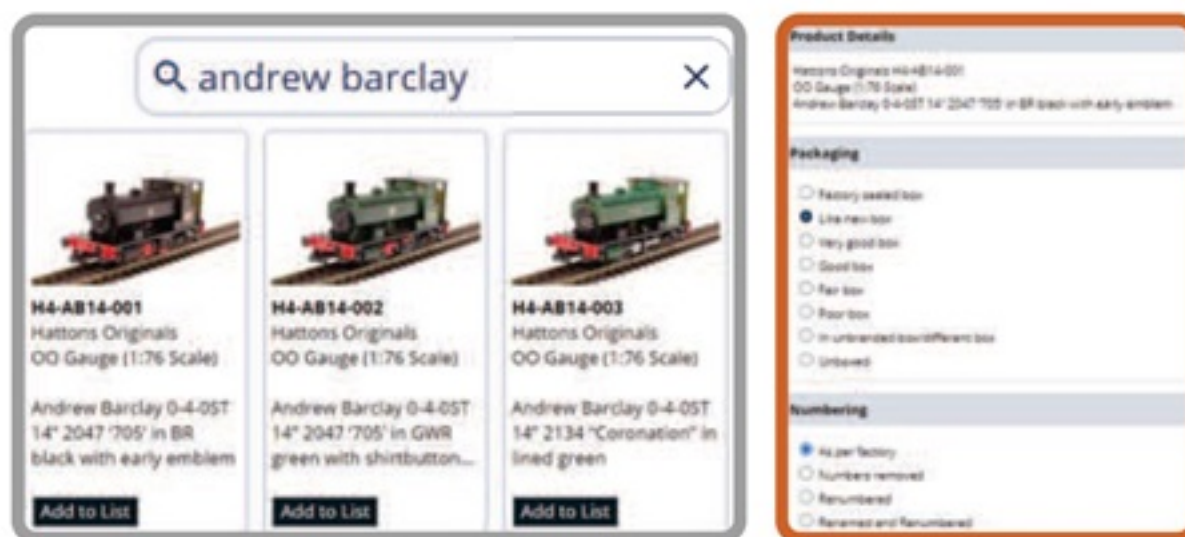


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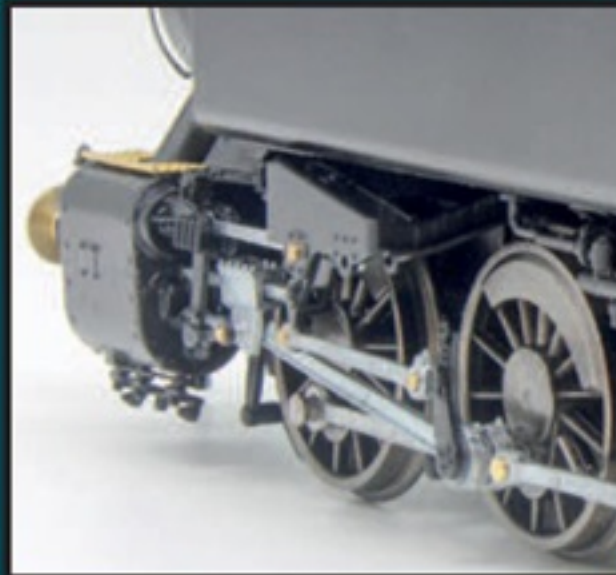
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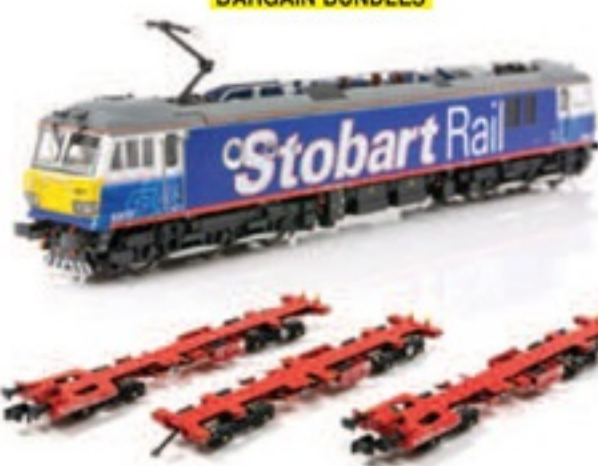
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Analogue Control



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What's the difference between the controller types?

Our **Mains Powered Cased Controllers** come complete with transformer, and can just be plugged in, connected to the track, and away you go. **Panel Mounted Controllers** require a separate transformer, and also need to be mounted onto a control panel to be used effectively.

We also produce various controllers with **Feedback** and **Simulation**, two effects controlled by the controller itself. **Feedback** senses the load on the circuit and helps maintain the locomotive at a steady speed up and down gradients. **Feedback** controllers are not suitable for use with locomotives with coreless motors. **Simulation** (also known as Inertia) allows a train to accelerate, coast, and be braked to a standstill, by use of a regulator and a brake.

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GMC-COMBI Single Track Controller/Transformer
Most Suited for HO/OO/N Scale Layouts



Fantastic for small layouts or beginners upgrading a starter set, the Combi has both a 12V DC output to run one track, and a 16V AC output for accessories.

**BEST FOR
Beginners**

GMC-D Twin Track Controller
Most Suited to HO/OO/N Scale Layouts



Our best selling controller. Runs a two track railway with minimum of fuss. The D Controller has two 12V DC track outputs, as well as a 16V AC output for accessories.

**BEST FOR
Twin Tracks**

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Most Suited to OO/HO/N Scale Layouts



The best selling four track controller available today. It offers impressive value for money with its four 12V DC track outputs, and two of 16V AC and 12V DC outputs for accessories.

**BEST FOR
Four Tracks**

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GMC-100 Single Track Panel Controller
Most Suited to OO/HO/N/Z Scale Layouts



Some modellers may wish to incorporate their controller into an overall panel to control their layout. The Model 100 Controller has a single 12V DC output.

**BEST FOR
Control Panels**

GMC-U Single Track Controller with Simulation
Most Suited to OO/HO/N/Z Scale Layouts



With the brake knob controlling the 12V DC track output, this controller allows you greater realism when running locomotives.

**BEST FOR
Realistic Running**

GMC-W Single Track Walkabout Single Track Controller
Most Suited to OO/HO/N/Z Scale Layouts



Fitted with 1.5m of cable, this controller allows you the freedom to move around your layout while still controlling your layout. It has a single 12V DC track output.

**BEST FOR
Flexibility**

Did you know...

If you don't have a handy accessory output from a controller, the **GMC-WM1** Wall Mounted Transformer can be used on its own to power accessories, such as point motors and lights from the 16V output. Just plug it in and connect it up!

GMC-WM1
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See the GM365 Gaugemaster Catalogue (£5.00) for more details

GAUGEMASTER products are available from your local model shop or, in case of difficulty, direct from ourselves

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E&OE tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com

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WHICH PRODIGY CONTROLLER?

Prodigy is compatible with most other DCC systems and manufacturers' DCC Decoders. It's strength is 'simplicity without compromise' and Prodigy systems can grow with your layout, allowing you to use the system at a level appropriate to you, your layout, and style of operation. All Prodigy Walkaround Controllers have backlit displays as standard.

Prodigy Express consists of a Base Station, Walkaround Controller, and Power Supply and is an ideal entry level system for a newcomer. Extra features can be unlocked by using the **DCC14 Prodigy Advance Walkaround Controller**.

Prodigy Advance2 is our best selling DCC Controller. It consists of a Base Station, Walkaround Controller, and a Power Supply, and is suitable for operating most sizes of model railway.

Prodigy Wireless offers all the features of the **Prodigy Advance** unit but this system offers you wireless control, giving you maximum freedom to operate your layout. Its rechargeable handset and radio transmission ensures easy and reliable performance.

DCC CONTROLLER STARTER PACKAGES

DCC01 Prodigy Express Starter Package
Most Suited for HO/OO/N Scale Layouts



BEST FOR
Beginners

- INPUT: 15-16V regulated DC at 2 amps
- OUTPUT: DCC Signal with 14.5V amplitude
- MAXIMUM CURRENT: 1.6 amps
- MAXIMUM NUMBER OF WALKAROUNDS: 20
- ADDRESS CAPABILITY: 2 Digit or 4 Digit
- SPEED STEPS: 14/28/128
- ACCESSORY FUNCTIONS: 28 (F0 to F28)
- DOUBLE HEADING
- PROGRAM & MAIN TRACK PROGRAMMING
- "READ LOCOMOTIVE" CAPABILITY



DCC80 DCC Autofrog

The DCC Autofrog is a simple switch that automatically changes the frog polarity of your electrofrog point. With the frog powered by the DCC80 it will automatically detect the incoming polarity of the wheels and switch the frog to match. The DCC Autofrog is available as a single item, and also as a handy pack of three.



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DCC02 Prodigy Advance2 Starter Package
Most Suited to HO/OO/N Scale Layouts



BEST FOR
General Layouts

- INPUT: 15-14V DC 3.5 amps
- OUTPUT: DCC Signal with 14.5v amplitude
- MAXIMUM CURRENT: 3.5 amps
- MAXIMUM NUMBER OF WALKAROUNDS: 99
- ADDRESS CAPABILITY: 2 Digit (1-127) or 4 Digit (1-9999)
- SPEED STEPS: 14/28/128
- ACCESSORY DECODER AND ROUTE SETTING
- DOUBLE HEADING
- PROGRAM & MAIN TRACK PROGRAMMING
- "READ LOCOMOTIVE" CAPABILITY



DCC05 Prodigy WiFi

This unit allows you to run your Prodigy-powered layout from your phone or tablet, using one of the recommended apps on Android or IOS. The Prodigy WiFi is compatible with the JMRI Engine Driver and WiThrottle apps.

The Prodigy WiFi is also available as a starter pack, combined with a Prodigy Express under part number DCC06.

See our website for articles showing how to use the two apps which are compatible with the Prodigy WiFi on your specific operating system.
Android: JMRI Engine Driver
iOS: WiThrottle

DCC04 Prodigy Advance Wireless Starter Package
Most Suited to OO/HO/N Scale Layouts



BEST FOR
Large Layouts

- ALL THE FEATURES OF DCC02 PRODIGY ADVANCE
- WIRELESS RADIO CONTROL (UK Approved)
- OUTSTANDING RANGE (80ft Plus)
- BUILT-IN RECHARGABLE BATTERY
- BATTERY CHARGE LEVEL INDICATOR
- OPERATES WIRED & WIRELESS WALKAROUNDS
- PROGRAM & OPERATE WHILE CHARGING
- FREEDOM & MOBILITY DURING OPERATION

DCC40 Auto Reverse Module

This useful module allows digital modellers to overcome a problem that faces most railway modellers at some point, that of how to operate a reverse loop or turntable bridge.



Controllers & Handsets

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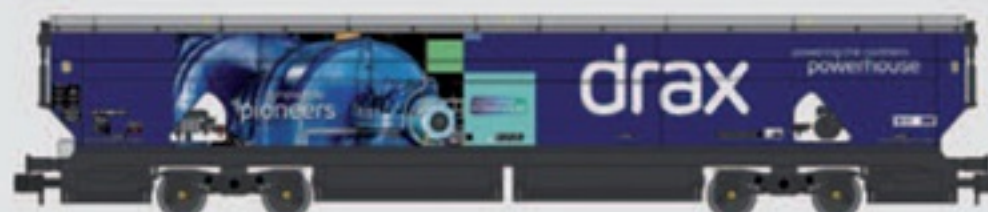
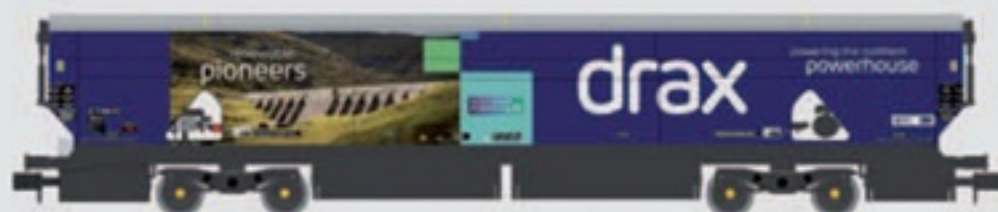
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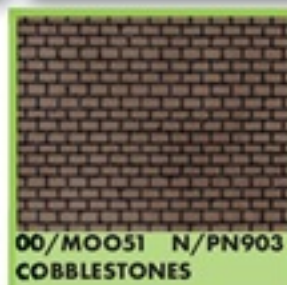
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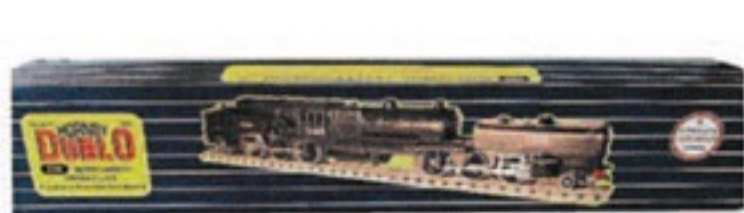
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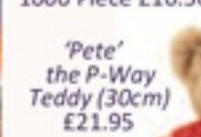
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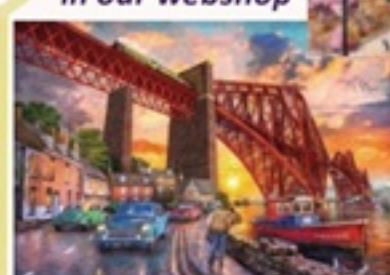
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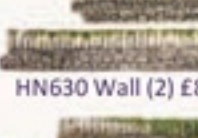
HN661 Hump Bridge
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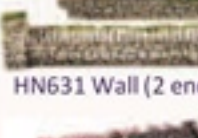
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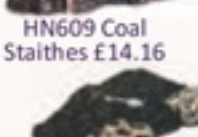
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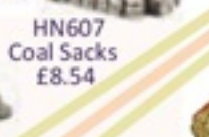
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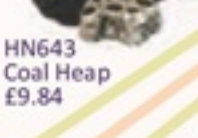
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Welcome from the Editor



The 1970s – a decade worth modelling

For me at least, any reference to Britain's railways of the 1970s instantly brings to mind images of diesel and electric locomotives in uniform blue liveries, widespread applications of the British Rail 'double arrows' symbol and the then new High Speed Trains on main line express passenger services.

On the face of it, this was a decade that also saw the rail network continuing its long programme of modernisation. There were numerous ambitious projects taking place, including the introduction of TOPS (Total Operations Processing System) to provide computer-based management of locomotives and rolling stock; major infrastructure upgrades including the electrification of the West Coast Main Line between Crewe and Glasgow; and experimental technology being developed for the construction of Advanced Passenger Trains.

But scratch below the surface and countless vestiges of the rail network's ancestry could be seen, including examples of grand Victorian architecture, instances of pre-Nationalisation rolling stock surviving in operation – and even steam locomotives still under BR ownership! Therefore, far from the clean, progressive corporate image that British Rail was striving to present, the reality was one of stark contrasts, where evidence of old and new existed alongside each other – and were often intertwined. Away from BR metals, steam locomotives continued to ply their trade in private industrial use, whilst Britain's heritage railway movement – seeking to preserve what was being swept aside – was gaining momentum.

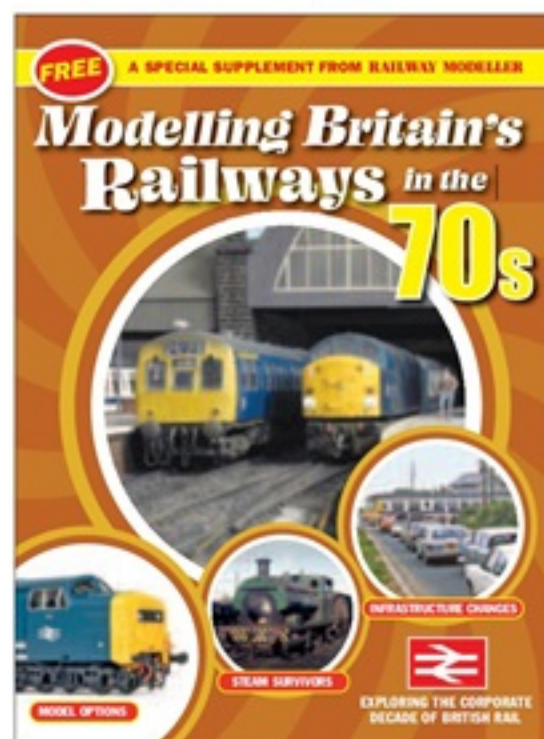
Taken as a whole, the rail scene of the 1970s presents modellers with a surprisingly eclectic array of opportunities, which we have delved into with the special supplement given free with this issue. This separate 32-page publication takes the form of an appetiser for this fascinating period of time, which is intended to tempt and inspire further exploration of the 1970s as a decade worth modelling.

The 'BR blue' era has of course long been a popular timeframe for modellers, with extensive support from ready-to-run manufacturers: over the years there has been a huge range of suitable locomotive, multiple unit and rolling stock models for OO, N and O. As outlined in the closing section of our supplement, there are many more models suited to the 1970s period that are currently in development for upcoming release – including a Brush Type 2 (Class 31) for TT:120 from Heljan (see News).

This is indeed an exciting development for TT:120 – a new scale for British outline – following the launch of products by Peco that we reported on in our July issue. Unsurprisingly, there was a tremendous amount of reaction to this from readers and we have published a selection of these responses in our letters pages on p588.

On this note, and to address a concern expressed to me by a 3mm Society member, I would like to reassure the society, its members and our readers that, although there is naturally a desire to promote the new TT:120 range through the pages of RAILWAY MODELLER, we will in the future continue to shine a light on examples of 3mm scale modelling in the same way that we have always done.

Craig Tiley



You can keep up to date between issues by visiting our new website at www.peco-uk.com and our Facebook page. You can also follow us on Twitter: @RailwayModeller



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In both cases, please remember to include your full name, address and contact details with your correspondence.

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And for all enthusiasts modelling overseas railways.

CONTINENTAL MODELLER

Published on the third Thursday of the preceding month.

Railway of the Month

Photography by Neil Davis



North West steam in the garden

PETER HOLLAND spent his formative years spotting in and around Manchester and was enchanted by the sights and sounds of steam locomotives on express passenger services, including holiday specials hauled by double-headed Stanier Jubilee 4-6-0s. Having dreamt for many years of one day recreating his childhood memories in O gauge, this large twin-level system with outdoor running is proof that dreams really can come true!

My parents presented me with a Hornby O gauge train set for my 10th birthday in 1952; I just loved driving the locomotive around at speed, and I recall it had a large working light on the front. The noise it made sounded just like the real thing to me, although I soon thought better of putting my hand on the track as the 20V could give a bit of a shock!

I also recall how, during the school holidays, I was allowed to go into Manchester to spot steam locomotives, including at two of the city's stations – Manchester Victoria and the adjacent Manchester Exchange. I used

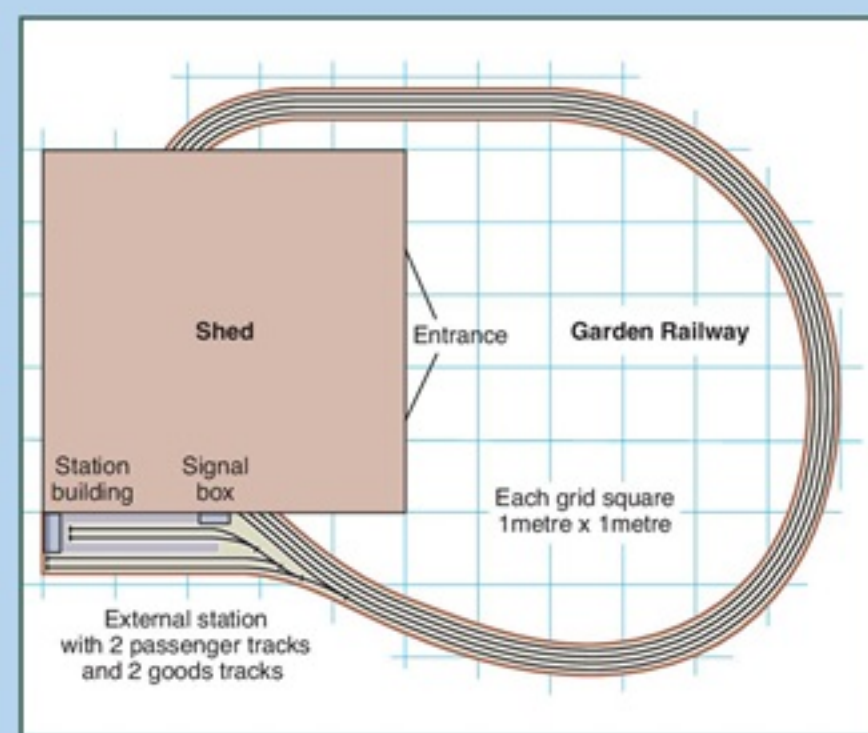
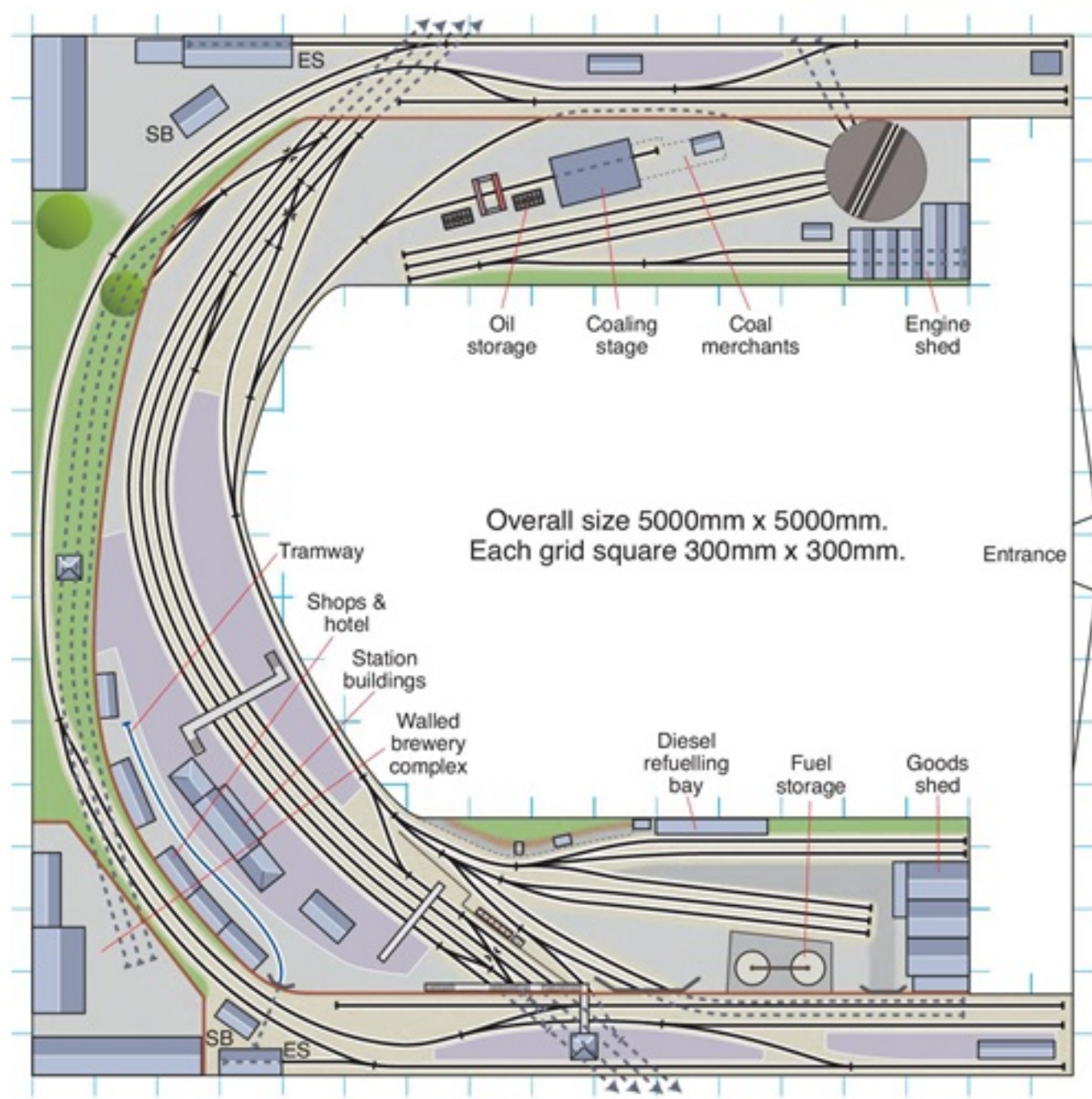
to take a packed lunch, together with four old pennies to cover bus fare, a platform ticket and a drink. The undoubted highlight was the Liverpool-Newcastle expresses which ran in holiday times only, sometimes double-headed by Stanier Jubilees. These lengthy trains were crammed with holidaymakers and such combinations of motive power were needed for the climb up Miles Platting bank.

Before catching the bus home, I would sometimes walk the short distance to Corporation Street to stare in awe at the magnificent O gauge models in the

Left
Fairburn 2-6-4T No.42140 is held at the signal, while the (Dapol) Sentinel 0-4-0VBT has its work cut out with the weight of the rail-mounted crane.

Right
Work-stained Ivatt 4MT 2-6-0 No.43017 traverses the outside section of the layout, which takes the form of a four-track main line complete with some scenic detailing.





Bassett-Lowke shop window. The staff would answer all my questions about the models on display and I recall how, when I told them about my Hornby O gauge railway, they gave me a Hornby sprung buffer stop together with a 1943 edition of the Bassett-Lowke Model Railway Handbook – both of which I still own!

I remember back then how I dreamt of one day building an O gauge railway and recreating the scene of those holiday trains hauled by a pair of Jubilees...

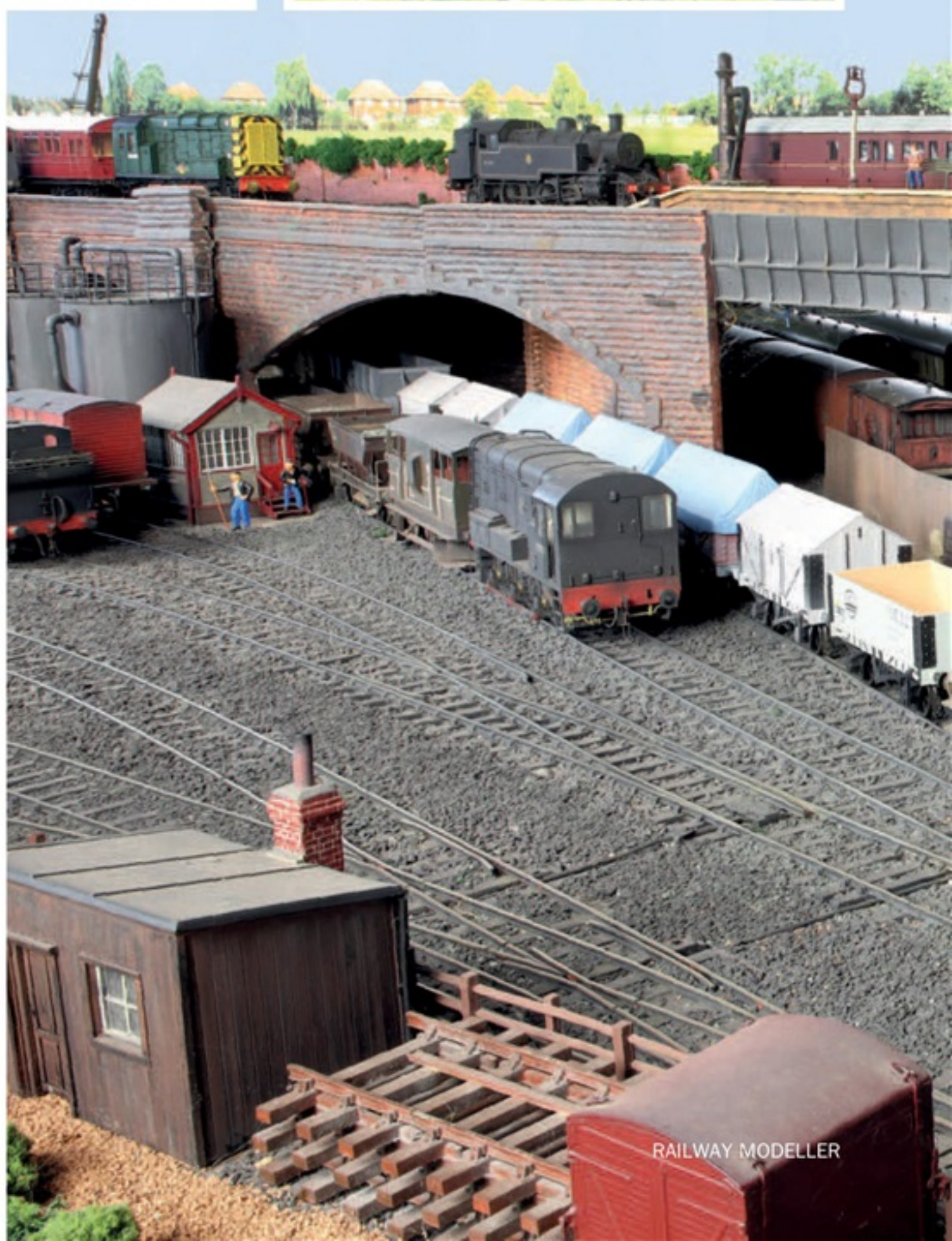
A railway in the garden

On completion of my service in the RAF my priority was to buy a property with a large garden that could accommodate an O gauge garden railway. We soon found the house that would allow my long-held dream to come true. Following this we attended a county show where there was a trader selling log cabins. My wife noticed one in particular, commented, "This is just what you are looking for" and before she changed her mind, I



Above

An overall view of the outside section showing how a roof was added to keep the worst of the weather at bay. The support system is cleverly masked with hedging which adds a maintenance task not associated with indoor layouts.



Below
Double-heading in O gauge remains an ambition for most modellers but one that has been realised on Peter Holland's layout as Jubilee 4-6-0s Nos.45697 *Achilles* and 45581 *Bihar* and *Orissa* make headway through Appleton Junction.

Left
The Jubilees break into the sunshine. The author stated that 'One day I would build an O gauge railway and recreate a passenger set double-headed by Jubilees'. Did he imagine that the result would be so impressive?

placed an order for this chalet-style shed measuring 5m². The garden was on a slight slope, so I laid more than 100 heavy concrete blocks to support the new structure, which arrived, in kit form, and took almost three months to complete and paint. Mains electricity was fitted together with strip lighting – plus dimmable lighting to simulate night-time running when the main lights are turned off.

Fortunately, one of my neighbours was a joiner and I was able to enlist his help to construct the outside baseboards and supports in the garden part of the railway. These were set at a height of 4' to allow for comfortable viewing and were protected using a very durable liquid rubber paint, obtained from a DIY supplier. My neighbour also assembled and fitted kitchen cupboards inside the shed to support the baseboards for the indoor section of the layout.

Planning for friends

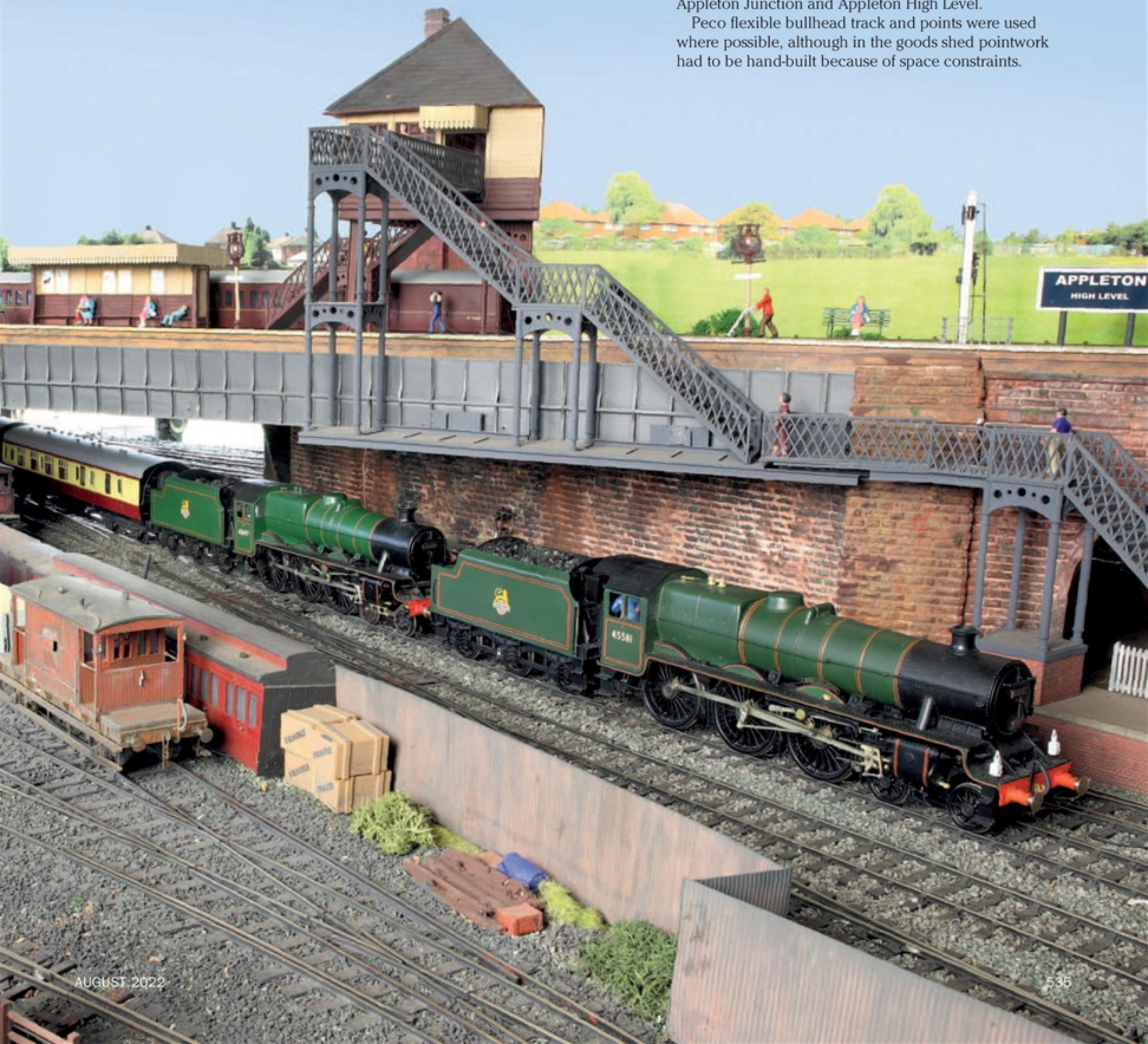
I have several friends who are O gauge modellers, some of whom have their own railways but others, not.

I wanted a railway that I could use myself whilst also providing the operational capacity for my friends when they visit as well; they could bring their own locomotives and I would supply the rolling stock. An engine shed with turntable was essential, together with a goods depot, which allows shunting when needed.

From these basic thoughts, a plan for the trackwork was prepared. This took the form of a twin-level system, with the lower level featuring a main four-track circuit that runs through each side of the shed and around part of the garden on the outdoor section. Equipped for 12V dc analogue control, the plan shows how the indoor part of the lower level includes a through station (Appleton Junction), goods yard and locomotive servicing facilities. There are also hidden sidings for stock storage that run underneath the upper level.

The upper level is contained within the shed and provides a station-to-station branch line that is equipped for digital control. Although there is no physical rail link between the upper and lower levels, there is a passenger link by way of a footbridge between the platforms for Appleton Junction and Appleton High Level.

Peco flexible bullhead track and points were used where possible, although in the goods shed pointwork had to be hand-built because of space constraints.



I was very fortunate to have a friend who built a 56-lever fully interlocked mechanical lever frame with facing point locks, point and signal levers, which covers all the pointwork and signals in the shed. He also built all my signals which are operated by memory wire.

Covering the outside tracks

Soon after laying the track it was realised that running trains in anything less than perfectly dry conditions outside was just not going to be possible. To address this I decided to build a cover over the outside parts of the layout, supported on aluminium angle; this allows for the viewing of trains from both sides of the track when running outside – whatever the weather!



Left

The author's memories of double-headed Stanier Jubilees working through Manchester Victoria station provided the inspiration for the layout project. An example of this combination is recorded at the station on 22 October 1960 with No.45643 Rodney leading No.45556 Nova Scotia. Photo: Colour Rail

A further refinement was dimmable lighting installed under the covers, which adds another dimension to the running of the railway on dark nights.

Buildings and scenery

A lot of the scenery was purchased second-hand. For example, I was offered a box of buildings for a nominal sum which made up into the magnificent brewery which can be seen on the upper level. Other buildings I bought at Guildex events from the Gauge O Guild's bring and buy stall.

I built the goods shed together with Stockton Heath station, but Appleton Junction station and the engine shed were professionally built to fit in the space available.

For the first year of running adjustments were made to the track to iron out any trouble spots. Once all was

Far left

A view underneath the roof that shelters the outside section. The boards are mounted at 4' from the ground to provide an almost eye-level viewpoint.

Below

The whole reason for building the layout was to allow for express passenger trains in 7mm scale. This has been achieved with great success, which can be seen here as No.46229 Duchess of Hamilton roars past Appleton Junction Box.





Right
Diesel traction has managed to slip in unnoticed on the upper level with a two-car DMU showing the way forward. Visitors' locomotives make for an interesting mix of motive power on the layout.

running well, I then ballasted the track both inside the shed and outside. I used fine granite chippings which were brushed onto the track and then fixed with a 50/50 PVA/water mix. The advantage of this is that should I have a problem – as I did when a point failed – it was just a case of soaking the ballast until it started to loosen, remove the point, replace it with a new one and then re-ballast the track.

Locomotives and rolling stock

I have a large collection of locomotives, mostly built from DJH or Gladiator kits. Whatever their origin I fit to

each a 30:1 reduction gearbox coupled to a Canon motor, which ensures reliable running. Passenger stock is predominantly from Haywood Models bought fully finished in eight-coach rakes, whilst goods wagons are from a variety of kits supplemented by an ever-growing number produced by Dapol.

A fair proportion of my locomotives and rolling stock has been weathered to a very high standard by my local model railway shop, Grimy Times.

My collection of stock enables me to run the layout in three different time periods covering broadly 1920-1930, 1930-1948 and 1948 onwards.



Right
Bulleid Merchant Navy 4-6-2 No.35022 *Holland America Line* and a Lord Nelson 4-6-0 head a passenger train composed of green stock on an excursion to the north. All the track is from the Peco Code 124 bullhead range.



Operating the railway

There are five different operating areas on the railway: The first is the signal box with its 56 levers; adjacent to this is the track plan showing all points and signals, together with repeater lights for signals that cannot be seen by the operator. Nothing can move on the railway until the signaller has pulled the correct levers.

The second is a bank of four panel-mounted controllers for the four main running tracks. In addition, this position also controls Stockton Heath station which is situated outside the shed (see plan). A small camera covers this station which can be seen by the operator on the monitor in the shed.

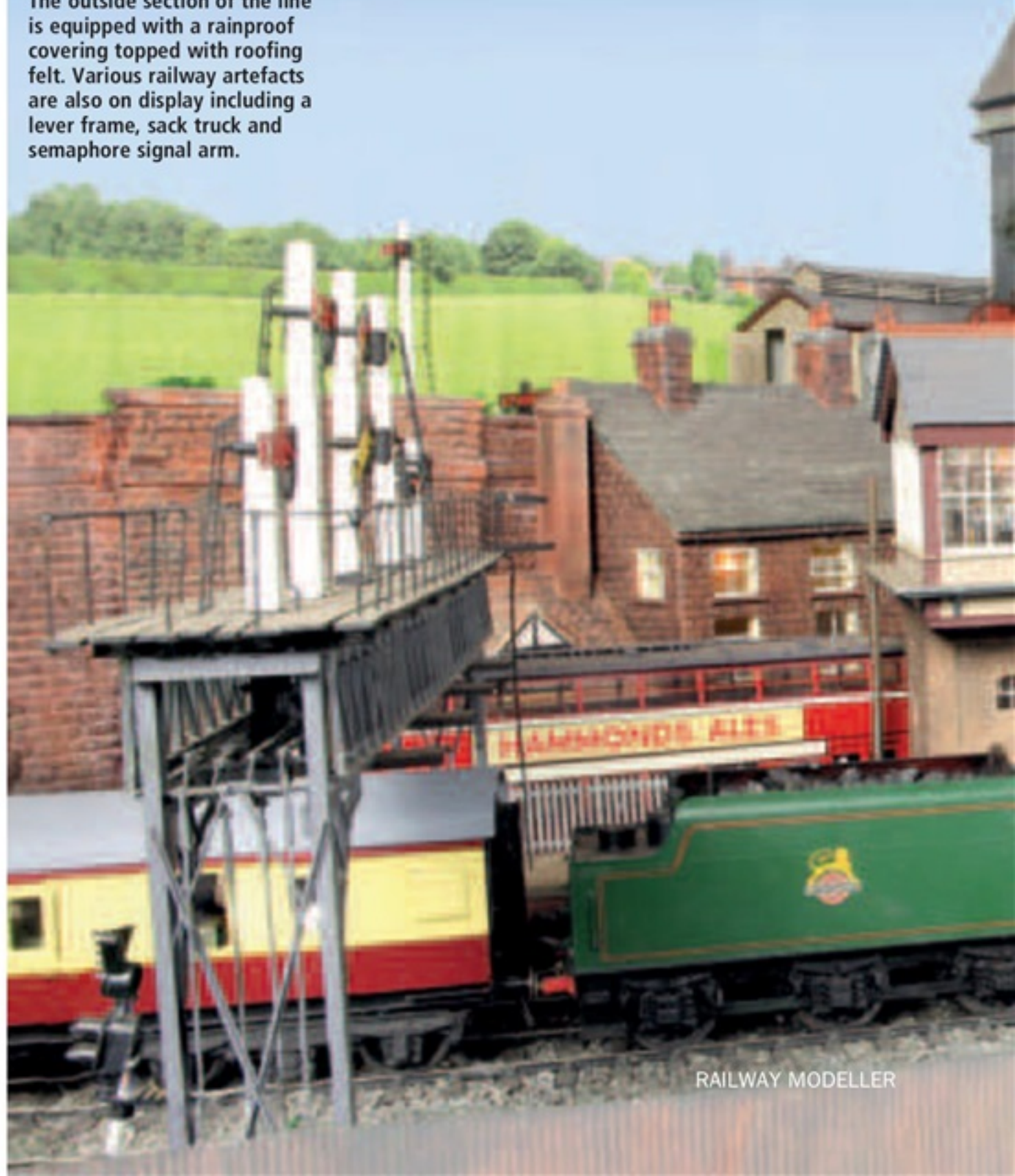
The third is the goods shed with its own controller and isolation switches for each track. This controller can be switched so that it allows wagons to be taken out of the shed or switched so that the operator on the main railway can take them out.

The fourth is the engine shed with its own controller which includes isolating sections and a switch to change power from tracks to the turntable motor. Visiting locomotives are not permitted to be lifted onto the track



Below left

The outside section of the line is equipped with a rainproof covering topped with roofing felt. Various railway artefacts are also on display including a lever frame, sack truck and semaphore signal arm.



Left

Space is tight in the motive power depot though there is room for a turntable. Standard Class 3MT 2-6-2T No.82009 and Britannia Pacific No.70046 Anzac wait for their next turns outside the running shed.

Right

Smaller items of motive power are not excluded and here an English Electric 350hp shunter and Ivatt 2-6-2T No.41216 pause in-between duties. The goods stock below is a mix of kit-built and newer R-T-R vehicles.

Below

The realisation of a schoolboy's dream! A parting shot of double-headed Jubilees heading through Appleton Junction, rekindling the author's memories of the steam era in Manchester.



because of damage that may be done to signals etc. Instead, they must go onto shed and be turned on the turntable so that they are pointing in the right direction before handing them over to the main line controller.

The fifth section is for the DCC (all with sound) upper railway. All signals, points and locomotives are controlled by the operator.

Help from my friends

There are so many friends that have helped me to build this railway and help keep it running today – far too

many to mention them individually – but I would like to thank Jon Penn who inspired me to build the railway and assisted with designing the track plan and laying the track, John Casson for the buildings he made and for spending so much time helping correct problem areas in the trackwork, the late Fred Tonks who built the lever frame and all the signals, and finally to Harry Crossley who was involved from the start and is still helping today. He has wired all the electrics in the shed and on the track plus many other jobs – I don't think there is a problem that Harry cannot fix!



Talking Points

Topical issues from the world of model railways

Fourdees – 3D printed ready-to-run in OO9

Since the release of its first narrow gauge locomotive in 2014, this small model manufacturer has established itself as a supplier of ready-to-run models for OO9, all of which are assembled by hand using 3D printed parts. Chris Ford talks with the company's founder, Daniel Wells, to discover how Fourdees was started and learn what plans the firm has in store for the future.

Recent years have seen a tremendous growth of commercial support for OO9, from larger manufacturers such as Peco, Heljan and Bachmann, together with a wide array of smaller, more specialist suppliers. One of the latter is Fourdees Limited, the brainchild of narrow gauge aficionado Daniel Wells, who looked to utilise his Computer Aided Design (CAD) expertise to fill the gaps in the ready-to-run ranges of British outline narrow gauge locomotives. I was keen to find out more about the background to this company, which during the past eight years has released an impressive range of products – with more set to follow in the months ahead...

Chris Ford: Firstly, are you able to give us a bit of background about your railway modelling interests and how this led to you starting Fourdees as a commercial enterprise?

Daniel Wells: I have long had an interest in narrow gauge railways, dating back to family holidays to Wales in the 1990s. To me, there is nothing like the sight and sound of a narrow gauge locomotive climbing through the picturesque mountains of Snowdonia and mid-Wales. So ever since my early modelling in OO9, I longed for a ready-to-run UK prototype, or at least a kit that was easy to build. My first layout, the *Llanfair and Porthnewydd Railway* (RM June 2011) had to rely on continental HOe models. At that time I could but dream of being able to look through a range of ready-to-run models of narrow gauge prototypes and choose a particular model in the same way that OO and N gauge modellers could. I had started to gain experience with using 3D CAD software, but this was purely for my own interest and not for any commercial benefit. However, it was reading an article by David Malton titled *A loco from Styrene sheet* (RM July 2013) that inspired me to explore the idea of designing a narrow gauge body, which could be 3D printed and then put on a commercial chassis.

CF: What sort of research and development work did you undertake for the models that you launched your business with?

DW: Not wanting to over-complicate my first project, I wanted something that would be simple to build and reliable to run. Therefore I opted for a skirted locomotive on a Kato chassis and the Glyn Valley Tramway locomotives were the perfect choice. My goal was for the detailing and standard of finish to be on a par with OO and N gauge models, so I set about developing the most accurate and highly detailed model that I could. The model was designed to be painted prior to assembly, and so the wealth of parts on the initial batch of GVT models was quite comprehensive (this would be simplified in later batches). I



▲ The debut Fourdees model was this Glyn Valley loco (see RM November 2014).



▲ The models of Hunslet 2-6-2T Russell followed in 2018 (see RM December).



▲ Recent releases have included prototypes with more complicated valve gear, including this Leek & Manifold 2-6-4T (see RM July 2020).

“ – The initial business aim was to ‘fill the gaps’ amongst the first mass-produced OO9 models – ”

Daniel Wells
Fourdees Ltd

did have to make compromises for batch production, particularly regarding livery application, as it was not feasible for me to line by hand a batch of 20 or 30 models at a time, and so I researched and developed dry-rub transfers and simplified the livery as much as I thought the market would accept. The models were released in the summer of 2014, and to date they have been one of the best sellers in the range, helped by the arrival of the Peco GVT coaches.

CF: What particular technical challenges have you faced with your products, and how have you overcome them?

DW: The initial business aim was to ‘fill the gaps’ amongst the first mass-produced OO9 models, so I decided to offer as wide a range of products as possible, in small production quantities. Narrow gauge locomotives often have outside frames hiding the wheels, and the Graham Farish (by Bachmann) Class 08 shunter is the perfect donor chassis for many prototypes. I had already released some models on this chassis with simplified outside motion, but to expand the range I needed to be able to create a representation of more complex valve gear arrangements. However, my first attempts were complicated, slow to build and a bit too delicate once assembled.

A better alternative was required and resulted in using a mixed media approach of brass and plastic valve gear and motion. This underwent numerous durability tests to check the performance of the materials, so that they could then go into production. The first models on this chassis, Welsh Highland Railway 2-6-2T *Russell* and the Welshpool & Llanfair Light Railway Sierra Leone No.85, were released in 2018, and over a period of about eight months, I designed and tested a further seven new models which were released in 2019.

CF: What sort of manufacturing processes are you using, and are these the ones you had envisaged when you started Fourdees?

DW: From the beginning I have been using additive manufacturing, or in other words 3D printing. In 2013 3D printer technology was still in its infancy, and for the required budget the main suppliers of 3D prints could either produce something highly detailed, but in a very brittle and hard to clean resin, or something in a nice durable resin but with very little detail. A chance conversation gave me a connection to a Rapid Prototyping firm, whose machines could combine the high detail and durable resin to give the quality required. Whilst the parts were relatively expensive, it was a route to creating a OO9 model that could be run 'out of the box' and be comparable with mass-produced products from established companies. The ready-to-run range continues to use parts from this company, and they are high-definition stereolithographic assemblies. These machines (that cost upwards of six figures) are often used by manufacturers of mass-produced models to test the CAD before the cutting of steel tooling begins. The parts made by these machines are almost to the same standard as injection moulded items and, as supplied, they need very little finishing. This means I can take advantage of some economies of scale to paint, assemble and finish each model in small batches. Home 3D printers are getting better and better. The machines available to purchase now are a world away from what was available when I started in 2013, and it is this latest generation of machines that are helping to produce the new range of kits. The parts have the same amount of detail as those made using Rapid Prototyping, they just need a little bit more finishing before assembly.

CF: How do you choose which prototypes to produce? Is this personal preference, customer driven, or based upon a technical aspect?

DW: A mixture of all three. The models involve a lot of design hours, so the subject needs to be something of interest to myself to commit the time. This first rule is usually overridden if there is significant demand for a certain prototype, such as the case of *Russell* when it was cut down to fit the Ffestiniog Railway loading gauge. There is a fondness in the narrow gauge community for the locomotive in this form, so I did bring out a model of this version. Given that I work with commercially available chassis, the prototype has to be something that will actually work on that chassis and the mechanism should be suitably accommodated within the boiler/cab/tank structure without significant compromise.

CF: How do you go about researching a model – especially where a suitable full-size prototype may no longer exist?

DW: I usually start by seeing what is available in terms of the original manufacturer's drawings, so it is about trying to find where the archives are stored and what drawings (if any) are still available. I tend to start with the General Arrangement (GA) drawing and go from there. Some GAs are really good, and have pretty much everything you need to model up the majority of the locomotive. The Kitson & Company Ltd GA for the two Leek & Manifold locomotives is very clear, with pretty



▲ The NWNGR 0-6-4T *Beddgelert* was reviewed in the September 2018 issue.

much all the motion, frames, and pipework detail that are needed to create the CAD for the full model. In comparison, the Hunslet Engine Co GA for *Beddgelert* is basic, and little more than an outline of the locomotive exterior. The most useful drawing after the GA is often the Pipe Arrangement. This shows all the pipework and controls for the locomotive and shows the detail needed to make an accurate model. If drawings aren't available, and photographs are unclear, I look at similar prototypes from the same manufacturer, using established standard parts and common layouts.

CF: You use standard ready-to-run chassis for your models – what sort of compromises do you have to make in the bodywork design to fit?

DW: Surprisingly few. I only develop a model if it can be designed to look satisfactory on the chassis. The prototype will have a similar wheelbase (approximately 3' + 3' spacing for the Farish 08) and the motor will fit within the boiler/tanks/saddle. I've had many requests to bring out a W&L *Earl/Countess* as this is an 0-6-0 (like the Farish 08 chassis). However, the wheel-spacing of these locomotives is 4' + 6', which would be too much of a compromise.

CF: You currently produce models for OO9 – are there plans to move into other scales?

DW: Yes, I would like to do more in O-16.5, having already had some success with the Snailbeach hopper wagon. As with the OO9 range, it is about finding an appropriate chassis for a prototype. Peco's recent announcement of TT:120 has the potential to open up new possibilities, depending on what chassis come to the market. Something like a Class 08 shunter chassis to 12mm gauge would be perfect to create models at 5.5mm scale, or in OO_{N3}, for example.

CF: Finally, are you able to reveal any plans or ideas you have for future Fourdees models?

DW: I can't reveal any of my long-term plans for ready-to-run models or kits, mainly because these are dependent upon the availability of suitable donor chassis. The release of products in OO9 is certainly gathering momentum, however, particularly through the investment and recent releases from Bachmann and Peco, and hopefully the increased interest will enable me to bring out ever more interesting models in

the future. In the short term I am looking to expand the range of kits to cover more prototypes at the smaller and larger ends of the narrow gauge spectrum, where the economics of a ready-to-run model are less favourable. In addition, I have had a freelance O9 scale locomotive (based on machines built by the Exmoor Steam Railway) at an advanced stage of CAD for nearly eight years, which has never gone to production as the ready-to-run market has never looked large enough to justify the cost. A short batch of kits, on the other hand...



▲ A 7mm scale development sample of a Snailbeach hopper wagon for O-16.5.



▲ Kits have been released (see News) including this Sharp, Stewart style loco.

Further information

Refer to the Fourdees website for details of the full range of locomotive and rolling stock products that are available:

Fourdees Ltd, 57 Wentworth Road, Barnsley S74 0RP
Tel: 07887 803 737
email: info@fourdees.co.uk www.fourdees.co.uk

North Ballachulish

*Walking up Scottish mountains inspired **ANDY COOPER** to make a switch from modelling Lancashire in the 1960s to the Highlands in the early 1970s. A might-have-been Callander and Oban branch forms the basis of this 4mm scale model.*

Photography by Karl Crowther

Over the years I have been committed to modelling the railway scene around where I spent my formative years – the Oldham and Rochdale area in sunny Lancashire – and always in the early 1960s steam period. It came therefore as a bit of a surprise to my friends and colleagues in the Rochdale Model Railway Group when one club night I announced that I was going to make my next exhibition layout a Scottish-

based model. Not only that but it was going to be set in the 1970-72 period and have blue diesels. What piqued my interest in particular was the opportunity to model the transition from green to corporate 'banger' blue.

Highland inspiration

To be honest, they should have seen this one coming: I had been doing a lot of 'Munro bagging' (walking up Scottish mountains that

have an elevation in excess of 3,000', of which the country has 282 in total) in the West Highlands, usually basing myself in Glencoe; specifically, in the Clachaig Hotel where our youngest son was the bar manager

Darlington-built Type 2 (later Class 24) No.5099 makes steady headway with a rake of hoppers. The locomotive is a Sutton's Loco Works model, supplied ready-to-run for EM. In the foreground a Hillman Imp makes its way along the A82.



The station building design is based on the existing Holly Bush restaurant at Kentallen in the style of Charles Rennie Mackintosh. BRCW Type 2 D5389 (a Heljan model) waits at the platform with a train from Fort William composed of BR Mk.I stock.

at that time. From here I would often journey along the A82 between Glencoe and Fort William. As you cross the Ballachulish Ferry Bridge over the narrows, after about a mile or so the road takes a sharp bend through North Ballachulish. It is rather picturesque, with a small church, St Brides, sat above the road and a group of cottages. A few hundred yards further on there's also a wonderful 'wiggly tin' village hall at Onich. Over the many times I passed this I did wonder what it would have been like if the Callander and Oban Railway had extended its branch over the narrows and on to Fort William.

It was in Fort William that I first came across John Thomas's book, *The Callander and Oban Railway*, and a copy was





immediately bought. It's a good read, documenting the eventful story of the railway and the struggles of its driving force, John Anderson, to build the line. More importantly it also brought into light the ideas proposed by both the Caledonian and the North British railway companies to extend not only to Fort William but beyond up the Great Glen to Inverness. And so, the seed was well and truly sown.

The back story to North Ballachulish then is that the Callander & Oban was driven on by the Caledonian to gain a foothold in the North East of Scotland. The Ballachulish branch as was continued over the entrance to Loch Leven, up to Fort William, amalgamated the Fort Augustus railway and pushed on through to Inverness, much to the ire of the North British. In this alternate reality, however, the West Highland line was never constructed.

Realising it in 4mm scale

Taking the actual landscape as my cue, the baseboards were built in a 16' x 14' L-shape using 4' strips of 9mm ply as sides and cross members with a 9mm ply baseboard top. The middle board rests on what is in effect an open topped table with the two outer boards piggybacking off it. The fiddle yards are basically fold-up tables using the cassette system. This gives a real degree of flexibility in operating the layout.

All the pointwork is hand-built. Having designed the trackplan using Templot, a print off was placed on the boards to ensure it all looked satisfactory before commencing on the track. On my previous layout, *New Hey* (see RM October 2009), I had glued the plan to the baseboards, but experienced the paper bubbling a bit when ballasting, so this time I just used the plan as a guide for positioning the sleepers and rails. Cork sheet was used as an underlay and to help form a

▲ The station building model is based on the example at Kentallen (now a restaurant) and was constructed using measurements taken on site and plans sourced from the Scottish Records Office.

Modelling a real place, albeit one that never had a railway, can provide a convincing setting. The buildings are based on examples in the North Ballachulish area.



ballast shoulder with ply sleepers cut to size and glued directly onto this. I'm adept at filing closure rails and common crossings these days and tend to batch-build components, so that when it comes to building the track everything is ready to go.

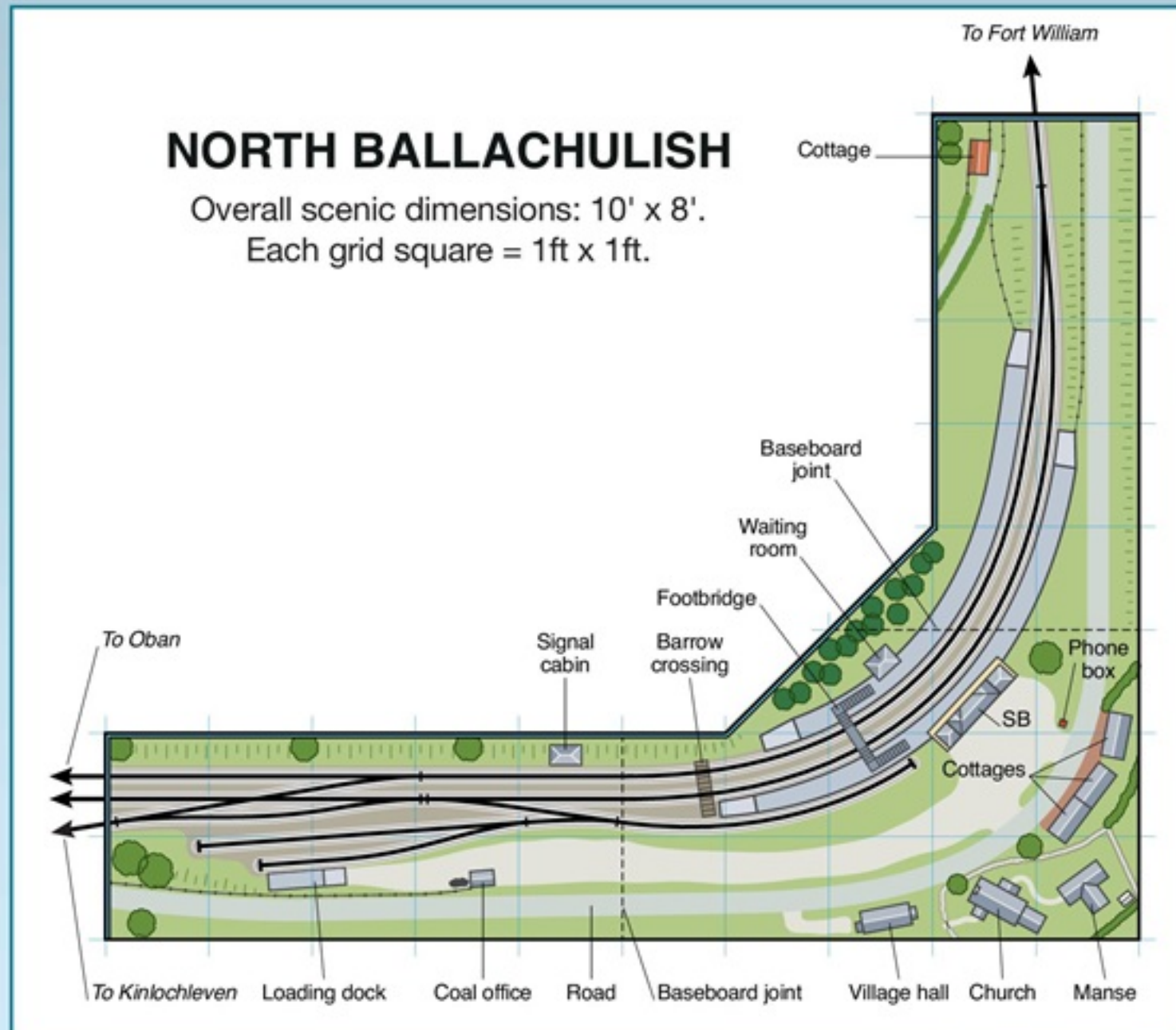
The rails were threaded with Exactoscale plastic chairs; before I laid these I drilled a pilot hole and inserted a brass pin in between every fifth sleeper (directly under where the rail web goes). Then, as the chairs were glued into place, I could solder the rail bottom to the pin.

This might seem a little over the top, but it had a purpose; I found years ago that when using plastic chairs on plywood sleepers, the curved stock rails and closure rails still had tension in them (even though they were pre-curved before laying) which had the effect of pushing the chairs along the sleepers ever so slightly, thereby throwing the point out of gauge. The soldered joint stops this completely and as a bonus you can solder your feed wires to the exposed end of the brass pin, meaning no unsightly wires coming through the board.

The plain track is standard C&L EM gauge flexible track. The layout is wired conventionally on a common return and is 12V dc in operation, although there is provision for DCC operation via a changeover switch on the control panel to 16V ac. Point tiebars are operated using Tortoise slow-action motors.

NORTH BALLACHULISH

Overall scenic dimensions: 10' x 8'.
Each grid square = 1ft x 1ft.



Setting the Highland scene

I knew that a central focal point of the layout had to be the bend in the road which contains St Brides Church, its vicar's manse

and the two dwellings below. During one of our visits the buildings were extensively





photographed and measured and on this visit I discovered that a former vicar of St Brides, The Rev David Railton MC, the originator and driving force behind the idea of the tomb of

A BRCW Type 2 draws a mixed freight through the station while a diesel railcar stands in the adjacent platform with a service to Fort William. A MacBraynes bus heads along the A82.

the unknown soldier in Westminster Abbey, was buried there. To me the backstory of the layout is as important as the build itself – it gives it a sense of purpose and *raison d'être*. I'd also been to the Holly Bush restaurant at Kentallen – this is in the old Kentallen station building and in the style of Charles Rennie Mackintosh. The building design was

common to most stations along the Ballachulish branch and therefore was destined to become the basis of North Ballachulish's station building. I was able to obtain copies of the original watercoloured plans from the Scottish Records Office and with these to hand, construction commenced.



◀ **NBL Type 2 D6129 (a Dapol model) slows past North Ballachulish box with a passenger train to Fort William. The backscene depicts Loch Linnhe and was painted using acrylics by Mike Raithby; the scene was based on a series of photos taken by the author.**

All structures were made from plasticard, layered up and with internal strengthening to prevent warping. Embossed styrene sheet (usually South Eastern Finecast) was used for the brickwork. All the rest of the buildings on the layout were constructed in a similar manner. The only structure that was subcontracted due to an impending show deadline was the manse, which was constructed for me by Phil Taylor.

Converting the stock

All the stock is either proprietary or kit-built and has been re-wheeled or re-gauged for EM. I am parsimonious when it comes to modelling, so where possible I pull the wheels on ready-to-run OO gauge locos out by around a millimetre either side using a specialist wheel puller from GW Models. Now, whilst this is a simple operation (the hardest part is usually working out how to get the keeper plates off) it requires a fairly cautious approach as too much on one side and the wheel can come off the axle. Constant checking with a vernier and a back-to-back gauge is required, but it's a quick way of converting OO stock to EM.

Robert Stephenson and Hawthorn-built Type 1 (Class 20) D8101 was one of many of the class that worked in Scotland. It is pictured arriving from the Kinlochleven branch with a freight.

There is usually some thinning down of brake gear to do to avoid the blocks or rodding binding on the wheels, which can often be achieved by shaving them with a scalpel. The only locos I had trouble with were the Heljan Class 26 and 27s where there was just not enough meat on the axle ends to hold the wheels on firmly, so an order to Ultrastyle for drop-in wheelsets was placed.

Passenger and goods stock all use wheels from the Alan Gibson range. Some of the newer ready-to-run companies, such as Accurascale and Rapido Trains UK, are factoring in those of us who model in EM and P4, and accordingly are manufacturing stock where there is plenty of clearance for the finer scale wheels to just drop in.

Exhibition operation

Exhibitions are the only means of running *North Ballachulish*, as although the scenic boards were designed to be erected in the back bedroom at home, that's now occupied by a new layout build. Furthermore, taking the layout to shows provides opportunities to chat with other modellers. You sometimes get some remarkable comments; I had someone speak to me who had their wedding reception in the village hall, whilst many recognise the cottages and church on the corner and there was even a chap who claimed to have caught a train at North Ballachulish! He got a bit uppity when it was pointed out that there never was a railway there, but eventually reconciled himself that it might have been at Appin on the real Ballachulish branch.

I am very conscious of the need to entertain the paying public at exhibitions – years of experience of shows have taught me that if nothing is happening, observers move on very quickly and, whilst we all try to create cameo scenes for visitors to study in between moves, these seldom hold the attention span for very long.

Therefore, to keep things moving, a train will arrive from the Fort William end and come to a stop before a train from Oban is signalled in, so there's only one train moving at any time. Inevitably there are lulls in action when we are changing cassettes in the fiddle yard, so to keep things moving, the modelled stretch of the A82, which runs the full length of the layout, has cars, lorries and buses running on it using the Faller car system. Pride of place on this must go to the MacBraynes bus created by Phil Taylor, which is radio controlled and stops at the station – truly an integrated transport system in model form!

Further information

A video of the layout can be seen at: www.youtube.com/watch?v=_A8wX4IznX4

The layout is booked to appear at The EM Gauge Society's **expoEM Summer exhibition at Wakefield on 20 & 21 August** (see Societies & Clubs), at **Wigan on 1 & 2 October**, and at **Southampton in January 2023**.





▲ The Lancashire & Yorkshire Railway crest incorporates the red and white roses of the respective counties that it served.



Modelling the pre-Grouping railways

Lancashire & Yorkshire Railway spotlight

To mark this year's centenary of the Lancashire & Yorkshire Railway's merger with the LNWR, **TOBY JENNINGS** looks at the prospects for modelling the L&Y.

Photography as credited



Where the London & North Western Railway proudly called itself 'The Premier Line', the Lancashire & Yorkshire Railway similarly adopted the title of 'The Business Line' – but a century ago this year, the latter company disappeared in a merger with the former.

The L&Y – or the 'Lanky' as it is also affectionately known – remains somewhat under-rated by comparison with many other pre-Grouping companies, yet the 'Business Line' moniker was by no means undeserved. Connecting the great cities of the industrial north, it was one of the busiest, most profitable and forward-thinking railways of its day, possessing the fifth largest locomotive

fleet in the country, and was among the pioneers of suburban electrification.

Buildings and infrastructure

As its name suggests, the L&Y connected the 'Red Rose' and 'White Rose' counties, having originated as the Manchester & Leeds Railway – the L&Y name being adopted in 1847 following amalgamation with other smaller companies. With its absorption of the East Lancashire Railway in 1859 and the West Lancashire Railway in 1897, it achieved a virtual monopoly of railway traffic in Lancashire, and its network extended across the country to Goole. Such a system offers boundless possibilities for layouts, set amid

▲ A 1910s view of Lancashire & Yorkshire Railway superheated 4-4-0 No.1112 at the east end of Poulton station with a two-coach express. Photo: Rail Online

rugged Pennine fells or gritty industrial landscapes, in the style of *Askrigg Bank* (RM March 1996), *Dewsbury Midland* (RM December 2000) or *Tetleys Mills*.

Like most railways, the L&Y inherited a mix of architectural designs from its constituent companies, and initially delegated responsibility for buildings to the contractors undertaking the construction of new lines. Later, however, it developed its own distinctive style of 'standard' station buildings, often with relatively plain but

Todmorden station in West Yorkshire, close to the border with Lancashire, with one of the L&Y's ubiquitous passenger 2-4-2Ts carrying out shunting duties. The station building has typical examples of the company's glazed canopies. Photo: The L&YR Society.



handsome glazed canopies. Modellers with some scratch-building experience should be able to reproduce these structures without too much difficulty, while the card kits for the island platform building, platform canopy, wall-backed platform canopy and parcel offices available in OO and N scale from Metcalfe Models would look the part on an L&Y layout, or could form a good basis for some kit-bashing.

A 4mm scale card kit of a standard L&Y Size 6 signal box is available from Hall Royd Junction (see panel) along with 4mm and 7mm scale transfers for L&Y signal arms. The latter are intended for use with etched parts by Model Signal Engineers, available from Wizard Models Ltd along with several other L&Y items, including coach kits.

Locomotives and rolling stock

Only two L&Y locomotive classes are currently available ready-to-run, but fortunately, they are two of the most useful to modellers. In OO gauge, Bachmann produces the Aspinall Class 5 2-4-2T, a class of 330 engines allocated to virtually all the L&Y's sheds, and which was estimated in 1909 to be responsible for 56% of the company's passenger train mileage. A model of No.1042 in L&Y lined black features in this year's catalogue, and preserved National Collection example No.1008 has previously been produced for the Locomotion Models range. With these, and the forthcoming L&Y six-wheeled and four-wheeled coaches in the Hattons 'Genesis' range, a representative 'Lanky' passenger train can be assembled.

This OO gauge L&Y-liveried 2-4-2T, No.1042, will be available from Bachmann in June or July. Photo: Bachmann

Perhaps the most popular L&Y locomotive design was also one of its smallest – the Aspinall Class 21 or 'Pug' 0-4-0ST, immortalised by the Kitmaster plastic kit that later passed into the hands of Airfix and still features in today's Dapol range. The 'Pug' is available ready-to-run in OO gauge from Hornby and in O gauge from Tower Models, allowing you to recreate the L&Y on even the smallest layout.

There is surely scope, however, for a

manufacturer to produce one or both of the 'Lanky's ubiquitous 0-6-0 freight engines – the Barton Wright Class 25 'Ironclad', which could be offered in green livery as applied to preserved example No.957 for the filming of *The Railway Children*, or the Aspinall Class 27, which could be found in other locations including Nuneaton and North Wales.

If modelling the LMS or BR period, 'Crab' 2-6-0s (produced by Bachmann in OO gauge) are essential; effectively L&Y engines in all

The distinctive L&Y brown and umber colour scheme is to be offered in the Hattons 'Genesis' range of four- and six-wheeled coaches. Image: Hattons





▲ **Class 27 0-6-0 No.1300 pilots Class 25 0-6-0 No.957 with L&Y carriages at the Keighley & Worth Valley Railway on 14 October 2012.**
Photo: Neil Harvey

but name, these were designed by the company's last Chief Mechanical Engineer, George Hughes, but did not appear until after the Grouping, under Sir Henry Fowler. Though used across the LMS network, they always remained most closely associated with their spiritual home of the L&Y's operating area.

For kit-builders, a good selection of L&Y locomotives, coaches and wagons can be obtained – even some of the more obscure designs. A full list for 2mm, 4mm and 7mm scales can be found on the Lancashire & Yorkshire Railway Society's website (see panel) but one of the most remarkable finds was via the 3D printing platform Shapeways, which offers bodies for L&Y steam railmotors, and even the electric multiple units which were introduced on the Bury-Manchester line in 1916.

Some ROD (Robinson 8K) 2-8-0s briefly worked on the L&Y after the First World War before being transferred to the LNWR, and

Calderwood, built by John Dilnot and Dave Kirby, depicts an L&Y station in the West Riding of Yorkshire circa 1910, in OO gauge.

Photo: The L&YR Society



with Bachmann's model available this year in LNWR livery – along with other R-T-R LNWR engines such as the 'Coal Tank' and 'Super D' – there is also scope for a layout to represent the latter-day history of the L&Y/LNWR merger.

Alternatively, if you really want a challenge or to build something that stands out from the crowd, why not imagine what might have been if the merger, or indeed the Grouping, had never taken place? Towards the end of his reign as the first CME of the LMS, Hughes had designs on the drawing board for Pacifics and 2-8-2s that looked for all the world like enlarged 'Crabs', and in 1914 had proposed a four-cylinder 2-10-0 for the L&Y to handle trans-Pennine coal traffic.

Smaller scale

There is even something for narrow gauge modellers – the extensive 18" gauge internal railway system which extended for 7½ miles around Horwich Works, operated by a fleet of 0-4-0STs built there and at Beyer Peacock. While this would rely on scratch-building, it raises some interesting possibilities for cameo scenes combining standard and 18" gauge on the same layout, and keeping alive the memory of the L&Y's great workshops – the last major railway works in Britain to be built on a greenfield site, and which has

The L&YR Society

Formed in 1950, the Lancashire & Yorkshire Railway Society is the UK's oldest line society, with an extensive archive of photographs and drawings, and has produced several useful publications. It also has Modelling Coordinators for 2mm, 4mm and 7mm scales, as well as Gauges 1 and 3. More details, and its list of currently available L&Y models, can be found at www.lyrs.org.uk

Membership Officer Ken Carter can be contacted at: 11 Waveney Close, Arnold, Nottingham NG5 6QH.



been referred to as 'a Swindon of the North', but has sadly been partially demolished to make way for a new road.

If you need any more inspiration, plans to mark the centenary of the merger this year include guided tours of the remains of Horwich Works, and an event at Manchester Victoria in September, while operational L&Y engines can be found at the East Lancashire and Keighley & Worth Valley railways.

All these things, hopefully, will restore the rather overlooked 'Lanky' to its rightful prominence – and it's also an appropriate time to do so by celebrating the company in miniature.

Useful links and suppliers

Hall Royd Junction
J K Wallace, 7 Portobello Close,
Chesham, Bucks HP5 2PL
www.hall-royd-junction.co.uk

Wizard Models Ltd
PO Box 70, Barton-upon-Humber,
DN18 5XY
www.wizardmodels.ltd

Shapeways
www.shapeways.com

The Lancashire & Yorkshire 'Dreadnought' 4-6-0s

To complement our spotlight on the L&Y on p548, we present these 4mm scale drawings for one of the company's most imposing locomotive designs. The drawings were prepared – together with the accompanying prototype notes – by the late **IAN BEATTIE**.



The Lancashire & Yorkshire Railway did not contain the route mileage for sustained high-speed passenger operation, so that when a class of colossal 4-6-0 express engines emerged from the LYR works at Horwich beginning in 1908 there were many surprised reactions, and indeed the LYR's lack of experience in designing modern main line hauliers (the Aspinall Atlantics notwithstanding) quickly became evident: the locomotives (L&Y Nos.1506-25) were reputed to be asthmatic, sluggish, had a horrendous coal consumption and suffered from too many inherent faults such as inadequate bearing surfaces.

On top of all this the 4-6-0s were very heavy on maintenance, to the point where at any one time up to 50% of the class were undergoing repairs simultaneously. Clearly something drastic had to be done to resolve the situation, and to the LYR locomotive chief George Hughes' great credit, he thoroughly revamped what should have been his masterpiece, though the advent of WWI

delayed the planned rebuilding programme.

Having been launched at much the same time as the huge Royal Naval capital ships, Hughes' very big 4-6-0s were given the same name – 'Dreadnought' – which remained with the class throughout all its subsequent duplications and vicissitudes. Rebuilding entailed so complete a transfiguration that in essence the results were entirely new engines: the original 20 'Dreadnoughts' comprised saturated boilers (in an era when superheating was a very recent but recognisably desirable breakthrough) and four cylinders directed by inside-mounted Joy's

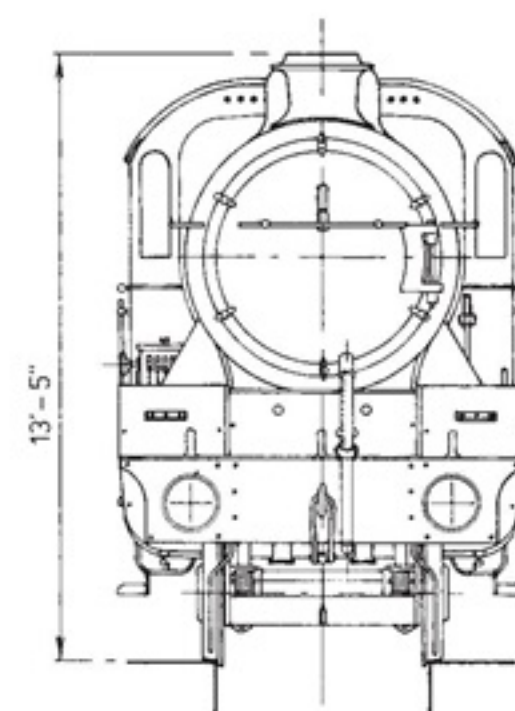
An early 1920s view of No.1515 leaving Poulton-le-Fylde with an Up express from Blackpool. This was one of the first batch of 20 locomotives completed in 1908, 15 of which were subsequently rebuilt; No.1515 was one of five that remained in original condition. Photo: Rail-Online (J M Tomlinson/Rail Archive Stephenson)

▲ No.1674 is pictured at Crewe North MPD in 1923 and is reflective of the configuration depicted with the drawings. Completed at Horwich in June of that year, the locomotive was one of the batch built under LNWR auspices (the L&Y having been amalgamated with the LNWR in 1922). It was withdrawn in February 1936 as LMS No.10445. Photo: Rail-Online





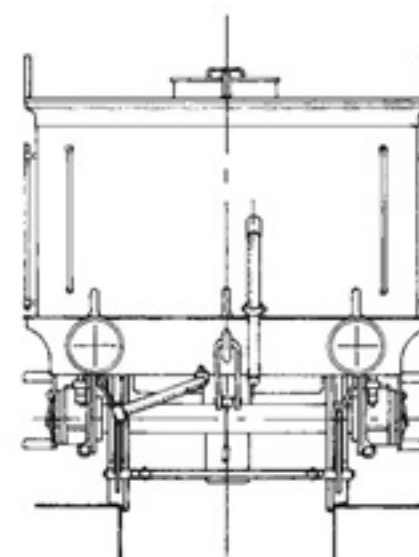
L&Y 'Dreadnought' 4-6-0



Technical data

Overall length:	60' 1 3/4"
Cylinders: bore/stroke	4, 16 1/2" x 26"
Boiler pressure:	180 lbs/sq in
Heating surface:	2238 sq ft
Grate area:	27 sq ft
Tractive effort:	28,800 lbs
Weight in working order:	119 tons
Coal capacity:	6 tons
Water capacity:	3000 gallons
Number built:	75, 1908-25

FT 0 1 2 3 4 5 6 7 8
SCALE



valve gear beloved of the LYR – the outer cylinders were controlled by rocking arms. The rebuilds were equipped with the latest technology, ie. superheated boilers, four piston-valved cylinders controlled by outside Walschaert's valve motion (in the reverse of the original, the inside cylinders were linked by rocking arms to the gear), stretched wheelbase via new frames, windowed cabs and much more besides.

The rebuilt engines were still not perfect – five of the original batch were not revamped (L&Y Nos.1507/08/12/13/15), and were scrapped in 1925-6 – but at least they could handle with confidence the greatly increased size of trains for which they had been hurriedly mooted in the first place. 10 further 'Dreadnoughts' were outshopped by the LYR in 1921 (L&Y Nos.1649-1658), followed by 25 more (Nos.1659-1683) in 1922-23 under the aegis first of the LNWR (the LYR and LNWR merged in January 1922) and then, upon the Grouping in January 1923, the newly formed London Midland & Scottish Railway.

A series of 30 4-6-4 tank locomotives intimately based upon the 'Dreadnoughts' was propounded to release the 4-6-0s from mundane duties to exclusively main line use; however, after only 10 hit the rails the remaining 20, for which components had been gathered, were turned out as tender locomotives and formed the final, somewhat mechanically revised batch of 'Dreadnoughts' of 1924-5 (L&Y Nos.1694-1713). These last engines sported longer (7' wheelbase) bogies, slightly redesigned boilers and – externally obvious – much more sharply angular cab rooflines; the longer frames are evident in their sweeping curve to the bufferbeam, which provide the best visual balance of all the three 'Dreadnought' variations.

As performers the entire class (LMS

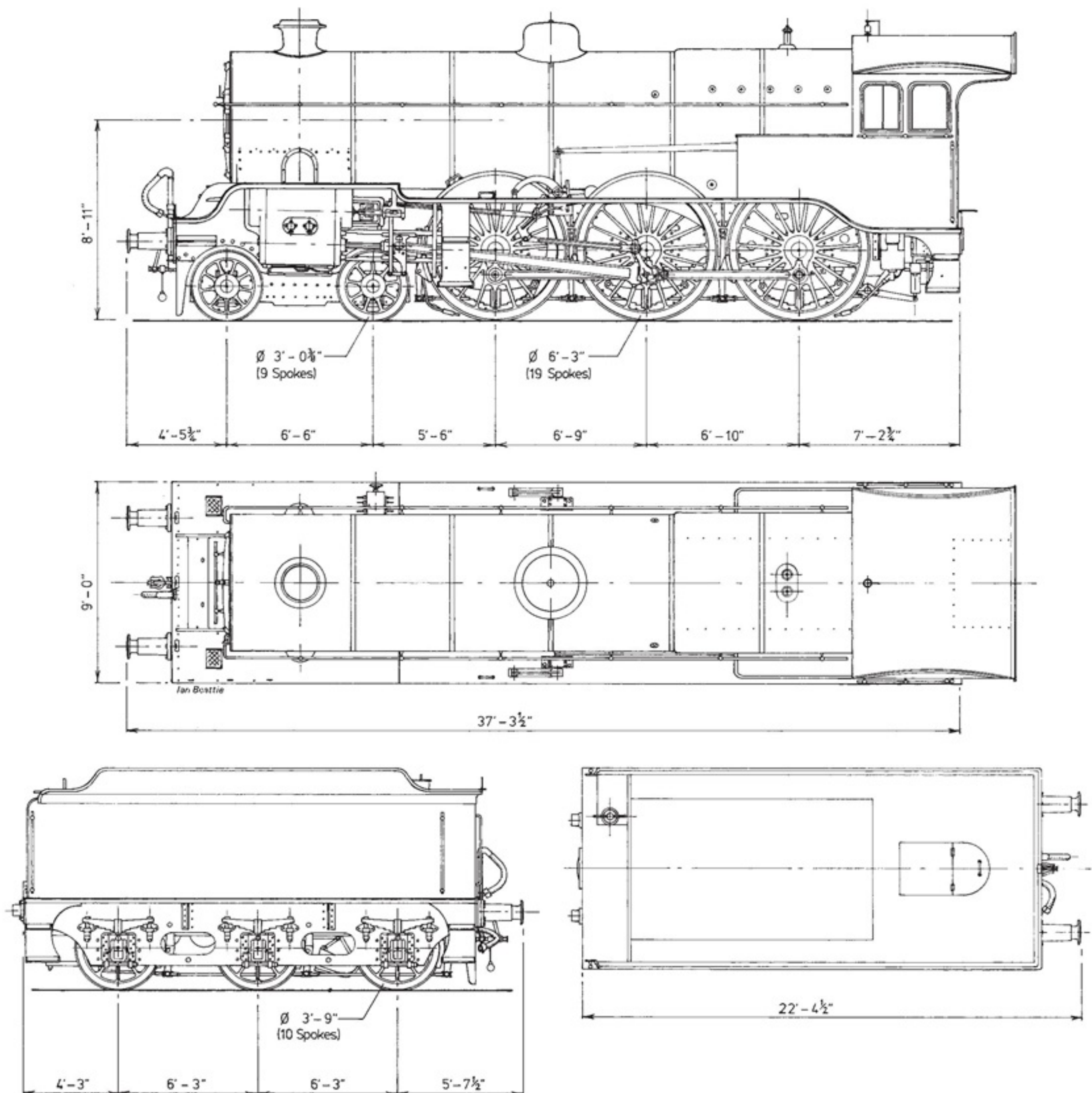
One example of the class received a BR number, this locomotive being from the last batch of 20 to be built – distinguishable from the previous batches with the longer (7' wheelbase) bogie, more angular cab roofline and longer frames; the latter created a shallower sweeping curve of the front section of running plate to the bufferbeam. Numbered 50455, the sole BR-liveried class member is pictured at York on 1 July 1951, during its farewell railtour. Photo: Rail Photoprints

▲ No.10435 is pictured at Wakefield shed in 1930 in lined LMS livery. Entering service in January 1923 as No.1664, it was withdrawn from traffic in February 1936. Note the smaller pattern of tender fitted to this example. Photo: Mike Morant Collection

Nos.10405-74) was in actuality on a par with their close contemporaries, the LNWR 'Claughton' 4-6-0s. George Hughes became CME of the LMS in 1923, which was fortuitous for his largest designs, and they were put to use handling West Coast Main Line expresses alongside their LNWR counterparts. Hughes retired in 1925, leading the way to different engine philosophies against which his 4-6-0s suffered by comparison: relative to the pure LMS Royal Scots and Patriots, and thereafter the excellent Stanier 4-6-0s, the 'Dreadnoughts' were far from competitive and, on boiler replacements falling due – no spares had been manufactured – the class was progressively withdrawn, from 1933 onwards. No fewer than 55 left revenue-earning service in 1934-7 and had WWII not intervened the class would have been extinct earlier than 1951, when the final example (No.50455, the only one to receive its BR number) was withdrawn and unfortunately not preserved.

As an experiment 1924-built 'Dreadnought' No.10456 was converted in 1926 to compound working, incorporating 22" x 26" low pressure and 16" x 26" high pressure





cylinders, to evaluate such a scheme for a proposed Hughes Pacific, which came to nought. Although very successful and certainly popular with footplate crews, the compound eked out its existence working between Crewe and Carlisle and was shedded together with its 19 last-batch stablemates at Carlisle Upperby.

In model form

The 'Dreadnought' 4-6-0s have not to date been the subject of a ready-to-run model in any of the popular modelling scales.

A 4mm kit was produced many years ago by Millholme Models. Long since out of production, examples do occasionally become available second-hand.

As an aid to 4mm scratch-builders there are profiled and milled locomotive and tender frames in brass (designed to fit the Millholme kit) available from Alan Gibson Workshop: www.alangibsonworkshop.com

A 7mm kit that was originally available from Warren Shephard is now listed as part of the Lanky Kits range: www.lankykits.co.uk

Notes on the drawings

The drawings and the specification table refer to the 'second' style of 'Dreadnought', ie. the rebuilds of the original engines plus the 1921-23 versions, comprising 50 engines in total or the majority of the class. The tender is the large LYR-type fitted to most post-1923 engines (and earlier ones eventually) of LMS provenance. As a sop to clarity the fallplate is shown with the engine – in fact it was hinged to the tender footplate. [Ed – These drawings and prototype notes were originally published in the February 1987 issue of RM]

Summerseat

CHRIS FORD outlines the history of this passing station on the former East Lancashire Railway route north of Manchester, which is presented here as a practical suggestion in OO gauge.

Summerseat station opened on 28 September 1846 and formed an intermediate stopping point on the East Lancashire Railway route between Clifton Junction and Rawtenstall.

Set on an embankment with two low platforms, a single-storey station building was situated on the southbound side (containing booking and waiting facilities), together with a small goods yard that comprised a stone-built goods shed, three sidings, and a 1½ ton crane. A waiting shelter was provided on the northbound platform. Access between the platforms was originally by a barrow crossing which was located opposite the station building, this arrangement being replaced by a footbridge in 1961.

At the time of opening five passenger trains ran on weekdays in each direction between Manchester and Rawtenstall, with four trains on Sundays. With the opening of the Accrington line in 1848 Summerseat was also served by trains running between Colne and Manchester Victoria.

The L&Y takes over

In 1859 the Lancashire & Yorkshire Railway absorbed the ELR, the company using the route as an alternative between Manchester and Leeds, and routed holiday traffic through Summerseat.

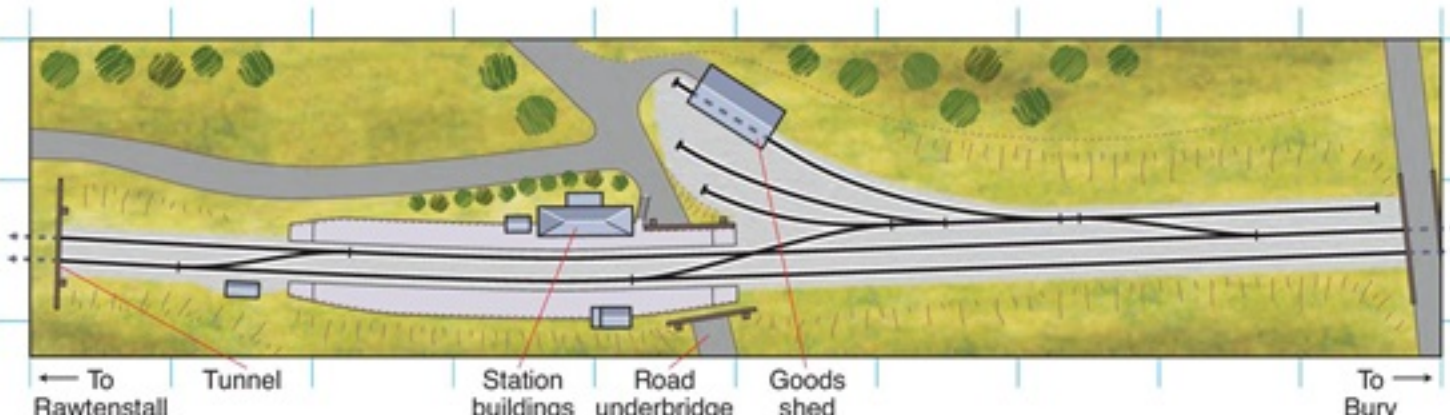
The station building was rebuilt in the same style as the original and in 1875 a wooden signal box was opened at the north end of the northbound platform, replacing the small box on the southbound platform. In 1890 the platforms were extended and raised.

Grouping and beyond

Following the 1923 Grouping, Summerseat became part of the LMS via the London & North Western Railway. By the mid-1950s Summerseat was host to 35 trains in each direction between Bury and Bacup and 13 in



▲ Summerseat station looking north towards Rawtenstall, showing the signal box at the north end of the platform and the goods yard on the right. Photos: www.railwaystationphotographs.co.uk



Overall layout size 10' x 2' 3". Each grid square = 1' x 1'.

each direction between Colne and Manchester Victoria, this providing a half-hourly service.

Closures

The goods yard closed in 1964, the signal box in 1967 and in 1968 Summerseat became unmanned. In April 1970 the northbound track was lifted, the line becoming a single-track branch with the station building being demolished in 1971. The last passenger service ran on 3 June 1972, though coal trains serving a depot at Rawtenstall continued to pass through Summerseat until 1980. In 1981 a railtour ran before the line was officially closed.

Rebirth

A year later a railtour ran that was organised by the East Lancashire Railway Preservation Group, and negotiations were conducted with British Rail with a view to reopening

the line from Bury to Rawtenstall. These were successful and a stretch of line from Bury to Ramsbottom was acquired in July 1987, with trains running through to Rawtenstall from 27 April 1991.

In its preserved form Summerseat retains only the former southbound platform in use and just a single-track running line, with little evidence of its original infrastructure remaining. However, the former goods shed still stands, albeit now in private use.

Notes on the plan

The accompanying plan has been drawn for OO gauge with a scenic area measuring 10' x 2' 3". The key elements of the station site in its steam era heyday have been retained, with the time period set prior to the installation of the footbridge. Some selective compression has been undertaken, whilst the portal of Brooksbottom Tunnel has been repositioned much closer at the north end of the station to provide a convenient scenic break. A road overbridge has been suggested at the south end.

The scenic section could be arranged as part of an end-to-end scheme with two fiddle yards, or as part of a continuous run with a single fiddle yard.

Further information about the history of Summerseat station can be found online at: www.disused-stations.org.uk



◀ A view looking south towards Bury, with the goods shed visible on the left. Note also the change in platform level showing where this structure was extended in 1890.

A postscript to the Thames-Forth Express

IAN NUTTALL describes how a Ratio Midland Railway clerestory coach kit was used as the basis of a Diagram 536 MR 45' Full Brake in 4mm scale, which he constructed to replace a less accurate Tri-ang based version running as part of the 'Thames-Forth Express' formation on his *Settle & Carlisle*-inspired layout.

Photography by the author

In the article describing my depiction of the 'Thames-Forth Express' published in the July 2021 issue, I mentioned how I had discovered that the clerestory full brake, assigned as an extra coach to the basic 'Thames-Forth' formation by my late, valued friend Ron Smith, proved not to be accurate. The model had hoodwinked me, and no doubt others, as it had looked the part and provided an extra, characterful dimension to the express. Hence, I approved of its presence in the train, but I decided it was time to have a go at building a more accurate replacement.

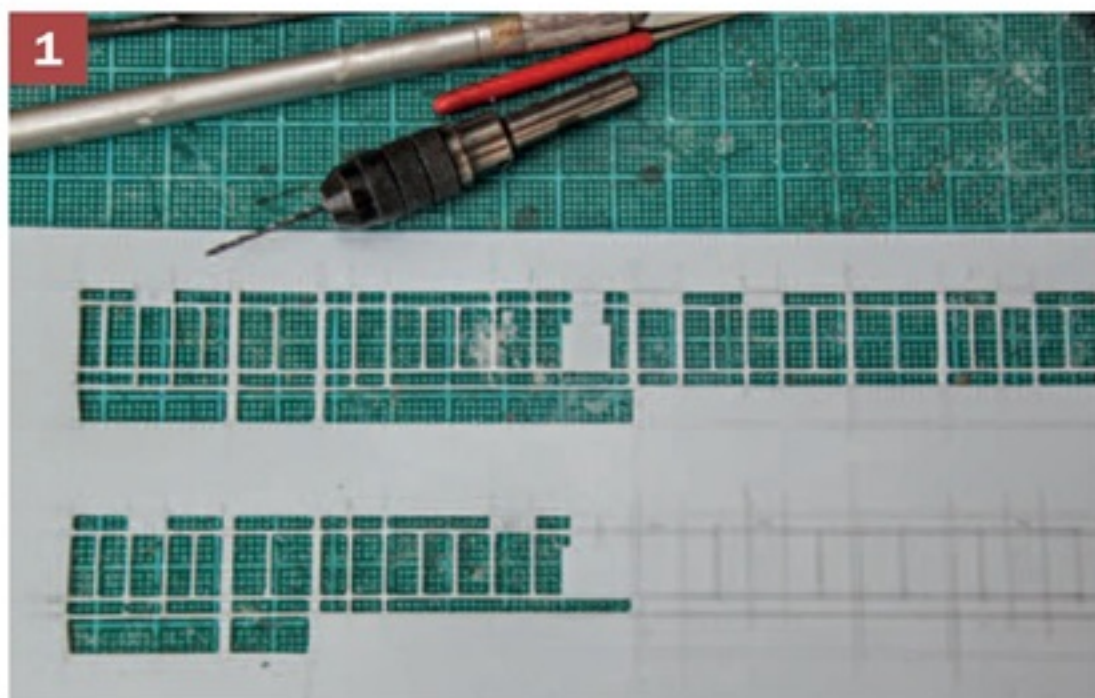
The venerable (and compromised) model constructed more than 50 years ago by Ron Smith that was based on an ex-MR 45' Gangwayed Passenger Full Brake. It was described in the July 2021 issue. It was most probably converted from a Tri-ang GWR clerestory ref. R332 (not R337 as indicated in the original article).

A chance to admire my replacement model of the clerestory full brake in action as an additional vehicle to the northbound 'Thames-Forth Express' over Dent Head Viaduct. Purists will note this should be southbound if it has the two milk tanks attached. The pair of tankers were converted from Hornby R-T-R models to represent the four-bolster variant and were covered briefly in my *Milk Traffic for Dentdale* article (see RM June 2017).



▲ A view of the completed Midland Railway 45' Full Brake that forms the subject of this article, shown here prior to final weathering.





▲ If I couldn't create the beading for the panelling this project was a non-starter. Once I could see progress being made, I had the incentive to persevere. Those of you with a sharp eye will note one missing short waist beading. I added a 10 x 25thou replacement once the delicate lattice was secure on the coach side.

History

After earlier arc-roofed examples, the Midland Railway brought out its clerestory variations. To me, associated with the Bain era in Midland coach history, these are the archetypal, classic, handsome Midland carriages; they are elegant, stylish, photogenic, and proved worthy flag-bearers for the parent company and latterly earned their corn under the LMS banner.

Square panelling was the order of the day in the late 1800s, but an even more graceful effect was achieved when round-cornered panelling saw the light of day. My coach, built to Diagram 536 in 1906, was one of these. 16 appeared in the first batch, all built with special shelving for pigeon traffic. The second batch of 23 (Lot 675) had some which were not so specifically assigned; I hope mine is one of these.

However, the two reference tomes in my railway library do not quite concur. *Midland Carriages* by R Essery and D Jenkinson (OPC 1984), staunch supporters of all things Midland and LMS, table all 39 examples as for pigeon traffic, whilst Lacy and Dow, in their two volume treatise *Midland Railway Carriages* (Wild Swan 1984-6) say some of

the second batch were for more general traffic. Both sources are recognised bibles, yet both can't be right. I've gone with the latter in this instance as it suits my needs. As an aside, after waiting years for an authoritative work on Midland Railway coaches, two appeared at the same time; on a par with London buses! Essery and Jenkinson refer to my Dia.536 Passenger Brake, but Lacy and Dow go a step or two further by providing a scale drawing, to 3mm:1ft, so I had the means to forge ahead.

Adapting the Ratio kit

Since Ron Smith built his 45' Dia.536 ex-MR Full Brake over 50 years ago we, as modellers, have benefited from the introduction of ground-breaking techniques plus easy access to much more prototype information. The injection-moulded Ratio clerestory-roofed coaches are readily available and, whilst they are 48' long, could form the basis of an accurate model. I had several in my 'to be built one day' box, so I used one of these.

By reducing the floor, roof and solebars by 12mm, my Ratio kit was now the correct length for my vehicle. Use of a fine-toothed saw, filler and judicious sanding where required made the join invisible. I ditched the underframe trussing and substituted brass wire which was more robust; I was able to use the bogies as provided, adjusted the steps to suit and fitted brass bearings and Alan Gibson 14mm disc wheels to aid smooth running.



▲ The successful positioning of the beading overlay on the coach side provided me with a huge sense of relief! Note that all the window surrounds have been painted so that I could add the glazing and window bars before I painted the rest of the body.

The ends were fine mouldings, but the panel corners were square. By fitting tiny triangular fillets of 10thou plasticard I was able to replicate the rounded panelling required.

New plasticard sides

The major challenge was to make new sides, and by enlarging the 3mm drawing by 133% I had a 4mm scale drawing from which I could take measurements and transfer these to plasticard sheet. The 'inside' layer was 30thou with the windows cut out slightly larger than needed so the edges wouldn't be seen. The region below waist level was scribed to create a set of grooves or furrows at 1.5mm apart. This caused the sheet to curve and create the tumblehome. I added filler to the grooves which I then sanded smooth once it had set.

A 10thou layer with the windows was cut out to exact size, and finally the panelling layer was formed from the same material. Needless to say, this last layer formed the greatest challenge and occupied many an hour, although the tools required were very basic. Each panel corner was drilled 1.6mm, the holes were then joined up using a steel rule, new knife blade and steady hand. Flat and round fine needle files were used for any minor adjustments. Yes, patience and perseverance were necessary, but not any outlandish level of skill; the key was not to rush and to replace the knife blade if it lost any sharpness. I am sure the eagle-eyed will spot that not all the beading is exactly the

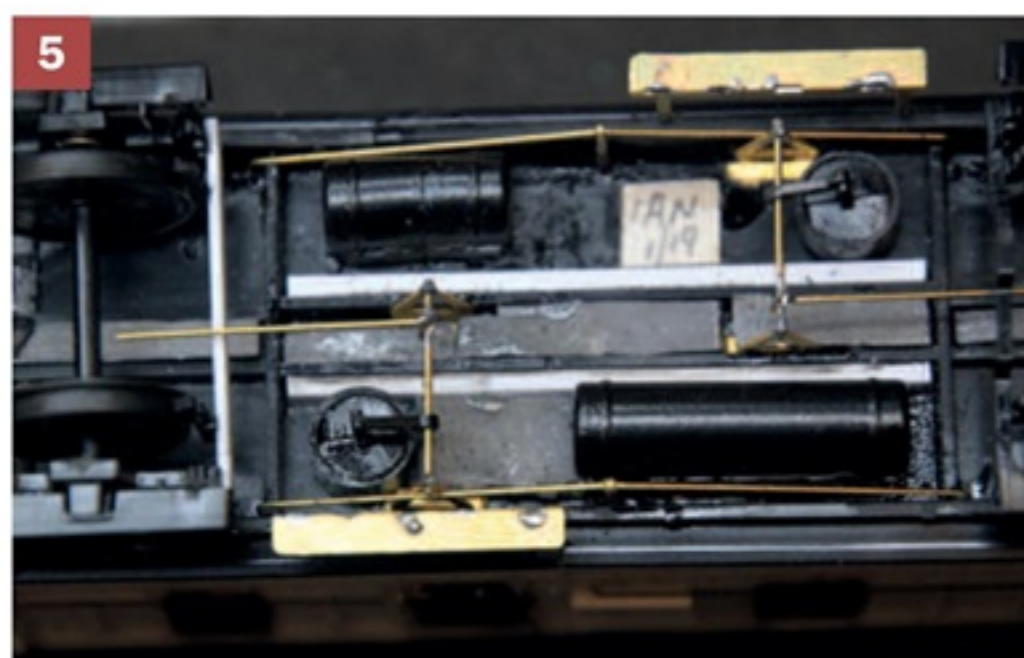


▲ I suspected prior to assembling the body that this bowing of the vehicle sides was likely to happen – despite the use of substantial 3.2 x 3.2mm square plasticard corner bracing; or maybe the bowing occurred because of them?



▲ I countered the bowing effect of the sides with partitions of 40thou plasticard. The chassis helped greatly at solebar level. My method of mounting the window bars is apparent, whilst the partitions stop short of cantrail height to allow me to make a sub roof of 60thou plasticard.

The two substantial white plasticard strips are acting as strengtheners to realign everything after I had cut out 12mm from the chassis length. One cylinder is shortened: I admit to following guidance in the Branchlines kit for the square-panelled version here. V-hangers are Comet, steps are spares from an etched brake van kit, and lead is added where possible as I forgot to put it in the body before I glued the roof on!



same width, as it would be from a CAD aided etching, but I trust the results are more than passable. Various details are needed to complete the project:

- i) The buffers are ABS short MR castings with the white metal heads and shanks replaced with turned brass items.
- ii) 0.35mm holes were drilled in lengths of 30 x 60thou plasticard to take the 0.3mm brass wire for the window bars; five to each plain window, none behind the door.
- iii) Corridor connections are from York Model Railway and are of a concertina pattern in thin card. I have added the MR style outer door from 10thou plasticard, the 'scissors' from 10 x 30thou strip with tissue over the top.
- iv) Side, roof and end handrails are all from 0.3mm brass wire, whilst the end lamp irons are from scrap brass etch.
- v) The roof rainstrip was not sufficiently proud so I replaced it with 10 x 25 thou strip.
- vi) The underframe trussing is 0.45mm brass wire with scrap etch for the brackets.
- vii) The low hanging steps between the bogies are from spares I had from an etched brake van kit...never throw anything away! Although 3 x 1mm brass angle could be used.
- viii) Internal partitions of 40thou sheet corrected any bowing of the sides, and I added a false roof of 60thou sheet to provide a stable base for the clerestory.



▲ This shows how I amended the York Model Railway corridor connections, together with my use of 0.3mm brass for handrails on the side and end, the tiny triangular fillets to replicate the rounded cornering to the end panels, scrap etch for the lamp iron and 10 x 25thou strip + 10thou cubes for rivets to embellish the handrails and lamp irons.

ix) Brake and steam heat pipes are from ABS and Comet respectively, whilst Comet is also the source for the V-hangers under the chassis.

x) Side vents above some of the windows are created by drawing a fine-toothed saw blade several times along a strip of plasticard to make the grooves; the door

runners are 10 x 30thou strip. Door handles are turned brass for the 'T' ones (luxury!) and 0.3mm brass wire crushed in a pin vice for the lever variety.

To finish, Phoenix Precision LMS Crimson Lake (Dull) was applied by brush, followed by HMRS LMS lining and coach transfers. Weathering was from a wash from a palette of Matt Black, Matt White, Earth and Rust followed by dry brushing and a hint here and there of weathering powders. A final touch was the lamps, Modelu for the end and Springside for the side.

Coupling upgrade

By this time I had spotted magnetic couplings marketed by GLR Services. Their core is miniature neodymium magnets which have proved to be seriously dynamic. These 3mm button magnets never give the impression that they would uncouple under load. I went for the firm's non-NEM version. Self-tapping screws are provided, which I applied to around 50% of my coaches, although for the rest, I was able to re-use the 8BA bolts already *in situ*.

However, the coaches now stood more than 3mm further apart. I bridged the gap by inserting an LMS Round Top concertina card corridor connection available from Fair Price Models; these proved admirable. When mounting the couplings, I found it crucial to incorporate vertical play, by the simple expedient of not tightening up the screw/bolt fully.

Conclusions

I am not suggesting that you all leap up and start building one of these yourself; after all my motivation was to do it as a tribute to my late friend, Ron Smith. But wouldn't it be wonderful if just a few fellow readers took up the baton and showed that they too could create a slice of stylish railway history in 4mm scale? I am pleased to have been able to complete this addition to my stock. I don't deny it was a challenge at times, but a fulfilling one, and I can now retire Ron's version gracefully.



▲ Here is the unblemished, pristine, finished Dia.D536 model. I have struggled to confirm exactly how many roof handrails I should feature; I think the number changed through the vehicle's life.



▲ I had decided that my coach would be entering its final years in service, hence a reasonable amount of weathering and distressing was in order, without, hopefully, wrecking my many hours of work. A very fine grey crayon was used to create the crossed-out 'St. Pancras' destination superseded by 'Carlisle'.

Southampton East goes West!

GEOFF GREEN describes the latest extension to his Network SouthEast-inspired N gauge system.

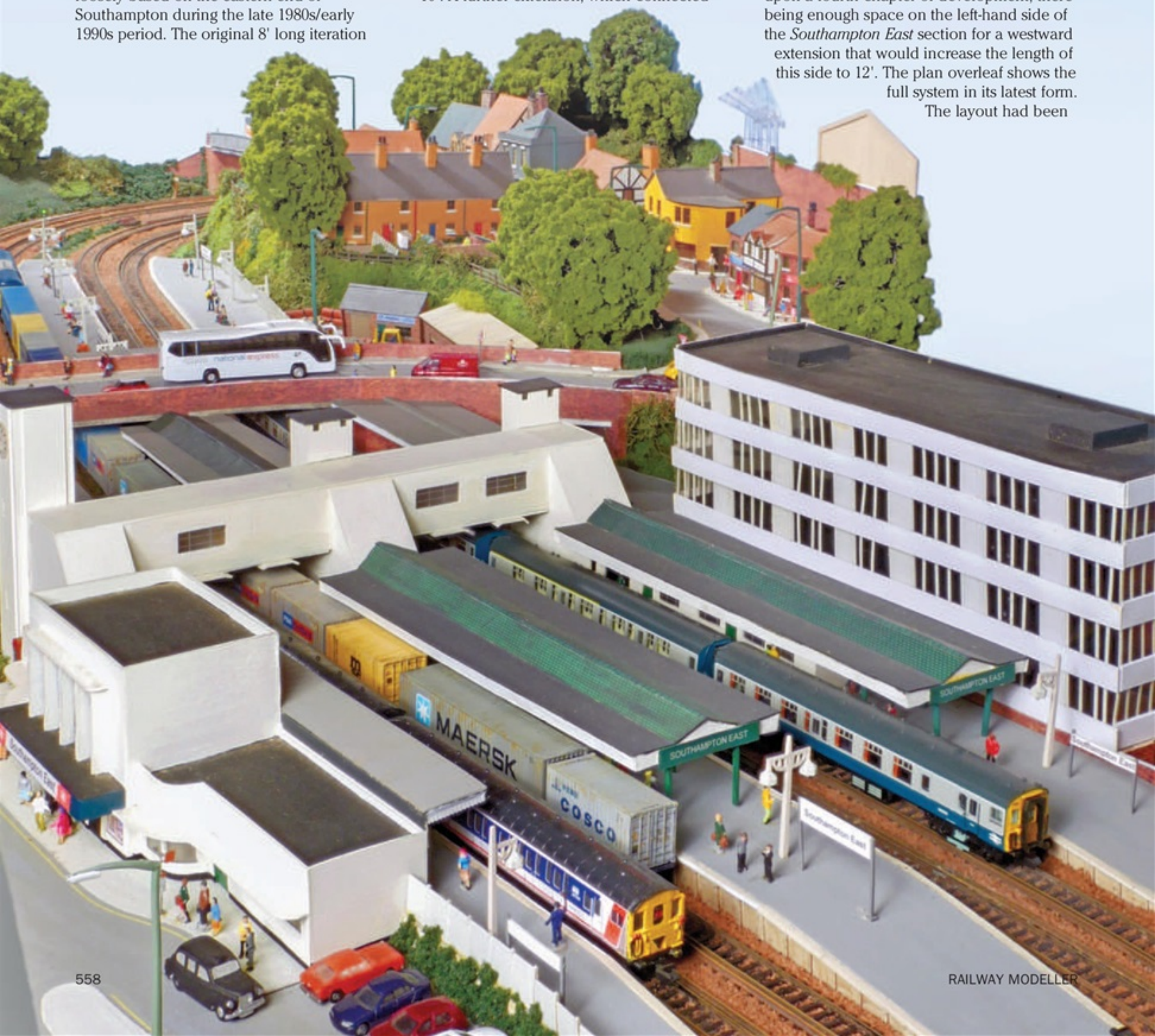
Photography by Ian Manderson

This is the fourth instalment in the pages of this magazine chronicling the gradual development of my N gauge model railway *Southampton East*. Located in the garage, this project started out as a model loosely based on the eastern end of Southampton during the late 1980s/early 1990s period. The original 8' long iteration

was described in the July 2007 issue, with the first extension (titled *Canute Road*) outlined in the July 2009 edition. This changed the layout footprint into an L-shape and saw the length of the original section increased to 10'. A further extension, which connected

onto the end of *Canute Road*, was presented in the January 2012 issue as *Ocean Quay*.

I had considered the evolution of the layout to have reached its conclusion, but a suggestion was made that led me to embark upon a fourth chapter of development, there being enough space on the left-hand side of the *Southampton East* section for a westward extension that would increase the length of this side to 12'. The plan overleaf shows the full system in its latest form. The layout had been





▲ A 4-CEP unit is ready to depart Platform 1 for London Waterloo while BR Class 47 Co-Co 47 096 heads a container train from the Midlands to the Western Docks.

◀ A busy scene at Southampton East looking westwards. The platforms have been re-aligned and extended for the layout extension. The new canopies were cut from Evergreen plastic sheet and finished with plain plasticard valances.

constructed on a framework of Dexion racking, so it was easy to add some more lengths of this to create a cantilevered support with suitable bracing for the latest extension. The baseboard comprises 12mm chipboard covered with insulation board and supported by 2" x 1" timber.

New platforms and track

I decided to extend the station platforms westwards, remove the hidden curves at that end of the layout and increase the scenic area. This gave an extra six square feet to model and kept me busy for a good few months! After selling some older locos and rolling stock, I purchased some more Peco code 55 finescale track to match the existing layout. I also had plenty of older code 80 track left over from a previous layout, which I stripped down so that I could re-use the rail to model the conductor rails; gluing the code 80 rail on top of the code 55 sleepers gave the correct height to the top of the conductor rail.

I had decided to add a facing crossover using large radii points so that trains going in

an easterly direction could access Platforms 3 and 4 and thus use all four platforms. This would allow trains to terminate and turn round; something that I had already been able to do with westbound trains. Initially I thought of just extending the existing platforms, but this would have created a rather sharp curve halfway along which would not have looked very satisfactory. Instead they were all rebuilt to achieve a more pleasing alignment with a long smooth curve. Now six-coach trains can be accommodated quite easily.

Hiding the curves

I decided to try and disguise the sharp curvature of the tracks at the west end by adding a road overbridge as the inner track

▲ The village scene buildings are largely made from card, and some use the now discontinued Builder Plus brick paper. About to cross the central bridge is a Routemaster bus which was originally part of a key ring!

▲ A Dapol BRCW Type 3 Class 33 rounds the curve into the station with the returning empty newspaper train to Waterloo. The 2-EPB is a long way from its usual south London habitat. This has Electra Rail self-adhesive sides over a Mk.I body shell sitting on a Farish railcar chassis. Note the lineside trunking, relay boxes and staff.





◀ The village scene is made up of buildings that were made over 35 years ago all from card after being inspired by John Ahern and Allan Downes, and these were updated and improved as required. An 'Aunt Sally' game is in progress at one of the pubs.

has a 9" radius. I think that this has worked because the curve is viewed as two separate vistas when looking from the platform ends to the bridge and then looking from the bridge along the rear of the layout. Unfortunately, there was no room to add additional tracks representing the sidings at the west end of the station, nor the signal box, but then I already had one placed at the other end of the central platform as at Woking.

Signalling or not?

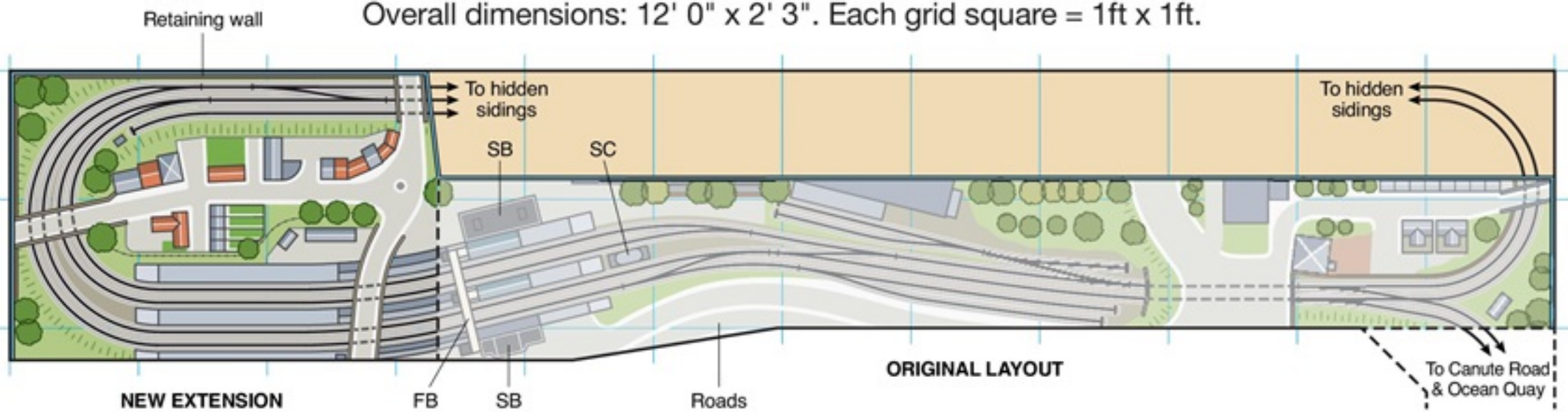
There are no signals on the rear of the layout, although there should be one for eastbound trains on the other side of the road bridge leading from the storage sidings with feathers to indicate the crossover and the slow line. I considered signals for westbound trains at the end of the four-track section to complete the vista, but these would only be about 350 scale feet from the signals at the platform

An overall view of the layout extension illustrating the new platform alignment, now long enough to take six-coach trains. All the buildings are scratch-built, most are 35 to 45 years old and have stood the test of time. The trees were made more recently.



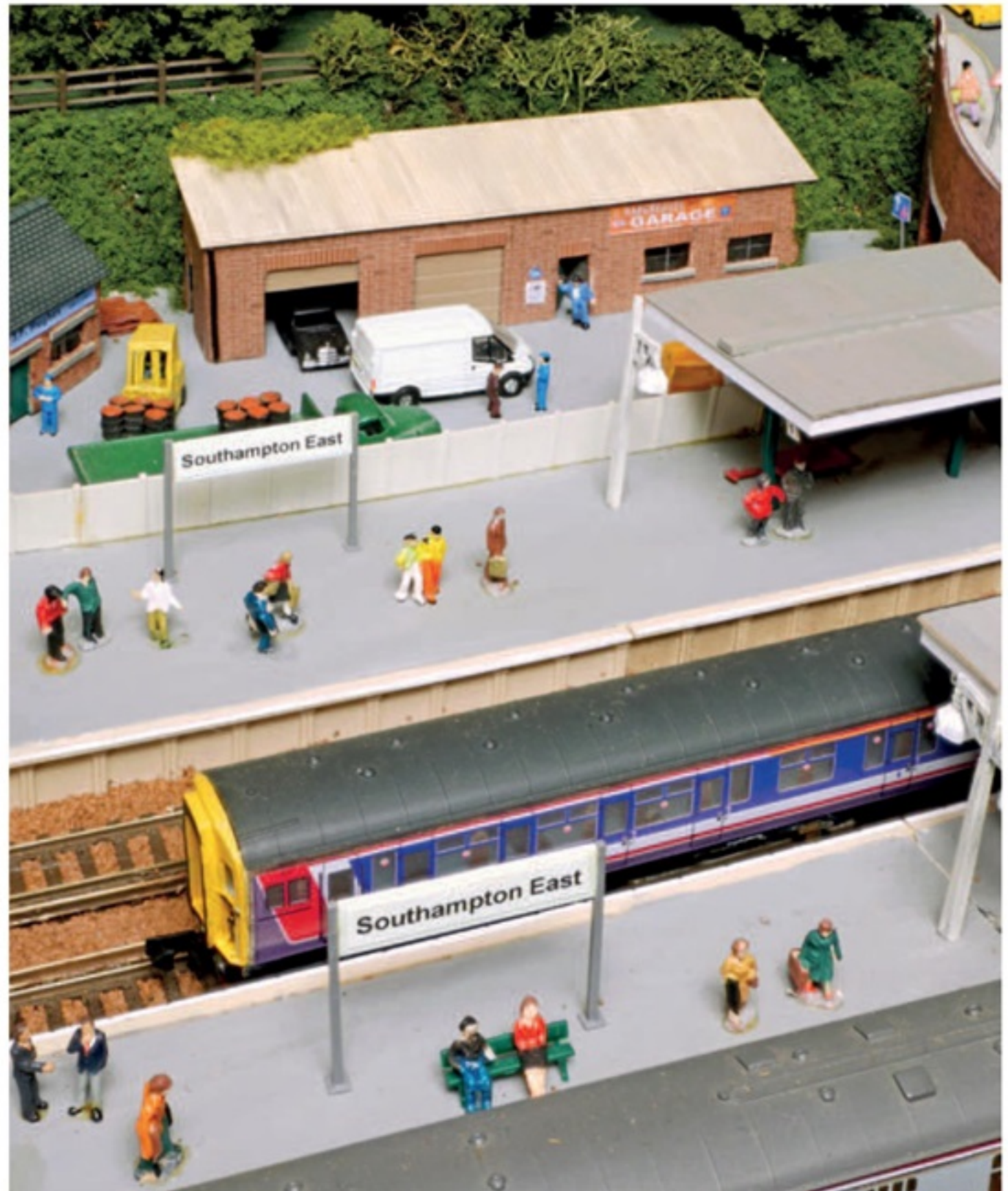
SOUTHAMPTON EAST

Overall dimensions: 12' 0" x 2' 3". Each grid square = 1ft x 1ft.



ends, so not far enough. The track layout follows the more conventional station layout with the fast lines using the inner platforms and the slow lines using the outer platforms like Woking and Basingstoke. The real Southampton Central has this the other way round with the fast lines on the outer platforms. This is because the next station down the line at Millbrook has an island platform serving the inner slow lines only. Fast trains pass by on the outer lines without stopping.

A typical lineside industry next to the station. The platform is populated with figures from Farish and various items picked up at exhibitions. A 4-VEP in NSE livery on a local service has just arrived.



Buildings and scenery

Luckily, I was able to re-use the existing station buildings which saved some time, and I made some additional platform canopies which were fitted to both sides of the re-aligned road bridge. The canopies were cut from Evergreen plastic sheet and finished with plain plasticard valances. The uprights were from the spares box left over from a Ratio kit.

I decided to reuse some of my older buildings that I had made over 35 years ago

all from card (cereal boxes) after being inspired by John Ahern and Allan Downes, and these were updated and improved as required. It would have been a shame not to use them in the freelance village setting that I decided to add on that part of the layout. Some structures have used the excellent Builder Plus range of brick papers but sadly these are no longer available. Fortunately, however, I had one sheet left intact which I was able to photocopy to produce a few more. This ensured the

A Dapol BRCW Class 33 rolls through on a westbound newspaper train, while a preserved 5-BEL unit arrives with an excursion. The conductor rail is stripped-out code 80 rail stuck to the Code 55 track sleepers.



appearance of the brickwork remained consistent.

The Tudor-styled buildings were made by adding strips of card onto the walls and then painting them a brown/black colour. Chimneys were made from the plastic tubing normally used for wire-in-tube point operation, whilst the drainpipes were from straightened paperclips. All the houses have gardens with grass, paving and sheds to different designs which I scratch-built one evening. Some also have children playing.

Detailing

I decided that one pub should have a game of Aunt Sally in progress; this is a league game local to Oxfordshire pubs where six wooden sticks are thrown at a wooden dolly which is supported on a metal pole. The object of the game is to knock the dolly off its support without hitting the metal pole.

I added a lot more trees (see RM Oct 2011 for a description of how I made these) to reinforce the rural aspect of the scene, some of which also fill in odd corners here and there. It seemed that modern cars, in particular from the 1980s onwards, were very hard to find but I am glad that Oxford Diecast have now begun producing suitable vehicles for the period, so more will be added in due course. The platforms are well populated with figures from the Graham Farish range and those found on stands at model railway exhibitions. After all, this is a busy commuter station although everyone seems to be waiting for the next train! The local streets are also busy with people going about their daily business.

EMUs and locomotives

A couple of years ago I purchased the Arnold/Hornby 5-BEL, which I suppose is preserved and being used on railtours. It runs superbly and looks great with the table lamps

The 2-EPB is on its way to the storage sidings along the back of the layout passing a Class 47 on freight duties. The ED Class 73 is paused in the engineers siding.



▲ The Black Swan pub stands sentinel over the scene with a classic Rail blue/grey 4-CEP and a NSE liveried Class 50 Co-Co 50 002 waiting at the platforms.

being lit. I am hoping that the firm will produce another EMU in due course. May I suggest a 4-CIG unit in both blue & grey and NSE liveries?

The 4-VEP has been updated with TPM cast cab ends and the latest coach sides from Electra Rail Graphics. In addition I bought a Graham Farish railcar and used the chassis to power a 2-EPB; the chassis fits easily under a Mk.I coach body without any modification. The only downside is that the detail parts for the railcar drive shafts are still present on the finished model as they are moulded to the

bogie sides, but they are hardly noticeable. Both of these units are in NSE livery.

What next?

I have been trialling Dapol Easi-shunt couplings on certain locomotives and rolling stock; they work well and certainly look good so these will be deployed across the fleet as required. I can only uncouple at fixed locations instead of anywhere on the layout using a 'shunters pole', but I suppose you can't have it both ways. I also hope to add seated passengers to the coaches in due course, although empty coaches are less noticeable than the steam engines with empty cabs that are often seen on layouts! And finally, many thanks to Ian Manderson for the superb photographs.



Rhapsody in blue

Attracted to the striking experimental blue livery schemes that were applied to express passenger locomotives during the early years of British Railways, **SACHA CASE** set about creating a collection of examples in miniature by repainting ready-to-run models.



◀ The photo that started it all! Gresley A4 No.60028 *Walter K. Whigham* is pictured at Grantham shed in June 1948 resplendent in British Railways experimental blue livery. Three other A4s received this livery scheme – Nos.60024/7/9. Photo: Colour Rail



This all began when I came across references in a book to the experimental blue/purple livery that was applied to a small number of Gresley A4 Pacifics early in the British Railways era.

Further investigation revealed that there was a version of Dapol's N gauge model that was commissioned by Modellbahn Union in this livery, finished as No.60027 *Merlin*. I managed to acquire one of these and it instantly became one of my most prized models.

I then discovered that it wasn't

just the Eastern Region A4s that received this livery, but also examples of the most powerful express passenger locomotives from each of the other three regions of BR: namely the Collett King 4-6-0s (Western), Stanier Coronation Pacifics (Midland) and Bulleid Merchant Navy Pacifics (Southern). In addition, on the Eastern Region, Gresley A3 Pacifics also received this colour scheme.

A good book for reference is *The Big Four in Colour 1935-50* written by David Jenkinson in

collaboration with John Smart and John Edgington (published by Atlantic, ISBN 0 906899 62 1).

I found myself captivated by these livery schemes and decided that I wanted to try and complete 'the full set' in miniature. However, I soon realised that N gauge ready-to-run models in these liveries tend to be few and far between.

A Duchess and an A3

The only option was to undertake the repainting of suitable models

▲ Examples of the author's N gauge steam locomotive models that have been repainted into experimental BR blue liveries, including a Gresley A3 Pacific finished as No.60075 *St Frusquin*. Model photos by the author except where stated

myself. This led me to invest in airbrush equipment – this being something I had not attempted before. I bought an inexpensive airbrush package that could be used on various modelling tasks,

BR(M) Coronation Pacific

Six Stanier Coronation Pacifics were outshopped in experimental blue with LNWR-style red, cream and grey lining. The prototype photo shows No.46241 *City of Edinburgh* at Camden shed in 1949, whilst the model is presented as No.46231 *Duchess of Atholl*. The other locomotives so treated were Nos.46224/7/30. Archive photo: Colour Rail



BR(E) Gresley A3 Pacific

Seven Gresley A3s received various permutations of experimental BR blue with 'BRITISH RAILWAYS' lettering, prior to the adoption of 'standard' BR blue. One of these seven was No.60084 *Trigo*, seen here at Newcastle Central in 1949 wearing a dark blue scheme. The model is presented as No.60071 *Tranquil*, (note the different identity to that in the photo of this model opposite) whilst the other five examples comprised Nos.60036/45/74/5/91. Archive photo: Colour Rail



from layout trackwork to detailing.

The first two models I tackled were the Coronation Pacific and Gresley A3. For the former I obtained a suitable Graham Farish (by Bachmann) model from the latest tooling. For the latter, I used a Dapol model finished as *Flying Scotsman* that I had acquired second-hand for a very reasonable sum.

Using this particular version of the A3 meant that there would be certain compromises, but I was happy to accept these to keep my project within budget. For example, the model featured the later LNER non-corridor tender, rather than the GNR pattern that the prototype of my model had. It also had a double chimney, which would not have been correct in this early BR period.

Repainting into blue

Now on to the process of repainting these two locos. I began by stripping the loco bodies and tender tops from the chassis and removing any separately fitted details that don't need to be painted like handrails and cab interiors.

My paint stripper of choice is the good old brown Dettol antiseptic disinfectant liquid. I have found that it works well for me, but if you aren't a fan of the smell, be aware as it lingers in the plastic for a while! (Note: I have found that white vinegar neutralises the smell and rinsing the stripped locomotive bodies in this can help.)

Once stripped, the next task was to apply a primer coat. For these first two models I used Phoenix Precision Grey Primer through the airbrush (I used Tamiya Fine Surface Grey Primer for the later models – both gave good results).

For the main body colour,

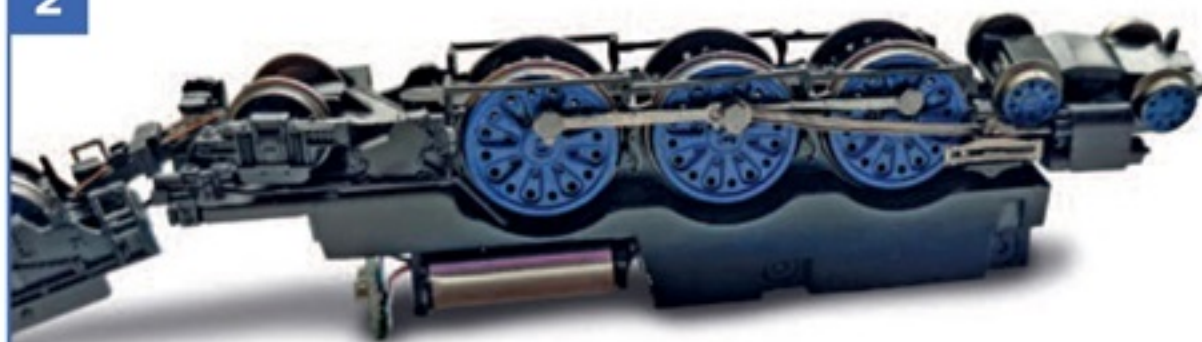
The repainting process described

1



The five models were all repainted in a similar manner, with only minor variations of methods outlined in the main text. After dismantling the locomotives, the bodies were first stripped of their factory-applied liveries using Dettol antiseptic disinfectant liquid. Illustrated is the Merchant Navy model after the paint had been removed.

2



The experimental livery being replicated on the Merchant Navy model required the locomotive wheel faces to be painted blue. This was undertaken using a brush to apply the Precision Paints enamel.

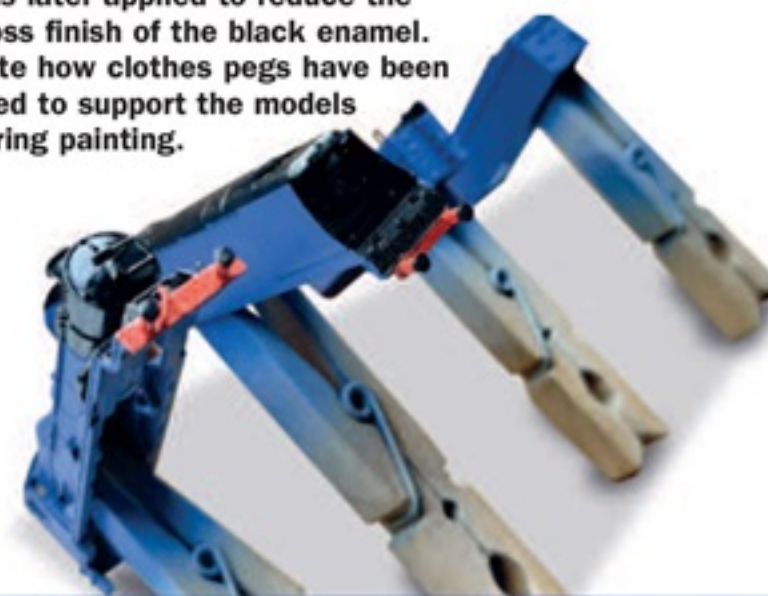
3



After first applying a grey primer, the locomotive bodies were then sprayed blue using an airbrush, with the enamel paint mixed in a 3:2 ratio of paint to thinners. Pictured here are the bodies of the Merchant Navy and 'King'.

4

With the blue base colour applied, black areas and the red bufferbeams were completed by hand. Matt varnish was later applied to reduce the gloss finish of the black enamel. Note how clothes pegs have been used to support the models during painting.



5

Lining and lettering was added using waterslide transfers obtained from Fox and Railtec (see panel). The lining is the LNER style with red/cream/grey colours, whilst the 'BRITISH RAILWAYS' lettering reflects the original post-Nationalisation branding that was carried prior to the adoption of the early BR emblem.



BR(S) Bulleid Merchant Navy

BR-built Bulleid Merchant Navy Pacific No.35024 wore experimental blue livery with red stripes and early BR emblems for a short period in early 1949. The stripes and painted wheel faces were derived from the Southern Railway livery style illustrated with this view of No.21C12 at Nine Elms in April 1947. The model below uses as its basis the current Graham Farish (by Bachmann) tooling, released in 2015. Archive photo: Colour Rail



each model received two coats of Phoenix Precision Steam Locomotive Dark Blue (ref. P174). This is quite a thick paint and I found that a 3:2 paint to thinner ratio gave the best results through my Harder & Steenbeck airbrush.

I found that painting the black parts (smokebox, cab roof, tender top and running plate etc.) was easier to do by hand using fine brushes ranging from 000 down to 10/0.

With the painting completed it was on to the lettering and lining. For all my models, I use Fox Transfers for both the waterslide decals and etched nameplates required. The experimental livery scheme uses the same lining style as the LNWR/BR mixed traffic scheme (as available from

Fox) consisting of red and cream lining.

To seal the transfers, a coat of gloss varnish was applied, again with the airbrush. A couple of thin coats of this helped keep everything fixed in place before going over the black sections using a matt varnish. (The more coats, the more matt the finish, so the number of coats is down to personal preference, depending what finish you're after.)

To complete the models it was then just a case of reattaching the bodies to the chassis. The first two models were then ready for service.

Next up – a Bulleid Pacific

The Bulleid 'air-smoothed' Merchant Navy Pacific was completed in much the same

way, using one of the latest generation Graham Farish (by Bachmann) models as the basis. The main difference was the painting of the wheel faces, a task that was made easier by removing all the non-driving wheels completely from the chassis and the brake rigging from the driving wheels. It would be possible to remove the drivers completely from the connecting rods and chassis as well, but I managed to work around it without a problem.

A Castle fit for a King

The model of the Collett King 4-6-0 presented a particular challenge because there isn't an RTR model in N gauge to modern standards. I could have sourced one of the last China-made

Graham Farish Kings with the lower profile blackened wheels and such. However, these models are from rather old tooling and wouldn't be straightforward to equip for DCC (my layout being configured for digital control).

Therefore I decided to use as my starting point a latest generation Graham Farish (by Bachmann) Castle model and convert it to the best of my abilities to resemble a King. I knew there would be compromises with this approach (including the diameter of the driving wheels) but overall, it seemed to present the best option.

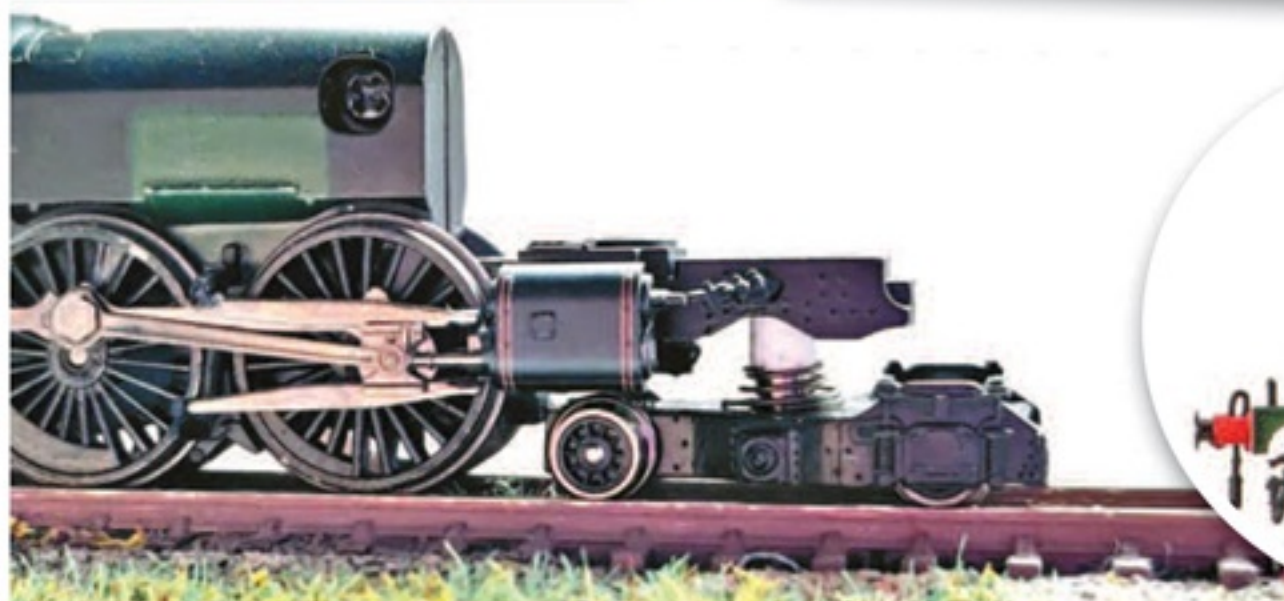
I set about finding all the parts I would need to undertake the conversion. The safety valve and

BR(W) Collett King 4-6-0

Collett King Class 4-6-0 No.6026 *King John* is pictured in May 1948 wearing experimental BR blue livery, as was also applied to class members Nos.6001/9/25. The model is presented as No.6026 and started out as a Graham Farish (by Bachmann) Castle. Archive photo: Colour Rail



To convert the Graham Farish (by Bachmann) Castle model into a representation of a King, a replacement front bogie was required. Observe the different method of attaching the King bogie to the model compared to the original arrangement on the Castle. Inset photo by Jolyon Sargent



front bogie were both parts that I could change over without too much difficulty; the new brass safety valve came from N Brass Locos, but where to find a King class bogie? Examples of the old R-T-R King model are becoming hard enough to find as it is, let alone one that's cheap enough to use as a parts donor. I tried the usual vendors like Peter's Spares and Farish 'N' Spares, but without success. It was suggested that I tried Bachmann Europe Plc (that now owns the former Graham Farish range) and amazingly, after a quick conversation, a brand new bogie and spring were on their way to me in the post.

With the Castle body off and stripped down for repainting, I started on the chassis changes. The Castle bogie was removed from its mounting arm, for which the retaining screw was attached from above. However, the mounting screw for the King bogie was intended to go up from the bogie into the loco body shell.

To get around this, I used some styrene tube that had an inside diameter slightly smaller than the outside diameter of the bogie mounting screw. This was to allow the screw to be turned into the tube and self-tap its way in. The tube was then attached to the Castle chassis using superglue, with shims of styrene added to fill the areas of daylight above the bogie (whilst ensuring that it could still rotate unimpeded).

Small styrene pieces were also added around the front of the locomotive body to better replicate the King's front-end appearance.

The Castle safety valve was ground off using a rotary tool

before being smoothed with files, sandpaper and a thin smear of superglue (to help fill any cracks that may show themselves after painting). I attached the new safety valve with superglue after first removing the injector pipes. This was because – for my inexperienced hands – these parts proved too fiddly to work with and get to sit neatly around the boiler. I may try and add these at some stage in the future, however.

Other than that there were no further changes to the body and the main painting process continued on in much the same way as described already.

A matching set – almost!

Once all the locos were completed, I created a display case for them from a picture frame and some custom sized pieces of polished clear acrylic to use as shelves. The five models were then displayed proudly on the wall. However, there was something that didn't quite sit right; my original Modellbahn Union model of *Merlin* was a slightly different shade to the rest. Eventually I decided to undertake the repainting of a suitable donor A4 model to provide me with a matching set, this one being finished as No.60024 *Kingfisher*.

A second A4

The repainting process for the second A4 followed much the same process as before with the other models, but on this occasion I elected not to separate the body from the chassis because there were a number of fragile components connecting the two together. Therefore, prior to priming and painting, I made a painting mask from styrene that



▲ A display case was made for the set of N gauge models, these covering examples of locomotive classes from each of the four BR regions. Note that the A4 pictured here is the Modellbahn Union release.

was slid between the cylinder cover and the cylinders themselves that covered the wheels completely. This worked surprisingly well, although I should have used small pieces of masking tape to cover areas where some minor overspray occurred.

The wheels also had to be painted black because the donor model was in LNER Garter blue with red wheels (this being the most affordable example I could find).

It was at this stage that I noticed a glaring error I had made with the previously completed models that is evident in some of the accompanying photos; the cabside numbers and 'BRITISH RAILWAYS' tender

lettering were all in white and not the correct off white/cream! I was able to rectify this using some spare Fox Transfers tender lettering that I had in the correct colour, in addition to which I purchased some Railtec decals with the required five-digit locomotive numbers and shed codes already made up – no more fiddling and trying to align separate digits!

A pleasing quintet

The set of five repainted models now look much more pleasing to the eye displayed in their case on the wall. I will eventually be fitting them all with sound (*Kingfisher* is the only one that has this currently) but that's another project for another day.

BR(E) Gresley A4 Pacific

The second of the author's Gresley A4s in experimental BR blue livery was finished as No.60024 *Kingfisher*. A Dapol model was repainted to suit.



Supplier information

Fox Transfers
www.fox-transfers.co.uk

N Brass Locos
www.nbrasslocos.co.uk

Bachmann Europe Plc (spares)
www.bachmann-spares.co.uk

Railtec Transfers
www.railtec-models.com

Raven Hill

After many years of modelling in OO9, **GRAHAM WATLING** decided to make the move to something larger. The result is this Yorkshire-based exhibition piece in 7mm scale.

Photography by the author

For the last few years, I have been trying to settle into a new, satisfying scale after modelling in OO9 for 35 years. My recent narrow gauge layouts comprised *Longstone* (RM August 2018) and *The Three Scenes of Longstone* (RM January 2020), both Gn15 scale, then *Shortwaite Hill* in OO9 (RM March 2021). If nothing else, my venture into Gn15 (G scale on 16.5mm gauge track to represent 15" gauge) proved that I could model in a scale other than 4mm, following which, seeing several narrow gauge layouts in O9 (7mm scale on 9mm gauge track) at shows and in the pages of *RAILWAY MODELLER* attracted me very much to this larger scale.

A scale trial

It took me a while to grasp 7mm scale and before jumping in at the deep end with a large exhibition layout, I decided to build an operating diorama in a drawer turned on its

The loco shed and goods depot area. *Oread* was the author's first loco in O9; a Unit Models O-081 diesel kit, seen here with *Satyr* on the engine shed siding.

front (see opposite). This immediately presented a ready-made diorama frame with baseboard, backscene boards and even a fixing for lighting above the scene. The baseboard scenic area was only 20" x 4½", but by using Peco OO9 irregular sleeper track I was able to lay two facing points which gave an approach track (through a hole inside the drawer) a goods platform, passenger platform and a loco siding. This little project gave me a good introduction to the scale and allowed me to test one or two new locos and shunt a few items of stock around.

This trial led to the building of the exhibition layout featured here named *Raven Hill*. It is 7' 2" long x 20" wide on two boards with an approach track along a scenic hillside ledge into a village through station.



▲ *Satyr* hauls the first two 7mm coaches that the author made, which are modified Hornby OO gauge four-wheelers on Peco OO9 bogies.





▲ The small diorama, which was built as a test piece (in a drawer turned on its end) prior to starting the construction of *Raven Hill*.

The two boards are mainly built of 6mm ply with 12mm ends. Although the Peco OO9 track is not quite to scale for 7mm, 15" gauge, it has been used again, and with a little weathering and weeds applied I am happy with it.

The track is pinned onto Woodland Scenics N scale foam trackbed and there are two storage loops behind the backscene. The points are controlled by wire-in-tube attached to slide switches to change the frog polarity.

A little background

In its fictional history the railway was built to supply timber and stone to a new building development high on the cliffs at Peak on the North Yorkshire coast, but the developers eventually pulled out after only a handful of structures were built, as there was no demand for houses so high on the cliffs and so far above the rocky beach below. This was inspired by the story of Ravenscar, in North Yorkshire. So now, on the model, and many years later, the line is being re-laid and

run by a local landowner with a few weird and wonderful locomotives for the benefit of the local population, holidaymakers and day-trippers.

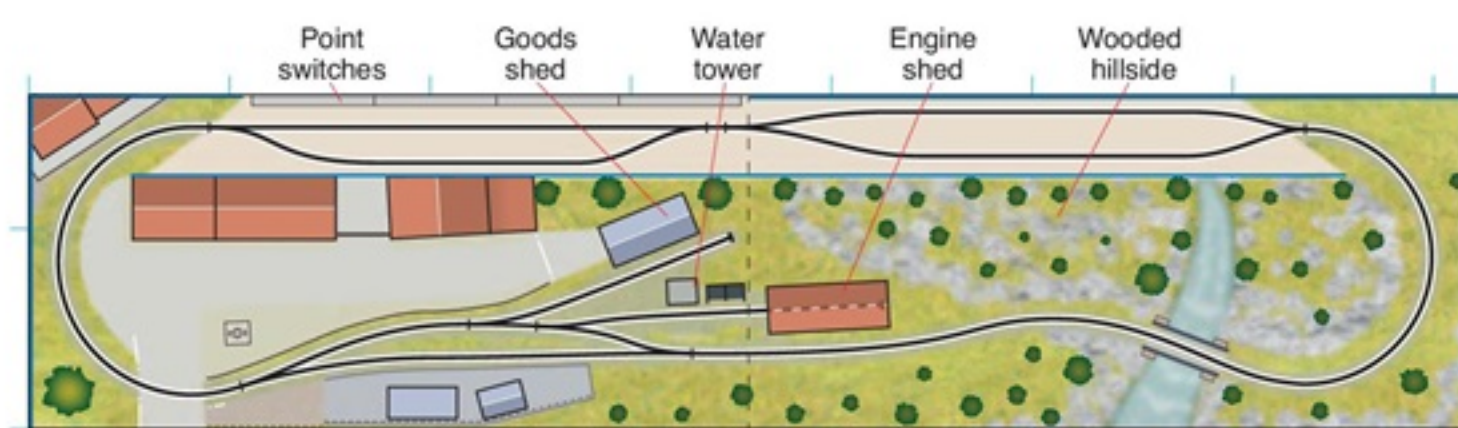
On to the scenery

As with previous layouts, my wife helped me with the scenery. We built the scenic hillside first and after I made the bridge abutments, my wife painted the sky on the backscenes. The hill was then built up using polystyrene blocks, and a mix of lightweight filler, sawdust, PVA, acrylic paint and a little water to make the ground cover and rock faces. We coloured this using several dry-brushed acrylic shades for rocks, ground

and weathering. The waterfall was made using wool threads and gloss varnish with matt white enamel to create a little white-water effect. My wife then added the greenery using scatters, with Woodland Scenics foliage glued onto their largest tree armatures for small trees in 7mm scale.

Scratch-built structures

The station buildings and goods shed are resin kits, while the engine shed is scratch-built using Wills 4mm Corrugated Asbestos and windows from a second-hand HO scale engine shed. For the village buildings I used foam board shells with Redutex self-adhesive coverings or Slaters embossed plastic sheets



Raven Hill Overall layout size 7'2" x 1'8". Each grid square = 1' x 1'.



▲ Despite being modelled in 7mm scale, the layout takes up little more space than an equivalent scene in 4mm (OO9).



▲ *Orthus* crosses Porrits Bridge, built over the local waterfall. *Orthus* is a resin kit by Barry McDowell for a freelance diesel on a Kato four-wheel tram chassis. The waterfall was made from wool threads and varnish with matt white enamel to create a white-water effect.

▶ *Drake* (a 3D printed body on a Kato chassis) arrives in the village. A low relief cottage was built to fill this corner. Watercolour pictures from a calendar provide depth.



for stonework, and Juweela (1:35 scale) individual pantiles on thick card bases. Windows and doors are by York Modelmaking, 422 Modelmaking and Peco. I have used Peco O gauge lamps and platform seats, while many of the goods items and wagon loads are Skytrex mouldings, or from a toy farm set that my sons played with many years ago! Most of the figures are whitmetal by Omen or Phoenix.

7mm scale 09 stock

The locomotives are either modified 3D prints, resin or brass kits and most run on the

◀ Steam tram *Pyralis* arrives with a passenger train. The author acquired a couple of these old Avalon Line locomotives which run on Graham Farish 0-6-0 chassis.

Steam tram locomotive *Pyralls* crosses the road in the village.

reliable Kato four-wheel tram or Graham Farish 1109 0-6-0 chassis. Wagons are also resin kits running on Peco NR-121 N gauge wagon chassis, while coaches are either adapted OO bodies or scratch-built using Wills 4mm sheets running on Peco GR-106 OO9 bogies. One is a modified continental O scale body on an HOe chassis. I mainly use Peco GR-101 OO9 couplings for the stock.

If short stock is used, I find that a layout in O9 7mm scale takes up hardly any more space than OO9 in 4mm, despite it being 75% larger in real terms! A little extra clearance either side of the track and at passing loops is all that is required. I have found it a very enjoyable scale to model in, and you can utilise N, OO, OO9, HO and O proprietary items to help with all sorts of details.

Help and inspiration

My local modelling group is the Norfolk & Suffolk Narrow Gauge Modellers who meet

The village showing The Beacon Inn, and Yorke's Animal Feeds warehouse. The village buildings are made from foam board shells with Redutex or Slaters embossed plastic sheets for stonework, and Juweela individual pantiles on thick card bases.

monthly. We have some very talented modellers within the group and I would like to thank them for their inspiration and ideas, and also of course my wife for her help and support.

Further information

Norfolk & Suffolk Narrow Gauge Modellers group: www.nsngm.org.uk

Graham Watling's modelling blog: www.grums009andgn15lines.blogspot.com

More information about 7mm narrow gauge modelling can be found on the Association website: www.7mmnga.org.uk



Small scene, large scale!

KEITH APPERLEY set himself the challenge of building a small static narrow gauge diorama with sufficient detail to hold the attention of observers. This characterful model in 16mm:1ft scale is the result.

Photography by the author

Modelling in SM32 scale (16mm:1ft) presents its own challenges – such as having to incorporate lots of detail – but the handling of parts is less tortuous for those of us whose eyesight is no longer as good as it was. Furthermore, working with a narrow gauge subject means that most things are acceptable; wagons, locomotives and coaches can be of a freelance design and detailed as you wish without the need to be true to any actual prototype.

However, despite these benefits of modelling a narrow gauge subject in a large scale, the space that would be required for even a modest layout scheme in 16mm proved for me to be prohibitive, so a diorama seemed to be the way to go. I do realise that the idea of a static scene is not to everyone's liking, but there was no alternative for me. The challenge was to create a model that was small (just 23" x 19") but would hold the attention.

A permanent way scenario

Several scenarios were considered but a permanent way depot was chosen since it could be compact and full of equipment and small details. I decided that it would be set when the track gang were back from working up the line, having brought back old sleepers, timber, and rubbish to be dropped off from the wagons left in the yard at the end of the day. Odd bricks, blocks, pipes, and fencing timbers could be strewn around the place ready for future use or just left where they were discarded from previous maintenance work.

The period is set in the present day on a heritage railway, hence the informal and assorted attire of the figures who are positioned to show various tasks being undertaken.

Working on a static scene proved to be a refreshing change, this enabling me to become completely absorbed in the appearance and detailing of the scene without having to consider aspects such as layout control systems or electronics.



▲ The pallets were made from ice cream sticks suitably reduced in width, with the corner blocks formed from stripwood cut into cubes. The assemblies were joined together with PVA glue, holding the parts in a simple cardboard jig to ensure they went together square and true. The stickwood has a fine grain and can be left in its natural state (it will darken over time). However, I added dirt using fine soot powder applied by hand, with a wash of brown acrylic paint smeared over to give an aged look. The concrete blocks that support the barrel on the loading dock were cut from pine stripwood 10mm square. These were painted with a buff titanium acrylic paint and sprinkled with finely ground limestone from the garden.

▶ 16mm scale figures are a mixture of ready painted farm (from Papo) or military toy outlines which are relatively cheap to obtain. However, they generally need carving up to some extent to get the required finish. The figure operating the chain block is a soldier from the M&C Toy soldier collection; the military paraphernalia was removed and the movable limbs set in the required position, with the joints then filled with Das air-drying clay before repainting.



▶ Vegetation is made from the tips of oregano plants that had gone to seed – suitably dried in a box filled with silica gel which dries the plant but keeps its colour. Dried grasses are gardening string teased out to individual strands and attached to the base individually. Odd small twigs from garden bushes were added to bulk out the scene, and static grass was also added to give a rough natural look. Leaves on the climbers and creeping plants are dyed coarse sawdust fixed in place with spray glue.





▲ The slate roof was made from thick paper layered on a card substructure. Each course is one piece with the gaps between the tiles created by cutting with a Stanley knife, which was fitted with two blades so that a slot was cut rather than just a line. Again, thin paint washes dirtied the tiles and moss was made by adding dyed sawdust to blobs of acrylic glue. The gutters are Plastruct pipe cut in half lengthways and downpipes are old aluminium knitting needles that were bent to shape.

▲ The main building was fabricated from individual panels of plywood covered in household filler. The mortar courses were then scored into it using a toothpick before it dried fully. The assembled sides were next painted with thin washes of acrylic paint, giving a weathered effect, whilst a suitable darkening of the stone was achieved with a wash of black paint where water was likely to have run down the walls.



▲ The rainwater barrel taking water from the shed roof was made from old vacuum cleaner tubing with plastic strip wrapped around to create the rings around the body. A card plug was fitted inside near the top, which was then painted black before pouring over a thin layer of varnish to simulate water. Textured rust patches were added using burnt umber acrylic paint with very finely ground terracotta pot dust sprinkled on top.

My approach is to study the real world for detail and inspiration, use natural materials as far as possible and be inventive with textures and finishes. The wagons at the loading dock were made from odd pieces of oak stripwood onto which 4mm scale wheels and longer 32mm gauge axles were mounted. The bodies are metal pressings that I picked up from industrial scrap bins years ago and have been lying around waiting for a purpose. The wagon loads are various bits of twig, moss, and coarse stone chippings.





A tribute to Toller

MYLES MUNSEY created a small OO gauge model of this wayside station to rekindle happy memories of the former Bridport branch in Dorset.

Photography by the author

I was re-reading the book *The Bridport Branch* by B L Jackson and M J Tattersall that prompted me to make a model of one of the locations along the branch. My interest with this particular route can be traced back to 28 April 1975 when I travelled on this charismatic line; then the last remaining branch line in Dorset, and days away from closure, it retained its charm right to the end. As an impressionable youngster it had a certain effect on me; I think it was the fact that it lingered on – seemingly in a time-warped – that captivated me.

I decided to create a small diorama to try and capture the character of the branch in 4mm scale.

Setting the scene

The first consideration was choosing which of the branch's stations to model – Toller, Powerstock, or Bridport? I marginally favoured Toller, which was set amongst the rolling hills of Dorset, as the overbridge at the Bridport end offered a convenient scenic break and Toller church would make for an interesting addition to the backscene.

Several pictures of Toller in the 1970s survive in book form and online, so reference is easy to come by. Furthermore,



Toller station building survives in use at Totnes Riverside on the South Devon Railway and therefore I was able to visit to take some measurements to assist with the construction of the station building. A visit to the actual village of Toller Porcorum confirmed that the platform lives on and has been incorporated into a private garden. There is also some spearpoint fencing which helps to picture how the station entrance would have looked.

Forming a correlation between today's scene and that of 1975 isn't entirely straightforward, however; the station here had an open aspect with an attractive field at the southern edge sitting slightly lower than the

▲ Single diesel railcar W55033 calls at Toller during April 1975 – the same month that the author visited and just before the branch closed. Archive photos: John Vaughan/Rail Photoprints

trackbed. This I felt would make for a very attractive foreground and would define the site, much as the road overbridge would.

A vestige of the GWR remained in the 1970s with a saw-tooth valance on the station building, a GWR seat and GWR spear fencing. The station colours, however, were green and cream; these being a legacy of Southern Region control, the paint being applied in 1957. Although the Western Region resumed control in 1963, the green and cream remained until the end.

Initial stages

I ordered a pair of Scale Model Scenery BB009 laser-cut diorama boards which were glued together complete with backboards in little more than half an hour. I avoided end boards as the overbridge at the Bridport end and the PW hut at the Maiden Newton end provide adequate view blockers. With the two boards bolted together I had an area of just 31" x 8".

The platform and trackbed are raised on foam sheets so that a natural looking drop

◀ The only item of rolling stock is diesel railcar W55023, which is correct for the period and even carries the right headcode!



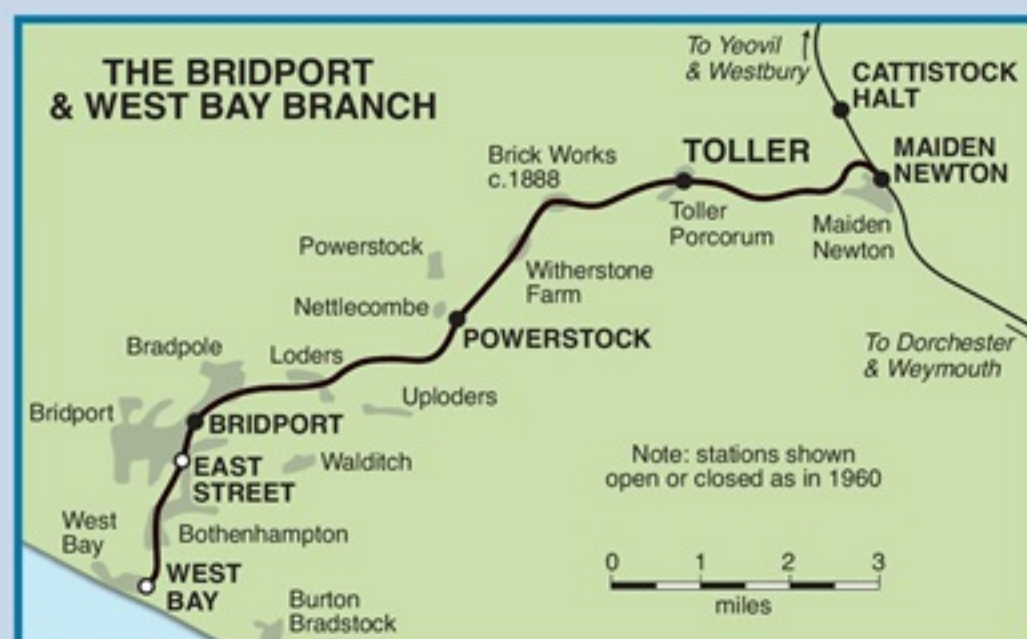
The Bridport and West Bay branch

The Maiden Newton to Bridport railway was opened by the Bridport Railway Company on Thursday 12 November 1857 and laid to broad gauge. Toller had to wait a further five years though for a station to be provided and this opened on 31 March 1862. The line was converted to standard gauge by the Great Western Railway. In September 1901 the Bridport Railway was wound up and control of the line passed to the GWR.

An extension to West Bay had opened on 31 March 1884, but hopes of developing this area as a tourist resort did not come to fruition, and passenger services on this section ceased as early as 1930; goods traffic continued until 1962.

Steam operation lasted until April 1959 when DMUs were provided by Weymouth shed until 1968, then Westbury. Initially a three-car set was provided but the inevitable competition from road transport led to progressive reduction to a two-car set and finally a single Class 121 railcar.

Toller lost its goods facilities on 4 April 1960 and there were no Sunday trains after September 1962. It became an unstaffed halt as of 11 April 1966. On 3 January 1975 the local press announced that the Secretary of State had finally approved the route's closure,



which was set for Monday 5 May 1975. Track lifting was carried out during November 1975 – a Class 25 diesel and steam crane removing the stop blocks at Bridport, then a Class 08 diesel shunter with a brake van, winch wagons and bogie bolsters removing the rails and sleepers through Powerstock and Toller.

down into the field became possible. The platform is a Ratio kit with ramps (ref.520). This was furnished with Wills Victorian stone paving slabs (ref.SSMP21). Although foreshortened, and with only the Bridport end ramp modelled, it nonetheless accommodates a single-car Class 121 DMU.

Using the measurements, plus reference to photos, I was able to produce a station building constructed from artist's mount card and plastic strip with a few commercial items added. The valancing was a mixture of Dornaplas (ref.B25) for the curved elements and Ratio ref.516 for the front piece. Notice boards were also Ratio ref.516. The roof is made from Wills slates ref.(SSMP203). The colour scheme is a much toned-down ex-Southern Region green and cream.

At the back of the platform Gaugemaster Scenics light green hedgerow (ref.GM160) was installed to simulate a laurel hedge. At the Maiden Newton end of the platform climbing roses (Tasma ref.00678) were used to augment the rambling vegetation that was beginning to take over in the declining years of the branch.

The weathered Toller station sign is a Springside whitemetal kit, as is the GWR station bench. The rudimentary lamp by the station nameboard is made from plastic odds and ends. The GWR spear fencing is also by Ratio (ref.434) whilst the ballast bin is by Peco Modelscene (ref.5002).

Trackwork

With the platform in place, I then worked on the track, which is just a single line. The plain track is DCC Concepts Legacy bullhead cut into sections to simulate the correct 60' panels and to create visible rail joints, with the rail sides painted with Vallejo hull red. The ballasting was completed using Woodland Scenics fine light grey granules.

Other scenery

The overbridge was constructed from the Wills vari-girder bridge spans (ref.SS57) and

A view from the platform looking eastwards in the direction of Maiden Newton, taken in September 1971. The station building survives today at Totnes in Devon.

The Toller station building was constructed from artist's mounting card and plastic strip with a few commercial items added.



English bond brickwork (ref.SSMP227).

For the backscene I used an image I took of the 13th century church of St Andrew and St Peter from across the valley. I was able to cut it out and apply it to the backscene. Using the same picture, I then set about hand painting the hills all around whilst holding true to the field patterns and hedgerows that were visible from afar.

In the foreground I added cauliflowers from Noch (ref.13216) whilst the edge of the field is fringed by nettles (Noch ref.14138) and GWR post and wire fencing. (I used EZ wire for the actual strands between the posts painted steel colour.)

The ground cover is all Woodland Scenics: for grass I used a blend of the firm's static grass in straw and light green shades, supplemented with various foliage clusters and field grasses.

The lone DMU

My Class 121 model is by Dapol in all-over BR blue with the double arrow symbols, numbered W55023 and bearing the correct headcode 2B51 for 1975.

The model trundles to and fro using a simple Gaugemaster Combi controller, with a brief pause of course to pick up the odd passenger. Two blasts on the horn – no whistles here – and the train departs for Powerstock or Maiden Newton. Silence then descends once again at Toller...

Further reading

The Bridport Branch
by B L Jackson and M J Tattersall
Oxford Publishing Company
ISBN 0 902888 85 4

Cornwall Railway Society
www.cornwallrailwaysociety.org.uk

Disused Stations
www.disused-stations.org.uk

Through the Railway Room Door

Port Joyce

Photography by Geoff Helliwell

My parents used to tell me that my love of railways was because I was born in Clacton Maternity Hospital, which at that time overlooked the railway station, so my first sight in this world would have been a B1, N7 or J15 simmering in the platforms.

When I was about 10, I would help my parents stack shelves in their greengrocers on a Saturday morning. Opposite was a toy shop and whilst very few model railway items were displayed, the owner always kept a good supply of Tri-ang Hornby models in his storeroom. Every so often I would take my hard-earned cash over to his shop along with my shopping list of must-have items. In those days everything was packed in red boxes of all shapes and sizes, so whilst you might have only spent a few pounds it looked like a major investment! As time went by various model railways were built in various scales, including some N and Z gauge models for the exhibition circuit.

Time to upsize

Fast forward to 2011 and I decided to hang up my suit and tie and look forward to a more relaxed period. Unlike some people, we wanted to slightly upsize our home because the size of the spare rooms in our current house restricted our hobbies. My wife suggested we look at a loft conversion in our current home with proper access stairs and Velux-style windows. I must admit that I had several reservations, but nevertheless, we approached a local specialist loft conversion firm and were told that subject to the necessary consents being obtained from the Local Planning Authority, the work could start within a month or so. After two weeks of building noise and a road closure to allow steel to be craned in, I had my dream 27' x 12' railway room for less than the price associated with a house move.

There are of course pros and cons when building layouts in a loft. Roller blinds are a

JOHN MARGERUM was at the point of moving to a larger property, but soon saw the benefit of a professionally constructed loft conversion which presented some 27' of modelling space; just right for a move to O gauge!

great help in keeping the sun at bay but dust particles blowing in through the open windows can cause problems, although removing the dust with one of those mini vacuum cleaners used to clean particles from keyboards normally does the trick. In my opinion it's a great place for a model railway; just make sure the work is done by a professional so there is no chance of you putting your foot through the ceiling!

From the USA to O gauge

Planning for a big layout started straight away and before long a two-tier HO gauge American model was under construction which took about three years to complete. After this my interest in US model railroads started to diminish and it was at about this time that Dapol started to introduce its range

Collett 0-6-0PT No.3716 starts a rake of suburban non-corridor stock from Port Joyce. The diesel servicing shed is a recent addition.





▲ Shipping is a feature on the layout and here a Clyde Puffer takes centre stage with an English Electric 0-6-0 in a supporting role shunting a handful of wagons on the dockside.

of O gauge British outline models. I was hooked and almost immediately I started to sell some of my American stock which gave me a reasonable sum to put by for the purchase of some O gauge equipment.

A day trip to Blackpool introduced me to Tower Models and this retailer's superb selection of models and very friendly service. A box of Peco flexible track, some points, a few Setrack curves, coal wagons and a Dapol 08 shunter were purchased and placed in a large box for the long journey home to Essex.

Planning begins

The initial plan was to design an end-to-end layout, because whilst at floor level the room width is approximately 12', at baseboard height the roof profile meant that about 8' was available for the layout if I allowed for a backscene board around the edge, so my initial thoughts were that a continuous running line could not be constructed in the

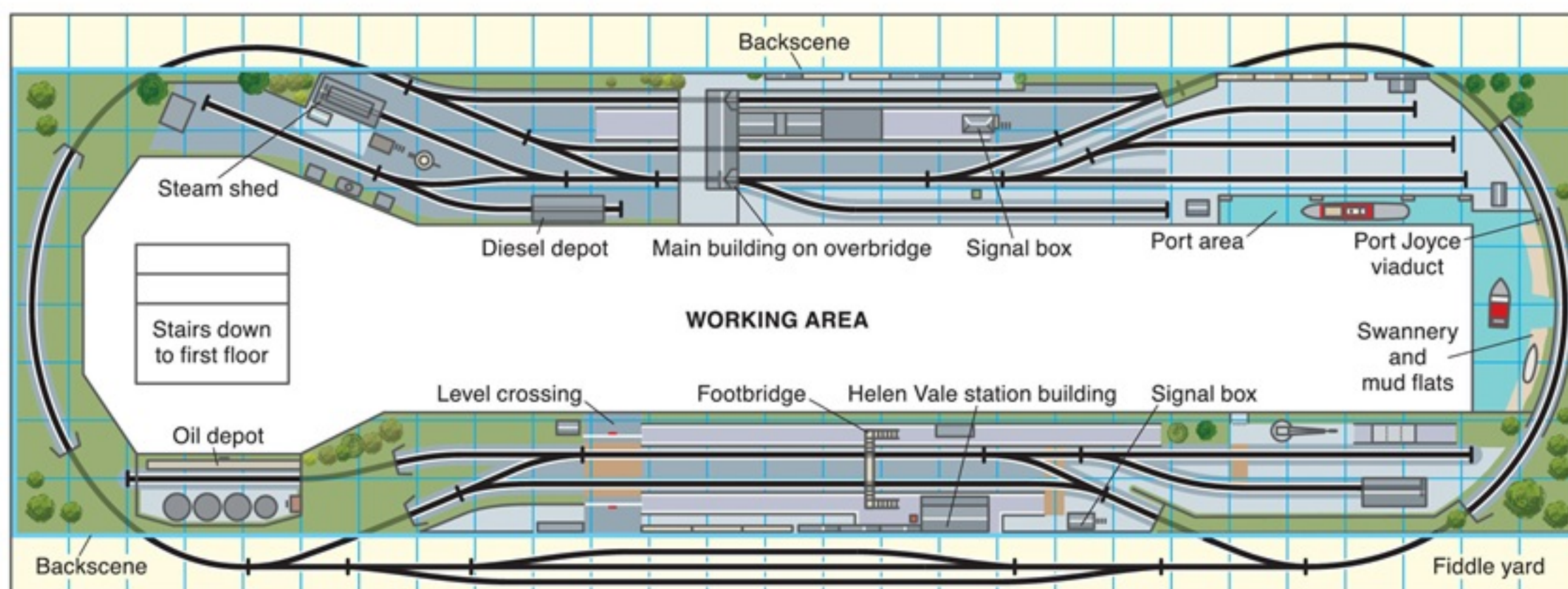
BTH Type 1 D8225 passes Port Joyce signal box. The wide availability of ready-to-run O gauge stock representing the 1950s/1960s period was the main driving force behind the creation of the layout.

available space. It then occurred to me that if the track went almost to the edge of the baseboard where it met the sloping ceiling, and I used Peco Setrack curves at each end of the layout, there would be just enough room to have a continuous circuit with just enough height for the tallest rolling stock. The backscene boards could be brought forward to allow approximately 10" of height,

not big by O gauge standards but adequate for my needs.

The concept worked and by having tunnels at each end, but still leaving some of the curves exposed, I was able to use the full length of the loft conversion incorporating a continuous circuit, which is always my preference when constructing model railways. It was never my intention to run





PORT JOYCE

Overall size: 27' x 10'. Each grid square: 1ft x 1ft.

Pacifics or large diesels, so the relatively sharp curves do not cause any problems with the rolling stock I use. Many recent ready-to-run products are in fact designed to negotiate Setrack curves, but of course you still must make a suitable allowance to avoid lineside structures being hit by overhanging vehicles. I have used both Peco medium radius and Setrack points, where necessary cutting a small length off the curved part of

the Setrack points to achieve the correct angle with its larger cousin when using them in parallel.

A journey along the line

I have always tried to incorporate my family names in the stations or industries on my layouts and *Port Joyce* is no exception, with the layout being named in honour of my wife. Helen Vale is named after

Fowler 3F 0-6-0T No.47673 passes over the level crossing at Helen Vale. Most of the buildings on the layout are scratch-built and arranged to suit the low head clearance in the loft.



▲ An oil depot is not a common sight in model form, but here it makes a very compact scenic feature in an awkward site over the stairs to the loft. A professional conversion does away with the traditional loft ladder and provides a safe modelling environment.



my late daughter with various factories and businesses incorporating my other two children's names or initials. The imaginary line is set in the late 1950s/early 1960s somewhere in Somerset with BR Western and London Midland Region influences.

The main station is Port Joyce which incorporates both a small steam shed and rudimentary diesel depot for servicing first generation locomotives and DMUs. At the far end of the station is the harbour complex which also serves as the goods depot for the town. Three sidings are provided plus a further track for the coal depot. At this point could I please make a plea to manufacturers to make some reasonably priced waterline model ship kits that could be used on O gauge layouts. I am happy to scratch-build some buildings, but coastal cargo vessels are beyond my capabilities.

Leaving the station, the line immediately enters a tunnel and then travels behind the harbour area before crossing the five-span Port Joyce viaduct. (The very rare Somerset Striped Alligator sometimes frequents the area below the viaduct when looking for its next duck dinner!) The line then curves through a cutting before entering another tunnel where the track again goes behind the backscene and there is an option to either enter the three-track fiddle yard or return into the open at Helen Vale. This station has two platforms with a small yard at one end and a branch line to a fuel depot leaving the main line at the other end. The main line then enters another tunnel and joins the track leading to the fiddle yard before re-emerging into open countryside and entering yet another tunnel under Port Joyce Common. The line, still in the tunnel, curves round behind the loco depot before entering Port Joyce again.

Operation

All points are operated by Peco PL-10 motors, many of which are now over 15 years old and still going strong. DCC operation offers considerable flexibility, and the track configuration allows for continuous running as well as an option for plenty of shunting if required. The incorporation of a fiddle yard into the layout also means that additional trains can be held off-scene to add further interest. It is unlikely that trains of more than three carriages or 12 wagons will be operated but some loop lines can accommodate slightly longer trains if necessary. Due to the number of tunnels on the layout, sections of back scenery (and in some cases, fields) can be lifted out to access these areas, because all seasoned railway modellers know that if there is going to be a problem, it is going to occur in the most inaccessible parts. I am not a great lover of timetable operation; I prefer to just sit back and enjoy seeing and hearing

The compact single-road locomotive steam shed at Port Joyce has had a diesel refuelling point added to service the resident Class 08 shunter. The scene demonstrates how much detail can be packed into what is a relatively small area in 7mm scale.



Collett 0-6-0PT No.7714 enters Helen Vale from the tunnel with a parcels train. The track is all Peco and is a mix of short and medium radius points operated by the reliable PL-10 point motors.





▲ The Gloucester Class 122 diesel railcar is a ready-to-run model and provides a very useful item of short passenger stock – not that there is much need for compression on this 27' long loft layout.

the trains of my childhood years running around the layout.

All the semaphore signals are manually operated. Not being a rivet-counter type of modeller, I am quite happy to use mainly Hornby OO gauge signals that have been 'reimagined'. This has been achieved by adding additional height to the signal posts and replacing ladders and handrails with more appropriate features.

Scenics

As mentioned previously, the roof profile in the loft does restrict the height of scenic

areas above track level, but nevertheless it has been possible to incorporate five tunnels, low hills, and plenty of cuttings. By dropping the height of the baseboard at one end of the layout, the harbour area was created and the impression is also given that trains passing over the viaduct are crossing a valley.

Cuttings and hills have been created from the rather 'hairy' green-coloured material used for hanging baskets which has been laid over shaped blocks of polystyrene packaging. The hanging basket material has proved to be a cheap and relatively convincing way of reproducing rough grass in 7mm scale. Various flock powders and foliage were added to give additional realism. Water in the harbour area was made by adding several layers of Deluxe Materials Aqua Magic over a painted plasticard base.

Almost complete

The layout is now nearing completion and I have been very pleased with my first sojourn into O gauge modelling. There are a few things I would have done differently if I had been starting the layout today, such as extending platform lengths slightly, but you learn by your mistakes even after 50-plus years indulging in the hobby, and nevertheless it has been a very enjoyable experience. My next project will be to weather most of the rolling stock but that will be for another day!

Port Joyce viaduct uses Wills vari-girder section for the main structure. Here Collett 0-4-2T No.1426 (a Dapol model) heads a pick-up goods. Don't miss the rare Somerset Striped Alligator hiding in the reeds!





RAILWAY MODELLING *Explored*



Projects, hints and tips for beginners from Railway Modeller readers and staff

THIS MONTH • A modular project in OO • A laser-cut structure kit build in TT:120

Stratton Park

A modular layout created and described by the Junior Section of the Basingstoke and North Hampshire Model Railway Society.

Photography by Steve Flint

Robin Gilchrist writes: myself, Adam Neale, John Smerdon, and Ian Morgan are mentors to the Junior Section of B&NHMRS.

The basis for the Society's Junior Section is to encourage young club members to learn about all aspects of railway modelling; from baseboard construction, track design and laying, electrical wiring, ballasting, model building and scenic techniques through to layout operations and fault finding.

We initially met with the six section members to determine some basic design rules for a modular style layout that could be assembled in any number of ways with any number of modular boards. It was also to be adaptable to enable future modules to be added. Thereafter each individual was to be responsible for their module.

Baseboards, track and points

Each module is formed using a pair of Scale Model Scenery BB002 Diorama Boards fixed together, making a module 81cm x 29cm and 24cm high. Neutrik DIN type plugs and sockets daisy-chain the modules together (Blue for the DCC power and grey for 12V DC supplies).

The track is standard Peco OO gauge and to establish rail alignment, each board has a single track positioned 5cm from the front edge, reinforced using copper clad sleepers.

DCC control capability has been standardised using Merg DCC Accessory

Decoders and Merg Servo Controllers (see Model Electronic Railway Group – www.merg.org.uk).

The juniors drive this using their mobile phones and have adopted a Roco Z21 WiFi DCC system and the corresponding app.

Parents, grandparents and other club members have actively participated and supported the juniors throughout. Once lockdown ended we quickly restored the Junior meetings and progress was maintained at a steady pace. In fact the Juniors' layout has often



▲ A Class 66 heads a short freight along the assembled modules, having just passed through the Stratton Farm Cottage scene.

progressed more quickly than some of the other club layouts – a fact not missed by the wider membership!

To date the Junior members have exhibited Stratton Park at Whitchurch in October 2021 and our own club show in March 2022. It has also

appeared at two club open days where exercises in finding and fixing faults, and making enhancements to the running kept them busy.



▲ Five of the members of the Junior Section are pictured with the completed layout at the Basingstoke show in March. From left to right: Marcus, Lydia, Morgan, Kayden and Jasper.

Marcus (12) – Stratton Industries

A small industrial area and PW storage yard occupies my module. It provides an ideal setting for shunting with my Class 08 and Ruston locos, and to display some of my Oxford Diecast British Rail vehicles. Metcalfe card kits and Ratio items (site office and lineside fencing) form the factory and loading platform. I used DAS modelling clay for the hard standing and am very pleased with the result. A mixture of medium and fine Woodland Scenics ballast was fixed with diluted Mod Podge and the whole track weathered with Railmatch spray paint.

For extra realism I added a few pre-made LED yard lights running off 5V so they are not too bright. I have really enjoyed building the layout but equally enjoy showing other children how to operate the layout from the Z21 app at our exhibition.



Lydia (10) – Stratton Farm Cottage

My module represents a railway line passing a small cottage with farm animals, and a pond, or possibly, a swimming pool with decking made from parts of coffee stirrers. The cottage was built from a Metcalfe kit and has a garden shed adapted from a plastic platelayers hut kit.

The baseboard surface was painted black, which helped when at a later stage I put glue down and sprinkled black Tarmac scatter on top to make the road.

Outside the cottage I added individual cobblestones, a flower bed and a picket fence, along with a climbing rose using home-made clump foliage. I enjoyed creating the landscape and scenery parts the most, especially the smaller details like the flowers.



Morgan (16) – Stratton Halt

This scene represents a relaxing countryside pub adjacent to a heritage line station. I bought the pub at the first club open day I attended, and it has a sentimental touch for me. It is a 'ready-to-plant' resin model.

The laser-cut wooden platform was fun to build. It looks great with the individually cut planks. Behind it in the car park, a car show is ongoing.

The model cars include a Ford Capri, an Audi Quattro, an old Bentley 8L and an MG – all great models from Oxford Diecast.

The figures on my module have been arranged to each tell a story and 'bring the layout to life'. For example there's a policeman talking to a man worse for drink who is banging on the closed pub door. In addition, two men can be seen in the car show discussing the sale of the Bentley.

I have loved building this model railway, making everything myself from the lights on the station to the flower beds at the pub; all skills I learnt from other members of the club.



Kayden (10) – Stratton Depot



A modern diesel depot with a small refuelling point is portrayed on my module. The depot building was a commission from Weston Models who attend our club exhibition. I really liked the detail of their model and asked if they could make one for me. The fuel depot was a card kit and with the surrounding concrete apron made from DAS clay. Inside the depot are two tracks with concrete maintenance areas which I will eventually enhance with accessories.

I would like to add some interior lights too, as already around the refuelling point I have the same type of 5V LED yard lights which Marcus used on his module. I love to run my Class 37 and Class 66 diesel locos and I really enjoyed learning to solder the track wires with my grandad – he still has the burn marks on his fingers from my early mistakes! I have learned a lot about modelling and think the actual building of the layout is more fun than operating it.



Jasper (10) – Stratton Park Station Yard

I wanted a typical small country station with two platforms and a passing loop. The platforms use Ratio platform edging and have wooden surfaces covered with Scalescenes tarmac finish. I used Gaugemaster's Mortimer Station and pedestrian bridge kits for the structures, the latter forming the scenic break to a simple double track fiddle yard. A small diorama shows three workmen repairing a platform lamp on the busy station. All the other platform lamps operate at 5V.



A pocket money purchase at an exhibition provided the signal box which has two signalling staff and a detailed interior to which I hope to add an interior light. I really enjoyed learning how to ballast the track and use the static grass applicator, being amazed how quickly the scenic grass layer changes the look of the layout.

I really enjoy using the mobile phone to operate the layout and working closely with the other members to ensure smooth operation of the layout when we exhibit it.



Harry (13) – Stratton Goods Yard

I was not able to attend the recent show with my module which represents a typical wagon yard. However I really enjoyed creating the design myself and thinking about what would work well on the layout. So far I have made the baseboards and laid my track. The next stage is to complete the wiring and electrics and then my module can be added into the layout. I will add scenery and perhaps some people, maybe a water tower and a coal hopper. I am the newest member of the club and I really enjoy meeting other young people who also have an interest in trains and modelling.

Working on my own module is my favourite part of modelling, but I love seeing what others have achieved. I'm really looking forward to choosing the things to put on my layout and seeing my section integrated with the other five.



“ – Thanks to all our juniors, and to the Basingstoke members who have supported them throughout their journey. We hope the layout will be invited to more exhibitions in the future, allowing many younger enthusiasts to see what they have achieved, and possibly form a junior section within their local model railway club themselves. – ”



Robin Gilchrist

The future

The club recently approved a budget to allow the juniors to build two scenic semi-circular end boards to allow continuous run operation and further new modules to be added. This will involve some baseboard building for the juniors who want to see a viaduct scene and a brickworks on the new sections.

There is no doubt that the B&NHMRS Junior Section has been a remarkable success and hopefully this lays the foundations for future modellers in the club for years to come.

Constructing a station building in TT:120

CALLUM WILLCOX demonstrates how to paint and assemble the new Country Station Building kit for 1:120 scale from Peco, which is based on the prototype example at West Bay and requires only basic tools and materials to complete.

Photography by the author

In the July 2022 issue of RAILWAY MODELLER, details were revealed of the pioneering programme of products being manufactured by Peco for British outline railway modellers. Marketed as TT:120, the range utilises the established European TT standards of 1:120 scale on 12mm gauge track, thus providing an accurate scale and gauge combination for British standard gauge prototypes.

TT:120 products that are being developed initially by the Devon-based manufacturer comprise trackwork items with correct sleeper spacing for British outline, together with a selection of wooden laser-cut lineside structure kits and accessories, with the latter based on Great Western Railway prototypes.

One such kit is the GWR Country Station

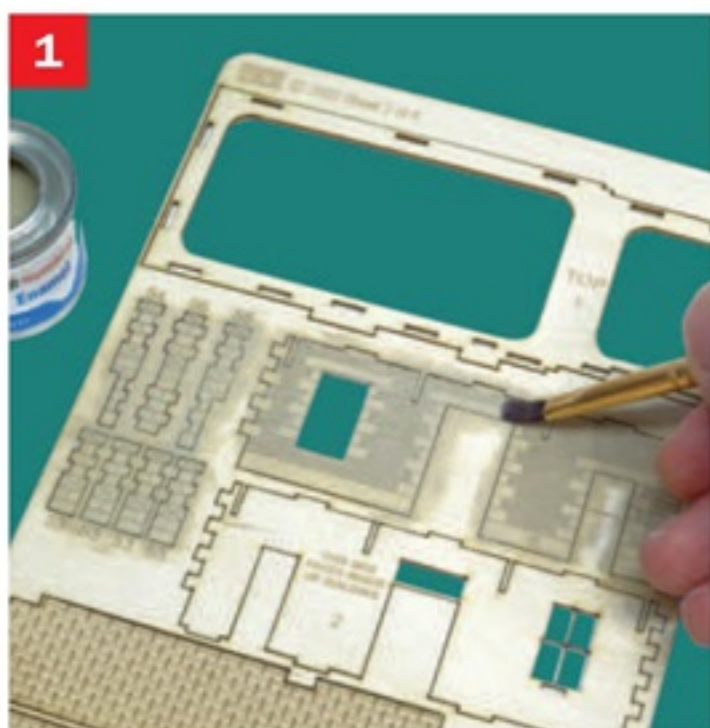
Building kit (ref.LK-12101), inspired by the prototype example that still stands at West Bay in Dorset. The kit – which consists of wooden and plastic components – contains all the parts and details that are needed, with only the minimum of tools and materials required to create a finished model. As such, the kit represents an ideal starter project for those wishing to try their hand at modelling in TT:120. The completed model has a footprint measuring 114mm x 47mm (excluding canopy).

The kits are available from Peco stockists. For more details, refer to the manufacturer's website: www.peco-uk.com

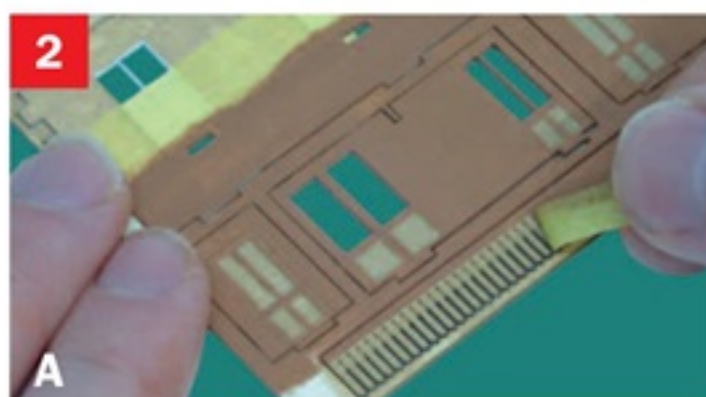


▲ The laser-cut wooden components for the Peco TT:120 Country Station Building kit, and (above) the finished product.

Building the kit



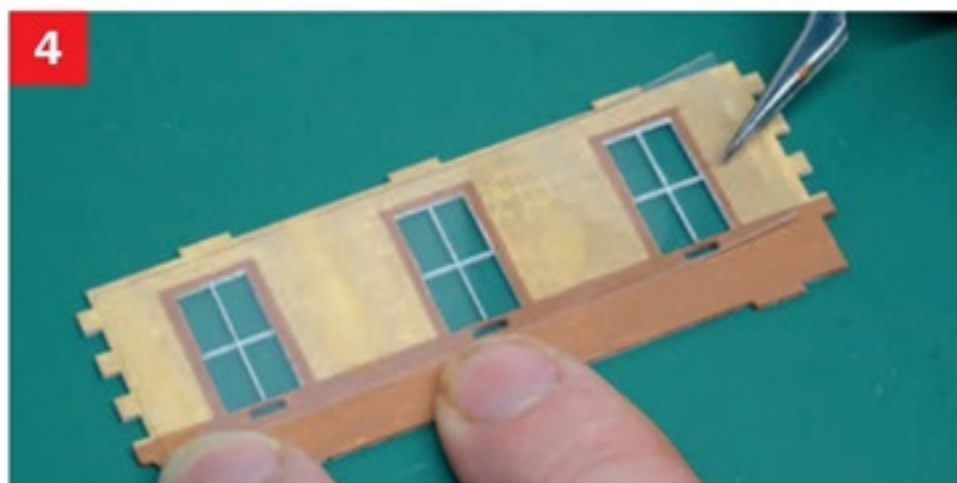
I elected to paint the wooden components prior to assembly. This was undertaken whilst the parts were still attached to the frets, making them much easier to handle. The stone components were brush-painted with Humbrol No.121 Pale Stone; a rough application combined with the grain effect of the wood makes for a convincing natural stone look.



With the stone colour fully cured, the wooden window frames received a coat of Humbrol No.34 Matt White, while details such as the canopy, doors, guttering and latticework were picked out in Phoenix Precision No.P21 GWR Light Stone and No.P22 GWR Dark Stone. Clean lines between the two colours were achieved with some careful masking. This process was repeated for the internal walls.



The wooden laser-cut components are held to the fret via a number of small connecting tabs located around the outer edges. Each part was released by applying light pressure with the blade of a craft knife on these tabs, before cleaning the edge of the components with a fine sanding block or nail file (Inset).



The kit is supplied with a sheet of clear plastic to be used as glazing material. Using a steel ruler and a sharp craft knife, a number of strips were formed; these were cut slightly oversize in order to avoid marks on the visible areas when fitted (*inset*). Once in position they were secured in place using small amounts of PVA glue.



5

The main parts of the structure have been designed with a slot and tab arrangement for assembly. Starting with the base strengthener plate, the internal walls were fixed into position using Deluxe Roket Card Glue (A). Each component is marked, denoting which way it is to be fitted. Once cured the doors were lowered into the locating slots provided – ensuring that they sit flush in the frames which were painted prior to fitting (B).



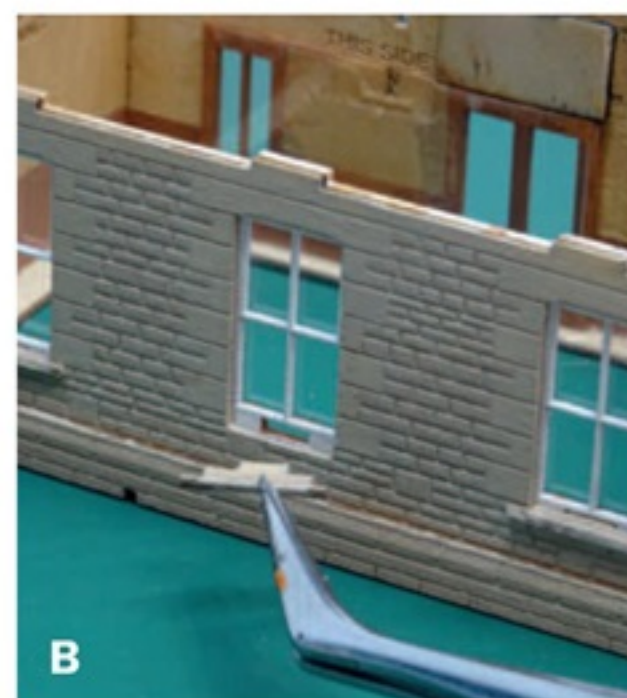
6



The corresponding outer walls were fitted using the same method – ensuring the joining edges were flush. Once cured, exposed edges were touched-in with Humbrol Pale Stone to match the rest of the stonework (*inset*). The external toilet block was then assembled in a similar manner to the main part of the building.



7



With the main structure assembled, details such as the window cills, raised stonework and fascia boards were added (A). Locating slots for the cills are provided within the frames; it's worth noting the ends of these components are particularly delicate, with extra care required when removing them from the fret and during fitting (B).

8



Citadel Agrax Earthshade was used to break down the uniform finish of the painted stonework and give it a more natural look. The wash was applied by brush, drawing it lightly in downward strokes to simulate the natural build-up of rain streaks and dirt deposits. A similar process was adopted for the roof sections; these components first receiving a coat of grey primer, followed by a dusting of Humbrol Smoke and DCC Concepts Mid Grey weathering powders (*inset*), all sealed with a quick mist of matt varnish spray.

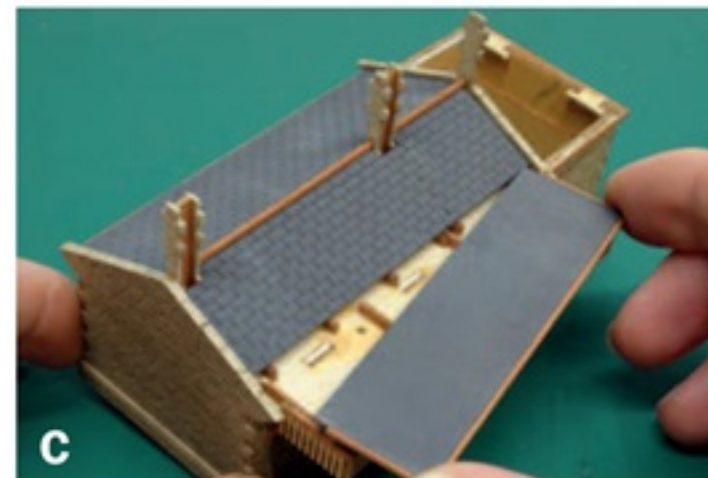
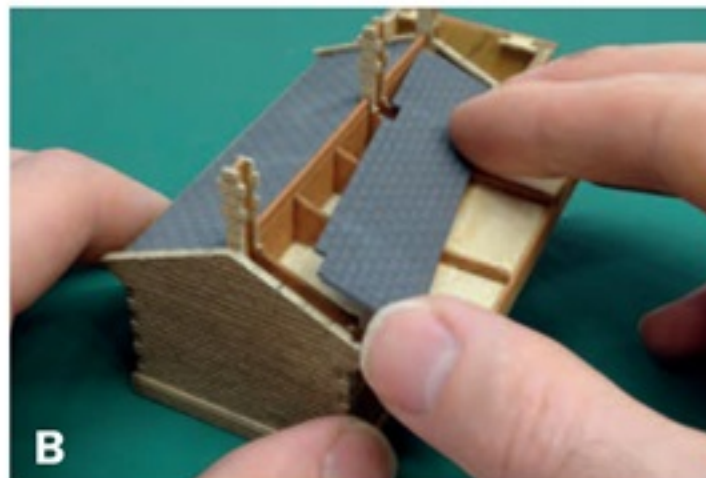


9

The canopy assembly starts with the fitting of the internal roof section. Locating slots on this component align with those along the top of the front wall, into which the decorative canopy supports could be slid (A). Once in position, the valancing sections were added, starting with the two end sections, which were glued flush with the corner edges of the internal roof panel (B).

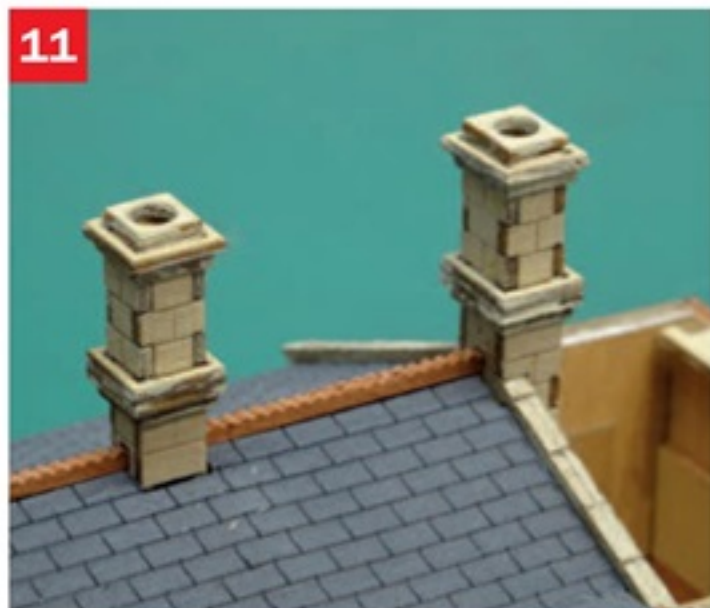


10



The structure of the roof is created with a number of internal formers that also set the pitch. These were glued into the corresponding locating points, along with the horizontal roof support, together with further angled supports – incorporating parts of the chimneys – slotted over the top of this assembly. Each angled component is marked to ensure the supporting tab for the canopy roof is facing the correct way, along with the chimney brick detail (A). Once cured, the roof panels were fitted – these are a friction fit with no glue required (B). Finally the flat canopy roof was glued into position, aligning this flush with the bottom edge of the pitched roof (C).

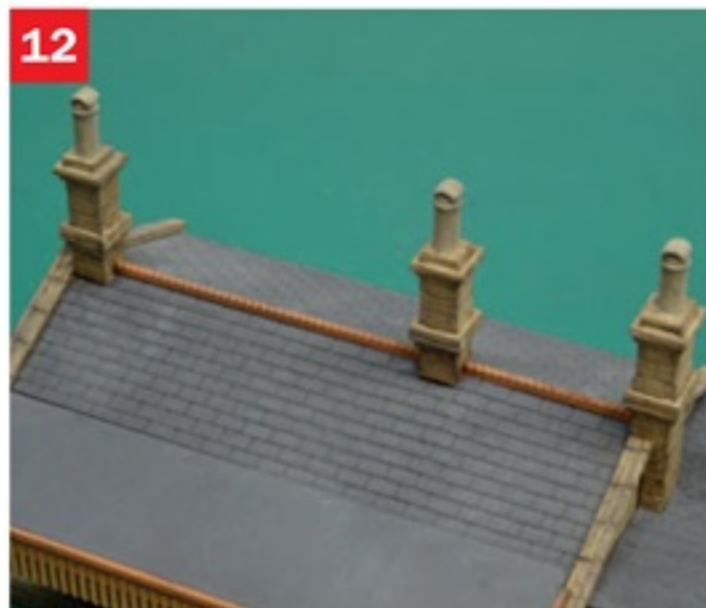
11



With the remaining sides of the chimneys fitted, the raised details and capping stones were added, with each component slid carefully onto the stack and secured in place with Roket Card Glue; the positions of these are marked with an 'X' (inset). Caution must be exercised when removing these details from the fret and during fitting, due to their delicacy. The completed assemblies were then painted and weathered in the same way as described previously.



12



A selection of chimney pots are supplied in the form of plastic mouldings (inset). These were primed and painted with Humbrol No.121 Pale

Stone before being attached atop the capping stones using contact adhesive. Final details included the addition of the downpipes, before finishing with a light dusting of weathering powders. Further embellishments that could be made to the model if desired include internal detailing and lighting, together with external signage and timetables/posters.



Dating from 1884, West Bay station building was designed by William Clarke. His distinctive work could be found across the GWR system.



Josh Williams



Back to basics?

“**W**hy do we become interested in this hobby? What is it about this pastime that encourages us spend countless hours of our time on it, as well as considerable amounts of money? For me, it is all about escapism – an opportunity for some respite from the challenges we all face as part of our daily lives. I am sure many others participate in railway modelling for similar reasons. So why, then, have we allowed such a pervasive aspect of daily life – consumerism – to infiltrate our great hobby?

With the term consumerism I am referring to the voracious appetite that seemingly now exists within the hobby for ready-to-run models that meet an ever-increasing set of criteria: exquisite detail, faultless and silent running characteristics, and full DCC sound functionality. The accuracy of these off-the-shelf models has, of course, never been greater; but with this seems to come higher expectations, and this, inevitably, often leads to disappointment, especially when several hundred pounds have been parted with and a long wait has been endured for the manufacturer to deliver the model in question.

Along with society at large, it seems that the desire for instant gratification has begun

to take over the hobby. Whilst it could be argued that the positive outcome of this has been the work of manufacturers in achieving near-perfection in the pursuit of realism, I would suggest that this is to the detriment of railway modelling. After all, what are we really doing when we are engaged in this pastime? If you ask me, the answer is simple: we are ‘playing trains’. Regardless of how seriously we may take our approach to the hobby, all we are really doing is having fun – why else would we do it otherwise? Therefore, perhaps we should go back to basics?

Recently, I have become interested in collecting older N gauge models. Some of these might have ‘pizza-cutter’ wheels or be devoid of much in the way of separately fitted detail (certainly by today’s standards) but I get a lot of enjoyment out of restoring and running these – perhaps more so than I would by plonking the latest sound-equipped masterpiece from a Chinese factory straight onto my track. If I choose to add detail, then I approach it the old-fashioned way and purchase parts from smaller suppliers; this has the benefit of helping to keep such cottage industries in business, as well as ensuring that individualism is promoted. Is it not preferable for modellers to customise their models themselves rather than rely on identical mass-produced items that might never spend time on the workbench receiving modifications?

If we were able to learn to enjoy railway modelling on a more basic level once again,

...If we were able to learn to enjoy railway modelling on a more basic level once again, then I think we will be all the better for it.

then I think we will be all the better for it. Rather than complaining on online forums about price rises or the latest delays in manufacturing, why don’t we take a moment to reflect on what it was that got us into the hobby in the first place? I feel that nostalgia can play a major role in this – even if it is as simple as retrieving your first train set from the loft. Or if you no longer have it, then perhaps procure the necessary items to replicate it – I am sure it would put a smile on many a face, as well as potentially helping to bring the next generation of enthusiasts into the hobby if children and grandchildren are also encouraged to get involved.

This brings me to my second suggestion, this time for the manufacturers. How about producing some more affordable and basic models that do not reveal a jumble of fragile wires if one has the courage (and the patience) to take them apart? Let those who want to add detail, lighting and sound do so if they desire – this in turn will lead to greater satisfaction and a sense of achievement if they put in this extra work. For those who just want to run trains, which is surely one of the most fundamental aspects of the hobby, then let them do so without negative judgement from others. Give me a simple, durable and affordable model over an accurate yet fragile one any day.

Railway modelling is about relaxation, enjoyment, and escapism. So why let the negative realities of modern life spoil it? Let’s just go back to playing trains and having fun.”

Josh Williams suggests that modellers should consider taking a moment to reflect on what it was that first ignited their interest in the hobby. For many this will be a train set of some form, such as the Hornby OO set pictured here with a model of *Flying Scotsman*. Photo: Craig Tiley

Josh Williams is studying at university and has an interest in the railways of South Wales.



Readers' Letters

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: editorial@railwaymodeller.co.uk

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

TT:120

Reactions from RAILWAY MODELLER readers

Ed – The July edition of RAILWAY MODELLER included extensive coverage of the new TT:120 range of products launched by Peco, which represents a new scale and gauge combination (1:120 on 12mm gauge track) for commercial British outline. We received a huge amount of correspondence on the subject from readers; a selection is presented here, starting with a letter received from the Chairman of the 3mm Society.

I read with interest the article written by Steve Flint in last month's RAILWAY MODELLER, charting the history of Tri-ang TT and the announcement by Peco of the launch of TT:120.

Since the demise of Tri-ang TT the 3mm Society has supported the scale and there have been many excellent 3mm layouts built and exhibited – including *Foxbury MkII* and *Porthdinllaen* that have appeared this year in the pages of RAILWAY MODELLER. I am sure our members would agree with Steve that it is 'just the right size'.

It is therefore with a tinge of regret that we note that Peco has decided to adopt the continental TT scale of 1:120 for the launch of its new product range. I appreciate that commercially produced continental outline locos and rolling stock are already available, which may have influenced that decision.

We still believe that 3mm is the perfect size, being midway between 2mm and 4mm, and although that does represent a compromise when using 12mm track, that is no different to OO modellers who also similarly compromise by using 16.5mm track.

No doubt some of our members will make use of the finescale 12mm gauge track and points that are being produced and some will utilise the forthcoming Heljan Class 31 chassis [Ed – see News] to power their 1:100 models – so there is plenty for 3mm Society members to welcome in these announcements.

MARTIN GENTLE

Chairman, 3mm Society

I am sure I am not alone in saying that I was stunned and amazed by the announcement in last month's issue stating that Peco is to introduce a whole new range of TT products, some of which were already in production. This must surely be one of the best kept secrets ever to emerge from the model railway industry and something which I feel is long overdue, for all the reasons stated in the magazine.

Peco is to be congratulated on its brave move into a 'new' market and I will eagerly await further product developments and announcements!

ROGER PRIDEAUX

When you kindly published my letter suggesting a TT gauge to 1:120 scale in the February 2016 issue, I did not appreciate that this scale and gauge combination was already used in Europe, hence the availability of some continental equipment. However, having resigned myself to the belief that the scale was not viable for the UK market, imagine my surprise and delight to read of the launch of TT:120. The strapline of 'True to Scale' on the Peco advertisement was noted.

Thank you Peco for such a bold venture and I will certainly be building a layout to 1:120. Will there be a starter set, say with an O-6-0 tank engine, in time for Christmas?

JEREMY BROWN

It was great to see the launch of so many UK outline products for TT:120 in the July issue. Will RAILWAY MODELLER be reviewing continental TT products for prototypes that operated on UK lines? For example, I read in the January 2021 edition of the German magazine *Eisenbahn Kurier* about the TT scale ferry wagons that have been produced by Arnold, which were built in 1935 to the UK loading gauge. A model based on the Harwich train ferry in TT:120, anyone?

PHIL BUTTON

I was very interested to read your articles on the new TT:120 scale, and I completely agree that there is a need for a scale between N and OO gauges. However, I was surprised to see no mention of the fact that Peco already produces track and points with a gauge of 12mm for its H0m range, which is a practical option for those wishing to model in a 1:120 scale. I realise that this track does not have the correct sleeper size or spacing for TT:120 and that the code 75 rail section is too high, but I cannot believe I am the only one who does not worry about such things.

JOHN HOPWOOD

I was really excited when I first heard about Peco introducing a new range of TT products, although somewhat disappointed to discover they had

chosen not to go with the old British TT scale of 3mm:1ft – thereby losing the opportunity to introduce a scale track gauge of 14.35mm. Irrespective of this, the proposed new range is still very much welcomed, negating the various disadvantages associated with N and OO.

I have long hoped that TT would be reintroduced commercially and am ready to sell up my N gauge collection and make the change. My only hesitation is whether a reasonable range of locos and rolling stock will eventuate reasonably soon. Although there are encouraging signs of supporting interest from some other manufacturers, how many potential adopters of the new scale will be prepared to commit and buy track now with rolling stock not yet available?

It would be a tragedy if low initial track sales were taken to represent a lack of interest (rather than an absence of available rolling stock) and TT:120 was to not realise its full potential. I do hope that some locomotive and rolling stock models will appear reasonably soon.

SIMON HOLTBY

I read with interest the articles about TT:120 in the July issue because this may now open up other opportunities as a result of various mainstream manufacturers producing R-T-R models for this scale. For example, powered chassis would be of interest to those few of us who model narrow gauge in 5.5mm:1ft scale (1:55). This was the original narrow gauge scale introduced by GEM way back in the 1960s and is currently supported by 55th Studios with kits. The arrival of any R-T-R

steam outline chassis in TT:120 would be very welcome indeed.

STEVE LAWRENCE

With reference to the TT:120-themed Talking Points feature in the July issue, the caption beneath the photo of the V200 diesel model by Tillig suggests it could provide a suitable mechanism for a British Railways 'Warship' diesel-hydraulic in TT:120, with a suitable body made using 3D printing.

As an advocate of 3D printing methods for modelling (see my article *3D Printing Explored* in the December 2020 issue, p953) I was sufficiently intrigued to take my current build project of a GWR Armstrong Standard Goods O-6-0 (which is a hybrid of traditional brass construction and 3D printed parts) and, as an initial experiment, see if I could generate a 1:120 scale CAD drawing which could then be 3D printed, not as a whole unit but as separate 'kit' parts for assembly.

The attached photograph shows the 1:120 3D printed result on the left. On the right is my OO gauge version of the part-completed locomotive for comparison. There is a long way to go yet, not least devising a way to power the smaller model, but I think the application of 3D printing techniques provides lots of opportunities for changing scale easily.

MIKE FLEMMING

[Ed – An article that will describe the construction of Mike's multi-media GWR Armstrong Goods O-6-0 tender locomotive in 4mm scale is planned for a future issue.]



Two of Mike Fleming's 3D printed models; in 1:120 (left) and 1:76 (right).

Information on Glasgow & South Western Railway resources

I was very pleased to see in the July RM an article on the Glasgow & South Western Railway 279 class O-6-0s. However, as a long-term modeller of the G&SWR and its locomotives I was rather surprised to read that the drawings had been based purely on

photographs; copies of the original General Arrangement drawing for this class are available to view from the Railway Museum at York (copies can also be arranged). Furthermore, for modellers of the pre-Grouping era, there is a wealth of information and

photographs available to the modeller from the various societies, the Glasgow & South Western Railway Association being one of the oldest; for more details go to: www.gswra.org

IAN MIDDLEDITCH

Filling in the gaps on the 'Collett Goods'

With reference to the Scale Drawings feature in the July issue (Collett '2251' class), it was stated in the accompanying notes that all the locomotives built during wartime had their cabside windows fitted after the war. At least one did not; I photographed No.2239 in store at Aberystwyth shed on 5 August 1962, which had been withdrawn in May that year and was without cabside windows. A copy of this photo was published in the March 1990 issue of RAILWAY MODELLER.

BRIAN GILLESPIE

The July RM shows No.2273 with Collett 3,000 gallon tender incorrectly described as a 3,500 gallon version. The diagram shows a 3,500 gallon tender which, to my knowledge, they were not paired with. I realise these tenders are difficult to discern between, but... Still an excellent magazine.

NIGEL WALL



Photo: www.railphotoprints.co.uk

Layout size is not the most important factor

I would like to thank Alan Poole and Simon James, together with many members of the 3mm Society, who emailed to compliment me on the article describing my 3mm layout *Porthdinllaen* in the May issue. I would also like to thank RAILWAY MODELLER for featuring layouts and articles on 3mm subjects for three months in succession – in particular the June Talking Points that focused on Geoff Helliwell's 'nearly ready-to-run' locomotive models.

I would, however, also like to respond to Simon James' comment about how much space the average enthusiast has available for their hobby. I know of several modellers who have layouts

(albeit not 3mm scale) that measure 25' plus length, whilst there are layouts in the June 2022 edition – including the excellent 'Railway of the Month' – that are also extensive in their size.

However, for me, the most important factor is not the size of the layout, but what you put into it as a modeller and, most importantly, the pleasure that you derive from this brilliant hobby. I have another exhibition layout – *Heybridge Wharf* – which is at the other end of the spectrum being only 6' long (including fiddle yard) and that gives me the same amount of pleasure as I get from operating *Porthdinllaen*.

MIKE CORP

Another model of Settrington – this time in OO

How pleased I was to see in the July issue Kevin Smith's N gauge model of Settrington station on the old Malton to Driffield line.

I purchased a OO gauge layout of Settrington from a well known online auction site. It was the best thing that I

ever did. I have added more detail and at the Malton end, I have resited the crossing gates and also added a signal box – which of course was never there in real life. The rest of the model is as



it was in BR days. The accompanying photo shows the layout with an NER Autocar from Rails of Sheffield – just for fun!

MALCOLM SLATER

Memories of Deganwy

Further to the article describing the OO gauge layout *Deganwy* in the July issue, here is a photograph of mine from 1963 showing the station with a southbound four-car Derby Lightweight DMU from Llandudno. Note that the train is complete with a tail lamp – not the integral red lights that seem to be the fashion nowadays on most DMU models depicting this period! Also visible in the background of the photograph are the carriage sidings for Llandudno; a hotel and residential properties now occupy this site.

ALISDAIR MACDONALD



Traditional modelling skills will survive

In his Comment piece in the July issue Patrick Collins said that he has great hope for the future of the hobby and modelling skills. I find myself in total agreement with him – although not necessarily for the same reasons.

He believes computers and 3D printers will complement the lathe and pillar drill, thus modernising the hobby. Fair enough, but to my mind a far more basic reason for people constructing their own items is the ever increasing prices of manufactured goods, be they locomotives, rolling stock or structures. It's no surprise that layouts seem to be getting smaller; how many of us can afford to replicate anything like King's Cross or Euston without the prospect of taking out a second mortgage?

A few issues ago, Julian Andrews, the master of the achievable micro, made the point that those on a budget should consider carefully whether they really needed whatever it was they were considering buying. I would add that if somebody wants something, they should first consider whether they can build it, rather than resorting to the cheque book or debit card.

Therefore, like Patrick, I can see a bright future for the hobby in that there will be a multitude of layouts full of individually-created buildings, scenery and even rolling stock.

Keep up the good work with a consistently tremendous magazine and could we please see some more micro layouts – as these are all that a lot of us will be able to afford!

PHIL STONEHAM

Snippets...

Mine miasma!

The O gauge layout *Norman Colliery* by Geoff Brain that featured in the July issue is superb. The model 'reeks' of a colliery; you can almost smell the mixture of coal dust, oil, steam, diesel and dampness! When I was a student in the early 1970s – the period of the layout – I had a friend who was studying Mining Engineering. He was sponsored by the National Coal Board and, as I also considered a position with NCB, I visited several collieries with him. Geoff's depiction transported me right back to those times.

JOHN TREAYS

Kit instructions

It was refreshing to read the Comment piece in the June RM by Peter Osborne regarding instructions supplied with model kits. In my experience instructions often seem to have been included as an afterthought, sometimes in the form of poor photocopies and with illustrations that are impossible to see clearly. However, in contrast, I have built many card kits by firms including Metcalfe Models and Scalescenes and find that these are supplied with clear, comprehensive instructions.

On a related note, my thanks to Ian Nuttall for his articles and photographs detailing rolling stock for *Dentdale*; these have been immensely helpful during my attempts at building wagon kits.

DAVID BECK

Jubilee models – Emmett style!

Further to the June issue of RM commemorating the Platinum Jubilee of HM The Queen, I marked this occasion by constructing a royal coach for my Roland Emmett-inspired layout. Motive power for the weekend was provided by *Sophia* – the workhorse of my railway – driven by Mr Silvers. The kits and the figures are from the Smallbrook Studio range.

BRADLEY BOTTOMLEY



Monorail scale

As part of the article published in the July edition of RM describing Rudi Newman's recreation of the Lartigue monorail, we neglected to cite the scale modelled, which is 1:148. Our apologies for omitting this vital bit of information!

FROM **RAILWAY MODELLER** BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

British Rail Class 319 EMU

Brand new from Graham Farish for N



It was confirmed in our May 2016 issue that Bachmann Europe Plc was to develop models of the British Rail Class 319 dual-voltage Electric Multiple Unit under its N gauge Graham Farish range.

Three four-car sets have been produced by Bachmann initially, comprising ref.372-875 Class 319/0 319 004 in Network SouthEast (revised) white/red/blue; ref.372-876 Class 319/3 319 382 in Thameslink blue/yellow; and ref.372-877 Class 319/3 319 362 in Northern Rail purple. It is a sample of the former that we received for review.

Our sample in detail

The four cars that make up our sample comprise DTSO-A (Driving Trailer Second Open) No.77297, MSO (Motor Second Open) No.62894, TSOL (Trailer Second Open Lavatory) No.71775 and DTSO-B (Driving Trailer Second Open) No.77296. A graphic illustrating the formation of the vehicles is included as part of the literature enclosed with the model.

The model reflects set 319 004 as introduced in 1987 and is equipped with a Stone Faiveley pantograph. The application of the striking lines of the Network SouthEast red, white and blue

colour scheme have been completed to a very high standard throughout, with clean breaks between colours. A fine orange cantrail stripe neatly runs along the roofline on each vehicle; the alignment of which is near perfect along the full length of the set. Renditions of the vehicle numbering, data markings, Network SouthEast branding and Thameslink motifs are crisp and clear – all of which are legible under a glass. Passenger window frames are neatly picked out in aluminium.

The construction of the vehicles

comprises largely plastic mouldings, with details such as elements of the pantograph and the fine separately fitted windscreen wipers formed from etched metal. Glazing is flush with smoke tint, affording views of the car interiors. The passenger cabins and



▲ The DCC socket inside the underframe.



▲ The MSO houses a central motor that drives both bogies. The upper PCB provides interior lighting.



The pantograph is of plastic construction and can be raised or lowered manually.



driving cabs are furnished to a certain extent, although this is somewhat restricted by the floor height, which has been raised to accommodate the associated wiring for pick-ups and lighting.

The 25kV overhead equipment is finely replicated, comprising numerous separately fitted parts. The pantograph is itself of plastic construction, with the exception of the push rod and pan head. The assembly is sprung and can be raised or lowered manually.

Each cab end is also equipped with working BSI-type couplings allowing for multiple 319 units to be coupled together. These function by manually pushing the outer ends of adjoining DTSOs together, while a robust pull is required for detaching them. The couplings are mounted on a swivelling self-centring mount on the underframe, aiding the negotiation of the model through curves and pointwork.

Mechanism and performance

As per the prototype, drive is provided solely by the MSO vehicle. The model of this vehicle is equipped with a centrally-mounted coreless motor that provides power to both bogies. All axles are driven and power is collected from all wheels.

A four-pin coupling arrangement is utilised for joining the cars together, which enables all four cars to be governed under DCC by a single Next18 decoder, located in the chassis of the TSOL; the socket is accessed via a detachable underframe moulding. The unit is ready-wired for sound, with a 10mm x 15mm speaker situated inside the aforementioned detachable moulding; electrical contacts for the speaker take the form of brass tabs.

Recommended for use on curves of no less than second radius (263.5mm), the model's performance was found to be smooth and quiet.



A four-pin coupling connects the cars together.

VINTAGE VIEW British Rail Class 319 EMU



Dual-voltage NSE-liveried Class 319 EMU 319 103 is pictured near Earlswood with a Bedford – Brighton service on 1 August 1992. Photo: Rail Photoprints

Prototype information

These EMUs were built by British Rail Engineering Limited (BREL) in York between 1987 and 1988 (Class 319/0) and 1990 (Class 319/1) for use on cross-London services, notably the then-new Thameslink service which commenced in May 1988 running between Bedford and Brighton.

The four-car units (each car was just over 65' in length) are fitted for dual 25kV ac overhead and 750V dc third rail operation, originally intended for operation north and south of Farringdon. The design shared the same Mk.III EMU body shells as Class 317 and 318 units, though with increased passenger capacity in the driving car and a sleeker cab end profile that was without end gangways; however, emergency doors in the cab fronts were required in case of the need for passenger evacuation in a tunnel.

On 10 December 1993, units 319 008 and 009 received the honour of ferrying the first British passengers through the Channel Tunnel on a special train from Cheriton to Coquelles in France during the handover from the tunnel contractors to Eurotunnel; the units later gained the names of the aforementioned places.

Following the introduction of the Class 700 units on the Thameslink network, examples of the 319s have been redeployed on electrified routes in the North West of England, with operators Northern Rail and West Midlands Railway.

The model is equipped with directional and interior lighting. Each car (with the exception of the MSO) incorporates axle point pick-ups which collect current directly from the rails, powering roof-mounted LED strips; these illuminate when power is applied to the track under 12V dc operation or constantly when on digital control – there is no option to switch these internal lights on/off on DCC. However, each DTSO features an on/off switch to enable the directional lights to be turned on/off manually.

With the firm's ref.36-567A Next18 decoder installed, the operation and lighting can be controlled as follows: F0 turns the directional lights on/off, F1 extinguishes the directional lights in DTSO-B, F2 operates dynamic braking and F3 extinguishes the directional lights in DTSO-A.

Bi-mode versions to follow

It came as little surprise that, as part of the manufacturer's Spring 2022 announcements (see News, RM April 2022), Bachmann confirmed the

production of models of the Class 769 Flex four-car Bi-mode Multiple Units – the full size versions having been developed from the Class 319 units. Converted by Brush Traction for service with operators including Transport for Wales, Northern Rail and Great Western Railway, this brings the story of these units right up to date, which will provide modellers with a welcome addition to the range of modern outline models available in N gauge. We look forward to having sight of one of these in due course.

Sample supplied by

GRAHAM FARISH,
Bachmann Europe Plc,
Moat Way, Barwell,
Leicestershire LE9 8EY.
www.bachmann.co.uk

PRICE
Ref:372-875 – £349.95



Peco has delivered to retailers the first stocks of the all new single-slip (ref. SL-U1180), double-slip (ref. SL-U1190) and long crossing (ref. SL-U1194) for its range of 00 gauge code 75 Streamline bullhead track.

These latest additions to the bullhead range supplement the existing items, which comprise plain lengths of flexible track (ref. SL-108F) and bullhead rail joiners (ref. SL-114), together with large radius left- (ref. SL-U1189) and right-hand (ref. SL-U1188) two-way points. These were reviewed in the January and November 2017 editions of RAILWAY MODELLER respectively. The 00 gauge bullhead range replicates the older style of chaired/timber-sleepered trackwork that was commonplace on Britain's railway network for much of the 20th century – until flat-bottom rail with concrete sleepers became favoured for main passenger routes.

Chiefly hand-assembled in the Peco factory in Devon, the trio of new items match the geometry of the manufacturer's standard flat-bottom rail code 75 and code 100 track systems; the slips and crossing measure 249mm in length with a 12° frog angle. Sleeper spacing matches that of the rest of the bullhead range.

The items exhibit a particularly impressive level of fine detail, with convincing moulded chairs and simulated wooden grain atop the sleepers. The rail joiners that support the machined (not stamped) switch blades on the slips incorporate representations of bolt heads.

The slips can be used in conjunction with the manufacturer's range of under-baseboard-mounted point motors for remote tiebar operation, such as the extended-pin PL-10E twin-solenoid motor (with the PL-9 mounting plate) and TwistLock solenoid motor ref. PL-1000. (Two motors would be

required for each slip.) It should be noted that the sleepering around the tiebars does not include the spigot holes to accept the PL-10 solenoid motor clipped directly to the underside.

As with all new releases of Peco Streamline 00 gauge pointwork items, the slips and crossing are supplied in Unifrog format, meaning that the modeller can choose at the time of installation whether to use the products as supplied with insulated frogs, or whether to power the frogs and employ

a means of switching their polarity, thereby providing electrical continuity throughout the items of pointwork – without the need for powered models to traverse any 'dead' sections. Each frog comes with a connecting wire pre-attached, which can be connected to an auxiliary switch – such as the Peco PL-13 Accessory Switch if using PL-10E motors, or TwistLock Turnout Micro-switch (ref. PL-1005) if using TwistLock motors – to change the frog polarity in tandem with the setting of the route.

The dead frog sections each measure 24.5mm in length, which might prove tricky for locomotives with very short wheelbases to traverse (unless they are DCC-equipped with 'stay alive' capacitors). On test we found that the Hornby Ruston 48DS (minus its extra pick-up wagon) with its 21mm wheelbase and Hornby Peckett (with its 23mm wheelbase) both sat well within this dead section; however any potential inability for these models to maintain electrical continuity when negotiating these items of trackwork can be avoided by using them in live frog mode, with associated polarity switches as noted above.

As with the two-way points, the electrical bonding on the slips prevents them from self-isolating; all four routes remain powered whether used in live or dead frog mode, and regardless of which way the routes are set. This is

advantageous to DCC users, but 12V dc analogue users will, however, have to install separate switched sections on adjoining trackwork if, for example, sidings are to be isolated by the route setting of the slip.

Instructions with diagrams are provided, which advise on all the aforementioned aspects of installation.

Modellers can finally rejoice with the arrival of these much-anticipated items of bullhead trackwork. The wait is also almost over for the manufacturer's forthcoming left- (ref. SL-U1196) and right-hand (ref. SL-U1195) medium radius points; these items were in production at the time of going to press with this issue and samples will be reviewed in the September RM.

Samples supplied by

PECO
Underleys, Beer, Seaton, Devon
EX12 3NA
www.peco-uk.com

PRICES
ref. SL-U1180 – single-slip
£70.00 each

ref. SL-U1190 – double-slip
£75.00 each

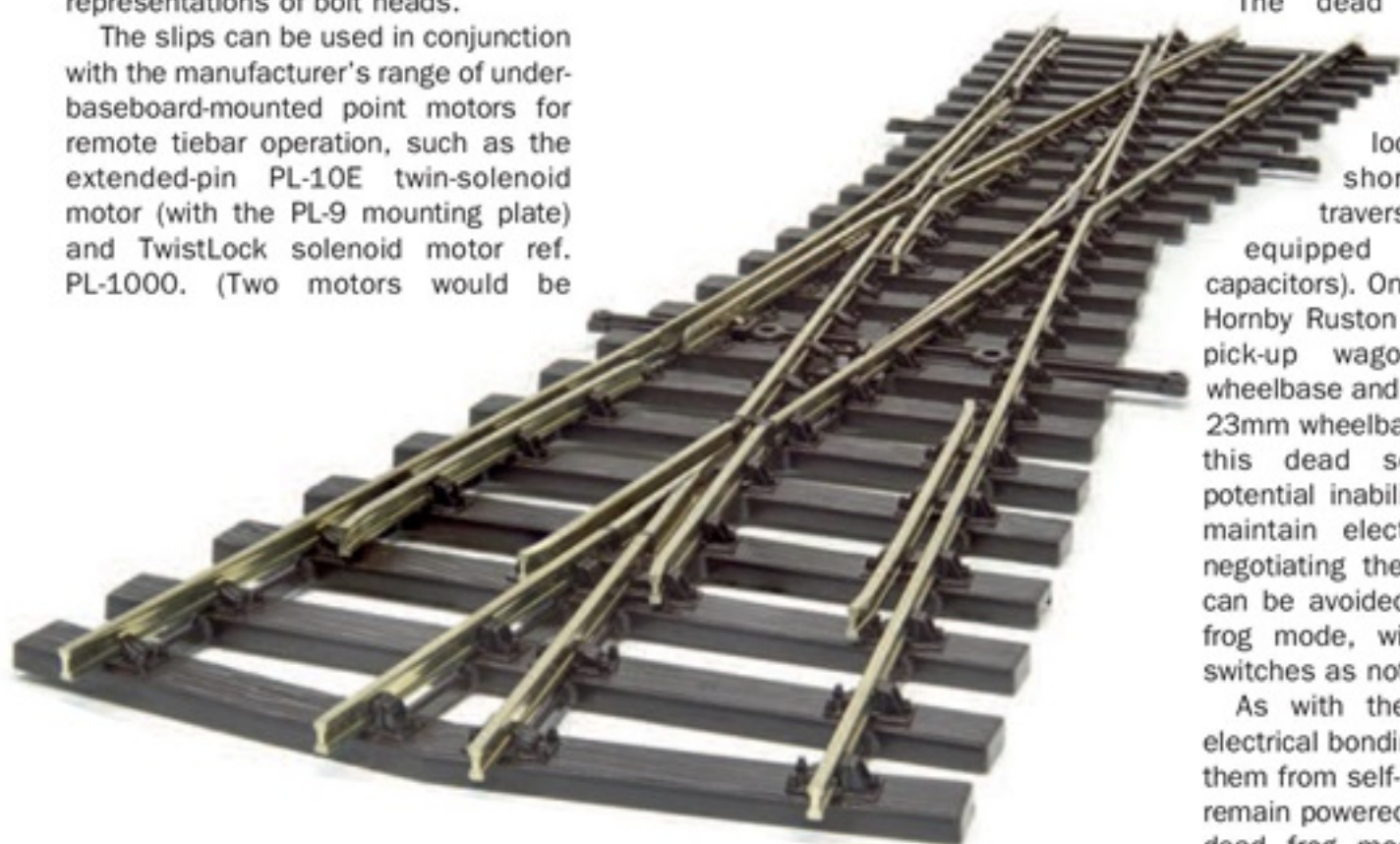
ref. SL-U1194 – long crossing
£30.00 each



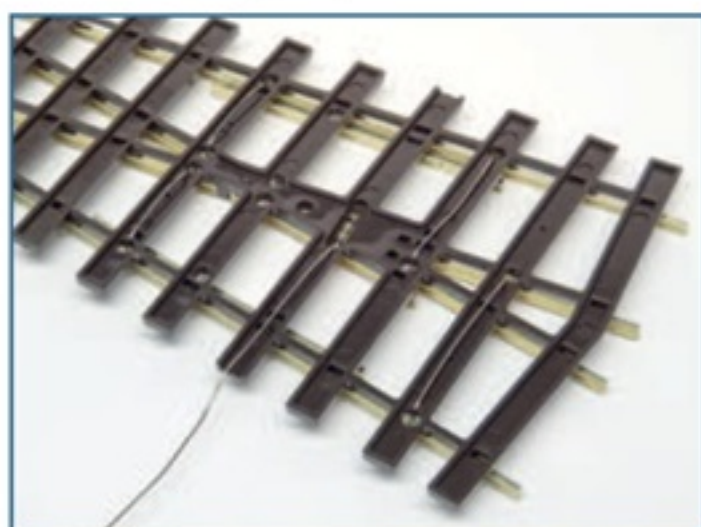
▲ SL-U1190 double-slip.



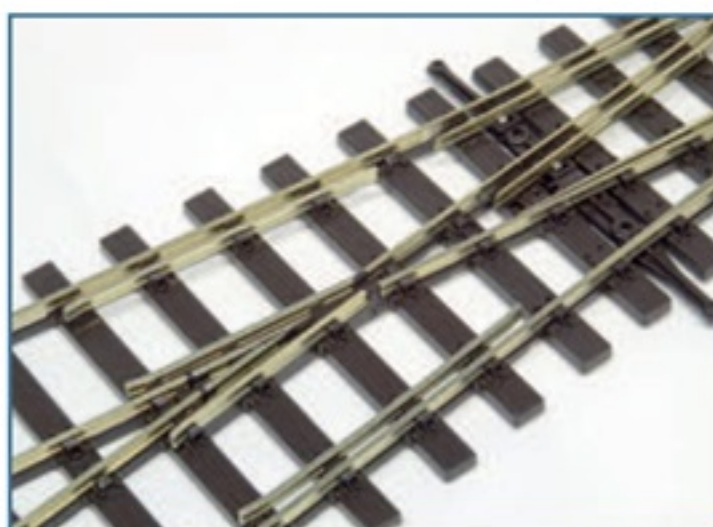
▲ SL-U1194 long crossing.



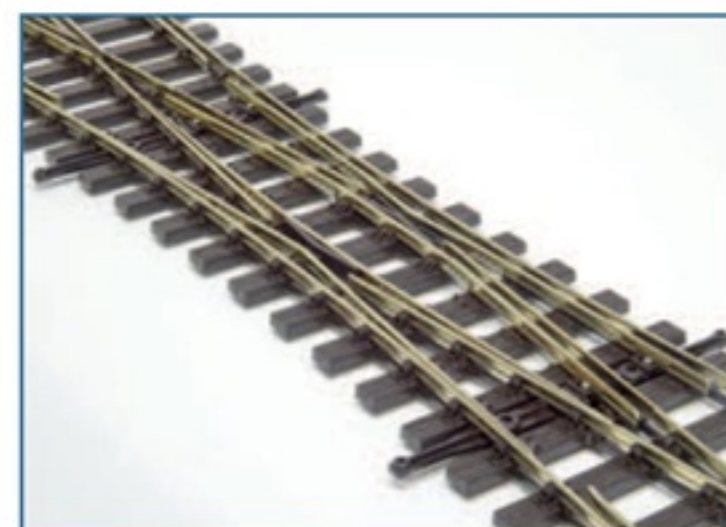
▲ SL-U1180 single-slip.



▲ Wires are provided to change frog polarity.



▲ Authentic bullhead chairs and sleeper grain.



▲ Rail joiners represent fishplates with bolts.

Accurascale has received stocks of the first models that will form part of its MGR (Merry-Go-Round) wagon family – the HAA 32½-ton coal hopper.

First revealed in the pages of our August 2021 issue as part of the firm's 'Powering Britain' series of coal wagons, the Accurascale project is also set to cover the HBA, HCA, HDA, HFA, HMA and HNA designs, as well as the CDA china clay hoppers and MHA ballast/spoil wagons in OO.

A total of 11,161 MGR wagons were built between 1964 and 1982, making them the most numerous air-braked vehicles to run on BR metals to date. All, with the exception of the two prototypes built at Darlington and 160 wagons built at Ashford, were constructed at Shildon. The basis of the design, combined with the track layout at the terminals, allowed for non-stop operation between collieries and power stations, with both loading and unloading being undertaken while travelling at low speed, without stopping or shunting. Initially coded 'HOP AB' complete with B-prefixed numbers, the wagons were reclassified HAA under the TOPS scheme, with the B prefix dropped during the mid-1970s.

The MGR design proved to be a huge success, over the years receiving numerous modifications leading to TOPS code variations including the HBA, HCA, HDA, HFA, HMA and HNA. With the decline in the UK mining industry from the 1980s, a number of vehicles were adapted for other purposes, including china clay (CDA), lime traffic (CBA) and gypsum (PFA); reclaimed chassis from retired members have also been rebuilt as MHA low-sided box spoil wagons for infrastructure and general use. The final MGR coal services ran from New Cumnock in Scotland to Earles Sidings in Derbyshire in 2010. Several examples have been preserved.

For review we received samples of HAA hopper packs ref.ACC2556HAA-FM2 – containing wagon numbers 350059, B 354016 and 356335 in BR unpainted steel with brown cradle; and

ref.ACC2561HAA-RR2 – containing wagon numbers 350053, 354592 and 356038 in BR unpainted steel with red cradle. Further running numbers in the liveries featured here can be purchased across several packs.

Supplied in triple-packs, construction takes the form of diecast metal for the chassis, with injection moulded plastic utilised for the hopper body and underframe details. The models, which are adorned with many fine separate plastic and etched metal components, tip the scales at 50g each. The renditions of the Oleo pattern buffers are metal and sprung, whilst the free-rolling disc wheelsets are metal and fitted within brass bearings.

NEM slimline tension-lock couplings come fitted as standard, mounted in sprung housings which are attached to the underside of the chassis via two screws; these can be removed to allow for the fitting of the instanter couplings that are supplied in a separate accessory pack. Also included for the modeller to fit are red and yellow air brake pipes, which have been fitted here for photography.

The undersides of the models include renditions of the 'X' pattern hopper doors and braking equipment; the latter,

true to the prototype, is a mix of disc- and clasp-braked systems, with plain-style disc brakes forming part of the wheel tooling (one per axle). This level of detail continues above the solebars with associated door operating latches; with two different styles of cover depicted between our samples.

The characteristic angular shape of the hopper bodies, which are moulded separate to the cradles, have been captured well. Particularly worthy of note is the extensive wealth of rivet detail – both inside and out – and the separately fitted internal strengthening bars that span between the sides, together with the representations of the internal door apparatus.

As with previous Accurascale products, livery application is to a very

▲ The wagons are available with hopper cradles in brown (left) or red livery.

high standard, with the printing of all data markings and builders plates fully legible to the naked eye. The demarcations between the unpainted steel colour of the hopper bodies and brown/red of the cradles is sharp and crisp, while details such as brake handles and lamp irons are neatly picked out in white. Naturally the overall pristine finish of these vehicles would benefit from a spot of weathering, to replicate the true look of these wagons in service.

Accurascale has delivered its allocation of these models to customers who placed pre-orders. However, remaining stocks are available from selected Accurascale retailers (see the firm's website for a list of stockists).

Samples supplied by

ACCURASCALE,
60 Windsor Avenue,
London, SW19 2RR.
www.accurascale.co.uk

PRICES
£74.95 per triple pack



▲ The hopper bodies include internal strengthening bars.



▲ Representations of the 'X' pattern hopper doors.



Snailbeach District Railway four-wheel hoppers from Peco

009



The Peco range of ready-to-run narrow gauge rolling stock has been increased with the arrival of models of the Snailbeach District Railway large capacity four-wheel hopper wagons in 009 — plans for which were first announced in our March 2019 issue.

The Peco model depicts the later version of the large capacity hopper, which incorporated steel linings, double dumb buffers and sprung horizontal drawgear. Built around 1905/06 for Granham's Moor Quarry Co. Ltd, but later coming under Snailbeach District Railway ownership, the three initial releases comprise ref.GR-340A No.32 in Snailbeach District Railway (SDR) grey (as restored by the Shropshire Mines Trust); ref.GR-340UB in plain

brown; and ref.GR-340UG in plain grey.

The models are predominantly an injection-moulded assembly, tipping the scales at just 3g. As with recent releases from the manufacturer, wheelsets take the form of spoked plastic centres and axles with metal tyres. Standard Peco couplings are fitted in pockets which are part of the chassis moulding.

The hopper bodies are finely moulded with crisp representations of the horizontal planking and accompanying metal fittings, complete with bolt detail. The internals of the hoppers capture the smooth steel lining with timber trap doors.

The wagon chassis exhibits a high level of fidelity, with fine renditions of



the framework, single steel strip dumb buffers, axleboxes and brakegear. Particularly worthy of note are the brake shoes (one per axle) which are set in line with the wheel treads.

These diminutive hoppers will no

doubt make an ideal partner for the Bachmann Baldwin 10-12-D model — specifically ref.391-030 finished as Snailbeach District Railway No.4, which was one of two Baldwins operated by the line.

Samples supplied by

PECO,
Underleys, Beer, Seaton, Devon
EX12 3NA
www.peco-uk.com

PRICE
£24.00 each

Latest wagon commission from Lancashire Mining Museum

00



The Lancashire Mining Museum has commissioned from Dapol a pair of 00 wagons in exclusive liveries, each limited to 100 models with profits going towards the running costs of the museum.

Both depict 10' wheelbase open wagons, each equipped with metal spoked wheelsets, removable coal loads and slim-line tension-lock couplings, mounted in NEM pockets.



The first of these wagons is a seven-plank 12-ton vehicle in the livery of Hodgson and Company, Liverpool, with the running number 272, in red with white lettering; the second is a four-plank 12-ton example that bears the livery of The Lancashire Steel Corporation Ltd Road Materials Department (Irlam), with the running number 2402, and is finished in black with white lettering and markings.

Samples loaned by

LANCASHIRE MINING MUSEUM
www.lancashireminingmuseum.org

PRICES
£15.00 each + £4.60 P&P
(Order in writing from:
Mr R Bruce,
16 Duke Street,
Astley, Tyldesley,
Manchester, M29 7GL.)

New containers from C-Rail Intermodal

N



C-Rail Intermodal continues to expand its range of fully-finished injection moulded shipping containers with a further selection of 2mm releases.

For review we received samples of the 40' x 8' 6" 'Drybox' which is available in five branding styles: Freightliner red & yellow (1970s), Genstar, Hamburg Süd, MSC and Maersk. In addition there are samples of the 20' x 8' 6" 'Drybox', which is also available in five livery schemes: Freightliner red (1970s), Tiphook, P&O, Hapag-Lloyd and Ocean Network Express (ONE).

Each container type has been tooled with either earlier style ribbed sides with advertising panels or later corrugated sides; two door styles have also been tooled.

As with previous releases, the printing and finishing is commendably sharp on all samples, with lettering and markings on the end doors fully legible.

The door latches and locking mechanism are separately fitted to the main moulding.

The models are suitable for use with a number of modern-era container wagons that have been produced in recent times in N, including the firm's forthcoming FSA/FTA bogie container flat wagons (see news, RM April 2021).

Samples supplied by

C-RAIL INTERMODAL,
Morven, Roome Bay Avenue,
Crail, Fife, KY10 3TR
www.shop.c-rail-intermodal.co.uk/

PRICES
20' Drybox — £6.50 each
40' Drybox — £7.00 each



Cavalex Models has delivered stocks of its TEA 102-ton GLW Class A bogie tank wagon in OO, plans for which were first revealed in the news pages of our May 2019 issue.

The models are based on the examples constructed by the Standard Wagon Company for Total Oil in the late 1980s. Built in two batches, these were fitted with Gloucester GPS 25 pattern bogies. Since their introduction, the vehicles have seen widespread use on traffic flows across the network; most notably serving oil refineries at Immingham and Milford Haven/Robeston, and flows from Holybourne to Fawley.

VTG currently owns the wagons, which are still in operation today, working out of the Lindsey oil refinery. In addition, four TEA tank wagons have been converted to KBA barrier wagons for use on London Underground S-stock transfers.

Three versions of the TEA have been produced – available as a single or triple-pack – covering original grey; original grey with weathered finish; and grey with 'Total' branding. A KBA barrier wagon twin-pack has also been produced. Examples of the model in original grey with weathered finish (ref. TEA WB) and grey with 'Total' branding (ref. TEA TG) are illustrated here.

The frame-mounted tank and chassis components are moulded in plastic, with various separately fitted details formed from metal wire and etched brass; most notably the fine mesh walkway atop the tank bodies. A weight is housed within the tank, giving the models an overall weight of 145g. Buffers are plastic, but sprung.

The models are equipped with slimline tension-lock couplings in sprung NEM pockets mounted to the underframe, which provides sideways articulation. Brake pipes and screw-link couplings are provided in a small pack for the purchaser to install, which have been fitted here for photography.

Underframe detailing is very impressive. However, it was noted that the presence of certain details may impede the free rotation of the bogies around sharp curves. The bogies run on free-rolling metal wheelsets; the manufacturer advises that the modeller will be able to substitute these for P4 or EM wheelsets using 26mm axles, without the need for modification to the model.

Livery application has been completed to a high standard across both versions; with lettering and data markings all crisply applied and fully legible under a glass. The finish of the weathered example simulates the distinctive



▲ The fine mesh walkway on top of the tank is of etched brass and separately fitted.

patchy build-up of grime these vehicles often accumulate in service. The effect has been achieved with multiple layers of tampo printing using various tones, combined with levels of airbrushing. As per the prototypes, the data markings exhibit signs of cleaning.

The models were available direct from Cavalex Models only by pre-order.

However, stocks are available from Cavalex stockists including Trains4U in Peterborough, C&M Models in Carlisle, Collett's Models in Exmouth, John Dutfield LLP in Chelmsford, Rainbow Railways in Linlithgow, Scotland, KMS Railtech in Aberdeenshire and Rails of Sheffield (see the manufacturer's website for further details).

Underframe detail.



Weathering is tampo printed.



Samples supplied by

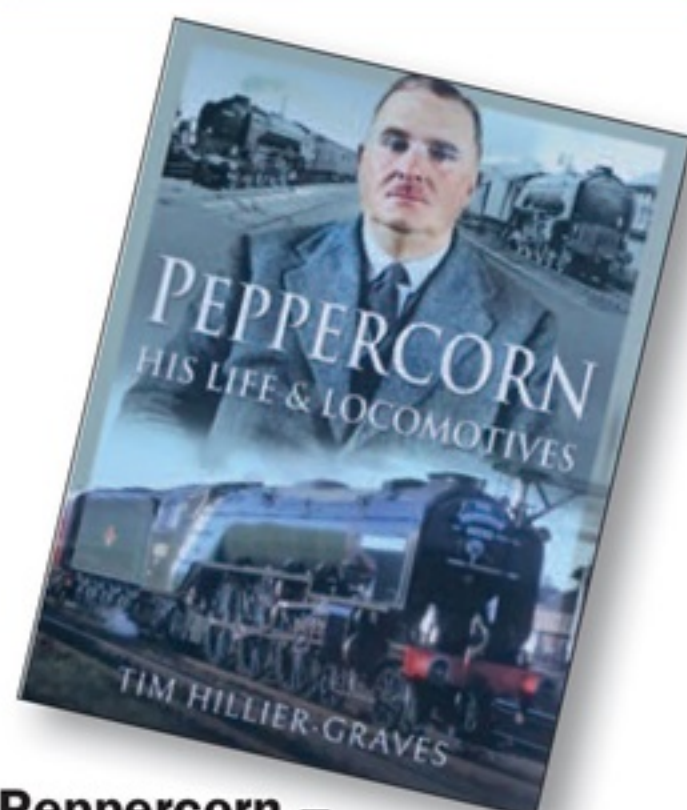
CAVALEX MODELS,
www.cavalexmodels.com

PRICES
Ref.TEA WB –
£45.00 single/£135.00
triple-pack

Ref.TEA TG –
£45.00 single/£135.00
triple-pack



Book Reviews



Peppercorn – His life and locomotives

Tim Hillier-Graves

Arthur Peppercorn almost breaks into a smile on the cover of this book, but the contents prove that while serious in his work, the humour was abundant. As the last of the LNER's Chief Mechanical Engineers he has the dubious reputation of being the tail-ender and is often marginalised in favour of Gresley. This is misguided and does the final CME a disservice as it's clear throughout this book that he was not only a capable engineer, with regard to all the aspects of railway equipment, but most of all an excellent manager of the workforce that worked both with, and under him. Technical skills are one thing but putting that into solid form via others is a skill in itself.

The first chapters of this book start unsurprisingly with Peppercorn's childhood, schooling and family life and his time with the Royal Engineers during WWI, before sliding neatly into his time working under Henry Ivatt (and with Oliver Bulleid) at Doncaster. His later moves to Retford, Stratford and back to Doncaster are detailed, in a logical order, but in many ways the accent is more on his relationships with his contemporaries and superiors, and moreover what they were doing and the technical developments therein. His steady and unremarkable rise through the carriage and wagon departments shows a man who was patient and prepared for a long haul – certainly not an ambitious cut-throat. He eventually succeeded Thompson as CME in 1946 just before the LNER was nationalised, and was therefore very much a man who had reached his time just as there was no time left.

This is a detailed book and sits nicely between the overly technical and the overly personal. Don't think that it is simply an account of family life and anecdotal wit. Peppercorn is the thread of the book, but it is as much about developments within the LNER and is just as much concerned with Gresley and Thompson as Peppercorn. Students and modellers of the LNER will find plenty of information in the pages as the text is profusely supported by photographs and drawings pertaining to locomotive development during the company's history. The reproduction of these is good and the size and presentation is easily large enough to make this worth getting just for this information alone. Highly recommended.

CF

Pen & Sword,
47 Church Street, Barnsley,
South Yorkshire,
S70 2AS.
www.pen-and-sword.co.uk

289mm x 224mm, 257pp
Hardback, £40.00
ISBN 978 1 526729859

Robinson Eight-coupled Locomotives

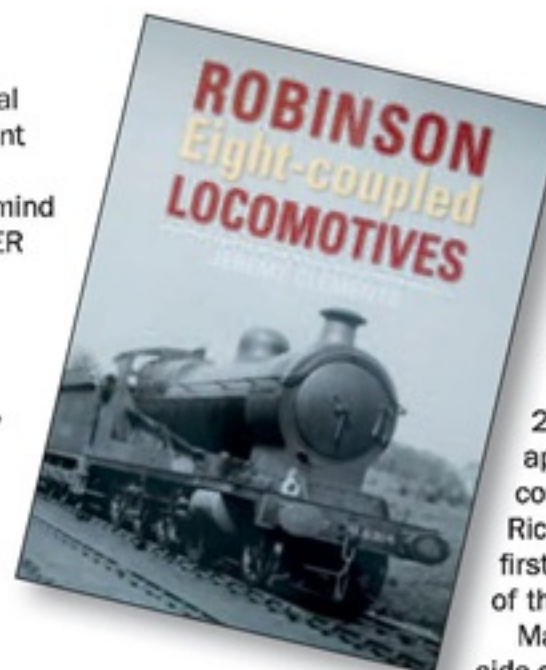
Jeremy Clements

Mention the name of Great Central Railway Locomotive Superintendent John George Robinson, and the locomotive class that springs to mind will probably be his 8K 2-8-0 (LNER Class 04). Though he designed many other fine locomotives, it is these heavy freight haulers that earned arguably the most significant place in railway history – serving their country in both world wars, working (even if only briefly) for most of the pre-Grouping companies after the first of those conflicts, and roaming as far afield as China and Australia.

This book traces their story, and that of his other eight-coupled designs, from the beginning – through the background to Robinson's appointment, and the GCR's need to improve the efficiency of its freight train working with more powerful locomotives, which prompted first the introduction of the Class 8A (LNER Q4) 0-8-0 – the design from which the 8Ks were derived. In mentioning the GCR's London Extension, however, it does fall into the trap of repeating the common but erroneous belief that this line was built to Berne loading gauge, which did not come into use until 1914.

It also includes the other classes that Robinson created through extensive use of standard components – the impressive Class 8H 0-8-4T shunters for Wath marshalling yard, and the Class 8M 2-8-0, a further development of the 8K but one which, in the light of its predecessor's success, was perhaps unnecessary. Some tantalising might-have-beens are mentioned, too – such as British and American proposals for a 2-10-2 to haul coal trains between South Yorkshire and Immingham docks, and a 'double 8K' Beyer-Garratt which, the author argues, would probably have been more successful than the Gresley Garratt that was built instead.

As for the 8Ks themselves, their wartime service



and various wanderings are comprehensively documented, along with the many modifications and rebuilds that created several 04 sub-classes (and Edward Thompson's reconstruction of the Q4s into Q1 0-8-0Ts).

Having covered the Robinson 2-8-0s in Australia, one of several appendices is devoted to their close cousins – the Kitson 2-8-2Ts for the Richmond Vale Railway that look at first glance like a tank engine version of the 04.

Many of the photographs are static side-on or front three-quarter shots of locomotives on shed – which might not

please those seeking scenes of them in action, but will certainly be of use to the modeller, especially when it comes to detail differences. The quality of the original pictures is very varied – some of the overseas and wartime shots in particular are quite poor, but their historical interest more than makes up for this, and thanks to good-quality paper, all have probably reproduced as well as possible.

It does full justice to the story of a hugely important and endlessly fascinating family of locomotives, and usefully shows it in the wider context of railway history. It will certainly be essential reading for anyone modelling the GCR, or many other areas of the LNER or Eastern Region, and seeking to assemble an accurate fleet of these machines.

TJ

Crécy Publishing,
1A Ringway Trading Estate,
Shadowmoss Road,
Manchester, M22 5LH.
www.crecy.co.uk

220mm x 287mm, 224pp
Hardback £25.00
ISBN 978 1 91080 968 6

Midland Railway and LMS 4-4-0 Locomotives

David Maidment

This is the latest in a series of locomotive portraits from the publisher and author, in this case the elegant family of 4-4-0s rooted around the Midland Railway stock. If a series of locomotives could be described as 'pretty' then this is it.

The designs emanate from the end of the 19th century when Samuel Johnson took one of his earlier Great Eastern Railway designs and reworked it for the Midland as an answer to the power requirements of the then new and heavier bogie express passenger stock. He thus set in place a root design that would define, and possibly plague, the Midland Railway and Region for decades. Aside from the 1902 Compounds the 'pretty' elegant lines remained throughout the breed even in British Railways days, though some of the lines are lost with the shift of lining designs on the splashers and cabsides.

The book follows the styling of previous volumes, working through a design chronologically with the accent heavily swayed toward statistics and performance. There is an opening chapter on the three designers – Johnson, Deeley and Fowler – before plunging straight into the numbers... and there are a lot of numbers!

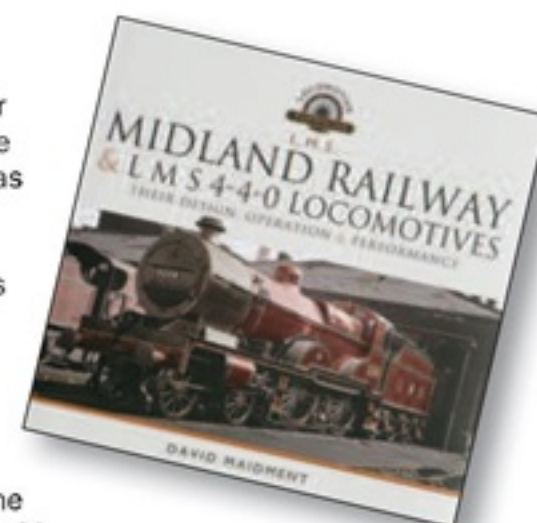
The book then moves through each of the major designs and each upgrade (which in the main tend to be boilers and associated reinforcement). Each of the 'tweaks' is discussed fully, and the changes are fully supported by tables of performance records and a quite astounding number of period photos reproduced to a very high standard.

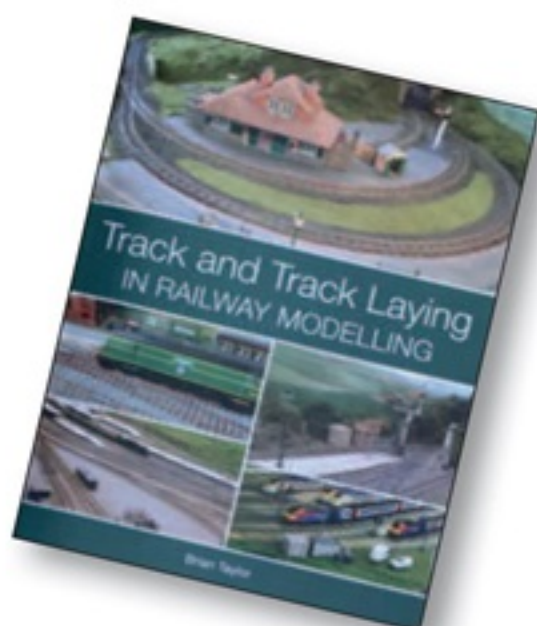
The modeller may not find the statistic-heavy text particularly appealing and there are no scale drawings included, only a series of weight diagrams in the final section. Don't let this put you off, though, as this (like the rest of the series) is as close to the most comprehensive portrait of the loco classes as you are likely to get. If you only view it as a very well-produced photo album of more than 400 images, you will still have easily got your money's worth! Note that most of the photos are from the Manchester Locomotive Society's collection and are generally not the usual suspects. The locomotives aside, these are more than valuable for the period record of the MR and LMS. Recommended.

CF

Pen & Sword,
47 Church Street, Barnsley,
South Yorkshire, S70 2AS.
www.pen-and-sword.co.uk

245mm x 260mm, 352pp
Hardback, £35
ISBN 978 1 526 772503





Track and Track Laying in railway modelling

Brian Taylor

The series of guidebooks from Crowood Press are now well known and seemingly endless with new titles appearing on a regular basis. This latest volume on trackwork is a little deceptive.

The author will be familiar to many RM readers as a contributor to the magazine and professional layout builder and artist. This artistry is front and centre in the book with many of the illustrations presented in a pen and wash style, all by the writer.

The text commences with a basic history of railway track, then describes transition curves and the model scale differences; so far so good. The book then goes somewhat off-piste and moves away from track and into a section on perspective.

Layout design and track plan shapes are discussed, followed by sections on baseboard construction and where to site your home layout. It is only in Chapter Five that we get back to the track itself. This is comprehensive and takes the reader from sectional track options through to pointwork and onward to basic soldering and wiring for analogue and DCC, rounding off with a couple of pages on hand-built track and a table of scales and gauges.

The deception suggested above is in the title. This is an excellent primer for the novice who is possibly moving on from the first train set, and the sections on layout design are worth the cover price. If you are expecting a comprehensive treatise on the dark arts of hand-built finescale track laying then this isn't it. If you want a good basic primer on layout design and building written by a professional, accompanied by some excellent characterful artwork, then I wholeheartedly recommend it to you; just ignore the title.

CF

The Crowood Press,
Ramsbury, Marlborough, Wiltshire SN8 2HR.
www.crowood.com

245mm x 190mm, 110pp
Softback £14.99
ISBN 978 178500 9952

The Deltics & Baby Deltics – A tale of success and failure

Andrew Fowler

A shade over 40 years ago the last Deltics were withdrawn from BR and probably remain the first-generation diesel that will make enthusiasts go a little misty-eyed. The story has been told before, so is it worth retelling? This book opines that it is, and presents the story in a very easy-to-read way with just enough technical information to keep it moving without getting bogged down in facts and figures.

The first section is split into four chapters: the first outlines the requirement (though this was questionable at the time) for a top-flight diesel locomotive to replace steam on the crack express services, and quickly details the earlier LMS No.10000 and the SR's Bulleid 1-Co-Co-1 machines. This leads on to a fascinating section on the prototype *Deltic* (thus named after the engine shape – Delta). This was not universally welcomed, but remained in service for some five years, though still owned by builders English Electric and not British Railways. This proved the design and gave rise to the order of 22 production machines, the story of which is dealt with in the fourth section.

Shoehorned into this in Chapter Three is the story of the ill-fated Vulcan Foundry D59XX Type 2s. The only relationship to the *Deltic* is the half-size EE 9-cylinder engine and the derivative common name; the outer styling being closer to the Type 4 Class 40. Overweight for the required brief and plagued with engine and transmission problems, it is a lengthy tale of woe and despite best efforts with part upgrades and eventual replacement of the EE engine, the class met a sticky end, just scraping into Rail Blue and the 1970s, but not receiving the TOPS number of Class 23.

The fourth chapter details the production Deltics and eases from full text to a mix of text and full-page colour and black and white photos. Most of the section is made up of an excellent pre-TOPS number loco-by-loco guide with extended captions. As a basic guide the book works very well; the presentation is excellent with good photo reproduction, though the photo-light Baby *Deltic* section feels odd and may have benefited from a separate dedicated volume.

Pen & Sword, 47 Church Street,
Barnsley, South Yorkshire, S70 2AS.
www.pen-and-sword.co.uk

178mm x 280mm, 197pp
Hardback, £25.00
ISBN 978 1 473 833913

CF

Railways in Cumbria – A snapshot from the fifties and sixties

Compiled by David Spaven

The cover picture of this book alone leaps out as a wonderful cameo scene to recreate in model form – an Ivatt 2MT 2-6-0 shunting Flusco lime works on the Cockermouth, Keswick & Penrith route, while a Land Rover waits to cross the line – and there is plenty more modelling inspiration within.

Its blurb states that it "sought to provide some unusual images from locations which have often been bypassed in the (literal and metaphorical) rush along the West Coast Main Line over Shap – but the latter has not been neglected either". In this it has succeeded – some fine images of Stanier engines on the WCML, the Settle & Carlisle and around the Border City certainly feature, but so do many other locations off the enthusiasts' beaten track, such as the Cumbrian Coast line and its varied branches for tourist and industrial traffic. There are plenty of interesting gems, such as the last surviving Furness Railway 0-6-0s, and a North Eastern Railway B16 4-6-0 making a rare foray over the Waverley Route.

Drawing on the extensive Transport Treasury archive, the publishers have chosen the photographs well – quality black and white images that are crisply reproduced – and their captions are very accurate and informative. One has misidentified an Ivatt 4MT 2-6-0 as a BR Standard 4MT 4-6-0, but this is understandable when the locomotive in question is fairly small in the picture, taken in a rain storm on Grayrigg bank!

The fact that the North West was such a magnet for enthusiasts in the final decades of steam has inevitably led to many photographic albums dealing with the area – but that very level of interest means there is always room for more, and in covering some of the areas that others miss, this is a worthy addition to the shelf.

TJ

Totem Publishing (an imprint of Transport Treasury Publishing)
16 Highworth Close, High Wycombe HP13 7PJ
www.totempublishing.co.uk

246mm x 192mm, 80pp
Hardback, £13.50
ISBN 978 1 913893 20 0

British Railways Mark 1 and Mark 2 coaching stock

Hugh Longworth

Part of the early plans for the new British Railways was to create a standard fleet of coaching stock and this new breed began to enter service from 1951, followed 13 years later by the Mk.II versions. Although there have been several books published on the subject, these have possibly not been as comprehensive as Longworth's 2013 book which has now been updated and revised.

The book opens with a brief description of the development of the stock, including the changing bogies and pre- and post-TOPS numbering schemes, followed by a very useful 'how to use this book', without which the reader would not be able to decipher some of the iconography within the listings. The listings that follow and naturally make up the vast bulk of the book are presented in coloured panel style accompanied by drawings of elevation and seating plans to approximately 1.5mm:1'. Each is classified by type (beginning with restaurant stock) and includes the builder, any renumbering details, scrappage dates, regional reallocations and any conversions.

Mk.I vehicles are followed by Mk.IIs in each type, some only one item and some larger batches running to a whole page. Both main line and suburban stock is covered, but also (and this may tip some wagon buffs into buying) the large fleet of Mk.I Non-Passenger Coaching Stock, departmental stock, Cartrics/Carflats and even several pre-Nationalisation vehicles which seemed to have slipped in unnoticed!

The quality of printing is top-notch and the selections of supporting photographs are clearly reproduced. Many will perhaps view this as a glorified spotter's book, which at some levels it is, but it will doubtless be a volume that is referred to much more regularly than the buyer originally envisaged.

Crécy Publishing,
1A Ringway Trading Estate, Shadowmoss Road,
Manchester, M22 5LH.
www.crecy.co.uk

304mm x 217mm, 305pp
Hardback, £35.00
ISBN 978-1-910 975 4

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AUGUST 2022 • NEWS P.598 • NEXT MONTH P.603

Heljan first in ready-to-run TT:120 with Brush Type 2 diesel



Heljan has announced the first ready-to-run British outline locomotive in the new TT:120 scale – a BR Brush Type 2 (Class 31) diesel.

The news follows Peco's unveiling in the July issue of its new range of track, buildings and lineside accessories for the all-new TT:120 scale. CAD work is already well advanced on the Class 31, and Heljan expects to begin tooling the model shortly.

Six liveries are planned for the initial release: D5637 in BR green; D5847 in BR green with small yellow panels; No.5820 in BR green with full yellow ends; 31 136 in early BR blue with headcodes; 31 198 in late BR blue with domino headcodes; and 31 970 in Railway Technical Centre red, grey, black and white livery. Further models in

refurbished condition, representing the 1980s and later periods, will follow if there is sufficient demand.

All versions will be available in DCC-ready format or with factory-fitted DCC sound, featuring LED lighting features and a Next18 DCC decoder interface. With 1:120 scale TT already in widespread use overseas, the models will be constructed to international standards for couplings and wheels, for maximum compatibility with other manufacturers.

The suggested retail price for the Class 31 is £199.00 for DCC-ready versions, while prices for DCC sound are to be confirmed. Delivery is expected in late 2023.

The Danish manufacturer is also considering two more locomotives as

possible future projects in TT:120 – the Hunslet Austerity 0-6-0ST and BR Class 08 0-6-0 diesel-electric shunter.

New product updates

Some of Heljan's ongoing projects in other scales – the LNER O2 2-8-0 and Class 25 re-run in OO, and the O gauge Mk.I and Mk.II coaches – have been delayed by the unexpected closure of one of its factories. Production of these models has been transferred to other factories but will take longer than planned.

The next Heljan models to arrive will be the latest batches of OO gauge Class 07 diesel shunters and O gauge Class 37/4s (both expected in July) and upgraded OO gauge Class 14 (expected in August).

The OO gauge Class 45 'Peak' from all-new tooling (announced in RM July 2019) is also expected to reach the UK in September, while the next batch of OO gauge Class 33s will be of the push-pull Class 33/1 and narrow-bodied Hastings line Class 33/2 varieties. One new livery – 33 103 in late 1990s/early 2000s Fragonset black – has been added to the range and a limited number are available to order.

Two changes have been made to the planned range of O gauge Class 55 'Deltics' from revised tooling – the new identities are 55 007 *Pinza* in BR blue with plated headcode panels, and 55 011 *The Royal Northumberland Fusiliers* in weathered BR blue.

For more details and updates, see: www.heljan.co.uk

TT:120 accessories from Osborn's Models



Bideford retailer Osborn's Models has released a selection of items that have been developed for 1:120 scale. The release of these items follows the news in the July issue that Peco has developed track and accessories for TT:120 – a new scale and gauge combination for British outline.

The Osborn's Models releases comprise a laser-cut GWR two-road lattice footbridge kit (ref.ALTT031 – price £29.99), laser-cut GWR six-ton yard crane (ref.ALTT032 – price £19.00), and two 3D printed vehicles – a Bedford Tipper and Massey-Ferguson tractor (contact the retailer for price details).

Note that the laser-cut kits are supplied with parts unpainted (assembled and painted versions are pictured here) whilst the 3D printed vehicles are supplied painted (the Bedford is shown unpainted).

N gauge versions of all of these products are available. For further

information:

Osborn's Models
7 Honestone Street,
Bideford,
North Devon EX39 2DL
Tel: 01237 423 453
email: info@osbornsmodels.com
www.osbornsmodels.com



GWR pagoda and Slinfold station in TT:120



▲ A CAD render of Recreation21's 3D printed GWR pagoda hut in TT:120.

Recreation21 has added two TT:120 scale buildings to its 3D printed range.

One is a GWR corrugated iron pagoda hut, of the type widely used across the company's network for waiting shelters at small stations and halts. The other is Slinfold station building, a mid-Victorian LBSCR structure on the Guildford to Horsham direct line.

Both are available to order printed in white nylon plastic, with the pagoda retailing at £17.39 and Slinfold station at £88.55 from:

www.shapeways.com
www.rue-d-etropal.com



▲ A CAD render for the model of Slinfold station.

"Full steam ahead" for Warley 2022

That was the message proclaimed by the organisers of this year's Warley National Model Railway Exhibition during a sponsors meeting that was held on 22 June 2022. Set to return after a three-year absence (it was last staged as a 'live event' in 2019) the two-day show will be taking place at the NEC in Birmingham on Saturday 26 & Sunday 27 November 2022.

Although supported by the sponsorship of Peco, Bachmann Europe Plc, Hornby and Crécy Publications, the event is organised entirely by members of Warley Model Railway Club. It is the biggest function of its kind in the UK and is, as has been remarked previously by Warley MRC Chairman Paul Jones, "...run by enthusiasts for the benefit of fellow enthusiasts in true club tradition".

In excess of 70 layouts are booked to attend, from Z gauge through to Gauge 1 (and bigger!) and covering a variety of British and overseas prototypes.

Amongst these will be *Northbridge* (pictured), which is a OO layout by members of Warley MRC that was featured in our December 2018 issue. In addition there will be more than 150 trade stands, together with modelling demonstrations, society displays and a full-size locomotive centrepiece exhibit in the form of new-build BR Standard Class 3MT 2-6-2T No.82045.

Advance tickets and show guides will be available via The Ticket Factory (see link via the website below). It should be noted that tickets will be on sale at the show, but payment can only be made by debit or credit card due to NEC requirements.

As with recent years, accompanied children aged 6-15 years can gain entry for just £1.00. This 'kids for a quid' ticketing initiative provides much better value than the Family ticket option available previously.

Look out for further details about this year's event, together with associated

articles and coverage, in *RAILWAY MODELLER* in the coming months.
www.thewarleyshow.co.uk

Northbridge, featured in RM December 2018. Photo Steve Flint



Sutton's Loco Works shows progress with 'Highland' Class 24 in 4mm



Samples of the latest versions of the 4mm scale Sulzer Type 2 diesels being developed by Sutton's Locomotive Workshop have recently gone on display. These represent the batch of BR Class 24/1s that spent almost all their time working in the Scottish Highlands from Inverness.

With plans for these versions first announced in the news pages of our December 2018 issue, the engineering prototypes illustrated here represent a significant step forward for a project which has been interrupted by the well-reported issues affecting global trade. The manufacturer advises that tooling is now complete and production models are expected to arrive next year. The most obvious feature is the illuminated roof-mounted headcode box, but there are also numerous other changes that mean this is, in essence, a brand new model from the solebars upwards (see review of previous versions, RM July 2020).

Two body types (early front doors with draught-proofing strips and the later re-plated front ends) have been produced and feature working twin headlights and a posable tablet catcher. Other details catered for include two styles of boiler air inlet

Two distinctly different versions of the SLW 'Highland' Class 24/1 have reached the sample stage. One represents the later green and early blue period retaining the front end doors (albeit sealed-up with strips to prevent draughts) and tablet catcher. The other is the final form with re-plated front ends and token equipment removed but cabside recess still *in situ*.

cowl, miniature three-part snowploughs and the distinctive slotted frost covers. Photo-etched radiator grilles are standard.

Three variants of the 'Highland' locos are available to reserve (analogue and DCC sound, with no deposit required) comprising BR green with small warning panels, early BR blue with four arrows and D-prefix identity, and the full corporate BR blue rendition with five-digit TOPS numbering. In addition there will be a second tranche of running numbers that will cater for those wanting to run double-headed trains (commonplace on the Highland main line in the 1970s). For more details:

Sutton's Locomotive Workshop
Tel: 01780 470 086
www.railexclusive.com

Decorated 9F arrives as Hornby plans Jubilee HST



Hornby has received the first decorated sample of its forthcoming BR 9F 2-10-0, and announced a fictional silver HST to mark HM The Queen's Platinum Jubilee.

Pictured is the sample of 9F No.92194 in BR black with late crest and BR1F tender; the other planned numbers and liveries are No.92167 in BR black and No.92220 *Evening Star* in BR green, and the models will feature a flickering firebox. Following any minor changes, the models are expected to be available by the end of the year.

The HST train pack contains two Class 43 power cars in a fictional LNER silver and grey livery, inspired by the 1935 guise of the first LNER A4 Pacific, No.2509 *Silver Link*, and numbered 43052 and 43022 after the Coronation Year of 1952 and the Platinum Jubilee year of 2022. Similarly, the Mk.III coaches (available separately) carry numbers marking the Coronation year and subsequent Jubilee years, such as ER0082 and ER0012.

Hornby 9F No.92194 with BR1F tender.

Profits increase

Hornby reported an increase in sales, revenue and profits in its annual report for the year ending 31 March 2022, despite supply chain difficulties and shipping delays.

The company has stated that it is seeking to increase direct sales to consumers through digital channels, which will include more products being announced outside the usual January period.

It was also revealed that the Hornby Group has been planning more production in countries outside its main China base. However, Executive Chairman Lyndon Davies said: "This should not be seen as a movement out of China, but rather an opportunity to work with new manufacturers on our extensive range of existing products; not just to supply ourselves, but also to build new demand in their local markets."

www.hornby.com



The Platinum Jubilee HST train pack.

New livery for Pecorama loco revealed at gala event

Crowds flocked to the special gala event that was held on the Beer Heights Light Railway at Pecorama on 2-4 June 2022 to celebrate HM The Queen's Platinum Jubilee.

All the 7½" gauge line's steam, diesel and electric locomotives were in operation, with double- and even triple-heading of trains, demonstration goods workings and shuttles. Trains ran to a special timetable that had been created for the event.

On the Thursday of the event a new livery was unveiled on the railway's diesel locomotive *Jimmy*. The accompanying photo shows the locomotive resplendent in a re-imagining of BR Intercity livery, complete with a

seagull motif in place of the swallow!

A rededication of the locomotive by BBC radio and television presenter Judi Spiers is planned for Wednesday 3 August 2022. More details will be posted on the Pecorama website closer to the time.

For details of opening times and future events at Pecorama, visit the website:

www.pecorama.co.uk

Visitors to Pecorama for the Platinum Jubilee gala enjoyed an intensive timetable on the Beer Heights Light Railway. A busy moment is captured here on the Thursday of the event, with newly repainted *Jimmy* pictured on the left. Photo: Callum Willcox



NMRA award presented in memory of Bob Phelps

A celebration of the life of Bob Phelps, the former Peco Senior Product Designer who passed away earlier this year, was held at Pecorama on 29 May for family members, close friends and colleagues from across the hobby.

One of those who attended was Mike Arnold, the Atlantic Director of the National Model Railroad Association, who presented an award on behalf of the NMRA to acknowledge Bob's huge contribution to the model railroading hobby, which included design work on the Peco HO scale Code 70 and Code 83 track ranges. Mike is pictured here alongside Liz Phelps, who received the award on behalf of her late husband.



▲ Bob Phelps NMRA award.

Modelling courses to resume at Pecorama

After a break of three years, Peco will once again be hosting modelling courses at its Devon headquarters, which is located in the fishing village of Beer overlooking Lyme Bay. The first of these will be one-day *Beginners' Guide to DCC* courses, which are planned to take place on 18 August and 3 September. These will be followed by two-day courses on 20-21 and 22-23 September covering *Tracklaying and Layout Electrics*.

Intended to provide participants with a practical introduction to the core aspects of railway modelling, much of the course content will be hands-on, with expert tuition provided by in-house Peco modelmakers.

For further information, including course times, prices and details of how to book, refer to the Pecorama website:

www.pecorama.co.uk

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Basingstoke group builds layout for Network Rail community safety campaign

The Basingstoke & North Hampshire Model Railway Society has constructed a layout for Network Rail as part of a community safety campaign.

The OO gauge modular layout has double-track with third rail, a station, level crossing (with sound, working lights and barriers) and a foot crossing, with figures depicted in dangerous positions. It will be taken to schools, youth clubs and communities in the Wessex Region to highlight the potential dangers of railways.

Marcia Burnett, Community Safety Manager

Basingstoke & North Hampshire Model Railway Society team members Adam Neale, Peter Thorpe, Ian Morgan and Chairman Robin Gilchrist (left to right) handing the community safety layout over to Marcia Burnett at Network Rail's Basingstoke ROC.

for Network Rail's Wessex Route, said: "I have given it a test run in the office and it got a lot of interest. I've safely stored it away from the big kids so that the little ones get a chance to see it in action! Thank you all – it is perfect in every way."



Scale length Salmon wagons in stock at Footplate



▲ Retooled Flangeway Salmon wagons for OO, presented in olive green, yellow and black liveries.

Model shop Footplate of Kidderminster has revised versions of its Flangeway Salmon wagon in stock, retooled to the correct 60' length.

The original OO gauge model of the BR engineers' bogie wagon, reviewed in RM April 2021, was subsequently found to be under scale length. Footplate and its partner Dapol have now resolved this issue, and the retooled models have

been despatched to customers who purchased the original items.

Limited quantities of the retooled models are now on sale in BR engineers' black, olive green and yellow liveries, with prices held at the pre-pandemic level of £49.95.

Tel: 01562 750076

www.footplate.co.uk

New BR Mk.I suburban stock in OO from Accurascale



Accurascale is to produce a new range of BR Mk.I suburban coaches in OO gauge to complement its forthcoming Brush Type 2 diesel.

Representing the non-gangwayed stock built on 56' 11" underframes between 1954 and 1956, the models will include two types that have never previously been offered in ready-to-run form – the Composite Lavatory and Second Lavatory Open. Also available will be the Third Open, Third, Second, Brake Second and Brake Third designs, initially in BR carmine and Rail blue liveries, with maroon to follow later.

Having seen use on several BR regions, these carriages ran with a huge variety of steam and diesel traction, including tank locomotives on the London Midland and Western Regions, N7s, N2s, B1s and L1s on the Eastern Region, and most classes of Type 2 diesel. The last examples survived on King's Cross suburban duties until 1977, hauled by Class 31s; Accurascale's model of the latter was announced in the April issue of RM.

Accurascale was assisted in its research by the North Norfolk Railway, which recently restored a four-coach rake of these vehicles. The model tooling will include details such as patch repairs to the bottom of the bodywork and different ventilator types, while the interiors will include lighting and etched metal luggage racks, and will be fully accessible thanks to a removable magnetic roof.

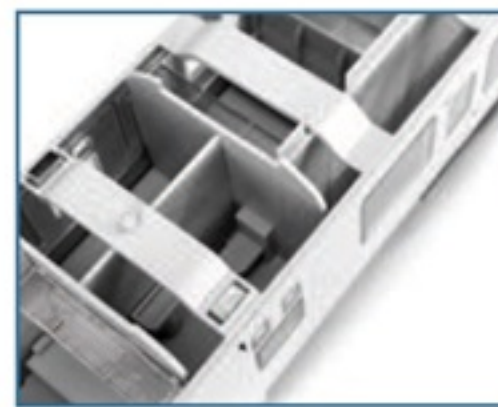
Initial samples have been produced and are now being assessed, with decorated samples due later this year and delivery expected in the third quarter of 2023; the price will be £64.95 per coach, with a 10% discount available for purchases of two or more.

37s in production; Mk.IIs to follow

Accurascale's forthcoming OO gauge Class 37 diesels are now in production, and an additional model has been added to the range – 37 402 Stephen Middlemore 23.12.1954 – 8.6.2013, in its current BR large logo livery applied by Direct Rail Services.

Part of the 'Accurascale Exclusives' range, 37 402 can be pre-ordered via the manufacturer's website, and is expected to arrive in October this year along with the other modern Class 37/4s and the Scottish Class 37/0s with car headlamps. The second batch

▲ A pre-production sample of Accurascale's forthcoming BR Mk.I suburban Composite Lavatory.



▲ The detailed interior of the Mk.I CL, complete with etched luggage racks, and accessible via a removable magnetic roof.



▲ 37 402 Stephen Middlemore 23.12.1954 – 8.6.2013 in BR large logo livery, as applied by Direct Rail Services.

to arrive will be the Class 37/6s and the Network Rail yellow Class 97/3s, in mid-December, and finally the BR blue and green Class 37/0s in February 2023. A second run of models will follow, including the 'heavyweight' Class 37/7s.

Decorated samples of Accurascale's Mk.IIb coaches have arrived, in BR blue and grey, Provincial and Network SouthEast liveries, as well as those in Northern Ireland Railways guise for sister brand Irish Railway Models. For more details and to pre-order, visit:

www.accurascale.co.uk

www.irishrailwaymodels.co.uk

Class 89 confirmed for OO

The retailer Rails of Sheffield has confirmed plans (first proposed in the July 2020 issue) to produce OO gauge models of the unique Class 89 Co-Co electric locomotive in conjunction with Accurascale.

Five versions are to be produced that will be available exclusively from Rails of Sheffield or direct via the manufacturer's website. These comprise ref.ACC2281 InterCity

Executive livery as carried from October 1986 to November 1988; ref.ACC2283 InterCity Swallow (original) livery with Avocet nameplates as carried from December 1988 to July 1992; ref. ACC2294 GNER livery (with white lettering) as carried from March 1997 to Jan 1999; ref.ACC2296 GNER livery (with gold lettering) as carried from February 1999 to June 2007; and ref.ACC2298 InterCity Swallow livery as worn currently. Each will be priced £299.99 DCC-ready and £329.99 DCC sound-fitted.

The specification of the model is set to mirror that of the manufacturer's forthcoming Class 92, including all-wheel drive (with sprung centre axle) and all-wheel pick up, transmission via a twin-flywheel fitted five-pole motor, a 21-Pin MTX DCC decoder interface, comprehensive lighting functions, and a pantograph that can be raised and lowered (under DC or DCC).

All versions can be secured for a £30.00 deposit via the respective Accurascale and Rails of Sheffield websites:

www.railsofshffield.com



▲ Livery artwork for two of the five planned Class 89 models in OO.

Chris Deith

Chris Deith sadly passed away suddenly on 12 May at the age of 76 whilst enjoying a holiday in Italy with his wife Bridget.

Chris will be remembered by many in the model engineering and hobby world as the mastermind and entrepreneur who created TEE Publishing, Engineering In Miniature, Meridienne Exhibitions and the Warwickshire Event Centre.

He is survived by Bridget, daughter Avril, daughter-in-law

Heather and three grandchildren. Sadly, his son Adrian who many will also have known passed away in December 2021.

Chris achieved many things during his lifetime and his daughter Avril, who has worked alongside him for many years and is now Exhibition Manager with Meridienne Exhibitions, will continue his legacy in his honour with the support of family and the team.

In brief

Hornby: A Model World is back

UKTV has commissioned a second series of *Hornby: A Model World* for its factual channel Yesterday.

Produced by Rare TV and narrated by Sunetra Sarker of *Casualty*, the new series will once again go behind the scenes at Hornby in Margate, observing the design processes of models including steam locomotives and aircraft. Currently being filmed, it will be screened on Yesterday next year.

The first series, screened in 2021, featured design work on Hornby's die-cast Merchant Navy, plus layouts by music producer Pete Waterman and the actor and comedian Eddie Izzard.

Longparish film

Dumpman Films has released the latest in a series of DVDs showing the remains of dismantled railways. The new film traces as much as possible of the Longparish branch line from Fullerton Junction to Whitchurch, which closed to passengers in 1931 and to freight in 1956. It can be ordered from www.dumpman.co.uk or via eBay, while further films of Winchester Chesil station and Hockley Viaduct, Winchester Junction to Itchen Abbas and Alresford, and the Allhallows-on-Sea branch, are in production.

4mm Irish forklifts

Twin packs of 4mm scale CIÉ (Córás Iompair Éirann) forklift trucks are now on sale at Irish Railway Models. In orange livery with CIÉ roundels, the injection moulded plastic models have poseable forks, while one vehicle is open and the other cabbed. They can be purchased at £10.94 from: www.irishrailwaymodels.com



Exmoor Rail returns

After a break of two years, Exmoor Rail will be held at the West Somerset College in Minehead on Saturday 6 August 2022.

The event is presented by the Exmoor Coast Railway Modellers, and a total of 16 layouts are booked to attend, together with modelling demonstrations and trade support. One of the layouts on display will be Callum Willcox's OO gauge model of the Fry's Somerdale Railway, which was featured in the October 2020 issue of RM.

See Societies & Clubs for further details including opening times, admission prices and other layouts due to attend.

More Southern wagons from Rapido in OO and N



Rapido Trains UK is to add to its range of Southern Railway wagons in both OO and N gauge.

In 4mm scale, the firm has revealed plans for a Southern Railway eight-plank open vehicle, which was the SR's most numerous wagon. Rapido's tooling will cater for the initial Diagram 1379 design with 9' wheelbase – of which 7,950 were built between 1926 and 1933 – and later examples built from 1935-37 with a 10' wheelbase. Detail differences such as disc or split-spoked wheels, and Morton or 'Freighter' brake gear, will also be recreated.

Having been used all over the SR system, including the Isle of Wight, the wagons will make a suitable combination with many locomotives, particularly Rapido's forthcoming LBSCR E1 0-6-OT.

Planned liveries are SR brown (both pre- and post-1936), BR grey, SR brown with BR lettering (all with different running numbers) and Motive Power Department black. Engineering prototypes have been produced and delivery is due in early 2023, with a price of £32.95.

▲ Engineering prototypes of Rapido's planned Southern Railway eight-plank wagons for OO gauge.

In response to requests from N gauge modellers, Rapido is also producing four of its recent OO gauge South Eastern & Chatham Railway wagons in 2mm scale: the Diagram 1349 five-plank open, Diagram 1355 seven-plank open, Diagram 1426 10-ton van and the six-wheel dual-verandah brake van. Liveries will be SECR, SR brown (pre- and post-1936) and BR grey.

They will be offered in 'Freight Train Packs' containing one of each vehicle, while the five- and seven-plank wagons, and the 10T van, will also be available in triple packs – each model with a unique running number – for modellers to increase the length of the train. If these are successful, Rapido will consider offering other wagons in packs, such as its forthcoming GWR iron Mink, four-plank open and Diagram AA20 Toad brake van.

Delivery is expected early next year,



▲ A CAD render for the 'Freight Train Pack' of four SECR wagons in N gauge.



▲ Engineering prototypes of the Titfield Thunderbolt set: Liverpool & Manchester Railway 0-4-2 Lion, the GWR Lorient Y wagon carrying Dan's coach, and the GWR AA20 Toad brake van.

with the 'Freight Train Packs' retailing at £99.95 and the triple packs at £69.95.

Retailer Train Times of Eastbourne is also selling an exclusive limited edition of the OO gauge Rapido brake van, in SECR grey as No.2010 – the sole surviving example, preserved at the Kent & East Sussex Railway. Limited to 100 models and priced at £39.95, this can be ordered from:

www.traintimesmodelshop.co.uk

Titfield train and bus

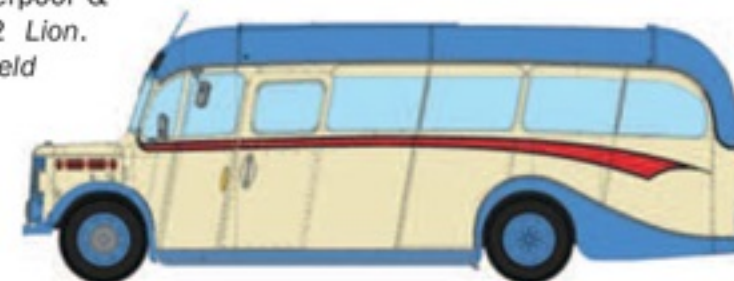
Rapido has also received engineering prototypes of its BR OAA wagon, as well as the GWR AA20 Toad and Lorient Y well wagon – the latter two allowing it to assemble a Titfield Thunderbolt train for the first time with Liverpool & Manchester Railway 0-4-2 Lion. The Lorient Y in the Titfield

Thunderbolt train packs will include Dan's coach, as seen in the 1952 film, with detailed interior.

Another part of Rapido's forthcoming Titfield Thunderbolt range is now in tooling and available to order – the 1:76 scale Bedford OB bus in Pearce & Crump livery. It is one of ten OB models being produced, with other liveries including Western National, Southern Vectis, BR crimson and cream, and Edinburgh Corporation.

The OB will retail at £44.95 and is expected in late 2022 or early 2023. More details can be found and orders placed at:

www.rapidotrains.co.uk



Pre-production artwork of Rapido's Bedford OB bus in Pearce & Crump livery from The Titfield Thunderbolt.



▲ Engineering prototype of the BR OAA wagon for OO.

Gauge O Guild website gets an overhaul

The Gauge O Guild has restructured its website at www.gaugeoguild.com to make it easier for visitors to navigate.

First created in 2003, the site has grown steadily since and now contains a great deal of information for O gauge modellers, but this had led to a complex structure. With Julian Best now leading the web team, the site has been reconstructed with fewer pages, a new design and a new home page, providing users with one-click access to the most frequently visited pages.

Non-members can take up a free introductory offer for one month, during which they can log into the full website with access to the members-only areas, and find out more about the benefits of membership. This includes access to the O gauge modelling 'Knowledge Base', the latest issue of

the Guild's Gazette and its archives, and a forum of modelling topics.

Jackie Kneeshaw, the Guild's Chairman of Events and Vice Chairman of the Management Committee, said: "O gauge is becoming more and more popular so the website might just have the information you are looking for."

"The Gauge O Guild one-day Summer Show in Doncaster on 11 June was another successful show with over 750 attendees and 10 new members signing up. You can now book advanced tickets for the big Guildex Event on 3 & 4 September by clicking on the advert which is on the front page of the website."

▲ A view of the Gauge O Guild's Summer Show in Doncaster on 11 June. Photo: Tony Andrews



New owners for Brynkits

Brian Golding, proprietor of Brynkits Models, has retired and sold his range of model kits and components to new owners.

His son Rob Golding has taken on the narrow gauge rolling stock, including Lynton & Barnstaple Railway coaches and other items in the pipeline, together with 3mm scale road vehicles. These will continue under the Brynkits brand

and can be found online at www.bibkits.co.uk

The 3mm scale range of standard gauge locomotive and rolling stock kits is now in the hands of the 3mm Society (www.3mmsociety.org.uk) while the 4mm scale model bus components have passed to:

A N Goodwin, Paragon Kits, 206 St James' Park Road, Northampton, NN5 5EU

Shop News

New premises for Collectables R Us

Model shop Collectables R Us has moved to new premises in High Wycombe.

Set up by Darryl and Carole Earle, the shop originally opened in a shopping centre in High Wycombe at the beginning of 2020, expanding into the model railway and plastic kit market after many years of dealing in die-cast models.

Since opening, and despite the coronavirus lockdowns, the business has proved very successful, initially moving into a larger unit in the first centre, before the latest move to the new premises in a more mainstream shopping centre.

The shop stocks Peco, Hornby, Bachmann, Wills, Modelscene, Gaugemaster and Oxford Diecast, and features an extensive and evolving display layout. Its new address is:



▲ The display layout at Collectables R Us.

**Collectables R Us, Eden Shopping Centre,
Unit 92, Union Parade, High Wycombe HP11 2BQ
Tel: 01494 440598
www.collectablesrus.co.uk**

Fourdees introduces new range of kits for 009

Fourdees Limited, already known for its ready-to-run 009 models (see Talking Points, p540-541), has moved into kit production.

All-new models are ref.41-181K freelance Peckett & Sons 0-4-0ST Jubilee (£57.49) and ref.41-253K Ffestiniog Railway diesel Harlech Castle in pre-2018 condition (£49.99). Based on existing R-T-R models in the Fourdees range are: ref.41-202K Glyn Valley Tramway Sir Theodore in c.1920 condition (£64.99); ref.41-205K Glyn Valley Tramway Glyn in c.1892 condition (£59.99); ref.41-121K freelance Sharp, Stewart

Clogher (£49.99); ref.41-146K freelance Hunslet tram Howrah (£69.99); and ref.41-131K freelance Hunslet 0-6-2T Leeds No.1 (£74.99).

Ref.41-205K Glyn (pictured) is designed to fit on a Kato ref.11-109 four-wheel chassis, and includes a fully detailed cab interior and parts needed to model the locomotive in original condition, such as the condensing apparatus and speed governor. Also supplied are resin windows, NEM pockets and Peco GR-102 couplings, painted etched name and works plates, and duplicate spare parts to allow for any breakages.



The quoted prices include UK post and packing, and the kits can be ordered from:

**Fourdees Limited
57, Wentworth Road, Blacker Hill,
Barnsley,
S74 0RP.
www.fourdees.co.uk**

New figures from Modelu and crowd-funding system planned

Modelu has added several shed staff to its range of figures, and is planning to launch a new crowd-funding system for future products.

Available in several scales, the figures depict railway staff such as cleaners, fitters and firelighters in various poses, including eating and drinking. Also new to the range are a pair of Border Colliers, one sitting and one standing.

The planned crowdfunding system is titled 'Steam Raiser', as Modelu's Chris Schiller explained: "What we are planning is a kickstarter-style system whereby we launch a number of prospective projects, and interested

customers can pre-order models in their chosen scale.

"Once we reach a target number of pre-orders, we can then light a fire under the project and commission our digital sculptor to get to work. This gives us an exciting opportunity to design and produce exactly what people need, especially where an alternative product doesn't already exist."

**Tel: 07887 803 737
www.modelu3d.co.uk**



▲ Modelu shed staff set to work cleaning GWR King No.6009 King Charles II.

Coming next month

**Sept. 2022 –
Out Thursday 11 Aug.**

Monksbury
A London & South Western
branch terminus in O



**Railway
of the
Month**

Mynydd Tatws
Welsh narrow gauge in 009



Mickelthwaite
An LMS makeover for a loft-based
railway empire in 00



inside ...

• **RETURN TO ALEXTON**
Shades of Dillicar with the rebuilding of this garage-based system in 00

• **ASHBY MAGNA**
A BR steam era depiction of the Great Central main line in N gauge

• **LMS UTILITY VAN UPGRADE**
Ian Nuttall describes how he gave a venerable 00 model a new lease of life

**plus all the
regular features**

Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling to a listed live event should contact the organiser first. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and notifications via email to clubs@railwaymodeller.co.uk are required six weeks prior to publication date. Entries must include the organiser's email or website and/or a telephone number.

OCTOBER 2022 issue deadline: August 1st

LIVE EVENTS

We recommend that visitors check with organisers before travelling.

Saturday 16 July

FILEY, North Yorkshire

Venue: St John's Church, West Avenue, Filey YO14 9AU.

Open: 1000-1630

Admission: By donation – children free.

Features: Filey Model Railway Show.

Contact: Chris Martin 01723 870166

Saturday 16 July

MILTON KEYNES, Buckinghamshire

Organiser: Milton Keynes MRS.

Venue: The Ridgeway Centre, Featherstone Road, Wolverton Mill South, Milton Keynes MK12 5TH.

Open: 1000-1630

Admission: Adults £5.00, children (5-16) £1.00, under 5s free.

Amenities: Free parking, refreshments. 25-30-minute walk or five-minute taxi ride from Wolverton station; closest bus route is Arriva No.4 from central Milton Keynes.

Features: Layouts in all major scales and trade stands.

Email: secretary@mkMrs.co.uk

Website: www.mkMrs.org.uk

Sunday 17 July

GAINSBOROUGH, Lincolnshire

Organiser: Gainsborough MRS.

Venue: Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.

Open: 1330-1730 (last admission 1630)

Admission: Adults £4.00, children £3.00, seniors £3.00, family (2+2) £10.00.

Features: Open day with 0 gauge East Coast Main Line layout in operation.

Contact: Barry Hodgkinson 01427 610475.

Email: gmrscntact@gmail.com

Website: www.gainsboroughmodelrailway.co.uk

Saturday 23 July

BAKEWELL, Derbyshire

Organiser: Gauge 1 North, Yorkshire Group of the Gauge 1 Model Railway Association.

Venue: Bakewell Agricultural Business Centre, Agricultural Way, Bakewell, DE45 1AH.

Open: 1000-1630

Admission: Adults £7.00, accompanied children under 16 free.

Amenities: Pay and display car park, cafe on site, five-minute walk to town centre.

Features: *Ridings* (Gauge 1 four-track scenic main line, steam and battery electric, with shunting for younger visitors), *Ruddlemoor Wharf* (Gauge 1 in a small space), demonstrations of scratch-building, pattern-making and miniature buildings.

Contact: 01246 590259

Website: gauge1north.org.uk

Saturday 23 July

BATH, Somerset

Organisers: Simon Castens and Jerry Clifford.

Venue: New Oriel Hall, Larkhall, Bath BA1 6RA.

Open: 1030-1630

Admission: £7.00, accompanied children free.

Amenities: Kim's Kitchen serving cakes.

Features: *Black Rock* (Gauge 1), *Upwatch Works* (7mm), *Peaseverm Yard* (7mm), *Bethesda Sidings* (OO), *Moredon South* (OO), *Shelfie 2* (OO), *Woodside* (OO) *Ovjsko Stari* (0 gauge Croatian), *Foxbury* (3mm), *Heybridge Wharf* (3mm) *Pitt Hill* (finescale 2mm), *Wisteria Collop* (OO9), demonstrations, trade stands.

Contact: 01225 462332.

Saturday 23 July

EASTLEIGH, Hampshire

Organiser: Southampton MRS.

Venue: Barton Peveril College, Chestnut Avenue, Eastleigh, Hampshire, SO50 5ZA (for satnav use SO50 5BX); around 30 minutes' walk from Eastleigh and Southampton Airport Parkway stations. By bus use Bluestar Route 2 and alight at Tennyson Road.

Open: 1000-1700

Admission: Adults £5.00, accompanied children free.

Amenities: Free parking, disabled access to all areas, hot and cold refreshments.

Features: At least 15 layouts, 10 trade stands and two demonstration stands.

Contact: David Barker 01489 799809 most evenings.

Website: www.southamptonmodelrailwaysociety.wordpress.com

Saturday 23 July

HYTHE, Hampshire

Organiser: Romsey & District Railway Modellers Society.

Venue: Hythe & Dibden Community Centre, Brinton Lane, Hythe, Southampton, SO45 6DU.

Open: 1030-1630

Admission: Free, but donations welcomed.

Amenities: Paid parking, refreshments. Venue is 250 yards from pier if arriving by ferry from Southampton.

Features: Layouts: *St Izzy* (OO), *Minington Central Tramway* (OO), *Woodley Castle* (OO, Thomas-themed), *Hirson Bellevue* (HO9), *Ashfield* (N), *Besses o' th' Barn Bridge* (OO), London Transport bus depot diorama (OO), sales stands. Part of centenary celebrations of Hythe Pier railway on July 23/24, organised by Hythe Pier Heritage Association.

Contact: Alan Titheridge or Allan Fairhead of Hythe Pier Heritage Association (alan.titheridge@hythepierha.org.uk or allanf_8@msn.com) or Howard Coulson of Romsey Modellers (eandhcoulson@googlemail.com)

Website: www.hythepierha.org.uk

Saturday 23 July

LEICESTER

Organiser: Train Collectors Society.

Venue: Trinity Methodist Church Hall, Narborough Road, Leicester, LE3 2RD.

Open: 1030-1700

Admission: Adults £3.00, accompanied under-16s free.

Amenities: 3 miles from M1 Jcn 21, public transport from city centre. Free street parking, cafeteria with hot and cold food.

Features: Around 10 vintage toy train displays in various gauges, around 10 traders, club 'bring and buy'.

Contact: Bryan Pentland 07801 818764

Email: bryanpentland@hotmail.co.uk

Website: www.traincollectors.co.uk

Saturday 23 July

MANSFIELD, Nottinghamshire

Organiser: East Midlands Narrow Gauge Group.

Venue: Portland College, Nottingham Road, Mansfield, NG18 4TJ. Follow signs for the visitors' car park, and Newstart Coffee Shop.

Open: 1030-1630

Admission: £5.00 (cash only, on the door), free entry to accompanied under 16s.

Amenities: Free parking, refreshments.

Features: 009 Society show: over 15 layouts (mainly 009), trade support and 009 Society sales stand.

Contact: Hugh Milward 07772 242135

Email: h.milward@btinternet.com

Saturday 23 July

SCARBOROUGH, North Yorkshire

Venue: Newby & Scalby Community Hall, Scalby Road, Scarborough YO13 0RA.

Open: 1000-1700

Admission: £5.00, children free.

Features: Scarborough Model Railway Show.

Contact: Chris Martin 01723 870166

Saturday 23 & Sunday 24 July

CUPAR, Fife

Organiser: Cupar & District MRC.

Venue: The Corn Exchange, St Catherine Street, Cupar, KY15 4BT.

Open: 1000-1630 both days

Admission: Adults £5.00, accompanied children (5-15) £1.00, under 5s free.

Amenities: Wheelchair accessible, light refreshments sold in aid of MacMillan Cancer Support.

Features: *Glen Tay* (OO), *Wenstead* (OO), *United Mills* (P4), *Loch Doune* (OO), *Jack Hills Loading Point 3* (OO), *Canal Way* (N), *Dalby Road* (OO), *Ashton Steel* (OO), *Crianlarich* (N), *Balbirnie Mills* (OO), *Inverbervie* (N), *Hill Park TMD* (O), *Old Blarney* (Irish OO), *St Adrian* (Swiss Z), *Shelf* (N), *Lego Railway*, demonstrations, societies, trade stands. Exhibit to mark 175 years of the railway in Cupar.

Website: www.cuparmrc.co.uk

Saturday 23 & Sunday 24 July

EDINGTON, Somerset

Organiser: Somerset & Dorset Railway Trust.

Venue: Edington Village Hall, Lippets Way, Edington TA7 9HA.

Open: Saturday 1000-1700

Sunday 1000-1630

Admission: Adults £5.00, seniors £4.50, accompanied children free.

Amenities: Free parking, level site, refreshments available.

Features: S&DRT 47th Model Railway Exhibition: over 10 layouts in various scales, Trust information and sales, trade stands.

Website: www.sdrtrg.org/edington-model-railway-exhibition

Saturday 23 & Sunday 24 July

GOATLAND, North Yorkshire

Organiser: Whitby & District MRC.

Venue: Goatland Village Hall, YO22 5AL.

Open: 1000-1600

Admission: £2.00, accompanied children free.

Amenities: Free parking, refreshments.

Features: Seven layouts, tombola.

Facebook: www.facebook.com/WhitbyMRC/

Saturday 23 & Sunday 24 July

LLANDUDNO, North Wales

Organiser: Colwyn MRC.

Venue: Craig Y Don Community Centre, Queens Road, Craig Y Don, Llandudno LL30 2YR.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: £5.00, children under 16 free if accompanied by an adult.

Amenities: Free parking; on or near bus routes; disabled access and facilities; refreshments.

Features: Layouts: *Neston Colliery Plateway* (16mm/ft), *Trix Twin Railway 1935-1965* (OO), *Tan Y Bryn* (OO), *Parkend* (OO & OO9), *North Winterton* (OO9), *Deganwy* (OO), *Talywain Loco Sheds* (OO), *Pentrefelin* (TT),

Huxley Junction (O), *Millhouses* (N), local bus diorama, *Mochdre Sheds* (OO), *Pula* (O9), *Onabujit Halt* (N).

Contact: John Farrell, Exhibition Co-ordinator, 07714 237248

Email: info@colwynmodelrailwayclub.co.uk

Website: www.colwynmodelrailwayclub.co.uk

Facebook: facebook.com/Colwynmrc/

Saturday 23 & Sunday 24 July

SUTTON COLDFIELD, West Midlands

DEMU Showcase 2022: The National Exhibition of Diesel & Electric Railway Modelling

Organiser: Diesel & Electric Modellers United.

Venue: Sutton Coldfield Town Hall, Upper Clifton Rd, Sutton Coldfield B73 6DA.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Adults £11.00, children (under 16) and DEMU members £8.00, family (2 adults and unlimited children) £27.00.

Amenities: Parking nearby, two-minute walk from Sutton Coldfield station.

Features: Layouts including *Burnrold Works* (OO), *Clackmannan Goods* (P4), *Euxton Junction* (OO), *Hinksey Yard* (N), *Oak Road* (OO), *Spott* (P4), *Towcester* (OO), *Wellpark* (OO), Simon Bendall demonstration 'Modelling British Railways', trade stands.

Website: www.demu.org.uk

Saturday 23 July to Sunday 11

September

CHESTER, Cheshire

Organiser: Chester Cathedral.

Venue: Chester Cathedral, St Werburgh Street, Chester, Cheshire CH1 2DY.

Open: 1000-1700 daily

Admission: £2.50 (under-5s free).

Amenities: Public car park at Delamere St (seven minutes' walk), disabled access, refreshments.

Features: 'Making Tracks 2' – brand-new OO gauge layout built by Pete Waterman and his team of model-makers.

Contact: 01244 500959

Website: www.chestercathedral.com

Sunday 24 July

WETHERINGSETT, Suffolk

Organiser: Norfolk & Suffolk Narrow Gauge Modellers/Mid-Suffolk Light Railway.

Venue: Mid-Suffolk Light Railway, Brockford Station, Wetheringsett, Stowmarket, Suffolk, IP14 5PW (just off the A140 Ipswich to Norwich road – look for the Mendlesham TV mast and follow the brown signs).

Open: 1100-1630

Admission: Adults £10.00, children (5-15) £5.00, under 5s free, family £25.00 (including entrance to railway and train rides).

Amenities: Free parking, disabled access, refreshments.

Features: Members' layouts and displays from OO9 to 16mm, Great Eastern Models.

Contact: Richard Doe 01502 471760

Websites: www.nsnm.org.uk and www.mslr.org.uk

Wednesday 27 July & every

Wednesday in August

BISHOPS LYDEARD, Somerset

Organiser: Taunton Model Railway Group.

Venue: Platform 1, Bishops Lydeard station, West Somerset Railway, Taunton TA4 3BX.

Open: 1000-1630

Admission: Small charge.

Features: *Bath Green Park*, view progress on construction of new layout *Tamerig New*.

Website: www.tauntonmodelrailwaygroup.co.uk

Saturday 30 July

BARNSTAPLE, Devon

Organiser: Barnstaple MRC.

Venue: Christ Church, Bear Street, Barnstaple EX32 7BU.

Open: 1000-1600

Amenities: Six minutes' walk from Barnstaple station, parking, disabled access, refreshments.

Features: Layouts including *Nosach Green* (O), *Lambourn* (4mm), *Robinson Heath* (N), *Mill St Shed* (4mm DCC), *Wheel Moor China*

Clay (1970s 4mm), *Shunting Puzzle*, *Young's Model Shop* (N), *Moorbath Holt* (4mm), *Franwood TMD* (4mm DCC), *Ashmouth*, associations, trade stands.

Website: www.barnstaplemrc.weebly.com

Saturday 30 July

BRIDPORT, Dorset

Organiser: Bridport & District MRC.

Venue: The Sir John Colfox Academy, Ridgeway, Bridport, Dorset DT6 3DT.

Open: 1000-1600

Admission: Adults £6.00, children (under 16) free, family (2+2) £10.00.

Amenities: Free parking, full disabled access, light refreshments all day.

Features: 34 layouts, demonstrations and trade stands.

Contact: Bob Ahrens 01308 861754 or 07927 312341.

Email: R_Ahrens@outlook.com

Saturday 30 July

HOUNSLOW, West London

Organiser: Twickenham & District MRC.

Venue: Kerswell Hall, Wills Crescent, Hounslow, TW3 2JF.

Open: 1000-1600

Admission: Donations welcomed.

Amenities: Refreshments.

Features: Open day with club layouts *Kensington Addison Road* (7mm), *Batty Moor* (N), *Eastgate* (OO) and *Northwick* (OO), OO and G layouts under construction, weathering demonstration, sales stand.

Contact: 01932 783253

Website: www.tdmrc.org.uk

Facebook: www.facebook.com/TwickenhamMRC/

Saturday 30 & Sunday 31 July

NEW MILTON, Hampshire

Organiser: South Coast MRC.

Venue: The Arnewood School, Gore Road, New Milton, Hants, BH25 6RS.

Open: 1000-1600 both days

Admission: £7.00, free for accompanied under-14s.

Amenities: Free parking, full disabled access, light refreshments.

Features: 14 layouts, trade support.

Contact: Steve Mosedale 07984 454787

Email: trainspotta@hotmail.co.uk

Website: www.southcoastmodelrailwayclub.com

Sunday 31 July

KEIGHLEY, West Yorkshire

Organiser: Aire Valley Railway Modellers Club.

Venue: Melbourne House, Dalton Lane, Keighley, West Yorkshire BD21 4LG.

Open: 1000-1600

Admission: Free, but donations welcome.

Features: Open day.

Facebook: www.facebook.com/airevalleymodels/

Sunday 31 July

THIRSK, North Yorkshire

Organiser: Thirsk Model Railway Show Committee.

Venue: Thirsk Town Hall, Westgate YO7 1QR.

Open: 1000-1600

Admission: Adults and concessions £5.00, accompanied children free.

Amenities: Refreshments.

Features: Over 10 layouts, demonstrations, trade stands.

Website: www.expo-thirsk.co.uk

Saturday 6 August

MINEHEAD, Somerset

Organiser: Exmoor Coast Railway Modellers.

Venue: West Somerset College, Bircham Road, Minehead, Somerset, TA24 6AY (on the main A39 road into the town).

Open: 1000-1630

Admission: Adults £6.00, children £2.00, family (2+2) £14.00 (including show guide).

Amenities: Free minibus from Minehead station; FirstBus service 28 from Taunton runs past the college. Free parking, disabled access, hot and cold refreshments.

Features: Exmoor-Rail 10th anniversary exhibition, featuring approximately 16 lay-

outs including *Abbots Morton* (4mm), *Bleat Wharf* (4mm), *Bradford-on-Tone* (4mm), *Cheffley* (4mm), *Dunball* (4mm), *Fry's Somerdale* (4mm), *Gravelly Oak* (2mm), *Lananta Quay* (3mm), *Otterhampton* (2mm), *Rannoch* (2mm), *Shwt* (EM), *Templegate Wagon Works* (7mm), *Watershoot Bay* (4mm), *Woodbury Wharf* (OO).

Demonstrations, societies, trade support.

Contact: 01643 706736 (in advance), 07811 063815 (during the show)

Email: nelhams66@yahoo.com

Saturday 6 & Sunday 7 August

BREDGAR, Kent

Organiser: Bredgar & Wormshill Light Railway and Invicta Model Rail.

Venue: Bredgar and Wormshill Light Railway, The Warren, Swanton Street, Bredgar, Sittingbourne, Kent ME9 8AT.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Entry to railway: Adults £20.00, children (4-16) £10.00, under 4s free, family (2+3) £50.00.

Amenities: Free parking, disabled access, refreshments (tea room and BBQ), shop.

Features: Model railway show, train and tractor rides all day.

Contact: 01622 884254

Website: www.bwlr.co.uk

Facebook: www.facebook.com/groups/bredgarrailway/

Saturday 6 & Sunday 7 August

HADLOW DOWN, East Sussex

Organiser: Tinkers Park.

Venue: Tinkers Park, A272, Hadlow Down, East Sussex TN22 4HS.

Open: 1000-1700

Admission: Adults £7.00, under 16s £2.00, under 5s free.

Amenities: Connecting free bus service, free parking, refreshments, real ale bar.

Features: Model railway exhibition, sales stands, narrow gauge and miniature railways, traction engine trailer rides, classic cars, bus rally (Sunday only), fair organ museum.

Website: www.tinkerspark.com

Saturday 6 & Sunday 7 August

LEYLAND, Lancashire

Organiser: Leyland Model Railway Club.

Venue: Leyland Civic Centre, West Paddock, Leyland, Lancashire PR25 1DH.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Adults £6.00, children £3.50, family £13.50.

Amenities: Free parking, disabled access, refreshments.

Features: *Gregorys Fold*, *Middleton Road*, *Museum of Transport*, *Rock N Rail*, *Shawbrook TMD*, *Thomas Themed* (all OO), *Earby*, *Heatherley*, *Hogwarts*, *Lyme Brooke Yard*, *Village in Snow* (all N), *Tan-yr-Allt* (OO), *Hakuho* (Z), *Arcadia Pennsylvania* (O), *California Dreamland* (1:87), *Burnaby* (HO), demonstrations, trade stands.

Contact: David White 07796 130295

Saturday 6 & Sunday 7 August

REDCAR, Cleveland

Organiser: Cleveland Model Railway Club.

Venue: Redcar and Cleveland College, Corporation Road, Redcar, Cleveland, TS10 1EZ.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Adults £5.00, children £3.50, family (2+2) £15.00.

Amenities: 10 minutes' walk from Redcar station; Arriva buses 62, 64 and 81. Free parking, disabled access, refreshments.

Features: Redcar Model Rail 2022: over 12 layouts in various scales and gauges, demonstrations, displays, club sales, trade stands, charity and tombola.

Website: www.clevelandmrc.club

Saturday 13 August

BEXHILL-ON-SEA, East Sussex

Organiser: Bexhill Model Railway Club.

Venue: St Richard's Catholic College,



▲ **Class 3F No.43620 on Abbots Morton**, which featured in RM July and is set to appear at Exmoor-Rail in Minehead on 6 August. **Photo:** Ian Manderson

Ashdown Road, Bexhill-on-Sea, East Sussex TN40 1SE.

Open: 1000-1700

Admission: Adults £6.00, children under 14 £2.00, family (2+2) £14.00.

Amenities: Free parking, refreshments.

Features: Layouts, sales stands.

Website: www.bexhillmrc.co.uk

Saturday 13 August

ROADE, Northampton

Organiser: Northampton & District MRC (sponsored by Bachmann Europe plc).

Venue: Roade Village Hall, Bailey Brooks Road, Roade, Northampton NN7 2LS.

Open: 1000-1600

Admission: Adults £5.00, children £3.00, family (2+2) £10.00.

Amenities: On bus routes from Northampton to Milton Keynes (X6/33), free parking, disabled access.

Features: Layouts including Channel 5 winner of *The Great Model Railway Challenge* (*Terra Profugus*). Trade support, Bachmann display and Collectors Club stand.

Contact: Secretary Richard Deacon 01604 890275

Website: www.nmrc.org

Saturday 13 & Sunday 14 August

ST ANDREWS, Fife

Organiser: East Neuk Model Railway Club.

Venue: Students Union, 22 St Marys Place, St Andrews, Fife KY16 9UZ.

Open: Saturday 1000-1730

Sunday 1030-1700

Admission: Adults £5.00, children £2.50, family (2+2) £12.00.

Features: Model railway exhibition.

Website: www.eastneukmrc.co.uk

Facebook: www.facebook.com/eastneukmrc/

Saturday 13 & Sunday 14 August

WELLS, Somerset

Organiser: Wells Railway Fraternity.

Venue: The Town Hall, Market Place, Wells, Somerset BA5 2RB.

Open: Saturday 1030-1730

Sunday 1000-1630

Admission: Adults £8.00, accompanied under-14s free, wheelchair users and children up to 17 £4.00.

Amenities: Parking £4.00 all day (follow Railwells Parking signs).

Features: *Flintcombe*, *Wantage*, *Yard No.2*, *Ventnor Town* (all P4), *Edge Hill* (EM), *Ventnor West*, *Arun Quay*, *Newton Heath Works*, *Hobson's Brewery* (all O), *Heybridge Wharf* (3mm) *South Walton* (N), *Bath Midland Shed* (2mm), *Modbury* (2mm), *Sefton Park* (¾mm), demonstrations, displays and trade stands.

Contact: Chris Challis 01749 938362

Website: www.railwells.com

Saturday 20 August

BEER, Devon

Organiser: Pecorama.

Venue: Gallery Lecture Theatre, Pecorama, Underleys, Beer, Devon EX12 3NA.

Open: 1000-1600

Admission: Included in Pecorama entry prices (adults £12.95, children 4-13 £10.95, under 4s and adults over 80 free).

Amenities: Parking, refreshments, disabled access (hilly site, unsuitable for three-wheel mobility scooters).

Features: Visitors are invited to operate Swiss HOm layout *The Andeer Line* (full instructions will be given).

Website: www.pecorama.co.uk

Saturday 20 August

MILTON KEYNES

Organiser: Silverfox DCC MRC.

Venue: Oakgrove School, Venturer Gate, Milton Keynes MK10 9JQ.

Open: 1000-1600

Admission: Adults £6.00, children (5-15) £3.00, under 5s free.

Amenities: Buses from Milton Keynes station to Oakgrove Brickhill Street, free parking, wheelchair access, refreshments.

Features: Over 20 layouts, trade support, children's Thomas layout.

Website: www.silverfoxdcc.co.uk

Facebook: www.facebook.com/SilverfoxDCC/

Saturday 20 August

SKIPTON, North Yorkshire

Organiser: Ellis Clark Trains.

Venue: Ellis Clark Trains, Toller Court, off Shortbank Road, Skipton BD23 2HG.

Open: 1000-1600

Admission: Free – please send RSVP to amey@ellisclarktrains.com

Amenities: Free parking at the Bailey Car Park, Skipton BD23 1UA, with free vintage bus to venue (or five-minute walk). Limited free disabled parking opposite premises in the old Cross Keys pub car park. Hog roast from 12.30pm (with vegetarian option) and complimentary drinks and cakes all day.

Features: Multiple layouts, demonstrations and trade stands, latest samples of O gauge Wickham trollies and 'Black Fives', new and pre-owned sales, raffle.

Contact: 01756 701451

Email: amey@ellisclarktrains.com

Website: www.ellisclarktrains.co.uk

Saturday 20 & Sunday 21 August

CARDIFF

Organisers: Cardiff Model Engineering Society, Cardiff 4mm group and North Gwent Railway Modellers.

Venue: Ice Arena Wales (Cardiff Ice Rink), Olympian Drive, Cardiff CF11 0JS.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: £8.00, under 12s free.

Amenities: Free vintage bus to and from Cardiff Central station (Saturday only), free parking, hot and cold refreshments.

Features: *Bath Loco Shed* (finescale N), *Bicsdale*, *Broadwell*, *Cwm Castell* (all OO),



▲ Peckett 0-4-OST No.2 on Copper, Wort & Co. Featured in RM April, this layout is due to attend the Pickering show on 20 & 21 August. Photo: Steve Flint

Fford-y-Deryn (16mm), *Kleine Albula* (H0m), *Llandafal* (OO), *Llawryglyn* (EM), *Margam Moor* (OO), *Midsomer Mardy* (Z), *Mostyn* (P4), *Pencader* (EM), *Plaxtol Road* (O), *Rydens Green Wharf* (OO9), *Sheep Dip* (OO), *Trevanna Dries* (EM), *Washery Halt* (O), *Canadian HO*, demonstrations, trade stands. Over £800 worth of prizes to be won (see website for details).

Contact: 029 2025 5000 opt 3 (0900 to 1700 Monday to Friday)

Email: enquiries@cardiffmodelrailwayshow.co.uk

Website: www.cardiffmodelrailwayshow.co.uk

Facebook: www.facebook.com/cardiffmodelshow

Saturday 20 & Sunday 21 August CARNFORTH, Lancashire

Organiser: Carnforth Station Heritage Centre.
Venue: Carnforth Station Heritage Centre, Warton Road, Carnforth, Lancs LA5 9TR.

Open: 1000-1600 both days.

Admission: By donation – adults £2.50, accompanied children under 16 free.

Amenities: Trains from Leeds, Manchester, Preston and Barrow, buses from Lancaster and Kendal. Car park nearby, Carnforth Models and Brief Encounter Bistro & Bar.

Features: At least eight layouts, trade support.

Contact: 07967 743126

Saturday 20 & Sunday 21 August CRICH, Derbyshire

Organiser: John Huddleston.

Venue: National Tramway Museum, Crich Tramway Village, Crich, Derbyshire, DE4 5DP.

Open: 1000-1730 (last admissions 1600)

Admission: Included in museum entry: adults £20.00, seniors (over 60) £17.50, children (4-15) £12.00, family (2+3 or 1+4) £44.00. For disabled/less-abled visitors: adults/seniors £13.50, children £9.00.

Amenities: Free parking, disabled access, refreshments.

Features: 10 layouts including *Whiteleaf Tramway* (G), *Cronenberg – Brawa/Eheim Trolleybuses*, *Camwell's View*, *Birmingham and model building demonstration*, *Upson Down Tramway Museum*, *Blackpool Trams* (2mm), *Falcon Travel*, *Fleetwood* (2mm), large scale vintage railway layout and Robert Whetstone layout, plus trade stands.

Contact: 01773 854321

Website: www.tramway.co.uk

Saturday 20 & Sunday 21 August LOUGHBOROUGH, Leicestershire

Organiser: Soar Valley Model Railway Club.

Venue: Loughborough Grammar School, Burton Walks, off Leicester Road, Loughborough LE11 2AQ.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Adults £8.00, children £4.00, family £20.00.

Amenities: 17 minutes' walk from Loughborough station; buses from Derby, Nottingham, Leicester, Coalville and Shepshed. Parking, disabled access, refreshments.

Features: *Brookfield*, *Fredstone Lane*, *James Street*, *A Power Station* (all N), *Uppingham* (3mm), *Purgatory Peak* (On30), *Aylett End*, *Blaendare Road*, *Britannia Mill*, *Dore Hill St Stevens*, *Dilhorne Colliery*, *Franton Mill*, *Holland Beck*, *Kettle Lane MPD*, *No Place*, *Pedwardine Forest*, *Pencoed*, *Penna Lane TMD*, *Southgates DMU*, *Tremawm* (all OO), *Harton Gill* (P4), *Neuburg* (HO), vintage O gauge, LGB G scale, demonstrations, trade stands.

Contact: Exhibition Manager Rob Howker 07761 973725.

Email: moggythetrucker@live.co.uk

Website: www.svmrc.co.uk

Saturday 20 & Sunday 21 August PICKERING, North Yorkshire

Organiser: Scarborough & District Railway Modellers.

Venue: Memorial Hall, 34 Potter Hill, Pickering, YO18 8AA.

Open: Saturday 1000-1700

Sunday 1000-1530

Admission: Adults £6.00, accompanied children free.

Amenities: Arriva bus X128 from Scarborough, Coastliner 840 from Malton and North Yorkshire Moors Railway from Whitby. Lift to all floors, hot and cold food.

Features: *Blythburgh* (1:48), *Copper Wort* (OO), *Shirebrook* (N), *Distillery Yard* (OO), *Pawson's Prize Pickles* (O), *Habbaniya* (forced perspective), *Sparkel* (HO), *Low Fell* (N), *Summers Mill* (OO9), *Thomas for Kids* (OO), demonstrations, trade stands, raffle.

Contact: 07897 595344

Email: Roland@sdrmwweb.co.uk

Website: http://sdrmwweb.co.uk/Pickering_Show.php

Saturday 20 & Sunday 21 August WAKEFIELD, West Yorkshire

Organiser: EM Gauge Society.

Venue: Kettlethorpe High School, Standbridge Lane, Sandal, Wakefield, WF2 7EL.

Open: Saturday 1030-1700

Sunday 1000-1600

Admission: 2-day ticket £10.00 (EM Gauge Society members £8.00).

Features: *Bawdsey* (EM), *Crioch* (EM), *Culm* (EM), *Eccleston* (S4), *Great Swilling* (EM), *Llanastr* (S4), *North Ballachulish* (EM), *The Depots - Rosedale East* (EM), EMGS test track for OO, EM and S4 stock, Chairman's Challenge competition entrant *Swanish*, guest layout, *Cameo Callaton* (2FS), demonstrations, societies, trade stands.

Website: www.emgs.org

Sunday 21 August

LENHAM, Kent

Organiser: The French Railways Society.

Venue: Lenham Community Centre, Groom Way, Lenham, Kent, ME17 2QT (just off the old A20; nearest station is Lenham).

Open: 1000-1630

Admission: Adults £5.00, under 16s free.

Amenities: Parking, disabled access (not to stage), refreshments.

Features: At least 10 layouts, mainly French outline, in O, HO, N and Z scales; stands including other European societies; presentations including author Andrew Martin.

Email: roger.allen1@btinternet.com

Website: www.frenchrailwaysociety.org

Saturday 27 August LONGBRIDGE, Birmingham

Organiser: Bournville Model Railway Club.

Venue: The Austin Sports and Social Club, 30 Tessall Lane, Longbridge, Birmingham B31 2SF.

Open: 1000-1630

Admission: Adults £5.00, concessions (65+) £4.00, accompanied children free, family £9.00.

Features: *Layout 7* (Marklin Z), *Rushwick* (OO), *Cannon Bay* (OO), *San Fayre Rhianne* (H0m), *Whartson Hall* (N), *Hackworth* (*Trafalgar Terrace*) (OO), *Bough Court* (OO), *Tipton Sub Division PA* (N), *Hawaiki* (On30), *Inglennook Sidings* (OO), *Illie Tow* (OO), *Canwells View* (OO), *Lakeside & Haverthwaite* (O), demonstrations, trade stands.

Website: www.bournvillemrc.co.uk

Saturday 27 & Sunday 28 August GAINSBOROUGH, Lincolnshire

Organiser: Gainsborough MRS.

Venue: Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.

Open: 1330-1730 (last admission 1630)

Admission: Adults £4.00, children £3.00, seniors £3.00, family (2+2) £10.00.

Features: Open days with O gauge East Coast Main Line layout in operation.

Contact: Barry Hodgkinson 01427 610475.

Email: gmrscontact@gmail.com

Website: www.gainsboroughmodelrailway.co.uk

Saturday 27 & Sunday 28 August HAYLE, Cornwall

Organiser: Hayle Railway Modellers.

Venue: Hayle Day Care Centre, TR27 4DE.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: Adults £5.00, under 14s free.

Amenities: Parking, refreshments.

Features: Layouts, trade support.

Website: www.haylerail.co.uk

Saturday 27 & Sunday 28 August HIGHBRIDGE, Somerset

Organiser: Burnham & District MRC.

Venue: King Alfred School, Burnham Way, Highbridge, Somerset TA9 3EE.

Open: 1000-1600

Admission: Adults £5.00, children free.

Amenities: Free parking, step-free access, light catering.

Features: *Norwich Central* (O), *Kleine Albula* (H0m Swiss), *The Clydach Railway* (OO9), *Westwick* (N), *Enoshima* (N Japanese), *Nosach Green* (O), *Clifford Chambers Street* (N), *North Hurst & North Park* (OO), *Venterol-Rousset* (HO), *Hookwood* (EM), *Robridge* (OO), *Heybridge Wharf* (3mm); Hornby live steam; layouts from Channel 5's *The Great Model Railway Challenge* including *Jurassic Park*; demonstrations, trade stands.

Website: www.bdmrc.co.uk

Saturday 27 & Sunday 28 August MACHYNLLETH, Powys

Organiser: Bill Newton, Corris Railway Society.

Venue: Y Plas, Machynlleth SY20 8ER.

Open: 1000-1630

Admission: Adults £4.00, children £2.00, family (2+2) £10.00.

Amenities: Parking available nearby, refreshments and light snacks available.

Features: 11 layouts from N gauge to 16 mm (visitors can operate one of the layouts), static display of models courtesy of the Mid Wales Gauge 1 Association, Trix Twin and Tri-ang Big Big trains, sales

stands. Trains running at Corris Railway 4½ miles away.

Website: www.corris.co.uk

Sunday 28 August BROXBOURNE, Hertfordshire

Organiser: Hoddesdon Model & Railway Club.

Venue: The Old Mill, Mill Lane, Broxbourne, Herts EN10 7AX.

Open: 1000-1600

Admission: Adults £3.00, children £1.00.

Amenities: Free parking (check website for updates due to road closures in the area), refreshments.

Features: Layouts in G scale, O gauge, OO and N, model boats in pool, train rides for children, sales stand, tombola.

Contact: 07708 579822

Website: www.hoddesdonmrc.org.uk

Monday 29 August HARROGATE, North Yorkshire

Organiser: Harrogate Model Railway Group.

Venue: Constance Green Hall, St Aidan's School, Oatlands Drive, Harrogate HG2 8JR.

Open: 1000-1600

Admission: Adults £5.00, children £2.00, family (2+2) £10.00.

Amenities: Free parking, disabled access to all areas, cafeteria.

Features: Layouts, trade stands, tombola.

Email: harrogatemrg@gmail.com

MEETINGS

Thursday 21 July – 1930

Marlow & District Railway Society

Oxford to Cambridge – Then and Now by Richard Crane

Venue: Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or Zoom.

Entry: Members free, non-members £3.00.

Contact: www.mdrs.org.uk or

Martin Stoolman on 07954 849390

Monday 25 July – 1930

Bubwith, Yorkshire

New group for anyone interested in railway modelling and/or railways in general – new members welcome.

Venue: Bubwith Leisure Centre bar, Main St, Bubwith, Selby YO8 6LX.

Contact: steve.grantham1@btinternet.com

Tuesday 26 July – 2000

Gauge O Guild

Scratch-building a footplate with Nick Dunhill

Venue: On Zoom.

Entry: Free to members, £2.00 for non-members. Booking essential via

www.gaugeoguild.com

Tuesday 2 August

The Enfield Transport Circle

Visit to Walthamstow Pumpouse Museum

Venue: 10 South Access Road, Walthamstow, E17 8AX.

Website: www.tetc.chessck.co.uk

Monday 8 August – 1930

Lincoln Railway Society

Strictly Freight Only (Part 1) by Brian Ringer

Venue: St Hugh's Church, Harewood Crescent, North Hykeham, Lincoln LN6 8JG.

Entry: Non-members £3.00.

Contact: 01522 698085 or email

grahamlighfoot1948@gmail.com

Thursday 18 August – 1930

Marlow & District Railway Society

Tracks in the Mist – Part 2 by Colin Brading

Venue: Bourne End Community Centre, Wakeman Rd, Bourne End SL8 5SX or Zoom.

Entry: Members free, non-members £3.00.

Contact: www.mdrs.org.uk or

Martin Stoolman on 07954 849390

Monday 29 August – 1930

Bubwith, Yorkshire

Railway group – new members welcome.

Venue: Bubwith Leisure Centre bar, Main St, Bubwith, Selby YO8 6LX.

Contact: steve.grantham1@btinternet.com

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Trade

Retailers' Announcements

YORK - MONK BAR MODEL SHOP LTD. 2 Goodramgate (by Monk Bar). Large selection of new model railways, Scalextric, plastic kits, diecasts, etc. Tel: 01904 659 423.

HULL - EAST YORKSHIRE'S ONLY SPECIALIST model railway shop. Model Railways bought, sold and exchanged. 53A MODELS, 430 Hessle Road, Hull. (Closed Mondays). www.53amodels.co.uk Tel: 01482 227 777.

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PLUS DAUGHTERS OF BASILDON ESSEX. N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. PLUS DAUGHTERS, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon, Essex, SS13 1EU. Tel: 01268 726 211 www.plusdaughters.co.uk

DINGWALL (ROSS-SHIRE)-SPORTS & MODEL SHOP, 66 High Street, Dingwall, IV15 9RY. PLASTICS-METALS-SCENICS. OO and N scale, PECO, Hornby, Bachmann, Metcalfe, Wills, Ratio & Dapol. Tel: 01349 862 346.

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Email: members-sales@009society.com.

For society details see www.009society.com

Wanted Private

(This column is reserved for readers who are private Model Railway enthusiasts or collectors and require items purely for their own personal use).

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KIT ITEMS, WHITEMETAL, BRASS OO & O, DJH, Wills, Millholme, Alan Gibson etc. Engines, coaches, wagons, unmade, part made, engines working or not. Kit spares – will travel. *Tel: 01302 481 274. Mobile: 07767 356 890. Email: simonecull06@tiscali.co.uk*

O GAUGE WANTED - LOCOS, ROLLING STOCK, ACCESSORIES, TRACK/POINTS, SPARES AND BUILDINGS. Large or small collections purchased. *Tel: 01302 481274. Mob: 07767 356 890. Email: simonecull06@tiscali.co.uk*

G SCALE AND OTHER GARDEN RAILWAYS WANTED. Gauge 1, SM32, Bachmann, LGB, Piko, Accucraft, Roundhouse etc. Locomotives, rolling stock, track, points and accessories all required. *Tel: 01302 371623. Email: sophiecul99@gmail.com*

WANTED – OO GAUGE MODEL RAILWAY ITEMS AND COLLECTIONS. Yorkshire, Lincolnshire, Nottinghamshire areas. *Phone Peter on 07804 267 732.*

BOXED TIMPO TRAINS, WILD WEST AND WW2 BATTLE. Also 1960/70's loose, especially boxed plastic toy soldiers, cowboys, Knights, Romans, Britain's plastic garden folk. *Tel: 0782 344 3474.*

Swapmeets • Sales • Fairs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so. All advertisements include the organiser's town and telephone number. The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

July 2022

20 July (Wed) 6.30pm-8.30pm

TONBRIDGE TOY FAIR – NEW DATE, The Angel Centre, Angel Lane, Tonbridge TN9 1SF. *Contact COLIN PENN 07810 513 576 or JOE LOCK 07866 641 215*

24 July (Sun) 10am-2pm

POTTERS BAR TOY & TRAIN FAIR, Elm Court Youth and Community Centre, Mutton Lane, EN6 3BP. *Telephone Colin on 01279 301 407 or Joe 07866 641 215.*

31 July (Sun) 10.30am-3pm

MINEHEAD TOY & TRAIN SALE. 75 tables. Free parking. Cafe. New Venue: West Somerset College, Bircham Road, Minehead, TA24 6AY (Main A39 road). Details at www.chrisdyerfairs.co.uk *Tel: 07966 694579.*

August 2022

7 August (Sun) 10.30am-3pm

6th ANNUAL TOY & TRAIN COLLECTORS FAIR, National Brewery Centre, Horninglow Street, Burton Upon Trent, DE14 1NG. Adults £4,

Children £1. Price includes entry to Brewery Museum.

Contact NATIONAL BREWERY CENTRE: 01283 532 880 or CHRIS EATON: 07877 346 371.

7 August (Sun) 10.30am-3pm

BUXTON – 150 STALLS, The Pavilion Gardens, St Johns Road, Buxton, Derbyshire, SK17 6XN. Adults £3, Senior Citizens £2.50, Children £1. Many stalls full of OO and O gauge trains. *Tel: 01604 846 688 www.bpfairs.com*

9 August (Tues) 6.30pm-8.30pm

HOOK TOYFAIR - Community Centre, Ravenscroft RG27 9NN. The evening monthly toy and train fair, always the second Tuesday of the

month. Free parking, café. Friendly dealers ... mostly! Just £1 entry.

stevedevizes@aol.com 07936 441 381 & 01380 725 322.

11 August (Thurs) 7pm-9pm

THEYDON BOIS, Theydon Bois Village Hall, Coppice Row (B172), Theydon Bois, near Epping, Essex CM16 7ER. Close to M25 J26 and M11 J5. Admission £1. Contact JOE LOCK.
Tel: 07866 641 215.

14 August (Sun) 10.30am-3pm

EXETER (WESTPOINT) TOY AND TRAIN COLLECTORS FAIR, Westpoint Arena, Clyst St Mary, Exeter. One mile from the M5. Up to 300 stalls selling model railway items, obsolete and new. Hot and cold refreshments and free parking for thousands of cars. For more information and Stall availability contact: BULLDOG FAIRS.
Tel: 01373 452 857 or 07917 125 641
www.bulldogfairs.com

14 August (Sun) 9.45am-1.15pm

RAYLEIGH ESSEX, The Swayne, Park School, Sir Walter Raleigh Drive, off London Road, SS6 9BZ. SRP TOYFAIRS.
Tel: 01322 662 729 or 0773 999 8012
www.srpoyfairs1.co.uk

14 August (Sun) 10.30am-3pm

STAFFORD – MORE THAN 300 STALLS. The Prestwood Centre, Stafford County Showground, Weston Road, Stafford, ST18 0BD. Adults £4, Seniors £3.50, Children £1. There are many stalls selling OO and O gauge trains.
Tel: 01604 846 688 www.bpfairs.com

21 August (Sun) 9.45am-1.15pm

ORPINGTON, CROFTON HALLS, by Orpington Railway Station, Kent BR6 8PR. SRP TOYFAIRS.
Tel: 01322 662 729 or 0773 999 8012
www.srpoyfairs1.co.uk

21 August (Sun) 10.30am-2.30pm

OXFORD, Exeter Hall, Oxford Road, Kidlington OX5 1AB, M40 J9. Up to 60+ tables, Hornby & Bassett Lowke, 'O' Gauge Trains, 'OO' Trains, Tinsplate Toys, Dinky Toys, Corgi, Meccano, Britains Soldiers, refreshments, Free Parking.
Further details, telephone: 01869 347 489
www.toytraincollectorsfairs.com

27 August (Sat) 10.30am - 2pm

CHESTER, The Cheshire Country Sports Club, Plas Newton Lane (near Upton), Chester CH21 1PR (off A41). Approximately 3 minutes from junction 12 of M53 and junction 41 of A55. Admission £2.50. TONY OAKES FAIRS.
Tel: 01270 652 773 & 07825 631 323.

29 August (Bank Holiday Monday) 10.30am-2pm

BRIDGNORTH LEISURE CENTRE, High Town, WV16 4ER. 70 + stalls of all types of model railways, Dinky's, Corgis, various die-casts etc. Admission £2.50. TONY OAKES FAIRS.
Tel: 01270 652 773 or 07825 631 323.

30 August (Tues) from 6pm

GARSTANG TOYFAIR, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON.
Tel: 01282 439 009.

September 2022

25 September (Sun) 10am-2.30pm

HULME HALL, PORT SUNLIGHT CH62 2DH. Admission £3.
For bookings contact DAVID on 07967 681 080 or ALAN 07904 777 605.
September 2022

25 September (Sun) 9.45am-1.15pm

WORTHING – NEW EVENT – DURRINGTON HIGH SCHOOL The Boylevard, Worthing BN13 1JX. Car parking for 350 cars. SRP TOYFAIRS.
Tel: 01322 662 729 or 0773 999 8012
www.srpoyfairs1.co.uk

Opportunity Yours

RM Classified Advertisement Order Form — last date for receiving copy for October 2022 issue is 18th July.

Please insert the following advertisement under the section headed:

PLEASE NOTE: If you do wish to renew an existing advert, phone, write or email within 3 months of the original advert being placed.

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Advertisements without a box number must include your name, address or telephone number. Standard abbreviations (LMS, META, 0-6-0), normal hyphenations (O-gauge, 2-rail, pre-war) and prices count as one word. Telephone numbers, whether exchange or all figure, count as two words.

CONDITIONS OF ACCEPTANCE

All copy is subject to our approval. We reserve the right to request amendments.
Scale or gauge should be mentioned when referring to models. Swapmeets and other events must include a telephone number at which the organiser can be contacted.
Advertisers are expected to acknowledge all enquiries that enclose an SAE (even if the item has already been sold).
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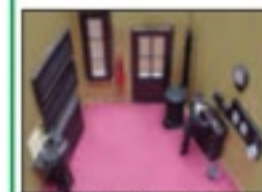
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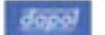
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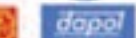
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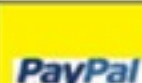
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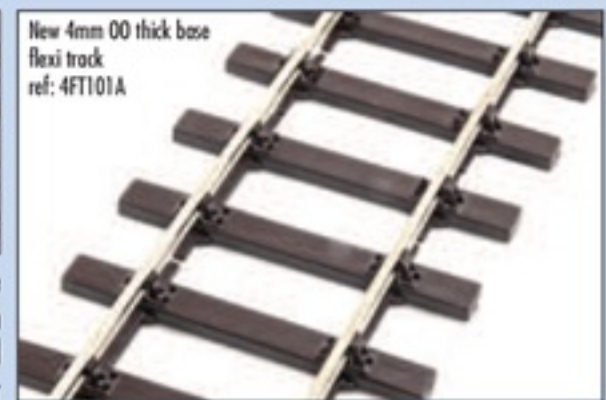
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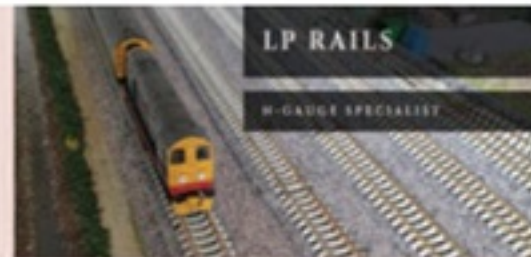
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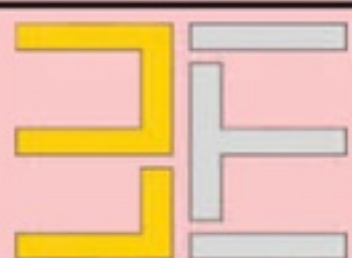
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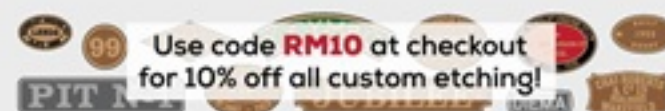


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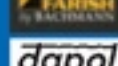
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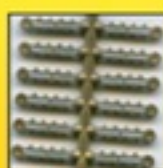


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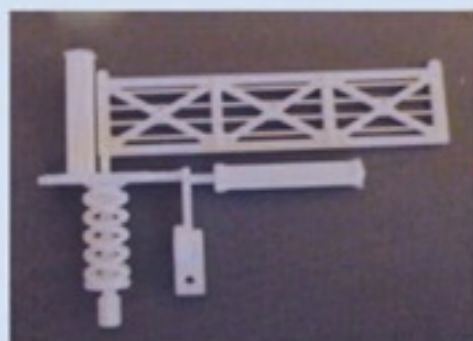
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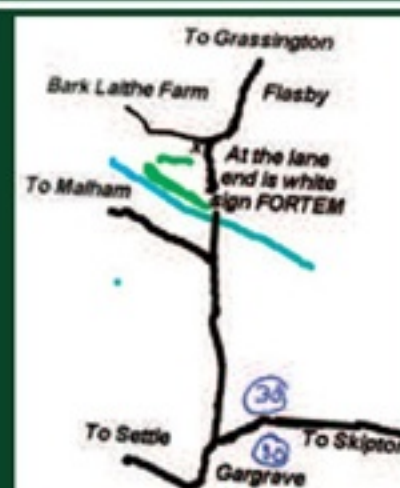
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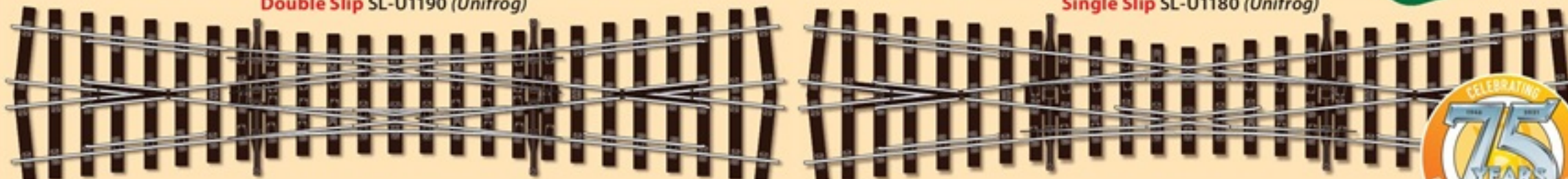
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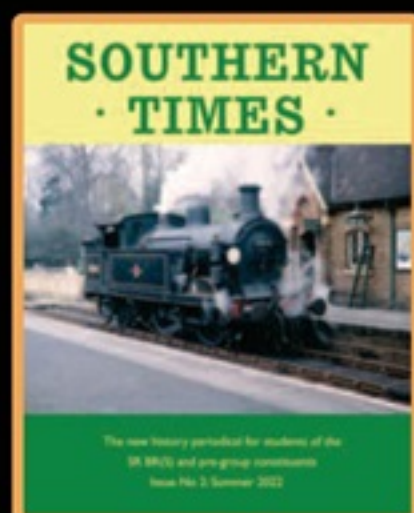
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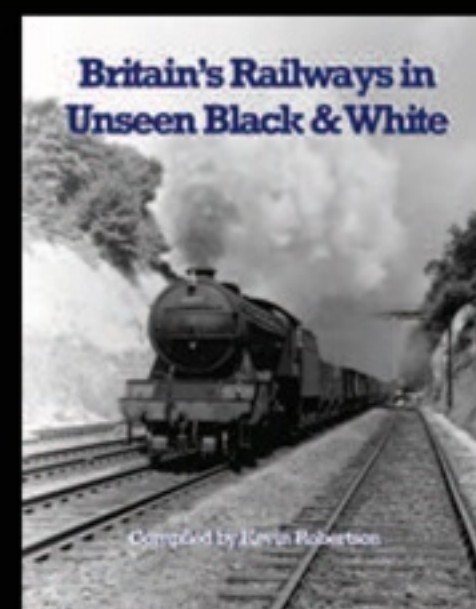
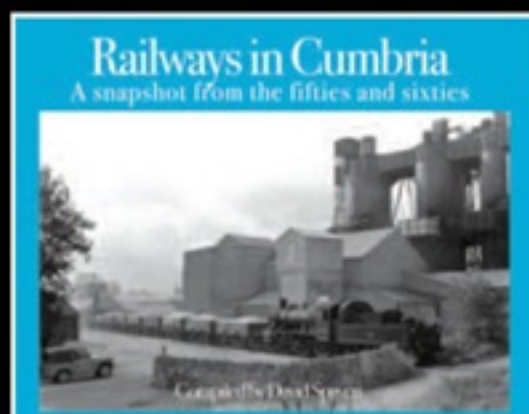
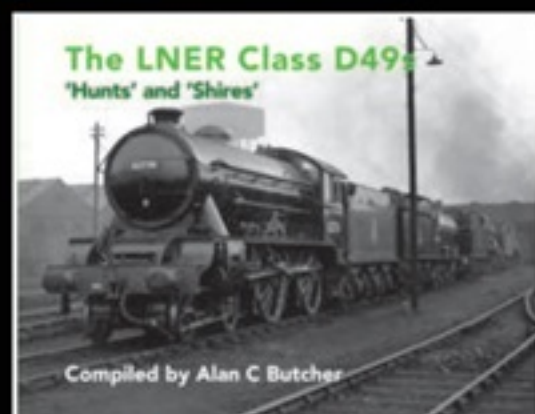
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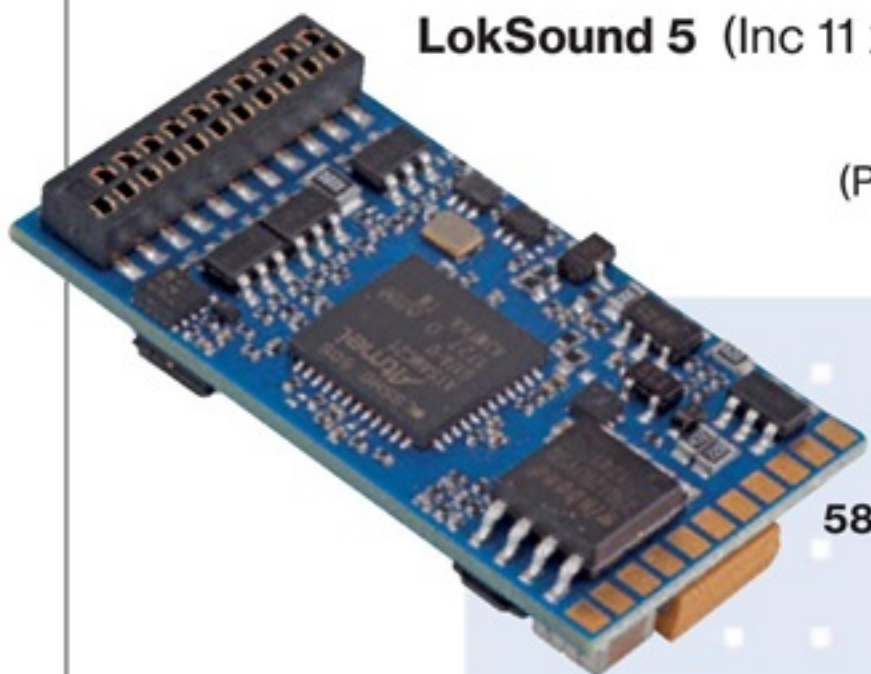
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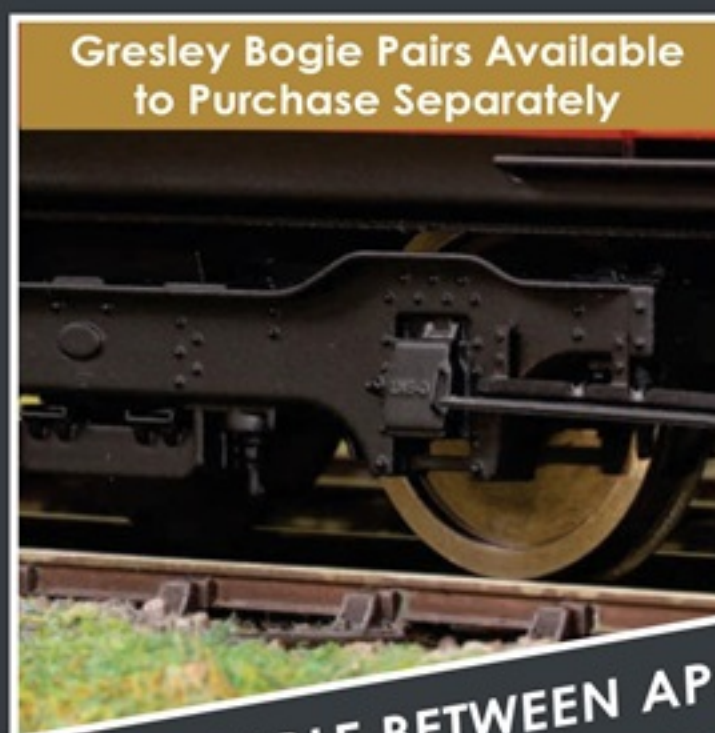
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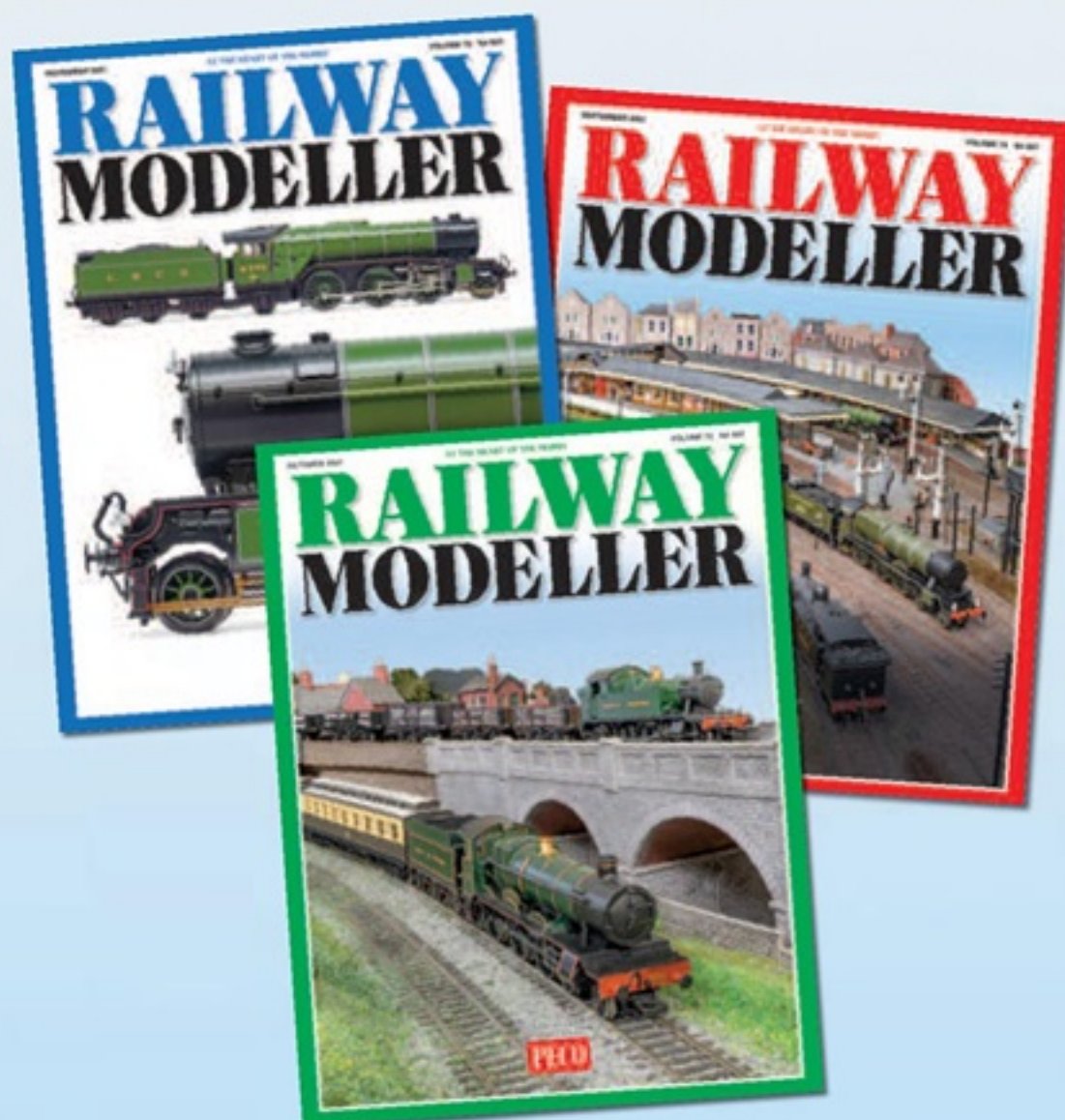
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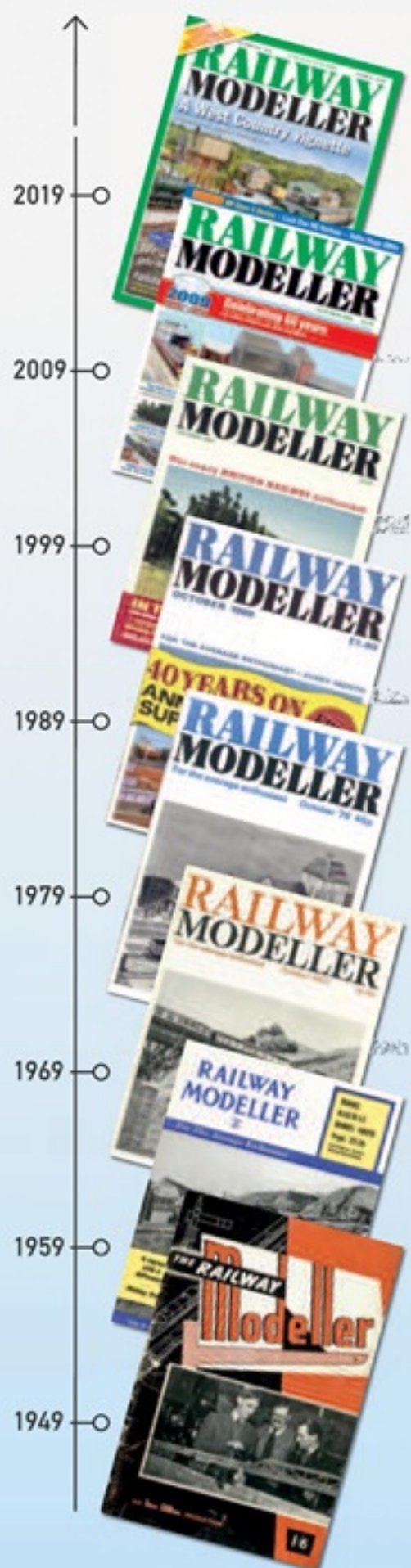
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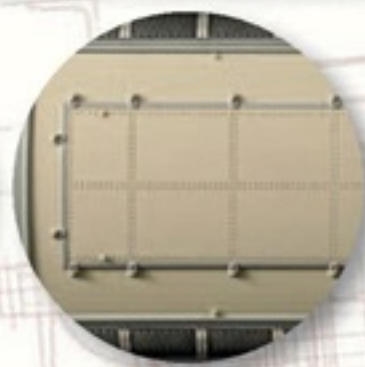
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**EXPLORING THE CORPORATE
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Britain's railways in the 1970s

As reflected in the pages of RAILWAY MODELLER month in, month out, the glory of the steam age is still commemorated in miniature by many thousands of model railway hobbyists.

Why then might we propose that the decade immediately following the end of steam on the national network be considered seriously as a period in our railway history that's worth modelling?

To begin to answer that question we have to turn back the clock to the immediate post-Nationalisation phase of Britain's railways, beginning in 1948.

In those early years a lot of deliberation took place at senior levels over the future of the railways, and numerous committees produced very well researched reports on all aspects of the management and operation of the business.

Of particular relevance to our cause is perhaps the report on Types of Motive Power published in 1951. There were three main elements in its conclusions:

a) That full electrification of the network, whilst ideal, would not be possible for many years due to high capital cost and the need for longer-term guarantees of power supplies from the national electricity grid.

b) That the use of steam locomotives into the future was not sustainable, citing as key factors: labour-intensive operation and costs, the declining security of high-quality steam coal supplies, and growing environmental concern over smoke and soot pollution.

c) That main line diesel locomotive implementation was, at that time, not yet technically feasible. A 2,000hp traction unit with plenty of low-end torque was considered necessary for main line



Above: **Seventies summer days by the sea.** A scene so typical of the decade at Dawlish with Class 52 No.1041 *Western Prince* on a down train accompanied by a National motor coach, a Mk.I Ford Escort van and, naturally, a pair of red K6 telephone kiosks. *Photo: Colour Rail*

passenger and freight workings. That meant a suitable diesel engine combined with either electric or hydraulic transmission; and fitting all that equipment into a locomotive built to the British loading gauge was still proving elusive. Scope existed however for the use of smaller diesel units for shunting and lightweight diesel mechanical railcars on branch and secondary services.

Below: **Virtually unchanged from steam days,** Towyn goods yard sees an unidentified Class 24 shunting a Cambrian line pick-up freight as Up and Down DMUs pass each other in the loop. The rake includes gunpowder vans from Portmadoc, whilst beyond, a pair of 1950s style flatbed coal lorries can be spotted by the coal stack. *Photo: Steve Flint*



In many ways the nascent British Railways had found itself between a rock and a hard place. However, the BR standard steam locomotive building programme was already underway and the difficult decision about the long-term future of railway motive power could be put off until another day.

In fact, four years were to elapse before the report's key recommendations were acted upon with certainty with the enactment of Modernisation and Re-equipment of British Railways Plan – better known as the 1955 Modernisation Plan.

The search for that elusive 2,000hp diesel locomotive design had been accomplished opportunely in the intervening years with the appearance of the Ashford-built D16/2 No.10203 in 1954. This, together with other prototypes under development, brought any apparent procrastination to an end and gave the British Transport Commission sufficient confidence to press ahead with phasing out the steam locomotive in favour of diesel and electric traction.

Numerous other aspects of the 1951 report recommendations were also included in the '55 plan, such as suburban electrification schemes, and, where practicable, the introduction of diesel railcars on secondary and branch line services.

In acknowledgement of the long-term desirability of an electric-powered railway network, both the southern sections of the East and West Coast trunk routes were earmarked for electrification, as were the Kent coast lines. This was of course the mid-1950s and the plan's fulfilment would take many years; indeed the authors



Left: A significant ancestor of BR's diesel-electric fleet, the Bulleid/English Electric Class D16/2 No.10203 seen in 1957. It proved that 2,000hp locos for main line services were feasible.

Photo: Rail Photoprints

Below: How BR executives always wanted the railway to look: smart, bright main line trains powered by electric traction, as illustrated by 86 023 on a rake of Mk.IId coaches at Euston station in July 1975.

Photo: Rail Photoprints



Below: Also earmarked in 1955 for early electrification was the East Coast Main Line, though coverage of the entire route would not be accomplished for another 35 or so years. Hence the deployment of the Class 55 Deltics which held sway in corporate image colours for almost all of the 1970s. Holloway Bank sees pre-TOPS No.9009 Alycidon climbing away from Copenhagen Tunnels in June 1970.

Photo: Dave Cobbe/Rail Photoprints



anticipated that the transition would take around 15 years to complete. Already we can see how those committee recommendations and the 1955 Modernisation Plan would have immense influence on the 1970s railway scene; yet more changes were still to come.

An interim review

A reappraisal of the '55 plan was undertaken by the British Transport Commission just four years later. Published in 1959 it highlighted the achievements of the original plan, but only insofar as to reinforce the confidence and assurances the authors had with the original proposals. More significantly, it divulged that the plan had failed, within its prescribed timescale, to reduce the financial burden of the nationalised railway on the nation. More drastic measures were considered necessary.

The term 'Rationalisation' became the byword for an increased push in the years 1959 - 1963 to abandon many more duplicated trunk routes, branch lines, stations, depots and old marshalling yards that were considered unremunerative.

Presented in the reappraisal as a positive process, Rationalisation would further speed up recovery and 'reinvigorate the rest of the network'. Though, as history has since shown, it really only heralded the closure of many more route miles which the country was soon to witness.

Within the political arena many of those with power and influence were intent on seeking ways to further reduce the cost of Britain's railways and, simultaneously, create a national road network that could exploit and benefit from the convenience and rapidly developing road transport of the day.

It seemed to those in political power to be a blindingly obvious strategy and the government commissioned

Below: **Still antiquated: Barnetby East Junction in March 1979. Although much of the track is to modern standards, the upper quadrant signal arms probably date from British Railways Eastern Region days, whilst the lattice gantries and wooden posts will be pre-Nationalisation and the box pre-Grouping (GCR). Photo: Steve Flint**

another report: The Reshaping of British Railways, hiring Dr Richard Beeching to spearhead the task.

As is widely accepted, the report was a bottom line financial appraisal on a route-by-route, service-by-service profit and loss analysis. Any railway undertaking that was not profitable on a stand alone basis was to be axed.

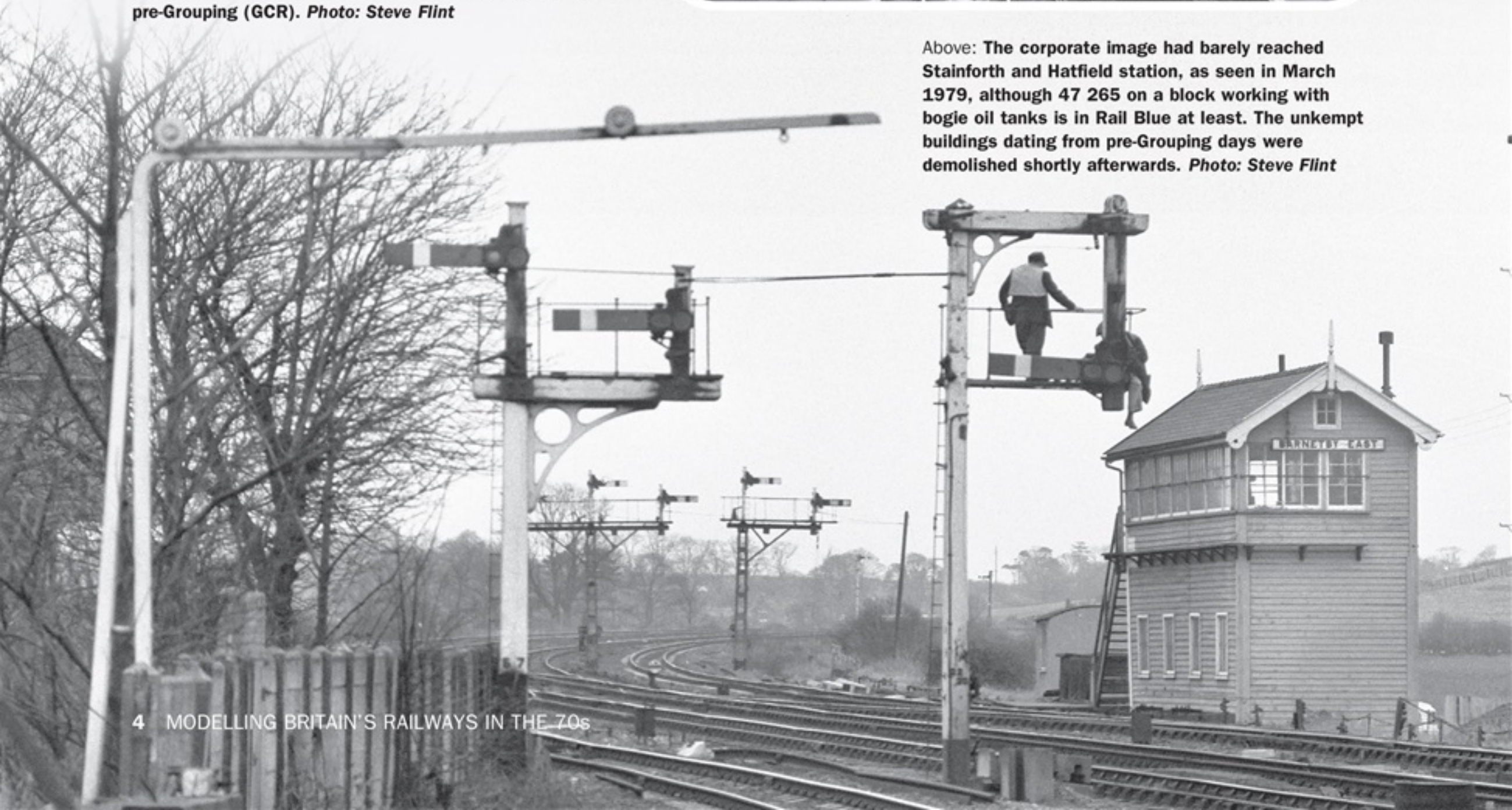
Whilst some social obligations were ostensibly taken into account, the main outcome of the report was the notorious closure of hundreds more stations and route miles. It also completely recast the 1955 plans for freight traffic which subsequently, by the dawn of the 1970s, gave rise to an incredible – if unintended – array of freight wagon types. All really interesting stuff, however, to engross the freight vehicle modeller endlessly (see page 16).

Still antiquated

For many enthusiasts the end of steam traction on Britain's main railway network in 1968 was the major blow, but initially the network itself saw little immediate change. Although some of the main line routes had previously been at the receiving end of partial modernisation during the 1960s, elsewhere much of the network that survived the Beeching report closures of the mid-1960s was,



Above: **The corporate image had barely reached Stainforth and Hatfield station, as seen in March 1979, although 47 265 on a block working with bogie oil tanks is in Rail Blue at least. The unkempt buildings dating from pre-Grouping days were demolished shortly afterwards. Photo: Steve Flint**





Above: With ECML electrification still a long way off, the HSTs began ousting the Deltics from the crack services in the late seventies. This set was seen racing through a rather tidy Grantham station on 25 May 1978. Mail and newspaper traffic awaits patiently for its train in 'BRUTE' trolleys on platform 1. Photo: Steve Flint

effectively, an Edwardian-built railway. Even by 1970, track layouts had remained unaltered in many localities, and whilst some cosmetic and signalling improvements under the nationalised British Railways had occurred on a regional basis, a great many stations, buildings, depots and other lineside structures like signal boxes, were often still as built by the pre-Grouping railway companies, 60, 70 or more years earlier.

A corporate identity

In the aftermath of Beeching's report the British Railways Board adopted the British Rail branding in 1965 and developed a corporate image styling guide for its entire operations.

The aim was to standardise all visible elements of the railway; from station signage and staff uniforms to locomotive and rolling stock liveries. It was another bold plan, and one that would yet again take a long time to implement fully. Nevertheless, from a visual point of view it had some impact on the outward appearance of the railways, but was most noticeable as the 1960s drew to a close on locomotive and rolling stock liveries.

The Monastral blue and grey paint scheme was applied universally across the network and apart from a few locomotive stragglers, very little remained of the old British Railways colours by 1970. But whilst new staff uniforms and a fresh coat of paint on passenger stock gave the impression of corporate modernisation, most of the old buildings and other fixed assets were left to decay further, until such a time as they could be swept away and replaced by something usually far less prepossessing.

A decade worth modelling?

We can see therefore, as British Rail approached the 1970s, it did so with a complex legacy – a tangle of historical endowment and part-implemented strategies that had given rise to a prodigious assortment of modern locomotives and rolling stock types, architectural styles, plant, equipment and operations.

Photographic records of the period often provoke a sense that the network was dilapidated, untidy and bleak: a far cry from the rose-tinted, bucolic highways and byways which modellers so love to recreate with their layouts.

Yet, as we hope to show in this booklet, the unique atmosphere and characteristics of the 1970s rail scene, and the sheer variety of traffic, stock and services, all provide an abundance of inspiring prospects and new challenges for modellers.

1970s layouts gallery



Above: Kier Hardy's *Wibdenshaw* was developed almost 20 years ago before the latest super-detailed models became available, but with his skilful upgrades the layout captured the unique characteristics of the 1970s rail scene successfully.



Above: *Hornsey Broadway* is Kier's current project which aims to exploit many of the up-and-coming new models emerging from the manufacturers.



Above: *Shenston Road* is a home-based layout by Greg Brookes and friends and aims to depict railway operations in the late 1960s and early 1970s and is set in the Black Country.

Photos: courtesy www.emgauge70s.co.uk

The 1970s – a decade of change

Britain's railways were witness to considerable change during the 10 years that elapsed between the beginning of 1970 and the end of 1979; not just the national rail network, but the myriad preserved lines and surviving industrial steam operations.

Here we take a journey through the decade, year by year, and highlight just some of the many significant events and changes that took place. Naturally, it is impossible to provide a definitive account of each of these years in just four pages, so what follows is a potted medley of events – all selected with the modeller in mind.

1970



Left: A model of No.47445 was produced by Bachmann several years ago in OO for Collectors Club members.
Photo: Hattons

- Journey times were reduced to less than six hours for express passenger services on the West Coast Main Line between London Euston and Glasgow, with trains composed of brand new Mk.IIb coaching stock. North of Crewe these services were now hauled (on the non-electrified lines) by double-headed Class 50 locomotives.
- Passenger services over the Woodhead route between Sheffield and Manchester were withdrawn on 5 January, with freight-only operations continuing on the line after that date.
- The Severn Valley Railway re-opened with public services on the first 5½ miles of running line on 23 May. Hawksworth 0-6-0PT No.1501 also arrived later the same year, it having been in private use at Coventry Colliery.

- The line to Merehead Quarry in Somerset was opened for Foster Yeoman stone traffic, with a spur created from the East Somerset branch line that joined the Great Western main line at Witham (south of Frome).
- Ex-BR Fowler 3F 0-6-0T No.47445 was withdrawn from private service at Crigglestone near Wakefield during August, its duties being assigned to an ex-BR Class 14 diesel-hydraulic.



Above: Class 76 E26049 at Dunford Bridge on the Woodhead route in 1970. From this year onwards, only freight trains would be seen on the line. Photo: Colour Rail

1971

- The nascent Dart Valley Railway in South Devon lost its Ashburton terminus on 2 October, the section between there and Buckfastleigh being lost to the building of the A38 dual-carriageway.
- The last of the London Transport fleet of ex-GWR 0-6-0PTs were withdrawn in June.
- The new BR Mk.IId coaches entered service, which were equipped with air-conditioning.
- On 2 October privately-owned No.6000 *King George V* broke the BR steam ban by hauling a railtour on the national network.
- On October 4 'mail catching' lineside apparatus was used for the final time.
- Also in October Class 33 locomotives supplanted Western Region Warships on the London Waterloo to Exeter route.
- Changes on the Southern Region included the withdrawal of



Above: KR Models has announced models of the Bulleid 4-DD sets in OO gauge. A livery diagram is illustrated here.

2-BIL and 2-HAL EMU stock from the Brighton main line in August, whilst the pair of Bulleid 4-DD sets were taken out of service in October.

- Plans for the new Total Operations Processing System (TOPS) were announced, which sought to provide the rail network with a computerised method of train vehicle information and control. The first locomotive to receive a TOPS number was E26050, which became 76 050 (Class 76) on 12 November 1971.



Above: '5700' No.L94 at Barbican with London Transport's last steam-hauled train on 6 June 1971. Photo: Rail Photoprints

1972



Above: An N gauge model of the 5-BEL 'Brighton Belle' EMU was released in 2014 by Hornby under its Arnold brand.

- The first in a new generation of 'park and ride' railway stations, Bristol Parkway, was opened, situated in the northern suburbs of the city on the South Wales Main Line.
- In December, Adams B4 0-4-0T



Corral Queen (ex-BR No.30096) was delivered to the Bluebell Railway for preservation, following industrial service at Dibbles Wharf (PD Fuels Ltd) in Southampton.

- The Southern Region's 'Brighton Belle' service was withdrawn from 1 May, with the 'Golden Arrow' (then the region's last remaining Pullman service) withdrawn at the end of the summer timetable.
- In June a pair of Class 41 HST prototype power cars were completed at Crewe Works.
- In November a new Freightliner terminal was opened at Felixstowe in Suffolk.
- The last four Western Region 'Warship' diesel-hydraulic locomotives were withdrawn from service in December.

Left: A Class 71 electric locomotive with the last 'Golden Arrow' at Malden on 30 September 1972. Photo: Colour Rail

1973

- The first of the brand new Class 87 Bo-Bo electric locomotives was delivered in anticipation of the completion of the project to electrify the northern section of the West Coast Main Line between Crewe and Glasgow.
- Trials commenced with the prototype High Speed Train (HST-P), which set a new British speed record for diesel traction of 143mph.
- Public services commenced on the North Yorkshire Moors Railway on 22 April, followed by the Lakeside & Haverthwaite Railway on 5 May.
- The Western Region Metro-Cammell diesel-electric Pullmans, operating from Paddington to Bristol and South Wales, were withdrawn, with a commemorative tour taking place on 5 May.
- Steam returned to Scottish main lines with a railtour on 5 May hauled by privately-owned Gresley A4 Pacific No.60009 *Union of South Africa*.

- Public trials started on the Southern Region in June with a prototype 4-PEP Electric Multiple Unit.
- The new passenger concourse at King's Cross was opened on 25 June, which was completed as a single storey designed to complement the station's existing c.1852 terminus facade.
- The first trial runs of the experimental gas turbine Advanced Passenger Train Experimental (APT-E) took place in September.



Above: Hornby released a Class 87 model from new tooling in 2018. Illustrated is 87 035, which was named in April 1978.

1974

- From 6 May the section of the West Coast Main Line OHLE from Weaver Junction to Glasgow was switched on, thereby completing the four-year Anglo-Scottish electrification project. This enabled a reduction of the 'Royal Scot' schedule to just five hours between London Euston and Glasgow.
- 6 April saw the first steam passenger train service on the Strathspey Railway from Boat of Garten to Aviemore – a directors' special for the company's annual general meeting. The line would reopen to the public in 1978.
- A prototype for a programme of DMU refurbishment was revealed, with a Metro-Cammell three-car unit outshopped in a new white livery with blue bands and yellow ends. Interiors were upgraded to bring them closer to the new BR Mk.II and Mk.III coach types.
- TOPS numbering reached the two Barclay 150hp diesel shunters allocated to the isolated Holyhead Breakwater Railway, with Nos.2954 and 2955 becoming 01 001 and 01 002



Above: A Class 101 DMU in the new white livery with blue bands, pictured in September 1975. Photo: Colour Rail

respectively. Despite receiving these new numbers, both retained their BR black liveries with early emblems.

- Late in the year the prototype HST began testing on the Western Region, having previously undertaken trials on the Eastern and Scottish Regions.

1975

- The last boat trains into Heysham ran on 5 April upon the withdrawal of passenger shipping services between Heysham and Belfast.
- The first examples of BR Mk.III passenger stock entered service on West Coast Main Line services during the summer.
- On May 5 the HST-P entered passenger service for the first time, operating on the Great Western Main Line from London Paddington.
- Celebrations were held at Shildon in August to mark 150 years since the opening of the Stockton & Darlington Railway, with events including a parade of motive power headed by a working replica of *Locomotion*.
- The APT-E set a speed record of 152mph on 10 August



Left: A second run of APT-E train pack models in OO gauge has been produced by Rapido Trains UK.



Above: **Prototype High Speed Train 252 001, built at Derby in 1972, provides the crowds with a glimpse of the future at the Shildon cavalcade on 31 August 1975. Photo: Rail Photoprints**

during testing on the section of Western main line between Swindon and Reading.

- The new National Railway Museum at York opened its doors on 27 September.
- On 27 October the last stage of the TOPS commissioning process was completed, whereupon the system now covered all 11,300 route miles of the BR network.

1976

- Birmingham International was opened on 26 January, providing a five-platform station on the high-speed route between London and Birmingham to serve the National Exhibition Centre and Birmingham Airport.
- The first production HST achieved 125mph on a commissioning run from Derby to Morpeth on 5 March.
- The displaying of train description codes was phased out from this year onwards, with locomotives equipped with headcode boxes instead displaying '0000'. However, trains continued to use the train description codes electronically, as part of TOPS.
- Construction of Hornsey Electric Maintenance Depot was completed, providing facilities for the inauguration of EMUs operating on Great Northern suburban routes, including Classes 312 and 313.

- The first two Romanian-built Class 56 Co-Co locomotives were delivered to the UK on 4 August and began trials soon after.
- The first production HSTs entered revenue-earning service on the Western Region on 4 October, operating from London Paddington on high speed services to Bristol and South Wales.



Above: **Models of the production High Speed Trains (with Class 43 power cars) are available from Hornby. Illustrated here is a Western Region example finished in original blue & grey livery.**

1977

- The last of the Western (Class 52) diesel-hydraulic locomotives were withdrawn from service, with a final farewell tour, the 'Western Tribute', taking place on 26 February.
- The Watercress Line between Alresford and Ropley was opened to passengers on 30 April, with services hauled by Maunsell N Class 2-6-0 No.31874.
- Passenger traffic returned to the Derwent Valley Railway near York on 4 May with steam-hauled trains from Layerthorpe station to Dunnington



Above: **Class 52s Nos.1023 Western Fusilier and 1013 Western Ranger at Westerleigh on 26 February 1977. Photo: Rail Photoprints**

hauled by J72 0-6-0T No.69023.

- Following its sale to Hamersley Iron, in June No.4079 *Pendennis Castle* began its voyage to Western Australia, arriving there the following month.
- On 13 September an exhibition train was launched to promote BR air-braked freight services under a new 'Speedlink' branding.
- On 3 December a farewell passenger tour was organised to mark the withdrawal of the last remaining Class 71 and Class 74 electric locomotives operating on the Southern Region.

1978

- Class 50 locomotive 50 035 became the first member of the fleet of 50 machines to be named, its *Ark Royal* nameplates being unveiled on 17 January.

- The final 'Yorkshire Pullman' operated on 5 May, hauled by Deltic 55 002 *The King's Own Yorkshire Light Infantry*.

- HST services were introduced on the East Coast Main Line from 8 May, this signalling the start of Deltic locomotives being displaced from front-line duties.

- On 7 June the first electric Advanced Passenger Train (APT-P) was unveiled at Derby, ready for commissioning trials. The design was classified as Class 370.

- On 24 June the first trains were operated by the Festiniog Railway from Ddualt to Tan-y-Grisiau, following the extensive work to complete a deviation from the original formation of the route.

- 6 July was the final day of operation for the Waterside colliery system near Dalmellington, which included steam traction in the form of NCB 0-6-OT No.24.



Above: **Class 56 Co-Co 56 036 in experimental BR blue livery at Nottingham on 19 November 1978.**
Photo: Colour Rail

- During August BR Class 56 Co-Co 56 036 was outshopped in an experimental BR blue livery (a precursor to the large logo scheme) with large white double-arrows and numbers, silver roof, brown underframe and full yellow ends.

- On 25 October 1978 HM The Queen unveiled a plaque to mark the completion and official opening of the Merseyrail Underground System, which also saw the introduction of new Class 507 three-car EMUs.

Below: **A model of the Class 370 Advanced Passenger Train (APT-P) was released by Hornby in 2022.**



1979

- Travelling Post Office operations over the Chester-Holyhead line came to an end on 9 February.

- As part of promotional activity leading up to events (in 1980) to mark the 150th anniversary of the Liverpool & Manchester Railway, Class 86 Bo-Bo 86 235 was named *Novelty* on 4 June, whilst the BR blue livery was adorned with a 'Rocket 150' logo.

- On 1 August a replica of Stephenson's *Rocket* was tested on the Bowes Railway in readiness for the aforementioned 'Rocket 150' celebrations.

- The first of the Southern Region's four-car Class 508 EMUs were delivered in August.

- On the Scottish Region, pull-and-push operation was introduced on Edinburgh-Glasgow passenger



Above: **Shortly after its introduction into service, Class 508 EMU 508 010 pauses at Thames Ditton with an LCGB railtour on 9 February 1980.** Photo: Colour Rail

services, comprising Mk.III coaches with a Mk.II DBSO at one end and a Class 47/7 at the other.

- Iron ore trains of 2,740 tons – then the heaviest on BR – were allocated to pairs of the recently-introduced Class 56 locomotives as of 6 August, operating from Port Talbot Harbour to Llanwern steelworks.

Left: **BR Mk.II DBSO (Driving Brake Standard Open) vehicles are available in OO and N as part of the Bachmann Branchline and Graham Farish ranges respectively. The latter is illustrated here.**



A changing infrastructure



One of the knottiest problems facing the managers of Britain's railways as they attempted to fulfil the proposals of all the various modernisation plans and committee recommendations mentioned on page 2, was how to deal with the capital infrastructure which the railways had inherited.

Much of this; the stations, bridges, tunnels and other civil structures, was well over 100 years old and often the operating equipment that went with it was of a similar age.

The simple solution was to close down the operations and demolish everything and a lot of this had already occurred during the 1960s, especially that associated with steam traction – sheds and roundhouses, coal, ash and water facilities and so on – but as we have seen elsewhere in this booklet huge swathes of the network still resembled something out of the history books.

From a modeller's point of view, this mix of the old with the new, the dilapidated



Above: **Some steam age structures were not razed to the ground immediately after 1968. The ex GER/GNR shed at Lincoln (40A) was used for many years as a covered car park and is seen from within in May 1978 as an unidentified Class 47 heads westwards with a rake of Mk.1s. Both photos: Steve Flint**

Above: **Not all the old architecture was swept away and rebuilt as modernisation progressed. Some of it was adapted to fit at specific sites on a bespoke basis. In this view of Oxenholme on 17 September 1977 the Down platform canopy has been considerably remodelled to accommodate the 25kV catenary, whilst much of the Up canopy appears to be in its original state. Note also the K8 telephone kiosk on the Up platform.**

with the care for, can be a powerful trigger for layout ideas, and there was certainly plenty of that melange in the review period. On the other hand, several sections of the network had been fully modernised and rebuilt to reveal a gleaming embodiment of those 1950s planners' dreams.

However not all localities were fully upgraded as lines were electrified – if only to save money – and even on sections of the premier WCML electrification scheme north of Crewe, structures could be seen which had been only partially upgraded.

Furthermore, not all elements of the old



Above: **Bristol Parkway**, opened in 1972 and seen here in 1979. It was rather functional in design but heralded a lot of park-and-ride schemes in the years that followed. *Photo: Rail Photoprints*

railway Infrastructure was demolished quickly either. Take Lincoln for example where the joint GER/GNR locomotive shed and goods yard was located adjacent to the City's Brayford Wharf. For many years after the steam engines had departed the building was re-purposed as a covered car park. It has since been demolished and replaced with a modern structure under the auspices of Lincoln University, but the shell of the adjacent goods shed still remains, converted internally to a library.

Brand new stations were rare beasts during the period but a bright and airy modern one was constructed at Birmingham international and opened in 1976, whilst one of the first park-and-ride stations was commissioned at Bristol Parkway in 1972.

It is worth bearing in mind though that many of the modernised structures we can see on the modern railway today didn't actually appear until well after the turn of the decade. Stainforth and Hatfield station illustrated elsewhere in this booklet was a case in point; the station buildings and some track rationalisation only occurring in the early 1980s.

Because of the extended timescale of the rationalisation and modernisation process – some of which is still to be undertaken 67 years after the publication of the initial Modernisation Plan – it's a good idea to research the chosen period and environs of a proposed layout very thoroughly. As modellers we are accustomed to poring over books about locomotives and wagons but only by looking beyond the rolling stock may we be able to capture the real atmosphere of the locality we are modelling.

After all it's easy to fall into the trap of thinking that the level crossing you want to model was upgraded to automatic half barriers in the 1970s... but without a photograph of the specific level crossing at your chosen timescale, you could get it surprisingly wrong! Take for example the adjacent photograph of Barrow Road Crossing on the Barton-on-Humber branch. Can you take an educated guess as to when the image was taken? We'll reveal the actual date in the next issue of RAILWAY MODELLER.

This section has been a very brief look at the changing state of the railway infrastructure in the review period.

Right: **Barrow Road crossing** looks much like it was in pre-Grouping days, but when was this view captured? We'll publish the actual date in the next issue of RAILWAY MODELLER. *Both photos: Steve Flint*



Above: **The shape of things to come...** Birmingham International shortly after opening in 1976. A complete new-build reflecting building materials and clean design lines of the day. *Photo: Colour Rail*



Above: **Stockport in September 1976.** The 25kV OHLE was of particularly sturdy construction on this early section of the WCML electrification project, and may be one possible reason why this section rarely features as a subject for modelling. Note also the attire of the rail workers, which is far removed from the high visibility protective wear demanded on today's railway.



Steam survivors



As the 1970s dawned, steam had been officially banned from the national network and banished to a relative handful of preserved railways and steam centres – yet there were still steam locomotives in BR service carrying corporate blue livery, GWR pannier tanks running in the heart of London, and hundreds of engines, some of them ex-BR, still working hard on industrial sites. Steam may have been down, but it certainly wasn't out.

Five decades on, and a period that would once have come under the modelling heading 'modern image' can be considered as history worth recreating in its own right. For the modeller, the railway scene of the 1970s offers huge variety, with opportunities to run BR blue diesels and those steam survivors on the same layout. For those whose interests encompass both forms of traction, it could be the ideal combination.

Vale of Rheidol Railway

If asked when BR steam ended, most enthusiasts would rightly reply '11 August 1968' – a date etched into all our memories – but that was only on the standard gauge. Far into West Wales, on the narrow gauge Vale of Rheidol Railway from Aberystwyth to Devil's Bridge, it kept going for another two decades until the line was sold into private ownership in 1989.

The VoR's three 2-6-2Ts, Nos. 7 (*Owain Glyndwr*), 8 (*Llywelyn*) and 9 (*Prince of Wales*) thus became the only steam locomotives ever to wear the BR Rail Blue livery and double-

Above: BR blue-liveried Vale of Rheidol Railway 2-6-2T No. 7 *Owain Glyndwr* makes a smoky departure from Aberystwyth on 2 June 1971, while a Class 24 diesel stands in the main line platforms; it appears to be No. 5087, which Bachmann has produced in OO scale. Photo: Colour Rail

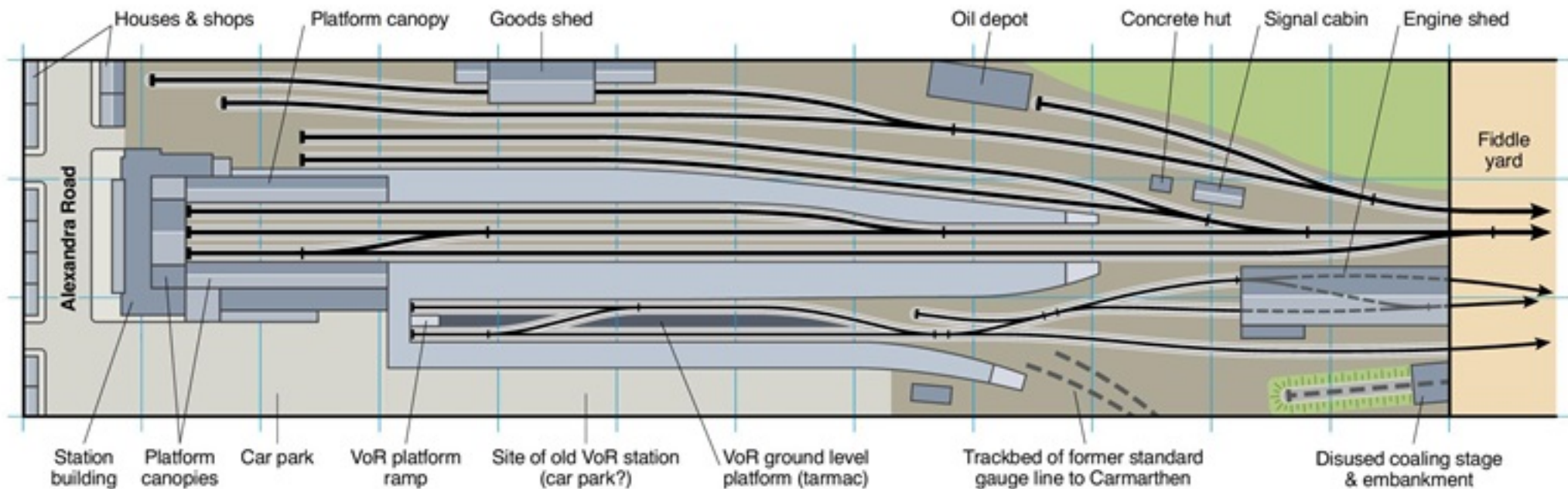
arrow logo – while the GWR-built coaches also received all-over blue to match. It stirred controversy among enthusiasts at the time, and even now, some consider it a period of the line's history that is best forgotten – but it would certainly make for an out-of-the-ordinary model...

What also makes it a good modelling prospect is the fact that, by the 1970s, the VoR had been re-routed to run into the bay platform at Aberystwyth that had formerly served the line from Carmarthen (closed in 1965). This provides the modeller with a relatively compact terminus, which fits a classic idea for a main line station with an interchange for a steam railway... except that in this case, the latter is still part of BR. With locomotive-hauled passenger and freight traffic still operating on the Cambrian Coast line in this period, mainly worked by Class 24 and 25 diesels, and the former standard gauge steam shed now in use by the VoR, it would make a fine tribute to a small but significant piece of BR history.

Our 4mm scale plan depicts this period, with some compression of the layout to fit into a scenic area of 12' x 3', but still allowing trains of four coaches to fit comfortably into the VoR platform (five or six coaches being the typical train

ABERYSTWYTH

Overall scenic dimensions: 12' x 3'. Each grid square = 1ft x 1ft.



length in reality). The locomotive shed, and its disused coaling stage and embankment, provide a view blocker to hide the entrances to the fiddle yard, while the layout of the goods yard has been simplified and the oil depot moved slightly closer to the goods shed. Note also that the plan does not include the additional canopies which formerly covered just under half the length of the platforms, as these were removed in February 1971.

What will make it a real 1970s period piece is the station car park at the front of the layout, plus the site of the VoR's old station; photographs taken in the late 1960s show the area in front of the latter being used for car parking, so there is plenty of space to display a collection of Morris Marinas, Austin Maxis and Hillman Avengers to set the scene.

London Transport

Four years after the last Bulleid Pacifics had left Nine Elms, Londoners could still hear the sounds of steam locomotives in the capital – albeit usually only at night. These were the GWR '5700' class pannier tanks purchased from BR by London Transport, the last of which would survive until 1971 on engineering work.

Opportunities for modelling the LT network have increased hugely in recent years, with the release of proprietary tube stock (Bachmann) and Metropolitan electric locomotives (Heljan). The red-painted panniers themselves have featured in the OO gauge Bachmann catalogue for many years, and more recently have also become available in O gauge from Dapol, and in the N gauge Graham Farish range.

With the panniers having roamed as far afield as Watford and

Right: Dapol's N gauge '5700' finished in London Transport red as No.L94 (originally GWR No.7752) which worked the last steam-hauled LT train on 6 June 1971.



Above: **Steam is still alive and well in the heart of London on 13 June 1968 – and such scenes would continue until 1971. London Transport '5700' O-6-0PT No.L92 shunts the BR sidings alongside the West London line at Lillie Bridge, with a rake of BR Mk.I horseboxes in Southern Region green on the right. Photo: Colour Rail**

Upminster, there are plenty of choices for locations to model, but perhaps the ideal spot would be Lillie Bridge depot alongside the West London line, or the nearby BR sidings pictured above.



Right: An Ulster Transport Authority Multi-Purpose Diesel unit at Belfast York Road shed in May 1970, with several LMS (NCC) Class WT 2-6-4Ts visible.
 Photo: Jonathan Allen

Northern Ireland

Much more challenging to model than the LT panniers is another stronghold of steam in the British Isles that must not be overlooked. LMS-design 2-6-4Ts remained in revenue-earning service on Northern Ireland Railways into the 1970s, and the last steam-hauled passenger train ran on 31 March 1970.

After this, the WT class locomotives were used for stone trains until May that year, in connection with the building of a new motorway in Belfast, and one (No.53) eked out its days as the station pilot at Belfast York Road until June 1971. As detailed by Colm Flanagan in RM June 2006, it is possible to model these locomotives by converting the Hornby Fowler 2-6-4T, the design on which the WT was closely based.

Industrial steam

Here, the modelling possibilities are almost endless, for many industrial concerns continued to use steam into the 1970s – and, in a few cases, well beyond. These included quarries (such as Nassington ironstone quarry in Northamptonshire, and Associated Portland Cement Manufacturers in Kent), dockyards (such as Par and Falmouth), and power stations (such as Castle Donington, whose bright blue RSH 0-4-OSTs would, amazingly, survive in service until 1990), plus, of course, the National Coal Board.

Then there were the ex-BR locomotives that had entered industrial service, with examples from all of the Big Four still to be found in the 1970s. Several GWR-design pannier tanks were sold to the NCB, including Nos.7754 (at Mountain Ash Colliery) and 9600 (Merthyr Vale), and the trio of '1500s' (Nos.1501/2/9) at Coventry Colliery. From the Southern,



Above: An LMS (Northern Counties Committee) Class WT 2-6-4T, converted from a Hornby Fowler tank by Colm Flanagan.



Above: Dapol's O gauge model of '5700' pannier tank No.7754 in NCB green; this was a limited edition, but the firm has also previously released it in N gauge, and Bachmann has produced a OO gauge limited edition for Modelzone.



Left: No.7754 in the shed at Mountain Ash Colliery on 5 May 1975, along with Andrew Barclay 0-6-OST No.2074 Llantunum Abbey. Photo: Rail Photoprints

ex-LSWR B4 0-4-OT No.30096 worked for fuel merchants Corralls at Southampton until 1972. The last LMS-design locomotive in commercial service in mainland Britain was Fowler 3F 0-6-OT No.47445, which ran at Craggstone colliery near Wakefield until 1970; while several LNER J94 0-6-OSTs moved to the NCB after withdrawal from BR, with No.68078 working at Widdrington in Northumberland until at least 1975.

However, ready-to-run ex-BR engines, while certainly nice to have, are by no means the only option; with several industrial designs having also appeared in R-T-R form in recent years (Hornby's Peckett 0-4-0 and 0-6-0 saddle tanks, and Hattons' Andrew Barclay 0-4-OSTs, plus the ubiquitous Austerity 0-6-OSTs) it's now easier than ever to model such locations.

We settled on Dunaskin coal washing plant, on the NCB's Waterside Colliery system near Dalmellington (part of which is now home to the preserved Doon Valley Railway). In 4mm scale, this would utilise Hattons' Barclay 0-4-OST in NCB Ayrshire lined black (ref.H4-AB16-001) and Class 26 and 27 diesels from Heljan.

Modelling the entire plant and its surrounding sidings would require a lot of space, so on a 10' x 1' 6" board, we've fitted in a smaller OO gauge layout somewhat akin to Dunaskin, and accordingly christened it 'Dunakin Washery'.

At the real location, the NCB sidings and the washing plant are on a higher level than the BR track; our plan is effectively a mirror reversal, using the washing plant to hide the entrances to the fiddle yard, but with the BR line placed on a higher level to keep it visible. The number of sidings under the plant has been reduced from six to five, and parts of the plant itself appear similar to the Biltzezi B2 Dairy, Factory or Goods Shed card kit, which could be used as an aid to scratch-building.

Photographs of the Waterside system (search for 'Waterside Colliery coal washery' at www.canmore.org.uk) show the steam

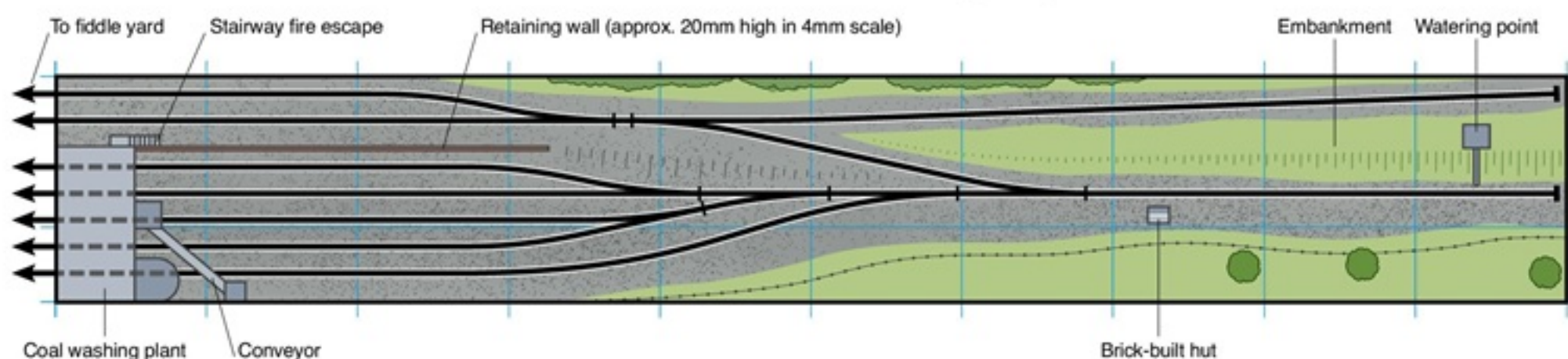


Above: **Peckett 0-4-OST No.1530 shunting in Falmouth Docks in April 1970. The locomotive remained at work here until 1986.**
Photo: Rail Photoprints

locomotives filling up from a delightfully ramshackle watering point perched on an embankment, so we've placed this structure at the other end of the layout for a little more visual interest. The headshunt has been designed to accommodate a Sulzer Type 2 diesel and six BR 16-ton mineral wagons, so would also be able to take a Barclay 0-4-OST and the same load – taking into account that these locomotives appear to have run at Waterside with internal-user NCB wagons coupled behind as makeshift tenders!

DUNAKIN WASHERY

Overall scenic dimensions: 10' 0" x 1' 6". Each grid square = 1ft x 1ft.

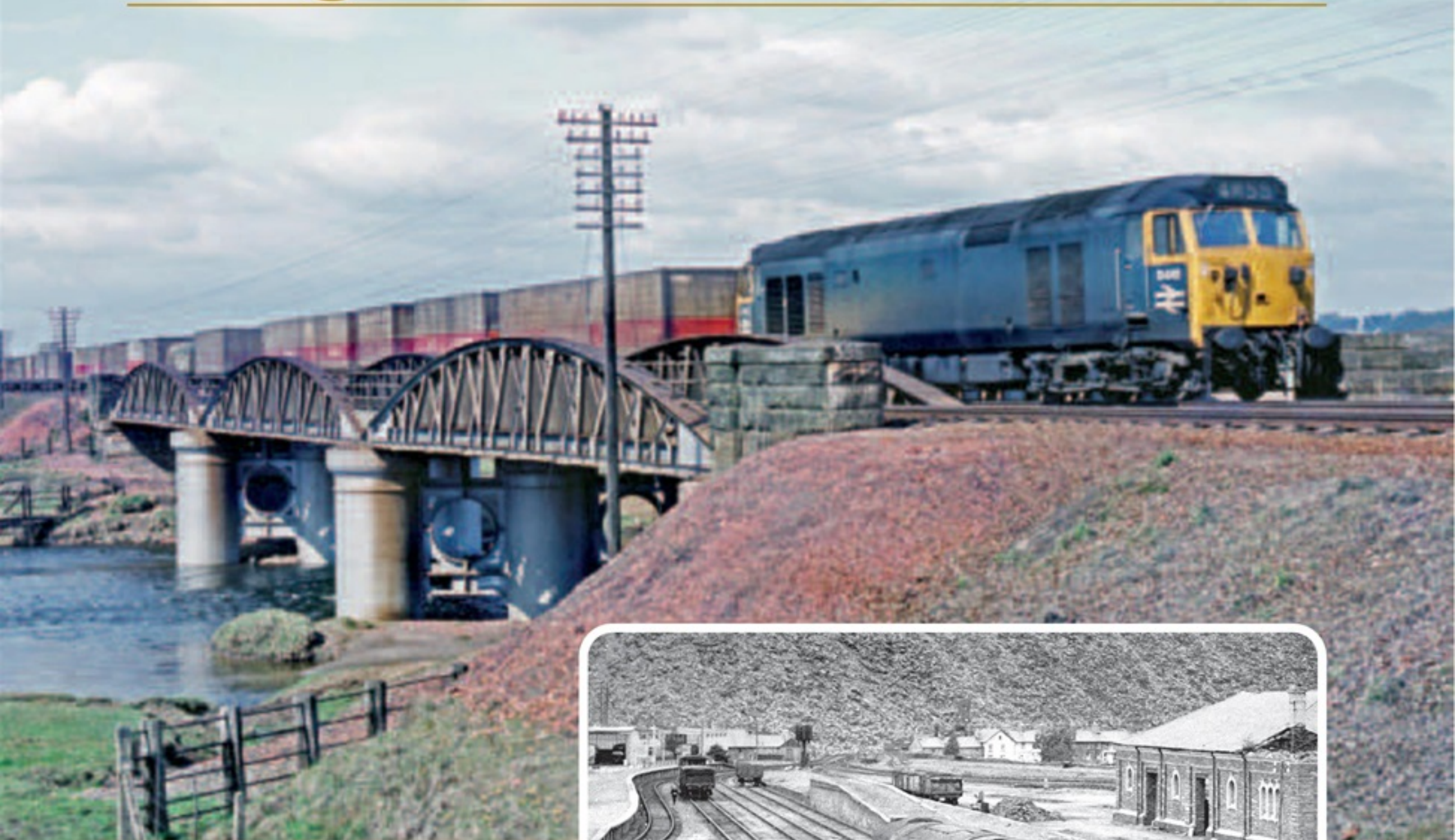


Above: **NCB West Ayr Area Andrew Barclay 0-4-OST No.21 in the sidings at Dunaskin Washery on the Waterside Colliery system, while Sulzer Type 2 D5408 passes with a trainload of coal for Ayr Harbour on 27 January 1970.**
Photo: Rail Photoprints



Above: **Hattons has produced a suitable Andrew Barclay 0-4-OST in OO gauge, in NCB lined black as Ayrshire Area No. 10.**

The 1970s railway freight scene



Back in the early days of the British Railways committees, another staff group had been drawn together to consider freight rolling stock provision. This was the Ideal Stock Committee which went on to initiate a grand building programme of brand new wagons – BR standard wagons – to replace the life-expired vehicles inherited from the Big Four companies and private owners in 1948.

The committee did not consider how the nature of freight traffic might alter in future years; instead it simply enacted a mostly like-for-like programme of replacements.

In essence, the Ideal Stocks Committee remained wedded to the concept of wagonload freight and short-wheelbase wagons. Hence many new BR standard wagons looked just like their earlier counterparts, but with various updated components like buffers, couplings, axleboxes etc.

Some new designs of larger capacity minerals and hoppers were included in the programme, mainly for traffic between industrial sites, with the 'standard' 16T mineral ostensibly being reserved for domestic coal transhipped into smaller depots.

Construction and replacement continued unabated, with the agenda modified only slightly by the publication of the 1955 plan.

Above: **The old order** – a green-liveried Sulzer Type 2 prepares to shunt the coal siding at Blaenau Ffestiniog in August 1970. This was just the sort of traffic highlighted in the Beeching report as being financially unviable. Perhaps we might agree? Photo: Steve Flint

It proposed that all freight wagons were to be fitted with continuous brakes – the automatic vacuum brake in this instance – either new or retrofitted. In the event though this was never accomplished completely, and right up until the 1970s both fitted and unfitted vehicles – mostly mineral wagons and hoppers – were still commonplace around the network.

Short-lived marshalling yards

In an attempt to make the transit of wagonload freight more efficient the 1955 plan inaugurated a strategy to construct huge



Left: **The new look Freightliner block trains became more commonplace throughout the review period and specifications for container types and wagon flats gradually altered to be in line with the international standards we are familiar with today. Here, a train of the original 1960s-styled containers is hauled south from Carstairs and across the River Clyde by pre-TOPS numbered D440 in 1970.**
Photo: Derek Cross/Rail Photoprints

shunting hubs - semi-automated marshalling yards - at key locations around the network. These new yards, which took many years to commission, were launched with great optimism but were effectively almost defunct by the time they were brought into use due to a combination of changes brought about following the 1959 reappraisal report and the Beeching report.

As highlighted on page 4, under the Beeching recommendations many of the sources and destinations of financially unviable wagonload traffic were to be axed and instead, the focus was to be on those aspects of freight movements deemed to be profitable and much more suited to rail haulage than road.

Hence the sporadic and irregular movements of freight between public goods sidings, often in single wagons requiring lots of re-marshalling en route, were phased out whilst expansion of containerisation (eg the new Freightliner trains) and block train movements befitting of coal, oil products and aggregates became prioritised. These latter commodities provided mostly regular flows of traffic between private railheads, such as mines to power stations, or oil refineries to regional distribution terminals, etc.

Concomitant with this transformation, a boom in the construction of many larger capacity tank and hopper wagon designs occurred from the mid-1960s onwards. Most of these were built by outside contractors and financed by wagon leasing firms – customers thus hiring the vehicles and paying the nationalised railway a carriage charge, in much the same way as the modern privatised railway functions today.

Meanwhile in 1965 BR and the Central Electricity Generating Board cooperated to develop the merry-go-round method of supplying the new breed of coal-fired power stations. Fleets of the new 32T coal hopper wagons were constructed for these services, and all were fitted with air-powered brakes from new.

Above: **A scene at Stainforth and Hatfield station on 3 March 1979 illustrating the mix of old and new wagon types which BR was still handling. Examples of unfitted, vacuum-fitted and air-braked stock are in view. Newly rebuilt Bogie Bolster Ds form the train of steel slabs hauled by 47 288, whilst in the distance under the Hatfield Colliery loading gantry is a rake of 32T MGR hoppers, both these latter sets being air-braked.** **Photo: Steve Flint**

From then on air braking became the standard for all new freight vehicles. The upshot of this further change (barely 10 years after the decision for a universal adoption of vacuum brakes) was that by the 1970s three different braking systems were in regular use on freight trains:

- a) Unfitted:** loose-coupled trains without continuous automatic brakes which almost always required a brake van.
- b) Vacuum fitted:** trains with continuous automatic brakes that could operate without a brake van.
- c) Air-braked fitted:** also with continuous automatic brakes that could operate without a brake van.

Wagons of all three brake types could be marshalled in the same train, but, without sufficient continuous braking provision such a consist would be classed as an 'unfitted train' and thus would require a brake van.

Some diesel locomotives were converted to dual-braked – air and vacuum systems – though only one or the other would be operable in a mixed rake.

Government grants

In a further increase to develop the train-load concept, from 1975 the Government made Section 8 grants available to companies for the installation of sidings on private land to create freight handling terminals – often referred to as Railheads – specific to their traffic type. The grants also provided for the construction of appropriate wagons, a popular example in the 1970s being the introduction of the Private Owner four-wheel aggregate hoppers, such as those



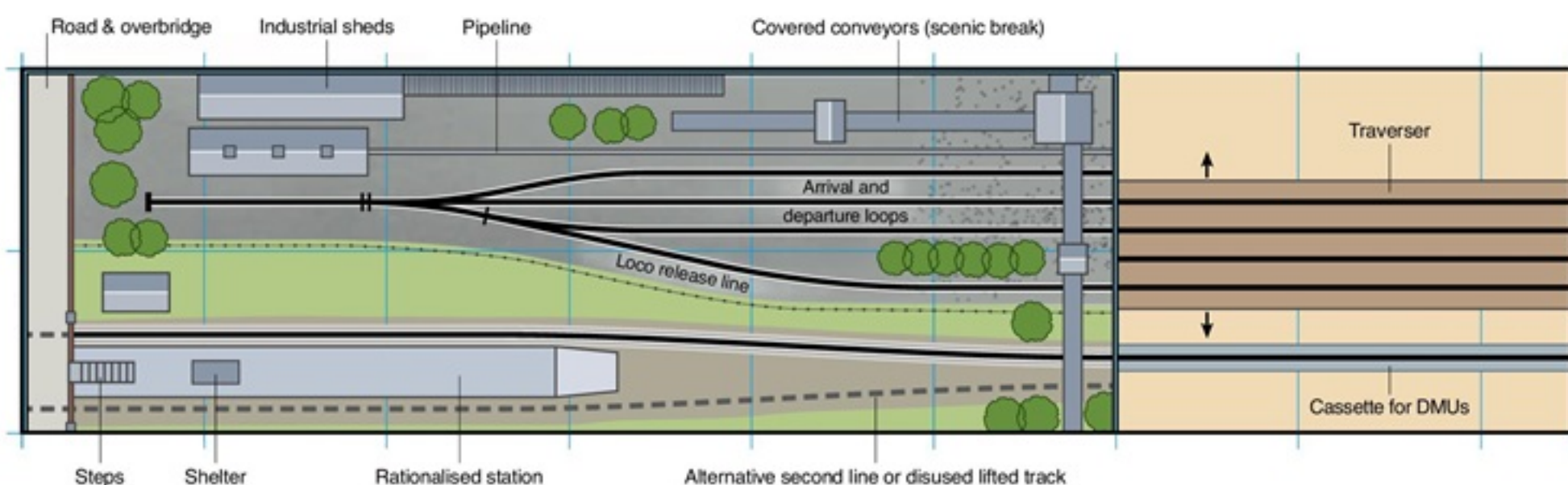
Colliery sidings

A scene at Silverdale Colliery, Staffordshire in August 1970 prior to it being substantially rebuilt and modernised. New MGR hoppers rub shoulders with 16T minerals and even older 7-plank wooden opens, presumably part of the internal user fleet. The shunter is a Bagnall 0-6-0DM which carried the name Hem Heath No.4. This view inspired the layout plan below which shows one end of the arrival and departure loops of a typical colliery. The idea is that only the front or rear halves of long trains appear in view, the covered conveyors acting as a scenic break. To add additional interest a separate 'rationalised' single line passenger stop is included across the front of the layout, though provision for a run-off would be needed at the left hand end. It could also be re-doubled if desired.

Photo: Steve Flint

COLLIERY & PASSENGER LINE

Overall dimensions: 8' 6" x 2' 0". Each grid square = 1ft x 1ft.



designated PGAs under the TOPS system. Ironically the development in this traffic was mostly due to a construction boom in the 1970s which also saw the UK motorway network greatly expanded.

Mixed braking equipment

The decade also saw British Rail finally begin to break away from the box van and open wagon designs that owed their ancestry to the early 20th century pre-Grouping types.

Beginning in 1971 the now familiar long-wheelbase vans and opens began to appear in minor design variants. These were all air-braked and were to form an 'Air Braked Network' of services on which customers could book wagon transits in advance. This later formed the backbone of the Speedlink services in the 1980s, but as their introduction occurred steadily throughout the review period both new and traditional wagon types could be seen running together.

In consequence the new vans appeared in mixed-brake freight trains marshalled as unfitted vehicles usually between

Left: 08 010 shunts a pair of long-wheelbase vans in the sidings at Grantham in May 1978. Coded under TOPS as VCA these were the forerunners of the familiar VDA air-braked vehicles. Just discernible on the far-end doors are examples of the early and later Railfreight branding insignia. Photo: Steve Flint

Right: In the mid-1970s the Settle and Carlisle line became the preferred route of Anglo-Scottish mixed freights after the inauguration of full WCML electrification. As a consequence it was possible to see many trains composed of goods vehicles with incompatible braking equipment. On a wintry 2 April 1979 one such mixed freight is seen approaching Ais Gill behind 40 156. The train is composed mostly of 16T minerals carrying scrap, but at least three VDA vans can be spotted in front of a shorter cut of unfitted minerals, hence the obligatory guard's van bringing up the rear to comply with braking rules. Photo: Steve Flint

the vacuum-fitted and unfitted cuts of wagons. Whether the VDAs were merely empties in transit between yards, or actually in revenue service has not been established, but they made up into interesting mixed freight trains in which the wagons were arranged in a particular sequence to meet braking regulations. Two such examples are illustrated here, though whether this was mandatory across the network has yet to be confirmed.

Traditional pick-up freight trains, so beloved by the modeller, were however well in decline by this time, though some could still be found hanging-on at numerous localities around the UK during the 1970s. Domestic coal was by far the last bastion of this type of train. It actually survived in various part-modernised guises well into the 1980s, but the manually-emptied 16T mineral wagon in a local rail-served coal yard was almost defunct by the end of the review period. One particular example – the coal depot at Rawtenstall – wasn't closed until 1980 and gives rise to the possibility of incorporating a traditional coal yard on a 1970s-themed layout (see page 21).

Rich variety

We can discern, once again, that the various management decisions and U-turns taken up to 20 years previously had a consequential effect on railway freight operations during the 1970s. From a model builder's perspective though, the huge variety and operating complexity of British railway freight trains, as was played out day after day in our time-frame, makes for many extraordinary and interesting modelling opportunities.

Right: 26 043 heads a mixed freight away from Huntly in September 1979. The train is composed firstly of a rake of AFI vacuum-braked grain hoppers, followed by two air-braked VDA vans, the front one of which is freshly turned out in the then new flame red and grey livery. The air brakes on the VDAs would be un-operable but the through vacuum pipes fitted to them would enable the brakes on the next three vacuum-fitted minerals to function. The last wagon in grey is an unfitted 16T mineral, and this time an LMS-designed brake van rounds off the consist. Photo: Steve Flint



1970s passenger trains worth modelling

The term 'worth modelling' is a subjective one of course. It will be dependent on one's preferences and personal experiences of travel therein, so, before we wax lyrical about personal preferences, let's consider the state of passenger services at the beginning of the decade, and how it changed as the ten-year span rolled on.

By the start of the 1970s, passenger train formations could be considered conveniently in three distinct groups as follows:

- a)** Locomotive-hauled carriages. These were deployed on main line and inter-regional services, with frequent use of older carriage stock on seasonal holiday excursions, football specials, and so on. Hilly and remote areas of the network where DMUs were unsuitable, such as north and west Scotland, also saw locomotive-hauled trains throughout the decade.
- b)** De-luxe DMU/EMU sets. Found on specific inter-city and cross-country routes where a higher standard of passenger comfort and facilities was expected. Sets did find occasional use on fill-in turns and specials outside of their rostered routes.
- c)** Standard DMU/EMUs. Basically utilised on secondary provincial routes, branch lines, and suburban services radiating from larger cities; the latter usually with high density stock, or geographic specific sets (such as the electric networks around Glasgow, Liverpool, Manchester, etc).

Most of the passenger stock running on 1 January 1970 originated from the proposals contained within the various reports, plans and revisions set out in the first years of nationalisation (see page 2), though by then many early-built examples, such as the pre-TOPS DMU classes and the less successful pilot scheme locomotive designs had been, or were about to be, withdrawn.

Even so there was still sufficient variety around to offer a choice of some novel and interesting modelling opportunities, especially so for those possessing insufficient space to accommodate full-length double-headed passenger trains.

In this section therefore, and notwithstanding the stunning opening view by Derek Cross, we will take a look at some alternative themes covering 1970s passenger services that can be modelled authentically without the need to compromise too much on train length.

Right: Third-rail EMUs do make for agreeable alternatives to the more customary DMU-stocked layout and the Bury branch with its two-car Class 504 sets is an ideal example. M77177 leads this set at Bury Bolton Street in 1975. Photo: Rail Online



Above: In the days before electrification reached Glasgow, many WCML passenger expresses were powered by double-headed EE Type 4s (later Class 50s) to/from Crewe. Pre-TOPS numbered D407 leads just such a pair on the down 'Royal Scot' near Lamington in June 1970. Photo: Derek Cross/Rail Photoprints

The Manchester-Bury line

The Lancashire and Yorkshire Railway was a pioneer in electric traction for many of its suburban routes and its Manchester to Bury line began electric train services in 1916. In 1959 new British Railways EMU units were built at Wolverton for use on the branch which had a unique side-contact third-rail power supply. These were operated as two car-sets (Class 504) throughout the 1970s in Rail Blue livery and provided a frequent suburban commuter service. A kit was available from DC Kits until recently, although the cab mouldings can still be obtained (www.dckits-devideos.co.uk).



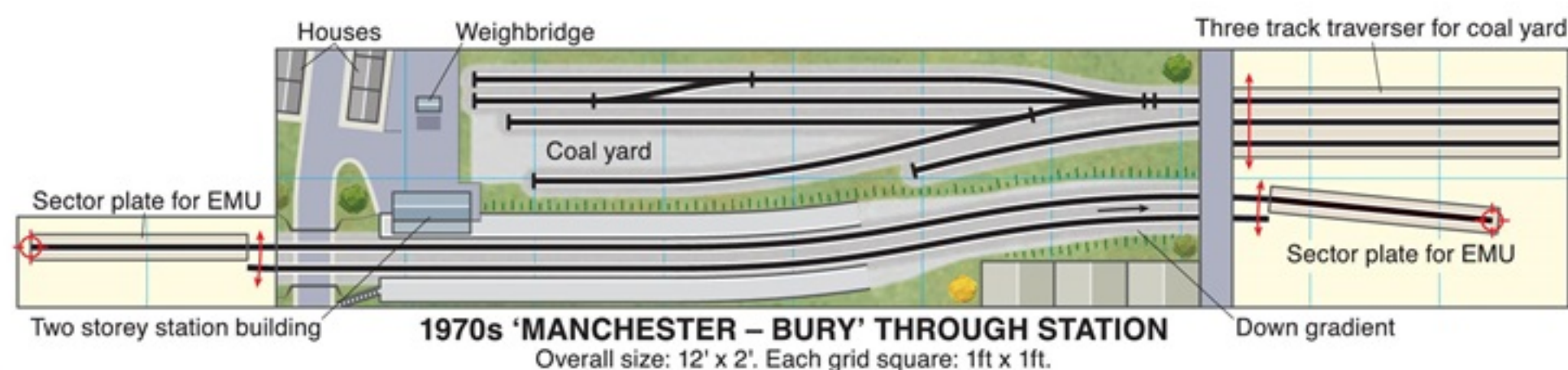


The Manchester-Bury line

This layout plan is loosely inspired by the intermediate Bury line station at Radcliffe which had possessed, prior to our review period, an adjacent goods yard to the east of the station on a slightly lower level. Hence the twin-level design of the proposed scheme which keeps goods traffic separated from the third-rail system.

The station itself was rebuilt in 1959 in the style of the times with less grandiose two storey buildings and canopies. They were in use throughout the 1970s, as seen here with a Manchester-bound Class 504 on 24 May 1978. Even today the steel framework is still extant on what is now a stop on the Manchester Metrolink network.

Photo: Steve Flint



Above: As an alternative to the Bury sets, this Southern Region 2-BIL unit dating from 1938, as photographed at Guildford in July 1970, survived long enough to operate in Rail Blue livery. A ready-to-run model is available in the Hornby range. *Photo: Rail Photoprints*

During our review period surviving freight traffic could have been routed along the branch to reach the coal depot at Rawtenstall (which was still operational until 1980) although an alternative non-electrified route via Castleton S & W Junctions on the Rochdale line would have been more likely.

However a possible twin-level layout scenario combining a station and a still operational low-level coal yard is shown in the accompanying layout suggestion.

If the Bury branch doesn't appeal, the scheme could be readily adapted for any two- or three-car EMU service. There are plenty of examples to choose from across the UK, including Southern Region 2-EPBs, 2-HALs and 2-BILs, the latter just scraping into the review period, and not forgetting the three-car North London Line units which could provide a thought-provoking urban scheme set somewhere between Broad Street and Richmond.

Right: The London District three-car sets are somewhat geographically restricted but can be modelled by cutting down Bachmann 2-EPB units to fit the shorter 57' underframes. One such Class 501 set was seen at Old Oak Junction in 1978. *Photo: Rail Photoprints*

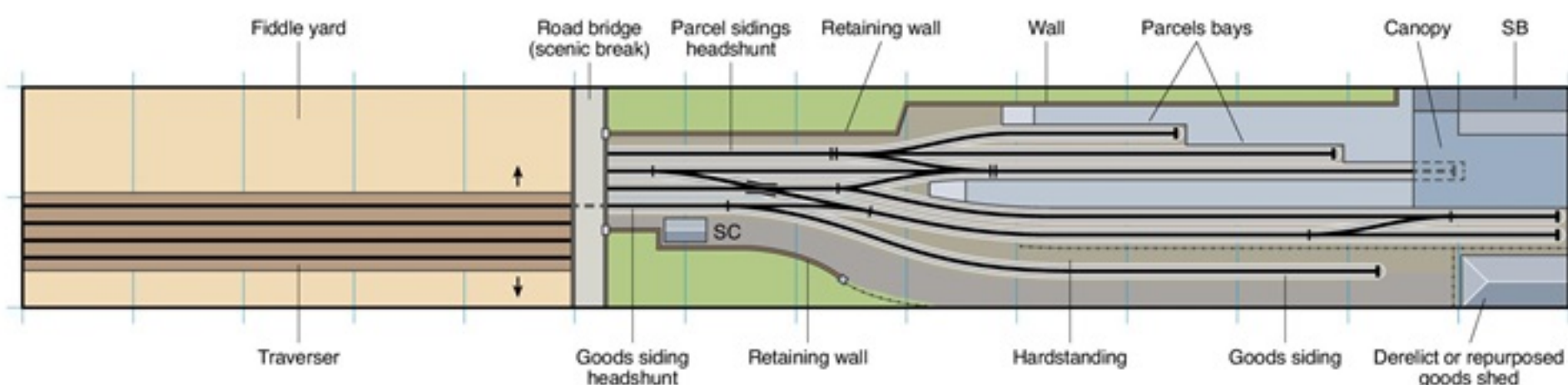


A 1970s provincial terminus

This scheme represents a smaller provincial town terminus served by one (or more) of the split main line trains referred to in the main text. As it stands it is just long enough to accommodate a four coach rake and locomotive. It also enables secondary DMU operations and includes a couple of parcels bays and a hardstanding area (supposedly where the former goods yard may have been) for either coal deliveries or some spot traffic in standard vans.

If built as a portable/exhibition layout (as drawn), it does require some lengthy fiddle yard provision in the guise of a traverser. Consider though if fitted around the walls of a spare room with a 180° curved section installed, the fiddle yard would fit neatly along the opposite wall.

Overall dimensions: 14' x 2'. Each grid square = 1ft x 1ft.



Split loco-hauled services

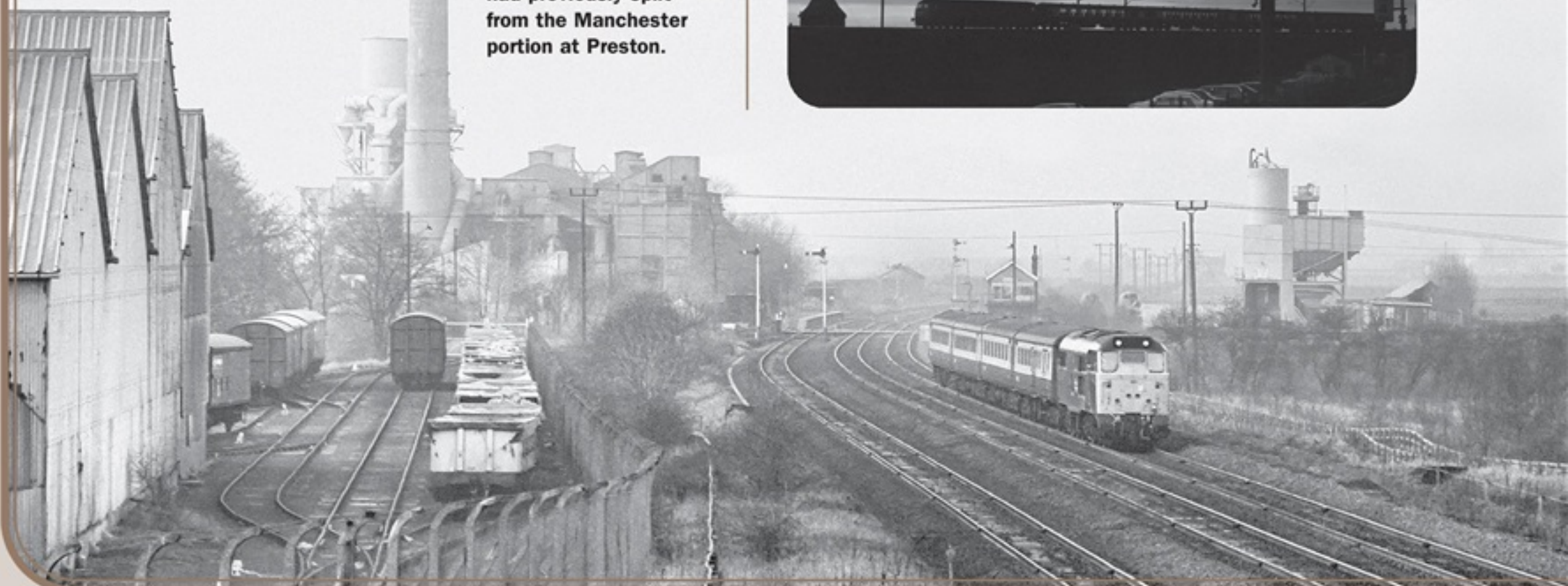
Today on the rail network it is very easy for long DMU or EMU trains to be split into two shorter trains at intermediate stations so as to serve two separate destinations. Providing that the train is formed of two or three fixed rake sets, it is simply divided at an intermediate driving end and a second driver is deployed.

This quite sensible operational procedure was a frequent feature of the steam railway, though you needed a second locomotive as well as extra crew.

The practice was still employed during the diesel age, though less frequently, but typical examples from the 1970s include Inverness-Glasgow/Edinburgh, Glasgow-Liverpool/Manchester and London Kings Cross-Hull/Bradford.

Below: **The Hull portion of the London KX – Hull/Bradford mid-day service heads away from Melton Halt in January 1978.** Few traces survive of the halt and the once busy rail-served industrial sidings on the left. **Photos: Steve Flint**

Right: **Glimpsed from Wigan Wallgate station on a darkening 14 September 1976 is an unidentified Class 47 heading the four-coach Liverpool portion of an ex-Glasgow WCML service which had previously split from the Manchester portion at Preston.**



The latter was a mid-day service and would be split at Doncaster with the train loco heading the Bradford portion, and typically, a Class 31 and four coaches heading for Hull. By the late 1970s the rake was usually composed of air-conditioned Mk.IId coaches representing main line carriage stock in a short formation ideal for modelling in smaller spaces.

Moreover models of all the elements in the train are either currently available, or soon with new upgraded versions planned from the mainstream manufacturers.

Mixed parcel/passenger trains

These proved to be rare beasts during research for this feature, with few photographs turning up on the usual picture libraries.

We are looking here at parcels trains that include some passenger accommodation, often just a single coach in the consist. They operated between provincial centres, principally as parcels and newspaper services and usually overnight.

The writer is confident to recall travelling on them as timetabled passenger trains in the early 1970s, but confirmation that these were in fact included in the public timetable has been elusive.

It has been suggested that the addition of passenger coaches was to make provision for staff who would act as 'sorters' en route; they would then travel back by normal service train. This may have been the case on the main lines, but certainly in the far-flung corners of the network, parcel vehicles could sometimes almost equal the number of passenger coaches in a given train.

Certainly such trains ran in our review period and, either way, provide modellers with yet another 1970s-style short train worth modelling. Example services were noted on the Newcastle-Carlisle, Exeter-Barnstaple and York-Hull routes. The illustration below shows a Cleethorpes-Sheffield Midland mixed parcel/passenger service, the coaches having earlier been used on an



Below: **EE Type 4 No.419 runs into Penrith with a short parcels train on 3 March 1973. The first coach is a passenger vehicle possibly making it a mixed service. Electrification columns have been erected in this view but the old lamp standard and London Midland Region running-in board are still in situ. Photo: Rail-Online**

overnight newspaper/passenger train from Manchester Piccadilly to Cleethorpes.

Accordingly the provincial terminus plan opposite includes platform areas for the loading of parcels vehicles and interestingly, there were still numerous older Big Four parcels vehicle designs in service well into the review period.

Below: **40 044 rolls into Retford (lower) c.1976 with the Cleethorpes - Sheffield Midland mixed passenger and van train mentioned in the main text. Photo: Rail-Online**





Above: On 2 March 1979, 08 392 in the headshunt of Scunthorpe yard prevents a clear view of a three-car Class 101 DMU as it heads east from the station with a single CCT in tow.

Multiple unit 'Swingers'?

Another take on the mixed passenger/parcels train is the coupling of parcels vans to the rear of a DMU as tail traffic. These were known in the north as 'swingers' though that probably was not their official title!

The vans were usually four-wheel CCTs, but single bogie vans have been recorded on occasions, even with single-engined two-car units.

On arrival at their destinations the vans would be detached and, it is generally assumed, would be shunted by a station pilot, though anecdotal evidence suggests that they were also shunted by the DMUs. Nevertheless they are short trains which are very rarely seen modelled and would be equally at home on the provincial terminus plan.

At this point it's worth noting that for most of the decade the low-density DMU sets which operated on secondary services appeared in all-over blue livery, but towards the latter half of the period refurbished units were appearing in white with a blue waistline stripe. This was the first break with the corporate



Above: Another three-car Class 101 with two utility vans as tail traffic is seen heading west at Hesse Haven on the evening of 25 June 1976. Both photos: Steve Flint

image as far as passenger train colours were concerned, though the livery was short-lived and by the early '80s refurbished vehicles had the standard blue and grey applied.

So, there we have a small handful of not-so-typical passenger workings from the 1970s which offer unfamiliar options to modellers, especially where layout space is limited in the home.

The main line passenger challenge

The top grade passenger train of the decade was, of course, the High Speed Train – the Inter-City 125 – which, along with other full-length locomotive-hauled main line services, usually require some significant baseboard space if modelled in OO and larger. A spacious attic, garage or shed-sized railway room – or an exhibition scheme – is a prerequisite.

N gauge is well suited in respect of depicting main line scale

Below: The HST probably contains the newest of any vehicles in this delightful period panorama as it runs into Bath Spa while forming a Bristol-Paddington service on 11 December 1977. Photo: Rail Photoprints.



length trains and ready-to-run models of most 1970s types are, or have been, available from the manufacturers, though not necessarily in the blue/grey livery of the 1970s.

Looking again at 4mm and the larger scales, there have been novel approaches to tackling the conundrum of dealing with long trains in small spaces.

One such scheme built a few years ago by Hughie Flynn took the idea of a half-station on scene, with the other half off scene. Here shorter trains (DMUs, parcels, etc) could run straight through but long main line trains terminated. Only the locomotive and the first three coaches of the main line train appeared on scene. The loco would uncouple and head away to a remote stabling point leaving the coaches to be hauled off back the way they came, ostensibly by another loco attached to the opposite end of the rake off scene.

All rather smoke and mirrors, but at least main line engines and stock could be dealt with on a small layout, and the theme can be applied to any region you choose: so Westerns and Warships can be accommodated in one session, Deltics and 40s in another. It could work equally well with 25kV electric locomotives, HSTs and even steam specials which returned to the tracks as the 1970s progressed.

And finally...

The remoter highland areas of Scotland, parts of Wales, and the West Country continued to see short locomotive hauled passenger trains throughout the review period.

Some, such as the West Highland and Far North line services were never replaced by DMUs (until the arrival of the second generation sets around 1989) and make for ideal small layout schemes. We make no apologies therefore for closing this

Wellington Road



Above: **Hughie Flynn's 1970s Eastern Region layout in which main line trains could be featured in a small space, as described in the main text. Photo: Steve Flint**

section with yet another view of Kyle of Lochalsh showing 26 032 shunting the mid-morning passenger stock which included a pre-Nationalisation Stanier-designed bogie van.

This picture was taken on 14 September 1979 and in a few weeks time the decade would roll over, and the years which were to follow would witness many more profound changes to Britain's rail scene. But that is for a future booklet.

Below: **Even by late 1979, Kyle of Lochalsh terminus looked much the same as in Highland and LMS days – only the locomotives and stock had been updated! This too would change soon though, making this sight a fitting end to our review of British Rail operations during the 1970s. Photo: Steve Flint**



The growth of the preservation movement

Although the concept of railway preservation began on the narrow gauge in the 1950s (with the Talylyn and Ffestinog railways), and on the standard gauge in the following decade (with the Middleton, Bluebell, Keighley & Worth Valley and Dart Valley lines), the 1970s was arguably the decade in which the movement became truly established and began to gather real momentum – particularly with the return of steam to the main line in 1971.

Like the working steam survivors discussed in Chapter 4, the 1970s preservation industry offers a stark contrast to the BR scene of the period – not least in the juxtaposition of weathered BR blue diesels and shiny restored steam locomotives, the latter usually decked out in pre-Nationalisation or pre-Grouping liveries, or even a heritage line's own house colours. A preservation-based layout set in this period could be full of interest, and evoke as many fond memories as any model depicting the BR steam era.

Main line steam

At the beginning of the 1970s, the main line steam ban imposed by BR in 1968 was still in force, and the only locomotive that had been exempt from it – *Flying Scotsman* – was touring the United States. Nonetheless, there were many examples of preserved locomotives unofficially breaking the rule, and occasions on which some were given permission to run within the confines of yards



Above: **GWR King 4-6-0 No.6000 *King George V* accelerates out of Paddington station on 1 March 1979, closely followed by a blue and grey High Speed Train. Photo: Colour Rail**

and sidings, for events such as open days – which could provide an ideal theme for a diesel depot layout.

Just one year into the decade, however, came the triumphant return of steam to the national network, in the form of GWR 4-6-0

Below: **A Cravens Class 105 DMU departs from Chappel & Wakes Colne station in the mid-1970s, while the Stour Valley Railway Preservation Society occupies the goods yard. RSH 56 class 0-6-0ST No.60 *Jupiter* is on the left. Photo: The Transport Treasury**



No.6000 *King George V*'s 1971 trial run with the Bulmers-liveried Pullman stock. By the end of the decade, the same locomotive had brought steam back to a London terminus (Paddington, in 1979), and BR had approved many more engines for operation on an increasing number of routes – not least the Settle & Carlisle, where LNER V2 2-6-2 No.4771 *Green Arrow* was the first to run in preservation, in 1978. There is thus plenty of scope for main line layouts set in this period to feature celebrity locomotives such as *Clan Line*, *Leander* or *Evening Star*, at the head of railtours formed of blue and grey Mk.I stock.

1975 brought preservation's biggest event yet, and indeed one of the biggest in railway history – the cavalcade at Shildon to celebrate 150 years since the opening of the Stockton & Darlington Railway. With 35 locomotives ranging from the replica *Locomotion* to the prototype High Speed Train, this would surely be worth recreating in model form – and, such have been the changes wrought on Shildon in the intervening years, a historic period piece in itself.

There was one other aspect of preserved steam on the main line in the 1970s that is already somewhat forgotten, but would also make an interesting cameo scene on a main line layout, and could be modelled easily. In the early days of the Barry scrapyard rescue operation, many of the locomotives departing Dai Woodham's famous yard for preservation were moved to their new homes by rail, towed by a diesel and sometimes in convoy – until the deteriorating state of the engines in the yard prompted BR to call a halt to this practice in 1976. By then, all sorts of locomotives from GWR Prairies to BR Standards had been transported in this fashion; so, with suitable weathering and inscriptions reading 'Sold to...', this offers a role for elderly models that might otherwise be cast aside.

Preserved railways

In much the same vein, even though there were far fewer heritage lines in the 1970s than today, and not all of those that did exist



Above: GWR 4575 2-6-2T No.4588 at Paignton on 13 June 1975. Photo: Colour Rail

Right: Goodrington Sands Carriage Sidings, just south of Paignton, offers a ready-made cameo scene on which to run BR blue diesel-hydraulics and preserved GWR locomotives, with a bridge at each end for a scenic break. The Paignton-Kingswear line, run by the Dart Valley Railway, is on the left of this picture as Class 52 D1010 *Western Campaigner* pulls out of the sidings with the empty stock for the 2B25 15.27 Paignton-Newton Abbot train on 15 January 1977. Photo: Rail Online



Above: S&D 7F No.53808 leaves its first preservation home of Radstock for the West Somerset Railway on 9 January 1976, towed by a Class 33 diesel.

Photo: Mike Rimes

Right: The BR certificate for the move.



had achieved a main line interchange, we still had plenty of choice when it came to possible layouts that combine preserved steam and BR blue diesels. Here's a few that we considered and why:

Dart Valley Railway: BR blue diesel-hydraulics and GWR engines with chocolate and cream coaches: two very popular modelling subjects on the same layout? There are two possible ways of achieving this: if space is available to model a fairly large station, Paignton (or nearby Goodrington carriage sidings) could be the prototype – the DVR having taken over the line from there to Kingswear in 1973. Offering a little less operational variety is Ashburton Junction just outside Totnes – where, in the then absence of a station, DVR trains from Buckfastleigh could only edge to within a few yards of the main line connection before heading back the way they had come. But if you just want a



Left: A Class 100 DMU departs from Grosmont with a North Yorkshire Moors Railway 'National Park Scenic Cruise' special working in 1978. The junction with the Esk Valley line to Whitby is visible in the background. Photo: Steve Flint

Strathspey Railway: Scotland in the BR blue diesel era is always a popular subject, and by modelling Aviemore in 1978 – when the Strathspey Railway reopened the line from there to Boat of Garten – you can have preserved LMS engines and industrials alongside the Sulzer Type 2s.

Then there were the numerous steam centres of the day, many now closed or no longer open to the public – Ashchurch, Carnforth, Tyseley – or the locations such as Bodmin, Chappel & Wakes Colne, Cheddleton or Parkend, where preservationists had achieved a small foothold in the former engine shed or goods yard of an operational BR line.

Part of the attraction of a preservation-based layout, of course, is the ability to do, and run, whatever you like – so there's also scope for fictional layouts set in the 1970s to combine elements of some or all of these, or to imagine what might have been if a favourite lost line had been saved, or if one of the many abortive preservation schemes had in fact succeeded.

Back to reality, or something close to it: we drew up two layout plans for a limited space, one based on a steam centre adjacent to a still-operational BR line, and one heritage line, both keeping alive the memory of preservation schemes that are sadly no longer with us.

stretch of main line to watch a procession of weathered, faded blue Westerns and Warships speed by, punctuated by the occasional arrival of a 1400 or pannier tank with an autotrain...

North Yorkshire Moors Railway:

Locomotive-hauled freight traffic continued, in ever-decreasing form, on the Esk Valley line to Whitby until April 1983. At Grosmont, alongside the blue Class 31s, and the DMUs on the passenger services, the North Yorkshire Moors Railway had recommenced services to Pickering in 1973. With a fair variety of steam locomotives on the 'Moors Line' even then, and a less extensive, much more compact track layout at Grosmont than today, this too could make an excellent model.



Above: German Pacific No.01.1104 came to Steamtown Railway Centre at Carnforth in the 1970s, and is pictured there on 17 May 1975. Also visible are French Pacific No.231K22, GWR 4-6-0 No.4079 Pendennis Castle, LNER V2 No.4771 Green Arrow, B1 No.1306 Mayflower and Ivatt 2MT No.6441. Photo: Colour Rail

Dinting Railway Centre

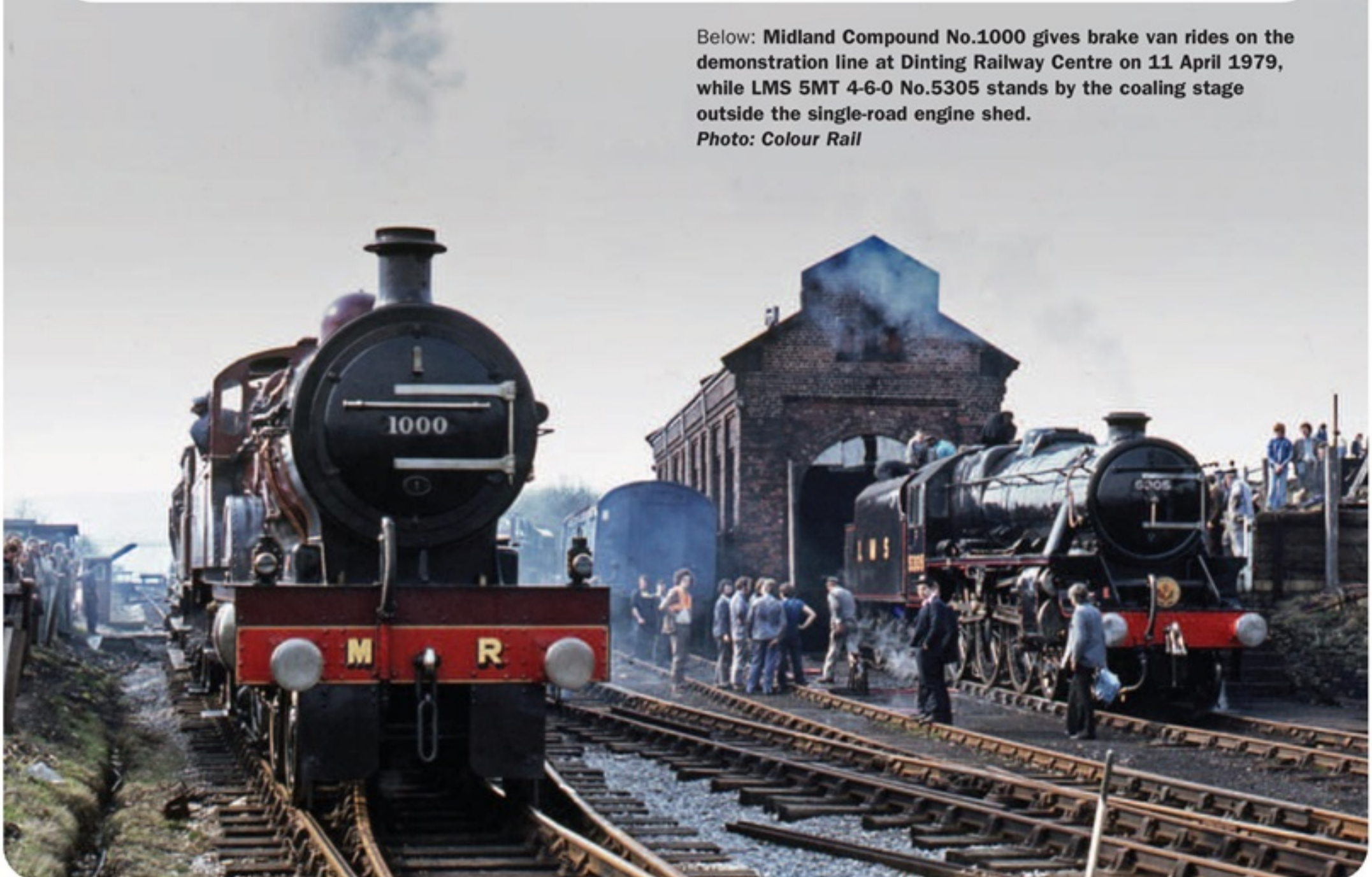
In the 1970s, Dinting Railway Centre was one of the biggest names on the preservation map, being home to such celebrities as LMS Jubilees Nos.5690 *Leander* and 5596 *Bahamas*, and Royal Scot No.6115 *Scots Guardsman*, together with visits from even more famous machines including A4 No.4498 *Sir Nigel Gresley* and A2 No.532 *Blue Peter*. Sadly, it closed in 1990 and the site has now returned to nature, though the characterful single-road engine shed still stands.

With that engine shed having been modelled by Bachmann as part of the Scenecraft range (ref.44-173), the centre and its demonstration line could be the ideal compact OO gauge layout to house a collection of big main line engines, with the added interest of something else now sadly lost – the adjacent electrified Woodhead Route.

The layout of Dinting – with the shed and demonstration line at right angles to the main Woodhead line, and the Glossop branch curving round behind – lends itself best to an L-shaped layout, although the Woodhead line in this case would be purely representational, with a shortened impression of Dinting station in which to display a Class 76 or 77 electric.



Below: Midland Compound No.1000 gives brake van rides on the demonstration line at Dinting Railway Centre on 11 April 1979, while LMS 5MT 4-6-0 No.5305 stands by the coaling stage outside the single-road engine shed.
Photo: Colour Rail



Below: John Cameron's A4 No.60009 *Union of South Africa* at Lochty in 1970, standing in the short platform that once formed a cattle dock.
Photo: Colour Rail



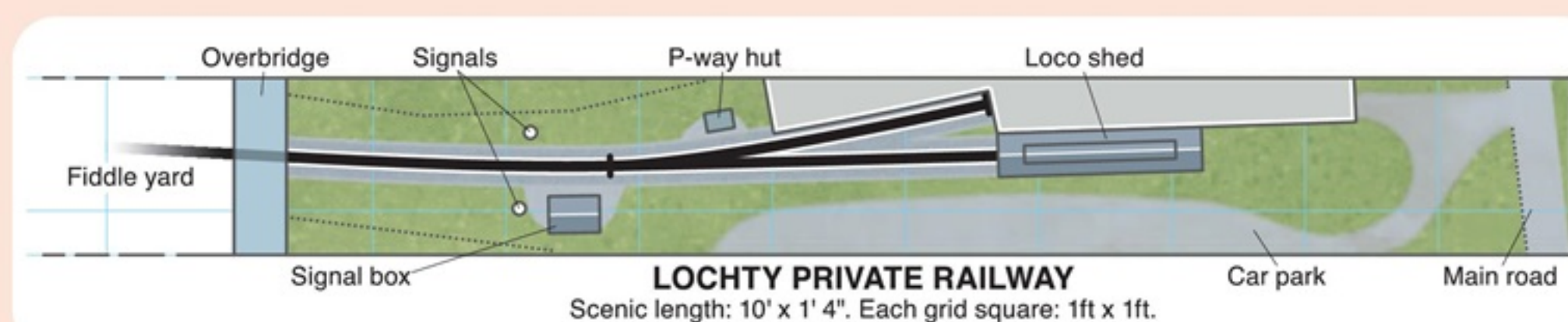
Lochty Private Railway

Picture this: you're the proud owner of a beautiful O gauge model of A4 Pacific No.60009 *Union of South Africa*, but you don't have anywhere near enough space to model a stretch of the East Coast Main Line on which to run it. Must it become no more than a static ornament on the mantelpiece?

Not at all: such a model can be entirely prototypical on a layout with a scenic section of just 10'. From 1967, when it was saved for preservation, until 1973, when it returned to the main line, No.60009 ran on the Lochty Private Railway, a 1¼-mile line laid on the former East Fife Central Railway through the farmland of the locomotive's owner John Cameron.

Our 7mm scale plan is based on Ian Futers' O gauge model of Lochty, featured in RM December 2019, but with a shorter scenic section and the even simpler track layout of the LPR in the early 1970s. With very little rolling stock on the line in this period – at first there was only No.60009 and a former LNER 'Beavertail' observation coach – another option would be to dispense with the fiddle yard and model the other end of the line at Knightsward, where another small platform was constructed.

Further sidings were laid in later years, and prior to the line's closure in 1992, other locomotives included an Austerity 0-6-0ST, Peckett 0-4-0ST and Ruston 88DS 0-4-0 diesel shunter – all of which are available ready-to-run in OO gauge, so it could also make an ideal layout for a small space in 4mm scale.



Ready-to-run models for the 1970s era



The 1970s era has seen increased interest amongst modellers in recent times, with a panoply of ready-to-run models being made available to suit this period. Below is an overview of some of the yet-to-be-released models that are currently being developed by proprietary manufacturers in the popular commercial scales.



Above: A decorated sample of Accurascale's Mk.IIb coach in BR blue and grey.

Forthcoming models to be offered by **Accurascale** include a Brush Type 2/Class 31 in OO, with tooling to include London Underground trip-cock gear equipped examples. Also earmarked for production are models of the English Electric Type 3/Class 37; one of which depicts 37 001 in early 1970s condition, in BR blue with four-character display split headcode. Rolling stock from the firm comprises BR Mk.I 57' non-gangway coaches, in corporate blue – complementing the aforementioned London Underground trip-cock gear equipped Type 2s – and BR Mk.IIb stock.

Bachmann is currently developing models of the Sulzer Type 2/Class 25 from all-new tooling. Proposed models include 25/1 25 057 featuring four-character headcodes in weathered BR blue.

Cavalex Models has announced plans to produce a newly tooled Class 56 in OO. Catering for Romanian, Doncaster and Crewe-built examples, two versions are to be released suitable for the period; Romanian-built 56 008 and Doncaster-built 56 070 – the latter having entered service on 21 December 1979. The firm is also to produce OO gauge HAA 'Merry-Go-Round' hopper wagons, in conjunction with the retailers KMS Railtech and Trains4U.

Dapol has announced two new ready-to-run industrial locomotives – a Hawthorn Leslie 0-4-OST in OO gauge, and a Hunslet Austerity (LNER Class J94) 0-6-OST in O gauge.



Above: CAD image of Dapol's planned Hunslet Austerity 0-6-0 saddle tank.



Above: An early tooling sample of Cavalex Models' Class 56 Co-Co.

Members of both classes survived into the 1970s at various industrial sites around the UK; one such example to be produced is Hunslet Austerity Works No.3694 *Whiston* which ran at Bold Colliery near St Helens throughout the decade. O gauge models of the TTA 45-ton four-wheel tank wagon are also to be produced, with colour schemes to include Shell, BP and ESSO.

Heljan has announced a plethora of new tooling projects suitable for the period in a variety of gauges and scales. Forthcoming models in OO gauge include: the Birmingham Railway Carriage & Wagon Co. Type 2/Class 26/1 with Inverness headlights; BRCW Type 3/Class 33, covering 33/1



Above: CAD render of Heljan's Class 31 in TT:120 – the first R-T-R British outline locomotive in this new scale.



Above: **CAD render of Hornby's Ruston & Hornsby 88DS.**

& 33/2 variants; Sulzer Type 4/Class 45, catering for examples with split centre headcode and sealed beam marker light; Brush Type 4/Class 47, including 47/0, 47/3 & 47/4 sub-classes; BRCW Class 104 DMU; and Ruston 165DE diesel shunter in a selection of industrial liveries.

Diesel traction in O from the firm includes: the Yorkshire Engine Company 0-4-0 diesel-hydraulic shunter/Class 02, with TOPS BR green, BR blue and industrial colour schemes; Class 26/1 including examples with Inverness headlights; BRCW Type 2/Class 27; Class 37/0 with split headcodes; English Electric Type 5/Class 55 Deltic; and English Electric Class 73/1 electro-diesel. Three DMUs are also planned in this gauge; the Class 117, with 2- and 3-car variants proposed; and single-car Class 121 and 122.

The manufacturer has also revealed that it is developing TT:120 models of the Class 31 – the first British outline ready-to-run locomotive in the scale.

Forthcoming models from **Hornby** include the Ruston & Hornsby 88DS diesel shunter with one example finished as Departmental No.20 in BR blue.

Currently in development by **KR Models** is Bulleid's experimental 4-DD 'Double-Decker' Southern Region EMU in OO, which continued in service until October 1971. Also to be produced are examples of the BR Liner Train Match wagons, which were adapted from Palbrick vehicles.

New-tooling projects in OO from **Rapido Trains UK** include the 16" Hunslet 0-6-OST, with Markham Main Colliery *Arthur* and Ackton Hall Colliery *Beatrice* among those in operation during this period. Suitable rolling stock in development includes the BR Diagram 1/191 OAA open wagon and Dia.1/227 VIX ferry van.

Revolution Trains is to offer N gauge models of the BR Class 128 motor parcels van, and the following rolling



Above: **A tooling sample of Rapido Trains UK's VIX ferry van.**



Above: **Inspection saloon Caroline by Revolution Trains in OO.**

stock: 35T class A/class B four-wheel tank wagon; MTV ballast/sand open wagon; Borail 60' bogie engineers wagon and Cartic-4 car carrier, the latter two also being set for release in OO. Also in the process of manufacture in both OO and N is the historic celebrity inspection saloon No.975025 *Caroline*, with the model set to be released in BR blue and grey. The firm is also to produce OO gauge TUA caustic soda/china clay tank wagons, in conjunction with the retailer KMS Rainbow Railways.

Sutton's Locomotive Workshop has revealed that it is producing ready-to-run models of the Sulzer Type 2/Class 25 in 4mm scale – specifically catering for later examples of the 25/2 and 25/3 sub-classes that were introduced to the network from 1963. The firm is also set to release Highland variants of its Class 24/1, equipped with headcode boxes and tablet catchers.



Above: **Engineering samples of Sutton's Locomotive Workshop Class 24/1s in Highland main line condition.**

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Front cover photos

Main image: Wbdenshaw in EM by Kier Hardy (RM July 2008).

Photo by Ian Manderson

Inset image 1: Deltic decorated sample. *Photo courtesy Accurascale*

Inset image 2: Peckett 0-6-OST No.1426 at Brynlliw Colliery in 1970.

Photo: Rail Photoprints

Inset image 3: A view of Royal Oak level crossing in July 1977.

Photo: Colour Rail

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