

**BRITAIN'S No.1  
MODEL RAILWAY MAGAZINE**

SEPTEMBER 2021 £4.95

AT THE HEART OF THE HOBBY

VOLUME 72 No 851

# RAILWAY MODELLER

## Elybridge

A celebration of the  
Big Four era in OO



### Templecombe Lower

An extensive N gauge system  
measuring just 8' x 4'

### Adding realism to modern wagons

Weathering bogie tipplers  
without an airbrush in OO

### The Edenham Branch

A just-supposing OO  
scheme for this former  
Lincolnshire route

### The Fowler 4F 0-6-0s

These versatile goods workhorses –  
drawn, described and modelled

### Reviewed inside ...

L&B 2-4-2T Lyn from Heljan in OO9  
LNER J27 0-6-0 from Oxford Rail in OO  
Coronation Scot coaches from Hornby in OO

### Hebden Mill

A working scenic feature –  
modelled in 7mm scale

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# RAILWAY MODELLER

## Contents

September 2021 vol 72 No.851

### 634 **The Amlwch Branch** **Railway of the Month**

Robert Ogden presents his 7mm scale depiction of this former terminus on the Anglesey Central Railway, which has a variety of rolling stock allowing the operation to be switched between three different time periods.

### 642 **A Fowler 4F in O – Part 2**

Last month John Cockcroft commenced his build of a Connoisseur Models etched kit for an LMS 4F 0-6-0. This month he makes a start on the locomotive chassis and superstructure.

### 646 **The Fowler 4F 0-6-0s** **Scale Drawings**

To complement John Cockcroft's build of one of these locomotives, we present these scale drawings for the class that were produced by the late Ian Beattie.

### 648 **Edenham**

Ian Wilson supposed what this Lincolnshire terminus may have looked like had it been taken over by the Great Northern Railway and survived into the 1950s.

### 654 **Fence Houses – Part 3**

Bob Jones concludes his conversation with RM Senior Editor Steve Flint and explains the logistical challenges involved with taking the layout out to exhibitions.

### 657 **Templecombe Lower**

David Plumridge unearthed a 35-year-old layout at his grandson's request. The result is this large single-board layout with tremendous operating potential and shades of the former Somerset & Dorset route.

### 662 **Talking Points**

RM Senior Editor Steve Flint looks ahead to the programme of traditional model railway events that are planned to take place in the coming months.

### 664 **Modelling Hebden Mill**

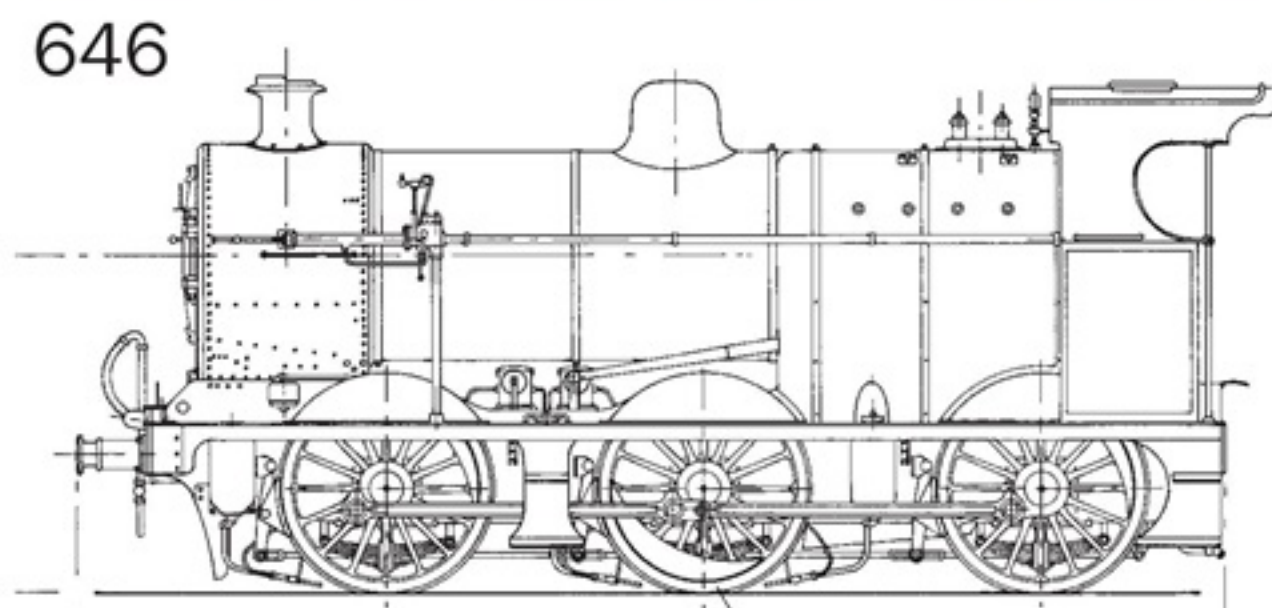
Bob Hordern built this water mill as a feature on his 7mm scale layout based on a prototype situated alongside the River Ribble at Settle in Yorkshire.

### 668 **Fox End Basin and Brewery**

Mick Payne describes how clearing away a seldom-used OO gauge exhibition layout provided yet more space for him to extend further his loft-based 4mm scale layout, which combines EM gauge and OO9 running lines.

### 672 **The Ashover Light Railway** **Plan of the Month**

Steve Croucher serves up an idea for a slice of rural Derbyshire in OO9 gauge – designed to reside in a spare bedroom – which blends scenic richness and industrial clutter with plenty of operational interest.







## RAILWAY MODELLING *Explored*

Projects, hints and tips for beginners from Railway Modeller readers and staff

### 683 **Middleton**

Roy Dowding recounts the story behind his home layout, which has been created with the flexibility to depict different eras and places.

### 686 **Adding realism to modern wagons in OO**

Callum Willcox shows how he gave a rake of Foster Yeoman bogie tipplers a work-worn finish.

### 688 **Curving plastic sheet**

RM Senior Editor Steve Flint describes a method of making curved roofs for 4mm scale coaches and vans from 30thou plastic sheet.



## WHAT'S ON THE COVER

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**Cover photo:** A view on Lance Cadell's OO gauge layout *Elybridge*, which is featured on p678. Pictured is Maunsell 4-6-0 Lord Nelson class No.851 Sir Francis Drake which, having reversed onto the pristine coaches forming the Southampton special, is seen shunting ahead under the ground signal. This train will reverse into platform 3 where passengers travelling on the RMS Queen Mary to New York will board the direct train. Photo: Steve Flint

### 675 **Innes Mill Micro Marvel**

Inspired by a visit to Scotland, Valentijn van der Haegen built this small O-16.5 scale industrial layout specifically for an exhibition competition in Belgium.

### 678 **Elybridge Through the Railway Room Door**

With his aged loft layout in need of significant repair work, Lance Cadell took the opportunity to completely rebuild with the accent on operation and signalling.

### 689 **Comment**

Opinions and observations across the field of railway modelling. This month, Richard Bardsley suggests that actually reading the instructions may be a good idea.

### 690 **Readers' Letters**

### 692 **New Product Reviews**

Our headline review item this month is the brand new Heljan model of Lynton & Barnstaple Baldwin 2-4-2T No.762 Lyn for OO9. In addition we evaluate the new LNER J27 0-6-0 in OO from Oxford Rail, coaching stock developed from new tooling by Hornby for the 'Coronation Scot' and KFA/PFB Warflats exclusively from Trains4U in OO.

### 700 **Book & DVD Reviews**

### 702 **News**

In the news this month – crowds flock to Chester Cathedral to see the mammoth OO gauge layout built by Pete Waterman and his team of modelmakers, Kernow Model Rail Centre shows engineering samples for its forthcoming GWR Steam Railmotor in OO and Peco provides a model of Castle Cary station to help tell the story of this Somerset town's history.

### 37a **Societies & Clubs**

A number of live events are planned to take place in September but visitors should check before attending.

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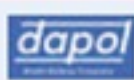
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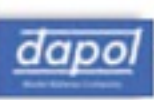
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| PO285 - Industrial Unit.....                             | £11.05 |
| PO289 - Fire Station.....                                | £16.15 |
| PO291 - Castle Gatehouse.....                            | £16.60 |
| PO292 - Castle Watch Tower.....                          | £5.55  |
| PO293 - Castle Curtain Walls.....                        | £9.80  |
| PO313 - Double Engine Shed.....                          | £20.40 |
| PO320 - Booking Hall.....                                | £20.40 |
| PO321 - Parcels Office.....                              | £18.70 |
| PO322 - Island Platform Building.....                    | £17.00 |
| PO330 - GWR Signal Box.....                              | £13.60 |
| PO331 - Red Brick Single Track Engine Shed.....          | £13.60 |
| PO332 - Stone Single Track Engine Shed.....              | £13.60 |
| PO333 - S&C Station Building.....                        | £19.55 |
| PO334 - S&C Station Shelter.....                         | £9.80  |
| PO335 - S&C Station Masters House.....                   | £12.75 |
| PO336 - S&C Goods Shed.....                              | £20.40 |
| PO337 - S&C Stone Engine Shed.....                       | £20.40 |
| PO340 - Platform Canopy.....                             | £13.60 |
| PO341 - Wall Backed Platform Canopy.....                 | £13.60 |
| PO400 - Platform Underpass.....                          | £5.55  |
| PO410 - Wooden Pavilion.....                             | £5.55  |
| PO415 - Nissen Hut.....                                  | £6.40  |
| PO421 - Low Relief Timber Framed Shop.....               | £7.25  |
| PO430 - Small Signal Box.....                            | £8.50  |

## OO Gauge Building Papers



Each pack contains 8 sheets of A4 (approx.) sized card.

|  |       |
|--|-------|
| M0051 - Cobblestones.....                              | £5.80 |
| M0052 - Dressed Gritstone.....                         | £5.80 |
| M0053 - Engineers Blue Brick.....                      | £5.80 |
| M0054 - Brick Sheets.....                              | £5.80 |
| M0055 - Paving Sheets.....                             | £5.80 |
| M0056 - Tarmac Sheets.....                             | £5.80 |
| These tarmac sheets can be used for both OO & N gauge. |       |
| M0057 - Cut Stonework.....                             | £5.80 |
| M0058 - Semi Cut Stonework.....                        | £5.80 |

### N Gauge Kits

|   |        |
|---|--------|
| 202 - Cattle Dock.....                                | £15.70 |
| 203 - Engine Shed.....                                | £13.60 |
| 204 - Station Building.....                           | £20.20 |
| 205 - Station Building Canopy.....                    | £4.00  |
| 206 - Locomotive Servicing Depot.....                 | £15.70 |
| 207 - GWR Station Train Shed.....                     | £15.70 |
| 208 - Apex Platform Canopy.....                       | £8.30  |
| 209 - Straight Platform Sections.....                 | £4.35  |
| 210 - Platform Ramps (2 per pack).....                | £4.35  |
| 211 - Telegraph Poles (10 per pack).....              | £3.85  |
| 212 - Water Crane & Fire Devil.....                   | £7.90  |
| 213 - Station/Street Lamps (4).....                   | £4.50  |
| 214 - Yard Crane.....                                 | £4.80  |
| 215 - Square Water Tower.....                         | £8.35  |
| 216 - Lineside Fencing White.....                     | £4.00  |
| 217 - Lineside Fencing Wood Brown.....                | £4.00  |
| 218 - Signal Laddering.....                           | £4.35  |
| 219 - Concrete Fencing Gates.....                     | £4.00  |
| 220 - Stone Goods shed.....                           | £13.60 |
| 221 - Pallets Sacks Barrels.....                      | £3.85  |
| 222 - Concrete Footbridge.....                        | £8.35  |
| 223 - GWR Wooden Signal Box.....                      | £13.60 |
| 224 - Signal Box Interior.....                        | £5.55  |
| 225 - Flat Roof Platform Canopy.....                  | £8.35  |
| 226 - Pump House/Boiler House.....                    | £13.15 |
| 227 - Weighbridge hut.....                            | £6.40  |
| 228 - Oil Depot.....                                  | £7.05  |
| 229 - Coal Depot.....                                 | £6.80  |
| 230 - Round Water Tower.....                          | £6.95  |
| 231 - Carriage Shed.....                              | £13.60 |
| 232 - Coal/Builders Merchant.....                     | £13.60 |
| 233 - Loading Gauge.....                              | £4.25  |
| 234 - Level crossing with Gates.....                  | £8.35  |
| 235 - Level Crossing with Barriers.....               | £8.35  |
| 236 - Midland Signal Box.....                         | £13.60 |
| 237 - Lineside Huts.....                              | £6.55  |
| 238 - 2 Concrete Huts.....                            | £6.55  |
| 239 - Retaining Wall (350mm).....                     | £9.20  |
| 240 - Steel Truss Bridge.....                         | £15.30 |
| 241 - Steel Truss Span.....                           | £14.30 |
| 242 - 2 Steel Trestles.....                           | £8.90  |
| 243 - GWR Station Fencing White.....                  | £5.00  |
| 244 - GWR Trackside Fencing White.....                | £5.00  |
| 245 - GWR Spear Fencing Black.....                    | £5.00  |
| 246 - GWR Spear Fencing Black Ramps.....              | £5.00  |
| 247 - Coal Tower.....                                 | £21.15 |
| 248 - Modular Covered Footbridge.....                 | £18.70 |
| 250 - Remote Control.....                             | £3.55  |
| 251 - Three Arch Viaduct.....                         | £20.80 |
| 252 - Extra Arch & Pier.....                          | £8.95  |
| 253 - River/Canal Bridge.....                         | £8.95  |
| 254 - Two Stone Piers.....                            | £10.30 |
| 255 - Water Trough.....                               | £7.40  |
| 256 - Large Water Tower.....                          | £13.40 |
| 257 - Relay Boxes.....                                | £6.25  |
| 258 - Concrete Trunking.....                          | £5.10  |
| 260 - Home or Distant Signal (Lower Quadrant).....    | £3.40  |
| 262 - Junction or Bracket Home or Distant Signal..... | £5.35  |
| 270 - Home or Distant Signal (Upper Quadrant).....    | £3.30  |
| 271 - Pratt Truss Gantry.....                         | £3.55  |
| 300 - Gutters & Drainpipes.....                       | £4.35  |
| 301 - Brick Walling.....                              | £4.35  |
| 302 - Coarse Stone.....                               | £4.35  |
| 303 - Paving Slabs/Crazy Paving.....                  | £4.35  |
| 304 - Wood Planking.....                              | £4.35  |
| 305 - Slate Roofing.....                              | £4.35  |
| 306 - Tile Roofing.....                               | £4.35  |
| 307 - Chimneys.....                                   | £4.35  |
| 308 - Flagstones.....                                 | £4.35  |
| 309 - Industrial Windows.....                         | £4.35  |
| 310 - Domestic Windows.....                           | £4.35  |
| 311 - Doors.....                                      | £4.35  |
| 312 - Corrugated Sheet.....                           | £4.35  |
| 313 - Roughcast Walling.....                          | £4.35  |
| 314 - Industrial Chimneys & Fittings.....             | £4.45  |
| 315 - Oil Tanks.....                                  | £4.45  |
| 316 - Coal Stairthes.....                             | £4.45  |
| 317 - Corrugated Roof.....                            | £3.30  |

### OO Gauge Kits

|  |        |
|--|--------|
| 411 - GWR/LMS Joint Gauge.....                             | £3.95  |
| 412 - SR Loading Gauge.....                                | £4.95  |
| 413 - Water Crane & Fire Devil.....                        | £8.80  |
| 419 - Concrete Fence Posts gates.....                      | £5.70  |
| 420 - GWR Station Fencing White.....                       | £4.40  |
| 421 - GWR Station Fencing White.....                       | £4.05  |
| 422 - GWR Station Fencing Black.....                       | £4.05  |
| 423 - GWR Lineside Fencing.....                            | £4.05  |
| 424 - Lineside fencing White.....                          | £4.05  |
| 425 - Lineside Fencing Black.....                          | £4.05  |
| 426 - LMS (MR) Station Fencing White.....                  | £4.05  |
| 427 - LMS (MR) Station Fencing Black.....                  | £4.05  |
| 429 - Concrete Fencing.....                                | £4.40  |
| 430 - Picket Fencing Green Gates & Ramps.....              | £4.40  |
| 431 - Picket fencing green Straight.....                   | £4.05  |
| 432 - SR Precast Concrete Pale Fencing.....                | £5.40  |
| 432A - SR Precast Concrete Pale Fencing Gates & Ramps..... | £5.65  |
| 434 - Spear Fencing (straight only).....                   | £4.45  |
| 435 - Spear Fencing (gates & ramps).....                   | £4.95  |
| 436 - Security Fencing.....                                | £11.00 |
| 437 - Wooden Fencing.....                                  | £4.95  |
| 451 - Signal Laddering (4 lengths).....                    | £3.55  |
| 452 - Telegraph Poles (16 per pack).....                   | £4.05  |
| 453 - Swan Necked lamps (9 per pack).....                  | £4.50  |
| 454 - Concrete Lamps.....                                  | £4.50  |
| 455 - Modern street lights.....                            | £5.40  |
| 456 - Station Barrow Crossing.....                         | £4.95  |
| 460 - GWR Home.....  | £10.30 |
| 461 - GWR Distant.....                                     | £10.30 |
| 462 - GWR Home & Distant.....                              | £12.20 |
| 465 - Ground Signals (4 per pack).....                     | £3.80  |
| 466 - GWR Square Post Signal.....                          | £8.30  |
| 467 - GWR Round Post Signal.....                           | £8.30  |
| 468 - GWR Round Post Signal.....                           | £10.30 |
| 469 - GWR Junction/Bracket Signal.....                     | £11.50 |
| 470 - LMS Home Signal.....                                 | £10.30 |
| 471 - LMS Distant Signal.....                              | £10.30 |
| 476 - LMS Round post Signal.....                           | £8.30  |
| 477 - LNWR Square Post Signal.....                         | £8.30  |
| 478 - Pratt Truss Gantry.....                              | £8.30  |
| 486 - LNER Latticed Post Signal.....                       | £8.30  |
| 493 - SR Rail Built Signal Upper Quadrant.....             | £13.10 |
| 500 - GWR Signal Box.....                                  | £17.00 |
| 501 - Grounded coach Body.....                             | £5.70  |
| 502 - Cattle Dock.....                                     | £17.00 |
| 503 - Platform/Ground level Signal Box.....                | £17.00 |
| 504 - Station Building.....                                | £27.70 |
| 505 - Coal Stage.....                                      | £8.50  |
| 506 - Water Tower.....                                     | £17.00 |
| 507 - Grounded Van Body.....                               | £5.35  |
| 508 - Pump House/Boiler House.....                         | £17.00 |
| 509 - Occupation Crossing.....                             | £5.40  |

|   |        |
|---|--------|
| 510 - Industrial Fittings.....              | £3.55  |
| 511 - Wooden Lineside Huts (2).....         | £5.70  |
| 512 - Skylights.....                        | £3.55  |
| 513 - Provender (Goods) Store.....          | £16.95 |
| 514 - Pack of Assorted Pallets & Sacks..... | £4.05  |
| 515 - Platform Canopy.....                  | £14.30 |
| 516 - Station Valancing Notice Board.....   | £4.05  |
| 517 - Concrete Footbridge.....              | £12.20 |
| 518 - Concrete Lineside Huts (2).....       | £5.70  |
| 519 - Large Grounded Mess Van/Store.....    | £8.50  |
| 520 - Platform/Including Ramps.....         | £8.30  |
| 521 - Industrial Window Arches.....         | £4.05  |
| 522 - Engine Shed Inc. Hut.....             | £26.80 |
| 523 - Industrial Windows.....               | £4.05  |
| 524 - Weighbridge & Hut.....                | £5.70  |
| 525 - Coal/Timber Merchants.....            | £22.10 |
| 526 - Coal Sacks (48 per pack).....         | £4.25  |
| 527 - Carriage Shed (320mm x 105mm).....    | £22.10 |
| 528 - Water Tower GW Pillar.....            | £12.50 |
| 529 - Oil Depot.....                        | £14.10 |
| 530 - Oil Tanks (2).....                    | £11.20 |
| 531 - Yard Crane.....                       | £8.50  |
| 532 - Coal Depot.....                       | £9.45  |
| 533 - Coal Stairthes.....                   | £5.30  |
| 534 - Stone Goods Shed.....                 | £27.70 |
| 535 - Yard Office.....                      | £4.05  |
| 536 - Midland Signal Box.....               | £21.90 |
| 537 - Retaining Walls.....                  | £8.05  |
| 538 - Gutters/Downpipes.....                | £4.75  |
| 539 - Midland Signal Box Window.....        | £4.75  |
| 540 - Locomotive Servicing Depot.....       | £22.10 |
| 543 - Hoist.....                            | £9.40  |
| 544 - Carriage Cleaning Platform.....       | £7.60  |
| 545 - Loco Lifting Hoist.....               | £20.80 |
| 546 - Traversing Crane.....                 | £22.35 |
| 546A - Rolling Underframe.....              | £5.20  |
| 547 - Coal Tower.....                       | £24.90 |
| 548 - Modular Covered Footbridge.....       | £21.60 |
| 549 - Fogmans Hut.....                      | £6.95  |
| 550 - Water Trough 768mm.....               | £9.35  |
| 551 - Large Water Tower.....                | £17.85 |
| 552 - GWR Brick Signal Box.....             | £22.80 |
| 553 - Signal Box Interior.....              | £12.50 |
| 554 - ARP Wartime Signal Box.....           | £15.75 |
| 558 - Nissen Hut.....                       | £9.85  |

### OO Gauge Kits

|  |        |
|--|--------|
| CK10 - Farm Cottage Scene.....             | £15.20 |
| CK11 - Semi-detached Houses.....           | £15.20 |
| CK12 - Two Road Engine Shed.....           | £15.20 |
| CK13 - Black Horse Inn.....                | £15.20 |
| CK14 - Single Road Engine Shed.....        | £15.20 |
| CK15 - Stone Goods Shed Bank & Crane.....  | £15.20 |
| CK16 - Country Station Brick Built.....    | £30.35 |
| CK17 - Country Station Building.....       | £31.75 |
| CK18 - Semidetached Shops.....             | £15.20 |
| CK19 - Goods Shed Brick Type.....          | £15.20 |
| CK20 - Post Office.....                    | £15.20 |
| CK21 - Semidetached Stone Cottages.....    | £15.20 |
| CK22 - Water Mill.....                     | £23.90 |
| SS10 - Victorian Gents Toilet.....         | £4.70  |
| SS11 - Taxi Mens Rest Hut.....             | £4.70  |
| SS12 - Station Garage with Pumps.....      | £7.10  |
| SS13 - Domestic Garage.....                | £4.70  |
| SS14 - Tea Kiosk.....                      | £4.70  |
| SS15 - Coal Yard & Hut.....                | £4.70  |
| SS16 - Weighbridge & Hut.....              | £4.70  |
| SS17 - Coal Bunkers.....                   | £4.70  |
| SS18 - Station Forecourt Shops.....        | £5.25  |
| SS19 - Grotty Huts & Privy.....            | £4.85  |
| SS20 - Greenhouse & Cold Frames.....       | £5.20  |
| SS21 - Hoardings with Bill Poster.....     | £5.30  |
| SS22 - Lamp Huts With Oil Drums (2).....   | £5.40  |
| SS23 - Bicycle shed & Bicycles.....        | £4.70  |
| SS24 - Conservatory with Garden Seat.....  | £4.70  |
| SS25 - Station Halt Complete.....          | £4.70  |
| SS26 - Victorian Bridge.....               | £4.70  |
| SS27 - Station Halt with Waiting Room..... | £5.35  |
| SS28 - Occupational Bridge with Stone..... | £5.35  |
| SS29 - Ground Level Signal Box.....        | £4.70  |
| SS30 - Stone & Timber Barn.....            | £5.40  |
| SS31 - Village Forge.....                  | £5.40  |
| SS32 - Occupational Bridge.....            | £6.95  |
| SS34 - Water Tower & Stone Base.....       | £6.95  |
| SS35 - Pagoda Building Iron Type Hut.....  | £4.70  |
| SS36 - Dressed Stone Type Wall.....        | £4.70  |
| SS37 - Market Stalls.....                  | £5.10  |



## OO Gauge Steam Locos



**Class 5101 'Large Prairie' 2-6-2T**  
Stage: Painted Samples Due: Sep 2021  
Price: from £119.60 Digital: NEXT-18  
[hattons.co.uk/dapollargeprairie](http://hattons.co.uk/dapollargeprairie)



**Class 78xx 'Manor' 4-6-0**  
Stage: Engineering Sample Due: Q4 2021  
Price: from £140.72 Digital: NEXT-18  
[hattons.co.uk/dapolmanor](http://hattons.co.uk/dapolmanor)



**Class J26 0-6-0**  
Stage: Early Development Due: Q4 2021  
Price: from £93.50 Digital: 8-pin  
[hattons.co.uk/oxfordj26](http://hattons.co.uk/oxfordj26)



**Class 78xx Manor 4-6-0**  
Stage: Engineering Sample Due: Q4 2021  
Price: from £169.99 Digital: 21-pin  
[hattons.co.uk/accurascalemanor](http://hattons.co.uk/accurascalemanor)



**Class 43xx 'Mogul' (Tool Update)**  
Stage: CADs Due: May 2022  
Price: from £142.76 Digital: NEXT-18  
[hattons.co.uk/dapolmogul](http://hattons.co.uk/dapolmogul)



**Class O2 'Tango' (Tool Update)**  
Stage: CADs Due: Q2 2022  
Price: from £169.15 Digital: 21-pin  
[hattons.co.uk/heljantango](http://hattons.co.uk/heljantango)

## OO Diesels & Electrics



**Class 59**  
Stage: Painted Samples Due: Q4 2021  
Price: from £136.32 Digital: 21-pin  
[hattons.co.uk/dapol59](http://hattons.co.uk/dapol59)



**Class 45**  
Stage: Early Sample Due: Q4 2021  
Price: from £143.65 Digital: 21-pin  
[hattons.co.uk/heljan45](http://hattons.co.uk/heljan45)



**Class 47**  
Stage: Prototype Due: Q4 21 - Q1 22  
Price: from £184 Digital: 21-pin  
[hattons.co.uk/heljanoo47](http://hattons.co.uk/heljanoo47)



**Class 86/4 & 86/6**  
Stage: Early Development Due: Q2 2022  
Price: from £165 (EST) Digital: 21-pin  
[hattons.co.uk/heljan864](http://hattons.co.uk/heljan864)



**Class 37/0, 37/4 & 37/6**  
Stage: Engineering Samples Due: Q2 2022  
Price: from £169.99 Digital: 21-pin  
[hattons.co.uk/accurascale37](http://hattons.co.uk/accurascale37)

## OO Diesel Multiple Units



**Class 104**  
Stage: Early Development Due: TBC  
Price: from £180 Digital: 21-pin  
[hattons.co.uk/heljan104](http://hattons.co.uk/heljan104)

## OO Coaches & Wagons



**Genesis 4 & 6 Wheel Coaches**  
Stage: Painted Samples Due: Q3 2021  
Price: from £30 Digital: 18-pin  
[hattons.co.uk/genesis](http://hattons.co.uk/genesis)



**JHA Hopper Wagons**  
Stage: Painted Samples Due: Aug 2021  
Price: from £38.25 Digital: 6-pin  
[hattons.co.uk/dapoljha](http://hattons.co.uk/dapoljha)



**BR Pilchard Wagons**  
Stage: Deco Samples Due: Q3 2021  
Price: £13 Digital: N/A  
[hattons.co.uk/oxfordpilchard](http://hattons.co.uk/oxfordpilchard)



**GER 10t Covered Van**  
Stage: Deco Sample Due: Aug/ Sep 21  
Price: £13 Digital: N/A  
[hattons.co.uk/oxfordgerman](http://hattons.co.uk/oxfordgerman)



**LNER 10t Banana Van**  
Stage: Prototype Due: Aug/ Sep 21  
Price: £12.50 Digital: N/A  
[hattons.co.uk/oxford10tbanana](http://hattons.co.uk/oxford10tbanana)

## N Gauge Steam Locos



**LSWR Class M7 0-4-4T**  
Stage: Prototype Due: Dec 2021  
Price: £83.26 Digital: N/A  
[hattons.co.uk/dapolm7](http://hattons.co.uk/dapolm7)

## N Gauge Diesel Locos



**Class 59**  
Stage: CADs Due: Q3/ Q4 2022  
Price: from £136 Digital: NEXT-18  
[hattons.co.uk/dapoln59](http://hattons.co.uk/dapoln59)

## N Gauge Wagons



**Class B Tanks (New Batch)**  
Stage: In Development Due: Q1 2022  
Price: £26.95 Digital: N/A  
[hattons.co.uk/dapoln59](http://hattons.co.uk/dapoln59)

## N Diesel Multiple Units



**Class 142 Pacer (New Batch)**  
Stage: In Production Due: August 2021  
Price: from £123.12 Digital: NEXT-18  
[hattons.co.uk/dapol142](http://hattons.co.uk/dapol142)

## O Gauge Diesels/ Electrics



**Class 26**  
Stage: Prototype Due: Q1 2022  
Price: from £509.15 Digital: ESU XL  
[hattons.co.uk/heljano26](http://hattons.co.uk/heljano26)



**Class 47**  
Stage: Early Sample Due: Q4 2021  
Price: £594.15 Digital: ESU XL  
[hattons.co.uk/heljano47](http://hattons.co.uk/heljano47)



**Class 56**  
Stage: Early Sample Due: Q4 21 - Q1 22  
Price: £594.15 Digital: ESU XL  
[hattons.co.uk/heljan56](http://hattons.co.uk/heljan56)



**Class 73**  
Stage: CAD Due: Mid-2022  
Price: from £636.65 Digital: ESU XL  
[hattons.co.uk/heljan73](http://hattons.co.uk/heljan73)

## O Diesel Multiple Units



**GWR Streamlined Railcar**  
Stage: Painted Samples Due: Nov 2021  
Price: from £297.50 Digital: 21-pin  
[hattons.co.uk/dapolrailcar](http://hattons.co.uk/dapolrailcar)



**Class 121 'Bubble Car'**  
Stage: Painted Samples Due: Oct 2021  
Price: from £263.05 Digital: 21-pin  
[hattons.co.uk/dapolo121](http://hattons.co.uk/dapolo121)

## O Coaches & Wagons



**Stroudley 4 Wheel Coaches**  
Stage: Prototypes Due: Q4 2021  
Price: from £96.77 Digital: Yes TBC  
[hattons.co.uk/dapolstroudley](http://hattons.co.uk/dapolstroudley)



**Mk2 & Mk2A Coaches**  
Stage: Prototypes Due: Q1 2022  
Price: from £237.15 Digital: N/A  
[hattons.co.uk/heljanmk2](http://hattons.co.uk/heljanmk2)



## OO Gauge (1:76 Scale)

### Dapol - Wagons

NEW



4F-010-010 JNA 'Falcon' bogie ballast wagon in Network Rail yellow - NLU29042 NEW... £25.50  
4F-010-009 JNA Falcon bogie ballast wagon Network Rail yellow - NLU29008 NEW... £25.50  
4F-010-011 JNA Falcon bogie ballast wagon Network Rail yellow - NLU29009 NEW... £25.50  
4F-010-012 JNA Falcon bogie ballast wagon Network Rail yellow - NLU29144 NEW... £25.50

### Hattons Originals - Steam locos



H4-P-016 SECR P Class 0-6-0T 31556 in BR black early emblem (RRP £99)... **BARGAIN** £84

### Diesel locos



H4-66-037 Class 66 66587 in Freightliner/ONE pink 'AS ONE, WE CAN' (RRP £150)... **BARGAIN** £119



H4-66-036 Class 66 66623 in Freightliner/G&W orange (RRP £150)... **BARGAIN** £119



H4-66-024 Class 66 66727 in GBRI/First group 'Andrew Scott CBE' (RRP £150)... **BARGAIN** £119



H4-66-033 Class 66 66743 in GBRI/Royal Scotsman (RRP £150)... **BARGAIN** £119

### Heljan - Diesel locos



3463 Class 33/0 33023 in BR blue grey roof and orange cantrail stripe - weathered NEW... £143.65



3458 Class 33/0 33025 in BR Civil Engineers 'Dutch' grey and yellow - weathered, marks where nameplates & crests removed NEW... £143.65



3459 Class 33/0 33029 in Direct Rail Services blue NEW... £135.15



3461 Class 33/0 33035 in Network SouthEast blue - Ltd Ed for Olivias Trains NEW... £159



3466 Class 33/0 33052 'Ashford' in BR blue NEW... £135.15

NEW



3462 Class 33/0 D6508 'Eastleigh' in 1990s BR green small yellow panels NEW... £135.15

### DMUs

19400 GWR AEC diesel railcar 22 in GWR chocolate and cream white roof and shirubuton emblem NEW... £160.65

NEW



19401 GWR AEC diesel railcar 29 in GWR chocolate and cream grey roof and coat of arms emblem NEW... £160.65

NEW



19403 GWR AEC diesel railcar W20W in BR crimson and cream dark grey roof and white cab rooves NEW... £160.65

19402 GWR AEC diesel railcar W21W BR crimson and cream grey roof NEW... £160.65

NEW



19405 GWR AEC diesel railcar W26W in BR green speed whiskers grey roof NEW... £160.65

19404 GWR AEC diesel railcar W32W BR green speed whiskers white roof NEW... £160.65

### Hornby - Steam locos



R3865 Class 7MT Britannia 4-6-2 70013 'Oliver Cromwell' in BR green early emblem... £180

NEW



R3861 Class 8P 'Merchant Navy' 4-6-2 35017 'Belgian Marine' in BR green early emblem NEW... £170.50



R3448 Class B17 4-6-0 61619 'Welbeck Abbey' in BR green early emblem (RRP £120.99)... **BARGAIN** £109

**Peco Products - Track - Code 100 Streamline**  
SL-100 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track... £3.10  
SL-100 Pack of 25... £76

### Track - Code 75 Finescale

SL-100F 1 yard (91.5cm) length of Nickel Silver Finescale flexible track... £3.20  
SL-100F Pack of 25... £78

## OO9 Narrow Gauge

### Heljan - Steam locos

NEW



9982 Lynton & Barnstaple Baldwin 2-4-2T 'Lyn' in L&B dark green (post-1906) NEW... £203.96

NEW



9981 Lynton & Barnstaple Baldwin 2-4-2T 'Lyn' in L&B dark green (pre-1906) NEW... £203.96

NEW



9983 Lynton & Barnstaple Baldwin 2-4-2T E762 'Lyn' in SR ex-L&B dark green NEW... £203.96

NEW



9984 Lynton & Barnstaple Baldwin 2-4-2T E762 'Lyn' in SR olive green 1929 - 1932 NEW... £203.96

NEW



9980 Lynton & Barnstaple Baldwin 2-4-2T in plain works black NEW... £203.96

## N Gauge

### Dapol - Steam locos

NEW



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2S-002-006 Class V 'Schools' 4-4-0 30939 'Leatherhead' BR green early emblem... £127.52

NEW



2S-002-009 Class V 'Schools' 4-4-0 924 'Haileybury' in SR olive green NEW... £127.52

NEW



2S-002-008 Class V 'Schools' 4-4-0 927 'Clifton' in SR malachite green NEW... £127.52

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K10-1671 Class 800/0 5-car BiMU IET 800021 in GWR green... £178

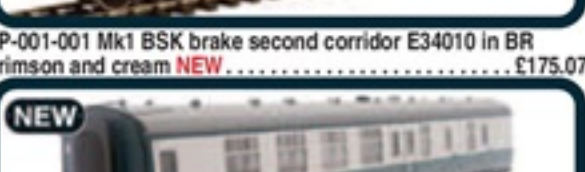
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SL-300F 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track... £3.20  
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## O Gauge (1:43 Scale)

### Dapol - Coaches



7P-001-002 Mk1 BSK brake second corridor E34008 BR crimson and cream NEW... £175.07

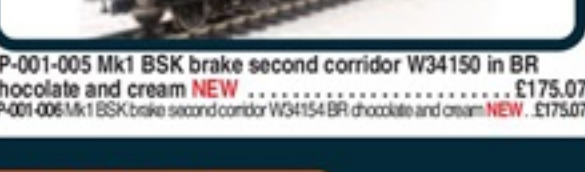


7P-001-001 Mk1 BSK brake second corridor E34010 in BR crimson and cream NEW... £175.07



7P-001-009U Mk1 BSK brake second corridor in BR blue and grey - unnumbered NEW... £175.07

7P-001-009 Mk1 BSK brake second corridor M34393 BR blue and grey NEW... £175.07



7P-001-005 Mk1 BSK brake second corridor W34150 in BR chocolate and cream NEW... £175.07

7P-001-006 Mk1 BSK brake second corridor W34154 BR chocolate and cream NEW... £175.07

NEW

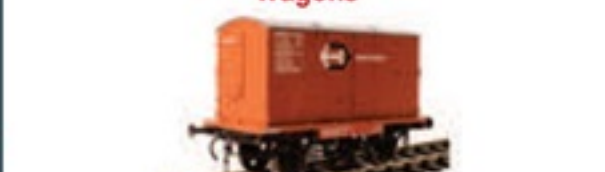


7P-001-201 Mk1 SK second corridor E24154 in BR crimson and cream NEW... £175.07

7P-001-205 Mk1 SK second corridor M25362 in BR blue and grey NEW... £175.07

7P-001-203 Mk1 SK second corridor W24164 BR chocolate and cream NEW... £175.07

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7F-037-005 Conflat 'A' flat wagon B735364 in BR bauxite BD type container in BR bauxite 'Door to Door'... £57.14

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H7-TC115-002 Gresley Teak coach Diagram 115 Corridor Third 23896 in LNER Teak... £179

H7-TC115-001 Gresley Teak coach Diagram 115 Corridor Third unnumbered LNER Teak... £179



H7-TC175-002 Gresley Teak coach Diagram 175 Brake Corridor Composite 24068 in LNER Teak... £179

H7-TC186-002 Gresley Teak coach Diagram 186 Open Third 23956 LNER Teak... £179

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H7-A3-001 Class A3 4-6-2 2750 'Papyrus' in LNER Grass green unstreamlined corridor tender - 'Record Breaker' (RRP £750)... **BARGAIN** £579

H7-A3-006 Class A3 4-6-2 60077 'The White Knight' in BR green late crest and unstreamlined non-corridor tender (RRP £750)... **BARGAIN** £579



H7-A3-007 Class A3 4-6-2 60103 'Flying Scotsman' in BR green late crest and unstreamlined corridor tender (RRP £750)... **BARGAIN** £579



H7-A4-009 Class A4 4-6-2 60008 'Dwight D Eisenhower' in BR green late crest and streamlined non-corridor tender (RRP £750)... **BARGAIN** £579

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DCR-8PIN-Direct Box of 5... £77

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### Hornby - Digital decoders

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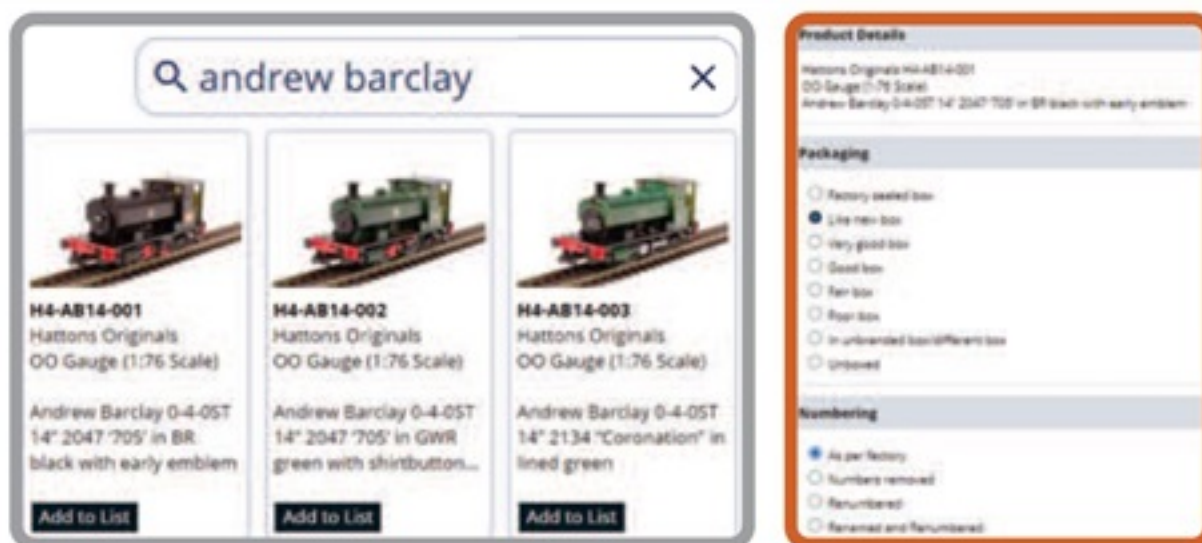


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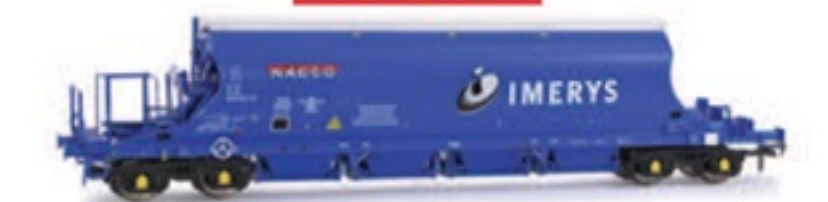
R40134 SR 6 Wheel Brake 3rd Class Coach No. 3750 SR Olive Green.....£32.49

### N GAUGE



2F-028-015 45ft Hi Cube Container Twin Pack Russell 459644 & 459677 0.....£16.55  
2F-028-016 45ft Hi Cube Container Twin Pack Russell 459644 & 459677 0, weathered.....£16.55

### EFE Rail



E87500 JIA Nacco Wagon No. 33-70-0894-007-0 Imerys Blue.....£37.95  
E87501 JIA Nacco Wagon No. 33-70-0894-008-8 Imerys Blue.....£37.95  
E87502 JIA Nacco Wagon No. 33-70-0894-020-3 Imerys Blue.....£37.95  
E87503 JIA Nacco Wagon No. 33-70-0894-000-5 Imerys Blue.....£37.95

E87504 JIA Nacco Wagon No. 33-70-0894-001-3 Imerys Blue, light weathering.....£39.95  
E87505 JIA Nacco Wagon No. 33-70-0894-002-3 Imerys Blue, light weathering.....£39.95



E87506 JIA Nacco Wagon No. 33-70-0894-009-6 Imerys Blue, light weathering.....£39.95  
E87507 JIA Nacco Wagon No. 33-70-0894-010-4 Imerys Blue, light weathering.....£39.95



E87508 JIA Nacco Wagon No. 33-70-0894-011-2 Imerys Blue, heavy weathering.....£41.95  
E87509 JIA Nacco Wagon No. 33-70-0894-012-0 Imerys Blue, heavy weathering.....£41.95  
E87510 JIA Nacco Wagon No. 33-70-0894-003-9 Imerys Blue, heavy weathering.....£41.95  
E87511 JIA Nacco Wagon No. 33-70-0894-004-7 Imerys Blue, heavy weathering.....£41.95



### O GAUGE



3120 Class 31 Diesel Loco Railfreight Grey/Yellow 31296 Amwch Freightliner, weathered.....£466.65  
3121 Class 31 Diesel Locomotive Railfreight "Red Stripe" Grey.....£466.65  
3122 Class 31 Diesel Locomotive Railfreight Railroad Freight Grey Unbranded.....£466.65  
3123 Class 31 Diesel Locomotive BR Civil Engineers Grey/Yellow.....£466.65  
3140 Class 31 Diesel Locomotive BR Blue FYE.....£466.65  
3141 Class 31 Diesel Locomotive InterCity Mainline.....£466.65  
3142 Class 31 Diesel Locomotive Regional Railways.....£466.65  
3143 Class 31 Diesel Locomotive No. 31466 EWS Red/Gold.....£466.65  
3144 Class 31 Diesel Locomotive Network Rail Yellow.....£466.65



## OO GAUGE

### BACHMANN

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|---|---------|
| 31-170 L&YR 2-4-2 Steam Tank No. 50764 BR Black.....                            | £69.99  |
| 31-318A Robinson Class J11 (GCR 9) Steam Loco No. 5954 LNER Black.....          | £104.99 |
| 31-426C Class 411 4-CEP 4 Car EMU No. 7122 BR SR Green.....                     | £349.99 |
| 32-166 Southern N Class Steam No. 1406 SR Black Bullied Sunshine lettering..... | £109.99 |



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| 32-529C Class 55 Deltic Diesel No. D9010 'The King's Own Scottish Borderer'.....         | £139.99 |
| 32-560 Class A1 Steam Locomotive No. 60117 BRITISH RAILWAYS Apple Green.....             | £114.95 |
| 32-650DS Class 44 Diesel Locomotive No. D3 'Skiddaw' BR Green, DCC Sound.....            | £199.99 |
| 32-830A Ivatt Class 2MT 2-6-0 Steam Locomotive No. 6418 LMS Plain Black.....             | £99.99  |
| 32-992 Wickham Type 27 Trolley Car BR Engineers Yellow.....                              | £45.95  |
| 33-831 25 Ton Queen Mary Brake Van SatLink, weathered.....                               | £17.99  |
| 33-832 25 Ton Queen Mary Brake Van YTX No. KDS 56305 EWS.....                            | £19.99  |
| 37-040 5 Plank Wagon Steel Floor No. 362 I.C.I. Ltd with load, weathered.....            | £14.95  |
| 37-097 Coal Trader Pack. 3 x 5 Plank Wagons, weathered.....                              | £37.95  |
| 37-114 7 Plank Fixed End Wagon Baldwin, weathered.....                                   | £12.95  |
| 37-185A 7 Plank Wagon No. P156917 w/Coke Rails (BR) Cory, weathered.....                 | £14.95  |
| 37-535C 20 Ton Brake Van CAR No. B955016 BR Railfreight.....                             | £17.95  |
| 38-105 Triple Pack 34 Ton PNA Wagons Railtrack 5 Rib, weathered.....                     | £41.99  |
| 38-329A 13 Ton High Sided Steel Wagon 278985 LNER Grey 'Not to work off LNE system'..... | £16.95  |
| 38-400A SR 25 Ton Pill Box Brake Van No. 56462 SR Brown.....                             | £18.95  |
| 38-553A Midland 20 Ton Brake Van No. 134900 LMS Bauxite without Duckets.....             | £17.95  |
| 38-554 Midland 20 Ton Brake Van No. M623 Midland Railway Grey.....                       | £17.95  |
| 39-003 MK1 Coach Pack - BR Blue & Grey NSE, weathered.....                               | £69.99  |
| 39-004 MK1 Coach Pack - BR Blue & Grey ScotRail, weathered.....                          | £69.99  |
| 39-005 Highlander Coach Pack.....  | £97.99  |
| 39-082B BR Mk1 BSK Brake Second Corridor Coach No. 35329 Network SouthEast.....          | £27.95  |
| 39-127B BR Mk1 CK Composite Corridor Coach No. E15055 BR Crimson & Cream.....            | £27.99  |
| 39-153D BR Mk1 FK First Corridor Coach No. S13006 BR (SR) Green.....                     | £33.99  |
| 39-177C BR Mk1 BG Full Brake Coach No. M80565 BR Crimson & Cream.....                    | £27.95  |
| 39-186 BR Mk1 BG Full Brake Coach QRX ADB 975612 Departmental Olive Green, whrd.....     | £27.99  |
| 39-240 BR Mk1 FO First Open Coach No. M3001 BR Crimson & Cream.....                      | £32.99  |
| 39-250A BR Mk1 RFO Restaurant Car No. M5 BR Blue & Grey.....                             | £27.99  |
| 39-251D BR Mk1 RFO Restaurant Car No. M1 BR Maroon.....                                  | £27.99  |
| 39-265 BR Mk1 RMB Miniature Buffet Car No. M1865 NSE.....                                | £28.99  |
| 39-310C BR Mk1 SP Pullman Second Parlour Coach No. 350 Umber & Cream.....                | £37.99  |



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| 39-312 BR Mk1 SP Pullman Second Parlour Coach No. E352E BR Blue & Grey.....           | £36.99 |
| 39-410A BR Mk2a BFK Brake First Corridor Coach No. W17069 BR Blue & Grey.....         | £28.99 |
| 39-412A BR Mk2a BFK Brake First Corridor Coach No. 17097 Network SouthEast.....       | £27.99 |
| 39-413 BR Mk2a BFK Brake First Corridor Coach 35516 Regional Railways, weathered..... | £47.99 |
| 39-502A BR Mk1 SLSTP Second Class Sleeper Car No. E2591 BR Maroon.....                | £28.99 |
| 39-578 BR Auto Trailer No. W234 BR Crimson & Cream.....                               | £49.99 |

### HORNBY

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| R1223 Freightmaster Train Set.....   | £64.99  |
| R3373 Class 71 Electric Locomotive No. E5001 BR Green with small yellow panel..... | £109.99 |
| R3374 Class 71 Electric Locomotive No. 71 012 BR Blue with full yellow end.....    | £109.99 |
| R3408 4-6-0 5000 King Class Steam Loco No. 6016 'King Edward V' GWR Green.....     | £114.99 |
| R3409 4-6-0 6000 King Class Steam Locomotive No. 6002 'King William IV' BR LC..... | £114.99 |
| R3417 K1 Class Steam Locomotive No. 62065 BR Black.....                            | £89.99  |
| R3418 K1 Class Steam Locomotive No. 62006 BR Black.....                            | £94.99  |
| R3458 Schools Class Steam Locomotive No. 921 'Shrewsbury' SR Black.....            | £129.99 |
| R3527 N17 King Arthur Class Steam Locomotive No. 742 'Camelot' SR Black.....       | £99.99  |
| R3558 Royal Scot Class Steam Locomotive No. 46165 'The Ranger' BR Green LC.....    | £104.99 |
| R3569 Class 71 Electric Locomotive No. E5005 BR Blue.....                          | £99.99  |
| R3614 Patriot Class Steam Locomotive No. 5521 'Rhyf' LMS Black.....                | £129.99 |
| R3627 A3 Class 4-6-2 Steam Loco No. 60103 'Flying Scotsman' BR Blue E/E.....       | £134.99 |



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| R3740 Class 92 Co-Co Electric Loco No. 92 023 'Flying Scotsman' BR Blue E/E..... | £64.99 |
| R4447B LMS Period III Corridor First Coach No. M1047M BR Crimson & Cream.....    | £24.99 |
| R4449B LMS Period III Corridor Brake Third Coach M5914M BR Crimson & Cream.....  | £24.99 |



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| R4519B 51ft Gresley Non-Vestibled Suburban First Coach No. E81032E BR Maroon.....       | £28.99 |
| R4520B 51ft Gresley Non-Vestibled Suburban First Coach No. E82190E BR Maroon.....       | £28.99 |
| R4521C 51ft Gresley Non-Vestibled Suburban Composite Coach No. E88067E BR Maroon.....   | £28.99 |
| R4522C 51ft Gresley Non-Vestibled Suburban Brake Third Coach No. E86109E BR Maroon..... | £28.99 |
| R4684A Collett Bow-Ended Corridor Third Coach No. W4910W BR Crimson & Cream.....        | £29.99 |
| R4685A Collett Bow-Ended Corridor Third Coach RH W4925W BR Crimson & Cream.....         | £29.99 |
| R4689 LMS Non-Corridor 57ft Composite Coach No. M16574.....                             | £27.99 |
| R4690 LMS Non-Corridor 57ft Third Class Coach No. M11912M.....                          | £27.99 |

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| R4691A ex LMS Non-Corridor 57ft Third Class Brake Coach No. M20788M BR.....           | £24.99 |
| R4792 SR 58ft Maunsell Rebuilt (ex LSWR 48ft) 8 Compartment Brake 3rd Coach 2638..... | £29.99 |
| R4793 SR 58ft Maunsell Rebuilt (ex LSWR 48ft) 6 Compartment Brake 3rd Coach 2628..... | £29.99 |
| R4799 LMS Suburban Non-Corridor Composite No. M16635 BR Crimson.....                  | £29.99 |
| R4817A Maunsell Kitchen Dining First Coach No. S7858A BR Green.....                   | £34.99 |



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| R4836 Maunsell Corridor Brake 2nd Coach 6 Compartment S2763S BR SR Green.....     | £26.99 |
| R4876 GWR Collett 57ft Bow Ended D98 Six Compartment Brake 3rd Coach LH 4971..... | £26.99 |



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| R4879A GWR Collett 57ft Bow Ended E131 9 Compartment Composite Coach RH..... | £24.49 |
| R4938A Mk3 Sliding Door TGS Coach No. 44052 Cross Country Trains.....        | £26.99 |
| R4940C Mk3 Sliding Door TS Coach No. 42370 Cross Country Trains.....         | £26.99 |
| R6802 SR Brake Van No. 55891 SR Brown.....                                   | £14.99 |

### HORNBY WAGONS UNDER £10

|   |       |
|---|-------|
| R6746 4 Plank Wagon No. 8 'Stephens & Co. Kidwelly'.....      | £9.99 |
| R6749 5 Plank Wagon No. 18 'Farndon Rugby'.....               | £9.99 |
| R6750 5 Plank Wagon No. 354 'Shap Tarred Granite'.....        | £9.99 |
| R6754 6 Plank Wagon No. 988 'London Brick Company'.....       | £9.99 |
| R6758 7 Plank Open Wagon No. 3018 'Arthur Wharton Leeds'..... | £9.99 |

### BACHMANN REP EXCLUSIVE MODELS

|  |         |
|--|---------|
| 32-780U Class 37 Diesel Locomotive No. 37 261 DRS.....               | £119.99 |
| 32-780UDC Class 37 Diesel Locomotive No. 37 261 DRS, DCC Fitted..... | £139.99 |



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| 32-780UDS Class 37 Diesel Locomotive No. 37 261 DRS, DCC Sound Fitted..... | £219.99 |
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| 32-775NDFS Class 37/0 Diesel 37 104 Railfreight Triple Grey, DCC Sound Fitted.....       | £209.99 |
| 32-775TL Class 37/0 Diesel Loco No. 37 055 'Rail Celebrity' Mainline Freight.....        | £129.99 |
| 32-775TLD Class 37/0 No. 37 055 'Rail Celebrity' Mainline Freight, DCC Sound Fitted..... | £209.95 |

### BARGAIN BUNDLES

|   |         |
|---|---------|
| 31-676APACK Bachmann Class 85 Electric Loco E3057 + 37-238Z.....    | £179.99 |
| 36-609 BULK Bachmann 3rd Radius Double Curve 5050mm. Pack of 4..... | £8.49   |



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|---|---------|
| K9963 66783 GBRf Flying Dustman Pack.....                                 | £149.99 |
| K9965 The Sapper Cargowagon Pack.....                                     | £149.99 |
| K9972 Mainline Freight Ballast Pack.....                                  | £199.99 |
| K9998A Bachmann Scenecraft China Clay Dries Pack with FREE Landrover..... | £99.99  |



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| R3951APACK Hornby Class 66 Diesel 66 780 + R8121 TTS Decoder..... | £84.99 |
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## N GAUGE

### BARGAIN PACK



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| K9964 Graham Farish Class 66 + HKA x 2..... | £169.99 |
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### DAPOL

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| 2D-022-003 Class 68 Diesel Loco No. 68 010 Chiltern..... | £124.95 |
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### GRAHAM FARISH

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|---|--------|
| 374-135 BR Mk1 GUV QRV No. KDB977557 Satlink.....                       | £26.99 |
| 374-167 BR Mk1 FK First Corridor Coach No. 13225 Regional Railways..... | £26.99 |



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| 374-191 BR Mk1 BSK Brake Second Corridor Coach QVA No. ADB977109 BR..... | £28.99 |
| 374-192 BR Mk1 BSK Brake Second Corridor Coach No. M35451 Intercity..... | £20.99 |
| 374-258C BR Mk1 CK Corridor Composite Coach M16153 BR Blue & Grey.....   | £22.95 |
| 374-415 Southern PMV Passenger Luggage Van No. S1068S BR Crimson.....    | £16.95 |



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| 374-417 Southern PMV Passenger Luggage Van No. S1733 BR Blue.....           | £16.95 |
| 374-612 BR Auto Trailer No. W237W BR Crimson.....                           | £26.99 |
| 374-643 BR MK 1 CCT Coach QOX No. ADB 977076 BR Re-Railing Yellow.....      | £16.95 |
| 374-830C Stanier Brake Corridor First Coach No. 5057 LMS Crimson Lake.....  | £20.95 |
| 374-831C Stanier Brake Corridor First Coach M5056M BR Crimson & Cream.....  | £20.95 |
| 374-847A Stanier Corridor First Coach No. M1053M BR Maroon.....             | £20.99 |
| 374-875 LMS 50ft Inspection Saloon No. 45028 LMS Lined Crimson Lake.....    | £25.95 |
| 374-876 LMS 50ft Inspection Saloon No. M45029M BR Maroon.....               | £25.95 |
| 374-902A BR Mk1 Travelling Post Office Coach No. M1053M BR Blue & Grey..... | £29.95 |
| 377-062 5 Plank Wagon No. 132701 NE Grey with Load.....                     | £12.99 |
| 377-089 7 Plank Fixed End Wagon No. 18166 SR Brown.....                     | £9.95  |
| 377-379 20 Ton Toad Brake Van BR Departmental Yellow.....                   | £16.99 |
| 377-954A 13 Ton High Sided Steel Wagon No. 278985 NE Grey.....              | £12.99 |
| 377-957 13 Ton High Sided Steel Wagon E279122 BR Grey w/smooth sides.....   | £11.99 |
| 377-976A 12 Ton Eastern Ventilated Van No. E236010 BR Early Bauxite.....    | £19.95 |

### GRAHAM FARISH BUSES & COACHES

|   |       |
|---|-------|
| 379-500 Bristol VRT Western National.....                           | £6.95 |
| 379-501 Bristol VRT Double Decker Bus - Maidstone and District..... | £6.95 |
| 379-503 Bristol VRT Double Decker Bus - NBC United Dairy.....       | £6.95 |
| 379-516 Harrington Cavalier Coach - Maidstone and District.....     | £6.95 |
| 379-517 Harrington Cavalier Coach - Southdown.....                  | £6.95 |
| 379-518 Harrington Cavalier Coach - Valliant.....                   | £6.95 |
| 379-530 Bristol RELH - Bristol Greyhound.....                       | £6.95 |
| 379-532 Bristol RELH - Royal Blue.....                              | £6.95 |
| 379-533 Bristol RELH - Southern National.....                       | £6.95 |
| 379-545 Leyland Titan PD2 Bus - Southdown.....                      | £6.95 |
| 379-546 Leyland Titan PD2 Bus - Stockport Corporation.....          | £6.95 |
| 379-547 Leyland Titan PD2 Bus - Trent.....                          | £6.95 |
| 379-548 Leyland Titan PD2 Bus - Leicester Corporation.....          | £6.95 |
| 379-560 Guy Arab II - London Transport.....                         | £6.95 |
| 379-561 Guy Arab IV Double Decker - Birmingham.....                 | £6.95 |
| 379-562 Guy Arab II Double Decker - Midland General.....            | £6.95 |
| 379-563 Guy Arab IV Double Decker - Devon General.....              | £6.95 |
| 379-576 Leyland National - United Counties.....                     | £6.95 |
| 379-577 Leyland National Bus - Alder Valley.....                    | £6.95 |
| 379-578 Leyland National Bus - Midland General.....                 | £6.95 |
| 379-605 Leyland Atlantean Bus - Tyneside.....                       | £6.95 |
| 379-607 Leyland Atlantean Bus - Plymouth City Transport.....        | £6.95 |
| 379-608 Leyland Atlantean Bus - Northern.....                       | £6.95 |

### GRAHAM FARISH SCENECRAFT

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|--|--------|
| 42-0001 Two Road Brick Engine Shed.....    | £21.95 |
| 42-150 Road Side Farm Shop.....            | £12.00 |
| 42-191 Red Star Parcels Office.....        | £13.50 |
| 42-219 Low Relief Stone Factory.....       | £19.99 |
| 42-227 Low Relief Rear Victorian.....      | £25.95 |
| 42-228 Low Relief Double Track Tunnel..... | £9.00  |
| 42-236 Low Relief Railway Hotel.....       | £12.95 |
| 42-254 Low Relief Signal Box.....          | £13.50 |
| 42-298 Low Relief Boiler House.....        | £19.95 |
| 42-547 Cycle Cabinets.....                 | £3.50  |

### WOODLAND SCENICS

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|--|---------|
| ST4801 Mod-U-Rail Straight Module..... | £99.99  |
| ST4802 Mod-U-Rail Corner Module.....   | £119.99 |



# **KMRC EXCLUSIVES / IN STOCK / OO GAUGE**

## **KERNOW MODELS**

K2605 Class 41 Warship D602 "Bulldog" BR Blue small yellow ends.....£179.99  
 K2606 Class 41 Warship D600 "Active" BR Green. Headcode disks & original grilles.....£179.99



K2607 Class 41 Warship D601 "Ark Royal" BR Green, weathered.....£189.99  
 K2608 Class 41 Warship D604 "Cossack" BR Green. Headcode disks & later grilles.....£179.99  
 K9001 Fire Iron Accessory Pack.....£2.99  
 K9002 Y25C52 Bogle with wheels - as supplied with our JIA NACCO & PBA Clay Tiger models.....£4.99  
 K9003 Bournemouth Belle Headboard.....£5.00  
 K9004 Golden Arrow Headboard.....£5.00  
 K9005 Atlantic Coast Express Headboard.....£5.00  
 K9006 Royal Scot Headboard.....£5.00  
 K9007 Cornish Riviera Headboard Express.....£5.00  
 SB006M YCV Turbot Bogie Ballast Wagon No. DB978026 Civil Engineers Dutch.....£29.99  
 SB006N YCV Turbot Bogie Ballast Wagon No. DB978080 Civil Engineers Dutch.....£29.99  
 SB006Y YCV Turbot Bogie Ballast Wagon, pristine. Pack of 2.....£54.99  
 SB007B PRA 38 Tonne glw Covered Box Wagon No. RLS6314 Grey.....£39.99  
 SB007C PRA 38 Tonne glw Covered Box Wagon No. RLS6315 Grey.....£39.99  
 SB007D PRA 38 Tonne glw Covered Box Wagon No. RLS6316 Grey.....£39.99

## **KERNOW MODELS + REVOLUTION TRAINS OO**

OO-EAL-105A Revolution Trains JNA-T Ealnos Box Wagon 8170 5500 188-4 VTG Mendip.....£55.99  
 OO-EAL-105B Revolution Trains JNA-T Ealnos Box Wagon 8170 5500 174-4 VTG Mendip.....£55.99  
 OO-EAL-105C Revolution Trains JNA-T Ealnos Box Wagon 8170 5500 227-0 VTG Mendip.....£55.99



OO-EAL-105D Revolution Trains JNA-T Ealnos Box Wagon 8170 5500 171-0 tail lamp.....£59.99  
 OO-EAL-105Z Revolution Trains JNA-T Ealnos Box Wagon VTG Mendip Rail. Pack of 4.....£209.99



OO-EAL-106D Revolution Trains JNA-T Ealnos Box Wagon 8170 5500 473-0 tail lamp.....£59.99

## **BACHMANN**



31-657Z Class 47 Diesel Loco No. 47 701 "Old Oak Common".....£159.99  
 31-657ZDC Class 47 Diesel Loco No. 47 701 "Old Oak Common". DCC Fitted.....£179.99  
 31-657ZDS Class 47 Diesel Loco No. 47 701 "Old Oak Common". DCC Sound Fitted.....£259.99



31-662Z Class 47/4 Diesel No. 47 537 "Sir Gwynedd", weathered.....£189.99  
 31-662ZDS Class 47/4 Diesel No. 47 537 "Sir Gwynedd", weathered. DCC Sound Fitted.....£274.95  
 32-640Z Class 491 4-TC Unit No. 416 BR Blue with small yellow warning panel.....£269.95  
 32-641Z Class 491 4-TC Unit No. 404 BR Blue & Grey.....£269.95  
 32-642Z Class 438 4-TC Unit No. 8022 BR Blue & Grey Network SouthEast branding.....£269.95  
 32-643Z Class 438 4-TC Unit No. 8023 Network SouthEast.....£269.95  
 32-644Z Class 438 4-TC Unit No. 410 Premier Charter Blue.....£269.95  
 32-646Z Class 438 4-TC Unit No. 8007 BR Research Department.....£269.95



32-816Z Class 47 No. 47 306 "The Sapper" Railfreight Distribution, weathered.....£159.99  
 32-816ZDC Class 47 No. 47 306 "The Sapper" Railfreight Distribution, wthrd. DCC Fitted.....£179.99  
 32-816ZDS Class 47 No. 47 306 "The Sapper" Railfreight Distribution, wthrd. DCC Sound Fitted.....£259.99  
 32-935X Class 150/2 2 Car DMU No. 150 216 First Great Western livery Purple.....£189.95  
 32-935XS Class 150/2 2 Car DMU No. 150 216 First Great Western Purple DCC Sound Fitted.....£309.99  
 33-080T China Clay Wagon BR Bauxite with flat tarpaulins, weathered. Pack of 3.....£54.95  
 33-300W 20 Ton Toad Brake Van ZTO No. DW35377 BR Departmental Green.....£16.99  
 33-300Y 20 Ton Toad Brake Van No. W68366 BR Grey "Gwinear Road".....£15.99  
 33-825X 25 Ton Queen Mary Brake Van ADS56289 BR Engineers Grey.....£19.99  
 33-825Y 25 Ton Queen Mary Brake Van LDS56293 BR Bauxite "Electrification".....£19.99  
 37-238Z 16 Ton Steel Mineral Wagon Triple Pack, weathered.....£59.99  
 38-140X 29 Ton (ex VDA) Sliding Door Box Van No. ADC200666 BR, weathered.....£24.95  
 38-326Z High Sided Steel Wagon No. DB479526 "S&T Cable Wagon Guildford", weathered.....£24.95  
 38-328Z High Sided Steel Wagon No. ADE282721 "AME St Blazey Stores", weathered.....£24.95

## **BACHMANN SCENECRAFT**

44-007Z Cornish Roundhouse.....£69.99  
 44-008Z Cornish Roundhouse Exterior Walls.....£27.99



44-058Z Derelict Cornish Engine House.....£29.99  
 44-059Z China Clay Dries.....£73.95  
 44-060Z China Clay Dries Chimney.....£43.95  
 44-061Z LSWR Brick Signal Box.....£29.99

44-062Y Single Track Stone Engine Shed.....£59.99



44-074Z LSWR Signal Box - Petersfield.....£79.99  
 44-097Z GWR Water Tower - St Ives.....£49.99  
 44-125Z Cornish Terraced Cottages.....£39.95  
 44-182Z LSWR Ground Frame Hut - Boscombe Junction.....£19.99

## **DJMODELS**



SB002IU/K/L PBA TIGER China Clay Wagons ECC International blue.....£44.99 EACH  
 SB002MNO/P JIA TIGER China Clay Wagons ECC International blue, weathered.....£49.99 EACH



SB004A/B/C/D ZAA PIKE Open Wagons Dutch Civil Engineers.....£29.99 EACH  
 SB005A/B/C/D SPA Open Wagons BR Railfreight, weathered.....£32.99 EACH  
 SB005E/F/G/H SPA Open Wagons EWS.....£29.99 EACH

## **HELJAN**



3362 Class 33/1 Diesel Locomotive No. 83 301 Intercity.....£149.00

## **HORNBY**

R3672 Class 50 Diesel Locomotive No. 50 010 "Monarch" BR Large Logo.....£169.99  
 R3672DS Class 50 Diesel Locomotive No. 50 010 "Monarch". DCC Sound Fitted.....£289.99  
 R3675 Class 31 Diesel No. 97 204 BR Research livery.....£169.99  
 R3675DS Class 31 No. 97 204 BR Research. DCC Sound Fitted.....£289.99

## **OXFORD DIECAST**

SP47 Land Rover Series 1 Registration English China Clays International.....£4.99

# **KMRC EXCLUSIVES / IN STOCK / N GAUGE**



371-465Z Bachmann Class 37/0 Diesel Locomotive No. 37 207 William Cookworthy.....£129.95



374-130Z Bachmann Mk1 GUV No. E86247 BR Maroon with Parcels Express branding.....£34.99



377-490Z Bachmann China Clay 5 Plank Wagon Pack. English China Clays. 3 x UCV Wagons.....£46.99



N92017 Revolution Trains Class 92 Electric Locomotive No. 92 2017 "Bart the Engine" Stobart Rail livery.....£179.95

# **BARGAIN KMRC EXCLUSIVES / OO GAUGE**



31-650QDS Class 47 Diesel Locomotive No. 47 829 Police. DCC Sound Fitted.....£229.99  
 31-651Z Class 47 Diesel Locomotive No. 47 973 "Midland Counties".....£129.99

31-651ZDC Class 47 Diesel Locomotive No. 47 973 "Midland Counties". DCC Fitted.....£149.99  
 31-651ZDS Class 47 Diesel Locomotive No. 47 973 "Midland Counties". DCC Sound Fitted.....£229.99  
 31-653Z Class 47 Diesel Locomotive No. 47 706 ScotRail NSE branding, weathered.....£135.99  
 31-653ZDC Class 47 Diesel Locomotive No. 47 706 ScotRail NSE, weathered. DCC Fitted.....£152.99  
 31-653ZDS Class 47 Diesel Locomotive No. 47 706 ScotRail NSE, weathered. DCC Sound Fitted.....£219.99  
 32-066Z Class 43 Warship No. D845 "Sprightly" BR Green.....£139.99  
 32-066ZDC Class 43 Warship No. D845 "Sprightly" BR Green. DCC Fitted.....£156.99  
 32-135X Class 4575 Prairie No. 5541 BR Green, weathered.....£99.99  
 32-727W Class 66 Diesel Locomotive No. 66 709 "Sorrento" GBRI.....£127.95  
 32-727WDC Class 66 "Sorrento" GBRI. DCC Fitted.....£143.95  
 32-727WDS Class 66 "Sorrento" GBRI. DCC Sound Fitted.....£209.99  
 32-762Z Class 57 No. 57 305 Network Rail known as "Sand Castle".....£134.95  
 32-762ZDC Class 57 No. 57 305 Network Rail known as "Sand Castle". DCC Fitted.....£149.95  
 32-762ZDS Class 57 No. 57 305 Network Rail known as "Sand Castle". DCC Sound Fitted.....£214.99

32-780U Class 37 Diesel Locomotive No. 37 261 DRS.....£119.99  
 32-780UDC Class 37 Diesel Locomotive No. 37 261 DRS. DCC Fitted.....£139.99  
 32-780UDS Class 37 Diesel Locomotive No. 37 261 DRS. DCC Sound Fitted.....£219.99  
 38-131Z 40 Ton Seacow YGB Bogie Hopper Wagon Departmental Dutch, weathered.....£45.95  
 38-132Z 40 Ton Seacow YGB Bogie Hopper Wagon EWS, weathered.....£45.95  
 39-273Z BR Mk1 GUV Coach S86804 BR (SR) Green.....£39.99  
 K2201 0-6-0 1361 Steam Locomotive No. 1361 BR Black L/C.....£69.99  
 K2202 0-6-0 1361 Steam Locomotive No. 1362 BR Black E/E.....£69.99  
 K2203 0-6-0 1361 Steam Locomotive No. 1363 GWR Green with Shirtbutton emblem.....£69.99  
 K2204 0-6-0 1361 Steam Locomotive No. 1364 GWR Green with GWR lettering.....£69.99  
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 SB005Z SPA Open Wagon EWS Bulk Pack of 4.....£79.99



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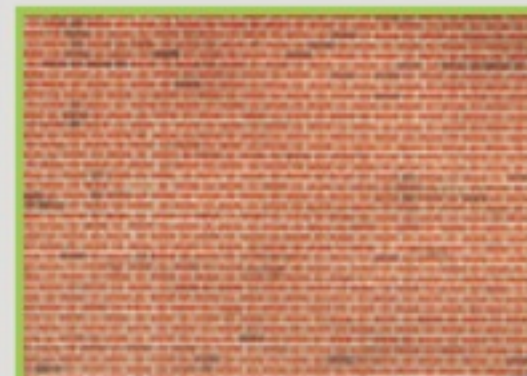
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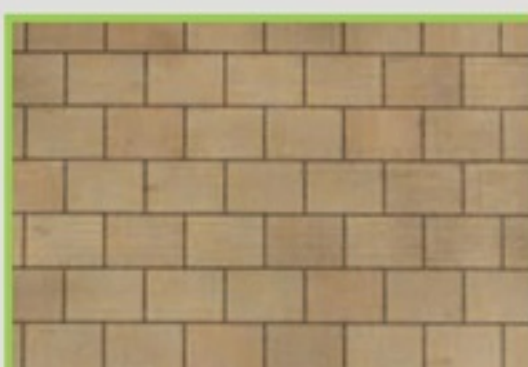
**DRESSED GRITSTONE**  
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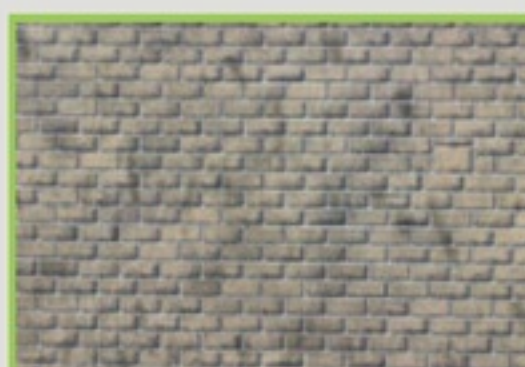
**RED BRICK**  
00 CODE M0054 / N CODE PN900



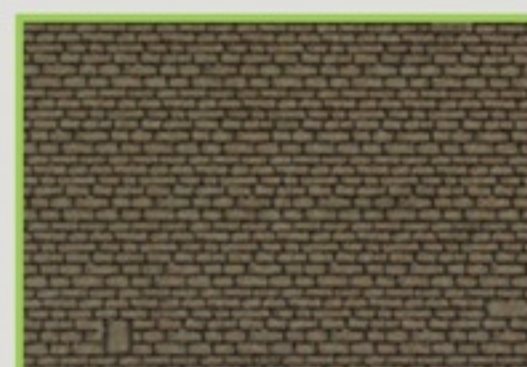
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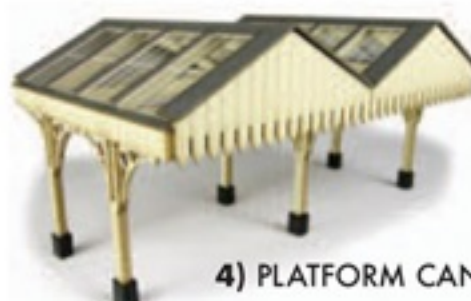
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| R2917  | 2800 Class BR          | £103.99 |
| R3233  | D16/3 LNER             | £100.95 |
| R3235  | D16/3 British Railways | £100.95 |
| R3240  | Class 700 BR Early     | £96.45  |
| R3242A | K1 BR Early            | £116.95 |
| R3243B | K1 BR Late             | £116.95 |



|       |                       |         |
|-------|-----------------------|---------|
| R3312 | BR Class A3           | £143.45 |
| R3329 | BR Late S15           | £123.99 |
| R3333 | BR Early Adams Radial | £107.95 |
| R3334 | BR Late Adams Radial  | £107.95 |
| R3380 | LNER J15              | £96.90  |
| R3381 | BR Early J15          | £96.90  |
| R3411 | SR S15                | £125.95 |
| R3413 | S15 Late              | £119.45 |
| R3414 | LNER J15              | £99.45  |



|       |                    |         |
|-------|--------------------|---------|
| R3418 | K1 BR Early        | £114.45 |
| R3420 | BR Late 700 CLASS  | £99.45  |
| R3421 | BR Early 700 CLASS | £99.45  |
| R3422 | SR Adams Radial    | £109.45 |
| R3423 | BR Late Adams      | £109.45 |
| R3432 | BR Late B12        | £127.95 |



|       |                  |         |
|-------|------------------|---------|
| R3445 | BR W/C Camelford | £147.45 |
| R3462 | BR Class 42xx    | £106.45 |
| R3523 | BR B17           | £127.99 |
| R3614 | LMS Patriot      | £129.45 |
| R3615 | Peckett 0-4-0ST  | £89.99  |
| R3621 | LNER J36         | £125.75 |
| R3622 | BR J36           | £125.75 |



|       |                  |         |
|-------|------------------|---------|
| R3623 | LMS Coronation   | £171.00 |
| R3634 | SR Lord Nelson   | £152.95 |
| R3635 | Lord Rodney      | £152.95 |
| R3676 | Class A4 Mallard | £151.50 |



|       |                     |         |
|-------|---------------------|---------|
| R3677 | Duchess of Hamilton | £171.00 |
| R3681 | LMS Coronation      | £184.00 |



|       |                    |         |
|-------|--------------------|---------|
| R3682 | BR Blue Coronation | £184.00 |
| R3767 | BR Early Terrier   | £80.75  |
| R3781 | K&ESR Terrier      | £80.75  |

|          |                          |         |
|----------|--------------------------|---------|
| R3603TTS | BR Lord Nelson           | £175.00 |
| R3780    | LBSCR Terrier            | £80.75  |
| R3782    | SECR Terrier             | £80.75  |
| R3805    | BR Black 5               | £157.45 |
| R30006   | BRITISH RAILWAYS Terrier | £89.99  |
| R30008   | BR Early Terrier         | £89.99  |
| R3999    | LMS Princess             | £188.99 |
| R30001   | LMS Princess             | £188.99 |
| R3832    | LNER CI A2/3             | £171.00 |

## HORNBY Diesel Electric Locomotives

|       |                   |         |
|-------|-------------------|---------|
| R3262 | Class 31 D & C    | £142.45 |
| R3349 | Class 67 EWS      | £123.95 |
| R3373 | NRM Class 71      | £134.95 |
| R3374 | BR Blue Class 71  | £134.95 |
| R3376 | BR Green Class 71 | £134.95 |



|       |                 |         |
|-------|-----------------|---------|
| R3473 | BR Class 56 Rft | £135.99 |
| R3482 | Sentinel MSC    | £51.45  |
| R3573 | Class 66 GBRF   | £79.45  |
| R3577 | Sentinel Graham | £58.00  |



|       |                      |         |
|-------|----------------------|---------|
| R3685 | GWR HST              | £261.00 |
| R3580 | Class 87 BR Blue     | £139.40 |
| R3582 | Class 87 Intercity   | £139.25 |
| R3659 | Class 67 Colas       | £144.00 |
| R3705 | Ruston 48DS Dewars   | £71.99  |
| R3706 | Ruston 48DS Army     | £71.99  |
| R3707 | Ruston 48DS Longmorn | £71.99  |
| R3915 | Network Rail CI.121  | £71.10  |
| R3773 | BR Class 156         | £112.50 |

## HORNBY TRACK

|      |                       |        |
|------|-----------------------|--------|
| R083 | Buffer Stop           | £2.50  |
| R207 | Track Fixing Pins     | £3.25  |
| R394 | Hydraulic Buffer      | £6.95  |
| R600 | Straight              | £2.15  |
| R601 | Double Straight       | £2.55  |
| R604 | Curve 1st Rad.        | £2.55  |
| R605 | Double Curve 1st Rad. | £3.25  |
| R606 | Curve 2nd Rad.        | £2.15  |
| R607 | Double Curve 2nd Rad. | £3.25  |
| R608 | Curve 3rd Rad.        | £2.55  |
| R609 | Double Curve 3rd Rad. | £3.25  |
| R610 | Short Straight        | £1.65  |
| R614 | LH Diamond Crossing   | £13.95 |
| R615 | RH Diamond Crossing   | £13.95 |
| R617 | Uncoupling Ramp       | £3.95  |
| R618 | Double Isolating Rail | £9.90  |
| R620 | Railer Uncoupler      | £6.95  |
| R628 | Half Curve 3rd Rad.   | £2.80  |

|       |                     |        |
|-------|---------------------|--------|
| R643  | Half Curve 2nd Rad. | £2.25  |
| R8072 | LH Point            | £11.85 |
| R8073 | RH Point            | £11.85 |
| R8075 | RH Curved Point     | £18.85 |
| R8076 | Y Point             | £13.90 |
| R8077 | LH Express Point    | £18.85 |



|       |                  |        |
|-------|------------------|--------|
| R8078 | RH Express Point | £18.85 |
| R8206 | Power Track      | £8.95  |
| R602  | Power Clip       | £2.80  |
| R8241 | DCC power track  | £8.90  |

## HORNBY Track Accessories

|      |                     |        |
|------|---------------------|--------|
| R076 | Footbridge          | £16.20 |
| R189 | Single Brick Bridge | £12.80 |
| R626 | Point Underlay      | £6.90  |



|      |                       |        |
|------|-----------------------|--------|
| R636 | Double Level Crossing | £28.80 |
| R638 | Track Underlay        | £15.85 |
| R645 | Single Level Crossing | £18.85 |
| R658 | Inclined Pier Set     | £9.90  |
| R659 | High Level Pier Set   | £8.10  |

## HORNBY Trackside

|      |                      |       |
|------|----------------------|-------|
| R460 | Straight Platform    | £4.00 |
| R462 | Large Curve Platform | £4.00 |
| R463 | Small Curve Platform | £4.00 |
| R464 | Platform Ramp        | £3.00 |



|      |                  |        |
|------|------------------|--------|
| R510 | Platform Shelter | £14.85 |
|------|------------------|--------|

## BACHMANN Steam Locomotives

|        |                  |         |
|--------|------------------|---------|
| 31-088 | BR Black Earl    | £118.95 |
| 31-785 | BR Modified Hall | £157.20 |
| 31-786 | BR Modified Hall | £157.20 |



|         |                |         |
|---------|----------------|---------|
| 31-191  | BR Jubilee     | £169.95 |
| 31-186A | BR Jubilee     | £169.95 |
| 35-050  | LNWR Coal tank | £101.95 |
| 35-051  | LMS Coal tank  | £101.95 |
| 35-052  | BR Coal tank   | £101.95 |
| 31-614  | BR Class V3    | £110.45 |



|         |           |         |
|---------|-----------|---------|
| 35-175  | ROD 2-8-0 | £148.70 |
| 35-025A | GWR 94xx  | £123.20 |
| 31-741  | LMS 1P    | £127.45 |
| 31-690  | LMS Mogul | £135.95 |
| 31-627B | LMS 3F    | £97.70  |

## BACHMANN Diesel/Electric Locomotives

|         |             |         |
|---------|-------------|---------|
| 32-701A | BR Class 46 | £135.95 |
|---------|-------------|---------|



|          |                 |         |
|----------|-----------------|---------|
| 32-940   | GWR Class 150/2 | £271.95 |
| 32-390DB | EWS Class 37    | £144.45 |
| 35-126A  | HNRC Class 20/3 | £152.95 |
| 35-526   | BR Class 121    | £131.70 |
| 35-527   | NSE Class 121   | £131.70 |
| 31-367   | NCB Class 03    | £106.20 |
| 31-361B  | BR green CI 03  | £106.20 |

## DAPOL LOCOMOTIVES

|            |             |        |
|------------|-------------|--------|
| 4S-018-005 | BR Late B4  | £93.50 |
| 4S-018-007 | B4 Guernsey | £93.50 |
| 4S-018-008 | Southern B4 | £93.50 |



|            |                    |         |
|------------|--------------------|---------|
| 4S-043-002 | GWR Mogul          | £135.95 |
| 4S-043-003 | BR Mogul           | £135.95 |
| 4D-011-006 | GWR Railcar No. 10 | £125.75 |
| 4D-011-009 | GWR Railcar No. 16 | £125.75 |
| 4D-011-100 | GWR Parcels car    | £125.75 |



|            |                   |         |
|------------|-------------------|---------|
| 4D-014-000 | BR Class 29 green | £129.15 |
| 4D-014-002 | BR Class 29 blue  | £129.15 |
| 4D-014-003 | BR Class 29 blue  | £129.15 |



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We have been producing Model Railway Controllers for over forty years. Our analogue controllers have stood the test of time, and so has our lifetime guarantee:

*"We undertake to replace, free of charge, any parts found defective within the lifetime of the unit providing that the item has not been tampered with."*

#### What's the difference between the controller types?

Our **Mains Powered Cased Controllers** come complete with transformer, and can just be plugged in, connected to the track, and away you go. **Panel Mounted Controllers** require a separate transformer, and also need to be mounted onto a control panel to be used effectively.

We also produce various controllers with **Feedback** and **Simulation**, two effects controlled by the controller itself. **Feedback** senses the load on the circuit and helps maintain the locomotive at a steady speed up and down gradients. **Feedback** controllers are not suitable for use with locomotives with coreless motors. **Simulation** (also known as Inertia) allows a train to accelerate, coast, and be braked to a standstill, by use of a regulator and a brake.

### MAINS POWERED CASED CONTROLLERS

**GMC-COMBI** Single Track Controller/Transformer  
Most Suited for HO/OO/N Scale Layouts



Fantastic for small layouts or beginners upgrading a starter set, the Combi has both a 12V DC output to run one track, and a 16V AC output for accessories.

**BEST FOR**  
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**GMC-D** Twin Track Controller  
Most Suited to HO/OO/N Scale Layouts



Our best selling controller. Runs a two track railway with minimum of fuss. The D Controller has two 12V DC track outputs, as well as a 16V AC output for accessories.

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Twin Tracks

**GMC-Q** Four Track Controller  
Most Suited to OO/HO/N Scale Layouts



The best selling four track controller available today. It offers impressive value for money with its four 12V DC track outputs, and two of 16V AC and 12V DC outputs for accessories.

**BEST FOR**  
Four Tracks

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**GMC-100** Single Track Panel Controller  
Most Suited to OO/HO/N/Z Scale Layouts



Some modellers may wish to incorporate their controller into an overall panel to control their layout. The Model 100 Controller has a single 12V DC output.

**BEST FOR**  
Control Panels

**GMC-U** Single Track Controller with Simulation  
Most Suited to OO/HO/N/Z Scale Layouts



With the brake knob controlling the 12V DC track output, this controller allows you greater realism when running locomotives.

**BEST FOR**  
Realistic Running

**GMC-W** Single Track Walkabout Single Track Controller  
Most Suited to OO/HO/N/Z Scale Layouts



Fitted with 1.5m of cable, this controller allows you the freedom to move around your layout while still controlling your layout. It has a single 12V DC track output.

**BEST FOR**  
Flexibility

#### Did you know...

If you don't have a handy accessory output from a controller, the **GMC-WM1** Wall Mounted Transformer can be used on its own to power accessories, such as point motors and lights from the 16V output. Just plug it in and connect it up!

**GMC-WM1**  
Wall Mounted  
Transformer  
16V or 12V DC 1.1a



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#### Cased Controllers

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| GMC-COMBI   | Single Track Controller/Transformer          | £47.50  |
| GMC-100M    | Single Track Controller                      | £120.00 |
| GMC-100MO   | Single Track Controller for O Scale          | £125.00 |
| GMC-10LGB   | Single Track Controller for G Scale          | £130.00 |
| GMC-10LGBSF | Single Track Controller for G Scale with Fan | £220.00 |
| GMC-P       | Single Track Controller with Simulation      | £130.00 |
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| GMC-T5      | Three Track Controller with Simulation       | £225.00 |
| GMC-Q       | Four Track Controller                        | £235.00 |

#### Panel Mounted Controllers (Orange text shows transformer required)

|           |   |         |
|-----------|---|---------|
| GMC-100   | Single Track Controller (GMC-T1/M1)                       | £47.50  |
| GMC-100.O | Single Track Controller for O Scale (GMC-T2/M2)           | £52.50  |
| GMC-U     | Single Track Controller with Simulation (GMC-T1/M1/WN1)   | £57.50  |
| GMC-UF    | Single Track Controller with Feedback (GMC-T1/M1/WN1)     | £52.50  |
| GMC-UD    | Single Track Controller with Simulation for O (GMC-T2/M2) | £75.00  |
| GMC-UD5   | Twin Track Controller (GMC-T1/M1/WN1)                     | £67.50  |
| GMC-UD5   | Twin Track Controller with Simulation (GMC-T1/M1/WN1)     | £95.00  |
| GMC-UQ    | Four Track Controller (GMC-T1/M1/WN1)                     | £105.00 |

See the GM365 Gaugemaster Catalogue (£5.00) for more details

#### Walkabout and Hand Held Controllers

(No GMC-T1/M1/WN1)

|        |  |        |
|--------|--|--------|
| GMC-W  | Single Track Walkabout Controller              | £42.50 |
| GMC-HH | Single Track Handheld Controller with Feedback | £46.50 |

#### Transformers

|         |  |        |
|---------|--|--------|
| GMC-M1  | Cased Transformer 16V AC                               | £62.50 |
| GMC-M2  | Cased Transformer 18V AC 2.5V                          | £62.50 |
| GMC-M3  | Cased Transformer 24V AC                               | £62.50 |
| GMC-M4  | Cased Transformer 12V AC                               | £62.50 |
| GMC-T1  | Open Transformer 2x 16V AC 1a                          | £32.50 |
| GMC-T2  | Open Transformer 18V AC 2.5a                           | £32.50 |
| GMC-T3  | Open Transformer 24V AC                                | £32.50 |
| GMC-T4  | Open Transformer 2x 12V AC 1a                          | £32.50 |
| GMC-WM1 | Wall Mounted Transformer 16V or 12V DC 1.1a            | £26.00 |
| GMC-WM2 | Wall Mounted Transformer 9v DC (1600mA) 1.6A           | £21.00 |
| GMC-WM3 | Wall Mounted Transformer Kato Unitrack UK Power Supply | £21.00 |
| GMC-WM4 | Wall Mounted Transformer 12v DC Smoothed for Lighting  | £21.00 |
| GMTFK1  | Transformer Fitting Kit for T1/T3/T4                   | £15.50 |
| GMTFK2  | Transformer Fitting Kit for T2                         | £15.50 |

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EXCITING NEW MODELS IN OO9, OO AND O GAUGE!

### CENTRE HEADCODE CLASS 40 O GAUGE

AVAILABLE SOON



|      |             |  |
|------|-------------|--|
| 4060 | Class 40 V2 | BR green (unnumbered)                                  |
| 4061 | Class 40 V2 | BR green with small yellow panels (unnumbered)         |
| 4062 | Class 40 V2 | BR green with full yellow ends (unnumbered)            |
| 4063 | Class 40 V2 | BR blue with full yellow ends (2 x arrow) (unnumbered) |
| 4064 | Class 40 V2 | BR blue 40155 (with domino headcodes)                  |

SRP **£549.00**

### 'OO9' L&BR BALDWIN 2-4-2T LYN

- #9980 Baldwin 2-4-2T Lyn Black - undecorated
- #9981 Baldwin 2-4-2T Lyn L&BR dark green (pre-1906)
- #9982 Baldwin 2-4-2T Lyn L&BR dark green (1906-22)
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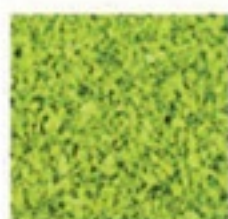
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# APT Advanced Passenger Train

## 00 GAUGE MODEL RAILWAYS

The Advanced Passenger Train (APT) programme is remembered by many as one of British Rail's greatest embarrassments. The APT-P multiple units incorporated ideas that were well ahead of their time, most famously including a computerised tilting system which allowed the trains to navigate the tight turns of Britain's ageing railways up to 40% faster than anything else of the time. The first APT-P unit was introduced in 1979 and the trains were progressively developed until 1987, the

highlight being a period of passenger service between 1983 and 1985. Ultimately the complexity and cost of the unreliable APT saw it replaced by the simpler and considerably cheaper IC125 HST programme, but lessons learnt were incorporated into the IC225 programme and later the tilting Alstom Pendolino trains still operating on the West Coast Mainline.



**R3873** BR, Class 370 Advanced Passenger Train, Sets 370 003 and 370 004, 5-Car Train Pack - Era 7

DCC READY 8 PIN CONNECTION

**R3874** BR, Class 370 Advanced Passenger Train, Set 370 001 and 370 002, 7-Car Train Pack - Era 7

DCC READY 8 PIN CONNECTION



**R40011** BR, Class 370 Advanced Passenger Train 2-Car TS Coach Pack, 48203 + 48204 - Era 7

**R40012** BR, Class 370 Advanced Passenger Train 2-Car TRBS Coach Pack, 48403 + 48404 - Era 7

**R40011A** BR, Class 370 Advanced Passenger Train 2-Car TS Coach Pack, 48201 + 48202 - Era 7

**R40012A** BR, Class 370 Advanced Passenger Train 2-Car TS Coach Pack, 48201 + 48202 - Era 7



**R40013** BR, Class 370 Advanced Passenger Train 2-Car TU Coach Pack, 48303 + 48304 - Era 7

**R40014** BR, Class 370 Advanced Passenger Train 2-Car TF Coach Pack, 48503 + 48504 - Era 7

**R40013A** BR, Class 370 Advanced Passenger Train 2-Car TU Coach Pack, 48301 + 48302 - Era 7

**R40014A** BR, Class 370 Advanced Passenger Train 2-Car TF Coach Pack, 48501 + 48502 - Era 7



**R3948** BR, Class 370 Advanced Passenger Train Non-Driving Motor (NDM), 49004 - Era 7

**R4970** BR, InterCity APT-U Ex-TS Development Vehicle, Sc48204/977527 - Era 7



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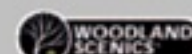
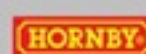
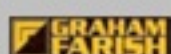
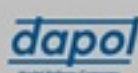
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|   |      |        |
|---|------|--------|
| 1 1/4" wide x 1/16" thick x 98" long..... | each | £14.74 |
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|                                 |      |       |
|---------------------------------|------|-------|
| Natural 0.5/1.0mm, 80g bag..... | each | £5.70 |
| Natural 2.0/3.0mm, 80g bag..... | each | £5.70 |
| Natural 4.0/5.0mm, 80g bag..... | each | £5.70 |

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| Grey 2.0/3.0mm, 80g bag.....        | Down to each | £5.70 |
| Light green 0.5/1.0mm, 80g bag..... | Down to each | £5.70 |
| Mid green 0.5/1.0mm, 80g bag.....   | Down to each | £5.70 |

#### CORK BARK FOR SCENIC DISPLAY

|                    |      |       |
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| Per large bag..... | each | £8.55 |
|--------------------|------|-------|

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# TOWER MODELS DISCOUNT 'O' GAUGE

## FULLY FINISHED R-T-R LOCOS

|                                   |               |
|-----------------------------------|---------------|
| L&Y/LMS/BR Pug 0-4-0T             | from £525.00  |
| GWR/BR 1361 0-6-0ST               | from £695.00  |
| GWR/BR 8750 Pannier tank          | from £695.00  |
| GWR/BR Parcels/Passenger railcars | from £810.00  |
| LNER/BR J39 0-6-0                 | from £950.00  |
| LMS/BR Original Patriot           | from £1650.00 |

Over 100 R-T-R locos available off the shelf

## GLADIATOR LOCO KITS

|                              |         |
|------------------------------|---------|
| GL1 LNER/BR D10/11 4-4-0     | £340.00 |
| GL20 LMS/BR Princess         | £460.00 |
| GL21 LMS/BR Royal Scot       | £400.00 |
| GL24 LMS/BR Original Patriot | £400.00 |
| GL36 LBSC/SR/BR H2 Atlantic  | £410.00 |
| GL50 LNER/BR 2-8-2 Garratt   | £650.00 |
| GL58 LNER/BR B1 4-6-0        | £380.00 |
| GL59 LNER/BR K1 2-6-0        | £370.00 |
| GL70 SR/BR Schools 4-4-0     | £410.00 |

## TOWER COLLECTION LOCO KITS

|                                       |         |
|---------------------------------------|---------|
| TC1 Class 02 Diesel complete kit      | £250.00 |
| TC2 LMS/BR Streamlined Coronation     | £595.00 |
| TC3 Andrew Barclay 0-4-0 complete kit | £250.00 |
| TC5 LMS/BR De-streamlined Duchess     | £595.00 |
| TC7 LMS/BR Black Five                 | £595.00 |
| TC14 LMS/BR Stanier 8F 2-8-0          | £595.00 |

## D.J.H. LOCO KITS

|                                      |         |
|--------------------------------------|---------|
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| K301 BR Standard Class 2 2-6-0       | £502.70 |
| K302 LMS / BR Ivatt 2P 2-6-2T        | £438.90 |
| K303 LMS / BR Fairburn 2-6-4T        | £456.50 |
| K320 LMS / BR 2MT 2-6-0              | £502.70 |
| K338 LNER/BR G5 0-4-4T complete kit  | £502.70 |
| K340 Class 14 complete kit           | £502.70 |
| K344 NEW LNER/BR Hunt Class 4-4-0    | £577.50 |
| K345 NEW LNER/BR Thompson A2/1 4-6-2 | £721.60 |

## PECO 'O' GAUGE TRACKWORK

|                                   |        |
|-----------------------------------|--------|
| SLE786BH Curved R/H Point         | £58.00 |
| SLE787BH Curved L/H Point         | £58.00 |
| SLE700BH 12 yards flexitrack      | £85.75 |
| SLE791BH Right Hand Point         | £46.25 |
| SLE792BH Left Hand Point          | £46.25 |
| SLE797BH Y Point                  | £46.25 |
| STU750BH Right Hand Setrack Point | £44.25 |
| STU751BH Left Hand Setrack Point  | £44.25 |
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| SL740BH Buffer stop               | £3.00  |
| ST700 Setrack Straights (8)       | £48.00 |
| ST725 Setrack Curves (8)          | £51.25 |
| PL80 Power Feed                   | £7.50  |
| TP1 Long Track pins (15mm)        | £3.25  |

## SLATERS COACH AND VAN KITS

|                                |              |
|--------------------------------|--------------|
| GWR 4 wheel coaches 4 types    | £79.00       |
| GWR bogie clerestory coaches   | £137.00      |
| MR/LMS 6 wheel coaches 7 types | £86.00       |
| MR/LMS bogie coaches 4 types   | from £131.00 |
| SR 50" Bogie Van 8             | £126.00      |
| SR 4 wheel PLV/GUV             | £80.00       |
| GWR 6 wheel milk Syphon        | £79.00       |
| LNER/BR Goods brake van        | £49.00       |
| LMS/BR Goods brake van         | £49.00       |

## DAPOL R-T-R WAGONS

|                                  |        |
|----------------------------------|--------|
| 5 Plank Open various             | £39.90 |
| 7 Plank Open various             | £39.90 |
| 8 Plank Open various             | £39.90 |
| 6 wheel Milk Tank 5 types        | £70.00 |
| Pillbox brake 7 types            | £68.50 |
| B.R. Vanwide/VEA                 | £49.80 |
| B.R. Plank Van 2 types           | £43.75 |
| B.R. Plywood Van 2 types         | £43.75 |
| B.R. Meat Van                    | £43.75 |
| B.R. Insulated Van               | £43.75 |
| B.R. 10ft WB Open Wagons 3 types | £39.95 |
| Private Owner Salt & Lime Wagons | £43.75 |
| Banana Vans                      | £43.75 |

## DAPOL BOGIE BOLSTER & TURBOT

|                            |                  |
|----------------------------|------------------|
| 6 Liveries Bogie Bolster E |                  |
| 6 Liveries Turbot          |                  |
| RRP £72.40                 | Our price £61.54 |

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|  |                   |
|--|-------------------|
| 45xx Prairie Tank                          |                   |
| Great Western, Shireburn, British Railways |                   |
| Early and Late BR liveries available       |                   |
| RRP £512.30                                | Our price £435.48 |
| Also available with DCC and Sound          |                   |
| RRP £672.75                                | Our price £571.84 |

## NEW DAPOL B.R. MARK 1 COACHES

|  |                   |
|--|-------------------|
| Available as CK, SO, SK and BSK in crimson & cream, S.R. green, blue & grey, chocolate & cream and maroon, numbered or un-numbered |                   |
| Basic RRP £205.97  | Our price £175.07 |
| DCC fitted RRP £242.19   | Our price £205.86 |

## DAPOL/LIONHEART PANNIER & AUTO COACH

|                                |                   |
|--------------------------------|-------------------|
| 64xx or 74xx GWR or BR Pannier |                   |
| List price £399.00             | Our price £340.00 |
| Autocoach GWR or BR            |                   |
| List price £200.00             | Our price £170.00 |

## NEW DAPOL 14/48/58xx

|  |                   |
|--|-------------------|
| 0-4-2T G.W.R. or B.R.  |                   |
| Available in various G.W.R. & B.R. liveries, autofitted or not, numbered or un-numbered, basic, DCC or DCC sound fitted. |                   |
| BASIC RRP £232.88  | Our price £197.95 |
| Sound fitted RRP £414.00   | Our price £351.90 |

LAST FEW

## DAPOL 57xx & 8750

|                            |                   |
|----------------------------|-------------------|
| FULLY FINISHED R.T.R. LOCO |                   |
| GWR or BR Liveries         |                   |
| RRP £235.00                | Our price £206.75 |

## TOWER MODELS

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| 1950's:                               |  |
| Red Salt Vans (9 types) £42.50 each   |  |
| NOW AVAILABLE                         |  |
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| Blue Salt Vans (4 types) £42.50       |  |
| 5 plank Lime/Alkali (10 types) £40.00 |  |
| NOW AVAILABLE                         |  |

## DAPOL 121 & 122

|                       |                   |
|-----------------------|-------------------|
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| RRP £309.47           | Our price £263.05 |
| DCC Sound RRP £516.47 | Our price £439.00 |
| AVAILABLE NOW         |                   |

## HELJAN CLASS 45

|  |                   |
|--|-------------------|
| 6 versions from original green to later blue |                   |
| RRP £699.00                                  | Our price £550.00 |
| NOW AVAILABLE                                |                   |

## HELJAN CLASS 117 3 CAR DMU

|                                       |                   |
|---------------------------------------|-------------------|
| Choice of liveries (see website)      |                   |
| RRP £1100.00                          | Our price £935.00 |
| AVAILABLE LATE 2020 RESERVE YOURS NOW |                   |

## HELJAN CLASS 17 CLAYTON

|                      |                   |
|----------------------|-------------------|
| 4 Liveries available |                   |
| RRP £549.00          | Our Price £466.65 |

## NEW HELJAN CLASS 50

|                            |                   |
|----------------------------|-------------------|
| 3 Liveries Remaining       |                   |
| RRP £699.00                | Our price £594.15 |
| NOW AVAILABLE SELLING FAST |                   |

## NEW HELJAN CLASS 26 & 27

|                       |                   |
|-----------------------|-------------------|
| Various Liveries      |                   |
| RRP £599.00           | Our price £509.15 |
| AVAILABLE SPRING 2022 |                   |

## NEW HELJAN CLASS 37/4

|                                       |                   |
|---------------------------------------|-------------------|
| Choice of Five liveries (see website) |                   |
| RRP £599.00                           | Our price £509.15 |
| AVAILABLE NOW SELLING FAST            |                   |

## NEW HELJAN GWR/BR PRAIRIE

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|----------------------------------|-------------------|
| 5 standard versions              |                   |
| 7 limited editions               |                   |
| RRP £699.00                      | Our price £594.15 |
| NOW AVAILABLE                    |                   |
| See our website for full details |                   |

## NEW HELJAN 43xx MOGUL

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|--|-------------------|
| 7 standard versions in GWR & BR                        |                   |
| all un-numbered or 3 limited edition numbered versions |                   |
| RRP £779.00  | Our price £662.15 |
| NOW AVAILABLE  |                   |

## HELJAN WARSHIPS

|                       |                   |
|-----------------------|-------------------|
| 10 Liveries Available |                   |
| RRP £699.00           | Our Price £550.00 |

## NEW HELJAN CLASS 03

|                         |                   |
|-------------------------|-------------------|
| 14 versions RRP £395.00 | Our price £335.75 |
| SEE WEBSITE FOR DETAILS |                   |

## HELJAN DIESEL DEPOT

|                                   |                  |
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| Two Road Shed RRP £89.00          | Our price £75.00 |
| Extension Pack RRP £39.00         | Our price £35.25 |
| Spare Ends (3) RRP £19.00         | Our price £16.00 |
| SEE WEBSITE FOR DETAILS AND SIZES |                  |

## NEW HELJAN CLASS 40

|                            |                   |
|----------------------------|-------------------|
| Centre Headcode 5 Liveries |                   |
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| NOW AVAILABLE              |                   |

## NEW HELJAN CLASS 47

|                                 |                   |
|---------------------------------|-------------------|
| 2 Versions, 1960/70s & 1980/90s |                   |
| Eleven liveries planned         |                   |
| RRP £699.00                     | Our price £594.15 |
| AVAILABLE WINTER 2021           |                   |

## NEW HELJAN CLASS 56

|                       |                   |
|-----------------------|-------------------|
| Total of 8 liveries   |                   |
| RRP £699.00           | Our Price £594.15 |
| AVAILABLE WINTER 2021 |                   |

## HELJAN DELTIC

|                            |                   |
|----------------------------|-------------------|
| Limited Re-run 2 Liveries  |                   |
| List Price £599.00         | Our Price £509.15 |
| NOW AVAILABLE SELLING FAST |                   |

## HELJAN CLASS 31

|                                     |                   |
|-------------------------------------|-------------------|
| Limited Re-run 4 Liveries remaining |                   |
| List Price £549.00                  | Our Price £466.65 |
| AVAILABLE NOW, SELLING FAST         |                   |

## HELJAN CLASS 31/1 & 31/4

|                                    |                   |
|------------------------------------|-------------------|
| 9 Liveries to be produced          |                   |
| List Price £549.00                 | Our Price £466.65 |
| AVAILABLE SUMMER RESERVE YOURS NOW |                   |

## HELJAN CLASS 35 HYMEK

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| List Price £495.00        | Our Price £420.75 |
| AVAILABLE NOW             |                   |

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|                                 |                   |
|---------------------------------|-------------------|
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| Dark green with yellow panel    |                   |
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|                   |                     |
|-------------------|---------------------|
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|-------------------|---------------------|

|                              |                                    |                                 |
|------------------------------|------------------------------------|---------------------------------|
| HN602 Pipes on pallets £6.55 | HN604 Tarpaulin Covered Load £6.00 | HN605 Crates part covered £6.98 |
|------------------------------|------------------------------------|---------------------------------|

|                       |                        |                       |
|-----------------------|------------------------|-----------------------|
| HN642 Bags grey £6.00 | HN641 Bags brown £6.00 | HN607 Coal Load £7.78 |
|-----------------------|------------------------|-----------------------|

|                    |                            |             |
|--------------------|----------------------------|-------------|
| HN606 Crates £6.00 | HN608 Coal Sacks (2) £7.78 | HN652 Green |
|--------------------|----------------------------|-------------|

|                                     |           |   |
|-------------------------------------|-----------|---|
| Oil/Chemical Drum Groups £7.78 each | HN645 red | Other colours are: HN650 Red HN651 Blue |
|-------------------------------------|-----------|---|

|             |             |            |
|-------------|-------------|------------|
| HN646 green | HN647 brown | HN648 blue |
|-------------|-------------|------------|

|                          |                          |                             |
|--------------------------|--------------------------|-----------------------------|
| FL102 New Sleepers £5.86 | FL115 Plates Load £11.71 | FL109 Cowl Mixed Load £5.86 |
|--------------------------|--------------------------|-----------------------------|

|                        |                                  |                         |
|------------------------|----------------------------------|-------------------------|
| FL116 Coil Load £11.71 | FL134 Sacks of Stone Chips £7.61 | FL121 Large Sacks £5.86 |
|------------------------|----------------------------------|-------------------------|

|                               |                              |                           |
|-------------------------------|------------------------------|---------------------------|
| FL122 Cement Bags Brown £5.86 | FL123 Cement Bags Grey £5.86 | FL101 Timber Planks £5.86 |
|-------------------------------|------------------------------|---------------------------|

|                        |                         |                        |
|------------------------|-------------------------|------------------------|
| FL110 Cases load £5.86 | FL111 Crates load £5.86 | FL100 Wood Beams £5.86 |
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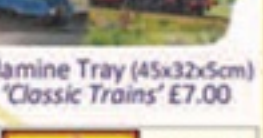
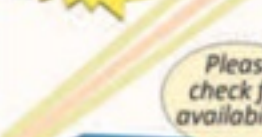
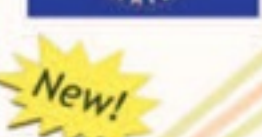
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## '00' Gauge

Crates - Sacks - Freight

|                          |                                  |                              |                                  |                                    |
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| FL181 Lumber Load £10.51 | FL122 Cement Bags Brown £5.86    | FL123 Cement Bags Grey £5.86 | FL110 Cases load £5.86           | FL111 Crates load £5.86            |
| FL180 OTA Load £11.71    | FL127 Aggregate Sacks £7.61      | FL101 Timber Planks £5.86    | FL100 Wood Beams £5.86           | FL133 Crates/Sacks shipment £11.71 |
| FL116 Coil Load £11.71   | FL134 Sacks of Stone Chips £7.61 | FL108 Mixed Load £8.21       | FL171 Assorted Merchandise £7.61 | FL120 Covered Load £5.86           |
| FL115 Plates Load £11.71 | FL109 Cowl Mixed Load £5.86      | FL121 Large Sacks £5.86      | FL112 Part covered Crates £7.61  | FL132 Crates shipment £11.71       |



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# Welcome from the Editor

## Unfinished projects

Whilst preparing the Scale Drawings feature for this issue I referred back to the December 1987 issue of RAILWAY MODELLER, in which the drawings of the Fowler 4F 0-6-0 by Ian Beattie were first published. As is often the case when looking back at past editions, I couldn't resist glancing through the rest of the magazine's contents; in doing so I happened across another set of scale drawings for the closely related Fowler 7F 0-8-0. Seeing these struck a chord with me because I had referred to them during my early teens when I set about building a OO model of one of these eight-coupled machines.

I recall how I had considered this prototype to be a sensible choice for my first attempt at scratch-building; there was no outside valve gear to contend with and it had a straight running plate, a parallel boiler, a flat-sided firebox, a straight-sided tender, straight handrails and little in the way of rivet detail - all aspects that should make for a more straightforward locomotive building project, or so I thought.

I fabricated the basic upperworks for the loco largely from styrene, sourced a tender from an Airfix 4F and then set about building the chassis.

A fellow member of Swindon Model Railway Club, the late John Sondermann, took me under his wing; he imparted his knowledge of modelmaking and taught me how to mark out, cut and drill my own mainframes from plain brass strip. He also taught me how to fashion a suitable dome from a piece of solid brass and how to turn a brass chimney using a lathe.

Unfortunately however, the youthful exuberance with which I had embarked on this project was not

matched by the patience needed to see it through; the model was never finished and I subsequently disposed of the majority of parts in the bin.

25 years on, there is still a part of me that regrets not persevering with my 'Austin Seven' (as these locos were known affectionately amongst enthusiasts). I am sure most readers can share similar experiences of past projects that stalled for whatever reason and were either later disposed of or perhaps continue to languish part-complete in a cupboard.

It's all too easy to reflect negatively upon such occurrences, but often there is a positive that can be taken from an aborted project. With mine it was the invaluable knowledge and competency that I acquired at the time of my introduction to working with brass and soldering, thanks to John. This served me well when, in later years, I successfully completed my first etched brass kit.

Therefore unfinished projects should not be perceived as negative experiences, but rather as evidence of new skills that have been acquired, methods practised and lessons learnt; a positive part of our own railway modelling journey.

## Celebrating 75 years of Peco

In next month's issue we will be marking the 75th anniversary of Peco with a special supplement charting the history of this Devon-based model railway manufacturer, together with a 'behind the scenes' look at how products are manufactured and an update with forthcoming releases. Make sure you pick up your copy of the October issue, which will be on sale from 9 September.

Craig Tiley

You can keep up to date between issues by visiting our new website at [www.peco-uk.com](http://www.peco-uk.com) and our Facebook page. You can also follow us on Twitter: @RailwayModeller



The simple lines of Fowler 7F 0-8-0 No.49511 are captured (with another class member standing alongside) at Newton Heath MPD on 7 September 1956. Photo: Colour Rail

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And for all enthusiasts modelling overseas railways.

**CONTINENTAL MODELLER**

Published on the third Thursday of the preceding month.

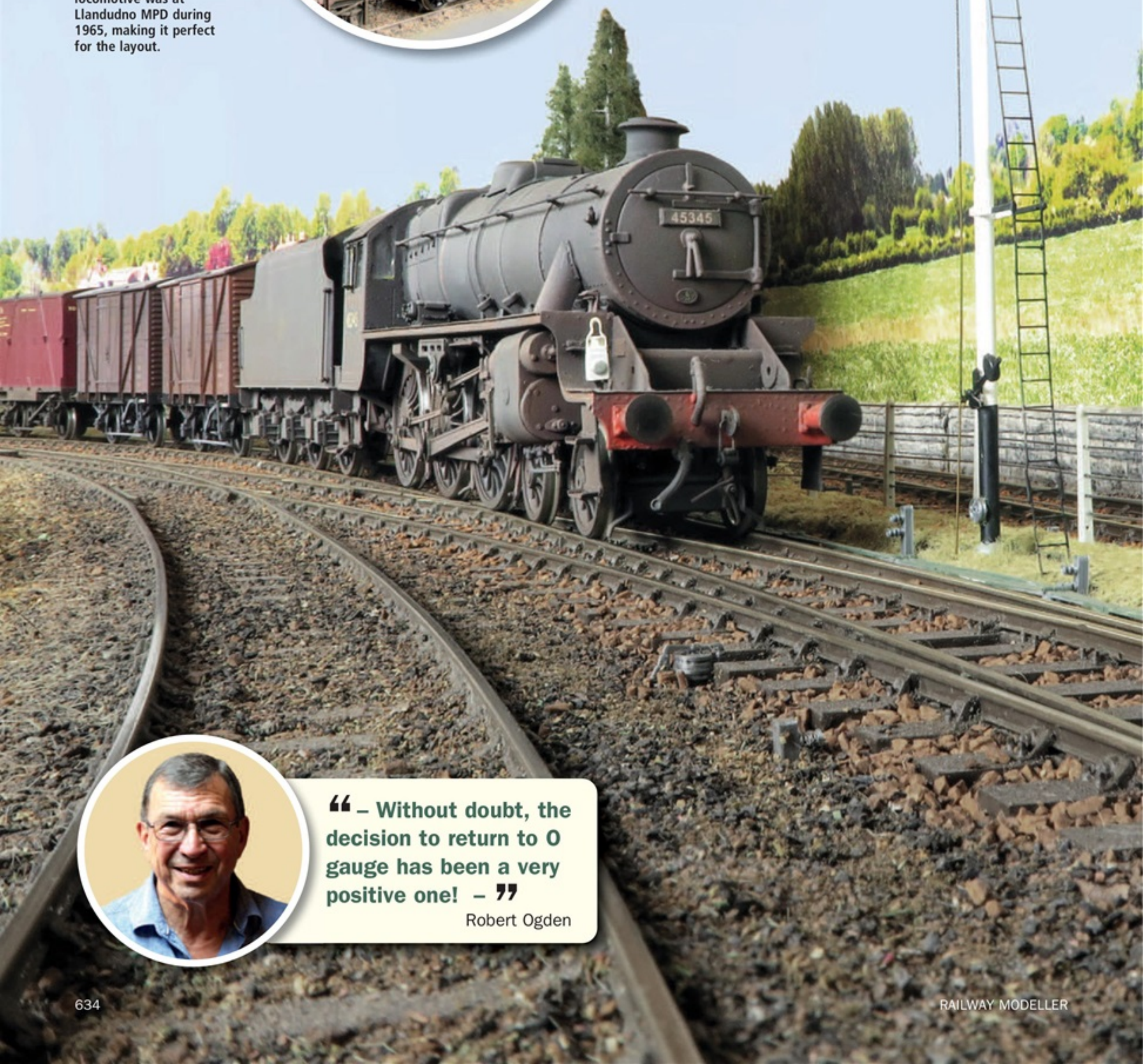


# Railway of the Month

Layout photography by Neil Davis

**Right**  
British Railways Riddles  
Class 2 2-6-0 No.78030  
positions a 13T open  
wagon loaded with pipes  
in the outer siding at  
Amlwch yard. The  
locomotive has been built  
from a DJH Kit.

**Below**  
Stanier 5MT No.45345  
(a Tower Models kit)  
leaves Amlwch with the  
afternoon goods. The  
locomotive was at  
Llandudno MPD during  
1965, making it perfect  
for the layout.



“ – Without doubt, the  
decision to return to O  
gauge has been a very  
positive one! – ”

Robert Ogden



A North Wales prototype modelled in O gauge

# The Amlwch Branch

**ROBERT OGDEN** presents his 7mm scale rendition of the original terminus on the Anglesey Central Railway line. Prototypical operation has been adopted, which can be switched between three different time periods.

My 'Plan of the Month' article that was published in the March 2019 issue described the possibilities of the Amlwch branch and why I chose it as the prototype for my return to O gauge modelling. With the layout now complete, this article sets out to describe my experiences of working in 7mm scale, what has been achieved along the way and the trains that are run.

## Baseboards and power

Baseboard construction followed my usual practice of 12mm plywood tops supported by 2" x 1" battens, the latter being located carefully to avoid the under-board point and signal motors. Operation is via DCC, as per my normal practice, with the entire track layout 'live' to

support sound-fitted locos. However, provision has been made for switching over to 12V dc to enable new locos to be tested before conversion to DCC sound, and to allow visiting locos to run.

## Curving the track layout

The track plan is based closely on how the prototype layout appeared during the 1950s and 1960s, but on more of a curve and with some reduction in length – the platform can accommodate a tender loco and three full length coaches rather than the prototypical four, and all the sidings are slightly shorter. Peco track and points are utilised throughout, with all but one of the points of the curved type. The nominal radii for these are 10' and 5' 3" – the latter is less than the 6' minimum radius normally



## Right

The new DMUs were introduced to North Wales from 1956 and were used on the branch from 1960. Here a Derby Lightweight (Bachmann) waits to carry passengers on a day trip to Bangor.





quoted for finescale six-coupled locomotives but all my models run without problem. Transition curves are used throughout to ensure buffer locking doesn't occur when shunting, with everything tested extensively prior to ballasting the track.

Clean ballast granules were used for the main running line, this being weathered by adding brown paint to the 25% PVA solution that I used to fix the ballast in place. Track in the run-round loop and sidings varies in its condition, some areas with grass and ashes present, whilst in places the sleepers are barely visible.

### Scenics

It is with the scenic modelling that the differences between 7mm and 4mm scale become apparent – the larger scale meaning that any details that are left out are much more obvious. For example, in my opinion the

point rodding, signal wire posts and point levers really do need to be included. I fabricated the point rodding by modifying OO gauge Wills items: the rods themselves are 0.8mm square (approx 1.3" in 7mm scale) so I inserted extra lengths of plastic square section rod between each set of rollers to get the spacing right for O gauge – job done!

Point levers are commercially available as brass kits, and I was fortunate that the signal wire rollers at Amlwch were carried on 3" square wooden posts rather than the usual angle iron. My friend Chris Morgan has a 3D printer and made the posts and rollers for me.

The concrete/packed earth areas in the station yard were fabricated using pieces of card painted brown, with real ash rubbed in. Cobbles/setts are butted up to the rail opposite the goods shed, as per the prototype. Rough grass areas are formed using plaster bandage over

### Above

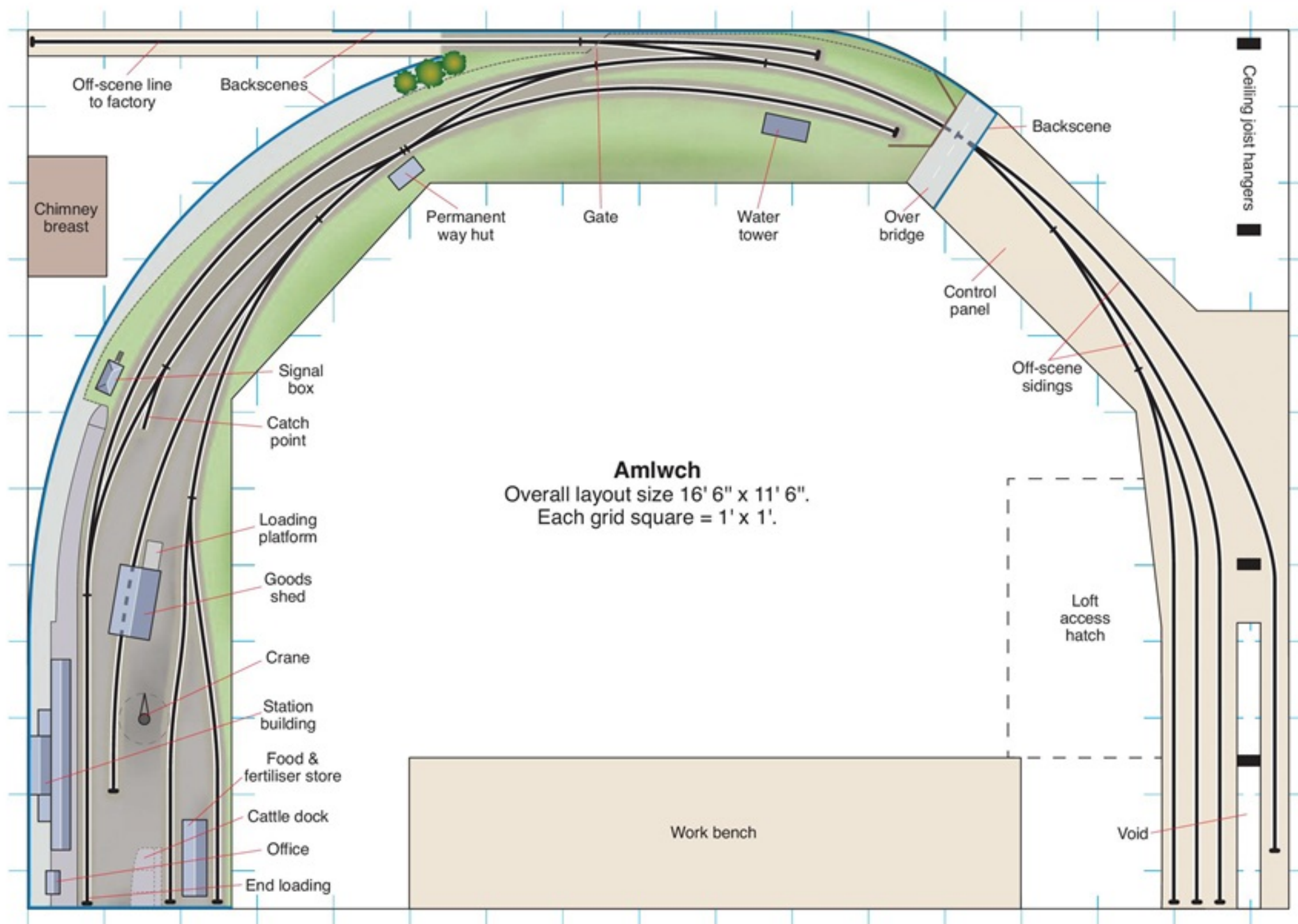
The mid-1960s brought the new diesels to Anglesey. Brand new Sulzer Type 2 Bo-Bo D7538 draws a rake of chlorine tankers away from the Octel siding. The tankers are based on R-T-R frames with scratch-built/3D printed bodywork.



### Left

Ivatt 2MT 2-6-0 No.46418 arrives with a goods. The water tower is a Skytrex product.





crumpled newspaper, or chicken wire where the ground rises towards the overbridge at the end of the scenic section. This is painted brown before adding flock and rubberised horsehair to represent bushes and brambles.

Trees are strategically placed to disguise the line that runs off to the Octel sidings and the end of the backscene by the overbridge.

For the backscene, sheets from the Gaugemaster and Peco ranges have been used.

### Buildings

I enjoy the research that goes into recreating a real location in miniature, studying photos and plans to ensure the buildings and other structures are adequately represented and in the right places. I use kits where appropriate, sometimes modified quite heavily. A good example of this is the station building which utilises Skytrex components with the rest scratch-built. The canopy is entirely scratch-built, with 3D printed roof

### Right

Ex-L&Y 0-6-0 No.52230 positions a couple of vans at Amlwch goods shed. The locomotive is from a Chowbent kit and shows a finish commensurate with its likely condition, just scraping into the layout's time frame before withdrawal in 1961.







trusses, end panels and the rather lovely canopy supports, produced by Chris Morgan from photos of the real things. He is also responsible for the inner framing of the scratch-built food and fertiliser store, the platform fencing posts, cattle dock gates and gradient posts.

The platform itself is scratch-built, with representations of the unusual fencing, flower beds and concrete facing

and edging of the prototype. The goods shed is also based on a Skytrex kit, but with the correct diamond framed windows on the rear elevation. The other main structures are the scratch-built office on the platform, water tower and yard crane from Skytrex and the permanent way hut from Ten Commandments.

### Signalling details

The signals and main line points were controlled from a ground frame in the rather basic signal cabin adjacent to the platform ramp; the single line token apparatus was in the station office. The levers here controlled the home signal, signals and points controlling access from the Octel line, starter and advanced starter signals, the loop points and their facing point locks and the loop catch point. The points in the sidings were operated by the yard staff. I have represented this cabin with a modified





**Left**

Bachmann Brassworks Ivatt 2MT 2-6-0 No.46418 eases into Amlwch with a mixed goods. All the trains that run on the layout are fitted with lamps that are repositioned for each journey.

**Right**

The station building was built from scratch using a number of Skytrex components. The canopy is constructed with 3D printed roof trusses, whilst the end panels and canopy supports were fabricated by Chris Morgan.



Peco kit, whilst the starter and advanced starter signals are Scale Signal Supplies models, powered by servo motors in 3D printed under-board mountings and controlled by a MERG circuit. The points themselves are actuated by Peco motors which are fitted with accessory switches to control the frog polarity.

**Prototype operation**

I always operate to timetables and for *Amlwch* I selected three periods to reflect the changes in locomotives and rolling stock across these times: 1960/1961, 1964 and 1965/1966. I have nearly completed the process of building or obtaining appropriate stock for all three of these.

The timetable I use is derived from the one published for Summer 1960 (see Fig.1) but this is also appropriate for 1963/1964 with minor changes to the timings. There

were seven passenger services plus two branch freights and one chemical tank train each way Monday to Friday. Eight passenger trains ran on Saturdays but only one freight train; there were no Sunday trains. Modeller's licence is applied for 1965/1966 when the line was only retained for the Octel chemical tank traffic. For my purposes, branch passenger and freight services were still operating to a timetable similar to that for 1960.

In 1960/1961, branch passenger services were mainly in the hands of the early generation DMUs which were introduced to North Wales from 1956. The exception was a steam-hauled Saturdays only service, normally comprising two or three coaches, hauled by an Ivatt 2-6-2T. Tender locos – usually Standard Class 2 2-6-0s but occasionally one of Bangor's Aspinall 0-6-0s – handled the goods trains, with 2-6-2Ts heading the Octel traffic.

In the 1963/1964 Summer timetables, the DMUs were utilised elsewhere and there was a return to pull-push working of most passenger services. The locomotives (ex-LMS Ivatt Class 2MT 2-6-2Ts and BR

Standard 2-6-2Ts), were

marshalled at the Bangor end of the trains. Ivatt 2-6-0 tender locos were the mainstay of the freight services, although ex-LMS 2-6-4Ts were also rostered from 1964.

A small amount of modeller's licence was required for the ex-GWR Collett 0-6-0 that appears occasionally, although one of these locos (No.3208) was based at Llandudno Junction MPD from winter 1963 and I have seen a photo of it on snowplough duties on the Amlwch branch. Therefore I don't consider it to be too much of a stretch for it also to

**Below**

Stanier 5MT 4-6-0 No.45345 leaves Amlwch with the afternoon goods as Fairburn 2-6-4T No.42067 stands on the adjacent line awaiting its next duty.

**Fig.1**

|            |                              |            |                       |
|------------|------------------------------|------------|-----------------------|
| 6.36       | 5.45 ex Bangor               | 6.50       | Bangor                |
| 7.35       | 6.40 ex Bangor               | 8.06       | Gaerwen               |
| 9.30       | 7.00 Goods ex Menai Bridge   |            |                       |
| 10.16 (SX) | 9.40 ex Gaerwen              | 10.40 (SX) | Bangor                |
| 10.21 (SO) | 9.45 ex Gaerwen              | 10.55 (SO) | Bangor                |
| 12.33 (SO) | 11.40 ex Bangor              | 11.00      | Goods, Menai Bridge   |
| 13.01 (SX) | 11.00 Goods, ex Menai Bridge | 12.54 (SO) | Bangor                |
| 13.38 (SO) | 12.40 ex Bangor              |            |                       |
| 13.41 (SX) | 13.05 ex Gaerwen             | 14.35 (SO) | Gaerwen               |
| 14.29 (SX) | Chemical tanks, ex Bangor    | 14.05 (SX) | Bangor                |
|            |                              | 15.15 (SX) | Goods, Menai Bridge   |
| 16.42      | 15.50 ex Bangor*             | 16.14 (SX) | Chemical tanks Bangor |
| 18.36 (SX) | 18.00 ex Gaerwen             | 17.10      | Bangor                |
| 18.50 (SO) | 18.15 ex Gaerwen             |            |                       |
| 21.35      | 20.45 ex Bangor              | 19.18      | Bangor                |
|            |                              | 21.40 (SX) | Bangor                |
|            |                              | 22.00 (SO) | Bangor                |

\* (SO) starts at Gaerwen at 10.06



## The Amlwch Branch

Branch line termini can often be perceived to present limited operating potential, but Amlwch (the original terminus of the Anglesey Central Railway, which connected with the LNWR's Bangor to Holyhead main line at Gaerwen) is certainly an exception. Firstly, for several years, during the late 1950s and early 1960s summer timetables, seven passenger trains ran each way Monday to Friday, interspersed with the additional freight services described above. One additional passenger train ran on Saturdays but only one goods train. (However, despite all this activity during the week, there were no trains on Sundays.) Secondly, all train departures, passenger and freight, ran outward from the platform road, the signal cabin containing the single line apparatus being adjacent to it, just prior to the starting signal.

Most of the local passenger trains ran to Bangor and, by the late 1950s, were DMU operated although there was a return to pull-push sets in the 1963 and 1964 summer timetables, as better use was found for the DMUs elsewhere. For steam locos the water supply was situated at the end of the former MPD siding, the shed itself having been demolished in the late 1930s. (Amlwch station building and platform is no more, but the goods shed remains.)

Freight traffic was mainly feed, fertiliser and other agricultural supplies inwards, and produce outward. There were several warehouses at intermediate stations along the branch, as well as at Amlwch itself. Some of the yards faced the Down direction, and others Up so there were usually two daily goods trains Monday to Friday (one on Saturday) to and from the marshalling yard at Menai Bridge. As a result, one of these goods trains did little more than run round at Amlwch.

### Chemical traffic

In 1953 the Associated Ethylene Company (later Associated Octel) opened a large chemical plant at Amlwch which required weekday trains of inbound chlorine in tank wagons. This was used to extract bromine from sea water in the production of ethylene dibromide, which in itself was tanked out to Ellesmere Port for use as a petrol additive in the days when leaded fuel was commonplace. A private line ran about three quarters of a mile from the BR metals, routed through the town itself.



### Above

Ivatt 2MT 2-6-2T No. 41226 awaits departure from Amlwch with the 1710 pull-push service to Gaerwen on 22 August 1964. Photo: Railphotoprints



Notably, steam locos were not permitted over this line. Stock was shunted to a fan of sidings adjacent to the BR station and the factory's own diesel shunter hauled vehicles to and from the plant. The traffic continued for many years even after the line was closed to passenger traffic in 1964. The factory eventually closed in 2003.

### In model form

The operating potential that Amlwch has to offer has spurred its recreation in model form on a number of occasions. One example, built to P4 standards, was presented by David Clarke in the May 2012 issue.



### Left

47 363 prepares to leave the Amlwch chemical works with tankers for Llandudno Junction on 16 August 1993. Photo: Railphotoprints

appear on freight workings. In my scenario, DMUs were back by 1965/1966, with a variety of locomotives on branch freight and chemical tank trains: ex-LMS 2-6-4Ts, Stanier Class 5s, BR Standard Class 4 4-6-0s and Type 2 (later Class 24 and Class 25) diesel locomotives.

Both of my 2-6-2Ts are vacuum pull-push fitted and these or DMUs were rostered for nearly all of the branch passenger workings. The exception was the Saturdays

Only 1140 from Bangor, returning from Amlwch at 1250 which was formed of corridor coaches, but still normally hauled by a 2-6-2T. In the event of DMU failure, whatever steam loco and passenger coaching stock that could be found was substituted.

Most of the passenger and goods rolling stock is appropriate for all three time periods, though I restrict my BR standard guards van to the 1965/66 period,



running only my ex-LMS and ex-GWR guards vans in the earlier periods. Nearly everything is kit-built, with the exception of the DMU and Ivatt 2-6-0 which were both bought as unpainted Bachmann Brassworks models, together with a Heljan Class 25 and some Dapol R-T-R wagons.

No suitable kits exist for the Octel tankers, but plenty of photos are available. I modified Dapol tankers to represent the liquid chlorine wagons and used Heljan class B tanker chassis with scratch-built bodies for the ethylene dibromide models. Chris' printing skills were again invaluable here.

### Operating the layout

I run the layout following the principles set out in Bob Essery's series of books about train operation for the railway modeller. This requires full-size railway practices and procedures to be replicated and time to be allowed, for example, for passengers to embark and disembark, and locomotive servicing. Also, I ensure the correct headlamps and tail lamps are deployed. These are positioned by hand, utilising Slater's hollow lost wax lamps or drilled out white metal lamps: a bit of Blu-tak in the cavity makes for secure fixing on lamp brackets.

Coupling and uncoupling of three-link and screw-link couplings is also undertaken by hand – this is the way it is carried out on the real railway so does it matter if my hand is massively over scale?

Most trains arrive on the main platform line, with all departures from the platform once head and tail lamps have been re-positioned and the single line token obtained from the porter/signalman. DMUs just run to and from the platform but steam locos take water. With pull-push trains, the loco remains coupled to the stock for its run to and from the water tower, but the stock of other passenger trains is propelled clear of the platform before the loco uncouples and uses the run-round loop; after taking water it rejoins the coaches and shunts back into the platform.

The branch goods usually runs directly into the loop before shunting the yard and reforming the departing train. The chemical tank trains arrive on the main line, with the loco running round and shunting the stock to

the off-scene line serving the Octel plant. The loco runs back on scene to take water before collecting the stock and shunting it to the platform prior to departure.

For most of the day, only one train is on scene at any one time, but two are present from 1016 to 1041 and from 1331 to 1405 Monday to Friday, when goods trains are in the yard during passenger arrivals and departures. The afternoon goods is still present until 1515 following the arrival of the Octel train at 1429.

At the end of a running session, stock needs to be drawn off to release the locos that had returned their trains to the fiddle yard, the lamps re-positioned, and fresh locos coupled to the trains ready for the next running session.

### Changing road vehicles

I usually change the road vehicles on the layout to be consistent with the period being operated. This is not a problem with cars and vans as there are plenty of 1:43 scale models available. However, it is more of a problem with lorries, as most commercially available models are to 1:50 scale.

I have managed to acquire a couple of 1:43 scale model lorries which would still have been around in 1960: a Sun Motor Company Austin and a demobbed Edition Atlas Bedford OYD which I have converted to a flat bed coal lorry. Bedford TKs appeared in 1963/64 and the Dinky Toys replicas produced by Atlas Limited Editions are close to 1:43 – I have coal and open wagon versions. There is also an IXO Models British Railways Ford Thames open lorry which is appropriate for all the periods featured.

### Conclusions

I consider *Almwch* to have been a very successful project, although there are still some tasks to complete including the building of the remaining items of stock, together with a fair amount of weathering.

Attempting to operate the layout prototypically has certainly made running the trains an engaging activity, with further interest created by depicting the different time periods. Without doubt, the decision to return to O gauge has been a very positive one!

### Below

It's a summer Saturday and heavier traffic is catered for. Ivatt Class 2MT 2-6-2T No.41227 (a DJH kit) makes light work of its three BR Mk.1 coaches.





## Part 2 – Building the locomotive

# A Fowler 4F in 0

Last month **JOHN COCKCROFT** commenced his build of a Connoisseur Models etched kit for an LMS 4F 0-6-0. With construction of the tender complete, this month he makes a start on the locomotive chassis and superstructure.

Photography by the author

The completed model that forms the subject of this series of articles. The real No.4385 was a North British-built example that was rolled out of the Glasgow company's workshops in 1926.



In the August issue I began my tale of how I built a Connoisseur Models etched metal 7mm scale locomotive kit for the London Midland & Scottish Railway 4F 0-6-0 goods locomotive. With the tender completed I next turned my attention to the locomotive.

Although the chassis and upperworks of the locomotive are designed to be assembled as two separate units, I chose to build them simultaneously to ensure that they would fit

together satisfactorily – it being easier to make adjustments during construction rather than retrospectively.

As a brief re-cap, the Connoisseur Models kit represents an LMS left-hand drive example with features that make it suited to the BR steam era [Ed – For further information about the real machines, refer to the Scale Drawings feature on p646]. However, I model the 1920s period and therefore a number of minor

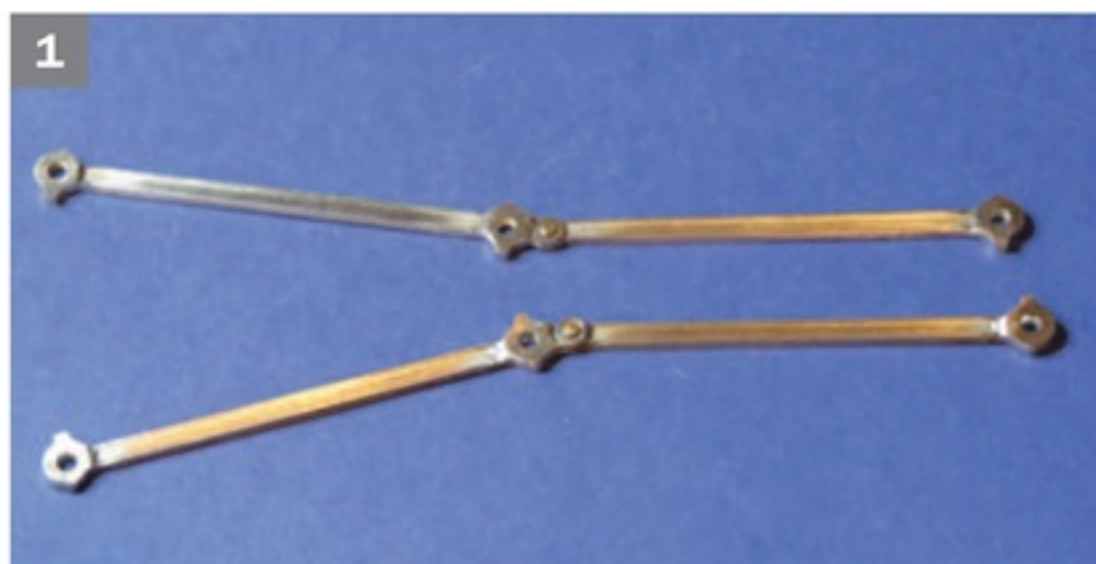
modifications were undertaken along the way to backdate my model to suit.

Wheels, motor and gears are not included with the kit. For this model I have sourced a set of Slater's wheels, together with an ABC mini gearbox and Maxon motor driving the centre axle.

For further information about the kit, together with downloadable sample instructions, go to: [www.jimmcgeown.com](http://www.jimmcgeown.com)

### Building the locomotive

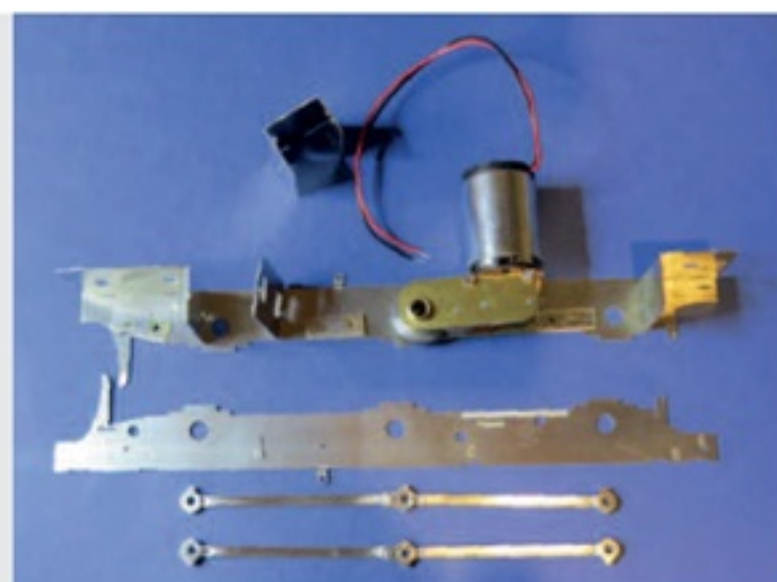
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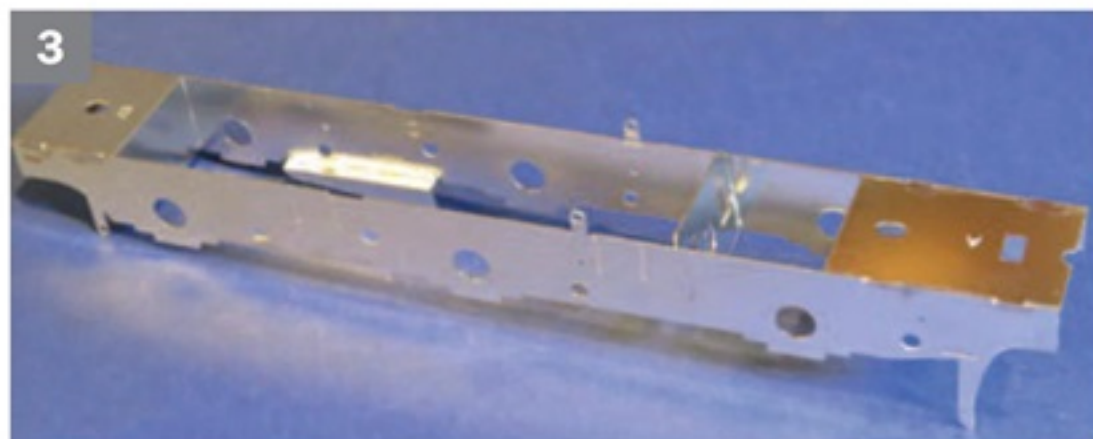
The first components I assemble on a locomotive are the coupling rods. I use these to ensure that the axle spacing on the main frames is exactly in line with the holes in the rods. I adapted them so that they pivot as per the prototype just behind the middle bearing: they have to pivot if you want to install some form of compensation.

2

Here I have cut out and tidied up the frames and spacers and have dry fitted them to determine how the motor and pick-ups will fit. I invariably use plunger pick-ups supplied by Geoff Stratford, these fitting on the rectangular pads that are visible on the frames. I had to omit some of the spacers supplied in the kit as they impeded my usual preference for having the motor driving the middle axle (the supplied instructions recommending that drive should be to the rear axle).



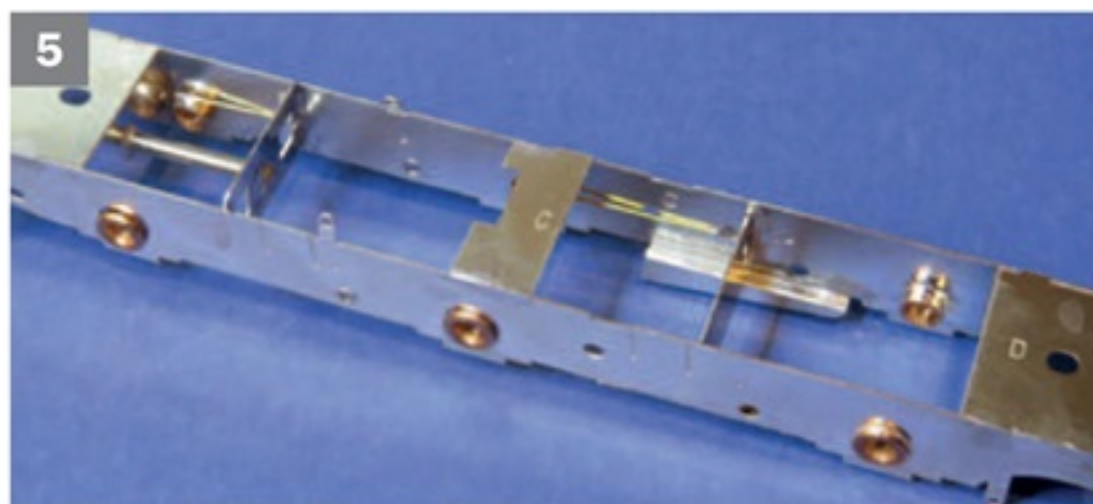




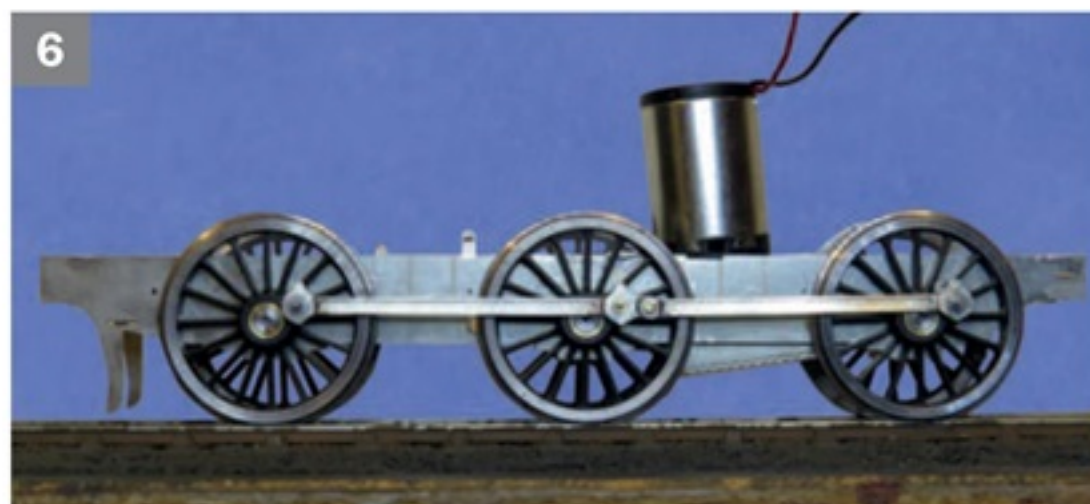
The basic chassis assembled. As recommended in the supplied kit instructions, I fitted the fold-up ashpan etches *before* assembling the frames. Notice also that I have pre-drilled the motion bracket spacer (between the leading and centre axles) to accept one end of a compensation beam that will support the front axle.



Here I have test fitted the wheels and bearings and found that the chassis is very accurate if built without compensation, so you don't need to install it. However, I find that a compensated chassis provides more reliable electrical pick-up because all wheels remain in contact with the running rails.



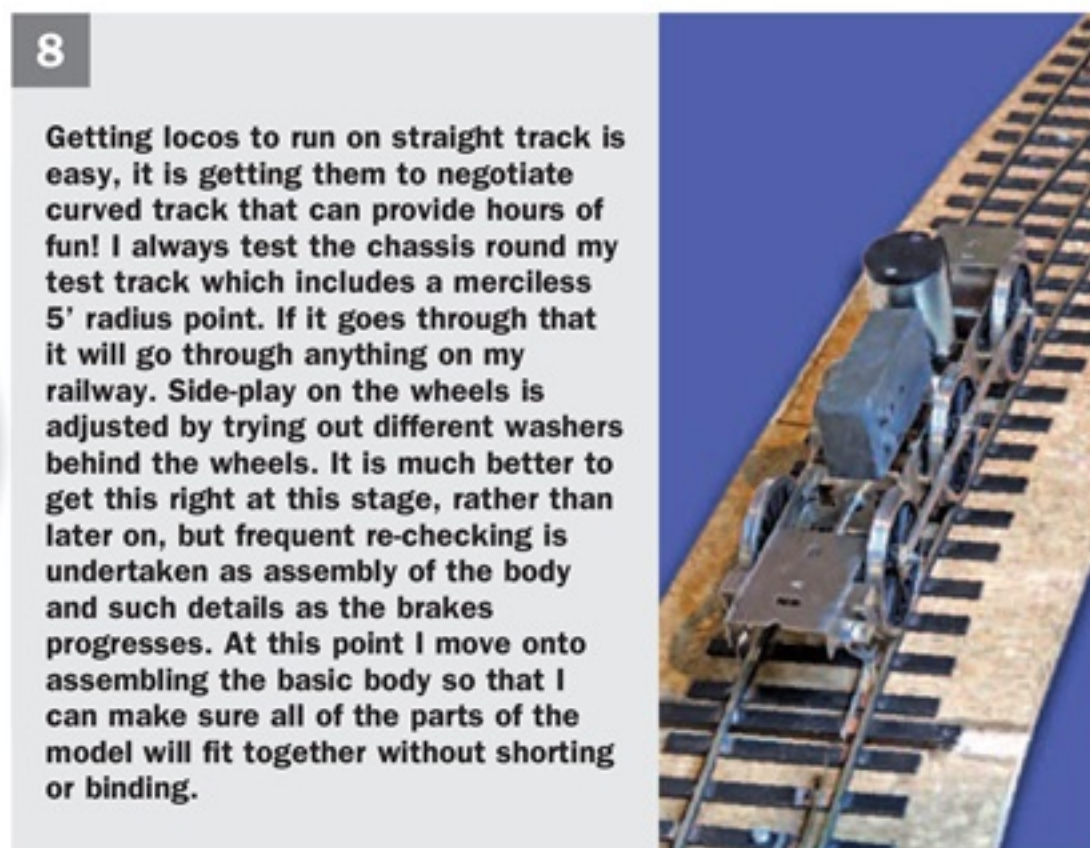
Fitting compensation: the round beam soldered between the front and middle spacers enables the leading axle to rock, whilst the front and middle bearing holes have been elongated slightly in a vertical direction to enable the bearings to move up and down. The leading and centre pairs of bearings are retained in the frames using lengths of wire, with each length soldered to the frames at one end and to the bearing at the other. This enables the bearings to move up and down, but not rotate with the axle. The middle axle is pushed downwards by the bearing wires to ensure that the wheels remain in contact with the rail at all times.



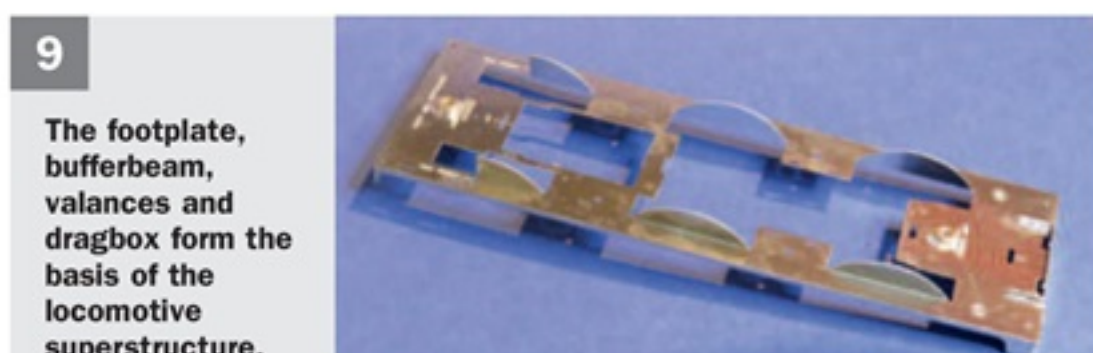
With the bearings fitted, I next tested the chassis using crocodile clips to provide power to the motor. This test allowed me to check that all of the parts are aligned accurately.



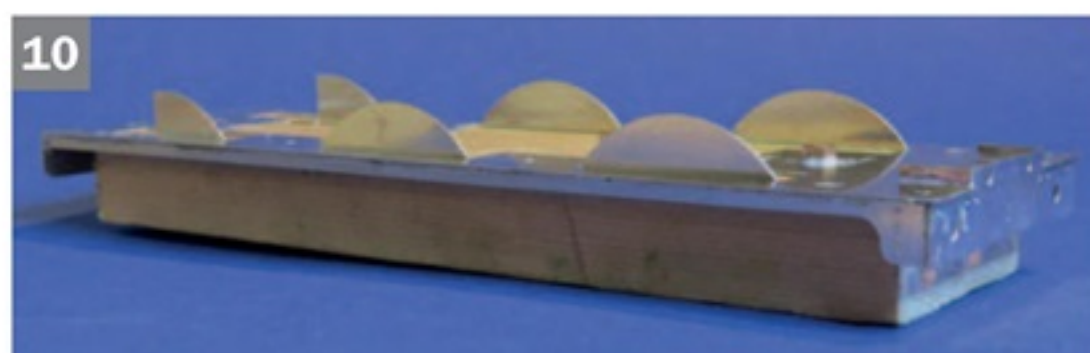
I complete the working parts of the chassis by fitting the plunger pick-ups referred to earlier. They consist of turned plungers with a slotted head inside the frames. A wire spring engages in the slot and pushes the plunger out from the frames onto the inside of the wheel. Here the rear pick-up and the terminal pad have been glued to a spacer so that the motor leads can be connected up.



Getting locos to run on straight track is easy, it is getting them to negotiate curved track that can provide hours of fun! I always test the chassis round my test track which includes a merciless 5' radius point. If it goes through that it will go through anything on my railway. Side-play on the wheels is adjusted by trying out different washers behind the wheels. It is much better to get this right at this stage, rather than later on, but frequent re-checking is undertaken as assembly of the body and such details as the brakes progresses. At this point I move onto assembling the basic body so that I can make sure all of the parts of the model will fit together without shorting or binding.



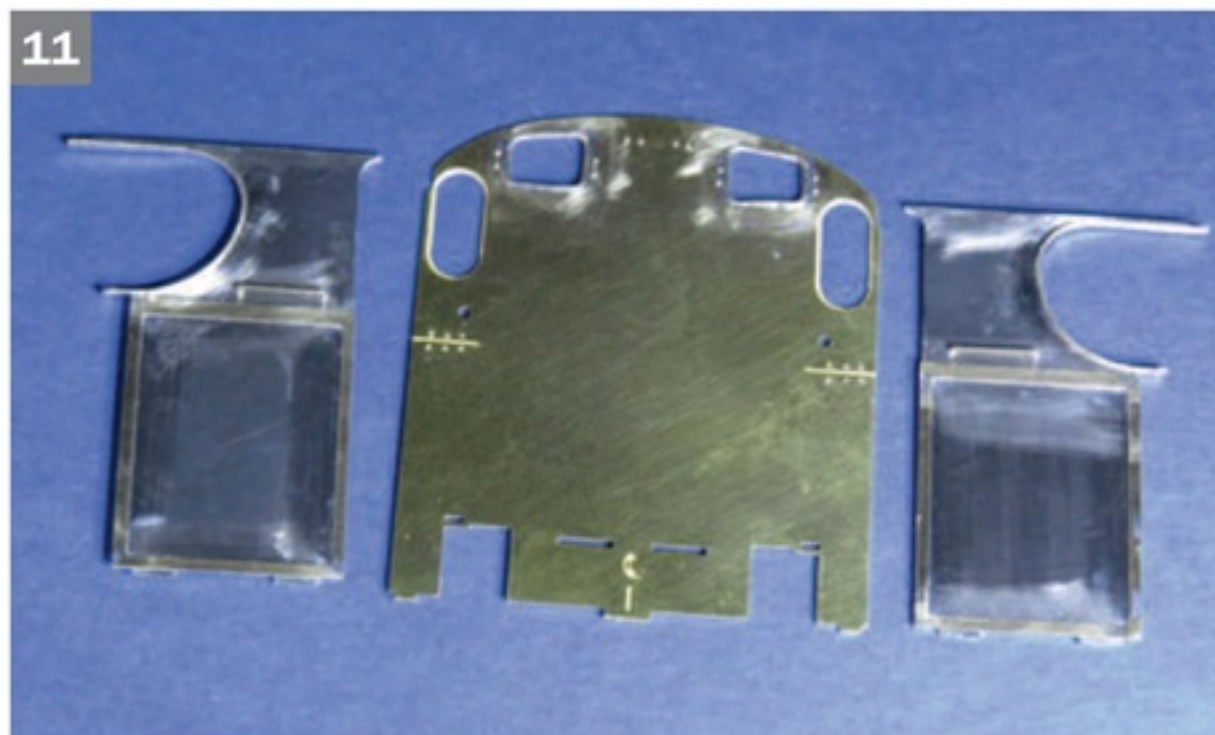
The footplate, bufferbeam, valances and dragbox form the basis of the locomotive superstructure. Before bending up the splasher sides I soldered the other parts to the underside of the footplate in the following order: first the buffer beam, then the valances which I soldered from the buffer beam end, working along tacking then seaming the solder. Finally, the dragbox is attached. Note the sides of the piston rod covers are etched with the buffer beam and bent back.



Once I have attached the valances I cut a wooden platform for the loco body to be supported on; it is easy to bend the footplate during construction if it rests only on the front and back bufferbeams. Here I have bent up the splasher sides and soldered on the two 6BA nuts provided to secure the chassis. The chassis and body were then test fitted to make sure the nuts aligned correctly.

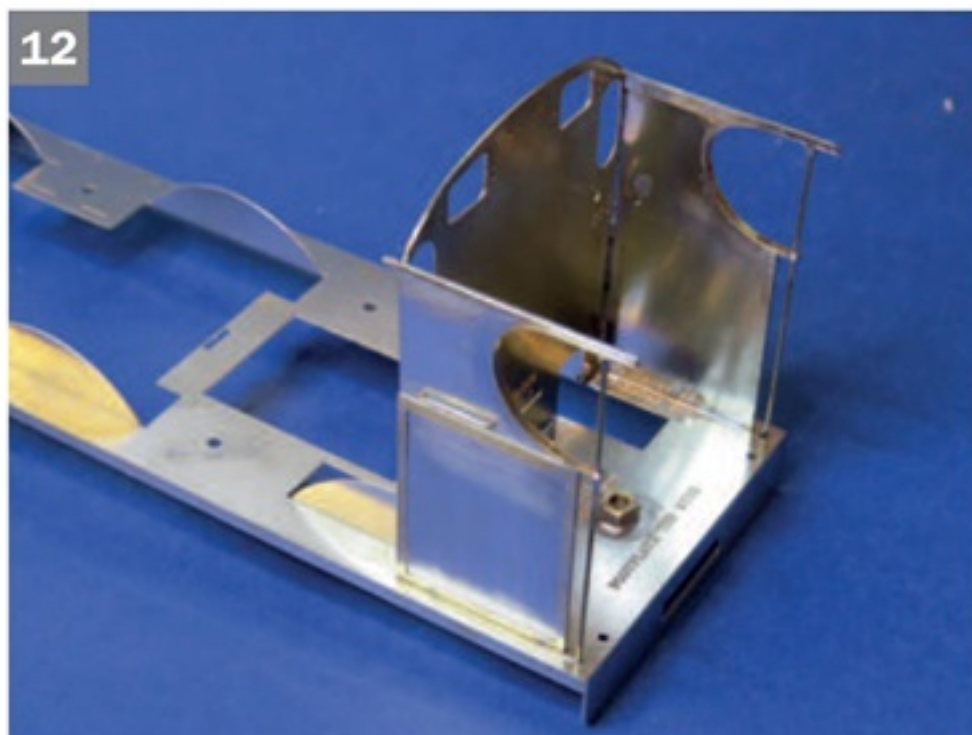


11



As demonstrated in the first article with the construction of the tender, it is a good idea to fit details to certain parts before assembling them. In this case the beading on the cabsides and around the cab cut-outs have been fitted, as have the upper window frames on the spectacle plate and side grab handles. With the beading around the cut-outs, I started with the bottom corner so as to ensure the cab handrail would be located correctly in the hole through the beading.

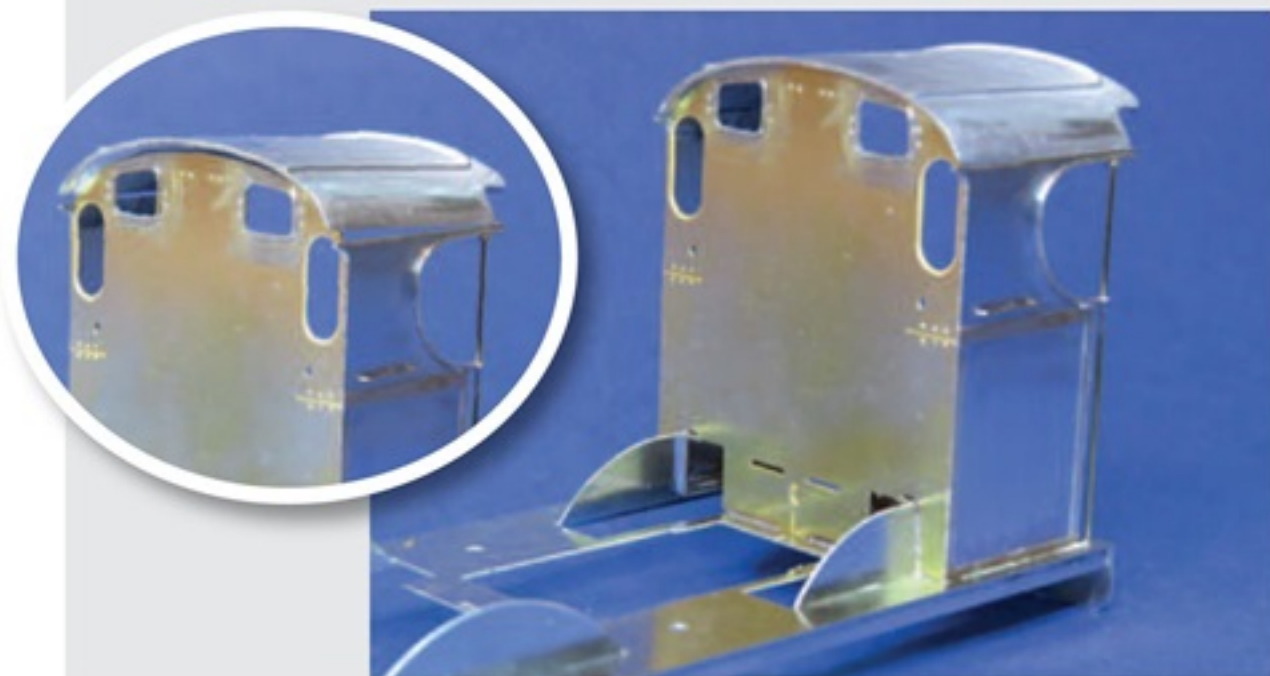
12



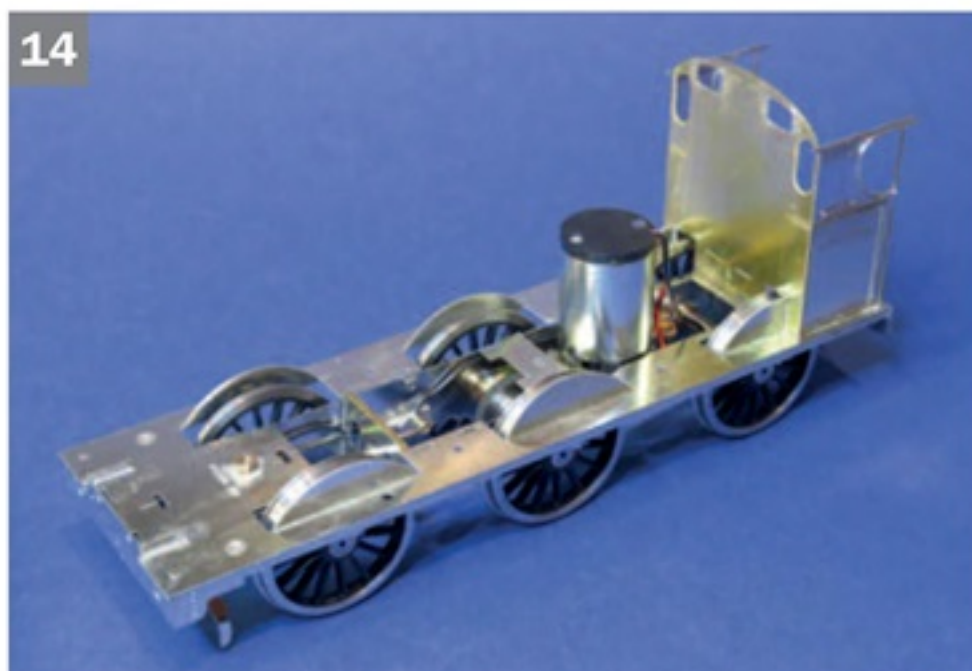
Here the cabsides and front spectacle plate have been assembled onto the footplate. As always, I first tack-soldered the parts together and then once happy everything was tight and square, seam joins were made. I always wash the model with hot water and Cif at the end of every session.

13

The cab roof in this kit is a large white metal casting. The inset view shows how it did not fit satisfactorily as supplied. However, after some fettling a good fit was achieved with the cab side sheets. Because I modelled the loco 'as built' with a smooth roof wrap-over, the join on each side needs to be very tight. But, if you model the late-1930s onwards the LMS thoughtfully put a rain strip right where the join to the roof is, making it much easier to hide! I didn't fix the cab roof at this stage but set it aside to avoid the danger of melting it during subsequent soldering on the model.

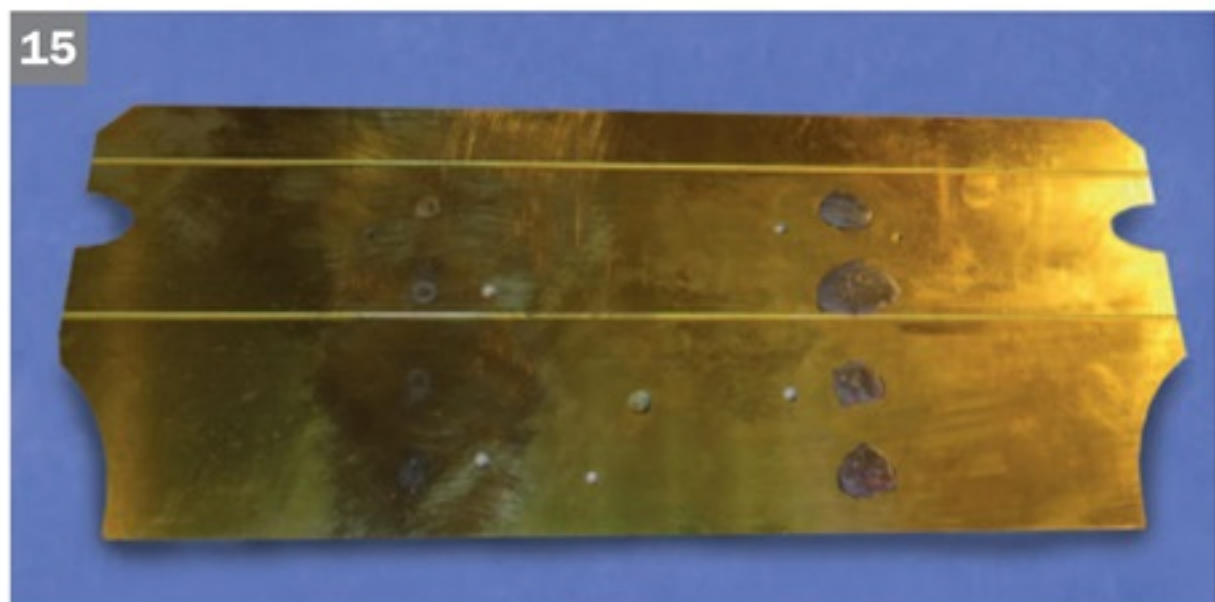


14



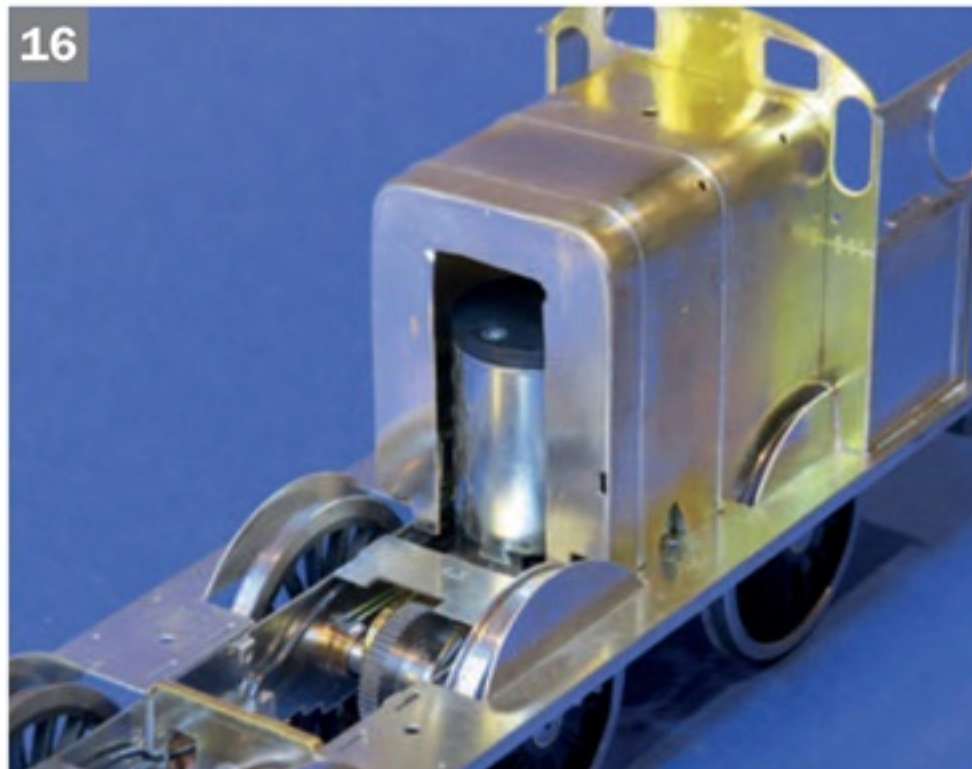
I keep saying that the body and chassis need to be test fitted regularly. Here you can see that I had to file a small, curved rebate on the footplate so that the motor would clear the body. I realised that I would have to make some clearance at the front of the firebox at this stage and cut away part of the etched front prior to assembly.

15



I had to make some minor alterations so that my model would be correct for 1920s condition. For example, the firebox wrapper has washout plugs etched on the sides but these were a later addition. I flooded them with solder and then rubbed them smooth. When bending up the wrapper (using the firebox front as a pattern) I used metal rods to form the curved upper edges.

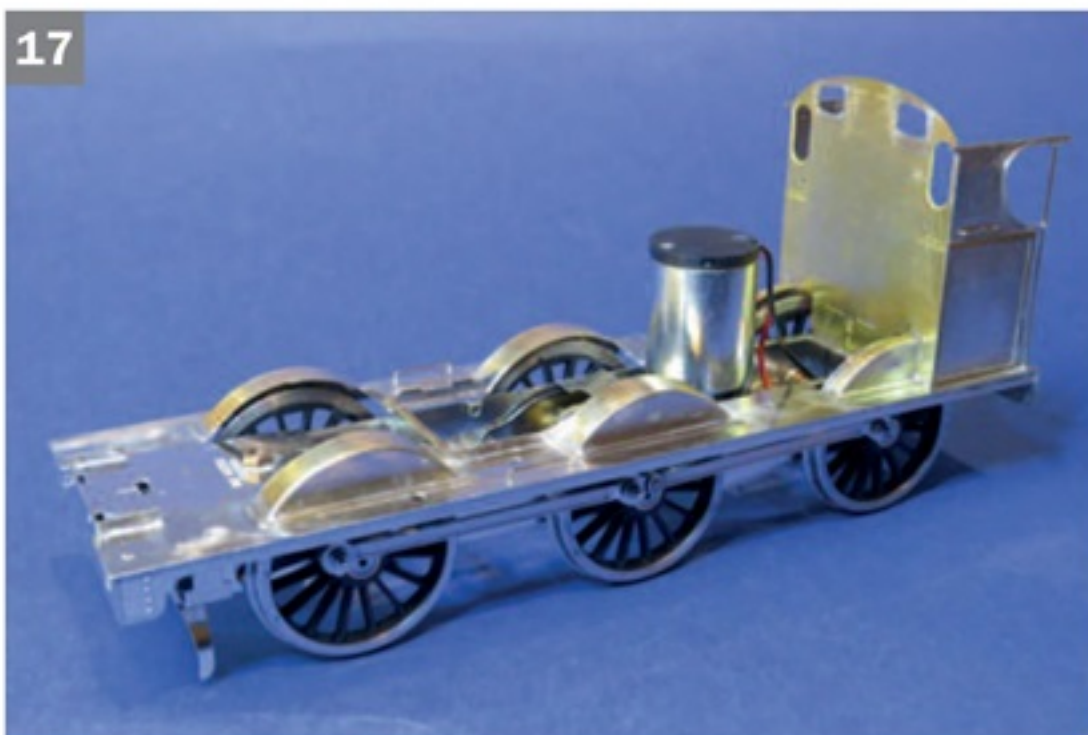
16



The firebox is dry-fitted onto the footplate to test the fit of the motor. I cut away the rectangular hole in the front primarily to clear the motor, but also to provide access to the inside of the boiler so I could insert lead weight at a later stage of the build.

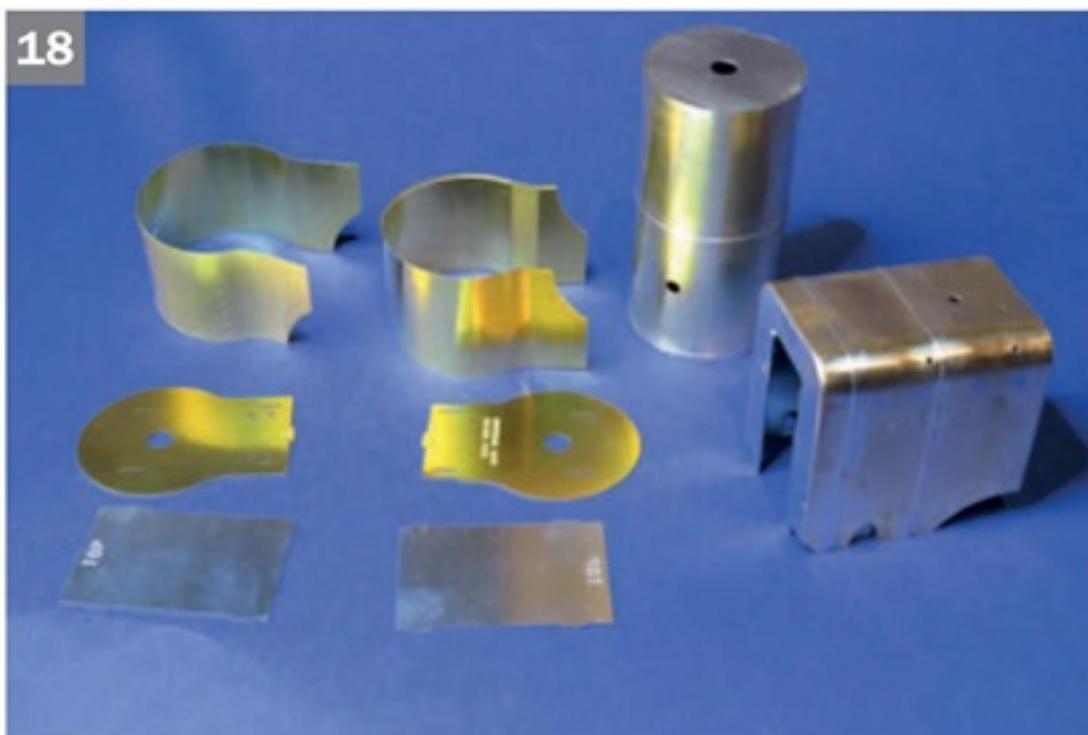


17



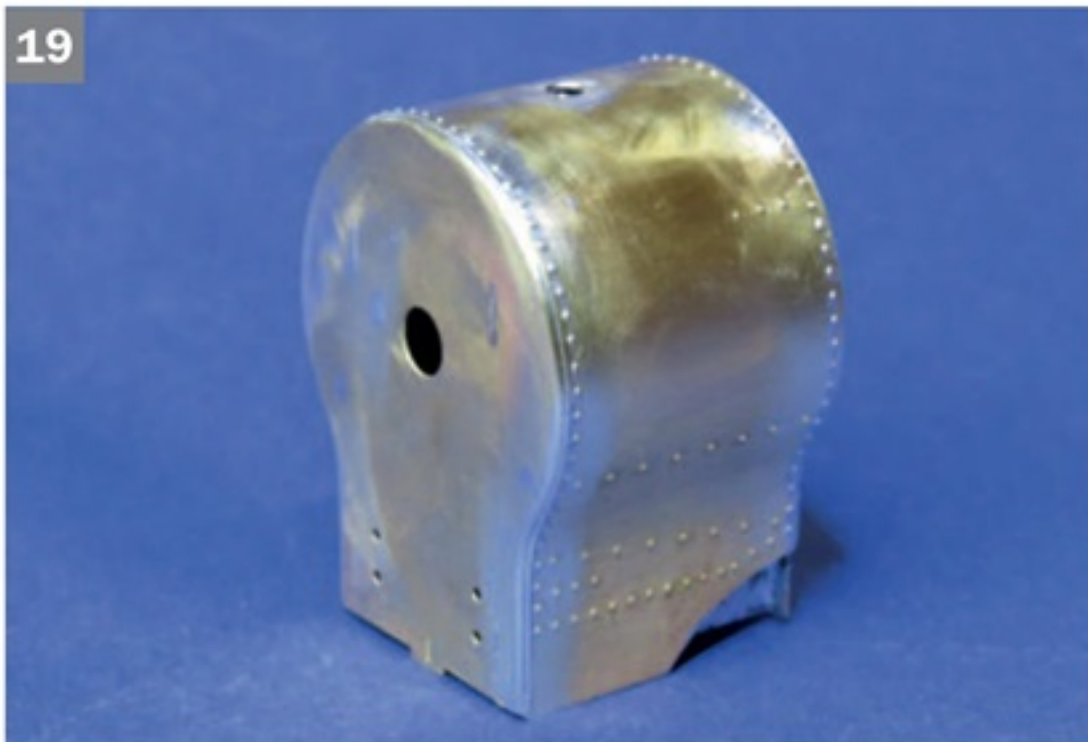
I curved and fitted the splashers tops. I found that the rear ones needed a small amount of filing to achieve a satisfactory fit against the firebox, but the main point to consider at this stage is ensuring that the splashers are clear of the wheels – otherwise there is the potential for an electrical short circuit to be created.

18



The boiler is supplied pre-rolled and the bulkheads fitted front and back proved to be a good fit. The smokebox components pictured here show that I pre-formed the wrappers using the front and back bulkheads as a pattern. The square plates in the foreground go between the front and back spacers, with the inner wrapper then tack-soldered tight to the bulkheads, followed by seam soldering them. I fitted the outer wrapper, again tack soldering to locate it before then using a blow torch to get the solder to flow between the two layers. Don't overdo the blow torch part though, otherwise you will end up with a kit again!

19



The completed smokebox unit. Note how I filed a slight radius around the front edge; this was to mimic how on the real thing this part was bent to a flange where the riveted wrapper was attached. The central tab visible on the bottom edge engages with a slot in the footplate.

20



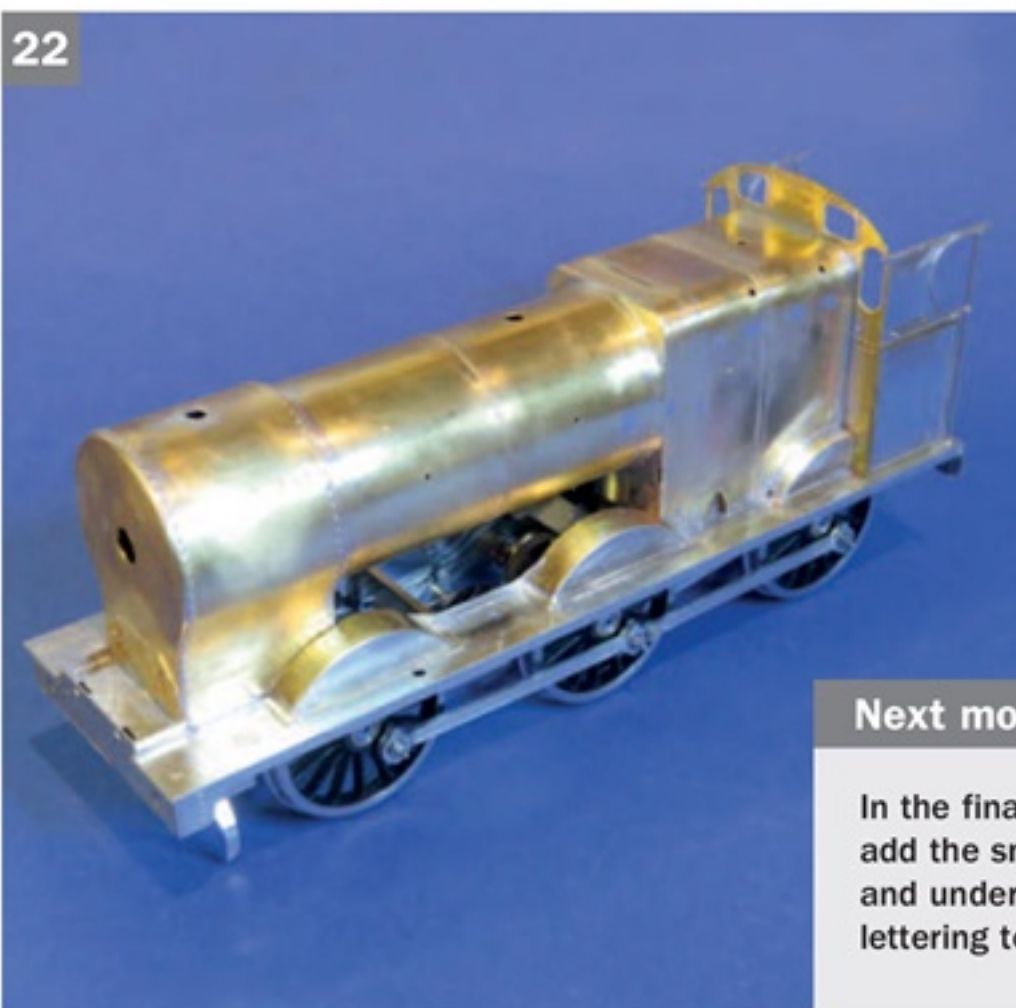
This is the recommended method of ensuring that the smokebox and the boiler line up. I used a brass tube and opened out the holes in the end bulkheads with a tapered reamer to provide a close fit. A small notch was then made on the top of the boiler front and smokebox rear to align the holes for the chimney and dome, before then soldering the two units together. 145-degree solder was used.

21

There is a radius join between the boiler and smokebox, so once I had soldered them together I used a length of thin copper wire to represent the radius. Starting under the boiler and pulling the copper tight, I tack-soldered it as I worked my way round. I then flooded the join around the wire with more solder and cleaned it up with a glass fibre brush.



22



The boiler and firebox units were then soldered together, ensuring first that they were lined up correctly. The whole lot was then soldered to the footplate after checking numerous times that it all lined up, that the seams were tight and that the footplate was straight. I fitted the chassis to the body and then test ran the model to ensure nothing fouled the wheels or motor.

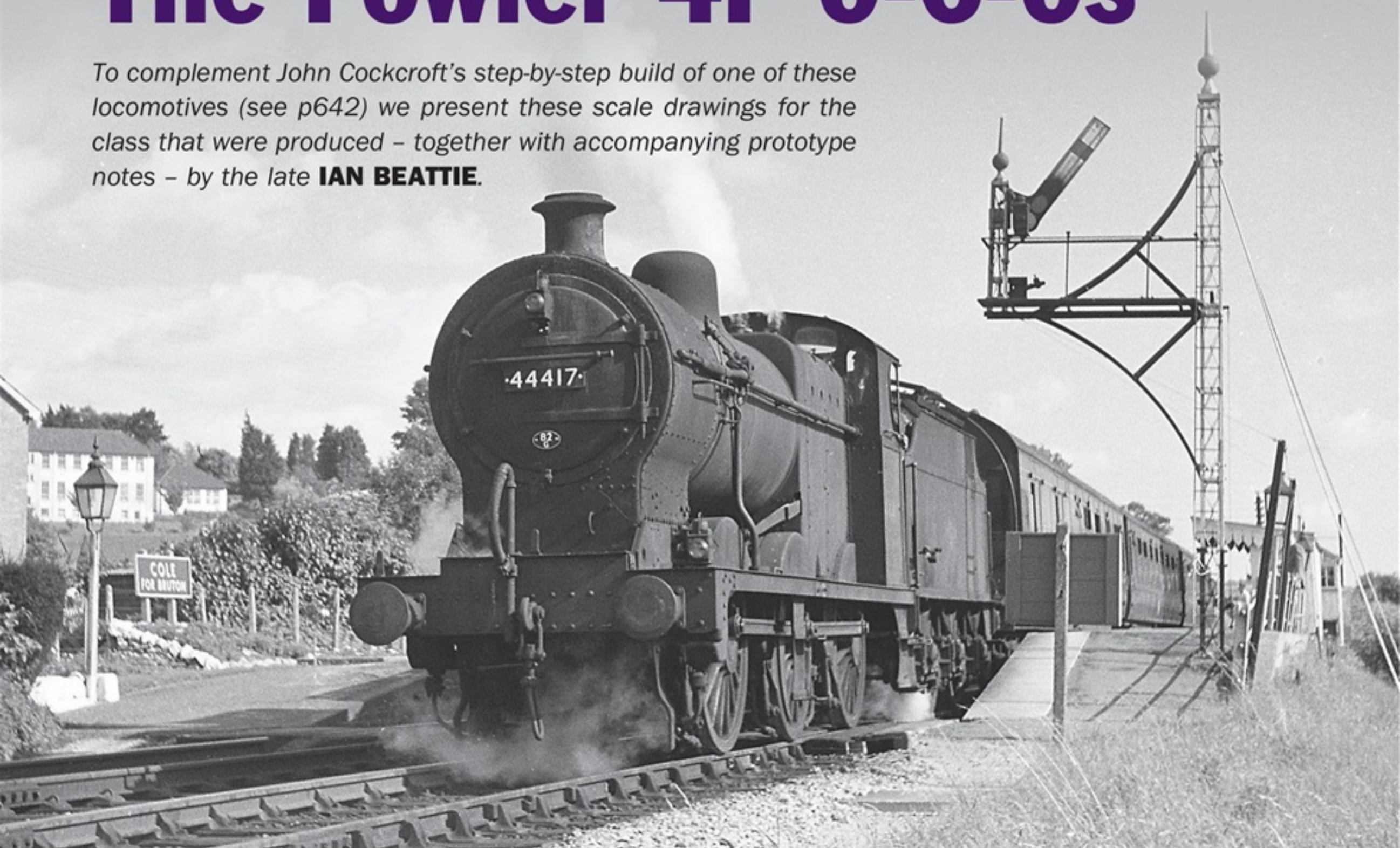
### Next month

In the final part of this series John will add the smaller details and fittings and undertake the painting and lettering to complete the model.



# The Fowler 4F 0-6-0s

To complement John Cockcroft's step-by-step build of one of these locomotives (see p642) we present these scale drawings for the class that were produced – together with accompanying prototype notes – by the late **IAN BEATTIE**.



The first examples of Sir Henry Fowler's Class 4F 0-6-0 rolled out of the Midland Railway works at Derby in October 1911, the design being the latest iteration in a long line of increasingly more powerful goods 0-6-0s that had begun with Matthew Kirtley in the very early days of the MR.

In fact the 4F was essentially an update of its precursor and could hardly be deemed a totally original concept. The Midland concern built 197 4Fs prior to the 1923 Grouping, this total including five for use as mixed-traffic

locomotives on the Somerset & Dorset Joint Railway.

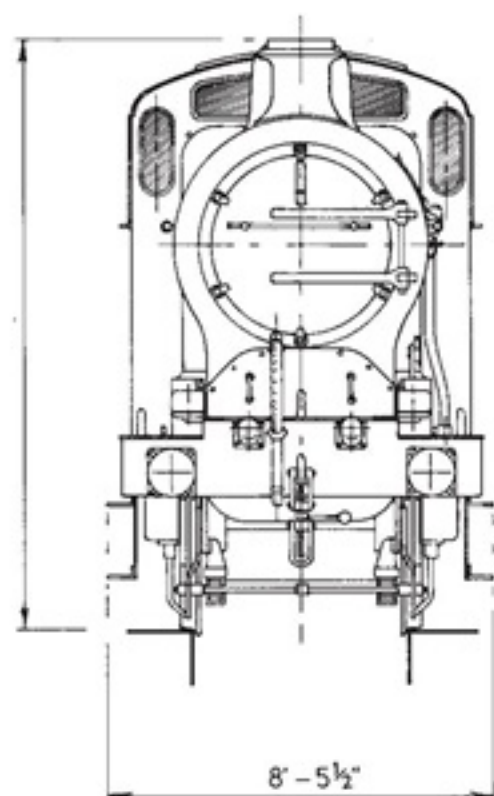
Immediately following the Grouping, the fledgling London Midland & Scottish Railway was

▲ Despite being classified as freight hauliers, the Fowler 4Fs acquitted themselves very well on local passenger duties, as exemplified here with Templecombe-allocated No.44417 (built 1927), which is seen at Cole with a 16.15 Templecombe – Bath service in July 1962. Note the Somerset & Dorset passenger headcode, and the tablet catcher apparatus on the tender. *Photo: Railphotoprints*

Most of the LMS-built 4Fs were paired with what could be referred to as the 'standard' Fowler pattern 3,500gal flat-sided tender, but the exceptions included such oddities as this high-sided 3,500gal tender, which is seen coupled to 1940-built No.44602 at Bolton on 17 September 1961. There were 10 tenders of this type constructed, originally for use with Stanier Jubilees. *Photo: Railphotoprints*







FT 0 1 2 3 4 5 6 7 8  
SCALE

These drawings were first published in the December 1987 issue of RAILWAY MODELLER.

dominated by Midland thinking and practice, so that continuation of the building programme of Class 4 engines as a standard goods design for the new company was entirely natural.

The Midland 4Fs were built to be driven from the right-hand side of the cab and the first few LMS-built 0-6-0s followed suit, however successive batches conformed to the decreed left hand drive for LMS engines (the MR and early LMS 0-6-0s were never altered in this respect).

No less than 575 examples were outshopped by the LMS, principally between 1924 and 1928, and during Stanier's regime from 1937 to 1941. The eventual class strength of 772 was a measure of the type's general utility, this total only being surpassed on the LMS by Stanier's Class 5 4-6-0s and Class 8F 2-8-0s.

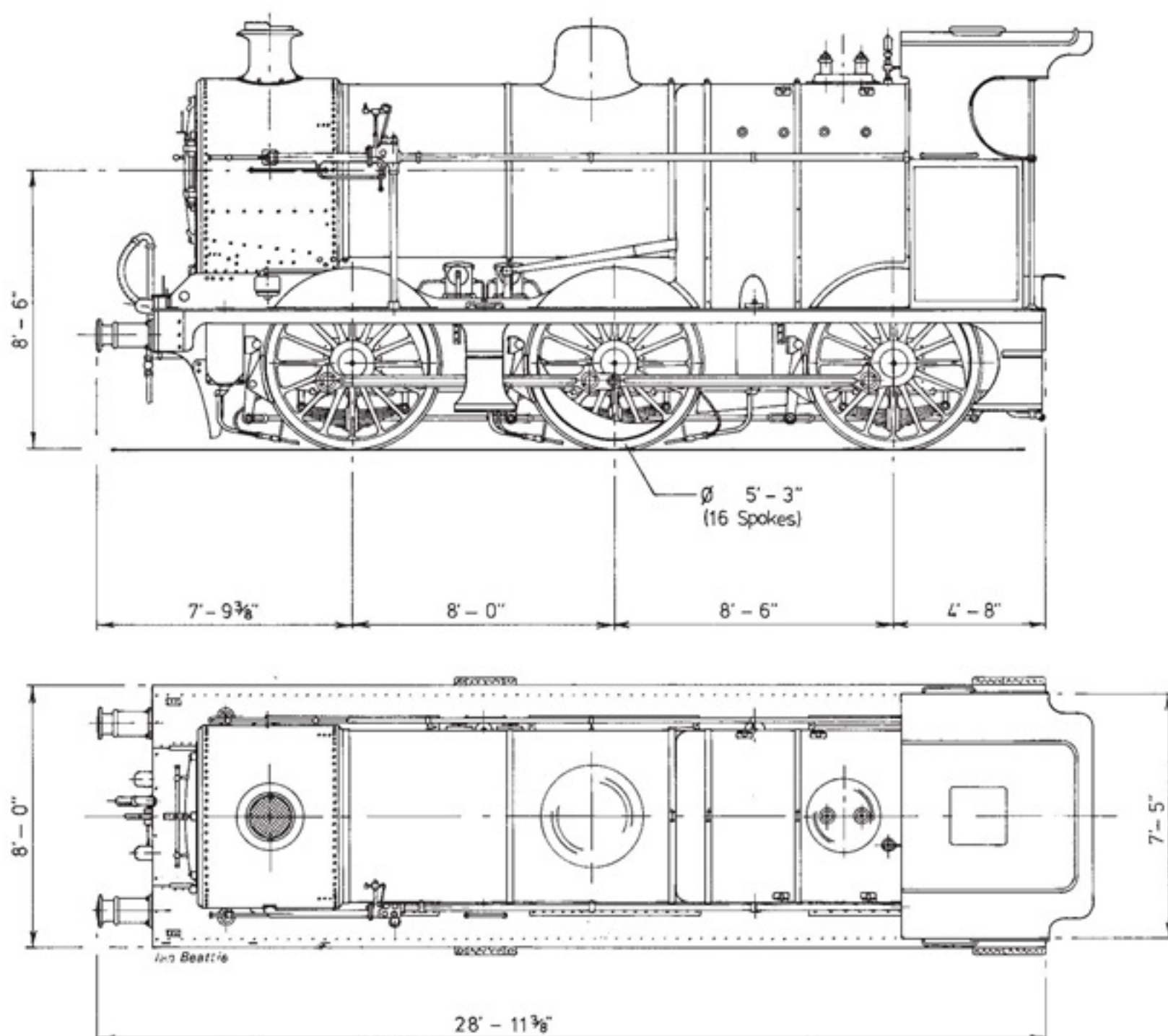
The 4Fs were powerful, sturdy locomotives with plenty of the

stamina needed in the coal traffic that constituted so much of the MR and LMS revenue. Unfortunately the MR 'small-engine' policy, whereby small but efficient engines in multiple, leading to double-heading as a common and entirely accepted routine, were favoured over bigger and more potent machines, led to lack of experience in detail fittings when eventually big engines were desperately needed. This also showed itself in some shortcomings in the MR 4Fs that were never corrected in the LMS variants: chiefly involved were axlebox bearings that were

far too parsimonious in their bearing area for the increasingly heavy demands made upon the engines. Valve chest characteristics were none too modern either, leading to constricted breathing, and again no real development took place.

All LMS 4Fs were vacuum-brake fitted, many with steam heating too, for fitted freight duties and

the occasional passenger working, which their quite large wheels allowed. Used throughout the LMS system and subsequent BR regions, the big 0-6-0s contributed heavily if mundanely to the overall operating picture of the railway. The 4Fs were withdrawn between 1954 and 1966, with four preserved; LMS Nos.3924, 4027, 4123 and 4422.



## Notes on the drawings

The drawings of the Class 4F 0-6-0 show a typical example as built in the late-1920s, complete with piston tailrod covers (removed by BR) protruding from the specially raised central portion of the buffer beam. As indicated by the siting of the reverse rod and brake ejector equipment, the locomotive is driven from the left-hand side of the cab.

The 4Fs built from 1937 onwards sported some small detail differences over the pure Fowler versions, notably the use of a Stanier pattern chimney.

Two examples from the quartet of preserved 4Fs are pictured at Minehead on 29 April 2017. Nearest the camera is Midland-built No.43924, which is a right-hand drive example dating from 1920, whilst in the background is LMS-built No.44422, which is a left-hand drive version dating from 1927.

Photo: Craig Tiley





# Edenham

**IAN WILSON** imagined what this Lincolnshire branch terminus may have looked like had it been taken over by the Great Northern Railway and survived into the 1950s.

Photography by Tony Wright

Surprisingly for somewhere so small, the village of Edenham – situated close to the market town of Bourne – once boasted a railway station served by a line linking it to the Great Northern main line at Little Bytham.

It was upon moving to South Lincolnshire in 1980 that I first discovered the remains of this railway, which had closed over a century before, and ideas for a OO gauge model began to form in my mind. I obtained a copy of *Lord Willoughby's Railway. The Edenham*

*Branch* by R E Pearson & J G Ruddock and, using reference in the book to the line's terminus at Edenham as a guide, a plan for a layout was sketched out.

## Re-imagining history

My model imagines that the Edenham Branch Railway's approaches to the Great Northern Railway to take over the running of the line (see panel) actually bore fruit, which resulted with a new station building and engine shed being built at Edenham. Perhaps



▲ A rare surviving artefact from the short-lived EBR – an Edmondson ticket proof. Photo: courtesy Paddington Ticket Auctions

more unlikely is supposing that the branch then survived into the early 1950s British Railways period.

In this imaginary scenario there was a regular passenger service that provided a link

Work-stained D16/3 4-4-0 No.62530 (a Hornby model) prepares to depart Edenham with a passenger service composed of Midland Region stock. All trackwork on the layout is from the Peco Streamline code 75 range, with points operated using Colbalt motors. The layout backscene uses photos of the actual location.





## The Edenham Branch Railway

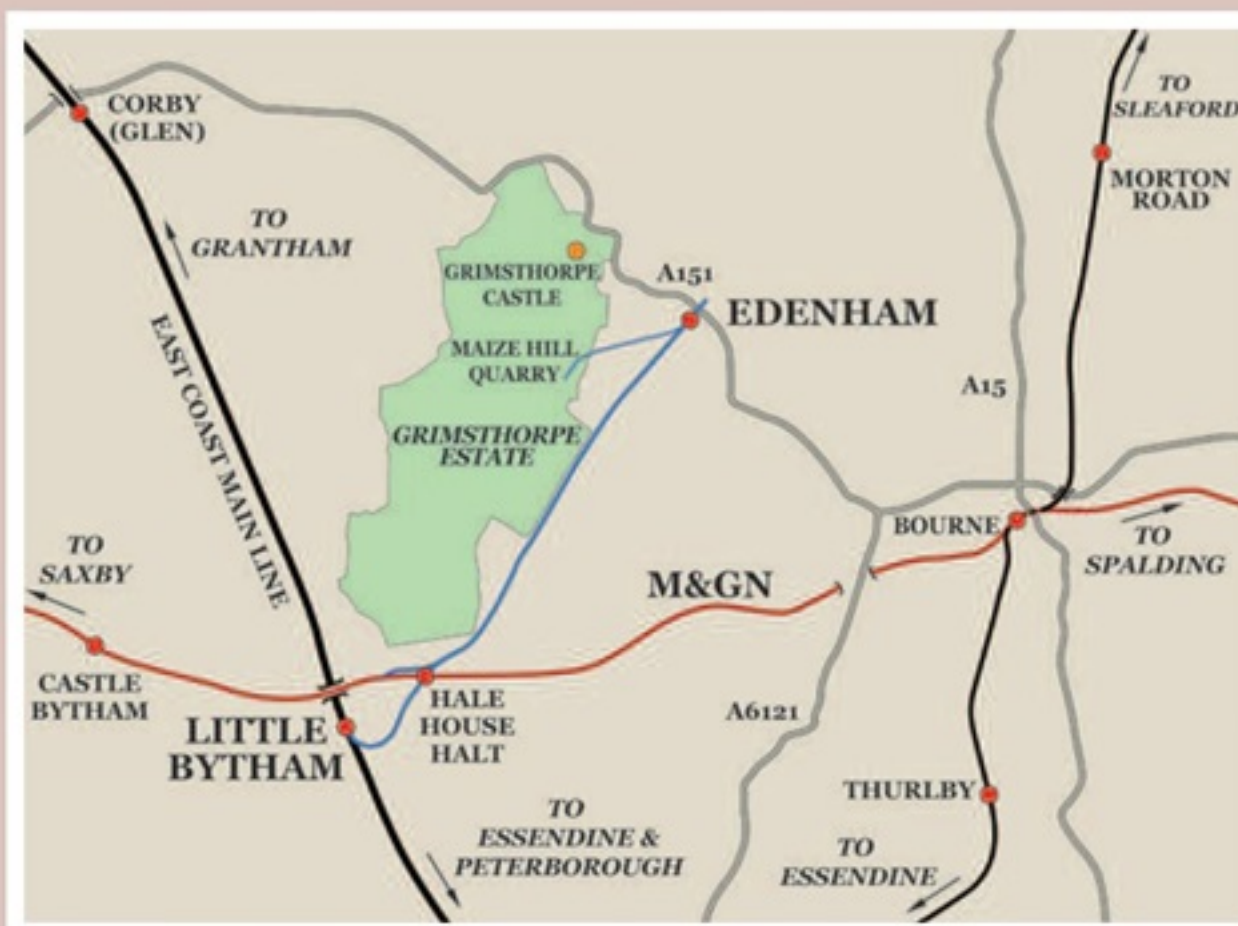
Built as a private railway by Peter Robert, Baron Willoughby d'Eresby, of Grimsthorpe Castle (located a mile to the north of the terminus at Edenham) the line was officially named the Edenham Branch Railway (EBR) but was sometimes known as 'Lord Willoughby's Railway'.

Lord Willoughby was an early pioneer of the use of steam power for agricultural purposes so, when the Great Northern Railway (GNR) was built through the west of his estate in 1852, it was quite natural for him to plan a road over estate land to provide a link to the main line at Little Bytham.

The first form of motive power to operate on the new road to Little Bytham was a steam traction engine designed by Daniel Gooch, known for his subsequent work for the Great Western Railway. The road proved to be unsuitable, however, so the decision was made to lay rails on the roadway, and the traction engine was converted to run on the railway, becoming one of three locomotives used on the original line.

Throughout the short life of the EBR efforts were made to persuade the GNR to take over the running of the line, albeit without success.

The EBR closed in 1873, but in 1894 a new railway was built over Lord Willoughby's estate – the Midland & Great Northern (M&GN) – from Saxby (near Melton Mowbray) to Bourne. The line provided links from the large cities of Nottingham and Leicester to the Norfolk coast and was an important holiday route. It was



carried over the Great Northern main line on a substantial girder bridge, whilst a short distance further on it also crossed the former trackbed of the EBR – about three miles from Edenham and a mile from Little Bytham – but perhaps surprisingly no station was built to give passengers access to the GNR, the main line from London (Kings Cross) to Edinburgh.

with main line trains at Little Bytham. Additionally, with the arrival of the Midland & Great Northern a passenger halt was built at the junction with the EBR – not only so that passengers from Edenham could reach places on the M&GN, but also so that M&GN passengers could reach the GNR main line at Little Bytham. The halt was named Hale House Halt after a nearby plantation.

The model also imagines that, at the time of the widening of the GN main line in 1901,

a new connection was made with the EBR at Little Bytham by re-routing the track across the valley of the River Glen. This alignment would have been on a new embankment constructed using the excavations from the track widening works and would allow trains to run into the new Up slow platform at Little Bytham station.

During the 1940s, at a time of heightened demand for iron ore during World War II, tracks were laid to the expanding local

quarry at Maize Hill, and exchange sidings were provided in the confined space at Edenham station. A small yard was opened at the junction with the M&GN to handle

**The guard has been quick to change the lamps as Class C12 4-4-2T No.67376 (from a Craftsman kit) prepares to uncouple and run round its train at Edenham. The station building has been largely constructed using traditional carcass and brick paper methods.**







▲ Edenham station with Class C12 4-4-2T No.67376 on a short passenger train while Class 4MT 2-6-0 No.43042 draws away from the coal drops with a recently unloaded coal wagon. The 16" Hunslet swaps the iron ore wagons in the exchange siding.

stone for the recently opened steelworks at Corby in Northamptonshire.

### Building the layout

Baseboards had been constructed previously and track laid, but progress stalled as other projects were worked on. However, during 2020, and with the encouragement of Tony Wright, I used the extra time available as an opportunity to progress the layout.

The main layout baseboard is built in one piece, 7' long and 2' wide. 6mm MDF was used for the top surface and supported by a timber frame.

A printed backdrop surrounds the layout on three sides. This features images of the landscape that exists at the actual location of the station at Edenham from a series of shots taken by Tony Wright which I then stitched together using Photoshop, at the same time removing the visible pylons and overhead power lines.

A separate board 3' long by 18" wide has two storage roads to handle the quarry traffic

and a three-road sector plate for Little Bytham bound trains. These tracks are hidden by the front facing control panel, which is laid out to represent a signal box diagram.

### Trackwork

The track is Peco Code 75 throughout, with points controlled by Cobalt motors. These eliminated the need for the built-in latching spring, so cosmetic modifications were made to the tie-bar area.

The track was ballasted using Green Scene ref.409 for the BR tracks and the darker Green Scene ref.411 for the quarry and goods yard sidings.

### Scenery

Scenic contours were made using Styrofoam coated in exterior Polyfilla™ – this being stippled to create a textured effect. Grass areas were given an initial coating of Sandtex exterior paint in Lovatt Green, while other areas had the same treatment but in Dark

Mushroom. This provided a base colour over all areas prior to detailing with various scenic scatter materials. The large trees which disguise the exit to the fiddle yard were originally on the late Dave Shakespeare's *Tetleys Mills* layout.

### Buildings

Most of the structures on the layout are scratch-built. The engine shed is based on Stamford East (which was one of the first kits that I produced in the early 1970s as part of the Prototype Models range of construction kits). Photographs of typical Great Northern Railway country stations were studied, and the station building is the result of finding that there were lots of variations in style!

The model of the weighbridge hut was easier as it is one of the few visible remains of the EBR at Edenham and can still be seen

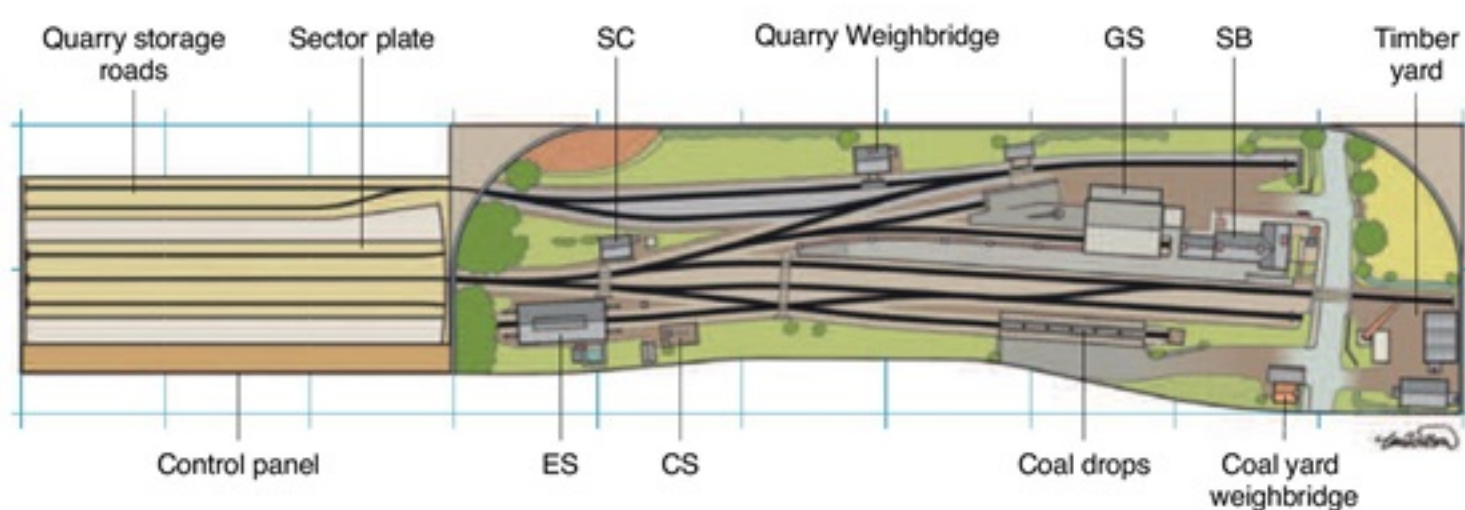
▶ A passenger train arrives at Edenham behind Fowler Class 4F 0-6-0 No.44403 as a C12 is serviced on shed. Note the photographer standing next to the signal.





## EDENHAM

Overall dimensions including fiddle yard: 10' x 2'.  
Each grid square = 1ft x 1ft.



The hidden sidings are masked by this control panel, finished in the style of a signal box diagram. Toggle switches control the track sections and route-setting of the points.







when passing by on the A151 road to Bourne. In order to suggest another architectural feature which had remained after the railway was taken into GNR ownership, the goods shed was deliberately made low-key and uses parts from a Ratio GWR carriage shed kit which had been languishing on my 'to build' shelf for some

time. There is also a second weighbridge hut on the layout at the quarry exchange sidings. This was built from a Stanton Ironworks plan dated October 1940, and the prototype was at Harston near Grantham on the Woolsthorpe Quarries system.

▲ The station staff are hard at work keeping the platform clean. The model illustrates what Edenham station may have looked like if the Great Northern Railway had taken over and rebuilt the site. The design of the main building was influenced by a number of GNR examples.

Class C12 4-4-2T No.67376 prepares to leave Edenham with a typical lightweight local passenger train consisting of a single coach and a van. All stock is fitted with prototypical three-link or screw couplings, although the actual linking is made with etched items from the Sprat & Winkle range.







When the original EBR was built a route beyond Edenham was surveyed and the model suggests that track was laid across the A151 by way of a level crossing, but that this line went no further and became a siding for a local timber merchant. The timber yard is a Wills kit, as is the yard office, which is a re-purposed corrugated iron chapel.

The remaining scenic feature is the coal yard, and the evocative coal cells are based on those at Alston in Northumberland, using plans in *North Eastern Branch Line Termini* by Ken Hoole as reference.

### Operating the layout

A source of reference for the working timetable between Bourne and Saxby was provided by *The District Controller's View No 12, The Midland & Great Northern Joint Railway* by Xpress Publications. I used this to integrate my Edenham Branch workings with the ore workings to and from Corby. Branch passenger timings were tied in to connect with both M&GN stopping passenger, and semi-fast trains at Little Bytham on the East Coast Main Line.

Branch passenger trains are handled by C12 4-4-2T locos from Peterborough New England shed, and are normally composed of a composite coach coupled to a four-wheel brake.

C12 tanks or J52 0-6-0STs handle goods trains on the branch as well, but occasionally goods from the Midland & Great Northern line appears too. This can be brought in behind a variety of motive power: an ex-GN J6 0-6-0, an ex-LMS 4F 0-6-0 or an Ivatt Class 4MT 2-6-0.

Empty iron ore hoppers from Corby steelworks arrive at the M&GN junction yard behind a loco from Kettering shed; usually an Ivatt 4MT 2-6-0. The loco makes trip workings to Edenham with six hoppers at a time and exchanges them for six loaded hoppers to take back to the junction.

The quarry workings are in the hands of a Hunslet 16" 0-6-0ST and an Andrew Barclay 14" 0-4-0ST.

### Motive power

At the time of writing the locomotive roster is a mixture of kit-built and R-T-R models, most with a chassis upgrade and which are largely the work of Tony Wright.

Branch locomotives comprise C12 4-4-2T No.67376 (Craftsman Kit), C12 4-4-2T No. 67394 (Wills Kit) and Class J52 0-6-0ST No.68817 (Hornby, Tony Wright chassis),

▲ The coaling stage at Edenham where Class C12 4-4-2T No.67394 is having its bunker topped up. The locomotive is built from a Wills (now South Eastern Finecast) kit.

whilst M&GN locos comprise J6 0-6-0 No.64190 (Nu-Cast Kit), Fowler 4F 0-6-0 No.44403 and Ivatt 4MT 2-6-0 No.43160 (Bachmann). In addition, the ore workings are delegated to Ivatt 4MT 2-6-0 No.43042 (Bachmann, modified by Tony Wright), whilst quarry locos take the form of a Hunslet 16" 0-6-0ST (Centre Models Kit, chassis by Tony Wright) and Andrew Barclay 14" 0-4-0ST (Hattons).

### Conclusion

Although *Edenham* started out as a home-based model, I have Tony to thank for encouraging me to complete it such that the layout could be taken to exhibitions if such opportunities arise. There are no bookings for it at present, but in the future it may venture out from time to time.

### Further reading

*Lord Willoughby's Railway. The Edenham Branch* by R E Pearson & J G Ruddock Published by Willoughby Memorial Trust, 1986.  
ISBN 978-095116-560-7

*North Eastern Branch Line Termini* by Ken Hoole. Published by OPC, 1985.  
ISBN 978-086093-219-2

*The District Controller's View No 12, The Midland & Great Northern Joint Railway* by W S Becket. Published by Xpress Publications.  
ISBN 978-190105-637-2



# Fence Houses – part 3

**BOB JONES** concludes his conversation with RM Senior Editor **STEVE FLINT** as they discuss the development of this mammoth 2mm Fine Scale project. This month, Bob explains the logistical challenges of taking the layout out to exhibitions and reveals his expectations for the future.

**Steve Flint (RAILWAY MODELLER).** *Although exhibitions have been curtailed this past year and more, how long does it take to get Fence Houses up and running, from the moment you arrive at an exhibition venue?*

**RVJ.** After unloading the van it normally takes between three and five hours to erect, check and get the first train running. As *FH* is in effect a continuous oval shape with an internal well for the operators to work in, it was essential to have all 14 baseboards made perfectly square so that everything aligns correctly when erected. Now and again we end up joining the first and last boards together where we have to 'nudge' one or both to get the them to fit has always worked out in the end. We use a total of 17 metal fold-up trestles to support the baseboards and these have been modified so that the height and level of each can be adjusted as required.

It is noticeable how varying humidity in a hall can impact upon the setting up time quite significantly, with different conditions affecting the amount of adjustment to cross-board rail joins that is required to achieve satisfactory running.

Strip-down after a show usually sees us all packed up and in the van inside of two hours. This is where those who have chosen to model a much smaller layout can walk past us lot with a smile on their face and the layout under their arm, while we are still labouring away dismantling things!

**RM.** *What event bookings are there in the pipeline for Fence Houses?*

**RVJ.** Our next planned 'exhibition' is in fact the twice cancelled (because of the Government restrictions in response to the global health situation) 60th Anniversary Expo, Diamond Jubilee event of the 2mm Scale Association, which is now due to take place on the weekend of 18 & 19 July 2022 at The Derby Conference Centre DE24 8UX. The event will be a great opportunity for people to see a wide range of 2FS layouts, many of which will feature N gauge models converted to run on 2mm fine scale track.

Then November 2022 is when we should be at the Workington show in Cumbria for the first time with *FH*. We had hoped to be exhibiting much nearer to 'home'

**Photography by**  
**Steve Flint and**  
**Rebecca Flynn**

**Below**  
Heading north on the  
Down line with the  
headshunt loading gauge  
on its left is BR 9F 2-10-0  
No.92099 (from an FHMF  
etched kit – see part two)  
with coal for power  
plants alongside the  
River Tyne.





during 2021 at both the Hartlepool and Newcastle shows, but the restrictions prevented both of these events from taking place. Hopefully we can rearrange things for a future time, 2023 perhaps? Time will tell.

**RM.** *Is the layout operated to a particular sequence or timetable, or are trains run however the operators choose?*

**RVJ.** Originally we were pretty laid back in our operation as we were just learning things and operating with what stock we had at the time.

However, Les has since devised a timed sequence of operations with detailed instructions for us all to follow. This

does demand a lot of concentration but when followed properly it really makes you feel that the whole process has meaning and purpose – far better than just sending out whatever is handy at the time.

The trains all now perform specific functions. Some 'local' trains are scheduled to stop in the station and are programmed to decelerate from certain points on the layout so that they do stop at the platform. Acceleration is programmed in automatically as well for onward departure.

We did a lot of testing to get this correct by timing all the locos over a length of track between two reed switches. Magnets on the loco triggered the switches and this gave out a reading on a digital display of the loco's speed, enabling Les to judge what speed settings were required for each loco.

All this and previous comments about 'controllers' may seem as though we have gone for full 'auto control' of the layout, but that's far from the case. A big concern with something the size of *FH* is monitoring whatever is happening over the enormous area of operations at any one time. Even with all operators on duty things can go wrong if there is even a momentary lapse in concentration. Considering the mileage we run it does not

happen very often, but if even just one pair of couplings come adrift on the front scenic side it can cause quite a bit of disruption, especially where our timed signals are concerned as these are programmed to work in conjunction with the speed settings of different trains.

This is just one example of why we have gone to the extent of having pre-set routes for different trains. Everything is then linked by operating one switch to timed activation of turnouts and signals so that when the route is chosen the operator can 'simply' be the driver and look after their own train movement.

**RM.** *Tell us about the team of modellers who help you with Fence Houses when on the road. How many operators are required for a normal running session?*

**RVJ.** Our current exhibition team comprises Les Waters, John Robson, Chris Mills, Stuart Barnes, Ross Barnes, Gillian Harrison, Mike Grainger, Phil Loades and myself.

As for 'normal running sessions', these to date have been rare, as over the last 12 years we have been adding and improving things most of that time. Let's face it – *FH* is exceptionally big for a private venture. The team members are not all retired and, although in regular contact by other means, we rarely see some of the team for months at a time.

Somewhat ironically, however, after all the control system revamps, upgrades and additions, as well as finally installing the signals and getting them to operate reliably, we were just starting to consider meeting at the club for running sessions when the global health crisis struck. That put paid to any such ideas.

Fortunately *FH* is permanently set up at the premises of The Blyth & Tyne Model Railway Society. Without this facility we would struggle to say the least! By the time you read this we are hopeful that we shall be back there and re-learning how *FH* works.

One event we attend annually (the Government restrictions over the last 18 months notwithstanding) is the society's open weekend during December. This is timed to coincide with when the local council relaxes some of the parking restrictions to encourage retail footfall during the run up to Christmas. We have had up to a thousand people through the doors for these events in the past. Members operate their layouts and chat just as if it were a normal exhibition, which certainly serves as good practice for when we exhibit further afield.



#### Above

Here we have a view of the signal box that stands between the colliery lines in the foreground and the BR running lines behind. As with the majority of other buildings on the layout, this structure is one of the 40-year old originals from the N gauge incarnation of *Fence Houses*.

### Layout technical details summary

#### Baseboard structure

12mm thick MDF was used throughout for the baseboards. Most are of 2.1m length, with varying widths of no more than 600mm. Side support rails are 100mm deep for strength with additional cross-bracing as a result of the 'open top' style of construction used. However, some areas of baseboard are covered where the trackwork requires it.

#### Plain trackwork and ballasting

Originally trackwork was constructed using rail soldered to PCB sleepers but later we adopted plastic sleeper bases with the rails threaded in place. Ballasting was undertaken using ash rescued from the remains of the real railway.

#### Pointwork

All pointwork has been hand-built. Points in the fiddle yards just use rail soldered directly to PCB sleepers, whilst etched chair plates are added to those in scenic areas.

#### Signals

These were designed and etched to suit. Operation is via servo motors that are electronically controlled and connected to the layout's MegaPoints operating system.

#### Buildings

Most structures on the layout were made some 40 years ago in plasticard for the original N gauge version of the layout. However, a few new structures were added for the latest version of *FH*. The viaduct

was made using a wooden substructure clad in Polyfilla™, scribed and painted to represent the stonework.

#### Landscaping and greenery

Polystyrene sheet base was carved to shape and sealed, then detail colouring to represent flowers etc. was applied on top. Trees were created using suitable garden shrubs and plants with added scatter material/foilage.

#### Backscene

Flexible polystyrene foam sheet material (3mm thick) was painted to represent scenes from the area. Each 2.4m sheet length is clamped to a baseboard to make up the whole 46' total length required.





**Above**  
An aerial view of Wapping Bridge that carries the A183. A BR Type 3 diesel hauls a train of metal plate and strip on the Down line as a Q6 0-8-0 passes with empties in the opposite direction.



"I was compelled to write the following poem after seeing in print so many instances of the misspelling of the name Worsdell, in published references to Thomas William Worsdell (1885-1890) and Wilson Worsdell (1890-1910), who were former Chief Mechanical Engineers of the North Eastern Railway."

### What's in a name? by Bob Jones

There once were some men called Worsdell  
Engineers of great note so they tell  
They'd beaver away making locos all day  
Then watched 'em run by at Low Fell  
One man had a brother  
Not him it's the other  
And he was called Worsdell as well  
They worked for The North Eastern Railway these two  
Not in charge at same time though you see  
One followed the other in't steps of his brother  
And managed the job with great glee  
Their fame spread about and with it came clout  
They could do all manner of things  
But one thing eluded too hard they concluded  
Like pigs in't sky with wings  
No matter the fame it was such a shame  
They could have thought oh what the hell  
But it's not hard to spell  
And it's NOT Wordsell  
It's Worsdell!!!  
It's Worsdell!!!  
It's Worsdell!!!

In addition to our regular team, over the years we have had many helpers providing cover and stock at shows up and down the country. In fact, that should be countries, as we have exhibited in Scotland as well as England! These people all know who they are and they have all kept the wheels turning with their sterling work and enthusiasm for *FH*, for which I am eternally grateful.

**RM.** And so to the future. Having been joined to *Fence Houses at the hip, as it were, for almost half a century, do you have any other plans for something else?*

**RVJ.** *Fence House Mk.II* is definitely the end of the road for me, not that this means we stop here – there is far more to do in terms of additional kit design and building to give us the additional items of stock that have always been in short supply for a layout of this size.

One other glaring omission, at least in part, is the lack of 'every day grime and clutter' especially around the colliery area, and of course the weathering of locos and other items of stock. One day we will get around to completing these tasks.

What has endured through all the years is a commitment from those involved to continue the development of *Fence Houses* in all of its guises and phases. As Les once said to me, "evolution not revolution!" And we have certainly evolved over the last 40 years!



**Above**  
The main station buildings, which were built mainly from embossed plasticard with added cast bay windows courtesy of Langley Models. The figures are Preiser products.



Shades of the Somerset & Dorset in 8' x 4'

# Templecombe Lower

Photography by Piers Howell



**DAVID PLUMRIDGE** unearthed a 35-year-old layout at his grandson's request and the modelling bug bit again. The result is this large single-board layout with tremendous operating potential.

It all started back in 1984 when my son was three years old. It was, of course, essential that he (or rather I) should have a layout and so an 8' x 4' fibreboard base was bought, reinforced with 2" x 1" timber. It was mounted on my son's bedroom wall with gate hinges so as to fold up when not in use.

N gauge was chosen to enable an intensive track layout to be accommodated, with

linked upper and lower levels so that trains passed through the station once every 40 seconds, rather than every 20. Pointwork was located such that it was all within easy reach of the operator, enabling the routes to be changed by hand.

A Somerset & Dorset theme was adopted and accordingly models of a Bulleid 'Battle of Britain' light Pacific and Stanier Class 5MT 4-6-0 (both Graham Farish) were purchased, along with eight BR Southern Region MK.I coaches and eight BR maroon MK.Is. I constructed station platforms, and added five

Peco engine shed modules, and that was that for 35 years...

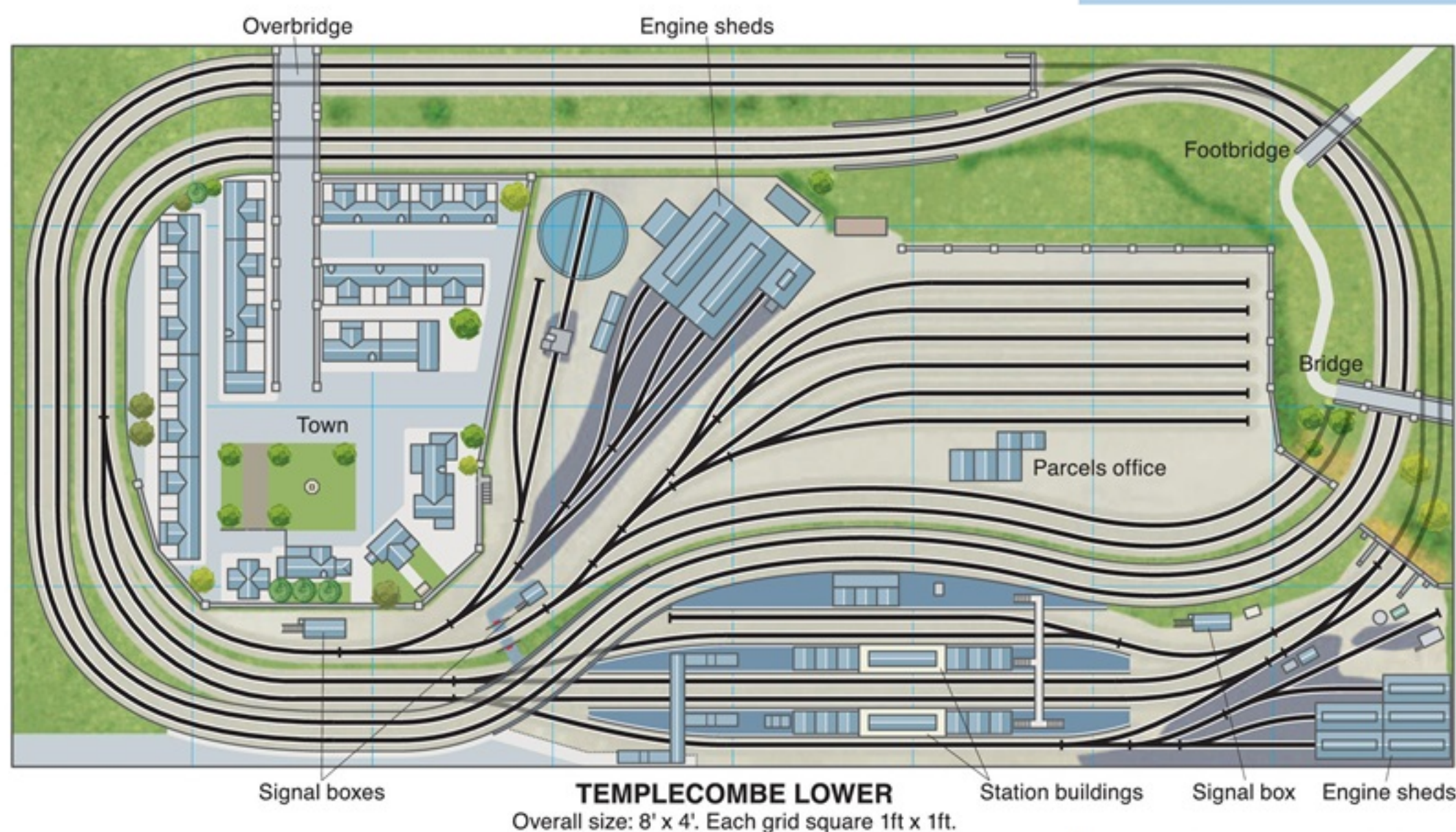
## Re-awakening

Fast forward to three years ago and my then four-year-old grandson discovered the layout stored behind plastic sheeting in the garage,

A busy moment is captured in the vicinity of Templecombe (Lower) No.2 signal box with mineral and passenger trains passing on the upper level as a local arrives below. The use of N gauge enabled an effective double track system to be accommodated in a small space.







Terraced housing is a key feature of the 'town end'. Metcalfe low-relief kits were used back-to-back to create a slightly smaller footprint than the standard full-depth kits.

unused for at least 20 years.

"Can we get it out, Grandad?" he asked enthusiastically.

I retrieved the layout from its prolonged period of hibernation. However, having spent 20 years unused in a garage, the electrical connections were found to be a bit erratic, so I re-wired these using a heavier grade of wire. I then asked Tom Hussey of Platform 3 Models of Tavistock to come and fettle the trackwork and solder the connections. (Originally the layout had operated with wire ends wound tightly around the rail connectors.)

Next, I designed and installed the motive power depot and sidings. Tom Hussey returned to solder the connections to my Peco turntable, along with connections so that I could operate the shed yard and goods yard separately, using a separate Gaugemaster controller.

### Scenery

I wanted the layout to have a 'town' end and a 'country' end. A visit to the shop at Pecorama, in Devon introduced me to the Metcalfe Models range of cardboard kits. On display in the shop were some constructed examples and I was immediately inspired. There followed many months of constructing kits for the structures that make up the town. I found that the Metcalfe Ultra Fine Glue Applicators were essential tools when building them.

The town had to work; that is have some means of connecting to the outside world from the centre of the layout. I used the



Metcalfe tapered retaining wall kits to create a sloping road that connected to a couple of the firm's road overbridges. Perfect; except that when it was all in place, the end of the road was about an inch and a half above the level of the baseboard! I had to raise the level of the entire town. I did this using a sheet of lightweight polystyrene sheet supported on wooden blocks, which then had to be surrounded with Graham Farish retaining wall sections.

The town evolved, but a main constituent was the terraced housing, all built from Metcalfe kits. I used the low-relief versions that are available with the smaller windows, glued back-to-back, preferring these to the full-depth terraced house kits. I also added railway embankment allotments and a village green war memorial cross, courtesy of Harburn Hobbies.

Next came the station, which was like nothing on the S&D, so I designated it as a re-imagined Templecombe Lower. Nothing





A Fowler 4F pilots a 2P 4-4-0 on a passenger train composed of BR Southern Region Mk.I coaches, as a BR Standard 4MT mogul passes with another passenger service on the lower level.

An overall view showing almost the entire layout, which illustrates how a complex track arrangement offering much operational interest has been achieved in an area measuring just 8' x 4'.



could be further from the truth, as most will know that the real station was a short single platform situated between two overbridges. Trackside Signs duly produced my station and signal box bespoke nameboards and signage.

### Seeking guidance

Up until this point everything that I had completed was within my comfort zone, but for me there were two challenging tasks that lay ahead; the ballasting, and the construction of the areas of raised scenery at the 'country' end.







For guidance on both aspects I referred to various publications, including past editions of *RAILWAY MODELLER* and Richard Bardsley's *Making a Start in N Gauge Railway Modelling*. I also watched numerous YouTube videos showing a number of different options and methods.

### Yard roadways

To create the road that runs from the parcel depot round to the rear of the engine sheds, I used fine grade buff coloured ballast granules. My 'hi-tech' applicator was a long-handled teaspoon, which I tapped the edge of to dispense the granules gradually over the surface, which had been previously coated with a layer of glue. Further light grey texturing was applied, along with a dusting of weeds, using Woodland Scenics Scenic Cement, applied using one of my Metcalfe applicators. The result prompted a big sigh of relief as it didn't look at all bad!

I attribute the success of this to a friend of mine who advised me to add a few drops of washing up liquid to the Scenic Cement to prevent 'balling'.

### Track ballasting

For ballasting the trackwork I started with the yard, where I wanted the ballast to have a dark, old appearance. I mixed quantities of different colour ballast granules in recycled plastic takeaway cartons, noting down the various ratios as I went along for future reference.

Once again I used the long-handled teaspoon to dispense the granules, with any excess ballast wiped away from between the

rails using a finger and an artist's fan brush. I then fixed the ballast in place using Scenic Cement and a Metcalfe applicator – with the tie-bars of all the points covered with masking tape beforehand.

So far, so good, but I realised my long-handled teaspoon wasn't going to cope with the main line sections, so I invested in a plastic ballaster instead. The ratio I used for the main line ballast mix was 3x Woodland Scenics Fine Light Grey to 1x Woodland Scenics Fine Brown.

### Raised groundwork

To create the raised areas of scenery I started by placing a quantity of expanded polystyrene at the centre of the hill to act as a central core, and then I added a series of formers around this; each rib was shaped individually to the desired profile, eventually creating the skeletal foundation seen in the accompanying photo.

Sections of Woodland Scenics Shaper Sheet were then used, followed by two coats of Woodland Scenics Flex Paste. Two coats of green paint were then applied, followed by the static grass. I used 2mm light green fibres for the base layer, which looked too 'lawn' like until patches of darker fibres were added.

The path on the ridge was formed from modelling clay, profiled to shape and then with 2mm straw-colour static fibres added to achieve a worn appearance.

### Conclusions

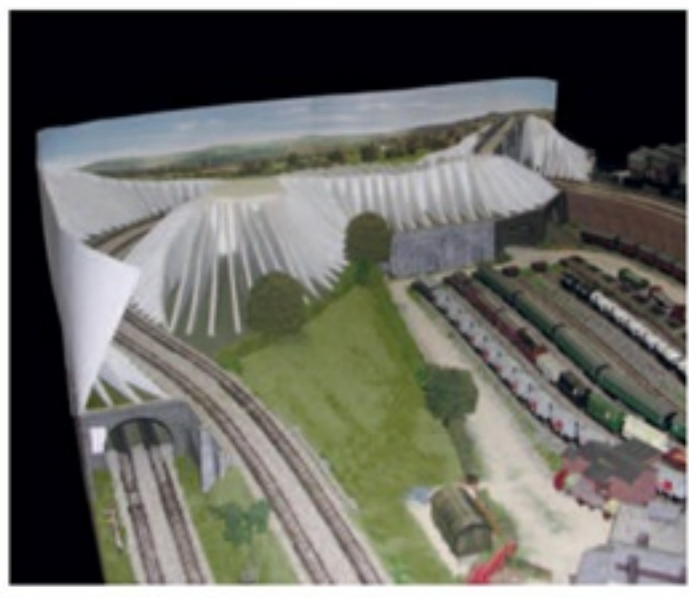
I am deeply indebted to the many suppliers who kept me going throughout the build,

▲ The station and signal box nameboards and signage for Templecombe Lower were produced as bespoke items by Trackside Signs.

▼ The extensive goods yard is shown here sitting alongside the motive power depot with its Peco turntable.







▲ Areas of raised scenery were formed by first creating this skeletal arrangement of supporting ribs, which were then covered using Woodland Scenics Shaper Sheet.

including Kernow Model Rail Centre at Camborne whose service is second to none. P&D Marsh supplied all street lighting, station lighting, signals and scrap junk in the shed yard, together with the footbridge in the corner at the 'country' end, which is in fact three extension sections linked by a full-length cardboard base, with top roof profiles made from unpainted green garden wire.

Shed yard lights were from Kytes Lights, with the OO gauge ladders changed for P&D Marsh N gauge versions.

Was it worth it? Certainly. Nothing could beat the joy of seeing the grin on my grandson's face as he sat down at the controls of the finished layout.



▲ As BR 9F No.92220 *Evening Star* prepares to depart Templecombe Lower with the 'Pines Express', on the raised formation a Stanier 8F negotiates the reverse curves with a rake of mineral wagons. The locomotives are Dapol and Graham Farish (Bachmann) models respectively.

I look forward to adding further S&D favourites to my motive power pool in the near future; a recent addition was a model of BR Standard Class 5 4-6-0 No.73050. Perhaps,

in the longer term, this may be joined by other S&D locomotives, with models of the Fowler 7Fs Nos.53808 and 53809. How about it Bachmann?





# Talking Points

Topical issues from the world of model railways

## Rebuilding the show circuit – how quickly can it be achieved?

With over half a century of exhibition visits under his belt, Steve Flint examines how societies and clubs might get their events back on the road as the health crisis continues to bite.

Notwithstanding my childhood visits to the summer season model railway displays hosted by Harold Elliot in the early 1960s in my home town of Scarborough (see RM October 1961), my first visit to a 'proper' model railway show occurred in 1970.

By 'proper' I mean of course, those provincial events hosted by model railway clubs in central town and city venues across the nation. By then I was living in Hull and one particular Thursday afternoon I bunked off sixth form classes early to make my way to Paragon station for a late afternoon departure to Leeds. The destination was the Leeds Model Railway Show, then staged in the Leeds Corn Exchange in Call Lane about a mile from Leeds City Station.

Amongst the exhibits I studied that October was David Jenkinson's *Garsdale Road*, based on Dent station on the Settle & Carlisle route, and, if truth be known, a model railway theme that has since inspired many modellers to pursue also.

Other legendary layouts included *Sundown and Sprawling*, built by diesel-outline modelling pioneer Mike Cole, alongside which appeared a section of Mike Cook's South Devon empire, this time a depiction of Dawlish station and adjacent beach.

Now, memories can falter, but I think it was also at the 1970 event that I spoke to a young modeller displaying his portrayal in 4mm scale of the Highland Railway terminus at Aberfeldy. It subsequently transpired that the builder was Dave Walker, a modeller who later went on to build some epic show-stoppers, virtually single-handedly, and an enthusiast whom I had a great deal of time for. Sadly Dave passed away earlier this year.

In 1970, the show circuit was a much smaller affair than has been seen in recent, pre-pandemic times. Long established societies and clubs, like Leeds, Manchester and the London-based MRC, for example, had for many years been holding public exhibitions, but it was to be the next two decades that witnessed a massive growth in new clubs and a boom time for provincial exhibitions. So much so, that by the turn of the century, our Societies & Clubs section would often run to six pages of events, large and small.

Of course, exhibitions would wax and wane as economic trends changed, and ever more activities came about for the general public

to do at weekends. I recall that changes in Sunday trading laws in the mid-1990s looked promising for show organisers, we thought it would enhance trading at our events, but the reality was that the general public went shopping in the high street malls instead!

Extreme weather could also knock us off balance for a weekend or two, but nothing, in all of the 50 years I have been involved with model railway shows, has ever struck the blow to our events calendar like the current health crisis. What steadily and steadfastly took over half a century to become established, was shut off in an instant at the end of March 2020.

With the vaccination programme well underway, there are glimmers of hope of a return to life as we knew it, and many of those aforementioned clubs are looking at restarting their annual exhibitions, but, is it going to be as simple as turning on the tap again? I think not.

### Are you confident to return?

The crucial factor is confidence. Following the course of the last year, evidence now suggests that crowded indoor spaces with poor ventilation are the most likely surroundings in which virus transmission occurs. I think that most people would agree with that notion: even the Government, considering the introduction of 'Covid Passports' for such situations, seems to concur.

Model railway shows take place in very similar environments, often with visitors three or four deep at the front of a popular exhibit.

Thus, as individuals, if we believe that such places form a 'high risk' situation, then personal caution and confidence becomes the biggest determining factor in our decision making process – irrespective of what the PM, representatives of Sage, or anyone else might say.

Look around and listen to your friends and colleagues, and even your loved ones, and the current mantra seems to be – trust only in your own judgement and act accordingly.

It's a situation that makes getting a public indoor event back on the road very challenging. Anecdotal evidence suggests that some exhibitors, traders and volunteer helpers are not confident to return at present: hence actually running the event is in jeopardy, never mind the worry of whether the public will turn up when the doors open.

These factors will have a greater impact on larger provincial and national shows which can involve hundreds of participants and need

**Seen at my first visit to a provincial model railway show in Leeds in 1970 was a 4F in the Up lay-by siding on David Jenkinson's *Garsdale Road* (left); a model of the prototype Deltic on Mike Cole's *Sundown and Sprawling* (centre); and a rendition of Dawlish station on Mike Cook's *South Devon Railway* (right). Photos by the author**





thousands of visitors through the door to ensure financial viability. That being so, it's a case of, the bigger the show, the bigger the risks all round. So, does it make sense to start the reopening programme with smaller exhibitions?

### Tentative steps

To find a possible answer to that question I contacted a random sample of organisations that are intending to open their doors this autumn. The first respondent, Trevor Smith of the Blyth & Tyne MRS came back with a very positive answer:

"Our model railway society has had an annual exhibition since the early 1980s. We've used for venues, a community centre, schools, council leisure facilities, and on one occasion even an empty office block. The main purpose of the event was to promote the hobby, to raise money for the club, and to give members the opportunity to showcase their efforts."

"We had planned an exhibition for 2020 before the lockdown, and had incurred expenses in booking the venue, booking exhibits, printing posters and flyers and arranging insurance.

Lockdown put an end to the exhibition, we were left out of pocket, but not to a significant extent."

Trevor went on to say that the Society's next public show is scheduled for 23 and 24 October.

"We have planned the 2021 exhibition on a no expenditure basis." He explained that this would mean layouts would be sourced from within the club, and operated by club members, thus avoiding any hotel bills or travelling expenses for outside layouts. Traders are to attend free of charge – no stand rentals will be collected but proceeds from the door entry will be shared pro-rata.

Trevor added, "The event is being run in partnership with Christ Church in North Shields which will provide the adjacent church hall free of charge and will be responsible for collecting entrance fees and income from refreshments."

"If Government regulations change and we have to cancel, neither the club nor the traders will be out of pocket."

On the face of it, this route to reopening which eliminates any financial risk does look promising. However Trevor pointed out that there is still uncertainty on health guidelines and until the Government offers further advice, many unanswered questions in connection with face coverings, cash and card payments, use of the test and trace app, all still need resolving.

"We still don't know if we will have to implement a one way system, apply social distancing, put a limit on numbers, possibly check visitors' vaccination certificates and so on."

Whatever happens says Trevor, the event will abide by the health guidelines that are in place at the time.

Avril Spence of Meridienne Exhibitions Ltd, the organisation which holds The International N Gauge Show (TINGS) at the Warwickshire Event Centre said that this year's event was going ahead as planned. It's a large national show with involves significant financial risk for the commercial organisers, so I was interested to hear what she had to say.

"Our customers and traders want to get back to normal routines as quickly as possible. The show is going ahead with full compliance of any guidelines that are in place at the time."

She added that although some of Meridienne's other shows have



▲ Smaller shows, such as that in Alton, which carefully manage their expenditure, will be better placed to ride the storm should visitor numbers drop dramatically. Photograph by the author

been cancelled, this one is still planned to go ahead on the 11 and 12 September this year.

If nothing else, a commercial show such as this promises to be a test case for the hobby and larger modelling events in general, the outcome of which is awaited with eager anticipation.

Finally, at the other end of the scale is the Epping Railway Circle, a small local club which is staging its annual show on 13 November 2021. The organiser John Wood has explained that both mandatory and voluntary health guidelines which are in force on the day will be observed, including wide gangways, provision of hand cleansing points, maximum ventilation and a likelihood of a one way system. The event is to be held in the village hall in Theydon Bois, Essex and the hall management have been very generous to the circle over costs.

"All exhibits are to be brought in from the immediate locality, keeping expenses to an absolute minimum," John advised. "We can only hope by November, people will be more confident about visiting those exhibitions which take precautions seriously". He confirmed that under normal circumstances the break even point for visitor numbers is about 300.

### Long recuperation

The next step in getting shows up and running again is going to be how we, as a nation, deal with our crowded indoor events, and until clear evidence and some good

Government advice emerges, it seems that the cautious approach shown by the three respondents in this article is the right thing to do.

But even then, eliminating as much financial risk as possible and sticking rigidly to Government advice, gives no real pointer as to whether the paying public will return in the numbers needed: we shall have to wait and see how these pioneering shows perform.

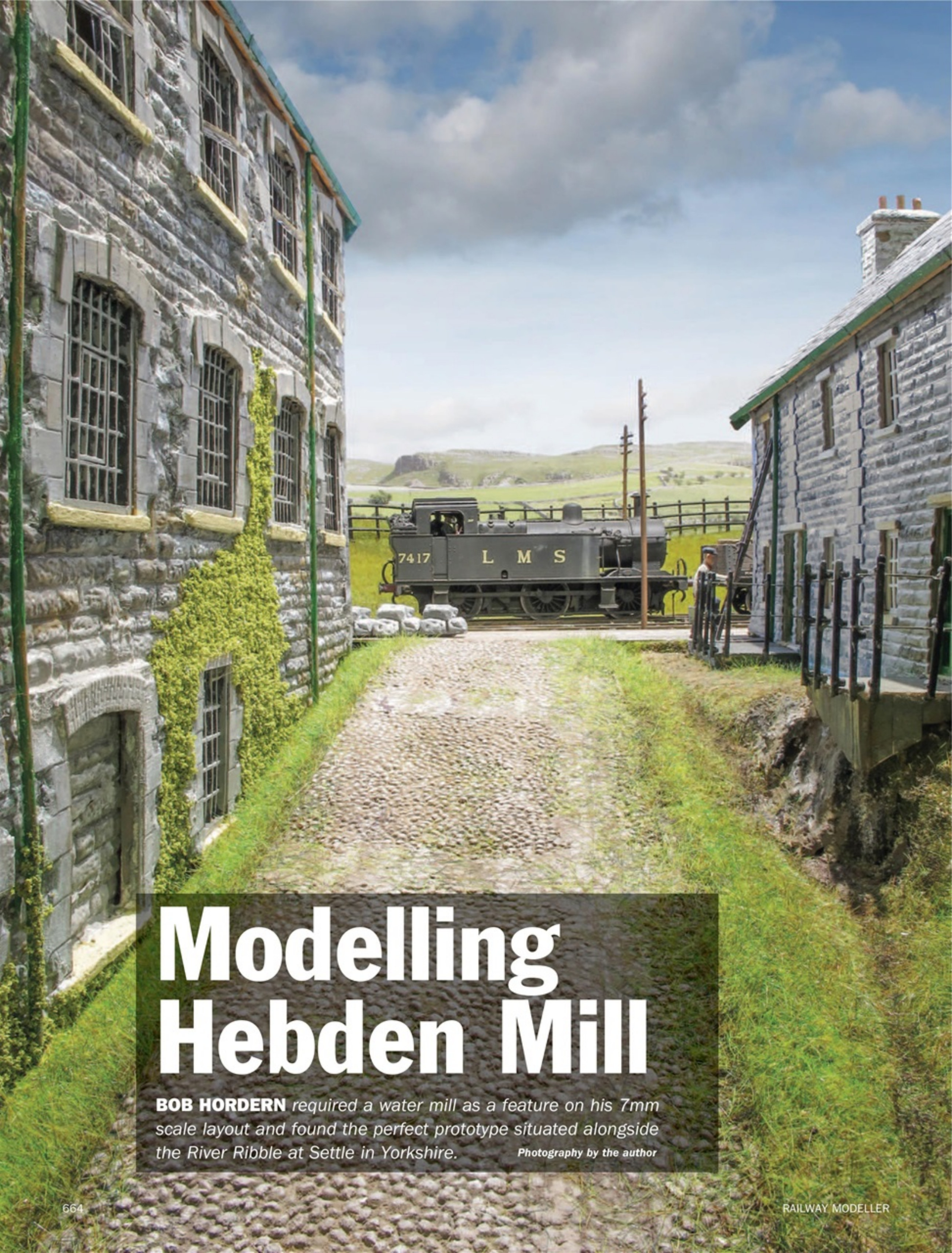
Larger shows with much more to lose will, I think, have to remain very cautious, cut their cloth accordingly, or seek out commercial sponsors prepared to underwrite any losses.

In all my 50 or so years of association with model railway events, I've never seen anything like it. Fortunately participation in the hobby remains steadfast, but the recovery from this greatest hiatus in our show calendar could well take longer than we ever dared imagine.



▲ Big provincial shows, even those held in airy, well ventilated venues such as at York seen here in 2015, will need to balance the expenditure risk carefully should numbers drop substantially when doors reopen. A possible solution may be to seek commercial sponsorship to contribute to underwriting any significant losses. Photo: Craig Tilley





# Modelling Hebden Mill

**BOB HORDERN** required a water mill as a feature on his 7mm scale layout and found the perfect prototype situated alongside the River Ribble at Settle in Yorkshire.

*Photography by the author*



During the closing years of the 19th Century, a group of local businessmen in upper Wharfedale raised funds to build a light railway to serve their estates, farms, mills and quarries. This convenient snippet of local history does mirror what happened in many parts of the Yorkshire Dales and the following focuses on one of these aspects: the real mill at Hebden, and its significance in the building of the 7mm scale mill on my layout *Kirtley Bridge*, which was featured in the May issue of *RAILWAY MODELLER*.

Firstly, it is important to realise that most water-driven mills were around long before the railways developed and often began life grinding grain. The later textile production in the Dales involved both cotton and wool and more recently paper. Inward and outward transport by horse and cart, then canal and eventually railways all played a key role.

My fictitious Hebden Mill was an important scenic feature on my layout as it would provide traffic and revenue for the branch, jobs for local people and – as a large building – would have become a prominent part of the local landscape.

The model mill needed to be situated adjacent to a river where there was some drop in level and the possibility of diverting water to turn the mill wheel. This was very much part of my overall design brief when constructing the original open top baseboards for *Kirtley Bridge*.

### Choosing a prototype

During my initial search for a suitable real mill to use as a basis, many were discarded on the grounds of either size, aspect or location. Eventually I came across Bridge End Mill at Settle, located very near to where I live. This meant that measurements and photographs were easy to obtain, and I even managed to find an old plan of the site.

Bridge End Mill was one of a number of mills built adjacent to the River Ribble at Settle, North Yorkshire. A large weir diverted water toward a sluice gate feeding it into a race that drove a single large wheel. This was a gravity back-shot type that released water underneath the mill, returning it downstream. (A separate and very modern micro-hydro turbine has been installed here in recent years.) The mill and its adjacent workers' cottages seemed an ideal prototype to use as the basis of the model.

My research revealed that an earlier version of the mill was somewhat shorter, this suiting my plan to reduce its size to fit my layout. Access to the real mill was by road and the outline of the building's main loading bay can still be seen in the stonework. This original feature has been re-instated on the model.

### New rail access

For rail access my solution was to create a length of inset track that could allow both

◀ **Fowler 3F 0-6-0T No.7417 is framed between Hebden Mill (left) and the adjacent workers' cottages (right) as it busies itself with shunting duties.**



▲ **The fisherman makes ready with the net. It looks as though he may land the catch of the day! Hebden Water is the where the water is held to be diverted into the mill sluice.**

The author's layout *Kirtley Bridge* showing Hebden Mill and the adjacent row of workers' cottages, also inspired by buildings at Settle.

rail and horse-drawn traffic to be loaded. The horse and cart hint at the previous form of transportation and add a period feel.

On my model the level crossing gives access across the railway to the mill yard and the end loading bay, though the latter is not easily seen from the front (viewing side) of the layout.



◀ **The rear of the mill showing details including the Invertrain windows and the combination road and rail access. The horse and cart adds a period feel to the scene.**





## Bridge End Mill, Settle

There is evidence of water-powered industry on the site of Bridge End Mill reaching back for several centuries and there is the possibility that there may have been a mill here for 'fulling' woollen cloth (water-powered fulling mills used hammers operated by a shaft on a waterwheel) during the 16th Century.

A forge to smelt iron is recorded on the site of the mill in the mid-18th Century which may have used water-powered tilt hammers. The natural water supply from the River Ribble presented enough flow to drive the wheel throughout most of the year though it was sometimes necessary to raise the height of the weir to enable sufficient flow to the wheel (which is now the only surviving example in the area).

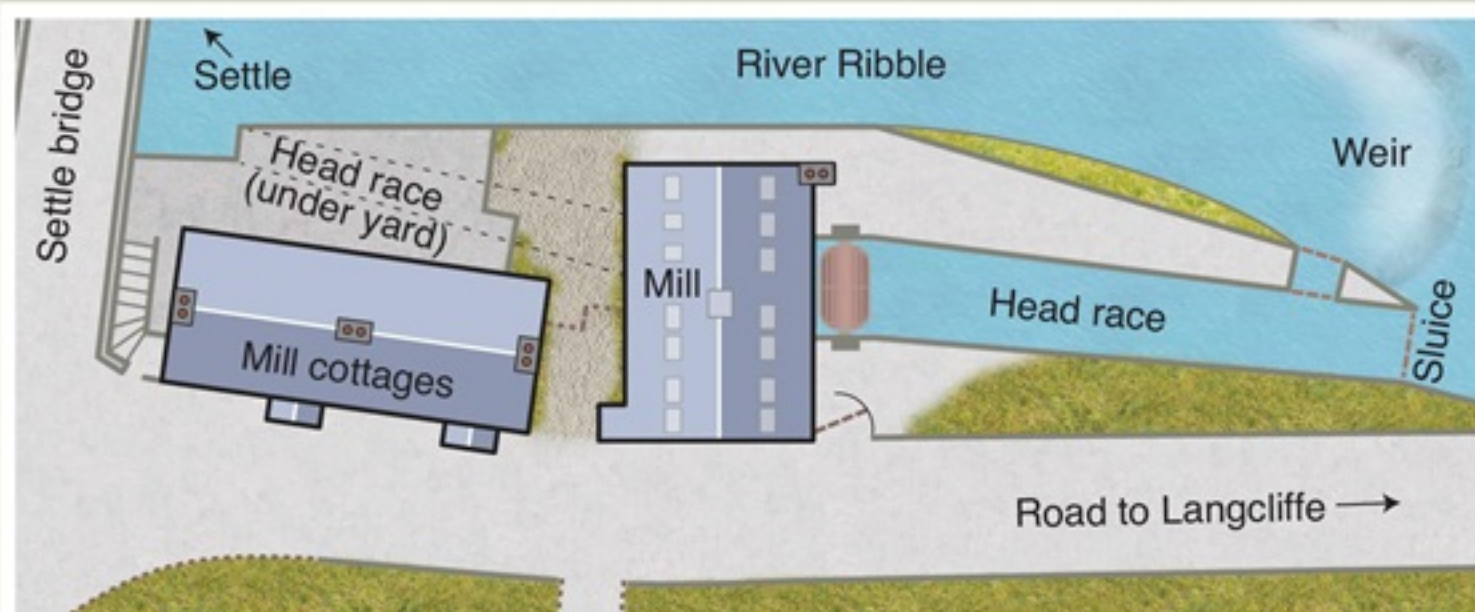
The current structure, which dates from around 1790, may have started water-powered cotton spinning at that time. The building seems to have been rebuilt at least twice and was described as a cotton producing mill by the early-19th Century. Contemporary sketches suggest that the height of the building was increased soon afterwards, supported by a date on a keystone which indicates substantial rebuilding in 1818. By the mid-19th Century spinning mules had been introduced and these male-only worked machines may have prompted the building of the cottages that stand nearby.

The current waterwheel was added when the building was refitted with a joiner's workshop during the 1860s.



▲ The mill has been developed into a series of apartments which are spread across four floors. The old mill yard provides parking. The wheel can be seen at ground level in the centre.

Production at Bridge End Mill appears to have continued until 1901 after which it went through several different uses including an antique dealer's workshop. During the late 1980s, in common with other similar buildings, the building was re-developed for domestic usage and although outwardly not much has changed, inside the structure now contains multiple flats.



◀ The sluice gate is still in working order and is seen here with the weir in the background. The height of this could be raised using boards during dry spells to keep the water level high enough to feed the wheel.

▶ The water wheel is still extant although it has fallen into disrepair.



▶ This view of the end of the mill building reveals the outline of the old loading bay. This was prototypically for road traffic only but adapting this scene for rail transport on the model was very straightforward.

strengthened with ply struts and covered in Slater's Plastikard embossed stone sheet. The industrial bow-topped windows were purchased from Invertrain while the rest of the features were scratch-built.

At 7mm scale the mill building has a large footprint and is quite tall so it has to be

removed from the layout and packed separately to travel to exhibitions. Though I may eventually decide to fix the lower floors permanently and make only the upper part removable.

The wheel and sluice gate were built around a discarded plastic drum, augmented

with slats fabricated from balsa wood and other parts salvaged from an old Wills 4mm scale plastic kit. In this type of wheel, the buckets have to fill up before the wheel rotates backwards, which is an altogether more sedate operation than with the overshot variety.





### Motorising the wheel

The wheel looked fine, but it cried out to be made into a working feature. The challenge was to produce a wheel that was not only slow in operation, but almost silent as well. This led to the question of how to power the wheel and therefore, through the good offices of the Gauge O Guild online forum, I sought guidance for how best to achieve this.

After numerous replies from fellow Guild members, I managed to devise a means of getting the wheel to turn at a sufficiently slow speed – and almost silently. The solution has been to use a small motor (see below) with a

voltage regulator to reduce the voltage from 12V dc to 1.2V dc, then wire a diode across the feeds to reduce it to 0.7V.

Finally, to create the representation of water feeding the wheel, I used Realistic Water from the Woodland Scenics range of materials.

**Looking along the river with the weir in the foreground and the sluice gates to the right.**

▲ The mill yard at Kirtley Bridge showing the Midland Railway signal box, crossing gates, mill wheel and riverside.

### Further information

[www.gaugeoguild.com](http://www.gaugeoguild.com)

[www.visitsettle.co.uk/settle-riverside-walk](http://www.visitsettle.co.uk/settle-riverside-walk)



▲ The water wheel assembly with the motor that powers it on the right, which is attached directly to the central spindle.





# Fox End Basin and Brewery

Photography by the author or as credited

**MICK PAYNE** described the first part of the extension to his loft-based EM gauge system in the August issue. This month he explains how the re-siting of a seldom-used OO layout provided space for a second new section, which depicts a bucolic waterside scene.

In last month's issue I described the planning and processes behind the project to extend my *East Derbyshire Mineral Railway* system, presenting the new section that comprises Cransley standard gauge station and the narrow gauge line that serves the adjacent village estate. This article details the second

new section to be created, which is referred to on the accompanying plan as Fox End. But first, a bit of background...

## Making more space

For a time, my small OO gauge exhibition layout *East Quay* was set up on the opposite side of the new area of loft space to Cransley station. I had built this OO layout as a change

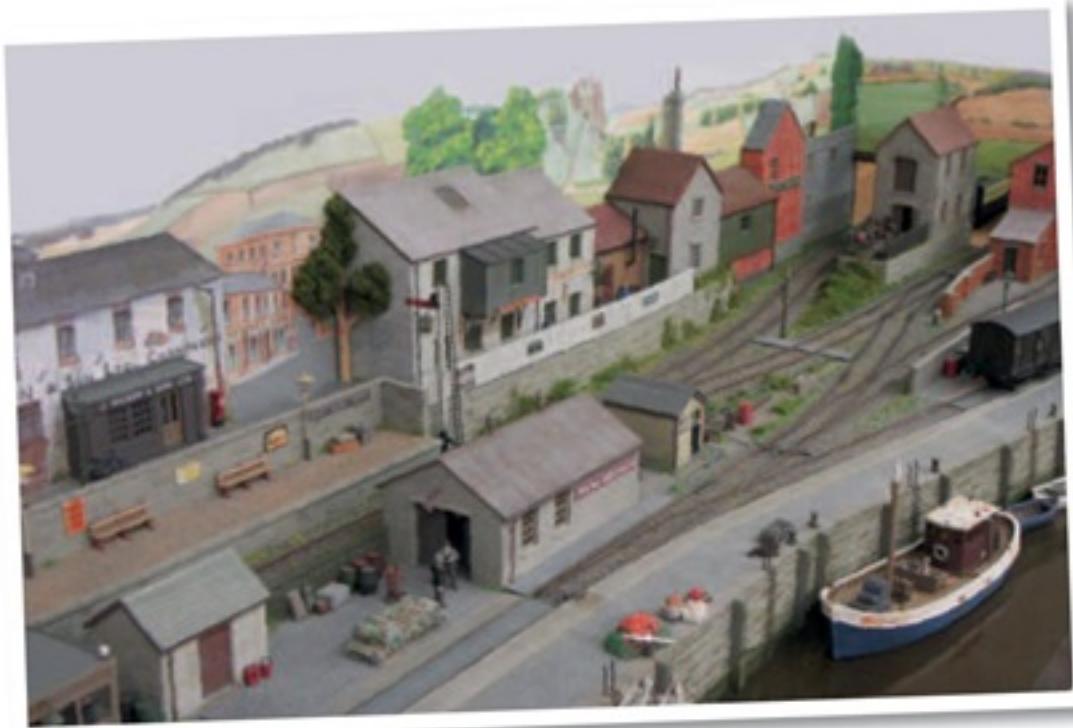
from my usual EM gauge modelling to try out some of the excellent ready-to-run locos and stock that have been made available in recent times.

The idea was that, in between taking it to shows, I could operate *East Quay* as and

**EDMR 0-4-0T No.11 draws some vans out of the brewery siding at Fox End. In the background a passenger train arrives on the narrow gauge.**







▲ A view of *East Quay*, the portable OO layout that was formerly stored in the part of the loft space that is now occupied by Fox End.

Before (inset) and after views showing the end of the original *EDMR* layout and how it has been adapted so that it can connect through the loft partition to Fox End. The wooden sheds in the foreground provide a point of reference between the two views. It should be noted that, despite the bridges that are visible in the background on the original version, the lines stopped where they met the backscene. However, what was a narrow gauge/standard gauge interchange siding is now the standard gauge running line that extends through the backscene.

*Inset photo: Paul Bason*

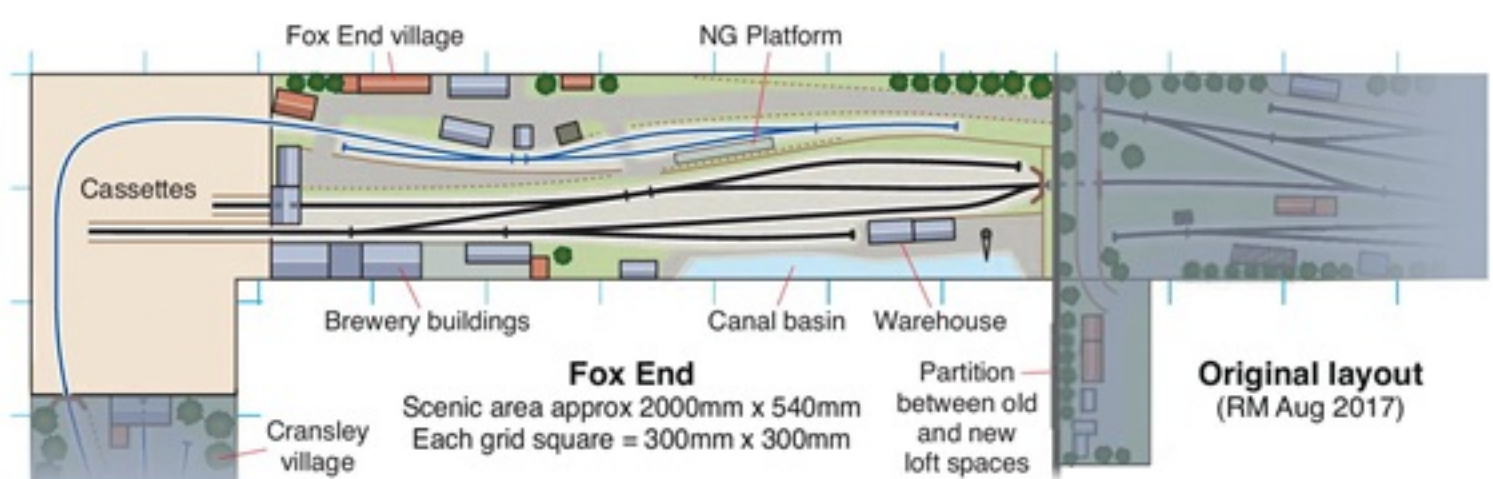
when I wanted to, whilst also testing any new stock that I purchased, prior to conversion to EM. The reality, however, was that the layout just sat there most of the time, unused and gathering dust. I realised that storing the layout in this way between shows was not the best use of the available loft space; I decided therefore to pack it away, storing it instead underneath the baseboards of the *EDMR* along with its accompanying OO stock.

### More EM gauge

The resultant space formerly occupied by *East Quay* provided an opportunity for further expansion of the *EDMR*. The question



▲ The corner with storage cassettes as seen on the accompanying plan. Curving round behind is the narrow gauge line.



was though, with what? It was planning time yet again.

For this second new section there was space available for a scenic area measuring approximately 2m long x 540mm wide, together with an additional 700mm length at the left-hand end that could accommodate a short fiddle yard with three cassettes.

At the right-hand end of the available space was where it would connect through to one end of the original *EDMR* system: when I studied the track at this end of the existing layout, I realised that there was a siding that could be realigned to form the approach road through onto the new section, which was named Fox End.

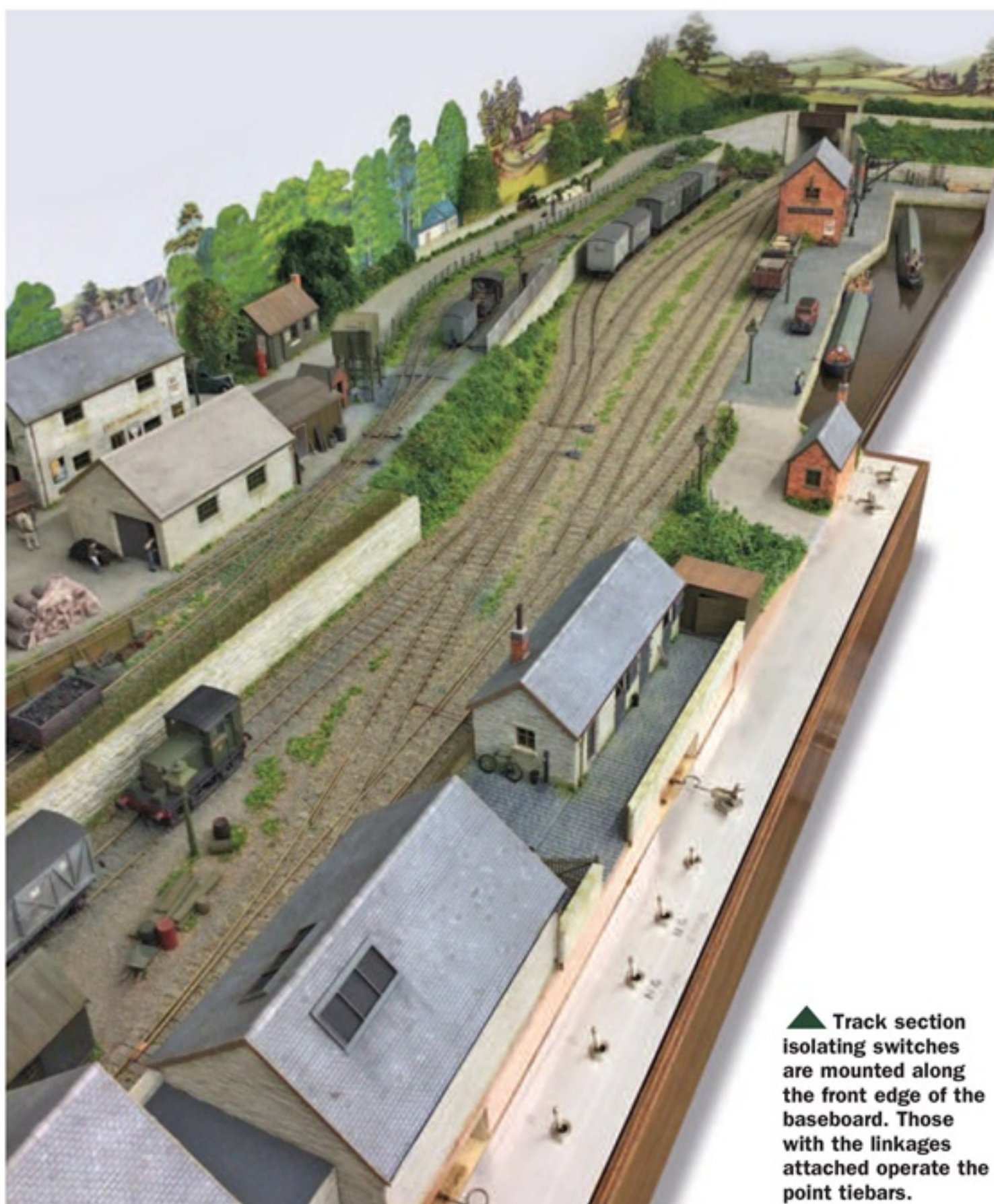
### A track plan for Fox End

The track layout for Fox End is fairly conventional, comprising a run-round loop off of the main running line and a siding into the brewery, together with a storage line for departing loaded wagons and a siding to serve the canal basin.

The section at the rear of the scene was raised by approximately 40mm. This facilitated its lining up with the OO9 narrow gauge line in the corner fiddle yard that emanates from Cransley village.

Construction of the standard gauge trackwork and electrics for the Fox End section followed the same methods as those described last month for Cransley station.





▲ Track section isolating switches are mounted along the front edge of the baseboard. Those with the linkages attached operate the point tiebars.

However, having now used all of the old ECM Compspeed controllers that I had in stock, a new All Components unit was purchased, which has proven to be a perfect modern equivalent.

### A canal adds to the scene

As well as my love of railways I am also quite partial to canals. It struck me that I now had the space at the right-hand side of the Fox End section to incorporate a section of canal basin with an adjoining goods warehouse.

At the left-hand end there was the need to disguise both the exit to the standard gauge corner fiddle yard and the exit of the narrow gauge line that runs through to Cransley village. Therefore, I decided to create an assortment of buildings to fill this area, these representing part of a small brewery complex.

### Structures

As with the Cransley section, all of the structures on Fox End are scratch-built from framer's mounting board and are clad with either Slater's embossed Plastikard sheet or Howard Scenics Victorian brickpaper. The vast majority of the architectural detailing has been carried out using door and window packs from both the Wills and Peco ranges.

The retaining walls were made using a thick card base which was then covered with Wills stone sheet. This really looks the part when it has been painted and weathered.

▼ Fox End canal basin with H G Slater's *Eliza* moored up. The view underlines the layout's main operational interest of goods dispatching using the many wagons on the layout, which are built largely from kits.







▲ A Baldwin 4-6-0T stands by the water tower at Fox End, its tanks being replenished before forming the next train back to Cransley.

### Conclusions

I have to say that the extensions to the layout created in the newly-developed part of the loft have been a great success and the operation of the whole EDMR is now vastly expanded. The additions of Cransley and Fox End not only provides increased flexibility, but in the way of American modellers, more destinations for goods stock to be worked both to and from.

Although the layout has been constructed in sections at different times, maintaining consistent constructional techniques throughout has meant that the system has a very homogeneous feel with no items that look as though they do not belong. The addition of working OO9 narrow gauge section has also added considerably to the operation; the narrow gauge on the original part of the layout having been just a static scenic feature.

Finally, thanks must go to my wife Chris for her support for the 'loft take over' project.

▼ No.6 *Victoria* shunts a rake of Hale Fuels wagons in Fox End yard. The height difference between the standard and narrow gauge formations is apparent in this view. It is hoped that the eclectic fleet of EDMR locomotives will be described in a future article.



▼ The extent of the Fox End module as viewed from the end with the canal basin.



### Previous articles

The Cransley station and village sections were described in the July 2021 issue, whilst the original part of the *East Derby Mineral Railway* system – situated in the other side of the loft space – featured in the August 2017 edition of *RAILWAY MODELLER*.



# The Ashover Light Railway

**STEVE CROUCHER** serves up this slice of rural Derbyshire in OO9 – designed for a spare bedroom location – which blends scenic richness and industrial clutter with plenty of operational interest.

Photography as credited

The Ashover Light Railway was a late arrival on the narrow gauge scene and a relatively short-lived one at that.

Opening for freight in 1924 and passenger services in 1925, it survived a mere quarter of a century before discreetly vanishing from this picturesque corner of north east Derbyshire.

Early proposals by the Clay Cross Company, which was behind the scheme, had been for a standard gauge line, running to Ashover from a connection to the Midland Railway at Stretton, with a 2' gauge rope-worked branch near Wooley serving a colliery at Alton. But the Consulting Engineer for the project and renowned advocate of light railways, Colonel Stephens, persuaded the company that building the whole railway to 2' (60cm) gauge, utilising surplus War Department locomotives, rolling stock and track materials, would be a more practical and cost-effective solution. It also meant that the company could afford to extend the line to Clay Cross, a few miles south of Chesterfield.

Although intended primarily for transporting limestone and other materials from the quarries at Ashover and Fallgate, along with milk from the farms in the valley, a passenger service was also to be provided. The company was quite generous in this respect and although the line was only 7½ miles in length, there were 14 stations and halts (see Fig.1, overleaf), which roughly averages out at one every half mile!

Trains were often mixed, a typical formation being one carriage and two or three wagons attached behind it. Ashover Butts itself was a very popular beauty spot and weekends and public holidays would often see the railway working at full capacity to bring in visitors who had travelled from the nearby towns and cities to enjoy its

scenic charms and to take refreshments at the unusually named Where The Rainbow Ends café.

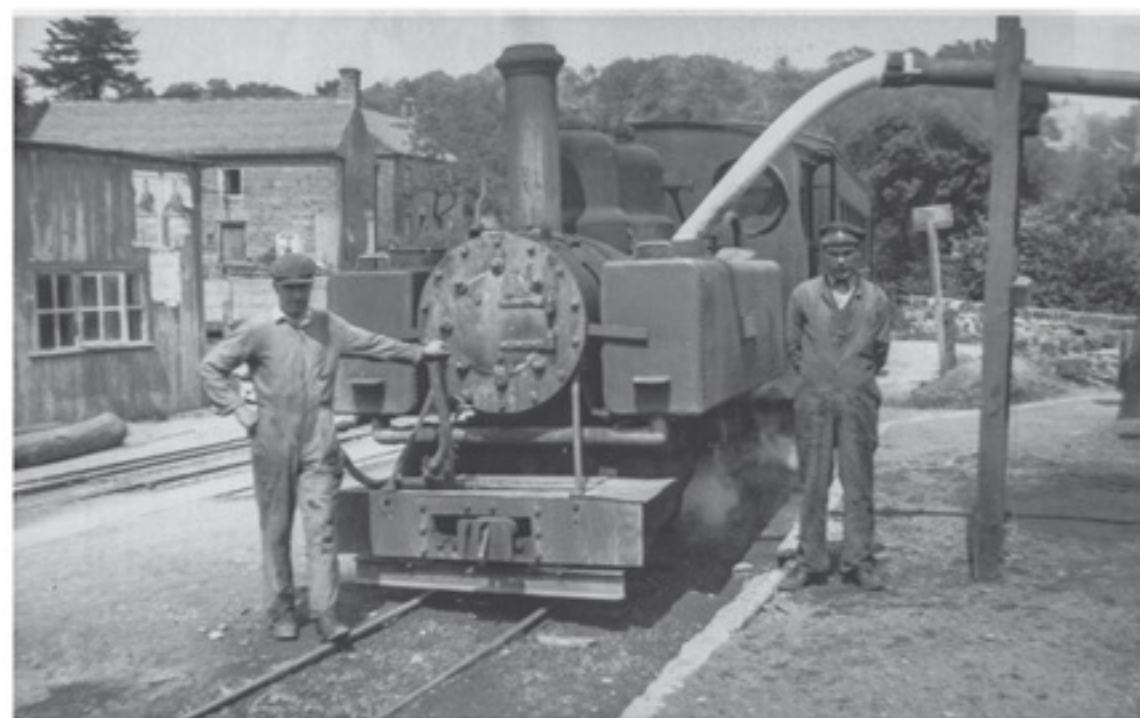
Bus services began to make inroads into the locality as early as 1926, and daily passenger services on the ALR were gradually trimmed back over the years before being permanently withdrawn in 1936, although special services were occasionally operated thereafter, for example on public holidays.

The ALR quietly went about its business for the next 14 years transporting freight along the valley until it eventually closed in 1950 and was dismantled, except for the section of the line around Fallgate, which was retained as an internal system for the quarry and fluorspar washing plant.

## Available stock

For a minor railway, The Ashover Light Railway has become surprisingly well catered for in 4mm scale in recent times. The introduction by Bachmann of its excellent Baldwin 4-6-0T model (see Reviews, RM May 2018) is what really makes this project very appealing, with *Bridget*, one of the ALR locomotives, available in its later guise with an extended cab and plain black livery. Added to this the ex-WD bogie wagons are also available in two ALR liveries from the same source and with the Gloucester Carriage Co. coaches also in development as part of the manufacturer's range, we could be inundated with layouts of this Derbyshire gem before too long!

It's worth mentioning that Dundas Models also produces plastic kits of the various ex-WD bogie wagons and the coaches are also available as a plastic kit from Narrow Planet, under the Meridian Models brand, although at the time of writing it is believed that they are out of stock.



▲ With a train from Ashover, the crew replenish the tanks of Baldwin 4-6-0T *Bridget* at Fallgate on 25 July 1934. Photo: Dr J R Hollick/courtesy Wild Swan Publications

## The plan described

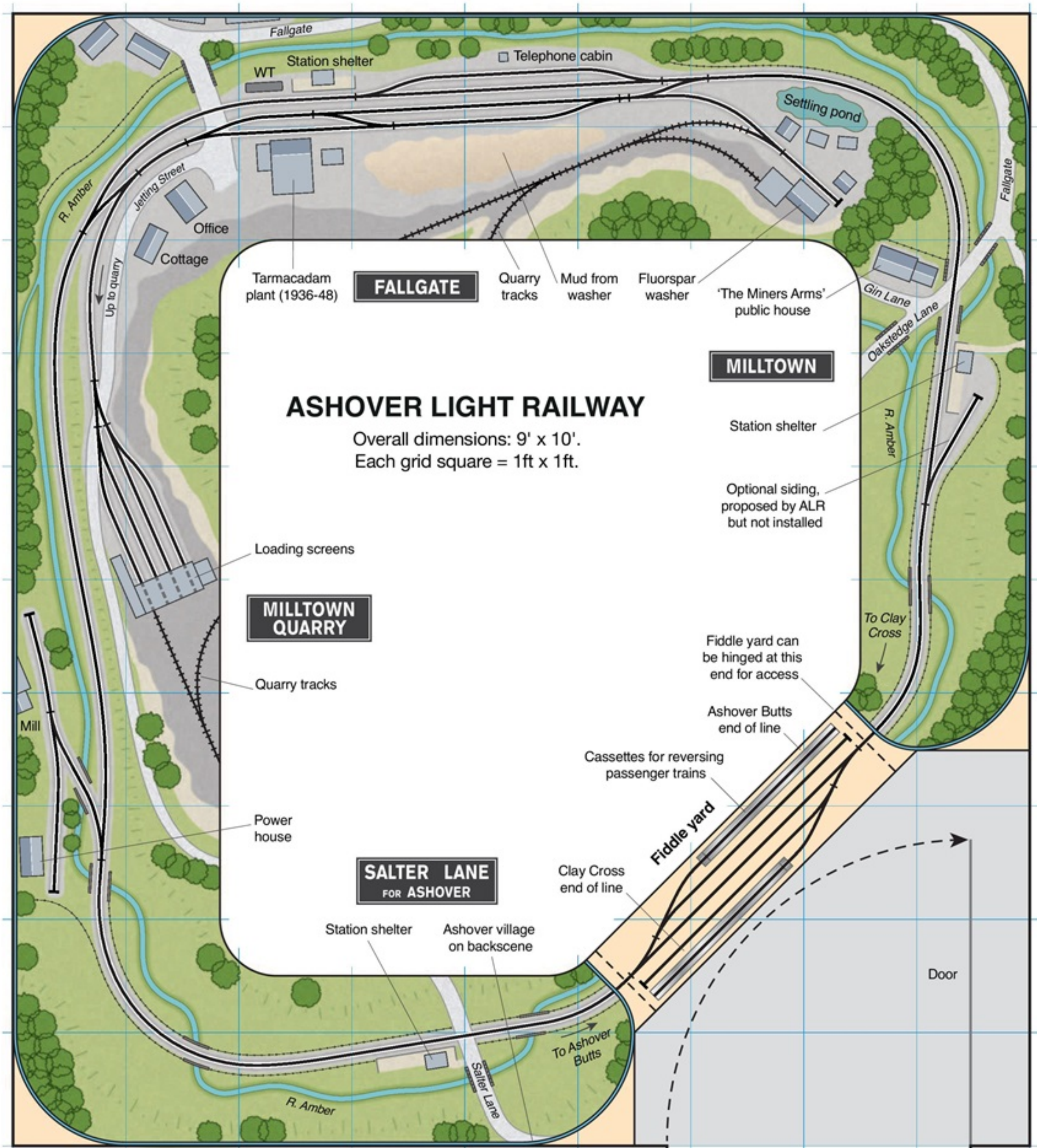
Instead of focusing on the terminus at Ashover Butts, which is attractive scenically but slightly awkward to accommodate as a model railway, this design draws on three intermediate locations along the railway's route.

Fallgate is the most significant one, incorporating a quarry, tarmacadam plant (operational from 1936 to 1948), and a fluorspar washing plant, along with basic passenger facilities. The other two are the halts at Salter Lane and Milltown, the former actually being much closer to Ashover village itself than the main Butts station, and the latter featuring the Miners Arms public house which the railway ran past over an unmanned level crossing. Combined, these three settings serve up a blend of scenic richness, industrial clutter, and a decent amount of operational interest, whilst allowing the model to breathe by resisting the temptation to cram too much into the available space.

An ever-present feature along the second half of the route to Ashover was the River Amber and, as on the prototype, this plan sees the railway frequently crossing over it on small bridges, with trains also venturing over unmanned road crossings in a number of places, all adding to the visual interest.

Designed to be housed in the second bedroom of a typical modern property, the layout occupies an area of 9' x 10', but it could be compressed into an 8' x 9' room without losing many of the key elements, although the Fallgate section might need to have some of the track simplified to avoid it





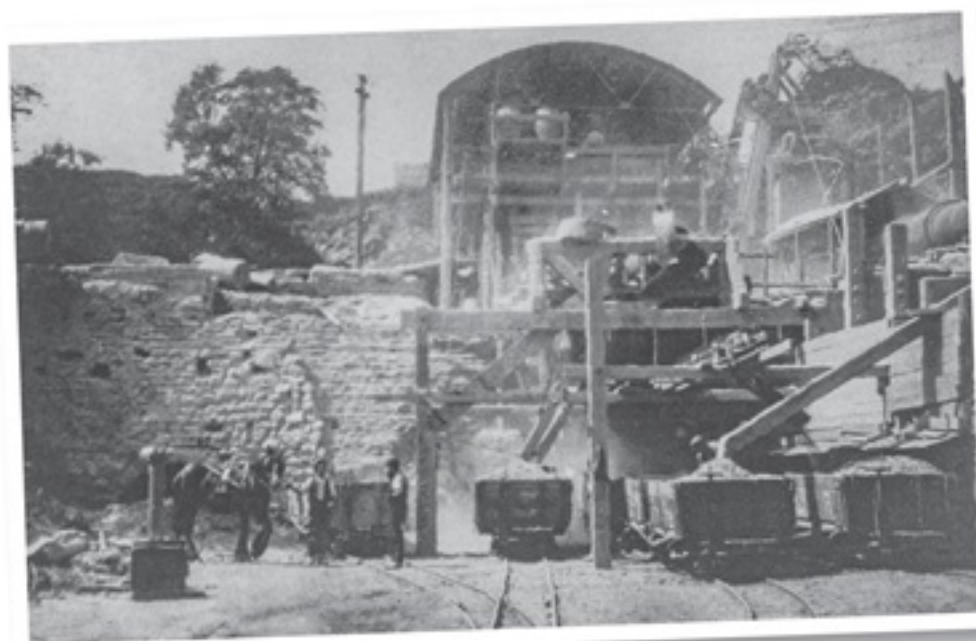
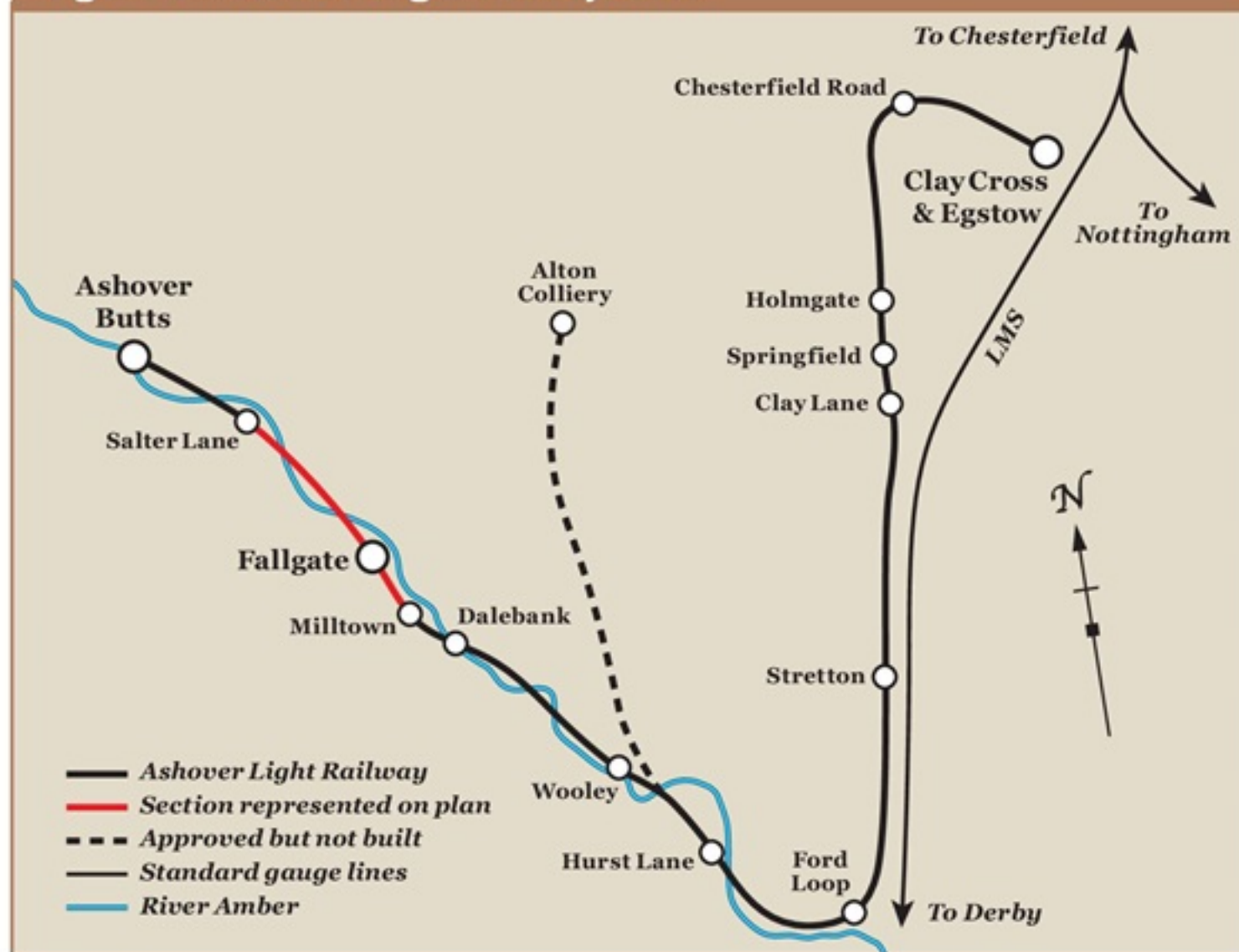
becoming too cramped. For example, the loop running past the telephone cabin could be omitted along with the short loop serving the tarmacadam plant, this structure being relocated to sit alongside the other loop that runs through the yard. The line running up to the quarry and the main line running behind it could also be shortened a little, whilst the

With the Miners Arms on the left, Peggy crosses Oakstedge Lane with a stone train for Clay Cross on 6 September 1946. Photo: R E Tustin/courtesy Wild Swan Publications





**Fig.1 – The Ashover Light Railway route**



▲ ALR bogie wagons being loaded at Milltown Quarry in c.1936. Photo: R Frost Collection/courtesy Wild Swan Publications

Milltown and Salter Lane sections could also be slightly condensed without any great disadvantage.

For convenience, the plan is designed around Peco Streamline OO9 track using the small radius turnouts. For modellers looking to make their own track, then the prototype's lightweight 30lb per yard rail section (that

Examples of ALR ready-to-run models produced for OO9 by Bachmann. Photo: Andrew Burnham

was spiked directly to the sleepers) could be replicated using PCB sleeper strip, cut to suitable lengths, with Code 40 flat bottom rail (available from [www.britishfinescale.com](http://www.britishfinescale.com)) soldered directly onto it.

An important consideration at the planning stage is the height at which the layout will be displayed because modern properties often have fairly low ceilings. As an example, those in my own home are only 84" (7') in height, so if the fiddle yard is designed to hinge upwards at one end to

allow easier access to the operating area, it's important to make sure it doesn't foul the ceiling when lifted. With the fiddle yard being approximately 36" long, this means the nominal track height for the layout will only be around 45". However, if the fiddle yard is constructed as a permanently fixed board, or one that can simply be lifted out occasionally for access, then the layout can be displayed at a greater height, providing a more realistic eye level view and making it easier to duck underneath it.

This also has the benefit that stock can be permanently stored in the fiddle yard, making the layout ready for operating at a moment's notice.

## Operating potential

The layout is designed as a circular scheme but with an intermediate fiddle yard between the Milltown and Salter Lanes scenes, allowing it to be operated in a fully prototypical end-to-end manner.

On the prototype, a siding was proposed at Milltown and a turnout installed in preparation. Although it was never actually built, the plan incorporates it as an option to add a little extra operational interest. Additionally, the power house was taken out of use in 1927 after which the siding to the mill, used to grind fluorspar, was only used occasionally and removed in 1941, but if you are willing to be a bit flexible with these facts, then this siding adds a further bit of interest to the running of the layout.

One thing to note is that the prototype had triangular loops at each end of the line, allowing passenger trains to be completely reversed, ready for the next journey back along the line, the normal practice being for locomotives to run chimney first. This means in the fiddle yard it will be necessary to have cassettes to turn the trains around, or as a compromise you could have conventional sidings and just pick up the locomotive itself, turn it through 180°, and place it on the other end of the train. Alternatively, the continuous run option allows you to just watch the trains circulate, without the need for running round or turning.

Modelling in OO9 offers a lot of potential to represent a fairly substantial stretch of railway and scenery in a reasonably modest area. Aside from the ALR, the basic concept proposed here could be applied to other narrow gauge schemes, including freelance railway systems.

## Further reading

*The Ashover Light Railway*  
by Robert Gratton & Stuart R Band,  
Wild Swan Publications Ltd  
ISBN 0 906867 72 X  
[www.titfield.co.uk](http://www.titfield.co.uk)

*The Ashover Light Railway*  
by K P Plant, Oakwood Press  
ISBN 0 85361 350 8  
(Includes some scale drawings of structures and stock.)

*The Ashover Light Railway 1925-1950*  
– An Illustrated Presentation  
60pp A5 photo book available from  
The Ashover Light Railway Society:  
[www.alrs.org.uk](http://www.alrs.org.uk)







# MICRO MARVELS

## THE SMALL LAYOUT GALLERY

# Innes Mill

Inspired by a visit to Scotland, **VALENTIJN VAN DER HAEGEN** built this small O-16.5 scale industrial layout specifically for an exhibition competition in Belgium.

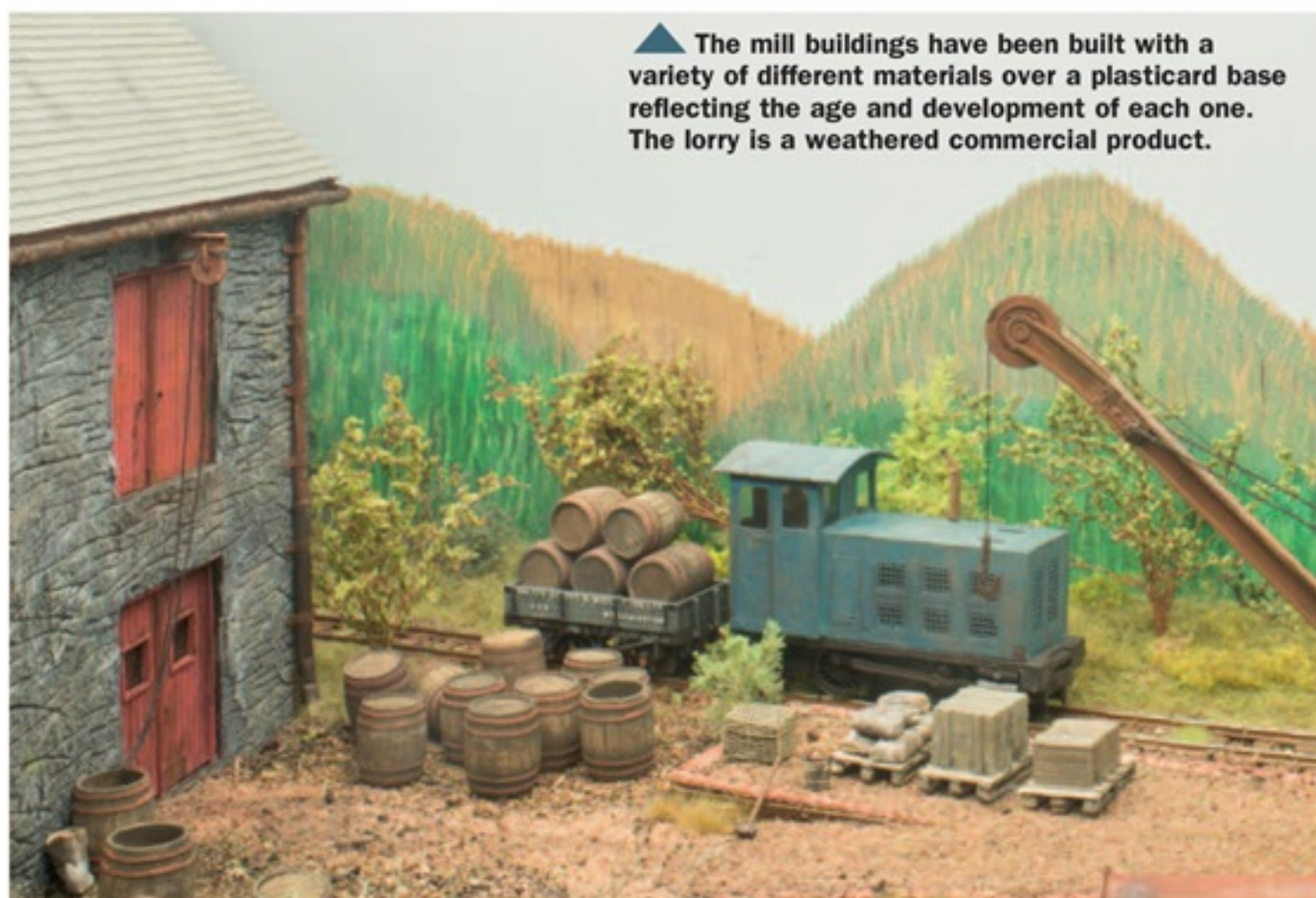
Photography by Andrew Burnham



Inspiration for a new layout sometimes bubbles away for years before construction can begin. Not so for this small layout. In 2018 I wanted to create an entry for the mini layout competition at that year's Modelspoor exhibition in Leuven, Belgium. Whilst browsing the internet I found a postcard showing a waterside factory and it was this, combined with a recent visit to Scotland, that provided the inspiration for my project, which was modelled in O-16.5.

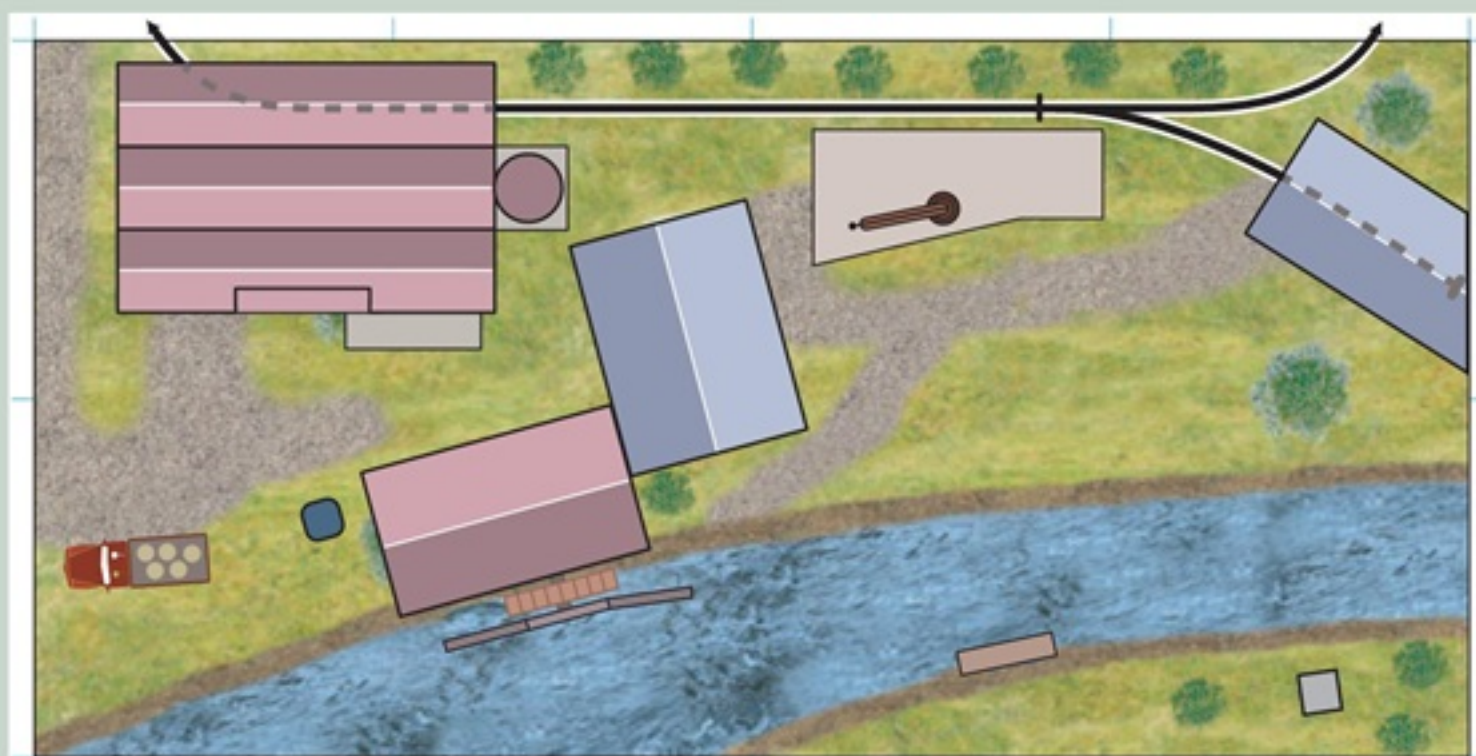
It was clear from looking at the postcard that the factory had been extended in stages over the years, with different types of building materials used. It had started out as a simple wooden building with a water mill and, a few generations later, an extension was built using natural stone. Then at the

One of the line's locomotives delivers a load of barrels. The locomotive is a brass body kit mounted on a basic O-4-O Hornby chassis unit. The wagon is an unaltered OO gauge Hornby three-plank item.



▲ The mill buildings have been built with a variety of different materials over a plasticard base reflecting the age and development of each one. The lorry is a weathered commercial product.





### Innes Mill

Overall layout size 1200mm x 600mm. Each grid square = 300mm x 300mm.

beginning of the last century a large workshop was built onto the warehouse in brick. This provided a perfect scenario for trying out different modelling techniques.

A design with the basic elements of the diorama was soon created, following which mock-ups of the 7mm scale buildings. The sizes and positions of the buildings were then adjusted as required.

### Foam baseboard and lighting

The layout consists of two modules: one with the scenic section measuring 120cm x 60cm and one with the fiddle yard at 120cm x 40cm. Both were built with a frame of timber and a surface of 5cm thick rigid foamboard from a local builders' merchant. This construction has the advantage of being both lightweight and sturdy. The structure includes the backboard which supports a top with lighting built in. Two 30cm by 20cm LED panels were used, these giving a good spread of light whilst also being fantastic for photography.

The level of scenery above the riverbed was formed from a second sheet of foam, which was shaped using a knife and a wire brush. The basic landscape surface was achieved by making a paste from broken-up egg cartons mixed 50/50

with white wall filler. This paste was then applied with a spatula and shaped to give the appearance of rock.

### Scratch-built buildings

The three buildings forming the factory were scratch-built at low cost using a base of 2mm plasticard. Following the prototype, I started with the oldest building, the water mill. After marking the dimensions on a piece of plasticard and determining the window apertures, these were cut out with a sharp hobby knife. The walls were then joined together with superglue. The wooden slats were salvaged from a Chinese calendar, cut to size, with the sides slightly chamfered

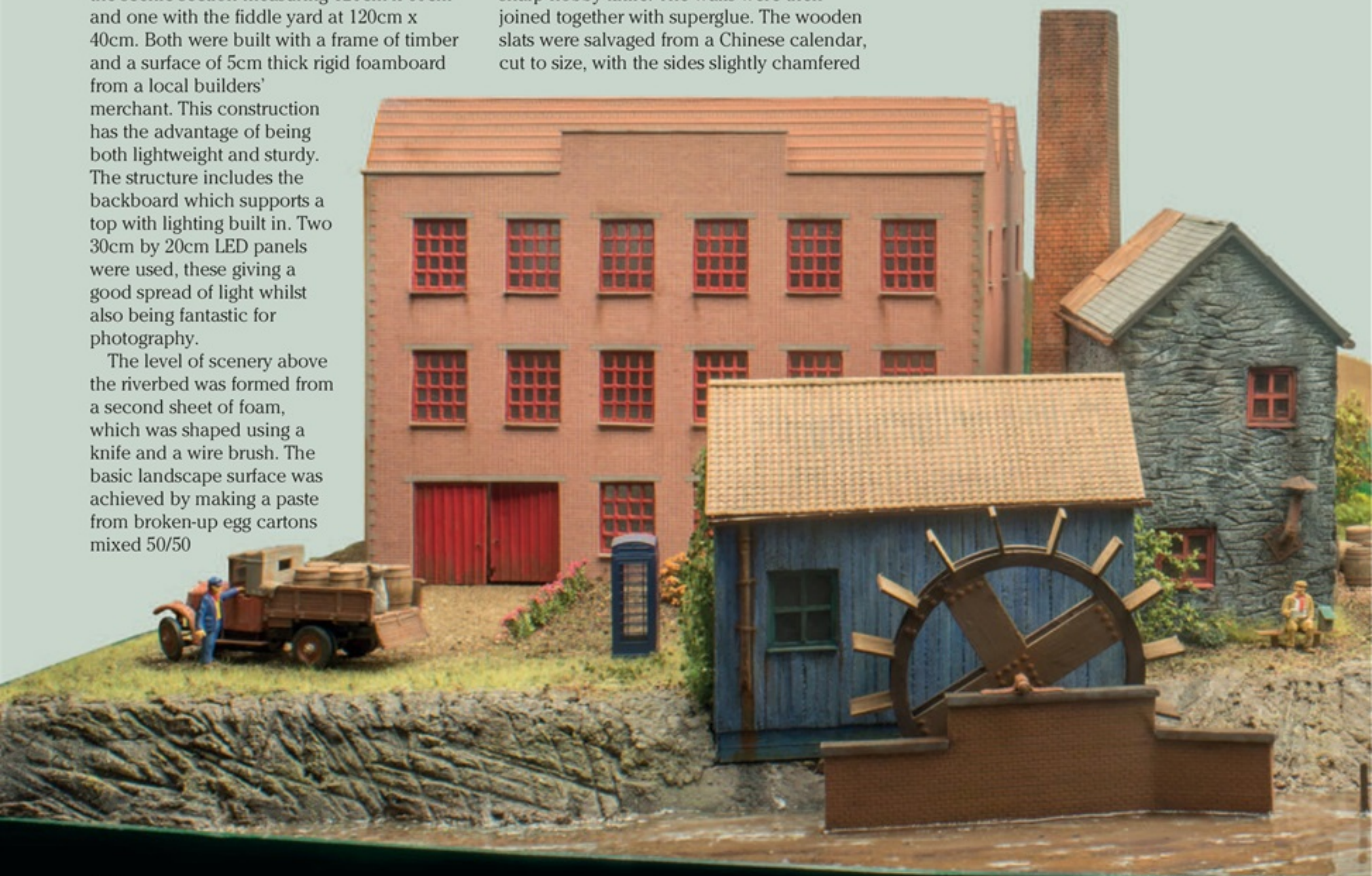
such that the grooves between the individual boards are visible after they are put in place.

The building was then coloured with different shades of blue and white. A sheet of self-adhesive roof tiles from Redutex was used for the roof, whilst the gutter was made from aluminium foil (recycled from a food container) and finished with bolthead detail. It was painted with a rust base colour before installation.

A piece of blister packaging was used for the window, onto which the frame was glued. The wooden strips for the frame were cut precisely to size using a NorthWest Short Line 'Chopper' tool.

The water wheel was drawn up using computer software with the parts then cut out using facilities available at a local FabLab (a small-scale workshop offering a range of personal digital fabrication services). The retaining wall in the water was not built until later, after the buildings were positioned.

The second and third buildings more or less followed the same construction method, but with different wall coverings. The second building was made from natural stone which was simulated by cladding the walls with air-drying clay and then scribing the individual pieces of natural stone after it dried. For the third building, brick was chosen: this was printed onto sheets of A3 paper and glued onto the already built base.





The timber engine shed was built in prototypical fashion with a framing structure covered with veneer pieces and is typical of buildings on narrow gauge and light railways all over the world.

## The engine shed

The final building is a small dilapidated loco shed. The framework for this was built from wooden beams which were then clad with pieces of veneer. The holes, rickety roof and peeling paint all add to the decaying atmosphere. Further finishing of the buildings was undertaken using items from the scrap box, after which everything was heavily weathered.

The loading platform has a base made from plasticard set with individual bricks by Juweela (a supplier of model bricks in different scales), supplemented with suitably coloured sand. The hand crane is a Peco kit (ref.LK-35) which was shortened to fit. Everything was given an aged look using a selection of rust paints and powders.

I find that scratch-building scenic items is a bit easier in 7mm scale, whilst the larger scale also allows more detail to be shown. Here for example, the oil barrels, pallets, sacks and bird house have all been made from scratch. The figures were purchased from UK suppliers. The lorry was found in a toy shop and weathered by my friend Samuel de Zutter in his inimitable style.

## Scenery

The riverbed was coloured with various shades of grey and brown. Pieces of stone were then glued in place here and there, and a small jetty was built – upon which a woman can be seen checking the lobster traps. Woodland Scenics Realistic Water was used, which levels itself after pouring, becomes clear, and hardens completely in 24 hours. Areas of 'white water' were then modelled with a paste applied with a brush, and later touched up with white paint.



The ground cover is from various brands, with different textures and colours used as required, these materials having been collected over the last few years, supplemented with dried used tea leaves. Dried sea foam flowers served as the basis for small shrubs, with some fibres and foliage added. To save time, a pair of commercial trees were also used.

## Operation

The factory has its own narrow gauge locos and stock for conveying material to and from an interchange with the local branch line.

Two trains alternate: one with empty wagons runs into the factory and a second comes out loaded with barrels. The change takes place at the back, in the fiddle yard, and is controlled by an Arduino micro-processor triggered by a few light sensors located in the track. The locomotives are brass body

kits fitted on Hornby mechanisms, whilst the wagons are OO gauge three-plank opens by Hornby, weathered to suit. The loads are barrels which I found at an exhibition in Sedan: with the right weathering, they are perfectly suited for the task.

## Conclusion

Some modellers think that a layout needs to be large to be satisfying, but even with a limited space, such as that specified for the mini-layout competition, it's surprising how much enjoyment can be had. *Innes Mill* was entered into the mini-layout competition at Modelspoor in October 2018 and was placed sixth. I hope that the finished model demonstrates that within the size limitations a very satisfying layout can be built. I found that the creative process was a pleasure in itself and has provided me with a lovely reminder of my visit to Scotland.

A panoramic view of the layout with the group of mill buildings on the left and the dilapidated engine shed on the right.





Through the Railway Room Door

# Elybridge

With his aged loft layout in need of significant repair work, **LANCE CADELL** took the opportunity to completely transform this extensive depiction of the Big Four era in OO, which is based on Great Eastern practice with the accent heavily swayed toward operation and correct signalling.

Photography by Steve Flint

Shortly after the war we were living in central London near Victoria Station where some of the Southern Railway's luxury services, which had been suspended upon the outbreak of hostilities, had only just been restored. I have fond memories of early mornings when my

brother took me to see the 'Night Ferry' arrive from Paris; a distinctive train with its SNCF baggage car and blue continental Wagon-Lits sleepers. On occasions we also saw the mid-morning arrival of the 'Brighton Belle'. I recall also the Station Master (wearing top hat) standing on the station concourse by the gate with the large 'Golden Arrow' emblem over the top and passengers arriving to board this prestigious Pullman service on the first leg of their journey to Paris. How could one ever forget!

A lifetime interest in model trains began with the gift one Christmas of a Trix-Twin train set on a board,

An ex-NER Raven Class H1 4-4-4T No.5129 pulls an Ipswich semi fast away from Platform 1 whilst LMS Johnson 3F 0-6-0 No.3709 departs Platform 5 for Bletchley with a stopping service (the horse box will be detached at Sandy). Piles of trunks on Platform 3 await the Southern special service for Southampton Ocean Terminal.



Lord Nelson 4-6-0 No.851 Sir Francis Drake gathers speed as it leaves LNER metals with the Southern special service carrying passengers for the RMS Queen Mary to Cherbourg and New York. This train is routed to Southampton Ocean Terminal via Oxford and Winchester.





which had to be stood against the lounge wall when not in use. Later, during the 1970s, the opportunity arose to build a decent sized layout. Inspired by articles by Peter Denny, work started on a layout based upon Ely in Cambridgeshire. However by 2010 following many years of use, wear on the original track, combined with modern finer wheels, resulted in running problems. The scenery was tired too: it was time to rebuild...

To broaden the operational potential of the original system, I chose to include the pre-war LMS operated Oxford to Cambridge route via Bletchley and Sandy. From Cambridge the GE mainline continues northwards towards Ely, the junction for Norwich, Kings Lynn and March. The fictional name of *Elybridge* fitted nicely.

### A salvage operation

I left both river crossings intact along with the original passing loops on the mainline beyond. Three main lines through the station were retained along with Platform 1. However, apart from this and a length of ballasted Peco track, little evidence of the old layout remained. Baseboards were cleaned down and resurfaced with cork sheet to provide a fresh surface to work on.

Track diagrams were drawn up showing two junctions, namely Elybridge North and Elybridge South, situated either side of an enlarged station. Both new junctions were then laid out using Peco code 75 trackwork to ascertain point motor positions. All of this was then permanently laid and connected to the remaining code 100 section. Point motors located beneath the baseboard were first mounted on a small ply frame wired to an attached screw terminal for simple fixing; once alignment of the frame is achieved, a single self-tap screw holds it in place.

Two new sub control panels were sited at chair height adjacent to the north and south junctions. With test running completed and the new junctions declared operational, surface point rod connections were then sealed with Vaseline, and the track painted and ballasted.

Gaugemaster 12V dc controllers and a high frequency track cleaner continue to give excellent service and are now into their fifth decade of use. I find that a major clean during the autumn and treatment with Track Magic will last throughout the following winter season.

### Developing the station

Elybridge station started to take shape with Platform 1 extended and two island platforms completed; all fabricated from jigsaw-cut plywood. Superquick station kits were adapted for the new platforms, whilst

**The heavy demand for coal means plenty of work taking place in the yard, with bagging up in progress for delivery to local homes. One of the coal piles actually covers a point motor and is removable. The station buildings are adapted Superquick kits, while the canopies are scratch-built. The footbridge is made up from a pair of Ratio kits to suit the location.**



▲ The ADM turntable is the main feature of the combined LNER/LMS depots. Newmarket is without turning facilities meaning that locos arriving there on race specials must be sent to Elybridge to be turned for their return journeys. Such is the case with the rare sighting of Gresley K3 2-6-0 No.1388, which is being turned for its journey north in the evening. Also on shed is LMS Patriot 4-6-0 No.5530 Sir Frank Ree and LNER J39 No.2980.

the entire station was wired for lighting. Canopies were made to lift off for access. A couple of Ratio footbridge kits (suitably adapted) connect the platforms. At this stage the off-scene mainline passing loops/storage roads were redesigned to accommodate more trains. A new fiddle yard was built between the LMS/ Newmarket routes.

### The motive power depot

The locomotive depot accommodates machines from both the LNER and LMS, with sheds and facilities for each company. However, both share the ADM powered turntable; an amazing piece of kit which is highly recommended. I found that the instructions were well written, with precise alignments easy to programme, resulting in a

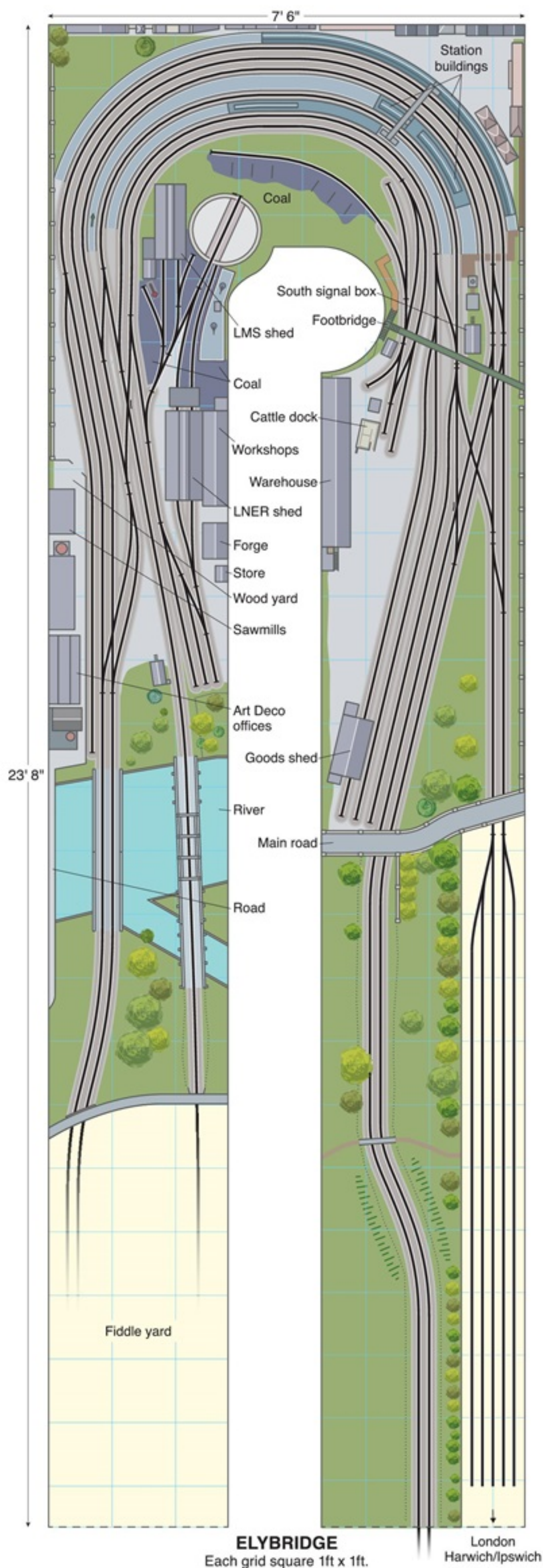
turntable that is a joy to operate.

More recently, quite by chance, I happened across Andy Pearce of Railmodel. Andy has been so helpful in producing the special laser etched MDF parts for the LNER shed and workshops. These are complemented in the goods yard with a reduced size model of the shed which existed at Gorleston in the 1930s, along with a lengthy warehouse which houses the lever frame. All of these buildings have greatly improved the overall appearance and atmosphere of the layout. York Modelmaking provided the laser etched parts for the long footbridge spanning many tracks at Elybridge South.

Siting a station on a tight 180° curve is naturally not a very common approach.







Transition curves were utilised to lead the trains through the less than generous radii, but the arrangement works very well. Incorporating many features to occupy attention along with the spacing out of buildings and canopy areas has all helped create a form of optical illusion. The result being that the radii appear much gentler because the track is also mostly hidden by platforms.

### Operation

The overhead main layout control panel, which hinges downwards for easy access, is not only a baseboard space saver but makes for a very good operating position. When the DCC Concepts lever frames became available all mainline point control was changed to use these. The levers are quite tall, which required a small area of baseboard to be cut away to accommodate the bank at a lower level.

The LMS/ Newmarket fiddle yard has a local panel and cab control which can be driven through *Elybridge*. Likewise, the two mainline cab controls can be driven through both branch lines. The locomotive depot has its own small panel and cab control.



▲ The main schematic control panel. The long warehouse was laser cut by Andy Pearce (of Rail Model laser cut buildings) and was designed to hold the lever frame whilst concealing it as part of the goods yard. The frames are set lower in a cut-out area due to their height. The open back and roof allows for easy operation of the levers.

### Refreshing the scenery

The scenic rebuilding meant creating more undulations. An inexpensive method was employed using card from old boxes and PVA. To increase the ground level where required, card supports were stapled or glued in place on top of the baseboard. More card was then glued over these supports and finally, small strips of newspaper were glued all over the surface in various directions, coated with PVA and left for a week to dry thoroughly. This resulted with a very strong and lightweight base that could be painted as required. For embankments, the process is reversed, with the trackbed supported on an open top frame.

Trees salvaged from the original layout have been supplemented by additions made from a variety of materials including natural twigs suitably cut to shape with sea foam and leaf scatter added. Hebe, being a small hardwood shrub makes excellent miniature tree trunks.

War World Scenics static grass has been used extensively on the layout. The same firm's small static applicator proved to be an indispensable tool for use in difficult to access areas of scenery or fine detailing.

### Locomotives and rolling stock

The loco fleet has been bolstered by many excellent ready-to-run items. Pacifics are employed to haul the heavier trains, although these were not a common sight on GER metals due to their weight. However, they do fulfil a very useful purpose. My earlier locomotives that date from the 1980s and are powered by Portescap motors were constructed by Peter Whittaker.



Early morning and the Inverness – London sleeper (diverted at Peterborough) runs through the north junction at speed en route to King's Cross, behind Gresley 4-6-2 No.4476. Pacific locos are a rare but welcome sight for early morning trainspotters. The travelling post office will have all the mail sorted for second London delivery. The first ex-Newmarket train of the day – hauled tender-first by Worsdell Class J15 No.5444 – is signalled into Platform 5.

A fairly comprehensive collection of passenger vehicles has been built up over the years, supplemented by some of the excellent ready-to-run items that are available. These are formed into fixed rakes that are close coupled using a hook and wire loop arrangement. It is on the agenda to

investigate the most suitable arrangement for vehicle couplings.

A method needs to be devised for close coupling of the coal train rakes, whilst many goods vehicles need attention of some sort, from weathering to adding or altering running numbers.







◀ Whilst the LNER claimed to have the most dining cars, other companies owned excellent catering vehicles. This ex-LSWR dining saloon forms part of the rake on the Southampton special service for first class passengers; third class dining is in the open third behind the kitchen (avoiding disturbance to first class). The rake has had all of the original bogies replaced for an improved ride.

I have supposed that the Southern Railway operates a special service on certain days to connect with the new *RMS Queen Mary*. This is routed from Southampton Docks via Oxford, thereby avoiding the need for passengers to change termini in London. This scenario provides an ideal reason to run a rake of rather nice LSWR coaches.

Military traffic also uses this route from the depot at Bicester, allowing munition trains a

**Approaching Elybridge North Junction, an LNER C1 Atlantic (a DJH kit with a Portescap motor) heads the 'North Country Continental Pullman' from Liverpool to Harwich, connecting with the LNER steamer to the Hook of Holland. Meanwhile a J15 heads an early afternoon train of mixed GC/GE vintage stock over the 'original' wooden bridge towards Newmarket. The tall chimney of the warehouse boiler dominates the skyline, whilst the trees have been trimmed in places and checked for safety due to their age and height.**

direct connection to GER metals. Race days at Newmarket also provide a reason for special traffic.

Incidentally, I make no apologies for the preponderance of Pullman cars that can be seen on the layout, these models rekindling my childhood memories of seeing and travelling on the 'Brighton Belle' with its extensive on board menu, ranging from poached egg on hot buttered toast at 1s6d (7½p) to sirloin steaks or halibut – all served by uniformed waiters at immaculate white damask clothed tables.

Fond memories indeed!

### Next time

Lance describes how he wired the two double junctions and provides an insight into the intricate signal gantries on the layout.







# RAILWAY MODELLING

## Explored



Projects, hints and tips for beginners from *Railway Modeller* readers and staff

**THIS MONTH** • Middleton • Weathering modern wagons • Curving plastic sheet

## Middleton

**ROY DOWDING** recounts the story behind his home based OO gauge layout, which has been created with the flexibility to depict different eras and places.

*Photography by the author*

The layout has its roots in 1983 as a 6' 6" x 3' 6" plywood door with 1" batten extensions down the long sides. This supported twin running tracks and some sidings with manual points, each of the tracks being controlled by separate analogue cased units.

The layout was originally fitted with legs to straddle the bed in a spare room, with the various items of stock kept in a separate dedicated box.

During the following years a few buildings (mostly printed card items from the Kellogg's Model Village and Waddingtons 'Pop Away' ranges) together with a Hornby engine shed, and other odd details, including an ornamental pond contributed by my daughter, began to appear.

An overall view of the layout showing the small extension at the top right. The layout makes full use of the many card structure kits that are available today.



An assortment of proprietary locomotives is used on the layout, including this Bachmann model of a Gresley V2 2-6-2.

### Expansion and a new support

The layout has survived two house moves, with the board gaining two chests of drawers and a pair of small cupboards as its means of support. It also gained a 4" extension at one end in readiness for the inclusion of a townscape and railway workshop.

In the most recent move, a spare bedroom has become known simply as 'the train room'. Taking advantage of an adjacent chimney breast, another extension 8" x 29" allowed space for an additional siding serving the





matching coaching and goods stock to recreate trains depicted in contemporary photos and as seen operating on the preserved lines.

### Rainy day revival

More recently I have had the time to rediscover all those kits, bits and pieces picked up at rail displays and exhibitions over the years that I had stored away for that elusive rainy day, and I began to formalise the landscaping of the layout at long last. Some of those early simple scenic additions have found their way onto the finished layout, albeit with some additional detailing. These have been augmented by several kits from

▲ Some extra space adjacent to the chimney breast allowed an extra section of baseboard to be added (see plan below). Note the adaptation of the Superquick engine shed kit visible at the rear of the scene.

engine shed and provided a means for rail access to the repair and maintenance shop.

### Classic named trains

As time passed, the trains operated on the layout were developed into reasonable representations of some well-known services including The Queen of Scots, The Elizabethan, The East Anglian, The Irish Mail, The Scotch Goods and so on, up to the present with railtours including The Northern Belle and West Coast Railways steam excursions.

In addition, much fun has also been derived from mixing and

Amongst the buildings on the layout is this structure incorporating an old carriage body. The model was inspired by...



▲ ...this example, which was photographed by the author at Darsham, Suffolk in 2006.

the Superquick and Builder Plus ranges that I'd had stored away for the last 30 years.

### Adapting the buildings

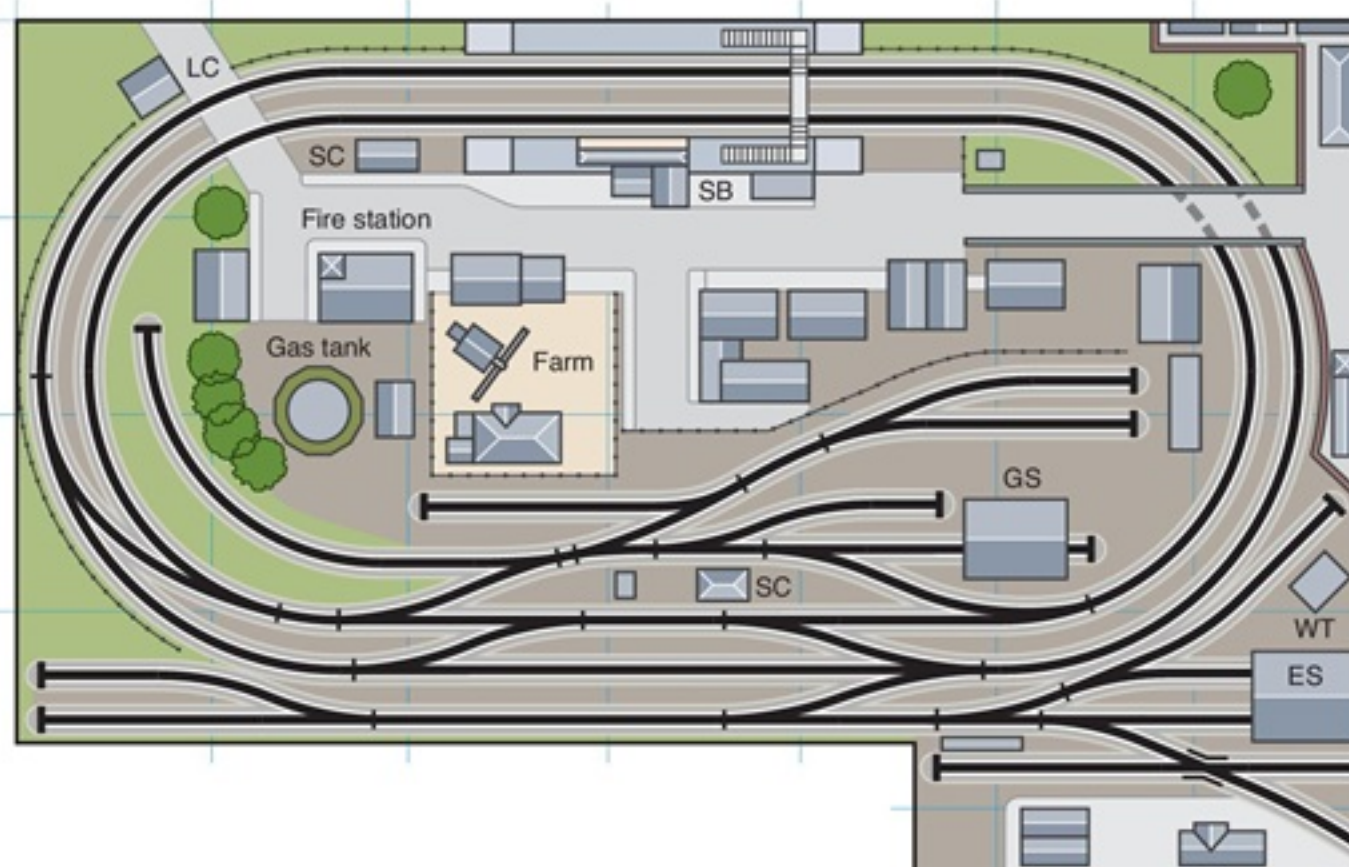
The track had originally been laid to maximise running and shunting operations, but with no overall grand design in mind for authenticity or the scenery. In a few instances, this has meant reducing the dimensions of one or two buildings in order to fit them into the available space.

Several of the ubiquitous Airfix fencing and gate accessory packs have been utilised, while the components of a Hornby footbridge kit have proved extremely useful in various locations. Some kits have been adapted to provide alternative buildings, a prime example being the Superquick two-road engine shed that is now a low relief structure influenced by the former long shop of Richard Garrett & Sons at Leiston in Suffolk. This two-storey former works building is now a museum. This kit also provided the material to make the primitive methodist chapel, based on the example in nearby Yoxford.

One or two buildings are scratch-built, notably one which incorporates a bisected Hornby four-wheel coach body. This was inspired by railway cottages that once stood near Darsham station on the East Suffolk Line, which comprised of a pair of ex-GER six-wheel carriages under a single roof, to form a bungalow. And therein lies the recurring theme of the layout - the intention to capture

## MIDDLETON

Main baseboard dimensions: 6' 10" x 3' 8". Additional board: 2' 5" x 8"  
Each grid square = 12" x 12".





some of the interesting aspects of local rural life that have already vanished or are threatened. Other examples abound – the windmill, cereal and provender merchant, the country station, crossing keeper's cottage, coal merchant, goods yard and the cattle dock.

### The gas works

Some aspects have their own backstory, like the gasometer. It is intended to represent a surviving relic of a former small-scale gasworks, which has been retained to serve as a pressure-regulator for the feed from a larger nearby town supply and latterly piped natural gas. Part of the land thereby released provided space for the police station and fire station to be built, while the remainder has become overgrown. The coal sidings, now truncated, survive – one utilised by the corn chandler and the other as a head shunt for the goods yard and pick-up goods. Nearly all the structures are made as modules to enable them to be lifted off for ease of moving the layout in the future.

### Scenic work

Ground cover has been created with whatever was to hand: this included proprietary mini grass mats, dark green nylon scouring pads to mimic gorse or coarse vegetation and sandpaper for shingle. Thin grey exhibition carpeting was used to simulate track ballast, whilst old floor tiles represent the cobbles in the goods yard and elsewhere.

Using these materials in this manner has meant that some borders are perhaps too sharp and overall the finish is too clean and tidy, but it was far quicker and less messy than using scatter materials.

### Any time, any place

At the heart of the layout is Middleton station, this name being chosen because it's one of the most common place names in the UK; it could therefore be anywhere, allowing trains from any region or era, from 1920 to 2020 to be run. To this end, the background colour of the station signs is a neutral white. A change of era does, however, require some chopping



▲ The raised roadway over the railway lines connects the high-level town with the lower-level village scene in the centre of the layout.

Sectional track was used, fixed to an underlay of thin grey exhibition carpeting to give the appearance of ballast.



▼ The road that runs through the village scene crosses the tracks via a bridge at one end by way of this gated level crossing at the other. Note the fire station and service vehicles.



and changing of age-appropriate road vehicles and attention to the level, and kind, of activities in the goods yard. For example, if running a present-day set-up, the former goods yard becomes a car park, with the many displaced vehicles from the 1930s – 1960s period arranged to form a classic car show.

### Conclusions

The realisation of the layout has proven to be a most rewarding and engrossing process. After so many years presented with an expanse of sparsely populated brown plywood with imaginary roads, gardens and back yards etc. it is satisfying to see the original board now properly dressed with appropriate scenic surroundings for the trains to run through.



## Adding realism to modern wagons in OO

**CALLUM WILLCOX** demonstrates how he gave this rake of Foster Yeoman bogie tippler wagons a convincing work-worn finish without using an airbrush, whilst at the same time adding suitable loads.



▲ The wagons in their original 'pristine' out-of-the-box condition, prior to weathering.

Photography by the author

The recent release by Accurascale of PTA/JTA/JUA bogie tippler wagons in OO (see reviews RM March 2021) has further expanded this manufacturer's growing range of freight rolling stock for the post-steam era modeller.

351 examples of these wagons were constructed by Redpath Dorman Long in three batches between 1972 and 1978, for transporting iron ore to British Steel facilities at Consett, Llanwern and Ravenscraig. With

the decline in ore traffic during 1980, many of the wagons took on new roles, with a number purchased by Foster Yeoman Limited, based near Frome, Somerset, for working Mendip stone trains from the company quarries at Merehead, Whatley and Tytherington.

The Accurascale models are available in a range of colourful liveries that are crying out for some judicious weathering to simulate the work-worn appearance of their full-size counterparts. In this step-by-step guide,

I demonstrate how I gave these models a realistic finish – together with suitable loads – utilising just a handful of basic tools and materials, including a selection of new weathering pencils.

### Weathering the models and adding loads

**1** A set of AK Interactive weathering pencils were used to replicate the various scratches and marks on the wagon bodies. Intended primarily for military modellers, these water-based pencils contain a core of semi-grease paint. I found the Earth Brown pencil from the firm's Dirt & Marks set to be a close match to that of rust. Using images of the real things as a guide, each mark was drawn individually onto the body (photo A), with the depth and tone of these marks determined by how sharp the point of the pencil was and how much pressure was applied. I found that for the best results a sharp point was key. Next a cotton bud (lightly moistened with water) was rubbed over each mark in a downward motion (photo B) to simulate the effect of rust bleeding onto the paintwork below. Care needed to be taken not to rub too hard and remove the rust effect completely. Once happy with the effect, each wagon was sealed with a light misting of Humbrol Matt Varnish spray, to protect the rust finish during subsequent handling (photo C).



**2** Due to the nature of their work the interiors of these wagons (as with all other open wagons of metal construction) didn't remain pristine for long. To replicate this wear and tear, first a 50/50 mix of Humbrol Matt Cote and Enamel Thinners was applied to the interior panels. Small quantities of Rust and Dark Earth Weathering Powders were then stippled on top using a stiff brush – the two applications blending together on the surface, forming a varied and realistic rust effect (photo A). Once this had dried fully, further powders were dry-brushed onto the surfaces (photo B); these were applied in a downward motion for the sides and side to side for the floor panels, before once again being sealed with a misting of matt varnish.







**3** The wheels were weathered by utilising the left over varnish/thinners mixture from the previous step. This was combined with a small quantity of Dark Earth weathering powders to form a wash, which was painted directly onto the wheel surfaces and left to dry completely.

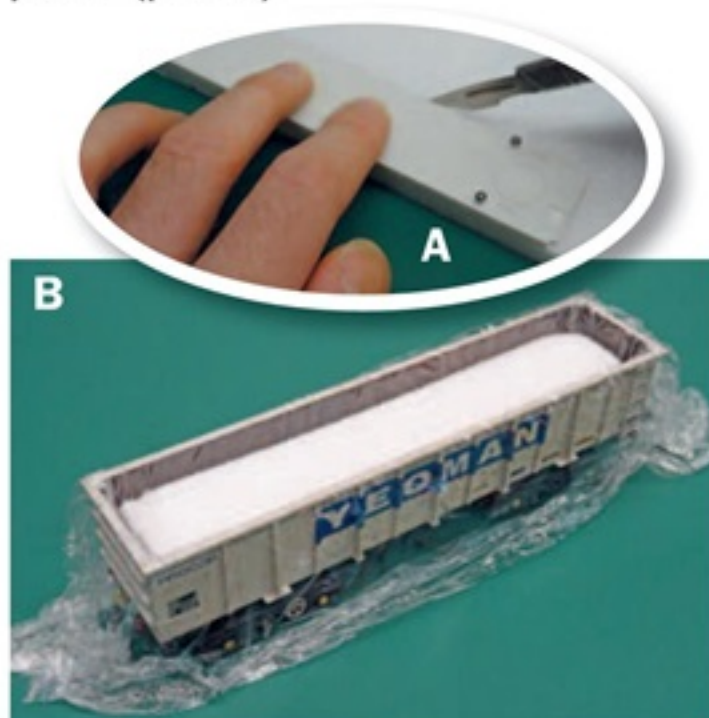


**4** The underframes and bogies were toned down with a light dusting of Black Humbrol weathering powders, this being worked into the various areas of detail, with certain areas highlighted with Dark Earth powders. The effect was once again sealed with a quick mist of Humbrol Matt Varnish.

**5** The build-up of aggregate dust that is synonymous with these vehicles was replicated using WWScenics Concrete Dust weathering powder. A generous quantity of powder was applied to each panel and then dragged in a downward motion towards and over the underframes and bogies (photo A). This encouraged the excess powders to congregate on horizontal surfaces (such as around the bases of the struts and above the bogies) as per the prototype. The process was then repeated for the tippler interiors (photo B), working the powders into the joins and corners. Once complete, the wagons received one final pass of Matt varnish.



**6** For the wagon loads I opted to fabricate these as removable inserts. Using the separate floor panel from one of the models as a template, the bases for the load inserts were cut from some spare polystyrene packaging (photo A). Other materials such as thick card are also suitable. Additional layers were added and then sanded to help give the load some height and shape. Once complete, each wagon interior was then lined with a cling film and the inserts pressed carefully into position (photo B).



**7** Next the load was added. I found Peco PS-315 Fine Weathered Brown Stone Ballast to be a reasonable match to the aggregate conveyed in the real wagons. After covering the base insert with a thin layer of PVA, the ballast granules were sprinkled over the top until covered (photo A), followed by a solution of 50/50 PVA and water applied using a pipette (photo B). Further ballast can be added at this stage to give the load a bit more shape, if desired, before being left to dry. Once cured, the finished loads were removed and the cling film peeled off (photo C).



**8** The completed rake. Finishing touches included a subtle build up of grime around the buffer heads.



## Supplier information

**AK Interactive**  
www.ak-interactive.com

**Humbrol**  
www.humbrol.com

**WWScenics**  
www.wwscenics.com

**Accurascale**  
www.accurascale.co.uk



## Curving plastic sheet

**STEVE FLINT** describes a method of making curved roofs for 4mm coaches and vans from 30thou plasticard.

I recently revisited a modelling project that I started in the late 1970s but which I set aside when other priorities and interests took over. The desire had been to create a branch terminus located in North Eastern Railway territory in BR steam days. Some rolling stock kits were gathered together, but the plan stalled and the collection of half-built items were stored away for the future.

The present impetus for dusting down the scheme came about with the anticipation of some new ready-to-run models in OO that would fit neatly with the concept of a terminus station in the north east; notably the forthcoming NER petrol autocar from Rails of Sheffield and the NER Class O/LNER/BR 0-4-4T Class G5 from TMC.

Hence, out of long-term storage came my pair of original Ian Kirk Gresley suburban coach kits. One kit, an all third, was almost complete, the other, a brake third still had some way to go, being devoid of underframe details, bogies, paint and a roof.

In the interim years of course, Hornby has produced excellent ready-to-run models of the Gresley suburbans, so I could have binned these old half-built kits and bought a set, but to finish off my own efforts, even at this late stage, does seem to be much more fulfilling.

As my kits from the Kirk stable were from a very early production batch, they were only provided with a sheet of flat plasticard from which the modeller had to curve his or her own roof, and then permanently glue it to the bodywork, using one's fingers as temporary clamps and/or lots of masking tape, so as to ensure the correct elliptical shape.

This time, prior to any fixing in place, I wanted to find a method of pre-curving the plastic sheet to as near to the correct profile as I could and it turned out that an offcut of domestic 40mm o/d sink waste pipe I had stashed in the garage provided a near match to the 4mm scale roof profile.

Using a hacksaw, two equal lengths of pipe were cut, slightly longer than the required roof so that about an inch or so would overhang at each end. The first length was to be the former around which the 30thou plasticard would be curved. The second length had a lengthways section removed to provide an outer top clamp that clips over the former and retains the plasticard in place during the heating stage which, after cooling, will set a permanent curve into the sheet. Some cable

ties were also wrapped around the top clamp to add additional restraint (see photo below).

### The heating stage

This is not as daunting as it sounds but does involve near boiling water on the kitchen cooker hob – nothing more dangerous though than boiling a pan of spuds, but do take care.

The entire 'moulding tool' complete with its plasticard insert is immersed in water in a large pan and brought up to a temperature of about 95°C. (You will need a large roasting tin across two rings if the required roof is particularly long!). Don't boil the water, or leave the mould in the hot pan for too long, as excess heat will deform the edges of the thin plasticard (the waste pipe is of course resilient to very hot water and does not deform).



▲ The component parts of the home made 'moulding tool' from left; the slip on cable ties; the split length of pipe to act as an outer top clamp; the inner pipe former around which the plasticard is held; a curved piece of plasticard fresh out of the mould after cooling.



▲ The mould assembled with the plasticard inserted, all ready for the heating stage.

Photography by the author



Just before the water boils, (if you have a thermometer, use that to test the water temperature) turn off the heat and, using tongs, transfer the mould quickly to a sink full of cold water and allow everything to cool. You can then slide off the cable ties and unclip or slide off the outer pipe to reveal a curved roof of almost the correct profile.

The method is also useful for goods van roofs, and possibly for other curved sections such as loco wheel splashers and boilers. It may also be readily adaptable to other scales, though I've yet to experiment further.

◀ The curved 30thou plasticard roof ready for fitting to the brake coach and detailing with vents and rain strips. There is sufficient flex in the plasticard to ensure a snug fit all round.



# Comment

## Observations and opinions on railway modelling

**Richard Bardsley**



### Always read the instructions!

“Remember a sketch by the comedian Eddie Izzard where he is trying to get his computer to print a paragraph of text. He tries everything and gets increasingly frustrated as it refuses to print. Eventually he calls a helpline and that's when you get the punch line – there's an 'on' button on the printer which he'd missed. If only Eddie had read the instructions...

Do you read the instructions when you are building a kit? I mean, really read them, thoroughly, line by line before you take any parts off the sprues or etches? Me neither. The arrival of a new kit brings with it equal amounts of pleasure and excitement for me. I open the packet and examine the parts and have a lot of fun trying to figure out how it all fits together. I know what the result should be because that's why I bought the kit. So how difficult can it be? Maybe if there is some unidentifiable part, I'll skim the instructions to see what it is and where it goes, but I do tend to just jump in and start sticking things together.

Of course, some kits are simple. I've made enough N gauge wagon kits of opens and vans that I can probably assemble them with my eyes shut. Yet even with simple open wagons it is possible to stick the sides on upside down. Some kits are more complex,

or they are for something I've never made before. So, I must resort to the instructions from time to time and that's when the trouble begins.

There's a trend now with some of the cottage industry kit makers not to include instructions in the kits; they require downloading off the internet. Most of us are connected to the web these days, but it's still a bit of an inconvenience. When you finally get the instructions printed, they can be somewhat disappointing.

I recently made a kit where the instructions were less than helpful. A series of photographs, and that was it. No text, no part numbers, no advice on which way round to orientate some of the components. So often it seems to be the case that the instructions are an afterthought. Why spoil the enjoyment of the build process, even the modeller's finished result, by skimping on the instructions?

What makes a good set of instructions? The writer should approach them like they've never made any kits before. Imagine that you are just starting out in the hobby. We want to encourage the novice modeller and guide them. Otherwise, if the instructions don't help them, they might give up. Today's novice is tomorrow's future for the hobby, and despite the plethora of help available in online forums and model clubs, not everyone will be able to avail themselves of such resources.

I've made a lot of kits over the years, and I've designed a few kits myself. I'm now the kit production officer for the N Gauge Society, responsible for the packing of over

70 different kits, including the instructions. It's a mammoth task helped by many volunteers and editing all the instructions and bringing them up to scratch is an ongoing process. Some of these instructions run to three or four sides of A4. However, the text is legible – there's no need for a magnifying glass just to read the text crammed onto a sheet of A5 – and we try to include diagrams. Furthermore, parts are

numbered and referred to by the number in the text, whilst the numbered parts list corresponds to an annotated image of the sprue.

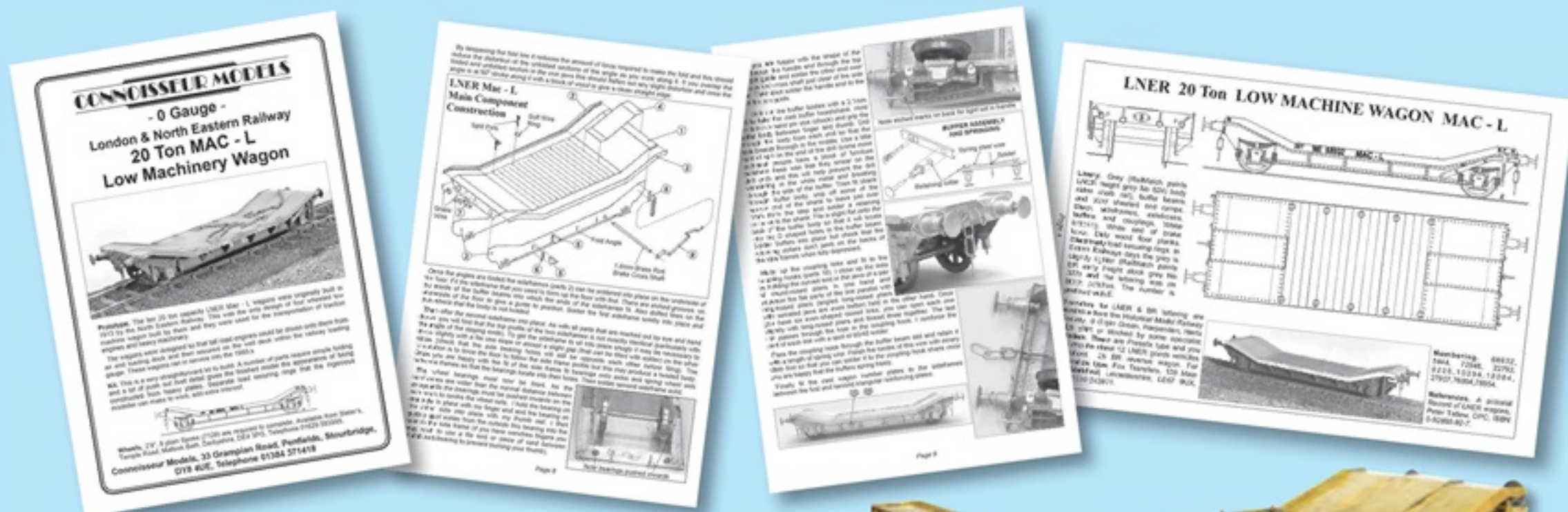
I think it is also helpful to break the construction down into sections like 'chassis' and 'body', and then numbered points within those sections to

guide the modeller step by step. It might seem like overkill if you are an experienced modeller, but someone out there will appreciate it.

A bit of figuring out of what goes where in a kit is part of the fun, but my plea to kit manufacturers is to round off the product with a decent set of instructions. We don't want to be scratching our heads for too long, or worse still, assemble the model incorrectly.

To modellers, I would say, there's no shame in consulting the instructions. They're there to help you get the best out of your purchase. Despite the temptation to just get stuck in... always read the instructions first!

**Richard Bardsley** models in various scales and is currently the Kit Production Officer for the N Gauge Society.



▲ The Connoisseur Models range of etched kits designed and manufactured by Jim McGeown for 7mm scale are supplied with clear and comprehensive illustrated instructions that guide the modeller through the build step-by-step, whilst providing additional information and advice to cater for those of lesser experience. The construction of this Lowmac L kit was described in the May 2020 issue.



# Readers' Letters

## Our Edge Hey and Strines reunion

Extending our *Edge Hey and Strines Railway* (See May 2020 *The tracks of our years*) became my lockdown project when the pandemic struck last year.

It is a layout which I built for, and with, my grandchildren, and in the process of building it, I came to appreciate my love of making realistic scenery.

When the lockdown restrictions took hold in March 2020, like so many other families, I could no longer have the grandchildren round to visit and do work on the railway, so, early on I decided to make and send videos to them showing the progress I made on the layout. I did this on a weekly basis and ended up sending them 52 videos in total!

I continued building and modelling throughout the year and managed to largely finish the layout in time for the first easing of restrictions in May 2021 which meant that the grandchildren could come and visit at last, and see the layout again.

At least, if not fully finished, everything was in place and all the trains were running! These photos record the first opportunities for the grandchildren to return to the railway.

Being able to let them use the layout in person has made all the effort and time worthwhile; and they absolutely loved it. Thank goodness!

CAROL FLAVIN



Josh and James turned out to be my first model railway enthusiasts!  
All photos: Carol Flavin

Left: Jody and I in front of the layout.  
Below: Finn standing in the pop-up hole where the removable section of Edge Hey village normally sits.



## Stuart K Baker OBE

I was disappointed by the critical comments contained in the review of the latest (15th) edition of Stuart K Baker's *Rail Atlas, Great Britain and Ireland* in the August issue. I assume that your reviewer was unaware that the latest volume was produced while Stuart was struggling with his health following a major stroke some years ago. Sadly Stuart experienced a major heart attack last October and subsequently died on 3 November 2020, aged 66.

He was both a railway professional and a railway enthusiast in the true sense of that word, and perhaps his greatest achievement was to win over some British Rail senior managers who were openly dismissive about an Oxford graduate producing 'an enthusiast's book'. History will record that the Baker

Atlas became an indispensable tool in the railway industry as well. Whether it will continue under another author is uncertain, and furthermore, that the 15th edition was produced at all, is quite remarkable in the circumstances.

PETER WHATLEY

[Ed. We had not heard of the passing of Mr Baker late last year. The book was received from the publisher during 2020, but because of our revised workflow practices under the pandemic regulations, some book reviews were set aside and thus delayed.

Naturally, had we known about Mr Baker's death we would have referred to it in the review and, accordingly, expressed our condolences to his family. Thanks also to Michael T Leahy for bringing this sad news to our attention.]

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

## Fare thee well, Tim and Derek

It was saddening to learn of the retirement of two gentlemen who have been associated with RM for such a long time, but I am pleased that you have given them much deserved valedictory notices.

Tim Rayner has made a considerable contribution to RM over many years, and his input will be much missed. The longevity of the RM's editorial team must surely be unique in the publishing world and is, I believe, a major factor in the consistent quality of the magazine.

With regards to Derek Shore I first heard of him when his *Avondale, Creston and Waterfoot* layout was published in 1970: I thought it to be an excellent layout, and largely used ready-to-run stock which made it achievable by the 'average modeller' to quote RM's masthead of the time. When the Shipley Club built its *Evercreech Junction* layout I made the trip from East Anglia to Shipley to see it, and subsequently visited their exhibition for several years afterwards to see the club's later layout *Tebay* in operation. Derek was always recognisable behind the layout, and one felt that one knew him from his many articles describing the construction and running of it.

MARK DEARMAN

Please pass on my best wishes to Tim on his retirement. I always found him to

be a real gent when dealing with him in relation to articles that I submitted to RAILWAY MODELLER.

I once asked him if he knew which issue a particular drawing was in so that I could get a back copy, and by return of post he sent me a photocopy of the drawing – that really is tremendous support for the model railway hobby and its adherents.

I'm sure Tim will enjoy the time to do some modelling for himself.

RICHARD BARDSLEY

I see from the latest RM that your production editor Tim Rayner is taking early retirement. Would you please pass on my best wishes to him through the pages of RM for a long, happy and healthy retirement!

KEITH WHITE

Could you pass on my good wishes to Tim Rayner on the announcement of his early retirement. He was very much a part of the fixture and fittings at Peco HQ. I'm sure he will be missed by the team at Peco.

TERRY ROBINSON

[Ed. Thanks also to all the readers and well wishers who have passed on their regards to both Tim and Derek, verbally via members of the editorial team.]

## Nostalgia for steam and diesel

The July issue Comment *Should we change with the times?* struck a poignant and emotional chord.

As a young boy in the late '60s and early '70s I recall my father trying to hide his sadness at my obsession with blue diesels. He'd tell me of his fine journeys to and from Liverpool on magnificent LMS expresses before the war, but I wanted to have models of what I could see, and back then it was the 'modern image' as it is known.

In actuality, where I grew up, we were overrun with EMUs, with occasional 37s or 31s to pine for, but those early diesels made such an impression on me that I resolved to move somewhere with 'better trains' when I got older. One

thing you couldn't really do back then was go and see a steam engine.

Not too long after that I unexpectedly lost my father, so focused on my studies, growing up, getting a job and putting my childhood hobby on the back burner.

Decades later, when my own son was born, he was inundated with Thomas toys for his first birthday, but didn't know what to do with them, so, having in fact by then moved somewhere with those 'better trains', I took him to Didcot shed to see a real 'Thomas'. It had a profound effect on him, and seeing his reaction, it had a profound effect on me too, as did the other hot, hissing, wonderful-smelling machines which shake the earth beneath you as they move – they are such visceral entities.

A *Timewatch* programme on the last days of steam broadcast in 2008 finally allowed me to see what such trains meant to the generations before me and gave me back a connection to my father.

Over the years, the diesels I've most loved on my travels, from the Class 50s to the SD40s of the Canadian Pacific Railroad have passed into decline or preservation, only to be replaced by something rather sterile. As I approach retirement I feel I have in my soul, a model railway calling out to be built, and it will be a transition era layout from the '60s with both steam and diesel, and set somewhere which existed before Beeching's report axed so many lesser lines and routes.

IAIN ALEXANDER



Peter Osborne's *Cleckhuddersfax* (RM Aug 2013) embraced both steam and diesel traction set in West Yorkshire in the transition years. Photo Steve Flint



## In Ian Beattie's defence... Further evidence your honour

I write not exactly in defence of the late Ian Beattie with regard to the criticism of the A4 drawing as reproduced in the April edition, but in appreciation of the difficulties in preparing a drawing.

Ian's A4 drawing was certainly not among his best, and Tony Wright raised three principal areas of concern with the drawing in the June issue letters; the proportions of the tender, the cab roof shape, and the streamlined fairing between and around the front buffers.

As regards the width of tenders, Tony is correct that only the corridor tenders were to the full width, but in mitigation, the non-corridor tenders seem much less evident in photographs than their numbers would suggest: in fact I consulted several reference books without seeing any at all. As Tony points out, the tender body is narrower than the cab, but in the typical photograph this is not easy to spot, though in *Gresley Locomotives* by Brian Haresnape there is a near head-on shot of Class P2 No.2001 *Cock of the North* showing it clearly. However, there might be cause for confusion in that the 1936 fully streamlined batch of P2s had full width tenders, with a cut-away buffer beam, but I have not been able to determine whether these were actually corridor tenders: but if not, we have a prototype for a wide body non-corridor tender as drawn by Ian. Can Tony clarify?

Regarding the cab roof, Tony, as did C J Allen in *British Pacific Locomotives*, describes the A4s as having a V-fronted

cab. With respect, I have to disagree. The Class V2 2-6-2s had a very similar cab with a V-front (as did the P2s after rebuilding as Pacifics); but on the A4 the cab front plates (the term 'spectacle plate' hardly seems appropriate) are angled at 45° but stop when they meet the sides of the streamlined cladding – as such there is no 'front' to the cab. In fact the cladding, which is unusually high to clear the banjo steam collector, is above cab roof level, and the cab roof itself has a slight but noticeable bulge above the firebox. On the V2s the front of the cab roof is formed from the top plate over the safety valves, whereas on the A4s I believe the safety valve cover plate is actually part of the boiler cladding and not the cab roof structure. This is a very difficult question to answer from photographs, as almost all shots are by photographers situated below the level of the boiler top and the area in question is never visible. Even those rare pictures taken from a footbridge or the top of a cutting are usually not clear enough.

I can record however that a photo of No.60027 *Merlin* in *The Gresley Pacifics* by O S Knock shows the cover plate to be quite obviously square at the front: a close match to Ian's drawing. It should be emphasised that this only applies to one locomotive at one time (other engines at other times might have varied) and I have no evidence of what was usual in 1938, except that the Hornby Dublo model agrees with Ian's plan!

Tony also has reason to criticise Ian's handling of the front end. It is obvious from any photograph that the shape of the aerofoil curve above the cylinders drops down more sharply to meet the buffer shanks than Ian had drawn it, and the horizontal curve of the fairing over the buffer bases was far rounder. But to draw the front between the buffers as a slight curve in plan view is only correct for the first four locomotives, and only for the first few years. This is because, following an accident in which a man was crushed, the buffer shanks were extended and the 'belly' in the plating between the buffers was flattened out and the cut-out for the front coupling eliminated.

This modification to all A4 class members had been completed by late 1936, so, no double chimney locomotive (from 1938) would have had a curved plate between the buffers. This feature is obvious in photographs, for instance, an engine number painted across the nose would show some distortion if the plate was bellied outward. In consequence a drawing of a double-chimney A4 should always show a straight front in plan view.

In conclusion, we might remember that many if not all of Ian's drawings were produced between the end of steam on BR and the establishment of the archive service at York Railway Museum. This means that he had to work only from published dimensions and photographs.

Even when an example of the class such as the A4 was preserved, it does not follow that he was able to climb all over it with a tape measure and camera, and, as drones with remote control cameras and scanning technology had not then been invented, he could not readily get views from above, even if the locomotive was accessible at ground level.

I do not claim any special knowledge of Gresley locomotives. My sources are photographs in common and cheap reference books – the sort that many enthusiasts will possess.

Finally, may I ask the readers of *RAILWAY MODELLER* if anyone has definite knowledge of how the LNWR painted the inside of its locomotive mainframes?

I have read three 'authoritative' accounts, and they are all different!

ALAN COX

◀ No.4468 *Mallard* at King's Cross in 1939 showing the longer buffer shanks as used on the A4s from 1936 and what appears to be a flat front plate between them. Photo Mike Morant



## Happy memories stirred by 'That issue'

What a fantastic season it has been for reminiscing, thanks to several recent articles in *RAILWAY MODELLER*.

The August 1980 edition was the first copy of the magazine that I ever purchased, and Paul Marshall-Potter's recollection of it in the May issue took me straight back to the very day. I'd missed a connection at Preston station and needed something to read. I had no idea of the extent of the model railway hobby and upon opening the pages I was totally intrigued by the scale and grandeur of *Cheslyn Bay* and the detail on Ian Futers' *Lochside*: to my eyes these layouts were amazing.

Two years later, and by then married,

we had a holiday along the Dawlish coast. I'd never been there before, but it all felt so familiar: and then I remembered why. Hence, on our return home I built a 14' x 3' layout based on *Cheslyn Bay*. It was far too big for a beginner, but I enjoyed the process and had it part completed. With a baby on the way, the layout didn't last and the equipment was confined to the attic.

In 2008 I had a long lay-off from work due to illness and needed something to keep me occupied. During a visit to Oyston Mill in Preston I came across some back copies of RM for sale – and guess what? The first one I picked up was the August 1980 issue: a very

pleasant surprise. My interest was stirred again, and I began a small layout which helped me through those times.

Back to the present, and you can imagine my elation, when following on from Mr Marshall-Potter's Comment page in May, Chris Morris' *Dawlish Warren* appeared in the June issue, closely followed in the July issue by another Scottish based harbourside scheme proposed by Ian Futers, and referencing his original *Lochside* layouts in both 4mm and 7mm.

It has been great reminiscing about the very RM issue that got me interested in this wonderful hobby.

KEV PANTHER

## Snippets...

### Metal kits and superglue

I'm having thoughts about building white metal locomotive kits using superglue and would like to enquire if anyone has any information or advice regarding the use of this type of adhesive, as my soldering skills leave much to be desired.

Does superglue last indefinitely, or will a model constructed with it eventually fall apart?

MICHAEL LINES

### Not enough 9F nines

Thanks for sending me a copy of the August *RAILWAY MODELLER*, which I'm gratified to say, again has done a great presentation job with my *Fence Houses* story, many thanks.

One point though, and depending if you plan to use anymore photos of my BR 9F loco number 92099... It is not 92009 as stated in the caption on page 566!

I did check my typed captions for this photo (No.20 and also No.11) and I do give the correct 92099 in both cases. You never know who is watching, and I hope there are not any 'letters to the editor' over this, but at least it can be corrected for next time! Best wishes.

BOB JONES



▲ An errant typo in the August issue resulted in the wrong number being captioned for Bob's model of 9F No.92099 – seen crossing his model of Victoria viaduct. Photo Steve Flint

### More Cordon gas tank advice

In response to the appeal for information on GWR Cordon Gas tanks by Bill Gabb, in a recent issue. I can recall a series of comprehensive articles in the *Wild Swan* periodical *Great Western Journal* which I am sure would offer answers to Bill's questions. It was a number of years ago now and I don't have details myself, I'm afraid, but a search in the *Wild Swan* archives may be worthwhile.

BILL FAULKNER

### What no steam?

The still images of the Gliddon's garden line in the August issue failed to portray the live steam effect, but the video on the Peco website (link: <https://youtu.be/cVYPXWgUV5A>) captures it perfectly!

JOHN BOYSON



FROM **RAILWAY MODELLER** BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

## Lynton & Barnstaple 2-4-2T *Lyn* Brand new from Heljan in 009

We were delighted to receive a sample of the long-awaited new model from Heljan of the 2-4-2T supplied by the American firm Baldwin in 1898 to the 2' gauge Lynton & Barnstaple Railway. Plans for the model were first announced in the news pages of our January 2019 issue, this following the various iterations of L&B Manning Wardle 2-6-2T released previously by Heljan (see Reviews, RM April 2017).

No fewer than five versions of the Baldwin are available, in different liveries with appropriate period detail. These comprise ref.9980 – ex-works black, un-numbered; re.9981 – L&BR dark green, pre-1906; ref.9982 – L&BR dark green, 1906-22; ref.9983 – dark green numbered SR E762 on plates, 1923-29; ref.9984 – Southern Maunsell green, 1929-32; ref.9985 – Southern Maunsell green, 1932-35 (with steam heat pipe).

We illustrate the final variant here, and plan to show the others in the next issue.

Detail differences between the aforementioned versions include – but may not be limited to – the smokebox door (round plate or handles), smokebox (with/without ash clearance holes), whistle (mounted on the cab front or cab roof), safety valves on dome (two styles), coal rails on bunker, chimney (capped or stovepipe pattern) and steam heat pipe.



### Our sample in detail

The new Heljan 4mm model closely matches published dimensions and scale drawings.

The main colours are dense and even, and the fine lining is well done. Various details are picked out in appropriate colours.

The boiler and tanks and the main chassis block (with pilot deck and cab floor) are metal castings, while the cab, smokebox, and many small detail parts are plastic mouldings. Pipework and rivets are all present and correct, with the ejector pipe along the top of the locomotive.

There is a slender operating rod to the side of the sand domes on top of the boiler, and the fine lubricating feeds to the top of the valve chests have not been forgotten.

The model is fitted with a headlamp of the correct pattern, illuminated according to direction by an LED.







At the other end, fine coal rails surround the bunker with a representation of the coal within. The windows are glazed, with simulated wooden frames. The rear windows have representations of the protective bars.

Brake hoses, safety valves, whistle, and cab door handrails are separate added parts.

The loco has outside cylinders with slide valves above, supplied by inside Stephenson valve gear with just the valve rod and slide bars outside the frames: a much simpler prospect to model – and less prone to damage – than the delicate outside Joy valve gear of the Manning Wardles. The slide bars are metal, the crossheads plastic.

The coupling and connecting rods are blackened metal, correctly not fluted. The flycranks are held onto the extended axles by cross-head screws – almost the only concession to a robust working model.

The pony trucks have been engineered with a certain amount of side movement, with 12"/305mm quoted as the minimum recommended radius for the model; it may manage slightly less but this is a long machine (96mm over buffer beams).

All wheels are spoked, with blackened metal tyres with fine flanges. The brake rodding is represented.

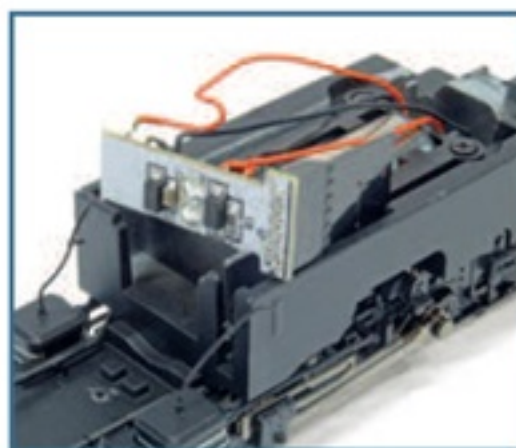
On our sample the smokebox was observed to be sagging forwards slightly, resulting in the chimney not being vertical. It is not clear whether this occurred during assembly or if the model was deformed slightly in the customary clear plastic wrap-around packaging which is designed to protect it in transit.

### Mechanism and performance

Phosphor-bronze wiper pick-ups are fitted to the backs of the four driving wheels and rear pony wheels.

A weight of 88g should make for a reliable runner. No traction tyres are fitted, and the pony trucks are only lightly sprung so most of the weight is carried on the coupled wheels. The real thing could haul four or five coaches, or the equivalent mixed train; the model should easily cope with such a loading with Peco coaches and wagons.

The small can motor is fitted with a flywheel, and performance is smooth and quiet, from walking pace to a realistic maximum. Running-in of 30 minutes in each direction at moderate speed is suggested.



▲ The PCB with six-pin decoder socket and blanking plug.

Three screws secure the body to the chassis, which can be removed to reveal a six-pin socket for a digital decoder and space within the boiler to house the device, which would have to be small.

The only supplied accessories are the working model couplings and ready shaped etched metal cowcatchers; these cannot be fitted if the couplings have the loop installed. There may also be a concern with the cowcatchers (fitted for the accompanying photographs) shorting across the rails if the track is not level – inevitable given the length of the real loco and no fault of the model.

### Conclusion

So, not quite perfect – but not far off. It appears much has been learned from the experience of producing and refining the Manning Wardle models.

This release is bound to give the modelling of the L&B in OO9 a further boost, but as the prototype was in effect a stock Baldwin off-the-shelf product it will be even easier to justify in a freelance context as well.

Worth the wait? Definitely!

AB

### Sample loaned by

HELJAN a/s, Rebslaggervej 6,  
DK-5471 Sønderø, Denmark.  
[www.heljan.dk](http://www.heljan.dk)

UK office for spares and retailer  
returns – Gaugemaster House,  
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Sussex BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
Ref.9985 – £239.95  
(as given on the Gaugemaster  
retail website)

## CONTEMPORARY VIEW L&B 2-4-2T Lyn



▲ The 2017-built replica Baldwin 2-4-2T No. 762 Lyn is seen approaching Woody Bay station on the revived section of the Lynton & Barnstaple route on 20 September 2020. Photo: Craig Tiley

### Prototype history

The L&B was initially equipped with three elegant 2-6-2Ts by Manning Wardle of Leeds, delivered in 1897, but the management soon realised that this left no spare capacity and a fourth loco was required. Manning Wardle and other British builders were unable to supply at short notice but Baldwin – geared up for mass production – could offer quick delivery of a stock item which met the specifications, works number 15965 of May 1898. The loco had outside bar frames and elaborately equalised pony trucks – perhaps required for rough track in America but hardly necessary on the well-laid permanent way of the L&B.

It was shipped in kit form and assembled at Pilton yard. It arrived in a standard catalogue dark green (almost black) livery with simple yellow ('gold') lining, and the name was painted on the cabsides in a typically American elongated lettering style. The maker's plates were on the smokebox sides.

The siren whistle on top of the dome with the safety valves was too tall and had to be offset before the loco could enter service.

An early modification was the replacement of the American smokebox door disc with regular handles, though the three lowest rim locks were retained.

In due course the full lined L&B dark green livery was applied and fabricated name plates with brass letters were mounted on the cab sides. About the same time coal rails were fitted to the bunker, and the rear lamp bracket moved first to the cab back and then to the top of the coal rails.

The Cole safety valves were replaced by Ramsbottoms with a short release lever, and the siren whistle gave way to a bell type on the cab roof.

A new boiler was required in 1907, at which time the ash clearance holes in the smokebox were not replaced.

The copper-capped chimney was replaced shortly before the Southern took control, though it was very much in the then current Southern style. The Southern initially applied brass number plates to the cabsides, moving the nameplates to the tanks.

The loco went to Eastleigh works for major overhaul in 1928 and returned in 1929 in standard Maunsell olive green with large lettering and a painted number on the tanks; the nameplates were moved back to the cabsides.

After 1932 an external steam heating pipe was added in an attempt to attract more passengers.

The loco was allegedly not popular with the crews, perhaps because the inside motion was more awkward to oil round than the Manning Wardle design. Also, it was nominally slightly more powerful but some five tons lighter so could be prone to slipping, though the 5' coupled wheelbase (compared to 6' 6") may have been easier on the track. Maybe it was just because it was different... whatever, it was not made obsolete when the Southern took over and acquired another Manning Wardle, the Baldwin surviving until the closure of the L&B, whereupon it was sold for scrap.

In 2017 a modern replica (which features some detail differences to the original) was completed and placed in service on the restored section of the L&B based at Woody Bay.



## New 'Coronation Scot' coaching stock from Hornby



See these models in action on our website – [bit.ly/HornbyCScoaches](http://bit.ly/HornbyCScoaches)

The Hornby 2020 programme of new product announcements (see News, RM February 2020) included plans to produce, from brand new tooling, coaches to recreate the London Midland & Scottish Railway's c.1937 'Coronation Scot' express train formation – these providing the perfect accompaniment to the firm's existing streamlined Princess Coronation pacific models in blue livery (see Reviews, RM April 2019).

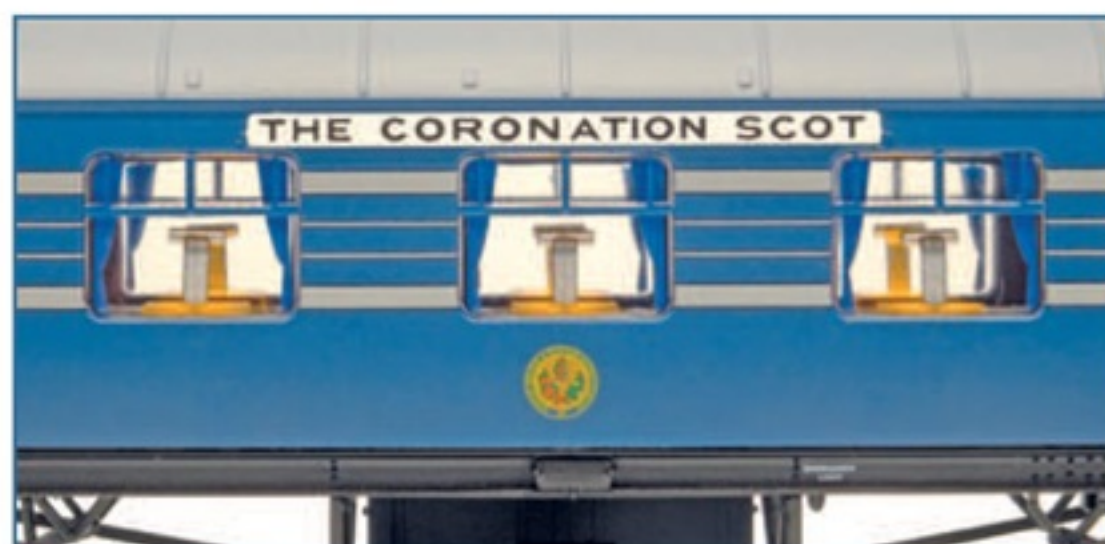
Introduced during the streamline era of the 1930s – widely regarded as the heyday of luxurious rail travel in Britain – the 'Coronation Scot' was a high speed restaurant car express service launched by the LMS on its West Coast Main Line route between London Euston and Glasgow Central. The scheduled journey time was six and a half hours, at an average speed of 61.7mph. A dedicated pool of motive power was provided in the form of Stanier streamlined pacifics Nos.6220-6224; the locomotives and nine coaches all being presented in uniform Caledonian blue livery with silver bands.

Prior to the inauguration of the service on 5 July 1937, a test run took place between Euston and Crewe on 29 June (comprising a reduced eight coach formation hauled by No.6220 Coronation) during which a new world speed record for steam traction of 114mph was set (eclipsed, famously, the following year by the LNER with No.4468 Mallard).

The service formation of the nine 'Coronation Scot' vehicles comprised Brake Corridor First (BFK), Corridor First (FK), Restaurant Open First (RFO), Kitchen (RK), Restaurant Open Third (RTO), Restaurant Open Third (RTO), Kitchen (RK), Restaurant Open Third (RTO) and Brake Corridor Third (BTK). Edward Talbot's book *The Coronation Scot – The Streamline Era on the LMS* (ISBN 0-9542787-1-2) includes – in addition to a fascinating and detailed account of the service – a useful diagram showing the correct orientation of the coaches (which were handed) in relation to the above formation. Furthermore, photographs suggest that the sets were turned between Up and Down workings, such that the locomotive was always coupled to the BTK end.

Most of the coaches were taken from the latest batches of Stanier Period III stock, rather than being built new, with modifications (primarily the refitting of luxurious internal fittings) undertaken at Wolverton Works. The exceptions were the FKs and BTKs, which were constructed specially for the service. Apart from the RKs, the vehicles featured pressure heating and forced air ventilation systems, with the coach roofs featuring distinctive boxed ventilation shrouds.

There were in fact three nine-coach sets used for the 'Coronation Scot'; two in service at any one time, with the other spare.



▲ All vehicles include interior lighting with power taken from wheel pick-ups.

Upon the outbreak of WWII the service was suspended and the vehicles put into store. In 1947 they were repainted into standard coach liveries and redeployed (as individual vehicles) in ordinary passenger service. Withdrawn during the 1960s, all were scrapped except for two of the RKs, which were subsequently converted into inspection saloons – both of which are now preserved.

### The models in detail

Hornby has developed a comprehensive suite of tooling to cover the six different diagrams of vehicles that made up the 'Coronation Scot' formation, comprising a Dia.1905 57' BTK, Dia.1961 57' BFK, Dia.1960 57' FK, Dia.1912

50' RK (x2), Dia.1902 65' RFO and Dia.1981 57' RTO (x3).

Hornby has released nine models to enable a full formation to be assembled, with the running numbers forming what is designated in the aforementioned publication as set No.1. For review we received seven of these releases – the absence of RTO No.8996 and BTK No.5812 precluding us from being able to assemble the complete formation.

The models depict the vehicles in 'as new' condition: by mid-1938 the separate white-backed carriage nameboards (present on the models) were removed, with instead 'THE CORONATION scot' lettering painted in silver directly onto the tops of the blue carriage sides. The models are all of moulded

▼ The corridor side of the Kitchen car (RK), flanked by a pair of Restaurant Open Thirds (RTOs).





The rear three coaches of the LMS 'Coronation Scot' formation showing the BFK (nearest), FK and RFO.



The kitchen side of one of the two Dia.1912 RKs in the formation, which were equipped (in difference to the other vehicles) with 9' wheelbase bogies.

plastic construction, with metal for handrails and some pipework, together with sprung metal buffers and metal wheels. Detailing is excellent, with particular attention paid across the various coach types to cater for the different arrangements of roof and underframe furniture.

Worthy of note are the bogies, which are from new tooling and are complete with representations of the crossbeams and also have the brake shoes set in-line with the wheel treads.

Livery application has been completed to a high standard, with crisp representations of the lettering, silver bands and LMS coach roundels. The impression of interior detailing includes table lighting, together with seating picked out in colours to reflect

the upholstery of the real vehicles. Tension-lock couplings are clipped into articulating NEM pockets, which are affixed to the underframes. Alternative Roco style couplings are included in an accessory pack (fitted for photography). Substitute corridor connection mouldings are also supplied, which are deeper than those fitted to the models and can be fitted if layout curvature allows. (These were also fitted for photography.)

The models each weigh between 136g (for the RKs) and 168g (for the RFOs).

The coaches are equipped with pick-ups to power the interior lighting that is a feature of all of the vehicles, including illuminated table lighting where appropriate (in reality it is the table surfaces

## VINTAGE VIEW 'Coronation Scot' stock



Stanier Princess Coronation pacific No.6220 Coronation heads north near Acton Bridge in 1938 with the Down 'Coronation Scot' from London Euston to Glasgow Central. Note how by this time the carriage nameboards (as fitted to the models) had been removed. Photo: Rail-Online

that are lit on the model, not the table lights, but the effect is convincing nonetheless).

The video that accompanies this review shows a Hornby streamlined Coronation pacific model hauling with ease our seven-car rake around the Pecorama loft layout, with its 1:48 gradients and 3' radius curves. However, the pick-ups on the wheels of some coaches were eased back slightly where excessive drag was noted.

### Future potential

Despite the 'Coronation Scot' having a revenue-earning existence of just over two years, its striking and opulent styling ensured that a lasting impression was made. Hornby should be commended for developing tooling to

bring to market an accurate recreation of the complete train, which is sure to prove popular with modellers.

There are various options for future releases: models could be released in the same c.1937 condition, but with numbering for one of the other sets. This would enable modellers to run Up and Down workings simultaneously (which in reality were booked to pass somewhere between Lancaster and Preston). Alternatively, models could be released without the carriage nameboards to cover the mid-1938 – WWII period. In addition, versions of certain vehicles could be presented in their post-WWII downgraded condition, with liveries including BR carmine & cream, and BR maroon.

CT



Except for the RKs, coach roofs feature boxed ventilation shrouds.



The 65' Restaurant Open First (RFO). The optional close couplings and deeper gangway connections were fitted for photography.

### Samples supplied by

**HORNBY HOBBIES Ltd**  
Westwood, Margate, Kent CT9 4JX  
[www.hornby.com](http://www.hornby.com)

#### PRICES

ref.R4961 – BFK No.5052 £47.99  
ref.R4962 – FK No.1069 £47.99  
ref.R4964 – RFO No.7507 £47.99  
ref.R4963 – RK No.30084 £47.99  
ref.R4965A – RTO No.8993 £47.99  
ref.R4963A – RK No.30086 £47.99  
ref.R4965 – RTO No.8961 £47.99



## Worsdell LNER J27 0-6-0 Brand new from Oxford Rail

Announced as a brand new project in 2019 (see News, RM March for that year), Oxford Rail has delivered the first stocks of its LNER J27 0-6-0 (North Eastern Class P3) in OO gauge.

Designed by Wilson Worsdell (not 'Wordsell' as Bob Jones champions on p656) the NER P3 class was a relatively minor modification of the existing NER P2 (later LNER J26) – the most significant change being a deeper firebox with shallower sloping fire grate. Initially 80 P3s were built between 1906 and 1909 in five batches, distributed amongst Darlington, North British Locomotive Co., Beyer Peacock & Co, and Robert Stephenson & Co. A further batch of 25 was built by Darlington with Schmidt superheaters and piston valves – delivered in 1921-2, followed by a final batch of 10 (now classed as the J27 under the newly formed London North Eastern Railway) in 1923. The superheated locomotives could be readily identified by their extended smokeboxes and balance weights on the centre drivers, however from 1932, these weights were also fitted to all of the saturated J27s.

When introduced these locomotives were initially allocated to long distance mineral and freight trains but were displaced to local mineral traffic as larger locomotives were introduced. The class was widely distributed over NER metals with allocations including Percy Main, Newport, Shildon and Sunderland.

After World War II the J27s stopped hauling goods trains but continued to work heavy mineral traffic. Withdrawals began in March 1959, with 36 still employed on coal trains around County Durham and South Northumberland in June 1966. The final J27s were withdrawn from the Blyth area where they operated the short trip workings between the nearby coalfield and shipping staithes, with the last withdrawal taking place in September 1967. A single example (No.65894) survives in preservation today.

The Oxford tooling currently caters for the saturated variants of the class, with Diagram 57 and 57a boilers, with Ramsbottom or Ross 'pop' safety valves.



### Our Sample in detail

For review we received a sample of No.1010 in LNER plain black, with Ramsbottom safety valves – one of four versions announced by the manufacturer; the others comprising No.65837 in BR black with early emblems, No.65817 in BR black with late crests and No.1214 in L&NER lined black.

Built at Darlington Works in 1908, No.1010 retained its NER number following the 1923 Grouping, before being renumbered to No.5807 in February 1946, and subsequently No.65807 in January 1949 after Nationalisation. In February 1950 No.65807 was allocated to Heaton (52B), before relocating to Percy Main (52E) during 1952 where it remained until its withdrawal from service on

14 May 1962. It was scrapped the following month.

The presence of balance weights on the centre drivers, combined with the livery and running number, dates the model from around 1932 until the locomotive's renumbering in early 1946.

The construction of the model takes the form of diecast metal for the running plate and chassis, with moulded plastic parts utilised for the boiler, cab and tender body – the entire model weighing in at 238g. Handrails on the boiler barrel are metal wire, while those fitted to the cabsides and tender are plastic. Just above the handrails along the boiler is what appears to be a seam line in the boiler moulding (one can assume for different tooling options). The etched fall plate between the locomotive and tender is adjustable, whilst the metal buffer heads are sprung.

Much like the prototype the detail below the running plate is pretty

sparse, with the exception of the brake rigging which is pre-fitted on both the loco and tender. The coal load is part of the tender body moulding.

Slimline tension-lock couplings in NEM pockets are fitted as standard, but these can be detached and the supplied three-link couplings fitted instead – however it's worth noting that the hole in the hook needs to be made into a slot to accept the top link. Also supplied in the accessory pack are a selection of fireman's tools and a spare tender drawbar.

Livery application and printing is very good, with the printed lettering and numbers clear and crisp – right down to the builder's plates which are fully legible under a glass. The otherwise flat black livery is offset by the representation of the inner motion (which is present in between the frames) and the bufferbeams, all picked out in red. Cab backhead detail is well represented with fittings picked out in appropriate colours – this extends to the printed dials on the gauge glasses.

### Mechanism and performance

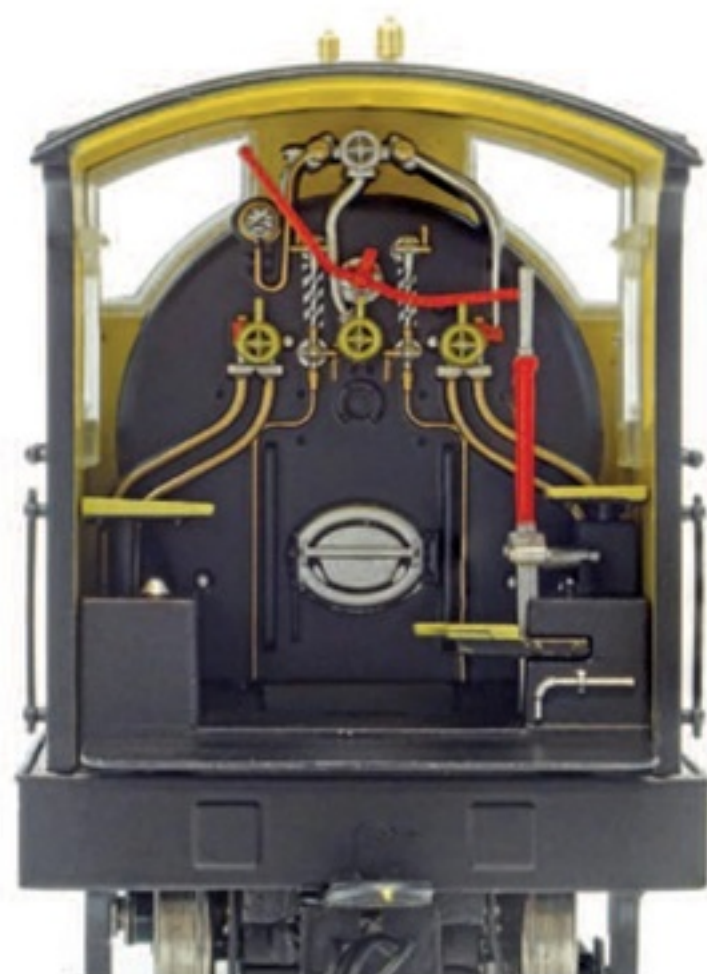
The model is equipped with a coreless motor and brass flywheel housed in the boiler/firebox area driving the rear coupled axle, with electrical pick-up from all of the wheels, with the exception of the rear axle on the tender. A 21-pin decoder socket is located in the tender, which is electrically connected to the locomotive via an adjustable drawbar, which provides close-coupling if layout curvature allows (as illustrated).

A compartment for a speaker is present underneath the PCB inside the tender: factory sound-fitted versions are planned by the manufacturer.



See this model in action on our website – [bit.ly/OxfordRJ27](http://bit.ly/OxfordRJ27)





Notes advising upon the removal of the locomotive body are not included in the literature enclosed with the model. However, this was achieved by first removing the large screw for the tender drawbar to give access to two smaller

screws located under the cab, with a third screw situated under the front NEM pocket. Once these were removed the body could be lifted upwards, with care taken not to snag the inside motion on the running plate. Removal of the tender body (which is illustrated in the manual) required two screws at the front to be removed; the body could then be tilted back thereby releasing the retaining lugs at the rear.



Care was taken during this process to avoid damaging the lamp irons on the tender rear.

When we first tested the model it ran intermittently; following some investigation it appeared that lubricant from the locomotive mechanism had contaminated the sprung electrical connections (located aft of the rear axle) that transfer power from the pick-ups (mounted on the chassis baseplate) to the motor. Also, a pick-up on one of the leading driving wheels was found to be twisted and not making good contact.

With the sprung connections cleaned and pick-up re-aligned, the model then performed much more satisfactorily. However, we cannot be sure if what we observed is specific to our sample.

The model was able to haul five Pullman cars around the Pecorama loft layout, with its 1:48 gradients and 3' radius curves.

Oxford Rail has captured well the look of these northern workhorses, which are sure to be popular with North Eastern/Eastern region modellers. With the J27's predecessor – the LNER P2/LNER J26 – on the horizon (see news, RM March 2020), we look forward to comparing these two counterparts side-by-side in the not too distant future.

CW



#### Sample supplied by

**OXFORD RAIL,**  
Oxford Diecast, PO Box 62,  
Swansea SA1 4YA  
[www.oxfordrail.com](http://www.oxfordrail.com)

**PRICES**  
Ref. OR76J27001 – £109.95  
(DCC sound-fitted £209.95)

## New BP/Shell liveried 10T tank wagon

Oxford Rail has delivered a further livery variant of its 10T tank wagon, an example of which we first reviewed in our November 2020 issue. This latest version carries the red and silver livery of Shell-Mex and BP Ltd, with the running number SM1061.

In 1932 petroleum companies Royal Dutch Shell and BP merged their UK operations to form Shell-Mex

and BP Ltd, which led to joint branding on items including rolling stock. Accordingly, the model features green BP branding on one side and red Shell branding on the other.

The branding, along with the numerous markings and lettering present on the model, has been applied neatly. The solebar is adorned with a number of intricate builder's and company

plates – all of which are fully legible; the main builder's plate located to the right of the vee-hanger reads; 'G.R. Turner Ltd. Builders Langley Mill, England. 1925'.

Published sources we referred to appear to show a discrepancy between the model and the real vehicle, however, with No. SM1060 documented as a 14T class A tank with a barrel

diameter in the region of 6' 7" (compared to the 5' 8" barrel on the model) although it is noted that the company did operate tankers of the smaller size, some of which were still in service during the early 1960s.

Nonetheless, this attractively-liveried model is sure to prove popular with post-War era modellers.

CW



#### Samples supplied by

**OXFORD RAIL,**  
Oxford Diecast, PO Box 62,  
Swansea SA1 4YA  
[www.oxfordrail.com](http://www.oxfordrail.com)

**PRICE**  
Ref. OR76JTK2007 – £21.95



Cavalex Models has delivered stocks of its KFA/PFB Warflats in 00, which have been produced exclusively for the Peterborough-based model railway retailer Trains4U.

A total of 65 KFA Warflat wagons were built at Shildon for the Ministry of Defence between 1976 and 1981. Originally dual-braked, with the TOPS classification PFB, they were converted to air-braked only during the 1990s. Some received the PFA code before the K prefix came into use (KFB/KFA). These wagons can be used for transporting a variety of loads; primarily soft skin and light armoured vehicles. KFAs are still in wide use by the MOD today.

The models have been manufactured using cast metal for the underframe, plastic mouldings for the deck (including buffer beams, and solebar) and bogies, plus etched components for some of the underframe details, including the buffer beam mounting points. Buffers are plastic and sprung.

The models are equipped with slimline tension-locks couplings in sprung NEM pockets mounted to the underframe, which provides sideways articulation. Brake pipes are provided in a small pack for the purchaser to install



(fitted here for photography). Fitting these on the PFB sample required some fettling of the locating holes.

The bogies run on free-rolling metal wheelsets and – as per the prototype – disc brakes are only present in opposing corners. We understand that the wheelsets can be swapped for P4 or EM versions without modification.

Livery application has been completed to a high standard across both versions, with lettering and data markings all applied crisply; note as well the different stencil styles used between the two vehicles. However, we did observe some minor paint bleed between the yellow jack ends and olive paintwork on our KFA sample.

The models are available exclusively from Trains4U, priced £35.00 each.

A pack of 12 wheel chocks is also available from the retailer (priced £10.00 painted/£7.25 unpainted), which is ideal for those wishing to add vehicle loads.

CW

## Samples loaned by

**TRAINS4U,**  
27 St. David's Square, Fengate,  
Peterborough, PE1 5QA.  
[www.trains4u.com](http://www.trains4u.com)

**PRICES (see text)**  
Ref.CM PF004 PFB(4) –  
Moda 95288 MOD PFB Warflat  
Ref.CM PF004 KFA(4) –  
Moda 95297 MOD KFA Warflat  
Ref.T4UPC – Chocks x12  
(painted)



# New releases from Gaugemaster

# 00 & N



New for N from Gaugemaster is a version of the Kato Pocket Line electric locomotive presented as E3682 in BR black livery with early emblems (ref.GM2260201), which is inspired by the North Eastern Railway class ES-1 design.

Based on a Japanese-outline tooling, this freelance model is fitted with knuckle couplings to suit operation with British outline stock. Weighing in at 28g the model is of a plastic construction with a photo-etched/wire adjustable pantograph fitted to the cab roof.

Being of overseas origin the loco lacks buffers, however a set of red buffer beam stickers are provided for the owner to fit if desired.

The model utilises the Kato 11-109 powered motor chassis, which has long-been proven to be a smooth and reliable runner – forming the basis of many 009 locomotive projects.

Priced at £49.95 the model may appeal to those starting out in N gauge, or as a quirky addition to an existing collection. Perhaps we will see a British-outline version of the



Pocket Line 0-4-0 tank locomotive in the future...

The retailer has also added two further 00 gauge plastic structure kits to its range. Firstly there is a kit for Teignmouth Signal Box (ref. GM481), which comprises 59 plastic parts moulded in four colours, including transparent fittings for the windows. Based on a GWR design, the completed model measures approximately 160mm(L) x 65mm(W) x 110mm(H).

Secondly, an art deco style seafront café can be modelled using kit ref.GM484, which contains 79 parts moulded in three colours, with transparent fittings. The dimensions of the completed model are 80mm(L) x 155mm(W) x 47mm(H).

CW

## Samples supplied by

**GAUGEMASTER CONTROLS,**  
Gaugemaster House, Ford Road,  
Arundel, West Sussex  
BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

**PRICES**  
Ref.GM2260201 – £49.95  
Ref.GM481 – £16.50  
Ref.GM484 – £13.25





## New locomotive crews from Masterpiece Figures

00

Three new sets of 4mm scale crew figures have joined the extensive Master Piece range of lead-free pewter models. These latest releases are designed to fit the Dapol Churchward Mogul (see review, RM January 2021) and SE&CR Wainwright D Class (RM August 2021).

Two sets are available for the Mogul: set A (ref.FF141) driver has his left hand to the regulator with the fireman shovelling, whilst set B (ref.FF242) has the same pose for the driver but with the fireman leaning out of the cab.

The figures for the Wainwright D class (Ref.143) have been designed to fit flush to the sides of the splashers, with the driver on the left-hand side of the cab and fireman leaning out of the right-hand side.

The sets are available in plain metal finish, or painted as illustrated here. Some have bases attached to assist with fixing in position.

CW



### Samples supplied by

**MASTERPIECE FIGURES,**  
28 Chiltley Lane, Liphook,  
Hampshire GU30 7HJ.

#### PRICES

Unpainted – £5.00 per set

Painted – £10.00 per set

P&P £2.50.

Please make cheques payable to 'M R Hill'.

## 3D printed bodysells from Thanet Locomotive Works

00



Thanet Loco Works by CDC Design has released a range of 3D-printed locomotive bodies – all of which are designed to fit proprietary OO gauge locomotive mechanisms.

Each body is manufactured in UV-cured resin and is supplied complete, with all details pre-fitted and picked out in appropriate colours. A donor chassis is required, together with paint and transfers for the livery of choice. A bespoke numbering and lettering service is available for an additional charge of £10.00 per model.

For review we received samples (pictured from left to right) of the firm's GER S56/LNER J69 class 0-6-0T (designed to fit the older style of Dapol/Hornby Terrier chassis), LMS 2441/3F and GWR '9700' class condensing 0-6-0PT (the latter two being designed to accept the Hornby 0-6-0 'Jinty' chassis).

At first glance it's hard to believe these bodies are 3D printed, with each featuring a wealth of fine detail including handrails, pipework, lamp irons and smokebox door handles. The rivet detail

and condensing apparatus on the pannier tank is worthy of note.

Evidence of 'stepping' from the printing process is noticeable on the front and rear of the models, although this can be addressed with some light sanding and painting. Each body is finished in matt black, with bufferbeams picked-out in red, buffer heads in silver, vacuum pipes in grey and safety valves and whistles in brass. The J69 also features a removable roof which assists the painting of the cab interior and installation of crew figures.

We also received a second sample of the LNER J69 finished with the firm's bespoke numbering and lettering service as Liverpool Street station pilot locomotive No.68619 in Great Eastern Railway lined blue livery with BR late crests. The livery has been applied with the use of custom vinyl stickers on the



tank and cabsides, along with the sand-boxes, Westinghouse pump, boiler bands and smokebox number plate. The boiler and running plate has been painted, with the blue colour a near perfect match with that of the stickers. Details such as the smokebox dart and destination board brackets are picked out in silver and the cab roof in white.

These bodysells certainly have plenty of potential, providing as they do a straightforward means for modellers to expand their locomotive fleets

without the need to kit- or scratch-build. One does wonder what other chassis could be fitted to the bodies illustrated here with a bit of modification, such as the 57xx/'8750' 0-6-0PT from Bachmann for the condensing Pannier perhaps...

The full range of 3D printed bodies can be seen on the firm's website, together with details of how to order.

CW

### Samples supplied by

**THANET LOCOMOTIVE WORKS,**  
[www.cdc-design.net](http://www.cdc-design.net)

#### PRICES

GER S56/LNER J69 –  
£42.00 + P&P

LMS 2441/3F class –  
£48.00 + P&P

GWR '9700' 0-6-0PT –  
£48.00 + P&P



## DCC Concepts powders

## MULTI-SCALE

The Settle-based supplier of model railway accessories, DCC Concepts, has released a selection of weathering powder sets.

For review we received samples of the five sets that have been produced, which each contain four pots of pigments and four brushes; Shades of Grey (ref.DCW-GRY), General/Layout (ref.DCW-GLW), Locos & Wagons (ref.DCW-LRD), Track & Turnouts (ref.DCW-TRK), and Buildings & Infrastructure (DCW-MIX) – priced at £11.95 each. A pack comprising 12 pots of pigment and four applicators is also available, priced at £29.95.



Decal Fix), before being chipped with a cocktail stick and a brush moistened with water – allowing the powders to bleed through. Finally, the overall wagon received a light dusting of the Black pigment – applied with the large soft brush.

The pigments proved to be very versatile, with the desired effect achieved quite easily. The selection of brushes supplied enabled the powders to be applied in an effective manner and will be useful for other tasks as well.

CW

We chose to evaluate the Locos & Wagons set by weathering a Bachmann BR 16 Ton mineral wagon using a chipping and flaking paint effect (as described in the March 2020 issue). Rich Old Rust and Brake Dust powders were worked onto the wagon body (which was prepared with a 50/50 wash of Humbrol Matt Cote and Enamel Thinners), building up a rusty finish with varied tones and textures – applied with the supplied sponge brush. This was subsequently painted over with the grey livery (thinned with Humbrol

### Samples supplied by

**DCC CONCEPTS LTD.,**  
Unit E, The Sidings, Settle, North Yorkshire  
BD24 9RP

**PRICES (see text)**

## New figures from Modelu

## 4mm



Modelu continues to expand its extensive range of 3D-printed figures, which are created using laser scans of real people and cover scales from 1:148 (N gauge) through to 1:32 (Gauge 1).

For review we received a sample of the new Wickham trolley crew pack (ref.11180), suitable for pre-Nationalisation (1923-1947) and British Railways (1948-1966) periods. The set comprises three seated figures, along with two shovels, a fishplate spanner, key hammer and crew bag. The set is ideal for populating the diminutive Wickham trolley from Bachmann – as illustrated here.

Also pictured here is a set of four 1950s-1960s period lineside photographers, each in suitable attire and armed with a different camera: ref.1670 – Photographer with Box Brownie camera (with jacket and hat), ref.1671 – Photographer with Box Brownie, ref.1672 – Photographer with Kodak No.2 camera, and ref.1673 – Photographer with Leica Mk.II camera.

All figures are supplied unpainted in a grey-coloured material and require painting. The full range of Modelu 3D printed figures and lineside accessories can be

seen on the firm's website, together with details of how to order.

A custom laser scanning service is also available by prior arrangement.

CW

### Samples supplied by

**MODELU,**  
Unit 50, Easton Business Centre, Felix Road,  
Bristol, BS5 0HE.  
[www.modelu3d.co.uk](http://www.modelu3d.co.uk)

**PRICES**  
**Wickham Trolley Crew Pack (4mm)**  
**ref.11180 – £18.00 (4mm/1:76)**

**Photographers (4mm)**  
**ref.1670/1671/1672/1673 – £4.50 each**

## Book Reviews

### Steaming Through the Chilterns – and Thereabouts

Compiled by Robert Freeman & Mary Casserley

The name H C Casserley will need little in the way of introduction for many of our readers; undoubtedly one of the most prolific railway photographers of the 20th Century, he amassed some 80,000 photographs during his lifetime, dating back to 1919 and covering all areas of Britain.

This photographic tribute to the work of this master lensman is in fact a sequel to *Steaming through Berkhamsted – Bourne End to Northchurch 1938-1968* (see book reviews, RM March 2018). However, whilst that first book was compiled by the photographer's son Richard Casserley (who passed away shortly before it was published) and granddaughter Mary Casserley, this follow-up work has been compiled by Mary with the assistance of railway enthusiast Robert Freeman, together with an introduction by former Virgin Trains CEO Chris Green.

The regional reference of the Chilterns in the title sets the parameters of this volume (although this is occasionally deviated from) and includes a further look at the West Coast Main Line at Berkhamsted (where HCC lived) but without repetition of what was included in the previous title.

Divided into sections, the book covers – the GC and GWR lines around Princes Risborough, electrified Metropolitan subjects, Watford (with rare views of ill-fated Turbomotive No.46202 in BR lined black livery), St Albans on the Midland Main Line, and Welwyn and Potters Bar on LNER territory.

There is a snowy interlude including some wartime views taken from HCC's study – the garden of his property backing onto the WCML, whilst the penultimate chapter deals with various unusual WCML workings, including views of Bulleid Pacifics engaged in exchange trials in 1948.

The book finishes with a look at the 'new order', with various early types of diesel and electric traction depicted, including views of one of the LMS diesel 'twins' No.10001 and Bulleid diesel No.10203.

Photographs are exclusively black and white, save for a solitary comparative view in colour on p58 of Aylesbury taken in 2020 by Robert Freeman. The images are presented postcard style in multiple per two-page spread for the most part; they are printed with a slight sepia tint and are accompanied by detailed captions for which much of the information was gleaned from meticulous notes taken by HCC.

The route map graphics that precede each section provide some geographical context for the locations seen, whilst images of original Edmondson tickets provide suitable 'atmosphere' on many of the pages.

With such a vast archive of images available, there must surely be scope for further photographic volumes of HCC's work in the future.

CT

Goose Publishing,  
17 Western Road, Tring, Herts HP23 4BQ.  
[www.marycasserley.com](http://www.marycasserley.com)

210mm x 247mm 170pp  
**Softback £22.00**  
ISBN 978-0-9543838-3-1





## Southern to the Coast

Compiled by Jeffery Grayer

Railway photo albums must be one of the most popular formats and for very good reason; they hold a huge amount of visual information that is essential for the historian and modeller alike. This publication is not new in covering the Southern Region, but covers what the SR (and the Southern Railway before it) did better than anyone else; take holidaymakers from London and the Home Counties to the seaside.

There is nothing remarkably new here, but by picking coastal stations (from Kent to Cornwall) the book does catch a few stragglers that are often overlooked. For example, Allhallows-on-Sea presents a very likely modelling subject, but has rarely been tackled, possibly because it was a failure, and it gets omitted from the usual lists. On the flipside Newhaven, which is hardly a seaside resort, is rarely covered in any great depth due in large part to its sheer size at its peak.

The usual suspects are included: Hayling Island and Lyme Regis for example, but there are gems such as the short-lived New Romney and the local (to this office) Seaton.

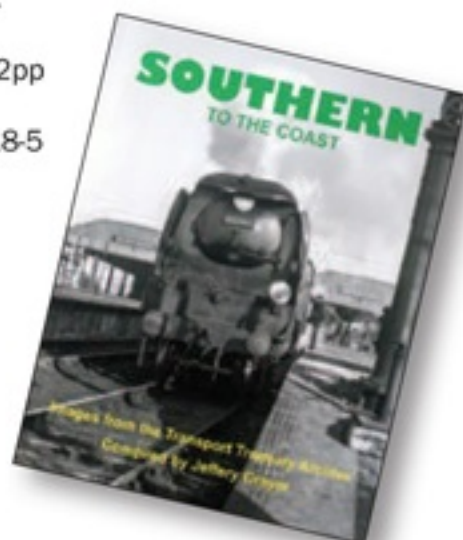
The spread of locomotive types is comprehensive from the tiny A1X Terriers though to Merchant Navy Pacifics. Oddities are included such as the O-6-OT R1s and ex-WR O-6-OTs that were used as bankers at Folkstone, WR 13XX O-6-OTs and the ancient O-6-0 01 Class. This is very much a steam album. Diesel and electric power are only lightly touched upon but due to the Western Region's take-over of part of the SR, a solitary Hymek squeezes in under the wire.

The photographic reproduction is second to none and the shots are laid out either in singles or doubles on the page. All are accompanied by medium length captions which in keeping with other albums in the series are informative without becoming mired in superfluous technical detail. This is just about as perfect a view of the Southern Region in its heyday as you could possibly find.

CF

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[www.ttpublishing.co.uk](http://www.ttpublishing.co.uk)

215mm x 273mm 112pp  
**Softback £14.50**  
ISBN 978-1-913251-18-5



## Unconsidered Trifles Geoff Kent

At the first thumb-through this book appeared to be just a collection of slightly random photos. However, closer study reveals this to be a unique collection with a very different and worthwhile angle of approach, namely recording the items of man-made detritus that is of interest to the modeller and urban historian, but which is being swept away rapidly.

The book is split into four quite loose chapters: Signs, Buildings, Railway and This, that and the other. The first will be useful for anyone modelling period street scenes and covers shop fronts and all the associated signage. Then, road signs, particularly the white finger posts that all layouts usually have in all their various styles followed by milestones and notably shots of chequered warning paint on bridge parapets.

The buildings section covers walls, lock-up shops, bridges, pubs and corrugated iron architecture, while the railway section looks at the more minor structures that remain, fencing and redundant carriage conversions. The final section wraps up with, essentially, the miscellaneous items that do not slot comfortably into the previous chapters.

The reproduction is of a high standard and typical of the publisher, though the colour is quite bold.

The modelling of the central part of the 20th century is still by far the preferred vehicle for most railway modellers and yet finding those details from the 1920s through to the 1970s becomes, seemingly, harder with each passing week. This book, while not comprehensive, will not only become a record of such details, but in many cases will become an inspiration for many scenes on layouts. Highly recommended.

CF

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[www.wildswanbooks.co.uk](http://www.wildswanbooks.co.uk)

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ISBN 978-1-912038-65-7



## Cornwall – Transition from Steam – The R C Riley Archive: Vol 6

Compiled by Jeremy Clements

Without a shadow of a doubt the Great Western Railway was, and is, the most popular subject for modellers, arguably driven by the commercial support it has received over the last few decades. Within that sphere of interest for all things GWR, it is the company's lines in the West Country that seem to receive the most attention. Accordingly, this photo-album style book will be well received as it ticks those boxes – albeit in the later than classic 1960s era.

The book is split into seven chapters: Viaducts (because there were a lot of them in the county) Main line, Branch line, Goods train, MPD, People and Next train. These categorise the photos into type, though in many ways this is only to provide convenient breaks and the book could almost work without them.

The expected locations are covered such as Fowey, Moorswater and St. Ives. As always with a large format photo book much of the useful modelling information is in the background; I hardly need to point out that the Western Region infrastructure hadn't changed much since the pre-War years, so a lot of this detail is applicable to earlier modelling periods.

The title of the book suggests that this is an album split between steam power and the incoming

diesels; not so. Although the last chapter does deal almost exclusively with the new hydraulic classes, the overall split is probably more than 80/20 in favour of steam.

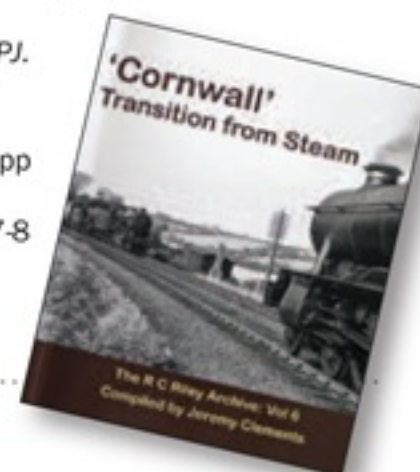
As usual with this publisher the photos are on high gloss paper and are beautifully presented. All are accompanied by extended captions that provide a perfect amount of information without drowning the reader in technical information.

This is an excellent, well-presented album, and the only reservation concerns the steam/diesel imbalance. However, if you want beautiful shots of steam traction from pannier tanks to Castles working in the Duchy in the 1960s, then this is the book for you.

CF

Transport Treasury Publishing Ltd.,  
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High Wycombe HP13 7PJ.  
[www.ttpublishing.co.uk](http://www.ttpublishing.co.uk)

215mm x 273mm 112pp  
**Softback £14.50**  
ISBN 978-1-913251-17-8



## Geoff Williams' Aylesbury LNWR – Researching and Modelling the Prototype Bob Williams

Long-lived layouts are always popular and quite rightly so. This is especially true if the construction has been documented and published over the same period giving the reader the feeling of personal attachment.

Aylesbury LNWR was begun in earnest in 1955 and was Geoff Williams' initial jump from OO gauge to EM. This first layout was scrapped and a new one started after a large amount of new information on the station came to light; such are the problems of modelling a prototype as opposed to freelancing. In 1963 the project was relocated into a loft space – its home until Geoff's demise in 1998.

The first part of this volume covers the building of the layout. This isn't dealt with in a blow-by-blow style but focuses on certain pertinent aspects, particularly the locomotives which were initially scratch-built using K's parts, then moving to modified GEM kits as these became available. Notable scenic items are covered such as the gas holder, perspective modelling and the LNWR footbridge which was an early example of 'all the way through' etching.

The layout was acquired by Risborough & District Model Railway Club and there are two short intermediate sections which describe the restoration and adaptation for exhibition use.

Part two of the book is slightly longer and describes the research carried out in order to build the layout. This includes numerous 'on-site' detail sketches and scrap detail photos of the line from Aylesbury to Cheddington. There are a handful of computer-generated drawings including the station building and engine shed. It is perhaps slightly unfortunate that the former is laid out across a two-page spread,

resulting with a portion of the drawing falling into the gutter of this perfect-bound book. Part two is supported by a series of photos taken by the author and by ex-porter Billy Sutton which provide an invaluable resource.

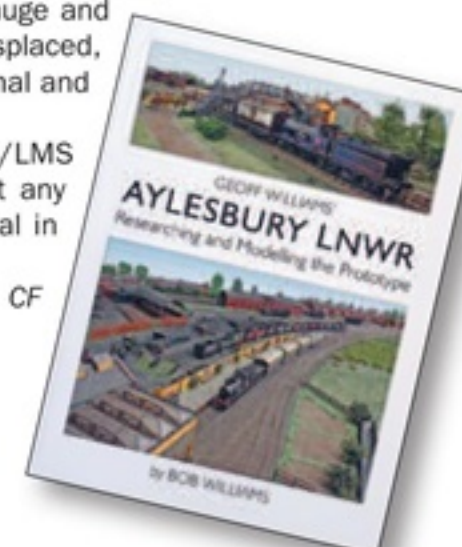
This is a magnificent work by Geoff Williams' son Bob who manages to deal with the subject in a very third person manner without resorting to personal recollection and emotional tone. There will of course be lazy comparisons with the Rev. Peter Denny's *Buckingham* which shared a gauge and loose contemporary timeline. This would be misplaced, however, as Aylesbury LNWR is a very different animal and stands independently.

This title should appeal not just to LNWR/LMS modellers (who will find the detail invaluable), but any modeller, finescale or not, who will find a great deal in this book to inspire.

CF

Wild Swan Books Ltd.,  
Tollbridge Studios, Toll Bridge Rd, Bath BA1 7DE.  
[www.wildswanbooks.co.uk](http://www.wildswanbooks.co.uk)

225mm x 275mm 119pp  
**Softback £26.95**  
ISBN 978-1-912038-64-0





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## Pete Waterman's tribute to the West Coast Main Line draws the crowds at Chester Cathedral event



▲ Pete Waterman alongside the layout, which is on display at the *Making Tracks* event. Photo: Dave Fenton

Since it was first unveiled to the public on Saturday 17 July, visitors have flocked to Chester Cathedral to see the mammoth 74' 00 gauge celebration of the West Coast Main Line that has been constructed by Pete Waterman OBE and his team of modelmakers.

The layout is the centrepiece of a special event, titled *Making Tracks - Discover the West Coast Main Line*, which is planned to run daily until Friday 3 September, opening at 1000 each day with last entry at 1615 (1600 on the final day).

Featuring full overhead catenary, the DCC-operated layout also celebrates the work of the notable civil engineer Thomas Brassey, with depictions of Cefn



Mawr viaduct and Shugborough Tunnel. (See our Talking Points feature in the July 2021 issue for more about the background and construction of the project.) Several organisations are supporting the



▲ A view of the completed layout, showing the impressive overhead catenary that has been modelled, for which some components have been sourced from the Peco range. Photo: courtesy Andy Gyde

▲ Pete is pictured surveying the extensive storage sidings for the 74' long layout. The model is on display within the vast interior of Chester Cathedral.

event, including Hornby and Chester Model Centre – the premises of this retailer being located a five minute walk from the cathedral.

Videos showing the layout in action – one of which is introduced by Pete – have been produced by Dave Fenton of Megapoints Controllers. For links to these videos go to: [www.megapointscontrollers.co.uk](http://www.megapointscontrollers.co.uk)

Admission is priced £2.50 per person, with tickets available upon arrival or by pre-booking: see the event web page link below or telephone 01244 500 959.

[www.chestercathedral.com/event/making-tracks/](http://www.chestercathedral.com/event/making-tracks/)

## Kernow reveals progress with GWR Steam Railmotor models in OO

Kernow Model Railway Centre has revealed the first assembled Engineering Prototypes (EPs) of its forthcoming GWR Steam Railmotors in OO, plans for which were first announced in our January 2015 issue.

The specification for the model includes working directional oil lamps and working interior lights. The models will also be DCC ready with a 21-pin decoder socket.

Electrical pick-ups are fitted on all wheels, whilst the working valve gear includes representations of the radius rod and piston valve stem. The motor is positioned vertically and is concealed within the boiler of the power bogie. The tooling caters for both the Diagram 'O'



▲ An engineering sample for the GWR Diagram 'R' Steam Railmotor model.

and Diagram 'R' type Steam Railmotors, with five versions confirmed (see website for details).

Graham Muspratt, Kernow MRC Development Manager advises that:

"The livery artwork is underway concurrent with testing these

engineering prototypes and we hope to see decorated samples by the autumn. We should then be in a position to start to arrange production. We thank the Great Western Society for its continued support and valued input as the project progresses."



The models have a revised retail price of £179.99 (£169.99 for pre-orders). A delivery date is to be confirmed.

For further information about the models and for details of how to pre-order, see the retailer's website:

[www.kernowmodelrailcentre.com](http://www.kernowmodelrailcentre.com)

## Sonic Models unveils Gresley J50 0-6-0T in N

Sonic Models has announced that the subject of its next N gauge powered model (following its Collett 0-6-2T) will be the Gresley J50 0-6-0T, in eight different liveries comprising four LNER and four BR schemes.

Tooling for the model is complete and EP samples have been received. The models will be available with detail variations including flush or raised Ross 'pop' safety valves, plain or fluted connecting rods, plated or open coal rails and LNER or GNR pattern buffers.

Some will also feature the extended shunter's step and handrail fitted to certain locomotives.

The models will be retailed exclusively by Rails of Sheffield and the RRP is expected to be £109.95. Delivery is planned for spring/early summer 2022.

For more information about these models including the numbers and liveries being produced, together with details of how to order, visit the retailer's website:

[www.railsofshffield.com](http://www.railsofshffield.com)





## International N Gauge Show returns for 2021!

The International N Gauge Show (TINGS) is set to return on the 11 & 12 September 2021 at Warwickshire Event Centre (near Leamington Spa, CV31 1FE).

There will also be more than 25 layouts on display depicting modern and steam era subjects of British and Continental outline. Layouts are set to include *Todmorden Midland* (see RM October 2016), as well as representation for five N gauge societies. Supplementing these will be specialist modelling demonstrations and extensive trade support.

Tickets are priced £11.00 for adults, £10.50 for senior citizens and £5.50 for children. All tickets must be purchased in advance to guarantee entry to the show – tickets will be valid for a specific day only.

The event organisers, Meridienne Exhibitions will continue to monitor

and act on advice from the Government and respond accordingly to guidance throughout the coming months, to ensure the event can safely be delivered with compliance to any Covid-19 requirements that may be in place at that time.

Leamington & Warwick Model Railway Society plans to launch a special limited edition wagon at the event, which it has commissioned from Dapol. With versions available in N and OO gauges, the models have been produced to assist one of the society's members with efforts to raise funds for the Myton Hospices Group, which is based in Warwick.

For more details about the model railway exhibition see our Societies & Clubs listings or go to:

[www.meridienneexhibitions.co.uk/events/the-international-n-gauge-show](http://www.meridienneexhibitions.co.uk/events/the-international-n-gauge-show)

*Todmorden Midland* is one of the layouts that is booked to appear at the 2021 TINGS event. Photo: Steve Flint



## Class 37 model presented to Direct Rail Services



Revolution Trains and Heljan recently presented Nuclear Transport Solutions/ Direct Rail Services with a display model of the exclusive O gauge model of 37 405 in Compass livery, which was produced exclusively for Revolution Trains by Heljan under licence from DRS (see News, RM May 2021). The model was exchanged at a ceremony in front of the real locomotive at Crewe Gresty Bridge depot.

Andy Grundy, Head of Engineering (Fleet & Technical) accepted the model on behalf of NTS/DRS from

Revolution's Ben Ando and Heljan's Ben Jones.

Ben Ando said: "NTS/DRS's stylish dark blue Compass livery has long been a favourite of model railway enthusiasts. We were delighted to be able to produce these models and would like to thank NTS/DRS for their assistance."

A small number of models are still available and can be purchased direct from Revolution Trains website, priced £599.00. For more details:

[www.revolutiontrains.com](http://www.revolutiontrains.com)



▲ The presentation of the model took place at Crewe Gresty Bridge depot in front of the real 37 405. Photographed from left to right: Ben Ando – Revolution Trains Director, Andy Grundy – NTS/DRS Head of Engineering (Fleet & Technical) and Ben Jones – Heljan UK.

Photo: Revolution Trains

## TMC exclusive BR 24.5T mineral wagons in OO



In the review pages of our August issue we presented examples of the exclusive OO gauge BR 24.5 ton mineral wagons available from the Yorkshire-based retailer The Model Centre (TMC), which have been produced from brand new tooling in partnership with Bachmann Europe Plc.

Unfortunately the images that accompanied the review were affected by a production issue; accordingly a further selection of images is presented here.

10 different versions of the wagons are available, with the tooling covering two types of axlebox, three types of buffers and three different bodies.

The models are available individually priced £25.50 or in twin-packs priced £50.95.

TMC is also offering wagons with its custom weathering finish, priced £40.50 (individually) or £80.95 (twin-pack). Options for screw-link couplings and real coal loads are also available at additional cost – see the retailer's website for further details and pricing.



**The Model Centre**  
Hill Farm, Beck Hole, Whitby, North Yorkshire  
YO22 5LF  
[www.themodelcentre.com](http://www.themodelcentre.com)



## Collector's guide for coaches in N

A new title from Engee Publishing is the *Collector's Guide to British Passenger Rolling Stock in N Gauge (1967-2020)*, which is a 44-page A5 softback publication with colour images. Listed broadly in chronological order is every item of coaching stock produced by manufacturers including Hornby, Minitrix, Lima, Graham Farish, Bachmann and Dapol. Copies can be obtained from the website below priced £5.95 (P&P free) or by sending an order with cheque (payable to 'Engee Publishing') to: 40 Windsor Road, Great Harwood, Blackburn BB6 7RR. Currently in preparation for release later this year is a *Collector's Guide to British Freight Rolling Stock in N Gauge*.

[www.ngaugemodelling.co.uk](http://www.ngaugemodelling.co.uk)

## Day Return to 1969 – Stephen Cornish

Stephen Cornish has published an autobiographical account of his extensive 40-year career working in the railway industry, which began with selling tickets and then saw him climb the management ranks – culminating with the role of Special Trains Manager for Network Rail. The book offers an insight into Stephen's life and career, together with the evolution of Britain's railways from 1969, through to Privatisation in 1994 and up to his retirement in 2009. Printed softback copies of this 305-page book (ISBN 9798722485359) are available priced £9.99 from Amazon, together with a Kindle Edition priced £3.99.



## No model show but more steam trains planned at Corris!

After consideration of the ongoing Covid guidance the Corris Railway Society has cancelled its planned model railway exhibition, which was due to take place on 28 & 29 August at Y Plas in Machynlleth. However, steam trains will continue to operate from Corris Station as advertised, together with additional operating dates on Saturdays 7, 14, 21 & 28 August – plus the Bank Holiday Monday on 30 August. Trains will leave Corris station at 1100, 1300 and 1500 and tickets must be pre-booked via:

[www.corris.co.uk](http://www.corris.co.uk)

# Dapol shows progress with Class 59 in N

Dapol has advised that it has revived its plans to develop a Class 59 in N with the publication of images from the CAD artwork, as illustrated here. Plans for 2mm versions of these prototypes were originally proposed by the manufacturer in our May 2012 issue.

The Class 59 was the first US produced, privately-owned locomotive to operate regularly in the UK. The first four class members were commissioned by the Somerset-based quarrying company Foster Yeoman. Built by General Motors Electromotive Division, Amey Roadstone Corp. (ARC) ordered a further four modified Class 59/1 locomotives, whilst National Power ordered six designated as 59/2s. The class formed the basis for the subsequent GM locomotive design – the Class 66.

Tooling will cater for all three sub-classes, with the models incorporating, a five-pole motor with all-wheel pickups, diecast chassis, NEM couplings, operating front/rear lights (independent operation for DCC, switched for 12V dc use) and switchable cab lighting. Etched nameplates and bufferbeam details will also be provided in an accessory pack.

Six liveries/identities are planned for the first batch of releases; 59 005 *Kenneth J Painter* in Foster Yeoman silver, 59 103 *Village of Mells* in ARC yellow, 59 206 *John F Yeoman* in DB Schenker red, 59 204 in National Power blue, 59 104 *Village of Great Elm* in Hanson blue/grey and 59 001 *Yeoman Endeavour* in Aggregate Industries livery – each with an expected retail price of £160.00 DCC ready, £190.00 DCC fitted and £270.00 for DCC sound

N gauge Class 59 CAD image.



Class 66 tooling samples in O.

fitted versions (only available direct via the Dapol website), with expected delivery during 2022.

### O gauge Class 66 update

The Chirk-based manufacturer has received the first tooling shots for its forthcoming Class 66 diesel-electric locomotive in O gauge, plans for which were first announced in our December 2019 issue. Tooling will cater for a multitude of UK and European variations, with two variants of the body shell illustrated here.

Equipped with two motors, there will be DCC ready (21-pin), DCC-fitted and DCC Sound-fitted options available, priced £325.00, £355.00 and £475.00 respectively. Six liveries/identities are planned for the first batch



Manor decorated samples in OO.

## Owen Rees Gibbon

It was with great sadness that we learnt that Owen Rees Gibbon passed away peacefully on 2 July 2021 at the age of 67. Owen was known to many prototype and model railway enthusiasts due to his involvement in numerous clubs and societies reflecting his interest in the railways of South Wales, the London & North Western Railway, and model railways in gauge O and larger.

At the time of his passing, Owen was managing director of the Association of Larger Scale Railway Modellers, and exhibition manager for the annual show held at STEAM, Museum of the GWR in Swindon.

Owen was born on 3 December 1953 in Neath, South Wales, and apart from four of his early years, spent his life in the South Wales valleys. He joined the police force but was unfortunately invalided out at a relatively young age. At this time Owen was involved in building and exhibiting layouts as a member of the North Gwent Group.

After several short-term occupations, Owen immersed himself in the model railway world, building layouts with his brother John in gauges O and 1, all of which have graced the model railway exhibition circuit, and have appeared in the model railway press. He also assisted and built layouts for the group at Tardebigge, in O and LGB. He was a competent and prolific builder of locos and stock, both for himself and others, in scales from 4mm to 5" gauge, although he was primarily an O gauge modeller. The layout for which he is best remembered (built with John) is *Ynysyawl Fach* – a superb rendition of the mining valleys of South Wales, capturing the atmosphere of the area around the turn of the 20th Century.

As well as the Association, Reading, and STEAM shows, Owen was often seen helping at numerous model railway events around the country, and many will have met him. He had an infectious enthusiasm for the subjects that

of releases – 66 001 in EWS livery, 66 504 in Freightliner Powerhaul livery, 66 709 in GBRf Sorrento MSC livery, 66 421 in DRS livery, 66 789 *British Rail 1948 – 1997* in GBRf BR blue large logo livery, and 66 783 *The Flying Dustman* in GBRf Biffa red livery.

A number of retailer commissions are also planned; 66 779 *Evening Star* in GBRf BR green (exclusive to Rails of Sheffield), 66 009 in DB Cargo Red (Gaugemaster exclusive), 66 720 in GBRf 'Rainbow' livery (KMS Railtech exclusive), 66 036 in Euro Cargo Rail/ex-EWS livery (European retailer Chrezo Sarl exclusive), and 66 111 *Highland Stag* in EWS livery (Flangeway exclusive). Refer to the retailers' respective websites for further details.

Dapol expects to receive a working engineering prototype for the model later this summer, with delivery anticipated for summer 2022.

### Decorated Manor samples

Dapol has also received factory decorated samples of its forthcoming Collett Manor in OO, plans for which were first outlined in the news pages of our May 2020 issue.

Eight liveries versions are planned priced £159.95 DCC ready, £185.95 DCC fitted and £259.95 sound fitted (only available direct via the Dapol website). It is expected that delivery will be towards the end of 2021.

See the manufacturer's website for further information about all the models referred to here:

[www.dapol.co.uk](http://www.dapol.co.uk)

interested him, and often drew other, perhaps unsuspecting, individuals into his fold. Very few will have had cause to complain at this, as many persons' lives have been enriched as a result. Owen was always prepared to share his experiences with others and to assist modellers as required.

Away from railways, Owen was a passionate supporter of Welsh rugby, and had played for the Gwent Police rugby team. In more recent years, he had taken a keen interest in cricket, supporting the Glamorgan and England teams with John, and more often than not, accompanied by a group of model railway friends.

With Owen's passing our hobby has lost a great ambassador and friend. His presence will be greatly missed. We send our sincere condolences to his brother John.

*Ed – Our thanks to Paul Jones, Chairman of Warley Model Railway Club, for preparing this obituary.*



## Peco model of Castle Cary station helps to tell the story of town's history

Peco was recently invited to provide a model of Castle Cary railway station for display as part of an exhibition organised by the Cary History Society. The 'Cary21' event, which opened on 24 July and runs until 5 September 2021, celebrates the history of the South Somerset market town in 21 objects – the station being No.17.

A small 4mm scale diorama was prepared by Peco Modelmaker Andrew Beard and Peco Southern Area Sales Representative Andy Page, constructed using the



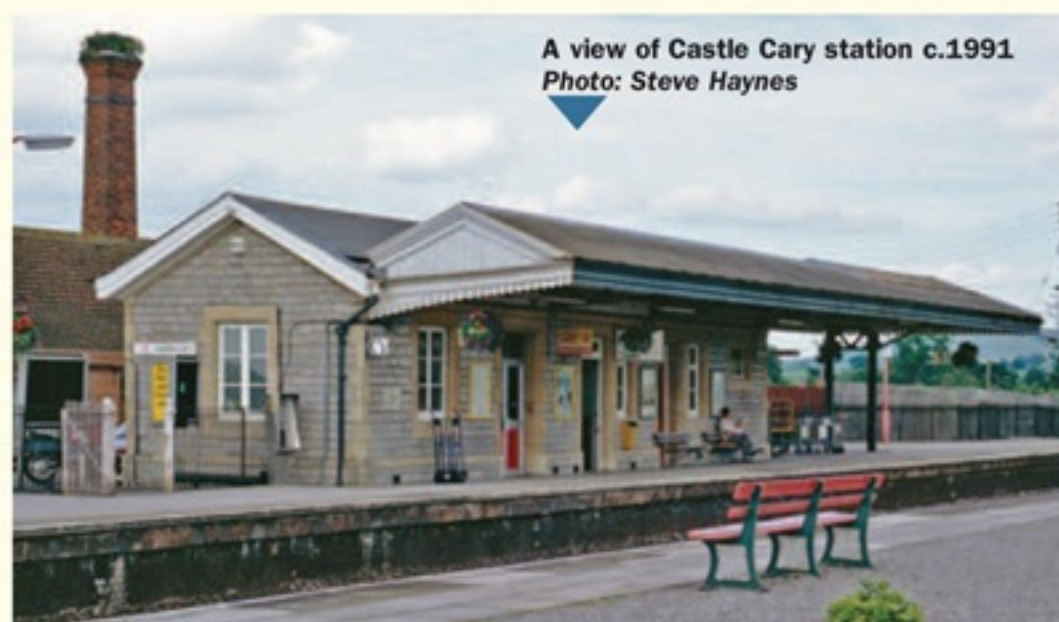
▲ The completed diorama, which in addition to the station building includes representations of the platforms and running lines, is seen on display in the exhibition following its presentation to representatives of the Cary History Society. Peco Southern Area Sales Representative Andy Page and CHS Secretary Pek Peppin are pictured. Photo: Cary History Society



Ratio kit ref.504 (above), which is based on the GWR stone-built station building at Castle Cary.

Taking place in the town's Assembly and Billiards Rooms (at the top of the Market House), the exhibition is open Monday to Saturday 1000 – 1600 and Sunday 1500 – 1700. For further details about the event and the Society's activities see the website:

[www.caryhistory.com](http://www.caryhistory.com)



A view of Castle Cary station c.1991  
Photo: Steve Haynes

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## Another helping of 'Hoovers' from Heljan

Heljan has confirmed production of a third run of its O gauge Class 50 model (see our review of the initial releases in our February 2020 issue).

Four new versions have been confirmed comprising 50 008 *Thunderer* in Hanson & Hall grey, 50 046 *Ajax* in BR large logo blue with black roof, 50 032 *Courageous* in Network SouthEast (weathered), and 50 033 *Glorious* in revised Network SouthEast dark blue.

Re-runs of three popular versions are planned; BR large logo blue with grey roof (unnumbered), BR large logo blue with black roof (unnumbered), and

50 036 *Victorious* in weathered BR large logo blue.

Prices have been held at £699.00 for pristine and £729.00 for weathered examples. Delivery of the models is expected later in 2021.

### BR Mk.I CCT tooling sample

Heljan has shown tooling samples for its forthcoming suite of BR Mk.I CCT vans in O gauge, plans for which were first announced in the news pages of our October 2019 issue.

The models will feature sprung buffers, screw-link couplings, factory

fitted bufferbeam pipework and compensated axles.

Nine livery versions are planned for the first series of releases including BR maroon, BR blue, Breakdown Train Unit (BTU) yellow, Engineering Olive and Tartan Arrow red & white.

It should be noted that the sample pictured is a hand assembled prototype and not fully representative of the finished models.

A number of issues have been identified for correction and/or modification before the project is signed off for production. Heljan advises that

the models are scheduled for release during the first quarter of 2022.

### Collett Goods halted

Heljan has taken the decision to halt progress on its O gauge GWR '2251' Collett Goods 0-6-0 project (plans for which were first announced in our January 2020 issue). The manufacturer advises that this is to assist with the completion and delivery of other products.

For further details on all of the aforementioned projects, go to:

[www.heljan.co.uk](http://www.heljan.co.uk)



▲ 50 008 *Thunderer* in H&H grey livery. Photo: courtesy Hanson & Hall Rail Services



▲ A tooling sample for the forthcoming BR Mk.I CCT in O.

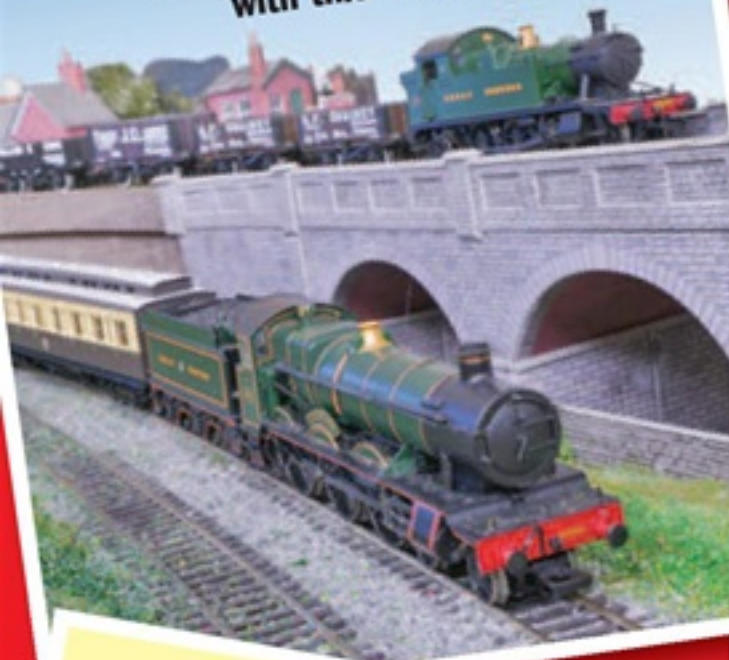


# Coming next month

Oct. 2021–

Out Thursday 9 Sep.

**A Western Odyssey**  
Through the Railway Room Door  
with this GWR medley in OO



**Peco at 75!**  
A special booklet  
celebrating the history  
of this model railway  
manufacturer



inside ....

• **WALLERS ASH**  
A tale of an outdoor railway in Gauge 1

• **CEMAES BAY**  
A North Wales coastal terminus in N

• **MODELLING ROCK FACES**  
Carol Flavin describes how she created these scenic features on her OO layout

plus all the regular features ....

## Shop News

### MIB Models celebrates 20 years of trading

The family-run Bridgend-based retailer MIB Models is this year celebrating two decades of trading.

The business started 20 years ago, initially as an online trading business using well-known auction sites for selling model railway items. In April 2003, the team opened its first shop in Porthcawl, South Wales, stocking brand new products from a range of manufacturers including Peco.



In 2019, the business relocated to a significantly larger premises just off the M4 motorway near Bridgend, as well as diversifying its product ranges with the addition of war gaming accessories. An e-commerce website was launched earlier this year.

The shop is open Tuesdays and Wednesdays 1100-1800, Thursdays and Fridays 1100-1900 and Saturdays 1100-1700. For more details:

**MIB Models**

Unit 2, The Highwayman Business Park, Castle View, Bridgend, CF31 1NJ

Tel: 01656 665 511

[www.mibmodels.co.uk](http://www.mibmodels.co.uk)

## Revolution plans BR Borail flat wagons for N & OO

Revolution Trains has revealed plans to produce the BR Borail EB/EC 'fishbelly' flat wagons, and air-braked YLA Mullet, YQA Parr and YQA Super Trench derivatives, as its next wagon project in both N and OO.

A total of 165 Borail flat wagons were constructed by British Railways from 1959 for carrying concrete beams, girders, lengths of rail and other heavy loads. The wagons had 8' plateback bogies and vacuum brakes, and solid fishbelly underframes, rather than traditional trusses, for strength. Examples were built with bolsters (classified as Borail B) and without (Borail C).

In 1981 most of the fleet was rebuilt with air brakes, new Y25 bogies, revised decks with tensioning ratchets and recoded BRA under TOPS. After a short period on steel traffic in the Speedlink network, most were transferred to the civil engineers and given new TOPS codes and names YLA Mullet (with bolsters) and YQA Parr (with longitudinal baulks for sleepers).

In 2010 some YQA Parr wagons were refurbished and fitted with ISO modules with drop-down side mesh doors. They retained the YQA code but were renamed Super Trench. Many remain in service in 2021, 60 years after they were built.

The Revolution Trains models will cover all variants and tooling allows for numerous detail differences and prototypical variations of buffers and brake wheels to enable specific wagons to be represented accurately.

N gauge models are to be sold as twin-packs, priced £70.00. The price for OO versions is TBC. Delivery for both projects is expected during the first half of 2022.

#### Samples of Mk.V & Mk.Va stock

Tooling samples have been received for the company's forthcoming N gauge Mk.V Caledonian Sleeper and Mk.Va TransPennine Express stock, plans for which were first outlined in our April 2019 issue.

The models will feature full interiors, switchable internal and tail lighting and external detailing including photo etched grilles on the equipment boxes.

Eight packs of the Caledonian Sleeper cars are planned, which allow the accurate formations of



▲ CAD image of BR Borail EB.



▲ CAD image of YQA Super Trench.

both the Highlander and Lowlander sets. Each book set will consist of four coaches, priced at £180.00, with the exception of Highlander Pack 1 (Aberdeen), which consists of six coaches, priced £270.00.

TransPennine Express sets are to be offered in complete five-car train packs, with two different running number sets planned, each priced £225.00.

#### Class B 35T tank wagons

Revolution Trains has also announced a re-run of its N gauge Class B 35T tank wagons (see reviews RM May 2018). The new batch will cover the same livery selection to that of the original run, but featuring all new running numbers to allow modellers to add to their rakes. These models will only be available to pre-order through Revolution approved retailers. Prices are £26.95 each.

For full details of all Revolution Trains products, including details of how to order and a list of stockists, go to: [www.revolutiontrains.com](http://www.revolutiontrains.com)



▲ Tooling samples of Mk.Va TPE stock in N.



# Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling to a listed live event should contact the organiser first. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and notifications via email to [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com) is required six weeks prior to publication date. Entries must include the organiser's email or website and/or a telephone number.

**NOVEMBER 2021 issue deadline: SEPTEMBER 1st**

## CANCELLATIONS

### Saturday 21 August LUDLOW, Shropshire

**Organiser:** Craven Arms & District MRC  
**Venue:** Ludlow Racecourse, Bromfield, Ludlow, Shropshire SY8 2BT.  
**Contact:** David Gotliffe 07867 752301  
In the light of the rapidly-increasing infection rate the CA&DMRC committee has concluded that it would now be unwise and unsafe to hold our exhibition this year.

## LIVE EVENTS

The following live events have been notified to us, but are subject to all changes in Government restrictions which may alter without notice. Visitors should check with the organisers before setting out to the venue.

### Saturday 14 August

**BEXHILL-ON-SEA, East Sussex**  
**Organiser:** Bexhill Model Railway Club  
**Venue:** St Richard's Catholic College, Ashdown Road, Bexhill-on-Sea TN40 1SE.  
**Open:** 1000 – 1700  
**Admission:** Adults £5.00, Children £2.00, Family ticket £12.00  
**Features:** Annual Model Railway Exhibition. Layouts, trade stands, wheelchair access, refreshments. Free parking on site  
**Contact:** 01424 576406  
**Website:** [www.bexhillmrc.org.uk](http://www.bexhillmrc.org.uk)

### Sunday 15 August

**FARNHAM, Surrey**  
**Organiser:** 009 Group of the Farnham and District MRC.  
**Venue:** Wrecclesham Community Centre, Farnham, Surrey GU9 8TJ.  
**Open:** 1000 – 1600  
**Admission:** £3.00 adult, children free.  
**Features:** Seven confirmed guest layouts, two of them new to the 009 circuit, in addition to the hosting group's new layout *Straight and Narrow*. The Open Day will be supported by the 009 Society sales stand. Light refreshments available.  
**Contact:** David Harrington, [009@farnhammrc.org.uk](mailto:009@farnhammrc.org.uk)  
**Website:** [farnhammrc.org.uk/?page\\_id=4013](http://farnhammrc.org.uk/?page_id=4013)

### Saturday 21 August

**GUILDFORD, Surrey**  
**Organiser:** Astolat Model Railway Circle  
**Venue:** National Trust, Dapdune Wharf, Wharf Road, Guildford, GU1 4RR  
**Open:** 1000 – 1700  
**Admission:** Free entry to National Trust Members. If not a member of the NT then NT entry cost is applicable, go to: [www.nationaltrust.org.uk/river-vey-and-godalming-navigations-and-dapdune-wharf](http://www.nationaltrust.org.uk/river-vey-and-godalming-navigations-and-dapdune-wharf) for current entry fees.  
**Features:** Club open day – club and members layouts including our new 24' 00 layout of Savernake, Wiltshire. Also our N/00 test tracks available, so bring your own locos to run, plus second-hand sales.  
**Contact:** David Wilde on 01483 722098 [davidwilde155@msn.com](mailto:davidwilde155@msn.com).  
**Website:** [www.astolatmrc.co.uk](http://www.astolatmrc.co.uk)

### Saturday 21 August

**NORTHAMPTON, Northamptonshire**  
**Organiser:** Northampton & District MRC  
**Venue:** Roade Village Hall, Bailey Brooks Road, Roade, Northampton NN7 2LS  
**Open:** 1000 – 1600  
**Admission:** Adult £5 Child £2 Family (2+2) £10  
**Amenities:** On bus routes Northampton to Milton Keynes (X6/33) free parking, disabled access.  
**Features:** All popular gauges, Trade support  
**Contact:** Richard Deacon 01604 890275  
**Website:** [www.ndmrc.org](http://www.ndmrc.org)

### Saturday 21 and Sunday 22 August

**CARNFORTH, Lancashire**  
**Organiser:** Carnforth Heritage Centre  
**Venue:** Carnforth Railway Station  
**Open:** 1000 – 1600  
**Admission:** Donation, suggested £2.00 adult. All proceeds to Carnforth Station Trust.  
**Features:** At least seven layouts, including Museum of Transport (OO) and brand new Eastgate Harbour (SR 00). Trade support on the covered platform. Carnforth Models open all weekend. Trains stop at Carnforth from Preston, Manchester, Barrow, Leeds.  
**Contact:** R Brogden, 07967 743126.  
**Website:** [www.carnforthstation.co.uk](http://www.carnforthstation.co.uk)

### Saturday 28 August

**LONGBRIDGE, Birmingham West Midlands**  
**Organiser:** Bournville Model Railway Club  
**Venue:** The Austin Sports and Social Club, 30 Tessall Lane, Longbridge, Birmingham B31 2SF  
**Open:** 1000 – 1630  
**Admission:** Adults £5, Concessions £4, Family £9, children under 16 free when accompanied by an adult  
**Amenities:** Excellent free parking and public transport. Hot & cold food and a public bar.  
**Contact:** 07875 179470  
**Website:** [www.bournvillemrc.co.uk](http://www.bournvillemrc.co.uk)

### Sunday 29 August

**BROXBORNE, Hertfordshire**  
**Organiser:** Hoddesdon Model & Railway Club  
**Venue:** The Club Site; The Old Mill, Mill Lane, Broxbourne, Herts. EN10 7AX  
**Open:** 1000 – 1600  
**Admission:** Adults £3.00 Children £1.00.  
**Amenities:** Parking for disabled adjacent to club site. Free parking for other users off Nazeing New Road (B194), at EN10 6TD. Follow the footpath from the car park, under the railway line to the club site. The club premises is a five-minute walk from Broxbourne station.  
**Features:** Club summer show – Working Model Railway Layouts 'G' Scale, 'O' Gauge, 'OO' Gauge and 'N' Gauge. Model Boats in Pool, Train Rides for Children. Radio Controlled Model Trucks. Sales Stand, Tombola and Refreshments. Trade Supported.  
**Website:** [www.hoddesdonmrc.org.uk](http://www.hoddesdonmrc.org.uk)

### Saturday 4 September

**TADWORTH, Surrey**  
**Organiser:** North Downs Model Railway Circle  
**Venue:** The Good Shepherd Church Hall, Station Approach Road, Tadworth, Surrey KT20 5AE  
**Open:** 1000 – 1700  
**Admission:** Adult £6 under 16s £3 Concession (Senior/Disabled) £5. In aid of the Good Shepherd Church.  
**Amenities:** Free parking, venue is close to

station and bus stop, refreshments, facilities for disabled visitors.

**Features:** 26 layouts, demonstration, and trade support

**Contact:** Stuart Robinson 077-5845-8827 & [stuart.robinson27@outlook.com](mailto:stuart.robinson27@outlook.com)

**Website:** [www.ndmrc.info/Home/Tadworth.htm](http://www.ndmrc.info/Home/Tadworth.htm)

### Saturday 4 September WORKSOP, Nottinghamshire

**Organiser:** The Model Bus Federation  
**Venue:** The Crossing Church, Newcastle Street, Worksop, S80 2AT  
**Open:** 1000 – 1600  
**Admission:** Adult £4 Child £1 MBF Mem. £2  
**Amenities:** Refreshments.  
**Features:** Model railway, bus and tram layouts, static dioramas, local sales stalls  
**Contact:** Bob Heathcote: [busbob64@gmail.com](mailto:busbob64@gmail.com)

### Saturday 4 and Sunday 5 September LIVERPOOL, Merseyside

**Organiser:** Liverpool Model Railway Society  
**Venue:** Old Christ Church, Waterloo Road, Liverpool L22 1RE  
**Open:** Sat: 1000 – 1700. Sun: 1000 – 1630  
**Admission:** Adult £5 Child £2 (under 5 free) Family £12 (2 + 2)  
**Amenities:** Refreshments, free on street parking, mobility impaired access.  
**Features:** 16 layouts in various gauges, trade support and shows-you-how.  
**Contact:** Bill Follett: 07764 942031  
**Website:** [www.lmrs.org.uk](http://www.lmrs.org.uk)

### Saturday 4 and Sunday 5 September ALNWICK, Northumberland

**Organiser:** Aln Valley Railway trust  
**Venue:** Lionheart Station, Lionheart Enterprise Park, Alnwick, Northumberland, NE66 2EZ  
**Open:** Sat: 1030 – 1600  
**Admission:** TBA  
**Amenities:** Standard gauge steam trains will be running and the railway's cafe will be open for refreshments.

### Saturday 11 September LEICESTER, Leicestershire

**Organiser:** All Trains Great and Small  
**Venue:** Methodist Church Halls, Narborough Road, Leicester, LE3 2RD  
**Open:** Sat: 1030 – 1700  
**Admission:** Adult £3 Under 14s free  
**Amenities:** Hot and cold Refreshments  
**Features:** Several layouts in a mix of scales and gauges, traders, bring and buy stall.  
**Contact:** Bryan Pentland 07801 818764 [bryanpentland@hotmail.co.uk](mailto:bryanpentland@hotmail.co.uk)  
**Website:** [www.traincollectors.co.uk](http://www.traincollectors.co.uk)

### Saturday 11 and Sunday 12 September LEAMINGTON SPA, Warwickshire

**Organiser:** Meridienne Exhibitions Ltd  
**Venue:** Warwickshire Event Centre, Nr Leamington Spa. On the Junction of the A425/B4455, Visitor Car Park CV31 1FE  
**Open:** Sat: 1000 – 1700. Sun: 1000 – 1600  
**Admission:** Adult £11.00 Child (5-14 inc) £5.50 Senior Citizen £10.50 ALL TICKETS MUST BE PURCHASED IN ADVANCE  
**Amenities:** Hot and cold Refreshments  
**Features:** The International N Gauge Show (TINGS) 40 specialist suppliers and over 25 layouts showcasing British, American and Continental railways, plus displays, demonstrations and five leading N Gauge societies  
**Contact:** 01926 614101  
**Website:** [www.ngaugeshow.co.uk](http://www.ngaugeshow.co.uk)

### Saturday 18 September LICHFIELD, Staffordshire

**Organiser:** Trent Valley Model Railway Society  
**Venue:** The Life Church, Lichfield, WS13 6TS  
**Open:** 1000 – 1630  
**Admission:** Adult £5 Child £3 Family (2+2) £13  
**Amenities:** Parking and disabled access, refreshments.  
**Features:** 10+ layouts and trade support.  
**Contact:** Michael Chapling 0121 329 2487  
**Website:** [www.tvrms.co.uk](http://www.tvrms.co.uk)

### Saturday 25 September PITSTONE, Bedfordshire

**Organiser:** Tring & District MRC  
**Venue:** Pitstone Memorial Hall, Vicarage Road,

Pitstone, LU7 9EY.

**Open:** 1015 – 1630

**Admission:** Adult £3 Child £1.50 Family (2+2) £7

**Amenities:** Refreshments, access for disabled visitors, free car parking

**Features:** Eight layouts from N to G scale, preservation, and trade

**Contact:** 01525 850703 or [r.longman@btinternet.com](mailto:r.longman@btinternet.com)

**Website:** [www.tringandaylesburymrc.co.uk](http://www.tringandaylesburymrc.co.uk)

### Saturday 25 and Sunday 26 September BOSTON, Lincolnshire

**Organiser:** Boston Model Railway Society  
**Venue:** Stickney Village Hall, seven miles North of Boston on the A16 PE22 8BG  
**Open:** Sat: 1000 – 1700. Sun: 1000 – 1600  
**Admission:** Adult £3 Child £1 Family (2+2) £7  
**Amenities:** Refreshments, free parking, easy access all on one level.  
**Features:** Seven layouts plus trade  
**Contact:** David Currie 07958 137122 [djcurrie@live.com](mailto:djcurrie@live.com)

## ONLINE EVENTS

We have not been notified of any planned on-line events

## GAUGE O GUILD

### Thursday 26 August 2021

**Organiser:** Gauge O Guild via Zoom  
**Website:** [www.gaugeoguild.com](http://www.gaugeoguild.com)  
**Open:** Starts 2000hrs BST  
**Admission:** Free to Gauge O Guild members, Non-members £2.00. Booking is essential as places are limited. Booking opens 27 July 2021. Visit the front page of the Gauge O Guild website to secure your place.  
**Features:** An Evening with Ellis Clark  
Ready to Run - from an idea to the first sale.  
**Contact:** email: [jackiegogevents@gmail.com](mailto:jackiegogevents@gmail.com)

### Sunday 26 September 2021

**Organiser:** Gauge O Guild via Zoom  
**Website:** [www.gaugeoguild.com](http://www.gaugeoguild.com)  
**Open:** Starts 2000hrs BST  
**Admission:** Free to Gauge O Guild members, Non-members £2.00. Booking is essential as places are limited. Booking opens 27 August. Visit the front page of the Gauge O Guild website to secure your place.  
**Features:** An Evening With Pete Scarborough  
A personal experience of building and running wagons.  
**Contact:** email: [jackiegogevents@gmail.com](mailto:jackiegogevents@gmail.com)

## MEETINGS

### Tuesday 14 September, 1930hrs

**Wells Railway Fraternity**  
Railways in Art presented by Eric Bottomley.  
**Venue:** Wells Town Hall, Wells, Somerset  
**Contact:** Andrew Tucker 01749 830695

### Tuesday 12 October, 1930hrs

**Wells Railway Fraternity**  
More Railway Tales by David Hartland.  
**Venue:** Wells Town Hall, Wells, Somerset  
**Contact:** Andrew Tucker 01749 830695

### Tuesday 9 November, 1930hrs

**Wells Railway Fraternity**  
My life in Transport by Jonathan Jones-Pratt  
**Venue:** Wells Town Hall, Wells, Somerset  
**Contact:** Andrew Tucker 01749 830695

### Tuesday 14 December, 1930hrs

**Wells Railway Fraternity**  
Quiz night with Jim Allwood, and AGM  
**Venue:** Wells Town Hall, Wells, Somerset  
**Contact:** Andrew Tucker 01749 830695

Planning a model railway event – live or online? To get listed please send details to: [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com)



# RAILWAY MODELLER

## Opportunity Yours

### Classified Advertisements

TO BUY, SELL OR EXCHANGE THROUGH  
THE COLUMNS OF GREAT BRITAIN'S  
BEST SELLING MODEL RAILWAY MAGAZINE

See order form on page 40a for advertising rates and conditions  
If, when responding to advertisements, you have a query which needs a reply, please enclose a stamped, self-addressed envelope.  
Replies to box numbers should be sent to:  
Peco Publications, Beer, Seaton, Devon EX12 3NA

## Trade

### Retailers' Announcements

**YORK - MONK BAR MODEL SHOP LTD.** 2 Goodramgate (by Monk Bar). Large selection of new model railways, Scalextric, plastic kits, diecasts, etc. Tel: 01904 659 423.

**HULL - EAST YORKSHIRE'S ONLY SPECIALIST** model railway shop. Model Railways bought, sold and exchanged. 53A MODELS, 430 Hessle Road, Hull. (Closed Mondays). [www.53amodels.co.uk](http://www.53amodels.co.uk). Tel: 01482 227 777.

**COLWYN BAY, CLWYD MODELS.** Opening times: Thursday, Friday + Saturday 10-1pm, 2-5pm. Second hand model railway books, videos, layouts, diecast bought, sold and exchanged. 447, Abergele Road, Old Colwyn. Tel: 01492 518 709.

**PLUS DAUGHTERS OF BASILDON ESSEX.** N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. PLUS DAUGHTERS, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon, Essex, SS13 1EU. Tel: 01268 726 211 [www.plusdaughters.co.uk](http://www.plusdaughters.co.uk)

**MODEL EXCHANGE.** Tel: 07580 065 017.  
Website: [www.themodelexchange.co.uk](http://www.themodelexchange.co.uk)  
BUY\*EXCHANGE\*SELL TOYS.

**DINGWALL (ROSS-SHIRE)-SPORTS & MODEL SHOP,** 66 High Street, Dingwall, IV15 9RY. PLASTICS-METALS-SCENICS. OO and N scale, PECO, Hornby, Bachmann, Metcalfe, Wills, Ratio & Dapol. Tel: 01349 862 346.

**SWANAGE MODEL RAILROADING CENTRE - N GAUGE TO G SCALE.** Good selection of Preowned models in N, OO and O gauge. Open Wednesday, Thursday and Saturday 9.30 to 2.30pm. Other times by appointment please call John on 07956 973 072.

### Trade Sales Products

**O GAUGE ETCHED KITS.** Locomotives, Carriages and Wagons. Connoisseur Models on line catalogue, [www.jimmcgeown.com](http://www.jimmcgeown.com) or send 2nd class address label for free printed catalogue. 1 Newton Cottages, Nr Weobley, Herefordshire, HR4 8QX.

**LOCOMOTECH MOTORIZING KITS FOR PECO AND SOUTH EASTERN FINECAST TURNTABLES.** Standard (3.5rpm) £27.99, Deluxe (2 rpm) £34.99, U.K. postage £3.25. 12V motor. Enclosed all-metal gearbox. Automatic Indexing - stops automatically at each exit. (Currently only for PECO LK-55/LK-555) 10 Exit version £64.99, NEW 32 Exit Roundhouse Version £67.99. Reviewed by Railway Modeller May 2016. More information and order online at [www.locomotech.co.uk](http://www.locomotech.co.uk) Telephone orders: 01903 871149.

**NEW WEBSITE:** [www.zetlandmodelrailways.co.uk](http://www.zetlandmodelrailways.co.uk)  
Good quality second-hand North American, Continental & British, "O", "HO", "OO", "N" & "Z" scales. Email: [Bob@zetlandmodelrailways.co.uk](mailto:Bob@zetlandmodelrailways.co.uk)

**OVER 1000 OO GAUGE LOCO, COACH AND WAGON KITS,** many no longer in production. Etched brass, white metal and plastic kits from over 20 Manufacturers and covering various Railway Companies. Also hundreds of items by Hornby, Bachmann, Dapol and others. Tel: 01444 413723 Email: [andrew@wmcollectables.co.uk](mailto:andrew@wmcollectables.co.uk) Website: [www.wmcollectables.co.uk](http://www.wmcollectables.co.uk)

### Trade Sales Books

**RAILWAY MODELLER BACK NUMBERS** - for more information on which issues are available, please contact: Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580. Please see website for prices.

### Trade Services

**WHITEMETAL AND BRASS KIT BUILDING SERVICE, QUALITY LOCOMOTIVES AT REALISTIC PRICES.** For costs plus current delivery dates, please phone 01325 382 452 or write to: DAVID TEMPLE, 28 Walworth Crescent, Darlington, County Durham, DL3 0TX.

**CUSTOM AND STANDARD DECALS. PLASTIC AND METAL KIT BUILDING,** modification and scratch building service. Locos, Rolling stock, track, structures and scenic items. Any scale. See [www.aardstorm-models.com](http://www.aardstorm-models.com)

**KIT BUILDING SERVICE, LOCOS, ROLLING STOCK, QUALITY WORK AT REASONABLE PRICES.** Contact: JOHN NOWELL, 28 Victoria Road, Tuebrook, Liverpool, L13 8AW. Tel: 0151 259 5957, email: [johnnowell@blueyonder.co.uk](mailto:johnnowell@blueyonder.co.uk) [www.tuebrooktrains.co.uk](http://www.tuebrooktrains.co.uk)

**BASEBOARDS BUILT IN TOP QUALITY PLY.** Also trestles and legs built. Friendly help & advice available. Delivery & Assembly possible. Based in Hampshire. Phone Phil 07305 632 546.

**HATCHER SCALE MODELS HAS 35 YEARS EXPERIENCE MAKING QUALITY BUILDINGS** and marine models to museum standard. We can build to your specification from scratch or kit. [www.hatcherscalemodels.co.uk](http://www.hatcherscalemodels.co.uk) [hatcherscalemodels@hotmail.com](mailto:hatcherscalemodels@hotmail.com)

### Wanted (Trade)

This section is reserved for the use of traders only.

**WE VALUE YOUR USED MODEL RAILWAYS** very highly. Best prices paid for good condition modern OO and N Gauge second hand items. Send us your list of surplus items in any gauge or make (s.a.e. please) for

our quotation by return post. We also buy tinplate of all kinds, Dinky Toys, Corgis and Lesneys. We will travel anywhere to collect. RAILWAY ROUNDABOUT, The Walnut Tree, Egremont Street, Glemsford, Suffolk, CO10 7SA. Tel: 01787 280 452, email: [bill.railwayroundabout@gmail.com](mailto:bill.railwayroundabout@gmail.com)

**MODEL RAILWAYS WANTED** - cash waiting for British & American N & OO/HO scales plus Continental HO model railway equipment. British and American O-Scale also considered along with interesting items of railwayana. (Closed Mondays). 53A Models, 430 Hessle Road, Hull. 01482 227 777. [www.53amodels.co.uk](http://www.53amodels.co.uk)

**AIRFIX/FROG/REVELL** and other makes of unmade Second-hand plastic kits wanted. Aircraft especially wanted. Any quantity. KINGKIT, Unit 8, Cedar Court, Halesfield 17, Telford, TF7 4PF. Tel: 01952 586 457.

**WANTED - TRAIN COLLECTIONS O, OO, N.** Always calling in areas SOMERSET, DEVON, CORNWALL, S-WALES, WILTS, HAMPSHIRE, most places in UK covered. Will dismantle layouts any size. Interested in British N gauge Farish, Mintrix, Peco, OO Bachmann, Hornby, Mainline, Lima, Wrenn, Tri-ang. Damaged, scrap locos, Peco points, track work, buildings, cars, people, books, diecast, EFE buses. Cash paid, friendly service offered. R.FOSTER 15 Rosebery Avenue, Yeovil, Somerset BA21 5LW. Tel 01935 424165 email [russelltrains@aol.com](mailto:russelltrains@aol.com)

**MODEL RAILWAYS ALWAYS WANTED,** including Hornby, Lima, Bachmann, Dapol, Modern Image, OO, O, N gauge, kits and kit built. Send lists or give me a call. BARRY JONES, 28 Marine Crescent, Worthing, BN12 4JF. Tel/Fax: 01903 244 655.

**MODEL RAILWAYS WANTED** - All Gauges including N, OO, O Gauge. Large collections to surplus items. British/American and Continental boxed or unboxed, no problem. Spares and track also purchased. We will also buy any diecast. Send list or telephone: MJH MODELS, S'Agaro, Goring Road, Woodcote, Reading, RG8 0QE. 01491 681 262 email: [mjhtoy@aol.com](mailto:mjhtoy@aol.com)

**WE CARE A GREAT DEAL MORE!** We realise that your collection large or small is a prized possession, so we will always give our highest price, we will respond quickly to receiving your list and collect, dismantle and remove your items efficiently because we care. Whatever you have to sell from single items to large collections. Give us a call and prepare to be amazed! S&J MODELS. Tel: 01606 872 786. Email: [sjmodels@tiscali.co.uk](mailto:sjmodels@tiscali.co.uk) Website: [www.sandjmodelrailways.co.uk](http://www.sandjmodelrailways.co.uk)

**THINKING OF SELLING YOUR MODEL RAILWAY COLLECTION?** If so contact me for an immediate evaluation, distance no object, collections large or small. PAUL CULL, telephone: 01302 481 274 or mobile: 07767 356 890 or send list to: The Bungalow, Dentons Green Lane, Kirk Sandal, Doncaster, DN3 1JP, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)



**HORNBY & BASSETT-LOWKE O GAUGE TRAINS**, wagons, coaches and accessories. Anything considered. Top prices paid for single items and/or complete collections. Tel: 01993 840 064 (Oxon) 0797 991 0760.

**ALWAYS WANTED BY TRI-ANGMAN**-collections, layouts, seeking, Tri-ang, Hornby, Dublo, Wrenn, Bachmann etc. Will collect across most of the UK. More than fair prices paid. Tel: 07966 333 605. Email: [Laurence@tri-angman.co.uk](mailto:Laurence@tri-angman.co.uk) [www.tri-angman.co.uk](http://www.tri-angman.co.uk)

**WANTED-MODEL RAILWAY ITEMS, FROM ANYWHERE IN THE UK**, single items to whole collections, any gauge, we pay cash and will beat any genuine offer on collections. We also buy diecast models + vintage toys. Tel: 01209 718 562 or 07876 457 114, email: [harvey.instance@sky.com](mailto:harvey.instance@sky.com)

**MODEL RAILWAY COLLECTIONS URGENTLY REQUIRED - SMALL & LARGE**, any scale and any condition. British, European and American collections all required. Nationwide collections, distances no object. Contact us today for a quick and friendly service. Telephone: 01302 371 623, mobile: 07526 768 178, email: [anoraksanonymous@googlemail.com](mailto:anoraksanonymous@googlemail.com)

**WANTED WANTED MODEL RAILWAYS, ALL MAKES, ALL SIZES** from N Gauge, 00 scale to G Scale and larger, will travel all UK. We also dismantle layouts, over 35 year's experience. Send lists to **DEREK BARNETT**, 6 Vale Gardens, Penkridge, Staffs, ST19 5LQ. Tel: 01562 750 076 daytime, or text me 07721 333 521 anytime and I will ring you back, email [footplate@btconnect.com](mailto:footplate@btconnect.com)

**WANTED TOP PRICES PAID! BUY-SELL-EXCHANGE ANY GAUGE, MAKE OR AGE.** Also Specialists in diecast Lorries, cars, buses and railwayana. Friendly, fast, professional service from our family run business, established for over 40 years. "We are a real shop not just a phone number" Distance no object, from single items to lifetime collections, instant settlement by your preferred payment method. See our main advert. Deal with confidence from a name that you can trust. *Rails of Sheffield* 21/29 CHESTERFIELD ROAD, SHEFFIELD, S8 0RL. Tel: 01142 551 436. [www.railsofsheffield.com](http://www.railsofsheffield.com)

**WANT TO SELL YOUR TRAINS - ALL GAUGES, ALL MAKES?** Hornby/Bachmann, OO, O, and N gauge. Top cash prices paid, collected, layouts dismantled. Please send list to **CLAIRE CLARKE**: 14 Poplar Close, St Martins, Oswestry, Shropshire SY11 3QJ. Email: [claireaclarke@aol.com](mailto:claireaclarke@aol.com) telephone: 01691 772 969.

**WE BUY ALL YOUR MODEL RAILWAY COLLECTIONS - ANY GAUGE, LOCOS, ROLLING STOCK, KITS ETC.** For a fast friendly professional service telephone **VINCENT**: 07721 087 810 or email [vincelonergan@hotmail.com](mailto:vincelonergan@hotmail.com) or send your list to **VINCENT - DORSET RAILS**, 5 Conway Walk, Dorchester, Dorset, DT1 2EJ.

**WANTED - COLLECTIONS OF TRAINS FROM THESE AREAS:-** North Wales, Cheshire, Merseyside, Manchester, and Lancashire. Also Diecast. If you really want the best price then send me or email me your list. Tel: 07511 899314. Email: [gambiacat@tiscali.co.uk](mailto:gambiacat@tiscali.co.uk)

**MODEL RAILWAY AND DIE-CAST COLLECTIONS WANTED**, ex shop stock, estates etc; confidentiality assured. Please contact **UK Toy and Model Auctions** 01270 652773 or [enquiries@uktoyauctions.com](mailto:enquiries@uktoyauctions.com)

## Holiday Accommodation

**HOLIDAY LET AVAILABLE IN BEER, DEVON NEAR PECORAMA.** Portland Heights is a stunning property offering a balcony with spectacular views overlooking Lyme Bay. Sleeps 6. Available all year round. All bookings made through **JEAN BARTLETT COTTAGE HOLIDAYS**. Website: [www.jeanbartlettcottageholidays.co.uk](http://www.jeanbartlettcottageholidays.co.uk) or telephone: 01297 23221.

## Private

### Property for Sale

*Cut the expense of moving house by advertising your property in the RAILWAY MODELLER or CONTINENTAL MODELLER for just 15p a word its great value for money!!*

**A ONE BEDROOM 1ST FLOOR FLAT IDEALLY LOCATED WITHIN A QUIET RESIDENTIAL COMPLEX**, two minute walk from the much sought after bars, restaurants and shops the location has to offer. The property is well proportioned and has recently been refurbished to a high standard. Situated at the end of the block this flat benefits from large windows in every room, including new picture windows in the lounge and bedroom, providing an abundance of light. This property is beautifully presented throughout and would be well suited to a first time buyer or an investment property. The asking price includes brand new kitchen and bathroom appliances, new fitted wardrobes, share of freehold, private parking. No chain. Box No 09/21.

## Private Sales

**AN EXCEPTIONAL WORKING DIORAMA RAILWAY.** OO gauge fine scale, size 7ft x 18" and 4ft x 2ft. Can be worked as U shape or L shape. Scenic with working signal and excellent running. Photos and full spec available and cost can be discussed. Contact email [John.phillips2@yahoo.com](mailto:John.phillips2@yahoo.com)

## Wanted Clubs and Societies

**NARROW GAUGE STOCK WANTED BY THE 009 SOCIETY**, collections, loco's, rolling stock, readymade BEMO, LILLIPUT, ROCO etc or kit and scratch-built. Contact: 009 Society, 1, Corinthian Road, Chandler's Ford, Eastleigh, Hampshire, SO53 2BA. Email: [members-sales@009society.com](mailto:members-sales@009society.com). For society details see [www.009society.com](http://www.009society.com)

## Wanted Private

(This column is reserved for readers who are private Model Railway enthusiasts or collectors and require items purely for their own personal use).

**TRIX TWIN AND TRIX EXPRESS.** Avid collector. TTRCA No26, anything will be considered. Please phone anytime, will collect. Tel: 01736 754 200. Mobile: 07969 499 586.

**RAILWAY PHOTOGRAPHS**-original 35mm slides (or negatives) any railway subject, steam to blue diesel era. Good price for quality material. **JOHN TURNER**, 01482 227 777 or 01652 632 370 (evenings).

**N OR OO GAUGE**, British outline Model Railways sought by collector, Wrenn, Hornby, Bachmann, Farish etc. Will travel to collect. Tel: 01702 461 214 after seven, 07399 73 44 94, daytime. Send lists to: [psdavis17@talktalk.net](mailto:psdavis17@talktalk.net)

**HORNBY O GAUGE** LMS Princess Elizabeth & Southern L1. Both 20 volt electric. Tel: 01993 840 064 (Oxon) or 0797 991 0760.

**RAILWAY RELICS LOCOMOTIVE PLATES.** Station signs, clocks, posters, hand lamps, signalling, would collect. Top prices. Dale, East Barn, Loads Road, Holymoorside, Chesterfield, S42 7HW. Tel: 01246 569 263.

**WANTED TRIANG TT AND LONE STAR TREBLE O ELECTRIC BY PRIVATE COLLECTOR.** Good price paid, will travel. Tel: 01980 862 387 or email: [hamptonrob1951@outlook.com](mailto:hamptonrob1951@outlook.com) (Wiltshire).

**WRENN 1966 TO 1992**-items wanted by collector preferably boxed, single or complete collections. Please telephone: 0773 0957 800/01582 401 053 or visit my website [www.wrennspecialist.co.uk](http://www.wrennspecialist.co.uk)

**KIT ITEMS, WHITEMETAL, BRASS OO & O, DJH, Wills, Millholme, Alan Gibson etc.** Engines, coaches, wagons, unmade, part made, engines working or not. Kit spares - will travel. Tel: 01302 481 274, mobile: 07767 356 890, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**O GAUGE WANTED - LOCOS, ROLLING STOCK, ACCESSORIES, TRACK/POINTS, SPARES AND BUILDINGS.** Large or small collections purchased. Tel 01302 481274, Mob. 07767 356 890. Email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**G SCALE AND OTHER GARDEN RAILWAYS WANTED.** Gauge 1, SM32, Bachmann, LGB, Piko, Accucraft, Roundhouse etc. Locomotives, rolling stock, track, points and accessories all required. Tel: 01302 371623 email: [sophiecul99@gmail.com](mailto:sophiecul99@gmail.com)

**IS THERE CASH IN YOUR ATTIC?** I will collect and pay best cash prices for model railway items, from single items to complete collections. Call **GEOFF**, for a no obligation chat on: 07986 936 058 or email me on: [cash4trains@live.co.uk](mailto:cash4trains@live.co.uk)

**WANTED - MODEL RAILWAY ITEMS AND COLLECTIONS.** Yorkshire, Lincolnshire, Nottinghamshire areas. Phone Peter on 07804 267 732.

**TRI-ANG RAILWAYS PANTOGRAPHES FOR R351 EM2 AND R257 D/E DIESEL.** R310 sidewall clips R73 canopy set. R393 overhead diamonds. R391 junction links. R448 O/T coach. Telephone: 07562 277770 (Sussex).



# Swapmeets • Sales • Fairs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so. All advertisements include the organiser's town and telephone number. The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

## August 2021

**26 August (Thurs) 17.30pm to 21.30pm**  
**WELLING, KENT CHARITY TOY AND TRAIN FAIR**, Falconwood Community Centre, 32 Falconwood Parade, The Green, DA16 2PG. Adults £1, accompanied children free. In aid of the Association of Bexley Charities '78. Sponsor: CADE CONSTRUCTION: 020 8311 9020. Organiser: ALAN BONE.  
Tel: 020 8310 5018.

**28 August (Sat) 10.30am - 2pm**  
**CHESTER**, The Cheshire Country Sports Club, Plas Newton Lane (near Upton), Chester CH21 1PR (off A41). Approximately 3 minutes from junction 12 of M53 and junction 41 of A55. Admission £2. TONY OAKES FAIRS.  
Tel: 01270 652 773 & 07825 631 323.

**30 August (Mon) 10.30am-2pm**  
**BRIDGNORTH LEISURE CENTRE**, High Town, WV16 4ER. 100+ stalls of all types of model railways, Dinky's, Corgis, various die-casts etc. Admission £2. TONY OAKES FAIRS.  
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## September 2021

**4 September (Sat) 10.30am-3pm**  
**BRECON TOY & TRAIN SALE**. Market Hall, Town Centre, LD3 7LG. Back in the fantastic atmosphere of the Victorian venue with 90+ tables (subject to the prevailing regulations & conditions) with special safety features built-in. You are advised to wear face-covering. Admission: £2.50. Website for very latest information: [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)  
Mob: 07966 694579

**11 September (Sat) 10.30am-2pm**  
**STOKE-ON-TRENT** Fenton Community Centre, Manor Street, Fenton. ST4 2PT.  
TONY OAKES FAIRS.  
Tel: 01270 652 773 & 07825 631 323.

**12 September (Sun) 10.30am-3pm**  
**COVENTRY - 160 STALLS**, The Connexion, Ryton on Dunsmore, Coventry, CV8 3FL. Lots of stalls full of new and used model railways - many at bargain prices. Adults £3, Seniors £2.50, Children £1. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**18 September (Sat) 10.30am-3pm**  
**SANDOWN PARK - 500 STALLS**, Europe's No1 Toy Collectors Fair, Sandown Exhibition Centre, Sandown Park Racecourse, Esher, Surrey, KT10 9AJ.

Adults £6.50, Senior Citizens £6, Children £2. Hundreds of stalls selling everything you want in model railways, come and see why Sandown is Britain's best loved toy show. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**19 September (Sun) 10am-4pm**  
**BLUEBELL RAILWAY COLLECTORS FAIR**, Horsted Keynes Station, Station Approach, Horsted Keynes, East Sussex, RH17 7BB. Admission by platform ticket, or free with train travel ticket or Bluebell membership card.  
For trader information contact Phil Cooper on 07752 867427 or [sudburymodelrailways@hotmail.com](mailto:sudburymodelrailways@hotmail.com)  
For tickets and train times go to: [www.bluebell-railway.com](http://www.bluebell-railway.com)

**19 September (Sun) 10.30am-2pm**  
**HAYDOCK PARK RACECOURSE**, The Exhibition Centre, WA12 0HQ (M6 Junction 23). Up to 85 tables. Adults £3, Children £1. TONY OAKES FAIRS.  
Tel: 01270 652 773 & 07825 631 323.

**26 September (Sun) 10.30am-3pm**  
**BUXTON - 150 STALLS**, The Pavilion Gardens, St Johns Road, Buxton, Derbyshire, SK17 6XN. Adults £3, Senior Citizens £2.50, Children £1. Many stalls full of OO and O gauge trains. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

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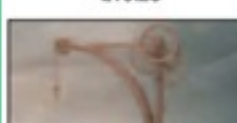
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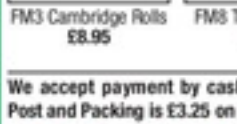
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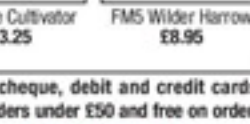
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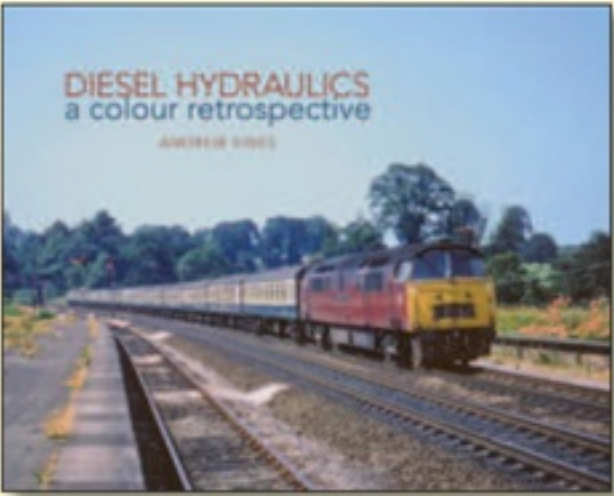
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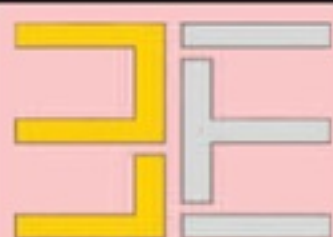


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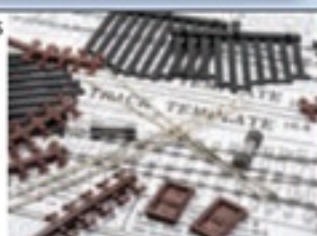


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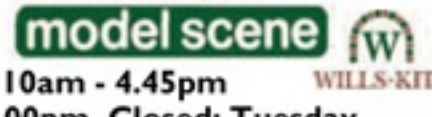
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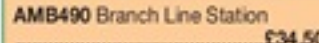
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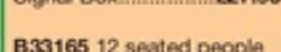
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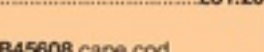
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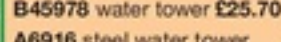
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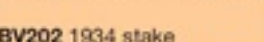
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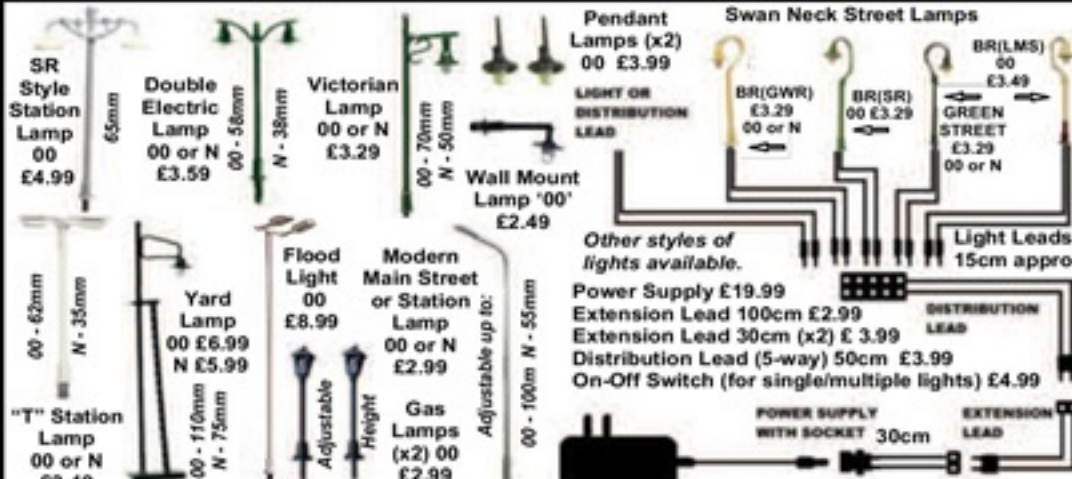
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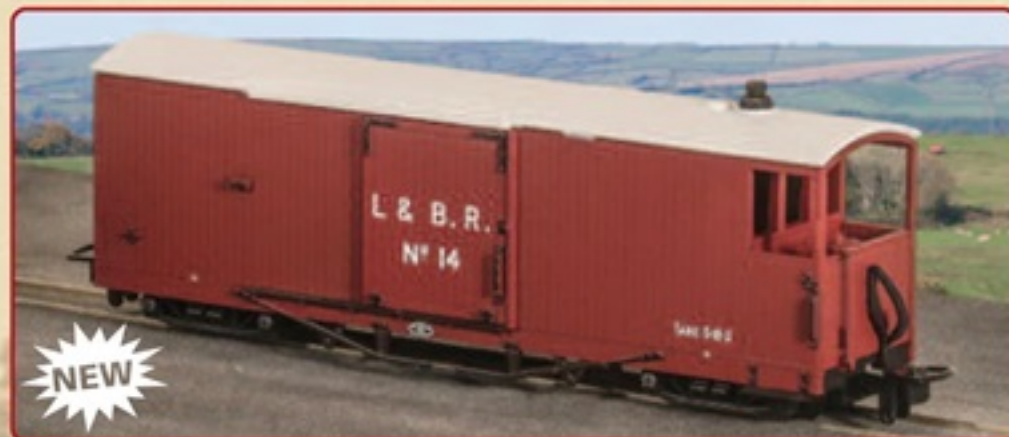
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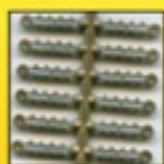
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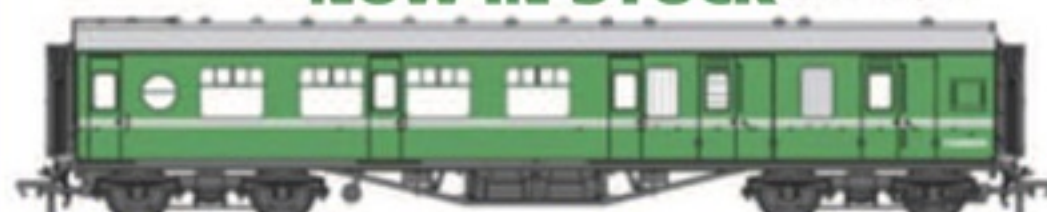


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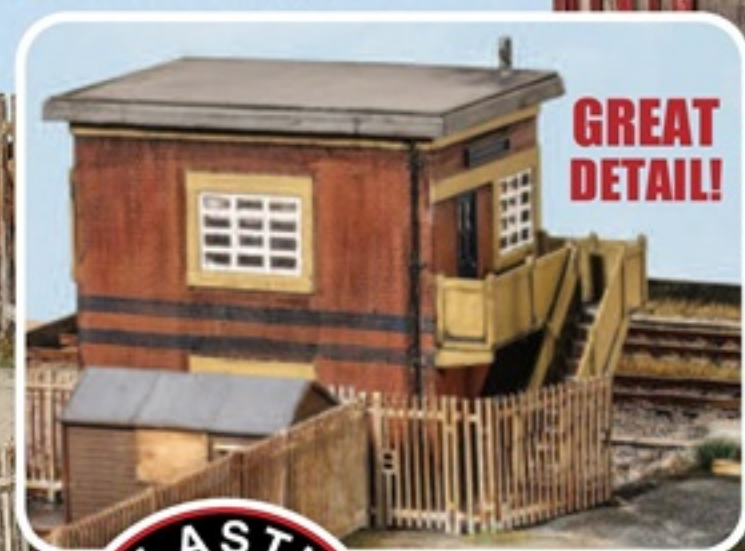
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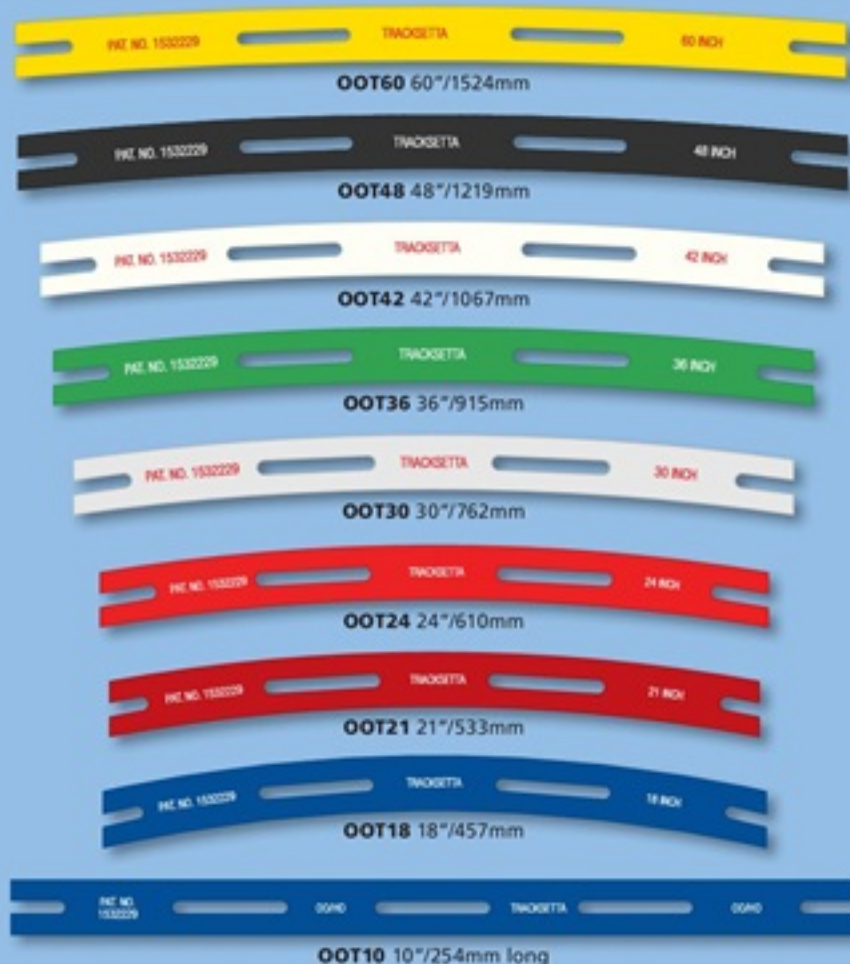
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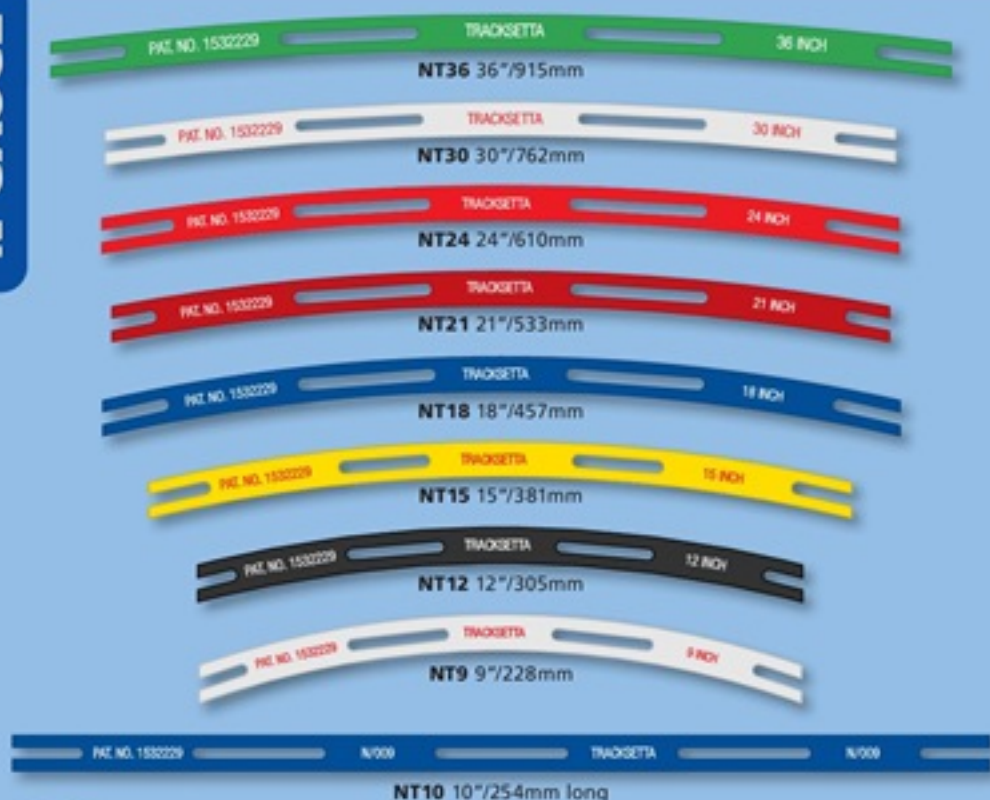
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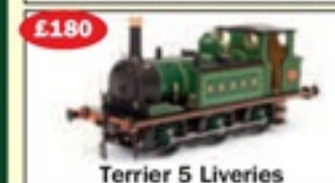
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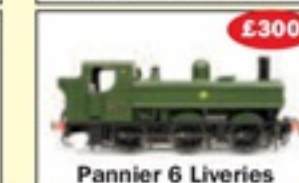
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