

CELEBRATING  
70 YEARS AS  
BRITAIN'S NUMBER ONE  
MODEL RAILWAY MAGAZINE

AUGUST 2019 £4.60

AT THE HEART OF THE HOBBY

VOLUME 70 No 826

# RAILWAY MODELLER

## Edwardian Elegance

Wainwright D Class 4-4-0  
New from Lee Marsh Model Co  
*Reviewed inside ...*



## Lincoln Central

Great Northern architecture  
modelling in 4mm scale

## Ambergate

Whatever happened to  
this 4mm masterpiece

## Greatford Crossing

Watching the trains  
go by in OO

## Whisky Galore!

A Scottish layout suggestion for  
OO Barclay and Ruston models

## Also reviewed inside ...

SR Bulleid 59' coaches from Hornby in OO  
LMS Porthole coaches from Bachmann in OO  
3D printed models from Osborn's in N

PUBLISHED BY

**PECO**

[www.pecopublications.co.uk](http://www.pecopublications.co.uk)



9 770033 893184

08



# RAILWAY MODELLER

## Contents

August 2019 vol 70 No.826

### 622 **The Brockhampton & Umbridge Railway**

#### **Railway of the Month**

Peter Butler built this outdoor railway in SM-32, taking the premise of a revived Welsh slate system, reborn as a preservation scheme, as its inspiration.

### 630 **Building Lincoln Central**

#### **Project of the Month**

Howard Leader introduces his mammoth project to recreate the former Great Northern station in OO gauge by showing off the structures completed so far.

### 634 **Sherton Abbas**

In a different take on the Great Western branch terminus theme, Dave Stone wound the clock back to the pre-Grouping era with this exquisite O gauge layout, complete with private siding.

### 641 **Invernevis**

David Greenwood was prompted to model this West Highland scene – set in the 1970s and 1980s – by plans of the original station building at Fort William.

### 646 **Striving for something better – part 2**

Jerry O'Reilly continues his upgrade of his N gauge layout *Parbourne and Floyd Bridge* with a makeover of some of the rolling stock and road vehicles.

### 650 **BR Siphon variations**

Richard Bardsley mixes-and-matches Lima bodies and Dapol chassis with added details to create three models in N gauge of these long-lived Great Western designs.

### 654 **Talking Points**

Topical issues from the world of railway modelling. This month, Peter Osborne demonstrates how model locomotives' haulage capabilities can be assessed.

### 656 **Woodbury Wharf**

Richard Holder refreshed and extended a competition entry in OO9 to create this canalside scene in a small space, designed for automated operation.

### 660 **A 'recycled' station building**

Roger Merry shows how a 7mm scale station building can be scratchbuilt from readily-available materials such as pizza packaging.

### 662 **Whisky Galore**

#### **Plan of the Month**

With the imminent arrival of the diminutive new Ruston & Hornsby 48DS in OO gauge from Hornby, Neil Rushby presents a layout suggestion tailor-made for it: an imaginary Scottish island, with distillery, connected via a train ferry to the outside world.







## RAILWAY MODELLING *Explored*

Projects, hints and tips for beginners from Railway Modeller readers and staff

### 681 Two layouts in one – the sequel!

Paul Page shows the second half of his 1980s layout evoking memories of BR(S) EMUs.

### 684 A country pub in N

Craig Tiley kitbashed a rural essential from several of the Peco Manyways house kits, and show how he did it in this step-by-step feature.

### 686 A narrow gauge water tower

The slate-built structure that slakes locomotives' thirsts at Dolgoch, on the Tallylyn Railway, is a well-known and photographed item; Michael Farr shows how it would make an ideal scratchbuilding project.



662

WHAT'S  
ON THE  
COVER

690

662

692



630

672

676

Cover photography by Craig Tiley

### 665 Ruston & Hornsby 48DS

#### Scale Drawings

Tim Rayner provides some background on the class intended for the Whisky galore plan; scale drawings by Bob Phelps are reproduced to 7mm scale.

### 666 Caradon Junction

The tangle of branch lines and freight-only routes in the Liskeard area were all the inspiration Mike Kelly needed for a tribute layout in OO gauge.

### 671 Mileposts – Leicester MRG marks its 70th anniversary

Brian Gillespie looks back at seven decades-worth of modelling by this prolific Midlands-based organisation, which has had some notable former members.

### 672 Ambergate

Charles Stevens explains how this 4mm scale model of the famous Midland Railway triangular station came to be constructed, and what became of it.

### 676 Greatford Crossing

In the first instalment of a two-part feature looking at the OO gauge layout installed in his purpose-built layout room, Graham Hobbs reveals the four-track 'watching the trains go by' side, inspired by an East Coast Main Line train-watching location.

### 687 Comment

Opinions and observations across the field of railway modelling. This month, John Rodway considers that all models are equal, no matter from what they were made.

### 688 Readers' Letters

### 690 Latest Reviews

Including this month the museum-quality Wainwright D Class 4-4-0 in 7mm scale from Lee Marsh Model Co; LMS 'porthole' coaches now in BR maroon in OO from Bachmann; 3D printed and laser-cut accessories from Osborn's Models in N, and much more.

### 696 Book & DVD Reviews

### 697 News

Including the Hornby open weekend to commemorate the revamp of its visitor centre; PTA bogie wagons to come from Accurascale in OO; and much more.

### 703 Societies & Clubs

The biggest and best guide to the exhibition scene in the UK.

**SUBSCRIBE**  
SEE PAGE 68a FOR DETAILS





Shop & telephone lines open  
9:30am to 5:15pm Monday to Friday, 9:00am - 5:15pm Saturday

Second hand section on our website updated regularly, as well as a vast selection of locos, coaches, wagons and accessories available in the shop.



[www.cheltenhammodelcentre.com](http://www.cheltenhammodelcentre.com)

Cheltenham Model Centre  
39 High Street  
Cheltenham  
GL50 1DX

Telephone: 01242 523117  
: 01242 234644

Email: [sales@cheltenhammodelcentre.com](mailto:sales@cheltenhammodelcentre.com)

**\*\*PECO PRICES REDUCED\*\***

**PECO** Railway Modeller Guide To.....Books



|                                                         |       |
|---------------------------------------------------------|-------|
| PM-200 - Your Guide To Railway Modelling.....           | £6.99 |
| PM-201 - Your Guide to Modelling American Railways..... | £6.99 |
| PM-202 - Peco Modellers Library Track Plans.....        | £3.95 |
| PM-203 - Your Guide To Narrow Gauge Railways.....       | £6.99 |
| PM-204 - Your Guide to N Gauge Railway Modelling.....   | £6.99 |
| M-206 - Your Guide to OO Gauge Railway Modelling.....   | £6.99 |
| PM-208 - Your Guide to O Gauge Railway Modelling.....   | £6.99 |



**PECO** Show You How Series Of Books

|                                                    |       |
|----------------------------------------------------|-------|
| SYH1 - Layout Planning and Design.....             | £0.50 |
| SYH2 - Building Baseboards.....                    | £0.50 |
| SYH3 - Laying The Track.....                       | £0.50 |
| SYH4 - Wiring The layout (First Steps).....        | £1.00 |
| SYH5 - Wiring the Layout.....                      | £1.00 |
| SYH6 - Making a Start in OO Gauge.....             | £0.50 |
| SYH7 - Making a Start in N Gauge.....              | £0.50 |
| SYH12 - Modelling water.....                       | £0.60 |
| SYH13 - Modelling the Landscape.....               | £1.00 |
| SYH14 - Detailing The Landscape.....               | £0.50 |
| SYH15 - Adding the Background.....                 | £0.50 |
| SYH16 - Creating the Backscene.....                | £0.50 |
| SYH17 - Introducing DCC.....                       | £0.75 |
| SYH18 - Railway Modelling Outdoors.....            | £0.50 |
| SYH20 - DCC Decoder Installation.....              | £1.00 |
| SYH21 - Wiring the Layout (Points).....            | £1.00 |
| SYH22 - Signalling the Layout (Semaphores).....    | £0.50 |
| SYH23 - Signalling the Layout (Colour Lights)..... | £0.50 |
| SYH24 - Operating Points & Signals.....            | £1.00 |
| SYH25 - Introducing DCC Sound.....                 | £0.50 |
| SYH26 - Modelling Overhead Catenary.....           | £1.00 |
| SYH27 - Building Wills Craftsman Kits.....         | £1.00 |
| SYH28 - Customising R-T-R Locomotives in OO.....   | £1.00 |

**PECO** OO Code 100 Insulfrog Points

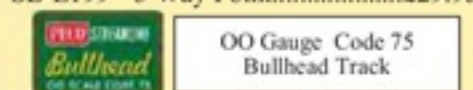
|                                      |        |
|--------------------------------------|--------|
| SL-84 - Right Hand Catch Point.....  | £8.25  |
| SL-85 - Left Hand Catch Point.....   | £8.25  |
| SL-86 - Curved Right Hand.....       | £12.50 |
| SL-87 - Curved Left Hand.....        | £12.50 |
| SL-88 - Large Radius Right Hand..... | £12.50 |
| SL-89 - Large Radius Left Hand.....  | £12.50 |
| SL-90 - Double Slip.....             | £34.50 |
| SL-91 - Small Radius Right Hand..... | £9.60  |
| SL-92 - Small Radius Left Hand.....  | £9.60  |
| SL-93 - Short Crossing.....          | £8.50  |
| SL-94 - Long Crossing.....           | £10.00 |
| SL-95 - Medium Radius Right.....     | £10.60 |
| SL-96 - Medium Radius Left Hand..... | £10.60 |
| SL-97 - Small Radius Y.....          | £9.50  |
| SL-98 - Large Radius Y.....          | £11.00 |
| SL-99 - 3 Way Point.....             | £27.45 |

**PECO** OO Code 100 Electrofrog Points

|                                       |        |
|---------------------------------------|--------|
| SL-E86 - Curved Right Hand.....       | £12.50 |
| SL-E87 - Curved Left Hand.....        | £12.50 |
| SL-E88 - Large Radius Right Hand..... | £12.50 |
| SL-E89 - Large Radius Left Hand.....  | £12.50 |
| SL-E91 - Small Radius Right Hand..... | £9.60  |
| SL-E92 - Small Radius Left Hand.....  | £9.60  |
| SL-E93 - Short Crossing.....          | £8.50  |
| SL-E94 - Long Crossing.....           | £10.00 |
| SL-E95 - Medium Radius Right.....     | £10.60 |
| SL-E96 - Medium Radius Left.....      | £10.60 |
| SL-E97 - Small Radius Y.....          | £9.50  |
| SL-E98 - Large Radius Y.....          | £11.00 |
| SL-E99 - 3 Way Point.....             | £25.00 |

**PECO** OO Code 75 Finescale Points

|                                    |        |
|------------------------------------|--------|
| SL-E180 - Single Slip.....         | £34.50 |
| SL-E186 - Curved Right Hand.....   | £12.99 |
| SL-E187 - Curved Left Hand.....    | £12.99 |
| SL-E188 - Large Radius Right.....  | £12.99 |
| SL-E189 - Large Radius Left.....   | £12.99 |
| SL-E190 - Double Slip.....         | £37.99 |
| SL-E191 - Small Radius Right.....  | £10.60 |
| SL-E192 - Small Radius Left.....   | £10.60 |
| SL-E193 - Short Crossing.....      | £10.85 |
| SL-E194 - Long Crossing.....       | £11.00 |
| SL-E195 - Medium Radius Right..... | £11.60 |
| SL-E196 - Medium Radius Left.....  | £11.50 |
| SL-E197 - Small Radius Y.....      | £11.00 |
| SL-E198 - Large Radius Y.....      | £12.85 |
| SL-E199 - 3 Way Point.....         | £29.95 |



|                                                                     |        |
|---------------------------------------------------------------------|--------|
| SL-U1188 - OO Gauge Code 75 Bullhead Large Radius Right Hand.....   | £26.00 |
| SL-U1189 - OO Gauge Code 75 Bullhead Large Radius Left Hand.....    | £26.00 |
| SL-114 - Code 75 Bullhead Fishplates.....                           | £5.50  |
| SL-1140 - OO Gauge Bullhead Rail Built Buffer Stop (Pack of 2)..... | £3.20  |

**PECO** N Code 80 Insulfrog Points

|                                             |        |
|---------------------------------------------|--------|
| SL386 - Right Hand Curved Point.....        | £10.50 |
| SL387 - Left Hand Curved Point.....         | £10.50 |
| SL388 - Right Hand Large Radius Point.....  | £10.50 |
| SL389 - Left Hand Large Radius Point.....   | £10.50 |
| SL393 - Short Crossing.....                 | £9.25  |
| SL394 - Long Crossing.....                  | £10.10 |
| SL395 - Right Hand Medium Radius Point..... | £9.10  |
| SL396 - Left Hand Medium Radius Point.....  | £9.00  |
| SL397 - Y Point.....                        | £9.00  |

**PECO** N Code 80 Electrofrog Points

|                                              |        |
|----------------------------------------------|--------|
| SLE386 - Curved Right Hand Point.....        | £10.50 |
| SLE387 - Curved Left Hand Point.....         | £10.50 |
| SLE388 - Large Right Hand Point.....         | £10.50 |
| SLE389 - Large Left Hand Point.....          | £10.50 |
| SLE395 - Right Hand Medium Radius Point..... | £9.50  |
| SLE396 - Left Hand Medium Radius Point.....  | £9.50  |
| SLE397 - Y Point Medium Radius.....          | £9.50  |

**PECO** N Code 55 Finescale Points

|                                               |        |
|-----------------------------------------------|--------|
| SLE380F - Single Slip.....                    | £34.50 |
| SLE383F - Scissors Crossover.....             | £61.50 |
| SLE386F - Right Hand Curved point.....        | £11.00 |
| SLE387F - Left Hand Curved Point.....         | £11.00 |
| SLE388F - Right Hand Large Radius Point.....  | £11.00 |
| SLE389F - Left Hand Large Radius point.....   | £11.00 |
| SLE390F - Double Slip.....                    | £38.00 |
| SLE391F - Right Hand Small Radius point.....  | £9.00  |
| SLE392F - Left Hand Small Radius Point.....   | £9.00  |
| SLE393F - Short Crossing.....                 | £9.50  |
| SLE394F - Long Crossing.....                  | £10.05 |
| SLE395F - Right Hand Medium Radius point..... | £10.00 |
| SLE396F - Left Hand Medium Radius Point.....  | £10.00 |
| SLE397F - Y Point Medium.....                 | £10.00 |
| SLE399F - 3 Way Point.....                    | £28.00 |

**PECO** O Gauge Points

|                                                   |        |
|---------------------------------------------------|--------|
| SL-E786BH - Curved Right Hand Point.....          | £54.00 |
| SL-E787BH - Curved Left Hand Point.....           | £54.00 |
| SL-E790BH - Double Slip.....                      | £98.00 |
| SL-E791BH - Medium Radius Right Hand Point.....   | £39.50 |
| SL-E792BH - Medium Radius Left Hand Point.....    | £39.50 |
| SL-E794BH - Long Crossing.....                    | £50.00 |
| SL-E797BH - Medium radius Y Point.....            | £39.50 |
| ST-U750 - Setrack Right Hand Point (Unifrog)..... | £43.00 |
| ST-U751 - Setrack Left Hand Point (Unifrog).....  | £43.00 |

**PECO** Electrics

|                                                         |       |
|---------------------------------------------------------|-------|
| PL-9 - Mounting Plates (Pack of 5).....                 | £2.85 |
| PL10 - Point Motor.....                                 | £5.95 |
| PL10W - Point Motor (Low Amp).....                      | £5.95 |
| PL10E - Point Motor Extended Arm.....                   | £5.95 |
| PL10WE - Point Motor Extended Arm (Low Amp).....        | £5.95 |
| PL11 - Side Mounted Point Motor.....                    | £7.50 |
| PL-12 - Motor Adaptor Base.....                         | £2.70 |
| PL-26B - Passing Contact Point Switch Black Lever.....  | £5.95 |
| PL-26R - Passing Contact Point Switch Red Lever.....    | £5.95 |
| PL-26W - Passing Contact Point Switch White Lever.....  | £5.95 |
| PL-26Y - Passing Contact Point Switch Yellow Lever..... | £5.95 |
| PL-41 - Track Rubber.....                               | £3.40 |

**PECO** OO Gauge Wills Kits

|                                                                               |        |
|-------------------------------------------------------------------------------|--------|
| SS10 - Victorian Gents Toilet.....                                            | £4.50  |
| SS11 - Taxi Men's Rest Hut.....                                               | £4.50  |
| SS12 - Station Garage with Pumps.....                                         | £6.80  |
| SS13 - Domestic Garage.....                                                   | £4.50  |
| SS14 - Tea Kiosk.....                                                         | £4.50  |
| SS15 - Coal Yard & Hut with Plastic Coal.....                                 | £5.10  |
| SS16 - Weighbridge & Hut.....                                                 | £4.50  |
| SS17 - Coal Bunkers.....                                                      | £4.50  |
| SS18 - Station Forecourt Shops.....                                           | £4.95  |
| SS19 - Grotty Huts & Privy.....                                               | £4.55  |
| SS20 - Greenhouse & Cold Frames.....                                          | £4.95  |
| SS21 - Hoardings with Bill Poster.....                                        | £5.05  |
| SS22 - Lamp Huts With Oil Drums.....                                          | £5.10  |
| SS23 - Bicycle shed & Bicycles.....                                           | £4.50  |
| SS24 - Conservatory with Garden Seat.....                                     | £4.50  |
| SS25 - Station Halt Complete With Fencing & Signs Plastic Kit.....            | £4.50  |
| SS26 - Victorian Bridge.....                                                  | £4.50  |
| SS27 - Station Halt with Waiting Room.....                                    | £5.10  |
| SS28 - Occupational Bridge with Stone Abutments Single Track.....             | £5.10  |
| SS29 - Ground Level Signal Box.....                                           | £4.50  |
| SS30 - Stone & Timber Barn Kit.....                                           | £5.10  |
| SS31 - Village Forge.....                                                     | £5.10  |
| SS32 - Occupational Bridge with Stone Abutments Double Track.....             | £6.55  |
| SS34 - Water Tower & Stone Base.....                                          | £6.55  |
| SS35 - Pagoda Building Iron Type Hut.....                                     | £4.50  |
| SS36 - OO Gauge Dressed Stone Type Wall.....                                  | £4.50  |
| SS37 - Market Stalls 4 wheel Barrows.....                                     | £5.00  |
| SS38 - Cattle Creep Stone Type.....                                           | £4.50  |
| SS39 - OO Gauge Crossing Keepers Cottage.....                                 | £4.50  |
| SS40 - Scrapyard.....                                                         | £4.50  |
| SS41 - Feather Edge Board Fencing & Gates.....                                | £4.50  |
| SS42 - Windows & Doors.....                                                   | £4.95  |
| SS43 - Concrete Fencing.....                                                  | £4.95  |
| SS44 - Larch Lap Fencing Including Gates.....                                 | £4.50  |
| SS45 - Rustic & picket Fencing.....                                           | £4.95  |
| SS46 - Buildings pack A Chimneys, Drainpipes & Sills.....                     | £4.95  |
| SS47 - Bow Plate Girder Bridge.....                                           | £7.65  |
| SS48 - Timber Signal Box.....                                                 | £12.75 |
| SS49 - Decked Girder Bridge.....                                              | £7.90  |
| SS50 - Platelayers Hut.....                                                   | £4.50  |
| SS51 - Goods Yard Crane.....                                                  | £4.50  |
| SS52 - OO Gauge Brick Retaining Arches.....                                   | £11.90 |
| SS53 - Brick Arch Bridge With Abutments.....                                  | £11.50 |
| SS54 - Station Canopy Length 180mm.....                                       | £7.65  |
| SS55 - Brick Arch Overlays.....                                               | £4.50  |
| SS56 - Level Crossing Gates inc Pedestrian wicket Gates.....                  | £7.45  |
| SS57 - Vari Girder Plate Girder Panels.....                                   | £5.00  |
| SS58 - Garden Sheds Timber Type (2).....                                      | £4.70  |
| SS59 - OO Gauge Brick Tunnel Mouth & Wing Walls Single Track Plastic Kit..... | £7.25  |
| SS60 - Station Platform Shelter Timber.....                                   | £7.45  |
| SS61 - OO Gauge Station Platform Sections 264mm Long Plastic Kit.....         | £5.00  |
| SS62 - Station Platform Ramps (pair).....                                     | £5.00  |
| SS63 - OO Gauge Goods Yard Store Timber Built Type Plastic Kit.....           | £7.45  |
| SS64 - Abutments with Wing Walls.....                                         | £7.65  |
| SS65 - OO Gauge Small Gents Toilet.....                                       | £4.70  |
| SS66 - Public Toilets Brick Built Small.....                                  | £7.65  |
| SS67 - OO Gauge Wayside Station Timber Slate Roof Brick Chimney Plastic.....  | £10.65 |
| SS68 - OO Gauge Platform Accessories Nameboards Trolleys etc Plastic Kit..... | £5.10  |
| SS69 - Stone Type Retaining Arches.....                                       | £10.85 |
| SS70 - OO Gauge Corrugated Iron Chapel.....                                   | £10.65 |
| SS71 - OO Gauge Round Top Windows (4).....                                    | £6.60  |
| SS72 - OO Gauge Village Scene Bench Horse Trough & Village Cross.....         | £4.65  |
| SS73 - OO Gauge Timber Yard.....                                              | £12.35 |
| SS74 - OO Gauge Pill Box Plastic Kit.....                                     | £4.50  |
| SS75 - OO Gauge Bus Shelter Plastic Kit.....                                  | £5.00  |
| SS76 - OO Gauge Quoins/Corner Stones.....                                     | £5.00  |
| SS77 - OO Gauge Period York Paving.....                                       | £5.10  |

|                                                                 |        |
|-----------------------------------------------------------------|--------|
| SS78 - OO Gauge Timber Island Platform Shelter Plastic Kit..... | £11.60 |
| SS79 - OO Gauge Parapet Bridge Walls.....                       | £5.00  |
| SS80 - OO Gauge Three Arch Viaduct.....                         | £23.95 |
| SS81 - OO Gauge Extra Arch & Pier.....                          | £9.80  |
| SS82 - OO Gauge River/Canal Bridge.....                         | £9.80  |
| SS83 - OO Gauge 2 Stone Piers.....                              | £10.65 |
| SS84 - OO Gauge Water Wheel & Sluice Gates Plastic Kit.....     | £7.05  |
| SS85 - OO Gauge Relay Boxes (set 1).....                        | £3.65  |
| SS86 - OO Gauge Window Doors Gates & Porch.....                 | £4.50  |
| SS87 - OO Gauge Concrete Trunking.....                          | £4.50  |
| SS88 - OO Gauge Relay Boxes (set 2).....                        | £3.65  |
| SS89 - OO Gauge OO/HO Point Rodding.....                        | £9.95  |
| SS90 - OO Gauge OO/HO Point Rodding Extension Kit.....          | £7.65  |
| SS91 - OO Gauge Lever Ground Frames.....                        | £7.65  |

|                                            |        |
|--------------------------------------------|--------|
| SS92 - Garden Buildings & Accessories..... | £12.70 |
|--------------------------------------------|--------|

Greenhouse, Conservatory, Garden Shed, Cold Frames, Water Butt, Wheelbarrow, Lawnmower, Lawn Roller, and Larch-Lap Fencing.

**PECO** N Gauge Ratio Kits

|                                                  |        |
|--------------------------------------------------|--------|
| 202 - Cattle Dock.....                           | £14.90 |
| 203 - Engine Shed.....                           | £12.85 |
| 204 - Station Building.....                      | £19.25 |
| 205 - Station Building Canopy Only.....          | £3.80  |
| 206 - Locomotive Serving Depot.....              | £14.90 |
| 207 - GWR Station Train Shed.....                | £14.90 |
| 208 - Apex Platform Canopy.....                  | £7.90  |
| 209 - Platforms (9' Long x 1 3/4' wide).....     | £4.15  |
| 210 - Platform Ramps (2 per pack).....           | £4.15  |
| 211 - Telegraph Poles (10 per pack).....         | £3.65  |
| 212 - Water Crane & Fire Devil.....              | £7.45  |
| 213 - Station/Street Lamps (4 per pack).....     | £4.25  |
| 214 - Yard Crane.....                            | £4.50  |
| 215 - Square Water Tower.....                    | £7.95  |
| 216 - Lineside Fencing White.....                | £3.80  |
| 217 - Lineside Fencing Wood Brown.....           | £3.80  |
| 218 - Signal Laddering (etched brass).....       | £4.15  |
| 219 - Concrete fencing Gates.....                | £3.80  |
| 220 - Stone Goods shed.....                      | £12.85 |
| 221 - Pallets Sacks Barrels.....                 | £3.65  |
| 222 - Concrete Footbridge.....                   | £7.95  |
| 223 - GWR Wooden Signal Box.....                 | £12.85 |
| 224 - Signal Box Interior.....                   | £5.25  |
| 225 - Flat Roof Platform Canopy.....             | £7.95  |
| 226 - Pump House/Boiler House.....               | £12.50 |
| 227 - Weighbridge hut.....                       | £6.05  |
| 228 - Oil Depot.....                             | £6.60  |
| 229 - Coal Depot.....                            | £6.40  |
| 230 - Round Water Tower.....                     | £6.60  |
| 231 - Carriage Shed.....                         | £12.85 |
| 232 - Coal/Builders Merchant.....                | £12.85 |
| 233 - loading Gauge.....                         | £4.00  |
| 234 - Level crossing with Gates.....             | £7.95  |
| 235 - level Crossing with Barriers.....          | £7.95  |
| 236 - Midland Signal Box (no interior).....      | £12.85 |
| 237 - 2 Lineside Huts (1 brick 1 wood).....      | £6.20  |
| 238 - 2 Concrete Huts.....                       | £6.20  |
| 239 - Retaining Wall (350mm long).....           | £8.75  |
| 240 - Steel Truss Bridge with Stone Piers.....   | £14.55 |
| 241 - Steel Truss Span with Steel Trestle.....   | £13.60 |
| 242 - 2 Steel Trestles.....                      | £8.50  |
| 243 - GWR Station Fencing White.....             | £4.75  |
| 244 - GWR Trackside Fencing White.....           | £4.75  |
| 245 - GWR Spear Fencing Black.....               | £4.75  |
| 246 - GWR Spear Fencing Black Ramps & Gates..... | £4.75  |
| 247 - Coal Tower.....                            | £20.15 |
| 248 - Modular Covered Footbridge.....            | £17.85 |
| 249 - Diesel Headlamps.....                      | £2.50  |
| 250 - Remote Control for Signals.....            | £3.40  |
| 251 - Three Arch Viaduct.....                    | £19.80 |
| 252 - Extra Arch & Pier.....                     | £8.50  |
| 253 - River/Canal Bridge.....                    | £8.50  |
| 254 - Two Stone Piers.....                       | £9.80  |
| 255 - Water Trough.....                          | £7.05  |
| 256 - Large Water Tower.....                     | £12.75 |
| 257 - Relay Boxes.....                           | £5.95  |
| 258 - Concrete Trunking.....                     | £4.85  |
| 260 - Home or Distant (Lower Quadrant).....      | £3.20  |
| 262 - Junction or Bracket Home or Distant.....   | £5.10  |
| 270 - Home or Distant (Upper Quadrant).....      | £3.15  |
| 271 - Pratt Truss Gantry.....                    | £3.40  |
| 300 - Gutters & Drainpipes.....                  | £4.15  |
| 301 - Brick Walling.....                         | £4.15  |
| 302 - Coarse Stone.....                          | £4.15  |
| 303 - Paving Slabs/Crazy Paving.....             | £4.15  |
| 304 - Wood Planking.....                         | £4.15  |
| 305 - Slate Roofing.....                         | £4.15  |
| 306 - Tile Roofing.....                          | £4.15  |
| 307 - Chimneys.....                              | £4.15  |
| 308 - Flagstones.....                            | £4.15  |
| 309 - Industrial Windows.....                    | £4.15  |
| 310 - Domestic Windows.....                      | £4.15  |
| 311 - Doors.....                                 | £4.15  |
| 312 - Corrugated Sheet.....                      | £4.15  |
| 313 - Roughcast Walling.....                     | £4.15  |
| 314 - Industrial Chimneys & Fittings.....        | £4.25  |
| 315 - Oil Tanks.....                             | £4.25  |
| 316 - Coal Stalkies.....                         | £4.25  |
| 317 - Corrugated Roof.....                       | £3.15  |

**Electric** Toggle Switches

|                                                             |       |
|-------------------------------------------------------------|-------|
| SMT1 Single Pole Single Throw, (On-Off).....                | £1.05 |
| 4 x SMT1.....                                               | £4.00 |
| SMT2 Single Pole Double Throw, (On-On).....                 | £1.20 |
| 4 x SMT2.....                                               | £4.50 |
| SMT3 Single Pole Double Throw, Centre Off, (On-Off-On)..... | £1.20 |
| 3 x SMT3.....                                               | £3.00 |
| SMT4 Double Pole Double Throw (On-On).....                  | £1.35 |
| 4 x SMT4.....                                               | £4.80 |
| SMT5 Double Pole Double Throw (On-Off-On).....              | £1.45 |
| 4 x SMT5.....                                               | £5.20 |
| SMT6 Point Toggle Switches.....                             | £1.45 |
| 6 x SMT6.....                                               | £7.80 |

Push to Make Switches  
Red, Green, Black.....50p Each  
6 of any colour.....£2.70

**Grain of Wheat Bulbs**

12v Bulbs  
Clear, Red, Yellow, Green or Amber  
Packs of 10.....£3.00

**Layout Wire**

Suitable for point motors, power feeds & more  
7/0.2mm, Single Core, Multi Strand

100 metre rolls available in 7 colours

Red/Black/Blue/Green/Yellow/Brown/White  
£6.99 a roll

Same colours available in

10 metre rolls.....£1.50 each

20 metre rolls.....£2.50 each

**WELLCALFE** OO Gauge Kits

|                                                          |        |
|----------------------------------------------------------|--------|
| PO205 - Low Relief Pub & Shops.....                      | £9.35  |
| PO210 - Paving Slabs.....                                | £5.85  |
| PO216 - Platform Kit.....                                | £10.65 |
| PO226 - Parish Church.....                               | £13.60 |
| PO228 - Coaching Inn.....                                | £8.95  |
| PO232 - Goods Shed.....                                  | £11.90 |
| PO233 - Signal Box.....                                  | £8.50  |
| PO235 - Stone Platform Kit.....                          | £10.65 |
| PO236 - Footbridge.....                                  | £9.80  |
| PO237 - Country Station.....                             | £12.75 |
| PO238 - Wayside Station Stone.....                       | £15.30 |
| PO239 - Stone Built Wayside Station Shelter.....         | £11.90 |
| PO240 - Double Track Viaduct Red Brick.....              | £11.90 |
| PO241 - Double Track Viaduct Stone.....                  | £11.90 |
| PO242 - Double Tunnel Entrance.....                      | £8.95  |
| PO243 - Single Tunnel Entrance.....                      | £8.50  |
| PO244 - Retaining Walls Red Brick.....                   | £11.50 |
| PO245 - Retaining walls Stone.....                       | £11.50 |
| PO246 - Railway Bridge Brick Double or Single Track..... | £12.35 |
| PO247 - Railway Bridge Brick Double or Single Track..... | £12.35 |
| PO248 - Tapered Retaining Walls Brick.....               | £11.50 |
| PO249 - Tapered Retaining Walls Stone.....               | £11.50 |
| PO250 - Manor Farm House.....                            | £8.95  |
| PO251 - Manor Farm Barn.....                             | £8.95  |
| PO252 - Manor Farm Buildings.....                        | £9.80  |
| PO253 - Village School.....                              | £8.90  |
| PO254 - Village Shop & Café.....                         | £10.65 |
| PO255 - Workers Cottages.....                            | £10.65 |
| PO256 - Stable Block.....                                | £6.80  |
| PO261 - Red Brick Terraced Houses.....                   | £8.95  |
| PO262 - Stone Terraced Houses.....                       | £8.95  |
| PO263 - Red Brick Corner Shop.....                       | £9.80  |
| PO264 - Stone Corner Shop.....                           | £9.80  |
| PO272 - Low Relief Shop Fronts, Red Brick.....           | £11.50 |
| PO273 - Low Relief Shop Fronts, Stone.....               | £11.50 |
| PO274 - Low Relief Red Brick Terraced House Fronts.....  | £10.65 |
| PO275 - Low Relief Stone Terraced House Fronts.....      | £10.65 |
| PO276 - Low Relief Red Brick Terraced Houses Backs.....  | £10.65 |
| PO277 - Low Relief Stone Terraced Houses Backs.....      | £10.65 |
| PO279 - Low Relief Department Store.....                 | £11.50 |
| PO281 - Service Station.....                             | £12.75 |
| PO282 - Full or Low Relief Warehouse Kit.....            | £12.35 |
| PO283 - Small Factory.....                               | £12.35 |
| PO284 - Boiler House with Chimney.....                   | £12.35 |
| PO285 - Industrial Unit.....                             | £16.60 |
| PO289 - Fire Station.....                                | £11.90 |
| PO313 - OO Gauge Double Track Engine Shed.....           | £19.95 |
| PO320 - Mainline Station Booking Hall.....               | £13.60 |
| PO321 - Parcels Office.....                              | £17.60 |
| PO322 - Island Platform Building.....                    | £16.60 |
| PO330 - GWR Signal Box.....                              | £12.75 |
| PO331 - Red Brick Single Track Engine Shed.....          | £13.60 |
| PO332 - Stone Single Track Engine Shed.....              | £13.60 |
| PO333 - Settle And Carlisle Railway Station.....         | £19.95 |
| PO334 - Settle & Carlisle Station Shelter.....           | £9.80  |
| PO335 - Settle & Carlisle Station Shelter.....           | £13.60 |
| PO336 - Settle Carlisle Station Goods Shed.....          | £20.65 |
| PO340 - Platform Canopy.....                             | £12.75 |
| PO341 - Wall Backed Platform Canopy.....                 | £13.60 |
| PO400 - Platform Underpass.....                          | £5.85  |
| PO410 - Wooden Pavilion.....                             | £5.85  |
| PO415 - Scale Nissen Hut.....                            | £6.80  |
| PO421 - Low Relief Timber Framed Shop.....               | £6.80  |
| PO430 - Small Signal Box.....                            | £8.95  |
| PO501 - G.W.R Benches.....                               | £5.85  |
| PO502 - Platform Benches.....                            | £5.85  |
| PO503 - Park Benches.....                                | £5.85  |
| PO510 - Picnic Tables.....                               | £5.85  |
| PO511 - Scale Show Jumps.....                            | £5.85  |
| PO512 - Garden Sheds.....                                | £5.85  |
| PO513 - Children's Play Area.....                        | £5.85  |
| PO515 - Station Clocks.....                              | £5.85  |
| PO517 - Platform Kiosk.....                              | £5.85  |
| PO520 - Pillboxes.....                                   | £5.85  |
| PO522 - Stone Fountain.....                              | £5.85  |
| PO525 - Bus Shelter.....                                 | £5.85  |
| PO530 - Market Stalls.....                               | £5.85  |
| PO540 - Goods Yard Crane.....                            | £5.85  |
| PO572 - Arcade Enhancement Kit.....                      | £5.85  |
| PO580 - Signal Box Interior.....                         | £5.85  |



UK Mainland Standard Postage Rate £4.00 per shipment  
Registered 24hr delivery (Mon to Fri) UK Mainland £7.00  
Worldwide postage at cost

Postcodes IM, BT, PH, PA, PO30-PO41, KW, AB, KA & IV will incur extra Postage - please enquire



# Western Limited Editions

**A trio of Dapol OO gauge Westerns exclusive to Cheltenham Model Centre.  
Available for pre order now! Delivery expected to be June/July 2019.**

## WESTERN KING

**New Tooling!  
Unique to D1039**



**Limited Edition Of Only 350**  
**Standard - £159.99**  
**DCC Fitted - £186.99**  
**DCC Sound Fitted - £284.99**

Between March and November 1964 D1039 was fitted with an experimental Clear View Screen designed by George Kents Ltd. It was hoped this style of wiper would perform better than the standard wipers being used on Westerns at this time. This model will represent the loco as it was when fitted with this style of windscreen wiper.

## WESTERN STALWART

**New Tooling!  
Unique to D1006**



**Limited Edition Of Only 350**  
**Standard - £159.99**  
**DCC Fitted - £186.99**  
**DCC Sound Fitted - £284.99**

This model will represent D1006 as it was between July 1966 - February 1967. D1006 was the second Western to be fitted with a Clear View Screen after improvements had been made over the original used on D1039. An earlier version that was fitted to D1006 had also had some modifications to improve visibility.

### These models will feature....

- Representations of vents on the valances on D1000
- Exclusive tooling for D1006 & D1039
- Etched name & number plates provided by Extreme Etchings
- Roof lifting rings and nameplates fitted at the factory
- Numbered Limited Edition Certificate
- Specially designed box, unique for each loco
- DCC sound fitted versions available with a choice of sound project
- Weathered versions also available for an extra £20

### Part Exchange Available!

Reduce the cost of these Westerns by taking advantage of our part exchange offer!

We are after...

- Hornby/Lima/Heljan/Dapol Westerns
  - Locos from Bachmann/Hornby/Lima/Farish
  - Wagons & Coaches odd ones of by the box full
  - Buildings, signals & other scenic bits.
- Pretty much anything considered! **Boxed** or **unboxed** items needed.

**Please contact us for more information.**

### PART EXCHANGE YOUR UNWANTED POINTS FOR ANYTHING ON THESE 2 PAGES

**Peco/Hornby Points**  
**Peco Points** We will allow £2.35 for each small or medium radius and £2.25 for large and curved radius 'OO' points. Also £7.00 each for double slip and 3-way point.  
Fine Scale points also required.  
N Gauge points also required.  
**Hornby Points** We will allow £2.25 for new type Nickel Silver points.  
£2.75 each for curved/express Nickel Silver points.  
**Point Motor** We will allow £1.50 each for any quantity of Peco or Hornby point motor in working order.

### Second-Hand Wanted

Boxed or unboxed  
OO, N & O Gauge  
Lima, Hornby, Bachmann,  
Graham Farish,  
Heljan  
Locos, Coaches or wagons.  
**Anything considered!**  
**Part Exchange against anything in the shop**

Please contact us for more information and before travelling with any part exchange.

### Stock Boxes for OO size locos & Carriages

**Large Size** - Tender Loco size boxes  
330mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black  
£3.65 each or  
Any 4 Large Boxes for £13.00  
**Medium Size** - 2-6-4 Tank size boxes  
220mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black  
**Small Size** - 08 or Pannier Tank size boxes  
157mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black  
£3.15 each or  
Any 4 Medium or Small Stock boxes  
For £10.00

### Sundeala Board

600mm x 1200mm (4' x 2')  
(approximate measurements)  
Single Sheets £11.75 Shop Only  
6 Sheets £60.00 + £9.30 P+P (UK)

### Cork Sheeting

36" x 24" (3ft x 2ft) per roll  
1/16 Roll £5.00 each 2 for £9.00  
1/32 Roll £4.00 each 2 for £7.00  
1/8 Roll £7.00 each 2 for £13.00

### Cork Sheeting



### Ballast Spreaders

N Gauge Ballast Spreader.....£11.00  
OO Gauge Ballast Spreader.....£11.00  
(Please note only fine ballast to be used with the ballast spreader)

### Dapol OO Gauge 121 DMU



4D-015-006 - Class 122 DMU NSE Route Learner.....£116.00  
4D-015-007 - Class 122 DMU Loadhaul Route Learner.....£116.00  
4D-015-008 - Class 122 DMU BR Green Speed Whiskers.....£116.00  
4D-015-009 - Class 122 DMU BR Green Small Yellow Panel.....£116.00  
4D-015-010 - Class 122 DMU BR Blue 55003.....£116.00

Also available for pre order....

## WESTERN ENTERPRISE

D1000 'Western Enterprise' BR Maroon Small Yellow Panel

With BR Aluminium Crests and larger nameplates. **Limited to 300 models**

**\*\*Standard £151.99 \*\* \*\*DCC Fitted £178.99\*\* \*\*DCC Sound Fitted £276.99\*\***

**We are expecting delivery to be Summer 2019. See the dedicated page on our website for more information.**





# GAUGEMASTER

## Analogue Control



### Choose your controller for life with the Gaugemaster Lifetime Guarantee...

We have been producing Model Railway Controllers for over forty years. Our analogue controllers have stood the test of time, and so has our lifetime guarantee:

*"We undertake to replace, free of charge, any parts found defective within the lifetime of the unit providing that the item has not been tampered with."*

#### What's the difference between the controller types?

Our **Mains Powered Cased Controllers** come complete with transformer, and can just be plugged in, connected to the track, and away you go. **Panel Mounted Controllers** require a separate transformer, and also need to be mounted onto a control panel to be used effectively.

We also produce various controllers with **Feedback** and **Simulation**, two effects controlled by the controller itself. **Feedback** senses the load on the circuit and helps maintain the locomotive at a steady speed up and down gradients. **Feedback** controllers are not suitable for use with locomotives with coreless motors. **Simulation** (also known as Inertia) allows a train to accelerate, coast, and be braked to a standstill, by use of a regulator and a brake.

### MAINS POWERED CASED CONTROLLERS

**GMC-COMBI** Single Track Controller/Transformer  
Most Suited for HO/OO/N Scale Layouts



Fantastic for small layouts or beginners upgrading a starter set, the Combi has both a 12V DC output to run one track, and a 16V AC output for accessories.

**BEST FOR**  
Beginners

**GMC-D** Twin Track Controller  
Most Suited to HO/OO/N Scale Layouts



Our best selling controller. Runs a two track railway with minimum of fuss. The D Controller has two 12V DC track outputs, as well as a 16V AC output for accessories.

**BEST FOR**  
Twin Tracks

**GMC-Q** Four Track Controller  
Most Suited to OO/HO/N Scale Layouts



The best selling four track controller available today. It offers impressive value for money with its four 12V DC track outputs, and two of 16V AC and 12V DC outputs for accessories.

**BEST FOR**  
Four Tracks

### PANEL MOUNTED CONTROLLERS

**GMC-100** Single Track Panel Controller  
Most Suited to OO/HO/N/Z Scale Layouts



Some modellers may wish to incorporate their controller into an overall panel to control their layout. The Model 100 Controller has a single 12V DC output.

**BEST FOR**  
Control Panels

**GMC-U** Single Track Controller with Simulation  
Most Suited to OO/HO/N/Z Scale Layouts



With the brake knob controlling the 12V DC track output, this controller allows you greater realism when running locomotives.

**BEST FOR**  
Realistic Running

**GMC-W** Single Track Walkabout Single Track Controller  
Most Suited to OO/HO/N/Z Scale Layouts



Fitted with 1.5m of cable, this controller allows you the freedom to move around your layout while still controlling your layout. It has a single 12V DC track output.

**BEST FOR**  
Flexibility

#### Did you know...

If you don't have a handy accessory output from a controller, the **GMC-WM1** Wall Mounted Transformer can be used on its own to power accessories, such as point motors and lights from the 16V output. Just plug it in and connect it up!

**GMC-WM1**  
Wall Mounted  
Transformer  
16V or 12V DC 1.1a



Find us on Facebook:  
**@Gaugemaster**  
Follow us on Twitter:  
**@Gaugemaster**  
Follow us on Instagram:  
**@gaugemaster\_controls**

### PRICES

#### Cased Controllers

|             |                                              |         |
|-------------|----------------------------------------------|---------|
| GMC-COMBI   | Single Track Controller/Transformer          | £44.95  |
| GMC-100M    | Single Track Controller                      | £94.95  |
| GMC-100MO   | Single Track Controller for O Scale          | £94.95  |
| GMC-10LGB   | Single Track Controller for G Scale          | £109.95 |
| GMC-10LGBSF | Single Track Controller for G Scale with Fan | £189.95 |
| GMC-P       | Single Track Controller with Simulation      | £104.95 |
| GMC-D       | Twin Track Controller                        | £104.95 |
| GMC-DS      | Twin Track Controller with Simulation        | £189.95 |
| GMC-TS      | Three Track Controller with Simulation       | £189.95 |
| GMC-Q       | Four Track Controller                        | £199.95 |

#### Panel Mounted Controllers (Orange text shows transformer required)

|           |                                                           |        |
|-----------|-----------------------------------------------------------|--------|
| GMC-100   | Single Track Controller (GMC-T1/M1)                       | £39.95 |
| GMC-100.O | Single Track Controller for O Scale (GMC-T2/M2)           | £49.95 |
| GMC-U     | Single Track Controller with Simulation (GMC-T1/M1/WM1)   | £49.95 |
| GMC-UF    | Single Track Controller with Feedback (GMC-T1/M1/WM1)     | £44.95 |
| GMC-UD    | Single Track Controller with Simulation for O (GMC-T2/M2) | £64.95 |
| GMC-UDF   | Twin Track Controller with Feedback (GMC-T1/M1/WM1)       | £59.95 |
| GMC-UDS   | Twin Track Controller with Simulation (GMC-T1/M1/WM1)     | £64.95 |
| GMC-UTS   | Three Track Controller with Simulation (GMC-T1/M1/WM1)    | £79.95 |
| GMC-UQ    | Four Track Controller (GMC-T3/M3/WM3)                     | £89.95 |
|           |                                                           | £94.95 |

#### Walkabout and Hand Held Controllers

(No GMC-T1/M1/WM1)

|        |                                                   |        |
|--------|---------------------------------------------------|--------|
| GMC-W  | Single Track Walkabout Controller                 | £39.95 |
| GMC-WS | Single Track Walkabout Controller with Simulation | £59.95 |
| GMC-HH | Single Track Handheld Controller with Feedback    | £41.95 |

#### Transformers

|         |                                                        |        |
|---------|--------------------------------------------------------|--------|
| GMC-M1  | Cased Transformer 16V AC                               | £52.95 |
| GMC-M2  | Cased Transformer 18V AC 2.5V                          | £57.95 |
| GMC-M3  | Cased Transformer 24V AC                               | £57.95 |
| GMC-M4  | Cased Transformer 12V AC                               | £57.95 |
| GMC-T1  | Open Transformer 2x 16V AC 1a                          | £29.95 |
| GMC-T2  | Open Transformer 18V AC 2.5a                           | £29.95 |
| GMC-T3  | Open Transformer 24V AC                                | £29.95 |
| GMC-T4  | Open Transformer 2x 12V AC 1a                          | £29.95 |
| GMC-WM1 | Wall Mounted Transformer 16V or 12V DC 1.1a            | £21.95 |
| GMC-WM2 | Wall Mounted Transformer 9v DC (1600mA) 1.6A           | £16.95 |
| GMC-WM3 | Wall Mounted Transformer Kato Unitrack UK Power Supply | £16.95 |
| GMC-WM4 | Wall Mounted Transformer 12v DC Smoothed for Lighting  | £16.95 |
| GMTFK1  | Transformer Fitting Kit for T1/T3/T4                   | £15.95 |
| GMTFK2  | Transformer Fitting Kit for T2                         | £15.95 |

GAUGEMASTER products are available from your local model shop or, in case of difficulty, direct from ourselves

**GAUGEMASTER** Controls Ltd, Ford Road, Arundel, West Sussex, BN18 0BN, United Kingdom

tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com

**www.gaugemaster.com**



# GAUGEMASTER

## The Model Shop for Big Imaginations



Online... **GAUGEMASTER.com**



- Possibly the **largest inventory** in the UK
- **Free UK Delivery** on orders over £15.00
- **Next Day Delivery** only £4.95\*

Here at Gaugemaster we stock the finest model railway items from companies all around the world, so you will almost certainly find what you need – no matter what scale, period, or location that you model.



Find us on Facebook:  
**@Gaugemaster**  
Follow us on Twitter:  
**@Gaugemaster**  
Follow us on Instagram:  
**@gaugemaster\_controls**

## RIGHT LINES

The fortnightly e-Zine from  
**GAUGEMASTER.com**

As well as information on new items, RIGHT LINES contains articles and guides to help you improve your railway. Published every other Friday straight to your inbox, go to [gaugemaster.com/rightlines](http://gaugemaster.com/rightlines) for details on how to subscribe.

Technical and Practical Guides - Product Spotlights - Railway Histories and Visits

## In Store... **The Engine Shed**

- Probably the **biggest store** in the UK
- Open **Seven days a week**, including Bank Holidays\*\*
- **Friendly Helpful Staff**



**OPENING HOURS**

Monday - Saturday: 9:00am - 5:30pm

Sunday & Bank Holidays: 10:30am - 3:30pm

The Engine Shed is conveniently situated right next to **Ford Railway Station** in West Sussex - which is regularly served by **Southern** rail services from **London Victoria, Brighton, and Portsmouth Harbour**.

For those **travelling by car**, we are located to the south of the historic castle town of **Arundel**, just off the **A27** and **A259** in an attractive tourist area, with the towns of **Worthing, Littlehampton, Bognor Regis, and Chichester** nearby, so why not make a day of it? There is plenty of free parking on site.

\*Terms and conditions apply, services not available to all UK addresses - see [gaugemaster.com/nextdayterms.html](http://gaugemaster.com/nextdayterms.html) for details \*\*Closed Christmas Day and New Years Day

**The Engine Shed**, Ford Road, Arundel, West Sussex, BN18 0BN, United Kingdom  
tel - 01903 884488 fax - 01903 884377 email - [sales@gaugemaster.com](mailto:sales@gaugemaster.com)

**[www.gaugemaster.com](http://www.gaugemaster.com)**



## NEW RELEASES - OO GAUGE

### BACHMANN OO



|                                                                  |        |
|------------------------------------------------------------------|--------|
| 39-466 LMS 60ft Porthole Corridor Composite Coach BR Maroon..... | £46.99 |
| 39-471 LMS 60ft Porthole Corridor Brake 1st Coach BR Maroon..... | £46.99 |
| 39-476 LMS 60ft Porthole Open Vestibule Coach BR Maroon.....     | £46.99 |
| 44-598 Scenecraft Platform Accessories.....                      | £19.00 |

### HORNBY OO



|                                                                          |        |
|--------------------------------------------------------------------------|--------|
| R3780 LB&SCR Terrier 0-6-0T No. 655 "Stepney" LNER.....                  | £80.99 |
| R4188D LMS Period II 68ft 6 axle Dining/Restaurant Car No. M236M BR..... | £44.99 |
| R4447B LMS Period III Corridor First Coach No. M1047M BR.....            | £44.99 |
| R4448B LMS Period III Corridor Third Coach No. M12135M BR.....           | £44.99 |



|                                                                     |        |
|---------------------------------------------------------------------|--------|
| R4448B LMS Period III Corridor Brake Third Coach No. M5914M BR..... | £44.99 |
|---------------------------------------------------------------------|--------|



|                                       |       |
|---------------------------------------|-------|
| R7237 Skaledale Ford Anglia 105E..... | £6.49 |
|---------------------------------------|-------|

### OXFORD RAIL OO

|                                                       |        |
|-------------------------------------------------------|--------|
| 76NQ2001 Nissam Qashqai J11 Storm White.....          | £5.75  |
| OR76MW5005 5 Plank Wagon ICI (Lime) Ltd Buxton.....   | £10.95 |
| OR76N7002 LNER N7 Steam Locomotive No. 8011 LNER..... | £93.95 |

## KMRC EXCLUSIVE - OO GAUGE

### HELJAN OO



|                                                                                |         |
|--------------------------------------------------------------------------------|---------|
| 45500 Class 45/1 Diesel Locomotive No. 45 110 "Medusa" BR Blue, weathered..... | £179.00 |
|--------------------------------------------------------------------------------|---------|

Complete with numbered certificate. Expected 3rd Quarter 2020. Pre-orders invited



### BACHMANN OO



|                                                                                 |        |
|---------------------------------------------------------------------------------|--------|
| 33-080T China Clay Wagon BR Bauxite with flat tarpaulins, weathered. Pack of 3. | £54.95 |
|---------------------------------------------------------------------------------|--------|



## BACHMANN SCENECRAFT - OO GAUGE

|                                           |        |
|-------------------------------------------|--------|
| 44-0019 Thatched Cottage.....             | £31.95 |
| 44-0036 Yard Crane and Loading Gauge..... | £31.95 |
| 44-004 Provender Store.....               | £36.95 |
| 44-0040 Toilet Block.....                 | £22.95 |
| 44-0049 Incline Winding House.....        | £23.95 |



|                                        |        |
|----------------------------------------|--------|
| 44-0077 Filling Station.....           | £38.95 |
| 44-0078 Crossing Keeper's Cottage..... | £26.95 |



|                                                         |        |
|---------------------------------------------------------|--------|
| 44-0079 Grain Warehouse.....                            | £69.95 |
| 44-0080 Beach Huts.....                                 | £19.00 |
| 44-0081 Stone Cattle Dock.....                          | £26.95 |
| 44-0082 Hampton Station Building with lights.....       | £62.95 |
| 44-0083 Hampton Station Booking Office with lights..... | £40.95 |
| 44-0084 Hampton Lodge with lights.....                  | £58.95 |
| 44-0085 Office Building with lights.....                | £44.95 |
| 44-0087 Industrial Yard Office.....                     | £17.00 |
| 44-0088 Industrial Stores and Canopy.....               | £49.99 |
| 44-0102 Narrow Gauge Slate Built Water Tower.....       | £19.00 |
| 44-0103 Narrow Gauge Slate Built Coal Store.....        | £19.00 |

|                                            |        |
|--------------------------------------------|--------|
| 44-0104 Narrow Gauge Timber Loco Lift..... | £24.95 |
|--------------------------------------------|--------|



|                                                     |        |
|-----------------------------------------------------|--------|
| 44-0105 Narrow Gauge Slate Processing Building..... | £45.95 |
| 44-0106 Narrow Gauge Slate Boiler House.....        | £21.95 |
| 44-0107 Narrow Gauge Slate Footbridge.....          | £21.95 |
| 44-0108 Narrow Gauge Slate Worker's Cottage.....    | £19.95 |
| 44-0109 Telecommunications Cabin.....               | £23.95 |
| 44-0110 Depot Storage Tanks.....                    | £38.95 |
| 44-0111 Steel Frame Crane.....                      | £50.99 |
| 44-0113 Brick Boiler Room.....                      | £21.95 |
| 44-0116 Fuel Storage Tanks.....                     | £40.95 |



|                                     |         |
|-------------------------------------|---------|
| 44-0117 Modern Servicing Depot..... | £89.95  |
| 44-036 Sectional Lineside Hut.....  | £10.99  |
| 44-041 Sanding Plant.....           | £26.95  |
| 44-050 Four Road Engine Shed.....   | £149.95 |
| 44-055 Site Office.....             | £26.95  |

TO SEE THE WHOLE RANGE WE STOCK VISIT [WWW.KERNOWMODELRAILCENTRE.COM](http://WWW.KERNOWMODELRAILCENTRE.COM)



# BARGAINS - OO GAUGE

## KERNOW EXCLUSIVES OO DIESEL + WAGON PACK + STEAM



32-384Z Class 37 Diesel Locomotive No. 37 672 "Freight Transport Association" £99.99  
32-384ZDC Class 37 Diesel Loco No. 37 672 "Freight Transport Association", DCC Fitted £129.99  
32-384ZDS Class 37 Diesel No. 37 672 "Freight Transport Association", DCC Sound Fitted £169.99



K2106 0-4-4T Class 02 No 30193 BR Black E/E £99.99



38-260Y Delabole Slate Prestlo Wagon Triple Pack, BR Bauxite £49.99

## BARGAIN BUNDLES OO

K2104PACK 02 and Water Column Pack £129.99



K9603 Warship WR Coach Pack £174.99  
K9980 Bachmann City of Truro Cornish Pack £171.95  
K9982 Bachmann Electrification Wagon Pack £49.99  
K9984 Bulwark Tanker Pack £12.99  
K9986 Bachmann Class 47 Midland Counties with HST Barrier Coach £154.99  
K9987 Bachmann Class 37 and Dapol IDA Wagon Pack £179.99  
K9988 Bachmann Premier Charter 4-TC and Dapol Class 73 Pack £299.99  
K9996 Kernow Class 37 Pack with weathered SPAs £229.99  
K9997A West Country Bus Pack - Western National £12.99

K9998A Bachmann Scenecraft China Clay Dries Pack with FREE Landrover £99.99  
K9999A Kernow PBA Tiger and Class 37 Pack with FREE Landrover £249.99

## DAPOL OO

4D-012-005 Class 22 Diesel Locomotive No. D6331 £79.99  
4D-012-006 Class 22 Diesel Locomotive No. D6327 £79.99  
DAGM101 Class 73 Electro-Diesel Locomotive No. 73 101 £134.95

## EXCLUSIVE FIRST EDITIONS OO

13502 Atkinson 4 Axle Oval Tanker Lorry Bulwark £14.99  
20633 Plaxton Pointer Dart Western National £9.99  
29307 Leyland Olympian Type A Southern National £14.99  
31401 ERF KV Flatbed 2 Axle Lorry Sandford Potteries £14.99  
34906 Leyland Olympian Coach Western National £5.99  
36708 Plaxton SLF Dart II Plymouth City £14.99

## HORNBY OO

R1173PACK Western Master Train Track £99.99  
R3359 Railroad Ex-GWR 0-4-0 Steam Locomotive 391 Rothery Industrial 101 Class £22.99  
R3361 Railroad Ex-SR 0-4-0 Steam Locomotive No. 6 Hogarth Stone £14.99  
R3422 Adams Radial 415 Class Steam Locomotive No. 3125 SR Black £83.99  
R3517 Royal Scot Class Steam Locomotive No. 6108 "Seaforth Highlander" LMS £139.99  
R3555 Princess Coronation Class Steam Loco 46256 "Sir William A Stanier FRS" £159.99  
R3566 Merchant Navy Class Steam Locomotive 35014 "Nederland Line" BR E/E £139.99  
R3612 A4 Class Steam Locomotive No. 4468 "Mallard" LNER Blue £149.99  
R4235C LMS Corridor 3rd Class Coach No. M1741M BR Maroon £34.99  
R4656A LMS Non-Corridor 57ft Composite Coach No. 16612 £24.99  
R4657A LMS Non-Corridor 57ft Third Class Coach No. 17718 £24.99  
R4660 All Steel K-Type Pullman Third Class Kitchen Car No. 72 £34.99  
R4677B LMS Non-Corridor 57ft Third Class Brake Coach £24.99  
R4677C LMS Non-Corridor 57ft Third Class Brake Coach £24.99  
R4689 LMS Non-Corridor 57ft Composite Coach No. M16574 £24.99  
R4691 LMS Non-Corridor 57ft Third Class Coach No. M20787M BR Maroon £24.99  
R4691A LMS Non-Corridor 57ft Third Class Brake Coach No. M20788M BR Maroon £24.99  
R4718 58ft Maunsell Rebuild ex LSWR 48ft 6 Compartment Lavatory Brake 3rd Class Coach £29.99  
R4736 Maunsell 6 Compartment Third Class Brake Coach No. 3797 SR Maunsell Green £29.99  
R4926 Sovereign Midland Commuter Coach Pack £99.96  
R4927 Sovereign BR Midland Suburban Coach Pack £99.96  
R6733 LMS 20 Ton Coke Hopper Wagon No. B448672 BR Brown £16.99

# BACHMANN BARGAINS - OO GAUGE

## BACHMANN OO LOCOMOTIVES

31-003A Robinson Class 04 Steam Locomotive No. 6184 LNER Black £77.99  
31-004A Robinson Class 04 Steam Locomotive No. 63762 BR Black E/E, weathered £82.99  
31-040 Class 450 4 Car Desiro EMU Set No. 450 073 South West Trains £239.99  
31-119 BR Standard Class 4MT No. 75035 BR Lined Black L/C, weathered £96.99  
31-169 LVR 2-4-2 Tank No. 50705 BR Lined Black E/E £72.99  
31-379 2EPB 2 Car EMU Set No. 5771 BR Green £124.99  
31-461A C Class 0-6-0 Steam Locomotive No. 1294 Southern Railway Black £74.99  
31-462A C Class 0-6-0 Steam Locomotive No. 31227 BR Black E/E £74.99  
31-480 G2A Steam Locomotive No. 9376 LMS Black £74.99  
31-614 V3 Steam Tank Locomotive No. 67646 BR Lined Black L/C £77.99  
31-615 V3 Steam Tank Locomotive No. 67690 BR Lined Black E/E & Straight Pipes £77.99  
31-627B Class 3F Steam Locomotive No. 3520 LMS Black with Deeley Tender £70.99  
31-728 GWR 3700 Class Steam Locomotive No. 3708 "Killarney" Great Western Green £91.99  
31-883 Midland Class 4F Steam Locomotive No. 3848 Midland Black Crest £71.99  
32-067A Class 43 Warship Diesel Locomotive No. D836 "Powerful" BR Blue £95.95  
32-068 Class 43 Warship Diesel Locomotive No. D838 "Rapid" BR Maroon £95.95  
32-069 Class 43 Warship Diesel Locomotive No. D841 "Roebuck" BR Green £95.95  
32-109 Class 08 Diesel Shunter No. 08 631 "Eagle" Network SouthEast £74.99  
32-176 LMS Crab Steam Locomotive No. 42765 BR Lined Black E/E £86.99  
32-178A LMS Crab Steam Locomotive No. 13174 LMS Lined Black £86.99  
32-227DS Fowler Class 3F 0-6-0 Jinty Steam Locomotive No. 7365 LMS Black £164.95  
32-235 Fowler Class 3F 0-6-0 Jinty Steam Locomotive No. 47673 BR Black L/C, weathered £84.99  
32-250A WD Austent 2-8-0 Steam Loco No. 79250 "Major-General McMullen" LMR Blue £122.99  
32-279A K3 Class Steam Locomotive No. 1304 LNER Lined Black £88.99  
32-281 K3 Class Steam Locomotive No. 61862 BR Lined Black E/E £88.99  
32-289 Class 101 2 Car DMU BR Refurbished White & Blue £136.95  
32-290DS Class 101 2 Car DMU Network SouthEast £203.99  
32-359A BR Standard Class 4MT Tank No. 80092 BR Black E/E £85.99  
32-389 Class 37/4 Diesel Locomotive No. 37 421 Colas Rail £115.95  
32-560 Class A1 Steam Locomotive No. 60117 BRITISH RAILWAYS Apple Green £114.99  
32-561 Class A1 Steam Locomotive No. 60122 "Curlew" BR Express Blue E/E £139.99  
32-860 BR Standard Class 9F Steam Loco No. 92211 BR Black L/C & (SR) Large Tender £137.99  
33-832 25 Ton Queen Mary Brake Van EWS £19.99  
34-328A 50ft Ex-LMS Parcels Van BR Blue, weathered £23.99  
34-630A Mk1 Suburban Second Brake Coach No. M43226 BR Lined Maroon £24.99  
34-630B Mk1 Suburban Second Brake Coach No. M43301 BR Lined Maroon £24.99  
34-700C Mk1 Suburban Composite Coach No. M41014 BR Lined Maroon £24.99  
37-070 5 Plank Wagon No. 24361 Wooden Floor LMS Grey £13.95  
37-088 7 Plank End Door Wagon No. 351270 LMS Grey £13.95  
37-089 7 Plank End Door Wagon No. 127916 NE Grey £12.00  
37-095A Coal Trader Pack, 3 x 7 Plank Private Owner Wagons, weathered £34.95  
37-185A 7 Plank Wagon No. P156917 with Coke Rails (BR) Cory, weathered £14.95  
37-327D 90 Tonne JGA Bogie Hopper BLI 19215 Buxton Lime, weathered £32.95  
37-356 13 Ton Steel Sand Tippler Wagon ZCO No. DB746085 BR £11.00  
37-529B 20 Ton Brake Van No. 182897 LNER Oxide, weathered £18.95  
37-533 20 Ton ZTO Brake Van No. DB951767 BR Grey & Yellow, weathered £19.99  
37-535C 20 Ton Brake Van Car No. B955016 BR Railfreight £17.95  
37-552C 46T POA Mineral Wagon No. TRL 5320 ARC TIGER £15.95  
37-754D 12 Ton GWR Fruit Van No. W134299 BR Bauxite (early), weathered £15.95  
38-042A 31 Ton OBA Open Wagon No. 110719 BR Plasmor Blockfreight £17.99  
38-105 Triple Pack 34 Tonne PNA Wagons Railtrack 5 Rib, weathered £41.99  
38-130B 40T Seacow YGH Hopper Wagon No. DB982624 BR, weathered £32.95  
38-132B 40T Seacow YGH Hopper Wagon No. DB980196 EWS £29.95  
38-243 MBA Megabox Bogie Box Wagon No. 500006 EWS, weathered with buffers £30.95  
38-245A MOA Low-Sided Bogie Box Wagon No. 500336 EWS, weathered £36.95  
38-400A SR 25 Ton Pill Box Brake Van No. 56462 SR Brown £18.95  
38-404A SR 25 Ton Pill Box Brake Van ZTO No. DS56471 BR Brown, weathered £18.95  
38-553A Midland 20 Ton Brake Van No. 134900 LMS Bauxite without Duckets £17.95  
38-554 Midland 20 Ton Brake Van No. M623 Midland Railway Grey £17.95  
38-602A 21 Ton Grain Hopper No. B885659 BR Bauxite (late), weathered £18.95  
38-775 Class A 14 Ton Anchor-Mounted Tank Wagon No. 652 Benzene £15.95  
39-002 Twin Pack BR Mk2A BFK HST Barrier Vehicles BR Blue £54.99  
39-025E Mk1 SK Second Corridor Coach No. M25704 BR Blue & Grey £27.99  
39-032 BR Mk1 SK Second Corridor Coach No. SC24859 Sealink £33.99  
39-053F BR Mk1 SO Second Open Coach No. S3848 BR (SR) Green £27.99  
39-075C BR Mk1 BSK Brake Second Corridor Coach No. M35322 BR Blue & Grey £27.99  
39-100C BR Mk1 RU Restaurant Car No. E1974 BR Blue & Grey £27.99  
39-125C BR Mk1 CK Composite Corridor Coach No. M16153 BR Blue & Grey £27.99  
39-126C BR Mk1 CK Composite Corridor Coach No. M15916 BR Maroon £27.99  
39-127B BR Mk1 CK Composite Corridor Coach No. E15055 BR Crimson & Cream £27.99  
39-150D BR Mk1 FK Corridor Coach No. E13241 BR Blue & Grey £28.99  
39-151F BR Mk1 FK 1st Class Corridor Coach No. E13103 BR Maroon £28.99  
39-152B BR Mk1 FK 1st Class Coach No. E13113 BR Crimson & Cream £27.99  
39-186 BR Mk1 BG Full Brake Coach ORX No. ADB 975612, weathered £32.99  
39-187 BR Mk1 BG Full Brake Coach NCV No. 95310 BR Blue £27.99  
39-188 BR Mk1 BG Full Brake Coach No. 92354 Network SouthEast £27.99  
39-225A Mk1 BCK Brake Composite Coach No. E21222 BR Blue & Grey £27.99  
39-226C Mk1 BCK Brake Composite Corridor Coach No. M21026 £27.99  
39-242 Mk1 FO First Open Coach No. M3002 BR Blue & Grey £27.99  
39-250A BR Mk1 RFO Restaurant Car No. M5 BR Blue & Grey £27.99  
39-251D BR Mk1 RFO Restaurant Car No. M1 BR Maroon £27.99  
39-274 BR Mk1 GUV No. E93326 BR Blue & Grey InterCity Motorail £27.99  
39-277 BR Mk1 GUV Coach No. E86380 BR Blue £28.99  
39-310C BR Mk1 SP Pullman Second Parlour Coach No. 350 Umber & Cream £41.99  
39-312 BR Mk1 SP Pullman Second Parlour Coach No. E352E BR Blue & Grey £41.99

39-360C BR Mk2A TSO Tourist Second Open Coach No. W5316 BR Blue & Grey £30.99  
39-361A BR Mk2A TSO Tourist Second Open Coach No. E5406 BR Blue & Grey "Inter City" £29.99  
39-410A BR Mk2A BFK Brake First Corridor Coach No. W17069 BR Blue & Grey £28.99  
39-412A BR Mk2A BFK Brake First Corridor Coach No. 17097 Network SouthEast £27.99  
39-470A LMS 60ft Port-hole Corridor Brake 1st Coach No. M5077M BR, weathered £31.99  
39-500 BR Mk1 SLF First Class Sleeper Car No. M2002 BR Maroon £32.99  
39-778 LMS 50ft Inspection Coach No. DM45029 EWS Maroon £47.99

## BACHMANN OO SCENECRAFT

42-0002 Stone Engine Shed with Tank £34.99



42-0009 Container Terminal Crane £96.99  
42-0010 Two Wooden Platforms £14.99



42-0013 Post War System-Built Station £28.99  
42-0018 Small Water Tower £19.99  
42-0026 Electrical Relay Room £14.95



42-0032 28ft Static Caravan £24.95



42-0041 Rail Over River Bridge £24.95

42-0050 Coal Drop £26.99



42-0051 Stables £14.95



42-0057 Wooden Barn £15.95



44-189 Gated Single Track Level Crossing £19.99  
44-273 Low Relief Rocket Ron's Record Shop £11.95



44-274 Low Relief Gibbs & Son Guitars £13.95  
44-278 Low Relief Kevin's Carpets £12.95  
44-279 Low Relief Balti Towers £13.95  
44-280 Low Relief Garrhams Getaways £13.95  
44-281 Low Relief Karen's Millinery £12.95  
44-282 Low Relief Jo's Jewellery £14.95  
44-523 CCTV Security Cameras £1.50  
44-534 Satellite Dishes £1.50  
44-535 TV Aerials £1.50  
44-547 Cycle Cabinets £3.00  
44-570 Quayside Corners £17.95  
44-571 Fresh Fish Stall £12.00



## NEW RELEASES - N GAUGE

**DAPOL N**

|                                                                                        |         |
|----------------------------------------------------------------------------------------|---------|
| 2D-022-008 Class 68 Diesel Locomotive No. 68 004 "Rapid" DRS Compass.....              | £119.99 |
| 2D-022-008D Class 68 Diesel Locomotive No. 68 004 "Rapid" DRS Compass. DCC Fitted..... | £140.99 |



|                                                                            |         |
|----------------------------------------------------------------------------|---------|
| 2D-022-011 Class 68 Diesel Locomotive No. 68 034 Compass.....              | £119.99 |
| 2D-022-011D Class 68 Diesel Locomotive No. 68 034 Compass. DCC Fitted..... | £140.99 |



2D-026-003 Class 86 Electric Locomotive No. 86 243 "The Boys Brigade" Intercity Executive £93.95  
2D-026-003D Class 86 Electric Loco No. 86 243 "The Boys Brigade" DCC Fitted £113.95



|                                                                                        |         |
|----------------------------------------------------------------------------------------|---------|
| 2D-022-009 Class 68 Diesel Locomotive No. 68 019 "Brutus" Trans Pennine Express.....   | £119.99 |
| 2D-022-009D Class 68 Diesel No. 68 019 "Brutus" Trans Pennine Express, DCC Fitted..... | £140.99 |



|                                                                                        |         |
|----------------------------------------------------------------------------------------|---------|
| 2D-026-001 Class 86 Electric Locomotive No. 86 401 "Mons Meg" Caledonian Sleeper.....  | £93.95  |
| 2D-026-001D Class 86 Electric Locomotive No. 86 401 "Mons Meg" Caledonian Sleeper..... | £113.95 |



|                                                                                 |         |
|---------------------------------------------------------------------------------|---------|
| 2D-026-004 Class 86 Electric Locomotive No. 86 216 "Meteor" BR Blue.....        | £93.95  |
| 2D-026-004D Class 86 Electric Loco No. 86 216 "Meteor" BR Blue, DCC Fitted..... | £113.95 |



|                                                                                   |         |
|-----------------------------------------------------------------------------------|---------|
| 2D-022-010 Class 68 Diesel Locomotive No. 68 026 DRS Plain Blue.....              | £119.99 |
| 2D-022-010D Class 68 Diesel Locomotive No. 68 026 DRS Plain Blue, DCC Fitted..... | £140.99 |



2D-026-002 Class 86 Electric Locomotive No. 86 259 "Less Ross/Peter Pan" Electric Blue.....**£93.95**  
2D-026-002D Class 86 Electric Loco No. 86 259 "Less Ross/Peter Pan" Electric Blue.....**£113.95**

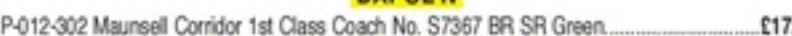
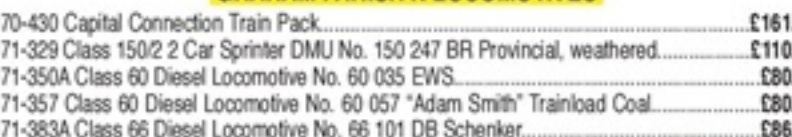


371-658 Class 57/3 No. 57313 West Coast Railways.....£114.95

FOR OUR FULL RANGE PLEASE VISIT [WWW.KERNOWMODELRAILCENTRE.COM](http://WWW.KERNOWMODELRAILCENTRE.COM) // PRICES CORRECT AT TIME OF PRINT & ARE SUBJECT TO CHANGE

## BARGAINS - N GAUGE

**DAPOL N**

**GRAHAM FARISH N LOCOMOTIVES**

|                                                                                      |          |
|--------------------------------------------------------------------------------------|----------|
| 371-398 Class 66 Diesel Locomotive No. 66 779 "Evening Star" GBRI Green.....         | \$84.95  |
| 371-505 Class 101 2 Car DMU Set No. L837 BR Network SouthEast.....                   | \$107.95 |
| 371-877A Class 108 2 Car DMU BR Blue & Grey.....                                     | \$114.95 |
| 372-310 Merchant Navy Class Steam Loco No. 35024 "East Asiatic Company" BR Blue..... | \$114.95 |
| 372-311 Merchant Navy Class Steam Loco No. 35023 "Holland-Africa Line" BR Green..... | \$114.95 |
| 372-312 Merchant Navy Class Steam Locomotive No. 35028 "Clan Line" BR Green.....     | \$118.95 |
| 372-313 Merchant Navy Class Steam Locomotive No. 35021 "New Zealand Line" BR.....    | \$114.95 |
| 372-654 BR Standard Class 4MT Steam Loco No. 76063 BR Lined Black.....               | \$85.95  |
| 372-800A Class A1 Steam Loco No. 60163 "Tornado" BR Lined Brunswick Green.....       | \$110.95 |
| 372-933 N Class 2-6-0 Steam Locomotive No. 810 SECR Grey.....                        | \$105.95 |

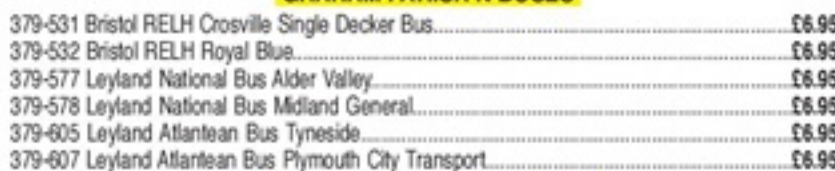
**GRAHAM FARISH N WAGONS + COACHES**



|                                                                             |        |
|-----------------------------------------------------------------------------|--------|
| 374-711 BR Mk2A TSO Tourist Second Open Coach No. 5410 Network SouthEast    | £22.95 |
| 374-830C Stanier Brake Corridor First Coach No. 5057 LMS Crimson Lake       | £20.99 |
| 374-831C Stanier Corridor Brake First Coach No. M5056M BR Crimson & Cream   | £20.99 |
| 374-845C Stanier First Corridor Coach No. 1055 LMS Crimson Lake             | £22.95 |
| 374-846C Stanier First Corridor Coach No. M1081M BR Crimson & Cream         | £20.99 |
| 374-847A Stanier First Corridor Coach No. M1053M BR Maroon                  | £20.99 |
| 374-852A Stanier Vestibule Composite Coach No. M9739M BR Maroon             | £20.99 |
| 374-875 LMS 50ft Inspection Saloon No. 45028 LMS Lined Crimson Lake         | £25.95 |
| 374-876 LMS 50ft Inspection Saloon No. M45029M BR Maroon                    | £25.95 |
| 374-877 LMS 50ft Inspection Saloon No. M45030M BR Blue & Grey               | £26.99 |
| 374-902A BR Mk1 Travelling Post Office Coach No. E80304 BR Blue & Grey      | £29.95 |
| 374-926A BR Mk1 SLF Sleeping First Coach No. M2109 BR Maroon                | £20.99 |
| 374-927A BR Mk1 SLSTP Sleeping Second Coach No. E2681 BR Blue & Grey        | £20.99 |
| 374-990 Mk1 Coach Pack 'Works Train' BR Blue & Grey, weathered              | £39.95 |
| 374-992 Mk1 Coach Pack Hunslet-Bardley Weed Killing Train                   | £40.95 |
| 377-059 5 Plank Wagon Wooden Floor No. 91 Salt Union Ltd. Stone Works       | £10.50 |
| 377-060 5 Plank Wagon Wooden Floor No. 1 A.E. Moody Coal Merchant Sharpness | £10.50 |
| 377-088 7 Plank Wagon End Door No. 06515 GWR Grey                           | £9.95  |



**GRAHAM FARISH N BUSES**



GRAHAM FARISH N SCENECRAFT



|                                            |        |
|--------------------------------------------|--------|
| 42-191 Red Star Parcels Office             | £13.50 |
| 42-192 Platform Buffet                     | £22.95 |
| 42-194 Taxi Office                         | £9.50  |
| 42-197 Coal Distribution Building          | £15.00 |
| 42-198 Power Station Chimney               | £22.95 |
| 42-209 Low Relief Butchers                 | £8.00  |
| 42-219 Low Relief Stone Factory            | £21.95 |
| 42-227 Low Relief Rear Victorian Tenements | £25.95 |
| 42-228 Low Relief Double Track Tunnel      | £9.00  |
| 42-232 Low Relief Pub                      | £10.95 |



|                                           |        |
|-------------------------------------------|--------|
| 42-233 Low Relief Rear of Terraces.....   | £11.50 |
| 42-234 Low Relief Public Convenience..... | £7.95  |
| 42-236 Low Relief Railway Hotel.....      | £12.95 |
| 42-245 Low Relief Corner Chemists.....    | £12.00 |
| 42-253 Low Relief Greengrocers.....       | £12.00 |
| 42-254 Low Relief Power Signal Box.....   | £13.50 |
| 42-296 Low Relief Boiler House.....       | £19.95 |
| 42-299 Low Relief Turbine Hall.....       | £37.95 |
| 42-547 Cycle Cabinets.....                | £3.50  |



We do a DECODER FITTING SERVICE for OO & N Gauge DCC Ready Locomotives...£9.99. If you have other locos you are interested in having fitted with a decoder or sound please phone or email us for a quote



# KERNOW MODEL RAIL CENTRE LIMITED EDITION EXCLUSIVES - OO GAUGE

## KERNOW MODELS OO BULLEID DIESELS - IMMINENT DELIVERY



K2701A Bulleid 1-Co-Co-1 Diesel Loco No. 10201 BR Black E/E...£179.99  
K2702A Bulleid 1-Co-Co-1 Diesel Loco No. 10202 BR Black E/E...£179.99  
K2703A Bulleid 1-Co-Co-1 Diesel Loco No. 10203 BR Black E/E...£179.99  
K2704A Bulleid 1-Co-Co-1 Diesel Loco No. 10203 BR Green L/C...£179.99  
K2705 Bulleid 1-Co-Co-1 Diesel Loco No. 10201 BR Green L/C...£179.99  
K2706 Bulleid 1-Co-Co-1 Diesel Loco No. 10202 BR Green L/C...£179.99

## KERNOW MODELS OO

K2600 Class 41 Warship D600 "Active" BR Blue full yellow ends...£179.99  
K2602 Class 41 Warship D602 "Bulldog" BR Green small yellow ends...£179.99  
K2603 Class 41 Warship D603 "Conquest" BR Green small yellow ends...£179.99



K2604 Class 41 Warship D604 "Cossack" BR Green no yellow ends...£179.99  
K2605 Class 41 Warship D605 "Bulldog" BR Blue small yellow ends...£179.99  
SB006E / F / G / H - YCV Turbot Bogie Ballast Wagon EWS...£29.99 EACH  
SB006I / J / K / L - YCV Turbot Bogie Ballast Wagon Civil Engineers Dutch, weathered...£29.99 EACH  
SB006M YCV Turbot Bogie Ballast Wagon No. DB978026 Civil Engineers Dutch...£29.99  
SB006N YCV Turbot Bogie Ballast Wagon No. DB978080 Civil Engineers Dutch...£29.99  
SB006Y YCV Turbot Bogie Ballast Wagon Pack x 2, pristine...£49.98  
SB006Z YCV Turbot Bogie Ballast Wagon Pack x 4, weathered...£99.96

## BACHMANN OO

31-650L Class 47 Diesel Locomotive No. D1670 "Mammoth" BR Two Tone Green...£149.99  
31-650LDC Class 47 Diesel Locomotive No. D1670 "Mammoth", DCC Fitted...£169.99  
31-650LDS Class 47 Diesel Locomotive No. D1670 "Mammoth", DCC Sound Fitted...£249.99  
31-650Q Class 47 Diesel Locomotive No. 47 829 Police...£149.99  
31-650QDC Class 47 Diesel Locomotive No. 47 829 Police, DCC Fitted...£169.99  
31-650QDS Class 47 Diesel Locomotive No. 47 829 Police, DCC Sound Fitted...£249.99



31-651Z Class 47 Diesel Locomotive No. 47 973 "Midland Counties"...£149.99  
31-651ZDC Class 47 Diesel Locomotive No. 47 973 "Midland Counties", DCC Fitted...£169.99  
31-651ZDS Class 47 Diesel Locomotive No. 47 973 "Midland Counties", DCC Sound Fitted...£249.99  
31-653Z Class 47 Diesel Locomotive No. 47 706 ScotRail NSE branding, weathered...£159.99  
31-653ZDC Class 47 Diesel Loco No. 47 706 ScotRail NSE, weathered, DCC Fitted...£179.99  
31-653ZDS Class 47 Diesel Loco No. 47 706 ScotRail NSE, weathered, DCC Sound Fitted...£259.99  
31-657Z Class 47 Diesel Loco No. 47 701 "Old Oak Common"...£159.99  
31-657ZDC Class 47 Diesel Loco No. 47 701 "Old Oak Common", DCC Fitted...£179.99  
31-657ZDS Class 47 Diesel Loco No. 47 701 "Old Oak Common", DCC Sound Fitted...£259.99



31-662Z Class 47/4 Diesel No. 47 537 "Sir Gwynedd", weathered...£189.95  
31-662ZDS Class 47/4 Diesel No. 47 537 "Sir Gwynedd", weathered, DCC Sound Fitted...£274.95



32-066Z Class 43 Warship No. D845 "Sprightly" BR Green...£164.95  
32-066ZDC Class 43 Warship No. D845 "Sprightly" BR Green, DCC Fitted...£184.95  
32-135X Class 4575 Prairie No. 5541 BR Green, weathered...£119.95



32-384Z Class 37 Diesel Locomotive No. 37 672 "Freight Transport Association"...BARGAIN £99.99  
32-384ZDC Class 37 Diesel Loco No. 37 672 "Freight Transport Association", DCC Fitted...£129.99  
32-384ZDS Class 37 Diesel No. 37 672 "Freight Transport Association", DCC Sound Fitted...£169.99

32-640Z Class 491 4-TC Unit No. 416 BR Blue with small yellow warning panel...£269.95  
32-641Z Class 491 4-TC Unit No. 404 BR Blue & Grey...£269.95  
32-642Z Class 438 4-TC Unit No. 8022 BR Blue & Grey Network SouthEast branding...£269.95  
32-643Z Class 438 4-TC Unit No. 8023 Network SouthEast...£269.95  
32-644Z Class 438 4-TC Unit No. 410 Premier Charter Blue...£269.95  
32-646Z Class 438 4-TC Unit No. 8007 BR Research Department...£269.95



32-727W Class 66 Diesel Locomotive No. 66 709 "Sorrento" GBRf...£169.95  
32-727WDC Class 66 "Sorrento" GBRf, DCC Fitted...£189.95  
32-727WDS Class 66 "Sorrento" GBRf, DCC Sound Fitted...£279.95  
32-762Z Class 57 No. 57 305 Network Rail known as "Sand Castle"...£169.95  
32-762ZDC Class 57 No. 57 305 Network Rail known as "Sand Castle", DCC Fitted...£189.95  
32-762ZDS Class 57 No. 57 305 Network Rail known as "Sand Castle", DCC Sound Fitted...£269.95  
32-777W Class 37 Diesel Locomotive No. 97 304 "John Tiley" Network Rail, weathered...£159.95  
32-777WDC Class 37 "John Tiley" Network Rail, weathered, DCC Fitted...£179.95  
32-777WDS Class 37 "John Tiley" Network Rail, weathered, DCC Sound Fitted...£259.95  
32-780U Class 37 Diesel Locomotive No. 37 261 DRS...£149.99  
32-780UDC Class 37 Diesel Locomotive No. 37 261 DRS, DCC Fitted...£169.99  
32-780UDS Class 37 Diesel Locomotive No. 37 261 DRS, DCC Sound Fitted...£249.99



32-816Z Class 47 No. 47 306 "The Sapper" Railfreight Distribution, weathered...£159.99  
32-816ZDC Class 47 No. 47 306 "The Sapper" Railfreight Distribution, with DCC Fitted...£179.99  
32-816ZDS Class 47 No. 47 306 "The Sapper" Railfreight Distribution, with DCC Sound Fitted...£259.99  
32-935X Class 150/2 2 Car DMU No. 150 216 in First Great Western livery Purple...£189.95  
33-300W 20 Ton Toad Brake Van ZTO No. DW35377 BR Departmental Green...£16.99  
33-300X 20 Ton Toad Brake Van No. 68642 GW Grey "Penzance"...£15.99  
33-300Y 20 Ton Toad Brake Van No. W68366 BR Grey "Gwinear Road"...£15.99  
33-825X 25 Ton Queen Mary Brake Van No. ADS56289 BR Engineers Grey...£24.95  
33-825Y 25 Ton Queen Mary Brake Van No. LDS56293 BR Bauxite "Electrification"...£24.95  
37-238Z 16 Ton Steel Mineral Wagon Triple Pack, weathered...£59.99  
37-537Y 20 Ton Brake Van B954687 BR "TO WORK WITH AIR BRAKED TRAINS ONLY"...£24.95  
38-131Z 40 Ton Seacow YGB Bogie Hopper Wagon Departmental Dutch, weathered...£54.95  
38-132Z 40 Ton Seacow YGB Bogie Hopper Wagon EWS, weathered...£54.95  
38-140W 29 Ton ZXA (ex VDA) Sliding Door Box Van No. ADC201055 Serco...£27.95  
38-140X 29 Ton (ex VDA) Sliding Door Box Van No. ADC200666 BR, weathered...£32.95  
38-326Z High Sided Steel Wagon No. DB479526 "S&T Cable Wagon Guildford", weathered...£24.95  
38-328Z High Sided Steel Wagon No. ADE282721 "AME St Blazey Stores", weathered...£24.95  
39-001T Network SouthEast Mk2 BR Blue & Grey Coach Pack...£59.99  
39-273Z BR Mk1 GUV Coach S86804 BR (SR) Green...£44.95  
39-380Z BR Mk2a BSO Brake 2nd Coach W9426 BR Blue & Grey Network SouthEast...£29.99  
39-411Y BR Mk2a BFK Brake 1st Corridor Coach ADB975654 HST BARRIER VEHICLE...£29.99

## BACHMANN SCENECRAFT OO

44-007Z Cornish Roundhouse...£69.99  
44-008Z Cornish Roundhouse Exterior Walls...£27.99



44-011X GWR Type 7 Signal Box - Truro...£59.99  
44-011Z GWR Type 3 Signal Box - SR...£44.99  
44-059Z China Clay Dries...£73.95  
44-060Z China Clay Dries Chimney...£43.95



44-061Z LSWR Brick Signal Box...£29.99



44-062Y Single Track Stone Engine Shed...£59.99  
44-097Z GWR Water Tower - St Ives...£49.99  
44-120Z SR Boom Water Column and Arm...£22.99

## SCENECRAFT by Bachmann Cornish Terraced Cottages



44-125Z Cornish Terraced Cottages...£39.95



44-500Y Holmans Compressor Twin Pack...£19.99

## DJ MODELS OO

K1003 1914 LSWR Push-Pull Gate Set No. 363 BR Crimson...LOW STOCK £129.99  
K1004 1914 LSWR Push-Pull Gate Set No. 373 BR (SR) Green...£129.99  
K2056 2-4-0WT Beattie Well Tank Steam No. 30587 BR Black L/C...LOW STOCK £109.99  
K2056DC 2-4-0WT Beattie Well Tank Steam No. 30587 BR Black L/C, DCC Fitted...£139.99  
K2102 0-4-4T Class O2 No. 16 "Ventnor" BR Black L/C...£124.99  
K2103 0-4-4T Class O2 No. 30182 BR E/E & push-pull equipment...£124.99  
K2104 0-4-4T Class O2 No. 30225 BR Black L/C...£124.99  
K2105 0-4-4T Class O2 No. 225 Southern Black (1949)...£124.99  
K2106 0-4-4T Class O2 No. 30193 BR Black E/E...BARGAIN £99.99  
K2201 0-6-0 1361 No. 1361 BR Black L/C...£124.99  
K2201A 0-6-0 1361 No. 1361 Photographic Grey...£124.99  
K2202 0-6-0 1361 No. 1362 BR Black E/E...£124.99  
K2203 0-6-0 1361 No. 1363 GWR Shuntbutton Emblem...£124.99  
K2204 0-6-0 1361 No. 1364 GWR with GWR Lettering...£124.99  
K2205 0-6-0 1361 No. 1365 BR Black L/C, weathered...£124.99  
SB002I/K/L PBA TIGER China Clay Wagons ECC International blue...£44.99 EACH  
SB002M/O/P JIA TIGER China Clay Wagons ECC International blue, weathered...£49.99 EACH  
SB004A/B/C/D ZAA PIKE Open Wagons Dutch Civil Engineers...£29.99 EACH  
SB005A/B/C/D SPA Open Wagons BR Railfreight, weathered...£32.99 EACH  
SB005E/F/G/H SPA Open Wagons EWS...£29.99 EACH  
SB006E/F/G/H YCV Turbot Bogie Ballast Wagons EWS...£29.99 EACH  
SB006 YCV Turbot Pack EWS x 4...£109.99  
SB006I/K/L YCV Turbot Bogie Ballast Wagons Civil Engineers Dutch, weathered...£29.99 EACH  
SB006Z YCV Turbot Bogie Ballast Wagon Pack x 4...£109.99  
SB006MN YCV Turbot Bogie Ballast Wagons Civil Engineers Dutch...£29.99 EACH  
SB006Y YCV Turbot Bogie Ballast Wagon Pack x 2...£54.99

## HELJAN OO

K4101 Dogfish BR Black, weathered. Pack of 4...£99.99  
K4102 Dogfish BR Olive, weathered. Pack of 4...£99.99  
K4103 Dogfish BR Civil Engineers, weathered Dutch. Pack of 4...£99.99

## HORNBY OO

R3672 Class 50 Diesel Locomotive No. 50 010 "Monarch" BR Large Logo...£169.99  
R3672 Class 50 Diesel Locomotive No. 50 010 "Monarch", DCC Sound Fitted...£289.99  
R3673 Class 50 Diesel Locomotive No. 50 007 "Sir Edward Elgar" GW150...£169.99  
R3673DS Class 50 Diesel Locomotive "Sir Edward Elgar", DCC Sound Fitted...£289.99  
R3674 Class 31 Diesel Locomotive No. D5579 Golden Ochre...£169.99  
R3674DS Class 31 Diesel Locomotive No. D5579 Golden Ochre, DCC Sound Fitted...£289.99  
R3675 Class 31 Diesel No. 97 204 BR Research livery...£169.99  
R3675DS Class 31 No. 97 204 BR Research, DCC Sound Fitted...£289.99  
R4869 MK3 Driving Van Trailer No. 82124 Network Rail livery...£69.99

## OXFORD DIECAST OO

SP47 Land Rover Series 1 Registration English China Clays International...£4.99

# KERNOW MODEL RAIL CENTRE LIMITED EDITION EXCLUSIVES - N GAUGE



371-465Z Class 37/0 Diesel Locomotive No. 37 207 William Cookworthy...£129.95



374-130Z Mk1 GUV No. E86247 BR Maroon with Parcels Express branding...£34.99



377-490Z China Clay 5 Plank Wagon Pack. English China Clays. 3 x UCV Wagons...£46.99



# THIS MONTH AT

Compiled between 23rd May and 21st June 2019

*Hattons*  
• MODEL RAILWAYS •

## Latest News



### HobbyZone Modular System

The perfect way to keep your modelling area clean and organised. Unlimited potential and opportunity for expansion.

[www.hattons.co.uk/tmah28](http://www.hattons.co.uk/tmah28)



### Dapol Class 86 Latest Batch

The latest of Dapols N Gauge Class 86 have now arrived with versions covering liveries throughout the locomotives history.

[www.hattons.co.uk/tmah29](http://www.hattons.co.uk/tmah29)



### New Heljan Class 45 'Peak'

Covering the 45/0 and 45/1 sub-classes, the model will feature multiple tooling variations and liveries across 14 versions.

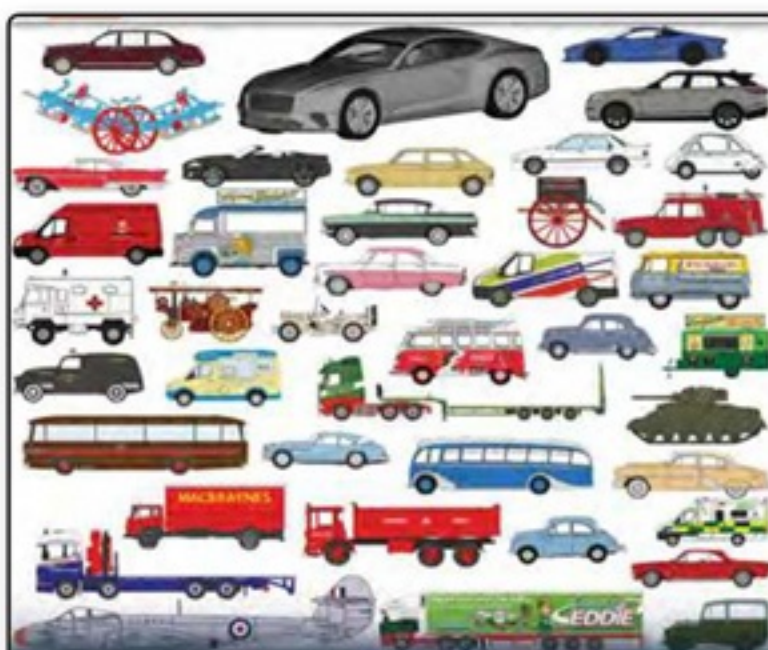
[www.hattons.co.uk/tmah30](http://www.hattons.co.uk/tmah30)



### Additional Hornby Announcements

Hornby revealed an additional 15 products to be added to their 2019 range including additional 'Terriers' and Network Rail Mk3s.

[www.hattons.co.uk/tmah31](http://www.hattons.co.uk/tmah31)



### New Oxford Diecast Items

The 2nd of Oxford's annual announcements includes over 70 items in multiple scales and eras. See them all on our website.

[www.hattons.co.uk/tmah32](http://www.hattons.co.uk/tmah32)



### Crewe 'All Change' Open Day

We recently attended the fantastic charity event hosted at Crewe Diesel Depot. Read Simon Bendall's article on our website now.

[www.hattons.co.uk/tmah33](http://www.hattons.co.uk/tmah33)

## Top 5 Best Sellers



**Hattons**  
HAT-Poster-01 - £6.50



**Atlas Editions**  
4642102 - £3



**Hornby**  
R3292 - £14.50



**Oxford Diecast**  
76JFP004 - £6



**Oxford Diecast**  
76DC001 - £17



# Forthcoming Releases from Hornby & Dapol

All available to pre-order now at: [www.hattons.co.uk](http://www.hattons.co.uk)

**HORNBY®** 2019 Range Announcement - see details at [www.hattons.co.uk/hornby2019](http://www.hattons.co.uk/hornby2019)

## LMS Princess Royal 4-6-2

Pre-order from £171. DCC-fitted £184.50  
Due in Stock February 2020



R3709 6201 "Princess Elizabeth" in LMS crimson lake  
R3711 46206 "Princess Marie Louise" in BR express passenger blue  
R3713 46207 "Princess Arthur of Connaught" in BR maroon

## Class 5101 'Large Prairie' 2-6-2T

Pre-order from £126. DCC-fitted £144  
Due in Stock August/September 2019



R3719 4154 in GWR green  
R3721 6110 in GWR green  
R3723 6145 in BR black with early emblem  
R3725 4160 in BR lined green with late crest

## 4wDM Ruston 48DS

Pre-order for £72  
Due in Stock August 2019



R3704 269595 in pale green  
R3705 458957 in John Dewar and Sons red  
R3706 Army 802 in War Department green  
R3707 "Queen Anne" in Longmorn Distillery livery

## Class B2 Peckett 0-6-0ST

Pre-order from £99. DCC fitted for £117  
Due in Stock June 2019



R3693 No.4 "Sherwood" in Sherwood Colliery lined green  
R3694 "Westminster" in Associated Portland Cement green  
R3695 1455 in National Coal Board lined blue

## Mk3 Sliding Door coaches

Pre-order for £31.50 each  
Due in Stock June to November 2019



36 versions now available to pre-order. Liveries include: GWR, ScotRail and Cross Country. Numerous vehicle types available to allow prototypical formations.

## Mk2F "Aircon" coaches

Pre-order for £31.50 each  
Due in Stock June to November 2019



17 variations now available to pre-order. Liveries include: ScotRail 'saltire', Network Rail, BR blue and grey & Intercity Swallow. Numerous vehicle types available to allow prototypical formations.

## dapol Forthcoming Items

See the full range of forthcoming items from Dapol at [www.hattons.co.uk/dapolpreorders](http://www.hattons.co.uk/dapolpreorders)

### N Gauge

#### Class 50

Pre-order from £123.21  
Due in Stock: August 2019



2D-002-000 D406 in BR blue - unrefurbished  
2D-002-001 50043 in BR blue - unrefurbished  
2D-002-002 50040 "Leviathan" in BR large logo blue  
2D-002-003 50037 "Illustrious" in original Network SouthEast livery

#### Class 67

Pre-order from £108.76  
Due in Stock: August/September 2019



2D-010-008 67006 "Royal Sovereign" in Royal Trail livery  
2D-010-009 67027 "Charlotte" in Colas Rail Freight livery  
2D-010-010 67021 in Belmond British Pullman umber and cream  
2D-010-010 67029 "Royal Diamond" in DB silver

### OO Gauge

#### Class 43xx GWR Mogul 2-6-0

Pre-order from £135.96  
Due in Stock: TBC



4S-043-001 6336 in GWR green with Great Western lettering  
4S-043-002 6385 in GWR green with shirtbutton emblem  
4S-043-003 GWR green with BR smokebox numberplate  
4S-043-004 7324 in BR black with early emblem  
4S-043-005 6364 in BR lined green with early emblem  
4S-043-006 7310 in BR lined green with late crest

#### Class 59

Pre-order from £124.43  
Due in Stock: October to December 2019



4D-005-000 59002 "Alan J Day" in Foster Yeoman livery  
4D-005-001 59103 "Village of Mells" in ARC livery  
4D-005-002 59206 "John F Yeoman" in DB Schenker livery  
4D-005-003 59204 "Vale of Glamorgan" in National Power livery

### O Gauge

#### Class Y3 Sentinel 4wVB

Pre-order from £148.75  
Due in Stock: October to December 2019



7S-005-001 42 in LNER black  
7S-005-002 68163 in LNER black  
7S-005-003 7164 in LMS black  
7S-005-004 7160 in LMS black  
7S-005-005 68164 in BR black with early emblem  
7S-005-006 2 "Isebrook" in GWR green  
7S-005-007 14 in National Coal Board livery

#### BR Mark 1 coaches

Pre-order for £169.15  
Due in Stock: October to December 2019



BSK, SO, SK and CK coaches available in multiple liveries, including: BR crimson and cream, BR green, BR chocolate and cream, BR maroon and BR blue & grey.

All codes start 7P-001-XXX

Tel: 0151 733 3655

Pre-orders accepted  
via credit or debit card only  
Pre-order prices subject to manufacturer RRP.



## 00 Gauge (1:76 Scale)

Bachmann Branchline - Steam locos



30-525 loco Class 49xx 'Hall' 4-6-0 4965 'Rood Ashton Hall' in Great Western green - split from 30-525 set ..... £79



31-921 Class H2 Atlantic 4-4-2 32424 'Beachy Head' in BR black early emblem (RRP £189.95) ..... **BARGAIN** .. £124.50

Hattons Originals - Wagons



H4-RHTT-001 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules ..... £118



H4-RHTT-004 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules - weathered ..... £126

Heljan - DMUs



8940 Class 128 parcels DMU M55993 in BR Blue full yellow ends and flush fronts (RRP £119.95) ..... **BARGAIN** .. £54

Hornby - Steam locos



R3439 Class A1 4-6-2 2554 'Woolwinder' in LNER apple green - gloss finish (RRP £137.50) ..... **BARGAIN** .. £99



R3600TTS Class J36 0-6-0 673 'Maude' in North British Railway lined black - as preserved - TTS sound fitted .. £148.50



R3420 Drummond Class 700 0-6-0 30346 in BR black late crest (RRP £124.99) ..... **BARGAIN** .. £59

## Top 30 Best Sellers

Last 30 Days

Diesel locos



R3605TTS Class 60 60044 'Dowload' in DB Schenker red - TTS sound fitted ..... £184.50

Train sets - digital



R1125 Somerset Belle train set Class 3F 0-6-0 S&DJR 0-6-0 steam loco & 3 wagons - DCC control (RRP £210.99) ... **BARGAIN** .. £130

Digital decoders



R8123 TTS digital sound decoder - Class 50 diesel ..... £38

Track - Code 100 Setrack



R070 Electrically Operated Turntable ..... £64

Oxford Diecast - Buses



76DC001 Duple Commander MkII - Southdown Motor Services **NEW** ..... £17

Peco Products - Track - Code 100 Streamline



SL-100 Pack of 25 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track ..... £66

Track - Code 75 Finescale



SL-100F Pack of 25 1 yard (91.5cm) length of Nickel Silver Finescale flexible track ..... £72

N Gauge

Dapol - Diesel locos



2D-022-009 Class 68 68019 'Brutus' in TransPennine Express **NEW** ..... £119.85

HSTs



2D-019-009 Class 43 HST 4-car book set in GWR Green £160.89



2D-019-007 Class 43 HST 4-car book set in Virgin Trains East Coast **NEW** ..... £160.89

Graham Farish - Diesel locos



371-398 Class 66/7 66779 'Evening Star' in BR green GBRF branding (RRP £134.95) ..... **BARGAIN** .. £89.50

Peco Products - Track - Code 55 Finescale



SL-300F Pack of 30 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track ..... £84

O Gauge (1:43 Scale)

Heljan - Steam locos



4300 Class 43xx Mogul 2-6-0 4339 in GWR green Great Western lettering ..... £662.15

Diesel locos



2006 Class 20 in BR blue full yellow ends, 1980s style warning flashes and headcode discs - Exclusive to Hatton's (RRP £599) ..... **BARGAIN** .. £395

Peco Products - Track - Code 124 Bull-head Streamline



SL-700BH Box of 12 1 Yard O Gauge Flexible Nickel Silver Track (Bullhead Nickel Silver Rail) ..... £75



SL-E792BH Left hand point bullhead rail and electrofrog .. £43



SL-E791BH Right hand point bullhead rail and electrofrog .. £43

Any or Multiple Scales

DCC Concepts - Point motors



DCP-CB12DIP Cobalt ip slow-action digital point motor - pack of 12 ..... £220

Hattons Originals - Digital decoders



DCR-21Pin-Direct-V2 21-pin 6-function 1.1Amp direct plug decoder back EMF ..... £16.50

NCE - Digital controllers



524-042 NCE Power Cab 2 Amp Digital DCC controller ... £154

Peco Products - Point motors



PL-10E Bag of 6 Point Motor Extended Pin (use PL-9) ..... £34



PL-11 Side/surface mounted point motor ..... £8



**Customer  
Service Quality  
Award Winner  
2015 - 2018**

# British Model Railway Awards 2018

## OO Gauge (1:76 Scale) Bachmann Branchline - Steam locos



32-236 Class 3F 'Jinty' 0-6-0T 47619 in early British Railways black (RRP £109.95) ..... **BARGAIN** ..... £74.50  
32-227DS Class 3F Jinty 0-6-0T 7365 in LMS black - DCC sound fitted (RRP £214.95) ..... **BARGAIN** ..... £168



30-525loco Class 49xx 'Hall' 4-6-0 4965 "Rood Ashton Hall" in Great Western green - split from 30-525 set ..... £79  
31-691 Class 5P4F Stanier Mogul 2-6-0 42999 in BR lined black early emblem (RRP £159.95) ..... **BARGAIN** ..... £112



31-214 Class 6P Patriot 4-6-0 45538 'Giggleswick' in BR Green early emblem (RRP £164.95) ..... **BARGAIN** ..... £107  
31-138 Class D11/2 4-4-0 62682 'Haystoun of Bucklaw' in BR black early emblem (RRP £139.95) ..... **BARGAIN** ..... £94  
35-077 Class E4 Brighton tank 0-6-2T 32556 in BR black early emblem (RRP £109.95) ..... **BARGAIN** ..... £76



31-921 Class H2 Atlantic 4-4-2 32424 "Beachy Head" in BR black early emblem (RRP £189.95) ..... **BARGAIN** ..... £124.50



32-166 Class N 2-6-0 1406 in Southern Railway black (RRP £159.95) ..... **BARGAIN** ..... £108

### Diesel locos

32-993 Type 27 Wickham Trolley and trailer MPP0007 in BR engineers yellow wasp stripes (RRP £79.95) ..... **BARGAIN** ..... £55

### DMUs

32-289 Class 101 2 car DMU in BR white and blue passenger figures (RRP £229.95) ..... **BARGAIN** ..... £162

### Coaches



30-525Coaches 2 x Pullman Cars (349 & 335) & 1 x Mk1 BSK in Pullman Umber & Cream - separated from the Shakespeare Express set ..... £70

## Corgi "Trackside" (inc Lledo Diecast Vehicles) - Commercial vehicles

DG148018 Scammell Scarab fatbed & load "The Calico Printers" (RRP £12) ..... **BARGAIN** ..... £4

### Dapol - Diesel locos



4D-022-013 Class 68 68004 "Rapid" in Direct Rail Services blue £131.71  
4D-022-012S Class 68 68015 in Chiltern Railways - DCC Sound fitted (RRP £295.55) ..... **BARGAIN** ..... £220

### DMUs

4D-009-HAT01 Class 121 single car DMU 'Bubblecar' 120 in 'GWR 150' chocolate and cream - Hatton's limited edition (RRP £129.95) **BARGAIN** ..... £105



4D-009-HAT04 Class 121 single car DMU 'Bubblecar' 121020 in Chiltern Railways blue - Hatton's limited edition (RRP £129.95) ..... **BARGAIN** ..... £89  
4D-009-005D Class 121 single car DMU 'Bubblecar' 121027 "Bletchley TMD" in Silverlink purple and green - Digital fitted **NEW** ..... £145  
4D-009-006D Class 121 single car DMU 'Bubblecar' 55026 in BR blue and grey Highland Rail stag - Digital fitted **NEW** ..... £145  
4D-009-007D Class 121 single car DMU 'Bubblecar' 55032 in BR blue and grey Welsh Dragon emblem - Digital fitted **NEW** ..... £145  
4D-009-008D Class 121 single car DMU 'Bubblecar' 55032 in Midline West Midlands - Digital fitted **NEW** ..... £145



4D-009-HAT06 Class 121 single car DMU 'Bubblecar' 960010 in Railtrack 'coaching stock' maroon - Hatton's limited edition (RRP £129.95) ..... **BARGAIN** ..... £89  
4D-009-009D Class 121 single car DMU 'Bubblecar' 977723 in Railtrack red and white - Digital fitted **NEW** ..... £145  
4D-009-003 Class 121 single car DMU 'Bubblecar' W55029 in BR blue & grey (RRP £145) ..... **BARGAIN** ..... £109

### Track Maintenance vehicles

B800 Non-motorised OO Track Cleaner motorised clearing heads & vacuum blue ..... £64

### Signals

4L-002-001 Signal - Motorised, LMS home (OOSIGN3) ..... £25  
4L-001-001 Signal - Motorised, Square post GWR home lower quadrant (OOSIGN1) ..... £25

### DJ Models (Dave Jones) - Steam locos



H1415 Class 14xx 0-4-2T 1432 in BR Lined green late crest - Heavily weathered (RRP £99) ..... **BARGAIN** ..... £74  
H1403 Class 48xx 0-4-2T 4871 in GWR Unlined green Shitbutton logo - Lightly weathered (RRP £99) ..... **BARGAIN** ..... £69  
H1410 Class 58xx 0-4-2T 5819 BR Unlined black early emblem (RRP £99) ..... **BARGAIN** ..... £74

### Golden Valley Hobbies - Diesel locos

GV2015 YEC Janus 0-6-0DE shunter No.201 Port of London blue (RRP £99.95) ..... **BARGAIN** ..... £72

### Hattons Originals - Steam locos



H4-AB14-003 Andrew Barclay 0-4-0ST 14" 2134 "Coronation" in lined green ..... £99



H4-AB16-003 Andrew Barclay 0-4-0ST 16" 2226 "Katie" in lined maroon (RRP £99) ..... **BARGAIN** ..... £89



H4-P-001 SECR P Class 0-6-0T 178 in SE&CR full lined green (with brass) ..... £99



H4-P-015 SECR P Class 0-6-0T 27 in SE&CR full lined green (with polished brass) ..... £99

### Wagons



H4-RHTT-001 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules ..... £118



H4-RHTT-004 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules - weathered ..... £126  
H4-RHTT-005 Rail Head Treatment Train 'Water Jet' 2 wagons and water jetting modules - weathered ..... £126  
H4-RHTT-003 Rail Head Treatment Train 'Water' wagon 3 water modules to extend either RHTT pack to 3 wagons ..... £59

### Heljan - Steam locos



3911 Class 0/3 Tango 2-8-0 63954 in early British Railways black stepped tender (RRP £184.95) ..... **BARGAIN** ..... £84  
3922 Class 0/24 Tango 2-8-0 63975 in BR black late crest flush tender (RRP £184.95) ..... **BARGAIN** ..... £84

### Diesel locos

3529 Class 35 'Hymek' 7001 in BR blue - weathered ..... £118.96



3527 Class 35 'Hymek' D7088 in BR green small yellow panels £110.46

### Electric locos



7704 Class 77 EM2 Woodhead electric 27001 "Ariadne" in BR green - Ltd Ed for Olivias Trains (RRP £165) ..... **BARGAIN** ..... £99



7702 Class 77 EM2 Woodhead electric 27004 "Juno" in BR electric blue - Ltd Ed for Olivias Trains (RRP £165) ..... **BARGAIN** ..... £99

### DMUs



8940 Class 128 parcels DMU M55993 in BR Blue full yellow ends and flush fronts (RRP £119.95) ..... **BARGAIN** ..... £54

### Man-made scenic structure - railway-related



89121 27.4 metre (90ft) Motorised turntable - ready for installation. For HO/OO scales. DCC ready ..... £210

### Hornby - Steam locos



R3292 Class 0F Pug 0-4-0ST 56011 in BR black - Hornby 2014 Collectors Club special edition (RRP £24.99) ..... **BARGAIN** ..... £14.50  
R3213 Class 101 Holden 0-4-0T 3102 in SR Olive Green - Hornby 2013 Collectors Club limited edition (RRP £24.99) ..... **BARGAIN** ..... £16  
R3619 Class 4073 "Castle" 4-6-0 5013 "Abergavenny Castle" BR green late crest ..... £144



R3616 Class 5MT 'Black Five' 4-6-0 5089 in LMS black ..... £153  
R3494 Class 5MT Black 5 4-6-0 45025 in BR Black late crest - Railroad Range ..... £80  
R3552 Class 6800 "Grange" 4-6-0 6890 "Aberporth Grange" GWR green shirtbutton emblem ..... £132  
R3618 Class 7P6F Rebuilt Battle of Britain 4-6-2 34050 "Royal Observer Corps" in BR green late crest ..... £144



R3638 Class 7P6F Streamlined West Country 4-6-2 34019 "Bideford" in BR green late crest ..... £166.50



R3623 Class 8P 'Streamlined Coronation' 4-6-2 6221 "Queen Elizabeth" in LMS coronation blue ..... £171



R3555 Class 8P Princess Coronation 4-6-2 46256 "Sir William Stanier F.R.S" in BR maroon ..... £189



R3439 Class A1 4-6-2 2554 'Woolwinder' in LNER apple green - gloss finish (RRP £137.50) ..... **BARGAIN** ..... £99  
R3284TTS Class A1 4-6-2 4472 'Flying Scotsman' in LNER green - TTS sound fitted - Railroad range ..... £114



R3781 Class A1 Terrier 0-6-0T 5 "Rolvenden" in Kent and East Sussex Railway blue ..... £81



R3782 Class A1 Terrier 0-6-0T 751 in South Eastern and Chatham Railway green ..... £81  
R3518 Class A3 4-6-2 108 "Gay Crusader" in LNER apple green - "The Final Day" special edition (RRP £189.99) ..... **BARGAIN** ..... £120



R3437 Class A3 4-6-2 2503 'Firdaussi' in LNER apple green - gloss finish (RRP £137.50) ..... **BARGAIN** ..... £99



R3508TTS Class A3 4-6-2 60103 "Flying Scotsman" in BR green late crest - as preserved - TTS Sound fitted ..... £187



R3395TTS Class A4 4-6-2 4468 "Mallard" in LNER Garter Blue - TTS sound fitted - Railroad Range ..... £116  
R3447 Class B17 4-6-0 2842 "Silverstone Hall" in LNER green ..... £136



R3600TTS Class J36 0-6-0 673 "Maude" in North British Railway lined black - as preserved - TTS sound fitted ..... £148.50



R3621 Class J36 0-6-0 722 in LNER black ..... £126  
R3668 Class J83 0-6-0T 8482 in LNER green - Railroad range ..... £40.50



R3461 Class L1 Thompson 2-6-4T 67702 in LNER apple green £132



R3603TTS Class LN 'Lord Nelson' 4-6-0 30850 "Lord Nelson" in BR green late crest - TTS sound fitted ..... £175.50



R3635 Class LN 'Lord Nelson' 4-6-0 30863 "Lord Rodney" in BR green early emblem ..... £153  
R3634 Class LN 'Lord Nelson' 4-6-0 851 "Sir Francis Drake" Southern Railway olive green ..... £153



R1183Loco Class P2 2-8-2 2001 "Cock O' The North" in LNER green - Split from Set ..... £70  
R3440 Class P2 2-8-2 2001 "Cock O' The North" in LNER apple green - gloss finish ..... £132



R3329 Class S15 4-6-0 30830 in BR black late crest (RRP £134.99) ..... **BARGAIN** ..... £94



**UK P&P £4 per order**

(free for orders over £175)

**£2.75 per single diecast item**

**£7 guaranteed Next Day delivery**  
(Orders before 2pm)



R3615 Class W4 Peckett 0-4-0ST 560 in Peckett works leaf green builders lettering ..... £90



R3420 Drummond Class 700 0-6-0 30346 in BR black late crest (RRP £124.99) ..... **BARGAIN** ..... £59



R3421 Drummond Class 700 0-6-0 30698 BR Black early emblem (RRP £124.99) ..... **BARGAIN** ..... £59



R3419 Drummond Class 700 0-6-0 693 in Southern Railway black (RRP £124.99) ..... **BARGAIN** ..... £69

### Diesel locos

R3577 4-wheel Sentinel 0-4-0 "Graham" in red ..... £84

R3576 4-wheel Sentinel 4wDH 19 in green chevrons ..... £84



R3504TTS Class 08 08623 in DB Schenker - TTS Sound fitted £139



R3661 Class 31 D5509 in BR green ..... £153



R3658 Class 50 50033 "Glorious" in revised Network SouthEast ..... £136



R3605TTS Class 60 60044 "Dowlow" in DB Schenker red - TTS sound fitted ..... £184.50

R3657 Class 60 60070 "John Loundon McAdam" in Load-Haul triple grey ..... £162

### HSTs

R3510 Pair of Class 43 HST Power Cars 43167 and 43168 in GWR green - Ltd Ed (RRP £289.99) ..... **BARGAIN** ..... £246

### Electric locos



R3580 Class 87 87035 "Robert Burns" in BR Blue ..... £144

### Train sets - analogue

R1247 Hornby Junior starter Train Set - "Paddington Bear" - battery powered **NEW** £31.50

### Train sets - digital



R1173 e-Link DCC Western Master train set GWR Class 2721 steam loco & 3 wagons ..... £130



R1125 Somerset Belle train set Class 3F 0-6-0 S&DJR 0-6-0 steam loco & 3 wagons - DCC control (RRP £210.99) ..... **BARGAIN** ..... £130



R1184 Western Express digital train set eLink TTS sound Hall steam loco (RRP £329.99) ..... **BARGAIN** ..... £199

### Coaches

R4525 Collect restaurant car 9578 in GWR chocolate & cream - Railroad Range (RRP £21.99) ..... **BARGAIN** ..... £10

R4168D ex-LMS Period II 12-wheel dining car M236M BR crimson and cream **NEW** ..... £45

R4448B ex-LMS Period III brake third M5914M in BR crimson and cream **NEW** ..... £45

R4447B ex-LMS Period III FK first corridor M1047M in BR crimson and cream **NEW** ..... £45



R4448B ex-LMS Period III SK second corridor M2139M in BR crimson and cream **NEW** ..... £45



R4619A Mk1 BG full brake M80532 in BR maroon (RRP £24.99) ..... **BARGAIN** ..... £18



R4621A Mk1 TSO second open E4558 in BR maroon (RRP £24.99) ..... **BARGAIN** ..... £18

R4643A Mk1 TSO second open M4473 in BR maroon (RRP £24.99) **BARGAIN** ..... £18

R4666B Mk4 TSO second open 12425 in East Coast (RRP £28.49) **BARGAIN** ..... £14

### Wagons



P6836 8 ton box vans in LMS bauxite - pack of three - railroad range ..... £21

R6727A Horse box 42442 in LMS maroon (RRP £21.99) ..... **BARGAIN** ..... £9

R6885 Pack of three HEA coal hopper wagons in EWS maroon ..... £38

### Digital decoders



R8123 TTS digital sound decoder - Class 50 diesel ..... £38

### Digital control accessory decoders

R8247 DCC accessory decoder (for points and motors) ..... £41.50

### Digital controllers

R8213 DCC Select unit (boxed) ..... £98

### Point motors

R8014 Mark 2 Point Motor (RRP £9.99) ..... **BARGAIN** ..... £5

R8243 Surface Mounted point motor ..... £8.50

### Electronic components

R8211 Rolling road for all types of steam loco (utilises live rollers) ..... £76

R803 Buffer Stop ..... £1.80

R803 Box of 6 ..... £9.50

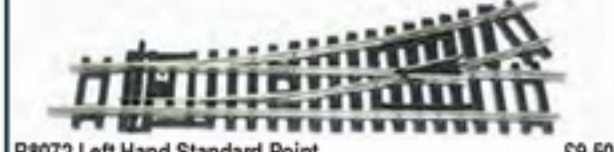
### Track - Code 100 Setrack

R601 Double Straight ..... £2

R601 Box of 24 ..... £47



R070 Electrically Operated Turntable ..... £64



R8072 Left Hand Standard Point ..... £9.50

R8073 Right Hand Standard Point ..... £9.50

R600 Standard Straight ..... £1.50

R600 Box of 24 ..... £34

### Cars

R7237 Mr Weasley's enchanted Ford Anglia - Harry Potter range **NEW** ..... £6

### Kadee - Model railway spares

18KADEE NEM362 Kadee coupling - Medium (8.63mm) - Pack of four ..... £5

### Northcord Model Company - Buses



UK6509 ADL Enviro400 MMC - "Abellio London" **NEW** ..... £51

### Oxford Diecast - Buses



76MW6005 Bristol MW6G - Thames Valley **NEW** ..... £17



76DC001 Duple Commander MkII - Southdown Motor Services **NEW** ..... £17

76LRT009 Leyland Royal Tiger Alexander Bluebird ..... £15

### Cars

76JFP003 Jaguar F Pace Italian Racing Red **NEW** ..... £5

76NQ2001 Nissan Qashqai J11 Storm White **NEW** ..... £5

76JFP004 Jaguar F Pace - Police demonstrator vehicle **NEW** ..... £6



76SFE010 Scania CP31 Pump Ladder - Shropshire Fire & Rescue **NEW** ..... £16

76TN004 Thornycroft Nubian Isle of Man Airports Board Fire Service **NEW** ..... £16

### Commercial vehicles



76DXF004 DAF XF Euro 6 Livestock Transporter - Skeldons Transport **NEW** ..... £21

76TX400CC TX4 Taxi - Coca Cola **NEW** ..... £5

### Farming and construction



76JS002 JCB JS220 - Commemorative Silver Millionth Machine built **NEW** ..... £20

76SET64 2 Piece Set First Overland **NEW** ..... £11

### Military ground vehicles



76JXC002 JCB 3CX (1980s) Army **NEW** ..... £18.50

### Peco Products - Model railway spares

SL-11 Insulated rail joiners/fishplates (for OO, HO & O gauge code 100 rails incl. Hornby, Peco & Peco Streamline) - Pack of 12 ..... £2

SL-10 Metal rail joiners/fishplates (for code 100 rails incl. Hornby, Peco & Peco Streamline) - For OO, HO & O gauge - Pack of 24 ..... £2.20

Man-made scenic structure - railway-related LK-65 76ft Well type turntable ..... £41

### Track - Code 100 Setrack

ST-244 Setrack curved double radius right handed point insulfrog ..... £14.50

ST-241 Setrack No.2 radius left handed point insulfrog ..... £9

ST-240 Setrack No.2 radius right handed point insulfrog ..... £9

### Track - Code 100 Streamline



SL-100 Pack of 25 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track ..... £66

SL-100 Single ..... £3

SL-102 1 yard (91.5cm) length of Nickel Silver concrete-sleeper flexible track ..... £3

SL-102 Pack of 25 ..... £70

SL-E99 3 way medium radius point - electrofrog ..... £27

SL-99 3 way medium radius point - insulfrog ..... £27

SL-90 Double slip - insulfrog ..... £33



SL-E96 Left hand medium point - electrofrog ..... £10.50

SL-96 Left hand medium point - insulfrog ..... £10.50

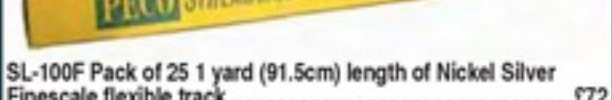
SL-92 Left hand small point - insulfrog ..... £9.50

SL-E88 Right hand large point - electrofrog ..... £12



SL-E95 Right hand medium point - electrofrog ..... £10.50

SL-95 Right hand medium point - insulfrog ..... £10.50



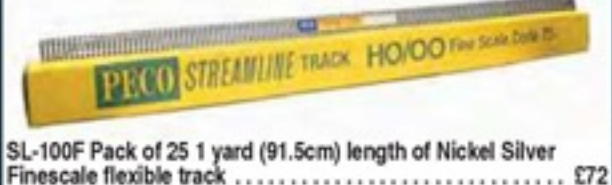
SL-E91 Right hand small point - electrofrog ..... £9.50

SL-91 Right hand small point - insulfrog ..... £9.50

### Track - Code 75 Finescale

SL-108F 1 yard (91.5cm) length of Code 75 Wooden-sleeper nickel silver bullhead rail flexible track ..... £4

SL-108F Box of 25 ..... £98



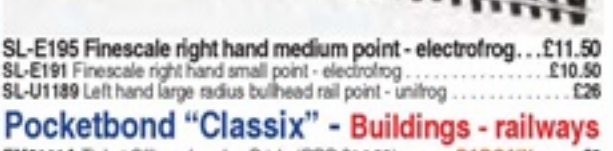
SL-100F Pack of 25 1 yard (91.5cm) length of Nickel Silver Finescale flexible track ..... £72

SL-100F Single ..... £3



SL-E196 Finescale left hand medium point - electrofrog ..... £11.50

SL-E192 Finescale left hand small point - electrofrog ..... £10.50



SL-E195 Finescale right hand medium point - electrofrog ..... £11.50

SL-E191 Finescale right hand small point - electrofrog ..... £10.50

SL-U1189 Left hand large radius bullhead rail point - unitrog ..... £26

### Pocketbond "Classix" - Buildings - railways

EM6111A Ticket Office - London Brick. (RRP £14.99) ..... **BARGAIN** ..... £8

### Cars

EM76834 Morris Minor Convertible in maroon hood up (RRP £3.99) **BARGAIN** ..... £2.50

### Commercial vehicles

EM76662 Austin A30 van "County Fire Service" (RRP £4.25) **BARGAIN** ..... £1.50

EM76509 Jen-Tug & Articulated trailer "British Railways" Fleet No. YE 3062 N (RRP £6.99) ..... **BARGAIN** ..... £3

### N Gauge

### Dapol - Steam locos

2S-011-002D Class A10 4-6-2 103 "Flying Scotsman" in LNER wartime black - DCC Fitted ..... £144



2S-012-010 Class A1X "Terrier" 0-6-0T 32636 in BR black late crest ..... £67.96



2S-012-009 Class A1X "Terrier" 0-6-0T 40 "Brighton" in LB&SCR improved engine green ..... £67.96



2S-012-012 Class A1X "Terrier" 0-6-0T 734 in London and South Western Railway green ..... £67.96

2S-012-011 Class A1X "Terrier" 0-6-0T 751 South Eastern and Chatham Railway green ..... £67.96

### Diesel locos

2D-022-008 Class 68 68004 "Rapid" in Direct Rail Services blue **NEW** ..... £119.85

2D-022-002S Class 68 68005 "Delfant" in DRS - DCC Sound fitted ..... £242.72

2D-022-004 Class 68 68014 in Chiltern Railways (RRP £135) **BARGAIN** ..... £84



2D-022-009 Class 68 68019 "Brutus" in TransPennine Express **NEW** ..... £119.85



2D-022-010 Class 68 68026 in Direct Rail Services plain blue **NEW** ..... £119.85

2D-022-011 Class 68 68034 in Direct Rail Services blue **NEW** ..... £119.85

### HSTs

2D-019-006 Class 43 HST 4-car book set in East Coast silver ..... £160.89



2D-019-008 Class 43 HST 4-car book set in GNER blue ..... £160.89



2D-019-009 Class 43 HST 4-car book set in GWR Green ..... £160.89



2D-019-007 Class 43 HST 4-car book set in Virgin Trains East Coast **NEW** ..... £160.89

### Electric locos

2D-026-004 Class 86/2 86216 "Meteor" in BR blue ..... £93.46

2D-026-004D Class 86/2 86216 "Meteor" in BR blue - DCC fitted **NEW** ..... £113.27



2D-026-003 Class 86/2 86243 "The Boys Brigade" in Intercity Executive ..... £93.46

2D-026-003D Class 86/2 86243 "The Boys Brigade" in Intercity Executive - DCC fitted **NEW** ..... £113.27



# Looking for that perfect second-hand model?

[www.hattons.co.uk/latestpreowned](http://www.hattons.co.uk/latestpreowned)



2D-026-002 Class 86/2 86259 "Les Ross/Peter Pan" in BR electric blue - as preserved. £93.46  
2D-026-002D Class 86/2 86259 "Les Ross/Peter Pan" in BR electric blue - as preserved - DCC fitted **NEW** £113.27  
2D-026-001D Class 86/4 86401 "Mons Meg" Caledonian Sleeper blue - DCC fitted **NEW** £113.27

## DMUs

2D-009-003D Class 121 "Bubble Car" DMU W55033 BR green speed whiskers - DCC fitted. £135.78



ND116B Class 142 'Pacer' 2 car DMU 142081 in Regional Railways. £115.18



ND116D Class 142 'Pacer' 2 car DMU 142085 in Arriva Trains Wales (RRP £135.50). **BARGAIN** £94  
ND116DD Class 142 'Pacer' 2 car DMU 142085 Arriva Trains Wales - DCC fitted £136.93

## Coaches

2P-005-423 Mk3 TFO first open 41083 in Virgin Trains East Coast **NEW** £21.39



2P-005-422 Mk3 TFO first open 41159 in Virgin Trains East Coast **NEW** £21.39  
2P-005-437 Mk3 TSO second open 42130 in Virgin Trains East Coast **NEW** £21.39  
2P-005-434 Mk3 TSO second open 42163 in Virgin Trains East Coast **NEW** £21.39  
2P-005-436 Mk3 TSO second open 42228 in Virgin Trains East Coast **NEW** £21.39  
2P-005-435 Mk3 TSO second open 42286 in Virgin Trains East Coast **NEW** £21.39

## Graham Farish - Diesel locos



371-398 Class 66/7 66779 "Evening Star" in BR green GBRF branding (RRP £134.95). **BARGAIN** £89.50

## Oxford Diecast - Buses

NWFA002 Weymann Fanfare Grey Cars **NEW** £7

## Cars

NMGB003 MGB Roadster British Racing Green **NEW** £4

## Peco Products - Model railway spares

SL-310 Rail Joiners/Fishplates for N & OO gauge (24 per pack). £2

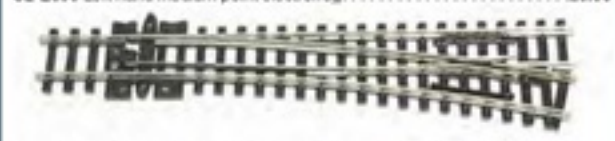
## Track - Code 80 Setrack

ST-11 Double straight track. £1.60  
ST-6 Setrack left hand point insulfrog. £8.50  
ST-5 Setrack right hand point insulfrog. £8.50

## Track - Code 80 Streamline



SL-300 Pack of 25 1 yard (91.5cm) length of Wooden-Sleeper Nickel Silver Flexible track. £64  
SL-300 Single. £3  
SL-E395 Left hand medium point electrofrog. £9.50



SL-E395 Right hand medium point electrofrog. £9.50  
SL-395 Right hand medium point insulfrog. £9.50

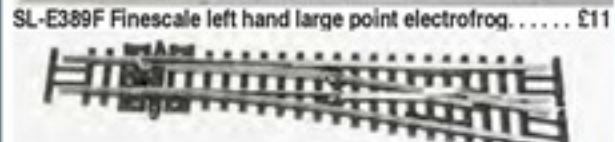
## Track - Code 55 Finescale



SL-300F Pack of 30 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track. £84  
SL-300F Single. £3  
SL-E399F 3-way finescale turnout. £28



SL-E389F Finescale left hand large point electrofrog. £11



SL-E391F Finescale right hand small point electrofrog. £9.50

## OO9 Narrow Gauge

### Peco Products - Track - Code 80 Setrack

ST-400 Setrack narrow gauge starter track pack - first radius. £68

### Track - Code 80 Streamline

SL-400 1 yard length of OO9 narrow gauge flexible track worn wooden sleepers. £3.50  
SL-400 Box of 25. £83

## O Gauge (1:43 Scale)

### Corgi Collectables - Cars

**NEW**



VA11912 Ford Cortina Mk4 3.0 Savage - Strato Silver **NEW**. £27

**NEW**



VA04118 Ford Lotus Cortina Mk2 FVA - Group 5, Graham Hill, 1967 British Saloon Car Championship **NEW**. £27  
VA13005 MGB Roadster V8, Don Hayter's car, Brooklands Green **NEW**. £27  
VA08102 Volkswagen Type 2 Camper - Sea Blue and Cumulus White **NEW**. £27

### Dapol - Steam locos

7S-007-0045 Class 57xx 0-6-0PT pannier L94 in London Transport maroon - DCC sound fitted (RRP £428.20). **BARGAIN** £314  
7S-025-001 Class 74xx 0-6-0PT pannier 74111 GWR green (RRP £400) **BARGAIN** £295  
7S-007-003S Class 8750 0-6-0PT pannier 8659 GWR green - DCC sound fitted. £367.60

## Coaches



7P-004-003 GWR 59' Auto Coach in GWR chocolate and cream shirtbutton. £170

## Wagons

7F-056-013 12-ton van planked sides B758511 in BR bauxite. £42.46  
7F-018-003 4 wheel salt van "Tees Salt" **NEW**. £42.50  
7F-018-003W 4 wheel salt van "Tees Salt" - weathered **NEW**. £42.50

### Heljan - Steam locos

6125 Class 41xx "Large Prairie" 2-6-2T 4144 in BR black early emblem. £594.15



4300 Class 43xx Mogul 2-6-0 4339 in GWR green Great Western lettering. £662.15



4313 Class 43xx Mogul 2-6-0 5306 in BR unlined green late crest. £662.15  
6102 Class 61xx "Large Prairie" 2-6-2T 6144 in BR black early emblem. £594.15

## Diesel locos



2596 Class 05 shunter in industrial maroon with wasp stripes on bufferbeam (RRP £395). **BARGAIN** £195

2595 Class 05 shunter in NCB National Coal Board green red detailing and wasp stripes (RRP £395). **BARGAIN** £195



2006 Class 20 in BR blue full yellow ends, 1980s style warning flashes and headcode discs - Exclusive to Hatton's (RRP £599). **BARGAIN** £395



2558 Class 25/3 in BR blue - unnumbered. £446.25  
2555 Class 25/3 in BR green small yellow panels - unnumbered. £446.25  
4202 Class 42 Warship D869 "Zest" BR maroon small yellow panels (RRP £099). **BARGAIN** £495

5286 Class 52 D1061 "Western Envoy" in BR maroon small yellow panels. £509.15

## DMUs



8951 Class 128 parcels DMU 55993 in Royal Mail red (RRP £599). **BARGAIN** £295

8913 Class 128 parcels DMU M55994 BR blue yellow ends (RRP £599) **BARGAIN** £295

8950 Class 128 parcels DMU M55995 BR blue yellow ends (RRP £599) **BARGAIN** £295



8914 Class 128 parcels DMU W55991 in BR blue - weathered (RRP £599). **BARGAIN** £295

8912 Class 128 parcels DMU W55992 BR green yellow doors (RRP £599). **BARGAIN** £295



8911 Class 128 parcels DMU W55993 in BR green speed whiskers (RRP £599). **BARGAIN** £295

## Coaches

4921 Mk1 BSK brake second corridor in BR maroon - unnumbered. £169.50

4931 Mk1 FK first corridor in BR maroon - unnumbered. £169.50

## Wagons



1043 200xx VBA sliding door van in EWS (RRP £84.95). **BARGAIN** £29.50



5053 Bogie covered IWB Cargowagon 2797589 - "GB-Continent" (RRP £189). **BARGAIN** £79

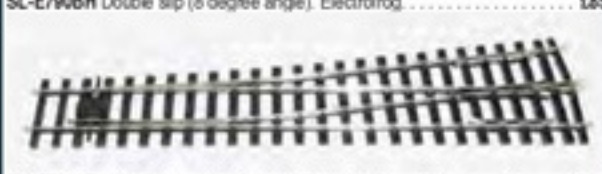
## Oxford Diecast - Commercial vehicles

43LR006 Land Rover Lightweight Hard Top - "Fred Dibnah". £14.50  
43LR2A002 Land Rover Series IIA SWB Hard Top Royal Mail (PO Recovery) **NEW**. £14.50

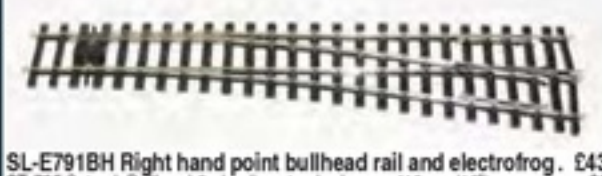
**Peco Products - Track - Code 124 Bullhead Streamline**



SL-700BH Box of 12 1 Yard O Gauge Flexible Nickel Silver Track (Bullhead Nickel Silver Rail). £75  
SL-700BH Single. £6.50  
SL-E700BH Double slip (8 degree angle). Electrofrog. £85



SL-E792BH Left hand point bullhead rail and electrofrog. £43



SL-E791BH Right hand point bullhead rail and electrofrog. £43

ST-725 Setrack Bullhead 2nd radius standard curve 410mm (16"). £8

ST-4751 Setrack second radius Unifrog left hand point. £43

## Any or Multiple Scales

### Bachmann Branchline - Digital control accessory decoders

36-561 EZ Command Dynamics 4-output point decoder (similar to Homby R8247) (RRP £45.35). **BARGAIN** £24.50

### Dapol - Digital decoders

Imperium1 "Imperium" 21 Pin MTC 6-function DCC decoder. £19.51

### DCC Concepts - Digital decoders

DCD-ZBHP-6-3 Zen Black - O and large scale 6 function decoder stay alive - pack of three. £170

### Digital control accessories

DCD-ABC-6 ABC module for Zen Black decoders - pack of six **NEW**. £51

DCD-ABC-3 ABC module for Zen Black decoders - pack of three **NEW**. £30



DCD-AEC Cobalt Alpha Central - 12-way switch control panel £111

## Point motors



DCP-CB12DIP Cobalt slip slow-action digital point motor - pack of 12. £220

DCP-CBSS-12 Cobalt SS surface mounted point motors controller - pack of 12. £245

### Gaugemaster Controls

#### Analogue controllers



COMBI Combi 12v 1Amp Single Track Controller Transformer £34



D Double/Twin track power controller. £86

Q Quadruple 4 circuit power controller. £149

### Analogue point control

CDU Capacitor Discharge Unit. £11

### Digital decoders

BPDC27 8 & 21-pin 4-function 1A (1.8A peak) small OMNI decoder (measures 231 x 27w x 10d mm) x 5. £79

### Digital controllers

DCC01 Prodigy Express 1.6 Amp DCC controller - complete starter pack £159.50

### Digital control accessories



BPDC80 Pack of three DCC autofrog polarity reversal switches - for electrofrog points. £14.50

### Point motors

PM-1 Sleep point motor accessory switch. £5

### Hattons Originals - Digital decoders



DCR-21Pin-Direct-V2 21-pin 6-function 1.1Amp direct plug decoder back EMF. £16.50

DCR-4PIN-Direct 8-pin 2-function 1.1Amp direct plug decoder back EMF. £17

DCR-8PIN-HarnessMini 8-pin (harness) 2-function 1.1Amp small & thin (mini) decoder back EMF. £16

DCR-8PIN-HarnessMini Box of 5. £72

DCR-8PIN-HarnessMini Box of 10. £128



DCR-8PIN-Harness Box of 10 8-pin (harness) 4-function 1.1Amp decoder back EMF. £120

DCR-8PIN-Harness Single. £15

DCR-8PIN-Harness Box of 5. £68

DCR-8PIN-Direct 8-pin compact direct 4-function 1.1Amp decoder back EMF. £17

DCR-8PIN-Direct Box of 5. £77

DCR-8PIN-Direct Box of 10. £136

### Hornby - Digital decoders



R8249 Box of 6 8-pin 4-function 2-sided 0.5A (1A peak) decoder. £92

R8249 Single. £16

R8117 TTS DCC Sound Decoder 8 pin plug - "Princess Coronation" 4-6-2 steam loco. £36

R8101 TTS DCC Sound Decoder 8 pin plug - Class 31 diesel. £36

R8110 TTS DCC Sound Decoder 8 pin plug - Class 4073 "Castle" 4-6-0 steam loco. £36

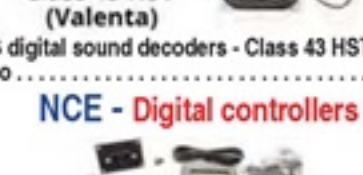
R8106 TTS DCC Sound Decoder 8 pin plug - Gresley Class A1 and A3 4-6-2 steam locos. £36

R8107 TTS DCC Sound Decoder 8 pin plug - Gresley Class A4 4-6-2 steam loco. £36



R7146 TTS digital sound decoder - Class 08 shunter. £38

R7142 TTS digital sound decoder - Gresley Class P2 steam loco. £38



R7140 TTS digital sound decoders - Class 43 HST Valenta - Pack of two. £63

### NCE - Digital controllers



524-042 NCE Power Cab 2 Amp Digital DCC controller. £154

### Digital control accessories

524-037 SB5 5 amp Power Booster DCC system for use Power Cab controller. £177

### Peco Products - Point motors

PL-10 Point Motor (Switch Machine). £6

PL-10 Bag of 6. £34



PL-10E Bag of 6 Point Motor Extended Pin (use PL-9). £34

PL-10E Single. £6



PL-11 Side/surface mounted point motor. £8

### Switches and levers

PL-13 Accessory Switch for fitting to turnout motor PL10. £3

PL-26R Passing Contact Switch - Red Lever. £6

PL-26Y Passing Contact Switch - Yellow Lever. £6





# SELL YOUR SECONDHAND LOCOS & ROLLING STOCK FOR CASH

• Trade in with Hatton's for immediate payment on the day that we receive your goods!\* •

Get cash for your items in just four easy steps...



“An excellent service everytime without fault. I honestly cannot think of anything which could be improved.”  
Mr A Manning, Ipswich

“Excellent service, fair offer. I will not hesitate to use their service again and recommend it. Thank you.”  
Mr P Shaddock, Portsmouth

“Very simple to trade-in. Beforehand I had thoughts of problems with couriers and the like, but all very easy.”  
Mr D Spottiswoode, Kilmarnock

ALL brands purchased including:



**Sell yours today at [www.hattons.co.uk/preowned](http://www.hattons.co.uk/preowned)**  
**Call us on 0151 541 6914 or email [preowned@hattons.co.uk](mailto:preowned@hattons.co.uk)**

**17 Montague Road, Widnes WA8 8FZ**

Phone opening times: Mon to Sun 9am-5pm.

\*Same day payments apply for collections received Monday to Friday before 2pm. Any collections received after 2pm will be paid on the next working day.  
Please note: In the unlikely event that we revise our offer price for your collection, you will be paid after responding to our offer.  
For full Terms and Conditions, visit [www.hattons.co.uk/preowned/terms](http://www.hattons.co.uk/preowned/terms)



# 'Something for Everyone'

## New! New! New! New! New! New!

**VALUE WEATHERING** - We are constantly striving to improve our products and services. Even our usual 'standard' weathering level relentlessly increases in detail. Our value weathering range has evolved through constant advances in our production process, which is how we are able to offer our weathering at a reduced price. This revised pricing structure takes this efficiency into consideration.

*"Our value weathering service results in stunningly enhanced models every time!"*

Value weathering is completed to a light to medium finish, representing great value for money. So whether you intend to continue with us on our quest for ever higher detail (both standard & especially deluxe weathering ranges) or you wish your money to go even further (value weathering), we have **'Something for Everyone'**.



## WEATHERING PRICING

### Value | Standard | Deluxe



#### SMALL WAGONS

N & OO - £3.50 | £6.00 | £9.50  
O - £8.00 | £14.00 | £25.00



#### COACHES

N & OO - £7.00 | £10.00 | £15.00  
O - £15.00 | £22.00 | £35.00



#### MODERN IMAGE LOCOMOTIVES

N & OO - £20.00 | £30.00 | £50.00  
O - £60.00 | £90.00 | £120.00



#### LARGE WAGONS

N & OO - £7.00 | £10.00 | £15.00  
O - £15.00 | £22.00 | £35.00



#### STEAM LOCOMOTIVES

N & OO - £20.00 | £30.00 | £50.00  
O - £60.00 | £90.00 | £120.00



#### 2 CAR MULTIPLE UNIT

N & OO - £35.00 | £50.00 | £80.00  
O - £60.00 | £90.00 | £120.00

Please note customer supplied pricing is slightly more expensive, contact us or visit our website for details. Additional cars for multiple units charged at coach prices. Some models don't fit into the above categories and prices may vary as a result.

#### CONTACT US

sales@tmc-direct.com  
01947 899125  
www.themodelcentre.com

#### OPENING HOURS

Monday - Friday: 8am - 5pm  
Saturday: 8am - 2pm  
Sunday: By Appointment

#### VISIT US

The Model Centre  
Hill Farm, Beck Hole  
Whitby, YO22 5LF

#### POSTAGE

All prices subject to £4.95 P&P.  
Next day delivery available from £6.95  
Highlands and islands subject to surcharge and will be sent via Royal Mail, size/weight restrictions apply.



# THE HEREFORD MODEL CENTRE

4 COMMERCIAL ROAD, HEREFORD, HEREFORDSHIRE, HR1 2BB

Email: [sales@herefordmodels.co.uk](mailto:sales@herefordmodels.co.uk)  
www.herefordmodels.com

SALES LINE 01432 352809

OFFICIAL HORNBY  
COLLECTORS CENTRE

FOR THE YOUNGER RAIL ENTHUSIAST  
WE ARE NOW STOCKING  
BRIO WOODEN TRAINS

NEW EXCLUSIVE  
PECO N GAUGE TTA TANK WAGONS  
AVAILABLE NOW



|           |                               |        |
|-----------|-------------------------------|--------|
| NR-P987A  | Shell/BP black no. 5170       | £13.45 |
| NR-P987B  | Shell/BP black no. 5175       | £13.45 |
| NR-P987C  | Shell/BP black no. 5177       | £13.45 |
| NR-P987DW | Shell/BP black no. 5169 withd | £14.25 |
| NR-P987EW | Shell/BP black no. 5172 withd | £14.25 |
| NR-P987FW | Shell/BP black no. 5178 withd | £14.25 |
| NR-P987G  | Shell/BP black no. 5171       | £13.45 |
| NR-P987H  | Shell/BP black no. 5176       | £13.45 |
| NR-P987I  | Shell/BP black no. 5174       | £13.45 |
| NR-P987JW | Shell/BP black no. 5173 withd | £14.25 |
| NR-P987KW | Shell/BP black no. 5179 withd | £14.25 |
| NR-P987LW | Shell/BP black no. 5168 withd | £14.25 |



|           |                            |        |
|-----------|----------------------------|--------|
| NR-P993A  | BP Grey no. BPO67266       | £13.45 |
| NR-P993B  | BP Grey no. BPO67082       | £13.45 |
| NR-P993C  | BP Grey no. BPO67095       | £13.45 |
| NR-P993DW | BP Grey no. BPO67168 withd | £14.25 |
| NR-P993EW | BP Grey no. BPO67177 withd | £14.25 |
| NR-P993FW | BP Grey no. BPO67184 withd | £14.25 |
| NR-P994A  | BP Green no. BPO67161      | £13.45 |
| NR-P994B  | BP Green no. BPO67162      | £13.45 |
| NR-P994C  | BP Green no. BPO67368      | £13.45 |
| NR-P994DW | BP Green no. BPO67765      | £14.25 |
| NR-P994EW | BP Green no. BPO67783      | £14.25 |
| NR-P994FW | BP Green no. BPO67574      | £14.25 |



|           |                       |        |
|-----------|-----------------------|--------|
| NR-P995A  | BP Black no. BPO65762 | £13.45 |
| NR-P995B  | BP Black no. BPO65764 | £13.45 |
| NR-P995C  | BP Black no. BPO65771 | £13.45 |
| NR-P995DW | BP Black no. BPO65770 | £14.25 |
| NR-P995EW | BP Black no. BPO65765 | £14.25 |
| NR-P995FW | BP Black no. BPO65766 | £14.25 |

## OFFERS ON ABOVE WAGONS

|                          |        |
|--------------------------|--------|
| Any 3 Pristine           | £39.60 |
| Any 3 Weathered          | £42.00 |
| 3 weathered + 3 pristine | £79.50 |

## HELJAN LOCOMOTIVES

|       |                                 |         |
|-------|---------------------------------|---------|
| 15081 | Class 15 BR Green TOPS          | £87.50  |
| 3526  | Class 35 BR Green               | £110.45 |
| 3527  | Class 35 BR Green               | £110.45 |
| 3528  | Class 35 BR Early Blue          | £110.45 |
| 3529  | Class 35 BR Blue withd          | £118.95 |
| 3530  | Class 35 Preserved Dutch livery | £110.45 |

## HORNBY Steam Locomotives

|        |                        |         |
|--------|------------------------|---------|
| R2744  | Schools Blundells BR   | £84.95  |
| R2844  | Schools St. Lawrence   | £84.95  |
| R2917  | 2800 Class BR          | £103.99 |
| R3004  | Serby Hall             | £116.45 |
| R3004X | Serby Hall weathered   | £133.45 |
| R3233  | D16/3 LNER             | £100.95 |
| R3235  | D16/3 British Railways | £100.95 |
| R3240  | Class 700 BR Early     | £96.45  |
| R3242A | K1 BR Early            | £116.95 |
| R3243B | K1 BR Late             | £116.95 |



|       |                         |         |
|-------|-------------------------|---------|
| R3311 | BR Schools Westminster  | £119.45 |
| R3312 | BR Class A3             | £143.45 |
| R3323 | BR Black 5              | £119.45 |
| R3327 | SR S15                  | £123.99 |
| R3329 | BR Late S15             | £123.99 |
| R3333 | BR Early Adams Radial   | £107.95 |
| R3334 | BR Late Adams Radial    | £107.95 |
| R3338 | BR Class B1 Apple Green | £128.95 |
| R3380 | LNER J15                | £96.90  |
| R3381 | BR Early J15            | £96.90  |
| R3408 | GWR King                | £142.25 |
| R3411 | SR S15                  | £125.95 |
| R3413 | S15 Late                | £119.45 |
| R3414 | LNER J15                | £99.45  |
| R3416 | J15 BR Late             | £99.45  |



|       |                    |         |
|-------|--------------------|---------|
| R3418 | K1 BR Early        | £114.45 |
| R3420 | BR Late 700 Class  | £99.45  |
| R3421 | BR Early 700 Class | £99.45  |
| R3422 | SR Adams Radial    | £109.45 |
| R3423 | BR Late Adams      | £109.45 |
| R3424 | LNER Q6            | £125.45 |
| R3425 | BR/Early Q6        | £125.45 |
| R3426 | BR Late Q6         | £125.45 |
| R3432 | BR Late B12        | £127.95 |
| R3445 | BR W/C Camelford   | £147.45 |
| R3452 | BR Grange          | £119.25 |
| R3462 | BR Class 42xx      | £106.45 |



|       |                          |         |
|-------|--------------------------|---------|
| R3517 | The Final Day Royal Scot | £149.00 |
| R3520 | BR Britannia             | £144.45 |
| R3522 | BR Class A4              | £134.45 |
| R3523 | BR B17                   | £127.99 |
| R3525 | BoB Archibald Sinclair   | £131.99 |
| R3533 | J94 Lord Phil            | £67.45  |
| R3555 | Sir William Stanier      | £161.00 |

|          |                    |         |
|----------|--------------------|---------|
| R3557    | LMS Royal Scot     | £142.00 |
| R3560    | Q1 BR Late         | £89.45  |
| R3565    | LMS 8F             | £134.00 |
| R3566    | BR Merchant Navy   | £154.95 |
| R3614    | LMS Patriot        | £129.45 |
| R3618    | BR Rebuilt BoB     | £139.45 |
| R3619    | BR Castle          | £129.45 |
| R3630    | Class A4 Woodcock  | £135.99 |
| R3634    | SR Lord Nelson     | £152.95 |
| R3692    | 14xx BR            | £47.95  |
| R3681    | LMS Coronation     | £184.00 |
| R3682    | BR Blue Coronation | £184.00 |
| R3767    | BR Early Terrier   | £80.75  |
| R3781    | K&ESR Terrier      | £80.75  |
| R3603TTS | BR Lord Nelson     | £175.00 |

## HORNBY Diesel Electric Locomotives

|       |                         |         |
|-------|-------------------------|---------|
| R3261 | Class 08 Network        | £84.45  |
| R3262 | Class 31 D & C          | £142.45 |
| R3268 | CL67 Arriva             | £111.95 |
| R3342 | Class 08 BR             | £104.45 |
| R3343 | Class 08 FGW            | £104.45 |
| R3349 | Class 67 EWS            | £123.95 |
| R3353 | Sentinel Balfour Beatty | £49.45  |
| R3373 | NRM Class 71            | £134.95 |
| R3374 | BR Blue Class 71        | £134.95 |
| R3376 | BR Green Class 71       | £134.95 |



|       |                        |         |
|-------|------------------------|---------|
| R3473 | BR Class 56 Rft        | £135.99 |
| R3482 | Sentinel MSC           | £51.45  |
| R3483 | Sentinel Crossley      | £51.45  |
| R3484 | BR Class 08 Green      | £109.45 |
| R3485 | BR Class 08 Blue Laira | £109.45 |
| R3573 | Class 66 GBRF          | £79.45  |
| R3577 | Sentinel Graham        | £58.00  |
| R3653 | Class 50               | £137.25 |
| R3658 | Class 50               | £129.25 |
| R3685 | GWR HST                | £261.00 |
| R3580 | Class 87 BR Blue       | £139.40 |
| R3582 | Class 87 Intercity     | £139.25 |
| R3659 | Class 67 Colas         | £144.00 |

## HORNBY Train Packs



|       |                                 |         |
|-------|---------------------------------|---------|
| R3607 | 15 Guinea Special               | £270.00 |
| R4871 | Brighton Belle Pullman car pack | £179.99 |

## HORNBY TRACK

|       |                       |        |
|-------|-----------------------|--------|
| R083  | Buffer Stop           | £1.65  |
| R207  | Track Fixing Pins     | £2.50  |
| R394  | Hydraulic Buffer      | £4.95  |
| R600  | Straight              | £1.15  |
| R601  | Double Straight       | £1.85  |
| R604  | Curve 1st Rad.        | £1.65  |
| R605  | Double Curve 1st Rad. | £2.05  |
| R606  | Curve 2nd Rad.        | £1.40  |
| R607  | Double Curve 2nd Rad. | £1.95  |
| R608  | Curve 3rd Rad.        | £1.65  |
| R609  | Double Curve 3rd Rad. | £2.25  |
| R610  | Short Straight        | £1.10  |
| R614  | LH Diamond Crossing   | £9.25  |
| R615  | RH Diamond Crossing   | £9.25  |
| R617  | Uncoupling Ramp       | £2.60  |
| R618  | Double Isolating Rail | £6.85  |
| R620  | Railer Uncoupler      | £4.35  |
| R628  | Half Curve 3rd Rad.   | £1.80  |
| R643  | Half Curve 2nd Rad.   | £1.20  |
| R8072 | LH Point              | £7.95  |
| R8073 | RH Point              | £7.95  |
| R8074 | LH Curved Point       | £12.60 |
| R8075 | RH Curved Point       | £12.60 |
| R8076 | Y Point               | £8.95  |
| R8077 | LH Express Point      | £12.60 |
| R8078 | RH Express Point      | £12.60 |
| R8261 | 4th Radius curve      | £1.85  |

## HORNBY Track Accessories

|      |                         |        |
|------|-------------------------|--------|
| R076 | Footbridge              | £15.10 |
| R169 | Junction Home Signal    | £10.35 |
| R170 | Junction Distant Signal | £10.35 |
| R171 | Single Home Signal      | £7.95  |
| R172 | Single Distant Signal   | £7.95  |
| R406 | Colour Light Signal     | £19.00 |
| R574 | Trackside Accessories   | £12.95 |
| R626 | Point Underlay          | £4.20  |
| R636 | Double Level Crossing   | £20.15 |
| R638 | Track Underlay          | £10.55 |
| R645 | Single Level Crossing   | £12.45 |
| R460 | Straight Platform       | £3.45  |
| R464 | Platform Ramp           | £3.45  |
| R495 | Platform Subway         | £3.45  |

ORDER BY TELEPHONE  
Orders taken 6 days a week  
9am-5pm Monday to Saturday  
**01432 352809**

## Mail Order Service

All prices valid to 30/06/2019  
You must state these prices when ordering  
POSTAGE & PACKING RATES PER ORDER:  
• UK Standard Postage and Packing Rate £3.95  
• UK Insured Postage and Packing Rate £7.10  
• Worldwide at cost + VAT free to non EC  
\*\*Train Sets/Packs and Controllers/Handsets -  
Postage charged at cost\*\*  
PRICES SUBJECT TO CHANGE  
WITHOUT NOTICE  
\*\*\*Please allow 7 days for clearance when  
paying by cheque\*\*\*

PLEASE QUOTE OUR SPECIAL PRICES  
WHEN YOU VISIT US

Credit Cards Welcome  
Please state expiry date





# NEW MODELS

All versions are now available for ordering from your local HELJAN stockist!



**OUT NOW!**

**EE/BR Class 37/4 Co-Co diesel - 'O'**



**Features:** Authentic details - LED lighting - DCC friendly - Twin motors **SRP £599,00**



**3740 - 'Heritage' green with small yellow panels**  
(as carried by 37411 / D6990)



**3741 - BR large logo blue**



**3742 - InterCity / Mainline**



**3743 - EWS**



**3744 - Regional Railways**



**3745 - Triple-grey sector livery**

**EXPECTED SOON!**



**Beyer Garratt LMS / BR - 'OO'**

**266213**  
LMS 4993

**266218**  
BR 47978

**266214\***  
LMS 4984 - *lightly weathered*

**266219\***  
BR 47988 - *lightly weathered*

**266215**  
LMS 4982

**266220\***  
BR 47993 - *heavily weathered*

**266216**  
LMS 7973

**266221**  
BR 47971

**266217**  
BR 47979

**266222**  
LMS works grey 4986

**SRP £249,00**

\*SRP, weathered versions £259,00

**OUT NOW!**

**NINE NEW VERSIONS!**



**Ruston & Hornsby / BR Class 07 - 'OO'**

**V1 2904 - D2986 in BR/SR green**

**V1 2905 - D2988 in BR/SR green**

**V1 2906 - D2991 in 'Eastleigh' works green**

**V1 2907 - D2998 in BR blue**

**V1 2908 - 07002 in BR blue**

**V1 2909 - '07006' in industrial Powell Duffryn blue/white**

**V2 2914 - 2989 in BR blue**

**V2 2915 - 07009 in weathered BR blue**

**V2 2916 - 07005 'LANGBAURGH' in industrial ICI grey**

**SRP £139,95**





# Selling your model railways?

Warwick and Warwick have an expanding requirement for early to modern collections of British and Foreign trains and train sets from large accumulations to items of individual value. Our customer base is increasing dramatically and we need an ever larger supply of quality material to keep pace with demand. The market has never been stronger and if you are considering the sale of your collection, now is the time to act.



**FREE VALUATIONS** - We will provide a free, professional valuation of your collection, without obligation on your part to proceed. Either we will make you a fair, binding private treaty offer, or we will recommend inclusion of your property in our next public auction.

**FREE TRANSPORTATION** - We can arrange insured transportation of your collection to our Warwick offices completely free of charge. If you decline our offer, we ask you to cover the return carriage costs only.

**FREE VISITS** - Visits by our valuers are possible anywhere in the country or abroad, in order to assess more valuable collections. Please phone for details.

**ADVISORY DAYS** - We are staging a series of advisory days and will be visiting the following towns within the next few weeks: Hinckley, Gravesend, Braintree, Aylesbury, St. Helens, Wirral, Cirencester, Cannock, Finchley, Plymouth, Torquay, Blackburn, Oldham, Luton, Portsmouth and Southampton. Please visit our website or telephone for further details.

**EXCELLENT PRICES** - Because of the strength of our customer base we are in a position to offer prices that we feel sure will exceed your expectations.

**ACT NOW** - Telephone or email Richard Beale today with details of your property.



*Get the experts on your side!*



**Warwick & Warwick**

AUCTIONEERS AND VALUERS  
[www.warwickandwarwick.com](http://www.warwickandwarwick.com)

Warwick & Warwick Ltd., Chalon House, Scar Bank, Millers Road,  
Warwick CV34 5DB England

Tel: (01926) 499031 • Fax: (01926) 491906  
Email: [richard.beale@warwickandwarwick.com](mailto:richard.beale@warwickandwarwick.com)



# accurascale

ORDER ONLINE: [WWW.ACCURASCALE.CO.UK](http://WWW.ACCURASCALE.CO.UK)



## THERE'S CLASS... AND THEN THERE'S CLASS 92

BRITISH RAIL CLASS 92 IN 1:76.2/00 SCALE COMING IN EARLY 2020

AVAILABLE TO PRE-ORDER NOW - DCC READY: £160 / DCC SOUND: £250





# METCALFE

Card Construction Kits

## RAILWAY

### WAYSIDE STATION

00 CODE PO238 - £18.00  
N CODE PN138 - £10.00



### WAYSIDE STATION SHELTER

00 CODE PO239 - £14.00  
N CODE PN139 - £10.00



### FOOTBRIDGE

00 CODE PO236 - £11.50  
N CODE PN136 - £10.00



### PLATFORM KIOSK

00 CODE PO517 - £5.75  
N CODE PN817 - £5.50

### PLATFORM KITS

00 STONE CODE PO235 - £12.50  
N STONE CODE PN135 - £9.50  
00 RED BRICK PO216 - £12.50  
N RED BRICK PN110 - £9.50



### COUNTRY STATION

00 CODE PO237 - £15.00  
N CODE PN137 - £12.00

### SIGNAL BOX

00 CODE PO233 - £10.00  
N CODE PN133 - £9.00



**ULTRA FINE-TIP  
GLUE APPLICATORS**  
CODE MT907 - £4.00  
Glue not included.



**SHOP ONLINE: [WWW.METCALFEMODELS.COM](http://WWW.METCALFEMODELS.COM)**

METCALFE MODELS & TOYS LTD, BELL BUSK, SKIPTON, N.YORKS, BD23 4DU  
TEL: 01729 830072. POSTAGE £2.50 FOR ORDERS UP TO £19.99 / £4.50 THEREAFTER

Join the conversation on Facebook. Follow @MetcalfeModels and join the official group forum.



# METCALFE

Card Construction Kits

A hobby  
in itself!

## TOWN

### TERRACED HOUSES

00 RED BRICK CODE PO261 - £10.50  
00 STONE CODE PO262 - £10.50  
N RED BRICK PN103 - £8.50  
N STONE PN104 - £8.50



### CORNER SHOP

00 RED BRICK CODE PO263 - £11.00  
00 STONE CODE PO264 - £11.00  
N RED BRICK PN116 - £8.50  
N STONE PN117 - £8.50



### 00/N INDUSTRIAL UNIT

00 CODE PO285 - £13.00  
N CODE PN185 - £7.50



### 00/N FIRE STATION

00 CODE PO289 - £19.00  
N CODE PN189 - £12.00

## STRUCTURES



### DOUBLE TRACK VIADUCT

RED BRICK 00 CODE PO240 - £14.00  
STONE 00 PO241 - £14.00  
RED BRICK N CODE PN140 - £12.00  
STONE N CODE PN141 - £12.00



### RETAINING WALLS

RED BRICK 00 CODE PO244 - £13.50  
STONE 00 CODE PO245 - £13.50  
RED BRICK N CODE PN145 - £9.50  
STONE N CODE PN144 - £9.50



### DOUBLE TRACK TUNNEL ENTRANCE

00 CODE PO242 - £10.50  
N CODE PN142 - £8.00



### TAPERED WALLS

RED BRICK 00 CODE PO248 - £13.50  
STONE 00 CODE PO249 - £13.50  
RED BRICK N CODE PN148 - £9.50  
STONE N CODE PN149 - £9.50

## MINI KITS



### GOODS YARD CRANE

00 CODE PO540 - £6.50  
N CODE PN840 - £5.00  
00 pictured below. N version comes without hook.



### PILLBOXES

00 CODE PO520 - £5.50  
N CODE PN820 - £5.00



### PICNIC TABLES

00 CODE PO510 - £5.50  
N CODE PN810 - £5.50



### FOUNTAINS

00 CODE PO522 - £5.75  
N CODE PN823 - £5.75  
00 as pictured. N comes with three variations.



### NISSEN HUTS

00 CODE PO415 - £7.50  
N CODE PN815 - £5.75



### 00 PLATFORM BENCHES

00 CODE PO502 - £5.75



### PLAY AREA

00 CODE PO513 - £6.50



### 00 MARKET STALLS

00 CODE PO530 - £6.50



# THE GREAT RAILWAY ADVENTURE

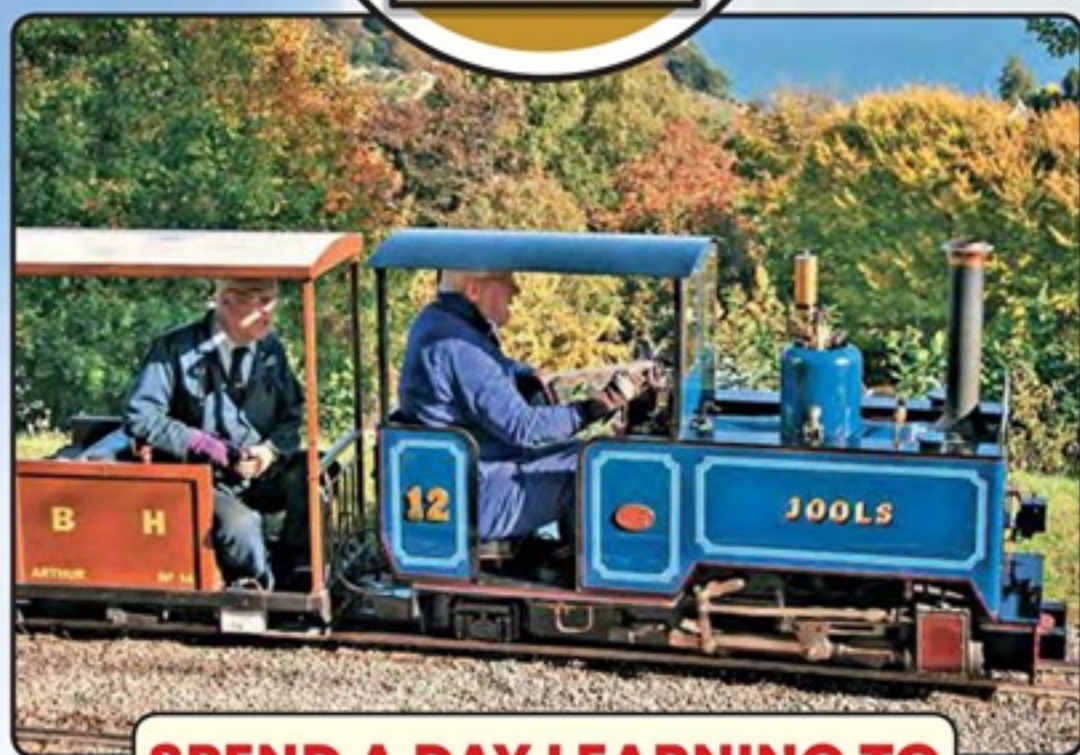
## Pecorama

at Beer, Devon

**SPECIAL  
EVENT!**

*A must for railway  
enthusiasts!*

### DRIVER EXPERIENCE COURSE



**SPEND A DAY LEARNING TO  
OPERATE A STEAM LOCO!\***



**THE PERFECT GIFT FOR  
BIRTHDAYS & CHRISTMAS!**

\*Details on our website. Booking is required.

### Pecorama FESTIVAL OF Steam AND FOOD 17<sup>TH</sup> - 26<sup>TH</sup> AUGUST 2019

#### TRAINS TO ADORE & FOOD GALORE!

Join us between Saturday 17th and Monday 26th August for a week-long celebration of steam locomotives! Featuring guest loco's from around the country, and with the entire Peco Fleet in operation\* this special event marks the highlight of the BHLR Summer Season!



... and a warm welcome always awaits at the **PECO PERMANENT EXHIBITION OF MODEL RAILWAYS & THE PECO MODEL SHOP**

\*Locos operating throughout the whole week, not simultaneously. Subject to availability.

**PLUS!**

#### EVERY TUESDAY 30<sup>TH</sup> JULY - 27<sup>TH</sup> AUGUST KIT CONSTRUCTION DAYS

Get expert tuition from well-known modellers Gordon and Maggie Gravett. They will show you techniques for assembling kits and how to place them on a model railway layout. Two drop-in sessions starting at 11am and 2pm. Great fun for kids of all ages! Last admission is 20 minutes before the end of the session. A nominal charge is made for kits. Children under 8 years of age must be accompanied by an adult.







RAILS OF SHEFFIELD

railsofsheffield.com



Corporate Partners of

RAILWAY MUSEUM

**BARGAIN HELJAN O GAUGE****CLASS 128 DPU****RRP: £599 OFFER PRICE: £295.00 EACH**

|      |                                                                  |         |
|------|------------------------------------------------------------------|---------|
| 8910 | CLASS 128 DPU BR GREEN (SINGLE SPEED WHISKERS) LOCOMOTIVE W55991 | £295.00 |
| 8911 | CLASS 128 DPU BR GREEN (DOUBLE SPEED WHISKERS) LOCOMOTIVE W55992 | £295.00 |
| 8912 | CLASS 128 DPU BR GREEN (YELLOW ENDS) LOCOMOTIVE W55992           | £295.00 |
| 8913 | CLASS 128 DPU BR BLUE (FULL YELLOW ENDS) LOCOMOTIVE M55994       | £295.00 |
| 8950 | CLASS 128 DPU BR BLUE (FULL YELLOW ENDS) LOCOMOTIVE M55995       | £295.00 |
| 8951 | CLASS 128 DPU ROYAL MAIL LETTERS LOCOMOTIVE 55993                | £295.00 |

**CLASS 05 SHUNTERS****RRP: £395 OFFER PRICE FROM: £195.00 EACH**

|      |                                                                 |         |
|------|-----------------------------------------------------------------|---------|
| 2590 | HUNSLET CLASS 05 BR GREEN (NO WASP STRIPES) LOCOMOTIVE          | £195.00 |
| 2591 | HUNSLET CLASS 05 BR GREEN WITH WASP STRIPES (LATE) LOCOMOTIVE   | £195.00 |
| 2592 | HUNSLET CLASS 05 BR GREEN WITH WASP STRIPES (EARLY) LOCOMOTIVE  | £195.00 |
| 2593 | HUNSLET CLASS 05 BR BLUE WITH WASP STRIPES (LATE) LOCOMOTIVE    | £225.00 |
| 2594 | HUNSLET CLASS 05 CEGB YELLOW WITH WASP STRIPES LOCOMOTIVE       | £195.00 |
| 2595 | HUNSLET CLASS 05 NCB GREEN WITH WASP STRIPES LOCOMOTIVE         | £195.00 |
| 2596 | HUNSLET CLASS 05 INDUSTRIAL MAROON WITH WASP STRIPES LOCOMOTIVE | £225.00 |

**GREAT PRICES ON PECO**

|                      |                                                                                     |              |
|----------------------|-------------------------------------------------------------------------------------|--------------|
| SL-100               | 25 YARDS CODE 100 WOODEN SLEEPER TYPE NICKEL SILVER FLEXIBLE TRACK                  | £65.00       |
| SL-100F              | 25 YARDS NICKEL SILVER FINESCALE FLEXIBLE TRACK WOODEN SLEEPER                      | £72.00       |
| SL-102F              | 25 YARDS FINESCALE CONCRETE SLEEPER TYPE NICKEL SILVER FLEXIBLE TRACK               | £82.00       |
| SL-91/E91/92/E92     | SMALL POINTS RIGHT & LEFT HAND INSUL OR ELECTROFROG                                 | £9.50 each   |
| SL-95/E95/96/E96     | MEDIUM POINTS RIGHT & LEFT HAND INSUL OR ELECTROFROG                                | £10.50 each  |
| SL-88/E88/89/E89     | LARGE POINTS RIGHT & LEFT HAND INSUL OR ELECTROFROG                                 | £12.50 each  |
| PL10/ PL10X6         | PL10 POINT MOTOR (SWITCH MACHINE) INDIVIDUAL OR 6 PACK                              | £6.00/£34.00 |
| SL-300               | 25 YARDS CODE 80 NICKLE SILVER FLEXIBLE TRACK WITH WOODEN TYPE SLEEPERS             | £63.50       |
| SL-300F              | 30 YARDS CODE 55 FINESCALE N/S FLEXIBLE TRACK WOODEN SLEEPER                        | £82.00       |
| SL-395/96-E95/E96    | MEDIUM POINTS RIGHT & LEFT HAND INSUL OR ELECTROFROG                                | £9.50 each   |
| SL-U395F/U96F        | MEDIUM RIGHT HAND TURNOUT UNIFROG CODE 55                                           | £10.00 each  |
| SL-388/389/E388/E389 | LARGE POINT ELECTROFROG RIGHT & LEFT HAND POINTS                                    | £10.50 each  |
| SL-700BH             | 12 YARDS O GAUGE FLEXIBLE NICKLE SILVER TRACK (BULLHEAD) WOODEN SLEEPER             | £75.00       |
| SL-700FB             | 12 YARDS O GAUGE UNIVERSAL FINE CODE 143 WOODEN SLEEPER                             | £75.00       |
| SL-E791/792BH        | MEDIUM RADIUS RIGHT & LEFT HAND POINTS WITH BULLHEAD RAIL                           | £42.50 each  |
| SL-E791FB/E792FB     | MEDIUM RADIUS RIGHT AND LEFT NICKEL SILVER ELECTROFROG POINT (CODE 143 FLAT BOTTOM) | £45.50 each  |

**AMAZING OFFERS ON**

|         |                                                      |         |
|---------|------------------------------------------------------|---------|
| D       | TWIN TRACK POWER CASED CONTROLLER                    | £82.00  |
| Q       | FOUR TRACK CASED CONTROLLER                          | £149.00 |
| COMBI   | SINGLE TRACK CONTROLLER/ PLUG IN TRANSFORMER (1 AMP) | £34.00  |
| DCC01   | PRODIGY EXPRESS PACKAGE                              | £149.50 |
| DCC02   | PRODIGY ADVANCE PACKAGE DIGITAL CONTROLLER           | £254.50 |
| DCC05   | PRODIGY WIFI RECEIVER                                | £89.00  |
| 10LGB5F | SINGLE TRACK CASED CONTROLLER FOR G SCALE WITH FAN   | £159.50 |
| 100MO   | SINGLE TRACK CASED CONTROLLER FOR O SCALE            | £84.00  |

**SEE OUR WEBSITE FOR FULL LISTINGS INCLUDING SPECIAL OFFERS & GREAT DEALS!!**[www.railsofsheffield.com](http://www.railsofsheffield.com)**PRICING POLICY FOR IN STOCK AND AVAILABLE ITEMS:** The price advertised on Rails of Sheffield website will be the price charged at the time of the order being processed. This excludes errors in pricing (E & OE) in the event of an error with a price you will be contacted prior to the order being processed and given the option to accept the correct price or cancel the order.**PLEASE NOTE:** All items currently in stock are despatched immediately. Forthcoming releases will be despatched immediately upon release. If your order contains both current items and forthcoming releases then these will be despatched separately and subject to separate delivery charges.**ALSO STOCKED AT BARGAIN PRICES!****RAILS TV & SOCIAL MEDIA...**

Follow and like us on:



railsofsheffield

railsofsheffield

railssheffield

railsofsheffield

**RAILS ON EBAY 2 STORES, 100'S OF ITEMS ADDED DAILY...****RAILS-SHEFFIELD**  
Ebay store!**RAILS-VAULT**  
Ebay store!**SHIPPING COSTS: ROYAL MAIL 48 HOUR TRACKED £4.00:** Royal Mail aim to deliver within 2 days of dispatch including Saturdays (this is not a guaranteed service and parcels can take longer to arrive).**ROYAL MAIL 24 HOUR TRACKED £5.00:** Royal Mail aim to deliver within 1 day of dispatch including Saturdays (this is not a guaranteed service and parcels can take longer to arrive).**SPECIAL DELIVERY £8.50:** Guaranteed delivery by 1pm the next day Monday to Saturday. With full track and trace system, your parcel is delivered speedily and securely (orders must be placed by 2pm to guarantee same day dispatch).**PARCEL FORCE COURIER SERVICE £7.50:** Next day delivery for much of the UK, including Saturdays. Full track and trace system. Used for parcels over 1.8kg without packing or if you would like a speedy secure delivery of your parcel. This excludes the following post codes BT, JE, GY, IM, KW, IV, PA and PO30-44, KA27-28, PH17-26, PH49-50 and ZE. The cost for these postcodes is £12.00 and will be delivered within two to three working days (orders must be placed by 3pm to guarantee same day dispatch).**WORLDWIDE POSTAGE:** For all orders outside the UK, postage will be charged at cost, based on the weight and size of the parcel.

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 ORL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)Web: [railsofsheffield.com](http://railsofsheffield.com) [railsofsheffield](https://www.youtube.com/railsofsheffield) [railsofsheffield](https://www.facebook.com/railsofsheffield) [railssheffield](https://twitter.com/railssheffield) [railsofsheffield](https://www.instagram.com/railsofsheffield)





RAILS OF SHEFFIELD

railsofsheffield.com



Corporate Partners of

RAILWAY  
MUSEUM

# WANTED

## CASH OR EXCHANGE

WE WANT **ANYTHING** MODEL RAILWAY RELATED,  
ANY AGE, ANY GAUGE, FROM A SINGLE ITEM TO A  
LIFETIME COLLECTION. DISTANCE NO OBJECT.  
PLEASE TELEPHONE, OR EMAIL YOUR LISTS TO  
SECONDHAND@RAILSOF SHEFFIELD.COM

**BUY · SELL · EXCHANGE · ANY GAUGE · ANY AGE**

**RAILS OF SHEFFIELD VALUE YOUR SECOND HAND COLLECTION VERY HIGHLY**

LOCOS - COACHES - WAGONS - TRACK-WORK - CONTROLLERS - ACCESSORIES  
DIE-CAST - LORRIES - BUSES - PLANES - SOLDIERS - RAILWAYANA - AND MORE

**DECEASED ESTATES - EX SHOP STOCK A SPECIALITY**  
FAST SERVICE WITH COMPLETE DISCRETION ASSURED

— **NO COLLECTION IS TOO LARGE** —

FOR LARGER COLLECTIONS WHERE IT IS IMPOSSIBLE TO LIST ALL ITEMS - WE CAN ARRANGE A PERSONAL VISIT

**IMMEDIATE PAYMENT BY BANK TRANSFER OR CASH**  
OR WHY NOT TRADE YOUR ITEMS IN FOR EXCHANGE FOR AN EVEN BIGGER ALLOWANCE

**We URGENTLY require collections of any size, featuring products from the following manufacturers:**



Telephone our Second Hand team on: (0114) 255 1436 or email: [secondhand@railsofsheffield.com](mailto:secondhand@railsofsheffield.com) - Ref: RM2019

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 ORL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)

Web: [railsofsheffield.com](http://railsofsheffield.com)

[railsofsheffield](https://www.youtube.com/railsofsheffield)

[railsofsheffield](https://www.facebook.com/railsofsheffield)

[railssheffield](https://twitter.com/railssheffield)

[railsofsheffield](https://www.instagram.com/railsofsheffield)





RAILS OF SHEFFIELD

railsofsheffield.com



Corporate Partners of

RAILWAY MUSEUM

WE VALUE YOUR  
**PRE-OWNED &  
SECOND HAND**  
MODEL COLLECTION  
**VERY HIGHLY!!**

ALL BRANDS

FOR CASH

**CONTACT US**

get in touch by email,  
phone or post with  
your list of items

STEP  
01



STEP  
02

**WE MAKE AN OFFER**

our friendly, experienced staff  
review your list and make an  
honest valuation and offer

**BRING, SEND OR COLLECT**

either visit the shop, send us your  
items, or for larger collections we will  
arrange collection for you

STEP  
03



STEP  
04

**PAYMENT MADE!**

we confirm the offer and make  
a fast, secure payment via your  
choice of payment method

**TOP  
GUARANTEED  
PRICES  
PAID**

**FAMILY  
BUSINESS  
FRIENDLY  
SERVICE**

FOR MORE DETAILS see our website: [railsofsheffield.com/wanted](http://railsofsheffield.com/wanted)

**FULL COLLECTIONS PURCHASED**  
**UK & WORLDWIDE COLLECTION AVAILABLE**

**BUY · SELL · EXCHANGE · ANY GAUGE · ANY AGE**

TELEPHONE OUR SECOND HAND TEAM ON: (0114) 255 1436  
OR EMAIL: [SECONDHAND@RAILSOFSCHEFFIELD.COM](mailto:SECONDHAND@RAILSOFSCHEFFIELD.COM)

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 0RL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)

Web: [railsofsheffield.com](http://railsofsheffield.com)

[railsofsheffield](https://www.youtube.com/railsofsheffield)

[railsofsheffield](https://www.facebook.com/railsofsheffield)

[railssheffield](https://twitter.com/railssheffield)

[railsofsheffield](https://www.instagram.com/railsofsheffield)





# RAILS



railsofsheffield.com  
/exclusive

LARGE  
RETAILER  
OF THE  
YEAR!

HORNBY  
MODEL RAILWAY  
AWARDS  
2018  
WINNER

Corporate Partners of

## RAILWAY MUSEUM

**EXCLUSIVE TO RAILS PRE-ORDER NOW!**  
SEE WEBSITE FOR MORE DETAILS AND THE FULL RANGE

OO GAUGE MODEL

PRODUCED BY  
**CAVALEX**  
Models



**BLA**  
THREE PACK  
VARIATIONS  
AVAILABLE

**BLA BOGIE STEEL WAGONS**  
RAILFREIGHT

SET OF  
**THREE**

Each pack contains three wagons each with a unique  
running number variations and other livery elements

PACK  
**£135**

**BBA**  
TWO PACK  
VARIATIONS  
AVAILABLE



**BBA BOGIE STEEL WAGONS**  
RAILFREIGHT

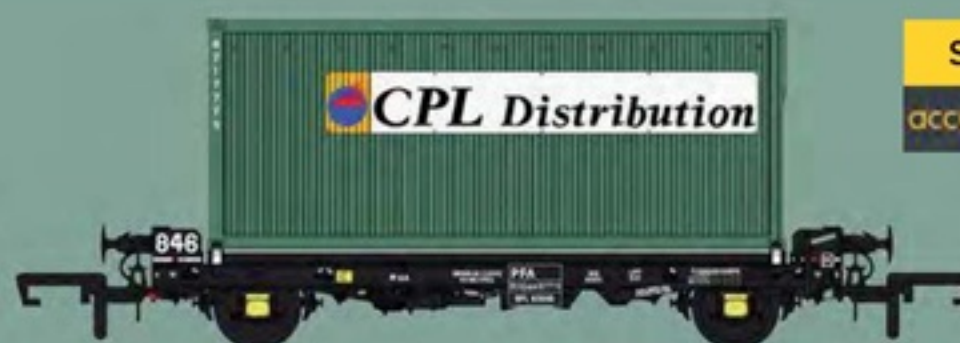
SET OF  
**THREE**

Each pack contains three wagons each with a unique  
running number variations and other livery elements

PACK  
**£135**

**EXCLUSIVE TO RAILS  
LIMITED TO 250 PRE-ORDER NOW!**

**PFA 2 AXLE CONTAINER  
FLAT WAGON PFA**  
CPL DISTRIBUTION CONTAINERS



SET OF THREE **£69.95**

accurascale

WAGON No's.  
BFL 92842  
BFL 92846  
BFL 92785

SEE WEBSITE  
FOR DETAILS

# — NORTH EASTERN RAILWAY — ELECTRIC AUTOCAR

*A WORLD FIRST...*



Secure yours for  
**ONLY £30 DEPOSIT**

PRE-ORDER PRICE £199.95

OO GAUGE MODEL

**3 VERSIONS** AVAILABLE

North Eastern Railway red/cream No. 3170 (1904-23)

North Eastern Railway red/cream No. 3171 (1904-30)

LNER brown No. 3170 (1923-31)



IN  
ASSOCIATION  
WITH

**HELJAN**

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 0RL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)

Web: [railsofsheffield.com](http://railsofsheffield.com)

[railsofsheffield](https://www.youtube.com/railsofsheffield)

[railsofsheffield](https://www.facebook.com/railsofsheffield)

[railssheffield](https://www.twitter.com/railssheffield)

[railsofsheffield](https://www.instagram.com/railsofsheffield)





# RAILS



railsofsheffield.com  
/exclusive



Corporate Partners of

## RAILWAY MUSEUM

PRODUCED BY  
**accurascale**

## NEWLY TOOLED OO DELTIC

DCC Ready **£160**  
DCC Sound **£250**

Secure yours for  
**ONLY £30 DEPOSIT**



**NOW  
AVAILABLE  
TO PRE-ORDER  
SOUND  
FITTED**

**SIX VERSIONS  
AVAILABLE**

55015 'Tulyar' / D9019 'Royal Highland Fusilier' / D9009 'Alycidon'  
D9002 'Kings Own Yorkshire Light Infantry' / 55 002 'Kings Own  
Yorkshire Light Infantry' / D9008 'The Green Howards'

IN PARTNERSHIP WITH  
**RAILWAY MUSEUM** THE DELTIC  
PRESERVATION SOCIETY

OO GAUGE MODEL

OTHER VARIANTS ARE AVAILABLE DIRECT FROM ACCURASCALE EXPECTED Q4 2019

## STROUDLEY A1 / A1X TERRIER 0-6-0

NEWLY TOOLED OO GAUGE MODEL

IN  
PARTNERSHIP  
WITH  
**dapol**  
Model Railway Company

**COMING SOON†**

Secure yours  
for **ONLY**  
**£30 DEPOSIT**

**ORDER BOOK  
FILLING UP  
FAST!**



**6 VERSIONS  
AVAILABLE**

**DCC READY VERSION  
PRE-ORDER PRICE £110**

† FOR DEPOSIT PRE-ORDERS,  
PLEASE SEE OUR WEBSITE FOR THE LATEST  
ESTIMATE OF ARRIVAL DATE

## CALEDONIAN CLASS 812



Courtesy of: Hugh Llewelyn



**PRE-ORDER PRICE**  
**£199.95 PREMIUM BLUE**  
**£179.95 BLACK VERSIONS**

Secure yours for  
**ONLY £30 DEPOSIT**

OO GAUGE MODEL

**FIVE VERSIONS  
AVAILABLE:**

Caledonian No. 828 (as preserved)  
Caledonian No. 828 (as built)  
LMS Black Livery No. 17566  
BR Black Early Emblem No. 57565  
(weathered)  
BR Black Late Crest No. 57566

IN  
ASSOCIATION  
WITH



21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 0RL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)

Web: [railsofsheffield.com](http://railsofsheffield.com)

[railsofsheffield](https://www.youtube.com/railsofsheffield)

[railsofsheffield](https://www.facebook.com/railsofsheffield)

[railssheffield](https://twitter.com/railssheffield)

[railsofsheffield](https://www.instagram.com/railsofsheffield)



# The Signman's Office



## LK-710 Signal Box Interior



An authentically and  
richly detailed signal box  
interior\* to complement  
the Peco LK-709 and  
LK-715 kits

\* Plastic kit requiring assembly and painting



**PECO**

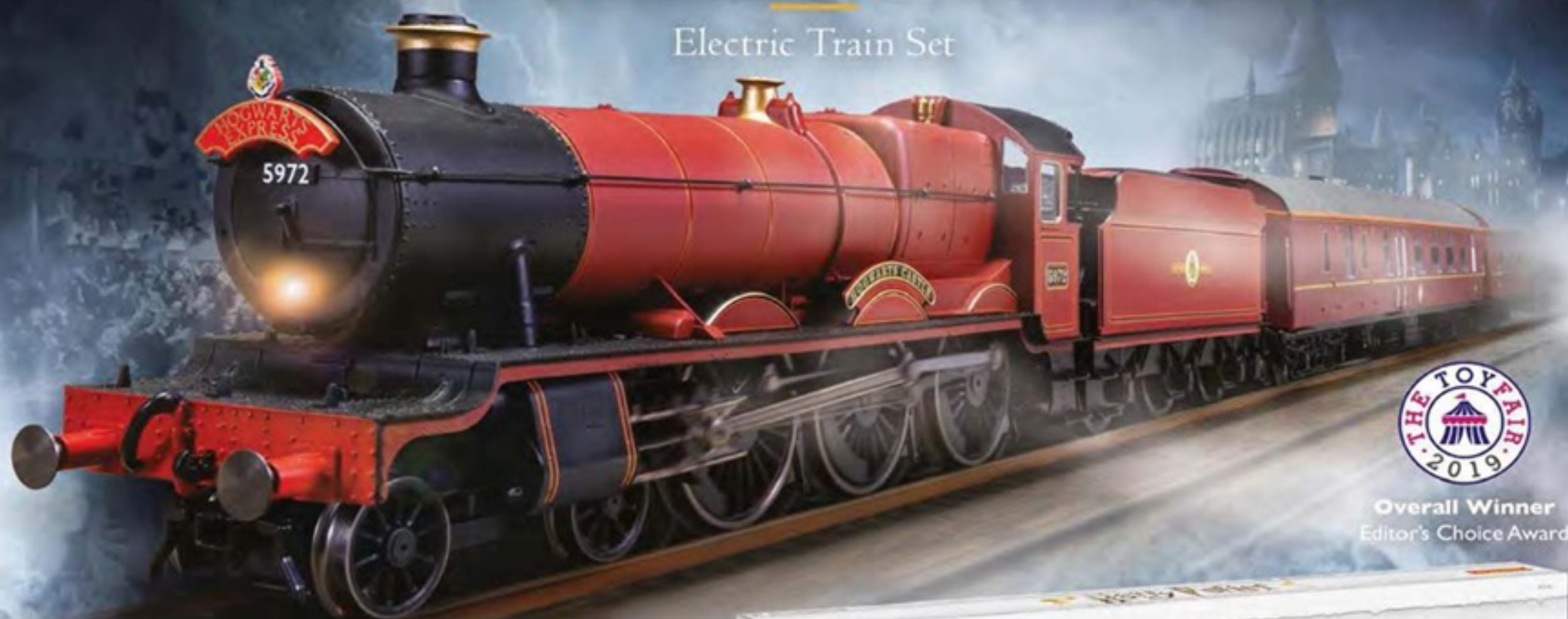
## O Gauge Lineside and Scenic



# Harry Potter

## JOURNEY TO HOGSMEADE WITH HOGWARTS EXPRESS

Electric Train Set



Overall Winner  
Editor's Choice Award

Add a touch of magic to your collection  
with the Hogwarts Express Train Set.

### CONTENTS:

- Hogwarts Castle Locomotive with working headlight
- Hogwarts Express Composite Coach '99716'
- Hogwarts Express Brake Coach '99723'
- 3rd Radius Oval with Siding, Power Track, Left Hand Point, Buffer Stop
- Train Controller



RI234 DCC READY

'Hogwarts Express' Train Set

### EXPAND YOUR LAYOUT

R4934/A  
Hogwarts, Mk1 SK  
Nos. 99716/99721

R3804 DCC READY  
5972 'Hogwarts Castle'  
with Working Headlight



R4935/A  
Hogwarts, Mk1 BSK  
Nos. 99723/99312

R3803TTS DCC READY  
5972 'Hogwarts Castle'  
with Sound and Working Headlight

Visit [www.hornby.com](http://www.hornby.com) to find your nearest retailer or call 01843 233512 to place your order today



HARRY POTTER characters, names and related indicia are © & ™ Warner Bros. Entertainment Inc. WB SHIELD: © & ™ WBEL J.K. ROWLING'S WIZARDING WORLD ™ J.K. Rowling and Warner Bros. Entertainment Inc. Publishing Rights © JKR. (S19)







White Rose Modelworks  
Unit 10, The Craft Yard,  
Bedale Station, BEDALE,  
North Yorkshire DL8 1BZ  
Tel: +44 (0)1677 422444

Check our new website:  
[www.whiterosemodelworks.co.uk](http://www.whiterosemodelworks.co.uk)  
Email your enquiries:  
[info@whiterosemodelworks.co.uk](mailto:info@whiterosemodelworks.co.uk)



**Modular Lasercut Baseboards**  
**40 Standard Sizes to Suit Your**  
**Space & Needs**  
**Control Shelves and Backscenes**  
**Unique Plug and Latch System for**  
**Easy Joining**



**Lasercut Helixes in 4 Standard Sizes**  
**Also Stretched & Bespoke Helixes**  
**All Baseboards & Helixes Made from**  
**Baltic Birch Plywood**



**Tracklaying, Electrics and Scenics**  
**Control Systems for DCC & Analogue**

Available in Kit format, Assembled Baseboards or  
fully Electrified and Sceniced Layouts

see the full range of  
kits and building papers at...

**www.superquick.co.uk**



A06 Signal Box & Coal Office

**FEATURING**  
**NEW**  
**COAL STAITHES**



A5 Locomotive Shed (double track)

**ALL DIE-CUT**  
**CARD KITS**



A16 Stone Bridge/Tunnel Entrance



A13 Ash Plant



A14 Red Brick  
Bridge/Tunnel Entrance



A12 Cooling Tower



B24 Farmhouse



C5 Victorian Terrace House Backs



Fine OO/HO detail from...

**SUPERQUICK**  
MODEL KITS

Available from all good model shops & online NOW!



# TOWER MODELS DISCOUNT 'O' GAUGE

## FULLY FINISHED R-T-R LOCOS

|                                   |               |
|-----------------------------------|---------------|
| L&Y/LMS/BR Pug 0-4-0T             | from £925.00  |
| GWR/BR 1361 0-6-0ST               | from £695.00  |
| GWR/BR 8750 Pannier tank          | from £695.00  |
| GWR/BR Parcels/Passenger railcars | from £810.00  |
| GWR/BR King 4-6-0                 | £1450.00      |
| LMS/BR J39 0-6-0                  | from £950.00  |
| LMS/BR Original Patriot           | from £1650.00 |

## GLADIATOR LOCO KITS

|                                          |         |
|------------------------------------------|---------|
| GTR3 LNER/BR J11/J11/3 GCR POM POM 0-6-0 | £330.00 |
| GTR20 LMS/BR Princess                    | £425.00 |
| GTR24 LMS/BR Original Patriot            | £365.00 |
| GTR28 LMS/BR 2P 4-4-0                    | £300.00 |
| GTR50 LNER/BR Beyer Garratt              | £635.00 |
| GTR61 LBSG/SR/BR H1 Atlantic             | £380.00 |
| GTR62 SR/BR King Arthur Urie             | £425.00 |
| FT3 LMS/BR B16/1 4-6-0                   | £365.00 |
| FT4 LNER D17/1, D17/2 4-4-0              | £300.00 |

## TOWER COLLECTION LOCO KITS

|                                       |         |
|---------------------------------------|---------|
| TC1 Class 02 Diesel complete kit      | £240.00 |
| TC2 LMS/BR Streamlined Coronation     | £550.00 |
| TC3 Andrew Barclay 0-4-0 complete kit | £240.00 |
| TC5 LMS/BR De-streamlined Duchess     | £550.00 |
| TC7 LMS/BR Black Five                 | £550.00 |
| TC14 LMS/BR Stanier 8F 2-8-0          | £550.00 |

## D.J.H. LOCO KITS

|                                      |         |
|--------------------------------------|---------|
| K300 BR Standard 84xx 2-6-2T         | £380.00 |
| K301 BR Standard Class 2 2-6-0       | £435.00 |
| K324A BR 9F with BR1G tender         | £625.00 |
| K324B BR 9F with BR1G tender         | £625.00 |
| K326 BR Class 03 Diesel complete kit | £435.00 |
| K338 LNER/BR G5 0-4-4T complete kit  | £435.00 |
| K340 Class 14 complete kit           | £435.00 |
| K344 NEW LNER/BR Hunt Class 4-4-0    | £499.00 |
| K345 NEW LNER/BR Thompson A2/1 4-6-2 | £625.00 |

## PECO 'O' GAUGE TRACKWORK

|                                   |        |
|-----------------------------------|--------|
| Curved R/H Point                  | £54.75 |
| Curved L/H Point                  | £54.75 |
| SL700BH 12 yards flexitrack       | £80.00 |
| SL791BH Right Hand Point          | £43.75 |
| SL792BH Left Hand Point           | £43.75 |
| SL797BH Y Point                   | £43.75 |
| STU750BH Right Hand Setrack Point | £41.95 |
| STU751BH Left Hand Setrack Point  | £41.95 |
| SL790BH Double Slip               | £92.25 |
| SL10 24 metal railjoiners         | £2.80  |
| SL740BH Buffer stop               | £2.80  |
| ST700 Setrack Straights (8)       | £45.50 |
| ST725 Setrack Curves (8)          | £48.75 |
| TP1 Long Track pins (15mm)        | £3.25  |

## SLATERS COACH AND VAN KITS

|                                |              |
|--------------------------------|--------------|
| GWR 4 wheel coaches 4 types    | £77.50       |
| GWR bogie clerestory coaches   | £135.00      |
| MR/LMS 6 wheel coaches 7 types | £82.75       |
| MR/LMS bogie coaches 4 types   | from £125.00 |
| SR 50" Bogie Van B             | £125.00      |
| SR 4 wheel PLV/GUV             | £78.75       |
| GWR 6 wheel milk Syphon        | £77.50       |
| LNER/BR Goods brake van        | £47.00       |
| LMS/BR Goods brake van         | £48.50       |

## DAPOL R-T-R WAGONS

|                                  |        |
|----------------------------------|--------|
| B.R Standard 20T Brake Van       | £67.95 |
| 5 Plank Open various             | £38.88 |
| 7 Plank Open various             | £38.88 |
| 8 Plank Open various             | £38.88 |
| 6 wheel Milk Tank 5 types        | £70.00 |
| Pillbox brake 7 types            | £68.50 |
| B.R Plank Van 2 types            | £42.50 |
| B.R Plywood Van 2 types          | £42.50 |
| B.R Meat Van                     | £42.50 |
| B.R Insulated Van                | £42.50 |
| B.R 10ft WB Open Wagons 3 types  | £39.50 |
| Private Owner Salt & Lime Wagons | £42.50 |
| Banana Vans "Due Autumn"         | £42.50 |

## DAPOL BOGIE BOLSTER E & TURBOT

|                            |                  |
|----------------------------|------------------|
| 6 Liveries Bogie Bolster E |                  |
| 6 Liveries Turbot          |                  |
| RRP £69.95                 | Our price £59.45 |

AVAILABLE 2019

## NEW DAPOL LIONHEART

|                                            |                   |
|--------------------------------------------|-------------------|
| 45xx Prairie Tank                          |                   |
| Great Western, Shireburn, British Railways |                   |
| Early and Late BR liveries available       |                   |
| RRP £495.00                                | Our price £420.75 |
| Also available with DCC and Sound          |                   |
| RRP £650.00                                | Our price £552.50 |

## DAPOL/LIONHEART 'B' SET COACHES

|                                           |                   |
|-------------------------------------------|-------------------|
| R.T.R. Fully finished coaches with lights |                   |
| 2 car set GWR or BR                       |                   |
| List price £470.50                        | Our price £399.99 |
| 4 car set GWR or BR                       |                   |
| List price £823.50                        | Our price £699.99 |

## DAPOL/LIONHEART PANNIER & AUTO COACH

|                                |                   |
|--------------------------------|-------------------|
| 64xx or 74xx GWR or BR Pannier |                   |
| List price £399.00             | Our price £340.00 |
| Autocoach GWR or BR            |                   |
| List price £200.00             | Our price £170.00 |

## DAPOL 57XX PANNIER

|                                             |                   |
|---------------------------------------------|-------------------|
| FULLY FINISHED R.T.R. LOCO                  |                   |
| BR Early Crest or London Transport Liveries |                   |
| RRP £235.00                                 | Our price £199.75 |
| LT Livery RRP £242.50                       | Our price £206.25 |

AVAILABLE NOW

## DAPOL 8750 PANNIER

|                            |                   |
|----------------------------|-------------------|
| FULLY FINISHED R.T.R. LOCO |                   |
| GWR or BR Liveries         |                   |
| RRP £235.00                | Our price £199.75 |

## DAPOL JINTY 0-6-0T

|                                   |                   |
|-----------------------------------|-------------------|
| FULLY FINISHED R.T.R. LOCO        |                   |
| LMS, BR or SDJR liveries          |                   |
| Available numbered or un-numbered |                   |
| RRP £225.00                       | Our price £191.25 |
| Any of the above Sound fitted     |                   |
| RRP £400.00                       | Our price £345.00 |

LAST FEW

## DAPOL CLASS 121 & 122

|                       |                   |
|-----------------------|-------------------|
| Choice of 5 Liveries  |                   |
| RRP £299.00           | Our price £254.15 |
| DCC Sound RRP £499.00 | Our price £424.15 |

AVAILABLE SPRING RESERVE YOURS NOW

## HELJAN CLASS 128 PARCELS RAILCAR

|                                     |                   |
|-------------------------------------|-------------------|
| 6 versions, see website for details |                   |
| RRP £599.00                         | Our price £475.00 |

AVAILABLE NOW

## HELJAN CLASS 117 3 CAR DMU

|                                  |                   |
|----------------------------------|-------------------|
| Choice of liveries (see website) |                   |
| RRP £1100.00                     | Our price £935.00 |

AVAILABLE LATE 2019 RESERVE YOURS NOW

## NEW HELJAN CLASS 37/4

|                                       |                   |
|---------------------------------------|-------------------|
| Choice of Five liveries (see website) |                   |
| RRP £599.00                           | Our price £509.15 |

AVAILABLE SPRING RESERVE YOURS NOW

## NEW HELJAN GWR/BR PRAIRIE

|                     |                   |
|---------------------|-------------------|
| 5 standard versions |                   |
| 7 limited editions  |                   |
| RRP £699.00         | Our price £594.15 |

NOW AVAILABLE

See our website for full details

## NEW HELJAN 43xx MOGUL

|                                                        |                   |
|--------------------------------------------------------|-------------------|
| 7 standard versions in GWR & BR                        |                   |
| all un-numbered or 3 limited edition numbered versions |                   |
| RRP £779.00                                            | Our price £662.15 |

NOW AVAILABLE

## HELJAN WARSHIPS

|                       |                   |
|-----------------------|-------------------|
| 10 Liveries Available |                   |
| RRP £699.00           | Our price £550.00 |

## HELJAN CLASS 25/1

|                         |                   |
|-------------------------|-------------------|
| Green with yellow front |                   |
| RRP £599.00             | Our price £475.00 |

## NEW HELJAN CLASS 25/3

|                                       |                   |
|---------------------------------------|-------------------|
| Choice of Five Liveries (see website) |                   |
| RRP £525.00                           | Our price £446.25 |

NOW AVAILABLE

## HELJAN CLASS 45

|                                              |                   |
|----------------------------------------------|-------------------|
| 6 versions from original green to later blue |                   |
| RRP £699.00                                  | Our price £550.00 |

NOW AVAILABLE

## HELJAN CLASS 05

|                          |                   |
|--------------------------|-------------------|
| 7 versions (see website) |                   |
| RRP £395.00              | Our price £315.00 |

AVAILABLE NOW

## NEW HELJAN CLASS 20

|                         |                   |
|-------------------------|-------------------|
| Centre Headcode Version |                   |
| 4 Liveries Available    |                   |
| RRP £599.00             | Our price £475.00 |

## NEW HELJAN CLASS 03

|                         |                   |
|-------------------------|-------------------|
| 14 versions RRP £395.00 | Our price £335.75 |
|-------------------------|-------------------|

SEE WEBSITE FOR DETAILS

## HELJAN DIESEL DEPOT

|                           |                  |
|---------------------------|------------------|
| Two Road Shed RRP £89.00  | Our price £75.00 |
| Extension Pack RRP £39.00 | Our price £35.25 |
| Spare Ends (3) RRP £19.00 | Our price £16.00 |

SEE WEBSITE FOR DETAILS AND SIZES

## HELJAN DELTIC

|                           |                   |
|---------------------------|-------------------|
| Limited Re-run 4 Liveries |                   |
| List Price £599.00        | Our Price £509.15 |

AVAILABLE LATE 2019 RESERVE YOURS NOW

## HELJAN CLASS 31

|                           |                   |
|---------------------------|-------------------|
| Limited Re-run 6 Liveries |                   |
| List Price £549.00        | Our Price £466.65 |

AVAILABLE LATE 2019 RESERVE YOURS NOW

## HELJAN CLASS 35 HYMEK

|                           |                   |
|---------------------------|-------------------|
| Limited Re-run 4 Liveries |                   |
| List Price £495.00        | Our Price £420.75 |

AVAILABLE EARLY 2020 RESERVE YOURS NOW

## HELJAN AC RAILBUS CLEARANCE

|                                 |                   |
|---------------------------------|-------------------|
| Light green with speed whiskers |                   |
| Dark green with yellow panel    |                   |
| List Price £495.00              | Our Price £299.95 |

WHILST STOCKS LAST

## HELJAN MODERN FREIGHT STOCK

|                             |         |
|-----------------------------|---------|
| Class B Tankers 4 types     | £70.00  |
| HAA Vans 4 types            | £70.00  |
| OAA Open Wagons 5 types     | £70.00  |
| Cargowaggons 5 types        | £155.00 |
| Cargo Flat Wagons 4 types   | £155.00 |
| Catfish and Dogfish 8 types | £79.95  |

## HELJAN GUV AND BG

|                     |                        |
|---------------------|------------------------|
| FULL PARCELS BRACKS |                        |
| Choice of liveries  |                        |
| RRP £325.00 each    | Our Price £169.15 each |

LAST FEW

All prices include VAT @ 20%. Correct at time of going to press but subject to alteration without notice.

## TOWER MODELS, 44 Cookson St., Blackpool FY1 3ED

E-mail: sales@tower-models.com Website: [www.tower-models.com](http://www.tower-models.com)

Tel: 01253 623797 or 623799

TELEPHONE CREDIT CARD ORDERS WELCOME WORLDWIDE MAIL ORDER

Visit our well stocked shop, 200 yards from Talbot Rd. Bus Station (Blackpool North Railway Station 400 yards). Open Mon to Sat 9.30 to 5.00

## POSTAGE CHARGES

Orders up to £35.00 plus £2.50 p&p.  
Orders £35.01 to £150.00 plus £6.00 p&p.  
**ORDERS OVER £150 POST FREE**  
Overseas orders postage charged at cost.



[www.harburns.co.uk](http://www.harburns.co.uk)

Harburn Hobbies, 67 Elm Row, Edinburgh EH7 4AQ

Tel. 0131 556 3233 Email: sales@harburnhobbies.com

See the full details at:  
[www.harburns.co.uk](http://www.harburns.co.uk)



P&P  
£3.00  
per order

'00'  
scale



Ready  
to  
place



Ready Made & Painted  
Ready to Appreciate!

'00'  
scale



Order by Phone: 0131 556 3233 Securely Online: [www.harburns.co.uk](http://www.harburns.co.uk) By Post: Harburn Hobbies, 67 Elm Row, Edinburgh EH7 4AQ





# RAISING THE BAR

OO Scale Class 90 Locomotives

Our Branchline Class 90 is the first British outline Electric Locomotive to feature a servo motor-operated Pantograph which can be raised or lowered using Digital Command Control (DCC).



**32-612** Class 90 90042  
Freightliner Powerhaul

**32-610** Class 90 90005  
'Financial Times' BR InterCity Swallow

**32-611** Class 90 90037  
BR Railfreight Distribution Sector

Available in three popular BR business sector era and post-privatization liveries, our highly detailed Branchline Class 90 models make an electrifying addition to your layout!

## FEATURES INCLUDE:

- Highly Detailed Servo Operated Pantograph
- Sprung Buffers
- Retractable Buffers (on certain models)
- Full Depth Cab Interior
- Switchable Day & Night Running Modes
- Independent Head & Tail Lights
- Accessory Pack



To raise and lower the pantograph under DCC operation, we offer a decoder that is preconfigured for the Class 90 Locomotives.



**36-569**  
E-Z Command®  
21MTC Class 90  
Decoder

Please Note: Damage may occur to the servo or decoder if a non-compatible decoder is used.

**AVAILABLE TO BUY FROM YOUR LOCAL BACHMANN STOCKIST TODAY!**



WATCH OUR CLASS 90 VIDEO  
VISIT OUR  
**YOUTUBE**  
CHANNEL

OO Scale

To find your nearest retailer, please visit [www.bachmann.co.uk](http://www.bachmann.co.uk)

Model • Collect • Create





**Editor & Photographer**  
Steve Flint

**Production Editor**  
Tim Rayner

**Features Writer**  
Craig Tiley

**Editorial Assistants**  
Julie Newbery,  
Nicole Charlton, Sue Davis

**Art Director**  
Adrian Stickland

**Review Photographer**  
Jolyon Sargent

**Graphic Illustration**  
Dave Clements, Gary Bickley,  
Steve Croucher, David Malton

**Advertisement & General Manager**  
John King

**Advertisement Assistant**  
Sue Davis

**Classified Advertisements**  
Nicole Charlton

**Direct Subscriptions**  
Alicia Flint

**Chairman**  
C.M. Pritchard

**Editorial Office & Subscriptions:**  
Tel. 01297 20580

Website:  
[www.pecopublications.co.uk](http://www.pecopublications.co.uk)

**Published on the second Thursday  
of the preceding month.**

ISSN 0033-8931

Annual subscription (12 issues):  
United Kingdom - £45.00 post free.  
Overseas including Eire -  
£74.00, Air Saver Post.

Airmail rates on request.  
Peco Technical Advice Bureau,  
address as below.  
Back numbers information - see Classified  
advertisement under 'Trade Sales Books'.

**Peco Publications & Publicity Ltd,**  
Beer, Seaton, Devon,  
EX12 3NA, England.  
Telephone: 01297 20580  
Fax 01297 20229  
Website: [www.peco-uk.com](http://www.peco-uk.com)  
Email: [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

© Peco Publications & Publicity Ltd. 2019

All rights reserved. No part of this magazine may be reproduced or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording or by any information storage and retrieval system, without prior permission in writing from the copyright owners. Multiple copying of the contents of this magazine without prior written approval is not permitted.

That it shall not without the written consent of the publishers be lent, resold, hired out or otherwise disposed of by way of trade at a price in excess of the recommended maximum price or in

## Contributing to RAILWAY MODELLER

We welcome submissions on all aspects of railway modelling, be they layout projects or suggestions, constructional articles, advice and encouragement for beginners, prototype features or what-have-you. We prefer material to be sent through the post to the address below left, on CD-R if digital images are involved, but short introductions to the article involved and a few sample photographs can be emailed in the first instance to [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

In both cases, please remember to include your full name, address and contact details with your correspondence.

# Welcome from the Editor

## Running up

**D**uring the preparation of this month's RAILWAY MODELLER, I spent some time away from the office recording the second series of Channel 5's The Great Model Railway Challenge. The location was again at Fawley Hill, Oxfordshire, where the late Sir William MacAlpine's private railway collection is housed. Kathy Millatt and I once again undertook the judging of the layouts.

The first series of the programme was aired last autumn and was met with wide approval from the viewing public and modellers alike. Such was the success that the second series was given the go-ahead by the programme commissioner.

Of course, not everyone liked the show, bemoaning that it was far too superficial and did not portray the true nature of the hobby.

Mmm, so what is the true nature of the hobby anyway?

Well, that's a difficult question, as railway modelling means different things to different people. Perhaps the question should be: what has The Great Model Railway Challenge done for the hobby?

The answer is that it has done the hobby proud - really proud.

Reaching up to 1.4 million viewers, the programme was able to promote our wonderful and worthwhile pursuit far beyond the usual agencies of model railway magazines and books, exhibitions and social media platforms.

Model shop proprietors and show managers all reported increased footfall following the broadcasts in October and November and the RAILWAY MODELLER saw an upturn in its circulation.

Coupled with James May's recent two part BBC4 programme Big Trouble in Modern Britain, which

described the turnaround in the fortunes of Hornby Hobbies, and numerous other TV shows covering railway topics, our hobby is getting some terrific exposure across the nation.

That's got to be good.

Of course, The Great Model Railway Challenge themes, which the teams of contestants have to interpret in their builds, are strikingly unlike the usual subjects we modellers tackle. So being capable of reaching far beyond one's normal modelling comfort zones has to be an essential characteristic in those individuals competing.

Modellers are usually scholarly in their approach to layout building, aiming for authentic and historically accurate depictions of railway sites and locations, be they based on real places or might-have-been scenarios. In fact, the Challenge takes the might-have-been concept to stratospheric new heights, compelling even the most perfectionist of hobbyists to unleash all sorts of imaginative thoughts and creations.

Indeed, a sizeable contingent of contestants are what we might call 'purist' modellers; enthusiasts of long standing with lots of modelling experience

and capable of producing exquisite layouts. What is most heartening is that they all were keen and willing to embrace even the most outlandish of themes that the contest threw at them.

Moreover, we must not forget that the big manufacturers, including Peco, Hornby, Bachmann and Metcalfe amongst others are also giving huge support to the programme. The first episode of The Great Model Railway Challenge, Season 2, is due to be broadcast from late September 2019.

Steve Flint

You can keep up to date between issues by visiting our new website at [www.peco-uk.com](http://www.peco-uk.com) and our Facebook Page. You can also now follow Steve Flint on Twitter: @steveRMed



The Great Model Railway Challenge presenters, Car SOS host, Tim Shaw (left) and James Richardson, football podcaster and broadcaster, discover some truly inspirational material in the July issue of RAILWAY MODELLER.

Distribution to the model trade, direct subscriptions (Home & Overseas)  
Pritchard Patent Product Co Ltd (address and telephone as above).

Distribution to the newsagency trade (Home & Overseas)  
Marketforce (UK), 2nd Floor, 5 Churchill Place, Canary Wharf,  
London E14 5HU

Printed by  
William Gibbons & Sons Ltd., P.O.Box 103,  
26 Planetary Road, Willenhall, West Midlands WV13 3XT

And for all enthusiasts modelling overseas railways.

## CONTINENTAL MODELLER

Published on the third Thursday of the preceding month.



# Railway of the Month

Ffestiniog Railway Double Fairlie Merddin Emrys hauls the 'Heritage Train' through the crossing gates at Brockhampton.

Photography by Craig Tiley





# The Brockhampton & Umbridge Railway

A narrow gauge adventure in the garden, modelled in 16mm scale

**PETER BUTLER** describes the background to this supposed preservation revival of a former Welsh quarry system, developed as an outdoor extension to a layout that was retired from the exhibition circuit.

Way back in 1973 I joined a local model railway group and, to my amazement and delight, many of the members were narrow gauge modellers, albeit in 7mm scale at that time. We became close friends and colleagues and produced a number of O-16.5 layouts for exhibitions, gaining familiarity with the regular exhibitors and traders, whilst learning the routine of exhibition life – particularly the whims of our visiting public!

We soon felt that something completely different was required to entertain paying customers so a new venture began in the late 70s, this was to be in 16mm scale on 32mm

gauge, track powered (no steam), fully detailed and complete with scenery. At this time, many of the other exhibitors modelling in the scale were seemingly using Mamod locos, belching out fumes from the meths or solid fuel burners and running like scalded cats on plain test tracks! We were having none of that – our reputation in 7mm scale was based on detail, and plenty of it!

Although small in number, we had youth and enthusiasm on our side. What we undertook was something which would deter the majority of 'sensible' modellers, particularly as there was so little in the way of manufactured items available at the time.

With the benefit of hindsight it shouldn't have happened at all, but that is what made the adventure more appealing.

Working with limited time, funds and space, we somehow ended up with a 25' x 10' exhibition standard layout which toured UK and continental exhibitions, winning awards for stock, scenery and even 'Best in Show' at the 1990 Warley NEC Show... not bad for a dedicated group of only three regular participating members at that time.

**O-4-OT Lucy in charge of the goods train. My garden railway has its own miniature line, a modified, battery powered toy train set.**







▲ Early I P Engineering all steel diesel outline locomotive *Emma* working the indoor section of the line.

### Retired from the show circuit

However, things changed, and so it was that the layout was retired and put into storage whilst our group members concentrated on career and family matters. The layout was, in fact, destined never to see an exhibition hall again. Instead it came to live with me when my family and I moved from the West

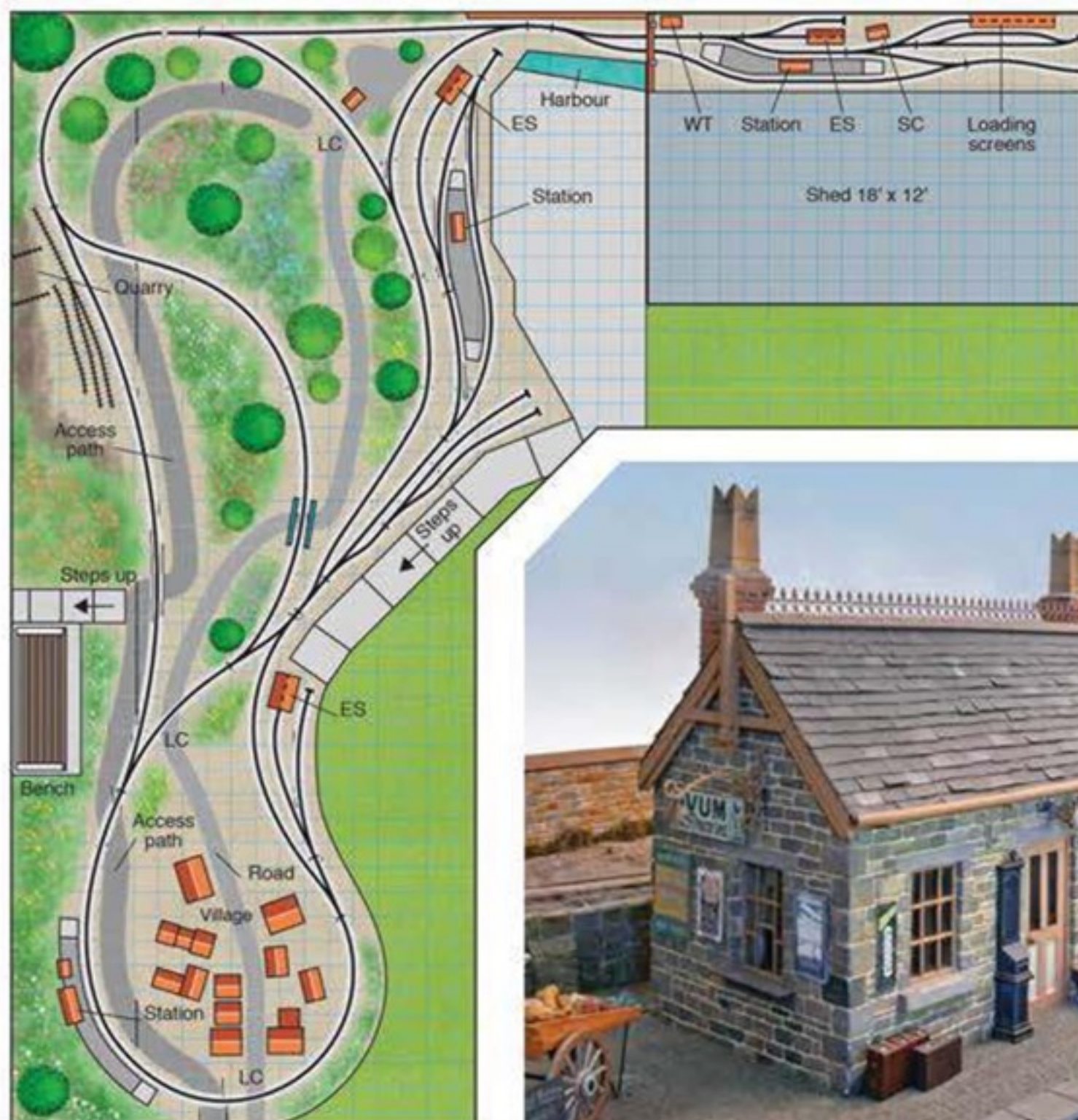
Midlands to West Wales. It continued its slumber in darkened sheds awaiting the call, which eventually came in 2012 when I began to extend the modified and truncated layout into the garden.

The main scenic section from the original *Umbridge* layout, which comprises a terminus station, storage sidings, engine shed, signal box and run-round is now comfortably housed in a purpose-built 18' long shed. It opens out through a small hatch at one end into a garden railway with continuous loop, terminus station, through station, quarry sidings, quayside with warehouses, reversing triangle and fully detailed village. Our home location has a generous supply of local stone which enabled me to build a natural backdrop to my railway.

My years of exhibiting have left an indelible mark on my wish to create something equally detailed which can withstand the Welsh climate, comparable to structures which would normally be created for indoor use only. I doubt I have done much by the way of inventing new methods of building for outdoor railways, but what I have produced is both pleasing and satisfactory when it comes to withstanding wet, dry, cold and (rare) hot conditions.

## BROCKHAMPTON & UMBRIDGE

Overall area of layout: 46' 6" x 44' 0". Each grid square represents 1ft x 1ft.



Lucy at Umbridge station with a short four-wheel passenger set.







### A fictional back-story

The village the railway serves began as a settlement before 1300 AD. It is near the coast and trading was established by water as well as land routes. The castle ruins testify to a thriving community and a stronghold to protect it. Ancient structures, including the Market Hall and the old Forge, have fortunately survived as they became essential during the time of development of the stone quarry, located just outside the village.

In the early 1800s, the high quality stone from the quarry became the 'must have' construction material by noted architects commissioned to build large country houses and municipal buildings, resulting in a rapid expansion of the village. Facilities for workers and services to support the industry became paramount and business blossomed.

As demand for the stone increased, so transport systems had to keep pace and a 2' narrow gauge railway was built in 1849. From the start, steam power was introduced as

there was ample water and coal available nearby. The quay, offering transport by sea for such heavy loads, was an immediate improvement on the previous horse and cart option, so increasing revenue and the ability for the landowner to construct his stately 'pile', Dylan House. The Zion Chapel was constructed in 1854 in an effort to influence the workers and keep them away from the local hostelrys, which were making a handsome living at the time.

High demand for the stone inevitably led to the quarry being worked out and consequently the demise of the local economy. As workers moved away the village fell into decline and the railway closed.

It was a fortunate coincidence when the current 'Lord of the Manor' stumbled on the ruins and decided to invest in its reconstruction, along with the reopening of the railway as a tourist attraction. It wasn't long before enthusiasts came onboard to assist and bring with them locomotives and

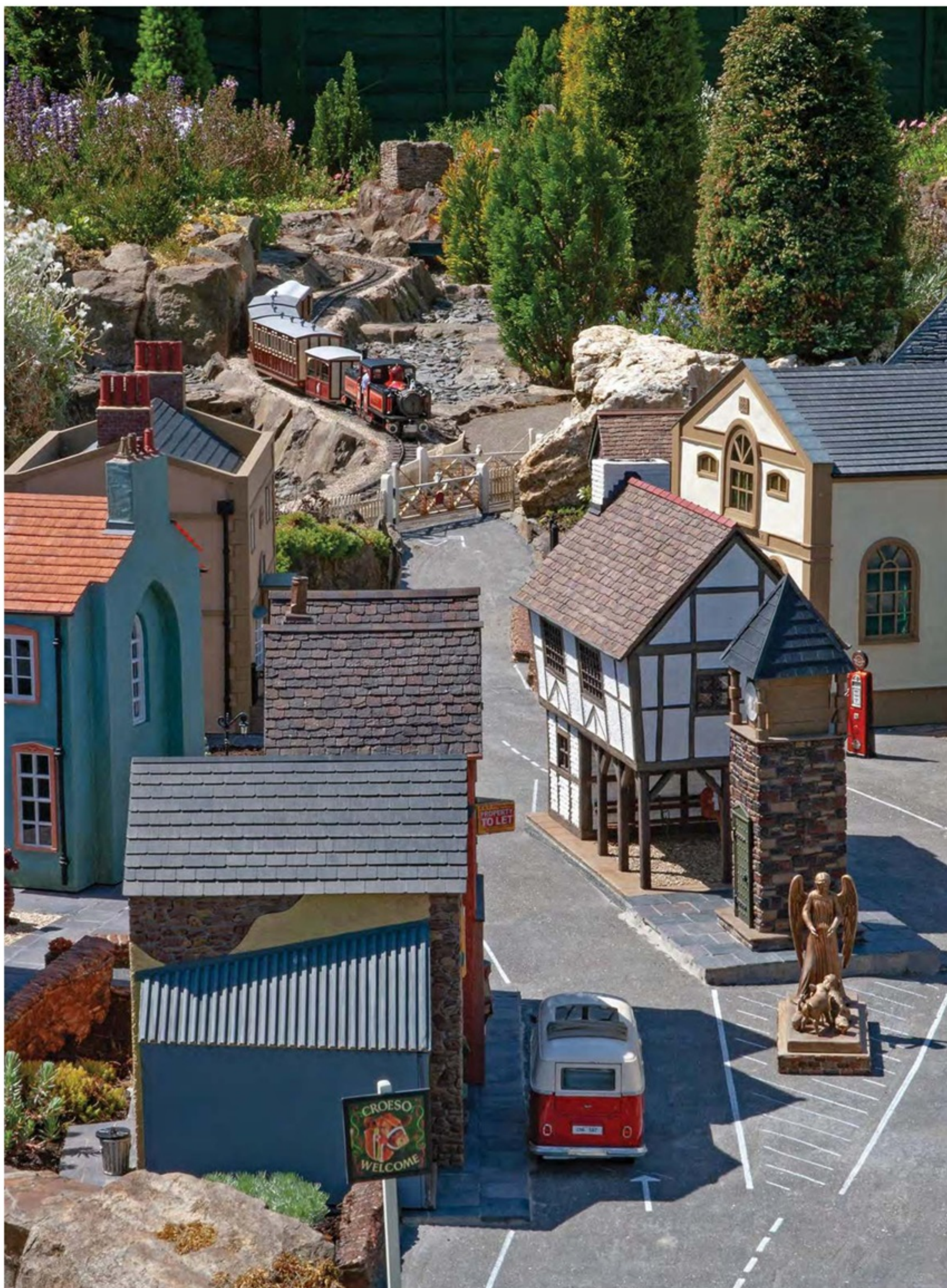
▲ Slater's kitbuilt Baldwin 4-6-0T passes a rake of bogie coaches – including an observation car – waiting at the station.

rolling stock to satisfy the demand from paying passengers on the preserved railway. Soon, a business plan evolved to cater for the increasing visitor numbers by opening shops, pubs and restaurants, combined with restoration of property as holiday accommodation and places of historical interest. And that is the background to the Brockhampton & Umbridge Railway Preservation Society.

### Venturing into the garden

I had absolutely no experience of building an outdoor layout before I began in December 2012. All I had was the retired exhibition layout which I placed at 44" (112cm) height inside my shed. Having cut a trap-door at one end to allow running outside I was then faced with sloping terrain







## Garden railway construction



▲ Construction began during winter 2012/3. The bank (left)...



▲ ...was excavated by the author's son during March 2013.



▲ Once the site was level, paving slabs were laid. April 2013.



▲ Supports and planking were assembled from treated timber.



▲ The decking is almost complete, April 2013. The hatch to the left of the door...



▲ ...was given a waterproof cover complete with draught excluder along the bottom.



▲ By July 2013, panelling had been installed to tidy the appearance, and painted.



▲ The author relaxing in the sunshine on a wonderful day – an overall view of the entire railway showing how the changes in elevation are catered for in its construction.

◀ The Double Fairlie eases through the curves in this view across the village with distant stone quarry workings and derelict castle.

to extend my line into. Luckily it slopes upwards – away from the shed – so it is at waist height at the front of the layout and cut into the ground at the rear, some 8m away. That is where the rock-cut 'galleries' are located which represent the original stone workings.

My method of construction was to have all track laid on gravel board (pressure treated timber platforms), supported off the ground by treated posts. In the intervening years since construction I haven't had any issues with 'ground heave' or rot, so remain perfectly happy with this method.





▲ Natural moss growth helps give reality to the scene. Photo (and right) by the author



▲ Pedestrian area in the heart of the village. The seated figures are 3D printed.

## A very permanent way

From the start I wanted to create a scenic layout with structures, landscape and planting as near to scale as reasonably possible. All of my Peco SM-32 track is ballasted using a fine crushed stone stuck down with SBR (Styrene Butadiene Rubber), a flexible, waterproof bonding agent. Most of the railway is level with one exception; a short, through siding (an afterthought), passing the disused quarry inclines to add to the visitor attractions.

All rock is local from the garden and is cemented in place to prevent any movement. Due to the low ground level and heavy clay soil, moss growth has taken over the rock faces and help disguise most of the cement joints. Nowhere are the track supports visible with stone embankments placed against the track-bed or planting level with it.

## Weatherproof infrastructure

Some of my structures are from Cain Howley, a local manufacturer of high quality buildings in concrete, whilst others are my own scratchbuilt buildings made from 2mm thick HIP (High Impact Polystyrene), internally braced. For brick or stone cladding I have used embossed sheet from Jackson's Miniatures and, in many cases, coated recessed panels with waterproof tile cement powder mixed with SBR. This gives a most satisfactory textured surface, similar to rendering. The surfaces are all sprayed with plastic primer and painted with Citadel acrylics from Games Workshop which withstand even the worst weather conditions West Wales can throw at them!

Most of the windows I use are from Jackson's Miniatures, but others are either of German manufacture or 3D printed to order. Other scenic detail includes street lamps (Christmas decorations), benches, street furniture and 3D printed figures from designs can print 3d. In the planning stage of my village scene I realised that, with concrete foundations and roads, I would have a flooding problem. To prevent this I have installed an underground drainage

system from domestic overflow pipework, with drain covers specially milled from solid alloy by a model club friend. They work perfectly!

## Locomotives and stock

The vast majority of my locomotives and rolling stock are scratchbuilt, again from HIP. Some are of prototypes but most are my own design, just to be different!

All locomotives are battery powered and radio controlled by a Deltang based system, originally built by Rik Bennett who established RC Trains, now taken over by Phil Partridge, who has continued to supply these wonderful products.

Recently I built a 16mm scale brass and cast resin locomotive kit for the very first time... talk about being thrown in at the deep end! Just opening the box for the Slater's Baldwin loco kit and seeing the vast array of etched, cast and moulded parts was a shock to the system, but the instructions were well illustrated and explained. It made into a very detailed locomotive which, after some surgery to the front bogie, works well and looks magnificent.



▲ Emma with the logging train passes the disused quarry workings. The gallery has now been relaid and included in the 'visitor experience' for rail tourists.

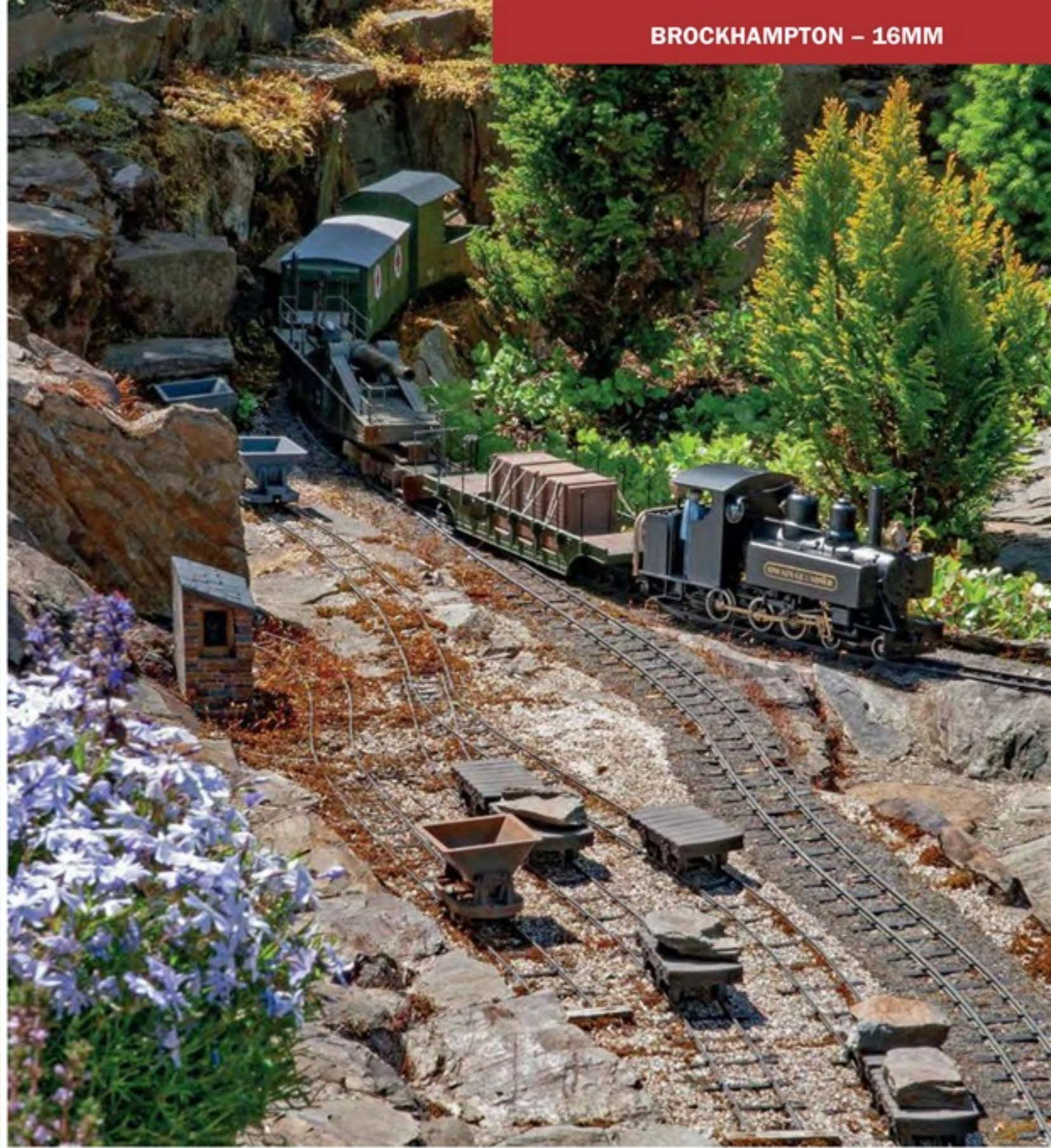




▲ Deltang-based RC Trains hand-held controller developed by Rik Bennett. One handset is capable of controlling up to 12 locomotives; forward, reverse, inertia, speed, lighting and sound cards.

Other kits I have recently discovered are from The Lineside Hut which are very well thought out and produced, particularly the Ffestiniog Railway Curly Roof Van. Also, a newcomer to the kit market, Jerry Irwin supplies highly detailed MDF rolling stock at very competitive prices.

When building my models I make every effort to recreate a convincing surface texture, particularly wood grain. This is done by scraping a razor saw blade along the HIP to scratch random lines into the surface before assembly. After some gentle



▲ Owain Glyndwr right at home with some of the military railway items on the line.

sanding it takes primer and acrylic paint well and can be aged with colour washes to show wear. Painting and weathering stock is a subject unto itself and has filled many a published volume. I have also discovered some useful tips from watching videos published online.

All of my rolling stock is fitted with steel wheels to give better running characteristics. I have recently undergone a programme of retro-fitting brake gear to all of my stock, along with vacuum hoses; small details but ones which help to improve their general appearance. In this large scale, although usually seen from a distance, such details become conspicuous by their absence.

### In closing...

Having gone to all this trouble to build my garden system, readers may be surprised to learn that I rarely use my railway, other than to host 16mm Association and South-West Wales Local Area Group meetings – my preference being to build rather than run trains!

For further details of my garden railway construction from the beginning and modelling techniques visit: <https://gardenrails.org/viewtopic.php?f=41&t=6537>





# Building Lincoln Central

Photography by Paul Bason

The size of the main station building can be gauged by the way it towers over a Bachmann Derby Lightweight DMU on the eastbound platform.



**HOWARD LEADER** introduces this mammoth project to recreate the former Great Northern station in the city, which (unlike the ex-Midland St Marks, close by) retains an active role in passenger and freight transportation today. The OO model, set in the 1960s, includes structures now demolished or altered.

Lincoln Central Station has, without doubt, been the biggest modelmaking challenge I have ever undertaken. To begin with, it is a very large building with a great deal of complex and grandiose architectural detail. In addition, the building has changed somewhat between 1965, the period of the layout, and what we see there today. On the plus side, at least the building is still there and I could occasionally visit to check details.

This model will occupy the middle of a large OO gauge layout describing Lincoln Central in the mid to late 1960s. I have already completed some other buildings for the layout including the crew accommodation block and the long-since demolished Great Northern Hotel with its adjacent parade of shops along the high street, but the station itself was always going to be a feat of modelling endurance.

The original structure was built in 1848 as the railway was driven through to link Peterborough with Gainsborough via Boston on what became known as the 'Loop'. Lincoln Central was a far cry from the

traditionally 'frugal' Great Northern buildings and was designed to harmonise with the historical city it served, hence its stone dressings, pointed arches, Tudor gables and a forest of tall ornate chimneys.

Fortunately, as one of the region's most unusual railway buildings, a set of architect's drawings has survived, lovingly preserved by a local railway enthusiast. These have proved invaluable as a source of measurement for scaling, architectural detail and a very good idea of how the interior was originally laid out, of which more later on.

## Island platform test piece

Studying the ornate stonework around the windows and other stone details among the yellow, or technically 'grey' brick, I decided that I would practice some of the construction techniques for the model by starting with the island platform structure as this was a much more straightforward build with a long curtain wall separating the platforms. On that model I worked out how to replicate the window design with its stone mullions, sills and lintels, each one made by

hand, forming the shapes and relief with plastic strip. After a bit of trial and error I settled on a colour for the brickwork and stonework that mirrored the prototype. At the same time I worked out a formula within the brick pattern to ensure that the stone quoin window surrounds were symmetrically matched. All of the quoins and corner stones had to be let-in to the brickwork as the walls are all flush in the prototype building.

## Working strip lights

The other technique to be mastered was the design, construction and installation of working strip lights, a prominent feature of the station in the 1960s, which we couldn't really leave out – and having made the decision to include them, well they obviously had to work! At the time this build began, scale working strip lights were not commercially available, so the layout's designer, who is an electrical genius well known to Lincolnshire railway modellers for his 'geekery', set to and produced around two dozen tiny strip lights which, once painted, decaled 'LINCOLN CENTRAL' and



The distinctive and still-extant gates were produced especially for the project. Working strip lights in 4mm scale cause much comment.

installed under the platform roof, have occasioned much comment at exhibitions.

Building the island platform first also gave me a template for the construction of the very distinctive gullwing canopies with their deep valance and long corrugated clear-panelled sections. I work mainly with plasticard, using paper or thin card for some of the detailing. Both the island platform buildings and the main station building are some 4' in length. Due to the sheer size of these structures, in places where sections might be prone to warping I reinforced with brass rod. Once we were happy with the island platform, I could no longer put off the moment to start on the main building.

### Jumble of interconnected buildings

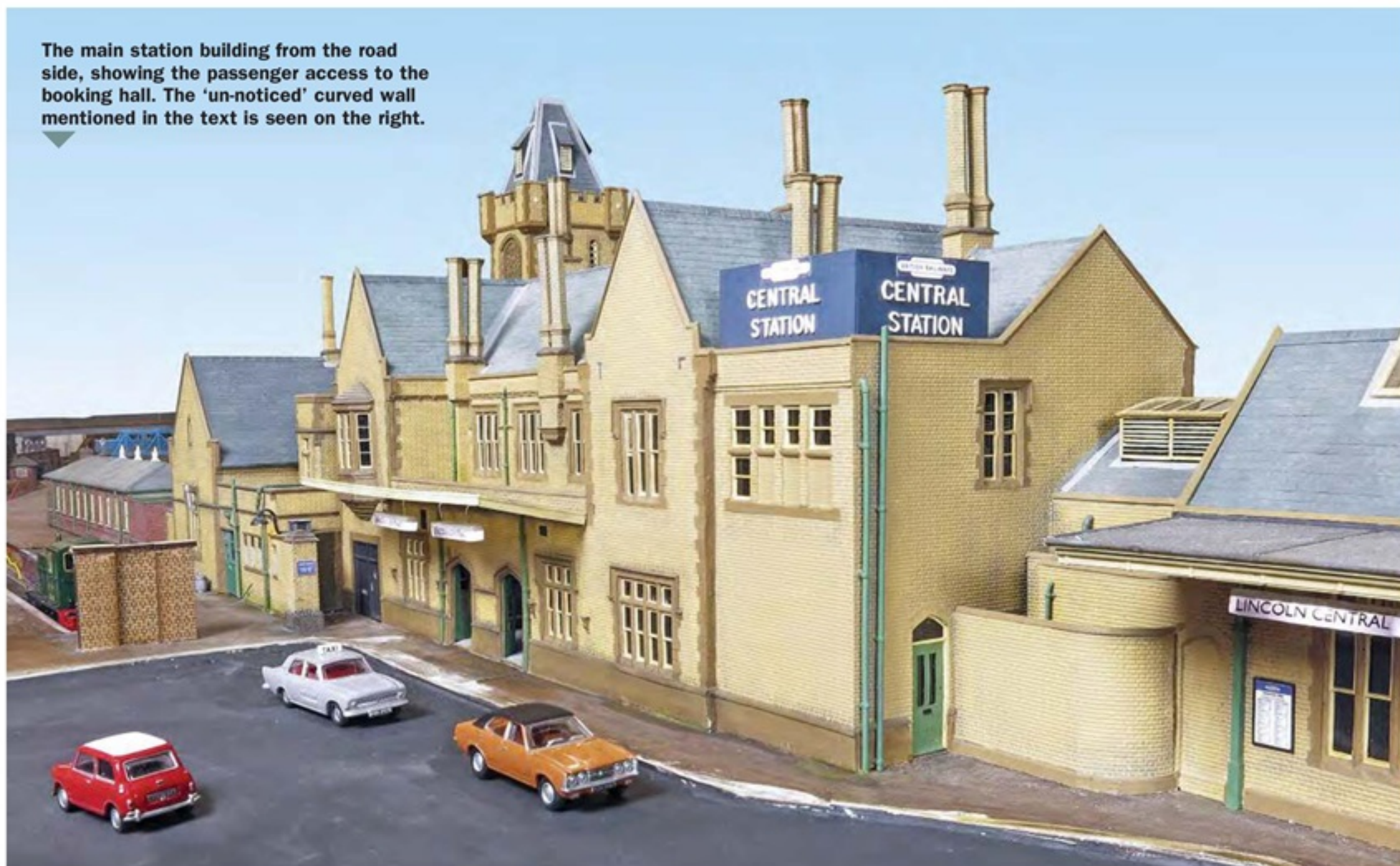
Study of a pile of photographs and drawings soon revealed that this is actually a jumble of many different interconnected buildings which include six pavilions, a main 'house' with upstairs offices, a real house tucked into the middle and another long single-storey structure which served as a parcels and mail sorting depot, canteen and more offices. But the single most distinctive feature of the building is the apparently pointless though gloriously impressive Baronial tower. Having analysed every segment, it was really just a question of setting about making each separate 'building' and slowly joining them together. I do mean slowly; this build has taken around two years!



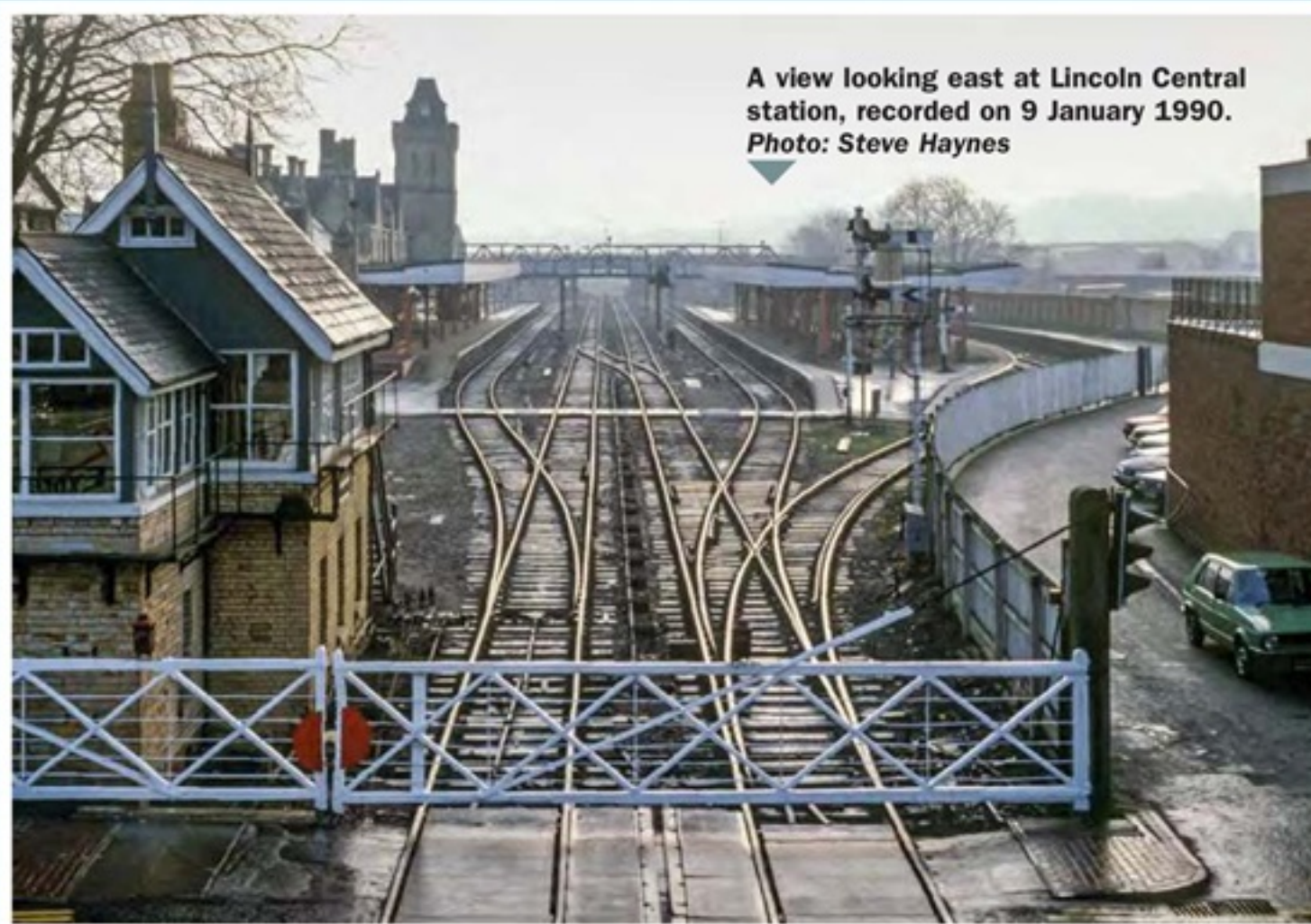
A lot of extra time went into the interior appointment of some of the spaces – the booking hall is a must for lighting and needed to look fairly busy. I also designed an interior for the upstairs board room which, in

the photo set I have from the period, shows a window wide open – presumably to let out all the hot air! Taking this cue I decided that my board room would have a discussion in progress – long into the night when the

The main station building from the road side, showing the passenger access to the booking hall. The 'un-noticed' curved wall mentioned in the text is seen on the right.







A view looking east at Lincoln Central station, recorded on 9 January 1990.  
Photo: Steve Haynes

layout operators are running the night-time scene. Downstairs there are clerical staff hard at work in one of the offices and further along activity in the post room is in full swing, although the foreman seems to have popped out of his office, leaving his coat hanging on the peg.

### Challenges – routing wires, and windows

With this level of interior lighting and detail, two additional challenges present themselves. Firstly, a lot of thought has to go into deciding where wires and resistors may conveniently be housed. Anywhere there is an apex roof makes the task of soldering wires and concealing resistors

easier, but where there are flat roofs, or worse in the case of the very slender over-platform canopy, finding space to cover them all without distorting the finished profile can be a conundrum.

The other challenge is windows. They are easy enough to make if you are only viewing them from outside, but if the interior is to be seen, then those bits of Perspex fixed by offcuts which look so convincing from the 'outside' are a complete eyesore when you view the 'inside'. Thus, a lot more care has to be taken

to finish the window interiors so they do not detract from the overall scene in the room.

### Quirky bits of bodgery

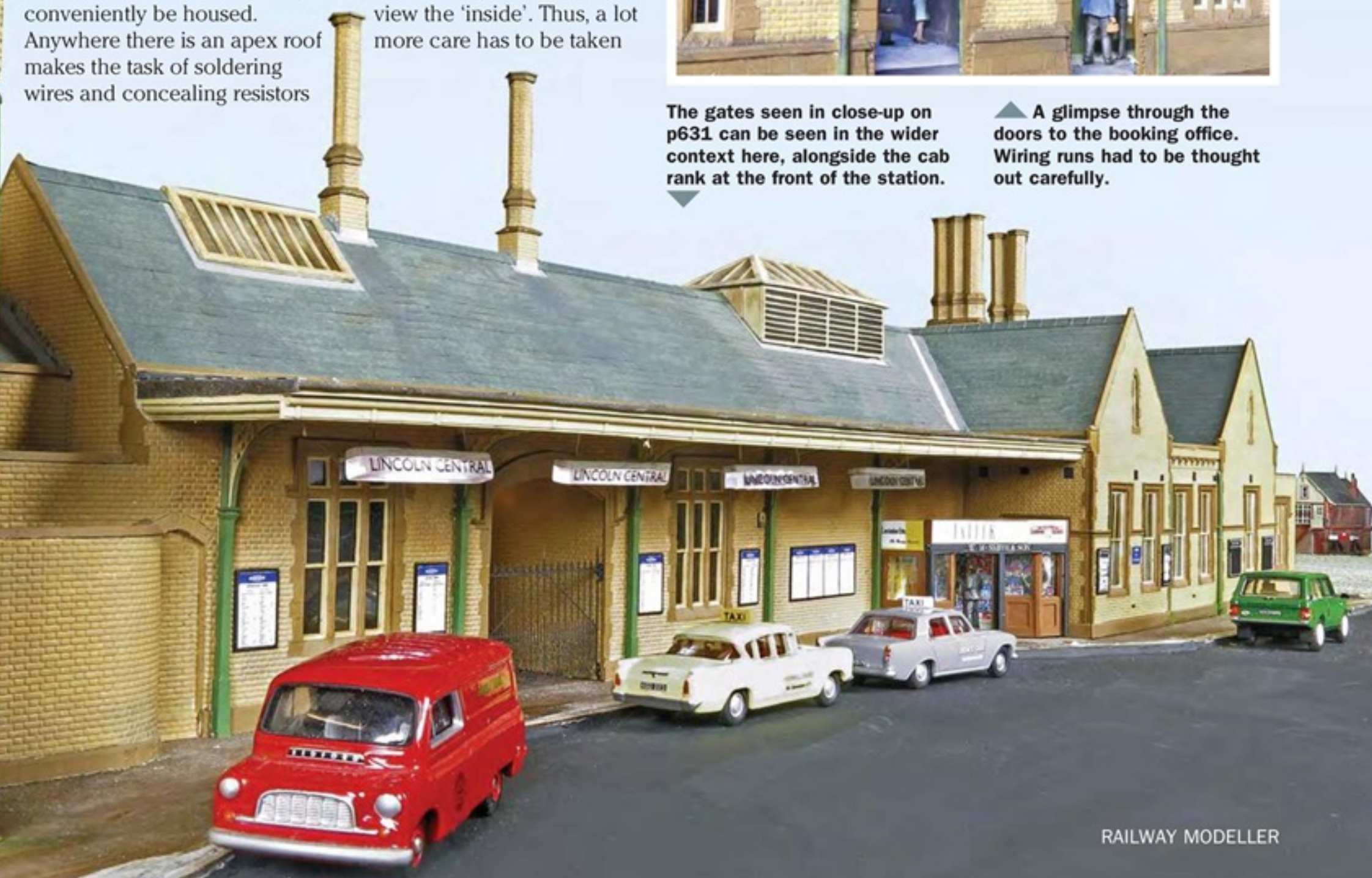
Constant reference to a large selection of 1960s photographs revealed all manner of quirky bits of bodgery with drainpipes running horizontally above flat roofs to take the most direct line to the drop, ventilation flues cut through glass panes in sash windows and evidence of doors and windows having been moved as the building's use changed over time.

Luckily a railway enthusiast in the 1960s took a comprehensive set of pictures and I have tried to include in the model everything that appears in those images. Such is the size and complexity of the structure that, when I was showing the project as work-in-progress at the Spalding exhibition, a chap who actually works at Lincoln Central Station was studying the model and queried a small curved wall outside the kitchen doors at the front of the building. He said "I walk past the kitchens every day, I am sure there isn't a curved wall there now." The following day he returned to confirm, to his surprise, that the wall is still there, he had just never noticed it.



The gates seen in close-up on p631 can be seen in the wider context here, alongside the cab rank at the front of the station.

A glimpse through the doors to the booking office. Wiring runs had to be thought out carefully.







An elevated viewpoint of the road side of the building shows the office of the British Transport Police – as located today – and the elaborate clock in the tower. The plain canopy over the entrance has been replaced with a more attractive glazed arrangement.



▲ The oddly-shaped W H Smith kiosk beside the cab rank was reproduced with the aid of photographs. Period magazine covers and advertisements complete the scene.

### Chimneys and gates

For quite a while I contemplated how I would reproduce the chimneys. Had they all been the same, the obvious answer would have been 3D printing or casting, but as there are no fewer than six different sizes and styles on the building, I decided simply to build them out of plasticard which probably took me less time than producing CAD drawings for each.

The detail that presented the biggest headache was the ornate pair of gates in the archway at the front of the building where presumably mail and newspapers were offloaded. We discussed this at some length and again concluded that anyone who knows the building well, bearing in mind those gates are still there and very prominent, would spot instantly if ours were any different. There was

only one thing to do – bite the bullet and have them cast. I must say in praise of the casters they are an extraordinary likeness and should avert any comments about “the wrong gates”.

### Cover star

One feature I enjoyed including was the long-gone W H Smiths magazine stand under the front

canopy, (also long-gone!) It was an odd-shaped structure but the period pictures gave me enough to go on and I was even able to read and reproduce some of the advertising on the over-board. There is a man inside looking for the August '65 edition of RAILWAY MODELLER!

### Conclusion

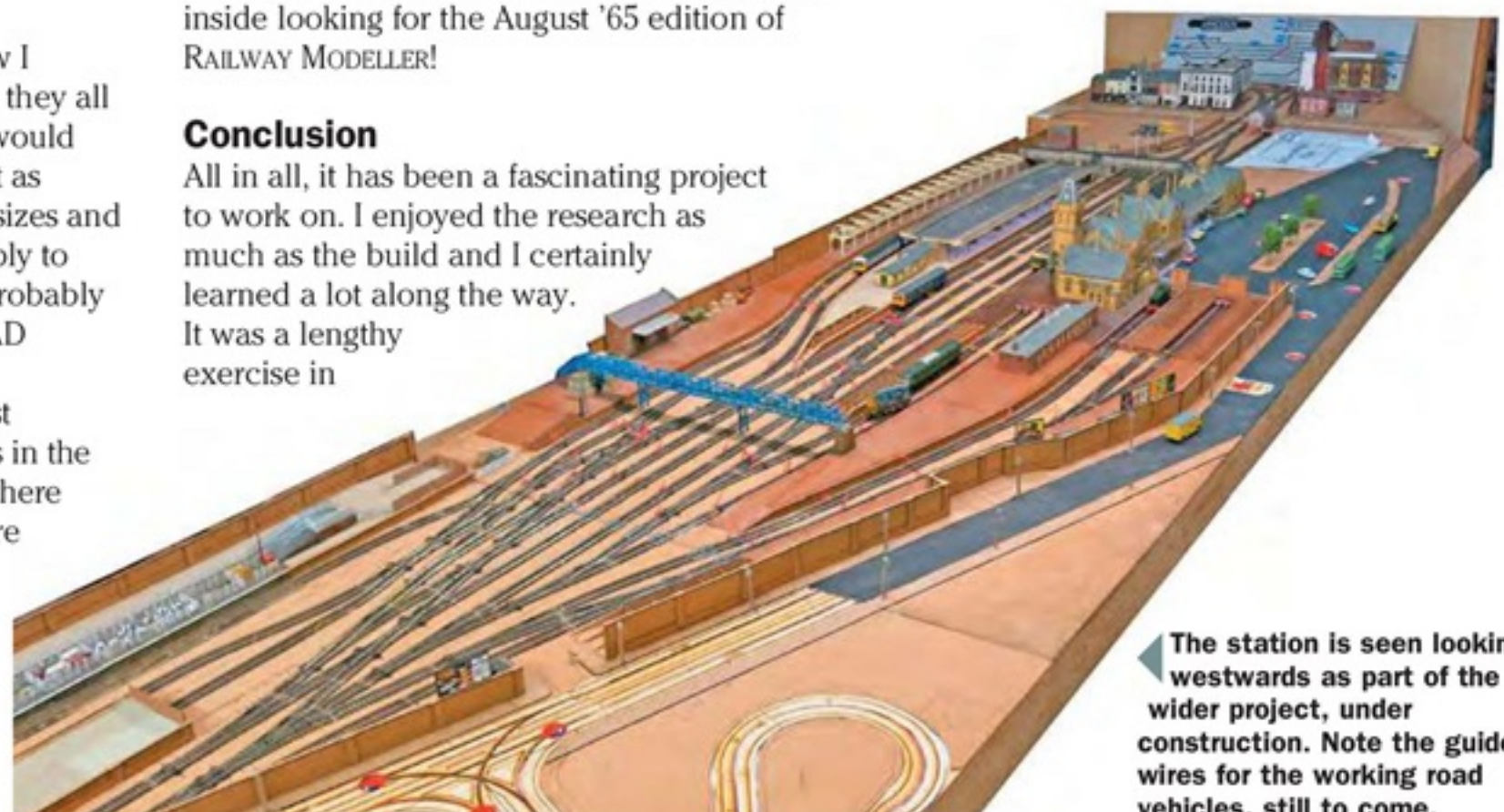
All in all, it has been a fascinating project to work on. I enjoyed the research as much as the build and I certainly learned a lot along the way. It was a lengthy exercise in

thinking five steps ahead and did, at times, require stamina in order to maintain the modelling mojo, but finally it is in its place on the layout which will now take shape around it.

Currently the layout has a comfortable home in Digitrains in Lincoln where customers are welcome to view it as it develops. The layout will ultimately tour the exhibition circuit; indeed it will be making a few appearances this year as a demonstration of work-in-progress.

My next assignment is to make the stable block on the opposite side of the High Street from the hotel, an interesting building, also with an elaborate tower and home to the station's shunting horses... but that, no doubt, will be another story.

*Ed. – Digitrains is at 15 Clifton Street, Lincoln LN5 8LQ. [www.digitrains.co.uk](http://www.digitrains.co.uk)*



▲ The station is seen looking westwards as part of the wider project, under construction. Note the guide wires for the working road vehicles, still to come.



# Sherton Abbas

The Great Western Railway branch line terminus has long been a popular theme for many railway modellers, but few have chosen to depict the pre-Grouping era. **DAVE STONE** describes the construction of his exquisite 7mm scale rendition, which bristles with Edwardian splendour.

Photography by Craig Tilley

**H**aving for many years constructed layouts in 4mm scale, in both OO and EM gauges, recently my curiosity was piqued by the prospect of modelling in a larger scale; at exhibitions I would find myself spending more time looking at layouts built in 7mm scale, so in a rush of enthusiasm I joined the Gauge O Guild and purchased a Slater's 7mm wagon kit!

The Guild proved to be a mine of useful information about modelling in 7mm scale and I found building the wagon to be a very satisfying experience. The 7mm scale bug had well and truly bitten and I decided there was no going back to 4mm scale...

## Wish listing

I've long been a fan of all things Great Western and I'm particularly interested in the Edwardian era. Locomotives and carriages of this period are fully lined with loads of

polished brasswork, which to my eyes show the railway at its most elegant.

My first thoughts for a 7mm project started with writing a list of features that I wanted to incorporate on the layout, with a view to searching for a suitable prototype station that fulfilled my 'wish list'. The criteria included:

- Great Western Railway circa 1905;
- 7mm:1ft / O gauge finescale;
- Hand built trackwork (min. 6' radius);
- Branch line terminus;
- Station platform facing viewing side;
- Small goods yard with goods shed and cattle dock;
- Engine shed;
- Private siding leading to an off-stage industry of some kind (eg creamery, brickworks, or brewery);
- Not more than 6m long (inc. fiddle yard);
- Sufficient width of baseboards to allow decent scenic development;







▲ Dean Goods No.2467 arrives at the terminus with a short goods train. The industrial Manning Wardle shuffles wagons in the yard.

William Clarke was, by the 1870s, engineer to several lines which went on to have Great Western connections, such as the Bristol & North Somerset, and the Abbotsbury Railway. ▶

◀ Metro 2-4-0T No.1500 prepares to depart Sherton Abbas for the main line.

- Sectional baseboards, enabling layout to be transported to exhibitions;
- Elliptical layout design, with curved back scene avoiding any 90° corners.

Not unsurprisingly, I couldn't find a prototype that exactly fitted my requirements, so decided to make a fictitious composite design based on prototype practice and taking buildings from various Great Western stations. The name 'Sherton Abbas' comes from Thomas Hardy's Wessex novels and is what he renamed Sherborne in Dorset, although quite what he would have made of my railway is anyone's guess!



### Layout foundations

I thoroughly enjoy doodling track plans and a number of drafts were made before settling on the final design. I drew this out at full size onto some lining paper to get a sense of the layout's proportions – it was at this stage I realised just how big 7mm scale models are!

I constructed the baseboards using beams made from 6mm MDF and softwood blocks as described in Barry Norman's excellent book *Designing a Layout: Building a Model Railway* (published by Wild Swan). The baseboard tops were then glued and screwed to these beams in areas where the





▲ 517 Class 0-4-2T No. 539 tacks a six-wheel Siphon van onto the front of the branch service. Note the colour of the point rod.

track was to be laid. The finished baseboards have proved to be very stable, but are heavy and in retrospect I probably should have used plywood which is considerably lighter.

The layout splits into four sections; each board is 1.4m long and 1.2m deep. This results in a total layout footprint of 5.6m x 1.2m, designed to fit along one wall of my modelling room. The boards are aligned to one another using brass patternmakers' dowels, which ensures accurate assembly of the layout.

### Handbuilt trackwork

In areas where the track was to be laid 3mm cork was fixed to the baseboards using Copydex latex adhesive. This was used rather than PVA because once set it remains rubbery, which allows

the track to 'float' slightly and helps sound absorption.

The track was constructed using nickel silver bullhead rail, held in position by plastic two-bolt chairs mounted on wooden sleepers. All these products were purchased from C&L Finescale. Once the track had been assembled the sleepers were coloured using Colron wood stain. During the period modelled the Great Western Railway laid its track in 32' panels, so cosmetic joints and fishplates were applied to the rails to simulate this.

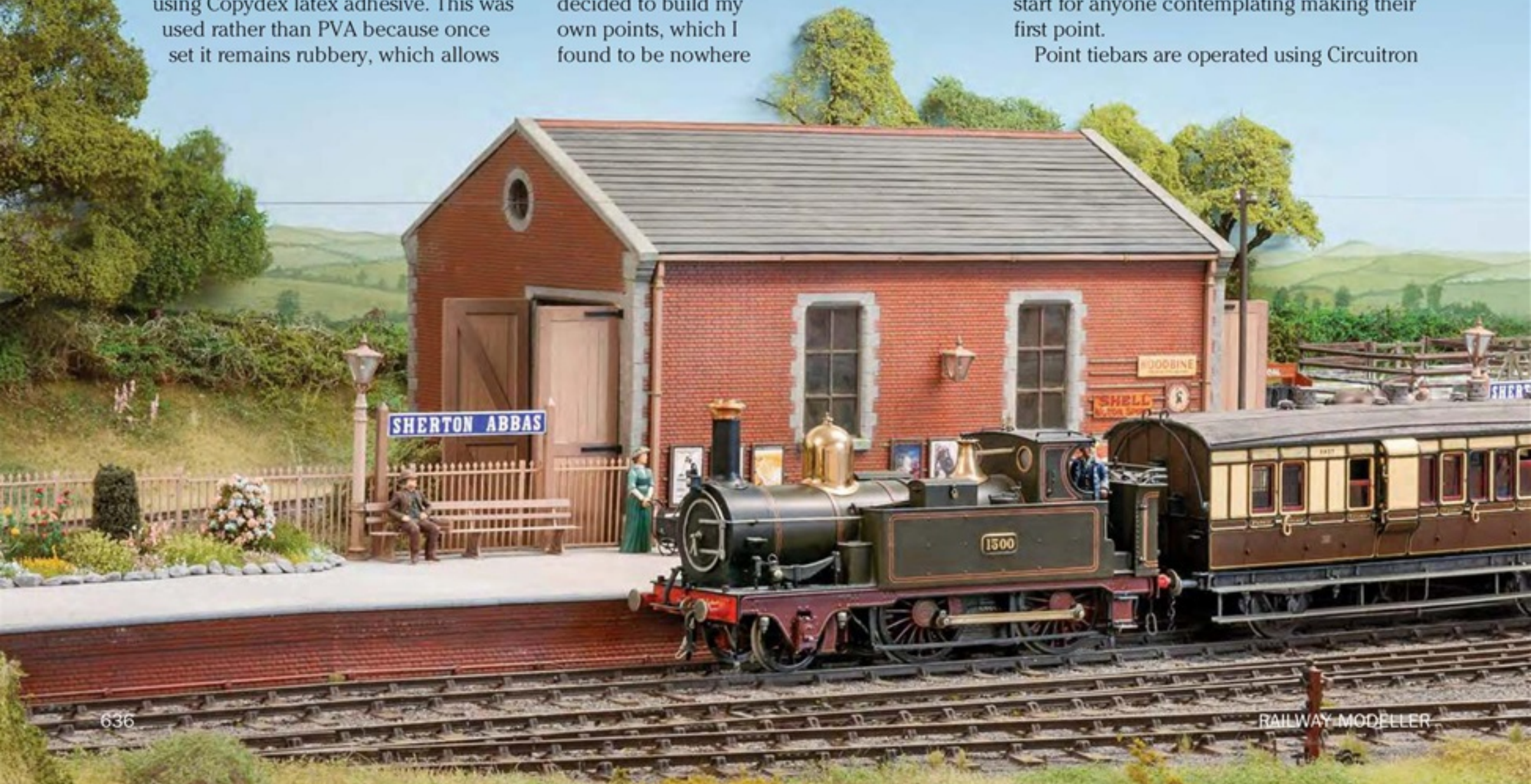
Prototype track exhibits a sinuous flow through pointwork which I find very attractive, but struggle to replicate using proprietary ready-to-lay points. Therefore I decided to build my own points, which I found to be nowhere



▲ It's easy to overlook the vivid colours found in the pre-WW1 era, conditioned as we are by black & white photographs.

near as difficult as it sounds, especially with the use of jigs to hold the rails in the correct position. C&L Finescale has in its range a 'point kit in a bag' which is a really useful start for anyone contemplating making their first point.

Point tiebars are operated using Circuitron





Tortoise point motors fitted under the baseboards, which have so far performed faultlessly. On previous layouts I've used solenoids, but have never been happy with the way the blades are fired from left to right so rapidly. The Tortoise motor moves the blades in a much slower manner and also puts less strain on the tiebar.

The track was then ballasted using a variety of Woodland Scenics ballast scatters. I used the firm's finest particle size in the yard to represent ash ballast and coarser material for the main running lines where more substantial granite ballast was often used.

### Point rodding

Point rodding is a conspicuous aspect of prototype trackwork and its absence is very apparent, particularly in 7mm scale. My representation was made using whitened metal stools and brass compensator cranks from Model Signal Engineering, together with 0.7mm brass wire to represent the rodding runs.

The GWR 1894 and 1907 Signal Department painting instructions state that point rodding and stools were painted in a colour referred to as 'Red, Torbay bright'. The exact shade of this colour is hard to establish, but it certainly wasn't black as I had originally assumed.

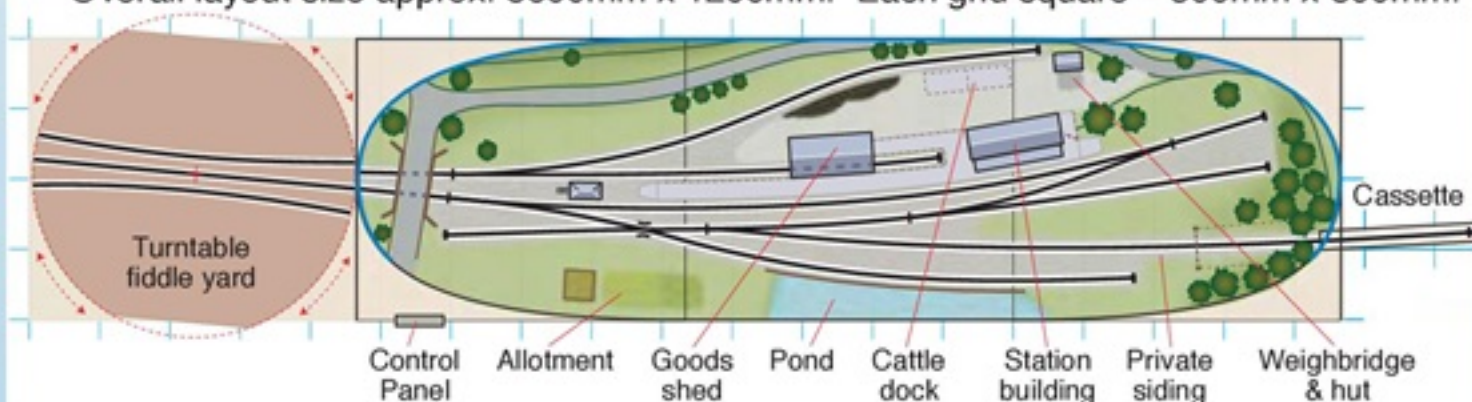
It's all too easy when looking at black and white pictures of the era to form the impression that the Victorians/Edwardians lived in a dull monochrome world. In fact, the more I discover about the subject, the more I realise how wrong that impression is!

### Scenic aspects

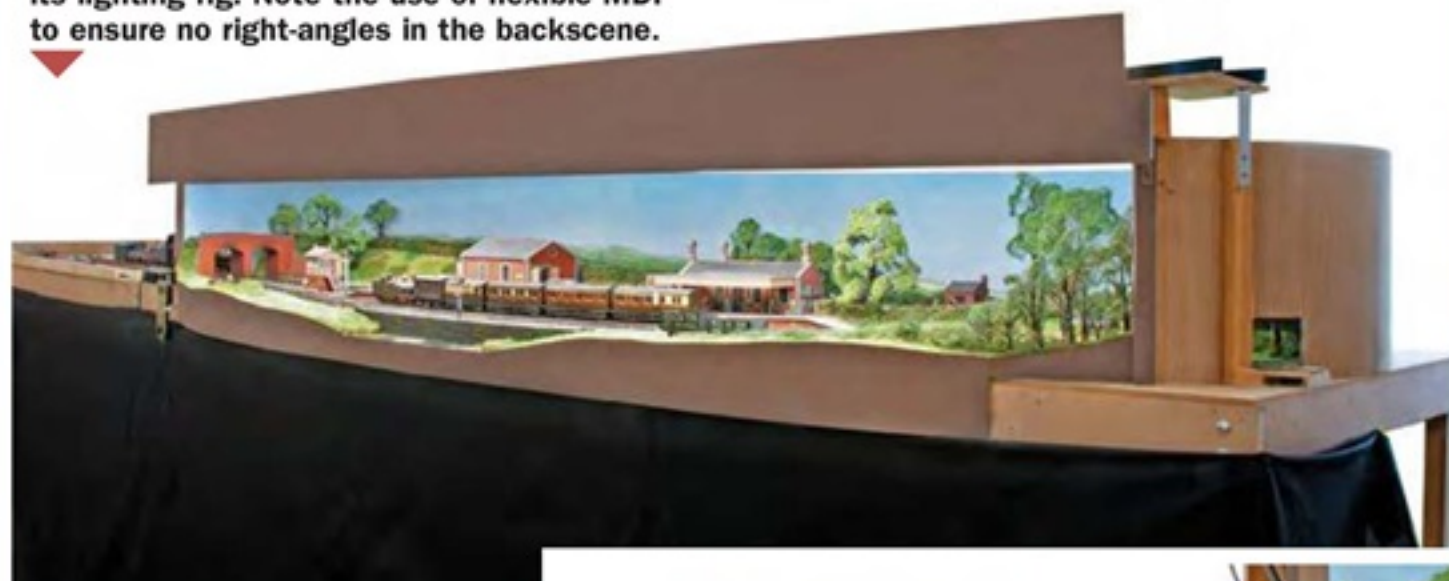
Once I was happy that the layout ran satisfactorily a start was made on the scenery, which is probably my favourite stage of layout building. The basic contours were made from expanded polystyrene sheets laminated together with PVA adhesive and then sanded with a surform into a rolling landscape. This polystyrene substructure was then sealed with a mixture of brown acrylic paint and more PVA glue.

### Sherton Abbas

Overall layout size approx. 5600mm x 1200mm. Each grid square = 300mm x 300mm.

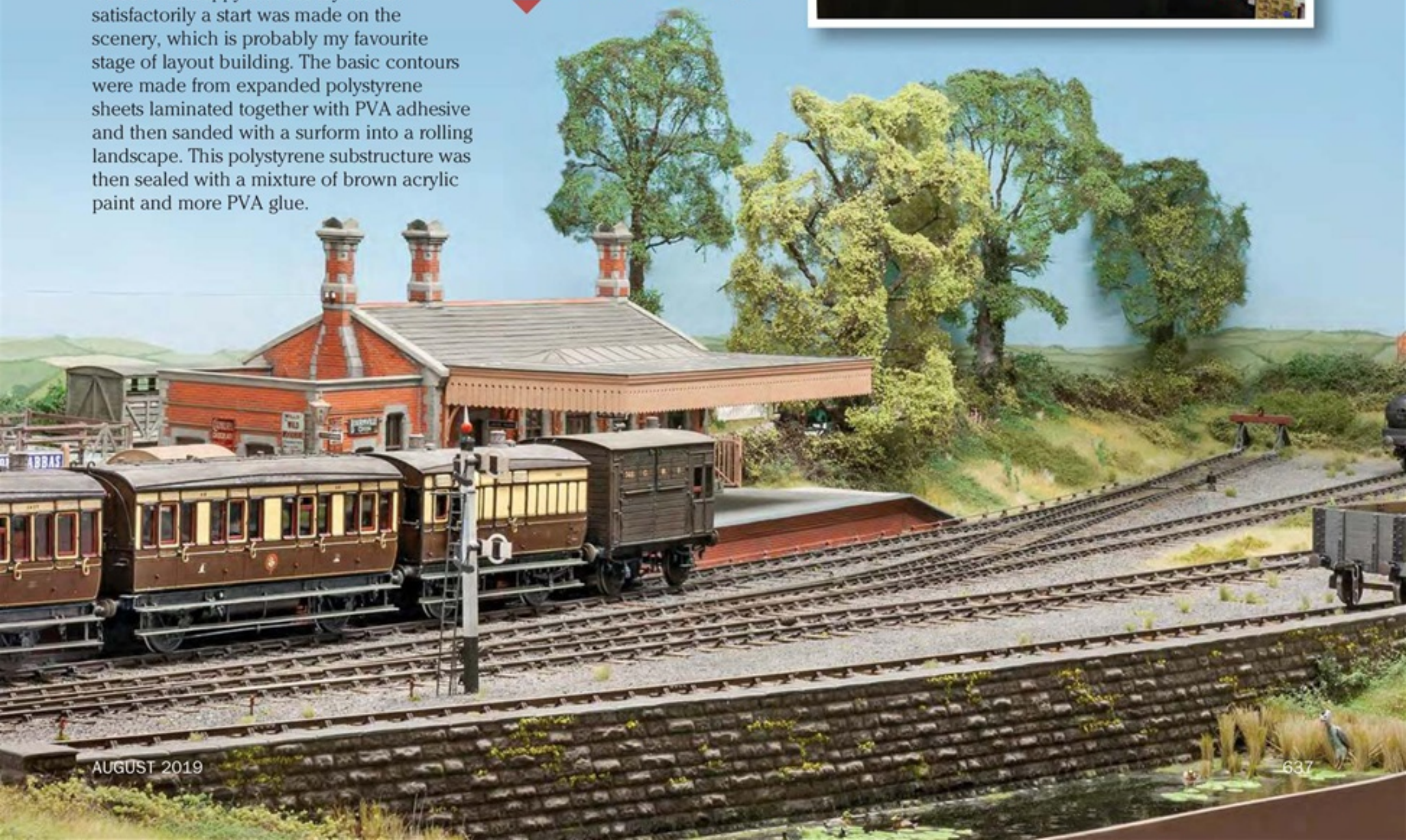
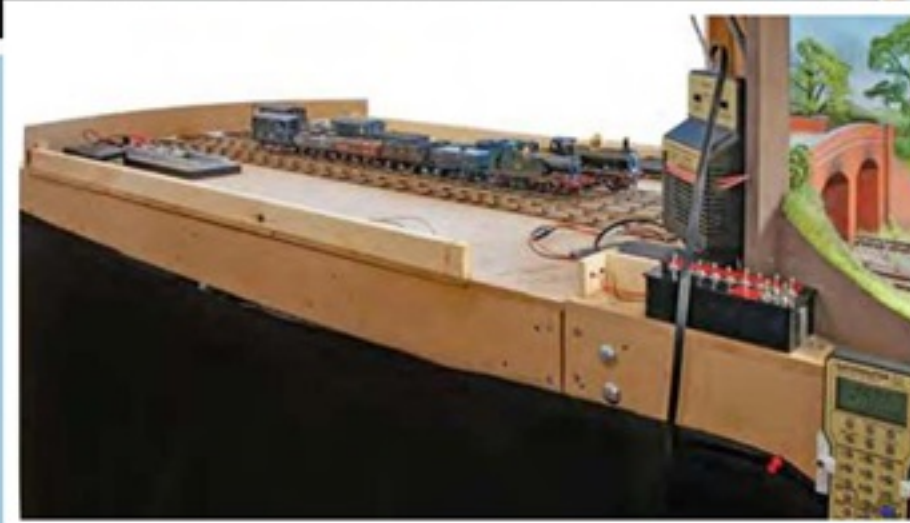


The layout is shown off to best advantage by its lighting rig. Note the use of flexible MDF to ensure no right-angles in the backscene.



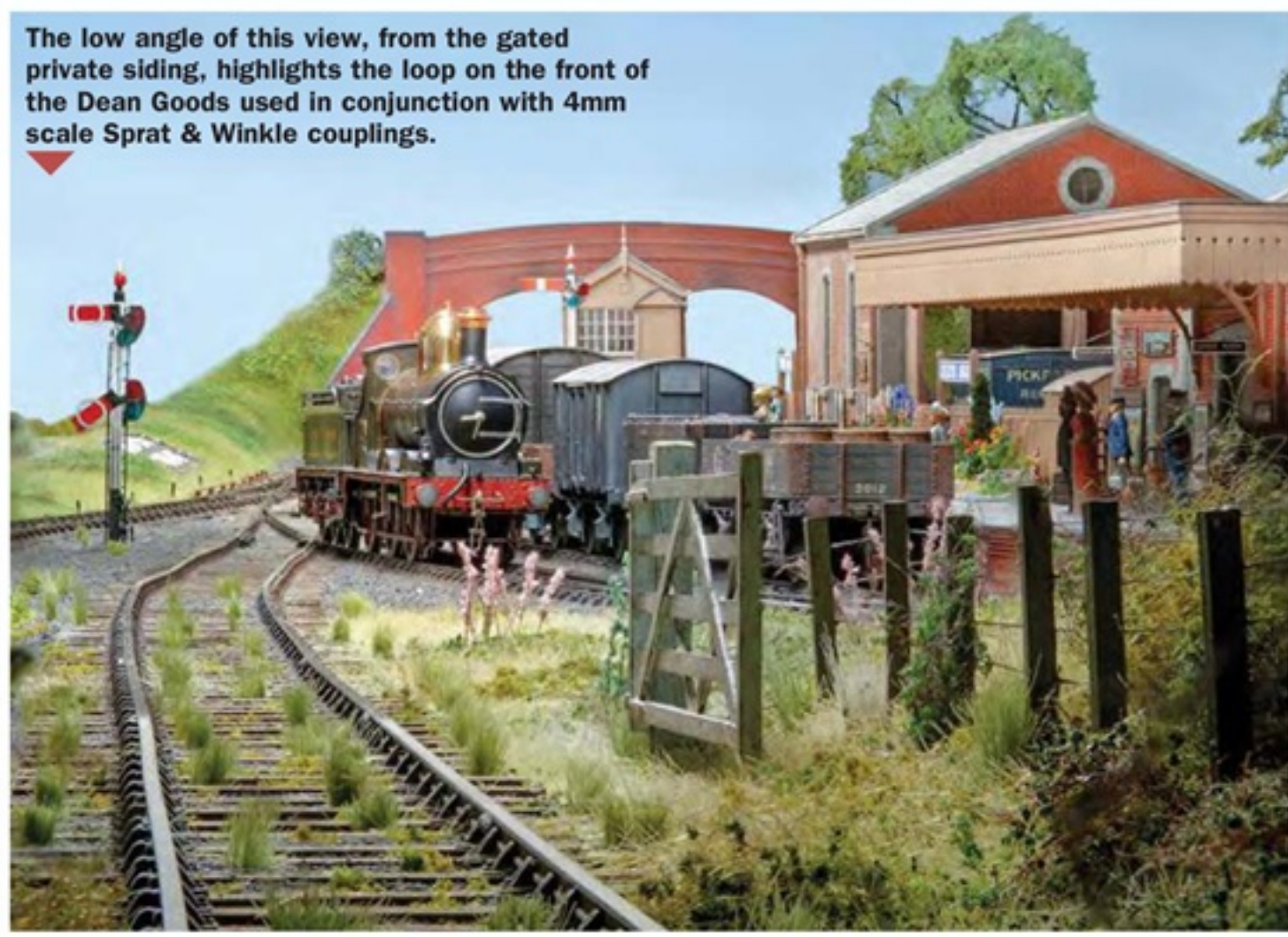
The fiddle yard takes the form of a train turntable, which minimises the need to lift and rerail models. Control is digital, via a Gaugemaster prodigy Advance system.

The goods shed was designed in a similar fashion to Clarke's output of station buildings; note the raised horizontal detailing.





The low angle of this view, from the gated private siding, highlights the loop on the front of the Dean Goods used in conjunction with 4mm scale Sprat & Winkle couplings.



Everything was allowed to dry and then static grass fibres (from the Green Scene range of materials) were applied to the landscape surface using a static grass applicator.

Hedgerows were produced by cutting rubberised horse hair into strips and then teasing the fibres apart to make a loose open structure. Foliage was once again from Green Scene, sprinkled on top of the horse hair and held in place by the application of acrylic matt varnish sprayed using an aerosol can.

The goods yard is served by a spur off the train turntable fiddle yard that is independent of the main running line to the platform.

Paler straw coloured fibres were then applied around the base of the hedge to simulate the longer grasses that usually grow in this area.

I find making convincing trees to be one of the more difficult aspects of scenic modelling; an elm tree 100' tall would measure out at 70cm in 7mm scale, which would have looked faintly ridiculous towering above the backscene on the layout! Some height compression was therefore needed, resulting in the largest trees on the layout measuring 35cm tall, which represent full-size 50' specimens.

The trees were made by twisting an armature from florists' wire and coating this in a mixture of PVA and Artex plaster to simulate the bark. Once painted, postiche theatrical hair was used to represent the fine twigs at the end of the main branches. More Green Scene flocks were used to represent the leaves, once again held in place with

matt aerosol varnish. All these techniques are explained in Gordon Gravett's excellent books on tree construction (published by Wild Swan), which I can't recommend highly enough for anyone looking to improve the trees on their layout.

### Buildings and structures

The buildings on the layout were scratchbuilt from styrene sheet and painted using Humbrol enamel paints. I used embossed brick sheets from South Eastern Finecast for the majority of the buildings, laminated onto layers of styrene sheet for strength.

William Clarke designed a number of small station buildings in the mid to late 19th century and I find his designs to be very aesthetically pleasing. Examples of his buildings can be found at Portesham, Abbotsbury and Clutton. The ornate chimneys and large platform canopy were characteristics that I wanted to capture in my model.

Plans for Abbotsbury station building were found in *Great Western Branch Line Termini – Volume Two* by Paul Karau. The dimensions on the drawing were scaled up to 7mm scale and the parts were cut from styrene sheet using these dimensions. The prototype building was constructed from stone, but I like the warm colours of red brick so chose to model my building in that finish.

As far as I know William Clarke didn't design any platform mounted goods sheds, but as I think they make interesting looking buildings I decided to design my own in the style of the aforementioned architect. Quite how successful I've been in my interpretation of his style is for others to judge, but I rather like it!

The signal box is the only structure that I didn't scratchbuild and started out as a Peco platform mounted signal box. The kit







▲ The Dean Goods prepares to depart with its remارشalled freight train. The model was constructed from a Martin Finney kit.

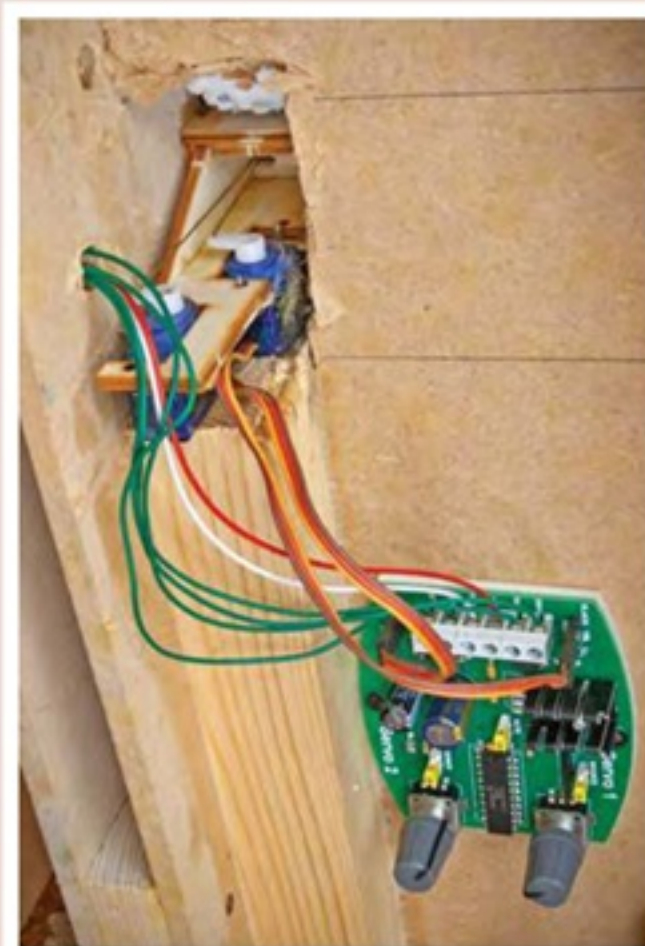


## Bouncing semaphores!

The signals were constructed using etched brass and whitemetal components from the Model Signal Engineering range of parts. I was keen to make them work properly; at a local show I had seen Derek Mundy demonstrate working signals that had a rather nice 'bounce' action to the semaphore arms when they were returned to danger. They were operated by a module from G F Controls and powered by miniature servos. I was really impressed with the setup so decided to use this method of operation for my signalling. The modules are fitted under the baseboards and allow adjustment of the signal arms to ensure that the arm is completely horizontal when set to danger.



▲ The signals are mounted on removable bases set into the baseboard, allowing transportation in secure packing off-layout.



▲ The servos are operated by G F Controls adjustable 'bounce' units. For more details see the website: [www.gfcontrols.co.uk](http://www.gfcontrols.co.uk)



In a nod to the location's Hardy heritage, the backscene includes a representation of Sherborne Abbey, half-hidden behind the hedgerow. Lady Jayne propels a couple of wagons away from the station...



...and having closed the gate behind him, the driver remounts and opens the regulator once more. A cassette awaits in reality.

## Conclusion

The layout has taken me about four years to build and I'm generally pleased with the way it's turned out. The deep baseboard width allows the track to 'sit' in a landscape and gives context to the station and its surroundings. Taking the layout to exhibitions has highlighted the fact that although I like the look of the increased base board width, I'm not nearly so enamoured when transporting it! I suppose the layout could best be described as 'transportable' rather than 'portable', but still feel the aesthetic benefits of deep boards outweigh any transportation issues.

## Forthcoming exhibitions

*Sherton Abbas* is due to appear at the Gauge O Guild's Guildex event in Telford on 31 August - 1 September 2019 and the Cardiff Model Railway Show over the weekend of 19 & 20 October 2019.

(ref.LK-709) provides a useful starting point for modification into a number of different styles; I added a brick plinth, stairs and changed the roof line.

## Locomotives & rolling stock

I've built all my locomotives from etched brass kits, which although time consuming I have found very satisfying. They all employ sprung compensation which helps keep all the wheels on the track aiding electrical pick up and smooth running. I recently converted the layout to DCC and have so far managed to fit South West Digital sound chip decoders and speakers to three of the locos.

The loco fleet comprises the following:

- GWR 517 Class No.539 - built from a Malcolm Mitchell kit;
- GWR Metro tank No.1500 - built from a Roxey Mouldings kit;

- GWR Dean Goods No.2467 - built from a Martin Finney kit;
- GWR '2021' class 0-6-0ST No.2112 - built from a Zero Zephyrs kit; and
- Manning Wardle F class 0-4-0ST No.719 *Lady Jayne* - built from a Slater's Plastikard kit.

All the locos run on Slater's wheels and were painted using my airbrush using Railmatch enamel paints.

The coaching stock and wagons were built from a mixture of etched brass and plastic kits from a variety of manufacturers. They represent 'typical' vehicles running on the Great Western Railway at the turn of the previous century. They are all fitted with Sprat & Winkle couplings, which although fiddly to construct allow remote coupling and uncoupling by the use of magnets fitted under the track.

The author at the controls: operation is from the front, and the layout is set at a comfortable height for long sessions.





# Invernevis

**DAVID GREENWOOD** presents his N gauge interpretation of a much loved and long lost West Highland Railway terminus.

Photography by Andrew Burnham

The spark that led to the building of this layout came while exhibiting my previous railway, *Poldeen*, also in N. That layout was set in Cornwall, in post 2010. My most-used loco at exhibitions was a Class 20, not only a favourite loco from my childhood in the 1980s, but, as a small Bo-Bo diesel, it ran well and looked appropriate for a small layout.

Primed for some 1980s nostalgia I mused about where else I might run a fleet of small diesels, with prototypically short and interesting trains, on a layout of small proportions.

It wasn't a big leap to choose a 1980s Scottish terminus... but with some altered history. I settled on the prototype of the old station at Fort William after discovering a range of old photos on the Disused Stations website [www.disused-stations.org.uk](http://www.disused-stations.org.uk). Little did I realise just what a warmly regarded and iconic station it is today, even though it was

razed to the ground in 1975. A new station for the town was built a mile or so inland away from the side of Loch Linnhe, so my model is very much a 'just supposing' depiction had the original survived into the 1980s.

The situation of the old station made it an interesting prototype, with the lack of run-round facilities leading to much shunting of the loco-hauled trains. Catering vehicles, goods wagons and parcels vans were routinely added or removed from services and it was not uncommon for all three platforms to be in use at the same time.

Conveniently Fort William also had the loco shed and goods yard a short distance from the station allowing locos and stock to be moved 'off scene' without contrivance. In

▲ There's little else that shouts 'West Highland in the 1980s' better than a large logo Class 37/4, and *Invernevis* features two.

steam days it wasn't unknown for goods trains bound for Mallaig to reverse back down to the station from the yard to clear the appropriate signal for departure.

▼ The original station was situated on the banks of Loch Linnhe, but replaced in 1975 by an out-of-town structure of less architectural merit.







▲ The station pilot marshals parcels stock. The classic BRUTE trolleys can be obtained as a fret of three from BH Enterprises.

### Flexible histories

The railway is very loosely set in a 10-year window centred around 1980, but you will still occasionally see a Class 37 in (c.1986) large-logo livery passing a Class 24/1 (all withdrawn by 1978). The main rule is that so long as it 'looks right' for the approximate period and wider location, it runs! I don't just stick to typical West Highland Line trains either, there are many other interesting services which plied other Scottish lines to Kyle of Lochalsh and the Far North Line; these also look the part.

### A little bit of help

I slightly shortened the station, but only a bit – the real platforms could accommodate six carriages whereas my model can manage five at a push. I also changed a few details and added a siding for extra interest.

▲ The layout is smartly presented with a lighting pelmet fitted with LEDs and a Velcro-attached curtain to hide the trestles etc.

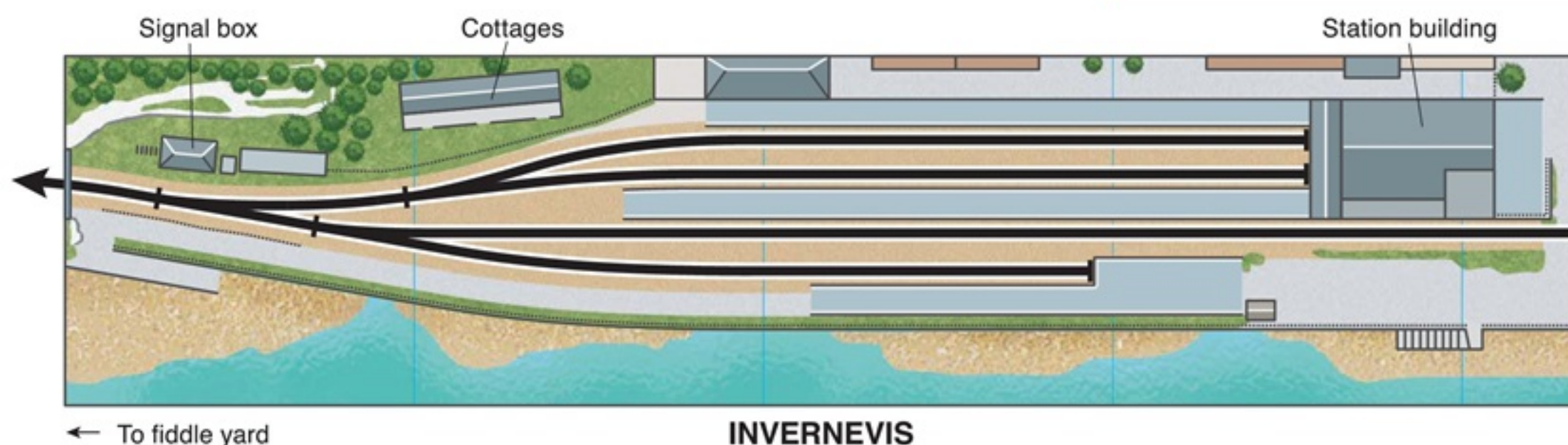
Like many modellers, I don't have access to professional power cutting tools or workshop facilities, my carpentry area being my back garden, in which I deploy an old Work-Mate™ folding bench. I was therefore concerned about whether I could make two baseboards which were sufficiently similar to achieve smooth and level tracks across the central joint. My solution was to make some enquiries with a local timber merchant and draw up a cutting list of the parts needed for my baseboards and emailing them through. 24 hours later I was able to pick up a nicely wrapped set of parts of 9mm ply, cut perfectly square and millimetre accurate – a bespoke baseboard kit!

I then glued, pinned and screwed the parts together, creating two flat, level and matching baseboards. These were painted on all sides, and underneath, to ensure the wood was sealed and looked smart. A pair of cabinet-maker's dowels give alignment and case-clips clamp the two boards together. All easy DIY work but with the added advantage of getting the wood cut professionally.

Lighting is provided by a strip of







Scenic: 130cm x 30cm. Each grid square 30cm x 30cm.

warm-white LEDs. The layout stands on four basic metal builder's trestles, giving a far sturdier and more level foundation than any set of legs I could have made from wood.

At exhibitions the legs are hidden by a curtain, which is simply a pair of navy blue bedsheets cut to size and held in place by iron-on Velcro.

An expert with a sewing machine I am not!

### Track and electrics

I had considered building my own track and points by using some of the new N gauge systems now on the market, but the challenges of devising point actuators, and

maintenance of the delicate rail profiles, caused me to reconsider and I opted for proprietary code 55 items and solenoid point motors. I have no real issues with the visual appearance of code 55 track once it is carefully ballasted and the platforms and scenery are in place.

A key aspect of the operation of this layout is a diode-matrix route selection system. This is an old-school analogue system which uses push buttons to select any of the 10 routes that can be selected (six in the fiddle yard and four on scene for the three platforms and the loading dock siding. If you want to drive a train from fiddle yard road No.2 to

platform No.1, you just need to press the two relevant buttons on the control box, and the various point motors change simultaneously to set the route.

*[Ed. – a diode matrix route selection system is described in our Shows You How Booklet No.5: Wiring the Layout Part 2. It is an analogue electrical system suitable for solenoid point actuators.]*

Control of the locomotives is also

**The West Highland frequently ran mixed trains, such as this arrival with a couple of tankers of fuel for the diesel depot. Approaching the Mk.I mini-buffet coach is the award-winning 24/1.**





extremely traditional, a Gaugemaster cased transformer beneath the layout provides power to a hand-held walkabout controller. Track section switches are installed to isolate locomotives in the time-honoured fashion. The transformer also provides power to the points via its auxiliary supply which is fed through a Capacitor Discharge Unit.

### Scenery and buildings

The station building was made from scratch using plain and embossed plasticard sheet and the scale drawings in Ian Futers' book *Scottish Layout Projects* (Santona Publications, ISBN 978-1-907094-19-4) as a guide. The proportions of the building were changed slightly in order to fit my site, but I am really pleased with the outcome. Other structures were made using parts from the Woodland Scenics DPM range of US-outline buildings. The cottages are a budget-price foreign kit, but they look OK now painted.

The rocky cutting and trees at the fiddle yard end is much more akin to the approaches to Oban or Kyle of Lochalsh than Fort William, but in the space available Fort William's buildings would just not fit. The mountainous backscene is a photographic scene from ID Backscenes: it is actually of Llanberis, but it passes for Ben Nevis well enough! The town areas are sections cut from Peco backscenes.

The water effect was created by painting the base a very murky green-brown then stippling on a gloss PVA called Mod Podge. This is a product intended for the *découpage* (posh *papier-mâché*/collaging) hobby. It cost

£3.99 for a large tub from a craft shop, and is a much less expensive product than the alternative options.

One minor detail which gets a lot of comment at exhibitions is the really fine telegraph wires between a number of the buildings. This is a product called EZ-line, marketed in this country for model aeroplane enthusiasts to make the fine wires between the wings of biplanes. It is very elastic and strong, while being only 0.3mm diameter, which means it doesn't snap when snagged. I use the light green colour, which looks good for oxidised copper when viewed close up.

### Rolling stock

Collecting, modifying and detailing the stock for this project has been a real pleasure, and furthermore, most items were purchased second-hand. All were renumbered and some were full resprays. Favourites include my Class 24/1; an extensive conversion from a Farish 24/0 for which I won a model making prize in an N Gauge Society competition. The full fleet includes;

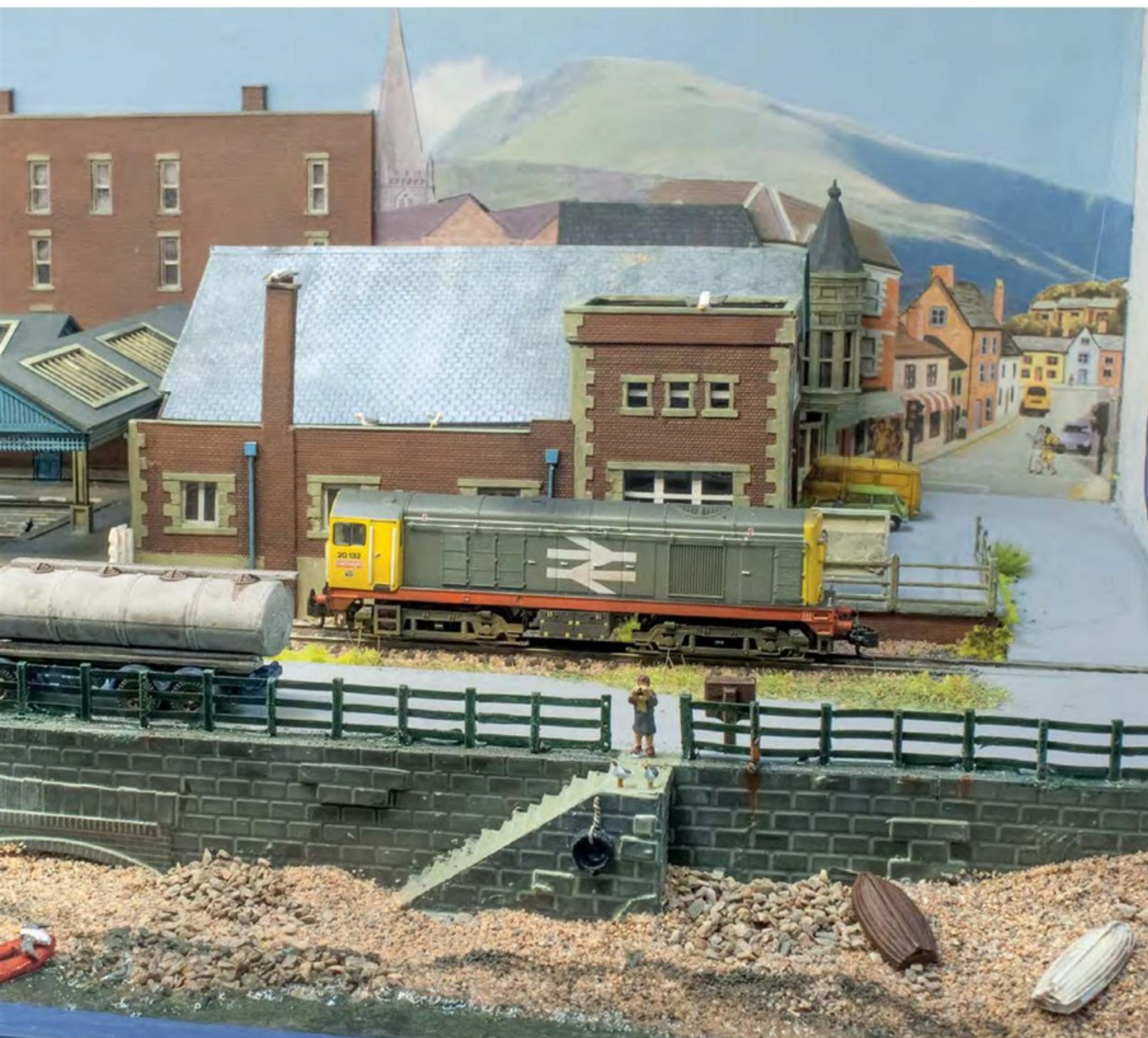
- 2x Class 20,
- Class 24/1,
- 3x Class 26,
- Class 37/0,
- 2x Class 47,
- Class 24/0,
- Class 25,
- 2x Class 27,
- 2x Class 37/4,

The station building was scratchbuilt with the aid of plans prepared by long-standing West Highland enthusiast Ian Futers. Other items were built from kits of varying provenance.

Although shortened slightly from scale length, the atmosphere of the prototype remains.







Coaches are formed into three-coach rakes, with Dapol fixed knuckle couplings between them giving extremely close coupling and gangway connections touching. Operational Dapol Easi-Shunt couplings are fitted to the end of each rake and on all locos. The coaches have been renumbered using an original 1980 BR Coaching Stock Pocket Book as a reference, and I added curtains using coloured paper – BR orange in the first class sections and white roller blinds in the sleepers. A few spare vehicles are set up for adding to services, these include a Restaurant Miniature Buffet (RMB), a converted BSO(T), and BG full brakes, as per prototype practice.

Goods stock is also formed into mini-rakes to minimise accidental uncoupling. There is only one permanent magnet on the layout, which is situated at the end of platform 1.

This was predominantly used as the main arrival platform (at the real Fort William) and is where, more-or-less, every arrival on the model will uncouple the loco. I decided against more permanent magnets to achieve full hands-free shunting because I would need so many of them, and it would have been virtually impossible to avoid accidental uncoupling. Uncoupling is instead achieved by hand using a Kadee Pick, an implement designed for unhooking knuckle couplings; after all, real locos are uncoupled by hand!

All stock is weathered in exactly the same way, again ensuring a degree of uniformity and making sure that the scene looks right. Every item of stock gets a thin wash of dirty coloured paint all over, followed by airbrushing Roof Dirt from the top down, then Frame Dirt from the bottom up. The process was simple but effective.

## Conclusion

I must at this point mention Chris' Crafts and Model Railways on Exeter Street in Plymouth – it's my local model shop – which was the source of the majority of parts for this layout: I wholeheartedly recommend supporting your local independent model shops if at all possible.

*Invernevis* has been my most satisfying model railway project by far. It is small enough to store easily at home, light enough to set up quickly to use with my children on a Sunday afternoon, yet detailed enough to engage visitors and operators for the duration of an exhibition. It even won 'best in show' on its second outing.

Indeed the project has worked out so well, I have found that my burning desire to build its replacement has become somewhat dampened...



Parbourne and Floyd Bridge revisited

# Striving for something better – part 2

Photography by Paul Bason or as credited

Continuing his account of how the older parts of the layout have been revisited and upgraded, **JERRY O'REILLY** describes how freight rolling stock has also been improved to provide greater fidelity to the GWR prototype. The first part of Jerry's article appeared in last month's issue.

The need was for greater authenticity of goods vehicles for the period between the early 1920s and the early 1930s, in which the model is set. Although the branch line is fictional, its steady development over the years has increased my interest in seeking greater fidelity to the GWR prototype. Realistically there are often limitations to what can be achieved, but nevertheless a great deal of pleasure has come from endeavouring to create a layout with convincing GWR character, through a

process of building a model which has its constituent parts based closely on the real thing. I find that researching the many aspects of the prototype can often provide as much satisfaction as the construction of the model itself, no doubt because the two processes are so interconnected.

Much of the freight stock which has been replaced comprised old generic R-T-R items, which I felt were no longer satisfactory. In contrast, the current standard of goods vehicles now routinely produced in N gauge by most manufacturers is greatly improved. With these models having 'raised the bar', it

was time to scrap the older wagons, and introduce new stock with greater authenticity to accompany the better proprietary items that were running on the layout.

At the same time, a growing interest in GWR 'Brown Vehicles' prompted the introduction of more examples of these types of non-passenger stock. It was also decided to replace a number of road vehicles with more appropriate models, better suited to the chosen period.

## Replacement goods wagons – selected types

The replacement wagons represent a range of workaday stock that would have been seen on a typical GWR rural branch line. Relatively uncommon specialist vehicles



A 45xx hauling a train of new kit-built freight vehicles and converted proprietary models. This section of baseboard dates back to the 1970s, with the halt platform at Floyd Bridge, the canal lock, the Crown Inn, and viaduct all surviving from that period. The two road bridges however are amongst the new replacement structures.



have therefore been avoided. The new goods vehicles are a variety of Private Owner wagons, railway company open wagons and vans, which include GWR, ex-LSWR and ex-MR types. Two additional cattle wagons have also been introduced. The new stock is mostly a mix of kits and conversions of proprietary items, as follows:

*Private Owner Wagons, seven-plank, RCH c.1908*

These are all Mathieson Models (1). Fortunately a number were purchased a while ago, as sadly they are now discontinued. They are accurately modelled with fine detail, including chassis with separately fitted metal brake levers and V hangers. Wagons bearing the names of mainly west country merchants have been acquired. Additional kit versions have been adapted to make GWR Opens.

*GWR 15'6" Opens, five- and seven-plank*

These GWR standard Opens are modified Mathieson seven-plank PO wagons, which are dimensionally quite similar. The seven-plank versions (2) have been made by simply repainting in GWR livery, and the five-plank versions are formed by cutting away the top two planks. Although this leaves the ends of the diagonal bracing disconnected from the corner frames, this can be disguised with tarpaulins (3).

*LMS and GWR five-plank Opens*

These longer Opens are similar in design to the Graham Farish five-plank PO wagons, which have been repainted in LMS and GWR liveries (4).

*SR eight-plank Open, 1925-32*

This is an N Gauge Society kit (5). Maunsell's design, based on LSWR types, has side hung 'cupboard' doors above the five-plank drop door. The earlier 9' w/b version is modelled using a Peco chassis kit.

*GWR Iron Mink (6), GWR Mink C, and ex-MR outside framed van (7)*

These vans are all N Gauge Society kits. The longer wheelbase Mink C has a Dapol 12' w/b mineral wagon chassis, which matches the prototype closely and runs reliably.

*Ex-LSWR outside framed vans*

There are three types, all with resin bodies on Peco chassis. Two are from the Gramodels range – an 8T ventilated meat van with sliding doors (8), and a 10T insulated van with diagonally braced side hung doors (9). The third is an 8T low roof van, originally produced for the N Gauge Society in the 1980s by R Snelling, and now refurbished. All are finished in Southern Railway livery.

*LMS Diagram 1661 cattle wagon, 1923-26*

An upgraded early Farish model, the body is dimensionally a little short, but nevertheless captures the look of the prototype (10). However, as originally manufactured, this is



▲ A mix of current freight stock. Behind the 57xx are a Peco GW ventilated van, a Mathieson Models PO wagon (1), and an ex-LSWR 8T ventilated meat van built with a Gramodels resin body on a Peco chassis (8). The sheeted GW five-plank Open is converted from a Mathieson Models PO wagon (3), and the SR eight-plank Open is an NGS kit (5). Photo by the author

spoilt by the over-scale double plastic bars at the upper openings, and over-deep eaves panels below the roof edges. To rectify this, the bars have been replaced with twin lengths of brass rod bonded to the outer faces, and the eaves panels reduced to the correct depth. Raised seams have been added to the roof.

*GWR Small Mex, Diagram W3 short cattle wagon, 1888*

This has a heavily modified Peco body (11). The vertical and diagonal bracing on the shortened sides has been cut away with a chisel blade, and replaced with repositioned diagonal bracing made with brass 'L' sections. The door type as produced by Peco is the later GW pattern, but a 1930s photo conveniently shows that these were

retrofitted to Small Mex No.38195. The Peco doors therefore need no modification if this particular vehicle is modelled. The over-scale plastic bars at the upper openings have been replaced with brass rod, and a flatter profile roof with raised seams has been added. The chassis is a shortened Peco 9' w/b kit.

**Brown Vehicles**

Like many GWR modellers, I have something of a fascination with Siphons and other types of non-passenger stock which are collectively known as 'Brown Vehicles'. Adding the creamery to the layout warranted an increase in milk traffic, and this in turn provided an incentive to build more Siphons to run on the line. Although the long 50' Siphons G and H for the express carriage of milk churns

Open cab pannier No.1506 hauls a train of new stock across the farm embankment. The GW Iron Mink is a NGS kit (6), and the ex-LSWR 10 ton insulated van has a Gramodels resin body (9). The GW Opens are adapted from PO wagons by Farish (4) and Mathieson (2). The Hodgkins PO wagon is also a Mathieson model (1), unmodified except for the added load of real coal. Photo by the author







▲ **Converted proprietary stock, headed by a 64xx pannier. The LMS cattle wagon is an upgraded early Graham Farish model with replacement bars at the upper openings and lowered eaves panels (10). The GW Small Mex has a shortened Peco body with repositioned diagonal bracing and a replacement lower profile roof (11). The LMS and GW Opens are modified proprietary PO wagons (4). The ex-MR van is an NGS kit (7). Photo by the author**

have been nicely modelled by Dapol and Lima, shorter wheelbase types seemed more appropriate for branch line operation, so the stock has been developed accordingly.

#### *Six-wheel Siphon 04, 1896-1902*

This six-wheel Siphon was built several years ago using a Mill Lane Sidings kit (12). The distinctive double running board chassis is made from two Peco 10' w/b brake van chassis spliced together, and an Ultima aluminium three-arc roof has been added. This Siphon has GWR character in abundance, and it inspired me to build more Brown Vehicle types.

#### *40' Siphon F, c.1907*

This Etched Pixels/Ultima model was my first attempt at assembling an etched brass rolling stock kit (13). The structural shell was conventionally soldered together, but my

limited soldering skills were not up to fixing the finely detailed outer body panels onto the inner shell. I therefore opted to use spray-mount adhesive. This sounds odd, but it actually worked well, resulting in a flat and tight lamination. The three-arc roof is detailed with ventilators made with brass 'top hat' bearings and rain strips made with Microstrip. Dapol auto-coach bogies have been fitted.

#### *Four-wheel Siphon C, c.1907*

The starting point for building a Siphon C was another 40' Siphon F, but this time one

▼ **Four 'Brown Vehicles', headed by a 64xx 0-6-0PT. Leading is the 40' Siphon F (13), an Etched Pixels brass kit, followed by the six-wheel Siphon 04 (12), a Mill Lane Sidings kit; the four-wheel Siphon C (14), running on an extended Peco chassis; and the Bloater fish van, converted from a Dapol Fruit D (15).**

with a plastic body, of unknown origin obtained second-hand. Siphons C and F had the same repeating modules of outside framed louvre panels and doors, so it was a simple matter to shorten the Siphon F to match a Siphon C, by cutting away one module from each side of the longer vehicle. The double running board four-wheel chassis is again by Peco, this time a 15' w/b brake van kit, extended to the required length by inserting a section from a second kit at the midpoint. An Ultima three-arc roof has once more been fitted (14).

#### *Bloater fish van, Diagram S8, c.1916*

The Bloater van as modelled (15) represents the prototype introduced by the GWR in 1916. It is a conversion of a Dapol Fruit D built by the GWR in 1939, which despite the 23-year age gap had matching dimensions to the Bloater. The later Fruit D differed visually however by having plain boarded doors, unlike the earlier Bloater's doors which had outside framing with diagonal bracing. The conversion was undertaken by cutting out the six pairs of doors in the Dapol body, and replacing them with diagonally braced Mink C doors from N Gauge Society kits. Other modifications include the addition of lower running boards, and adjustments to the positions of roof ventilators. The Bloater 'Fish Traffic Only' decals are from the NGS.

#### **Painting, lettering and weathering**

All kitbuilt and converted freight vehicles have been primed, then painted with either brush-applied Phoenix Precision enamels, or RailMatch aerosol spray enamels. Fox Transfers lettering has been used in most cases. Weathering has been applied to van roofs, and all stock is finished with RailMatch spray matt varnish. The printed graphics on several proprietary PO wagons have been toned down by rubbing over with a glass fibre pencil, which also gives a pleasing weathered appearance.

#### **GWR Road Vehicles**

The Great Western Railway's extensive fleet of road vehicles contributed significantly to







▲ The Creamery has provided an incentive to construct additional Siphons to run on the line. In this view, a six-wheel Siphon 04 is shunted into the covered loading bay with its consignment of milk churns. The 1922 Ford TT GWR lorry waiting on the bridge is a modified Fleetline kit (17).

the overall railway scene. A bus waiting in a station forecourt, a lorry bringing cattle to the dock for loading, or one laden with milk churns arriving at a creamery, all demonstrated how the railway company conducted its operations through the integration of road and rail transport. With this in mind, I felt it was time to replace the rather random mix of road vehicles that had been 'making do' on the layout, and introduce models which more accurately represented vehicle types that would have been in operation during the chosen period.

#### *Bedford WLB GWR bus, c.1931*

This is a P&D Marsh whitmetal kit. Added details include window glazing, a roof luggage rack with rear access ladder, registration plates from P&D Marsh, and an engine crank handle made with handrail wire. It is painted in GWR chocolate and cream livery, with Fox Transfers gold lettering.

#### *Thornycroft forward control GWR cattle wagon, c.1929*

Some of the rather chunky components of this Dornaplas kit have been replaced, including fitting thinner profile roofs made with 15thou plasticard. The front overhang details of the cab roof have been corrected and windscreen glazing has been added. Finishing touches again include registration plates and an engine crank handle. White 'G.W.R.' lettering and numbers are Fox Transfers (16).

#### *Ford TT flatbed GWR milk lorry, c.1922*

A Fleetline whitmetal kit, with minor modifications (17).

#### *GWR horse-drawn vehicles c.1910-25*

These include a parcels van and a light delivery van (18), both whitmetal kits by Fleetline. Their black canvas tilts have added advertising panels, and horses' reins are made with fuse wire. Lettering is again by Fox Transfers.

### What's next?

Improving the freight rolling stock, together with the replacement of older buildings and bridges, has given the overall layout a real lift. It has been a very worthwhile and satisfying process.

Although another stage in the history of this little branch line has now concluded, I am not sure yet what will follow. However, I am confident that something will come along to inspire more work to be done, as I'm definitely one of those modellers who enjoys refining and improving the layout as the years go by. One of the sayings so often applied to railway modelling is that 'a layout is never finished'. In the case of *Parbourne and Floyd Bridge*, this definitely holds true.

▲ A milk train with a four-wheel Siphon C arrives at Parbourne. The yard crane, a recent replacement, is a modified Fleetline kit mounted on a Ratio circular plinth clad in brick paper. In the background, a horse-drawn GWR delivery van (18) approaches the newly replaced station building at the end of the platform.



▲ A 55xx Small Prairie brings a Bloater van, followed by a four-wheel Siphon C, into Parbourne Station. The Small Mex cattle wagon in the foreground has a shortened and heavily modified Peco body. The 1929 Thornycroft cattle lorry is a Dornaplas kit with replacement roofs and added detail (16).

### Useful references

*A Pictorial Record of Great Western Wagons* by J H Russell (Oxford Publishing Co)

*Great Western Wagons Plans* by J H Russell (Oxford Publishing Co)

*GWR Goods Wagons*, by A G Atkins, W Beard and R Turret (OPC, ISBN 978-0-86093-657-2)

*G W Siphons* by J N Slinn and B K Clarke (HMRS, ISBN 978 0 902835 10 8)

*Road Vehicles of the Great Western Railway*, by Philip J Kelley (Oxford Publishing Co)



Mix & match in N gauge...

# BR Siphon variations

*The long-lived Great Western prototypes have been matched in model form by the similarly long-lived Lima model (introduced 1980).*

**RICHARD BARDSLEY** combined old Lima and newer Dapol models to create a variety of replicas for his BR blue period layout.

*Photography by the author*

The Siphon G bogie milk van was the longest running of all the varied designs developed by the GWR for the carriage of milk churns. Given this longevity, it's unsurprising that there were numerous changes in terms of external appearance as well as bogies and equipment (for internal lighting). Four diagrams were issued over several decades and it is perhaps remarkable that British Railways was still building them between 1951 and 1955 (diagram O.62) largely unaltered from the final GWR design yet nearly 10 years after nationalisation.

## Design chronology

The obvious external differences make it easy for the modeller to identify each diagram, although there were variations, notably the bogies used. Chronologically, the four diagrams were as follows.

- O.11** outside-framed, horizontal planks
- O.22** inside-framed, horizontal planks
- O.33** inside-framed, vertical planks
- O.62** inside-framed, vertical planks, lower bodyside ventilators.

The earliest diagram O.11 vans were introduced with multi-bar trussing (queen posts with wire), thereafter, angle-iron trussing



was used. This change in the underframe also mirrored the move from gas to electric for the internal lighting whereby gas cylinders were replaced by battery boxes and dynamos.

Some design features were consistent across all four diagrams: full length louvre vents at the top of the body; tri-arc roof; a 50' body length; four double doors per side; and gangway connectors.

A notable departure was the introduction by BR on diagram O.62 of vents with sliding covers on the lower part of the body.

The 9' heavy steel bogie was usually fitted under most if not all of the diagrams, though other GWR designs such as the 9' American bogie were also used. Bogie swaps probably happened, so modellers need to do their research if modelling a particular van at a particular point in time.

Some of the diagram O.33 vans lasted into the British Rail era, and long enough to receive rail blue livery. Unsurprisingly, as the newest (relatively speaking), the diagram O.62 vans lasted the longest, though by the 1970s they

▲ A work-stained 37 251 brings three rail blue siphons into the station, loaded with newspapers.

were largely employed as parcels or newspaper vans. Most were withdrawn by the late 1970s though many of both these diagrams survived even longer as stores vans or as Enparts (Engine-Parts) vans, the latter sometimes being worthy of a repaint into olive green.

## Modelling possibilities

Most N gauge modellers will be aware of the Lima and Dapol Siphon Gs; there's still a ready supply of the former on eBay and at swapmeets, while the latter is still in production. The two models are at almost opposite ends of the Siphon G timeline. The Dapol model represents a diagram O.22 van though the chassis has the angle-iron trussing but gas cylinders for internal lighting. Lima's model is a diagram O.33 van.

Railway companies sometimes made minor changes over time to the overall dimensions of a design, but despite the changes to the outward appearance, the GWR (and British Railways) did

not see any need to change the basic length, width and height at 50', 8'6" and 7'9" respectively. The Dapol model is virtually 100% accurate at 103.1mm x 17.5mm x 16mm. What surprised me after all these years was that the Lima model is pretty close too, at 102.3mm x 17.5mm x 16mm.

As I had examples of both the Lima and Dapol models I wondered if the generally acceptable Lima body could be mated to the superior Dapol chassis. I was also interested in producing diagram O.62 vans, as the all-important sliding vents were available from Etched Pixels. You can imagine how surprised I was when the Lima body fitted to the Dapol chassis almost perfectly. It looked like this was going to be an easy project; as we shall see, I could not have been more mistaken.

## Project specification

I had three Dapol vans going spare (one was actually a Siphon H, but the chassis is exactly the



same as the Siphon G model), and a Lima van. Two further Lima vans were purchased on eBay as a pair. The Dapol models have been in continuous production for over a decade so they are easy enough to find second-hand if you want a donor chassis, but best of all, Dapol now sells the chassis on its own.

### Some disassembly required

The Dapol model is quite easy to disassemble. First remove the bogies; pinch the chassis between thumb and forefinger, then pull the bogie off the chassis. Do this carefully as it requires moderate force; I used the thumb and forefinger on the chassis to sort of lever off the bogie while pulling it with the other hand. This reveals a crosshead screw at each end which when unscrewed, allows the body to be detached. Some of the older models have a steel weight atop the chassis which can be removed if necessary.

The Lima model requires a thin chisel blade between the body and the chassis to lift it off at one end. Once it's off, the gangways can be removed by inserting the blade of a small screwdriver between the internal bodyside and the clip on the gangway; they are worth removing as it makes repainting (if required) a lot easier. Lever the clip inwards while pulling the gangway off; it's a tricky operation, although if you are not refitting the gangways you can just break the clips off. If necessary, use a screwdriver on the clips inside that hold the roof on; it's not vital to remove it, though it's handy if you need to strip the model. See photo 1.

### Gangways or not?

A common feature of the downgrading of stock to mere parcels use was the removal of unnecessary equipment to simplify the maintenance requirements. One of the most visual changes was the removal of gangways, and many Siphon Gs were treated in this way in later life. By the mid-1970s it appears as though few if any of the diagram O.33 vans retained gangways, while for the later diagram O.62 it was a bit more varied; they tended to retain gangways for certain duties, notably newspaper traffic (photo 2).

I decided to do my diagram



O.33 van without gangways, and for the other two which were going to be diagram O.62, I went for one with and one without gangways.

Once gangways were removed, the door was plated over and this is easy to represent on the model with 10thou plasticard. On the Lima model there is an indented area where the gangway was, and the plasticard can be shaped to this area, thus giving an almost flush finish (photo 3).

### Body swap

I'd been extremely surprised how close a fit the Lima body was onto the Dapol chassis, but it's not quite a perfect fit. At the first trial fitting it was noticed that the chassis bowed downwards from the body in the middle (photo 4). I used an emery board to file down the tabs at the end (photo 5). There's also a slight edge at the end which is worth filing as well. I test fitted the body again, repeating the process until the

chassis did not bow, but there was sufficient interference fit to just about hold the body to the chassis.

I wanted the ability to remove the body in the future if necessary for some reason. The screws into the Dapol body went into moulded pieces inside the body which were obviously not there on the Lima body. I made my own out of scraps of plasticard (photo 6). With these in place, I fitted the chassis and drilled a







pilot hole through the chassis into them which would accept the screws (photo 7).

### Buffers

The Lima chassis employed oval buffers while the later Siphons used round ones. I had some turned brass buffers with securing spigots; they are probably meant for wagons as the heads are not quite big enough, but they were to hand and I am impatient.

Removing the old buffers was easy with a sharp knife. Adding the replacements was not as easy. The spigot on the turned brass buffers was 0.75mm in diameter while the depth of the headstock was a little over 1mm. I'm not a good enough modeller to drill 0.75mm holes dead level in exactly the right place on the headstock. My solution was to cut slots into the headstock in the right place (actually aligned with the plates left from having cut off the Dapol buffers, photo 8). Next I used a craft knife to enlarge the slot slightly to be wider than the width of the saw blade. Finally, I used a 0.75mm drill held in a pin vice as a kind of file to open out the slot to the required 0.75mm (photo 9). The brass buffers were then glued into the slots with Araldite, taking care to make sure that they were level.

### Underframe

I imagined that all that would be required would be to remove the gas cylinders and add battery boxes, retaining the moulded vacuum cylinders, but herein was the start of my troubles.

The gas cylinders are quite a thick moulding, too difficult to just cut off with a knife, especially as they are a little inaccessible between the relatively delicate moulding of the trussing. I drilled several holes from above the chassis into the cylinders and then used a slitting disc in a rota-

ry tool to cut and grind away the moulding. I fitted a piece of 20thou plasticard over the resulting gaps to make a nice base for the replacement underframe equipment. (photo 10 shows the removal sequence from top left to bottom right).

I had intended to leave the moulded vacuum cylinders in place. There are two side-by-side at one end of the chassis; however, this is only appropriate for diagram O.33 as with the later diagram O.62 the cylinders are placed diagonally at opposite ends. Further study of photographs revealed that even for diagram O.33, the cylinders are not quite in the right place in relation to the battery boxes that would be fitted. So out came the rotary tool once more, and the cylinders were removed. The replacements were made from some 4mm plastic rod with a piece of bent 0.5mm brass wire for the actuating lever.

Not surprisingly, the V-hangers are also not right for diagram O.62, indeed, even for the O.33 they were wrong because the vacuum cylinders had been moved. So off they came using a fine saw where they meet the solebar and a sharp knife where they meet the trussing. Replacements were fashioned from 0.5mm brass rod. Before gluing them on I ran a file over the face of the V, which helped to flatten the roundness of the wire to something more like the metal strip that would have been used on the prototype.

The diagram O.62 vans used a long lever brake rather than the short one moulded on the Dapol chassis. Even then there are photographs of diagram O.33 with long levers, but I decided that making four new brake levers was enough, so for my O.33 it has retained the short lever. The new levers and the securing ratchet were again made from 0.5mm



wire, flattened slightly with a file.

The remaining chassis details (short and long battery boxes, and dynamo) are whitmetal castings from P&D Marsh. I found when I offered these up to the chassis that they hung slightly too low – that piece of plasticard I'd added as a smooth surface was causing a problem, so off it came (a difficult job since copious amounts of Araldite had made a very good bond indeed). I still had to file some off the tops of the battery box castings to get them right.

### Something's not quite right...

At last all the chassis were complete, so I temporarily refitted the bogies to see how it all looked. You can imagine the consternation that ensued when the models were placed on the track and the battery boxes were virtually dragging along the tops of the rails. Something was not right.

I returned once again to the reference material but the position of the battery boxes in relation to the truss rods looked to be just right. Eventually it dawned on me that Dapol has used wagon wheels on the Siphon model but they should be coach wheels. The difference is only a couple of

millimetres but in N gauge, that's a big difference.

I tried both Parkside and Graham Farish coach wheelsets but neither alternative would fit without opening up the cups on the bogies that take the pinpoints significantly. The Parkside axles are the same diameter as the Dapol ones, so one option would be to swap the wheels over. For this you need a wheel-puller and a gauge to check the back-to-backs. With 24 wheels to do for three models, I decided that this was a step too far.

It still left the problem of the low-hanging battery boxes. I removed them from the chassis and filed even more off the tops to raise them up a bit. It's a compromise, and they still look too low when the models are on the track. Alternatively, I could have scratchbuilt the battery boxes, and thus made them a proportionally better fit.

### Adding ventilation

On the Lima body there are some L-brackets by the door frames that should not be there for diagram O.62, but with eight per side, I decided it would be quite tricky to get them all off as neatly as possible. They do, however, provide a handy reference for





adding the vents. It was while counting planks on the diagram in the HMRS book to see where the vents needed to go that I discovered that the Lima planking is incorrect; it is wider and therefore there are not as many of them as they should be.

The Etched Pixels vents are made from etched brass. There's enough for three vans and some of the vents are in the open position; looking at photographs, it seems that it was quite common for one or two vents to be randomly left in the open position. These etches are quite fragile where the runners are at the top, as this is very thin brass indeed, so all too easy to bend it if you're not careful. I cut the etches out on a hard surface to minimise bending (don't use a rubber cutting mat or the opposite will happen). Where the odd one got a little bent I was able to straighten it by squeezing it in the jaws of a pair of pliers.

I used tiny blobs of Araldite behind the vent cover to fix them in place; don't try to glue the runners as they are just too thin. Once the glue had hardened, I straightened any misaligned runners with a pair of tweezers. Then I used a fine paintbrush to apply some Johnson's Klear (some thinned gloss varnish would probably also work) around the edges of the vents and along the runners. This dries very quickly and effectively seals the thin etched runners onto the van body.

There is a moulded grab handle at each end on the side, but because the planking is incorrectly spaced it is not quite in the right position and therefore it interferes with where the vents should go. These handles were still extant even by the late 1970s.

In order to fit the vents, there was no option but to remove the handles so they were carved off with a sharp chisel blade. Results were mixed across the eight corners on the two diagram O.62 vans as it's quite hard to remove moulded detail that sits over planking. The solution was quite simple – the less than perfect corners are covered by an open vent, so there's a greater proportion of open vents at the corners (photo 11).

### Painting and finishing

Before painting, the chassis and bodies were placed into an ultrasonic cleaner to 'shake' off dirt

and grime, so a couple of minutes in there gives a nice clean model. The parts were left to dry under cover for 24 hours before receiving a coat of Halfords grey car body primer (photo 12).

The top coat is Railmatch BR Blue. Despite having wooden sides, the blue seems to have lasted well on the actual vans; perhaps the BR paint was less prone to fading when on wood rather than steel. Therefore, the blue was applied without too much fading or scuffing.

Transfers are from Railtec. Frustratingly, the firm produces a sheet to cover these vans in OO but not N, so I bought two separate sheets covering Western Region vans and 'newspaper' branding. The van numbers are incorrect, but unless you get very close it is impossible to tell in this scale (photo 13).

### Conclusion

This project was supposed to be completed in a few evenings, but it dragged on for a few weeks as I found more and more things that needed to be done. Therein lies the lesson, that it's worth doing more thorough research upfront rather than figuring it out as you go along (though that's sometimes half the fun).

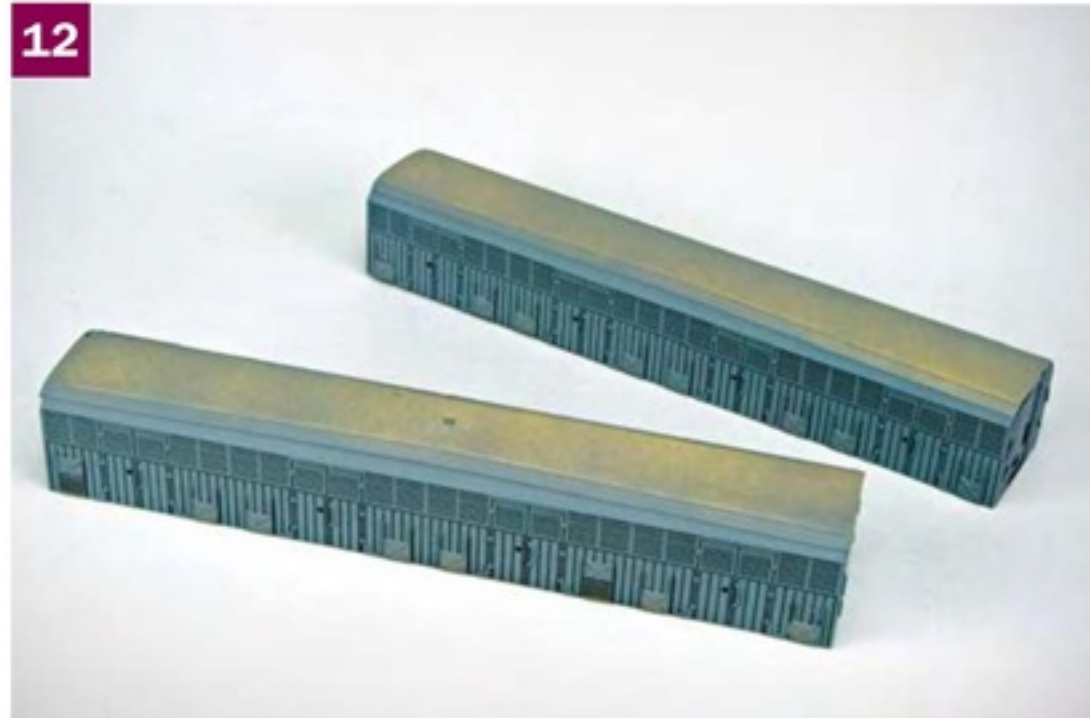
Was it worth using the Dapol chassis as a replacement for the Lima one? I think so, as despite having to replace so many details on it, the basic chassis looks fine and above all, it's a good runner. The compromise on the wheels was a disappointment but the old adage that most models are viewed from above means that these shortcomings are easily overlooked.

Even just a straight chassis swap would result in a better model without the need to go the

11



12



extra mile on buffers and under-frame equipment. It's really up to each modeller how far they take it, and what compromises you're personally prepared to put up with. Overall, it's been an interesting project. Would I do it again? Well, there's all those Enparts vans still to do...

### References

*GW Siphons* by Jack N Slinn & Bernard K Clarke pages 85-89 (HMRS ISBN 978 0 902835 10 8)  
*BR parcels and passenger-rated stock vol 1* by David Larkin

pages 46-47 (Kestrel Railway Books ISBN 978-1-905505-33-3)  
*BR parcels and passenger-rated stock vol 2* by David Larkin page 65 (Kestrel Railway Books ISBN 978-1-905505-34-0)

*Great Western Coaches Part II* by J H Russell pages 180, 229, 262 (OPC ISBN 0 902888 04 8)

*Great Western Coaches Appendix vol 2 (Specific Duty Coaches and the Brown Vehicles)* by J H Russell page 233 (OPC ISBN 0 86093 154 4)

<http://paulbartlett.zenfolio.com/paulbartlettsrailwaywagons>

13





# Talking Points

Topical issues from the world of model railways

## Locomotive tractive effort – how is it calculated?

Peter Osborne delves into how model locomotives' haulage capabilities can be measured and assessed, and presents some useful recommendations when planning your layout.

*Photography as credited*

Along with other train-spotters of my generation I avidly took note of the tractive effort figures given in my Ian Allan Combined Edition, the loco-spotters' 'Bible'. I have often pondered how does this translate into model form and would it be possible to quantify the pulling power of our models? Surely this would be better than stating a specific loco would haul X number of coaches around Y curve or up Z gradient?

So... I decided to carry out an experiment. Some may say with justification that I should get out more!

### Locomotive performance

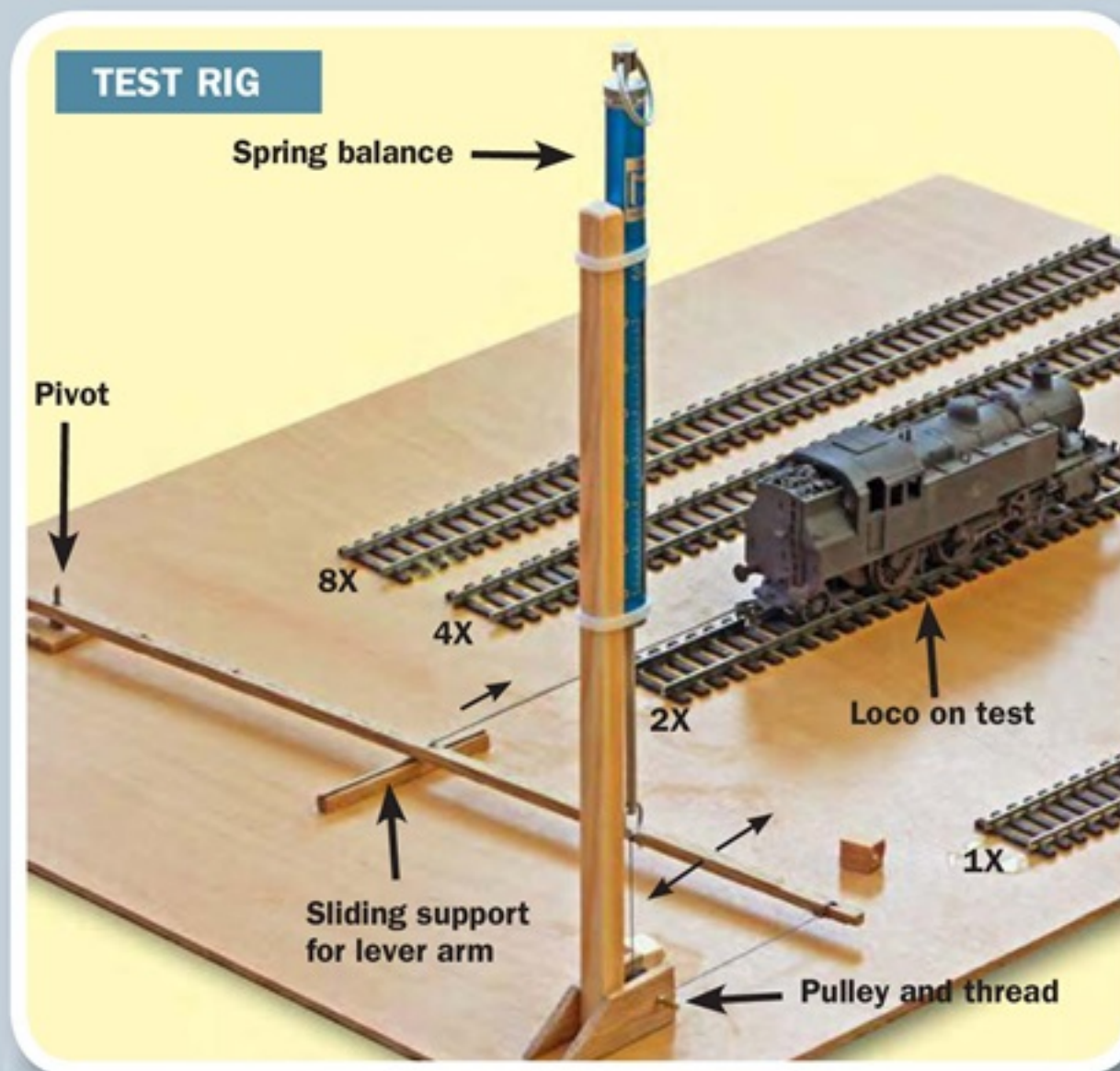
Firstly we must briefly consider the forces at play. Tractive effort for a steam loco is a theoretical figure based upon drawbar 'pull' (horizontally) at 85% steam pressure at the cylinders, measured in pounds. Typically this figure may be below 20,000 lbs for a small tank engine and around 40,000 lbs (18 tons) for a Duchess or GW King. Don't ask me how the equivalent is calculated for diesel and electric locos, or how one accounts for the wrong type of snow or leaves on the line!

It would be impossible to scale friction as the full-sized rail head scaled down would be polished to an optical quality! We can however take a brief look at the forces at work. Friction is either static (two surfaces in contact) or dynamic (one surface sliding over another). Dynamic friction is always a lower figure; when a loco's wheels start to slip some tractive effort is lost. In the full-size railway static friction is always the dominant force as the cone-shape of the wheel tread 'settles' onto the rail and this compensates for the fact that on curved track the outer rail is longer than the inner rail.

(Incidentally, Frank Hornby had this phenomenon sorted as the wheels of early O gauge trains could rotate individually about the axle, necessary for relatively weak clockwork motors on very tight curves.)

In model form our curves are tighter than the full-sized railway so we can assume that at least one wheel per axle is always slipping. For this reason I have used dynamic friction (simply, wheels spinning) for my tests. The speed at which the wheels are slipping makes negligible difference to the figures as does code 75 or code 100 rail.

The test rig is shown in the photo. This uses a 0-30g spring balance with a piece of fine thread run around a pulley to give a horizontal movement. A horizontally pivoted bar provides mechanical moments



▲ The author's test rig, showing a vertically-mounted spring balance and horizontal lever arm. An Ivatt 2-6-2T is the guinea pig.

of 2x, 4x and 8x (0-60; 0-120; and 0-240 grams force or gmf) for the more powerful locos measurements; most were in the 0-60 gmf range. I just knew my O level physics would come in handy one day; some 50 years later! This allowed my 0-30 gram spring balance to be used to measure greater forces. While not 100% accurate this set-up would be good enough to enable comparisons of locomotive performance.

A table sets out the test results of my stud of locos. This gives the loco class and official prototype power classification; the loco running number as in 1957-1960; the mass of the model in grams (ignoring leading and trailing wheelsets) and the crucial tractive effort as measured in grams force taken from the tests. The prototype mass in imperial tons and tractive effort in pounds is given for comparison.

Table 1 throws up some interesting anomalies. Whilst it would be expected that kitbuilt models with heavy whitmetal bodies would have a higher tractive effort it appears this is offset by the smoother tread of Romford driving wheels used on the Comet chassis; compare the two V2 models in the table. Smoothness of wheel treads also varies; dirty wheels grip the rails better! Further, the haulage capacity in no way reflects the capabilities of the prototype; the humble Jinty equals the Robinson O4 and beats the Raven Q6 hands down; surely this a point for our manufacturers to note?

### Haulage requirement

Now we can look at the requirements of how much of that valuable tractive effort do we need to perform what function, comfortably, without slipping.

I used Bachmann goods vans and BR Mk.I coaches as a standard for my measurements.

On curved track moving a load will require greater effort due to increased friction firstly of one wheel per axle dragging due to the inner rail being shorter than the outer rail and secondly due to the flanges bearing against rail as side forces come into play.

Rather than try to set up a spring balance between a moving OO gauge loco and its train I related the gmf tractive effort as tested to what that loco would actually pull in tests. A 90° 600mm radius curve and test 1:100; 1:75, 1:50 and 1:30 gradients were also introduced. I then distilled these figures into a gmf value per wagon and per coach.



**Table 1 – tractive effort test results chart**

| Locomotive                 | Power class | No.   | Mass (g) | TE (gmf) | Actual mass (T) | Actual TE (lbs) | Notes                                     |
|----------------------------|-------------|-------|----------|----------|-----------------|-----------------|-------------------------------------------|
| Bachmann Peak 1Co-Co1      | Type 4      | D193  | 655      | 76       | 138             | 70,000          |                                           |
| Bachmann EE 1Co-Co1        | Type 4      | D325  | 608      | 84       | 133             | 52,000          |                                           |
| Bachmann Drewry 0-6-0      | –           | D2280 | 240      | 48       | 30              | 16,850          |                                           |
| Bachmann 08 0-6-0          | –           | D3336 | 280      | 38       | 47              | 35,000          |                                           |
| Bachmann Bo-Bo             | Type 2      | D5211 | 499      | 68       | 75              | 45,000          |                                           |
| Bachmann Ivatt 2-6-2T      | 2MT         | 41202 | 339      | 40       | 63              | 17,410          |                                           |
| Bachmann Ivatt 2-6-2T      | 2MT         | 41272 | 345      | 36       | 63              | 17,410          |                                           |
| Bachmann Horwich 2-6-0     | 5MT         | 42789 | 245      | 38       | 66              | 26,580          |                                           |
| Bachmann Ivatt 2-6-0       | 4MT         | 43106 | 270      | 54       | 59              | 24,170          |                                           |
| Kitbuilt 4F 0-6-0          | 4F          | 44570 | 263      | 28       | 49              | 24,555          | Whitemetal, Comet chassis, Romford wheels |
| Hornby Black Five 4-6-0    | 5MT         | 44742 | 342      | 108      | 74              | 25,455          | Weighted tender drive, traction tyres     |
| Kitbuilt Jubilee 4-6-0     | 6P5F        | 45675 | 435      | 72       | 79              | 26,610          | Whitemetal, Comet chassis, Romford wheels |
| Bachmann Jubilee 4-6-0     | 6P5F        | 45715 | 313      | 54       | 79              | 26,610          |                                           |
| Bachmann Jinty 0-6-0T      | 3F          | 47410 | 236      | 38       | 49              | 20,835          |                                           |
| Bachmann Jinty 0-6-0T      | 3F          | 47412 | 236      | 38       | 49              | 20,835          | Renumbered                                |
| Heljan Garratt 2-6-0+0-6-2 | –           | 47975 | 726      | 112      | 155             | 45,620          |                                           |
| Kitbuilt Johnson 0-6-0     | 2F          | 58246 | 203      | 36       | 38              | 19,420          |                                           |
| Kitbuilt V2 2-6-2          | 7P6F        | 60852 | 518      | 44       | 93              | 33,730          | Whitemetal, Comet chassis, Romford wheels |
| Bachmann V2 2-6-2          | 7P6F        | 60903 | 370      | 76       | 93              | 33,730          | Extra weight added                        |
| Bachmann B1 4-6-0          | 5MT         | 61190 | 303      | 58       | 71              | 26,680          |                                           |
| Bachmann K3 2-6-0          | 5P6F        | 61949 | 360      | 52       | 72              | 30,030          |                                           |
| Hornby Q6 0-8-0            | 8F          | 63429 | 225      | 28       | 66              | 28,880          |                                           |
| Bachmann O4/1 2-8-0        | 8F          | 63601 | 260      | 38       | 73              | 35,520          |                                           |
| Bachmann J39/2 0-6-0       | 4P5F        | 64970 | 257      | 24       | 52              | 25,665          |                                           |
| Bachmann V1 2-6-2T         | 3MT         | 67666 | 340      | 44       | 84              | 22,465          | Extra weight added                        |
| Bachmann J72 0-6-0T        | 2F          | 69025 | 242      | 36       | 39              | 16,760          |                                           |
| Bachmann BR5 4-6-0         | 5MT         | 73069 | 290      | 32       | 76              | 26,120          |                                           |
| Bachmann BR4 2-6-4T        | 4MT         | 80002 | 327      | 38       | 88              | 25,100          |                                           |
| Bachmann WD 2-8-0          | 8F          | 90445 | 300      | 58       | 70              | 34,215          |                                           |

**Table 2 – tractive effort required, grams force per vehicle**

|       | Straight level | 600mm 90° curve | 1:100 | 1:75 | 1:50 | 1:30 |
|-------|----------------|-----------------|-------|------|------|------|
| Wagon | 1.15           | 1.26            | 1.52  | 2.11 | 2.71 | 3.8  |
| Coach | 4.7            | 5.16            | 6.33  | 7.6  | 9.5  | 12.6 |

These are presented in Table 2; I will spare you the arithmetic!

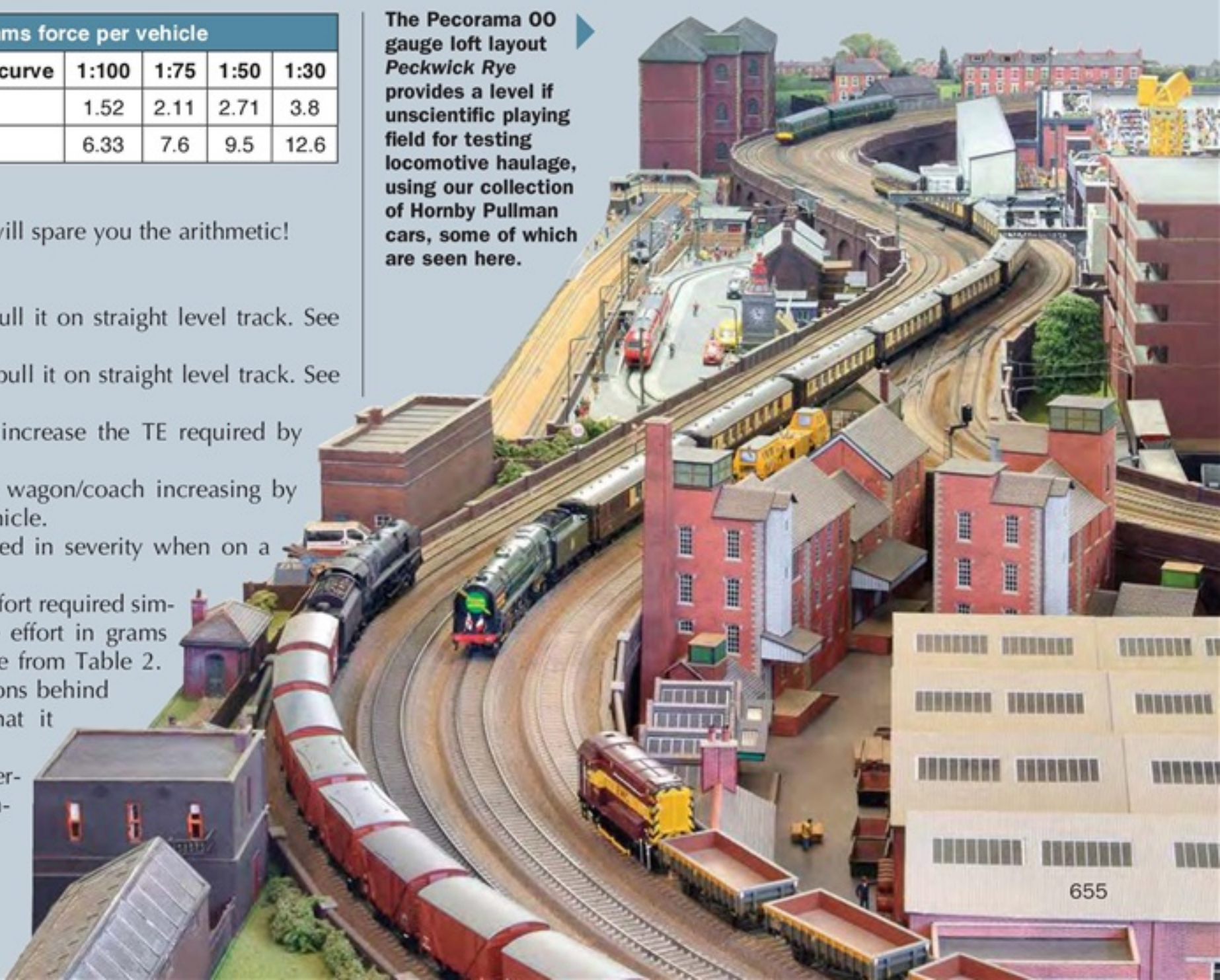
## Conclusions

- Each coach requires 4.7 gmf to pull it on straight level track. See Table 2 for more results.
- Each wagon requires 1.15 gmf to pull it on straight level track. See Table 2 for more results.
- Curves (say 90°, 600mm radius) increase the TE required by 10%.
- Longer trains may require TE per wagon/coach increasing by 10% due to increased drag per vehicle.
- Gradients should be roughly halved in severity when on a curve.

To work out the haulage tractive effort required simply divide the loco's actual tractive effort in grams force (gmf) by the requirement figure from Table 2. And, yes, I did once have 100 wagons behind my Heljan Garratt just to see what it would pull!

Hopefully this will provide an interesting insight into my attempt to quantify model loco performance.

The Pecorama 00 gauge loft layout Peckwick Rye provides a level if unscientific playing field for testing locomotive haulage, using our collection of Hornby Pullman cars, some of which are seen here.





# Woodbury Wharf

Modelling and words: **RICHARD HOLDER** Photography: **ANDREW BURNHAM**

**W**oodbury Wharf started as a small triangular layout built as an entry in the Dave Brewer Challenge at Expo Narrow Gauge at Swanley, Kent, in October 2013. It was originally called *Windrush Wharf* after the River Windrush in Oxfordshire. The task was to create a working narrow gauge layout on a triangular baseboard with sides in the ratio of 30:40:45 and a maximum size of 54cm x 72cm x 81cm. Working on a triangular base not much bigger in area than a cafeteria tray was not easy!

These dimensions were chosen because, in 2013, three anniversaries were celebrated: 30 years of Expo Narrow Gauge, 40 years of Meridian Models and 45 years of the Greenwich & District Narrow Gauge Railway Society. No external fiddle yard was permitted. Everything had to be contained within the overall dimensions, apart from transformer, controller and any electrical switches.

The whole of the layout in one photograph: the railway photographer is on the left-hand bridge, whilst the low relief warehouses, served by rail and canal, are at the right.



▲ 0-4-0WT *Lucy* began life as a Bachmann (USA) *Rheneas*, with face and other Awdry characteristics replaced with scale fittings.

The original canal-side scene included just one loco and three coal wagons. This short train was originally hidden under a road over-bridge. The train would enter the scene, drop off one of the wagons outside the door of the warehouse, then continue to leave the other two wagons on a wharf siding, next to a moored canal barge. Greenwich Narrow Gauge couplings were used. Uncoupling was achieved using Gaugemaster electromagnets designed for N gauge and energised using push button switches.

The loco would then return to its starting place under the road bridge. After a pause the loco re-appeared on scene to collect the wagons and push them back under the road bridge and the sequence repeated.

Points were operated using SEEP point motors with trackwork by Peco. To say that the operational potential of the original little layout was limited, is an understatement! After appearing at ExpoNG in October 2013, *Windrush Wharf* appeared on one other occasion – The

Oxfordshire Narrow Gauge Modellers' Open Day in June 2014 before being left in a spare room, and almost forgotten.

## Windrush Wharf becomes Woodbury Wharf

Early in 2018 I dismantled my home NG layout to start a new one. Our three young grandsons were coming to visit and my wife and I realised that they would be so disappointed if there was nowhere to run Grandpa's trains. I retrieved *Windrush Wharf* and extended the original little layout to create a trapezium shaped baseboard. The new track plan included a continuous circuit so that the boys (and Grandpa!) had somewhere to play trains! The layout stayed in this form for several months. There was no scenery apart from features contained in the original triangular section, but that did not seem to worry the boys – the trains went round a simple loop arrangement – they had a controller to make them move – result happiness!

Each July, the South West Group of the OO9 Society puts on a little 'show within a show' at the large two-day Exeter exhibition. I had previously attended with my







▲ **0-6-0ST No.8 passes the canal traffic with a couple of loaded Bachmann ex-WDLR bogie wagons.**

*Wantage* layout (See RAILWAY MODELLER September 2013) and in 2017 I took *Launceston* (see RAILWAY MODELLER May 2017). In spring 2018, Geoff Bowyer, con-

venor the South West OO9 Group and Treasurer of the OO9 Society, asked me if I had anything that I could take to the Exeter Show in July. I decided to add more scenery to the unfinished little layout and take the new and renamed *Woodbury Wharf*. It was a rush getting the layout ready for the

first weekend in July. I was still working on the scenics on the Thursday before set-up on the Friday, but the layout was very well received by the visitors to the exhibition. The only disappointment was that I had not found the time to create a curved backscene, so there were very

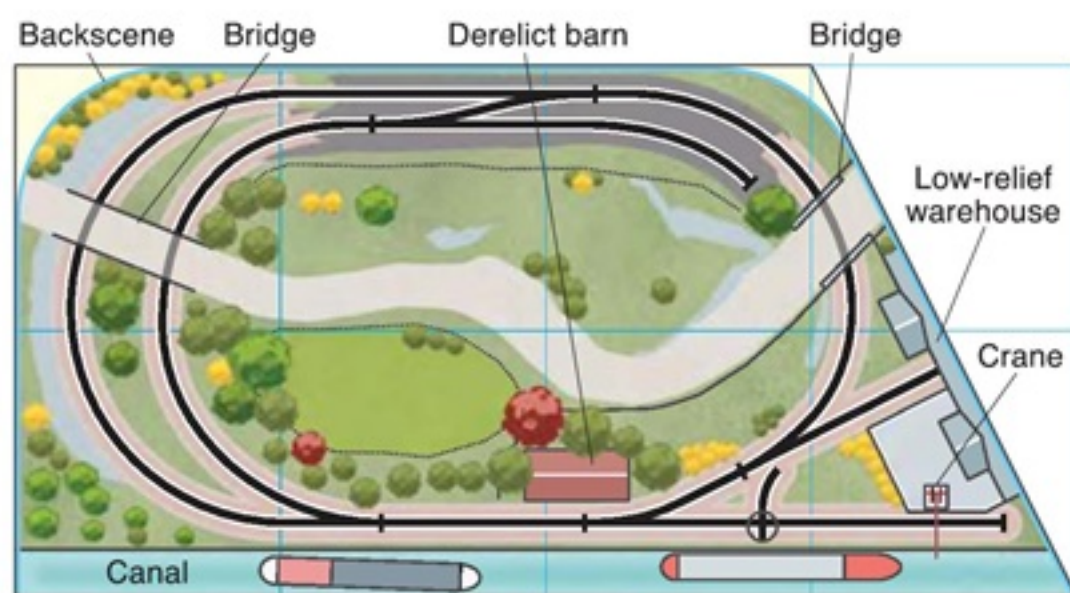
visible 'joins' in the sky. Since the Exeter exhibition, I have replaced the original backscene with a curved one, and the improvement is significant.

#### **Automatic control**

The layout can still be operated as before using a hand-held







### WOODBURY WHARF

Overall size: 1200mm x 600mm. Each grid square 300mm x 300mm.



▲ This derelict structure is part of a suite of woebegone buildings from the Hornby Skaledale range.

Gaugemaster analogue controller with the original push button uncoupling and switched points. However, I have now added a Heathcote Electronics passing loop module, which enables the trains to run automatically. This is helpful at exhibitions as I am free to talk to the visitors and Quarry Hunslets in auto mode allow chats with show visitors.

explain the joys of narrow gauge modelling in OO scale.

In automatic mode, a train runs across the front of the layout from left to right and stops out of sight at a hidden sensor in the loop. After a short pause, the two points that create the passing loop change position, and a second train runs across the scene from right to left and disappears from view, stops, and the sequence continues until I choose to switch the operation back to manual control.

### Locomotives and rolling stock

I can use a wide range of locomotives and rolling stock at exhibitions. Clearly large locos would not be appropriate for this little scene. When in 'auto' mode it is important to use locos that are well matched. I generally use two of the Brian Madge kitbuilt Quarry Hunslets which I borrow from my *Launceston* layout. They run beautifully smoothly and, as they use the same chassis and motor, their operating characteristics are very similar.

I have quite a large collection of small steam and diesel engines that can be used with a selection of wagons and vans, mostly kit-built, and a few ready-to-run Bachmann OO9 wagons.

### Buildings, backscene and figures

I have used one of the ID Backscenes 'Hills and Dales' sheets at the rear of the layout, mounting it on flexible MDF to create a curved backscene. I have used this range of backscenes with all my layouts. I like the muted colours and haziness that gives a real sense of distance.

The low-relief warehouse has always been a principal feature that gives the layout height and it is located flat against the backscene on the right-hand side. It is a Langley Models kit.

The derelict barn at the front of the layout is a resin casting that I bought second-hand. I have detailed and weathered it. I believe that it originally came

from the Hornby Skaledale range [Ed. – yes indeed, it's the R9648 stables.] All sorts of clutter has been left outside the building including old tools, a wheelbarrow, oil drums, a ladder, and a tin bath.

There are a few figures in view. A group of warehousemen are enjoying a tea break in the sunshine. The driver of the Rover 75 has stopped his car on the bridge and stepped out to get a good view of the passing trains. A farmer inspects his sheep from the field gate, and an enthusiastic bird-watcher has his binoculars trained on a hawk as he stands near the stile while his dog waits patiently. That must be the bird-watcher's Morris Countryman parked in the lay-by. Two policemen are checking a Morgan sports car that has been left abandoned at the side of the road. Could it be a stolen vehicle, perhaps?

The bridges have been created using materials from the Wills range and the canalside crane is built from a Ratio kit.

### Landscaping and scenics

The raised ground was created using Celotex insulation material available from many builders' merchants. It is very easy to cut and shape and was fixed in place using PVA adhesive. The scene was finished with a number of scatters and static grasses of different colours and textures from a variety of manufacturers.

There is a wide selection of trees, bushes and wild flowers from several different sources. Some were ready made, others have been created using materials mainly from the Woodland Scenics range. I do think that it is important to incorporate different shades of colour into the vegetation so that it truly reflects the huge variety in nature.

The 'water' in the canal was made using brown paint and several coats of gloss varnish. The two canal boats were built from kits. One is a resin kit from Langley Models and the other is a simple balsa kit made by Craftline Models.

### Finishing touches

The layout has been completed with a black front surround that bears the layout's name and LED lighting has been installed to illuminate the scene. Self-adhesive LED tape was bought from an on-line supplier and this was







▲ Warehousemen taking a break. Note how the bridge and lock gate provide a very effective way of terminating the canal visually.

fixed to a section of aluminium L-shaped strip which was attached to the rear of the front panel.

The layout sits on two metal DIY trestles which were bought very inexpensively 'off the shelf' from a well-known DIY store. It is very quick and easy to set-up. The whole layout now has a maximum width of 120cm and a depth of 60cm and can easily be transported in the back of my car.

With hindsight, it would probably have been easier to start this project from scratch on a new bare baseboard. Extending the original triangular-shaped board was not an easy task with a num-

ber of odd angles to measure and cut accurately.

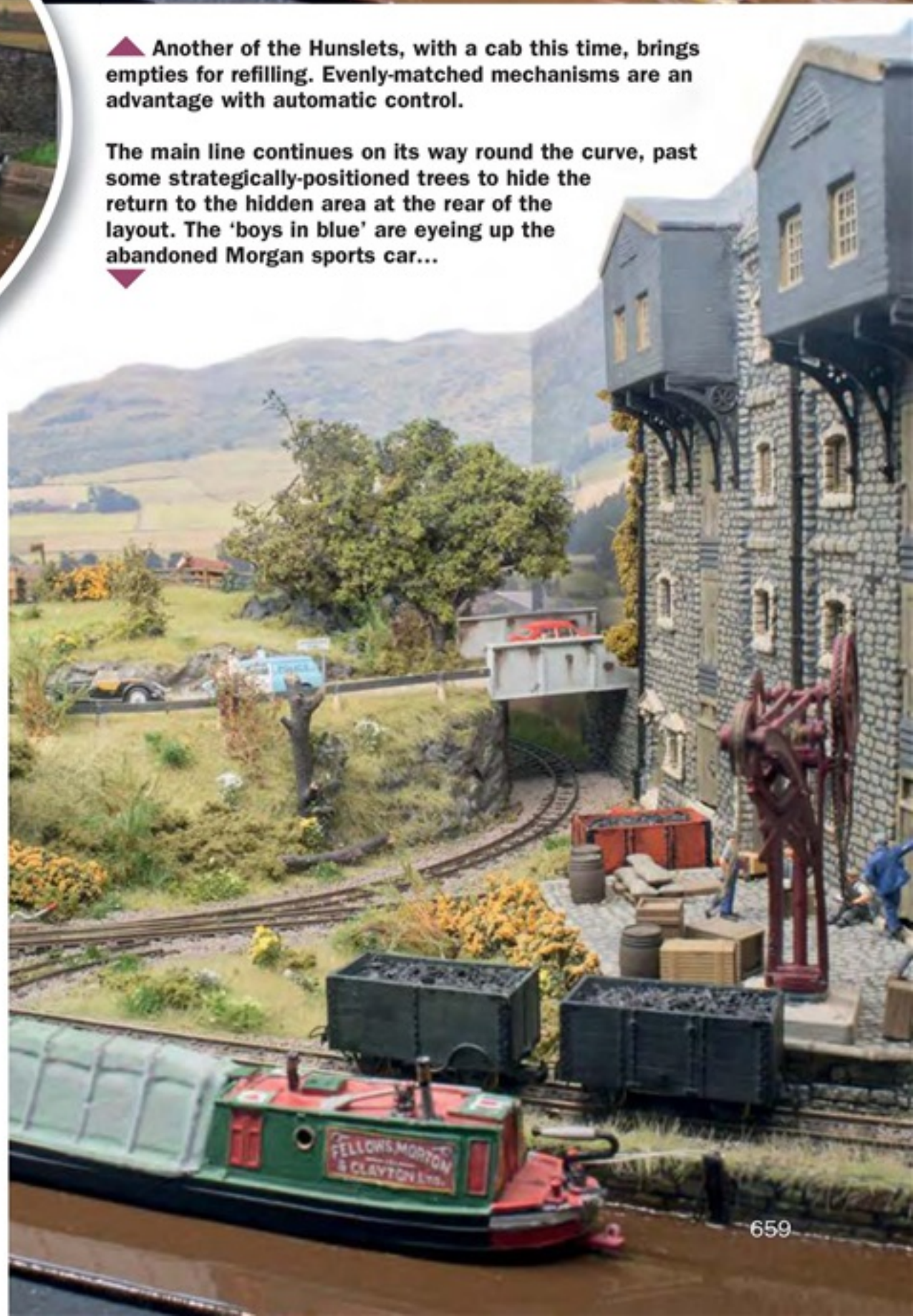
However, I have enjoyed the project and it has been fun to work on and the public seem to really appreciate the result.

With ready-to-run models now available from Peco, Minitrains, Bachmann and Heljan, there has never been a better time to have a go at modelling narrow gauge in 4mm scale. If anyone is inspired to have a go, I would strongly recommend joining the OO9 Society – see website at [www.009society.com](http://www.009society.com).

Thanks must go to my grandsons, Oliver, Jacob and Edward. Without them, this project may never have been started.

▲ Another of the Hunslets, with a cab this time, brings empties for refilling. Evenly-matched mechanisms are an advantage with automatic control.

The main line continues on its way round the curve, past some strategically-positioned trees to hide the return to the hidden area at the rear of the layout. The 'boys in blue' are eyeing up the abandoned Morgan sports car...





# A 'recycled' station building

Following on from his 'pizza base' goods shed (see RM April), **ROGER MERRY** presents another 7mm structure made from readily available materials.

Photography by the author

Some of the basic techniques used to make this model are the same as for the goods shed, but this project involves several new ones, including brickwork, slate roof, and a way to make neater corners.

Polystyrene pizza base, which is very easy to cut and scribe and strong enough to form the underlying structure, is used again: no separate sub structure or outer textured shell is required. It also takes most types of paint, except cellulose based, without the need to use a primer undercoat. Pizza bases are the best source of this material because of their larger size.

Photo 1 shows a wall of the model with window and door apertures cut out.

## The prototype source

Although freelance, the model is based very loosely on the LMS station at Shenton and is to be deployed on a narrow gauge



O-16.5 layout. I've borrowed those elements from the prototype which appeal and combined them into the model to suit my layout. In this case, I liked the arrangement of Shenton's windows and doors, but needed a narrower overall structure to fit in the space available.

## Lintels and brickwork

Photo 3 shows one of the main walls with the corner stones and lintels scribed in. Painting them at this stage helps to identify the areas to be avoided at the next

1

▲ The completed structure on its platform, made from the same raw material.

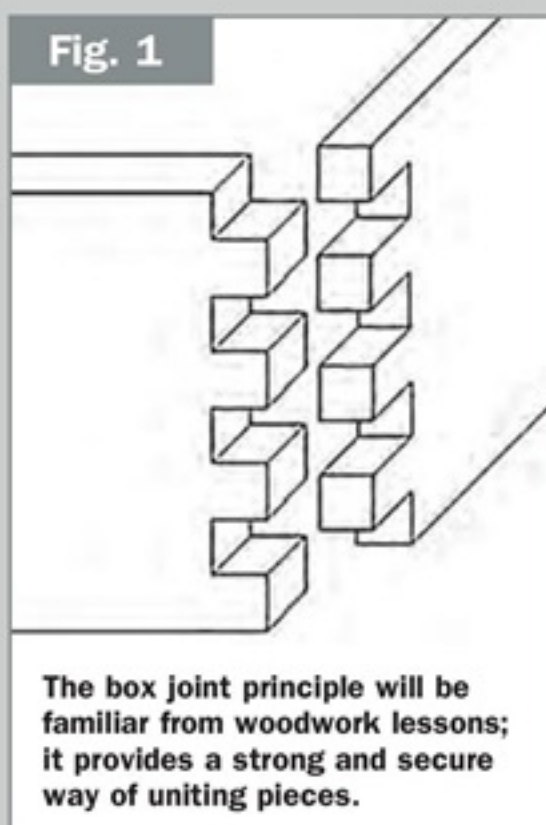


stage – scribing either brick or stone courses. In the smaller scales, and when representing bricks rather than stones, it can

be difficult to see previously scribed lines, so instead of a metal scriber, I now use a very sharp soft pencil, which leaves a

## Interlocking corners

Photos 2a, 2b, 2c and 2d show the stages in my technique for making building corners. Known in woodwork as a 'box joint' (see Fig. 1), it's quite different from the butt or mitre joints I've used previously.



My initial trial piece with the two walls taped together to ensure the stones will match up.



The lines scribed to mark out the corner stones: the thickness of the material is 5mm so this amount must be cut away to provide the interlocking gaps.



Showing the stones marked out, the crosses mark the pieces of material to be cut away.



My first completed box joint. It fitted together quite well and gave me confidence to continue with the technique.



nice visible mark. Some wobbly bits show up in the close-up, but from normal viewing distances the overall effect is acceptable.

Photos 4 and 5 show the four walls painted using emulsion sampler pot colours. A single coat is usually enough. The black bricks, a feature of the original building, were coloured with a felt tip pen, which I found to be more accurate than a brush.

### Mortar, weathering and assembly

In photo 6, the walls have been assembled using Copydex, and a 'mortar' coat has been added which tones down the rather garish tones seen earlier.

This mortar technique is particularly easy to do with pizza base; a sloppy mix of filler with a dash of black paint added is applied and worked between the bricks with a finger. The filler mix is then wiped off using a tissue, leaving the mortar in the courses. The texture is subtly different from the brickwork itself, and more realistic than painted mortar. It made the time spent doing all that scribing worthwhile!

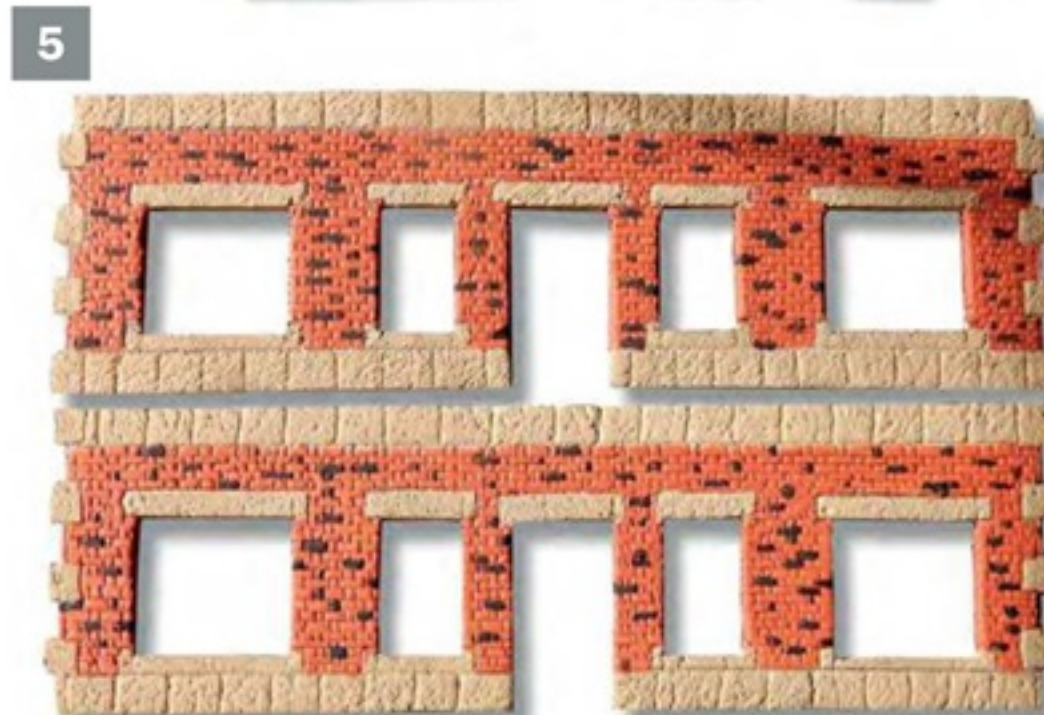
For weathering, talc or proprietary powders work well on pizza base, and grime is easy to add with a soft pencil, rubbing it around gently with a finger. Hold the material gently though – it is very soft and gripping it too hard may leave indentations on the surface.

Photo 7 is a close-up of one of the corners, showing the interlocking of the corner stones. It gives a very solid joint, especially if you add some internal bracing.

At this stage, I decided that the outline of the building still looked a bit plain, so I added a gents' toilet as an outbuilding made in exactly the same way as the main structure but using corrugated cardboard for the roof.

### The slate roof

To depict slates, and using a straight edge as a guide, scribe equidistant horizontal lines in some more pizza base material. Use a sharp blade but make only very shallow cuts. Before removing the straight edge, press it down hard into the material, so as to create a shallow ridge between each row – then scribe in the individual slates along each. This technique takes hardly any longer than drawing the slates with a pencil and gives a nice textured effect. Photo 8



shows the roof sections made this way in place.

Doors and windows have also been added, the window frames and bars were simply cut from old greetings cards – recycling again! A useful tip here is to prepare a whole lot of strips at once, but don't cut them right off the card, leave both ends of the strips attached by a short fillet; this stops them curling up and keeps them to hand.

### Small details

For guttering the bottom edge of the roof section is painted black and a strip of black card along it, as per the goods shed method. My chimney stacks were made from two equal lengths of pizza base glued together with Copydex to make a double thickness strip. When dry this was cut lengthways and the two pieces glued back to back again to make a strip only a quarter width of the original size, but four layers thick. Stacks were cut to length and scribed with brick or stone, painted and fitted in place. The fluted chimney pots are part of the 'bendy' section of plastic drinking straws, though these may soon be in short supply. The plastic doesn't take paint well, but wrapping a little masking tape around the straw helps it key.

For drainpipes I used wooden skewers with tiny strips of masking tape to look like joints and brackets, though if bends were required, the use of wire might be appropriate.

The final photo shows an LMS style canopy attached, to see how it looked. I must admit that the thought of cutting out all that fancy valance was too much for me, so I cheated and actually bought some.

That's it – a bit of enjoyable modelling for virtually no cost at all.



"Pour yourself a dram and drift away"

# Whisky Galore

With the imminent arrival of the Hornby R&H 48DS in OO gauge (including versions in distillery company liveries), the time is ripe for a micro-layout set in remote western Scotland. **NEIL RUSHBY** has gone one better with an island railway, serving a distillery and which is linked to the outside world by a small train ferry.

Artwork by the author; location photography by Tim Rayner

There's a long tradition in cinema and television, starting with *Whisky Galore* in 1949, of whimsical films set in the remote splendour of Scotland. The wilder, less populated parts of our country seem to lend themselves to small stories about people and communities. In a village you can't fade into anonymity; everyone is known, everyone matters. The landscape can't help but be one of the stars of the show too. There is the contrast in scale between open and rugged mountain, the broad expanse of loch and sea, and the relative smallness of the human activity which takes place in that location.

▲ A typical Western Isles landscape of clear sea, white sand, scrubby machair and mountain. The photo was taken near Allasdale, on the western side of Barra, on 8 May 2019.

If the highlands and islands work as a setting for the visual arts then I reason they should work just as well for our purposes as railway modellers. If cinema and television can exploit the location as a backdrop for a whimsical approach to storytelling then maybe we can too. We're fortunate that the mass market, commercial arm of our hobby in the shape of Hornby has provided us with an appropriately fanciful piece of motive power in the form of the Ruston 48DS. Even better, two of the four initial livery variants are for whisky distillers, most appropriate for the region. Initial news of this got me mentally dusting off a notion I'd had some years ago for a Scottish island layout setting.

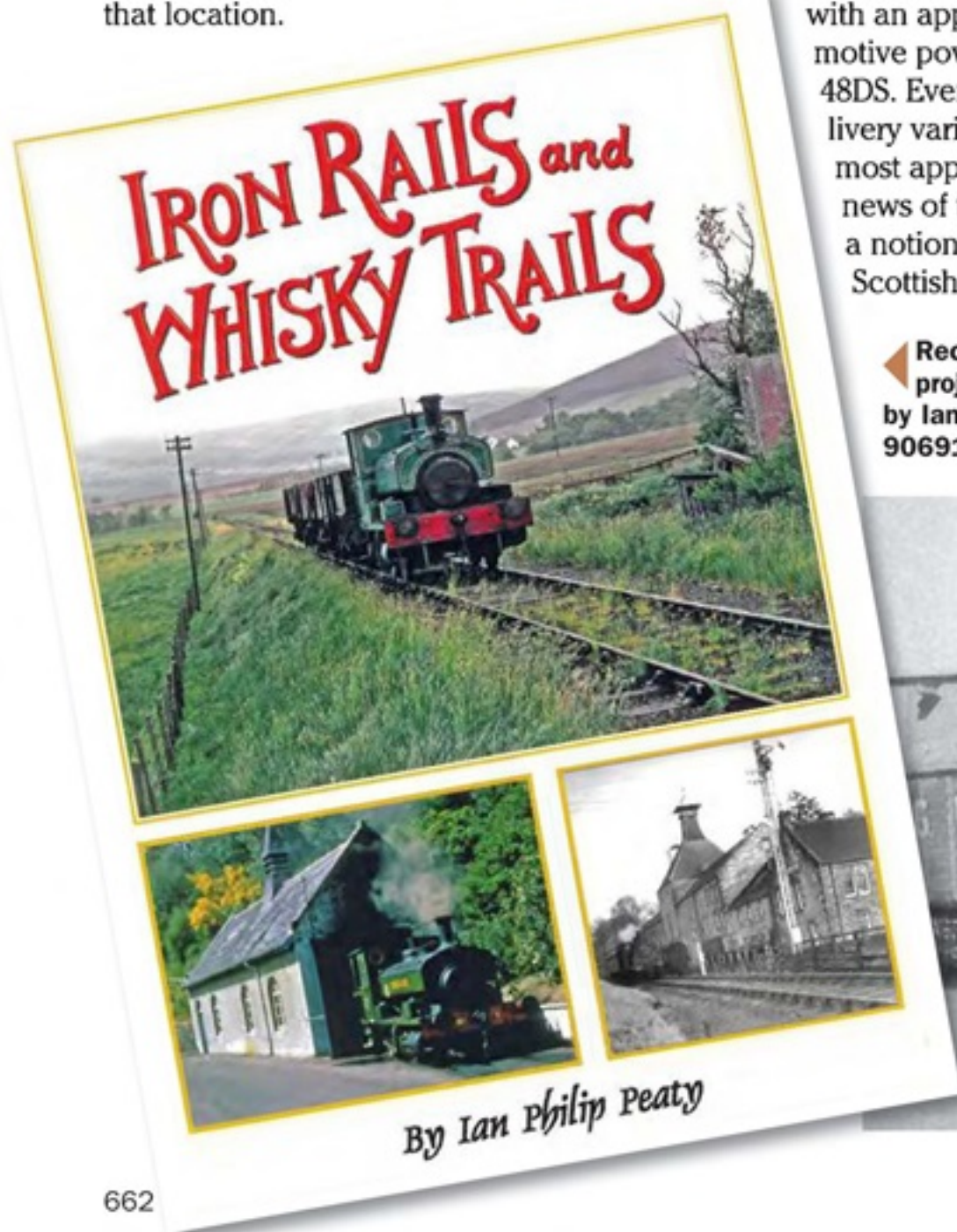
▲ Recommended reading for this layout project is *Iron Rails and Whisky Trails* by Ian Peaty (Irwell Press, ISBN 978 1 906919 53 5).



▲ Coming soon – ideal motive power in the shape of the R&H 48DS from Hornby.

I had in mind the sort of small habitable isle which occasionally crop up in some of the Scottish lochs. However islands which support a railway are usually large affairs and I had a desire to see if it would be possible to

▲ A 48DS shunting Leith General Warehousing peak-roof vans; models were available from Harburn Hobbies as special commissions, but none are currently available.





cram the whole island in a relatively small space and yet still have it believable. Making the layout look spacious would be key. Normally when designing a layout we focus on the available length as it's that which determines what we can achieve. Because railways are linear affairs it normally sets train and station length and is often viewed as the major limiting factor in the design process. However I've noticed that there's something else at play, particularly when it comes to achieving a spacious look and that's the relative depth of scene. Short tubby layouts are particularly good at emphasising depth and somehow the eye and mind combine to interpret this as spaciousness. Good depth to the scene would give the possibility of designing an island in the round if short trains are thought suitable.

### Island-building

I took as my starting point of my design a solid board 7' long by 5' deep. Obviously if I was to depict a whole island then it would have to be surrounded by water so I couldn't go right to the edge and whatever railway I designed would have to take this into account. I also felt that I didn't want a too regular shape for the land mass which further reduced the space available, as a good part of the layout would be the loch. In these circumstances I reasoned that the best shape for the railway would be a U. It allowed for the maximum length of run in the space allowed and if I distorted it by bringing the ends of the two legs closer together it would give an egg-shaped island; It wouldn't feel as though the island was following the edge of baseboard. Once these bare bones were decided on the rest of the layout fell into place pretty quickly.

At the end of the left-hand leg of the U is the distillery, the reason for the line's existence. It is, and has to be, something of a caricature. A proper full-size distillery would take up far too much of the island, so I pared it back to the bare minimum and then pruned some more. Hopefully there's just sufficient to make it look like the railway was worth putting in place. A siding runs into the distillery here and one runs in front of it by the loch side and that's pretty much it. Leaving the distillery complex, if a couple of buildings can be called a complex, the railway heads out alongside the beach before swinging round in a tight curve between the castle and a rocky outcrop. The castle, like the distillery, will have to be a scaled-back representation. Some Scottish castles are fairly minimal affairs so I don't feel too bad about reducing the one on the model to a tower and a turret. The rocky outcrop that the line passes is at the end of the rising ground in the middle of the island. This hill in the middle provides a visual barrier that serves to separate one end of the line from the other. The afforested skyline

► **Plausible steam alternatives to the 48DS in OO include the 14" and 16" Andrew Barclay 0-4-0ST industrials, available from Hatton's.**



WHISKY GALORE Scenic area 7' x 5' Each grid square 1' x 1'

further helps this separation. After passing the castle the line branches out. One leg heads towards the harbour side with a siding and storage/transshipment shed, the other into a loop and onto the jetty, with a kick back siding to the small loco shed. It's the jetty that provides the railway's connection to the wider world. I didn't want the railway to be a self-contained, insular affair. Though I know I stretch credibility with the entire concept, I thought it would be a step too far to introduce further loading and unloading that harbourside transfer would entail. Far better, I reasoned that full and empty wagons could arrive and leave by ferry.

### The train ferry

Though far from the norm it's the train ferry link to the outside world which lends a veneer of reality. Arriving from elsewhere is a device used by the film industry to 'ground'



▲ This view of the Caledonian MacBrayne ferry Loch Alinn about to load vehicles at the slip on Eriskay gives a flavour of how the model harbour could look.

the story about to be told; a classic example would be Spencer Tracy arriving by train in *Bad Day at Black Rock*. The ferry also allows for a larger selection of stock to be used on the layout than would otherwise be the case. Though there will be a comfortable number







▲ The Ruston & Hornsby model is to be supplied with a conflat wagon with extra pickups to aid current collection, which can be coupled to either end of the loco: it can be disguised as the 'reach' wagon used to place wagons onto or disembark wagons from the ferry, if the loco is not to venture to sea.

of wagons that the line can accommodate without getting hopelessly gummed up, the ferry allows a far greater number to be rotated through the layout.

As the ferry is perhaps the key to making the layout work in both the practical as well as conceptual sense it's perhaps a good time to look at how it might work. The Dutch firm of Artitec produces a couple of resin kits for train ferries; either would suit. Another possibility would be a conversion of the Airfix landing craft kit.

I think that real water would be impractical in the context of my design, so I have assumed that the loch would have a flat surface, my favourite being paint and varnish layers over the baseboard top. Though the ferry could be permanently berthed at the jetty and stock removed and replaced by hand, I think we would miss a trick if the ferry couldn't move.

To do this either wheels, rubber tyres so as not to scratch the water surface, or felt skids hidden in the ferry's hull would be required. It may be possible to incorporate a driving and steering mechanism into the ferry, but I think some form of remote traction based on the Magnorail system might be a better way forward.

## Motive power

As we've started to consider the working elements of the layout perhaps it's time to take a look at motive power. As mentioned earlier there's the new Ruston 48DS from Hornby, a featherweight at around 7½ tons. Being so light on its wheels opens out the operational possibility of the loco heading out onto the ferry to collect wagons from off scene exchange sidings and returning with a load or loads. The process would be reversed to get wagons off the island.

Other typical Scottish distillery motive power would be the Andrew Barclay saddle tank, available in a number of

guises from Hatton's. It would perhaps be a little heavy to voyage to and from the mainland regularly, but this opens up the alternative operating interest of using reach wagons to place stock onto the ferry or remove it from the vessel. Hornby provides an alternative in its Peckett 0-4-0ST and while it doesn't scream 'Scotland' in the manner of the Barclay it's still credible motive power.

## Plausible traffic

Grain, barley to be specific, would arrive by the wagon load. Bachmann produces an excellent model of the British Railways 21T grain hopper. Parkside has a kit for the earlier LNER bulk grain hopper in its range suitable for both prewar and the British Railways era. Various manufacturers have made models of the blue BRT bulk grain wagons over the years but they seem an awkward fit with the quaint nature of the layout design.

The other staple inward traffic would be coal, for the distillery's boilers and in steam days as loco fuel. Again there are ready-to-run and kit options of both the ubiquitous 16T steel mineral wagon and its wooden-bodied antecedents. In the diesel era there would be the occasional tank wagon of loco fuel tripped across by ferry to the engine shed.

Traffic out would be the whisky itself in vans. Photos of freight trains in whisky

Rocky outcrops such as this on Barra will break up the sameness of the island's beaches – and why not use actual rock chippings?



country also show barrels being transported in open wagons, I assume that these would be empty casks, fulls being treated to the extra security of covered vans.

## Concluding thoughts

I hinted at the beginning of the article that this scheme has been around for some while. Just like the best whisky it's had time to mature and as such, other ways that the plan could be further developed, have come to mind. The first is that it could be squeezed down from 7' x 5' to 6' x 4' without too much trouble and I rather like the notion that it would then occupy the footprint of many of Cyril Freezer's classic designs.

Others centre around the possibility of modelling the far end of the ferry's journey as well. With a few gentle tweaks to the design as it stands, it would be possible to introduce a small section of the mainland shore with jetty and approach track disappearing under a bridge or behind a lochside stand of trees. Given greater space then far more of the far shore should be possible and more of the exchange sidings too.

If significantly less space is available I think it should be possible to squeeze the concept drastically down to suit. A true minimum space model should be achievable though you would probably have to forgo the U shaped track plan and prune back some of the peripheral features too.

All these, like the initial plan, are daydreams, but isn't that what model railways are all about? One of the joys of our hobby is that it connects us to happy times and places. On a wet winter's night in the suburbs of a big city we can be transported lochside on a warm summer's evening, the sun twinkling on lapping waves driven by a gentle breeze. We can be back in an era when freight went by the wagonload and wasp stripes weren't a mandatory feature of shunting engines. Put Mark Knopfler's soundtrack to *Local Hero* on the stereo, pour yourself a dram and drift away.

Longer grass, such as here at the RSPB reserve at Balranald on North Uist, will give variety to the treeless sections of landscape.





# Ruston & Hornsby 48DS

**TIM RAYNER** provides some background context to the model loco intended for the Whisky Galore suggestion; **BOB PHELPS** prepared the drawings of these tiny machines, reproduced here to O gauge.

With a full-size overall length about the same as a 7mm scale version of the *Whisky Galore* layout, it's clear to see that these are very small locomotives indeed! The scale drawings first appeared in the June 2007 issue of *RAILWAY MODELLER*, and a précis of the text follows.

In all, 204 of these industrial shunters were built at the Ruston & Hornsby factory in Lincoln, between July 1941 and October 1967. Their model designation stands for 48hp Diesel Shunter: the Ruston 4YC four-cylinder engine drove 2'6" diameter wheels by means of chains. As well as industrial users, British Railways Southern Region operated one, latterly at its sleeper works at Broadclyst, near Exeter. It was also to be found at Yeovil, which makes the location of the preserved example in the photograph entirely apposite.

Built in December 1961 (works No.458959), this 48DS was delivered to AEI and used at its glass-



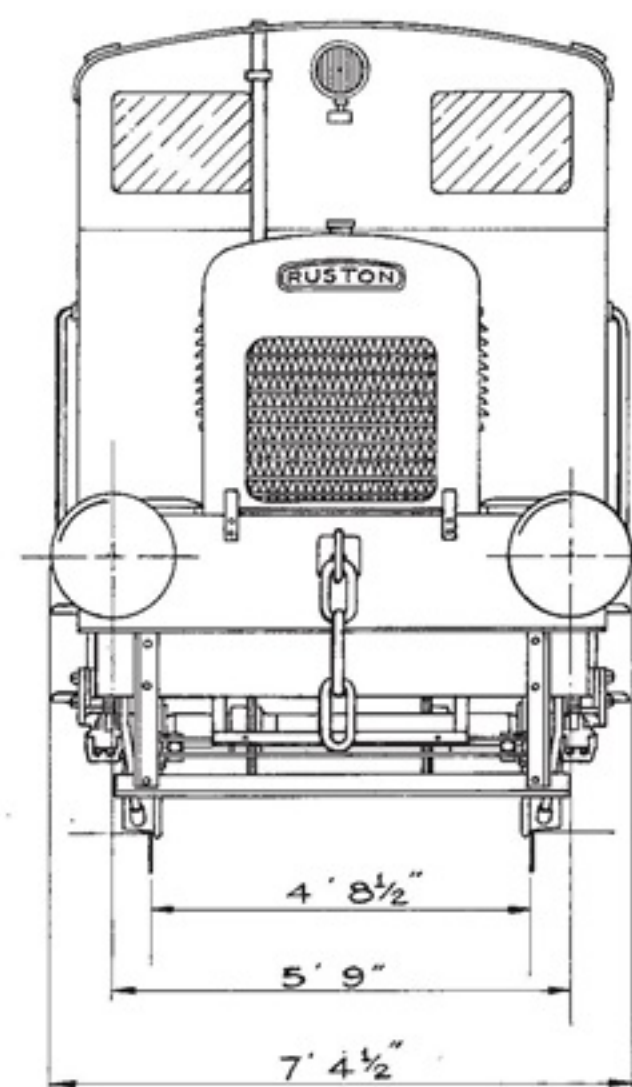
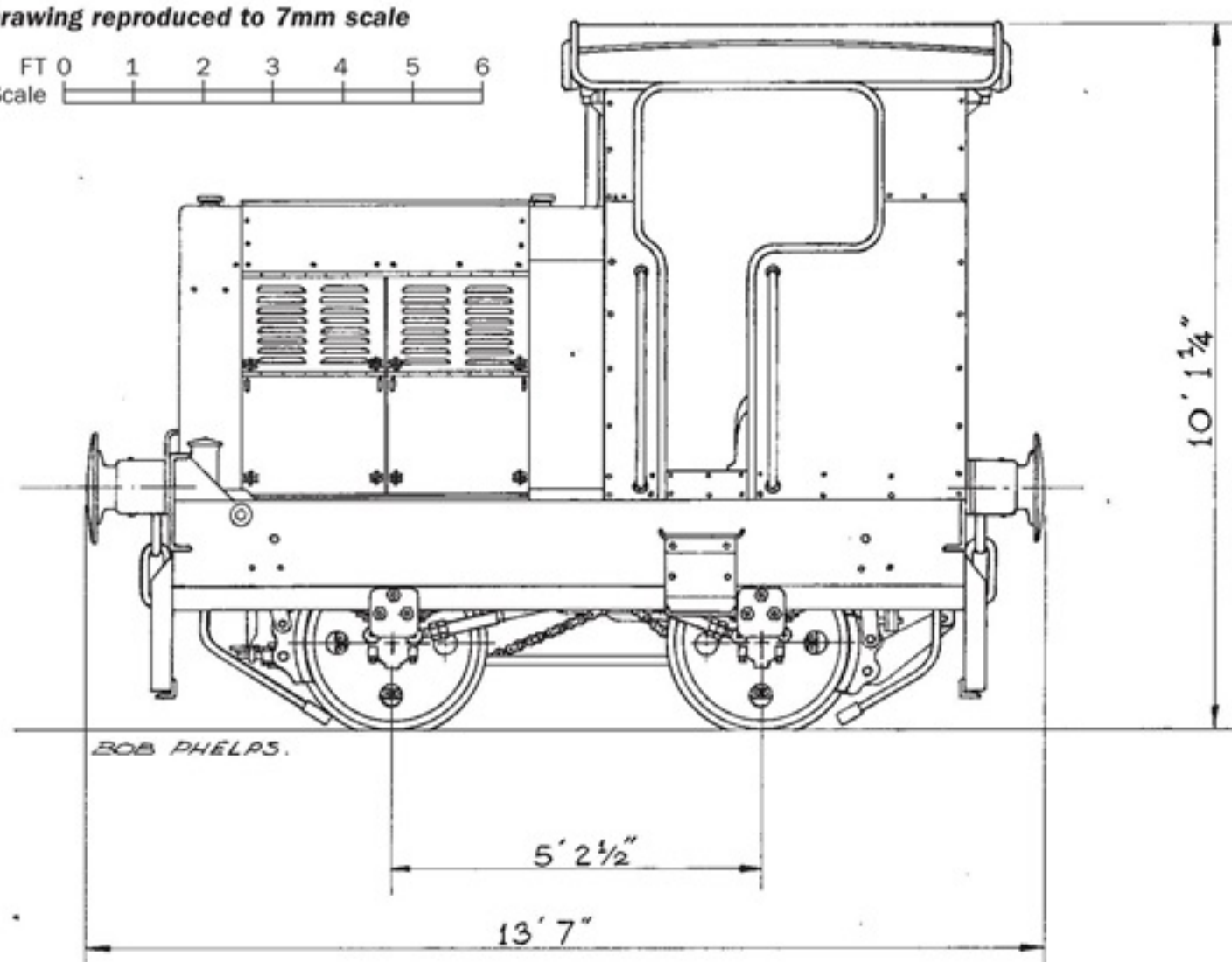
works in Chesterfield, and later at another AEI plant in Nottinghamshire. As a tribute to its BR-service stablemate – which after withdrawal in 1972 was scrapped at Cohen's in Kettering,

one of several 48DSes cut up there – it carries No.DS1174, the next number up from the last real Departmental Series machine. Post-preservation, vacuum braking equipment has been fitted.

Owned by the Ruston Group and based at the steam centre at Yeovil Junction, No.DS1174 *River Yeo* is seen in August 2006. Fitment of vacuum braking has required body panel alterations. Photograph: Steve Haynes

Drawing reproduced to 7mm scale

FT 0 1 2 3 4 5 6  
Scale





# Caradon Junction

Inspired by the china clay branches in eastern Cornwall – especially the Looe branch – **MIKE KELLY** created this OO gauge layout to complement his other exhibition layout and collection of models.



“ – Trees play a big part in bringing a layout to life, and are very useful in achieving scenic breaks – ”

Mike Kelly

Way back in June 1987 I purchased a copy of the last-ever edition of the *Model Railway Constructor* because it had an interesting article on a china clay layout called *Caradon* – with two ‘r’s – *Junction* by a John Cox. Some five years later I met a John Cox with a layout called *Kettlewell* at the Derby Exhibition who turned out to be one and the same person and we have been very good friends ever since.

I had for sometime been thinking of another layout to complement my *Cromer* exhibition layout of 2008 (see *RAILWAY MODELLER* January 2011); it would also feature my blue and grey stock from *Everbury*





(RAILWAY MODELLER August 2014). I did have a few ideas in mind but for some reason I kept thinking back to *Caradon Junction* so that is what inspired my new project.

My *Caradon Junction* is based on but not an exact copy of Coombe Junction on the Liskeard to Looe branch line in Cornwall. In reality the platform is about where the clay dries are on my layout; the dries themselves were much further away at Moorswater. It is also of course a tribute to John Cox's original layout without which mine would not have existed. Caradon is an area around Liskeard which at one time had a district council of that name. Incidentally, Coombe Junction is the second-least-used station on the rail network having an average of one passenger every 14 days.

I read again the original *Caradon* article

and when I informed John he was delighted at my choice, so much so that he presented me with his Middleton Press book on the Liskeard to Looe branch. Armed with the book I then decided – as I had done when building my other layouts – to make a site visit to get an appreciation of the place, and this I did in October 2015. The railway sits at the bottom of quite a narrow valley where river, railway and road seem to converge. A long line of silver birch trees makes a perfect backdrop and gives quite a hemmed-in feeling which was ideal for a narrow baseboard as it tends to make the layout look longer than it is. During the visit photos were taken and a number of measurements obtained. I am much more interested in getting the feel of a place right rather than having exact dimensions.

### Baseboards and planning

When I built *Cromer* I have to admit that I gave little thought as to where it would be stored when not in use; my idea of a shortened layout in my shed (RM January 2011) and supported on adjustable shelf brackets over the *Everbury* fiddle yard didn't really work out. It now lives in a built-in wardrobe except for exhibitions or when put up in the lounge, which is the only place (at 14'6") it will fit at home.

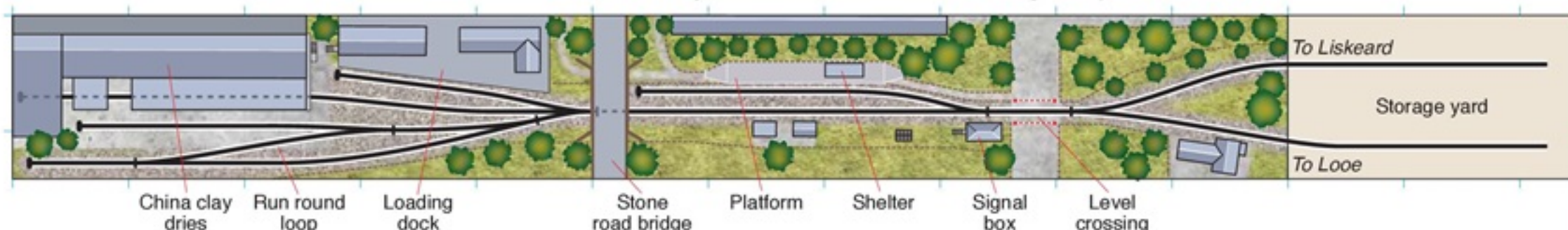
This time it was designed from the start to be stored in another built-in wardrobe with a clear width of 820mm, or by changing boards

**No.37 254 descends the gradient towards Caradon Junction with a couple of vans for bagged clay. The Class 122 waits its turn.**





# **Caradon Junction** Overall layout size 13' 6" x 1' 5". Each grid square = 1' x 1'.



The end boards have an additional strip of timber 32mm x 12mm at the bottom to give extra strength and a fixing for the brass locating dowels which, with case clips, hold the boards together. The shorter boards do not require any additional strengthening but on the longer boards I fitted two diagonal strengtheners to prevent twisting after the track had been laid and point motors fitted. This avoided the motors getting in the way of the diagonals. The boards constructed this way are quite light but strong which is very beneficial when the years catch up with you.

Rather than having built-in legs I decided to standardise the supports by using the same A-frame lightweight timber trestles on all my layouts; this also meant that only two extra frames were required. These were all made from planed timber using 44mm x 12mm for the verticals and diagonals and 34mm x 18mm for the horizontals all linked together with 63mm hinges. Adjustment is made by a piece of the 32mm x 12mm timber with holes

▲ Back to hydraulic days as a North British Class 22 arrives at the dries. The locomotive is a weathered Dapol item.

around erecting it to fit into my office which has a wall space of 3230mm. The idea was to divide the layout into three separate sections; clay dries, junction and incline. This would give me a couple of 1200mm x 450mm boards (which would fit in the car easily) – one for the clay dries and one for the junction – and two 810mm x 450mm boards, one for the incline and one for the fiddle yard. The longer boards stack vertically and the shorter boards horizontally. For use in the office the two horizontal boards mentioned above are omitted and a new 810mm x 450mm fiddle yard board inserted. The layout is operated from the rear at exhibitions and the front at home, hence two fiddle yards are required, especially as the levels are also different; the other fiddle yard board also stacks vertically.

For the track layout I used John's original plan but with an additional siding parallel to the dries which allows for more shunting movements.

The baseboards are as previously constructed for *Everbury* and *Cromer*, 6mm plywood cut to size. The front boards have a minimum depth of 80mm and are contoured for the landform. The rear boards are 260mm deep and form the backscene; they could not be deeper as they have to fit into the wardrobe. The boards are strengthened with 32mm x 12mm planed timber and fixed together with PVA glue and wood screws.





drilled in it hinged to the horizontal which drops over a M6 bolt fixed to a Conti board joiner on the other horizontal.

The timber was obtained from B&Q, the brass locating dowels from Layouts 4U and the other items from Wilko.

### Trackwork and electrics

All the track and electrofrog points are Peco Streamline code 100 items, which I find quite acceptable providing it is carefully ballasted to hide the sleeper thickness. The sleeper spacing on the plain track was increased slightly to give a marginally better appearance. With the clay dries baseboard being only 1200mm long it did present problems in fitting in the points and a loop track to hold five clay hood wagons. I ended up using a three-way point under the bridge and a small radius point at the far end (which does not look too obvious) with two medium radius points in between.

The junction board was also slightly problematic in that I needed the shunter to be able to withdraw itself and five clay

hoods from the dries without preventing access to the platform road by the branch DMU and staying clear of the ground signal for Liskeard which was all a rather tight fit. By comparison the incline board was quite simple as all I had to do was to make the real life 1 in 40 gradient believable in the 810mm length of board; this was achieved by making it 1 in 36 up to Liskeard and 1 in 78 down to Looe, which gave a reasonable grade separation before entering the fiddle yard. Fitting in the alternative front-operated fiddle yard was quite easy as it only had to butt up against the junction board.

The fiddle yard uses cassettes which are very space saving and are made from 19mm aluminium (B&Q) glued to 9mm MDF; some date back to my *Northbridge* layout, so have been in use for over 15 years. Shorter ones are used for locos to cut down on handling.

Points are operated by Peco point motors, some hidden in the bridge superstructure, some fixed under the baseboard and others (PL-11 with self-installed longer operating arms) buried in the landform. Frog polarity is changed by a Hex Frog Juicer which automatically detects which track the loco is on. The layout is DCC controlled using the NCE system and all individual pieces of track and points have dropper wires attached and

in turn soldered to self-adhesive copper tape bus bars on the underside of the layout. A 16V ac transformer operates the points and a similar 12V dc version operates the signals and both are inside a box which sits on the floor.

A Heathcote Electronics kit using servo motors operates the bracket signal which is a Ratio product assembled by John and adapted so as to use bent wire rather than cranks. Very small LEDs have been used to backlight the spectacle plates. Having had problems with the very nice looking Dapol lower quadrant signals the latest ones controlling the junction are powered by a PP3 9V battery and operate perfectly.

The various switches are mounted on a plastic box connected to the layout via umbilical cords and can be fitted either side. Connections are made via the choc-box type as my soldering is not good enough for the 25-way type.

The level crossing gates are operational and are part of another Heathcote Electronics kit using servo motors which can be made to move quite slowly which visitors find amusing.

**The 'set' length for a freight being shunted is five hoods plus a loco, in this case the Sentinel diesel (from Hornby).**





## Landform and buildings

The track and sleepers were first painted with Railmatch sleeper grime and then, doing about 100mm at a time, Green Scene chippings (sometimes mixing different colours) were fixed down in the normal way. Once dry the surplus ballast was vacuumed off for reuse.

Cable troughing (quite narrow in reality so home made) and Peco point rodding, which was an afterthought, were installed.

Expanded polystyrene sheet was used for the landform which was stuck down with No More Nails glue and left to set having been weighed down. It was then sculptured with a springy old carving knife and smoothed with a small Surform; this was very messy but easily done and quick to vacuum up. After this it was covered with about three or four layers of newspaper cut into approximately 100mm squares, stuck down with wallpaper paste. The whole scenic area was then covered with a spring green grass mat. Various lengths and colours of static grass fibres, all from Green Scene were applied to achieve the required effect.

Trees play a big part in bringing a layout to life and are very useful in achieving scenic breaks; whilst I like them a lot I have no real interest in

making them so as usual they were purchased from The Model Tree Shop. The hedges, which may be a bit uniform, come from the same supplier and it is assumed that they have been recently cut.

The post and wire concrete fencing uses 2mm square Plastruct that was drilled and superglued into holes set in the field and then threaded with Lycra; the posts and Lycra were then painted. The wooden posts are a mixture of Peco and Ratio products and painted before fixing.

John Cox asked if he could help when informed of my intention to build *Caradon* so I suggested that as he had made the original buildings some 30 years ago that he could make them again! I have to say he made a splendid job of them. The buildings were made from mount board covered with Wills plastic sheets; the clay dries are of a generic design rather than a copy of the much larger ones at Moorswater. The loading dock and its buildings were copies of the originals sized to fit and were a mixture of adapted Ratio kits and Wills sheets. The Devon & Cornwall Wools building was made with the aid of photos but scaled down so as not to overpower the station area. A Ratio GWR signal box kit was made up to give an impression of the one that stood at Coombe Junction. The platform and shelter were also made by John. I produced the road bridge which has a mount board base covered with Wills random stone plastic sheet.

## Rolling stock

As mentioned at the beginning some blue and grey stock was to hand so the main additions were the rakes of china clay hood wagons, which really set the scene, and suitable shunters.

The two shunters are a Hornby Sentinel deliberately badly repainted and a very scruffy green Bachmann 03 which is assumed to have been purchased from BR. Both are fitted with sound and stay-alive capacitors and run very slowly when required.

The Bachmann clay wagons have replacement hoods made from folded paper which I think look better.

The main line locos are again with sound and comprise classes 22 (Dapol), 25 and 37 (Bachmann), and 52 (Heljan), all of which would have worked the branch at some time.

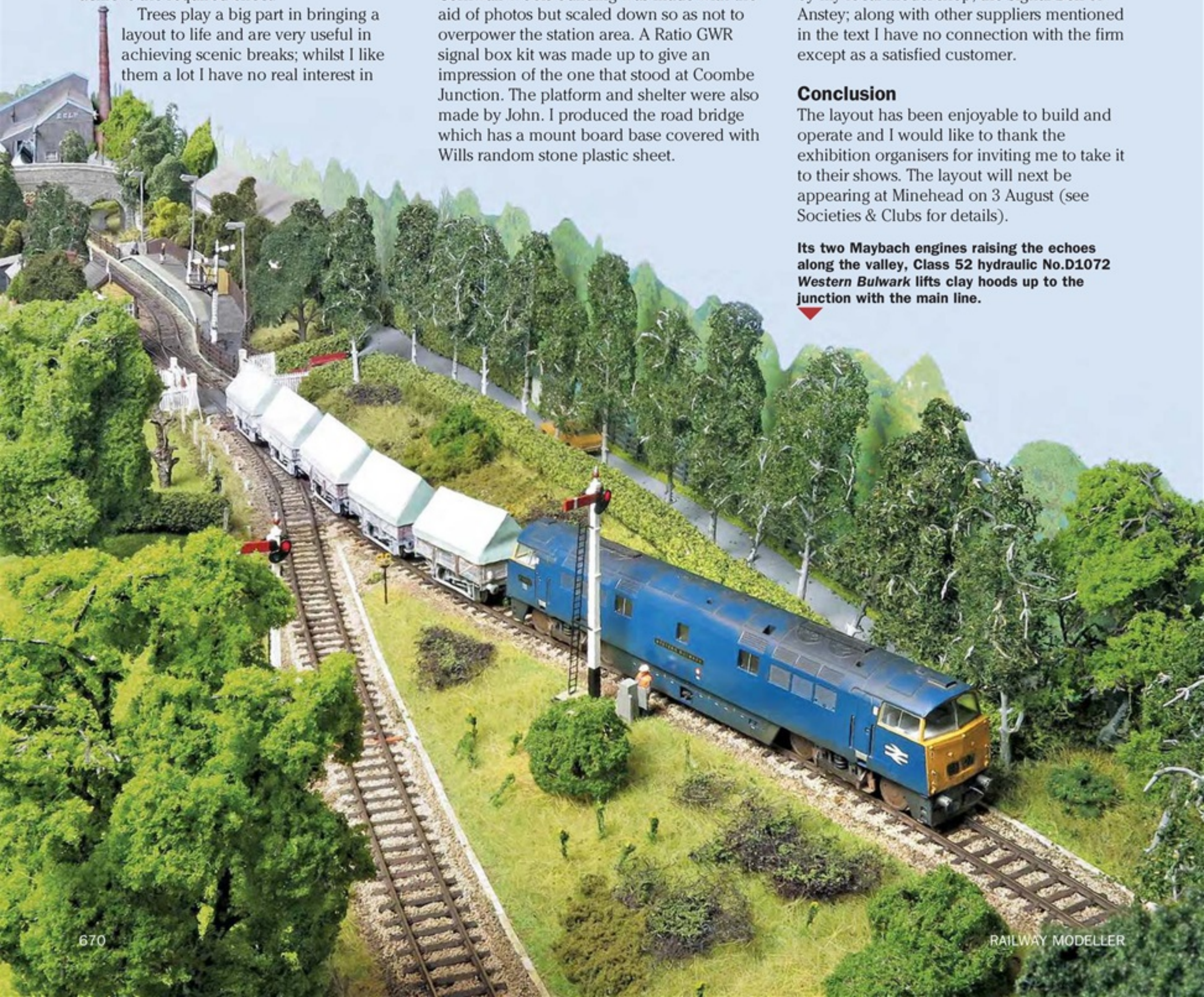
The passenger stock is by Dapol, with sound fitted single DMUs of classes 121 and 122 which operated on the branch for many years.

Most of the 4mm scale items were supplied by my local model shop, the Signal Box of Anstey; along with other suppliers mentioned in the text I have no connection with the firm except as a satisfied customer.

## Conclusion

The layout has been enjoyable to build and operate and I would like to thank the exhibition organisers for inviting me to take it to their shows. The layout will next be appearing at Minehead on 3 August (see Societies & Clubs for details).

**Its two Maybach engines raising the echoes along the valley, Class 52 hydraulic No.D1072 Western Bulwark lifts clay hoods up to the junction with the main line.**







# Leicester MRG marks its 70th anniversary

*In advance of its anniversary show this month, Brian Gillespie looks back at the life and times of this long-lived Midlands model railway group.*

**L**eicester Model Railway Group can lay claim to having nurtured a few noteworthy modellers in its 70 year history. Distinguished names on the club's roll of honour include the Revd Teddy Boston, of Olton Priors fame; N gauge virtuoso Andy Calvert,

responsible for masterpieces like *Nether Stowey*, *Calverdale* and *Moorcock Junction*; and Ray Tustin, an early pioneer of 7mm narrow gauge modelling.

At its founding, the first clubroom was in Knighton Fields Lane just south of Leicester city centre. It was upstairs, and access was by what would now be classed as open-plan staircase. Removing the layouts once a year for exhibitions was very difficult. There were two rooms, in the large one there was the library, a small circulation area and two layouts.

Around the outside was the O gauge coarse scale track (more like the modern test track). Inside this circuit was the OO layout – featured in the January 1961 *Model Railway Constructor* – which was originally part of an end to end layout; the two stations were joined by a model of one span of Brunel's Royal Albert Bridge. In the other room was a TT layout, later replaced by an end to end OOn3 narrow gauge layout.

Shortly after I joined the LMRG in October 1959, the group moved its monthly meetings to Charles Street Baptist Church. When we first moved there, the church was still supplied by the old Leicester Corporation tram electric circuits, and we had to use 110V projectors for illustrated slide shows.

Over the next few decades, the club moved to various places in Leicester, such as the Adult Education Centre on Western Road; Halford Street (near the old Odeon cinema; and a church hall on Melbourne Road. This was followed in the late 1970s to St. Andrew's Church Hall, King Richards Road; here we held a few week-long exhibitions. We moved to our present premises in 1999.

In addition to our own shows, the group also used to exhibit static models at The Model Railway Club's Exhibition at Central Hall, Westminster, London, and at other major shows, such as Manchester.

Our 70th-anniversary exhibition will include members' layouts and photos and will be open from 1030 – 1700 on 17 August 2019, and 1300 – 1600 on 18 August 2019 at St Margaret's Church, St Margaret's Street, Leicester.



▲ Uphill Pantin station on the LMRG's OO layout which appeared in the January 1961 *MRC*.



Top: Inside the long-time venue of the LMRG shows, the Boot and Shoe Trades Hall (below). Photos: courtesy Leicester MRG

**RAILWAY MODELLER**  
For the average enthusiast February '73 18p



◀ Group effort: the combined Maunsell Parva to Wells O gauge layout appeared as Railway of the Month in the February 1973 *RAILWAY MODELLER*.

## Leicester show 2019

The LMRG's anniversary show will take place over the weekend of 17 & 18 August in the centre of the city (see Societies & Clubs). For more about the group see [www.lmrg.co.uk](http://www.lmrg.co.uk)



*Whatever happened to ...*

# Ambergate

Words: **CHARLES STEVENS** Photography: **STEVE FLINT**



**T**his 4mm scale layout based on the prototype Midland Railway triangular junction at Ambergate just north of Derby was constructed over a number of years by the late John Webb, who started it in 1983.

In its heyday, the real Ambergate was the junction between two Midland Railway main line routes: the line north to Chesterfield and Sheffield, and the route to Manchester via Bakewell and Miller's Dale through the Derbyshire Peak District, latterly the route of the Midland Pullman in the 1960s. To the south of the junction was Derby, the home of the Midland Railway.

Ambergate is still a junction to this day, where the line to Matlock – on the truncated Manchester route – leaves the northern route. The triangular aspect to the site is however long gone and Matlock trains venture only as far as Derby.

## **Not triangular, but tadpole shaped!**

I became a custodian of the layout in later years, responsible for organising the operating team and the transport and erection of it at shows which was a major logistics exercise in its own right.

Basically the layout is 'tadpole shaped', some 36' long by 18' wide at the junction end. So quite a beast by all accounts. There are 14 main boards which were transported to shows in coffin-like crates. They certainly did the job but there was an art to stacking them in a hire van, or indeed in John's garage after a show.

Each unique board was made of heavy

grade plywood, with selected components of industry grade materials which made them exceedingly weighty. Each crated board requiring two people to lift them.

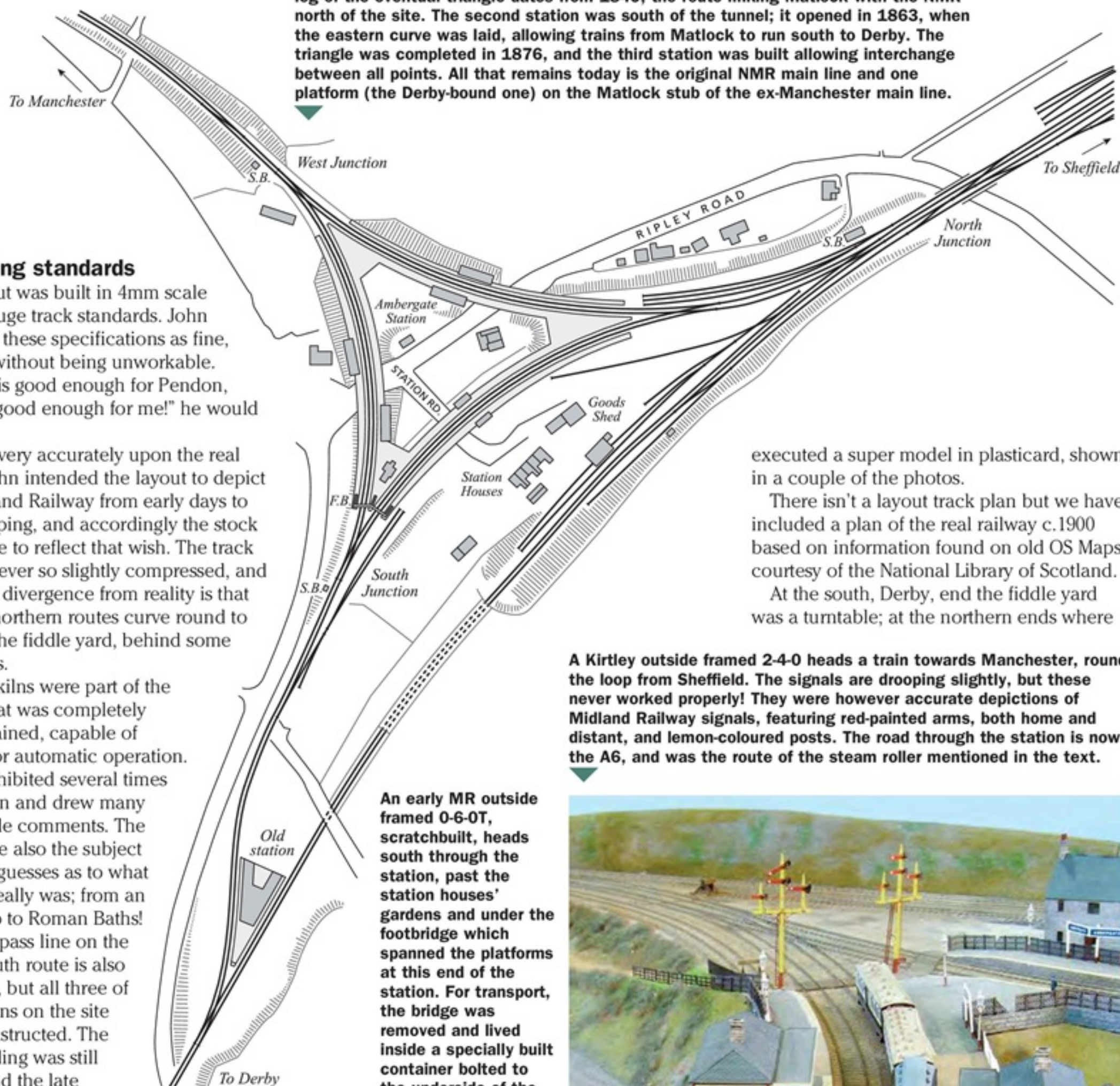
The central part of the layout, which features the junctions and stations, rested on a triangular piece of Dexion framework (I did say industry grade materials were used) which was colloquially known amongst the operating team as the 'sheep pen'. Each of the boards rested on this contraption, each being bolted into place in sequence. Once the last one was lowered into the gap, all bolts were tightened and the whole thing became as solid as a rock!

**In the early 20th century, some of the major railways faced a loco crisis. Their own works could not turn out sufficient new locos to meet demand, neither could the outside contractors. The railways turned to the USA where the makers had spare capacity and bought several 'off the shelf' 2-6-0 designs. The GNR had similar locos, but this model depicts one bought from Baldwin for the Midland Railway. Not always successful in real life, this model, scratchbuilt by John, ran as an 0-6-0 for a couple of shows until the front pony problem was sorted out. John had a loco shortage crisis too!**





The site had a somewhat piecemeal development: the double track line through the tunnel is the original North Midland Railway Derby – Chesterfield route of 1840, and the first station was close by the northern junction of the later triangle. The north-to-west leg of the eventual triangle dates from 1849, the route linking Matlock with the NMR north of the site. The second station was south of the tunnel; it opened in 1863, when the eastern curve was laid, allowing trains from Matlock to run south to Derby. The triangle was completed in 1876, and the third station was built allowing interchange between all points. All that remains today is the original NMR main line and one platform (the Derby-bound one) on the Matlock stub of the ex-Manchester main line.



### Modelling standards

The layout was built in 4mm scale to EM gauge track standards. John regarded these specifications as fine, enough without being unworkable.

"If EM is good enough for Pendon, then it's good enough for me!" he would say.

Based very accurately upon the real place, John intended the layout to depict the Midland Railway from early days to the Grouping, and accordingly the stock was made to reflect that wish. The track layout is ever so slightly compressed, and the main divergence from reality is that the two northern routes curve round to meet in the fiddle yard, behind some lime kilns.

These kilns were part of the layout that was completely self-contained, capable of manual or automatic operation. It was exhibited several times on its own and drew many favourable comments. The kilns were also the subject of many guesses as to what the site really was; from an ICBM silo to Roman Baths!

The bypass line on the north/south route is also depicted, but all three of the stations on the site were constructed. The first building was still extant and the late Barry Fleming had

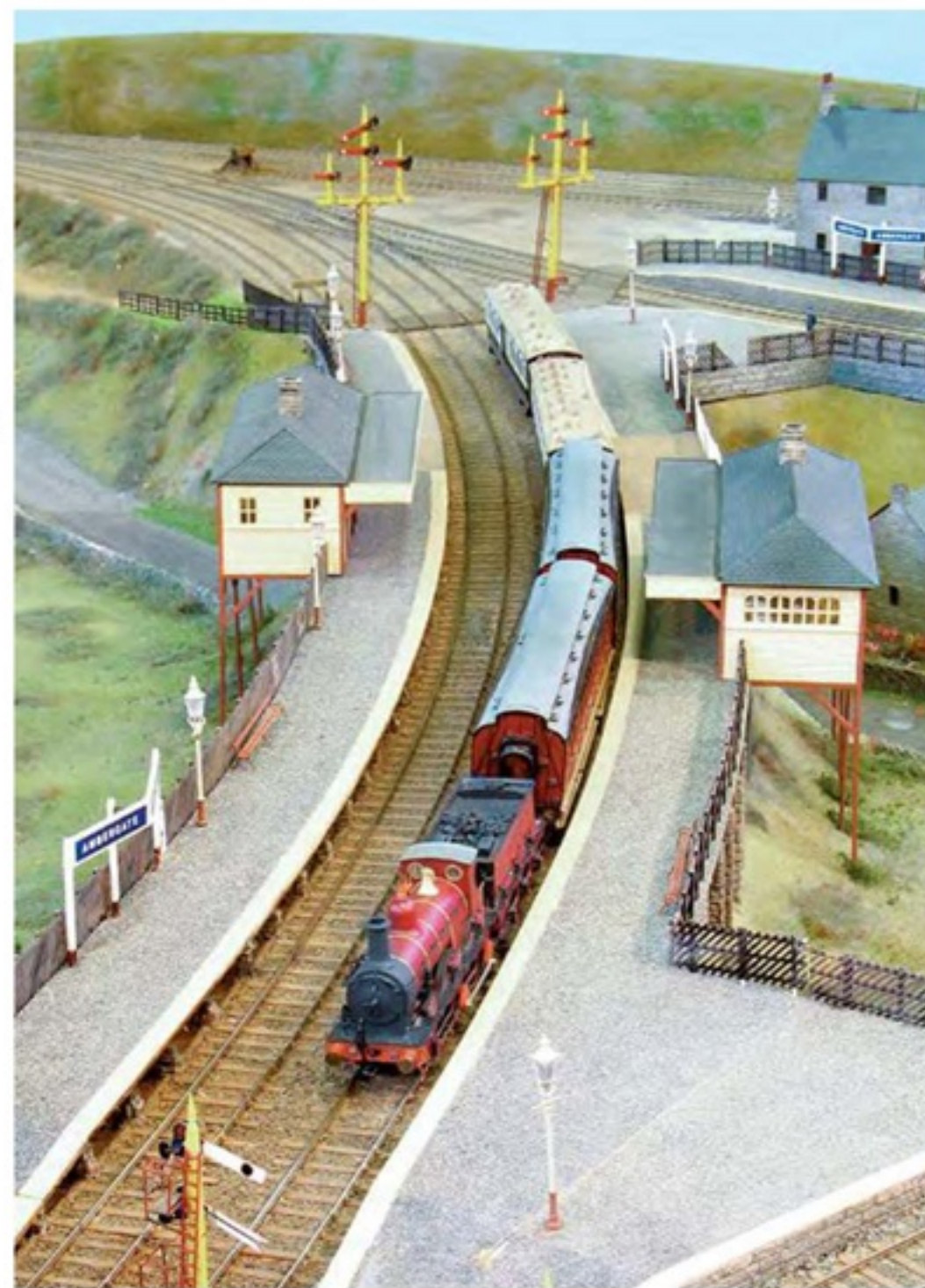
executed a super model in plasticard, shown in a couple of the photos.

There isn't a layout track plan but we have included a plan of the real railway c.1900 based on information found on old OS Maps courtesy of the National Library of Scotland.

At the south, Derby, end the fiddle yard was a turntable; at the northern ends where

A Kirtley outside framed 2-4-0 heads a train towards Manchester, round the loop from Sheffield. The signals are drooping slightly, but these never worked properly! They were however accurate depictions of Midland Railway signals, featuring red-painted arms, both home and distant, and lemon-coloured posts. The road through the station is now the A6, and was the route of the steam roller mentioned in the text.

An early MR outside framed 0-6-0T, scratchbuilt, heads south through the station, past the station houses' gardens and under the footbridge which spanned the platforms at this end of the station. For transport, the bridge was removed and lived inside a specially built container bolted to the underside of the baseboard.

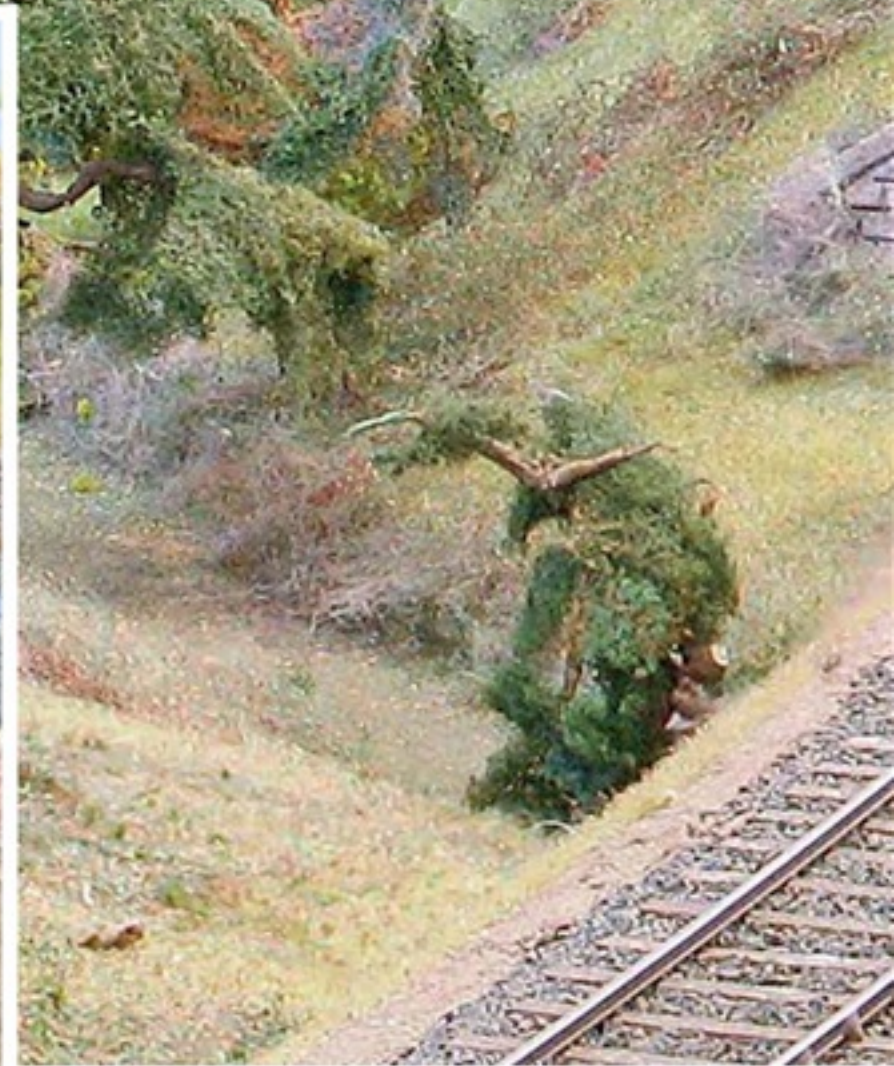






▲ This picture shows that as in the real thing, the station buildings were built on stilts; these buildings too had their own transport boxes which they fitted snugly and securely. The models were built from plasticard and wood strip.

From time to time borrowed locos appeared, as John had never completed enough to run the layout; Keith Foster lent this LNWR 0-8-0 to him to appear on freight trains. This was prototypical as LNWR did have running rights and the company's trains appear in photos occasionally. In this picture a freight train, making a return from Sheffield, heads south past the old station building.



the lines came together, formed another fiddle yard. There was the capability for circular running using the Sheffield to Manchester routes and, on one occasion, when there was a major failure elsewhere (fortunately rare) the steam railmotor was set chugging around, which kept the



A shot which shows all three stations on the triangular section. The same train in the elevated viewpoint on p673 is seen again in a slightly wider shot, showing that the signal box too was cantilevered out over the side of the embankment. Behind this box ran a road and the canal.



viewing public engaged. An alternative distraction was a steam roller chasing a dog along the road (A6): this used the Faller road system, with the magnet in the dog and the battery in the van behind!

The layout was basically complete but the signals, although installed, did not always

work. John was working on a realistic operating mechanism involving a bounce but never was able to get many posts working. That meant for a lot of the time they were ignored which led to a few comments at shows!

The layout was taken to most of the major

shows and even abroad, to Utrecht. We no longer have any association with it and it is now believed to be part of a larger layout in a barn somewhere, attached to *Buxton* and relaid to P4 standards!

I wonder what John would have thought of that?



Through The Railway Room Door...

# Greatford crossing

For a retirement project, **GRAHAM HOBBS** decided to recreate his favourite train spotting location, a minor road crossing south of Essendine on the East Coast Main Line in OO, along one side of a purpose-built railway room in his garden. The other side continues the ECML theme – but you'll have to wait until the September issue to find out what it looks like...

Photography by Paul Bason

**M**y previous permanent layouts have all been built to include a significant run in the garden in order to accommodate scale length trains, however, with retirement looming and an increasing reluctance to be outside on anything but a warm day I was keen to build one which could be enjoyed throughout the year.

In 2003 it was discovered that the original

shed housing the indoor portion of the last creation was rotten at the back allowing mice to get in. Like me they were devouring lots of magazines! The section needing repair was close to a wall and difficult to reach so a radical solution was required. I decided to build a new custom-designed shed.

To hit the ground running I had already rescued the old indoor section which was

fully functioning and 20' long so too good to throw away. I had managed to extract it by cutting it into two parts and this involved carefully slicing through platforms and several lines of track. Also I had several boxes of unused Peco code 100 rail and lots of reusable lengths from outside. I was pleasantly surprised how good the condition was of Peco's excellent product as it had





been down for 16 years and good enough to use again therefore any ideas of going EM or scale 4mm was dismissed for economic reasons. I have not regretted it but one day would like to try building in a finer scale.

After fitting the ready-made section the rest of the new layout was built *in situ* with 4" x 1" frames supporting 9mm ply and arranged so that if one day it had to be moved there are easy places to separate the trackbed. This thinking applied to the scenery and separate backboards were made from 6' x 2' lengths of hardboard which were all spray painted outdoors with matt emulsion first to give a basic graded sky effect and then the detail painted on later to match the scenery. Any scenic work that does touch the backscenes can be detached easily by carefully running a knife blade between the two.

Of course, during the earlier stages a detailed track plan was drawn and duplicated several times which enabled me to make diagrams for trackwork, electrical connections, signalling, plotting structures and arranging scenery.

Perhaps the most important yet tedious job was to lay and ballast the four track main line and that which continued around the curves to the station on the other side. Eventually it was done and the scenery began to take shape with embankments completed in the time-honoured method of chicken wire supporting plaster bandage.

### Choice of location

Why Greatford? In reality it was a rather insignificant level crossing on a minor road between Essendine to the north and Tallington to the south. It is largely a personal choice because in the summer of 1963 I cycled out from Peterborough to Casewick bridge near Uffington several times to enjoy a quiet time in the countryside whilst trainspotting. In those days you could park your bike safely on the roadside and trot off into the fields armed with a picnic and notebook; I have featured these happier times on the model.

The October 1993 copy of *Steam World* contained an article by Chris Leigh about Greatford Crossing with some lovely photographs by Noel Ingram. These clearly showed the attractive GNR type 1a box looking diminutive next to the 'Italian villa' styled railway cottages and the five and four bay gates. I then started to collect images of the area and found that Philip Wells also spent time there with his camera. His pictures are available through the RCTS but relate more to the 1950s.

My original idea was to recreate my own train spotting experience and at about the

same time I saw the excellent layout *Stoke Summit* for the first time, which inspired me to want to move in this direction.

Greatford is but one part of my layout and portrays a four track section of the ECML near to the meandering River Glen in the rolling South Kesteven countryside. The signals before colour lights were all concrete post construction and there was a crossover from the down slow to the down main immediately north of the signal cabin.

### Rolling stock and motive power

Intentionally the layout depicts the last two years of main line steam on the southern end of the ECML with a few examples of diesels beyond this date. Older models include Proscale, DJH, Steven Poole and Nucat kits along with superdetailed R-T-R releases. My June 1980 *RAILWAY MODELLER* article showed how I re-sculpted the then new Hornby *Mallard*, which as *Silver Fox* is still in use on the layout. Today we can luxuriate in the excellent products from all the main manufacturers including newer and more accurate A4s. Many new models are so good they only need customising with, in some cases, renumbering to our personal taste.

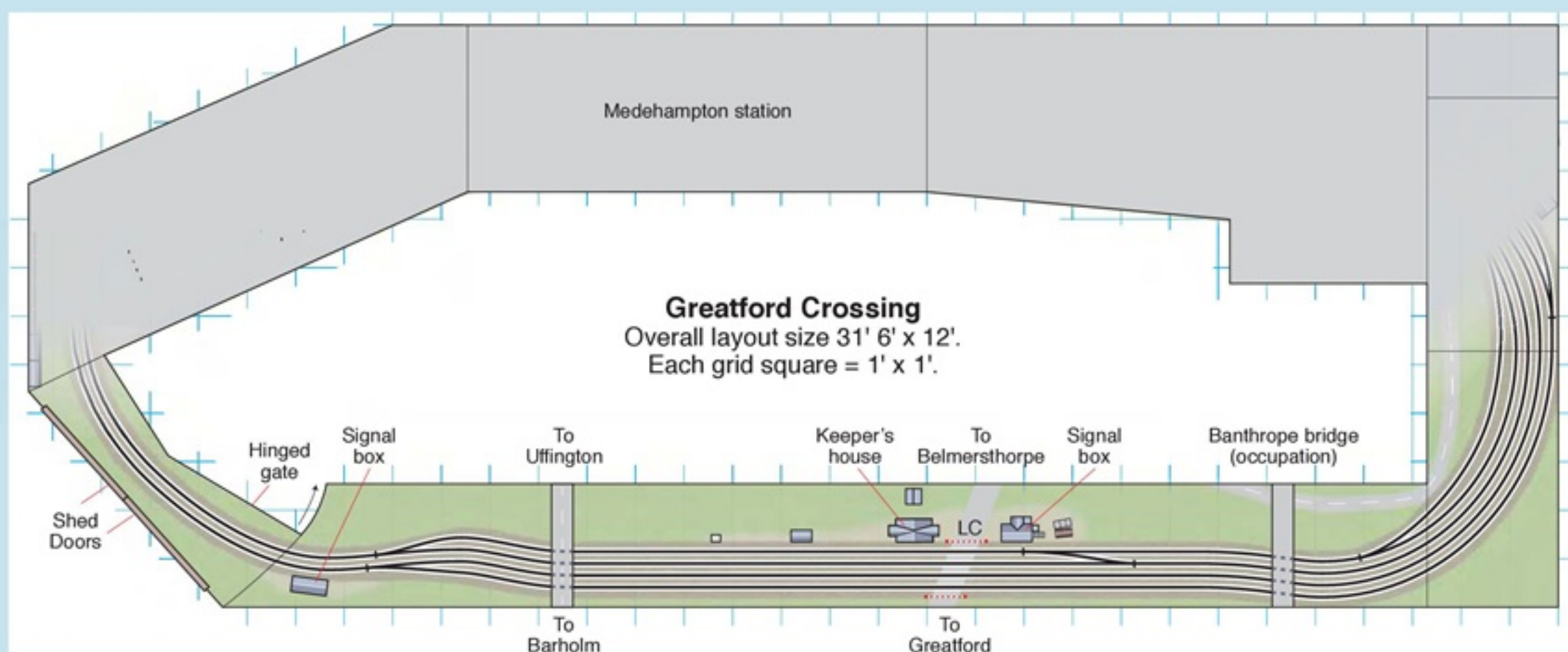
My loco pool is determined by those that I photographed as a schoolboy during the 1960s and augmented by others noted in my ABCs that I did not capture on film but can still inform my memories of those experiences during the final years of East Coast steam on the old GNR.

A Craftsman C12 represents the first engine's footplate I ever stood on when I was

The southbound *Talisman* roars past the Italianate cottages behind Deltic No.D9007 *Pinza*. Carriage boards and correct headcodes for each express add realism.







about six. These lovely old GNR tanks would simmer gently on pilot duty in the parcels bay car park at Peterborough North where my father would take me to watch trains on Thursday afternoons. A PDK J17 (awaiting assembly) will portray the subject of the first picture I ever took with an old Kodak Brownie as it passed under Crescent bridge followed by an N2 69529 (one of the later pilots) catered for by a Steven Poole kit, fitted with my own brass built chassis. My A3s cover most of the later variants and a Proscale A3 *Flying Scotsman* with correct tender for the time shows the split handrail so memorable on the 'racehorses' in their final guise!

My collection also includes early diesels and prototypes like *Lion* and DP2 which extend the timeframe by a year or so. The latter loco was for a long time a Lima example with Craftsman detailing but has recently given way to a DCC fitted Heljan model. I can always find an excuse to run specials or 'iconic' trains and this enables me

**Gresley A4 No.60021 Wild Swan rushes past 01 No.63647, plodding north with a train of 16T mineral wagons.**

to show off those superb models commissioned by the NRM with other out of region examples such as *City of London* which I photographed at Peterborough in June 1964.

Coaching stock follows the same pattern with a mix of R-T-R Bachmann, Hornby and kitbuilt Ian Kirk Gresleys. Finally, and despite not ever seeing one, who could resist the Bachmann Blue Pullman? Yes, it can now be seen re-routed over ER metals, and was more recently joined by the Stirling single from Locomotionmodels.com.

### DCC power

From the start of this project it was determined that the new layout would incorporate DCC primarily to allow free movement and control of locomotives. After lots of research in 2004 I decided to go with NCE and as far as possible use TCS decoders. The original section of the old layout was altered to suit and all new areas constructed

with provision at three locations for hand-held controllers. These would service my main NCE Power Pro system and an additional Power Cab. Points, and there are lots, have SEEP motors but are all controlled by analogue switching which for me is a little more fun. When eventually they become fully operational, signalling also will be part of this analogue system. However, seeing the 'spaghetti' under the layout there may not be room for more wiring and that could mean I will have to consider digital options.

The whole layout is divided into three power districts which cover the main lines, loco depot and storage sidings.

### Greatford key structures

#### Bridges

There are two bridges on the Greatford stretch. The one north of the crossing is an







▲ The four-track section becomes double on the approaches to the hinged gate that allows access to the railway room.

occupation bridge at Banthorpe adjacent to the Banthorpe Lodge farm and the other is Casewick bridge on the road between Barholm village and Uffington. Both bridges were extensively photographed from various angles and measurements made by walking

paces and brick counting. When the ECML was electrified these bridges were raised substantially although visually not enough to make a lot of difference in 4mm scale.

The construction was all card with the abutments covered in embossed plastic sheet. Beyond the Banthorpe bridge the layout veers to the left whereas in real life the track turns right towards Essendine and here the scenery melts into a distance effect enhanced by a diminishing perspective created by using N scale features. Two of these are Severn Models etched pylons which were a delight to assemble and easy to solder.

The signature scene at the crossing itself involved mostly Noel Ingram's photographs (courtesy of Chris Leigh). I was also assisted early on by Paul Tilley, a local railway historian who kindly advised me on a host of details from his own memories of the area.

#### *The house*

The GNR gatekeeper's house seems big by comparison and has a much larger footprint. These were standard on the railway and one still exists at Hurn Road north of Werrington Junction although it has lost some of the height of the tall chimney and is now one residence not a pair of semis. My main problem with the house was trying to discover the arrangement of the outbuildings which I later discovered comprised two sheds either side of a communal path each with a coal store next to a privy.

I was lucky enough to be invited by the occupier at Hurn Road to have a look around but, sadly, the shed had been removed with only foundations left although

Photographer Noel Ingram's archive shots prompted the installation of a miniature version, along with his Wolseley 1500.







▲ The Great Northern crossing keepers' cottages were built to a standardised design with multipurpose outbuildings.

I was informed about the arrangement of the old structure. This visit though did enable me to take some detailed measurements and photos of the house itself which was immensely useful in creating the model. Strangely, as far as I can work out, these GNR designs did not feature a back door so access to the outside toilet would have involved a long trip...

The chimney stack on my model when removed allows the roof to be lifted for lighting to be fitted although in 4mm the windows are probably too small to enable any other inside detail to be seen.

I wanted to feature Noel Ingram along with his Island Green Wolseley 1500. The car had to be made from a Scalelink whitmetal kit as to date no other models exist of this or even the Riley version. Along the length of the fields on the western side are fences made from plastic posts drilled to take fine strands of wire separated from electric cables and fed

through the holes using tweezers. Vertical strainers are then gently soldered across the wires then trimmed to size. From available photographs it seemed that fences on the opposite side are all wooden.

#### Signal boxes

The box modelled for *Greatford* is a facsimile of the 1872 GNR type 1 box. It has a removable roof revealing a detailed interior and apart from the actual levers everything was made from plastic bits and bobs.

The box at the 'southern' end of the Greatford stretch carries a name that is a mix of Tallington and Lolham – 'Lollington'. It is a D&S kit of a GNR style superstructure with a card and plastic base again with lighting inside and detailed interior.

#### Trees

Various shapes and sizes of these were built around armatures of soldered copper strands from electrical cables covered with teased out horse hair or wire wool and all following the well documented tried and tested methods. Sprayed in carpet glue then

sprinkled in dyed sawdust or other materials the results gave a variety of effects and colour largely as a result of experimentation using diverse materials.

The foliage on the Scots Pines that were (and still are) growing near the crossing is represented by tufts of dark green kitchen scouring pads which have a coarse needle like texture. In addition sea moss provides a finer, more delicate foliage and some of this I managed to grow myself.

At some point in the future I will also want to model more specific types of tree if and when time allows.

#### Landscaping

The scenery at the end beyond Banthorpe bridge is raised partially to hide the track curves which originally was to be hidden behind a screen. Realising that this would pose access problems I opened up the area and increased the number of tracks to provide holding sections mainly for different types of freight traffic.

Most of this landscape and the main line embankments are covered in 'grass' from hanging basket liners with added extras like long tufts of sisal string for thistles and ground textures. With a fairly narrow depth for the main line scenery the fields had to be curved up to the backscene carefully to create a 3D effect showing distance without an obvious change of angle. Graduating colour is an essential technique but also the use of relief texture on the hedgerows and painted trees has enhanced the illusion.

The backscene was all hand painted with some reference to the locality; brewing on the horizon is a thunderstorm!

#### Next time

Graham will show off the other 'half' of his ECML layout, based largely on Peterborough, in the September issue, out on Thursday 8 August.

▲ A northbound express comprising Gresley coaches trails A1 Pacific No.60124 Kenilworth as it passes the box. The structure has a removable roof, revealing a fully detailed interior.







# RAILWAY MODELLING *Explored*



*Projects, hints and tips for beginners from Railway Modeller readers and staff*

**THIS MONTH** • Two layouts in one • A country pub in N • A scratchbuilt water tower

## Two layouts in one – the sequel!

**PAUL PAGE** shows us the second half of his layout, the first having been published in RM in July 2018.

A couple of years ago, I created the layout Capel Hill, a modelling project depicting a fictitious Southern Railway layout around the Capel-Le-Ferne area near Folkestone and set in the mid 1950s. This was due to the fact that I have a strong connection to the Southern as my great-grandfather, grandfather and father were all drivers on the SR. Also, hailing from Kent myself it seemed the obvious choice.

Furthermore, I had accumulated a fair amount of both early and late BR (SR) stock, so this particular period seemed best served. Capel Hill appeared in RM July 2018, and was built on a 5' x 18" framed board. Charwall was

A short freight arrives at Charwall behind a Class 47. Through the bridge, the Capel Hill part of the layout can be seen. ▶



Photography by Paul Bason



◀ 4-CEP units bring back childhood memories of the real things. No.7113 is alongside a 2-EPB.





built on the original Capel Hill fiddle yard, so the two stations are now face to face, each acting as the fiddle yard for the other, and separated by a scenic break. Hence the title: Two layouts in one.

My own personal memories of the Southern Region are from the 1980s, when as a family we regularly travelled from London Charing Cross to Folkestone Central to visit family. These journeys occasionally involved having a ride 'up front' on a 4-CEP with my dad and one of his driver friends.

### DEVELOPING THE FIDDLE YARD

My thoughts turned to the urban areas of London served by the Southern, to make a contrast with Kent on Capel Hill. The centrepiece of the new layout is described in the panel on p683.

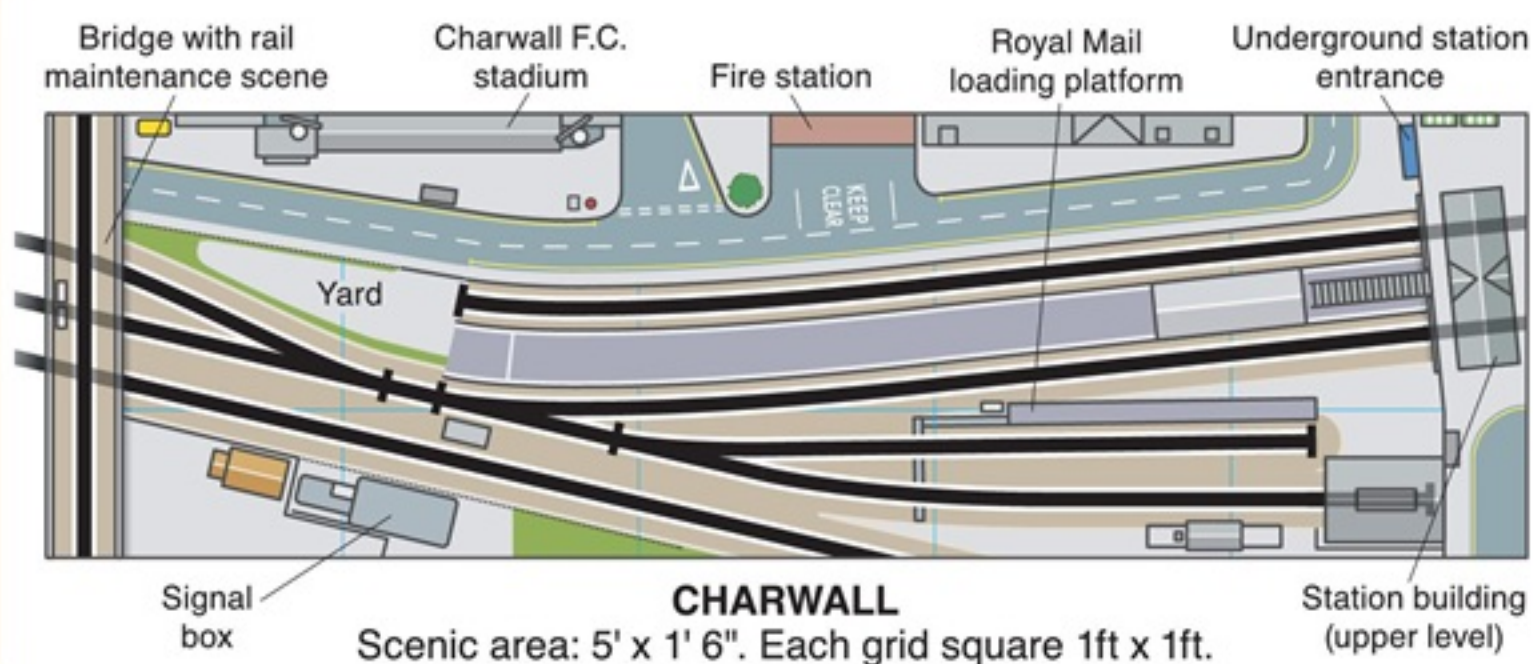
The street scene features a fire station with a nod to TV's London's Burning, and a couple of Metcalfe shop kits, with names changed to suggest the era, eg Tandy's. The scenic break adjacent to the football stadium features an entirely scratchbuilt bridge with signal gantry, and shops underneath the arches as is typical of the location. Over this runs a dead line that is purely for show depicting a rail maintenance/trackwork scene.

◀ The scenic break between the two sections of the layout features a disused single line for display purposes.

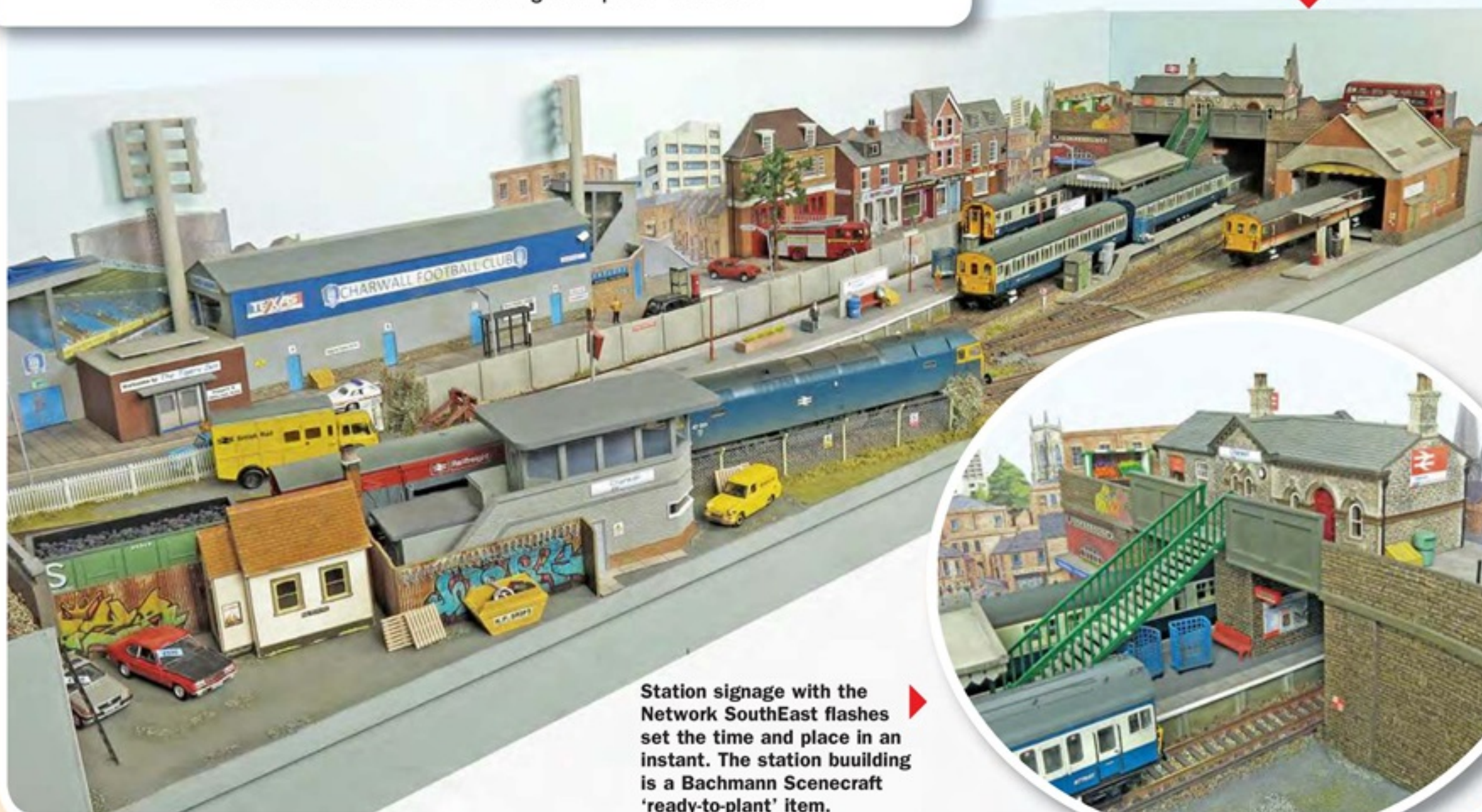
Next to this is a small corrugated iron fence-enclosed used car dealership (Dodd and Gee Car Sales), where you would get anything but a good deal!

The depot (South Thames TMD) is a heavily modified Superquick kit with NSE signage. All buildings on the layout, as with the original Capel Hill are fully removable and interchangeable in order to make the layout more interesting. To complete the scene, I have added third rail that is constructed from strips of Peco SL-100 flexible track and coloured darker than the running rails.

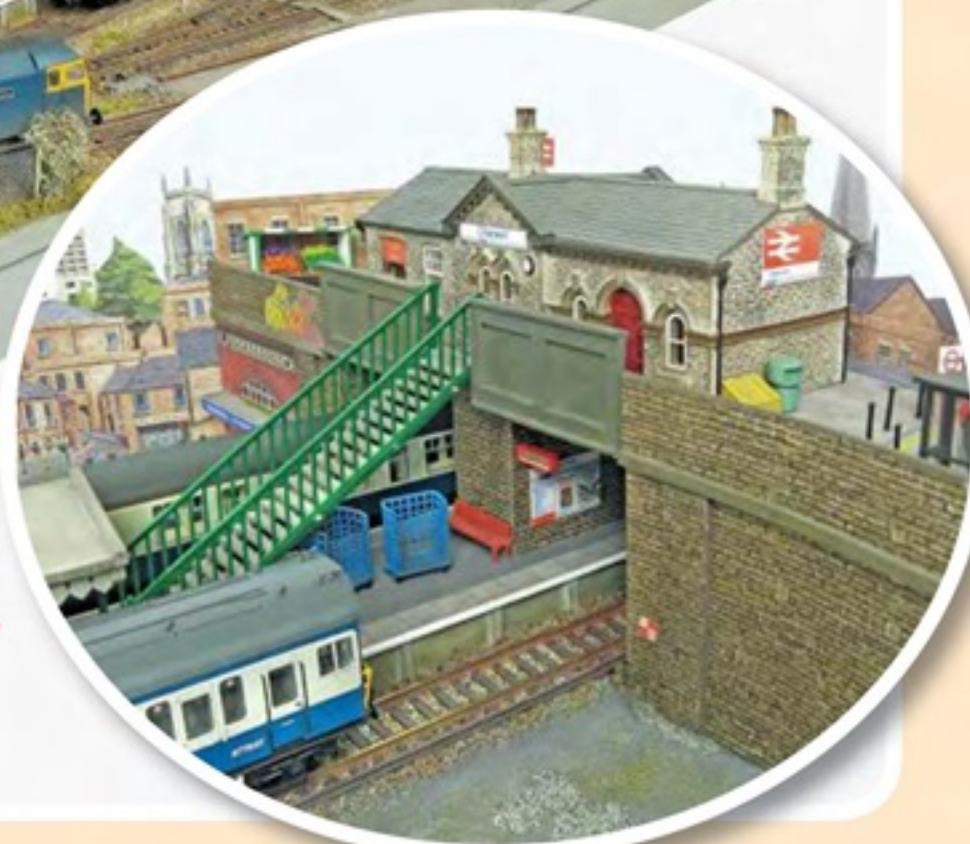
Operation of the layout is via the same 12V



Overall view of the layout, with the stadium as its centrepiece.



Station signage with the Network SouthEast flashes set the time and place in an instant. The station building is a Bachmann Scenecraft 'ready-to-plant' item.







▲ The Motor Luggage Van enters the shed, running off-juice using its batteries for power.

dc Bachmann analogue controller used for the Capel Hill layout, and this section acts as a fiddle yard for the Charwall section, and vice versa.

Track workers are assisted in their task by a Plasser OWB10 crane (available from the Bachmann range).

▼ The fire brigade's Dennis appliance stands outside the station. Classic London black cabs abound.



## South London soccer rivals...

With the blank canvas of the fiddle yard for Capel Hill available, I decided to make best use of it and create a small South London terminus with as much 1980s scenery detail as possible to 'take me back'. I decided that this layout would feature a station, depot and something that is not found on your everyday layout. I opted for a football stadium. This was entirely scratchbuilt from backing card with the floodlights formed from

balsa wood. I had no idea what to call the team that played their home games there, so due to the location I took the 'Char' from Charlton Athletic, where I was a season ticket holder for many years, and the 'wall' from Millwall to make Charwall. A bit odd to combine the two, I know, as they have long been rivals! But I like the name and naturally the station name became Charwall too.



## FUTURE THOUGHTS

My plans for this layout are developing all the time. I intend, at some point to build some temporary 'put up/take down' boards in order to create a full loop so as to be able to run trains continuously. This would however involve a lot of modification to the Capel Hill station. As things stand I am happy with what I have, but I am always looking to progress and improve my skill level. Watch this space!



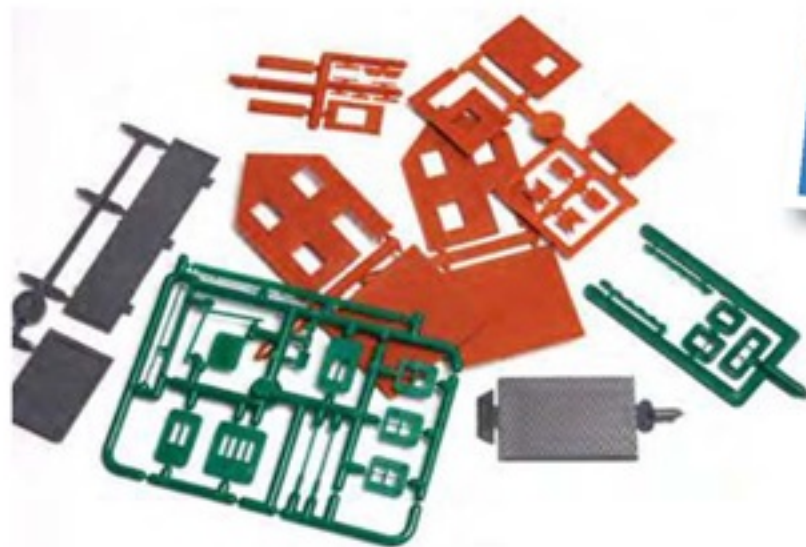
## Modelling a country pub in N

**CRAIG TILEY** shows how parts from the Peco Manyways Station House plastic kits can be adapted to create a representation of a classic country public house.

The N gauge railway modeller is well served with plastic kits for structures found inside the boundary fence – such as signal boxes, station buildings and goods sheds etc. – but period civilian properties are a little less readily available in this medium.

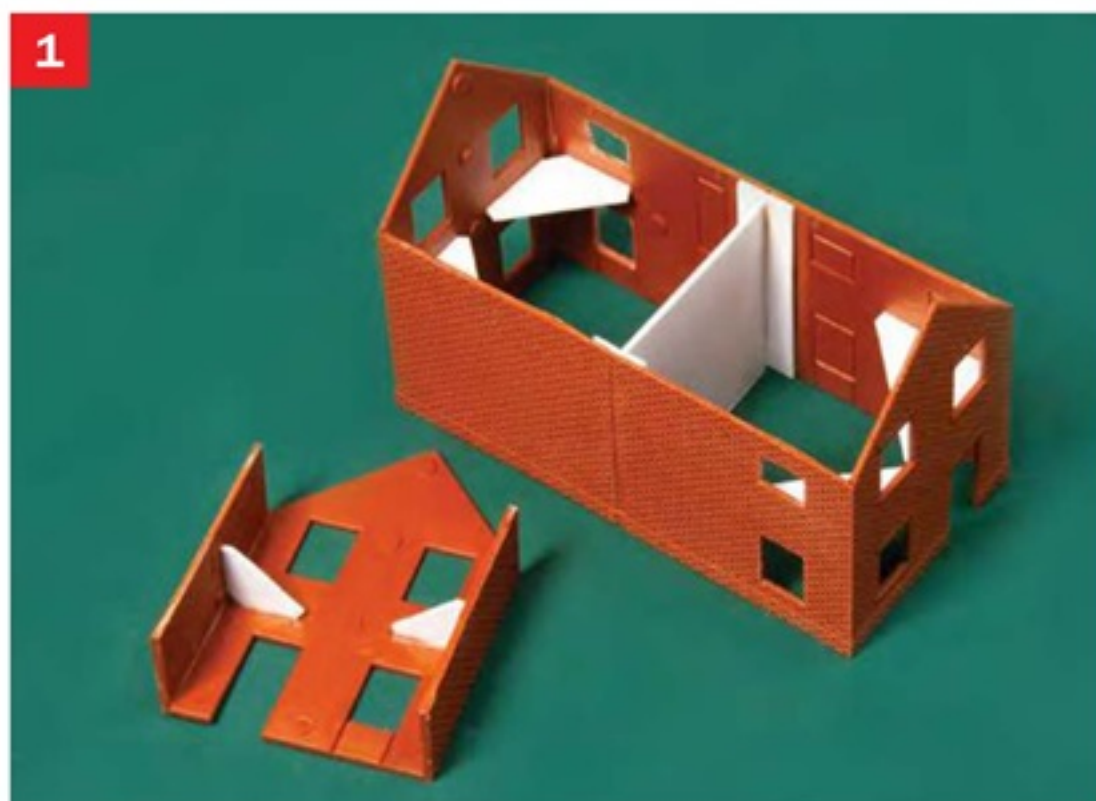
One option is to look at the ranges of card and downloadable kits (together with various 'ready-to-place' resin items) where models of domestic housing, shops, churches and public houses etc. are all readily available. But integrating models on a layout that have been constructed using different materials isn't always straightforward; the lack of surface relief on a printed card kit becomes much more apparent if placed alongside a plastic kit with moulded surface detail, for instance.

An alternative is to look at the plastic kits that are available, and adapt them to suit. Such is the basis of this project, where a representation of a classic country pub was created by adapting parts from Peco Lineside Station Houses kit packs (ref.NB-14, x2).

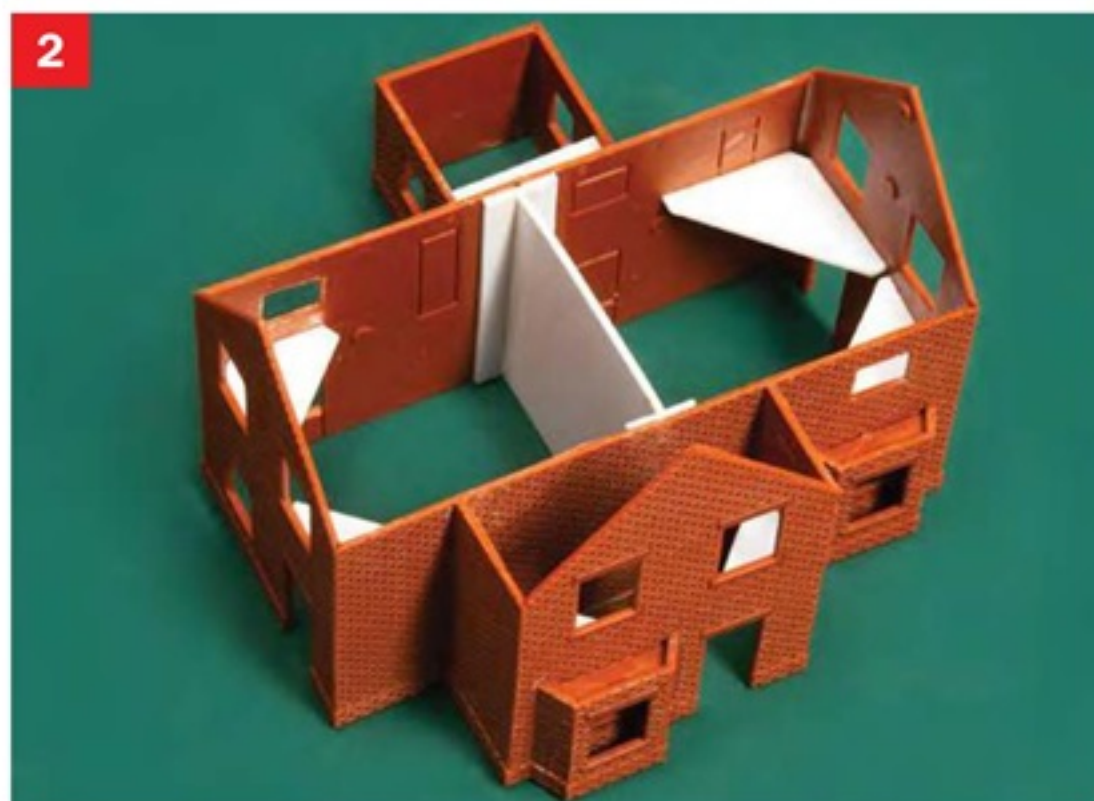


◀ A representative selection of the different mouldings supplied in the Peco kit (ref.NB-14), sufficient to construct the pair of houses as illustrated on the packaging. Parts from two kits were used to create the pub model, with plenty of spare components left over for use on future projects. *Model photography by the author*

### Making the model



1 The main pub structure was formed by combining together parts for two station houses, creating a footprint of 73mm x 36mm. An extension to be attached to the front wall of the pub (pictured face down) was formed using a gable end moulding and narrow wall sections cut to 14mm depth. Note the internal bracing pieces that have been added using 40thou styrene to ensure the assembly remains rigid and square.



2 Two bay windows were added to the front wall and an extension was attached to the rear wall. Note how the front and rear extensions are positioned to cover the joins in the main walls of the structure. Note also the part-cut apertures visible for alternative window and door positions. All of the assembly seen in this picture was completed using liquid polystyrene cement, applied using a small fine brush.





Painting is best undertaken as construction proceeds; a mortar wash was added to the external wall surfaces of the ground floor using Humbrol cream enamel. This colour was also used to pick out the sills and lintels on the doors and windows. Internal walls and bracing parts were given a cursory coat of dark grey/brown to ensure they wouldn't be visible through the windows of the finished model. Once dry, 20thou styrene was used to cover the upper storey of the buildings. The picture shows how the sections for the gable ends were marked out together, before cutting and fixing in place.



Windows and doors were added next, these being pre-painted with enamels whilst still attached to the sprue, and then glazed before fixing in place on the model. Pre-painting these items in this way is so much easier and ensures that there is crisp separation of colour between the frames of the doors/windows and adjoining brickwork.



Strips of 1.5mm black paper were used to form exposed edges of the beams on the upper storey. The thickness of the paper (c.90gsm) provides sufficient relief in 2mm scale. The strips were fixed in place using tiny spots of Deluxe Materials Roket Card Glue. The sections were attached over-length, and then trimmed back by running the blade of a scalpel around the roof line.



The roof parts were added next. Each half of the main roof was created by joining together two mouldings from the kit, trimmed slightly to fit between the gable ends. The sections for the front extension were then cut to size using trial and error, gradually removing material until a satisfactory fit was achieved. Chamfers were added along the diagonal edges to ensure the joins with the main roof were as neat as possible.



Flashing was added along the diagonal joints using strips of masking tape. Ridge tiles and a chimney were also added at this stage, with the roof then painted a dark grey colour.

8 A light grey-brown enamel wash was dry-brushed over the roof to give a weathered appearance and define the surface detail of the tiles. Barge boards, guttering and downpipes were then added (these items also having been pre-painted prior to fixing in place). Suitable signage was created on computer and printed onto paper; the pub name sign was attached by folding the paper over a section of fine brass wire, which was then glued into a hole drilled in the wall.





## A narrow gauge water tower

**MICHAEL FARR** shows how this lineside structure can be fabricated from scratch using just basic materials.

The structures for The Skarloey Railway (see *RM* June 2019), were all constructed using straightforward techniques and readily available materials. These structures include a water tower, based on the example that stands at Dolgoch station on the Talyllyn Railway. Despite its rudimentary appearance, this item of lineside infrastructure is an iconic feature of the Welsh narrow gauge line, having appeared in countless photos over the years.

The water tower was filled from a stream high up on the neighbouring hillside. The base, which has a distinctive taper, is of dressed slate construction, atop which sits a wooden water tank. Drawings for this structure were published in the July 1965 issue of



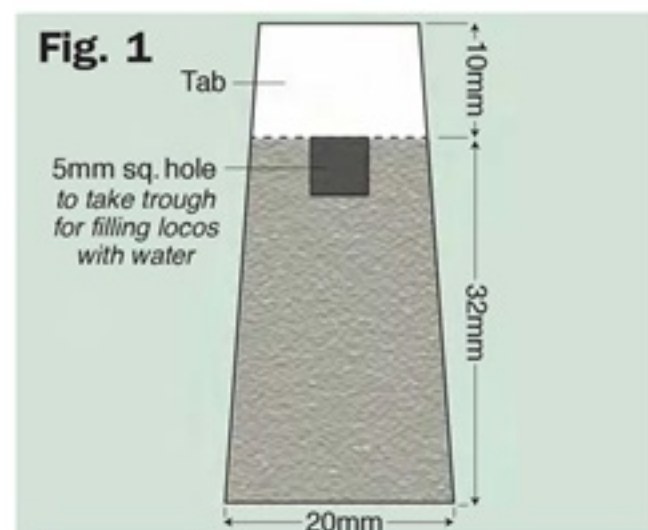
Photograph: Steve Flint

*RAILWAY MODELLER* as part of an article timed to coincide with the railway's centenary.

These drawings were used to create a template for one of the sides of the tower base (Fig. 1), which includes provision for the aperture through which the water trough was supported.

Construction of the model then proceeded as described below:

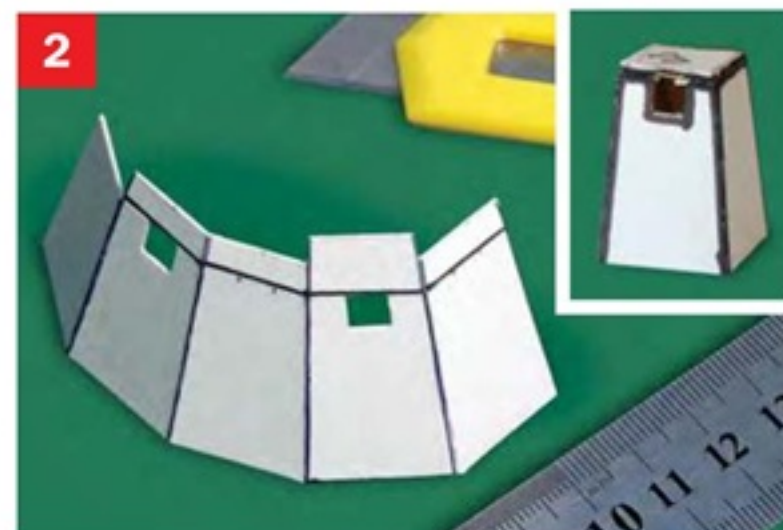
### A water tower in 009



A template of one of the sides for the base of the water tower, reproduced here at 4mm:1ft scale.



The template was used to mark out the whole of the tower base onto a sheet of card (recycled from a cereal packet), which included gluing tabs for assembly.



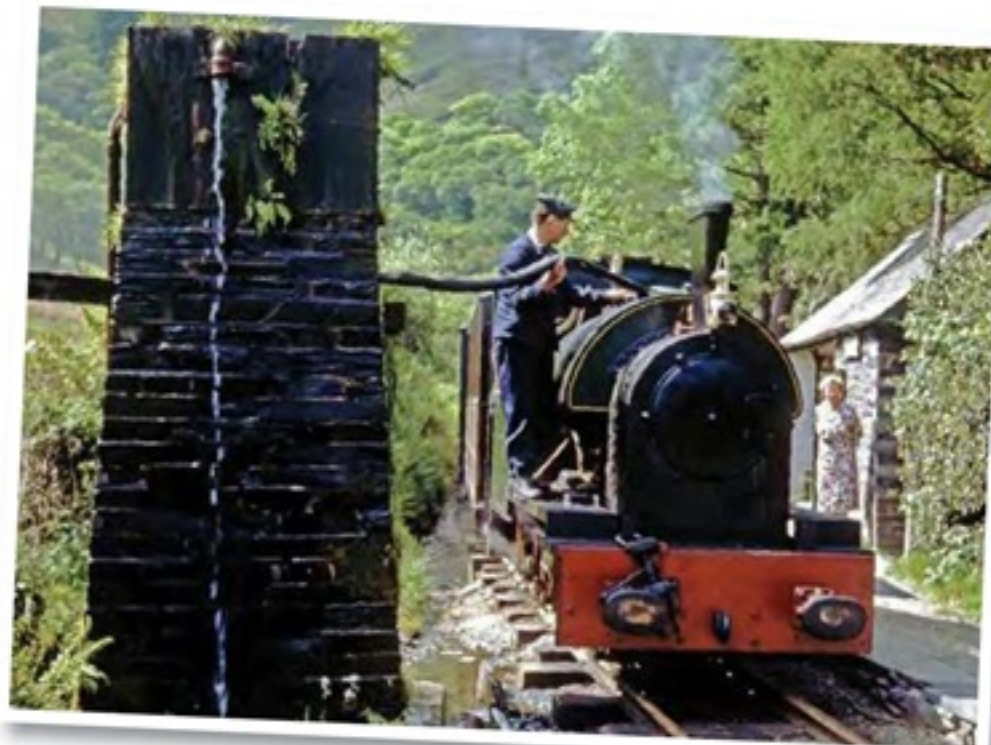
After scoring the fold lines and then cutting the parts from the sheet, the base was glued together using PVA (inset).



The water tank was fabricated from planked styrene sheet, with the operating linkage formed from scrap wire and section of pipe from waste plastic sprue. The grain of the wood was simulated by rubbing coarse glasspaper across the surface, before colouring using a brown felt tip pen.



With the tank and base fixed together, the base was covered using Redutex self-adhesive texture sheet; random slate was used in place of the prototype's dressed slate sheet, which wasn't available. Finally, the trough was folded to shape from card and inserted through the base.



▲ Talyllyn Railway No.4 *Edward Thomas* (complete with Giesl ejector) has its tanks replenished at Dolgoch station on 30 May 1962. Photo: Michael Farr



### John Rodway



### Simply to support the paint...

“Is there a self-appointed hierarchy amongst railway modellers? Do those who scratchbuild from metal sheet, rod and bar consider themselves superior to those whose starting point is an etched brass kit? Do those who solder their components together think they are somehow better modellers than those who use adhesives and solvents? And what about those whose material of choice is wood, ply or even card? Are they floundering at the bottom of the league table?”

Metal provides strength where it's necessary without excessive thickness. So sheet steel on the prototype is represented by scale-thickness brass on the model. But scratchbuilding requires both a well-equipped workshop and technical expertise with a wide range of specialist tools. Etched brass kits remove the need for accurate scribing and cutting of every component, though some shaping, forming and fettling may still be required.

Are models made from plastic sheet and section somewhere in the middle of the status table? Do plastic kits occupy the lower third, having inherited a reputation for being cheap-and-cheerful – a hangover from the early offerings in the 1950s and 60s? Even though present-day injection-moulded models and kits are far superior, better researched, more ingeniously designed, and highly accurate, there are still those purist modellers who look down upon them and the modellers who use them.

Can you guess what this 1:16 scale model of a North Staffordshire Railway Class H1 0-6-0 No. 92 is made from? Long standing readers of RAILWAY MODELLER may recall that it is the work of John Heywood who built numerous large scale static models out of marine ply, card and plasticard during the 1980s and 1990s. Wheels came from the discontinued 3½" gauge Stephenson's Rocket model, and the boiler was produced out of a piece of plastic drain pipe. Although unmotored, it would be possible to add a tender drive mechanism to propel the model, if anyone so wished.

Photo by Ken Ratcliffe

Ply and solid wood are accepted as suitable building materials in the larger scales, especially for garden railways. However, is there still a certain amount of snobbery amongst the *cognoscenti* if they are used for O gauge or smaller?

And as for locomotives, carriages and wagons made from card – are they not beyond the pale? No real modeller would use card, would they? That's kiddies stuff, isn't it? It's cheap and readily available, often as scrap. Surely the modellers who use it must be poor in terms of finance, skill and aspiration?

However, when a well-built, detailed model is put on a layout, it's not always possible to tell, by just looking and watching it run, from what materials it has been constructed. Each class of material requires its own particular tools and skill-set, together with an understanding of its physical and working properties. Of course there is some overlap. Irrespective of the material being used, the common abilities are manual dexterity, good hand-eye co-ordination, and excellent spatial awareness, together with a willingness to discard any item that turns out to be sub-standard or inaccurate.

But does it really matter from which material a model has been made? Should the craftsman's choice affect the esteem with which he and his models are held by fellow modellers? This begs the question: What is the purpose of using a particular material?

When we look at a finished model we don't see brass, plastic, wood or card. All we see is a film of coloured pigment. The underlying structure is just there as an invisible, stable, correctly shaped and smooth surface on which to lay down coats of paint. It must also provide mechanical support for the wheels and motors. Does it

***This begs the question, what is the purpose of using a particular material?***

matter from what the structure is made provided it fulfils the twin functions of contributing form and providing strength?

Those we might call the 'Material Purists' may take great pride in making the bulk of their models out of metal, or from plastic, or in card. They may insist that their material is the only true modelling medium and all others and their users are inferior. They

certainly spend inordinate time and ingenuity proving this by fabricating components that might be easier and quicker to make from a different material – maybe even 3D printed

parts. Some see it as a challenge to make almost everything from their favoured material, be it locomotive chassis out of plastic, tapered boilers out of card, or coupling rods from laminated brass. To them, achieving success is a source of great personal satisfaction, and they are certainly worthy of recognition and celebration by others, irrespective of the material used.

But there's another way of considering the choice of materials – matching their properties to the task(s) each component has to carry out, and then selecting the best for each particular application, whether this is providing strength under stress, or its ease of cutting, delicate shaping and accurate bending, or the provision or reduction of weight, but above all, their ability to take paint and leave a smooth finish.

The choices made could result in a hybrid model, where card, wood, plastic and metal are all used, unified in a single purpose. The materials are there simply to support the paint. Surely that determines what should be at the top of the league table? Paint.

***John Rodway is a freelance technical editor and models mostly in British outline O gauge.***





# Readers' Letters

## Operating Broad Aston

It was a pleasant surprise to find the article *Whatever Happened to Broad Aston* in the latest issue (July 2019) by Howard Burchell. Whilst I did not read the original article when it was first published, I caught up with the issue a few years later.

Howard ran the school model railway club where I was educated, and I have been privileged over the years to both visit and operate *Broad Aston* on a number of occasions over the years. It is truly a magnificent model both to view and equally importantly to run. It has been designed and built with operation in mind and certainly many a happy hour can be spent working it just like a real railway. It was good to read his no nonsense approach to ensuring that trains were reliable and when they were not, the root cause is fully inves-

tigated and rectified; I still clearly remember this approach on the extensive school layout.

It was good to catch up in print, the latest developments at *Broad Aston*. Howard is a true inspiration having encouraged me into the great hobby of railway modelling and ultimately into my career in our wonderful railway industry. Having avidly read the latest update, *Broad Aston* (and Howard) continues to be inspiring.

Well done and keep inspiring.

**RICHARD ELWEN**

**For the record, we misidentified the Hall in Howard's article as No.4916; Howard was kind enough to point out that the loco is actually the far more apposite 4986 – Aston Hall of course.**

**Photograph: Rebecca Flynn**



## Outlets for children's creativity

As soon as I heard about the devastation [suffered by Market Deeping MRC – Ed.], I added some specific notes and links from our *Templeford* webpage, which seemed particularly appropriate for several reasons. These were that *Templeford* (see Talking Points, RM November 2018) can be regarded as a continuation of our own childhood railway experiences; that we do endeavour where appropriate to encourage children to participate (despite the risks of some damage as happened to one of our hand-held controllers recently); that Market Deeping is only a matter of 14 miles from where we are going to exhibit at Spalding; that railway modelling is very much in the ascendancy again as has been demonstrated by the success of the Great Model Railway Challenge; and that children desperately need some sort of outlet or out-

lets for their own creativity.

I think back to how lucky we were, that we had a dad who was so practically minded and who did not stand in the way and even encouraged us with our various hobby experiences; how our parents coped with having a five-piece rock band practising in their front room; and how every church had its own youth club which helped to keep aimless teenagers off the streets – and also gave us venues in which to take a budding rock band!

So the bottom line is that I think that even we do not fully realise just how special *Templeford* is, not just to us but to the model railway-minded people of the world, as it represents one of the most important starting points into the entry into the wonderful world of railway modelling.

**CLAIRE SHEPPY**

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

## Hammant & Morgan forever!

Getting behind with my reading of *RAILWAY MODELLER*, I have just been going through the April edition again and came across John Henstock's letter on feedback and coreless motors. On my layout I run locos with motors ranging from Hornby tender drive units through Portescaps to Maxxon coreless. I gave up on ECM and Compspeed years ago in favour of an H&M variable transformer controller. Despite its age (AD 1956) it performs faultlessly with all these different motor types and, in the event of a short-circuit, the cutout comes out with a loud click, a great advantage over the silence of the digital device.

Only once have I had to replace the rectifier and the carbon brush. It's not semiconductor or digital, I grant you, but perhaps it is time to re-invent this most reliable form of analogue control.

**CHRISTOPHER KYAW**



*Ed – not only is there a treasured H&M Duetta in the RM Office for occasional test running, but this one was spotted in the modelling room at Fawley Hill during the recording of Series 2 of The Great Model Railway Challenge...*

## Capability challenges in modelling

Like Philip Back (Letters, July) I too suffer from Essential Tremor. I showed early symptoms when I was 10 but was really significant by my 30s. Now at 66 it has become a major hindrance in spite of years of medication, such that I am being assessed for neurosurgical intervention. It increasingly limits the modelling projects I can take on – I have been in N gauge since 1976. My current exhibition layout, *Allanbrae*, is relatively minimalist in concept as a result, compared to *Abersoch* (*RAILWAY MODELLER* August 2003) or *Portpatrick Town* its successor, now dismantled after 12 years on the circuit. But I am determined to carry on even if my efforts become simpler.

**COLIN HEATH**

Phil Back has voiced exactly my thoughts: as we age, so we start to reach the stage where arthritis, bad backs and dodgy eyesight kicks in making delicate tasks a bit trickier than before. In my case I looked forward to retirement after a strenuous life (in PSV bus maintenance) and to relaxing and 'slowing down.'

For me small layouts, ready-to-run, and a little secondhand collecting, is

the order of the day.

I just do a bit at a time, when I feel like it, and I avoid the things I don't enjoy. In closing, thanks in advance for the new small layout feature, as a staunch small layout person! I hope a track plan is included in each case though.

**M HUNT**

I have just read Phil Back's letter in the RM and sympathise with his problems. No longer wishing to crawl around under baseboards to install wiring, I now do all the wiring on the surface, then hide it under the (expanded polystyrene) scenery. The obvious disadvantage of this method is that the wiring is no longer easily accessible for repair or modification, but so far (touch wood!) no problems... Just a thought, which may suit some others.

**ANTONY HARVEY**

[Ed. – Older readers may remember Christopher Trace on the BBC's *Blue Peter* programme building scenery in the form of removable modules. By adopting this method, any board-top wiring can be hidden easily and accessed quickly should the need arise.

## Catalogue your collection

Further to the snippet in the July issue 'Don't leave your collection in a state' from Darryl Foxwell, I too have considered this problem for myself.

I have set up a list of all my 'stock' with photographs of each item, their cost and a detailed description which includes the name of the manufacturer and its catalogue number.

The list refers to any box that may

be available and these are stored separately. Each item in the list is given a reference number and the respective number is affixed underneath of the corresponding item to enable quick and easy identification.

It was a lengthy task but once done it is easy to add to if there are new things bought.

**DAVID BECK**



## Making white-water streams thanks to *Fairhaven*

I read with interest in the July issue of RM, the account of *Fairhaven* built by Isobel and David Fairgrieve, in particular Isobel's method of constructing the harbour water using soft toilet tissue. I too discovered the YouTube videos by marklinofsweden, so decided to use his methods as they gave a much longer open time to manipulate the water effect than plaster.

A mountain stream flows over rapids and under a viaduct before cascading over a weir. The river bed is constructed of cardboard packaging so I sealed it with two coats of black acrylic before applying the first coat of toilet tissue. The now translucent tissue revealed the black paint underneath which gave me an idea to simplify the colouring of the water. After it dried, I painted the first layer with all the colours of the various depths.

Once dry, I applied three further layers of tissue and stippled these with a dry shaving brush, thinning out the deep water areas whilst leaving thicker



areas where water would foam and appear white. At the bottom of the weir, deep foam was achieved by rolling wet tissue into a sausage shape and blending into the base layers.

Splashes around rocks and banks was achieved by applying dabs of cov-

ing adhesive. When dry the toilet tissue reverts to its opaque finish.

When the grass and foliage have been applied I shall finish the water with a coat of moulding resin. The photo shows progress so far.

DAVE MAUNDER

## Maunsell Q Class 0-6-0 and other R-T-R possibilities

In the book review on page 608 of the July RM it is mentioned that the Southern Q Class has not yet been made in R-T-R form, which puzzles me as the 4F chassis is fully applicable in regard to its coupled wheelbase and with only a 2" difference in wheel diameter. Given that Hornby and Bachmann both have a 4F in their range it seems an opportunity is being missed!

The same could be said for the Midland 2F, a 'limousine cab' Fowler 4MT tank, the Standard Class 2s and 3MT mogul, all of which would use existing tooling to some degree.

There are more such opportunities to exploit from existing tooling but I wonder if the manufacturers are possessed of the broader enthusiasts' knowledge needed to appreciate it?

DAVID HANMER



Who'd have thought it – the birdcage coaches are available ready-to-run but the loco isn't! Q No.30543 shunting ECS at Oxted, June 1953. Photo: Rail Photoprints

## Models sold out prior to advertising in magazines

A manufacturer announces a new product (in this case an SECR box van from Rails of Sheffield) advising that it is a limited production run. When it is eventually advertised and highlighted in the model railway press, I phone to order one only to find that reservations for all the stock have been allocated and no more are planned. You can therefore imagine my mild irritation to be told in an almost gleefully condescending way (I rang three separate times to be sure) that the ECSR box van was sold out even before the ads appeared.

Very entrepreneurial as Arthur Daley would say, but it leaves lots of people disappointed.

PETER NORTH

[Ed. – Several other readers have mentioned this issue so we asked Rails of Sheffield to comment. A spokesman for the firm responded as follows]

The decision to take the bold step to try to produce a niche, low volume, never previously produced R-T-R product, utilising a new cutting edge technology and materials was always going to be a steep learning curve for us.

Many hours of research and development of the SECR van, included the optimum production run to make the project viable. Only when this was settled was the project announced and the production contract signed. At that point we had no idea how it might be received by customers or how strong the demand would be. It had not been done before.

Promotional material was produced at the same time as the announcement, including a magazine advert. There is approximately a four week lead time on magazine adverts before publication. When the advert went to press the van was still available for sale. Due to an overwhelming response in pre-orders we gratifyingly hit our optimum run number very quickly and therefore soon after the magazine advert appeared the vans had sold out. This was not our intention but unfortunately reality.

We announced at the launch that with this first project we were "proving the process". The van project is now in the final stages and is almost complete. There is a possibility the same van or alternative versions will be pro-

duced again in the future, however we first need to evaluate the whole process and method.

Our apologies to anyone who missed out on the van, we would love to supply everyone and hopefully in the near future this will become possible.

As with all of our exclusive models, pre-ordering is highly recommended to secure your model.

A big thanks to everyone who did pre-order the van, and as I write this, production is in the final stages, with delivery to customers imminent.



This SECR/SR box van model in OO was announced as a limited run 3D printed product by Rails of Sheffield during the York Show last Easter.

## Snippets...

### Class 74 in N

Regarding the Plan of the Month on Bournemouth Central (July), it is stated that there is no N gauge Class 74 available. A1 Models produced kits for both the 74 and the 71; I have both running on my layout. They were fitted to Dapol Hymek chassis. Other EMUs were made from the Taylor Plastic Models inlays to fit the original Graham Farish Poole-era Mk.I coaches.

KEITH MATHEWS

### Chalford help

I'm in the process of building a layout based on the GWR station at Chalford, but I'm missing photos of the horse landing area, the timber shed, the permanent way hut, wood store and engineers cabin. If anyone can provide photographs of the areas concerned, I'm happy to cover the costs for these.

NIGEL ROBERTSON

ncrobertson@btinternet.com

### 'Ben' location

It was very good to find two substantial HR-based articles in the July issue. If I may shed some light on the 'bit of a mystery' (page 578) about the mountain after which Ben-y-Gloe was named, this usually appears on maps in its purer Gaelic form of Beinn a' Ghhlò. It refers to a range of peaks, the highest being Càrn nan Gabhar at NN 971733.

In this and other cases the Highland Railway seems to have used popular phonetic versions of hill names rather than those on Ordnance Survey maps.

IAN WALTER

### Two eras

Yet another great issue of the RAILWAY MODELLER, but one that may prove expensive. Like most modelers I have built and bought too much rolling stock. However, there is always an excuse to create and buy more.

This inspiration came from Blair Atholl to Druimachdar where Howard Geddes runs Highland and LMS stock: why not, I reasoned, change the stock on my own BR(SR) 1950s inspired layout! BR Blue Cromptons and EDs at one end, back to LSWR liveried Adams Radials and other pre-Grouping stock. Brilliant idea! Where will I be able to store it all...?

SIMON JAMES

### No trains today

Cracking prototype photo of the Class 90 in the July RM (page 603). I made a visit to the exact same location in May on a motoring holiday to find that whole section of the WCML closed under a possession. All weekend. Bowled!

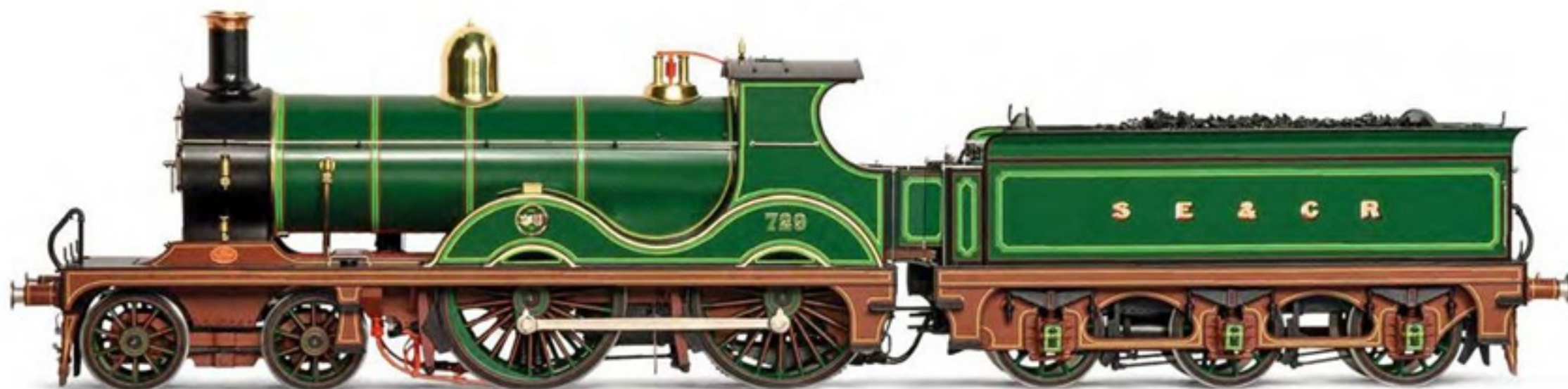
E COURT



FROM **RAILWAY MODELLER** BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

## Wainwright D Class 4-4-0

### Brand new from Lee Marsh in 7mm



A question – was there ever anything, in all steam locomotion's myriad forms, so graceful as the Victorian or Edwardian British inside-cylindered 4-4-0? And of those 4-4-0s, surely a place can be reserved on the podium for Harry Wainwright's D Class, which entered service on the South Eastern & Chatham Railway in 1901 in the spectacular form seen here, as modelled in 7mm scale by Lee Marsh Model Co: transportation as an art form has never been so effective.

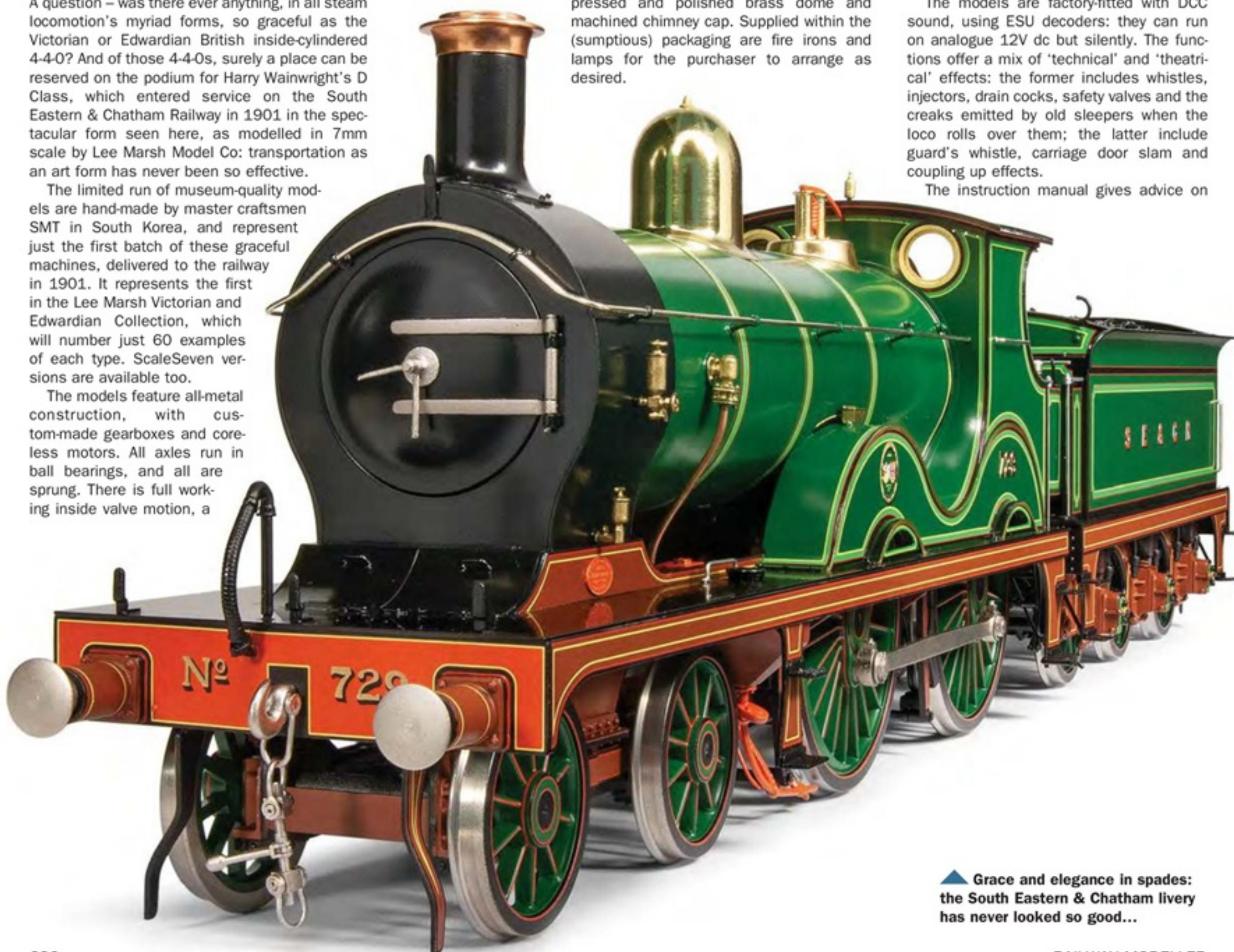
The limited run of museum-quality models are hand-made by master craftsmen SMT in South Korea, and represent just the first batch of these graceful machines, delivered to the railway in 1901. It represents the first in the Lee Marsh Victorian and Edwardian Collection, which will number just 60 examples of each type. ScaleSeven versions are available too.

The models feature all-metal construction, with custom-made gearboxes and coreless motors. All axles run in ball bearings, and all are sprung. There is full working inside valve motion, a

pressed and polished brass dome and machined chimney cap. Supplied within the (sumptuous) packaging are fire irons and lamps for the purchaser to arrange as desired.

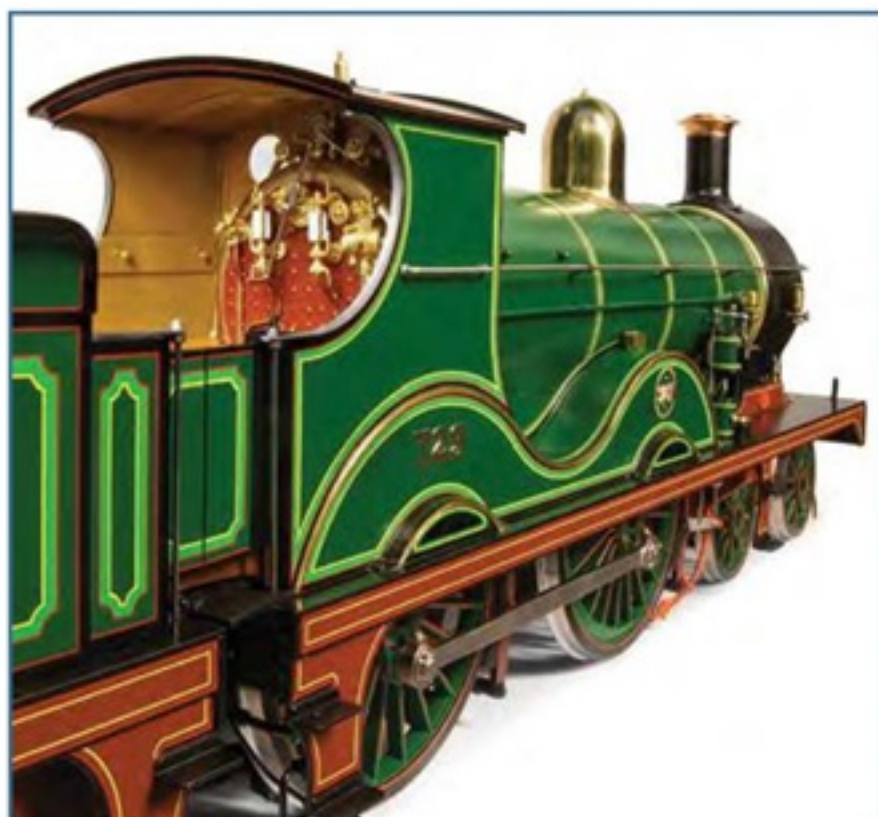
The models are factory-fitted with DCC sound, using ESU decoders: they can run on analogue 12V dc but silently. The functions offer a mix of 'technical' and 'theatrical' effects: the former includes whistles, injectors, drain cocks, safety valves and the creaks emitted by old sleepers when the loco rolls over them; the latter include guard's whistle, carriage door slam and coupling up effects.

The instruction manual gives advice on



▲ Grace and elegance in spades: the South Eastern & Chatham livery has never looked so good...





## VINTAGE VIEW Wainwright D Class 4-4-0



▲ One of the seven Sharp Stewart machines to make it to British Railways ownership of the 10 built in 1901, No.1733 is seen in Southern Railway days with a train near Ticehurst Road station (renamed Stonegate, 16 June 1947) on 1 August 1938.  
Photo: J H Venn, Mike Morant Collection

### Prototype history

Penning by Robert Surtees with oversight by Harry Wainwright, 51 D Class locomotives were built both at Ashford works and by several private builders between 1901 and 1907. They were part of Wainwright's standardised fleet for the South Eastern & Chatham, such as the C Class 0-6-0s and H Class 0-4-4Ts. Between 1921 and 1927 21 were modernised by Maunsell as the D1s; British Railways inherited 28 of the Ds in 1948. Withdrawals proceeded apace through the 1950s, and the last ones were taken out of service in December 1956. Ashford-built No.737, the last of the first batch of machines as modelled by Lee Marsh, was taken into the National Collection in 1975; it remains on static display at York in its magnificent SECR livery.

building up these effects to create a fuller aural experience, including the use of the 'coasting' mode (F5). Our sample's suite of effects was excellent save for the 'chuff' which didn't convince, strangely.

The model's *pièce de résistance* is of course its matchless finish. All the subtle nuances of the livery have been captured to perfection, down to the axle-box lettering and the lining on the steam reverser. The works plates and company crests are, naturally, first class.

It might be argued that to have these beautiful but expensive items in a mainstream magazine is overkill, but – in the manner of an Italian supercar in a motoring magazine – they deserve their place if only to remind ourselves of the capability of those who created them; their skill and patience.

As a salute to the folk who designed, built and operated the real things of course, and to the pre-Grouping railway period itself, the Lee Marsh D Class is without peer. We cannot wait for the next one in the collection!

Tim Rayner



▲ Within the smokebox is the 8-pin decoder socket; a blanking plug for 12V use is supplied with the model.



### Sample loaned by

LEE MARSH MODEL CO,  
Unit 5, Lancaster House, 9 Moss Way, Hillend Industrial  
Park, Dalgety Bay, Fife KY11 9JS.  
[www.leemashmodelco.com](http://www.leemashmodelco.com)

PRICE £2,850.00 including VAT



## Bulleid 59' gangwayed coaches brand new from Hornby

00

As announced at the Hornby press briefing at the end of last year, the SR/BR(S) Bulleid-designed 'shortie' 59' coaches are now available, in the SR malachite green livery of Maunsell's successor, as here, and BR(S) green. Hornby has followed faithfully the Southern's penchant for running coaches in sets, so the two brake thirds which will be available to sandwich the composite seen here are consecutively numbered, making the three coaches properly Set 965.

The reason for the shorter length dates back to a carriage building programme Bulleid initiated in 1938, taking as his cue the many fleets of Maunsell 59' coaches then serving the Southern's three divisions. An order was placed with Lancing works for 54 underframes (57'11" long), but the war intervened – and with it a prohibition on new coach construction, especially all-steel vehicles. The underframes were stored until 1944 when, with victory in sight, the rules were relaxed sufficiently to allow the vehicles to be completed



(between November 1945 and April 1946), which were 59' over the ends of the bodies. The more familiar 64' long coaches (as per the Bachmann examples) came later into the 1940s.

The 54 coaches constructed were formed into 18 three-car sets, Nos.963 – 980; in the early 1960s many were remarshalled to longer formations, or withdrawn from service.

At the time of going to press, the centre composite of the three coach set had been released, giving a fine foretaste of the other types to come. Usefully weighty at 141g, the coach

matches published dimensions very well. Fine details such as the end handrails and toilet tank filler pipes are formed from fine wire and stand proud of the bodywork. Aspects such as the torpedo ventilators being positioned above the corridor – and not over the compartments, as one might imagine to be the case – are correctly reproduced in model form.

Glazing is near-flush, and lettering and fine details such as the FIRST CLASS brandings and seat number identifications are very well printed indeed.

All is fine below the waterline too:



the buffers are sprung, and metal wheels roll the coach smoothly along. Slimline tension-lock couplers are clipped into NEM pockets on sprung self-centring mountings; they can be unclipped and substituted for designs of the purchaser's choice if required: Hornby includes Roco-style close couplers with the packaging.

These superb new models will be a delight to those modelling the last few years of the Southern, and the first 15 years of the BR(S). *Tim Rayner*



### Sample supplied by

**HORNBY HOBBIES Ltd,**  
Westwood Industrial Estate,  
Margate, Kent CT9 4JX.  
[www.hornby.com](http://www.hornby.com)

**PRICE**  
ref.R4882, £47.99

## CIE ballast plough brake vans new from Irish Railway Models

00

The latest addition to the range of indigenous rolling stock in 4mm scale from Irish Railway Models is the pack of two departmental ballast plough brake vans, presented in pristine (and retina-challenging!) 'tunnel yellow'.

Five of the originals were constructed in 1978 for use with permanent way department possessions, running in the same manner as the UK Sharks; hauled slowly behind ballast hoppers (once on site) with the plough deployed to spread the material evenly around the track. Normally a rake of hoppers would be 'topped and tailed' by plough brakes, the blades pointing inboard of the train. Displaced by high output ballast machines in 2010, the last was withdrawn in 2013.

The models come very finely mould-



ed indeed, and match the dimensions quoted on the IRM information sheet supplied in the packaging. The fine handrails are formed from wire, and the mesh on the bodysides are very finely etched. Underfloor detail is impressive, the plough itself being very well moulded indeed. Switchable lighting is fitted: two underfloor switches

control the interior light on/off, and tail lighting on/off, directionally controlled. (The lights are at the end customarily at the extremities of the train, ie facing away from the plough blade.) On 12V dc the brightness increases with the track voltage, rendering the interior detail visible (good) along with the wiring (less easy on the eye). On DCC, once a suitable decoder is inserted into the 8-pin socket, F0 and F1 will control the lighting: the decoder's CVs can be adjusted to give a constant brightness whether the van is in motion or not.

Slimline tension-lock couplers are clipped into NEM pockets mounted rigidly on the underframe. Buffers are rigid too. Provision has been made for the correct 21mm gauge should the builder desire; the brake shoes are in the scale alignment, thus slightly outboard of the wheel treads that are set to 16.5mm gauge. The brake pipes running along one side of the vans is supplied within the packaging and are for the modeller to install.

These distinctive models will look the part on any Irish outline layout set over the last 40 years or so.

*Tim Rayner*



### Sample supplied by

**IRISH RAILWAY MODELS,**  
9 Crumlin Business Centre,  
Stannaway Drive, Dublin 12,  
D12 VH27, Republic of Ireland.  
[www.irishrailwaymodels.com](http://www.irishrailwaymodels.com)

**PRICE**  
€119.95 for two vans.





## LMS 'porthole' coaches now in BR maroon from Bachmann

00



Remarkably, it's been five years since Bachmann released its six varieties of LMS-designed 'porthole' coaches in OO gauge. They were the 57' gangwayed third (D2170); 57' gangwayed corridor first (D2162); 57' gangwayed corridor brake third (D2161); 60' gangwayed corridor composite (D2159); 60' gangwayed corridor brake first (D2168; and 60' gangwayed vestibule first (D2160). Now the stock is available in the BR

maroon livery with which most saw out their days in the 1960s.

All six versions are offered in the scheme, of which we illustrate the brake first (M5066M, Wolverton 1949) and corridor third M13109M – by then classified second of course (Metro-Cammell, 1950). Both wear the restrained yet smart livery very well, there being excellent definition of shades between sides and ends.

The models are supplied with two

gangway cover boards, buffer beam detail – including working scale couplings – and the Bachmann semi-permanent bar couplings as found on its Mk.I coaches, for use if the coaches are to be marshalled in a set rake.

These coaches could be found all over the London Midland Region of BR and beyond, so the new releases will prove very popular with those recreating the ever-modellable 1960s.

Tim Rayner

### Samples supplied by

**BACHMANN EUROPE plc,**  
Moat Way, Barwell,  
Leicestershire LE9 8EY.  
[www.bachmann.co.uk](http://www.bachmann.co.uk)

### PRICES

**Corridor second (ref.39-451) –**  
£54.95  
**Brake first (ref.39-471) –**  
£54.95

## More FIA intermodal container flats from Graham Farish

N

Last seen in these pages in the April 2012 issue, the Graham Farish FIA twin-unit intermodal flats make a welcome return, this time laden with two 45' containers in Asda advertising livery – one of several of the big supermarket chains to move products in containerised loads by rail. Other liveries available are Malcolm Logistics and Maersk Sealand, with subtle differences to the flats.

As before, the flats themselves are

superbly detailed, and can be propelled unladen without fuss. The containers themselves have tiny spigots on the corners, allowing them to be held securely to the flats whilst in motion. Holes at selected locations along each side enable other lengths of container to be carried.

The containers are held in the packaging by slim strips of foam, which should be removed before loading them on the wagon properly. The con-

tainers are very finely detailed, with most lettering legible under magnification. The Rapido-type couplings on the wagons are clipped into NEM pockets, enabling replacement with something neater if required.

As container traffic is a widespread facet of today's railway scene, these flats will be warmly welcomed.

Tim Rayner

### Sample supplied by

**GRAHAM FARISH,**  
Bachmann Europe plc, Moat  
Way, Barwell, Leicestershire  
LE9 8EY.

**PRICE**  
ref.377368, £84.95



## Modernised Warwell low-loader from Oxford Rail

00



In the September 2017 RM we reviewed the original release of the Warwell military low loader from Oxford Rail; here is its modern counterpart, TOPS-coded PFB and riding on brand new Gloucester bogies in place of the

diamond-frame types on the WW2-period models.

The models are supplied unladen; there are suitable items in the manufacturer's Diecast range of 1:76 road vehicles to work as a load if desired:

the legible loading instructions on the side of the wagon will help here!

A resealable bag of brake pipes is supplied with the model for the purchaser to install if the slimline tension-lock couplings are not required.

In short, this Warwell is definitely one for the enthusiast of modern military transportation by rail.

Tim Rayner

### Samples supplied by

**OXFORD RAIL,**  
PO Box 62, Swansea SA1 4YA.  
[www.oxfordrail.com](http://www.oxfordrail.com)

**PRICE**  
ref.0R76WW011, £32.95





Sankey Scenics has expanded its range of destination boards with the sets of GWR and BR(W) boards designed for the Hornby Hawksworth coaches in OO. In all, 38 boards have been produced to suit both periods, on quality photographic card.

The crisply printed sheets cater for a variety of destinations, such as PADDINGTON BRISTOL AND WESTON-SUPER MARE OR PADDINGTON BATH AND BRISTOL, and include

letters for the holder amidships on the body side. Sufficient boards to cover a full 12-coach express are included per pack. The examples shown have been edged with a black felt-tip pen to mask the white card that the boards are printed on; a maroon one would look better of course.

Sankey Scenics can cater for bespoke commissions of these boards, to suit destinations defined by

the modeller; they would make a through coach to the archetypal branch terminus in the west suitably believable if branded, for example, PADDINGTON NEWTON ABBOT AND BRIXCOMBE...

At present these boards are only available in 4mm scale, they can also be produced to other scales. They and the full range of signage can be found on the firm's website.

Tim Rayner



## Samples supplied by

**SANKEY SCENICS,**  
16 Norbreck Close, Great  
Sankey, Warrington WA5 2SX.  
[www.sankeyscenics.co.uk](http://www.sankeyscenics.co.uk)

**PRICE**  
£3.20 per pack. P&P extra.

# Laser-cut paving and street markings from SMS

Scale Model Scenery has expanded its range of laser-cut wood accessories with modular pavement sections.

The straight sections are supplied in a pack of four pieces, 192mm long x 28mm wide x 2mm thick. They are complemented by curved pieces, both representing 120° turns, and 90° corners; each of these come four to a pack. Thus it will be relatively easy to work up these street essentials, which only require painting to finish off.

Kerbs need street lines of course, and Scale Model Scenery has a wide selection of pre-printed street mark-

ings available as self-adhesive details. Double and single yellow lines, box junctions, BUS STOP markings, instructions such as KEEP CLEAR, NO PARKING, LOADING ONLY, are offered in a bulk pack including white direction arrows, cycle lanes and much more – see the website for the full description. Packs are also available with lining only; 1960s period markings are likewise offered, to match the modern ones illustrated.

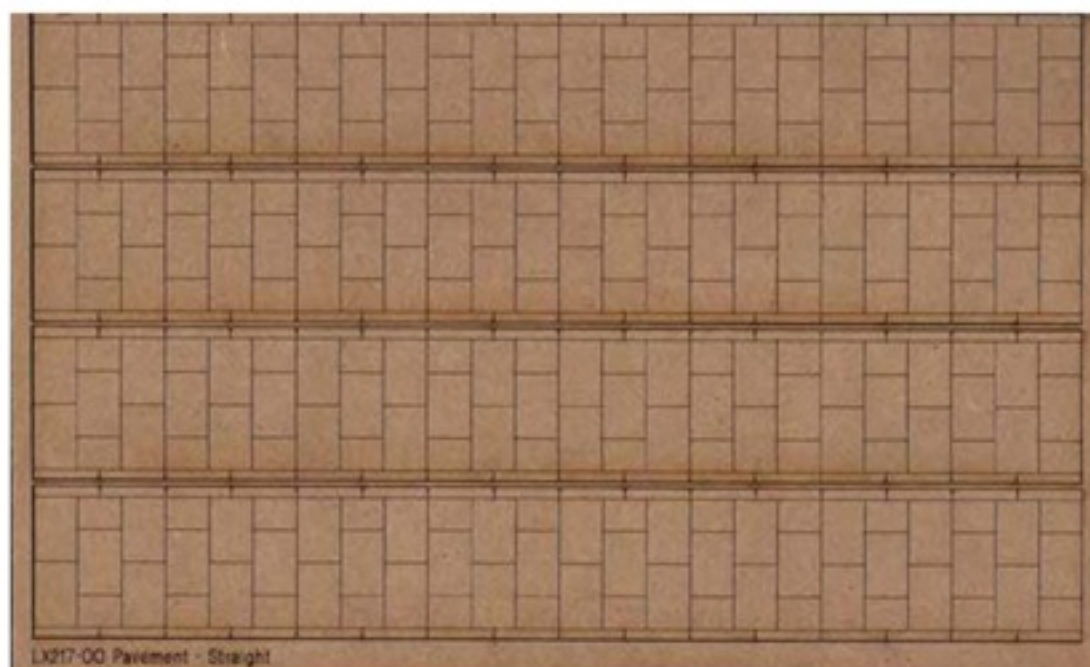
The chief advantage of these sheets is their ease of application: there are no individual letters to have to apply and line up by eye, it being a simple

matter of lifting the desired collection of lettering via low-tack tape and transferring it to the road surface in the required position, and pressing gently to release the tape. Care is needed not to lift adjacent markings – the sheets

are packed quite tightly – but effective results will occur.

Scale Model Scenery has a very extensive range of scenic products, which are being added to regularly.

Tim Rayner



## Samples supplied by

**SCALE MODEL SCENERY,**  
62 Station Road, Hugglescote,  
Leicestershire LE67 2GB.  
[www.scalemodelscenery.co.uk](http://www.scalemodelscenery.co.uk)

**PRICES**  
Pavement, straight – £3.99  
Pavement, curved 120° – £1.99  
Pavement, curved 90° – £1.99  
Street markings pack – £19.99





Osborn's Models has added several 3D printed and mixed-media items to its extensive range of wagons, structures and scenic items.

The Ransomes & Rapier crane is a ready-to-run model of the 45T type, available in GWR grey, SR grey, BR black, BR red (illustrated) and in Longmoor livery to order. Price for all versions £149.95 each. It has metal wheels throughout, and is designed to be pulled, weighing only just over 30g. Standard couplings are in NEM pockets on the outer end of the relieving bogie and on the jib runner.

The Brunel 'chalet' style station building, modelled on the one at Culham (see RM June 2019) is a Craftsman-specification mixed-media kit featuring 3D printed chimneys. Price £28.00, the N scale replica is of a charming and still extant prototype.

The Ransomes & Rapier petrol-electric mobile yard crane could be found all over the world. The model (£15.95) is supplied ready-finished including driver. It is an ideal item to be posed lifting Peco containers (not included) in a busy goods yard scene.

The pack of Harley-Davidson motorcycles and riders comes as a pair, ready-painted in two different colour schemes price £9.95. These really are finely detailed 3D printed replicas of the classic 1936 Knucklehead design.

Osborn's has released a selection of N gauge rub-down transfers for wagons of the Big Four, which have sufficient to letter three to six wagons depending on the type. They will finish off kits such as those available from Peco or the N Gauge Society. Prices range from £2.95 to £3.95 per pack.

Finally there are three 3D printed



signalmen figures, supplied individually, for the interior of a signal box. They are available in N (£1.50), OO (£2.20) and O gauges (£3.95), arranged in the classic pose of pulling a particularly awkward lever – note that the O gauge

'bobby' has his regulation duster – hands off the levers! The O gauge figure will also be a finishing touch to the forthcoming signal box interior detail kit from Peco; the OO one suits the Ratio ref.553 interior. *Tim Rayner*



## Samples loaned by

OSBORN'S MODELS,  
7 Honestone Street, Bideford,  
Devon EX39 2DL  
[www.osbornsmodels.com](http://www.osbornsmodels.com)

PRICES  
In text



## Book Reviews

### Twenty-first Century Narrow Gauge

James Waite

This substantial new book brings together an excellent selection of high quality photographs of many of the world's steam-worked narrow gauge railways which have survived into the twenty-first century. It is described as "a celebration of all the wonderful little railways around the world which provide much enjoyment for many people", so it is slightly ironic that the images are all the work of one man. As well as having the time and resources to visit so many far-away places, he often seems to have had privileged access wherever he went, frequently to attend a special event. As a result some of the photos were specially staged, but are no less worthwhile for that.

His pictures from all over the world are now well known in specialist magazines, and while the introduction gives some biographical information it does not reveal how he was able to undertake so much travelling, and still is, now in retirement.

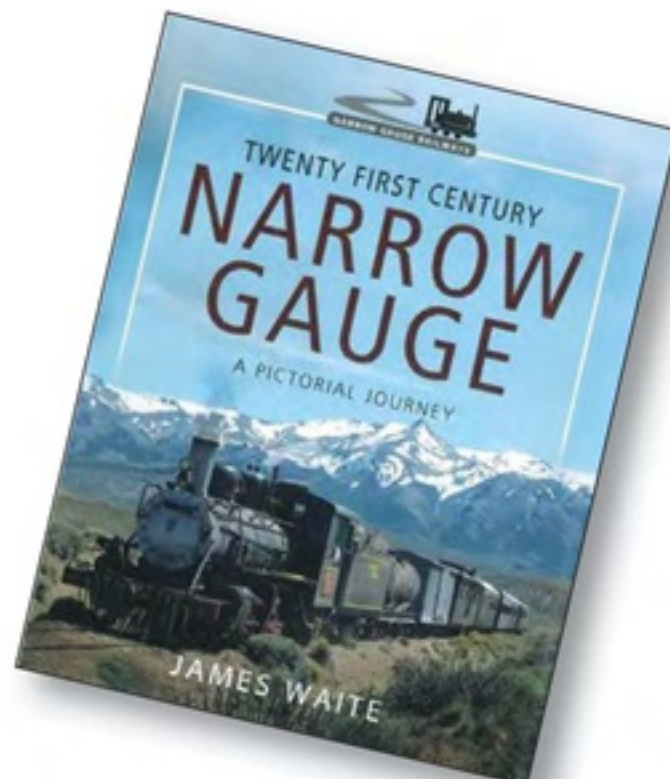
This is a pictorial record of some of his journeys, and most of the photos are previously unpublished. It is not intended to be a comprehensive guide to the world's narrow gauge railways, simply a record of those he has been fortunate enough to visit.

Some are still working (even if only just, and shadows of their former selves), others are now preserved. On those lines he has tried to seek authentic stock and scenes: heritage involves the conservation of old material, which makes for the most satisfying photos, though appealing to tourists may be necessary for survival at all.

After the introduction, the main collection is prefaced by three pages listing locomotive builders and the abbreviations used to identify them in the captions.

Then to the main body of the work. The countries featured are: Canada and the USA, Mexico, El Salvador, Costa Rica, Colombia, Brazil, Uruguay, Argentina, Iceland, Denmark, Norway, Sweden, Finland, Russia, Ukraine, Lithuania, Estonia, Latvia, Ireland, Isle of Man, Wales, England, The Netherlands, Germany, Poland, France, Switzerland, Austria, Czech Republic, Slovakia, Romania, Bulgaria, Spain, Italy, Bosnia, Serbia, Greece, Cyprus, Eritrea, Kenya, Mauritius, South Africa, Syria, Jordan, Pakistan, India, Burma, Sri Lanka, Thailand, Cambodia, Vietnam, The Philippines, Malaysia, China, Taiwan, Japan, Australia, and New Zealand. Quite an achievement for one man!

There is no explanation of the logic of the arrangement.



There are only one or two views of most locations, indeed even countries. It must have been quite a task to make the selection, as many more pictures must have been taken on each occasion.

Each image is accompanied by a long, detailed, and informative caption, the result of much research; also included are some anecdotes about his experiences while visiting, often acknowledging the assistance of key personnel in setting up scenes or advising on locations.

The captions avoid American terms for the various freight cars – they are vans and wagons here!

Without exception these are fine photos, all excellently reproduced on good quality paper.

There is a high proportion of trains in the landscape, and a fair number of roster shots of locos, sometimes in sheds or workshops, often in museum settings, but some plinthe and quietly decaying: overall a good and varied selection.

The presentation is refreshingly simple – either two large images on one page and one plus captions opposite, or larger single pictures on each page, sometimes verticals.

There are no fancy graphics or excessive use of colour or exotic fonts.

The whole production is (we imagine deliberately) reminiscent of the early and now classic publications of the globe-trotting enthusiasts of the past – the likes of Allen, Whitehouse, Small, Cross, Garratt, et al, some of whom are acknowledged as early influences. In many ways this is indeed an old-fashioned photo album produced to current technical standards – and there is nothing wrong with that.

Pen & Sword Books

47 Church Street, Barnsley, South Yorkshire S70 2AS.  
www.pen-and-sword.co.uk

288mm x 222mm 288pp

Hardback, £35.00

ISBN 978-1-47388-767-1



### LB&SCR Carriages volume 3: bogie stock 1879 – 1907

Ian White

In this third volume, the Brighton's extensive fleet of bogie vehicles are meticulously described and extensively illustrated, the latter often enlargements of photographs intended by the photographer to show a different subject. Following a detailed introduction into construction methods and equipment such as ventilators, the book covers the arc roofed stock (main line and suburban sets); clerestory roofed coaches – including the company's Royal Train – elliptical roof coaches (the famed 'balloons'); the stock for the crack City Limited businessmen's train; and motor trains, trailers and self-propelled motored stock. No fewer than eight appendices give fleet numbers, and information which has come to light since the publication of volumes 1 and 2 are given a separate chapter.

The work is supplemented by line drawings, many of which have been reproduced to 4mm scale.

As before, the royalties will be donated to the Bluebell Stroudley Coach Fund, in order to keep the railway's restored survivors rolling, and aid other projects. This will be another must-have purchase for enthusiasts and modellers of the Southern Railway and its predecessors.

Historical Model Railway Society,  
Book Sales, HMRS Museum and Study Centre, The  
Midland Railway – Butterley, Butterley Station, Ripley  
DE5 3QZ.  
www.hmrs.org.uk

302mm x 216mm 230pp

Hardback, £29.95 (£19.95 to HMRS members).

Please add £4.50 for P&P

ISBN 978-1-64516-144-8

## DVD Reviews

### The Hoppers – Moving Derbyshire into Cheshire

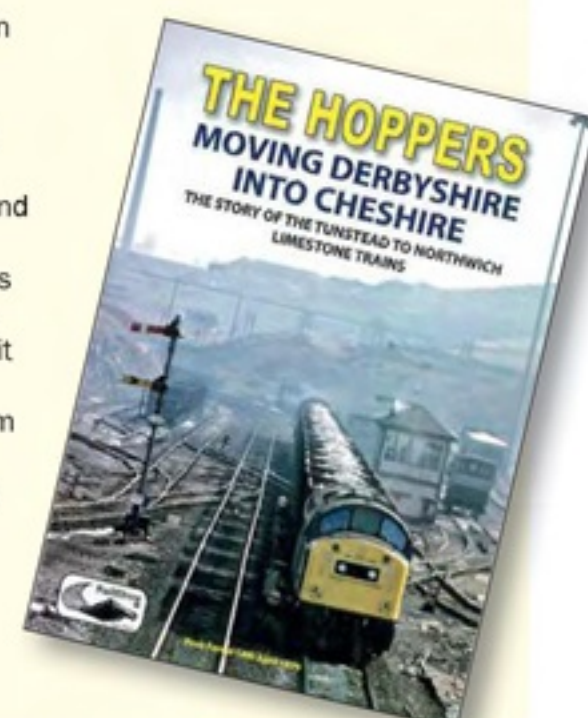
Limestone quarrying, like that in the Mendips, is synonymous with the Peak District. And like the intensive stone train service in Somerset, the rail routes around the Peak District have witnessed, and continue to see, continuous traffic to and from some significant man-made holes in the ground!

A lot of the Derbyshire limestone is used by the chemical industry. ICI's Northwich works started receiving limestone around 80 years ago using a fleet of distinctive vacuum braked bogie hoppers, and it is these hoppers that are the focus of this programme. Introduced by the LMS these workhorses remained on this traffic until 1997, a remarkable feat by any standard, and not surprisingly were hauled by several generations of motive power, from Midland 4F 0-6-0s, right through to double-headed Class 37s in the 1990s. But by then these hoppers were a bit long in the tooth and required more and more maintenance, plus vacuum train brakes were to be phased out, and the new Brush Class 60 locomotive could only supply air braking capabilities. So their reign on these services came to a natural end, but thankfully a handful of these by-then, ancient bogie hoppers made it into preservation.

This programme follows the history of these vehicles over the decades, using plenty of archive material of varying quality from a number of sources, plus some engaging personal accounts from enthusiasts and retired railwaymen who are more than happy to share their memories and anecdotes from the past. Their insight into the daily operation of these impressive trains is really insightful and an important record for the future. And now that OO scale models of these hopper wagons are available no doubt many modellers will find this programme a valuable resource for reference when creating train formations for their layouts.

Railfilms Ltd, Brook Farm, Broadwas, Worcester WR6 5NE.  
www.railfilms.com

DVD, 60 mins approx,  
£20.00 plus £2.50 first class UK mail





## Hornby invites modellers to special open weekend following visitor centre refurbishment

Following the recent return of the Hornby Hobbies Plc management team to its former Margate factory site (see News, RM June), and the refurbishment of the Hornby Visitor Centre, the renowned Kent-based manufacturer is to stage a special open weekend event on 17 & 18 August 2019.

The event will give visitors the opportunity to get up close to numerous full-size locomotive and rolling stock exhibits, which are displayed at the Margate site as part of the 'One:One Collection'. These include Gresley A4 Pacific No.4464 *Bittern* and Stanier Black Five 4-6-0 No.45379, together with other items of rolling stock and memorabilia.

Visitors will also be able to explore the newly refurbished visitor centre and meet with members of the Hornby product development team, who will be on hand to discuss the firm's upcoming product releases – including the newly-tooled GWR large Prairie (illustrated here with a decorated sample).

There will also be guest layouts attending, together with film shows and refreshments in the form of a barbecue and hog roast.

Opening times are 1000 – 1600 both days. Tickets can be booked online in advance (see link below) and are priced adults £10.00, concessions (children aged 5 – 15 and senior citizens) £6.00, and families (two adults and up to four children) £26.00. Advance ticket holders will be eligible for entry to the event from the earlier time of 0930 on both days. See the website for more details.

### Hornby Hobbies

Westgate, Margate CT9 4JX

[www.hornby.com/uk-en/hvcopenweekend](http://www.hornby.com/uk-en/hvcopenweekend)

### Hornby announces new additions to 2019 range

Hornby recently announced a number

of new additions to its current programme of new models. These include two new versions of the Terrier 0-6-0T (see Reviews, RM June 2019), presented in London Brighton & South Coast Railway livery as No.48 *Leadenhall* (pictured) and in lined Southern Railway green livery as Isle of Wight-allocated No.10 *Cowes*. Both models will be available DCC Ready or DCC Fitted.



Other additions include a Class 156 DMU in BR Provincial livery (set 156 401), a trio of Mk.III trailer cars for the Network Rail New Measurement Train, and new versions of the MHA and KFA Intermodal wagons. All the aforementioned items are scheduled for release during January/February 2020.

### Javelin named Hornby Visitor Centre

On 8 June 2019 Ramsgate Engineering



Photo: Steve Flint

train pack featuring the newly-named Javelin is to be released, which is planned for delivery in January 2020.

### Hornby announces annual results

Hornby Plc recently announced its financial results for the year ending 31 March 2019. Whilst revenue was slightly down at £32.8M (£35.7M in 2018), reported losses have been significantly reduced with net debt at 31 March 2019 standing at £1.8M (£3.9M in 2018).

Lyndon Davies, Hornby Chief Executive Officer, said, "Over the last 18 months we have created the foundations for the future across all parts of our business, we have really got to grips with the business now. We are firmly focused on the future and there is a passion reverberating around the corridors at our ancestral home in Margate which we returned to earlier this year".

[www.hornby.com](http://www.hornby.com)



▲ Hornby's Simon Kohler (left) and Lyndon Davies (right) at the Javelin naming ceremony on 8 June 2019.





# Class 74 in OO from Silver Fox Models

Silver Fox Models has re-released its OO gauge Class 74 Bo-Bo locomotive model in both ready-to-run and kit formats.

Of the 24 Class 71 electrics that were originally built, 10 were deemed surplus to requirements in 1964, withdrawn and placed in storage. In 1967 they were brought out of storage, shipped to Crewe and rebuilt as Class 74 electro-diesels, a more powerful version of the Class 73 which had impressed BR Southern Region. The Class 74s subsequently worked a variety of routes across the Southern Region for the best part of a decade.

The ready-to-run model features a cast resin bodyshell with flush glazing, resin cast bogie sideframes and other ancillary parts. The kit also comes supplied with transfers, detailed instructions and is designed to be used with a Hornby Class 90 chassis.

The ready-to-run models are supplied in BR blue with full yellow panels and are numbered according to customer choice; these are priced at £150.00 each. The kit is priced at £47.50.

For more information visit:  
[www.silverfoxmodels.co.uk](http://www.silverfoxmodels.co.uk)



# Accurascale announces PTA/JTA/JUA bogie wagons in OO

Accurascale has announced that it is developing 4mm scale ready-to-run models of the PTA/JTA/JUA bogie iron ore/stone tippler wagons – as built by Redpath Dorman Long from 1972.

Constructed in three batches by the British Steel subsidiary for transporting iron ore to Consett, Llanwern, and Ravenscraig, these wagons became synonymous with some of the heaviest trains to operate on the British Rail network; the Port Talbot to Llanwern trains saw 27 British Steel PTA tippers triple-headed by Class 37s, for example, before the introduction of pairs of the then new Class 56 on 30-wagon trains.

Following the closure of Consett in 1980, a batch of PTAs were put to work on Mendip stone trains, going both to Foster Yeoman and ARC. Their use was in block formations from quarries at Merehead, Whatley and Tytherington to depots in London and the home counties.

The PTAs continued to provide sterling service for British Steel across Britain as well as Yeoman, ARC and its successor Mendip Rail into the 2000s. The wagon leasing company VTG took on rakes of JTA/JUAs in the mid-2000s and these were used on spoil trains as well as sand and sea dredged aggregates until withdrawal.

Design work for the wagons is complete and is now in China with tooling underway. A CAD render is pictured here.

Liveries to be catered for in the first run include; British Steel, Yeoman, ARC and VTG. The wagons will be sold in 'book' sets of five, with both outer and inner wagons. There will be additional book sets of five inner



▲ CAD render of outer bogie tippler wagon

wagons in both Yeoman and ARC liveries to boost rakes to prototypical lengths.

NEM tension lock couplings will be provided on outer wagons, with knuckle couplers on inner wagons as per the prototypes. The models will also feature sprung buffers and provision for conversion to EM and P4 gauges – together with a flashing tail-light on one outer wagon per pack.

The price of each book set is £149.95, with a 10% discount if both the outer and inner sets of the Yeoman or ARC wagons are purchased together. Delivery is expected in January 2020.

Accurascale has also received tooling samples for its PFA four-wheel container flat wagons in OO. Stocks of these are planned for delivery in July. (Full details of the planned PFA models were published in our May 2019 issue.)

Accurascale has also welcomed Gareth Bayer on board as its Senior Project Manager. Gareth brings with him several years of experience of designing North American and British outline R-T-R models in N, HO and OO scales.

[www.accurascale.co.uk](http://www.accurascale.co.uk)



▲ PFA container flat tooling sample

# Trains4U and Cavalex Models to release MOD KFA/PFA Warflat in OO

The Peterborough-based retailer Trains4U is teaming up with Cavalex Models to produce OO gauge ready-to-run models of Ministry Of Defence KFA Warflat wagons, 65 of which were built at Shildon for the MoD between 1976 and 1981.

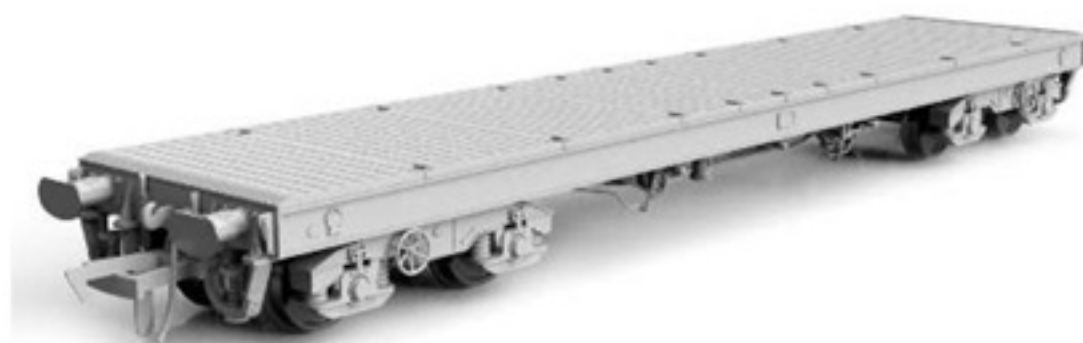
Originally dual braked, with the TOPS classification PFB, they were converted to air braked only during the 1990s. Some received the PFA code before the K prefix came into use (KFB/KFA). These wagons can be used for trans-

porting a variety of loads; primarily soft skin and light armoured vehicles. KFAs are still in wide use by the MOD today.

CAD work is complete and the model is expected to go into the tooling stage once the required number of orders is reached. Deliveries are expected during late summer/early autumn 2020

The Cavalex KFA is set to feature; a close coupling mechanism, sprung buffers, zinc alloy frame and provision for conversion to P4/EM gauge.

Two liveries are announced for the



initial run, with four different running numbers available in each livery; PFBs in Olive green with green jacks (suitable for 1980s and early 1990s), and KFAs in Olive green with yellow jacks

and bufferbeams (suitable for late 1990s through to present day).

The wagons will be priced at £35.00 each and can be pre-ordered through [Trains4U.com](http://Trains4U.com) or by contacting the shop directly. (No deposits or up-front payments are required.)

**Trains4U Ltd**  
27 St David's Square, Fengate,  
Peterborough PE1 5QA  
Tel: 01733 895 989  
[www.trains4u.com](http://www.trains4u.com)





## EXHIBITION FOCUS

### Northampton station model to form centrepiece of club's annual exhibition

The Northampton & District Model Railway Club is celebrating its 50th anniversary this year. To mark the occasion it plans to display a layout of the town's Victorian Castle Station at the club's annual exhibition, which is due to take place on Saturday 24 August at Roade Village Hall, Bailey Brooks Lane, Roade, Northamptonshire NN7 2LS.

The OO gauge model of the town's Castle Station was originally constructed during the 1980s by local railway enthusiast, the late Leslie Roy, in the loft of his Northampton home. The model depicts Northampton's Castle Station in the 1930s and is based on photographs and actual measurements of buildings that he took before its demolition in 1965. His son, Timothy, donated the model to the club follow-



ing his father's death in June 2016.

The exhibition will bring together a number of layouts, demonstrations, trade and society stalls. Refreshments will be available. For details of admission prices and opening times, see Societies & Clubs or go to the Northampton club's website:

[www.ndmrc.org](http://www.ndmrc.org)

### Bexhill club to celebrate 50 years with annual show

Established in 1969, Bexhill Model Railway Club is this year celebrating its 50th anniversary, with members set to stage a special exhibition to mark the occasion on Saturday 10 August.

Taking place at St Richards Catholic College, Ashdown Road, Bexhill-on-Sea (TN40 1SE), there will be no fewer than 14 layouts in action, including Portsea (pictured), a 3mm layout that was featured in our December 2015 issue. See Societies & Clubs for further details, including opening times and admission prices.

The club has its own clubrooms in Clifford Road, Bexhill town centre and meets regularly throughout the week. The club always welcomes prospective new members looking to develop their modelling skills.

For more information including details of how to join, contact the secretary via the website: [www.bexhillmrc.org.uk](http://www.bexhillmrc.org.uk)

Photo: Geoff Helliwell



## Bachmann announces Class 45/0s in OO

Bachmann Europe Plc has announced that it is to release a quintet of Class 45/0 diesel locomotives as part of its Branchline OO range; the models are set to utilise body tooling that was only previously used on a limited edition model, in conjunction with its latest generation Class 45 chassis, which features directional lighting and allows for sound installation.

The Class 45s were built at British Rail's Derby and Crewe Works between 1960 and 1962. They were a development of the Class 44 and, together with the Class 46, the three classes were collectively known as the 'Peaks'. As built the entire class was fitted with multiple-working equipment and steam-

heat boilers but, later, some had their boilers removed and were fitted with electric train heating (ETH). With the implementation of TOPS in 1971, the steam-heat locos were designated Class 45/0 and the ETH fitted examples became 45/1s.

From 1975, during works visits the locomotives began to have their head-code equipment removed and this was replaced with sheet metal and two sealed beam marker lights and it is this condition that the Bachmann Class 45/0s will cover.

Five versions of the sealed beam Peak are to be produced, each with an RRP of £144.95 (£234.95 sound-fitted): ref.32-683RJ Class 45/0 No. 45



022 Lytham St. Annes in BR Blue; ref.32-684DB Class 45/0 No. 45 040 The King's Shropshire Light Infantry in BR Blue; ref.32-685SD Class 45/0 No. 45 041 Royal Tank Regiment in BR Blue; ref.32-686NF Class 45/0 No. 45 046 Royal Fusilier in BR Blue and ref.32-687TL Class 45/0 No. 45 049

The Staffordshire Regiment (The Prince of Wales's) in BR Blue.

It should be noted that the models are being released as retailer exclusives, with the item suffixes denoting the areas from which each model can be purchased (see website for more details) [www.bachmann.co.uk](http://www.bachmann.co.uk)

## Revised turntable motorising kit from Locomotech

Locomotech has announced a new 'Roundhouse' version of its automatic indexing turntable motorising kit. It is the same as the original version (see Reviews, RM May 2016) in every respect, except that it stops automatically at 32 different possible exit roads, each spaced equally at 11¼°, rather than just the 10 exit roads of the original version.

The manufacturer advises that if fewer exits are required then a little solder can be used to close off the slots in the brass indexing disc. The new 'Roundhouse' automatic indexing kit (ref.TTMRKE) is priced £67.99. Customers with existing kits, with or without indexing, will be able to upgrade to the new Roundhouse version if desired. The original 10-exit version will also remain available.

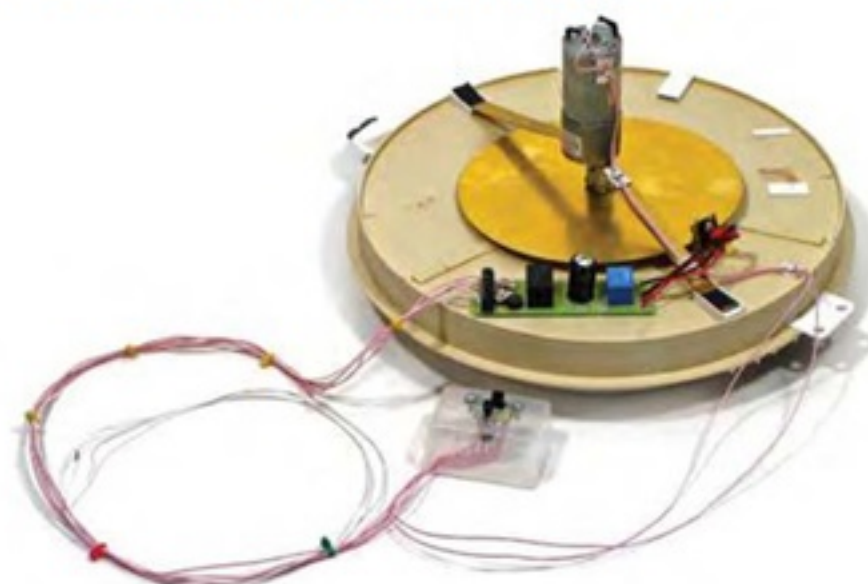
For more details, or to order:

**Locomotech**

Tel: 01903 871 149

email: [info@locomotech.co.uk](mailto:info@locomotech.co.uk)

[www.locomotech.co.uk](http://www.locomotech.co.uk)





## Trams, trolleybuses and trains at Crich!

A model tram, trolleybus and railway exhibition will be held on 17 & 18 August 2019 at the National Tramway Museum, Crich Tramway Village, Crich, Derbyshire (DE4 5DP). 12 exhibits will be present at the event, including a tramway layout more than 80' in length by Manchester MRS and Tissington, a OO gauge model based on the former LMS Ashbourne to Buxton branch, set around 1930. Normal museum entry charges apply. For details see Societies & Clubs or: **Tel: 01773 854 321**  
**www.tramway.co.uk**

## N Gauge Society acquires wagon kits

The N Gauge Society has purchased the tooling for the plastic wagon kits formerly produced by Taylor Precision Models (TPM). The wagon kits comprise: Air-braked Steel Carrier (SAA and derivatives); Iron Ore Tippler (PTA, later JTA/JUA); VCA Van; VDA Van; FPA Russell coal container; Private Owner (POA/PNA) and Limpet ballast spoil wagon (ZKA). The society plans to integrate the kits into its existing range (available to members only) over the next six months.  
**www.ngaugesociety.com**

## DJ Models goes into receivership

DJ Models – Dave Jones's model railway manufacturing enterprise – was placed into receivership on Tuesday 4 June 2019. A statement published on the DJ Models website read as follows, "It is with great sadness that DJ Models Ltd has now closed and is in receivership. Both DJModels e-mail addresses ('nthusiast@', and 'info@') are now out of use, and all aspects of DJ Models Ltd are in the control of the receivers, CG-recovery of Manchester." Incorporated in July 2013, products brought to market under the DJM brand included a J94 0-6-OST (see Reviews, RM October 2016) and Class 71 (RM July 2017) in OO.  
**www.djmodels.co.uk**

## 175 years of Shap's railway history

Events are planned to take place in Shap this summer to mark 175 years since construction of the Lancaster & Carlisle Railway commenced. The Shap Local History Society has an extensive railway archive at The Market Cross (open weekends in July and August), whilst the Shap Community CIO is staging an exhibition of railway art and photography in the gallery at The Old Courthouse from 10 August. A model railway exhibition will be held at Shap Memorial Hall on 18 August. For further details see entries in Societies & Clubs.  
**www.theoldcourthouse.org**

# IPA car carriers in OO and HOA samples in N from Revolution Trains

Revolution Trains – the company set up to use crowdfunding methods to bring niche models to the British model railway market – has received the first tooling samples for its N gauge IPA car carrier models.

The carriers were originally built in the 1950s and 1960s for domestic French car traffic as double-deck wagons. With the opening of the Channel tunnel, the need for car carriers that could operate internationally was identified and hundreds had the top deck removed (to fit the restrictive British loading gauge) and were reconfigured into twin sets.

To counter damage by thrown stones some twins were fitted with side stakes to allow canvas protective sheets to be mounted, while later the decision was taken to rebuild a large number again with side panels and roofs for added protection.

Illustrated here are two of the three tooling variants being produced; the plain flat version, and flat type with stakes. The models feature separate wire and photo-etched metal details, NEM couplers in kinematic mounts and blackened metal wheels. The decks are made of cast metal for weight.

Revolution has also announced that it is to offer the IPA twin car carriers as its next ready-to-run wagon in OO gauge. Orders are now being taken, and production is dependent upon sufficient orders being received.



### HOA hoppers getting nearer...

Revolution Trains has received painted samples of its forthcoming HOA stone hoppers in N. Once minor corrections have been made the models will be approved for production, with delivery expected before the end of the year.

Revolution is offering the HUA models in five variants (covering detail differences in bodyside appearance and braking as appropriate): ref.N-HOA-101 – EWS grey with 'three-beasties' logo; ref.N-HOA-111 – Cemex grey/blue; ref.N-HOA-121 – DB red; ref.N-HOA-131 – Ermewa/Tarmac grey; N-HOA-141 – VTG Mendip Rail silver.

The models are also available in 'A' and 'B' triple packs with different running numbers, enabling realistic rakes to be built up. The pre-order price is £30.50 for single wagons and £91.50 for triple packs. Orders can be placed via the website; the models will be

available in due course from selected retailers but at a higher price.

### Crowdfunding reassurance

Following the news that DJ Models has been placed into receivership (see separate story), Revolution Trains co-founder, Ben Ando, issued this statement: "Revolution Trains is saddened by the collapse of DJ Models, and sympathises with any enthusiasts who may have lost out. Revolution Trains has successfully delivered, or has in production, 11 products via crowdfunding in N and OO with a further four in tooling or at decorated sample stage.

We have a proven track record of delivering outstanding models that might otherwise have remained too niche for mainstream production, and look forward to continuing to operate for the benefit of our supporters and fellow modellers."

**www.revolutiontrains.com**



# Award winners at Kettering show

More than 2000 visitors attended the annual exhibition staged by the St Neots Model Railway Club on 8 & 9 June in Kettering – a new location for this year's event.

A number of awards were presented during the course of the two days; Bewdley (OO) scooped both the Jim Curle Shield for best layout as chosen by guest judge Paul Jones (Warley MRC Exhibition Manager), and the Brian Rawlings Shield as voted for by the public. Paul Jones also awarded highly commended certificates to the Handley family for *Sixpenny Handley* by James Handley (featured in RAILWAY MODELLER December 2018) and *Bartholomew's Hollow* by Nathanael Handley. The awards acknowledge the involve-

ment of young family members who will be the future of our hobby.

We are also pleased to report that David O'Rourke successfully completed repairs to his OO gauge layout *Burnham* (RM June 2019) in time for it to fulfill its booked attendance at Kettering, it having sustained damage during the vandal attack at the Market Deeping exhibition venue prior to the show on 18 May (see News, RM July).

Preparations are already underway for next year's show, which will be on the weekend of 13 & 14 June 2020.

**www.stneotsmrc.com**



▲ Pictured (left to right): Caroline Hoy, Chairman of St Neots MRC; Chris Manners & Graham Heald who own *Bewdley*; guest judge Paul Jones and exhibition manager Jackie Kneeshaw.



▲ Pictured (left to right), back: Richard Handley, James Handley, Jackie Kneeshaw and Paul Jones. Front: Silas Handley, Nathanael Handley, Nerissa Handley and Caroline Hoy. Both photos: Paul Bason



## Lights, camera, action for series two of *The Great Model Railway Challenge*

Filming is currently underway for the second series of Channel 5's *The Great Model Railway Challenge*, which is planned for transmission later in 2019. The format has been changed slightly this time around, with eight programmes instead of six, each of which will be 15 minutes longer than the episodes in season one to increase coverage of modelling techniques.

RM Editor Steve Flint and master modeller Kathy Millatt have reprised their roles as the two

judges, alongside presenters Tim Shaw and James Richardson. Filming is once again taking place at Fawley Hill, located in Henley-on-Thames, the estate of the late Sir William McAlpine.

Peco is again sponsoring the production of the programme through the provision of modelling materials for the teams to use, whilst RAILWAY MODELLER will provide 'behind the scenes' coverage of the series, so look out for further news, updates and features over the upcoming months.



## New Craftsman kit modelling course at Pecorama

Have you always wanted to construct a Wills Craftsman kit, but not been sure quite where to start? Building on the popularity of its established track and scenery modelling courses, Pecorama has decided to add a new course for 2019, specifically on this subject.

The course will take place over two days on Tuesday 17 and Wednesday 18 September 2019, here at Pecorama in Devon. Over the two-day course, students will be encouraged to build the small engine shed kit (ref. CK-14), with assistance and guidance in the skills and techniques necessary to construct this model, from our in-house modellers.

The aim is for students to take home an assembled model which will be spray painted in a grey base primer ready for them to paint in their chosen colour scheme at home. At each stage students will be advised on tips and techniques of the construction, with our own modellers building the same model alongside.

The course is aimed at the confident modeller who already has some experience of general kit building, whilst the skills acquired will be transferable to other plastic kits and will also assist with those wishing to scratchbuild structures using styrene and embossed sheet.



Included as part of the course will be a conducted tour of the Pecorama model railway exhibition, a ride on the Beer Heights Light Railway and a modellers tool kit (which participants can also take home). Refreshments will be provided.

Places on the course are priced £215.00. See the website for a full itinerary, together with FAQs and booking/cancellation policy details:

[www.pecorama.co.uk](http://www.pecorama.co.uk)



INCORPORATING PECO MODEL RAILWAYS • PECO PUBLICATIONS LTD • PECORAMA

## Roy Jackson

One of the hobby's most colourful and enigmatic modellers, Roy Jackson, has died following a short period of illness. A spirited and outspoken individual with an ever-so-slightly madcap side to his persona, he was widely known and admired by a huge contingent of modellers on the exhibition circuit and across the wider hobby.

He was also a staunch advocate of EM gauge modelling standards and was perhaps best known for his association with a series of well known layouts built and exhibited over a 50 year period. One of the earliest from the 1970s was *Gainsborough Central*, closely followed by *High Dyke*, a depiction of a junction on the ECML near Grantham.

*Dunwich*, set in East Anglia, came next before he embarked on a true-to-scale 4mm rendition in British Railways days of the stations and environs of Retford, another location on the east coast route.

Although an engineering manager at a power station in his day job, he and two associates set up a small kit manufacturing firm in the 1970s under the brand name of 3H Mouldings. The initial kits, of LNER wagon prototypes, set new standards of production over the typical offerings of the day; notably 3H kits were supplied with metal wheels and axles and inclusive of pinpoint bearings, setting the bar high for companies like Parkside which followed.

Roy's forte was building locomotives and coaches, and the construction and laying of track, but the layout projects were generally the result of collaborative

efforts. This was indeed true of the *Retford* scheme, (see below) which was steadily being brought to life by dozens of fellow modellers who volunteered their time and expertise. In fact the *Retford* project became not just a model railway, but a social hub for his countless friends and acquaintances from far and wide.

*Retford* was housed adjacent to his Lincolnshire home in a huge outbuilding which also provided accommodation for his collection of classic cars and those of his close friend and fellow modeller Geoff Kent.

Roy will be a difficult act to follow. His legacy to model railways will be to remind us that there is no such thing as a layout project that's too ambitious.

He leaves behind his former wife Janet, their two daughters Sally and Sarah, and two grandchildren.



Retford photo: Tony Wright





# Coming next month

Sept. 2019 –  
Out Thursday 8 Aug.

**Medehampton**  
ECML in the glory days,  
modelled in OO

**Railway of the Month**

**Ballykeale**  
Railway action in Ireland,  
modelled in N gauge

**Titherleigh**  
A Southern terminus in OO,  
inspired by a classic RM plan

## inside ....

- **BLACKADDER ANIMATIONS**  
Adding movement to a Great Model Railway Challenge entry
- **GROW YOUR OWN TREES**  
Scenic modelling advice for OO
- **PERSPECTIVE MODELLING 2**  
Creating a backscene for a Great Model Railway Challenge entry

plus all the regular features ....

## Shop News

### 20th birthday celebrations for Somerset retailer



Frome Model Centre is to celebrate 20 years of trading from its shop in the Somerset market town with a special birthday event on Wednesday 21 August 2019. With extended opening hours of 1000 – 2000, there will be product demonstrations (including Peco), competitions, goody bags and refreshments.

The family business was started in Hertfordshire more than 30 years ago by Simon & Sarah England. It began in 1986 with just a trade table at a toy fair in Market Harborough, followed by regular stands at shows in Northampton,

Milton Keynes, Dunstable and various other market towns. With their home gradually becoming filled with stock, Simon and Sarah made the decision to look for suitable shop premises and, in 1993, a store was opened in Northampton.

The family relocated to Frome in 1998, where a second shop was opened the following year. Both shops remained open until the Frome one became established, whereupon the Northampton one was closed.

The retailer stocks a wide array of modelling products covering railways, aviation, motoring and mili-

tary subjects, together with slot cars, wargaming and dolls house accessories. Model railway ranges include Gaugemaster, Metcalfe, Peco, Hornby and Bachmann.

A mail order service is available, and the firm advertises regularly in RAILWAY MODELLER.

Normal opening hours are Tuesdays – Saturdays 1000 – 1700 (closed Sundays and Mondays).

**Frome Model Centre**  
1-2 Catherine St, Frome,  
Somerset BA11 1DA  
Tel: 01373 465 295  
[www.fromemodelcentre.com](http://www.fromemodelcentre.com)

### Realtrack Models reveals PCA cement wagons in N

Realtrack Models has received factory-decorated samples for its forthcoming PCA hoppers in N. The models are being developed in conjunction with Accurascale, which itself has released OO gauge versions of these prototypes (see Reviews, RM June 2019).

Realtrack is releasing the wagons as triple packs, finished in STS, Rugby Cement and Castle Cement liveries, priced £78.00 per pack. Delivery is planned for September 2019, with orders currently being taken. For more details:

[www.realtrackmodels.co.uk](http://www.realtrackmodels.co.uk)



### Visitors flock to Frizinghall open day

The Yorkshire retailer, Frizinghall Models & Railways, welcomed a large number of visitors to its open day on Saturday 1 June 2019. Alongside a number of model railways brought in specially for the event, there was also support from the trade, including Peco, which was represented by the firm's Northern Sales Representative, Ian Kay.

The newly-refurbished N gauge layout *Ryburn Bridge* proved particularly popular with those attending; the layout was originally constructed by Peco modelmakers, but is now in the custody of the retailer.

Frizinghall Models & Railways is normally open Mondays – Saturdays from 0930 – 1730. For more details:

[www.modelrailshop.co.uk](http://www.modelrailshop.co.uk)

Photos: Derek Shore





# Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should contact the organiser first. Entries include the organiser's website and/or telephone number. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and copy by post or via email to [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com) is required six weeks prior to publication date.

October issue deadline: August 1st

## EVENTS

### Saturday 13 July

#### HEYWOOD, Lancashire

**Organiser:** Heywood MRG.  
**Venue:** Club Rooms, Unit 4, Park Works, River Street, Heywood OL10 4AB.  
**Open:** 1000 – 1600  
**Admission:** free. Donations appreciated.  
**Amenities:** light refreshments, ample free parking.  
**Features:** open day – layouts in N to G, bring your own locos to run, dc or DCC.  
**Contact:** [www.hmrg.co.uk](http://www.hmrg.co.uk)

### Saturday 13 July

#### HODDESDON, Hertfordshire

**Organiser:** The Friends of Robert Barclay Academy.  
**Venue:** The Robert Barclay Academy, Cock Lane, Hoddesdon EN11 8JY.  
**Open:** 1030 – 1630  
**Admission:** adults £4.50, concessions £3.00, family (2+2) £12.00. All profits to academy projects.  
**Amenities:** refreshments all day, free parking on site.  
**Features:** layouts include *Batcombe* (OO), *Battlefield Estate* (O), *Billton Goods* (3mm), *Binns Road* (O), *Brownsville* (HO), *Buntingford* (N), *Ellasweet Beet Factory* (OO), *Lindell Town* (OO), *Long Melford* (OO), *Longstone* (Gn15), *Mirkwood* (OO9), *New Walmington Pier* (OO9), *Schweizer Kleinstadt* (Z), *Small N Working* (N), *Stodden Hundred Light Railway* (O), *Tigley Yard* (O), *Townsend* (N), *Transcontinentally Tri-ang* (OO & HO), *Tudwick Road Siding* (O), *Under Hockney Skies* (N). Plus LEGO layout, trade support, society stands, demonstrations and more.  
**Contact:** 07866 641215

### Saturday 13 July

#### HOUNSLOW, Greater London

**Organiser:** Twickenham & District MRC.  
**Venue:** Kerswell Hall, Wills Crescent, Hounslow, Greater London TW3 2JF.  
**Open:** 1000 – 1600  
**Admission:** free. Donations appreciated.  
**Amenities:** venue is wheelchair friendly, refreshments.  
**Features:** six layouts in O, OO and N including *Eastgate* (OO), *Kensington Addison Road* (O), demonstrations, second-hand stand.  
**Contact:** 01784 459104  
[www.tdmrc.org.uk](http://www.tdmrc.org.uk)

### Saturday 13 July

#### MANSFIELD, Nottinghamshire

**Organiser:** East Midlands Narrow Gauge Group.  
**Venue:** Portland College, Nottingham Road, Mansfield NG18 4TJ.  
**Open:** 1000 – 1600  
**Admission:** adults £4.00, OO9 Society members £3.50, accompanied children under 16 free.  
**Amenities:** large free car park, hot and cold refreshments available all day.  
**Features:** OO9 Society open day – 22+ layouts (mainly OO9), trade support including OO9 Society Sales, presentations on layout design by Paul Lunn.

**Contact:** Hugh Milward 07772 242135

### Saturday 13 & Sunday 14 July

#### BEER, Devon

**Organiser:** Pecorama.  
**Venue:** The Station Gallery, Pecorama, Beer, Seaton, Devon EX12 3NA.  
**Open:** 1000 – 1700 both days  
**Admission:** free (normal Pecorama admission prices apply).  
**Amenities:** parking, refreshments, disabled access (hilly site renders three-wheel mobility scooters unsuitable).  
**Features:** *The Andeer Line* (Swiss HOm modular layout by Julia & Derek Boswell). Visitors are invited to help operate the layout, which employs digital control, with a central 'signal box' and trains driven individually using smart phones and tablets. On request, there will also be practical demonstrations of the foam base-board construction and the use of electrostatic grass. Drop in for as long as you wish. See website for details.  
**Contact:** [www.pecorama.co.uk](http://www.pecorama.co.uk)

### Saturday 13 & Sunday 14 July

#### HUNSLET, Leeds, West Yorkshire

**Organiser:** Middleton Railway Trust.  
**Venue:** Middleton Railway, The Station, Moor Road, Hunslet, Leeds LS10 2JQ.  
**Open:** 1000 – 1700 both days (steam trains operating between 1100 – 1600).  
**Admission:** adults £5.50, children £2.50, family £14.00. Exhibition/Engine House plus unlimited train rides adults £8.50, children £3.50, family £20.00.  
**Amenities:** light refreshments, disabled access to exhibition and trains but not footplate experiences.  
**Features:** layouts in many scales and gauges including *Deffors* (O), *Dornochbahn* (HOe), *John Street Yard* (EM), *Low Moor* (T), *Reevy Road West* (OO), *Roanoke Roundhouse* (HO). Vintage Tri-ang and hands-on layouts. Footplate experience on a full-size locomotive £10.00; minimum age 18, maximum 76, no need to book in advance. Trade support and other attractions on site including nearby retail outlets.  
**Contact:** Ian Dobson 01274 613687 (eves only)  
[www.middletonrailway.org.uk](http://www.middletonrailway.org.uk)

### Saturday 20 July

#### BRIGHTON, Brighton & Hove

**Organiser:** Sussex Vintage Model Railway Collectors.  
**Venue:** The Knogle Hall, Knogle Road, Preston Park, Brighton BN1 6RB.  
**Open:** 1000 – 1630  
**Admission:** adults £4.00, children free (must be accompanied).  
**Amenities:** 'Buffet Car' refreshments, wheelchair access to main hall but regretably not to first floor displays. Disabled toilet. Venue is off A23, close to main bus routes and a short walk from Preston Park station. Unrestricted and two-hour kerbside parking in the vicinity.  
**Features:** layouts featuring classic toy and model trains, including Hornby O gauge, Hornby Dublo, Bassett-Lowke, Lionel, Trix Twin, working Meccano and more. Trade support.  
**Contact:** 07785 567251  
[www.sussex-transport.co.uk/svmrc](http://www.sussex-transport.co.uk/svmrc)

### Saturday 20 July

#### CHINGFORD, London

**Organiser:** RAF Wings and RBL Poppy.  
**Venue:** The Royal British Legion, Hall Lane, Chingford, London E4 8HW.  
**Open:** 1100 – 1600  
**Admission:** adults £3.00, children 50p.  
**Amenities:** refreshments, parking.  
**Features:** six working layouts, trade support, demonstration.  
**Contact:** Brian Kennaby 0208 524 0730

### Saturday 20 July

#### FELIXSTOWE, Suffolk

**Organiser:** WorldWide Group (WWG) of the N Gauge Society.  
**Venue:** Trinity Methodist Church, 26 Hamilton Road, Felixstowe IP11 7AN.  
**Open:** 1000 – 1600; AGM 1610.  
**Admission:** adults £3.00, WWG members and accompanied children free. Admission price refunded to visitors who join WWG on the day.  
**Amenities:** disabled access (no parking on site), refreshments, parking nearby, railway station 10-15 mins walk.  
**Features:** N gauge layouts including *East Tyrone* and *Southerland* (American); *Hartenberg* (Austrian); *Depot de Camions*, *L'Isle de St George* and *Schiebourg* (French); *Hindembach* (German). Trade and members' sales.  
**Contact:** [secretary@ngaue-wwg.org.uk](mailto:secretary@ngaue-wwg.org.uk)

### Saturday 20 July

#### FILEY, North Yorkshire

**Organiser:** Filey Model Railway Show.  
**Venue:** St John's Church, West Avenue, Filey, North Yorkshire YO14 9AU.  
**Open:** 1000 – 1630.  
**Admission:** by donation (suggested £3.00, please Gift Aid if possible), children free. Proceeds toward the refurbishment of The Parish Centre at St John's.  
**Amenities:** refreshments.  
**Features:** layouts, model shop, stalls.  
**Contact:** Christine Wilson 01723 514529  
Christopher Martin 01723 870166

### Saturday 20 July

#### PEVENSEY, East Sussex

**Organiser:** Pevensey Bay MRC.  
**Venue:** Pevensey Memorial Hall, Church Lane, Pevensey, East Sussex BN24 5LA.  
**Open:** 1000 – 1630  
**Admission:** adults £4.00, children over 5 £2.00, family (2+2) £10.00.  
**Amenities:** free parking, disabled access to most areas, refreshments.  
**Features:** approx 12 layouts plus trade support.  
**Contact:** 07774 185651

### Saturday 20 July

#### STIBBARD, Norfolk

**Organiser:** Stibbard Village Hall & Methodist Church.  
**Venue:** Stibbard Village Hall & Methodist Church, Guist Bottom Road, Stibbard, Fakenham, Norfolk NR21 0AQ.  
**Open:** 1000 – 1600  
**Admission:** adults £4.00, concessions £3.50, family £12.00.  
**Amenities:** free parking nearby, disabled access, refreshments.  
**Features:** eight layouts plus trade support, second-hand, society, tombola.  
**Contact:** Bert Etherington 01328 829185

### Saturday 20 & Sunday 21 July

#### BURTON UPON TRENT, Derbyshire

**Organiser:** Nick Palette.  
**Venue:** Claymills Victorian Pumping Station, Meadow Lane, Stretton, Burton upon Trent, Derbyshire DE13 0DA.  
**Open:** 1000 – 1600 both days.  
**Admission:** adults £4.00, concessions £3.00, accompanied children free.  
**Amenities:** free parking, level access, refreshments available.  
**Features:** 10+ layouts and trade support – experience a model railway exhibition in atmospheric Victorian industrial surroundings (note that the pumping station will not be in steam).  
**Contact:** [n.palette@ntlworld.com](mailto:n.palette@ntlworld.com)

### Saturday 20 & Sunday 21 July

#### CUPAR, Fife

**Organiser:** Cupar & District MRC.  
**Venue:** The Corn Exchange, St Catherines Street, Cupar, Fife KY15 4BT.  
**Open:** 1000 – 1630  
**Admission:** adults £4.50, children £2.50, family (2+2) £12.00.  
**Amenities:** refreshments, free parking nearby, excellent public transport.  
**Features:** layouts include *Altdorf Ost* (HO), *Avin-A-Llyn* (OO9), *Barnsford Bridge* (OO), *Bathburn* (O), *Burlington Central* (N), *Cadray Sidings* (P4), *Deerfoot* (N), *Eden Road TMD* (OO), *Fife Cement* (OO), *Inverboyndie* (OO), *Kinlochewe* (OO), *Leven Bank* (N), *Ripon WI* (HO), *Rockfield Road* (N), *Wallacetoun Junction* (N). Plus a LEGO railway, trade support, club and society stands.  
**Contact:** [www.cuparmrc.co.uk](http://www.cuparmrc.co.uk)

### Sunday 21 July

#### GAINSBOROUGH, Lincolnshire

**Organiser:** Gainsborough MRS.  
**Venue:** Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.  
**Open:** 1330 – 1730 (last admission 1630)  
**Admission:** adults £4.00, seniors/children £3.00, family (2+2) £10.00.  
**Features:** open day. One of the largest O gauge model railways in the country; dozens of trains running on a total of over half a mile of track.  
**Contact:** B Hodgkinson 01427 610475  
[www.gainsboroughmodelrailway.co.uk](http://www.gainsboroughmodelrailway.co.uk)

### Wednesday 24 July

#### BISHOP'S LYDEARD, Somerset

**Organiser:** Taunton MRG.  
**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.  
**Open:** 1030 – 1630  
**Admission:** charges apply.  
**Amenities:** light refreshments.  
**Contact:** 07879 852736

### Saturday 27 July

#### BARNSTAPLE, Devon

**Organiser:** Barnstaple & District MRC.  
**Venue:** Christ Church, Bear Street, Barnstaple, Devon EX32 7BU.  
**Open:** 1000 – 1600  
**Admission:** £4.00.  
**Amenities:** car parking 2mins away, disabled-friendly venue, hot & cold refreshments available all day.  
**Features:** layouts and traders across three rooms.  
**Contact:** 01237 238872  
<https://barnstaplemrc.weebly.com/>

### Saturday 27 July

#### BRIDPORT, Dorset

**Organiser:** Bridport & District MRC.  
**New venue:** Sir John Colfax Academy, Ridgeway, Bridport, Dorset DT6 3DT.  
**Open:** 1000 – 1600  
**Admission:** adults £5.00, children 5 – 16 £1.00, under 5s free.  
**Amenities:** car parking nearby, light refreshments served all day.  
**Contact:** Bob Ahrens 01308 861754

### Saturday 27 July

#### MARKS TEY, Essex

**Organiser:** N Gauge Society.  
**Venue:** Marks Tey Parish Hall, Old London Road, Marks Tey CO6 1EJ.  
**Open:** 1030 – 1430  
**Admission:** adults £5.00, under 12s free, NGS members £3.00.  
**Features:** layouts plus trade support. N Gauge Society AGM commences 1430.  
**Contact:** <http://newweb.ngauesociety.com/>

### Saturday 27 & Sunday 28 July

#### GOATHLAND, North Yorkshire

**Organiser:** Whitby & District MRC.  
**Venue:** Goathland Village Hall, The Common, Goathland YO22 5JS.  
**Open:** 1000 – 1630 both days.  
**Admission:** £2.00, accompanied children free.



**Amenities:** refreshments, tombola, free parking.

**Features:** layouts, trade support, demonstrations.

**Contact:**  
www.facebook.com/WhitbyMRC

### **Saturday 27 & Sunday 28 July** **LLANDUDNO, Conwy**

**Organiser:** Conwy MRC.

**Venue:** Craig y Don Community Centre, Queens Road, Craig y Don, Llandudno, Conwy LL30 1TE.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.

**Admission:** adults £4.00, children £3.00.  
**Amenities:** refreshments, disabled access, free parking, venue on bus routes from stations.

**Features:** 15 layouts from Z to O, trade support and demonstration.  
**Contact:** John Farrell 07714 237248

### **Saturday 27 & Sunday 28 July** **NEW MILTON, Hampshire**

**Organiser:** South Coast Model Rail Club.

**Venue:** The Arnewood School, Gore Road, New Milton, Hampshire BH25 6PQ.

**Open:** 1030 – 1700 Saturday

1000 – 1600 Sunday.  
**Admission:** adults £5.00, children £3.50, family (2+2) £15.00.

**Amenities:** light refreshments available, full disabled access.

**Features:** 14 layouts from N to Gauge 3 plus trade support.  
**Contact:** G Tomlin 01425 616646

### **Saturday 27 & Sunday 28 July** **NORTH SHIELDS, Tyne & Wear**

**Organiser:** Blyth & Tyne MRS.

**Venue:** John Spence Community High School, Preston Grange, North Shields, Tyne & Wear NE29 9PU

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.

**Admission:** adults £7.00, children £3.00, family (2+2) £16.00.

**Features:** up to 30 layouts, trade support, demonstrations.

**Contact:** www.railexne.com

### **Saturday 27 & Sunday 28 July** **STOKE ON TRENT**

**Organiser:** St John's Charity Exhibition.

**Venue:** St John's Primary School, Wheatley Avenue, Trent Vale, Stoke on Trent ST4 6SB.

**Open:** 1030 – 1700 Saturday  
1030 – 1630 Sunday.

**Admission:** adults £5.00, children £2.00, family (2+2) £11.00.

**Amenities:** ample free parking, light refreshments, disabled access. Venue 3 miles Jc15 M6. Proceeds donated between Douglas McMillan Hospice and Midlands Air Ambulance.  
**Contact:** 07747 087050

### **Sunday 28 July** **THIRSK, North Yorkshire**

**Organiser:** Thirsk MRG.

**Venue:** Thirsk Town Hall, Westgate, Thirsk, North Yorkshire YO7 1QR.

**Open:** 1030 – 1630

**Admission:** adults £4.00, children £2.00, family £10.00.

**Features:** 12+ layouts, trade support, demonstrations.

**Contact:** 07766 697708

www.expo-thirsk.co.uk

### **Wednesday 31 July** **BISHOP'S LYDEARD, Somerset**

**Organiser:** Taunton MRG.

**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.

**Open:** 1030 – 1630

**Admission:** charges apply.

**Amenities:** light refreshments.

**Contact:** 07879 852736

## **AUGUST**

### **Saturday 3 August** **MINEHEAD, Somerset**

**Organiser:** Exmoor Coast Railway Modellers.

**Venue:** The Minehead Eye, Mart Road, Minehead, Somerset TA24 5BJ.

**Open:** 1030 – 1630

**Admission:** adults £6.00, children £2.00, family (2+2) £13.00; prices include a show guide (whilst stocks last).

**Amenities:** hot and cold refreshments, disabled-friendly venue, pay & display parking opposite, West Somerset Railway terminus next door.

**Features:** Exmoor-Rail 2019 – layouts include *Amiens 1918* (OO9), *Ashton Meadows* (OO), *Bicsdale* (OO) *Caradon Junction* (OO) *Elling* (N), *Moose Creek* (HO), *Mutton* (OO), *Tormouth Quay* (OO), *Trenance* (OO), *Tudwick Road Siding* (O), *Vale Of Oxbury* (N), *Wantage Tramway* (OO9), *Woody Lane* (OO9). Comprehensive trade support, demonstrations, society stands.

**Contact:** David Nelhams 01643 706736

### **Saturday 3 August** **STRATFORD UPON AVON, Warwickshire**

**Organiser:** South Warwickshire Narrow Gauge Modellers.

**Venue:** Stratford Upon Avon Methodist Church, Old Town, Stratford Upon Avon, Warwickshire CV37 6BG.

**Open:** 1000 – 1600

**Admission:** adults £5.00, accompanied children 5 – 15 £3.00, family (2+2) £13.00. Unaccompanied children not admitted. Event in aid of charity Brain Tumour Support.

**Amenities:** light refreshments, all areas wheelchair accessible.

**Features:** layouts include *A Taste Of Alber* (TTe), *Allerdale Farm* (O9), *Beacon Point* (OO9), *Bryn Tegid* (OO9), *Campbell's Quarry* (16mm), *Caroline Castle* (OO6.5), *Castle Wharf Kendal* (OO9), *Goodmanton* (OO9), *Hardley, Stonebrook & Swale* (O-16.5), *Lambury Towers Estate Railway* (Gn15), *Norton Wharf* (OO9), *Reely Grate & Primrose Hill* (O), *Shropshire Minerals Light Railway* (O-16.5), *Ste Fayre Rhianne* (HOM), *Tany-Bwlch* (OO9), *The End Of The Line* (O-14), *The Old Mineral Line* (OO9), *Tony's Forest* (O-14), *Waltham Wharf* (16mm). Trade support, demonstration, society stands.

**Contact:** none given, TBA

### **Saturday 3 & Sunday 4 August** **HADLOW DOWN, East Sussex**

**Organiser:** Tinkers Park (Model Railways plus).

**Venue:** Tinkers Park, Hadlow Down, East Sussex TN22 4HS.

**Open:** 1000 – 1700 both days.

**Admission:** adults £7.00, children £2.00.

**Amenities:** light refreshments, real ale, wheelchair access, free parking.

**Features:** 18+ layouts, trade support, narrow gauge railway, traction engine trailer rides plus bus rally on Sunday only.

**Contact:** www.tinkerspark.com

### **Saturday 3 & Sunday 4 August** **REDCAR, Cleveland**

**Organiser:** Cleveland MRC.

**Venue:** Redcar & Cleveland College, Corporation Road, Redcar TS10 1EZ.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.

**Admission:** adults £4.00, under 16s £3.50, under 5s free, family (2+2) £13.50.

**Amenities:** hot and cold refreshments, good disabled access, free onsite parking, Redcar railway station 10mins.

**Features:** 14+ layouts including club's *Stockton* (N) for the first time; also *Abbotts Ford*, *Albion Estates*, *Bullet Train*, *Canalside Ironworks*, *East Dock*, *Falcon Road*, *Grosmont*, *Lomond Street*, *Long*

*Framlington*, *New Marlton*, *Raventhorpe*, *Redcar*, *The Depots*, plus Thomas layout, Hornby Dublo three-rail, and shunting puzzle. Trade support, demonstrations, club sales.

**Contact:** www.clevelandmrc.club

### **Wednesday 7 August** **BISHOP'S LYDEARD, Somerset**

**Organiser:** Taunton MRG.

**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.

**Open:** 1030 – 1630

**Admission:** charges apply.

**Amenities:** light refreshments.

**Contact:** 07879 852736

### **Thursday 8 August** **WELLING, Kent**

**Organiser:** Association of Bexley Charities 78 Toy and Train Fair.

**Venue:** Falconwood Community Centre, The Green, Welling, Kent DA16 2PG.

**Open:** 1730 – 2130

**Admission:** adults £1.50, accompanied children free.

**Amenities:** refreshments available, free level parking. Buses on route B16 from Welling and Falconwood railway stations stops outside the venue.

**Features:** sales stands with new and second-hand model railways, spares, tinplate, diecast vehicles and much more.

**Contact:** 0208 310 5018

www.bexleych.wix.com/

bexley-charities-78

### **Saturday 10 August** **BEXHILL-ON-SEA, East Sussex**

**Organiser:** Bexhill MRC.

**Venue:** St Richards Catholic College, Ashdown Road, Bexhill-on-Sea, East Sussex TN40 1SE.

**Open:** 1000 – 1700

**Admission:** adults £5.00, children £2.00, family £12.00.

**Amenities:** refreshments, free parking.

**Features:** layouts, trade support.

**Contact:** www.bexhillmrc.org.uk

### **Saturday 10 August** **HEYWOOD, Lancashire**

**Organiser:** Heywood MRG.

**Venue:** Club Rooms, Unit 4, Park Works, River Street, Heywood OL10 4AB.

**Open:** 1000 – 1600

**Admission:** free. Donations appreciated.

**Amenities:** light refreshments, ample free parking.

**Features:** open day – layouts in N to G, bring your own locos to run, dc or DCC.

**Contact:** www.hmrg.co.uk

### **Saturday 10 & Sunday 11 August** **LEYLAND, Lancashire**

**Organiser:** Leyland MRC.

**Venue:** Leyland Civic Centre, West Paddock, Leyland PR25 1DH.

**Open:** 1000 – 1700 Saturday  
1000 – 1630 Sunday.

**Admission:** adults £5.00, children £3.50, under 5s free, family (2+2) £13.50.

**Amenities:** refreshments, full disabled access. Buses from Leyland railway station 109, 111, 112, 113.

**Features:** layouts, trade support, children's activity corner, demonstrations, club stand.

**Contact:** David White 07796 130295

### **Saturday 10 & Sunday 11 August** **SKIPTON, North Yorkshire**

**Organiser:** Skipton & District Railway Society.

**Venue:** Skipton Academy, Gargrave Road, Skipton BD23 1UQ.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.

**Admission:** adults £5.00, accompanied children under 16 free.

**Amenities:** refreshments, free parking.

**Features:** layouts *Crioch* (EM), *Croglam Castle* (OO), *Gibson* (OO), *Kexby* (OO), *Kings Cutting* (N), *Wards End* (O), *Winter Lane* (OO), *Zweitesfeld* (N). Trade sup-

port, demonstrations by members of Keighley MRC, club stands.

**Contact:** David Seymour 07714 579015  
www.skiptonrailsoc.org.uk

### **Saturday 10 & Sunday 11 August** **WELLS, Somerset**

**Organiser:** Wells Railway Fraternity.

**Venue:** The Town Hall, Market Place, Wells, Somerset BA5 2RB.

**Open:** 1030 – 1730 Saturday  
1000 – 1630 Sunday.

**Admission:** adults £7.00, children and wheelchair users £4.00, accompanied children 14 and under free.

**Amenities:** refreshments, charged parking nearby.

**Features:** Railwells 2019 – layouts include *Black Lion Crossing* (EM), *Brixham* (N), *Church Warsop* (EM), *Faringdon* (P4), *Great Bardfield* (P4), *Great Swilling* (EM), *Ilkley Engine Shed 1958* (P4), *Llangerisech* (2FS), *Marest, France August 1944* (OO), *Sefton Park* (T), *Sheepcroft* (EM), *The Somersetshire Coal Canal At Wellow* (P4), *Totnes* (N). Event includes Scalefour Southwest and special EM section, promoting the EM Gauge Society's Peco-produced track.

**Contact:** www.railwells.com

### **Sunday 11 August** **LLWYNCELYN, Ceredigion**

**Organiser:** West Wales Model Railway Exhibition.

**Venue:** Llwyncelyn Memorial Hall, Llwyncelyn, Ceredigion SA46 0HF.

**Open:** 1000 – 1630

**Admission:** adults £4.00, children 5 – 15 £2.00, family (2+2) £10.00.

**Amenities:** refreshments, free parking with disabled parking at entrance. Venue is on A487 2 miles south of Aberaeron.

**Features:** 10+ layouts/displays, trade support, books and ephemera.

**Contact:** 01545 571634

### **Sunday 11 August** **SHERINGHAM, Norfolk**

**Organiser:** Dakota Dibben.

**Venue:** Sheringham High School, Holt Road, Sheringham, Norfolk NR26 8ND.

**Open:** 1000 – 1700

**Admission:** adults £5.00, children £2.00, family £12.00.

**Contact:** 07595 255150

### **Wednesday 14 August** **BISHOP'S LYDEARD, Somerset**

**Organiser:** Taunton MRG.

**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.

**Open:** 1030 – 1630

**Admission:** charges apply.

**Amenities:** light refreshments.

**Contact:** 07879 852736

### **Saturday 17 August** **BRIGHTON, Brighton & Hove**

**Organiser:** Brighton MRC.

**Venue:** Clubrooms, London Road Railway Station, Shaftesbury Place, Brighton, Brighton & Hove BN1 4QS.

**Open:** 1000 – 1400

**Admission:** free.

**Features:** train sales day – a selection of preowned locomotives, rolling stock, track, scenic items and books. If you are interested in joining the club please come and have a look round and chat with the club members.

**Contact:** www.brightonmrc.com

### **Saturday 17 August** **BROMFIELD, Shropshire**

**Organiser:** Craven Arms & District MRC.

**Venue:** Ludlow Racecourse, Bromfield, Shropshire SY8 2BT.

**Open:** 1000 – 1600

**Admission:** adults £3.50, children free.

**Amenities:** free parking, disabled access, refreshments.

**Features:** 14 layouts in various gauges, trade support, static displays.

**Contact:** 01588 672145



### **Saturday 17 August GUILDFORD, Surrey**

**Organiser:** Astolat Model Railway Circle.  
**Venue:** National Trust, Dapdune Wharf, Wharf Road, Guildford, Surrey GU1 4RR.  
**Open:** 1100 – 1700  
**Admission:** National Trust members admitted to site free; otherwise adults £4.20, children £2.50, family £12.00.  
**Amenities:** free on-site parking, wheelchair friendly, NT tea rooms plus NT boat trips at extra cost.  
**Features:** club open day – club and members' layouts including 24' long OO layout of Savernake. OO & N test track, second-hand sales and loco doctor.  
**Contact:** David Wilde 01483 722098  
www.astolatmrc.co.uk

### **Saturday 17 August MILTON KEYNES**

**Organiser:** Silver Fox DCC MRC.  
**Venue:** Oakgrove School, Venture Gate, Milton Keynes MK10 9JQ.  
**Open:** 1000 – 1600  
**Admission:** adults £6.00, children 5 – 15 £2.00, under 5s free. Cash only.  
**Amenities:** hot and cold refreshments, free parking, wheelchair access.  
**Features:** 30+ layouts in various scales/gauges, including children's *Thomas At Sodor Island* layout. Trade support.  
**Contact:** www.silverfoxdcc.co.uk

### **Saturday 17 August NORTH WALSHAM, Norfolk**

**Organiser:** Midland & Great Northern Joint Railway Society.  
**Venue:** The Atrium, Spenser Avenue, North Walsham, Norfolk NR28 9HZ.  
**Open:** 1000 – 1600  
**Admission:** adults £5.00, concessions £4.00, juniors £2.00, family (2+2) £12.00.  
**Amenities:** light refreshments, ample free parking.  
**Features:** commemorating the 60th anniversary of the closure of the M&GN – layouts in O, OO and N, trade support, demonstrations.  
**Contact:** 01692 404342

### **Saturday 17 August PEWSEY, Wiltshire**

**Organiser:** Wiltshire Group of the OO9 Society.  
**Venue:** Bouverie Hall, North Street, Pewsey, Wiltshire SN9 5ES.  
**Open:** 1000 – 1600  
**Admission:** £4.00.  
**Features:** approx 15 layouts, trade, lardy cake plus MOMING'19 (MOdelling MINimum Gauge).  
**Contact:** Andy Cundick 01672 562674

### **Saturday 17 & Sunday 18 August BORTH Y GEST, Gwynedd**

**Organiser:** Paul Towers.  
**Venue:** Church Room, below St Cygnar's Church, Borth y Gest, Nr Porthmadog, Gwynedd LL49 9LU.  
**Open:** 1100 – 1700 Saturday  
1030 – 1600 Sunday.  
**Admission:** adults £2.00, accompanied children under 12 free.  
**Amenities:** light refreshments, disabled access.  
**Features:** model railway show – layouts, second-hand sales; come and have a chat in a clubroom atmosphere. Dogs with their walkers welcome!  
**Contact:** Paul Towers 01766 514889

### **Saturday 17 & Sunday 18 August CRICH, Derbyshire**

**Organiser:** Model tram, trolleybus, bus and railway exhibition.  
**Venue:** The National Tramway Museum, Crich Tramway Village, Crich DE4 5DP.  
**Open:** 1000 – 1700 both days.  
**Admission:** normal museum entry charges apply.  
**Features:** layouts include *Bridgesand, Combourg, Depot De Porte Cochon, Falcon Travel, Fleetwood, Leyburn Avenue – Norbreck Blackpool, Little Bispham To Starr Gate The Quick Way,*

*The Bridge, Tissington, Upsdown Tramway, Whitburn Corporation Tramway.*  
**Contact:** 01773 854321  
www.tramway.co.uk

### **Saturday 17 & Sunday 18 August HARMAN'S CROSS, Dorset**

**Organiser:** Dakota Dibben.  
**Venue:** Harman's Cross Village Hall, Haycraft's Lane, Harman's Cross, Swanage, Dorset BH19 3EB.  
**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.  
**Admission:** adults £5.00, children £2.00, family £12.00.  
**Amenities:** venue is next door to the Swanage Railway.  
**Contact:** 07595 255150

### **Saturday 17 & Sunday 18 August LEICESTER**

**Organiser:** Leicester MRG.  
**Venue:** St Margaret's Church, St Margaret's Street, Leicester LE1 3EB.  
**Open:** 1030 – 1700 Saturday  
1300 – 1600 Sunday.  
**Admission:** by donation (£3.00 is suggested).  
**Amenities:** refreshments, limited disabled parking on site, other parking sites available nearby. Venue is opposite main bus station in city.  
**Features:** layouts by members of the LMRG and Market Harborough MRG.  
**Contact:** Brian Gillespie 0116 284 8684  
www.lmrg.co.uk

### **Saturday 17 & Sunday 18 August MARGATE, Kent**

**Organiser:** Hornby Hobbies.  
**Venue:** Hornby Hobbies, Westwood Industrial Estate, Margate, Kent CT9 4JX.  
**Open:** 1000 – 1600 both days.  
**Admission:** adults £10.00, concessions (seniors and children 5 – 15) £6.00, under 5s free, family (2+up to 4) £26.00. Prebooking advised via website. Prebooked ticket holders admitted 0930 both days.  
**Amenities:** live band, BBQ and more.  
**Features:** access to Hornby's heritage and facilities; entry to Hornby visitors' centre museum; model railways and more inside the former factory building; view the One:One collection of locomotives and rolling stock for the first time.  
**Contact:** 01843 233524  
www.hornby.com/hvcopenweekend

### **Saturday 17 & Sunday 18 August PICKERING, North Yorkshire**

**Organiser:** Scarborough & District Railway Modellers.  
**Venue:** The Memorial Hall, Potter Hill, Pickering, North Yorkshire YO18 8AA.  
**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.  
**Admission:** £5.00, under 16s accompanied by an adult free.  
**Features:** 10 layouts, trade support, demonstration.  
**Contact:** www.sdrmweb.co.uk

### **Saturday 17 & Sunday 18 August ROSS ON WYE, Herefordshire**

**Organiser:** Ross on Wye Model Railway Exhibition.  
**Venue:** John Kyrle High School, Ledbury Road, Ross on Wye HR9 5HT.  
**Open:** 1000 – 1700 Saturday  
1000 – 1630 Sunday.  
**Admission:** adults £4.00, children £3.00, family £10.00. All proceeds to local charities and good causes.  
**Amenities:** free parking, refreshments, disabled access.  
**Features:** 20+ exhibits, trade support.  
**Contact:** Brian Cole 01594 543330

### **Saturday 17 & Sunday 18 August ST ANDREWS, Fife**

**Organiser:** East Neuk MRC.  
**Venue:** St Andrews Town Hall, Queen's Gardens, St Andrews, Fife KY16 9TA.  
**Open:** 1000 – 1730 Saturday  
1030 – 1700 Sunday.

**Admission:** adults £4.00, children £2.50.  
**Features:** 10 layouts, trade support.  
**Contact:** www.eastneukmrc.co.uk

### **Sunday 18 August SHAP, Cumbria**

**Organiser:** Shap Local History Society.  
**Venue:** Shap Memorial Hall, Main Street, Shap, Cumbria CA10 3NL.  
**Open:** 1030 – 1630  
**Admission:** £3.00, children 16 and under free.  
**Amenities:** refreshments available, disabled parking, access and toilet facilities.  
**Features:** layouts, static models, hands-on opportunities for children.  
**Contact:** www.theoldcourthouse.org

### **Wednesday 21 August BISHOP'S LYDEARD, Somerset**

**Organiser:** Taunton MRG.  
**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.  
**Open:** 1030 – 1630  
**Admission:** charges apply.  
**Amenities:** light refreshments.  
**Contact:** 07879 852736

### **Saturday 24 August BUDE, Cornwall**

**Organiser:** North Cornwall & Devon Model Rail Club.  
**Venue:** Bude Park House Centre, Ergue-Gaberic Way, Bude EX23 8LD.  
**Open:** 1000 – 1600  
**Admission:** £3.00, accompanied children free.  
**Contact:** Roger Winterbourne 01409 220089

### **Saturday 24 August HILLINGDON, Greater London**

**Organiser:** Hillingdon Railway Modellers.  
**Venue:** Yiewsley Baptist Church Hall (side entrance), 74 Colham Avenue, Yiewsley, Hillingdon UB7 8HF.  
**Open:** 1000 – 1600  
**Admission:** £4.00.  
**Amenities:** refreshments will be available all day. Nearest station West Drayton. Local buses 222, 350, U1, U3 & U5.  
**Features:** Test Track Open Day – not an exhibition but a chance to meet fellow modellers and try out your O, OO or N gauge models on the club's three test tracks. 12V dc and DCC is available, but no live steam, please. Expert help is on hand from experienced modellers in case of any problems with your models, and new members are very much welcome.  
**Contact:** Peter Storey 020 8368 4090  
www.hillingdonrailwaymodellers.co.uk

### **Saturday 24 August NORTHFIELD, West Midlands**

**Organiser:** Bournville MRC.  
**Venue:** The Hollymoor Centre, 8 Manor Park Grove, Northfield, Birmingham, West Midlands B31 5EU.  
**Open:** 1000 – 1630  
**Admission:** adults £5.00, concessions (over 65s) £4.00, accompanied children free, family £9.00.  
**Amenities:** refreshments.  
**Features:** variety of layouts, demonstrations and trade support.  
**Contact:** www.bournvillemrc.co.uk

### **Saturday 24 August ROADE, Northamptonshire**

**Organiser:** Northampton & District MRC.  
**New venue:** Roade Village Hall, Bailey Brooks Lane, Roade NN7 2LS.  
**Open:** 1000 – 1600  
**Admission:** adults £4.00, children £2.00, family (2+2) £8.00.  
**Amenities:** refreshments, tombola.  
**Features:** varied range of layouts in several gauges, plus trade support, demonstrations, society stands.  
**Contact:** www.ndmrc.org

### **Saturday 24 & Sunday 25 August HAWICK, Scottish Borders**

**Organiser:** Hawick & District Railway Society.

**Venue:** The Auld Baths, Bath Street, Hawick, Scottish Borders TD9 7DP.  
**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.  
**Admission:** adults £4.00, seniors £3.00, children £2.00.  
**Amenities:** refreshments, car parking nearby.  
**Features:** Border Rail 2019 – approx 10 layouts, trade support, demonstrations, society stands.  
**Contact:** Ian Bell 01450 375787 or Bill Renwick 01450 374363

### **Saturday 24 & Sunday 25 August HIGHBRIDGE, Somerset**

**Organiser:** Burnham & District MRC.  
**Venue:** King Alfred School, Burnham Road, Highbridge, Somerset TA9 3EE.  
**Open:** 1000 – 1600 both days.  
**Admission:** adults and teenagers £5.00, accompanied children up to 13 free.  
**Amenities:** refreshments by 'Country Bumpkins Catering', free parking, disabled access. Railway station 800yds.  
**Features:** Sedgemoor Rail 2019 – 15 layouts in O to OO9. Full trade support.  
**Contact:** www.burnhamanddmrc.co.uk

### **Saturday 24 & Sunday 25 August MACHYNLLETH, Powys**

**Organiser:** Corris Railway Society.  
**Venue:** Y Plas, Machynlleth SY20 8DL.  
**Open:** 1000 – 1630 both days.  
**Admission:** adults £4.00, seniors £3.50, children £2.00, family (2+2) £10.00.  
**Amenities:** refreshments, light snacks, free parking close by.  
**Features:** model railway exhibition; many layouts in various scales with an emphasis on narrow gauge, including *Crosby* (OOn3) and *Roarkes Landing* (HOn30). Trade support, society stands, tombola, plus full-size trains running on the revived Corris Railway 4 miles away.  
**Contact:** John Simms 01823 323106  
www.corris.co.uk

### **Saturday 24 to Monday 26 August BEER, Devon**

**Organiser:** Pecorama.  
**Venue:** The Station Gallery, Pecorama, Beer, Seaton, Devon EX12 3NA.  
**Open:** 1000 – 1700 both days  
**Admission:** free (normal Pecorama admission prices apply).  
**Amenities:** parking, refreshments, disabled access (hilly site renders three-wheel mobility scooters unsuitable).  
**Features:** *The Andeer Line* (Swiss HOm modular layout by Julia & Derek Boswell). Visitors are invited to help operate the layout, which employs digital control, with a central 'signal box' and trains driven individually using smart phones and tablets. On request, there will also be practical demonstrations of the foam base-board construction and the use of electrostatic grass. Drop in for as long as you wish. See website for details.  
**Contact:** www.pecorama.co.uk

### **Saturday 24 to Monday 26 August GAINSBOROUGH, Lincolnshire**

**Organiser:** Gainsborough MRS.  
**Venue:** Florence Terrace, Gainsborough, Lincolnshire DN21 1BE.  
**Open:** 1330 – 1730 Saturday & Sunday, 1030 – 1730 Monday (last admission 1630)  
**Admission:** adults £4.00, seniors/children £3.00, family (2+2) £10.00.  
**Features:** open day. One of the largest O gauge model railways in the country; dozens of trains running on a total of over half a mile of track.  
**Contact:** B Hodgkinson 01427 610475  
www.gainsboroughmodelrailway.co.uk

### **Sunday 25 August BROXBORNE, Hertfordshire**

**Organiser:** Hoddesdon Model & Railway Club.  
**Venue:** The Old Mill, Mill Lane, Broxbourne, Hertfordshire EN10 7AX.  
**Open:** 1000 – 1600



**Admission:** adults £3.00, children £1.00.  
**Amenities:** light refreshments, disabled parking at venue, other free parking at EN10 6TD.  
**Features:** working indoor and outdoor layouts from N to 5". Children's rides, radio controlled boats & lorries, working fairground models and more. Trade support and tombola.  
**Contact:** Paul 01707 898420  
[www.hoddesdonmrc.org.uk](http://www.hoddesdonmrc.org.uk)

**Sunday 25 August**  
**KEIGHLEY, West Yorkshire**  
**Organiser:** Aire Valley Railway Modellers Club.  
**Venue:** Melbourne House, Keighley, West Yorkshire BD21 4LG.  
**Open:** 1000 – 1600  
**Admission:** free, donations welcome.  
**Amenities:** refreshments available, venue only 5mins from Keighley station and K&WVR, but access is by stairs only.  
**Features:** 12+ layouts in N, TT, OO and OO9 plus trade support.  
**Contact:** [www.avmrc.org.uk](http://www.avmrc.org.uk)

**Monday 26 August**  
**HARROGATE, North Yorkshire**  
**Organiser:** Harrogate MRG.  
**Venue:** Bilton Grange United Reformed Church, corner of Skipton Road and Woodfield Road, Harrogate HG1 4LP.  
**Open:** 1000 – 1600  
**Admission:** adults £4.00, children £2.00, family (2+2) £10.00.  
**Amenities:** disabled access, refreshments available.  
**Features:** 10+ layouts in a variety of scales and gauges, trade support, demonstrations.  
**Contact:** 07534 829050

**Wednesday 28 August**  
**BISHOP'S LYDEARD, Somerset**  
**Organiser:** Taunton MRG.  
**Venue:** Platform 1, Bishop's Lydeard Station, West Somerset Railway, Taunton, Somerset TA4 3BX.  
**Open:** 1030 – 1630  
**Admission:** charges apply.  
**Amenities:** light refreshments.  
**Contact:** 07879 852736

**Saturday 31 August**  
**TADWORTH, Surrey**  
**Organiser:** North Downs Model Railway Circle.  
**Venue:** The Good Shepherd Church Hall, Station Approach Road, Tadworth, Surrey KT20 5AE.  
**Open:** 1000 – 1700  
**Admission:** adults £6.00, concessions (seniors/disabled) £5.00, under 16s £3.00. All profits to Good Shepherd Church.  
**Amenities:** free parking, venue is close to station and bus stop, refreshments, facilities for disabled visitors.  
**Features:** 27 layouts, demonstration, trade support.  
**Contact:** Stuart Robinson 07758 458827  
[www.ndmrc.info/Home/Tadworth.htm](http://www.ndmrc.info/Home/Tadworth.htm)

**Saturday 31 August & Sunday 1 September**  
**ALNWICK, Northumberland**  
**Organiser:** Aln Valley Railway Model Railway Exhibition.  
**Venue:** Lionheart Station, Lionheart Enterprise Park, Alnwick NE66 2EZ.  
**Open:** 1030 – 1630  
**Admission:** adults £4.00, children £2.00.  
**Amenities:** café, parking, museum, shop.  
**Features:** layouts include *Alwinton* (OO), *Catlin Twin Oaks* (OO), *Criddon Lane* (OO9), *Holegn Bridge* (OO9), *Llantrevelyn* (OO9), *More Friendly Engines* (OO), *Mynydd Fach* (OO9), *Ribbleton Moor* (OO), *Schlosshalt 2* (HO), *Scrooge Street* (OO), *Shunting Puzzle* (OO) and others to be confirmed. Standard gauge steam running on site.  
**Contact:** [www.alnvalleyrailway.co.uk](http://www.alnvalleyrailway.co.uk)

**Saturday 31 August & Sunday 1 September**  
**LOUGHBOROUGH, Leicestershire**  
**Organiser:** Soar Valley MRC.  
**Venue:** Loughborough Grammar School, Leicester Road, Loughborough, Leicestershire LE11 2AQ.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £8.00, children £4.00, family (2+3) £20.00.  
**Amenities:** refreshments, disabled access, free car parking.  
**Features:** layouts include *Arnold Lane* (OO), *Axmouth* (OO), *Barden* (O), *Batterdale* (N), *Bear Creek* (N), *Bergeller Bahn* (HOM), *Burnham on Sea* (N), *Charnwood Forest Branch Line* (O), *Croft Spa* (N), *Cuman ava go* (OO), *Dilthorpe Colliery* (OO), *East Dock* (EM), *Ellasweet Beet Farm* (OO), *Emsworth Hope Street* (OO), *Feny Quay* (OO), *Glenellen Castle* (N), *Great Minion Railway* (OO), *Greenacre Yard* (P4), *Hackworth* (OO), *Knot Littlefield* (N), *La Chaudanne-Les-Moulins* (HOM), *Leicester (Belgrave Road)* (OO), *Padden Flatts* (HO), *Poulter Bank* (OO), *Red Hook Bay* (HO), *Republic Steel* (Z), *South Town* (4mm), *Sowters Lane* (OO9), *St Frazal d'Ardèche* (Nm), *Timber Valley* (OO9), *Two Seasons* (N), *Uppingham* (3mm), *Utah & Colorado Western Railroad* (On30), *Weaver Hill* (OO), *Welby Lane* (OO). Plus Thomas & Friends, vintage O gauge electric layout and multi gauge test track. Full trade support, demonstrations.  
**Contact:** 07761 937725  
<http://www.svmrc.co.uk>

**Saturday 31 August & Sunday 1 September**  
**TELFORD, Telford & Wrekin**  
**Organiser:** Gauge O Guild.  
**Venue:** International Centre, St Quentin Gate, Telford TF3 4JH.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** £15.00, GOG members £10.00. Advance tickets available from Alan Jones, 8 Upper Cottbridge Terrace, Murrayfield, Edinburgh EH12 6AD (please include SAE and make cheques payable to Gauge O Guild Ltd). Tickets valid for both days and allow entry for purchaser, partner and two children under 16.  
**Amenities:** refreshments, disabled access. Shuttle bus to/from venue and railway station.  
**Features:** layouts, trade support, demonstrations. Guild AGM Sunday afternoon.  
**Contact:** [www.gauge0guild.com](http://www.gauge0guild.com)

## MEETINGS

**Thursday 1 August, 1900hrs**  
**RCTS – Bristol**  
 Dinner at the Hope & Anchor, Midford.  
**Contact:** Ronnie McAdam 0131 669 7503

**Saturday 3 August, 1300hrs**  
**Swiss Railways Society – Yorkshire Branch**  
*Switzerland – My First Visit* – a member's recollections of a first trip.  
**Venue:** Middleton Railway (Meeting Room First Floor), Moor Road, Hunslet, Leeds LS10 2JQ.  
**Contact:** Philip Lockwood 0113 253 2176

**Monday 5 August, 1930hrs**  
**Peak Rail Association – Sheffield Branch**  
*A Scenic Railway Tour Of Southern France* by Mike Bunn.  
**Venue:** The Harlequin, 108 Nursery Street, Sheffield S3 8GG.  
**Contact:** Dave Sharp 0114 274 5478  
[www.peakrail.co.uk/sheffieldbranch](http://www.peakrail.co.uk/sheffieldbranch)

**Tuesday 6 August, 1930hrs**  
**Gravesend Railway Enthusiasts Society**  
*Funicular Railways* by Dave Hanger.  
**Venue:** Emmanuel Baptist Church, Windmill Street, Gravesend DA12 1BB.  
**Admission:** non-members £1.00  
**Contact:** Henry Harryman 01322 370148

**Wednesday 7 August, 1930hrs**  
**Burton Railway Society**  
*I Moved It My Way* by Andrew Goodman.  
**Venue:** Marston's Sports & Social Club, Shobnall Road, Burton upon Trent, Staffordshire DE14 2BD.  
**Admission:** non-members £2.00  
**Contact:** Mark Ratcliffe 01283 221537  
[www.burtonrailwaysociety.co.uk](http://www.burtonrailwaysociety.co.uk)

**Saturday 10 August, 1930hrs**  
**Shap Local History Society**  
*Steam Over Shap* by Jean Scott-Smith.  
**Venue:** The Old Courthouse, Main Street, Shap, Cumbria CA10 3NL.  
**Admission:** £3.00. Places limited, please book in advance.  
**Contact:** [www.theoldcourthouse.org](http://www.theoldcourthouse.org)

**Sunday 11 August**  
**RCTS – South East**  
 Coach trip to Chinnor & Princes Risborough Railway and Didcot Railway Centre.  
**Venue:** The Railway Social Club, Beaver Road, Ashford, Kent TN23 7RR.  
**Contact:** [southeast@rcts.org.uk](mailto:southeast@rcts.org.uk)

**Monday 12 August, 1930hrs**  
**Lincoln Railway Society**  
*From Railways To Royalty* by Jack Boskett.  
**Venue:** St Hugh's Church, Harewood Crescent, North Hykeham, Lincoln LN6 8JG.  
**Admission:** visitors £3.00  
**Contact:** 01522 698085  
[grahamlightfoot1948@gmail.com](mailto:grahamlightfoot1948@gmail.com)

**Monday 12 August, 1930hrs**  
**SEG – Sussex Branch**  
*The Brighton Main Line In Sussex* by John Blackwell.  
**Venue:** The Deall Room, Southwick Community Centre, BN42 4TE.  
**Admission:** non-members £3.00  
**Contact:** [stuart.hicks90@gmail.com](mailto:stuart.hicks90@gmail.com)

**Tuesday 13 August, 1945hrs**  
**Pewsey Vale Railway Society**  
*Great Eastern Lines 1954 – 1962* by Ken Livermore.  
**Venue:** Woodborough Social Club, SN9 5PL.  
**Contact:** 07899 967221

**Tuesday 13 August**  
**RCTS – Merseyside, Chester and North Wales Branch**  
 Gloucestershire Warwickshire Steam Railway.  
**Contact:** Alan Turton 01606 854227  
[Merseyside@rcts.org.uk](mailto:Merseyside@rcts.org.uk)

**A 'Brit' at Liverpool Street – the ex-GE lines are the topic for the Pewsey Vale Railway Society talk on 13 August.**  
**Photo:** Dave Cobbe Coll./Rail Photoprints



**Thursday 15 August**  
**RCTS – West Riding**  
*Observations At Doncaster.*  
**Contact:** [w.r.bobgreen@gmail.com](mailto:w.r.bobgreen@gmail.com)

**Friday 16 August, 1930hrs**  
**Great Western Society – Taunton**  
*Fifteen Minutes Each* with m/c Francis Lewis.  
**Venue:** Village Hall, Stoke St Mary, Nr Taunton, Somerset TA3 5DE.  
**Contact:** 01823 334188

**Monday 19 August, 1930hrs**  
**SLS – South Lakeland**  
*Railway Images Taken By Percy Duff.*  
**Venue:** The Old Courthouse, Main Street, Shap, Cumbria CA10 3NL.  
**Admission:** £3.00. Places limited, please book in advance.  
**Contact:** [www.theoldcourthouse.org](http://www.theoldcourthouse.org)

**Tuesday 20 August**  
**RCTS – Thames Valley**  
*Observations At Didcot Parkway Railway Station.*  
**Contact:** Fixtures Secretary Andrew Jenkins 01793 783749; [tv-fixtures@rcts.org.uk](mailto:tv-fixtures@rcts.org.uk)

**Tuesday 20 August, 1930hrs**  
**The Leicester Railway Society**  
 Members' evening.  
**Venue:** The Cricketers, Grace Road, Leicester LE2 8AD.  
**Contact:** Stephen Hewitt 07980 008370  
[www.leicesterrailwaysociety.com](http://www.leicesterrailwaysociety.com)

**Saturday 24 August, 1430hrs**  
**Shap Local History Society**  
*Railway Sites In Shap* – a guide walk with Jean Scott-Smith.  
**Venue:** meet at The Old Courthouse, Main Street, Shap, Cumbria CA10 3NL.  
**Admission:** £3.00. Places limited, please book in advance.  
**Contact:** [www.theoldcourthouse.org](http://www.theoldcourthouse.org)

**Saturday 31 August, 1400hrs**  
**T&LRS – East Midlands Area**  
 Bring a thing – your opportunity to discuss your current tram interest.  
**Venue:** Sixth Beeston Scout Hut, Middle Street, Beeston, Nottingham NG9 1GA.  
**Contact:** David Hanger 01455 203307

## CLUB NEWS

Ian Pennicott writes: "As secretary of the **Wittering and District Railway Circle**, it is my very sad duty to inform readers that our model railway club has now disbanded and has been wound up. The proceeds of the club have been distributed to various preservation societies in Southern England and a local model railway club in Bognor. We had no financial worries; it was down to a lack of new members coming to join us, and a lack of affordable permanent premises that was the deathknell of the club. At the end there was just three of us, Chairman, Treasurer and Secretary."



# RAILWAY MODELLER

## Opportunity Yours

### Classified Advertisements

TO BUY, SELL OR EXCHANGE THROUGH  
THE COLUMNS OF GREAT BRITAIN'S  
BEST SELLING MODEL RAILWAY MAGAZINE

See order form on page 42a for advertising rates and conditions  
If, when responding to advertisements, you have a query which needs a  
reply, please enclose a stamped, self-addressed envelope.  
Replies to box numbers should be sent to:  
Peco Publications, Beer, Seaton, Devon EX12 3NA

## Trade

### Retailers' Announcements

**YORK - MONK BAR MODEL SHOP LTD.** 2  
Goodramgate (by Monk Bar). Large selection of new  
model railways, Scalextric, plastic kits, diecasts, etc.  
Tel: 01904 659 423.

**HULL - EAST YORKSHIRE'S ONLY SPECIALIST**  
model railway shop. Model Railways bought, sold and  
exchanged. 53A MODELS, 430 Hessle Road, Hull.  
(Closed Mondays). [www.53amodels.co.uk](http://www.53amodels.co.uk). Tel:  
01482 227 777.

**COLWYN BAY, CLWYD MODELS.** Opening times:  
Thursday, Friday + Saturday 10-1pm, 2-5pm. Second  
hand model railway books, videos, layouts, diecast  
bought, sold and exchanged. 447, Abergele Road,  
Old Colwyn. Tel: 01492 518 709.

**PLUS DAUGHTERS OF BASILDON ESSEX.** N  
gauge specialist for the UK, Continental and American  
Modeller. New and second hand, bought, sold or  
exchanged. PLUS DAUGHTERS, 8 Britannia Court,  
Burnt Mills Industrial Estate, Basildon, Essex, SS13  
1EU. Tel: 01268 726 211 [www.plusdaughters.co.uk](http://www.plusdaughters.co.uk)

**MODEL EXCHANGE.** Tel: 07580 065 017  
website: [www.themodelexchange.co.uk](http://www.themodelexchange.co.uk)  
BUY\*EXCHANGE\*SELL TOYS.

**DINGWALL (ROSS-SHIRE)-SPORTS & MODEL  
SHOP,** 66 High Street, Dingwall, IV15 9RY.  
PLASTICS-METALS-SCENICS. OO and N scale,  
PECO, Hornby, Bachmann, Metcalfe, Wills, Ratio &  
Dapol. Tel: 01349 862 346.

**LANCASHIRE - MODEL RAILWAYS AND  
DIECAST SHOP.** We buy and sell obsolete and new  
diecast and railway items. Tel: 01282 867 711.

**RHYL MODEL RAILWAYS,** 44 Wellington Road,  
Rhyl LL18 1BN. Opening times, please check  
website. Tel 01745 798460.  
[www.model-railways-rhyl.co.uk](http://www.model-railways-rhyl.co.uk)

**SHREWSBURY AND TELFORD - COME TO  
MODELSCAPE FOR YOUR MODEL RAILWAY  
NEEDS.** Open Monday 10-5, Wednesday 2-5, Friday  
2-5 and Saturday 10-12:30. Unit 8bc, Rodenhurst  
Business Park, Rodington, Shrewsbury, SY4 4QU.  
On B5062 between Roden and High Ercall. 01952  
771 115 [www.modelscape.net](http://www.modelscape.net)

### Trade Sales Products

**O GAUGE ETCHED KITS.** Locomotives, Carriages  
and Wagons. Connoisseur Models on line catalogue,  
[www.jimmcgeown.com](http://www.jimmcgeown.com) or send 2nd class address  
label for free printed catalogue. 1 Newton Cottages,  
Nr Weobley, Herefordshire, HR4 8QX.

**LOCOMOTECH MOTORIZING KITS FOR  
PECO AND SOUTH EASTERN FINECAST  
TURNABLES.** Standard (3.5rpm) £27.99, Deluxe (2  
rpm) £34.99, U.K. postage £3.25. 12V motor.  
Enclosed all-metal gearbox. Automatic Indexing -  
stops automatically at each exit. (Currently only for  
PECO LK-55/LK-555) 10 Exit version £64.99, NEW  
32 Exit Roundhouse Version £67.99. Reviewed by  
Railway Modeller May 2016. More information and  
order online at [www.locomotech.co.uk](http://www.locomotech.co.uk) Telephone  
orders: 01903 871149.

**NEW WEBSITE:**  
[www.zetlandmodelrailways.co.uk](http://www.zetlandmodelrailways.co.uk) Good quality  
second-hand North American, Continental & British,  
"O", "HO", "OO", "N" & "Z" scales.  
email [Bob@zetlandmodelrailways.co.uk](mailto:Bob@zetlandmodelrailways.co.uk)

### Trade Sales Books

**SPECIALIST IN OUT OF PRINT RAILWAY** books,  
timetables, photographs, official publications and  
relics. Lists welcome. Visitors by appointment. BARRY  
JONES, 28 Marine Crescent, Worthing, BN12 4JF.  
Tel/Fax: 01903 244 655.

**ACCIDENT REPORTS (38) 1950-1974, INCLUDES  
DAGENHAM 1958 AND CONNINGTON 1961.** £34  
postage included. SHOOTING STAR, 8 Mulberry  
Court, Holmer Green, Buckinghamshire HP15 6TF.

**RAILWAY MODELLER BACK NUMBERS** - for  
more information on which issues are available, please  
contact: Technical Advice Bureau, PECO, Underleys,  
Beer, Seaton, Devon, EX12 3NA, England, telephone:  
01297 20580. Please see website for prices.

### Trade Services

**WHITEMETAL AND BRASS KIT BUILDING  
SERVICE, QUALITY LOCOMOTIVES AT  
REALISTIC PRICES.** For costs plus current delivery  
dates, please phone 01325 382 452 or write to:  
DAVID TEMPLE, 28 Walworth Crescent, Darlington,  
County Durham, DL3 0TX.

**CUSTOM AND STANDARD DECALS. PLASTIC  
AND METAL KIT BUILDING,** modification and  
scratch building service. Locos, Rolling stock, track,  
structures and scenic items. Any scale. See  
[www.aardstorm-models.com](http://www.aardstorm-models.com)

**BASEBOARDS IN TIMBER PLYWOOD OR MDF -  
ANY SIZE AND SHAPE,** legs or trestles, any height,  
flat packs available. Phone for quote. Tel: 01795 479  
736.

**KIT BUILDING SERVICE, LOCOS, ROLLING  
STOCK, QUALITY WORK AT REASONABLE  
PRICES.** Contact: JOHN NOWELL, 28 Victoria Road,  
Tuebrook, Liverpool, L13 8AW. Tel: 0151 259 5957,  
email: [johnnowell@blueyonder.co.uk](mailto:johnnowell@blueyonder.co.uk)  
[www.tuebrooktrains.co.uk](http://www.tuebrooktrains.co.uk)

## Wanted (Trade)

This section is reserved for the use of traders only.

**WE VALUE YOUR USED MODEL RAILWAYS** very  
highly. Best prices paid for good condition modern OO  
and N Gauge second hand items. Send us your list of  
surplus items in any gauge or make (s.a.e. please) for  
our quotation by return post. We also buy tinplate of  
all kinds, Dinky Toys, Corgis and Lesneys. We will  
travel anywhere to collect. RAILWAY ROUNDABOUT,  
The Walnut Tree, Egremont Street, Glemsford,  
Suffolk, CO10 7SA. Tel: 01787 280 452, email:  
[bill.railwayroundabout@gmail.com](mailto:bill.railwayroundabout@gmail.com)

**MODEL RAILWAYS WANTED** - cash waiting for  
British & American N & OO/HO scales plus  
Continental HO model railway equipment. British and  
American O-Scale also considered along with  
interesting items of railwayana. (Closed Mondays).  
53A Models, 430 Hessle Road, Hull. 01482 227 777.  
[www.53amodels.co.uk](http://www.53amodels.co.uk)

**AIRFIX/FROG/REVELL** and other makes of unmade  
Second-hand plastic kits wanted. Aircraft especially  
wanted. Any quantity. KINGKIT, Unit 8, Cedar Court,  
Halesfield 17, Telford, TF7 4PF. Tel: 01952 586 457.

**WANTED - TRAIN COLLECTIONS O, OO, N.**  
Always calling in areas SOMERSET, DEVON,  
CORNWALL, S-WALES, WILTS, HAMPSHIRE, most  
places in UK covered. Will dismantle layouts any size.  
Interested in British N gauge Farish, Mintrix, Peco, OO  
Bachmann, Hornby, Mainline, Lima, Wrenn, Tri-ang.  
Damaged, scrap locos, Peco points, track work,  
buildings, cars, people, books, diecast, EFE buses.  
Cash paid, friendly service offered. R.FOSTER  
15 Rosebery Avenue, Yeovil, Somerset BA21 5LW.  
Tel: 01935 424165 email: [russelltrains@aol.com](mailto:russelltrains@aol.com)

**MODEL RAILWAYS ALWAYS WANTED,** including  
Hornby Dublo, Wrenn, Trix, Lima, Bachmann, Steam,  
Modern Image, OO, O, N gauge, kits and kit built.  
Send lists or give me a call. BARRY JONES,  
28 Marine Crescent, Worthing, BN12 4JF.  
Tel/Fax: 01903 244 655.

**MODEL RAILWAYS WANTED** - All Gauges  
including N, OO, O Gauge. Large collections to  
surplus items. British/American and Continental  
boxed or unboxed, no problem. Spares and track also  
purchased. We will also buy any diecast. Send list or  
telephone: MJH MODELS, S'Agaro, Goring Road,  
Woodcote, Reading, RG8 0QE. 01491 681 262  
email: [mjhtoy@aol.com](mailto:mjhtoy@aol.com)

**WE CARE A GREAT DEAL MORE!** We realise that  
your collection large or small is a prized possession,  
so we will always give our highest price, we will  
respond quickly to receiving your list and collect,  
dismantle and remove your items efficiently because  
we care. Whatever you have to sell from single items  
to large collections. Give us a call and prepare to be  
amazed! S&J MODELS. Tel: 01606 872 786  
email: [sjmodels@tiscali.co.uk](mailto:sjmodels@tiscali.co.uk)  
website: [www.sandjmodelrailways.co.uk](http://www.sandjmodelrailways.co.uk)



**THINKING OF SELLING YOUR MODEL RAILWAY COLLECTION?** If so contact me for an immediate evaluation, distance no object, collections large or small. **PAUL CULL**, telephone: 01302 481 274 or mobile: 07767 356 890 or send list to: The Bungalow, Dentons Green Lane, Kirk Sandal, Doncaster, DN3 1JP, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**HORNBY & BASSETT-LOWKE O GAUGE TRAINS**, wagons, coaches and accessories. Anything considered. Top prices paid for single items and/or complete collections. Tel: 01993 840 064 (Oxon) 0797 991 0760.

**MODEL RAILWAYS WANTED – ANY GAUGE, ANYWHERE. ALWAYS BEST CASH PRICES PAID.** Layouts dismantled. Telephone: 01924 824 748, anytime. Send your lists to: **GOING-LOCO**, 38 Potovens Lane, Lofthouse Gate, Wakefield, WF3 3JF. Email: [goinglocomodels@hotmail.com](mailto:goinglocomodels@hotmail.com)

**ALWAYS WANTED BY TRI-ANGMAN**-collections, layouts, seeking, Tri-ang, Hornby, Dublo, Wrenn, Bachmann etc. Will collect across most of the UK. More than fair prices paid. Tel: 07966 333 605. Email: [Laurence@tri-angman.co.uk](mailto:Laurence@tri-angman.co.uk) [www.tri-angman.co.uk](http://www.tri-angman.co.uk)

**WANTED-MODEL RAILWAY ITEMS, FROM ANYWHERE IN THE UK**, single items to whole collections, any gauge, we pay cash and will beat any genuine offer on collections. We also buy diecast models + vintage toys. Tel: 01209 718 562 or 07876 457 114, email: [harvey.instance@sky.com](mailto:harvey.instance@sky.com)

**HORNBY DUBLO WANTED – LOCOS, COACHES, WAGONS AND ALL RELATED ITEMS.** From single items to whole collections, cash paid, distance no object. **DARREN COOPER**. Tel: 01254 234 017.

**MODEL RAILWAY COLLECTIONS URGENTLY REQUIRED – SMALL & LARGE**, any scale and any condition. British, European and American collections all required. Nationwide collections, distances no object. Contact us today for a quick and friendly service. Telephone: 01302 371 623, mobile: 07526 768 178, email: [anoraksanonymous@googlemail.com](mailto:anoraksanonymous@googlemail.com)

**WANTED WANTED MODEL RAILWAYS, ALL MAKES, ALL SIZES** from N Gauge, OO scale to G Scale and larger, will travel all UK. We also dismantle layouts, over 35 year's experience. Send lists to **DEREK BARNETT**, 6 Vale Gardens, Penkridge, Staffs, ST19 5LQ. Tel: 01562 750 076 daytime, or text me 07721 333 521 anytime and I will ring you back, email [footplate@btconnect.com](mailto:footplate@btconnect.com)

**WANTED TOP PRICES PAID! BUY-SELL-EXCHANGE ANY GAUGE, MAKE OR AGE.** Also Specialists in diecast Lorries, cars, buses and railwayana. Friendly, fast, professional service from our family run business, established for over 40 years. "We are a real shop not just a phone number" Distance no object, from single items to lifetime collections, instant settlement by your preferred payment method. See our main advert. Deal with confidence from a name that you can trust. **Rails of Sheffield** 21/29 CHESTERFIELD ROAD, SHEFFIELD, S8 0RL. Tel: 01142 551 436. [www.railsofsheffield.com](http://www.railsofsheffield.com)

**WANT TO SELL YOUR TRAINS – ALL GAUGES, ALL MAKES?** Hornby/Bachmann, OO, O, and N gauge. Top cash prices paid, collected, layouts dismantled. Please send list to **CLAIRE CLARKE**: 14 Stourbridge Road, Lloyd Hill, Wolverhampton WV4 5NE email: [claireaclarke@aol.com](mailto:claireaclarke@aol.com) telephone: 01902 342 722.

**MODEL RAILWAYS WANTED – N GAUGE, OO GAUGE, O GAUGE – ANY MAKE.** Free valuations, home visits, layouts dismantled. Fast friendly, courteous service. Contact **MARTIN**: 01623 473 376, lists to: [modelpower@sky.com](mailto:modelpower@sky.com)

**SECOND HAND MODEL RAILWAYS WANTED – CASH PAID FOR QUALITY MINT BOXED OO/N** gauge single items to lifetime collections. Hornby, Bachmann, trains, coaches, wagons, equipment. Distance no object. Email: [info@modelrailwayemporium.com](mailto:info@modelrailwayemporium.com) or call 07725 112 352.

## Private

### Property for Sale

*Cut the expense of moving house by advertising your property in the RAILWAY MODELLER or CONTINENTAL MODELLER for just 15p a word its great value for money!!*

### Private Sales

**FOR SALE COLLINGS LANE N GAUGE LAYOUT AS SEEN IN THE MAY ISSUE 2019 ISSUE RAILWAY MODELLER.** Write for full details to **JOHN TURNER**, 19 Tilgate, Luton, Bedfordshire LU2 8RR.

**LARGE COLLECTION OF OO AND N LOCOS, COACHES, WAGONS & KITS – MOSTLY BRITISH.** Also cheap N layout 2 boarded 8' layout, needs TLC. Telephone: 07544 422882 for list.

**FOR SALE – BRIDGFORD, N GAUGE EXHIBITION LAYOUT**, 11ft 6ins X 6ft 6ins, central well, analogue, 20 storage loops, all scenery, buildings etc. Was in BRM in Jan 1998 & Railway Modeller June 2014. Trailer available if required for transport. Most of stock also for sale as I'm downsizing. Will be at Poynton on Sept 14/15 or can be erected by appt. here. Phone 07812 848649 or Email [kathall@scotland1145.plus.com](mailto:kathall@scotland1145.plus.com) Alastair Knox, Chinley, Derbyshire.

## Clubs and Societies

**GREAT EASTERN RAILWAY SOCIETY.** Railways in East London and East Anglia from the 1800's to today. Quarterly Journal and News magazines, publications and meetings. SAE for details to: **GERS**, 69 Birchanger Lane, Birchanger, Bishops Stortford, CM23 5QA, website [www.gersociety.org.uk](http://www.gersociety.org.uk)

## Wanted Clubs and Societies

**NARROW GAUGE STOCK WANTED BY THE 009 SOCIETY**, collections, loco's, rolling stock, readymade BEMO, LILLIPUT, ROCO etc or kit and scratch-built. Contact: 009 Society, 5 Quarry Road, Colehill, Wimborne, BH21 2NP. Email: [members-sales@009society.com](mailto:members-sales@009society.com) for society details see [www.009society.com](http://www.009society.com)

## Wanted Private

(This column is reserved for readers who are private Model Railway enthusiasts or collectors and require items purely for their own personal use).

**TRIX TWIN AND TRIX EXPRESS.** Avid collector. TTRCA No26, anything will be considered. Please phone anytime, will collect. Tel: 01736 754 200. Mobile: 07969 499 586.

**COLLECTOR PURCHASES QUALITY BRITISH OO COLLECTIONS**, plus 009 ON30 kit-built O fine-scale LMS/LNER. Phone for a friendly discussion: 0114 250 8009, email: [ellnitty@hotmail.com](mailto:ellnitty@hotmail.com)

**RAILWAY PHOTOGRAPHS**-original 35mm slides (or negatives) any railway subject, steam to blue diesel era. Good price for quality material. **JOHN TURNER**, 01482 227 777 or 01652 632 370 (evenings).

**N OR OO GAUGE**, British outline Model Railways sought by collector, Wrenn, Hornby, Bachmann, Farish etc. Will travel to collect. Tel: 01702 461 214 after seven, 07399 73 44 94, daytime. Send lists to: [psdavis17@talktalk.net](mailto:psdavis17@talktalk.net)

**HORNBY O GAUGE** LMS Princess Elizabeth & Southern L1. Both 20 volt electric. Tel: 01993 840 064 (Oxon) or 0797 991 0760.

**RAILWAY RELICS LOCOMOTIVE PLATES.** Station signs, clocks, posters, hand lamps, signalling, would collect. Top prices. Dale, East Barn, Loads Road, Holymoorside, Chesterfield, S42 7HW. Tel: 01246 569 263.

**HORNBY DUBLO & WRENN RAILWAYS WANTED, LOCOS, COACHES AND WAGONS.** Single items or whole collections, distance no object. Immediate cash settlement. **DARREN COOPER**, 26 Warmden Avenue, Baxenden, Accrington, Lancs BB5 2PR. Tel: 01254 234 017.

**WANTED TRIANG TT AND LONE STAR TREBLE O LECTRIC BY PRIVATE COLLECTOR.** Good price paid, will travel. Tel: 01980 862 387 or email: [hamptonrob1951@outlook.com](mailto:hamptonrob1951@outlook.com) (Wiltshire).

**WRENN 1966 TO 1992**-items wanted by collector preferably boxed, single or complete collections. Please telephone: 0773 0957 800/01582 401 053 or visit my website [www.wrennspecialist.co.uk](http://www.wrennspecialist.co.uk)

**CASH PAID ON COLLECTION OF YOUR UNWANTED MODEL RAILWAYS**, small or large collections OO or N gauge. Hornby, Lima, Bachmann, Farish etc. We will come to you and dismantle if needed. Please contact **STEVE** on: 01909 487 938 or send lists to: 27 Prince Charles Road, Worksop, Nottinghamshire, S81 7ES or email: [steve37425@hotmail.co.uk](mailto:steve37425@hotmail.co.uk)

**KIT ITEMS, WHITEMETAL, BRASS OO & O**, DJH, Wills, Millholme, Alan Gibson etc. Engines, coaches, wagons, unmade, part made, engines working or not. Kit spares – will travel. Tel: 01302 481 274, mobile: 07767 356 890, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**O GAUGE WANTED - LOCOS, ROLLING STOCK, ACCESSORIES, TRACK/POINTS, SPARES AND BUILDINGS.** Large or small collections purchased. Tel 01302 481274, Mob. 07767 356 890. Email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**G SCALE AND OTHER GARDEN RAILWAYS WANTED.** Gauge 1, SM32, Bachmann, LGB, Piko,



Accucraft, Roundhouse etc. Locomotives, rolling stock, track, points and accessories all required. Tel: 01302 371623 email: [sophiecul99@gmail.com](mailto:sophiecul99@gmail.com)

**YOUR UNWANTED RAILWAYS, SINGLE ITEMS OR QUANTITY, ALL GAUGES** – immediate cash. Tel: 0208 715 5304, (South London).

**HORNBY DUBLO AND WRENN RAILWAYS WANTED, LOCOMOTIVES, COACHES, WAGONS** and all associated items, from one item to large

collections. Happy to travel, immediate cash settlement. Jason Roger, send lists to 7 Broomways, Great Wakering, Southend on Sea, Essex SS3 0DP or email: [jaycossie@googlemail.com](mailto:jaycossie@googlemail.com) or telephone: 01702 749 183.

**IS THERE CASH IN YOUR ATTIC?** I will collect and pay best cash prices for model railway items, from single items to complete collections. Call GEOFF, for a no obligation chat on: 07986 936 058 or email me on: [cash4trains@live.co.uk](mailto:cash4trains@live.co.uk)

**AGEING ENTHUSIAST SUFFERING BOUTS OF NOSTALGIA WOULD LIKE TO PURCHASE** pre-1962 Bound Volumes of Railway Modeller and Model Railway Constructor. Can't travel far but postage no problem. Telephone JOHN: 07484 133 861.

**WANTED LIMA O GAUGE 33 DIESEL RAILFREIGHT GREY/BLUE;** MK1 coaches blue/green/maroon; Lima wagons/freightliners Dapol 08; Lima 0-4-0. Tel: 07711 631 800, email: [kevin.gill1@btconnect.com](mailto:kevin.gill1@btconnect.com)

# Swapmeets • Sales • Fairs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so. All advertisements include the organiser's town and telephone number. The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

## July 2019

**14 July (Sun) 10.30am-3pm**

**COVENTRY – 150 STALLS**, The Connexion, Ryton on Dunsmore, Coventry CV8 3FL. Lots of stalls full of new and used model railways – many at bargain prices. Adults £3, Seniors £2.50, Children £1. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**21 July (Sun) 10am-2pm**

**ASHFIELD – Festival Hall**, Kirkby in Ashfield NG17 7DJ. Good access, free parking, refreshments. Always a good selection of Rail and Diecast. Next event: 8th September. Contact MALC. Tel: 07951 072 790.

**21 July (Sun) 10am-2.30pm**

**ASTON CLINTON**, Green Park, Stablebridge Road, Aston Clinton, Bucks HP22 5NE. Admission £2. Contact JOE LOCK. Tel: 07866 641 215.

**21 July (Sun) 10.15am-3pm**

**WIMBORNE MODEL TOYS AND TRAIN COLLECTORS FAIR**, Queen Elizabeth School (QE), Wimborne Minster, Dorset, BH21 4DT. Large free car park, refreshments. Admission: £1.50. Enquiries to DAVE. Tel: 01202 590 158 or 07808 484 414.

**27 July (Sat) 10am-4pm**

**BLUEBELL RAILWAY COLLECTORS FAIR**, Horstead Keynes Station, Station Approach, Horstead Keynes, East Sussex, RH17 7BB. Admission by platform ticket, or free with train travel ticket or Bluebell membership card. Contact JOE LOCK. Tel: 07866 641 215.

**28 July (Sun) 10.30am-2.30pm**

**CORNWALL – LOSTWITHIEL** Community Centre PL22 0HE. Contact: MARK. Tel: 01637 871 412, 07887 888 670.

**28 July (Sun) 10.30am-3pm**

**MINEHEAD TOY AND TRAIN SALE**, Minehead Eye, Mart Road, Minehead TA24 5BJ. Opposite Minehead (WSR) Station – 2 minutes walk. 65 TABLES, MANY TOP DEALERS. Café, free parking, BUY\*SELL\*EXCHANGE. Admission £2. Tel: 01643 702 757 & 07966 694 579, website: [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)

**28 July (Sun) 10am-2pm**

**ORPINGTON, CROFTON HALLS**, by Orpington Railway Station, Kent BR6 8PR. Next event: 25th August. SRP TOYFAIRS. Tel: 01322 662 729 or 0773 999 8012 [www.srpoyfairs1.co.uk](http://www.srpoyfairs1.co.uk)

**30 July (Tues) 6pm-8pm**

**TONBRIDGE, ANGEL CENTRE**, Angel Lane, Kent TN9 1SS. Next event: 24th September. Tel: 01322 662 729 or 0773 999 8012 [www.srpoyfairs1.co.uk](http://www.srpoyfairs1.co.uk)

## August 2019

**3 August (Sat – new day) 10.30am-2pm**

**NORWICH – The Willow Centre**, 1-13 Willowcroft Way, Cringleford, Norwich, NR4 7JJ. Admission £2, under 16 free, early entry 9am - £4pp. Good parking, disabled access and refreshments. JOHN'S TOY FAIRS. JOHN PARSONAGE, telephone: 07881 555 283, email: [john.parsonage@btconnect.com](mailto:john.parsonage@btconnect.com)

**3 August (Sat) 10am-2pm**

**NOTTINGHAM – BLUECOAT ACADEMY**, (on ring road, 1 mile north of University) NG8 5GY. Good access, free parking, refreshments. Next event: 12th October. Contact MALC. Tel: 07951 072 790.

**4 August (Sun) 1pm-4pm**

**FALKIRK – Graeme High School**, Callendar Road, Falkirk FK1 1SY. Entry £1, under 12 free. BRIAN

McLAREN.

Tel: 01324 624 102  
[www.mclaren-models.com](http://www.mclaren-models.com)

**4 August (Sun) 10.30am-3pm**

**4TH ANNUAL TOY AND TRAIN COLLECTORS FAIR – NATIONAL BREWERY CENTRE**, Hominglow Street, Burton Upon Trent, DE14 1NG. Adults £4, Children £1. Price includes entry to Brewery Museum. Contact: NATIONAL BREWERY CENTRE. Telephone: 01283 532 880 or CHRIS EATON, telephone: 07877 346 371.

**4 August (Sun) 10.30am-3pm**

**PORTHMADOG TOY & TRAIN SALE**, Y Ganolfan Community Centre, High Street, Porthmadog, North Wales, LL49 9LU. Our venue is a niche centre, virtually next door to the terminus which serves both the Welsh Highland and Ffestiniog Railways – bliss! 60 tables will be packed by some of the UK's top traders. BUY\*SELL\*EXCHANGE. Café, plenty of parking nearby. CHRIS DYER FAIRS. Tel: 01643 702 757 & 07966 694 579 [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)

**8 August (Thurs) 17.30pm to 21.30pm**

**WELLING, KENT CHARITY TOY AND TRAIN FAIR**, Falconwood Community Centre, 32 Falconwood Parade, The Green, DA16 2PG. Adults £1, accompanied children free. In aid of the Association of Bexley Charities '78. Sponsor: CADE CONSTRUCTION: 020 8311 9020. Organiser: ALAN BONE. Tel: 020 8310 5018.

**11 August (Sun) 10.30am-3pm**

**BARRY ISLAND TOY & TRAIN SALE**, on the platform and under the canopy (all covered) at Barry Island Station, (Barry Tourist Railway), Station Approach, Barry Island, CF62 5TH. 60 tables, top dealers. TRAINS RUNNING ALL DAY. ATW trains also run on another platform at this station from all parts, so why not travel by train? Large car park close by. Great café. Admission £2. Tel: 01643 702 757 & 07966 694 579 [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)



**11 August (Sun) 10am-2pm**

**DITTON** – Ditton Community Centre, Kilnbar Road, Ditton, Kent ME20 6AH. Next event: 30th December. SRP TOYFAIRS.

Tel: 01322 662 729 or 0773 999 802

[www.srpoyfairs1.co.uk](http://www.srpoyfairs1.co.uk)

**11 August (Sun) 10.30am-3pm**

**EXETER (WESTPOINT) TOY & TRAIN COLLECTORS FAIR**, Westpoint Arena, Clyst St Mary, Exeter. One mile from the M5. Up to 300 stalls selling model railway items, obsolete and new. Hot and cold refreshments and free parking for thousands of cars. For more information and stall availability contact: BULLDOG FAIRS.

Tel: 01373 452 857 or 07917 125 641

[www.bulldogfairs.com](http://www.bulldogfairs.com)

**11 August (Sun) 10am-1.30pm, (last entry)**

**SOUTHAMPTON TOYFAIR**, Blighmont Barracks, Millbrook Road West, Southampton SO15 0AJ. £1 Admission fee (Children under 12 free). For more details or bookings please call or email.

Tel: 0238 077 2681 [r.lines@hotmail.co.uk](mailto:r.lines@hotmail.co.uk)

**11 August (Sun) 10.30am-3pm**

**STAFFORD – 250 STALLS**, The Prestwood Centre Stafford County Showground, Weston Road, Stafford ST18 0BD. Adults £4, Seniors £3.50, Children £1. There are many stalls selling OO and O gauge trains.

Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**13 August (Tues) 6.30pm-8.30pm**

**ODIHAM/HOOK**, Community Centre, Ravencroft,

Hook RG27 9NN. Second Tuesday monthly.

Tel: 01380 725 322, anytime.

**17 August (Sat) 10am-3pm**

**EXETER TRAIN & TOYFAIR**, Matford Centre, Matford Park Road, EX2 8FD. 200+ tables, admission £2. Superb venue, free parking, refreshments. Organised by RAY HEARD.

Tel: 01823 480 097.

Email: [rayheard66@btinternet.com](mailto:rayheard66@btinternet.com)

**17 August (Sat) 10.30am-3pm**

**SANDOWN PARK – 500 STALLS**, Europe's No1 Toy Collectors Fair, Sandown Exhibition Centre, Sandown Park Racecourse, Esher, Surrey, KT10 9AJ. Adults £6.50, Senior Citizens £6, Children £2. Hundreds of stalls selling everything you want in model railways, come and see why Sandown is Britain's best loved toy show.

Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**18 August (Sun) 10.30am-2.30pm**

**\*MEGA CORNWALL TOY & TRAIN SALE – BODMIN – NEW EVENT\*** Bodmin Leisure Centre, Lostwithiel Road, Bodmin, Cornwall. PL31 1DE (close to Bodmin General Station & Bodmin & Wenford Railway). Parking, Caterer. Over 100 tables BUY\*SELL\*EXCHANGE – MANY TOP DEALERS, working model railway layouts. Admission £2. CHRIS DYER FAIRS.

Tel: 01643 702 757 or 07966 694 579.

Website: [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)

**20 August (Tues) 7pm-9pm**

**WOOTTON BASSETT**, Memorial Hall, Station Road,

SN4 7EE. Toy and Train Fair. Third Tuesday monthly.

Tel: 01380 725 322, anytime.

**24 August (Sat) 10.30am-2pm**

**WORCESTER** – Perdiswell Leisure Centre, Bilford Road, WR3 8DX, (off A38 Droitwich Road), M5 junction 6. Admission £2. TONY OAKES FAIRS.

Tel: 01270 652 773 & 07825 631 323.

**26 August (Bank Holiday Monday) 10.30am-2pm**

**BRIDGNORTH LEISURE CENTRE**, High Town, WV16 4ER. 100+ stalls of all types of model railways, Dinky's, Corgis, various Die-castes etc. Admission £2. TONY OAKES FAIRS.

Tel: 01270 652 773 & 07825 631 323.

**27 August (Tues) 6pm-9pm**

**GARSTANG TOYFAIR**, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON.

Tel: 01282 439 009.

**31 August (Sat) 10.30am-3pm**

**OXFORD**, Exeter Hall, Oxford Road, Kidlington OX5 1AB, M40 J9. Further details, telephone: 01869 347 489.

**31 August (Sat)**

**\*RAILS IN WALES, RHYL – CANCELLED\*** Rhyl Town Hall, Wellington Road Rhyl LL18 1BA.

Contact: Win.

Tel: 01745 339 847 or

Email: [a.james858@btinternet.com](mailto:a.james858@btinternet.com)

**THE SWINDON RAILWAY FESTIVAL AT STEAM**  
MUSEUM OF THE GREAT WESTERN RAILWAY

**Sat 7 and Sun 8 September 2019 10am - 5pm**

**Admission**

|                               |        |
|-------------------------------|--------|
| Adult (16+ yrs)               | £13.10 |
| Child (3-15 yrs)              | £8.80  |
| Senior (65+ yrs)              | £10.00 |
| Student (ID required)         | £10.00 |
| Family Tickets available from | £22.10 |

Tickets are available online or on the door.

**20% off** all tickets purchased online before 31st Aug  
[www.steam-museum.org.uk](http://www.steam-museum.org.uk)

See the UK's finest model railways

Join us at: @steam\_museum steammuseum @STEAMMuseum

STEAM PECO RAILWAY MODELLER SWINDON BOROUGH COUNCIL

## Opportunity Yours

RM Classified Advertisement Order Form — last date for receiving copy for October 2019 issue is 22nd July. Please insert the following advertisement under the section headed: .....

**PLEASE NOTE:** If you do wish to renew an existing advert, phone, write, fax or email within 3 months of the original advert being placed.

**MINIMUM CHARGE** £4.20 (£3.50 + 70p VAT) (per advert).  
Prices below do not include VAT. Please add 20% VAT to total.  
**Rates per word:** Private 15p, Trade 35p, Swapmeets 65p.  
Box number £5.50 extra.

Tick if a box number is required ☐

Advertisements without a box number must include your name, address or telephone number. Standard abbreviations (LMS, META, 0-6-0), normal hyphenations (O-gauge, 2-rail, pre-war) and prices count as one word. Telephone numbers, whether exchange or all figure, count as two words.

### CONDITIONS OF ACCEPTANCE

All copy is subject to our approval. We reserve the right to request amendments. Scale or gauge should be mentioned when referring to models. Swapmeets and other events must include a telephone number at which the organiser can be contacted. Advertisers are expected to acknowledge all enquiries that enclose an SAE (even if the item has already been sold). Advertisers are reminded of the Trade Descriptions Act and must avoid misrepresentation of goods offered. The Business Advertisements (Disclosure) Order 1977 requires all who sell goods in the course of a business to indicate this clearly in the advertisement.

I enclose cheque/PO for £.....  
(made payable to Peco Publications).

It is unwise to send coins or banknotes either in payment for the advertisement or goods.

Name .....

Address .....

Post code .....

Please send this completed form and remittance to:

### Classifieds Department

Peco Publications, Beer, Seaton, Devon, EX12 3NA

Alternatively you can telephone: 01297 21542, fax: 01297 20229 or

email: [classifieds@peco.co](mailto:classifieds@peco.co)

This form may be copied to save cutting the page







**SWINDON**  
**SAT 7th SEPT**  
**SPECIAL EVENT**  
**RUMMAGE THROUGH OUR**  
**SECOND-HAND**  
**LOCOS AND ROLLING STOCK**  
 unsorted and in need of work –  
 some more than others  
**9am – 5pm**  
**WHAT WILL YOU FIND?**  
**SPOT-ON MODELS**  
 49 FLEET STREET, SWINDON,  
 WILTSHIRE SN1 1RE  
 TEL: 01793 617302  
 Email: [spotonmodels@gmail.com](mailto:spotonmodels@gmail.com)

Celebrating  
35 years in  
business

**THE TRAIN SHOP**

Geoff and Janet Jefferson  
 • Hornby Railways • Scalextric •  
 • Bachmann 'OO' & N gauge •  
 • Peco • Diecast • Thomas The Tank •  
 Plus much more in store  
 One of the largest stocks of model railways  
 Scalextric and accessories in the north of England

41 Eastborough, Scarborough YO11 1NH  
 Telephone/Fax: (01723) 354019  
[geoff.jefferson@gmail.com](mailto:geoff.jefferson@gmail.com)

PECO WILLS DAPOL METCALFE

**SPORTS & MODEL**

www.sportsandmodelshop.co.uk  
 66 High Street Dingwall IV15 9RY  
 01349 862346

**MODEL RAILWAY LAYOUTS**  
 EXCELLENT QUALITY AND VERY AFFORDABLE  
 any scale or size, standard designs or built to your  
 specification. (Complete or part built sections).  
 Fold-up design to allow ease of storing if required.  
 From train set boards to finescale exhibition quality layouts.  
 Friendly service guaranteed.  
 Please phone 01924 496053 or write to:  
**CHARMING LITTLE LAYOUTS**  
 THE ENCHANTED GARDEN, WHITLEY, DEWSBURY,  
 WEST YORKSHIRE WF12 0LZ (see for information)

**Model Layout Services**

Layout Building - Custom Buildings  
 Bespoke Baseboards - Scenery

www.modellayoutservices.co.uk  
 Tel: 01337 830007

**The Double O Gauge Association**  
 Visit: [doubleogauge.com](http://doubleogauge.com)

Run by OO modellers to help  
 OO modellers – join today

**INDEXES**  
 PLEASE SEND SAE  
 FOR DETAILS OF AVAILABILITY  
 AND CHARGES:  
 PECO TECHNICAL ADVICE  
 BUREAU, BEER, NR SEATON,  
 DEVON EX12 3NA.

**SIMON TENDER**  
 ready-built layouts in N gauge  
 YOUR LAYOUT HAS ALREADY BEEN BUILT  
**www.simontender.com**  
**Tel: 07704 704 643**  
 WEMBLEY N.W. LONDON  
 Please ask for Simon

**R&R HOBBIES**  
 3 Shoppers Walk, Swansea SA1 3AY  
 Tel: 01792 468749  
**www.randrhobbies.co.uk**  
 Mon-Fri 10am to 5pm Saturday 10am to 4.30pm  
 Balsa Wood, Plastics, Metals, Paints, Tools  
 Visit our new railway department upstairs!  
 N OO O scales stocked

**GRANDAD'S TRAIN SHOP**

Our shop address is:  
 Grandad's Train Shop, inside Selby Superbowl, Bawtry Road,  
 Selby, North Yorkshire, YO8 8NA  
 Our opening times are Friday 2pm to 6pm,  
 Saturday 10am to 4pm and Sunday 11am to 4pm.  
 For further updates and information please follow us on Twitter or like  
 us on Facebook @grandadstoyshop  
 Please look on our website and social media pages for our special  
 offers in store only.  
 www.grandadstoyshop.co.uk or visit our eBay shop on:  
<http://stores.ebay.co.uk/grandadstoyshop>  
 Email: [info@grandadstoyshop.co.uk](mailto:info@grandadstoyshop.co.uk) Tel: 07754 401209 (After 6 pm)  
 Grandad's Train Shop is a trading name of Grandad's Toy Shop

**RICHARDSONS OF FELTHAM**  
 6-7 Rochester Parade, High Street, Feltham,  
 Middx. TW13 4DX 020 8890 4399/9946  
 E-mail: [sales@richardsons-retail.co.uk](mailto:sales@richardsons-retail.co.uk)  
 www.richardsons-retail.co.uk  
 'N' GAUGE 'HO/OO' and 'O' GAUGE  
 Graham Farish Locos, Rolling Stock and Buildings,  
 Peco Track, Rolling Stock and Buildings  
 Hornby, Peco, Superquick, Gaugemaster Controllers,  
 Scenic Materials. Plus Scalextric Cars & Track.  
 BACHMANN STOCKISTS  
 HOBBIES PRODUCTS AND MUCH MORE

**HORNBY, DUBLO, WRENN**  
**AND TRIANG MOTOR REPAIRS**  
 Motors rewound & remagnetized.  
 DCC chips installed.  
 Old Triang wheels updated.  
 OO locos serviced from £17.50 plus parts & post  
**SCALE SPEED**  
 32 GOLOFINCH LANE, LEE ON THE SOLENT, HANTS PO13 8LN  
 Phone 02392 554107 or 07774 253352  
 E-mail: [scalespeed@btinternet.com](mailto:scalespeed@btinternet.com)  
**www.scalespeed.co.uk**

**Haslington Models**  
 134 CREWE ROAD  
 HASLINGTON, CREWE CW1 5RQ  
 Tel/Fax: 01270 589079  
 email: [haslingtonmodels@live.co.uk](mailto:haslingtonmodels@live.co.uk)  
 Appointed Stockists at:  
 HORNBY, BACHMANN, FARISH, HELJAN, DAPOL,  
 PECO, GAUGEMASTER, DCC SYSTEMS, HUMBROL,  
 PLASTRUC, PRECISION METALS, RATIO, WILLS,  
 KADEE COUPLINGS, etc.  
 Open THUR, FRI & SAT ONLY 10.00 - 16.00

**THE HOBBY GOBLIN**  
 54 Hamill Road, Burslem, Stoke-on-Trent,  
 Staffordshire ST6 1AU  
 OPEN: Tues/Wed/Fri 10.00am-4pm  
 Thurs 10am-5.30pm Sat 10am-2pm  
 T: 01782 818433  
 Email: [thehobbygoblin-uk@hotmail.com](mailto:thehobbygoblin-uk@hotmail.com)  
**www.thehobbygoblin.co.uk**  
 Decoder and sound fitting, repairs and servicing, weathering  
 and renumbering, layout advice and construction.

**KINGS LANGLEY JUNCTION 20 MODELS**  
 Hornby, Bachmann, Peco, Gaugemaster,  
 Airfix, Humbrol, Sundeala, etc.  
 Second hand bought and sold, collections purchased.  
 51 High St, Kings Langley, Herts WD4 9HU.  
 1/2 mile from M25, Junction 20  
 MON-FRI 10am - 6pm, SAT 10am - 5pm,  
 SUN 10.30am - 1.30pm  
**TEL: 01923 270247**  
 FREE PARKING

**THE RAILWAY SHOP**  
 33 Broad Street Blaenavon, Torfaen NP4 9NF  
 01495 792263  
 Bachmann - Hornby - Peco - Dapol - Wills  
 Ratio - Gaugemaster - Kadee - Metcalfe - Heljan  
 all at discount prices Mail order welcome  
**NEW • NEW • NEW**  
 Basil Jayne - 7 Plank Wagon  
 £10.25 + £2.85 p&p  
**NEW • NEW • NEW**  
 Open Weekdays 10.30-5.00 Sat. 10.30-4.00  
 Profits support the Pontypool & Blaenavon Railway

**MRSG**  
**MODEL RAILWAY SHOP GRANTHAM**  
 Supplier of kits card/plastic N to O by  
 Peco, Ratio, Wills, Metcalfe, Knightwing,  
 Sloters, Kestrel, Springside,  
 Eckon/Berko signals and Parkside.  
 46 Watergate, Grantham,  
 Lincolnshire NG31 6PR  
**Tel: 01476 563472**

**THE MODEL SHOP - EXETER**  
 SPECIALIST MODEL RAILWAY STOCKISTS  
**4 ST DAVID'S HILL**  
**EX4 3RG**  
 JUST OFF THE OLD IRON BRIDGE  
**NOW TAKING ORDERS FOR**  
**2019 RELEASES**  
**www.trainsandtrack.co.uk**  
 Open Tues - Sat 9.30am-5.00pm  
 (01392) 421906

**MODEL RAILWAY**  
**THE MODEL SHOP**  
 CENTRE  
 MON-SAT 9.00-5.30 SUN 10.00-2.00  
 65 KINGSTON ROAD, PORTSMOUTH, HANTS PO2 7DX.  
 PHONE: 02392 738111  
 PECO-HORNBY-BACHMANN-DAPOL  
 RATIO-GRAHAM FARISH ETC.  
 DIGITAL CHIPPING SERVICE  
 SECOND HAND BOUGHT & SOLD  
 NOW STOCKING R.C. CARS

**TUTBURY MODELS**  
 Celebrating 25 Years!  
**COLLECTABLES 01283 814777**  
**TUTBURY MILL MEWS, TUTBURY, Nr. BURTON-UPON-TRENT DE13 9LS**  
 BASS/WORTHINGTON VENT VAN...**AVAILABLE NOW**...£13.50 (plus £3.00 p&p each for up to six models)  
 6-WHEEL MILK TANK NESTLÉ 'OO' GAUGE...£17.50 (plus £2.80 p&p for either one or two)  
 BASS/WORTHINGTON GRAIN HOPPERS...£13.50 each (plus £2.80 p&p for either one or two)

On holiday? Please pay  
**PARRS Model Railways**  
 a visit.  
 252 London Road South,  
 Kirkley, Lowestoft NR33 0BE.  
 Tel: 01502 565695  
 We have everything for the  
 Model Railway enthusiast.  
 Mail Order Welcome

**CAISTOR LOCO**  
 8 Market Place, Caistor,  
 Lincs LN7 6TW  
**Tel: 01472 859990**  
 For all your Model Railway Needs  
 Opening Hours:  
 Mon, Tues, Thurs 10 till 3  
 Fri 10 till 3 & 5 till 7 Sat 10 till 5  
 Hornby • Parkside • Smiths • Peco  
 Cambrian • Metcalfe • Bachmann  
 Coopers • Mail Coach • Wills  
 Farish • Springside • Ratio  
 Railmatch Paint • Knightwing  
 Falter • Knoch • Jarvis • Woodlands  
 Kadee • P. D. Marsh and many more  
 ~ Mail order welcome ~

Bespoke and trade enquiries please call Nigel Julian on  
 07971 741478 or 01332 781795  
 email: [nigel@airframed.co.uk](mailto:nigel@airframed.co.uk)

**AirFramed**  
 Bespoke Glass Display Cases  
**www.airframed.co.uk**  
 OO gauge, O gauge, Gauge 1 and  
 Gauge 3 cases with  
 mirrored or clear back wall.  
 Prices start from £35.00 plus p&p.  
 Bespoke sizes made to order.  
 Prices on application.

**Platform 3 Models**  
 Layouts Designed and Built from N to  
 Garden Railways and based in Devon  
 All Loco Servicing & Repairs and  
 DCC decoder installation for N & OO  
 Contact Tom Hussey Tel: 07811 903231  
 Email: [platform3models@hotmail.co.uk](mailto:platform3models@hotmail.co.uk)  
 Web Search: Platform 3 Models

**BACHMANN, HORNBY & PECO**  
 at competitive prices.  
 Commission sales undertaken. Mail Order available.  
**TRANSPORT TREASURES**  
 Shop at:  
 2 London Road, Aston Clinton, Aylesbury, Bucks. HP22 5HQ.  
 Tel: 01296 631002.  
 Open 11-3 weekdays and Saturdays. Closed Thursdays. Sundays 11-1.

**TRIDENT TRAINS**  
 No10, The Craft Arcade, Dagfields Craft Centre  
 Crewe Road, Walsworth, Nantwich,  
 Cheshire CW5 7LG  
**www.tridenttrains.co.uk**  
**01270 842 400**  
 We stock  
 HORNBY - BACHMANN - GRAHAM FARISH - DAPOL OO & N  
 PECO - RATIO - WILLS - RAILMATCH PAINTS - HUMBROL  
 PLASTRUC - JAVIS - WOODLAND SCENIC - OXFORD DIECAST  
 METCALFE OO & N - SUPERQUICK - TCS DECODERS - LENZ DCC  
 GAUGEMASTER - TRACKSETTA OO & N + MUCH MORE.  
 WEDNESDAY - SUNDAY 10.00 TIL 5.00



# Roxey MOULDINGS

47 YEARS OF FINESCALE KITS



## 4mm/00 Markits Accessories

A small selection from our huge range of Romford/Markits products in 4mm/00 and 7mm/0. This includes rolling stock wheels and locomotive fittings.

|                                                     |        |
|-----------------------------------------------------|--------|
| 4MA001...Short Handrail Knobs(12).....              | £3.50  |
| 4MA002...Medium Handrail Knobs(12).....             | £3.50  |
| 4MA003...Long Handrail Knobs (12).....              | £3.50  |
| 4MA007...Short WD Handrail Knobs (12).....          | £3.50  |
| 4MA008...Long WD Handrail Knobs (12).....           | £3.50  |
| 4MA009...Coach Tee Door Handles (16).....           | £7.95  |
| 4MA010...Conduit Holders (12).....                  | £3.50  |
| 4MA011...Screw Link Couplings (1 Pair).....         | £5.70  |
| 4MA012...Three Link Couplings (1 Pair).....         | £3.60  |
| 4MA013...Romford Screwdriver.....                   | £5.20  |
| 4MA014...00 Back to Back Gauge.....                 | £6.85  |
| 4MA015...00 Roller Gauge Code 75.....               | £4.60  |
| 4MA016...00 Roller Gauge Code 100.....              | £4.60  |
| 4MA017...1/8" Hornblock Alignment Jig (3 Axes)..... | £4.95  |
| 4MA018...11mm Frame Spacers (3).....                | £6.55  |
| 4MA019...9.5mm Frame Spacers (3).....               | £6.55  |
| 4MA020...Standard 00 Romford Axle(1).....           | £1.62  |
| 4MA023...00 Extended Axle (1).....                  | £1.70  |
| 4MA025...Romford Crankpins (6).....                 | £2.90  |
| 4MA026...Deluxe Crankpins (6).....                  | £5.55  |
| 4MA037...Tender Waterscoop Column (1).....          | £3.70  |
| 4MA038...13mm Handbrake Column (1).....             | £3.70  |
| 4MA039...20mm Handbrake Column (1).....             | £3.70  |
| 4MA042...Washout Plugs Insert Type (12).....        | £2.65  |
| 4MA051...BR Smokebox Door Handle (1).....           | £4.95  |
| 4MA052...LMS Smokebox Door Handle (1).....          | £4.95  |
| 4MA053...GWR Smokebox Door Handle (1).....          | £4.95  |
| 4MA061...1/8" Axle Bush (12).....                   | £3.25  |
| 4MA062...1/8" Gearbox Bush (12).....                | £3.25  |
| 4MA063...2mm Axle Bush (12).....                    | £3.25  |
| 4MA064...1.5mm Axle Bush (12).....                  | £3.25  |
| 4MA071...Shouldered Pinpoint Bearings (50).....     | £3.50  |
| 4MA072...Waisted Pinpoint Bearings (50).....        | £3.50  |
| 4MA073...Flangeless Pinpoint Bearings (50).....     | £3.50  |
| 4MA075...Universal Joint Set (1.5mm Shafts).....    | £5.15  |
| 4MA076...Universal Joint Set (2mm Shafts).....      | £5.15  |
| 4MA079...Nut Spinner Set (6 to 16BA).....           | £23.60 |
| 4MA081...Tail Ross-Pop Safety Valves (1pr).....     | £5.40  |
| 4MA082...Fat' Ross-Pop safety valves (1pr).....     | £5.40  |
| 4MA092...Salter safety Valve Columns (2).....       | £5.40  |
| 4MA100...1 Rod Hex Crankpins 1.2mm (2).....         | £3.35  |
| 4MA101...2 Rod Hex Crankpins 2.6mm (2).....         | £3.35  |
| 4MA102...LMS Return Crank SKF End (1).....          | £9.50  |
| 4MA103...LMS Return Crank Plain End (1).....        | £9.50  |
| 4MA104...SR/LNE Return Crank Plain End (1).....     | £9.50  |
| 4MA105...SR/LNE Return Crank SKF End (1).....       | £9.50  |
| 4MA106...GWR O/Side Crank Axle+Cranks (1).....      | £7.90  |
| 4MA144...GWR Whistles (Pair).....                   | £5.70  |
| 4MA147...LSWR/SR Urie/Drummond Whistle (1).....     | £4.05  |
| 4MA156...Fowler Whistle (1).....                    | £4.05  |
| 4MA156...LNER A4 Chime Whistle (1).....             | £4.05  |
| 4MA159...Diesel Airhorns (2 Pairs).....             | £4.60  |
| UK P&P...Per order.....                             | £3.00  |

## Romford/Markits Driving Wheels

Now stocking many of the popular sizes of Romford/Markits 00 Driving wheels, Bogie Wheels, Tender Wheels and Rolling Stock Wheels. Phone

## 4mm/00 Gauge Gearboxes

|                                                                                                   |        |
|---------------------------------------------------------------------------------------------------|--------|
| by Markits Etched Nickel silver frame, steel worm & brass gear. 1/8inch Axle. P&P £3.00 per order |        |
| G38/1 38:1 Gearbox for 1.5mm shaft                                                                | £13.25 |
| G38/2 38:1 Gearbox for 2mm shaft                                                                  | £13.25 |
| G46/1 46:1 Gearbox for 1.5mm shaft                                                                | £13.25 |
| G46/2 46:1 Gearbox for 2mm shaft                                                                  | £13.25 |
| G50/1 50:1 Gearbox for 1.5mm shaft                                                                | £13.25 |
| G50/2 50:1 Gearbox for 2mm shaft                                                                  | £13.25 |

## MASHIMA 12 Volt Motors

Mashima motors have ceased production. Stocks are almost gone. Check for current availability. H1024 open frame motors and Tenshodo motors and motor bogies are not affected.

## NEW 1833 replacement.

TV18-33 18 x 33mm. 2mm Shaft .....£23.50  
P&P £3.00 per order

## Roxey Online Shop

[www.roxeymouldings.co.uk](http://www.roxeymouldings.co.uk)

**PRECISION Paints (Exhibitions Only)**  
**MARKITS Accessories / MASHIMA Motors**  
**TENSHODO Motor Bogies & Motors**  
**MARKITS/ROMFORD/SLATERS Wheels & Accessories** For Price List send C5 SAE

**EXHIBITIONS** August 10-11<sup>th</sup> Wells / Aug 31<sup>st</sup>  
Sept 1<sup>st</sup> Telford 0 Gauge / Sept 14-15<sup>th</sup> REC Woking /  
Sept 28-29<sup>th</sup> Scaleforum / Oct 19-20<sup>th</sup> Uddfield  
Prices inc VAT @ 20% E&OE Now on Facebook

## ROXEY MOULDINGS

58 Dudley Road, Walton-on-Thames,  
Surrey KT12 2JU Tel:01932 245439  
Website: roxeymouldings.co.uk  
Email: dave@roxeymouldings.co.uk  
VISA/Mastercard/Maestro accepted

# Aspire Gifts & Models

OPENING TIMES: Mon 2pm-5pm Tues, Wed, Thurs 10am-5pm

Suppliers of all Major Railway Brands, We Chip, Service and Repair Engines.

Layouts built from Track to finished item.

THOUSAND'S OF ITEMS AVAILABLE ON OUR WEBSITE CREDIT CARDS WELCOME

Unit 4. Court Farm Business Park, Buckland Newton, Dorset. DT2 7BT Telephone: 01300 345355

email:edward.aspire@btinternet.com

Website:www.aspiregiftsandmodels.co.uk

## CHURCH STREET MODELS

Open 5 days a week  
Mon, Wed-Sat 9.30-17.30



[www.churchstreetmodels.co.uk](http://www.churchstreetmodels.co.uk)

## Stockists of 00 & N gauge from:

Bachmann, BT Models, Dapol, Deluxe Materials,  
Dornaplas, EFE, Farish, Gaugemaster, Heljan,  
Hornby, Humbrol, Kestrel, Metcalfe, Modelscene,  
Oxford Diecast/Rail, Peco, Plastruct, Ratio,  
Superquick, Wills & Woodland Scenic.

10A Church Street, Basingstoke,

Hants RG21 7QE

01256 358060

## IMPRESSED WITH YOUR BALLASTING?

However satisfied you are with your finished trackwork, it can be done quicker and easier with Tracklay™ underlay – the trains will run quieter as well – try it yourself and see how much time and effort you save.

| PRICES INC. UK P&P | OO | 5m     | 10m    | 20m    | 30m    | 40m    | 50m    | TRIAL (OO, N or Z)                  |
|--------------------|----|--------|--------|--------|--------|--------|--------|-------------------------------------|
|                    | N  | £14.70 | £23.26 | £40.54 | £58.70 | £77.30 | £84.35 | £4.96                               |
|                    | Z  | £12.20 | £17.67 | £29.56 | £42.43 | £54.34 | £63.20 | includes £2 off first order voucher |
|                    |    | £11.30 | £15.06 | £25.30 | £35.36 | £44.14 | £50.85 |                                     |

order online at [www.tracklay.co.uk](http://www.tracklay.co.uk)

TRACKLAY SALES, 87 BABINGTON ROAD, BARROW-UPON-SOAR, LEICS. LE12 8NH. Tel: 01509 415594

## Durham Trains of Stanley



"more than just a model shop"

[www.durhamtrainsofstanley.co.uk](http://www.durhamtrainsofstanley.co.uk)

6 Station Road, Stanley,

Co. Durham DH9 0JL

Tel: 01207 232545

Mon/Tues/Friday/Sat 09.30-17.00

Thurs 11.00-19.00. Closed Wed/Sun.

FULL AND COMPREHENSIVE

DCC FITTING SERVICE

DETAILING AND WEATHERING

OF RTR MODELS

## JOHN DUTFIELD FOR MODEL RAILWAYS

Stockists of many brands including Peco, Bachmann, Hornby, Graham Farish, Dapol and Gaugemaster + a wide selection of scenics, kits, glues, tools and accessories, etc.

Opening hours 9am-5.30pm Mon, Tues, Thurs, Fri and Sat. Closed all day Wed, Sun and Bank Holidays

WARDS YARD, 133 SPRINGFIELD PARK ROAD

CHELMSFORD, ESSEX CM2 6EE ~ Tel: 01245 494455

All major credit and debit cards accepted Gift Vouchers available. Second-hand available.

Email: [heatherwilkinson@btconnect.com](mailto:heatherwilkinson@btconnect.com) [www.johndutfieldmodelrailways.co.uk](http://www.johndutfieldmodelrailways.co.uk)

## EAST SOMERSET MODELS

THE RAILWAY STATION,  
CRANMORE, SHEPTON MALLET,  
SOMERSET, BA4 4QP

## "MODEL RAILWAY SPECIALISTS"

We carry comprehensive stocks from the major manufacturers

Bachmann, Farish, Dapol, Oxford Rail, Heljan and Peco

We also hold a large range of modelling accessories, vehicles and scenic materials

Wednesdays and Saturdays 10.30am-4pm - Sundays 10.30am-2.30pm

(See our Website/Facebook Page for other opening days)

Telephone: 01749 880651 - [www.esmodels.co.uk](http://www.esmodels.co.uk) - Email: [mail@esmodels.co.uk](mailto:mail@esmodels.co.uk)

# www.ToppTrains.co.uk

## Shop for the railway model enthusiast

Wide range of stock from all your favourite suppliers ♦ Bachmann ♦ Peco ♦ Dapol ♦ Hornby ♦ Expo Tools, etc.  
♦ Sets ♦ Locos ♦ Rolling Stock ♦ Track ♦ Accessories ♦ Scenery ♦ Paints ♦ Brushes ♦ Tools

Repairs, weathering, renumbering & chipping locos. ♦ (Happy now Keith?)

Free parking. Near A34 bus route. 1m from town centre. 1.5m from M6 J14. 28, Marston Rd. Stafford ST16 3BS

Call us or see our website for details: Ask for Mike or Steve 01785 247883

## 3mm Scale Model Railways

4 Greenwood Drive, Redhill, Surrey, RH1 5PJ. Tel: 01737 761919

40 Page Full Colour Catalogue £4.50

Catering exclusively for TT and 3mm Scale

[www.3smr.co.uk](http://www.3smr.co.uk)

BlackBeetle, BullAnt, Halling, Tenshodo

[www.motorbogies.com](http://www.motorbogies.com)



# SPRINGSIDE

## MODELS

### Painted Head and Tail Lamps in 4mm Scale

Pack of 5 is £4.75 or pack of 10 is £8.95 except DA6 & DA7.  
DA1 GWR White Body. DA2 GWR Red Body. DA3 SR White Body. DA4 LMS White Body. DA4A LMS Black Body. DA4B Midland Maroon Body. DA5 LNER White Body. DA6 Guards Van Black Body pack of 5 £5.55 or £9.25p for 10. DA7 Railway Hand Signal Lamps pack of 6 only £4.95p. DA19 BR White Body. DA99 Modern Image White Body. DA20 Tail Lamps (all Red Jewels). State Region £4.75 packs of 5.

### Painted Head and Tail Lamps in 2mm Scale

No.1 GWR White Body. No.2 GWR Red Body. No.3 BR White. No.4 LMS Black Body. No.5 LMS White Body packs of 5 £4-75. Tail Lamps also available state region £4-75p for a pack of 5.

### Painted Head and Tail Lamps in 7mm Scale

State Region Packs of 5 £9.25. No.15 GWR White Body. No.16 GWR Red Body. No.89 SR White Body. No.21 LMS White Body No.22 LMS Black Body. No.23 Midland Maroon Body. No.90 LNER White Body. No.25 BR White Body. No.88 Modern Image White Body. Tail Lamps are available in packs of 5 please state region. No.17 Guards Van Black Body. £9.95p.

### Hand-Painted 4mm/00 Figures

DA52 Locomotive Crew Driver, Fireman, Guard £8.95



DA53 Locomotive Crew Driver, Fireman, Guard £8.95



DA54 Locomotive Crew Driver, Fireman, Guard £8.95



DA55 Locomotive Crew Driver, Fireman, Guard £8.95



DA105 Locomotive Crew Three Firemen £8.95



DA77 Modern Image Three Seated Drivers £8.95



DA74 Standing Passengers Set of 5 £12.25



DA85 Seated Passengers Set of 5 £12.25



DK1 Signal Box Detail

Kits of cast metal parts to enhance the interior of any signal box including lever frames, block signalling instruments, stove, arm chair, signal man pulling lever, warning bells, level crossing open gear, signal wire adjusters plus full assembly and painting instructions.

|                                                       |        |
|-------------------------------------------------------|--------|
| DK1 Signal Box Interior Detail Kit for large box..... | £16.95 |
| DK6 Signal Box Interior Detail Kit for small box..... | £10.75 |
| DA72 Extra levers 17 + 1 lever base.....              | £7.95  |

### 4mm/00 White Metal Kits

RV74 Kit Aveling and Porter Steam Roller, Living Van and Water Cart £28.95



RV43 Foden Kit Long Wheelbase Dropside Lorry £19.25



RV42 Foden Kit Short Wheelbase Tipper £



DS11 Road Repair Scene £15.95



DK5 Kit Platform Detailing Scene £17.95



All kits are 4mm/00 scale white metal kits

We accept payment by cash, cheque, debit and credit cards. Post and Packing is £2.25 on orders under £50 and free on orders over £50 UK only. Overseas customers please email us for shipping costs. Please note we do not receive visitors at the Dornfield Road address and only by prior arrangement at our units.

Email: [springsidemodels@outlook.com](mailto:springsidemodels@outlook.com)

[www.springsidemodels.com](http://www.springsidemodels.com)

SPRINGSIDE MODELS

PO Box 533,

Newton Abbot TQ12 9JF

Tel: 01803 813749

Mobile: 07720 939094

Catalogues Springside 4mm £3.95

Catalogues Springside 7mm £3.95

Dornaplas 2mm and 4mm £3.95

All catalogues are post paid



**www.KEEN-SYSTEMS.com** 16 ELM DRIVE, MARKET DRAYTON, SHROPSHIRE, TF9 3HE. Phone 01630 657881 [shop@keen-systems.com](mailto:shop@keen-systems.com)  
UK Postage £2.00 any quantity. Visa and Mastercard accepted. Make Cheques payable to R.S.Keen

**Close Coupling System 1 pair £3.50. 4 pairs £12.50**



Buckeye Coupler 5 Pairs £2.50



For use in none motorised tenders. Controlled by an accessory lead on the DCC decoder. **DCC Uncoupler £22.50**

**Mr Soundguy** *authentic UK locomotive sounds* **SOUND DECODERS WITH A DIFFERENCE**

Drive your trains like the Prototype with **"RealDrive"**  
*True Coasting - a First on any sound decoder True Braking - another First on any sound decoder*  
**NOTCH UP & DOWN SIMPLY USING THE THROTTLE!**  
No more fiddling with buttons  
*You've looked at the rest - Now try the Best*  
[www.mrsoundguy.co.uk](http://www.mrsoundguy.co.uk) \*see the demo video email: [mrsoundguyuk@gmail.com](mailto:mrsoundguyuk@gmail.com)

**ONLINE SHOP** **LENDONS** Over 70 years in business Est. 1944  
Suppliers of Model Railway Locomotives, Coach & Wagon Stock, Accessories and Parts for all popular gauges. Hornby, Bachmann, Peco, Graham Farish, ViTrains. Scenery building stock from Jarvis, Wills, Ratio, Woodland Scenics & European ranges.  
**Hornby Collectors Centre and Hornby Warranty Repair Centre**  
Massive range of locomotives always in stock with many brand new items long discontinued and Limited Editions. Please ring for enquiries on items not on our website.  
**192 Fidas Road, Llanishen, Cardiff CF14 5LZ**  
**029 2075 2563 9am-5pm [www.lendons.co.uk](http://www.lendons.co.uk)**

**B.R. LINES** **FARISH 'N' Gauge Coach Interiors**  
Mk. 2, Mk. 3, Mk. 4 1st and 2nd class available.  
Pack of four £3.75 + £3.60 post & packing.  
Prices include VAT where applicable.  
Mail Order & Trade enquiries welcome.  
Credit/Debit Cards accepted

**B.R. LINES 97 PARK LANE, GUISBOROUGH, CLEVELAND TS14 6PA.**  
**Tel: 01287 633036 [www.brlines.co.uk](http://www.brlines.co.uk)**

Service charge £17.50 per loco. Plus parts. Plus £3.60 post & packing. All work guaranteed

**Millennium Models**  
Hornby • Bachmann • Peco • Gaugemaster  
Jarvis • Dapol • Metcalfe • Classix Oxford  
Track Design Service and DCC Advice  
New / Second-hand - P/E considered  
**67 Queen Street, Morley, Leeds LS27 8EB Tel: 0113 2189286 [www.millenniummodels.co.uk](http://www.millenniummodels.co.uk)**

**YOUR LAYOUT BUILT**  
TO ANY SCALE TO EXHIBITION STANDARD TO YOUR OWN SPECIFICATION OR REQUIREMENTS  
**WITH 30 YEARS PROFESSIONAL EXPERIENCE OUR SERVICE INCLUDES:**  
BASEBOARDS; TRACKWORK (PECO OR HANDBUILT); WIRING CONTROL PANELS; SCENICS; BUILDINGS.  
Send SAE with your details or give us a ring on **01509 880659**  
**LITTLE JEM PRODUCTS**  
**5 Seymour Road, Burton-on-the-Wolds, Loughborough, Leics. LE12 5AH**

**NEW SHOP NOW OPEN**  
**Please phone 07956 973072 for opening details**  
• N Gauge good selection in stock  
• OO Gauge new and second-hand  
• O Gauge Golden Age Pullman Coaches and nice selection of O Gauge Locos  
• Repairs & Re-Magnetising  
**Swanage Model Railway Centre and Emporium**  
**44 High Street, Swanage BH19 2NX**

**PAIGNTON MODEL SHOP**  
*Stockist of:*  
Hornby, Bachmann, Peco, Dapol, Metcalfe, Ratio, Wills, Scenecraft, Scaledale, Diecast, Airfix, plus more.  
'O', 'OO', 'N' gauge.  
Loco Weathering Service Available  
**60 HYDE ROAD, PAIGNTON DEVON TQ4 5BY**  
**Tel: 01803 555882**  
**Email: [paigntonmodels@hotmail.co.uk](mailto:paigntonmodels@hotmail.co.uk)**  
**Website: [www.paigntonmodelshop.com](http://www.paigntonmodelshop.com)**

**BOOKS**  
Most in-print titles supplied - send title and publisher  
Having trouble finding that book over 9000 in stock. If we have not got it and it's in print we should be able to get it.  
We can also obtain and supply books on most subjects other than railways and we also import from the USA.  
*Book tokens accepted*  
Some titles discounted, some post free in UK  
All orders £30 plus post free in UK  
**BOB PEARMAN BOOKS**  
8 Ffolkes Place, Ructon Holme, Kings Lynn, Norfolk PE33 0AH  
Tel/Fax: 01553-810673 (24 hrs). E-mail: [pearman.books@idnet.com](mailto:pearman.books@idnet.com) Web: [www.pearman-books.com](http://www.pearman-books.com)  
PayPal (all cards)

**ITEMS MAIL ORDER LTD**  
**MAYFIELD, MARSH LANE, SAUNDBY, RETFORD, Notts DN22 9ES**  
**Tel/Fax: 01427 848880**  
BA SCREWS IN BRASS, STEEL AND STAINLESS.  
SOCKET SCREWS IN STEEL AND STAINLESS.  
DRILLS, RIVETS, TAPS, DIES, END MILLS, SLOT DRILLS, ETC.  
**PHONE FOR FREE LIST**

**RHUDDLAN MODELS**  
• Model Railways  
• Die Cast Models  
Corgi - Dinky - Matchbox  
• Plastic Kits  
Airfix - Revel - Etc  
**NEW AND USED ITEMS BOUGHT AND SOLD**  
**HIGH ST, RHUDDLAN**  
**01745 590048**  
**[www.rhuddlanmodels.co.uk](http://www.rhuddlanmodels.co.uk)**

**Alan Cliff's Railway Books**  
**Teens /Adults**  
**"Holy War" - The last narrow-gauge steam engine to work in a Welsh quarry £3**  
Children - Reading Age 6-9 yrs  
Jack the Station Cat Takes a Holiday (all colour)  
Jack the Station Cat and the Vanishing Lady (all colour)  
Jack the Station Cat and the August Day (all colour)  
Jack the Station Cat and the Snail Trail (all colour)  
Jack the Station Cat Takes Charge (all colour)  
Jack the Station Cat and the Midnight Mice (all colour)  
Jack goes to School (all colour)  
Jack and the Lost Kittens (all colour)  
Jack and the Great Little Trains Robbery  
Jack in a Signal Box  
Jack and the Tail's End Tickets  
Each book £2.95 - Set of 11 stories £28.50  
CD - The Jack Song £2 + post free when ordered with book(s)  
All above books post free  
Books signed. Cheques payable to:  
**Jack The Station Cat Ltd**  
**38 Clifton Park Road, Rhyl LL18 4AW**  
**Tel: 01745 344963**  
**[www.jackthestationcat.co.uk](http://www.jackthestationcat.co.uk)**  
Trade Enquiries Welcome

**BERKSHIRE DOLLS HOUSE & MODEL CO.**  
EST 1985  
The Old Fire Station, 9 Wargrave Road, Twyford, Berkshire RG10 9NY  
0118 9343700 - [howardphs@aol.com](mailto:howardphs@aol.com)  
[www.berkshiredollshousecompany.com](http://www.berkshiredollshousecompany.com)  
**SPECIAL OFFERS**  
**WIRING YOUR LAYOUT**  
**SPECIALIST IN OO, N AND G GAUGE**  
**WEATHERING EXPERT ON HAND**  
**DE-CODER FITTING**  
**FREE PARKING OPPOSITE**  
**ALL TOP BRANDS STOCKED**  
Bachmann/Woodland Scenics  
Hornby • Peco • Oxford Di-Cast  
Metcalfe • Ratio • Humbrol  
Gaugemaster • Airfix • Scalextric  
**NEW AND PRE-OWNED STOCK**

**ALPHAGRAPHIX**  
[sirberkeley@outlook.com](mailto:sirberkeley@outlook.com)  
  
**7mm Scale etched brass locomotive and carriage kits for Col. Stephens Light Railways**  
**Send 6x 2nd Class Stamps for ILLUSTRATED CATALOGUE**  
**23 Darris Road, Selly Park, Birmingham B29 7QY**

**INVERTRAIN**  
**Model Railways**  
**7mm O Gauge Specialist**  
Sole Supplier of Wayoh & Sparmac Coach Bodies  
  
**COAL OFFICE & STAITHES**  
**HCBK015**  
**£35.00**  
**(plus £4.00 p&p)**  
For Full Details Of All Our Products, Please See Our Website at [www.invertrain.com](http://www.invertrain.com)  
Or Send £4.00 for our Illustrated Catalogue To: 33 Rose Gardens, Cairneyhill, Dunfermline, Fife, KY12 8QS  
Tel: 01383 880844 (Answer-phone available)  
Please make cheques payable to Invertrain Model Railways



**S&D MODELS** 7 mm scale products  
 Kit & Model Manufacturers  
**LIGHT, INDUSTRIAL and  
 NARROW GAUGE RAILWAY  
 and ROAD VEHICLES and  
 DETAILING ACCESSORIES**

Manufacturers of  
**PHOENIX  
 Scale Miniatures**  
 1:43 scale figurines  
[www.sanddmodels.co.uk](http://www.sanddmodels.co.uk)

For a copy of our current PRODUCTS LIST please send 5 x 1st Class stamps to:  
 Dept. RM, Highbridge Works, P.O. Box 101, Burnham-on-Sea TA9 4WA

**H&A MODELS**  
 Tel: 01933 355531  
 Mobile: 07969 535699  
 19 Keats Way, Higham Ferrers,  
 Northants NN10 8DQ  
 Email: [sales@hamodels.net](mailto:sales@hamodels.net)

Coach & Wagon Kits, Wheels, Decals, Wagon Loads, Paint & Sundries  
 Parkside Models, Dundas Models, Cambrian, Slaters, Ian Kirk, Red Panda, Dapol, Kadec, Ratio, Peco,  
 Precision Paint, Modelmaster, Old Time Workshop, Labelle Oils, Alan Gibson, Romford, Seuthe, Smiths  
**SLATERS 4MM WAGON KITS NOW IN STOCK**

Order on line at: [www.hamodels.net](http://www.hamodels.net) or ring us (same day or next day despatch)  
 Precision Paint - pre order and pick up at exhibitions (minimum two weeks notice)  
 August Exhibitions - 24th Northampton NN7 2LS - 31st & 1st September Sour Valley, Loughborough LE11 2DU

**SIMON'S TRAINS.COM**  
 Train-Sets, Locos, Rolling Stock, Accessories, Airfix kits, Balsa Wood, Glue  
 Paint, Tools, Magazines, Figures, Point Motors, Decoders, Wire and more!  
**PECO** **HORNEY** **DAPOL**  
 Open 10am to 4pm Tuesday to Saturday  
 Call us on 01409 255510 or email to [sales.simonstrains@gmail.com](mailto:sales.simonstrains@gmail.com)  
 6 Bank House, Chapel Street, Holsworthy, Devon, EX22 6AR

**INVERGOWRIE MODEL CENTRE**  
 Stockists of Bachmann, Graham Farish, Hornby, Gaugemaster, Peco  
 and a large selection of kits and accessories by various manufacturers.  
**ALL AT DISCOUNTED PRICES**  
 Large stock of diecast and plastic models.  
**118 MAIN STREET, INVERGOWRIE, DUNDEE DD2 5BE 01382 561064**  
[angsgus@aol.com](mailto:angsgus@aol.com)

**On The Trail Of The Titfield Thunderbolt**  
 by Simon Castens  
 Re-live the action as Ealing Studios film the "Titfield Thunderbolt" on the Camerton Branch  
 line in the blazing hot summer of 1952. A complete guide to location filming, featuring  
 detailed maps, extensive photography (including many previously unpublished views),  
 anecdotes and historical details. £8.95 plus £3.25 P&P in the UK.

**The Somersetshire Coal Canal A Second  
 Pictorial Journey**  
 by Roger Halse  
 Explore the landscape of the film before the Camerton Branch was built with this  
 beautiful pictorial journey from Dundas to Timsbury Basin. Numerous views relate  
 to the film, including Monkton Combe, Dundas Aqueduct and Midford Viaduct.  
 £7.50 plus £3.25 P&P within UK.

**SPECIAL OFFER**  
 For the ultimate Titfield Experience take both books for just £11.50 plus  
 just £3.25 P&P within UK.

**WILD SWAN BOOKS LIMITED**  
 3A Upper Lambridge Street, Larkhall, Bath BA1 6RY  
 Tel: 01225 462332  
 Website: [www.titfield.co.uk](http://www.titfield.co.uk) Shop open Wednesday to Saturday 10.00 to 5.30

**MonkBar  
 Model Shop**  
 Est. 1963  
 2 Goodramgate  
 York - YO1 7LQ  
 01904 659423  
[info@monkbarmodelshop.co.uk](mailto:info@monkbarmodelshop.co.uk)

**OO, N & O Gauge Model Railways**  
 Diecast Planes & Vehicles - Plastic Kits & Figures - Scalextric - Jigsaws

**Opening Hours:**  
**Monday - Saturday**  
**9am - 5.30pm**

Located only 2 minutes  
 away from York Minster

Stockists of Hornby, Bachmann, Graham Farish, Peco, Dapol, Gaugemaster, Ratio, Willis,  
 Woodland Scenics, Metcalfe, Oxford Diecast, Base Toys and many more  
[monkbarmodelshop.co.uk](http://monkbarmodelshop.co.uk)

**TOWNSTREET**  
 From the  
 stonecast range that  
 brings realism to your layout

**MODEL ARCHITECTURE AT ITS FINEST**  
**TOWNSTREET 6 KIRK HILL, BLENCCARN,  
 Nr. PENRITH, CUMBRIA CA10 1TZ**  
 MAIL ORDER ONLY PLEASE - MON TO FRI 2-8pm  
 Tel: 01768 88456

**NEW**

**"OO"  
 REPAIR SHOP**  
**£69.50  
 + £4.60 p&p**

For a  
 selection of  
 our other kits, send  
 for our 60 page "OO" catalogue.  
 Full of buildings, bridges, platforms  
 and ideas for your layout £3.80 post free.

**NEW**

**GWR  
 Sand Bin  
 and  
 Furnace**  
**£19.50**

See our new extensive website  
 detailing all our products  
[www.townstreetuk.co.uk](http://www.townstreetuk.co.uk)

**THE SIGNAL BOX**  
 1 ALBION STREET, ANSTEY, LEICS. LE7 7DD  
 Tel: 0116 236 2901  
 e-mail: [signal\\_box@talk21.com](mailto:signal_box@talk21.com)

Our Shop is 3 miles from Rothley Station on the Great Central  
 & open Mon-Sat 9.30am-5.00pm.  
 Expert and Friendly Service

**BACHMANN RETAILER OF THE YEAR 2011**  
**MODEL RAILWAYS**

In 'OO' and 'N' gauges we carry comprehensive stocks from all major suppliers  
 plus a large range of second-hand.

We supply Digital Controllers and Decoders and offer a decoder fitting service  
 (free on DCC ready locomotives purchased from us).

We also stock Accucraft Live Steam Locomotives and Rolling Stock,  
 Slaters 16mm kits and Peco SM32 track.  
 Repairs and servicing carried out.

Our full range of DVDs are still available in the shop.

**SMALLBROOK STUDIO**  
 1:24 Gn15 Narrow Gauge Vacuum Cast Resin  
 Locomotive Body Kit 'COLUMBINE' £45.00

Designed in the tradition of  
 'Minimum Gauge Railways' as expounded by  
 Sir Arthur Heywood Bt.

The kit is designed to run on OO gauge track and fit  
 onto the Hornby basic outside cylinder 0-4-0 loco  
 chassis, available for £35.00.

Post & Packing on all orders:  
 UK £4.00, Europe £7.00, Rest of World £9.00.  
 Send large SAE with large stamp for our  
 illustrated Gn15 Heywood price list.  
 Payment by PayPal on our website, phone for all  
 major credit/debit cards or cheque.

**Smallbrook Studio, Smallbrook House, Gunville West,  
 Newport, Isle of Wight PO30 5LW. Tel: 01983 520584**  
 E-mail: [smallbrookstudio@gmail.com](mailto:smallbrookstudio@gmail.com) [www.smallbrookstudio.co.uk](http://www.smallbrookstudio.co.uk)

**THE IVO PETERS COLLECTION  
 ON DVD**  
**Volume Four**  
**North Wales Narrow Gauge Steam 1959/60**

In 1959/60 Ivo Peters filmed at  
 Vale of Rheidol Railway  
 Talylllyn Railway  
 Snowdon Mountain Railway  
 Penrhyn Slate Quarries  
 and Padarn and Dinorwic Quarry Railways

The DVD is available now from Julian Peters  
 for £16.95 (includes VAT and p&p).  
 Please make cheques payable to The Ivo Peters Collection and send to:

**The Ivo Peters Collection,**  
 2 Dark Lane, Steeple Ashton, Nr Trowbridge, Wiltshire BA14 6EY



# EXMOOR RAIL MINEHEAD

THE SHOW NEXT TO THE STATION!  
**SATURDAY 3rd AUGUST**  
10.00AM TO 4.30PM

THE MINEHEAD EYE, MART ROAD, MINEHEAD, TA24 5BJ  
(ACROSS THE CAR PARK FROM WSR STATION)

Admission £6.00, Children £2.00, Family (2+2) £13.00

Supported by: **RAY HEARD MODEL RAILWAYS**

TOP QUALITY LAYOUTS PLUS DEMONSTRATIONS, TRADE SUPPORT,  
REFRESHMENTS, FREE PROGRAMME, EASY-ACCESS VENUE,

CONTACT: DAVID ON 01643 706736

EXMOOR COAST  RAILWAY MODELLERS



Website:  
**www.merg.org.uk**  
Please send email to:  
**enquiries@merg.org.uk**  
for more information

**Learn about electronics with our model railway  
beginner kits at affordable prices**

**Simple kit PMP7 -  
DCC Train Detector  
powered by the track -  
£2.82 incl. p&p**

Available only for members



**DCC Supplies**  
Excellence in model control

Email: info@dccsupplies.com Website: www.dccsupplies.com  
Telephone: 01905 621999



- We take the time to find the right product for you
- Our own range of innovative products
- All encompassing range of stock products
- Wide range of Dapol Products (price match on locos & wagons)
- Special Commissions, Limited Editions
- DCC installations, service and repairs for any gauge
- Expert friendly customer support
- Informative DCC Workshops
- Teaching you the facts since 2006
- Visit us at our shop and try the demo facilities

#### Shop Opening Times:

- Mon-Thur 10am-5pm, Fri 12pm-5pm
- Sat 10am-4pm

#### Phone Opening Times:

- Mon-Fri 10.30am-12pm & 2pm-4.30pm

Unit 17A Top Barn Business Centre, Holt Heath, WR6 6NH UK

**www.expressmodels.co.uk**

## LIGHTING SPECIALISTS

**'THE LIGHT AT THE END OF THE TUNNEL'**

We manufacture 4mm scale working Gas type Lamps, along with Modern Image Depot Lamps and Buffer Stop Lamps. We make our own range of working Traffic Lights and Barrier Warning Lights. We also stock Viessmann working street & Station Lamps. Add realism to your layout with working Colour Light Signals.

We stock the excellent range of Traintronics Signals.

Visit our website which has secure online shopping

**EXPRESS MODELS, 65 CONWAY DRIVE, SHEPshed,  
LOUGHBOROUGH, LEICS LE12 9PP**

Tel: 01509 829008 Fax: 01509 560878

Email: [express.models@ntlworld.com](mailto:express.models@ntlworld.com)



## EILEEN'S EMPORIUM

Mail order: Unit 19.12 Highnam Business Centre  
Newent Road, Gloucester, GL2 8DN, ENGLAND

Tel: 01531 828009  
[sales@eileensemporium.com](mailto:sales@eileensemporium.com)  
[www.eileensemporium.com](http://www.eileensemporium.com)

*For Soldering Tools and Consumables  
Lead Solders, Paste and Liquid Fluxes  
Antex Soldering Irons  
Replacement Bits*

*Tip Tinner/Cleaner*

Visit our webshop [www.eileensemporium.com](http://www.eileensemporium.com)  
Visitors welcome by appointment only

## MALC'S MODELS

2 - 4 Pelham Street, Ilkeston, Derbyshire DE7 8AR.  
Telephone: 07786 896807

**All gauges of model railways.  
British, American & Continental Railways.**

### New & Secondhand

Also stockists of Airfix, Revel Kits  
& Die Cast Models.

We also repair most makes of locos.

Mon-Sat 10am-5pm Closed Wed & Sun

Credit Cards Accepted

[www.malcsmodels.co.uk](http://www.malcsmodels.co.uk)

e-mail: [malcolmgrubey@btinternet.com](mailto:malcolmgrubey@btinternet.com)

### Exclusive To MALC'S MODELS



N Gauge  
**ILKESTON &  
HEANOR wagon**  
£10.50 each  
(p&p £3.95)

### Exclusive To MALC'S MODELS



N Gauge  
**AWSWORTH  
wagon**  
£10.50 each  
(p&p £3.95)

### Exclusive To MALC'S MODELS



N Gauge  
**STANTON wagon**  
£10.50 each  
(p&p £3.95)

**LORD AND BUTLER CARDIFF**  
2000 sq ft model railway showroom CAFE & FREE PARKING



The Old Warehouse The Pumping Station  
Penarth Rd Cardiff CF11 8TT

OPEN Mon - Sat 10-5.30 Sunday CLOSED

029 2066 7225 [info@lordandbutler.com](mailto:info@lordandbutler.com) [lordandbutler.com](http://lordandbutler.com)

## Kent Garden Railways

### 16mm and G Scale Specialists

LGB \* Accucraft \* Peco \* Piko \* Bachmann \* Trainline \*  
Roundhouse \* Pola \* USA Trains and much more



### Great Stocks of OO, HO & N

We have large stocks from the leading manufacturers

Hornby \* Bachmann \* Peco \* Gaugemaster \* Metcalfe \* Heljan \*  
Graham Farish \* Dapol \* Woodland Scenics \* Kato \* Walthers \*  
Kadee \* Expo \* Noch \* Oxford Diecast \* Kestrel \* Atlas \* Parkside \*  
Ratio \* Wills \* Modelscene \* Minitrains and more



Accucraft 16mm IOM Mannin  
Indian Red or Holly Green  
Manual live steam £1440.00  
Electric £1240.00

66 High Street, St Mary Cray, Orpington, Kent, BR5 3NH

Tel 01689 891668 email [sales@kgrmodels.com](mailto:sales@kgrmodels.com) [www.kgrmodels.com](http://www.kgrmodels.com)

Shop open 0900-1700 Monday to Saturday



**CORK SHEET FOR TRACKLAYING**

|                         |      |        |                |        |
|-------------------------|------|--------|----------------|--------|
| 36" x 24" x 1/16" thick | each | £5.91  | 5 or more each | £4.77  |
| 36" x 24" x 1/8" thick  | each | £7.82  | 5 or more each | £6.85  |
| 36" x 24" x 3/16" thick | each | £12.20 | 5 or more each | £10.86 |

**CORK ROLLS FOR TRACKLAYING**

|                                       |      |        |
|---------------------------------------|------|--------|
| 1 1/4" wide x 1/16" thick x 98" long  | each | £14.17 |
| 7/8" wide x 1/16" thick x 98" long    | each | £11.17 |
| 7/8" wide x 1/8" thick x 98" long     | each | £12.36 |
| 2 3/16" wide x 1/16" thick x 98" long | each | £23.24 |
| 1 1/4" wide x 1/8" thick x 98" long   | each | £24.45 |
| 1 1/2" wide x 1/16" thick x 98" long  | each | £17.50 |
| 1 1/2" wide x 1/8" thick x 98" long   | each | £28.04 |

**CHAMFERED CORK FOR TRACKLAYING**

|                                          |        |        |
|------------------------------------------|--------|--------|
| 1" wide x 1/8" thick x 19 1/2" long      | per 10 | £9.29  |
| 1 3/4" wide x 3/16" thick x 19 1/2" long | per 10 | £12.48 |

**NATURAL GRANULATED CORK FOR BALLAST AND SCENIC DISPLAY**

|                            |      |       |
|----------------------------|------|-------|
| Natural 0.5/1.0mm, 80g bag | each | £5.48 |
| Natural 2.0/3.0mm, 80g bag | each | £5.48 |
| Natural 4.0/5.0mm, 80g bag | each | £5.48 |

**COLOURED CORK GRANULES FOR BALLAST AND SCENIC DISPLAY**

|                                |              |       |
|--------------------------------|--------------|-------|
| Grey 0.5/1.0mm, 80g bag        | Down to each | £5.48 |
| Grey 2.0/3.0mm, 80g bag        | Down to each | £5.48 |
| Light green 0.5/1.0mm, 80g bag | Down to each | £5.48 |
| Mid green 0.5/1.0mm, 80g bag   | Down to each | £5.48 |

**CORK BARK FOR SCENIC DISPLAY**

|               |      |       |
|---------------|------|-------|
| Per large bag | each | £8.22 |
|---------------|------|-------|

All items including postage Trade enquiries welcome

**QUALITY CORK SUPPLIES**

Unit F3, Anchorbrook Industrial Estate, Lockside, Aldridge WS9 8BZ.  
Tel: 01922 455736 Fax: 01922 745244  
or buy online at [www.charlescantrill.com](http://www.charlescantrill.com)

Model Trains & Accessories – We Sell All Makes, Models & Accessories  
Also Die Cast – Radio Controlled – Toys – etc.

# RAILWAY CONDUCTOR

**PECO** **GRAHAM FARISH** **HORNBY** **DAPOL** **GAUGEMASTER**

**T: 01327 439736 M: 07961 757094**  
Email: [therailwayconductor@outlook.com](mailto:therailwayconductor@outlook.com)  
[www.therailwayconductor.co.uk](http://www.therailwayconductor.co.uk)

Unit 2, The Old Dairy Craft Centre, Upper Stowe, Nr Weedon off A5, Northamptonshire NN7 4SH.

**OPEN 7 DAYS A WEEK**  
• Mon to Sat 10.30-5.30 • Sun 11.00-4.00  
out of hours please call mobile – thanks

We stock OO, N, O, OO-9 & G Gauge.  
Top prices paid for used items,  
please call to arrange a valuation.

We do Repairs, Servicing & DCC Fitting.  
We also build layouts to your specifications.

**VISA** **PayPal** **ebay**

# Pooleys Puffers

Partnership: David and Anne Poole

**PECO** **HORNBY** **DAPOL** **JAVIS** **WILLS-KITS**

**Model Railway Supplies,  
New & Second Hand Bought & Sold  
Diecast Models, kits & accessories**

Tel: 0191 4910202/4106386 Mobile: 07976 519178  
[www.pooleyspuffers.com](http://www.pooleyspuffers.com) email: [pooleyspuffers@btopenworld.com](mailto:pooleyspuffers@btopenworld.com)

382a Jedburgh Court, Team Valley Trading Estate,  
Gateshead, Tyne & Wear NE11 0BQ  
Mon-Sat 10am-4pm Sunday CLOSED  
When attending weekend exhibitions shop will be closed  
Please phone FRI/SAT/BANK HOLIDAYS for opening times.

# C & L Finescale Track Building Systems

125 Reaver House, 12 East Street, Epsom KT17 1HX  
Office hours: 10.00 to 14.00 Monday, Tuesday, Thursday  
14.00 to 19.00 Wednesday Closed Friday

**NEW** C&L OO track base now available  
Please refer to website.

C & L have been suppliers of well proven range of track components for over 30 years in both 4mm and 7mm scales with our easy to build point kits and flexi track including a range other useful materials check out our new website for up to date product listings and exhibition calendar. **New** 4mm code 75 bull head track 60ft panels and 2mm British Finescale range of parts.

**Telephone 01372 748490**  
[www.clfinescale.co.uk](http://www.clfinescale.co.uk)

# N Gauge Society

Five good reasons to be a member

- 1) Six full colour Journals per year
- 2) The largest range of N Gauge wagon kits
- 3) Exclusive RTR Products
- 4) Free Helpline if you have a problem
- 5) Meet other members via the extensive Area Group network

For introductory pack giving details of membership benefits, plus a free sample Journal, send £1.50 to:  
Dept RM, 52 Gainsborough Road, Wallasey, Merseyside CH45 8PT

[www.ngaugesociety.com](http://www.ngaugesociety.com)

# MODELMASTER.UK

31 Crown Street, AYR, Scotland, KA8 8AG  
[www.modelmaster.uk](http://www.modelmaster.uk) 01292 289770

**B.R. & L.N.E.R. Locomotive Lining, B.R. Loco Crests, Steam & Diesel Loco numbers now available for 7mm Modellers!**

**NEW Range of Etched Nameplates and Waterslide Transfers for Locos, Coaches & Wagons available for 2mm Modellers!**

**16,000 plus Etched Nameplates and Waterslide Transfers for Locos, Coaches & Wagons available for 4mm Modellers!**

**Don't Forget** Our range of Materials, Tools & Loco parts and accessories, plus Kits & Books **available for Everyone!**

See our unique range of 18,000+ Decals & Nameplates at [www.modelmaster.uk](http://www.modelmaster.uk)

# MODELIT

MODELIT Ltd, 5 HENACRE WOOD COURT, QUEENSBURY, BRADFORD, BD13 2LJ  
Tel: 01274 817344 (mail order only - no callers)  
Postage up to £30.00 value £3.50, over £30.00 £4.99

[www.modelitltd.co.uk](http://www.modelitltd.co.uk)

**Model Lamps for "N" & "OO"**

**"PLUG-IT" no soldering connecting system**

**NEW Building Interior Lights**

Simple to fit and connect to our "PLUG-IT" system. Consists of a LED and 12v resistor on a small board with self adhesive back. 30cm fine leads  
Pack of 2: £4.99

**Colour Light Signals**

**NEW** 2-light green/red 'N' - £7.99  
2-light green/red 'OO' - £7.99  
2-light green/red 'N' - £5.99  
2-light green/red 'OO' - £5.99

Control manually with On-On switch £3.99 (incl 30cm leads) For more details see our website

Swan Neck Street Lamp (green) 'N' or 'OO' - £2.99  
Gas Lamp (x2) 'OO' - £2.99

Light OR DISTRIBUTION LEAD  
Light Leads 15cm approx

POWER SUPPLY WITH SOCKET 30cm  
EXTENSION LEAD

Power Supply £19.99  
Extension Lead 100cm £2.99  
Extension Lead 30cm (x2) £ 3.99  
Distribution Lead (5-way) 50cm £3.99

# Conwy Valley Railway Museum

## BETWS-Y-COED LL24 0AL

**STEAM & DIESEL LOCOS OPERATING OUR 7 1/4" SCENIC RAILWAY. 10 MINUTE RIDE MUSEUM WITH MODEL RAILWAY LAYOUTS**

- 'OO' Hornby – Bachmann Branchline – 'N' Gauge – 'Z' Gauge
- IN STOCK 'N', 'HO' & 'G' GAUGE CABLE CARS
- 'O' Gauge Bassett-Lowke Locos and Rolling Stock
- Large stocks 'G' Gauge – Roundhouse Live Steam
- NOW IMPORTING FULL RANGE LIVE STEAM WILESCO
- Excellent stocks of Peco – 009 – 'HO' – Scenery

**Mail Order Tel: 01690 710568 Fax: 01690 710132**  
**OPEN DAILY ALL YEAR 10.00am to 5.00pm Large Car Park**  
e-mail: [info@conwyvalleyrailwaymuseum.co.uk](mailto:info@conwyvalleyrailwaymuseum.co.uk)  
web site: [www.conwyvalleyrailwaymuseum.co.uk](http://www.conwyvalleyrailwaymuseum.co.uk)



**MODEL  
LEYLAND  
RAILWAY CLUB**

## Model Railway Exhibition

**Saturday 10 August 2019**  
10am - 5pm

**Sunday 11 August 2019**  
10am - 4.30pm

Adult £5.00 Junior £3.50 Family £13.50 Under 5 FREE

**LEYLAND CIVIC CENTRE**  
West Paddock, Leyland PR25 1DH

Buses from Leyland Railway Station 109, 111, 112, 113  
(Correct at time of printing)

For details and info: **David 07796 130295**

Childrens  
Activity  
Corner

Prices  
as  
last year




**YOUR DREAM MODEL RAILWAY BUILT BY PROFESSIONALS**

\* Unique Hand-Crafted Layouts - From tiny but superbly sceniced dioramas to the most extensive and complex layouts

We offer the following services: layout consultation and design - baseboard construction, track laying, wiring - control panels - working signals - scenery and backscene painting by our professional artist - individual model buildings (kit or scratchbuilt) constructed and weathered by experts.

Pay us a visit (please phone first), and see the superb standard of our work, or send an A4 SAE for our illustrated brochure.

**Old Barn Model Craftsmen**  
The Cottage, 2a Brighton Road, Shoreham-by-Sea, West Sussex BN43 6RG.  
Tel: 01273 591024 (24 hr answering service)  
Email: [brian.john.taylor@outlook.com](mailto:brian.john.taylor@outlook.com)  
[www.oldbarnmodelcraftsmen.co.uk](http://www.oldbarnmodelcraftsmen.co.uk)



**blueRailways**  
email: [sales@bluerailways.co.uk](mailto:sales@bluerailways.co.uk)

**Wireless  
DC Controllers**






For wireless control of your Model Railway using Tablet, Smartphone or Model 720 hand-held Wireless Controller.

[www.blueRailways.co.uk](http://www.blueRailways.co.uk)



**01297 35557**  
Opening Times:  
Sun-Mon CLOSED  
Tues-Fri 10am-4pm  
Sat 10am-1pm

Colston Cross near Axminster,  
Devon EX13 7NF  
(On A358 between Chard & Axminster)  
[media@buffersmodelrailways.com](mailto:media@buffersmodelrailways.com)  
Most Major Cards Accepted

Est 2000 Family Run [www.buffersmodelrailways.com](http://www.buffersmodelrailways.com)

Second Hand Bought & Sold  
(In Store Only)

Sunny days & cream teas!

Locos - Wagons - Carriages -  
Track & Accessories - Scenery  
(both pre-built & scratch) -  
Paint - Adhesives - Tools -  
Power & Electrical

Hornby - Peco - Bachmann -  
Peco - Dapol - Woodlands  
Scenics - Gaugemaster -  
Tamiya - Games Workshop -  
and many many more.



Find us on  
Facebook/buffersmodelrailways

Please Call In, Have A Look, Some Fun  
And A Cuppa

Visit → [scalescenics.com](http://scalescenics.com)  
Download → Print → Build

Includes jig to make corrugated sheeting



**NEW China  
Clay Loading  
Facilities**  
Half relief.  
Includes  
functional  
sliding doors  
ONLY £5.99

Ramp, storeroom and platform  
can be placed at either end

Buy once unlimited prints View our large range of  
N and OO kits and texture sheets

[scale  
scenics  
.com](http://scale<br/>scenics<br/>.com)

**RAILWAYSCENICS**

01747 826269  
Secure online ordering

Online model shop supplying a wide range of tools,  
scenics, materials, electrical parts and components,  
kits and downloadable products to  
model railway enthusiasts.

[www.railwayscenics.com](http://www.railwayscenics.com)  
[sales@railwayscenics.com](mailto:sales@railwayscenics.com)

14 Woodsage Drive, Gillingham Dorset SP8 4UF

## BURE VALLEY MODELS

[www.burevalleymodels.com](http://www.burevalleymodels.com)

Check out our Hornby prices...

|          |                                           |         |       |                                         |         |
|----------|-------------------------------------------|---------|-------|-----------------------------------------|---------|
| R3374    | BR Class 71 BR Blue 71012                 | £99.00  | R3623 | LMS Princess Coronation Queen Elizabeth | £142.50 |
| R3410    | BR King Henry III BR Blue Early Crest     | £137.50 | R3627 | BR A3 4-6-2 60103 'Flying Scotsman'     | £135.00 |
| R3451    | BR B1 61032 Stembok BR Black Early        | £132.00 | R3638 | BR West Country Class 4-6-2 Blideford   | £138.00 |
| R3508TTS | BR A3 Flying Scotsman BR Green Late       | £165.00 | R3642 | Princess Coronation Duchess of Montrose | £142.50 |
| R3520    | Britannia Pacific 70007 Coeur De Lion     | £135.00 | R3659 | Colas Rail Freight Class 67 Bo-Bo 67023 | £120.00 |
| R3533    | J94 0-6-0ST Lord Phil                     | £65.00  | R3660 | Class 56 Co-Co Diesel 56 303 DCR Livery | £125.00 |
| R3573    | Class 66 GBRf Livery 66 751               | £75.00  | R3676 | LNER A4 Class 4-6-2 4468 'Mallard'      | £135.00 |
| R3575    | Class 153 DMU East Midlands Trains 153379 | £83.00  | R3677 | Princess Coronation Duchess of Hamilton | £142.50 |
| R3603TTS | Lord Nelson Class 4-6-0 30850             | £146.50 | R3682 | BR Blue 46225 Duchess of Gloucester     | £147.50 |
| R3606    | 5-BEL 'Brighton Belle' Train Pack         | £150.00 | R3768 | BR 'Terrier' 0-6-0T 32636 BR Black Late | £67.50  |
| R3607    | The 15 Guinea Special Train Pack          | £220.00 | R3780 | LB&SCR 'Terrier' 0-6-0T 655 'Stepney'   | £67.50  |
| R3618    | Battle of Britain Royal Observer Corps    | £135.00 | R3782 | SE&CR 'Terrier' 0-6-0T 751              | £67.50  |

Shop open 7 days a week 9am to 5pm  
Aylsham Station, Norwich Road, Aylsham, Norfolk, NR11 6BW  
01263 733858 [shop@bvrw.co.uk](mailto:shop@bvrw.co.uk)  
Postage £5.00 per order to UK destinations. All major credit cards accepted



**SAT MODEL RAIL**














Open: Mon, Wed, Thurs, Sat, Sun, 10am to 5pm  
Fri, 1.30pm to 5pm Closed: Tuesday

Unit G5, The Old Granary, Hawk Hill, Battlesbridge,  
Wickford, Essex. SS11 7RE Free Parking  
T: 01268 562588  
[www.satmodelrail.co.uk](http://www.satmodelrail.co.uk) [info@satmodelrail.co.uk](mailto:info@satmodelrail.co.uk)



**WIZARD MODELS** **51L**

**MAINLY TRAINS** **COMET MODELS**

**4mm scale complete signal kits – £10.50 each**

|                                                  |                                                |
|--------------------------------------------------|------------------------------------------------|
| S4/KE1 LNER/BR(E) tubular post                   | S4/KM3 LMS tall lattice post                   |
| S4/KE2 LNER (ex-NBR) lattice post                | S4/KM4 LMS wooden post                         |
| S4/KE3 LNER concrete post                        | S4/KM5 MR wooden post (lower quadrant)         |
| S4/KE4 LNER wooden post                          | S4/KS1 SR/BR(S) rail-built post                |
| S4/KE5 LNER slotted wooden post (lower quadrant) | S4/KS2 SR lattice post                         |
| S4/KE6 LNER (ex-GN) lattice post                 | S4/KW1 GWR wooden post (lower quadrant)        |
| S4/KM1 LMS/BR(M) tubular post                    | S4/KW2 GWR/BR(W) tubular post (lower quadrant) |
| S4/KM2 LMS (ex-CR) lattice post                  |                                                |

All may be built as home or distant

**UK P&P £3.90, Airmail at cost. For price list, send 5 1st class stamps**

**WIZARD MODELS LTD**  
 PO Box 70, BARTON upon HUMBER DN18 5XY  
 Phone 01652 635885  
 E-mail [andrew@modelsignals.com](mailto:andrew@modelsignals.com)  
 Shop online at [www.wizardmodels.ltd](http://www.wizardmodels.ltd)

Hornby ★ Peco ★ Bachmann ★ Metcalfe  
 Wills ★ Ratio ★ Harburn Hamlet ★ Modelscene

N Gauge & OO Gauge

# Modelmaniacs

Unit 9 Harris Rd, Calne, Wilts SN11 9PT Tel: 01249 817731  
[www.Modelmaniacs.co.uk](http://www.Modelmaniacs.co.uk)

Visit our Shop, Website or Give Us a Call!

Javis ★ Scenecraft ★ Superquick  
 Oxford Rail ★ Dapol . . . & more!

**Sunningwell Command Control Ltd.** **4 DCC**

*Always looking to bring you the best in Digital Command Control systems and support*

Blackstone Models

Run by railway modellers for railway modellers, with over 14 years of serving the DCC community with the experience and knowledge to match.

For further details please send a SAE to:  
 SCC Ltd, PO Box 381, Abingdon SO, OX13 6YB Tel: 01865 730455  
 Or visit our website at: [www.scc4dcc.co.uk](http://www.scc4dcc.co.uk) Email: [info@scc4dcc.co.uk](mailto:info@scc4dcc.co.uk)

## Thinking about Narrow Gauge?

There's so much more in 7 mil . . .

Join us?  
[www.7mmnga.org.uk](http://www.7mmnga.org.uk)

1979 - 2019  
 NARROW GAUGE  
 THE 7 MM  
 ASSOCIATION  
 40th ANNIVERSARY

As seen in Railway Modeller June 2019

Loco and photo:  
 Geoff Thorne

# ANDY LOWE Electronics

All your electronic components at LOWE prices

[www.andyloweelectronics.co.uk](http://www.andyloweelectronics.co.uk)

**MINI TOGGLE SWITCHES**  
 SP/DP-single/double pole, ST/DT-single/double throw  
 CCS Connectors to fit below (saves soldering) 8p each

|                                          |          |       |
|------------------------------------------|----------|-------|
| SMT1 on/off SPST                         | each 1+  | 20+   |
| SMT2 on/on SPDT                          | £1.20    | £1.10 |
| SMT3 on-off-on SPDT                      | £1.20    | £1.10 |
| SMT4 on-on or on-off DPDT                | £1.25    | £1.15 |
| SMT5 on-off-on DPDT                      | £1.30    | £1.20 |
| SMT6 [on]-off-[on] SPDT for points       | £1.40    | £1.30 |
| SMT7 [on]-off-[on] DPDT                  | £1.60    | £1.50 |
| Lever covers for identification and grip | £1.90    | £1.80 |
| Rd,yell,bl,bk,gn,wh,gray                 | 20p each |       |

**TAG BOARDS, STRIPS & TERMINAL BLOCKS**

|                                        |       |
|----------------------------------------|-------|
| CTB2 Mini Tag Board 2 rows x 18        |       |
| 117mm x 38mm                           | £2.45 |
| CT51 Standard Tag Strip 28 way x 267mm | £2.50 |
| CT52 Mini Tag Strip 28 way x 194mm     | £1.90 |
| CTB4 Screw Terminal Block 3amp         | 80p   |
| CTB5 As above 5amp                     | 95p   |
| CTB6 12-way Plug & Socket 6amp         | £5.50 |
| CTB10 As above, but 10amp              | £6.95 |

**VERY FINE WIRE**  
 ALL 10 colours available and in stock again.  
 EW53/c Overall dia 0.53mm; 7/40; 32AWG;  
 3amp Bk/bl/bn/gy/or/pu/rd/wh/yl  
 £3.75 for 2m

**TINY CONNECTORS**  
 With the very fine wire, use our tiny connectors which are ideal, for instance, for joining engine to tender for extra pick-up: CMP100 2 pin 3/8" x 3/16" CMP101 3 pin CMP102 4 pin & socket. Double rows also available.  
 1+60p/pr 10+55p/pr 25+50p/pr

**GRAIN OF WHEAT/RICE BULBS, LEDs**  
 with built-in 12v resistors in rd, gn, amber, bl, yell, clear

|                                                 |         |
|-------------------------------------------------|---------|
| 1+ ea                                           | 20+     |
| GW8 3mm dia x 10mm, 80ma, 120mm insulated leads | 40p 35p |
| 20 for £7 (35p ea) 100 clear £30 (30p ea)       |         |
| GR8 1.8mm dia 30ma                              | 90p 85p |
| LD14 12v LED 3mm rd,gn,yl                       | 40p 35p |
| LD32 As above in white                          | 40p 35p |
| LD15 12v LED 5mm rd,gn,yl                       | 40p 30p |
| LD1 1.8mm LED rd,gn,yl, amber                   | 30p 25p |
| LD31 As above in white                          | 70p 65p |
| Mounting Clips                                  | 8p      |

**EQUIPMENT WIRE**  
 Cols: rd,yl,bl,bk,wh,gn,pu, or,gy,pk,br

|                                                           |       |        |
|-----------------------------------------------------------|-------|--------|
| EW07/0.2 Multistrand wire 1.4amp                          | 10m   | 100m   |
| 7 strands of 0.2mm wire in plastic sleeving; ideal N & 00 | £1.50 | £8.40  |
| EW07/0.2 as above in bi-colour [striped]                  | £2.00 | £15.99 |
| EW16/0.2 Multistrand wire 3amp                            |       |        |
| 16 strands of 0.2mm wire, ideal 0 & 00                    | £2.25 | £16.99 |
| EW24/0.2 Multistrand wire 5amp                            |       |        |
| 24 strands of 0.2mm wire, ideal 0 & DCC                   | £3.25 | £16.99 |
| EW32/0.2 Multistrand wire 6amp                            |       |        |
| 32 strands of 0.2mm wire, ideal DCC rd,bk                 | £4.25 | £34.00 |

**HEATSHRINK SLEEVING 1.2m (2:1 shrinkage)**  
 Thin tubing which shrinks to seal wire joints when soldering iron hovered above it. Red or bk.  
 HS1 1.6mm £1.20 HS2 3.2mm £1.40  
 HS3 2.4mm £1.30 HS5 1.2mm £1.10

**SPIRAL CABLE WRAP**  
 CT6 6mm dia x 10m £2.75

T: 01981 540366 E: [andylowe50@aol.com](mailto:andylowe50@aol.com)  
 Hillcrest Cottage, Little Birch, Hereford HR2 8AY

VISA Cheq: Andy Lowe P&P: £2.99 PayPal

**ALTON MODEL CENTRE**

Possibly Hampshire's Largest Stockist of Model Railways, Diecast, Books, Magazines, Scalextric and much more...

Full range of model railway products from the leading manufacturers including:  
 Hornby, Bachmann, Dapol, DJ Models, Graham Farish, Heli-jan, Oxford Rail, Peco, Ratio, Wills, Superquick, Metcalfe, Gaugemaster, Parkside, Railmatch, Woodland Scenics and many, many more.

DCC equipment from DCC Concepts, ESU, Gaugemaster, Hornby and Bachmann.  
 Sound decoders from South West Digital.

Also stockists of Die cast models including:  
 Corgi, EFE, Oxford Die-Cast, Base Toys & Classic.

Modelling materials & tools.  
 Vallejo Model Colour  
 AK pigments & washes.

Wide selection of books and magazines.

Monday: Closed  
 Tuesday to Saturday: 9.00am to 5.00pm  
 Sunday: see website / call  
 Tel: 01420 542244  
[altonmodelcentre.co.uk](http://altonmodelcentre.co.uk)  
[sales@altonmodelcentre.co.uk](mailto:sales@altonmodelcentre.co.uk)  
 7a Normandy Street, Alton  
 Hants, GU34 1DD

**DUNDAS MODELS**

MILLIE STREET KIRKCALDY  
 FIFE SCOTLAND KY1 2NL  
 Telephone and fax: 01592 640896  
 Website: [www.dundasmodels.co.uk](http://www.dundasmodels.co.uk)  
 Email: [sales@dundasmodels.co.uk](mailto:sales@dundasmodels.co.uk)

Visit our Illustrated Website to see the complete range of Dundas Models 009 Kits (ex Parkside Dundas) and the products from 40 other 009 and NG Suppliers. A vast and varied selection of Narrow Gauge Products, RTR, Kits, Peco Track, Accessories & Books all at competitive prices.

£2.00 for our 009 Catalogue. UK Post & Packing £3.00 per Order.  
 Overseas Postage at cost (VAT deducted from Non EU Orders).

Exhibitions:  
 009 Society Members Day, Bouverie Hall, Pewsey, Wiltshire, SN9 5QE  
 EXPONG, White Oak Leisure Centre, Swanley, BR8 7BT  
 Warley International, NEC, Birmingham B40 1NT

17 August  
 26 October  
 23-24 November

VISA PayPal



## North Eastern Railway Association

### NEW & REPRINTED PUBLICATIONS

#### A History of North Eastern Railway Signalling



Edited by Neil Mackay  
with Co-Authors Richard Pollock,  
Nicholas Farnsworth and Mark Nicholson

#### A History of North Eastern Railway Signalling

A reprint of this much sought-after book is announced, the first book to be devoted exclusively to the historical development of signalling on the North Eastern Railway. It is the product of many years of primary research, drawing on original source material, and has been written to interest a broad cross-section of readers. Those with a general interest in the NER will find detailed insights and explanations of this little understood subject. Signalling professionals and serious students have not been overlooked: there is ample technical information. For modellers there is a wealth of material including scale drawings.

320 pages A4 size, casebound, 450 illustrations of which 95 are in colour, with line drawings, diagrams, tables and appendices.

Available now: £27.00 post free

#### Lesser Railways Around Darlington The Fighting Cocks, Croft Depot, Forcett and Merrybent Branches

This book contains the history of Darlington's long forgotten branch line railways. The Fighting Cocks branch began life as an integral part of the Stockton & Darlington Railway main line between North Road and a point east of Fighting Cocks station. The S&DR constructed branches off its main line, and one of these, the Croft Depot Branch opened in October 1829, is described next. The final two lines to be covered, the Forcett Railway and the Merrybent & Darlington Railway, both started life as independent undertakings constructed by entrepreneurs mainly to exploit local deposits of limestone which was crucial to the growing iron and steel industry on Teesside.

72 page A4 size, gloss colour card cover, Over 95 colour & monochrome photographs and line drawings.

Available now: £11.95 post free

#### North Eastern Railway Branch Lines I Lesser Railways Around Darlington Fighting Cocks, Croft Depot, Forcett and Merrybent Branches



Edited by Neil Mackay and John G. Hendrick  
with co-authors John P. McCulloch and Richard V. Wilson

#### Ordering

NERA publications are available at meetings, the NERA exhibition stand or by post from NERA Sales Officer (RM), 31 Moreton Avenue, Stretford, Manchester, M32 8BP. Cheques should be made payable to the 'NERA'.

For overseas orders please contact the sales officer, sales@nera.org.uk, stating the publications required, your full address and preferred postage method; surface or airmail, so the total cost can be advised and method of payment is agreed before an order is placed.

For further information on the North Eastern Railway Association, membership and further books & publications see our website: www.nera.org.uk.

## PHOENIX PRECISION



Precision Paints Railway & Military Colours

#### Back in Stock:-

|                |         |        |
|----------------|---------|--------|
| Superstrip 2 - | 125ml   | £12.70 |
|                | 250ml   | £24.20 |
|                | 500ml   | £45.10 |
|                | 1 litre | £79.10 |

Meet us in  
Person at

Gauge 'O' Guildex - Telford 31 Aug/1 Sept  
Mid Essex MRC - Shenfield 21 Sept  
Scaleforum - Aylesbury 28/29 Sept

Plates Etc. by  
No Nonsense Kits



CHURCHWARD  
MODELS

DMR  
Products

Etched Brass Steam Locomotive Kits and Fittings

EX Kirk  
Model Railway Kits

JE Detailing Parts  
The Former Jackson Evans range of bits and pieces

No Nonsense Kits MTK

Visit our online shop at [www.phoenix-paints.co.uk](http://www.phoenix-paints.co.uk)

Phoenix Precision Paints Ltd.

sales@phoenix-paints.co.

01268 730549

**FROME MODEL CENTRE**  
Service and quality the way it should be  
Established since 1986

**QUALITY MODELLING PRODUCTS**  
We stock all popular brands & more!

Here at Frome Model Centre we are a one stop shop for all of your railway modelling needs.

### Model Scene Scenics...Full Range Now In Stock!



Create Incredibly  
Detailed Scenes For Your  
Railway Layout!  
Range includes, trees,  
foliage, weeds, land/water  
plants, leaves, scatter and  
much much more!



Great Value  
Model Scene  
Scenics!

Pop in-store or check out our brand NEW website to see more of the Model Scene Scenics range!

### SERVICE & REPAIR - SECOND HAND - TECHNICAL ADVICE

FROME MODEL CENTRE  
1-2 Catherine Street,  
Frome, Somerset, BA11 1DA

TEL: 01373 465295  
Fax: 01373 451468

Email: sales@frome-model-centre.com

[www.fromemodelcentre.com](http://www.fromemodelcentre.com)

FREE  
PARKING

#### OPENING HOURS

Tues-Sat: 10am-5pm  
Sun/Mon: Closed



**KITTLE HOBBY**

RHONGYR UCHAF HOUSE, off Penwyllt Rd,  
Penycae, Swansea SA9 1GD (Callers welcome)

Tel. 01639-731005 Fax. 01639-731006

E-mail: banksofkittle@aol.com

Website: [www.kittlehobby.com](http://www.kittlehobby.com)



★ STILL A FAMILY BUSINESS GIVING PERSONAL SERVICE ★ MASTERCARD : VISA : SWITCH CUSTOMERS ORDER BY PHONE.  
LARGER PREMISES AND BIGGER STOCKS. ORDER WITH CONFIDENCE. ANSWERPHONE OUT OF HOUR

#### 'O' Gauge buildings p&p £4.99 (BU - Built up)



Victorian Station Kit (pre-coloured) 380mm x 120mm  
£39.45 whilst stocks last!



B45978  
water  
tower  
£25.70



Lattice Footbridge £25.95

|                               |        |
|-------------------------------|--------|
| L681629 Lumber Shed Kit       | £44.60 |
| A6902 Island Platform         | £35.95 |
| MTH9097 Gate Tower BU         | £29.70 |
| MTH Tunnel portal x 2         | £17.50 |
| L612888 Railroad Crossing Kit | £49.95 |
| Corrugated Tool Shed Kit      | £14.95 |



LMS Coronation blue & grey 4 Coach Set  
2 rail fine scale £460.00



[www.heathcote-electronics.co.uk](http://www.heathcote-electronics.co.uk)

Bring movement to your model railway with our **SERVO MOTORS** and **SERVO MOTOR CONTROLLER**. Control your points and semaphore signals realistically. Open and close both swinging and sliding doors & gates, eg. engine shed doors, level crossing and factory siding gates. Animate models of cranes, swings, fairground rides and moving figures. Set in motion waggon tipplers, uncouplers and Beam Engines.



Our simple to use servo motor controllers enable a servo motor to move between two adjustable positions at selected speeds when an on off switch is thrown or when a second servo has finished its movement. Continuous backwards and forwards movement can also be provided.

Single Servo controller £8.95 Dual Servo controller £14.95

Servo motor £4.95 Bracket £1.00

Bouncing Semaphore Controller £13.50 postage £3.80

We have an easily adjustable servo controller to make semaphore signals bounce when returning to clear

See our website for more information and videos.

[www.heathcote-electronics.co.uk](http://www.heathcote-electronics.co.uk)

email: [cah@heathcote-electronics.co.uk](mailto:cah@heathcote-electronics.co.uk) phone: 01538 756800  
Heathcote Electronics, 1 Haydock Close, Cheadle Staffs, ST10 1UE

## SMART SCREEN FROM TRAIN-TECH!

**NEW!** Customise your own animated screen!

Smart Screen is a real working screen which you can customise with your own messages and control by trains to bring your layout to life!



- 2 Line display - top line can show a clock & the bottom line automatically scrolls!
- Control messages by trains passing, point destinations, switches, cycling etc
- Enter your own multiple messages - up to 10 screens can be programmed!

• Multiple uses - Can be used for: Platform signs, Scoreboards, road signs, bus and coach signs.....

• Also destination signs on board trains which change automatically when the train changes direction!



- Enter your messages using standard DCC controller, but once set up can be used on 12v DC or DCC - just 2 wires!
- Control by Track Sensor, Switch, DCC

- SD1 - Single Smart Screen £50 • SD2 - Twin pack Smart Screen £90

A range of enclosures are also available for platforms and canopies

Plus more new products in our free brochure!



**Train-Tech**  
Model Technology Made Easy

Ask for our  
**FREE New  
Brochure!**

DCP Express  
Bryon Court  
Bow Street  
Great Ellingham  
NR17 1JB

01953 457800 / [www.Train-Tech.com](http://www.Train-Tech.com)

[sales@dcpmicro.com](mailto:sales@dcpmicro.com)

£4 UK P&P - this month quote RM819 for £2 off postage!



## FRIZINGHALL FMR THE MODELLERS CHOICE SINCE 1950

### BACHMANN OO GAUGE

|          |                                                           |         |
|----------|-----------------------------------------------------------|---------|
| 30-047   | Whiskies Galore Train Set with Digital Sound.....         | £280.50 |
| 31-060   | LNER J72 Class Lined Black - 2313.....                    | £105.99 |
| 31-061   | LNER J72 Class BR Black Early Emblem - 68733.....         | £105.99 |
| 31-062   | LNER J72 Class BR Black Late Crest - 68696.....           | £105.99 |
| 35-175   | ROD 1918 2-8-0 War Department Black.....                  | £148.75 |
| 35-176   | Railway Operating Division 2-8-0 2394 LNWR Black.....     | £147.75 |
| 32-027B  | Class 20 (Indicator Discs) BR Green D8011.....            | £119.00 |
| 32-035B  | Class 20/0 Headcode Box 20174 BR Blue.....                | £119.00 |
| 32-030DS | Class 20 BR Railfreight Red Stripe 20156 (DCC Sound)..... | £199.70 |
| 32-991   | Branchline Wickham Type 27 Trolley Car BR Maroon.....     | £76.50  |
| 32-994   | Branchline Wickham Type 27 Trolley Car BR Green.....      | £76.50  |

### HORNBY GRAHAM FARISH N GAUGE HORNBY OO GAUGE

|         |                                                              |         |
|---------|--------------------------------------------------------------|---------|
| 370-375 | Avro Vulcan Train Pack, Class 37/4 + 2x N Scale Vulcans..... | £169.99 |
| 370-160 | Castle Pullman Digital Sound Train Set.....                  | £318.50 |
| 372-775 | C Class 0-6-0 SECR Plain Green 271.....                      | £127.50 |
| 372-776 | C Class 0-6-0 Southern Railway Black - 1294.....             | £114.75 |
| 372-777 | SE&CR C Class 31227 BR Black Early Emblem.....               | £114.75 |
| 370-090 | North Eastern Freight Train Set.....                         | £169.95 |
| R3719   | GWR Class 5101 'Large Prairie' 2-6-2T 4154 - Era 3.....      | £125.99 |
| R3721   | GWR Class 61xx 'Large Prairie' 2-6-2T 6110 - Era 3.....      | £125.99 |
| R3723   | GWR Class 61xx 'Large Prairie' 2-6-2T 6145 - Era 4.....      | £125.99 |
| R3725   | GWR Class 5101 'Large Prairie' 2-6-2T 4160 - Era 5.....      | £125.99 |
| R3696   | GWR Class 43 HST 43096 Old Oak Common Era 11.....            | £260.99 |

FREE POSTAGE FOR ORDERS OVER £150.00 (UK Mainland Only)

OFFICIAL STOCKIST

[WWW.MODELRAILSHOP.CO.UK](http://WWW.MODELRAILSHOP.CO.UK)  
Tel: 01274 747447  
Email: [shop@fmrdirect.co.uk](mailto:shop@fmrdirect.co.uk)

DCC Sound & DCC Decoder Fitting Weathering & Renaming / Re-number

OPENING HOURS  
MONDAY to SATURDAY 09.30 - 17.30

8A Sapper Jordan Rossi Park  
Baildon, Otley Road  
West Yorkshire, BD17 7AX

## Trains4U.com 27 St. David's Square, Fengate, Peterborough, PE1 5QA

Open Tues-Fri 9:30-5:00, Sat 9:00-5:00, Sunday 10:30-3:30, closed Monday

### Revolution N trains



- N Gauge Class 92**
- N92003 Beethoven RFD Grey
  - N92001 Victor Hugo EWS
  - N92036 Bertolt Brecht, RFD/EWS
  - N92009 Marco Polo DB Red
  - N92032 Inst. of Mech. Engineers GBRF
  - N92081 Caledonian Sleeper
  - N920xx Un-Numbered RFD Grey

£175.00 each exp. Autumn 2019



- N Gauge VEA munitions Van**
- NVEA001 a/b/c/d/e BR Maroon
  - NVEA002 a/b/c/d/e BR Railfreight Red/Grey
  - NVEA005 a/b/c RFD Grey/Yellow

£19.95 each exp. late Summer 2019



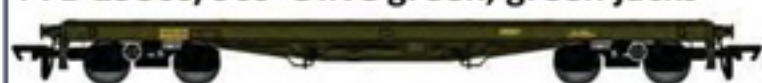
- OO Gauge MMA/JNA Box Wagon**
- OO-EAL-101 A/B/C/D/L DB Red
  - OO-EAL-102 A/B/C/D/L Ermewa Grey
  - OO-EAL-103 A/B/C/D/L Ermewa TarmacGrey

With battery powered flashing tail lamp (L version): £59.00 each

Delivery expected Summer 2019 Standard Model (A/B/C/D version): £55.00 each

### OO gauge MOD KFA / PFB WARFLAT

PFB 1980s/90s Olive green, green jacks



- CAPFB(1) MODA 95233
- CAPFB(3) MODA 95274
- CAPFB(2) MODA 95259
- CAPFB(4) MODA 95288
- CAKFA(1) MODA 95238
- CAKFA(3) MODA 95281
- CAKFA(2) MODA 95265
- CAKFA(4) MODA 95297

KFA 2000s - Present Olive green, yellow jacks



- Close coupling mechanism
- NEM coupling pockets
- Sprung buffers
- Weathered effect decking
- Detailed underframe
- Zinc alloy frame
- Convertible to P4/EM

Produced exclusively for Trains4U by:

**CAVALEX Models**

£35.00 each - Available to pre order now

No deposit required, payment will be requested on arrival in the UK (anticipated Autumn 2020)

All models are available to pre-order now. Order online at [Trains4U.com](http://Trains4U.com), call on 01733 895989, email: [Trains4U@BTConnect.com](mailto:Trains4U@BTConnect.com), or in our Peterborough store.

Follow us on:





★ A GREAT NEW SHOW AT AN AWARD-WINNING VENUE ★

9<sup>th</sup> - 10<sup>th</sup> November 2019 at the British Motor Museum, Banbury Road, Gaydon, Warwickshire CV35 0BJ

Up to 30 layouts across the scales ★ live steam ★ modelling skills area ★ family zone  
★ 25+ specialist traders ★ accessible venue ★ excellent catering

#### ADMISSION PRICES ON THE DAY

Adults £12, Children (5-16) £9,  
Family (2+3) £34, U5s Free

Includes admission to the extensive displays of the British Motor Museum

#### FREE PARKING!

Gates open 10am both days  
(9.30am for pre-booked tickets).  
Open until 5pm Sat, 4pm Sun  
(last entry one hour before close).

Enquiries 01507 499081 or  
use 'Contact Us' form on  
website.

PRE-BOOK and SAVE!  
Adults: £10, Children £7

FREE COURTESY BUS FROM  
LEAMINGTON SPA RAILWAY  
STATION (SAT ONLY)

SHOW  
SPONSOR

**HORNBY**  
COLLECTORS CLUB

**BRITISH  
MOTOR  
MUSEUM**

HOME TO THE WORLD'S  
LARGEST COLLECTION OF  
HISTORIC BRITISH CARS

**WWW.GBMRS.COM**

**GOLDEN VALLEY HOBBIES**

|                                                          |                                             |                                                          |                                         |
|----------------------------------------------------------|---------------------------------------------|----------------------------------------------------------|-----------------------------------------|
|                                                          |                                             |                                                          |                                         |
| Kibri 39456<br>Turntable<br>£16.32                       | Kibri 39452<br>3 Track Roundhouse<br>£48.13 | Vollmer 45754<br>3 Track Roundhouse<br>£84.50            | Auhagen 11383<br>Freight Shed<br>£41.40 |
|                                                          |                                             |                                                          |                                         |
| Auhagen 11400<br>Locomotive Shed & Water Tower<br>£40.90 | Kibri 39422<br>2 Water Cranes<br>£13.55     | Vollmer 45747<br>Cleaning Frame & Gantry Crane<br>£18.13 |                                         |
|                                                          |                                             |                                                          |                                         |
| Auhagen 11334<br>Coal Bunker<br>£13.5                    | Vollmer 49110<br>Engine Shed<br>£24.50      | Auhagen 11404<br>Weighbridge & Loading Gauge<br>£16.30   |                                         |

**www.GoldenValleyHobbies.com**  
Unit 1 Pontrilas Business Park, Pontrilas, Herefordshire, HR2 0AZ  
See our website, write or call to order. Trade enquiries welcome. 01981 241 237  
Prices are correct at time of print and are subject to change.

**Tennents Trains**

www.tennentstrains.co.uk  
facebook.com/tennentstrains  
@tennentstrains  
View our YouTube Channel

Our Latest DVD from the Archives of Mr. John Tennent

**WOODLAND SCENICS**  
Ready Cut Card Kits

**METCALFE**  
Ready Cut Card Kits

**SLATER'S**  
READY CUT CARDS

**PATKIN**  
READY CUT CARDS

**Bachmann - Hornby - Graham Farish**  
All new Bachmann, Hornby and Graham Farish locomotives and rolling stock are available at 15% discount off the RRP.

**HORNBY**  
**GRAHAM FARISH**  
**by BACHMANN**

**With so much more to the shop than what you read, we are always worth a visit!**

**Second Hand Bought**  
A reminder that we buy second hand items at competitive prices so please call in for a free, no obligation quote for your railway items. It is difficult to publish lists of our second hand stock as it consistently changes so please check the website or call the shop for the latest details. We also sell items on commission (rate 20%) where you set the price of your items and we display, advertise and insure your items held for commission sale. The items can be reclaimed at any time without charge.

**Large Scale**  
We exclusively stock Lionel Fastrack, O and 027 Gauge and GarGraves track systems, alongside the ranges of Peco SM32, LGB, Atlas track. We also have a range of rolling stock to suit so why not call the shop for details of what is currently in stock.

**Our DVD Range**  
We are proud to offer a range of DVD's that have been partly compiled from the personal cine film and still photographic collection of John Tennent. The titles in the series are: 'Bygone Days', 'Welsh Byways', 'The Last of the Industrial Narrow Gauge Railways', 'Buses & Trams', 'Miniature Railways of Yesteryear' and 'The Welshpool & Llanfair Light Railway'.

**Books**  
The book section has now moved to be online only to make way for the Woodland Scenics to be displayed in the shop.

**Our Repair Service**  
We offer a repair service for most makes and models in N, 00 and O Gauge, Vintage Lionel O Gauge and all MTH trains a speciality. We also offer other services such as DCC decoder fitting, kit building and painting, all at competitive prices.

Visit us online at [www.tennentstrains.co.uk](http://www.tennentstrains.co.uk) to check on the current availability of our new and second hand stock

Shop1, 130 Hagley Road, Hayley Green, Halesowen, West Midlands, B63 1DY.  
Opening Times: Mon-Sat, 11am-5pm  
0121 - 550 - 1266

**THE HOBBY SHOP**  
Est. 1988

www.hobb-e-mail.com 01795 531666 info@hobb-e-mail.com

**85 Preston Street, Faversham, Kent ME13 8NU**

Exclusive OO-9 Bredgar & Wormshill  
Light Railway wagon  
No.6 from Peco  
In stock NOW

**BACHMANN**  
**BRANCH**

**PECO**

**HORNBY**

**PLASTIC**  
**RATIO**  
MODELS

**OXFORD**  
RAIL

**BUSCH**

**dapol**  
Model Railway Company

**HELJAN**

**sundeala.co.uk**

**WILLS-KITS**

**herpa**

and much more...

Opening hours: Tuesday to Saturday 9.30 am to 5.00 pm

**TRY OUR SAME-DAY DESPATCH MAIL ORDER SERVICE.**  
**ALL MAJOR CREDIT/DEBIT CARDS ACCEPTED**



# OSBORNS MODELS

**READY-TO-RUN N GAUGE**

**Ransomes 45T Crane**

TO LIFT 45 TONS

SCALE CARRIAGE  
N GAUGE  
**OSBORNS**  
BIDEFORD  
& WAGON WORKS

**WWW.OSBORNSMODELS.COM**

7 Honestone St, Bideford, N. Devon, EX39 2DL

**01237 423453**

# VISIT FOX

EVERONE'S FAVOURITE DESTINATION

LONDON  
NIGHT FERRY  
PARIS • BRUSSELS

PEMBROKE COAST EXPRESS

ONE AND ALL

TORBAY EXPRESS

## CORNISH RIVIERA EXPRESS

**FOX TRANSFERS LTD • TEL +44 (0)116 319 4950 (DEPT RM)**  
Dept RM, Unit 5, Priory Business Park, Wistow Road,  
Kibworth, Leicestershire LE8 0RX, ENGLAND callers by appointment  
email: mail@fox-transfers.co.uk web: www.fox-transfers.co.uk

## BRESSINGHAM

WWW.BRESSINGHAM.CO.UK

### The East Anglia Garden Rail Show

Layouts / Stalls / Museum / Steam Railways

**Sat 10th August**  
**10:30-17:00**  
**Entry £6.50**

Bressingham Steam Museum  
Diss, Norfolk, IP22 2AA

[www.bressingham.co.uk](http://www.bressingham.co.uk)

Twitter Instagram Facebook

## HOWES

MODELS LTD

**75+!**  
1939-2019

UNIT 16B, STATION FIELD INDUSTRIAL ESTATE,  
ROWLES WAY, KIDLINGTON, OXFORD OX5 1JD  
**01865 848000**, Open 09:00-17:00 Mon-Fri; close at 16:00 Sat

### HOWES THIS FOR SOUND! AND NOW THE BEST FOR LESS PRICE ONLY £109.99!

Apologies for the price rise which is due to the exchange rate with the Euro. Hundreds of hours have been invested by our experts to ensure you receive what we believe to be the best UK sounds available. We can supply ESU Loksound decoders with our sound files and can also re-blow your ESU LOKSOUND decoders if you are unhappy with the sounds on them – our 're-blow' service is only £12.00 plus return postage charge.

**These are the latest LOK SOUND 5 decoders!**  
**BR diesels, electrics, multiple units & steam – huge choice – just visit us on-line to see our full selection!**

Sound decoder with speaker - **£109.99** for ESU/Howes V5.0 plus £6.50 delivery (UK).  
**Please remember that we offer a decoder fitting service, please phone for more details.**

All our sounds can be offered for 'O' gauge models but may require a different decoder, depending on current draw - please phone for details.  
ESU/Howes XL decoders for all Heljan 'O' diesels; are available at £219.99 inc speaker.

**www.howesmodels.co.uk**

## DJH

MADE IN THE UK  
GROUP LIMITED

Tel Sales Office  
Mon-Thur 09:00-17:00 Hrs  
Friday 09:00-14:30 Hrs

**KITS: O/OO/HO & FACTORY BUILT MODELS: O**

**VISIT OUR WEBSITE FOR DETAILS OF THIS MONTHS SPECIAL OFFERS**

Photo courtesy: Jony Wright, photo shown: K70 LMS/BR Duchess '00' scale

DJH Group Ltd, Project House, Villa Real, Consett, Co. Durham. DH8 6BP. Tel: 01207 500050 Web: [www.djhmodelloco.co.uk](http://www.djhmodelloco.co.uk) Email: [sales@djhmodelloco.co.uk](mailto:sales@djhmodelloco.co.uk)



**BEXHILL MODEL RAILWAY CLUB**

[www.bexhillmrc.org.uk](http://www.bexhillmrc.org.uk)

*50th Anniversary Year*

## ANNUAL MODEL RAILWAY EXHIBITION

**SAT 10th AUG 2019 • 10am to 5pm**

- Layouts
- Sales Stands
- Refreshments

| ADULTS | CHILDREN | FAMILY |
|--------|----------|--------|
| £5     | £2       | £12    |

**FREE PARKING ON SITE**

»—————>

**St Richards Catholic College,  
Ashdown Road, TN40 1SE  
BEXHILL-ON-SEA**

sponsored by HastingsDIRECT

## ALL COMPONENTS LTD

**Upper Owlbury House, Montgomery, Powys SY15 6SZ**  
**Tel: 01588 638678** [www.allcomponentsltd.co.uk](http://www.allcomponentsltd.co.uk)  
 Looking forward to meeting you at all the shows!  
 P&P £4 (UK mainland only)

### CATALOGUE NOW AVAILABLE £4.95

**P1B4 POINTS INDICATOR BOARD**  
 Shows the direction of up to 4 points. Operated by solenoid point motors (Peco, Hornby etc.) with 4 included sets of red and green LEDs and remembers the direction when you next power up the layout. Easy to fit with clear instructions. **£22.95** For up to 4 points

**ALWAYS KNOW THE WAY YOUR POINTS ARE SET – EVEN WHEN YOU FIRST SWITCH ON**

**VERY FINE WIRE 2m for £3.95**  
 3amp, flexible multistrand wire. EW53/0.53mm dia. in rd, bk, bl, gn, yl, bn, grey. HS5 heat shrink sleeving for above £1.30

**LU10 VARIABLE LIGHTING UNIT** **£84.95**  
 The OnTrack Controls LU10 powers up to 54 grain of wheat bulbs or 150 LEDs for lighting-up your layout, and also lets you vary the brightness of the bulbs. It has 2 x 12v DC outlets at 5amps on each winding. It is in a strong metal box, with moulded plug and lead.

**ROLLING STOCK DETECTOR**  
 Operating on supplied light-dependent resistors, this easy-to-fit board shows where up to 4 locos are located. **RSD1 £23.95**

**Capacitor Discharge Units**  
 One CDU protects all your points from blowing. A must for any layout! **CDU £10.95**  
 If you are changing more than 2 points on 1 switch use **HDCDU £15.95**

**CASED TRANSFORMERS**

|                        |        |
|------------------------|--------|
| 24v AC (4 points)..... | £39.95 |
| 12v at 1amp.....       | £39.95 |

**SWANN MORTON**

|                         |        |
|-------------------------|--------|
| Pk of 5 Blades.....     | £1.25  |
| Pk of 100 Blades.....   | £19.95 |
| Retractaway Handle..... | £7.55  |

**MINIATURE TOGGLE SWITCHES**

|                                         |       |       |
|-----------------------------------------|-------|-------|
|                                         | ea 1+ | 20+   |
| SMT1 SPST on-off.....                   | £1.50 | £1.40 |
| SMT2 SPDT c/o on-on.....                | £1.60 | £1.50 |
| SMT3 SPDT c/o on-off-on.....            | £1.70 | £1.60 |
| SMT4 DPDT on-on.....                    | £1.90 | £1.80 |
| SMT5 DPDT c/o c/off on-off-on.....      | £2.00 | £1.90 |
| SMT6 SPDT for points (on)-off-(on)..... | £2.00 | £1.90 |

Lever covers 20p CLC/bk, bl, gn, yel, grey, rd, wh

**HAND-HELD CONTROLLERS WITH FEEDBACK**

|                                         |        |
|-----------------------------------------|--------|
| HH5/N 1amp at 9v for N Gauge.....       | £49.95 |
| HH5/OO 1amp at 12v for OO Gauge.....    | £49.95 |
| HH5/O 3amp at 12v for O Gauge.....      | £49.95 |
| Power units for above, if required..... | £49.95 |

**BULBS**

|                      |                         |
|----------------------|-------------------------|
| Std 3mm or 5mm.....  | 30p ea or 20 for £5.50  |
| 12v, 3mm or 5mm..... | 50p ea or 20 for £9.00  |
| Grain of Wheat.....  | 45p ea or 20 for £8.00  |
| Grain of Rice.....   | 90p ea or 20 for £17.00 |

**LAYOUT WIRE**

|                         |               |
|-------------------------|---------------|
| EW06 single strand..... | 10 mtrs £1.95 |
| EW07 1.4amp.....        | 10 mtrs £1.95 |
| EW07 Bi-colour.....     | 10 mtrs £2.50 |
| EW16 3.0amp.....        | 10 mtrs £3.50 |
| EW24 6.0amp.....        | 10 mtrs £4.50 |

**XURON CUTTERS**

|                            |        |
|----------------------------|--------|
| Sprue Cutter.....          | £13.95 |
| Vertical Track Cutter..... | £18.95 |
| Track Cutter.....          | £18.95 |
| Photo Etched Scissors..... | £18.95 |

**READY-CUT CORK**

|                          |        |
|--------------------------|--------|
| N Gauge 1/16 15ft.....   | £2.95  |
| OO Gauge 1/16 15ft.....  | £3.95  |
| OO Gauge 1/8 15ft.....   | £5.95  |
| O Gauge 1/8 15ft.....    | £9.95  |
| 1/16 Roll 3ft x 2ft..... | £6.95  |
| 1/8 Roll 3ft x 2ft.....  | £13.95 |

## SouthWest digital

The digital sound experts

Distributor for the UK since 2006  
Trade Welcome

See website for supported trade

- Our 5th generation of LokSound has been totally re-developed
- High performance, 32-Bit microcontroller
- 10 simultaneous sound channels
- HiFi-Quality thanks to 16 Bit Resolution and 31250 Hz sample frequency
- A 128 Mbit flash memory (Approx 8.5 min at 16 Bit, 31250 Hz)
- Advanced B-EMF load control that runs with up to 50 kHz base frequency and is optimized for low-noise operation. This is important for coreless motors such as Faulhaber or Maxon
- Every LokSound 5 decoder is a true Quad-Mode Decoder that talks DCC with RailComPlus

- Can also be used with Analog DC controllers
- Based on the command station, up to 32 function buttons are supported that can be mapped to any output due to our superior ESU function mapping
- Three brake functions and a two-stage load simulation will allow even more realism

• Ask any one of our 65 supported dealers for 'LokSound' the industry's favorite decoder

**LOK SOUND**  
EST. 1999

**[www.southwestdigital.co.uk](http://www.southwestdigital.co.uk)**

**T: 01934 515382**  
**E: info@swd4esu.co.uk**

Test drive the latest loco sounds with stunning clarity at SWD  
Call now for more info

## PETER'S SPARES

### MODEL RAILWAYS LIMITED MAIL ORDER & REPAIR SPECIALISTS

- ♦ New & Used Hornby Spare Parts
- ♦ Our own spare parts made in China
- ♦ Locomotives and Rolling Stock
- ♦ Plastic Kits, Paints, Tools & Glues
- ♦ Digital Decoders & Controllers
- ♦ Locomotive Detailing parts
- ♦ Mega Steam Smoke Fluid
- ♦ Tracks & Train Sets
- ♦ We have it all at Peters Spares

## NEED HORNBY SPARES?

**Talk to Peter's Spares – over 2000 different Hornby Spares in stock**

East Kent Models parts are being listed on our website

OVER 10,000  
DIFFERENT ITEMS  
IN STOCK

## DON'T FORGET WE SELL:

**Locomotives, Rolling Stock, Scenics and much more!**

### NEW & USED MODEL RAILWAYS

Specialists in Model Trains since 1994 – Servicing, Repairs & DCC Decoder Fitting  
2J Brighthouse Business Village, Riverside Park, Middlesbrough, Cleveland TS2 1RT

**Tel: 01642 909794**

Open Mon-Sat 10am-5pm

**Web: [www.petersspares.com](http://www.petersspares.com)**

Email: [sales@petersspares.com](mailto:sales@petersspares.com)



**Phone 01752 665011**

# **Brewsters Ltd**

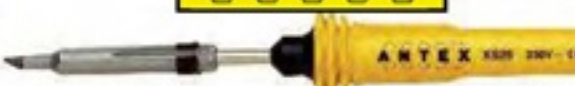
**FOR ALL YOUR SOLDERING NEEDS FROM ONE WEBSITE**

**69 NEW GEORGE ST  
PLYMOUTH PL1 1RJ**



**Weller**

For those larger jobs and for 0 gauge and larger.  
Irons from 40W to 200W both standard and thermal controlled  
Plus bits for all types



**ANTEX**

Irons from 18W to 100W  
Plus a variety of spare bits  
Both irons suitable for N & 00 gauge work.

**Books, Stands Solders Tools and other accessories**



**For ALL Your  
SOLDERING Needs**

**www.soldering-shop.co.uk**

**Fully tested and guaranteed for 5 years**

# **MORLEY CONTROLLERS**

**Experience the ultimate in controller technology!**

**60 Years**  
**BTVR**  
ESTABLISHED 1959

**VANGUARD ZERO ONE**

- 2 x 5mtr hand held controllers
- 1 x 12V DC accessory
- 0 gauge
- Twin track

**£133**  
PLUS £6.90 P&P



**VESTA ZERO TWO**

- 2 x 2.5mtr hand held controllers
- 1 x 12V DC accessory
- N gauge
- Twin track

**£75**  
PLUS £4.80 P&P



**VECTOR ZERO THREE**

- 2 x 2.5mtr hand held controllers
- 1 x 12V DC accessory
- 00/HO/N/009 gauge (1 amp per track)
- Twin track

**£75**  
PLUS £4.80 P&P



**VORTAK ZERO TEN**

- 2 x 5mtr hand held controllers
- 00/HO/N/009 gauge (1 amp per track)
- 2 x 12V DC accessories
- Four track

**£135**  
PLUS £6.90 P&P



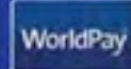
Each controller has state-of-the-art design, offering very best performance capability. Slow and fast speeds are superb. Internal separate transformers for each track from 220/240 AC mains.

**\* CDU FOR POINT MOTORS ON-BOARD \***

**www.morleycontrollers.com**  
(Secure order site)

Email: [jet@morleycontrollers.com](mailto:jet@morleycontrollers.com)

Tel: 01704 533 116 (Overseas enquiries +44 1704 533 116)

PO Box 501, Southport, PR9 9ZL

Also at PO Box 1230, Wangara BC, Perth, Western Australia 6947  
Tel: 0421 359 487 <http://aus.morleycontrollers.com>

Made in the UK



**INVICTA MODEL RAIL**

[www.invictamodelrail.com](http://www.invictamodelrail.com)

## Exclusive to Invicta – 009 Bredgar & Wormshill Coach



GR-500IMR 009 Bredgar & Wormshill Coach £26.50    Based on a coach on the Bredgar & Wormshill Light Railway

Made Exclusively by Peco for Invicta Model Rail Ltd.

To order call 0208 302 7774 or online [www.invictamodelrail.com](http://www.invictamodelrail.com) or visit us in-store

Invicta Model Rail Ltd, 130 Station Road, Sidcup, Kent DA15 7AB    Telephone: 0208 302 7774



NEW VENUE for 2019

## NORTHAMPTON & DISTRICT MODEL RAILWAY CLUB



### MODEL RAILWAY EXHIBITION

SATURDAY 24th AUGUST 2019

from 10.00am to 4.00pm

Road Village Hall

Bailey Brooks Lane ROADE

Northamptonshire NN7 2LS

Displaying a varied range of Model Railway layouts  
built in several gauges and scenarios

Trade support – Demonstrations – Refreshments

Societies – Tombola

Adult £4 Child £2 Family Ticket (2+2) £8

[www.ndmrc.org](http://www.ndmrc.org)

## WALTONSMODELS.CO.UK

Visit our Store – Shop Online –

Phone 0161-928-5940

Open Monday – Friday 9.30-5.00

Saturdays 9.30-1.00

Closed Wednesdays



Our shop established in 1961 has recently celebrated 50 years of trading. Most major ranges stocked at competitive prices. See website for current stock and prices.

We are located 8 miles south west of the centre of Manchester, and just a couple of minutes from the Metro station.

By car we are about 3 miles from M56 Junction 8, 7 miles from M6 junction 19 and 4 miles from M60 junction 7.

Parking: There is pay and display parking outside the shop 70p for an hour and £1.50 for 2 hours. Alternatively there is a multi storey car park 100 yards away. Further directions and links to Google maps and Multimap are on our website.



30 Stamford Street, Altrincham, Cheshire WA14 1EY

## YouChoos DCC Sound



No more restrictions on speaker heights!

Why compromise when you can have a speaker to exactly fit your model?

Introducing YouChoos' new 3D ranges of **SugarCubes**, **SugarStrands** and **IceCubes**, available in millimetre height steps - you won't get this much flexibility anywhere else!

High-quality resin-based 3D prints, produced in-house, matched with newly-sourced planar speaker components ensuring you get the quality you expect, in single and double-speaker formats.

**SugarCube** range, based on 15x11mm 8ohm 1W components (the classic):  
15x11mm (1W 8ohm): Heights 5-12mm; single  
24x15mm (2W 4ohm): Heights 6-11mm; dual  
35x15 (2W 4ohm): Heights 6-13mm; dual

**SugarStrand** range, based on 25x9mm 8ohm 1.5W components (longer and louder than the SugarCube):  
25x9mm (1.5W 8ohm): Heights 6-11mm; single  
25x20mm (3W 4ohm): Heights 6-13mm; dual

**IceCube** range, based on 18x13mm 8ohm 1W components (biggest and best):  
18x13mm (1W 8ohm): Heights 6-13mm; single  
25x18mm (1W 8ohm): Heights 6-13mm; single  
28x18mm (2W 4ohm): Heights 6-13mm; dual  
40x18mm (2W 4ohm): Heights 6-13mm; dual  
55x18mm (2W 4ohm): Heights 6-13mm; dual

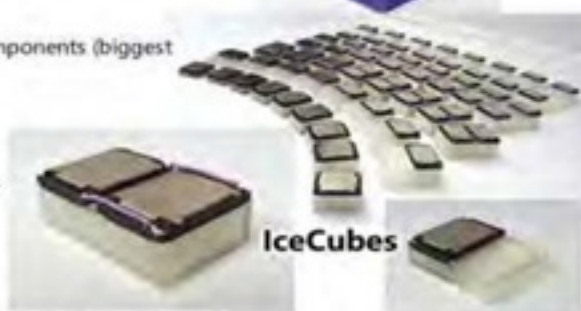
**Our favourites:**  
SugarCube 15x11 1W at 7mm and 9mm  
SugarStrand 25x9 1.5W at 7mm  
IceCube 25x18 1W at 7mm and 9mm  
IceCube 28x18 2W at 7mm and 9mm (totally awesome!)  
IceCube 40x18 2W at 7mm and 9mm

Overwhelmed with choices? First work out the available footprint, then pick the maximum height to fit your space. Still not sure? Contact us for advice!

\* Zimo MX648, MX649, MX658, MX659 and MX660 require minimum 8ohm  
\* Zimo MX645 and MX644 will work with any of the above speakers  
\* ESU LokSound V4/V5 will work with any of the above too  
\* Hornby TTS require 8ohm



SugarStrands



IceCubes

BE THERE WITH  
**immersive  
Drive**

01380 530530

[www.YouChoos.co.uk](http://www.YouChoos.co.uk)

[info@youchoos.co.uk](mailto:info@youchoos.co.uk)

## RON LINES



- ★ Hornby
- ★ Bachmann
- ★ Graham Farish
- ★ Peco
- ★ Expo Tools
- ★ Electrical Components

Large stocks of pre-owned railways, a selection of which is on our website.

We also purchase your unwanted railways, whether it's a single item or collection, anything considered.



Whether you're a novice or an expert

We are always happy to help!

So why not visit us today or give us a ring!

Tel: 02380 772 681

[www.ronlines.com](http://www.ronlines.com)

342 Shirley Road, Southampton, SO15 3HJ

Open Mon-Fri 10am-5pm & Sat 9am-4pm



**Illustrated Catalogues Inc. Post**  
 OO, 009 (1/76th) .....£6 in colour  
 N (1/148th) .....£5 in colour  
 O (1/43rd) .....£4 in colour

**MAIL ORDER BY RETURN P+P £4, over £40 Post Free**  
 Unpainted metal kits-glue with epoxy glue (£7)

**LANGLEY MODELS**  
 166 Three Bridges Road, Crawley, Sussex RH10 1LE  
 Tel: 01293 516329  
 www.langleymodels.co.uk

**'OO' SCALE**  
 Horse-drawn Plough & Figure £8.40  
 Telephone Box £9.60  
 Band Stand £17.00  
 Seat Band Figures £10

Loads of other kits in our illustrated colour catalogue or  
 Website: www.langleymodels.co.uk

|                                              |        |                                                 |        |
|----------------------------------------------|--------|-------------------------------------------------|--------|
| Cricket Game - 15 figures, stumps, etc.      | £9.90  | Painted Funeral Scene                           | £12.25 |
| NEW Cricket Sight Screen                     | £7.55  | Painted 6 Assorted Dogs                         | £8.40  |
| NEW Cricket Portable Scoreboard              | £4.40  | Painted Newspaper vendor and paper box          | £6.30  |
| NEW 6 seated Cricketers (waiting to play)    | £5.40  | Painted 2 x Freisian Cows (patched brown/white) | £7.35  |
| Morris Dancers & Musicians                   | £7.90  | Painted 2 x Highland Cattle                     | £7.85  |
| Loco Spotters & school boys                  | £7.80  | Painted White Wedding scene (8 figs)            | £21.50 |
| Victorian Wedding Party                      | £8.15  | Painted 6 x Working Action figures set1         | £14.30 |
| Town Hall Dignitaries - 3 figures            | £3.60  | Painted 6 x working figures set 2               | £14.30 |
| Lych Gate (suitable for churchyard)          | £7.55  | Painted Wharfside Figures                       | £14.30 |
| Church Notice Board & Crucifix               | £5.70  | Painted 2 Wooden Barrels Weathered              | £5.05  |
| War Memorial                                 | £6.25  | Painted Lifebelts and Stands                    | £9.05  |
| Rag & Bone Man & family                      | £8.65  | Painted Mangle, tin bath and bucket             | £6.50  |
| Street Characters                            | £8.00  | Painted Sundial, bird bath and flower pots/urns | £9.05  |
| Knife Grinder & Hot Chestnut Man             | £8.65  | Painted Water Butt and 4 Flower Tubs            | £9.65  |
| 4 Gardeners with tools, mower, etc.          | £6.25  | Painted Private Siding Gate                     | £11.20 |
| NEW 2 x Lawnmowers & Figs (ride on and push) | £5.20  | Painted Small 5 Bar Gate                        | £7.30  |
| Blacksmith's Forge Scene                     | £15.85 | Painted 10 Chimney Pots (terracotta)            | £4.40  |
| Illuminated Forge conversion (working kit)   | £4.80  | Painted 6 Kings and Queens Pots                 | £8.95  |
| 2 Street Traders with barrows                | £7.10  | Painted 12 Round Chimney Pots (weathered)       | £8.95  |
| 8 Soldiers & 1 Officer (1908-37)             | £7.50  | Painted Milkman & Milk Cakes                    | £10.45 |
| 8 Soldiers & 1 Mounted Officer               | £7.90  | Painted Loco Driver & Fireman                   | £5.65  |
| Gantry Hoist                                 | £5.50  | Painted single head Traffic Light x 4           | £7.30  |
| New Gibbet Crane (wall or floor mounted)     | £4.70  | Painted twin head Traffic Light x 4             | £7.40  |
| Air Compressor and Electricity Generator     | £6.70  | Painted Police Telephone Box                    | £5.60  |
| 4 Pallets & Hydraulic Pallet Truck           | £4.45  | Painted AA Box                                  | £5.60  |
| Beehives and Bee-Keepers                     | £4.70  | Painted Ballast Boxes (two wooden boxes)        | £5.80  |
| 6 Bulk Building Bags                         | £3.95  | Painted Trackside Relay Boxes                   | £5.70  |
| Band Stand                                   | £9.95  | Painted War Memorial                            | £6.45  |
| Air Conditioning Cabinets (2 wall, 4 floor)  | £3.65  | Painted 2 Plain Horses (unharnessed)            | £6.35  |
| 6 Assorted Rock Climbers                     | £5.00  | Painted Loco Crew                               | £4.25  |
| 4 Assorted Highland Cattle                   | £4.60  | Painted Swans & Ducks x 4 each                  | £6.85  |
| 6 Assorted Firemen in action poses           | £4.80  | Painted Washing Line & Figure                   | £7.50  |
| 6 Travellers/Fisherman Figures               | £4.60  | Painted Wild Animals                            | £7.40  |
| 3 Assorted Donkeys (unharnessed)             | £3.85  | Painted Assorted Seagulls x 8                   | £6.95  |
| 3 Beach Donkeys and Children                 | £4.60  | Painted Dogs 6 off                              | £6.85  |
| Beach Set (figures and equipment)            | £5.80  | Painted 6 x Pigs                                | £6.45  |
| 8 Guards Marching                            | £5.20  | Painted Sheep 8 off                             | £6.95  |
| Guards Colour Party (flags) (5 figs)         | £3.95  | Painted Chickens 10 off                         | £6.95  |
| Guards Marching Band                         | £5.60  | Painted Funeral Scene                           | £9.85  |
| Cub/Scout Camp Set                           | £5.80  | Painted Wedding Scene                           | £19.25 |
| 6 Assorted Cats                              | £3.20  | Painted Cricket Game                            | £28.40 |
| 2 x Pub Table/Bench (real wood)              | £3.95  | Painted Church Notice Board & Crucifix          | £7.90  |
| Church Stained Glass Windows/Spire           | £6.55  | Painted Lrg Wooden (Oak) Barrels x 4            | £6.45  |
| 6 Seated Passengers / Figs. Set 1            | £4.35  | Painted Flower Tubs and Water Butt              | £8.00  |
| 6 Seated Passengers / Figs. Set 2            | £4.35  | Painted Terracotta Chimneys (12x (3 types))     | £5.30  |
| 4 Buoys (2 float 2 wharf) 3 bit fenders      | £3.20  | Painted Welding Figures & Equipment             | £9.25  |
| 6 Station Platform Staff                     | £4.35  | Painted Relaxed Loco Crew x 2                   | £4.25  |
| Garage Accessories 4 Pillar Ramp             | £6.35  | Painted 9 x Ass Standing Figs Platform etc.     | £19.05 |

**'N' SCALE**  
 Church Lych Gate £4.90  
 6 Seated Passengers £4.35  
 two sets

**A C Models**  
 9 High Street, Eastleigh, Hants SO50 5LB  
 Tel: 02380 610100  
 Opening Hours: 9.00 - 5.00  
 Both Shops Closed Wednesdays and Sundays  
 email: info@acmodelseastleigh.co.uk  
 www.acmodelseastleigh.co.uk

**Your one stop shop for the model enthusiast, selling brand new and near-new model trains, slotcars, sets, diecast toys and much more.**

**NEW RADIO CONTROL DEPT NOW OPEN**

**NOW OPEN - our Spares Dept and Games Workshop at No.7 High Street.**

Visit us at:  
 www.acmodelssparses.co.uk

**Stockists of**  
**PECO**  
**BRACHMANN**  
**HORNBY**  
**Spectra**  
**AIRFIX**  
**SCALEXTRIC**  
**Games Workshop**  
 and much more!

**VISA** **PayPal** **MasterCard**

# THE FULL EXTENT...

## ... OF OUR OO/HO WALLING AND FENCING KITS

**WILLS KITS**

**SS36 Dressed Stone Wall**

**SS41 Feather Edge Fencing**

**SS44 Larch-Lap Fencing**

**SS43 Concrete Fencing**

**SS45 Picket & Rustic Fencing**

**Also available...**

**OO/HO GAUGE**

**SS92 Garden Buildings and Accessories**

See the full range of Wills products at Ratio House,  
 Mardle Way, Buckfastleigh, Devon TQ11 0NR t: 01364 642764  
 OPEN: Mon-Thur: 9-4.30, Fri: 9-1.30 CLOSED: Sat/Sun/Bank Hols.

Full details of all our kits can be found in the **PECO CATALOGUE**  
 Pritchard Patent Product Co. Ltd. • Beer • Devon • EX12 3NA  
 01297 626204 • www.peco-uk.com



# THE JUNCTION BOX

## WANTED – WANTED

### MODEL RAILWAYS



Top prices paid for  
N and OO gauge collections  
or individual items, such as

- Airfix • Bachmann • Graham Farish
  - Hornby • Lima • Mainline • Peco
- or any other makes.

*Will travel to collect.*

Please forward your list of items by E-mail to: [ian@ihunter10.plus.com](mailto:ian@ihunter10.plus.com)  
send to: The Junction Box, 15 Shepherd Terrace, Haltwhistle, Northumberland NE49 9LS

Or contact Ian **07763 806306 / 01434 320328**

Please visit our online shop for all your model railway requirements at:

**[www.thejunctionbox.net](http://www.thejunctionbox.net)**



# HOPPERS

**Coal - ore - gravel? Deliver it all with these great models!**

**BR 21 ton Coal Hopper** (diagram 1/146)  
**PC77**



**OO**

**BR 21 ton Rebodied Hopper Wagon**  
(Vacuum Braked)  
**PC78**



**OO**

**LNER 20 ton Hopper Wagon** (diagram 100)  
**PC80**



**OO**

**BR 21 ton Coal Hopper** (diagram 1/146)  
**PS104**



**O**

**LNER 20 ton Hopper Wagon** (diagram 100)  
**PS108**



**O**

**PLUS!  
COMING  
SOON**



**LNER 13 ton Steel  
Hopper Wagon**  
**PC90 (OO) PS116 (O)**  
(diagram 1/142, 1/144)

Images  
show  
3D  
printed  
samples

Parkside Models are available from all good model shops.

In case of difficulty, models available by post. Please add £2 per order towards P&P.

Pritchard Patent Product Co. Ltd, Beer, Devon EX12 3NA, England. Tel.01297 626204 [www.peco-uk.com](http://www.peco-uk.com)

Note: Parkside kits require assembly and painting

**DETAILS OF ALL PARKSIDE MODELS  
ARE IN THE LATEST PECO CATALOGUE**





FOR MODELLERS WHO  
REQUIRE QUALITY,  
ACCURACY, PERFORMANCE  
& DURABILITY

*The new name in brass  
O gauge RTR locomotives, built to  
the highest standards*

Fully finished by FM Models in South  
Korea, designed and developed in  
conjunction with L H Loveless



[www.55H.co.uk](http://www.55H.co.uk)  
[www.loveless.co.uk](http://www.loveless.co.uk)

55H Unit 3 Fulford Business Centre,  
35 Hospital Fields Road, York YO10 4DZ  
John: 01132 761759 / [John@55h.co.uk](mailto:John@55h.co.uk)  
Laurie: 01423 712446 / [Lawrie@loveless.co.uk](mailto:Lawrie@loveless.co.uk)

55H is a TRADING NAME of FINE SCALE BRASS

9F LAST FEW IN STOCK NOW!



WHY WAIT AND PAY MORE?

4 versions, choice of numbers BR1G, BR1C, BR1D and BR1F tenders, single/double chimney and Tyne Dock, late/early crests.

£2400.00 DC  
£2800.00 DCC & NEW Loksound 5  
£3025.00 DCC, sound & sync. smoke



NOW IN PRODUCTION!

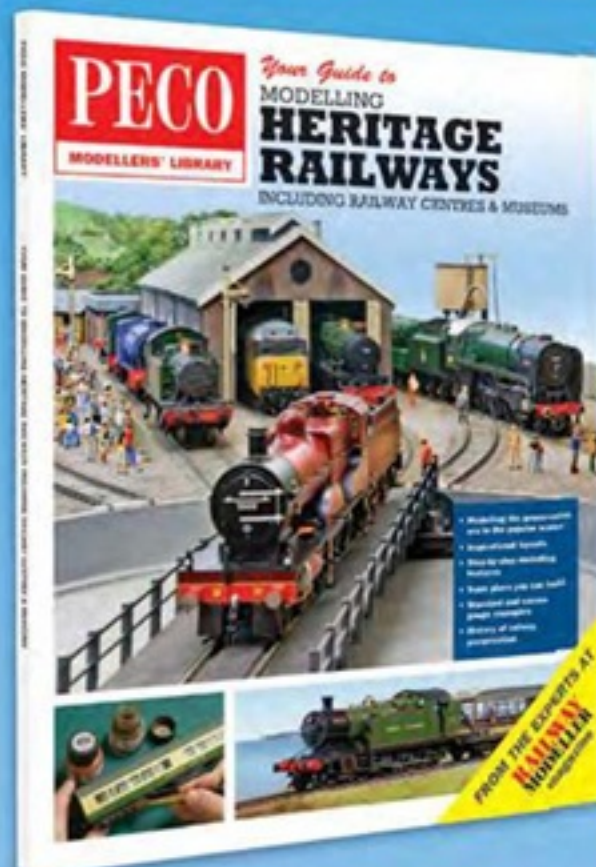


© Cliff Williams

In production for delivery  
mid-2019 7MT "Britannia" class  
and 8P Duke of Gloucester,  
choice of names, tenders, smoke  
deflectors, front steps and crests.  
Fully finished by the factory and  
ready to run on delivery.

Britannia £2750.00 DC  
Duke of Gloucester £2800.00 DC  
Add £400.00 for DCC & NEW Loksound 5  
Add £625.00 for DCC, sound & sync smoke

# CREATE A HERITAGE LAYOUT!



If you visit a preserved  
railway this summer,  
think about making it  
the subject of your  
next modelling layout!



FROM THE EXPERTS AT  
**NEW!** RAILWAY  
MODELLER

Discover the exciting  
possibilities of modelling  
heritage railways in this  
latest addition to the  
Peco Modellers' Library.

- Modelling the preservation era in the popular scales
- Inspirational layouts
- Step-by-step modelling features
- Track plans you can build
- Covering Standard and narrow gauge examples
- History of railway preservation

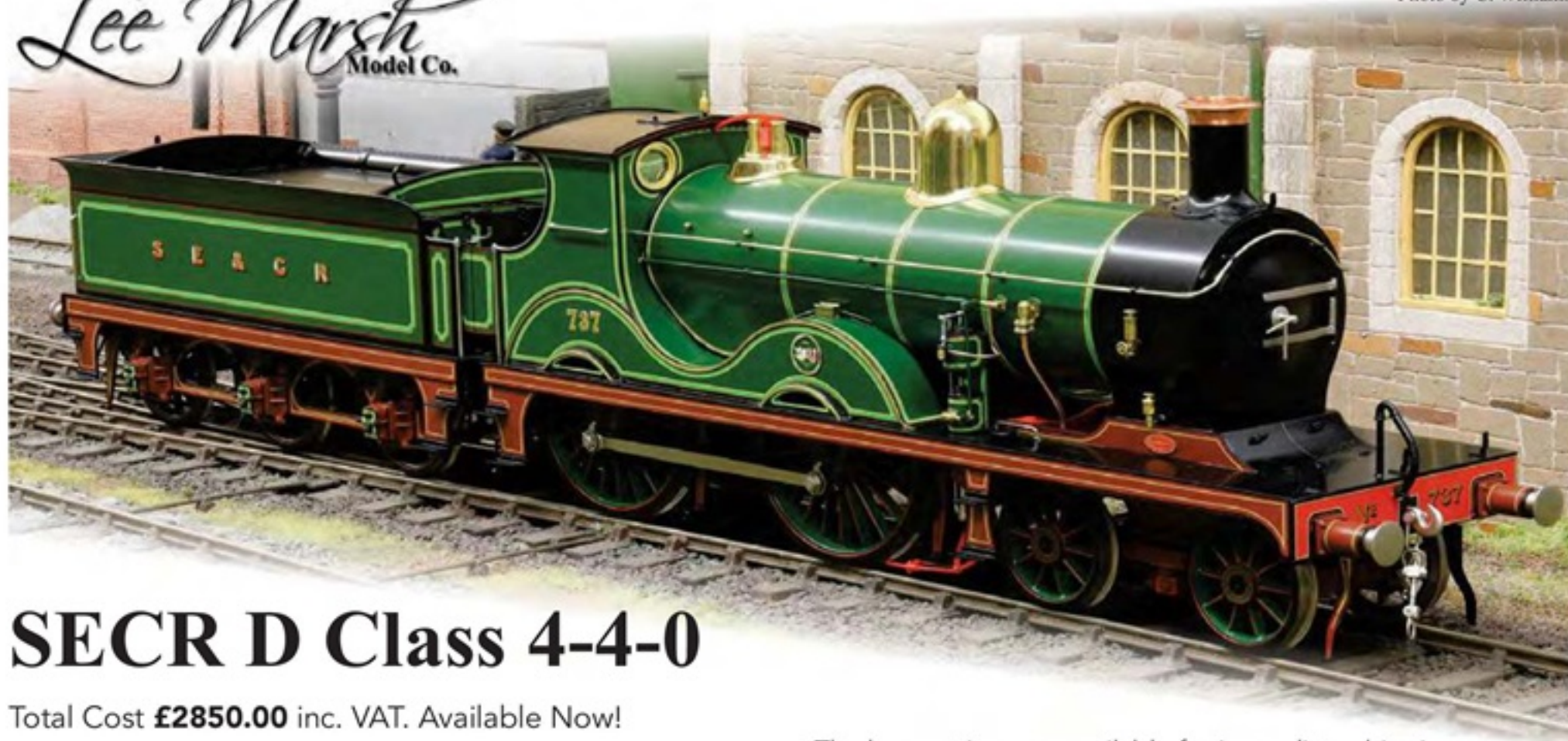
**Out Now!** ONLY  
**£6.99**

Available from: selected W.H. Smith outlets plus your local Peco Stockist. Also available by post direct\* (from the address below), or via our website [www.pecopublications.co.uk](http://www.pecopublications.co.uk) \*Please add £2 P&P.  
PECO PUBLICATIONS & PUBLICITY LTD, BEER, DEVON EX12 3NA, ENGLAND Tel: 01297 626204 Fax: 01297 202229



Lee Marsh  
Model Co.

Photo by C. Williams



## SECR D Class 4-4-0

Total Cost **£2850.00** inc. VAT. Available Now!

This project portrays the SE&CR Wainwright D class 4-4-0 in 1901 condition only. Locomotive numbers available are 726-737 of the initial 1901 batch built by Sharp Stewart & Co and Ashford Works.

There are only 60 units created for this project & they are selling quickly, so please do not hesitate to order your model now! Made exclusively from brass, RTR & comes fully painted & lined.

The locomotives are available for immediate shipping. Models also currently available for purchase now are the **GWR Dean Goods, GWR Clerestory Coaches, BR 9F & LMS Patriots.**

Forthcoming projects in production include the **GWR Castle, GWR 850, & LNER A1/A3.** Please check our website for further details on these projects.

[www.leemashmodelco.com](http://www.leemashmodelco.com)

Unit 5, Lancaster House, 9 Moss Way, Hillend Industrial Park, Dalgety Bay, KY11 9JS

tel:01383 829706

# THE GWR STANDARD

## 552 GWR BRICK TYPE SIGNAL BOX

Based on the familiar GWR prototype built between 1896 and 1921 this OO Scale Ratio kit is anything but standard.

Superb detail can be seen in this kit's windows, bricks, tiles, steps and pipework. Use with **Signal Box Interior Kit 553** to add even more eye-catching realism.

Length: 90mm  
Width: 48mm



**Note:** Supplied in pre-coloured moulded plastic parts. Will require additional painting and/or weathering to achieve the realism seen in the photograph opposite.

A 'Shows You How' demonstration on how to build and paint this kit appeared in the March 2015 issue of *Railway Modeller*.



### PLEASE TRY YOUR MODEL SHOP FIRST

In case of difficulty, models are available direct by post from the address below.  
Add £2 per order towards P&P.

Pritchard Patent Product Co Ltd • Beer • Devon • EX12 3NA  
t: 01297 626204 • f: 01297 20229 • [www.peco-uk.com](http://www.peco-uk.com) • email: [info@peco.co](mailto:info@peco.co)



# The Beautiful People

**NEW  
MODELS**

These two new sets have been created using brand new tooling, giving a much higher level of detail. *Hand-painted and designed to look attractive just about anywhere on your layout.*

**5302  
Standing  
People**

(5 figures per set)

**5301  
Seated People**

**PECO**

**model scene**

**5053 Platform Accessories  
Modern Era**

**ALSO RELEASED RECENTLY**

- 5005 Telephone Box & Caller
- 5044 Post Boxes
- 5047 Yard Lamps
- 5050 Speed Restriction Signs
- 5048 Platform Accessories  
Steam Era
- 5049 Station Nameboards



Note: Photographs include items not included in individual kits

**OVER 90 MODELS IN THE RANGE  
DETAILS IN THE PECO CATALOGUE**

PRITCHARD PATENT PRODUCT CO. LTD, BEER, DEVON EX12 3NA, ENGLAND • 01297 626204 • [www.peco-uk.com](http://www.peco-uk.com)

**Vectis**

The World's Leading Toy Auction House

Live internet bidding at  
[www.vectis.co.uk](http://www.vectis.co.uk)  
[www.invaluable.com](http://www.invaluable.com)

**MODEL TRAIN SALE**

Friday

**19**  
JULY  
2019



at Thornaby Auction Rooms

**10.30**  
AM



Contact Michael Bond **01642 750616** [michael.bond@vectis.co.uk](mailto:michael.bond@vectis.co.uk)  
Mike Delaney **01993 709424** [mike.delaney@vectis.co.uk](mailto:mike.delaney@vectis.co.uk)



**Vectis Auctions** Fleck Way, Thornaby, Stockton on Tees TS17 9JZ  
**Oxford Office** Unit 5A, West End Industrial Estate, Witney, Oxon OX28 1UB



# DARSTAED RTR FINESCALE 'O' GAUGE MK1 COACHES

FEATURING BRASS SIDES, SUPER-DETAILED INTERIORS WITH LIGHTS & MANY MORE FEATURES

ONLY £189 EACH OR £750 FOR 4

## NOW IN STOCK

CK Comp  
SK Second  
BSK Brake 2nd  
TSO Tourist 2nd Open  
BCK Brake Compo  
FK 1st Class  
RMB Mini Buffet  
RU Restaurant Kitchen  
RFO Restaurant 1st Open  
RSO Restaurant 2nd Open

SEE WEBSITE FOR  
UP TO DATE STOCK

## AVAILABLE LIVERIES

Blood & custard  
Chocolate & cream  
Blue/grey  
Maroon  
Southern green  
Intercity  
West Highland **NEW!**

"I THINK THE CONSTRUCTION  
METHODOLOGY IS A TOUR  
DE FORCE."

ROBIN MCHUGH, GOG REVIEWER

## FOR PRE-ORDER

57' BGs (now only £169)  
Sleepers (maroon & blue/grey)  
TSOs & BCKs (in early & late  
West Highland livery)



# DARSTAED RTR FINESCALE 'O' GAUGE BR 57' MK1 SUBURBAN COACHES

DUE Q1-Q2 2019 - PRE-ORDER TODAY WITH NO UP-FRONT PAYMENTS



**PART-EX**  
AVAILABLE  
on all  
DARSTAED

## FEATURING

- ✓ Brass sides
- ✓ Super-detailed interiors with lights
- ✓ Professionally painted & lined
- ✓ Die-cast underframe
- ✓ Compensated all-new MK1 bogies
- ✓ NEW! Brass luggage racks

## PRE-ORDER IN MAROON (LINED & UNLINED), CRIMSON & BLUE

- ✓ Composite
- ✓ Composite (with lavatory)
- ✓ Brake Second
- ✓ Open Second
- ✓ Second
- ✓ Second (with lavatory)

For more info visit:  
[ellisclarktrains.com/pages/darstaed](http://ellisclarktrains.com/pages/darstaed)



ONLY £169 EACH OR £665 FOR 4

ONLY AVAILABLE THROUGH **ELLIS CLARK TRAINS**

Ellis Clark Trains, Unit 1 Toller Court, Shortbank Road, Skipton, North Yorkshire BD23 2HG

01756 701451 ♦ [ellisclarktrains.com](http://ellisclarktrains.com)

Exclusively for:







Ellis Clark Trains  
Unit 1 Toller Court  
Shortbank Road, Skipton  
North Yorkshire BD23 2HG

# WANTED

**TOP PRICES PAID FOR YOUR COLLECTIONS  
OF MODEL RAILWAYS, DIECAST & TINPLATE**  
For more info call Ellis on 07799 554491

- ◆ Call, email or post a list
- ◆ All makes, gauges and ages considered
- ◆ British, Continental and American
- ◆ All gauges of kit/scratch built and live steam
- ◆ Diecast & tinplate cars, lorries, planes, soldiers
- ◆ Payment in your preferred method
- ◆ Happy to travel nationwide and view or collect
- ◆ Friendly and professional service
- ◆ Large collections a speciality



**T** 01756 701451  
**M** 07799 554491  
**E** [ellis@ellisclarktrains.com](mailto:ellis@ellisclarktrains.com)  
**W** [ellisclarktrains.com](http://ellisclarktrains.com)

**VISIT OUR  
WEBSITE &  
0 Gauge/larger gauges  
SHOWROOM**



# The North East's Premier Model Railway Show



Presented by  
**BLYTH AND TYNE MODEL RAILWAY SOCIETY**

at John Spence Community High School, Preston Road, North Shields  
Tyne & Wear NE29 9PU

**SAT 27th & SUN 28th  
July 2019**



**Sat 10.00am - 5.00pm  
Sun 10.00am - 4.00pm**

Quality Model Railway Layouts - Gauges Z to 1  
(including 'Thomas & Friends')  
Trade Stands - Modeling  
Demonstrations, etc.

Blue Arrow number 11 bus  
goes past venue

Nearest Station   
Tynemouth



## Admission

**Adults £7.00 Children £3.00**

**Family (2+2) £16.00**

**[www.railexne.com](http://www.railexne.com)**

# TRACKSHACK

*For Service, Price & Delivery*

## CRIKEY!

- Fast, easy to use website!
- Clear live stock levels!
- Unbelievably fast dispatch!
- Legendary rapid low cost delivery!
- Free delivery on orders over £200!
- Award winning customer service!
- And now . . .



We constantly compare our prices  
to ensure that you get the  
best deal at [Track-shack.com](http://Track-shack.com)

**feefo** ★★★★★  
Service Rating 6042 reviews



Woodland  
Scenics



**GAUGEMASTER**.com



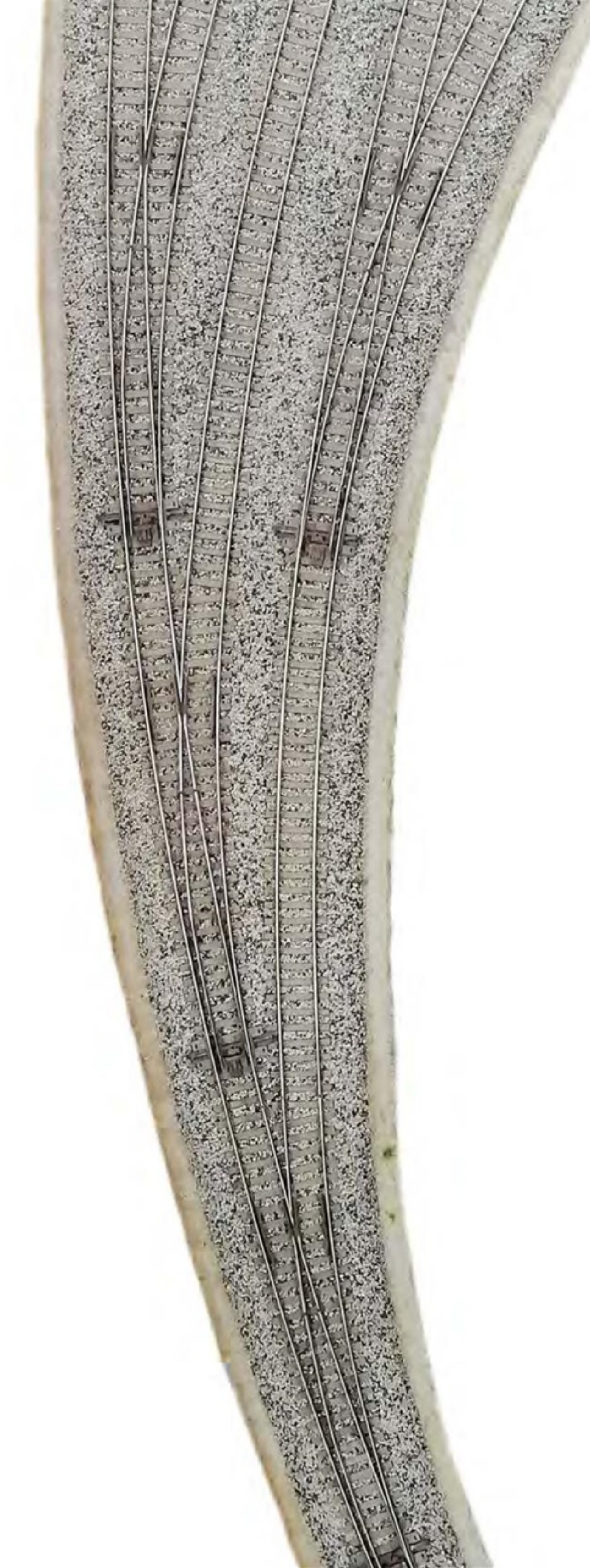
**[www.track-shack.com](http://www.track-shack.com)**

Tel: 0330 0885759 UK local rate call.

Or visit us in-store at:

Units 4-8 Olive Court, Ramsey Road, Peel, Isle of Man IM5 1RH





**PECO** *STREAMLINE*

*Prototypical and realistic, curved turnouts  
add grace and style to your layout...*



*...and save you space too!*

**00 GAUGE  
CURVED TURNOUTS**  
*Code 75 & Code 100*



Pritchard Patent Product Co. Ltd.  
Beer, Devon, EX12 3NA England

Tel: 01297 626204 Fax: 01297 20229 [www.peco-uk.com](http://www.peco-uk.com)



To: **PECO TECHNICAL ADVICE BUREAU**  
PECO PUBLICATIONS AND PUBLICITY LTD • BEER • SEATON  
DEVON • EX12 3NA • ENGLAND

Please send me:

- ☐ **6 ISSUES OF RAILWAY MODELLER**  
Includes free access to the digital version.
- ☐ **12 ISSUES OF RAILWAY MODELLER**  
Includes free access to the digital version.
- ☐ **24 ISSUES OF RAILWAY MODELLER**  
Includes free access to the digital version. **No Direct Debit.**

Starting with the  issue

Cheques payable to **Peco Publications & Publicity Ltd.**

Visa/Mastercard:

Security Code: last three numbers on signature strip

Valid from ...../..... Expiry date ...../.....

Cardholder's Name .....

Address .....

Postcode ..... Country .....

E-mail .....

Daytime Tel. No. ....

Recipient's Name (if different from above):

Address .....

Postcode ..... Country .....

E-mail .....

Daytime Tel. No. ....

### The Direct Debit Guarantee

- This Guarantee is offered by all banks and building societies that accept instructions to pay Direct Debits.
  - If there are any changes to the amount, date or frequency of your Direct Debit, Howe & Davis Ltd will notify you 10 working days in advance of your account being debited or as otherwise agreed.
  - If you request Howe & Davis Ltd to collect a payment, confirmation of the amount and date will be given to you at the time of the request.
  - If an error is made in the payment of your Direct Debit by Howe & Davis Ltd or your bank or building society you are entitled to a full and immediate refund of the amount paid from your bank or building society.
  - If you receive a refund you are not entitled to, you must pay it back when Howe & Davis Ltd asks you to.
  - You can cancel a Direct Debit at any time by simply contacting your bank or building society.
- Written confirmation may be required. Please also notify us.



### INSTRUCTIONS TO YOUR BANK/BUILDING SOCIETY TO PAY BY DIRECT DEBIT

To (Bank/Building Society)

Address

Originator's Identification Number

6 3 1 1 9 2

Date

Signature(s)

#### Instruction to your Bank or Building Society

Please pay Howe & Davis Ltd Direct Debits from the account detailed in this instruction subject to the safeguards assured by the Direct Debit Guarantee. I understand that this instruction may remain with Howe & Davis Ltd, and, if so, details will be passed electronically to my Bank/Building Society.

Post code

Reference Number (Office use only)

Name(s) of A/c Holder(s)

Bank/Building Society Account No.

Branch Sort Code



08.2019





# UNMISSABLE!

## SUBSCRIPTION OFFERS

### 6 Months

Only  
**£23**

UK only†

**Includes 6 Months' FREE access to the Digital Edition† worth £17.49**

### 1 Year

Only  
**£45**

UK only†

**Includes 1 Year's FREE access to the Digital Edition† worth £34.99**

### 2 Years

Only  
**£80**

UK only†

**Includes 2 Years' FREE access to the Digital Edition† worth £69.98**

*Please note: It is not currently possible to pay by direct debit on our two year subscription.*

All three options include **FREE DELIVERY** direct to your door, so you never need miss an issue!

Great Burden

**RAILWAY  
MODELLER**

**ALSO AVAILABLE  
Digital Only Subscription**

1 Year **£34.99**

Same content as the magazine including the advert sections and access to back issues. Get your Modeller on the move!  
(This offer is only available through our website)

## Subscribe now

online [www.pecopublications.co.uk](http://www.pecopublications.co.uk)

or by telephone 01297 626203

† The Digital Edition is available for your PC, Mac, tablet or mobile device including Kindle Fire. Access available only while subscription is active. Includes fully searchable back issues starting from January 2011, with live website and email links.

‡ Overseas subscribers – for details please visit [www.pecopublications.co.uk](http://www.pecopublications.co.uk)

## A SUBSCRIPTION ALSO MAKES A VERY SPECIAL GIFT!



# INDEX TO ADVERTISERS

## A

accurascale, Irish Railway Models  
[www.irishrailwaymodels.com](http://www.irishrailwaymodels.com) .....23a  
 A C Models, Eastleigh, Hants **B** .....59a  
 AirFramed Bespoke Glass Display Cases  
[www.airframed.co.uk](http://www.airframed.co.uk) .....44a  
 All Components [www.allcomponentsltd.co.uk](http://www.allcomponentsltd.co.uk) .....56a  
 Alphagraphix (M) .....46a  
 Alton Model Centre **B** .....51a  
 Andy Lowe Electronics .....51a  
 Aspire Gifts and Models, Nr. Dorchester **A** .....45a

## B

Bachmann Europe plc (M) .....36a  
 Berkshire Dolls House & Model Co **B**  
[www.berkshiredollshousecompany.com](http://www.berkshiredollshousecompany.com) .....46a  
 Bexhill M.R.C. Exhibition, East Sussex  
[www.bexhillmrc.org.uk](http://www.bexhillmrc.org.uk) .....56a  
 blueRailways [www.blueRailways.co.uk](http://www.blueRailways.co.uk) .....50a  
 Bolton Model Mart **F** .....43a  
 Branchlines (M) .....43a  
 Bressingham Model Railway Show  
[www.bressingham.co.uk](http://www.bressingham.co.uk) .....55a  
 Brewsters [www.soldering-shop.co.uk](http://www.soldering-shop.co.uk) .....57a  
 BR Lines, Guisborough **F** .....46a  
 Buffers Model Railways, Axminster **A** .....50a  
 Bure Valley Models, Aylsham, Norfolk  
[www.burevalleymodels.com](http://www.burevalleymodels.com) .....50a

## C

Caistor Loco, Lincs **F** .....44a  
 Cheltenham Model Centre **A** .....4a & 5a  
 Church Street Models, Basingstoke  
[www.churchstreetmodels.co.uk](http://www.churchstreetmodels.co.uk) **B** .....45a  
 C & L Finescale Track Building Systems  
[www.finescale.org.uk](http://www.finescale.org.uk) **F** .....49a  
 C&M Models, Carlisle  
[www.candmmodels.co.uk](http://www.candmmodels.co.uk) **F** .....43a  
 Connoisseur Models [www.jimmcgeown.com](http://www.jimmcgeown.com) **C** .....43a  
 Conwy Valley Railway Museum **C** .....49a

## D

DCC Supplies [www.dccsupplies.com](http://www.dccsupplies.com) .....48a  
 DJH Engineering **F** .....55a  
 The Double O Gauge Association .....44a  
 Dundas Models, Fife [www.dundasmodels.co.uk](http://www.dundasmodels.co.uk) .....51a  
 Durham Trains of Stanley **F** .....45a  
 John Dufield, Chelmsford **B** .....45a

## E

East Somerset Models **A** .....45a  
 Eileen's Emporium  
[www.eileensempporium.com](http://www.eileensempporium.com) **B** .....48a  
 Ellis Clark Trains  
[www.ellisclarktrains.com](http://www.ellisclarktrains.com) .....64a & 65a  
 The Enchanted Garden, Dewsbury **F** .....44a  
 ESR Electronic Components Ltd, North Shields  
[www.esr.co.uk](http://www.esr.co.uk) .....43a  
 Exmoor Rail, Minehead .....48a  
 Express Models (M) .....48a

## F

Features .....2a & 3a  
 Finescalebrass (UK) Ltd, York  
[www.finescalebrass.co.uk](http://www.finescalebrass.co.uk) .....61a  
 Fox Transfers (M) .....55a  
 Frizinghall Models & Railways  
[www.modelrailshop.co.uk](http://www.modelrailshop.co.uk) .....53a  
 Frome Model Centre, Somerset  
[www.fromemodelcentre.com](http://www.fromemodelcentre.com) .....52a

## G

Gaugemaster [www.gaugemaster.com](http://www.gaugemaster.com) **B** .....6a, 7a & OBC  
 Golden Valley Hobbies  
[www.goldenvalleyhobbies.com](http://www.goldenvalleyhobbies.com) **D** .....54a  
 Grandad's Train Shop, Snailth, East Yorkshire  
[www.grandadstoyshop.co.uk](http://www.grandadstoyshop.co.uk) .....44a  
 Great British Model Railway Show  
[www.GBMRS.com](http://www.GBMRS.com) .....54a

## H

H & A Models (M) .....47a  
 Harburn Hobbies, Edinburgh  
[www.harburnhobbies.com](http://www.harburnhobbies.com) **G** .....35a  
 Haslington Models, Crewe **F** .....44a  
 Hattons Model Railways [www.hattons.co.uk](http://www.hattons.co.uk) **F** .....12a, 13a, 14a, 15a 16a, 17a & 18a  
 Heathcote Electronics (M)  
[www.heathcote-electronics.co.uk](http://www.heathcote-electronics.co.uk) .....53a  
 Heljan [www.heljan.dk](http://www.heljan.dk) .....21a  
 Hereford Model Centre **D** .....20a  
 The Hobby Goblin, Burslem, Stoke-on-Trent **D** .....44a  
 Hobby Shop, Faversham  
[www.hobb-e-mail.com](http://www.hobb-e-mail.com) **B** .....54a  
 Hornby Hobbies .....33a  
 Howes Models/Railmatch Paints, Kidlington **D** .....55a

## I

Invergowie Model Centre  
[email: angsgus@aol.com](mailto:angsgus@aol.com) **G** .....47a  
 Invertrain (M) .....46a  
 Invicta Model Rail, Sidcup, Kent **B** .....57a  
 Irish Railway Models  
[www.irishrailwaymodels.com](http://www.irishrailwaymodels.com) .....23a  
 Items Mail Order (M) .....46a

## J

Jack The Station Cat Ltd (M) .....46a  
 Junction 20 Models, Kings Langley **B** .....44a  
 The Junction Box [www.thejunctionbox.net](http://www.thejunctionbox.net) .....60a

## K

Keen Systems (M) .....46a  
 Kent Garden Railways, St. Mary Cray **B** .....48a  
 Kernow Model Rail Centre, Camborne  
[www.kernowmodelrailcentre.com](http://www.kernowmodelrailcentre.com) **A** .....8a, 9a, 10a & 11a  
 Kittle Hobby, Swansea [www.kittlehobby.com](http://www.kittlehobby.com) **C** .....52a

## L

Langley Models **B** .....59a  
 Lendons of Cardiff [www.lendons.co.uk](http://www.lendons.co.uk) **C** .....46a  
 Leyland Model Railway Exhibition, Lancashire .....50a  
 Little Jem Products (M) .....46a  
 Lord & Butler, Cardiff  
[www.lordandbutler.com](http://www.lordandbutler.com) **C** .....48a

## M

Malc's Models, Ilkeston **D** .....48a  
 Marway Models & Hobbies, Sheffield **F** .....43a  
 Lee Marsh Model Co  
[www.leemmarshmodelco.com](http://www.leemmarshmodelco.com) .....62a  
 Metcalfe Models & Toys (M) .....24a & 25a  
 Millennium Models, Morley, Leeds **F** .....46a  
 Minerva Models  
[www.minervamodelrailways.co.uk](http://www.minervamodelrailways.co.uk) .....43a  
 Model Electronic Railway Group  
[www.merg.org.uk](http://www.merg.org.uk) .....48a  
 Model Layout Services  
[www.modellayoutservices.co.uk](http://www.modellayoutservices.co.uk) .....44a  
 Modelit Ltd [www.modelitltd.co.uk](http://www.modelitltd.co.uk) .....49a  
 Modelmaniacs Ltd., Calne, Wilts  
[www.modelmaniacs.co.uk](http://www.modelmaniacs.co.uk) **A** .....51a  
 ModelMaster UK [www.modelmaster.uk](http://www.modelmaster.uk) **G** .....49a  
 Model Shop, Exeter **A** .....44a  
 Model Shop, Portsmouth **A** .....44a  
 MonkBar Model Shop, York **F** .....47a  
 Morley Controllers  
[www.morleycontrollers.com](http://www.morleycontrollers.com) **F** .....57a  
 Morris Models, North Lancing  
[www.morris-models.co.uk](http://www.morris-models.co.uk) **B** .....52a  
 MRSG - Model Railway Shop Grantham **F** .....44a  
 Mr Soundguy [www.mrsoundguy.co.uk](http://www.mrsoundguy.co.uk) .....46a

## N

N Gauge Society .....49a  
 North Eastern Railway Association .....52a  
 Northampton & District MRC Exhibition  
[www.ndmrc.org](http://www.ndmrc.org) .....58a

## O

Old Barn Model Craftsmen (M) .....50a  
 Osborn's Models, Bideford  
[www.osbornmodels.com](http://www.osbornmodels.com) **A** .....55a

## P

Paignton Model Shop **A** .....46a  
 Parkside Models .....60a  
 Parr's Lowestoft **E** .....44a  
 Bob Pearman Books (M) [pearman-books.com](http://pearman-books.com) **E** .....46a  
 PECO Heritage Railways .....61a  
 PECO Model Scene .....63a  
 PECO O Lineside Kits .....32a  
 PECO OO/HO Turnouts .....67a  
 PECORAMA .....26a  
 PECO Subscriptions .....68a & 69a  
 The Ivo Peters Collection .....47a  
 Peters Spares, Middlesbrough **F** .....56a  
 Phoenix Precision Paints Ltd  
[www.phoenix-paints.co.uk](http://www.phoenix-paints.co.uk) .....52a  
 Platform 3 Models [www.platform3models.co.uk](http://www.platform3models.co.uk) .....44a  
 Pooleys Puffers, Gateshead  
[www.pooleyspuffers.com](http://www.pooleyspuffers.com) .....49a

## Q

Quality Cork Supplies (M) .....49a

## R

RAILEX NE 2019, North Shields, Tyne & Wear  
[www.railexne.com](http://www.railexne.com) .....66a  
 Rails, Sheffield [www.railssheffield.co.uk](http://www.railssheffield.co.uk) **F** .....27a, 28a, 29a, 30a & 31a  
 Railway Conductor, Northants .....49a  
 Railway Modeller Subscriptions .....68a & 69a  
 Railway Scenics [www.railwayscenics.com](http://www.railwayscenics.com) .....50a  
 The Railway Shop, Blaenavon **C** .....44a  
 Ratio Plastic Models .....62a  
 RECREATION21 [www.rue-d-etopal.com](http://www.rue-d-etopal.com) .....43a  
 Rhuddlan Models  
[www.rhuddlanmodels.co.uk](http://www.rhuddlanmodels.co.uk) **C** .....46a  
 Richardsons, Feltham, Middlesex  
[www.richardsons-retail.co.uk](http://www.richardsons-retail.co.uk) **B** .....44a  
 Ron Lines Model Railways, Southampton **B** .....58a  
 Roxey Mouldings (M) .....45a  
 R & R Hobbies, Swansea  
[www.randrhobbies.co.uk](http://www.randrhobbies.co.uk) .....44a

## S

SAT Model Rail, Wickford, Essex  
[www.satmodelrail.co.uk](http://www.satmodelrail.co.uk) .....50a  
 Scalescenes.com .....50a  
 Scalespeed (M) .....44a  
 S & D Models [www.sanddmodels.co.uk](http://www.sanddmodels.co.uk) .....47a  
 7mm Narrow Gauge Association .....51a  
 Signal Box, Anstey **D** .....47a  
 Signals Model Shop, Somerset **A** .....43a  
 Simon Tender [www.simontender.com](http://www.simontender.com) .....44a  
 Simon's Trains, Devon  
[www.anythingnarrowgauge.co.uk](http://www.anythingnarrowgauge.co.uk) **A** .....47a  
 Smallbrook Studio [www.smallbrookstudio.co.uk](http://www.smallbrookstudio.co.uk) .....47a  
 South Eastern Finecast (M)  
[www.sefinecast.co.uk](http://www.sefinecast.co.uk) **B** .....43a  
 South West Digital  
[www.southwestdigital.co.uk](http://www.southwestdigital.co.uk) .....56a  
 Sports & Model Shop, Dingwall, Scotland **G** .....44a  
 Spot-On Models, Swindon **B** .....44a



Springside Models  
[www.springsidemodels.com](http://www.springsidemodels.com) .....45a  
 Steam Museum of the Great Western Railway  
[www.steam-museum.org.uk](http://www.steam-museum.org.uk) .....42a  
 Sunningwell Command Control  
[www.scc4dcc.co.uk](http://www.scc4dcc.co.uk) .....51a  
 Superquick Model Kits  
[www.superquick.co.uk](http://www.superquick.co.uk) .....34a  
 Sutton's Locomotive Workshop  
[www.sulzerlype2.com](http://www.sulzerlype2.com) .....inside back cover  
 Swanage Model Railway Centre, Dorset .....46a

## T

Tennents Trains **D** .....54a  
 3mm Scale Model Railways  
[www.3smr.co.uk](http://www.3smr.co.uk) .....45a  
 TMC-Direct [www.tmc-direct.com](http://www.tmc-direct.com) .....19a  
 Topp Trains, Stafford **D** .....45a  
 Tower Models, Blackpool  
[www.tower-models.com](http://www.tower-models.com) .....35a  
 Townstreet (M) .....47a  
 Tracklay Sales .....45a  
 Track-Shack.com .....66a  
 Trains4U.com, Peterborough  
[www.Trains4U@btconnect.com](http://www.Trains4U@btconnect.com) .....53a  
 Train Shop, Scarborough **F** .....44a  
 Train-Tech [www.Train-Tech.com](http://www.Train-Tech.com) .....53a  
 Transport Treasures, Aylesbury **B** .....44a  
 Trident Trains, Walgerton, near Nantwich **F** .....44a  
 Tutbury Models,  
 Tutbury, Burton-upon-Trent **D** .....44a

## U

Upstairs Downstairs, Isle of Wight  
[www.udtow.com](http://www.udtow.com) **B** .....43a

## V

Vectis Auctions Ltd [www.vectis.co.uk](http://www.vectis.co.uk) .....63a  
 Victoria Court Models, Burnham on Sea .....43a

## W

Wagon & Carriage Works  
[www.wagonandcarriage.co.uk](http://www.wagonandcarriage.co.uk) .....43a  
 Waltons of Altrincham **F** .....58a  
 Warwick & Warwick Auctioneers & Valuers **D** .....22a  
 White Rose Model Works **F** .....34a  
 Wild Swan Books [www.wildswanbooks.co.uk](http://www.wildswanbooks.co.uk) .....47a  
 Willis Kits .....59a  
 Wizard Models (M) [www.wizardmodels.co.uk](http://www.wizardmodels.co.uk) .....51a

## Y

YouChoos [www.YouChoos.co.uk](http://www.YouChoos.co.uk) .....58a

(M) Denotes Mail Order only.



# New SLW Class 24s on their way...

Our museum-quality, 4mm scale, ready-to-run Class 24 model has met with widespread acclaim. Join the band of delighted modellers by choosing from these new releases due in the next few months. Production quantities are now fixed so please place orders as soon as possible to guarantee your model.

Cat No. 2409/2409A:

**No. 24 009 & 5099**

**British Rail Blue  
(ScR allocated) with  
Glasgow Works ends**  
(TOPS & Pre-TOPS number options)



**TOPS &  
PRE-TOPS  
VERSIONS**



**ETCHED  
HEADBOARD  
INCLUDED**



Cat No. 2408:

**No. D 5082 BR green  
small yellow panels**  
(inc. CONDOR headboard)

Cat No. 2406:

**No. 97 201**

**EXPERIMENT**  
**British Rail Research  
Department RTC**  
(inc. nameplate transfers)



**NAMEPLATE  
TRANSFERS  
INCLUDED**



**NEW  
RUNNING  
NUMBER  
ADDED**

Cat No. 2407A:

**No. D 5053**  
**British Railways**  
**two-tone green**  
**with small yellow**  
**warning panels**

**"The detail and running qualities are superb. The diesel engine sound effects are, by some considerable margin, the best I have ever heard from a DCC loco."** SLW customer

- Over 350 components in each model
- Available in 'OO', 'EM' & 'P4' gauges
- Produced from official Works drawings

- Incredible bogie and interior details
- Softly-sprung working 'Oleo' buffers
- Period specific detailing differences

- Authentic sounds from twin speakers
- Revolutionary new driving experience
- Stay-alive and super-capacitor upgrade

Eight production runs in green and blue have already sold out! These new models, although slightly delayed, are now expected from July 2019 onwards.



For the full story, detail, pricing and to download the free manual, visit:

**www.sulzertype2.com**

Call us to order ☎ 01780 470086



Search YouTube for videos: 'SLW Class 24'  
Find us on Facebook: suttonslocoworks



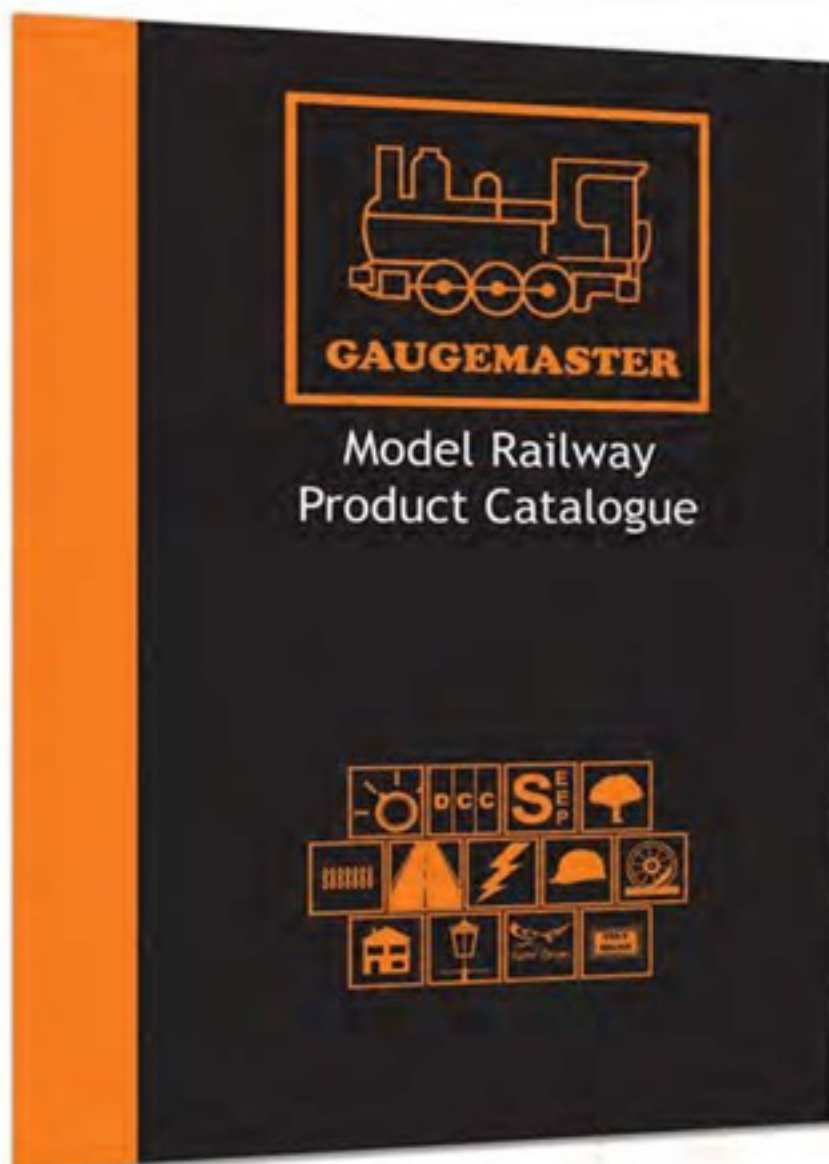
SLW is proud to be  
100% British owned





# GAUGEMASTER

## NEW Catalogue and Leaflets



GM360 Gaugemaster Catalogue - Only £4.95

**NEW GAUGEMASTER CATALOGUE OUT NOW**  
The Gaugemaster Model Railway Product Catalogue contains:

- 128 Full Colour Pages
- Listings and descriptions of all Gaugemaster products in the following ranges:
  - Analogue Control
  - Digital Control
  - SEEP Point Control
  - Scenics
  - Track
  - Highways
  - Electrics
- Articles in each section passing on knowledge and advice on how to use our products, as well as inspiration on how to turn your train set into a model railway.



**FREE GAUGEMASTER RANGE LEAFLETS**  
Available now in your local Model Shop:

Each of the 13 Gaugemaster sub-ranges broken down into a useful listing with pictures and descriptions.

- GM9950 Gaugemaster Analogue Control
- GM9951 Gaugemaster Collection
- GM9952 Gaugemaster DCC Control
- GM9953 Gaugemaster Electrics
- GM9954 Gaugemaster Highways
- GM9955 Gaugemaster Kestrel
- GM9956 Gaugemaster Lighting
- GM9957 Gaugemaster Scenics
- GM9958 Gaugemaster SEEP
- GM9959 Gaugemaster Structures
- GM9960 Gaugemaster Tiny Signs
- GM9961 Gaugemaster Tools
- GM9962 Gaugemaster Track



*Handy for in the workshop, so that you can note down exactly which items you need for you layout as you plan.*

These leaflets give a perfect snapshot of each part of our range to assist you with your projects. Planning an N Scale Digital British layout? Pick up the DCC Control, Kestrel, and Scenics. Electrifying points? Take a SEEP, Electrics, and Tools leaflet.

Find us on Facebook:  
@Gaugemaster  
Follow us on Twitter:  
@Gaugemaster  
Follow us on Instagram:  
@gaugemaster\_controls

**LOOK OUT FOR OUR FREE LEAFLETS IN  
YOUR LOCAL MODEL SHOP**



GAUGEMASTER products are available from your local model shop or, in case of difficulty, direct from ourselves  
**GAUGEMASTER** Controls Ltd, Ford Road, Arundel, West Sussex, BN18 0BN, United Kingdom  
tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com

**www.gaugemaster.com**