

CELEBRATING  
70 YEARS AS  
BRITAIN'S NUMBER ONE  
MODEL RAILWAY MAGAZINE

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AT THE HEART OF THE HOBBY

VOLUME 70 No 821

# RAILWAY MODELLER

## Return to Calcutta sidings

Modelling the  
Burton area in 4mm  
– 40' long!

### Little Salkeld

A wayside station on the  
Settle & Carlisle in N



**2019** Programme of new  
models in OO, N & 009

### City of Tiers

See the Aberdeen MRC's  
TV Challenge-winning OO layout at

**Model Rail Scotland**

### Reviewed inside ...

Hornby LNER J36 0-6-0 in OO  
Hornby SR Lord Nelson 4-6-0 in OO  
LNER N7 0-6-2T in OO from Oxford Rail

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# RAILWAY MODELLER

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Phil Eames undertook many changes to this 4mm scale recreation of Burton's railways in the 1988 – 1991 period; some major (doubling the length to 40") and others minor (changing from EM to P4). Modelling the brewery industry hereabouts meant modelling barrels – lots and lots of barrels!

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Once a calling point at the northern end of the Settle & Carlisle (just over 18 miles from the Border City) until it was closed in May 1970, this station has been brought back to life in N gauge by Paul Moss.

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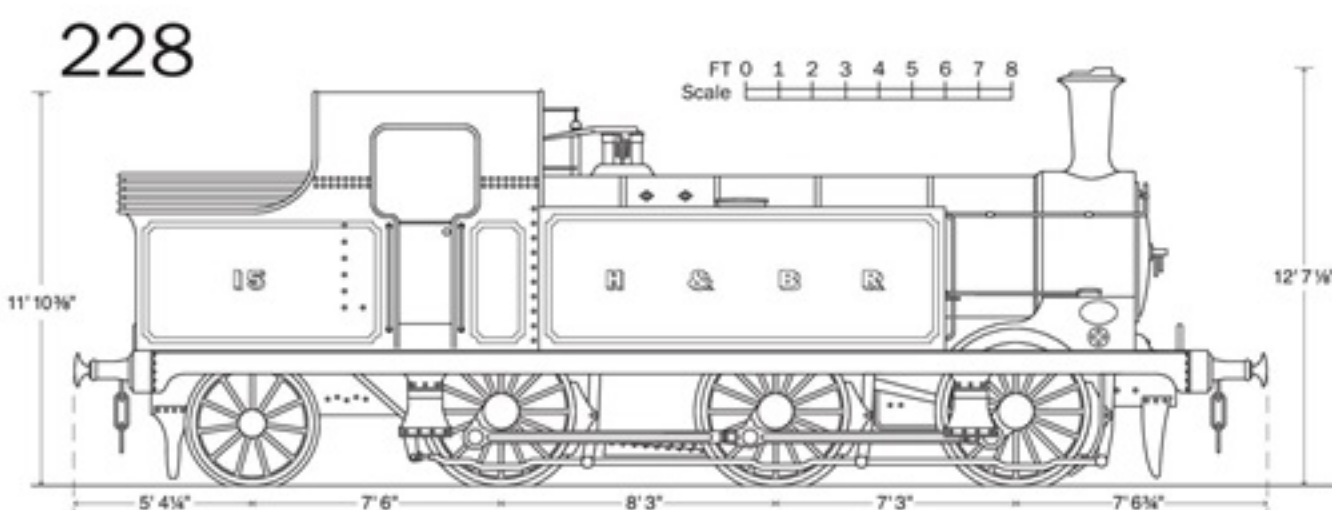


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Cover photography by Tom Silsbury

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| 4 x SMT1.....                                               | £4.00 |
| SMT2 Single Pole Double Throw, (On-On).....                 | £1.20 |
| 4 x SMT2.....                                               | £4.50 |
| SMT3 Single Pole Double Throw, Centre Off, (On-Off-On)..... | £1.20 |
| 3 x SMT3.....                                               | £3.00 |
| SMT4 Double Pole Double Throw (On-On).....                  | £1.35 |
| 4 x SMT4.....                                               | £4.80 |
| SMT5 Double Pole Double Throw (On-Off-On).....              | £1.45 |
| 4 x SMT5.....                                               | £5.20 |
| SMT6 Point Toggle Switches.....                             | £1.45 |
| 6 x SMT6.....                                               | £7.80 |

### Push to Make Switches

Red, Green, Black.....50p Each  
6 of any colour.....£2.70

### Grain of Wheat Bulbs

12v Bulbs  
Clear, Red, Yellow, Green or Amber  
Packs of 10.....£3.00



## Layout Wire

Suitable for point motors, power feeds & more  
7/0.2mm, Single Core, Multi Strand  
100 metre rolls available in 7 colours

Red/Black/Blue/Green/Yellow/Brown/White  
£6.99 a roll

Same colours available in  
10 metre rolls.....£1.50 each  
20 metre rolls.....£2.50 each

## METCALFE

|                                                           |        |
|-----------------------------------------------------------|--------|
| PN100 - Brick Paper.....                                  | £5.05  |
| PN103 - Terraced Houses Red Brick.....                    | £7.25  |
| PN104 - Terraced Houses Stone.....                        | £7.25  |
| PN105 - Low Relief High Street Shops.....                 | £5.65  |
| PN106 - Low Relief Bank & Shops.....                      | £5.65  |
| PN109 - Railway Station Buildings.....                    | £5.85  |
| PN110 - Platform Kit.....                                 | £8.10  |
| PN111 - Paving Slabs.....                                 | £5.05  |
| PN112 - Goods Shed.....                                   | £6.40  |
| PN113 - 2 Track Engine Shed.....                          | £6.40  |
| PN115 - Stone Sheets.....                                 | £4.99  |
| PN116 - Corner Shop Red Brick.....                        | £7.25  |
| PN117 - Corner Shop Stone.....                            | £7.25  |
| PN120 - Terraces House.....                               | £8.50  |
| PN123 - Low Relief Shop Fronts Red Brick.....             | £8.50  |
| PN124 - Low Relief Shop Fronts Stone.....                 | £6.25  |
| PN125 - Bus Garage.....                                   | £7.25  |
| PN126 - Parish Church.....                                | £7.10  |
| PN128 - Coaching Inn.....                                 | £8.50  |
| PN129 - Brewery.....                                      | £8.50  |
| PN131 - Parcels Office/Waiting Room.....                  | £8.50  |
| PN133 - Signal Box.....                                   | £7.75  |
| PN135 - Stone Platform.....                               | £8.10  |
| PN136 - Footbridge.....                                   | £8.50  |
| PN137 - Country Station.....                              | £10.20 |
| PN138 - Stone Built Wayside Station.....                  | £8.50  |
| PN139 - Wayside Station Shelter.....                      | £8.50  |
| PN140 - Red Brick Viaduct.....                            | £10.20 |
| PN141 - Stone Viaduct.....                                | £10.20 |
| PN142 - Tunnel Entrances.....                             | £6.80  |
| PN143 - Tunnel Entrance Single Track.....                 | £6.40  |
| PN144 - Retaining Wall in Stone.....                      | £8.10  |
| PN145 - Retaining Wall Red Brick.....                     | £8.10  |
| PN146 - Railway Bridge Red Brick.....                     | £10.65 |
| PN147 - Railway Bridge Stone.....                         | £10.65 |
| PN148 - Tapered Retaining Wall Red Brick.....             | £8.10  |
| PN149 - Tapered Retaining Wall Stone.....                 | £8.10  |
| PN150 - Manor Farm.....                                   | £7.65  |
| PN153 - Village School.....                               | £9.35  |
| PN154 - Village Shop & Café.....                          | £7.75  |
| PN155 - Workers Cottages.....                             | £8.50  |
| PN166 - Semi-Detached Houses.....                         | £7.35  |
| PN170 - Low Relief Cinema.....                            | £5.65  |
| PN174 - Brick Terraced House Fronts.....                  | £6.40  |
| PN175 - Stone Terraced House Fronts.....                  | £6.40  |
| PN176 - Low Relief Red Brick Terraced House<br>Backs..... | £6.80  |
| PN177 - Low Relief Stone Terraced House<br>Backs.....     | £6.80  |
| PN179 - Low Relief Department Store.....                  | £10.20 |
| PN181 - Service Station N Gauge.....                      | £8.95  |
| PN182 - Warehouse.....                                    | £9.40  |
| PN183 - Small Factory.....                                | £9.40  |
| PN184 - Boiler House & Factory Entrance.....              | £11.10 |
| PN190 - Low relief timber-framed shop<br>Fronts.....      | £7.50  |
| PN810 - Picnic Tables.....                                | £4.75  |
| PN812 - Garden Sheds.....                                 | £4.75  |
| PN815 - Nissen Hut.....                                   | £5.00  |
| PN816 - Platform Underpass.....                           | £4.75  |
| PN817 - Platform Kiosk.....                               | £4.75  |
| PN820 - Pillboxes.....                                    | £4.25  |
| PN821 - Pavilion.....                                     | £4.75  |
| PN822 - Stable Block.....                                 | £5.00  |
| PN823 - Stone Fountain Set.....                           | £5.00  |



UK Mainland Standard Postage Rate £4.00 per shipment  
Registered 24hr delivery (Mon to Fri) UK Mainland £7.00  
Worldwide postage at cost

Postcodes IM, BT, PH, PA, PO30-PO41, KW, AB, KA & IV will incur extra Postage - please enquire



www.cheltenhammodelcentre.com



## Western Limited Editions

A trio of Dapol OO gauge Westerns exclusive to Cheltenham Model Centre. Available for pre-order - due to arrive April

See the dedicated page on our website for more information about each loco and the Clear View Screens fitted to D1006 & D1039



D1039 'Western King' BR Maroon Small Yellow Panel  
With Experimental Rotary Windscreen Wiper  
**Limited Edition of 350**

Standard - £159.99

DCC Sound Fitted - £284.99

Between March 1964 - November 1964 D1039 was fitted with an experimental Clear View Screen designed by George Kents Ltd.



D1006 'Western Stalwart' BR Maroon Small Yellow Panel With Experimental Rotary Windscreen Wiper  
**Limited Edition of 350**

Standard - £159.99

DCC Sound Fitted - £284.99

This model will represent D1006 as it was between July 1966 - February 1967. D1006 was the second Western to be fitted with a Clear View Screen after improvements had been made over the original used on Western King.



D1000 'Western Enterprise' BR Maroon Small Yellow Panel  
**Limited Edition of 300**

Standard - £151.99

Each model will feature....

Etched name & number plates

Numbered Limited Edition Certificate

Specially designed box, unique for each loco

DCC fitted and DCC sound fitted versions available

We are expecting delivery to be Early 2019.

See the dedicated page on our website for more information!

### LIMITED EDITION WAGONS Exclusive To Cheltenham Model Centre

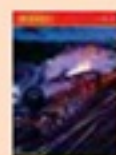


38-702z - 12 Ton ZDV Pipe Wagon  
Satlink S&T KDB741381  
**IN STOCK NOW!!!**



38-750z - 22 Ton STV Tube Wagon  
Satlink Western KDB730863  
**AVAILABLE NOW!**

### LIMITED EDITION WAGONS Exclusive To Cheltenham Model Centre



Hornby 2019 Catalogue  
£8.99



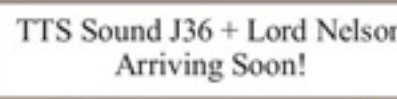
R3642 - Princess Coronation Class 46232  
'Duchess of Montrose' BR Green Early Crest.....£170.95



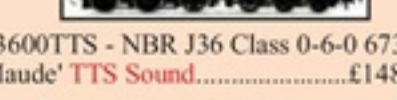
R3657 - Class 60 60070 'John Loundon McAdam' Loadhaul.....£161.99



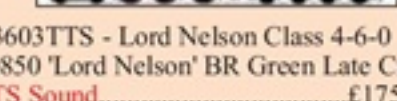
R3662 - Class 153 DMU 153368 GWR Green.....£98.99



TTS Sound J36 + Lord Nelson  
Arriving Soon!



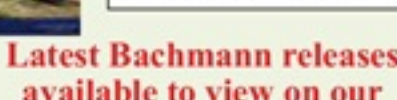
R3600TTS - NBR J36 Class 0-6-0 673  
'Maude' TTS Sound.....£148.49



R3603TTS - Lord Nelson Class 4-6-0  
30850 'Lord Nelson' BR Green Late Crest  
TTS Sound.....£175.49



Bachmann OO Gauge  
2019 Catalogue £9.95



Latest Bachmann releases  
available to view on our  
website



OO Gauge N7s In Stock/Due Soon

OR76N7001 - GER K85 (N7) 0-6-2  
1002.....£87.00  
OR76N7002 - LNER N7 0-6-2 8011  
LNER Black.....£87.00  
OR76N7003 - LNER N7 0-6-2 69621 BR  
Black Early Crest.....£87.00



OO Gauge Hymeks

3526 - Class 35 Hymek D7015 BR Plain Green.....£110.45  
3527 - Class 35 Hymek D7088 BR Green Small Yellow Panels.....£110.45  
3528 - Class 35 Hymek D7004 BR Blue Small Yellow Panels With White Windows.....£110.45  
3529 - Class 35 Hymek 7001 BR Blue Weathered.....£110.45  
3530 - Class 35 Hymek 35017 Dutch Livery (Preserved).....£110.45

OO Gauge Class 33

3417 - Class 33/0 D6504 BR Plain Green.....£118.95  
3418 - Class 33/0 D6518 BR Green Full Yellow Ends.....£118.95  
3419 - Class 33/0 BR Green Small Yellow Panels.....£118.95  
3437 - Class 33/0 D6558 BR Blue.....£118.95  
3439 - Class 33/0 33009 Dutch.....£118.95  
3446 - Class 33/1 D6580 BR Green Small Yellow Panel.....£118.95  
3458 - Class 33/1 33103 BR Blue.....£118.95  
3459 - Class 33/1 33111 BR Blue.....£118.95



OO-9 L&B Locos

9953 - Lyton & Barnstaple E760 EXE Southern (1927-29).....£178.45  
9955 - Lynton & Barnstaple 1362 EXE Green.....£178.45  
9956 - Lynton & Barnstaple 1363 TAW Green.....£178.45



Graham Farish N Gauge  
2019 Catalogue £8.95



371-468a - Class 37/0 97304 'John Tiley' Network Rail Yellow.....£110.45  
371-471 - Class 37/0 37261 DRS.....£110.45  
371-384A - Class 66 66111 EWS.....£114.70  
371-386 - Class 66 66416 Freightliner Powerhaul.....£114.70  
371-387 - Class 66 66846 Colas.....£114.70  
371-398 - Class 66 66779 'Evening Star' GBRf.....£114.70

BR Maroon Hawksworth Coaches

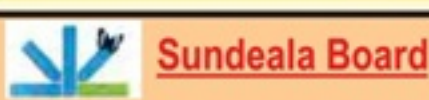


374-512 - Hawksworth Brake Second Corridor (BSK) BR Maroon.....£31.40  
374-537 - Hawksworth Second Class Corridor BR Maroon.....£31.40  
374-562 - Hawksworth Corridor Composite BR Maroon.....£31.40  
374-586 - Hawksworth Full Brake BR Maroon.....£31.40



OO Gauge Bolster E Due Early 2019

4F-061-001 - Bogie Bolster E BR Bauxite (TOPS BEV).....£25.45  
4F-061-002 - Bogie Bolster E BR Bauxite (TOPS YNV).....£25.45  
Dapol - 4F-061-003 - Bogie Bolster BR Bauxite (TOPS YRV (S&T)).....£25.45  
4F-061-004 - Bogie Bolster Wagon BR Bauxite (TOPS YNV).....£25.45  
4F-061-005 - Bogie Bolster Wagon BR Bauxite (TOPS YRV S&T).....£25.45



600mm x 1200mm (4' x 2')  
(approximate measurements)

Single Sheets £11.75 Shop Only  
6 Sheets £60.00 + £9.30 P+P (UK)

Stock Boxes for OO size locos & Carriages

**Large Size** - Tender Loco size boxes  
330mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black

**Medium Size** - 2-6-4 Tank size boxes  
220mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black

£3.50 each or

Any 4 Medium or Large Boxes for  
£13.00

**Small Size** - 08 or Pannier Tank size boxes  
157mm L x 48mm D x 62mm W  
Red/Blue/Green/Brown/Black

£3.00 each or

Any 4 Small Stock boxes  
For £10.00

### Cork Sheeting

36" x 24" (3ft x 2ft) per roll

1/16 Roll £5.00 each 2 for £9.00  
1/32 Roll £4.00 each 2 for £7.00  
1/8 Roll £6.75 each 2 for £12.00

### Cork Sheeting

PART EXCHANGE YOUR UNWANTED POINTS FOR  
ANYTHING ON THESE 2 PAGES

### Peco/Hornby Points

**Peco Points** We will allow £2.35 for each small or medium radius and £2.25 for large and curved radius 'OO' points. Also £7.00 each for double slip and 3-way point.

Fine Scale points also required.

N Gauge points also required.

**Hornby Points** We will allow £2.25 for new type Nickel Silver points.

£2.75 each for curved/express Nickel Silver points.

**Point Motor** We will allow £1.50 each for any quantity of Peco or Hornby point motor in working order.

### Authentic Stone Ballast

CMC Exclusive

207012 Medium Grade Ballast Grey (OO & O).....£3.00  
207013 Fine Grade Ballast Grey (N & OO).....£3.00  
2070131 Fine Grade Ballast Salmon (N & OO).....£3.00

Any 2 bags for £5.00

Bags 500g approx.



### Ballast Spreaders

N Gauge Ballast Spreader.....£11.00

OO Gauge Ballast Spreader.....£11.00

(Please note only fine ballast to be used with the ballast spreader)

Special Offer

Ballast spreader £10 when  
ordering with our ballast

### Second-Hand Wanted Boxed or unboxed

OO, N & O Gauge  
Lima, Hornby,  
Bachmann, Graham  
Farish, Heljan

Locos, Coaches or  
wagons.

Anything considered!

Part Exchange against  
anything in the shop

Please contact us for more  
information and before travelling  
with any part exchange.





# GAUGEMASTER *Spotlight*

## Scenic Trees & Hedges



### Ready-Made Trees

Our bulk pack of trees are an economical way of filling large areas at a keen price. These trees also make ideal starting points for providing extra detail using the complimentary range of **GAUGEMASTER** Scenics.

The heights of the trees are provided as generally model trees are suitable for all scales, unless where noted.



**GM120**  
Deciduous Trees (25)  
9cm to 15cm tall  
£23.95



**GM121**  
Mixed Forest Trees (25)  
9cm to 15cm tall  
£23.95



**GM122**  
Fir Trees (25)  
9cm to 15cm tall  
£23.95



**GM124**  
Spring Deciduous Trees (25)  
9cm to 15cm tall  
£23.95



**GM125**  
Spruce Trees (25)  
9cm to 15cm tall  
£23.95



**GM127**  
N Scale Mixed Trees (10)  
4cm to 10cm tall  
£10.95



**GM128**  
Spring Trees (10)  
8cm tall  
£12.95



**GM129**  
Summer Trees (10)  
8cm tall  
£12.95



**GM132**  
Tree Pack Mixed (10)  
10cm to 14cm  
£12.95



**GM180**  
Plum Trees (3)  
8cm tall  
£8.75



**GM181**  
Plum Trees in Blossom (3)  
8cm tall  
£8.75



**GM182**  
Fruit Trees (3)  
8cm tall  
£8.75



**GM183**  
Apple Trees (3)  
8cm tall  
£8.75



**GM184**  
Birch Trees (3)  
8cm to 10cm tall  
£8.75



**GM185**  
Weeping Willow Trees (3)  
8cm tall  
£8.75



**GM186**  
Poplar Trees (3)  
12cm tall  
£8.75



**GM187**  
Pine Trees (3)  
12cm tall  
£9.75



**GM189**  
Beech Trees (2)  
12cm tall  
£8.75

### Seafoam Trees



This starter set enables you to create trees, bushes, and hedging with a realistic appearance. Seafoam is a natural product which can be 'planted' straight from the box if you wish, but adding scatter material with spray adhesive is well worth the effort.

**GM195** Seafoam Tree Starter Set  
£19.95



**GM156**  
Light Green Scenic Leaves  
£4.45



**GM157**  
Mid Green Scenic Leaves  
£4.45



**GM158**  
Dark Green Scenic Leaves  
£4.45

### Grass Tufts

Ideal for building up vegetation on your layout.



**GM162**  
Grass Tufts  
Green  
6mm (42)  
£5.25



**GM149**  
Plants  
12mm  
Green (42)  
£5.25



**GM163**  
Grass Tufts  
Green  
12mm (42)  
£5.25



**GM159**  
Plants  
12mm  
Yellow (42)  
£5.25

### Hedgerows and Lichen

Hedgerows and lichen can be used to add additional green features to your layout right out of the packet! Combined with flocks/scatters you can get some great effects.



**GM160**  
Light Green Hedgerow  
Most suited to OO/HO  
Total Length 1 metre.  
£7.75



**GM161**  
Dark Green Hedgerow  
Most suited to OO/HO  
Total Length 1 metre.  
£7.75



**GM164**  
Light Green Lichen (80g)  
£5.25



**GM165**  
Dark Green Lichen (80g)  
£5.25



**GM166**  
Mixed Lichen (80g)  
£5.25



**GM194**  
Scenic Starter Set

The perfect way to begin scenic modelling - this pack contains a small selection of trees, flocks, scatters, ballast, glue scenic mat and a puffer bottle, in fact just about everything you need to get started in one convenient starter pack.

**£24.95**

Full details of our Scenics range can be found in the Gaugemaster Full Catalogue **AVAILABLE SUMMER 2018**.

It also contains details of our Analogue and Digital Controllers, Point Control, Electrics and much more in the Gaugemaster range.

It also contains selected items from many of the other brands that we stock.

GM353 Gaugemaster Full Catalogue

£3.95

**GAUGEMASTER** products are available from your local Model Shop or, in case of difficulty, direct from ourselves.

**GAUGEMASTER** Controls Ltd, Gaugemaster House, Ford Road, Arundel, West Sussex, BN18 0BN, United Kingdom  
tel - 01903 884321 Fax - 01903 884377 email - sales@gaugemaster.co.uk web - [www.gaugemaster.com](http://www.gaugemaster.com)





# GAUGEMASTER *Spotlight* Prodigy Digital Control



## Controller Types

### Which Prodigy Controller?

Prodigy is compatible with most other DCC systems and manufacturers' DCC Decoders. It's strength is 'simplicity without compromise' and Prodigy systems can grow with your layout, allowing you to use the system at a level appropriate to you, your layout, and style of operation. All Prodigy Walkaround Controllers have backlit displays as standard.

**Prodigy Express** consists of a Base Station, Walkaround Controller, and Power Supply and is an ideal entry level system for a newcomer. Extra features can be unlocked by using the **DCC14 Prodigy Advance Walkaround Controller**.

**Prodigy Advance2** is our best selling DCC Controller. It consists of a Base Station, Walkaround Controller, and a Power Supply, and is suitable for operating most sizes of model railway.

**Prodigy Wireless** offers all the features of the **Prodigy Advance** unit but this system offers you wireless control, giving you maximum freedom to operate your layout. Its rechargeable handset and radio transmission ensures easy and reliable performance.

## DCC Controller Starter Packages

### DCC01 Prodigy Express Starter Package Most Suited for HO/OO/N Scale Layouts



- INPUT: 15-16V regulated DC at 2 amps
- OUTPUT: DCC Signal with 14.5V amplitude
- MAXIMUM CURRENT: 1.6 amps
- MAXIMUM NUMBER OF WALKAROUNDS: 20
- ADDRESS CAPABILITY: 2 Digit or 4 Digit
- SPEED STEPS: 14/28/128
- ACCESSORY FUNCTIONS: 28 (F0 to F28)
- DOUBLE HEADING
- PROGRAM & MAIN TRACK PROGRAMMING
- "READ LOCOMOTIVE" CAPABILITY

**BEST FOR**  
Beginners

### DCC02 Prodigy Advance2 Starter Package Most Suited to HO/OO/N Scale Layouts



- INPUT: 15-14V DC 3.5 amps
- OUTPUT: DCC Signal with 14.5V amplitude
- MAXIMUM CURRENT: 3.5 amps
- MAXIMUM NUMBER OF WALKAROUNDS: 99
- ADDRESS CAPABILITY: 2 Digit (1-127) or 4 Digit (1-9999)
- SPEED STEPS: 14/28/128
- ACCESSORY DECODER AND ROUTE SETTING
- DOUBLE HEADING
- PROGRAM & MAIN TRACK PROGRAMMING
- "READ LOCOMOTIVE" CAPABILITY

**BEST FOR**  
General Layouts

### DCC04 Prodigy Advance Wireless Starter Package Most Suited to OO/HO/N Scale Layouts



- ALL THE FEATURES OF DCC02 PRODIGY ADVANCE
- WIRELESS RADIO CONTROL (UK Approved)
- OUTSTANDING RANGE (80ft Plus)
- BUILT-IN RECHARGABLE BATTERY
- BATTERY CHARGE LEVEL INDICATOR
- OPERATES WIRED & WIRELESS WALKAROUNDS
- PROGRAM & OPERATE WHILE CHARGING
- FREEDOM & MOBILITY DURING OPERATION

**BEST FOR**  
Large Layouts

## Accessories

### NEW! GO WIRELESS with the Prodigy WiFi



#### DCC05 Prodigy WiFi

This unit allows you to run your Prodigy-powered layout from your phone or tablet, using one of the recommended apps on Android or IOS. The Prodigy WiFi is compatible with the JMRI Engine Driver and WiThrottle apps.

Full details of our Digital Controller range can be found in the **Gaugemaster Full Catalogue AVAILABLE SUMMER 2018**. It also contains details of our Analogue Controllers, Scenics, Point Control, Electrics and much more in the Gaugemaster range.

It also contains selected items from many of the other brands that we stock.

GM353 Gaugemaster Full Catalogue £3.95



#### DCC80 DCC Autofrog

The DCC Autofrog is a simple switch that automatically changes the frog polarity of your electrofrog point. With the frog powered by the DCC80 it will automatically detect the incoming polarity of the wheels and switch the frog to match. The DCC Autofrog is available as a single item, and also as a handy pack of three.

#### DCC15 Prodigy Decoder Doctor

Makes programming and reading back your decoders easy! This device allows you to test decoders before or after installation.



There is a built-in 8 Pin socket, and the Decoder Doctor also comes supplied with an adaptor harness allowing you to also test 6 Pin decoders.

You can also attach it to a test track for testing already fitted decoders.

Power comes from either a 15V power supply (DCC65) or your DCC Main Track output.



#### DCC40 Auto Reverse Module

This useful module allows you to operate a reverse loop or turntable without needing to change the polarity of the track, a common situation that faces modellers with analogue control model railways.

## PRICES

### Controllers & Handsets

|       |                                           |         |
|-------|-------------------------------------------|---------|
| DCC01 | Prodigy Express Package                   | £179.95 |
| DCC02 | Prodigy Advance2 Starter Package          | £309.95 |
| DCC04 | Prodigy Advance2 Wireless Starter Package | £499.95 |
| DCC05 | NEW Prodigy WiFi                          | £99.95  |
| DCC13 | Prodigy Advance2 Wireless Walkaround      | £199.95 |
| DCC14 | Prodigy Advance2 Backlit Walkaround       | £149.95 |
| DCC15 | Prodigy Decoder Doctor                    | £94.95  |
| DCC51 | Prodigy Wireless Conversion Set           | £279.95 |

### Accessories

|        |                                          |         |
|--------|------------------------------------------|---------|
| DCC11  | Prodigy Extension Plate                  | £44.95  |
| DCC49  | Prodigy DCC Booster Unit (8 Amp)         | £209.95 |
| DCC55  | Prodigy Advance Wired Computer Interface | £64.95  |
| DCC60  | Spare Plug for Gaugemaster Prodigy       | £4.15   |
| DCC62  | Prodigy Universal Lead (2m)              | £6.25   |
| DCC63  | Prodigy Advance Power Supply Unit        | £27.95  |
| DCC64  | Prodigy Power Pack Lead                  | £7.75   |
| DCC65  | Prodigy Express Power Supply Unit        | £27.95  |
| DCC66  | Controller Caddy Walkaround Holder       | £7.35   |
| DCC71  | Prodigy DC Adaptor Plate/Decoder Tester  | £10.95  |
| DCC77  | Prodigy Walkaround Adaptor               | £10.95  |
| DCC80  | DCC Autofrog                             | £6.25   |
| BPDC80 | DCC Autofrog (Pack of 3)                 | £15.95  |

**GAUGEMASTER** products are available from your local Model Shop or, in case of difficulty, direct from ourselves.

**GAUGEMASTER** Controls Ltd, Gaugemaster House, Ford Road, Arundel, West Sussex, BN18 0BN, United Kingdom  
tel - 01903 884321 Fax - 01903 884377 email - sales@gaugemaster.co.uk web - [www.gaugemaster.com](http://www.gaugemaster.com)



*Hattons*  
ORIGINALS



# YOB Plasser 12t GPC Crane

## No Jib Support

**H4-GPC-001** DRP81504 in Plasser/BR yellow as built (1979 - 1990)

**H4-GPC-002** DRP81523 in Plasser/BR yellow as built (1980 - 1990)

**H4-GPC-003** DRP81508 in unbranded yellow (1979 - 2004)

**H4-GPC-004** DRP81521 in unbranded yellow (1980 - 2011)

**H4-GPC-005** DRP81527 in Jarvis yellow (1999 - 2011)

**H4-GPC-009** DRP81519 in Balfour Beatty white and blue (1999 - Present)

**H4-GPC-010** DRP81532 in Balfour Beatty white and blue (1999 - Present)

## Narrow Jib Support

**H4-GPC-006** DRP81514 in Jarvis/FastLine maroon (2004 - 2014)

## Goalpost Jib Support

**H4-GPC-007** DRP81524 in Carillion yellow (1999 - 2006)

**H4-GPC-008** DRP81526 in GTRM yellow (2004 - 2008)

- Diecast Chassis
- NEM Couplings
- Metal Handrails
- RP25 Wheel Profile

00  
Gauge

£69

Due in stock: Jan 2020

See the full list of specs and more at: [www.hattons.co.uk/GPC](http://www.hattons.co.uk/GPC)

©Peter Groom



# LMS Beyer-Garratt 2-6-0 0-6-2

**H2-BG-001** 4994 in LMS black

**H2-BG-002** 7983 in LMS black

**H2-BG-003** 47967 in BR black with "BRITISH RAILWAYS" lettering

**H2-BG-004** 47972 in BR black with LMS lettering and block-style numbers

**H2-BG-005** 47995 in BR black with number on cab and plain tanks

**H2-BG-006** 47996 in BR black with early emblem

**H2-BG-007** 47981 in BR black with early emblem

**H2-BG-008** 47992 in BR black with early emblem

Diecast Chassis • 5 Pole Motor  
NEM Couplings • 18 Pin Socket

N  
Gauge

£199

Due in stock: Jan 2020

See the full list of specs and more at: [www.hattons.co.uk/NGarratt](http://www.hattons.co.uk/NGarratt)



# Forthcoming Releases from Hornby & Dapol

All available to pre-order now at: [www.hattons.co.uk](http://www.hattons.co.uk)

**HORNBY® 2019 Range Announcement** - see details at [www.hattons.co.uk/hornby2019](http://www.hattons.co.uk/hornby2019)

## LMS Princess Royal 4-6-2

Pre-order from £171. DCC-fitted £184.50  
Due in Stock November 2019



R3709 6201 "Princess Elizabeth" in LMS crimson lake  
R3711 46206 "Princess Marie Louise" in BR express passenger blue  
R3713 46207 "Princess Arthur of Connaught" in BR maroon

## Class A1/A1X Terrier 0-6-0T

Pre-order from £81. DCC-fitted £99  
Due in Stock from March to May 2019



R3767 32655 in BR black with early emblem  
R3768 32636 in BR black with late crest  
R3780 655 "Stepney" in LBSCR improved engine green  
R3781 5 "Rolvenden" in Kent and East Sussex Railway blue  
R3782 751 in South Eastern and Chatham Railway green  
R3783 2662 in Southern Railway olive green

## Class 5101 'Large Prairie' 2-6-2T

Pre-order from £126. DCC-fitted £144  
Due in Stock July/August 2019



R3719 4154 in GWR green  
R3721 6110 in GWR green  
R3723 6145 in BR black with early emblem  
R3725 4160 in BR lined green with late crest

## 4wDM Ruston 48DS

Pre-order for £72.  
Due in Stock May/June 2019



R3704 269595 in pale green  
R3705 458957 in John Dewar and Sons red  
R3706 Army 802 in War Department green  
R3707 "Queen Anne" in Longmorn Distillery livery

## Class B2 Peckett 0-6-0ST

Pre-order from £99. DCC fitted for £117  
Due in Stock April 2019



R3693 No.4 "Sherwood" in Sherwood Colliery lined green  
R3694 "Westminster" in Associated Portland Cement green  
R3695 1455 in National Coal Board lined blue

## Mk3 Sliding Door coaches

Pre-order for £31.50.  
Due in Stock June to November 2019



36 versions now available to pre-order. Liveries include: GWR, ScotRail and Cross Country. Numerous vehicles types available to allow prototypical formations.

## dapol Forthcoming Items

See the full range of forthcoming items from Dapol at [www.hattons.co.uk/dapolpreorders](http://www.hattons.co.uk/dapolpreorders)

### N Gauge

#### Class 142 DMU

Pre-order from £115.18  
Due in Stock: Q1 2019



ND116A 142065 in Northern Rail livery  
ND116B 142081 in Regional Railways livery  
ND116C 142025 in Northern Spirit livery  
ND116D 142085 in Arriva Trains Wales livery  
ND116E 142021 in Tyne and Wear PTE livery

#### Class 50

Pre-order from £123.21  
Due in Stock: March/April 2019



2D-002-000 D406 in BR blue - unrefurbished  
2D-002-001 50043 in BR blue - unrefurbished  
2D-002-002 50040 "Leviathan" in BR large logo blue  
2D-002-003 50037 "Illustrious" in original Network SouthEast livery

### OO Gauge

#### Class 43xx GWR Mogul 2-6-0

Pre-order from £135.96  
Due in Stock: Summer 2019



4S-043-001 6336 in GWR green with Great Western lettering  
4S-043-002 6385 in GWR green with shirtbutton emblem  
4S-043-003 GWR green with BR smokebox numberplate  
4S-043-004 7324 in BR black with early emblem  
4S-043-005 6364 in BR lined green with early emblem  
4S-043-006 7310 in BR lined green with late crest

#### Class 59

Pre-order from £124.43  
Due in Stock: Feb/March 2019



4D-005-000 59002 "Alan J Day" in Foster Yeoman livery  
4D-005-001 59103 "Village of Mells" in ARC livery  
4D-005-002 59206 "John F Yeoman" in DB Schenker livery  
4D-005-003 59204 "Vale of Glamorgan" in National Power livery

### O Gauge

#### Class Y3 Sentinel 4wVB

Pre-order from £148.75  
Due in Stock: May/June 2019



7S-005-001 42 in LNER black  
7S-005-002 68163 in LNER black  
7S-005-003 7164 in LMS black  
7S-005-004 7160 in LMS black  
7S-005-005 68164 in BR black with early emblem  
7S-005-006 2 "Isebrook" in GWR green  
7S-005-007 14 in National Coal Board livery

#### Class 08 (Re-tool)

Pre-order from £191.25  
Due in Stock: June/July 2019



Partial re-tool covering later built locomotives. 5 variations available either DCC Ready, DCC Sound Fitted, Unnumbered and Unnumbered with DCC Sound.

**Pre-orders accepted  
via credit or debit card only**  
Pre-order prices subject to manufacturer RRP.



## N Gauge

Dapol - DMUs

NEW



ND116B Class 142 'Pacer' 2 car DMU 142081 in Regional Railways NEW ..... £115.18

Peco Products - Track - Code 55 Finescale



SL-300F Pack of 30 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track ..... £84  
SL-300F Single ..... £3

## OO Gauge (1:76 Scale)

Bachmann Branchline - Steam locos



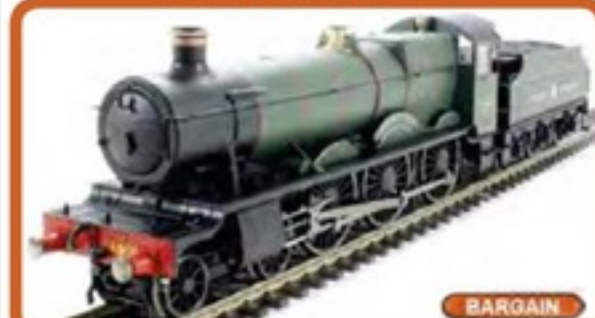
31-127 Class 30xx 2-8-0 ROD 3023 in BR black early emblem (RRP £129.95) ..... BARGAIN ..... £59

Heljan - DMUs



8920 Class 128 parcels DMU 55991 in BR blue yellow ends 'Parcels Service' branding (RRP £119.95) ..... BARGAIN ..... £50

Hornby - Steam locos



R3170 Class 49xx 4-6-0 4901 'Adderley Hall' in GWR Green - Railroad range (RRP £94.99) ..... BARGAIN ..... £49



R3623 Class 8P 'Streamlined Coronation' 4-6-2 6221 'Queen Elizabeth' in LMS coronation blue ..... £171



R3622 Class J36 0-6-0 65311 'Haig' in BR black early emblem. £126

# Top 30 Best Sellers

Last 30 Days

Track - Code 75 Finescale

NEW



R3634 Class LN 'Lord Nelson' 4-6-0 851 'Sir Francis Drake' in Southern Railway olive green NEW ..... £153



R3420 Drummond Class 700 0-6-0 30346 in BR black late crest (RRP £124.99) ..... BARGAIN ..... £59

Diesel locos



R3491 Class 42 Warship D805 'Benbow' in BR green - Railroad Range (RRP £64.99) ..... BARGAIN ..... £34.50



R3657 Class 60 60070 'John Loundon McAdam' in LoadHaul triple grey NEW ..... £162

HSTs



R3685 Pair of Class 43 HST Power Cars 43041 'Meningitis Trust Support for Life' and 43005 in GWR green (RRP £289.99) ..... BARGAIN ..... £229

Train sets - digital



R1125 Somerset Belle train set Class 3F 0-6-0 S&DJR 0-6-0 steam loco & 3 wagons - DCC control (RRP £210.99) ... BARGAIN. £130

Wagons



R6888TTS 4-wheel 10 ton vent van in BR bauxite - TTS sound fitted ... £36

Peco Products - Track - Code 100 Streamline



SL-100 Pack of 25 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track ..... £66  
SL-100 Single ..... £3



SL-100F Pack of 25 1 yard (91.5cm) length of Nickel Silver Finescale flexible track ..... £72  
SL-100F Single ..... £3

## OO9 Narrow Gauge

Heljan - Steam locos



9950 Lynton & Barnstaple 2-6-2T 'Yeo' in L&B dark green £189.95



9951 Lynton & Barnstaple 2-6-2T 760 'Exe' in Southern Railway green ..... £189.95



9952 Lynton & Barnstaple 2-6-2T 761 'Taw' in Southern Railway green ..... £189.95



9953 Lynton & Barnstaple 2-6-2T E760 'Exe' in intermediate Southern Railway (1927-29) ..... £209.95

## O Gauge (1:43 Scale)

Heljan - Steam locos



4312 Class 43xx Mogul 2-6-0 5330 in BR lined green late crest £662.15



4310 Class 43xx Mogul 2-6-0 5358 in BR black early emblem ... £662.15

Diesel locos



2558 Class 25/3 in BR blue - unnumbered ..... £446.25

Peco Products - Track - Code 124 Bullhead Streamline



SL-700BH Box of 12 1 Yard O Gauge Flexible Nickel Silver Track (Bullhead Nickel Silver Rail) ..... £75  
SL-700BH Single ..... £6.50

## Any or Multiple Scales

Bachmann Branchline - Digital decoders



36-553 8-pin 3-function 0.75A decoder back EMF (RRP £20.90) ..... BARGAIN ..... £12

ESU - Digital controllers



50210 EcoS Command Station DCC V2.1 controller full colour LCD touch screen & dual controllers - 6A output. .... £600

Gaugemaster Controls - Analogue controllers



D Double/Twin track power controller ..... £80

Hornby - Digital decoders



R8249 Box of 6 8-pin 4-function 2-sided 0.5A (1A peak) decoder .. £92  
R8249 Single ..... £16

NCE - Digital controllers



524-042 NCE Power Cab 2 Amp Digital DCC controller ... £154

Peco Products - Point motors



PL-11 Side/surface mounted point motor ..... £8



**Customer  
Service Quality  
Award Winner  
2015, '16 & '17**

# British Model Railway Awards 2017

## OO Gauge (1:76 Scale)

### Bachmann Branchline - Steam locos

31-434 Class 1F 'Hall Cab' 0-6-0T 41803 BRITISH RAILWAYS black (RRP £99.95) ..... **BARGAIN** ..... £86  
32-301A Class 2251 0-6-0 Collect Goods 3212 in BR black early emblem and Collect tender (RRP £114.95) ..... **BARGAIN** ..... £78



31-127 Class 30xx 2-8-0 ROD 3023 in BR black early emblem (RRP £129.95) ..... **BARGAIN** ..... £59



32-227DS Class 3F Jinty 0-6-0T 7365 in LMS black - DCC sound fitted (RRP £214.95) ..... **BARGAIN** ..... £168  
32-078 Class 56xx 0-6-2T 5637 in GWR green ..... £97.71  
31-690 Class 5P4F Stanier Mogul 2-6-0 2965 LMS lined black (RRP £159.95) ..... **BARGAIN** ..... £106  
31-692 Class 5P4F Stanier Mogul 2-6-0 42968 in BR lined black late crest (RRP £159.95) ..... **BARGAIN** ..... £112  
31-214 Class 6P Patriot 4-6-0 45538 'Giggleswick' in BR Green early emblem (RRP £164.95) ..... **BARGAIN** ..... £107



32-850A Class 9F 2-10-0 92220 'Evening Star' in BR green late crest (RRP £189.95) ..... **BARGAIN** ..... £149



31-531 Class A2 4-6-2 60536 'Trimbush' in BR lined green early emblem (RRP £179.95) ..... **BARGAIN** ..... £111



35-077 Class E4 Brighton tank 0-6-2T 32556 in BR black early emblem (RRP £109.95) ..... **BARGAIN** ..... £76



31-921 Class H2 Atlantic 4-4-2 32424 'Beachy Head' in BR black early emblem ..... £161.46  
32-166 Class N2 6-0 1406 in Southern Railway black (RRP £159.95) ..... **BARGAIN** ..... £108  
31-981 Standard Class 3MT tank 82021 in BR lined black early emblem ..... £98



32-250A WD Austerity 2-8-0 79250 'Major-General Mc Mullen' in Longmoor Military Railway blue (RRP £169.95) ..... **BARGAIN** ..... £115



### Diesel locos

32-119 Class 08 Shunter 08907 in DB Schenker red (RRP £124.95) ..... **BARGAIN** ..... £71  
32-790 Class 37/0 37049 'Imperial' in Railfreight Coal Sector triple grey ..... £135.96  
32-392 Class 37/5 37688 'Kingmoor TMD' Direct Rail Services compass blue ..... £135.96  
32-738 Class 66 66711 'Sence' GBRA/Aggregate Industries (RRP £144.95) ..... **BARGAIN** ..... £113



31-590 Class 70 70015 in Freightliner air intake modifications (RRP £169.95) ..... **BARGAIN** ..... £133



32-993 Type 27 Wickham Trolley and trailer MPP0007 in BR engineers yellow wasp stripes (RRP £79.95) ..... **BARGAIN** ..... £55



### Coaches

39-576 Hawksworth A38 Auto-trailer W228W BR maroon (RRP £58.95) ..... **BARGAIN** ..... £48  
39-261B Mk1 RMB Miniature Buffet W1816 in BR Maroon (RRP £47.95) ..... **BARGAIN** ..... £31  
39-050D Mk1 SO second open M4243 BR blue and grey (RRP £47.95) ..... **BARGAIN** ..... £35

### Wagons

37-806 12 ton ventilated van in British Army green ..... £16.96

### Analogue controllers

36-565 Power Controller (0.7amp) transformer plug and track connections for OO, HO, N and O09 scales ..... £42.46

### Corgi Collectables - Buses

OM46713A Wright Eclipse II - Transdev - 'The Shuttle' 662 Keighley Bus Station via Crossflats NEW ..... £33

### Dapol - Diesel locos

4D-012-004D Class 22 D6325 BR green no yellow panels and disc headcodes - DCC fitted ..... £146.91  
OLIV001 Class 73/2 73207 in Gatwick Express - Olivias Trains limited edition (RRP £140) ..... **BARGAIN** ..... £84

### DMUs

4D-009-HAT02 Class 121 single car DMU 'Bubblecar' 121032 in Arriva Trains Wales - Hatton's limited edition (RRP £129.95) ..... **BARGAIN** ..... £105



4D-009-001 Class 121 single car DMU 'Bubblecar' W55020 in BR green speed whiskers (RRP £145) ..... **BARGAIN** ..... £109  
4D-009-002 Class 121 single car DMU 'Bubblecar' W55028 in BR green small yellow panels (RRP £145) ..... **BARGAIN** ..... £109  
4D-009-003 Class 121 single car DMU 'Bubblecar' W55029 in BR blue & grey (RRP £145) ..... **BARGAIN** ..... £109

### Track Maintenance vehicles

B800 Non-motorised OO Track Cleaner motorised cleaning heads & vacuum in blue ..... £64



### Wagons

4F-026-013 HIA aggregate limestone hopper 369006 in Freightliner green ..... £22.52  
4F-026-014 HIA aggregate limestone hopper 369012 in Freightliner green ..... £22.52

### Signals

4L-001-001 Signal - Motorised, Square post GWR home lower quadrant (D00S1N1) ..... £25

### DJ Models (Dave Jones) - Steam locos

J9410 Austerity 0-6-0ST No 15 in Wernys Private Railway lined brown - Exclusive to Hattons (RRP £95.50) ..... **BARGAIN** ..... £69  
J9409 Austerity 0-6-0ST No 7 in NCB Littleton Colliery lined blue - lightly weathered - Ltd Ed of 200 (RRP £95.50) ..... **BARGAIN** ..... £79



H1415 Class 14xx 0-4-2T 1432 in BR Lined green late crest - Heavily weathered (RRP £99) ..... **BARGAIN** ..... £74



H1409 Class 14xx 0-4-2T 1470 in BR Lined black early emblem (RRP £99) ..... **BARGAIN** ..... £74  
H1403 Class 48xx 0-4-2T 4871 in GWR Unlined green Shitbutton logo - Lightly weathered (RRP £99) ..... **BARGAIN** ..... £69  
H1411 Class 58xx 0-4-2T 5801 in BR Unlined green GWR lettering - Lightly weathered (RRP £99) ..... **BARGAIN** ..... £74  
H1410 Class 58xx 0-4-2T 5819 BR Unlined black early emblem (RRP £99) ..... **BARGAIN** ..... £74

### Electric locos

OO71-004HAT Class 71 E5013 in BR Blue full Golden Arrow headboards, arrows and flags pre-fitted - Exclusive to Hatton's (RRP £139.95) ..... **BARGAIN** ..... £96



OO71-002HAT Class 71 E5015 in BR Southern Region light green full Golden Arrow headboards, arrows and flags pre-fitted - Exclusive to Hatton's (RRP £139.95) ..... **BARGAIN** ..... £99

### EFE - Buildings - general

99660 Art Deco Bus Garage and bus ..... £72.21

### Golden Valley Hobbies - Diesel locos

GV2014XS YEC Janus 0-6-0DE shunter British Petroleum green - DCC sound fitted NEW ..... £179

### Hattons Originals - Steam locos

H4-AB14-004 Andrew Barclay 0-4-0ST 14" 1803 in Caledonian Railway lined blue ..... £99  
H4-AB14-001 Andrew Barclay 0-4-0ST 14" 2047 '705' in BR black early crest £99  
H4-AB14-003 Andrew Barclay 0-4-0ST 14" 2134 'Coronation' in lined green ..... £99



H4-AB16-003 Andrew Barclay 0-4-0ST 16" 2226 'Katie' in lined maroon (RRP £99) ..... **BARGAIN** ..... £89



H4-P-001 SECR P Class 0-6-0T 178 in SE&CR full lined green (with brass) ..... £99  
H4-P-015 SECR P Class 0-6-0T 27 in SE&CR full lined green (with polished brass) ..... £99



H4-P-012 SECR P Class 0-6-0T 323 'Bluebell' in Bluebell Railway lined blue (2010s) ..... £99



### Heljan - Steam locos

1323 Class 1366 0-6-0PT 1367 in BR black early emblem ..... £135.96  
4781 Class 47xx 2-8-0 'Night Owl' 4705 in GWR green Great Western lettering ..... £154

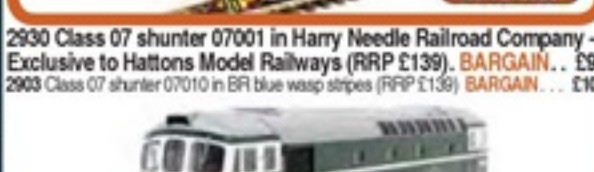


4785 Class 47xx 2-8-0 'Night Owl' 4709 in BR lined green late crest - as preserved ..... £154  
3913 Class 02/3 Tango 2-8-0 63952 in BR black late crest stepped tender (RRP £184.95) ..... **BARGAIN** ..... £89  
3911 Class 02/3 Tango 2-8-0 63954 in early British Railways black stepped tender (RRP £184.95) ..... **BARGAIN** ..... £84  
3922 Class 02/4 Tango 2-8-0 63975 in BR black late crest flush tender (RRP £184.95) ..... **BARGAIN** ..... £84  
3923 Class 02/4 Tango 2-8-0 63982 in BR black late crest flush tender (RRP £184.95) ..... **BARGAIN** ..... £84  
3921 Class 02/4 Tango 2-8-0 63983 in BR black early emblem flush tender (RRP £184.95) ..... **BARGAIN** ..... £89



### Diesel locos

2930 Class 07 shunter 07001 in Harry Needle Railroad Company - Exclusive to Hattons Model Railways (RRP £139) ..... **BARGAIN** ..... £95  
2903 Class 07 shunter 07010 in BR blue wasp stripes (RRP £139) ..... **BARGAIN** ..... £101



3417 Class 33/0 D6504 in BR green ..... £118.96  
3458 Class 33/1 33106 in BR blue ..... £118.96  
3446 Class 33/1 D6580 in BR green small yellow ends ..... £118.96



3530 Class 35 'Hymek' 35017 'Williton' in Civil Engineers 'Dutch' - as preserved NEW ..... £110.46



3529 Class 35 'Hymek' 7001 in BR blue - weathered NEW ..... £118.96  
3528 Class 35 'Hymek' D7004 in BR blue small yellow panels NEW ..... £110.46



3526 Class 35 'Hymek' D7015 in BR green no yellow ends NEW ..... £110.46



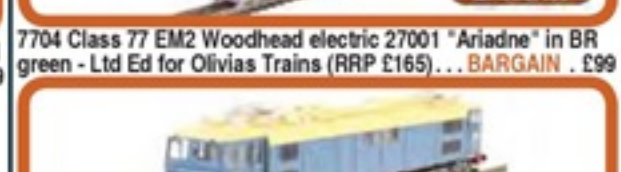
3527 Class 35 'Hymek' D7088 in BR green small yellow panels NEW ..... £110.46



5313 Class 53 D0280 'Falcon' in revised lime green and brown - Ltd Ed (RRP £125) ..... **BARGAIN** ..... £72

### Electric locos

7704 Class 77 EM2 Woodhead electric 27001 'Ariadne' in BR green - Ltd Ed for Olivias Trains (RRP £165) ..... **BARGAIN** ..... £99



7702 Class 77 EM2 Woodhead electric 27004 'Juno' in BR electric blue - Ltd Ed for Olivias Trains (RRP £165) ..... **BARGAIN** ..... £99

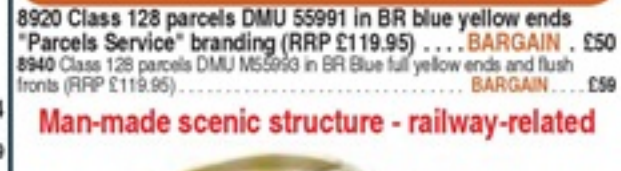


9002 Metropolitan Bo-Bo electric 1 in London Transport wartime grey (RRP £139.95) ..... **BARGAIN** ..... £64

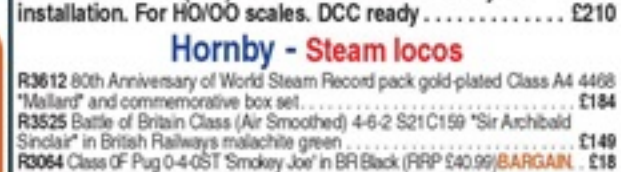


### DMUs

8920 Class 128 parcels DMU 55991 in BR blue yellow ends 'Parcels Service' branding (RRP £119.95) ..... **BARGAIN** ..... £50  
8940 Class 128 parcels DMU M55993 in BR Blue full yellow ends and flush fronts (RRP £119.95) ..... **BARGAIN** ..... £59



89121 27.4 metre (90ft) Motorised turntable - ready for installation. For HO/OO scales. DCC ready ..... £210



### Hornby - Steam locos

R3612 80th Anniversary of World Steam Record pack gold-plated Class A4 4468 'Mallard' and commemorative box set ..... £184  
R3525 Battle of Britain Class (Air Smoothed) 4-6-2 S21 C159 'Sir Archibald Sinclair' in British Railways malachite green ..... £149  
R3064 Class 0F Pug 0-4-0ST 'Smoky Joe' in BR Black (RRP £40.99) ..... **BARGAIN** ..... £18  
R2672 Class 0F Pug 0-4-0ST 272 in Caledonian Railway blue - Railroad range (RRP £28.49) ..... **BARGAIN** ..... £17



R3292 Class 0F Pug 0-4-0ST 56011 in BR black - Hornby 2014 Collectors Club special edition (RRP £24.99) ..... **BARGAIN** ..... £16  
R3213 Class 101 Holden 0-4-0T 3102 in SR Olive Green - Hornby 2013 Collectors club limited edition (RRP £24.99) ..... **BARGAIN** ..... £16  
R3619 Class 4073 'Castle' 4-6-0 5013 'Abergweny Castle' BR green late crest ..... £144



R3333 Class 415 Adams Radial 4-4-2T 30584 in BR black early emblem (RRP £119.99) ..... **BARGAIN** ..... £64



R3170 Class 49xx 4-6-0 4901 'Adderley Hall' in GWR Green - Railroad range (RRP £94.99) ..... **BARGAIN** ..... £49  
R3517 Class 7P 'Royal Scot' 4-6-0 6108 'Sealorth Highlander' in LMS black - 'The Final Day' special edition (RRP £189.99) ..... **BARGAIN** ..... £139  
R3618 Class 7P6F Rebuilt Battle of Britain 4-6-2 34050 'Royal Observer Corps' in BR green late crest ..... £144



R3682 Class 8P 'Princess Coronation' 4-6-2 46225 'Duchess of Gloucester' in BR express passenger blue ..... £184.50  
R3681 Class 8P 'Princess Coronation' 4-6-2 6241 'City of Edinburgh' in LMS post-war lined black ..... £184.50



R3623 Class 8P 'Streamlined Coronation' 4-6-2 6221 'Queen Elizabeth' in LMS coronation blue ..... £171







**UK P&P £4 per order  
£2 per single diecast item  
£7 guaranteed Next Day delivery  
(Orders before 2pm)**



R3555 Class 8P Princess Coronation 4-6-2 46256 "Sir William Stanier F.R.S." in BR maroon... £189  
R3436 Class A1 4-6-2 2554 "Woolwinder" in LNER apple green - gloss finish... £132.50  
R3518 Class A3 4-6-2 108 "Gay Crusader" in LNER apple green - "The Final Day" special edition (RRP £189.99)... BARGAIN... £120  
R3630 Class A4 4-6-2 4493 "Woodcock" in LNER apple green... £144



R3676 Class A4 4468 "Mallard" in LNER garter blue - as preserved... £152  
R3431 Class B19 4-6-0 61533 BR black early emblem (RRP £159.99) BARGAIN... £123



R3430 Class B12/3 4-6-0 8573 in LNER apple green (RRP £159.99)... BARGAIN... £122



R3622 Class J36 0-6-0 65311 "Halg" in BR black early emblem... £126



R3634 Class LN "Lord Nelson" 4-6-0 851 "Sir Francis Drake" in Southern Railway olive green NEW... £153



R3531 Class M7 0-4-4T 30129 in BR black late crest... £132  
R3560 Class Q1 0-6-0 33032 in BR black late crest... £104



R3615 Class W4 Peckett 0-4-0ST 560 in Peckett works leaf green builders lettering... £90  
R3550 Class W4 Peckett 0-4-0ST 883 in Lillieshall Co... £99.99



R3420 Drummond Class 700 0-6-0 30346 in BR black late crest (RRP £124.99)... BARGAIN... £59



R3421 Drummond Class 700 0-6-0 30698 in BR Black early emblem (RRP £124.99)... BARGAIN... £59



R3419 Drummond Class 700 0-6-0 693 in Southern Railway black (RRP £124.99)... BARGAIN... £69  
R3360 Freelance 0-4-0T 87 "Mosley Tarmacadam" in black - Railroad Range (RRP £34.99)... BARGAIN... £16  
R3524 Rebuilt "West Country" 4-6-2 34096 "Trevone" in BR green late crest (RRP £174.99)... BARGAIN... £136  
R3500 The Sir Nigel Gresley Collection (RRP £549.99)... BARGAIN... £474  
R9287 Thomas and Friends - 0-6-0T No.1 Thomas the Tank Engine... £36

#### Diesel locos



R3504TTS Class 08 08623 in DB Schenker - TTS Sound fitted... £139  
R3491 Class 42 Warship D805 "Benbow" in BR green - Railroad Range (RRP £84.99)... BARGAIN... £34.50  
R3666 Class 59/0 59004 "Paul A. Hammond" in Yeoman Aggregates - Railroad Range (RRP £84.99)... BARGAIN... £64



R3657 Class 60 60070 "John Loundon McAdam" in LoadHaul triple grey NEW... £162



R3659 Class 67 67027 "Charlotte" in Colas Rail Freight... £144

#### HSTs



R3685 Pair of Class 43 HST Power Cars 43041 "Meringitis Trust Support for Life" and 43005 in GWR green (RRP £289.99)... BARGAIN... £229  
R3510 Pair of Class 43 HST Power Cars 43187 and 43188 in GWR green - Ltd Ed (RRP £289.99)... BARGAIN... £246

#### Electric locos



R3582 Class 87 87010 "King Arthur" in Intercity Swallow... £144

#### EMUs



R3580 Class 87 87035 "Robert Burns" in BR Blue... £144



R3606 5-BEL Pullman Brighton Belle EMU end vehicles in Pullman umber and cream small yellow panels... £160



R4871 Pack of three centre coaches for 5-BEL Brighton Belle in Pullman umber and cream (RRP £179.99)... BARGAIN... £132

#### Train sets - analogue



R1183 "Master of the Glens" Train Set Class P2 2-8-2 "Cock O' the North" in LNER green and three LNER teak coaches (RRP £184.99)... BARGAIN... £145  
R1151 Caledonian Belle starter trainset 0-4-0 steam loco in blue 4 wheel coach & open wagon (RRP £49.99)... BARGAIN... £45

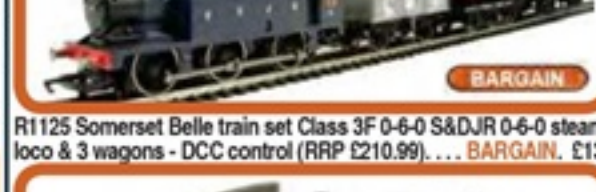


R1176 Class 373 "Eurostar" Train Set... £118



R1167 Flying Scotsman Train Set 3 x LNER coaches (loco has 3-pole motor)... £130  
R9283 Thomas the Tank Engine starter Train Set... £55

#### Train sets - digital



R1172 e-Link Majestic digital train set Class A1 4-6-2 in BR blue and Class 47 diesel (RRP £299.99)... BARGAIN... £250



R1125 Somerset Belle train set Class 3F 0-6-0 S&DJR 0-6-0 steam loco & 3 wagons - DCC control (RRP £210.99)... BARGAIN... £130

#### Coaches



R4873 LMS Stanier Period III coaches in LMS Coronation Scot crimson lake - pack of three - Railroad Range... £64  
R4630 Mk1 TSO tourist second open W3796 in BR chocolate & cream - Railroad range (RRP £22.99)... BARGAIN... £12



R4623 Mk2E FO first open W3244 in BR blue and grey - Railroad range (RRP £25.99)... BARGAIN... £12  
R4660 Pullman Third Class Kitchen Car - K-Type Pullman Car (RRP £49.99)... BARGAIN... £30

#### Wagons



R688TTS 4-wheel 10 ton vent van in BR bauxite - TTS sound fitted... £36



R6365 Breakdown Crane pack 20-ton brake van and 8-plank open wagon - Railroad Range (RRP £32.99)... BARGAIN... £14.50  
R6834 LNER 20 ton 'Toad E' brake van E175712 in BR grey... £19.80  
R6473 Pack of 3 16 ton mineral wagons in BR bauxite - Railroad Range (RRP £19.99)... BARGAIN... £18  
R6887 PTA 102 ton bogie wagon P26552 Yeoman Aggregates grey - Railroad Range... £15

#### Digital controllers



R8214 DCC Elite unit digital controller... £215

#### Digital control accessories

R8232 DCC Electro point clips (20 a pack - 2 used per point) (RRP £6.99) BARGAIN... £3

#### Point motors

R8014 Mark 2 Point Motor (RRP £9.99)... BARGAIN... £5  
R8243 Surface Mounted point motor... £8.50

#### Switches and levers

R044 Passing contact lever frame switch (for point motors) (RRP £11.99) BARGAIN... £6

#### Model railway spares

R0887 Track Cleaning Rubber (RRP £2.49)... BARGAIN... £1

#### Buildings - railways

R8000 Complete station kit platforms, fencing and station building (RRP £29.99)... BARGAIN... £19.50  
R8007 Station booking hall (RRP £26.99)... BARGAIN... £12

#### Man-made scenic structure - railway-related

R083 Buffer Stop... £1.80  
R083 Box of 6... £9.50  
R636 Double track level crossing (RRP £28.99)... BARGAIN... £14

#### Track - Code 100 Setrack

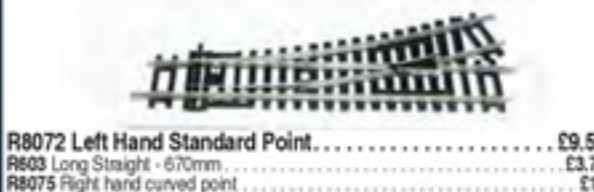
R605 1st radius double curve (8 make a circle)... £2.50  
R605 Circle of 8... £18  
R607 2nd radius double curve track (8 make a circle)... £2.50  
R607 Circle of 8... £18  
R609 3rd radius double curve track (8 make a circle)... £2.60  
R609 Circle of 8... £20  
R8262 Double 4th radius curve (8 make a circle)... £3



R601 Box of 24 Double Straight... £47  
R601 Single... £2



R070 Electrically Operated Turntable... £64  
R8222 Extension Pack B. Contains 1 x R8073, 2 x R800, 4 x R607, 1 x R606, 1 x R603... £26  
R8223 Extension Pack C. Contains 1 x R8073, 2 x R800, 4 x R607, 1 x R606... £24.50  
R8224 Extension Pack D. 1 x R8072, 3 x R605, 1 x R608, 1 x R636, 1 x R603... £41.50



R8072 Left Hand Standard Point... £9.50  
R603 Long Straight - 670mm... £3.70  
R8075 Right hand curved point... £14  
R8078 Right hand Express point... £14.50



R8073 Right Hand Standard Point... £9.50  
R821 Single 970mm length of flexible track... £3  
R821 Pack of 24... £64



R600 Box of 24 Standard Straight... £34  
R600 Single... £1.50

#### Railway publications

R8157 Hornby 2019 Catalogue - 65th Edition NEW... £8.50

#### Kadee - Model railway spares

18KADEE NEM362 Kadee coupling - Medium (8.63mm) - Pack of four... £5

#### Northcord Model Company - Buses

UK6507B ADL Enviro400 MMC - "Bluestar" NEW... £51  
UK0061 ADL Enviro400 MMC - "Reading Buses" NEW... £48

#### Oxford Diecast - Buses

76B1004 Beadle Integral Southdown... £16  
76P0206 Leyland PD812 Stratford Blue NEW... £18



#### Commercial vehicles

76D28003 DAF 3300 Short Van Trailer - "Pollock" NEW... £19.50



76MB008 Mercedes Actros SSC Tipper Ronnie S Evans NEW... £21.50

#### Peco Products - Cables & Wires

PL-80 Power lead joiners - Code 100/124 (x4 pairs)... £6

#### Model railway spares

SL-110 Finescale rail joiners/fishplates (for code 70, 75 & 83) for OO & HO gauge - Pack of 24... £2  
SL-11 Insulated rail joiners/fishplates (for OO, HO & O gauge code 100 rails incl. Hornby, Peco & Peco Streamline) - Pack of 12... £2  
SL-10 Metal rail joiners/fishplates (for code 100 rails incl. Hornby, Peco & Peco Streamline). For OO, HO & O gauge - Pack of 24... £2.20

#### Buildings - railways

LK-20X Double length overall arched station roof... £22.50

#### Man-made scenic structure - railway-related

LK-55 76t Well type turntable... £41

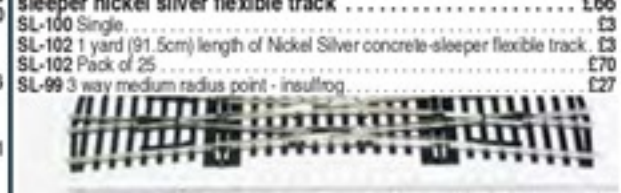
#### Track - Code 100 Setrack

ST-244 Setrack curved double radius right handed point insulfrog... £14.50  
ST-241 Setrack No.2 radius left handed point insulfrog... £9  
ST-240 Setrack No.2 radius right handed point insulfrog... £9  
ST-204 Setrack Quad Straight (670mm)... £3.50  
ST-204 Box of 24... £76

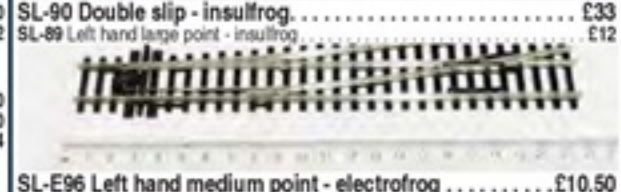
#### Track - Code 100 Streamline



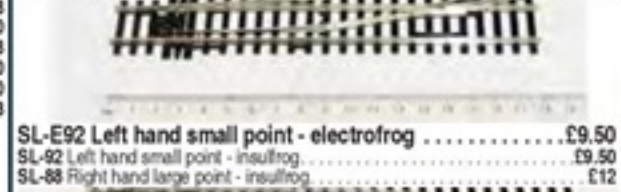
SL-100 Pack of 25 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track... £66  
SL-100 Single... £3  
SL-102 1 yard (91.5cm) length of Nickel Silver concrete-sleeper flexible track... £3  
SL-102 Pack of 25... £70  
SL-99 3 way medium radius point - insulfrog... £27



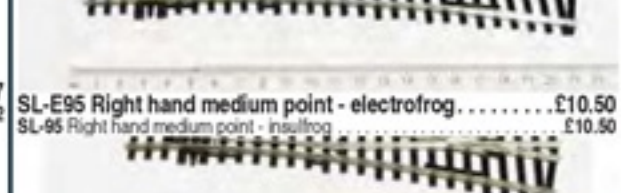
SL-90 Double slip - insulfrog... £33  
SL-89 Left hand large point - insulfrog... £12



SL-E96 Left hand medium point - electrofrog... £10.50  
SL-96 Left hand medium point - insulfrog... £10.50



SL-E92 Left hand small point - electrofrog... £9.50  
SL-92 Left hand small point - insulfrog... £9.50  
SL-88 Right hand large point - insulfrog... £12



SL-E95 Right hand medium point - electrofrog... £10.50  
SL-95 Right hand medium point - insulfrog... £10.50



SL-E91 Right hand small point - electrofrog... £9.50  
SL-91 Right hand small point - insulfrog... £9.50

#### Track - Code 75 Finescale

SL-102F 1 yard (91.5cm) length of finescale Nickel Silver concrete-sleeper flexible track... £3.30  
SL-102F Pack of 25... £82



SL-100F Pack of 25 1 yard (91.5cm) length of Nickel Silver Finescale flexible track... £72  
SL-100F Single... £3  
SL-E190 Finescale double slip - electrofrog... £38  
SL-E189 Finescale left hand large point electrofrog... £12.50  
SL-E196 Finescale left hand medium point - electrofrog... £11.50  
SL-E192 Finescale left hand small point - electrofrog... £10.50  
SL-E188 Finescale right hand large point - electrofrog... £12.50  
SL-E195 Finescale right hand medium point - electrofrog... £11.50  
SL-E191 Finescale right hand small point - electrofrog... £10.50  
SL-U1189 Left hand large radius bullhead rail point - unifrog... £26  
SL-U1188 Right hand large radius bullhead rail point - unifrog... £26

#### Train Tech - Signals

SS3 Sensor Signal - 3 Aspect Home... £36

#### OO9 Narrow Gauge

#### Heljan - Steam locos



Looking for that perfect second-hand model?

[www.hattons.co.uk/latestpreowned](http://www.hattons.co.uk/latestpreowned)



9950 Lynton & Barnstaple 2-6-2T 'Yeo' in L&B dark green £189.95



9951 Lynton & Barnstaple 2-6-2T 760 'Exe' in Southern Railway green £189.95



9952 Lynton & Barnstaple 2-6-2T 761 'Taw' in Southern Railway green £189.95

NEW



9953 Lynton & Barnstaple 2-6-2T E760 'Exe' in intermediate Southern Railway (1927-29) NEW £209.95

Peco Products - Track - Code 80 Setrack



ST-400 Setrack narrow gauge starter track pack - first radius £68

## N Gauge

Dapol - Steam locos

2S-008-008 Class A4 4-6-2 4468 'Mallard' in LNER garter blue valances £128.83

HSTs

2D-019-006 Class 43 HST 4-car book set in East Coast silver £160.89  
2D-019-008 Class 43 HST 4-car book set in GNER blue £160.89



2D-019-009 Class 43 HST 4-car book set in GWR Green £160.89

DMUs



NEW ND116E Class 142 'Pacer' 2 car DMU 142021 in Tyne and Wear PTE £115.18



NEW ND116C Class 142 'Pacer' 2 car DMU 142025 in Northern Spirit £115.18



NEW ND116B Class 142 'Pacer' 2 car DMU 142081 in Regional Railways £115.18

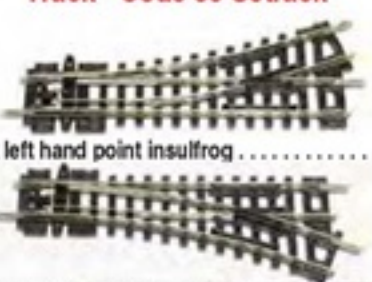
Graham Farish - Diesel locos

371-471 Class 37/0 37261 in Direct Rail Services blue £114.71

Peco Products - Model railway spares

SL-311 Insulated Rail Joiners/Fishplates for N & OO9 gauge (12 per pack) £1.70  
SL-310 Rail Joiners/Fishplates for N & OO9 gauge (24 per pack) £2

Track - Code 80 Setrack



ST-6 Setrack left hand point insulfrog £8.50

ST-5 Setrack right hand point insulfrog £8.50  
ST-3011 ST-11 double straight track - 174mm - pack of eight £12.50

Track - Code 80 Streamline



SL-300 Pack of 25 1 yard (91.5cm) length of Wooden-Sleeper Nickel Silver Flexible track £64  
SL-300 Single £3  
SL-E396 Left hand medium point electrofrog £9.50  
SL-396 Left hand medium point insulfrog £9.50  
SL-E395 Right hand medium point electrofrog £9.50

Track - Code 55 Finescale



SL-300F Pack of 30 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track £84  
SL-300F Single £3



SL-E390F Finescale double slip electrofrog £38  
SL-U396F Finescale left hand medium point unitfrog £10  
SL-E392F Finescale left hand small point electrofrog £9.50  
SL-E388F Finescale right hand large point electrofrog £11  
SL-E391F Finescale right hand small point electrofrog £9.50  
SL-E383F Finescale scissors crossover electrofrog £63

## O Gauge (1:43 Scale)

Corgi Collectables - Cars



NEW VA05212 Ford Granada Mk1 3.0 Ghia, Jade Green £24

Dapol - Steam locos

7S-026-001 Class 3F 'Jinty' 0-6-0T 16564 early LMS black (RRP £225) BARGAIN £159  
7S-026-001D Class 3F 'Jinty' 0-6-0T 16564 in early LMS black - DCC sound fitted (RRP £400) BARGAIN £305  
7S-007-001 Class 57xx 0-6-0T pannier 5717 in BR Black early emblem £199.75



7S-026-002 Class 74xx 0-6-0T pannier 7444 BR black late crest (RRP £400) BARGAIN £295  
7S-010-014 Class A1 'Terrier' 0-6-0T 734 in LSWR green (RRP £225) BARGAIN £179

Diesel locos

7D-008-010U Class 08 shunter in BR blue (without ladder) - unnumbered (RRP £225) BARGAIN £157

Wagons

7F-200-004 20-ton standard brake van CAP B951224 in BR blue £87.96

Heljan - Steam locos

4321 Class 43xx 'Mogul' 2-6-0 5322 in BR black - as preserved NEW £662.15  
4320 Class 43xx 'Mogul' 2-6-0 5372 in GWR green NEW £662.15



NEW 4302 Class 43xx Mogul 2-6-0 4320 in GWR green GWR lettering £662.15



NEW 4300 Class 43xx Mogul 2-6-0 4339 in GWR green Great Western lettering £662.15



NEW 4311 Class 43xx Mogul 2-6-0 4358 in BR lined green early emblem £662.15



NEW 4313 Class 43xx Mogul 2-6-0 5306 in BR unlined green late crest £662.15



NEW 4312 Class 43xx Mogul 2-6-0 5330 in BR lined green late crest £662.15  
4301 Class 43xx Mogul 2-6-0 5355 in GWR green shirbutton emblem NEW £662.15

NEW



4310 Class 43xx Mogul 2-6-0 5358 in BR black early emblem £662.15  
NEW 6126 Class 51xx 'Large Prairie' 2-6-2T 5158 in BR lined green late crest £594.15  
6124 Class 51xx 'Large Prairie' 2-6-2T 5184 GWR green Great Western lettering £594.15  
6101 Class 61xx 'Large Prairie' 2-6-2T 6106 GWR green GWR lettering - as preserved £594.15  
6100 Class 61xx 'Large Prairie' 2-6-2T 6110 GWR green Great Western lettering £594.15

Diesel locos



2015 Class 20 in Railfreight Red Stripe headcode boxes (RRP £599) BARGAIN £427



2558 Class 25/3 in BR blue - unnumbered £446.25



5288 Class 52 D1023 'Western Fusilier' in BR blue (RRP £599) BARGAIN £470



5286 Class 52 D1061 'Western Envoy' in BR maroon small yellow panels £509.15

Coaches

4953 Mk1 BG full brake in BR chocolate and cream (RRP £325) BARGAIN £141  
4956 Mk1 BG full brake in Royal Mail red (RRP £325) BARGAIN £147  
4990 Mk1 GUV full brake in Royal Mail Parcels red (RRP £325) BARGAIN £147  
4941 Mk1 RMB mini buffet in BR maroon - unnumbered £169.50

Wagons

1053 100xx OAA in Railfreight red and grey (RRP £84.95) BARGAIN £35  
1041 200xx VBA sliding door van in BR blue - late (RRP £84.95) BARGAIN £39  
5051 Bogie covered IWB Cargowagon 2797591 in silver and blue - weathered (RRP £189) BARGAIN £79  
1054 ZDA Bass open wagon in Civil Engineers 'Dutch' (RRP £84.95) BARGAIN £39

Oxford Diecast - Cars

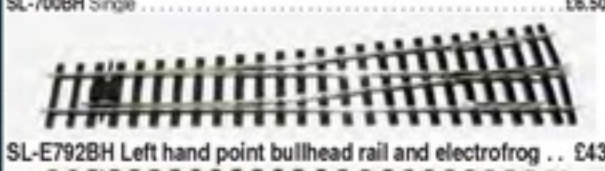
43ACE001 AC Aceca Vineyard Green NEW £21

Peco Products

Track - Code 124 Bullhead Streamline



SL-700BH Box of 12 1 Yard O Gauge Flexible Nickel Silver Track (Bullhead Nickel Silver Rail) £75  
SL-700BH Single £6.50



SL-E792BH Left hand point bullhead rail and electrofrog £43



SL-E791BH Right hand point bullhead rail and electrofrog £43  
ST-725 Setrack Bullhead 2nd radius standard curve 410mm (16") £6

## Any or Multiple Scales

Bachmann Branchline - Digital decoders

36-568 6 pin DCC decoder Back EMF and Railcom £24.61



36-553 8-pin 3-function 0.75A decoder back EMF (RRP £20.90) BARGAIN £12

DCC Concepts - Digital decoders

DCD-Z218-5 ZEN 218 21 & 8-pin 4-function 1.1A Decoder Stay Alive (22x16x4mm) (5 Pack) £85

Digital control accessories

DCD-AEC Cobalt Alpha - 12 way switch control panel £111

Point motors

DCP-CB12IP Cobalt ip slow-action analogue point motor - pack of 12 £169  
DCP-CB1DIP Cobalt ip slow-action digital point motor \* 1 £21



DCP-CB12DIP Cobalt ip slow-action digital point motor - pack of 12 £220  
DCP-CB8DIP Cobalt ip slow-action digital point motor - pack of 8 £118  
DCP-CB120MG Cobalt Omega slow-action analogue point motor x 12 £165

ESU - Digital controllers



50210 ECoS Command Station DCC V2.1 controller full colour LCD touch screen & dual controllers - 6A output £600

Gaugemaster Controls

Analogue controllers



COMBI Combi 12v 1Amp Single Track Controller Transformer £34



D Double/Twin track power controller £80



Q Quadruple 4 circuit power controller £149



M1 Transformer Output 2 x 18v AC 1A £40

Digital decoders

BPDC27 8 & 21-pin 4-function 1A (1.8A peak) small OMNI decoder (measures 23 x 27 x 10d mm) x 5 £79

Digital controllers

DCC49 Prodigy 8 Amp booster unit £169  
DCC14 Prodigy Advance 2 Backlit Walkaround controller £114

Digital control accessories

BPDC80 Pack of three DCC autofrog polarity reversal switches - for electrofrog points £13.50

Point motors

PM-2 Seep point motor £4  
PM-1 Seep point motor accessory switch £5

Hattons Originals - Digital decoders

DCR-21Pin-Direct-V2 21-pin 6-function 1.1Amp direct plug decoder back EMF £16.50



DCR-6PIN-Direct 6-pin 2-function 1.1Amp direct plug decoder back EMF £17



DCR-8PIN-HarnessMini Single £16  
DCR-8PIN-HarnessMini Box of 5 £72

Point motors

HAT-PM-01 Solenoid point motor including pin extension and 2-way arm for use points and signals. Exclusive to Hatton's £6.50  
HAT-PM-01 Pack of 6 £35

Hornby - Digital decoders

X9659 4-pin DCC decoder for Sentinels and Peckett locos £12



R8249 Box of 6 8-pin 4-function 2-sided 0.5A (1A peak) decoder £92  
R8249 Single £16



4-6-2

R8115 TTS DCC Sound Decoder 8 pin plug - 'Merchant Navy' 4-6-2 steam loco £36  
R8110 TTS DCC Sound Decoder 8 pin plug - Class 4073 'Castle' 4-6-0 steam loco £36  
R8116 TTS DCC Sound Decoder 8 pin plug - Class S15 4-6-0 steam loco £36  
R8107 TTS DCC Sound Decoder 8 pin plug - Gresley Class A4 4-6-2 steam loco £36  
R8120 TTS DCC Sound Decoders - Pack of 2 8 pin plugs - Class 43 HST (MTU) £59.50

Digital controllers

R8312PU e-LINK Module, Railmaster Software & 1 Amp European Plug Transformer (PC/Laptop Required) - Unboxed - Split from train set £58

NCE - Digital controllers

524-042 NCE Power Cab 2 Amp Digital DCC controller £154

Peco Products - Point motors

PL-10WE High performance turnout point motor extended pin £7  
PL-10 Point Motor (Switch Machine) £6  
PL-10 Bag of 6 £34



PL-10E Bag of 6 Point Motor Extended Pin (use PL-9) £34  
PL-10E Single £6  
PL-11 Side/surface mounted point motor £8

Switches and levers

PL-13 Accessory Switch for fitting to turnout motor PL10 £3





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# NEW AND FORTHCOMING

A SELECTION OF MODELS RECENTLY AND SOON TO BE RELEASED

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**GWR/BR 61xx 2-6-2T 'Large Prairie' 'O'**

Released!

SRP £699,00

- 6100 - GREAT WESTERN green (pre-numbered as 6110)
- 6101 - GWR green (pre-numbered as 6106)
- 6102 - BR unlined black w. early crest (pre-numbered as 6144(81A))
- 6103 - BR unlined green w. late crest (pre-numbered as 6111(81F))
- 6104 - BR lined green w. late crest (pre-numbered as 6132(81C))



**GWR/BR 43xx 2-6-0 'Mogul' 'O'**

Released!

SRP £779,00

- 4300 - (V1) GREAT WESTERN pre-numbered as 4339
- 4301 - (V1) GWR monogram pre-numbered as 5355
- 4302 - (V1) GWR pre-numbered as 5320
- 4310 - (V2) BR black w. early crest pre-numbered as 4378
- 4311 - (V2) BR lined green w. early crest pre-numbered as 4358
- 4312 - (V2) BR lined green w. late crest pre-numbered as 5330
- 4313 - (V2) BR unlined green w. late crest pre-numbered as 5306



**Class 33/0 V2 - 'OO'**

- 3417 - D6504 in green livery
- 3418 - D6518 in green with full yellow ends
- 3419 - D6540 in green with small yellow panels

**Class 33/0 V1 - 'OO'**

- 3437 - D6558 in blue with full yellow ends
- 3438 - 33056 'The Burma Star' in blue with full yellow ends
- 3439 - 33009 in engineers grey/yellow

**Class 33/1 - 'OO'**

- 3344 - 33114 in Network SouthEast livery
- 3346 - D6580 in green with small yellow panels
- 3358 - 33106 in blue with full yellow ends
- 3359 - 33111 in blue with full yellow ends

Released!

SRP £139,95



**BR/Sulzer Class 25/3 Bo-Bo diesel**

Released!

SRP £525,00

- 2555 - Two-tone green livery with small yellow panels
- 2556 - Two-tone green livery with full yellow ends
- 2557 - Pre-TOPS blue livery with full yellow ends and arrows on cabsides
- 2558 - TOPS blue livery with full yellow ends and arrows centrally on bodysides
- 2559 - TOPS blue livery with full yellow ends and arrows centrally on bodysides (2559 IS IN WEATHERED FINISH)

NINE NEW VERSIONS!



**Ruston & Hornsby/BR class 07 - 'OO'**

Expected Release - Soon!

SRP £139,95

- V1 2904 - D2986 in BR/SR green
- V1 2905 - D2988 in BR/SR green
- V1 2906 - D2991 in 'Eastleigh' works green
- V1 2907 - D2998 in BR blue
- V1 2908 - 07002 in BR blue
- V1 2909 - '07006' in industrial Powell Duffryn blue/white
- V2 2914 - 2989 in BR blue
- V2 2915 - 07009 in weathered BR blue
- V2 2916 - 07005 'LANGBAURGH' in industrial ICI grey



**Class 35 'Hymek' - 'OO'**

Released!

SRP £129,95

- 3526 - D7015 in green
- 3527 - D7088 in green with small yellow panels
- 3528 - D7004 in blue with small yellow panels
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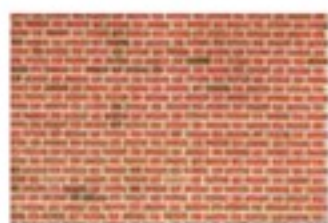
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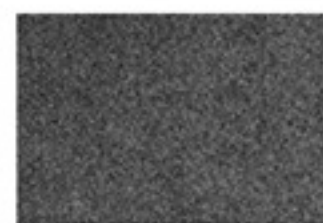
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| R4520B 51ft Gresley Non-Vestibuled Suburban 3rd Coach No. E82190E BR Maroon.....   | £44.99  |
| R4521C 51ft Gresley Non-Vestibuled Suburban Composite Coach E88067E BR Maroon..... | £44.99  |
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| 31-427B Class 411 4-CEP EMU Set 7106 BR Blue & Grey.....                        | £219.99 |
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| 31-980 BR Standard Class 3MT Steam Loco 82020 BR Green L.C. weathered.....      | £88.99  |

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| K2105 0-4-4T Class O2 No. 225 Southern Black (1949)                        | £124.99 |
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| K2201 0-6-0 1361 No. 1361 BR Black L/C                                     | £134.99 |
| K2201A 0-6-0 1361 No. 1361 Photographic Grey                               | £134.99 |
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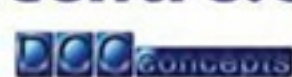
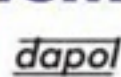


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| R3424 | LNER Q6                  | £125.45 |
| R3425 | BR/EARLY Q6              | £125.45 |
| R3426 | BR Late Q6               | £125.45 |
| R3432 | BR Late B12              | £127.95 |
| R3445 | BR W/C Camelford         | £147.45 |
| R3452 | BR Grange                | £119.25 |
| R3453 | BR Black 5               | £134.00 |
| R3456 | BR N15                   | £136.45 |
| R3462 | BR Class 42xx            | £106.45 |
| R3465 | LNER Class N2            | £82.95  |
| R3499 | BR Hall Class            | £72.45  |
| R3517 | The Final Day Royal Scot | £149.00 |
| R3520 | BR Britannia             | £144.45 |
| R3522 | BR Class A4              | £134.45 |
| R3523 | BR B17                   | £127.99 |
| R3525 | BoB Archibald Sinclair   | £131.99 |
| R3533 | J94 Lord Phil            | £67.45  |
| R3550 | Peckett W4               | £84.45  |
| R3555 | Sir William Stanier      | £161.00 |
| R3557 | LMS Royal Scot           | £142.00 |
| R3559 | Q1 SR                    | £89.45  |
| R3560 | Q1 BR Late               | £89.45  |
| R3565 | LMS 8F                   | £134.00 |
| R3566 | BR Merchant Navy         | £154.95 |
| R3614 | LMS Patriot              | £129.45 |
| R3618 | BR Rebuilt BoB           | £139.45 |
| R3619 | BR Castle                | £129.45 |
| R3630 | Class A4 Woodcock        | £135.99 |
| R3634 | SR Lord Nelson           | £152.95 |
| R3692 | 14xx BR                  | £47.95  |
| R3681 | LMS Coronation           | £184.00 |
| R3682 | BR Blue Coronation       | £184.00 |

## HORNBY Diesel Electric Locomotives

|       |                         |         |
|-------|-------------------------|---------|
| R3261 | Class 08 Network        | £84.45  |
| R3262 | Class 31 D & C          | £142.45 |
| R3268 | CL67 Arriva             | £111.95 |
| R3342 | Class 08 BR             | £104.45 |
| R3343 | Class 08 FGW            | £104.45 |
| R3349 | Class 67 EWS            | £123.95 |
| R3353 | Sentinel Balfour Beatty | £49.45  |
| R3373 | NRM Class 71            | £134.95 |
| R3374 | BR Blue Class 71        | £134.95 |
| R3376 | BR Green Class 71       | £134.95 |
| R3473 | BR Class 56 Rlt         | £135.99 |
| R3482 | Sentinel MSC            | £51.45  |
| R3483 | Sentinel Crossley       | £51.45  |
| R3484 | BR Class 08 Green       | £109.45 |
| R3485 | BR Class 08 Blue Laira  | £109.45 |
| R3573 | Class 66 GBRF           | £79.45  |
| R3577 | Sentinel Graham         | £58.00  |
| R3653 | Class 50                | £137.25 |
| R3658 | Class 50                | £129.25 |
| R3685 | GWR HST                 | £261.00 |
| R3580 | Class 87 BR Blue        | £139.40 |

## HORNBY Train Packs



|       |                                 |         |
|-------|---------------------------------|---------|
| R3607 | 15 Guinea Special               | £270.00 |
| R3606 | Brighton Belle power car pack   | £154.45 |
| R4871 | Brighton Belle Pullman car pack | £179.99 |

## HORNBY TRACK

|       |                       |        |
|-------|-----------------------|--------|
| R083  | Buffer Stop           | £1.65  |
| R207  | Track Fixing Pins     | £2.50  |
| R394  | Hydraulic Buffer      | £4.95  |
| R600  | Straight              | £1.15  |
| R601  | Double Straight       | £1.85  |
| R604  | Curve 1st Rad.        | £1.65  |
| R605  | Double Curve 1st Rad. | £2.05  |
| R606  | Curve 2nd Rad.        | £1.40  |
| R607  | Double Curve 2nd Rad. | £1.95  |
| R608  | Curve 3rd Rad.        | £1.65  |
| R609  | Double Curve 3rd Rad. | £2.25  |
| R610  | Short Straight        | £1.10  |
| R614  | LH Diamond Crossing   | £9.25  |
| R615  | RH Diamond Crossing   | £9.25  |
| R617  | Uncoupling Ramp       | £2.60  |
| R618  | Double Isolating Rail | £6.85  |
| R620  | Railer Uncoupler      | £4.35  |
| R628  | Half Curve 3rd Rad.   | £1.80  |
| R643  | Half Curve 2nd Rad.   | £1.20  |
| R8072 | LH Point              | £7.95  |
| R8073 | RH Point              | £7.95  |
| R8074 | LH Curved Point       | £12.60 |
| R8075 | RH Curved Point       | £12.60 |
| R8076 | Y Point               | £8.95  |
| R8077 | LH Express Point      | £12.60 |
| R8078 | RH Express Point      | £12.60 |
| R8206 | Power Track           | £5.95  |
| R8261 | 4th Radius curve      | £1.85  |
| R8262 | 4th radius double     | £2.65  |

## HORNBY Track Accessories

|      |                         |        |
|------|-------------------------|--------|
| R076 | Footbridge              | £15.10 |
| R169 | Junction Home Signal    | £10.35 |
| R170 | Junction Distant Signal | £10.35 |
| R171 | Single Home Signal      | £7.95  |
| R172 | Single Distant Signal   | £7.95  |
| R189 | Single Brick Bridge     | £7.95  |
| R406 | Colour Light Signal     | £19.00 |
| R574 | Trackside Accessories   | £12.95 |
| R626 | Point Underlay          | £4.20  |
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| R638 | Track Underlay          | £10.55 |
| R645 | Single Level Crossing   | £12.45 |
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|-------|---------------------------|--------|
| R460  | Straight Platform.....    | £3.45  |
| R462  | Large Curve Platform..... | £3.45  |
| R463  | Small Curve Platform..... | £3.45  |
| R464  | Platform Ramp.....        | £3.45  |
| R495  | Platform Subway.....      | £3.45  |
| R510  | Platform Shelter.....     | £10.95 |
| R513  | Platform Fencing.....     | £7.95  |
| R8001 | Waiting Room.....         | £17.95 |
| R8007 | Booking Hall.....         | £24.99 |

## HORNBY Control Equipment

|       |                                  |       |
|-------|----------------------------------|-------|
| R044  | Point Switch.....                | £8.45 |
| R046  | Two Way Switch.....              | £8.45 |
| R047  | On/Off Switch.....               | £8.45 |
| R602  | Power Connecting Clip.....       | £1.65 |
| R8014 | Point Motor.....                 | £6.75 |
| R8015 | Point Motor Housing.....         | £3.99 |
| R8243 | Surface Mounted Point Motor..... | £6.75 |

## HORNBY TTS Sound Decoders

|       |                          |        |
|-------|--------------------------|--------|
| R8101 | Class 31.....            | £33.95 |
| R8102 | Class 37.....            | £33.95 |
| R8103 | Class 47.....            | £33.95 |
| R8104 | Class 60.....            | £33.95 |
| R8105 | Class 67.....            | £33.95 |
| R8107 | Class A4.....            | £33.95 |
| R8110 | Castle Class.....        | £33.95 |
| R8113 | Class 9F.....            | £33.95 |
| R8116 | Class S15.....           | £33.95 |
| R8117 | Princess Coronation..... | £33.95 |
| R8118 | Class 20.....            | £33.95 |
| R8119 | Class 40.....            | £33.95 |
| R8120 | Class 43 MTU.....        | £55.99 |

## BACHMANN Steam Locomotives

|          |                              |         |
|----------|------------------------------|---------|
| 31-001   | Robinson BR Late Crest.....  | £94.99  |
| 31-002   | Robinson BR Early Crest..... | £94.99  |
| 31-003   | Robinson LNER.....           | £94.99  |
| 32-154A  | BR N Class.....              | £99.10  |
| 31-127   | CL3000 BR ROD.....           | £118.45 |
| 31-128   | BR ROD Weathered.....        | £102.45 |
| 31-931   | LMS Compound.....            | £112.90 |
| 31-932DC | BR Compound.....             | £182.50 |
| 31-933   | BR Compound Late crest.....  | £112.88 |
| 31-167DC | L & YR Tank BR Early.....    | £97.50  |
| 31-320DC | J11 BR.....                  | £91.75  |
| 31-465   | C Class BR Black.....        | £93.46  |



|         |                           |         |
|---------|---------------------------|---------|
| 31-980  | Standard 3MT BR.....      | £110.45 |
| 31-088  | BR Earl Early.....        | £118.95 |
| 32-227B | LMS 3F Jinty.....         | £72.20  |
| 32-235  | BR 3F Jinty.....          | £80.70  |
| 32-880  | Fairburn LMS.....         | £106.20 |
| 31-461A | C Class SR.....           | £106.20 |
| 31-462A | C Class BR.....           | £106.20 |
| 31-433  | Midland Class 1F LMS..... | £81.55  |
| 31-434  | Midland Class 1F BR.....  | £81.55  |



|         |                          |         |
|---------|--------------------------|---------|
| 31-204  | Crimson Patriot LMS..... | £131.70 |
| 31-003A | Robinson LNER.....       | £110.45 |
| 31-004A | Robinson BR Early.....   | £118.95 |
| 31-435  | 1F BR Early.....         | £81.55  |
| 31-480  | G2A LMS.....             | £101.96 |
| 31-481  | G2A BR Early.....        | £114.70 |



|          |                             |         |
|----------|-----------------------------|---------|
| 31-119   | BR 4MT 4-6-0 Weathered..... | £135.95 |
| 31-614   | BR Late V3.....             | £110.45 |
| 32-575A  | LMS 4MT.....                | £114.70 |
| 32-176   | BR Crab.....                | £123.20 |
| 32-178A  | LMS Crab.....               | £123.20 |
| 31-170   | L & Y Tank BR Black.....    | £93.45  |
| 31-690   | LMS Stanier mogul.....      | £135.95 |
| 31-318A  | LNER J11 Black.....         | £127.45 |
| 31-321DS | BR J11 DCC Sound.....       | £203.95 |
| 31-691   | BR Early Stanier mogul..... | £135.95 |
| 31-014   | SDJR 7F Black.....          | £125.75 |
| 31-015   | LMS 7F Black.....           | £125.75 |
| 32-881   | Fairburn BR Black.....      | £110.45 |
| 35-050   | LNWR Coal Tank.....         | £101.95 |
| 35-051   | LMS Coal Tank.....          | £101.95 |
| 35-052   | BR Early Coal Tank.....     | £101.95 |
| 31-761   | GNR Atlantic.....           | £169.95 |
| 31-910   | LBSC Atlantic.....          | £169.95 |

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|----------|------------------------------|---------|
| 31-677   | CL85 BR Blue.....            | £85.95  |
| 31-536   | CL105 Speed Whiskers.....    | £84.45  |
| 31-575   | Windhoff MPV NWR.....        | £104.00 |
| 31-576DC | Windhoff MPV NWR.....        | £114.00 |
| 31-577   | Windhoff Railtrack.....      | £104.00 |
| 31-268   | Motor Luggage.....           | £83.26  |
| 31-269   | Motor Luggage.....           | £83.26  |
| 32-937   | Class 150 Centro.....        | £127.45 |
| 31-679   | Class 85 Electric Blue.....  | £118.95 |
| 31-659   | Class 47 BR Blue.....        | £127.45 |
| 32-981   | Class 66 Freightliner.....   | £127.45 |
| 32-982   | Class 66 DRS.....            | £127.45 |
| 32-992   | Wickhams Trolley Yellow..... | £67.95  |
| 32-993   | Wickham Trolley Yellow.....  | £67.95  |
| 32-389   | Class 37 Colas.....          | £117.25 |

## KATO N GAUGE CHASSIS

|        |                      |        |
|--------|----------------------|--------|
| 11-104 | 4 wheel chassis..... | £18.00 |
| 11-105 | Bogie chassis.....   | £24.00 |
| 11-106 | Bogie chassis.....   | £24.00 |
| 11-107 | Bogie chassis.....   | £24.00 |

## PECO N GAUGE TRACK

|      |                                |       |
|------|--------------------------------|-------|
| ST1  | Standard Straight.....         | £1.05 |
| ST2  | Short Straight.....            | £0.85 |
| ST3  | 1st Radius Standard Curve..... | £1.05 |
| ST4  | 1st Radius Half Curve.....     | £0.85 |
| ST5  | R/H Turnout.....               | £8.30 |
| ST6  | L/H Turnout.....               | £8.30 |
| ST8  | Buffer Stop X2.....            | £1.75 |
| ST9  | Power Clips.....               | £2.75 |
| ST10 | Wired Straight.....            | £3.50 |
| ST11 | Double Straight.....           | £1.45 |
| ST12 | 1st Radius Double Curve.....   | £1.35 |
| ST14 | 2nd Radius Standard Curve..... | £1.35 |
| ST15 | 2nd Radius Double Curve.....   | £1.45 |
| ST16 | 3rd Radius Standard Curve..... | £1.55 |
| ST17 | 3rd Radius Double Curve.....   | £1.80 |
| ST18 | 4th Radius Standard Curve..... | £1.90 |
| ST19 | 4th Radius Double Curve.....   | £2.15 |
| ST50 | R/H Crossing.....              | £8.30 |
| ST51 | L/H Crossing.....              | £8.30 |

## PECO 009 GAUGE TRACK

|        |                                 |        |
|--------|---------------------------------|--------|
| SLE491 | Small Radius R/H Point.....     | £9.75  |
| SLE492 | Small Radius L/H Point.....     | £9.75  |
| SLE495 | Mainline R/H Point.....         | £10.50 |
| SLE496 | Mainline L/H Point.....         | £10.50 |
| SLE497 | Medium Radius Y Point.....      | £9.75  |
| ST401  | Setrack Standard Straights..... | £12.35 |
| ST403  | Setrack 1st Radius Curves.....  | £13.85 |
| ST405  | Setrack R/H Point.....          | £9.15  |
| ST406  | Setrack L/H Point.....          | £9.15  |
| ST411  | Setrack Double Straight.....    | £11.55 |
| ST412  | Setrack 1st Double Curves.....  | £11.95 |
| ST413  | Setrack Straight Prewired.....  | £6.75  |

## MINITRAINS HOe LOCOMOTIVES

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|------|-----------------------------------|---------|
| 1023 | Brigadelok Green.....             | £140.00 |
| 1026 | Brigadelok Red Chassis.....       | £140.00 |
| 1027 | Brigadelok Black Chassis.....     | £140.00 |
| 1052 | Schneider Locotracteur Green..... | £145.00 |
| 1053 | Schneider Locotracteur Red.....   | £145.00 |
| 1073 | Baldwin Black.....                | £140.00 |
| 2021 | Ns2f Diesel Green.....            | £115.00 |
| 2022 | Ns2f Diesel Red.....              | £115.00 |
| 2023 | Ns2f Diesel Blue.....             | £115.00 |
| 2024 | Ns2f Diesel Orange.....           | £115.00 |

## HELJAN O GAUGE LOCOMOTIVES

|      |                           |         |
|------|---------------------------|---------|
| 2011 | Class 20 Green.....       | £475.00 |
| 2013 | Class 20 Blue.....        | £475.00 |
| 2014 | Class 20 Blue.....        | £475.00 |
| 2593 | Class 05 Blue.....        | £315.00 |
| 2594 | Class 05 CEGB.....        | £315.00 |
| 5285 | Class 52 Green.....       | £475.00 |
| 5286 | Class 52 Maroon.....      | £475.00 |
| 5287 | Class 52 Maroon.....      | £475.00 |
| 5288 | Class 52 Blue.....        | £475.00 |
| 5290 | Class 52 Blue.....        | £475.00 |
| 6100 | 61xx GWR.....             | £550.00 |
| 6101 | 61xx GWR.....             | £550.00 |
| 6104 | 61xx BR Green.....        | £550.00 |
| 6000 | Class 60 Railfreight..... | £510.00 |
| 6001 | Class 60 LoadHaul.....    | £510.00 |
| 6002 | Class 60 EWS.....         | £510.00 |

## DAPOL O Gauge Locomotives

|             |                                |         |
|-------------|--------------------------------|---------|
| 7D-008-008  | Class 08 Green.....            | £189.95 |
| 7D-008-009U | Class 08 Green unnumbered..... | £189.95 |
| 7D-008-010U | Class 08 Blue unnumbered.....  | £189.95 |
| 7D-008-011  | Class 08 Blue.....             | £189.95 |
| 7S-010-008  | Terrier GWR.....               | £189.00 |
| 7S-010-012  | Terrier BR E/E.....            | £191.25 |
| 7S-010-013  | Terrier SECR 751.....          | £191.25 |
| 7S-010-014  | Terrier LSWR 734.....          | £191.25 |
| 7S-010-015  | Terrier Plain Black.....       | £191.25 |
| 7S-010-016  | Terrier LBSCR.....             | £191.25 |



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|------------|------------------------|---------|
| 7S-025-001 | 74xx GWR.....          | £340.00 |
| 7S-024-002 | 64xx GWR.....          | £340.00 |
| 7S-024-003 | 64xx BR E/E Black..... | £340.00 |
| 7S-024-004 | 64xx BR L/C Green..... | £340.00 |
| 7S-007-001 | 57xx BR E/E Black..... | £199.75 |
| 7S-007-003 | 57xx GWR Green.....    | £199.75 |
| 7S-026-001 | 3F Jinty LMS.....      | £180.00 |
| 7S-026-002 | 3F Jinty LMS.....      | £180.00 |
| 7S-026-003 | 3F Jinty S & D.....    | £191.25 |

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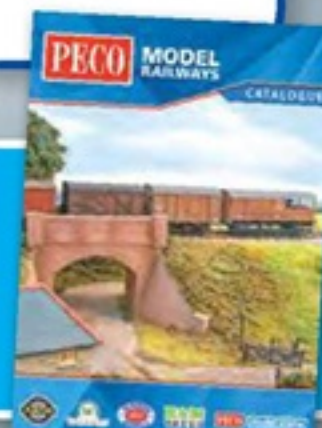


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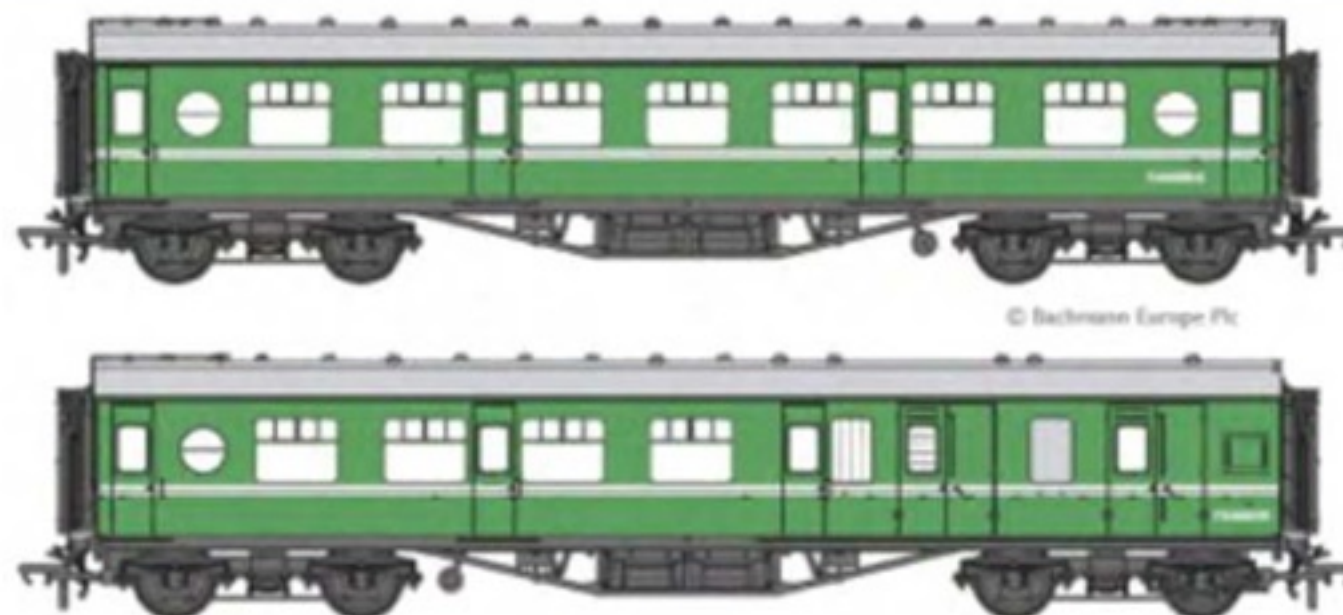
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It also contains selected items from many of the other brands that we stock.

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## PRICES

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| GMC-COMBI   | Single Track Controller/Transformer          | £44.95  |
| GMC-100M    | Single Track Controller                      | £94.95  |
| GMC-100MO   | Single Track Controller for O Scale          | £94.95  |
| GMC-10LGB   | Single Track Controller for G Scale          | £109.95 |
| GMC-10LGBSF | Single Track Controller for G Scale with Fan | £189.95 |
| GMC-P       | Single Track Controller with Simulation      | £104.95 |
| GMC-D       | Twin Track Controller                        | £104.95 |
| GMC-DS      | Twin Track Controller with Simulation        | £189.95 |
| GMC-TS      | Three Track Controller with Simulation       | £189.95 |
| GMC-Q       | Four Track Controller                        | £199.95 |

### Panel Mounted Controllers (Orange text shows transformer required)

|           |                                                           |        |
|-----------|-----------------------------------------------------------|--------|
| GMC-100   | Single Track Controller (GMC-T1/M1)                       | £39.95 |
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| GMC-UF    | Single Track Controller with Feedback (GMC-T1/M1/WM1)     | £44.95 |
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| GMC-UDF   | Twin Track Controller with Feedback (GMC-T1/M1/WM1)       | £64.95 |
| GMC-UDS   | Twin Track Controller with Simulation (GMC-T1/M1/WM1)     | £79.95 |
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| GMTFK1  | Transformer Fitting Kit for T1/T3/T4                   | £15.95 |
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PECO STREAMLINE CODE 124 with Bullhead profile rail features on Dave Hall's 7mm layout Royd Hall Drift Mine. Photo by Derek Shore.

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# Welcome from the Editor

## Northern soul

**T**his issue brings us completely up to date with the 2019 tranche of new item announcements from the big manufacturers. In our news section starting on page 254 you will find bulletins from Bachmann, Oxford Rail and Peco: Hornby's headlines were covered in last month's issue. It is not just the big firms beguiling us with their latest models either. New producers seem to be popping up like spring daffodils these days. Take for instance the pair of brand new PGA hoppers reviewed on page 250. These superb looking OO gauge vehicles have sprung from the forge of another new name in the ready-to-run stakes: Cavalex Models. Moreover, there's more planned from this embryonic firm in the guise of a 1970s BBA bogie steel carrier and N Gauge versions of its debut PGA hoppers.

Our headline review on page 246 this month is of Hornby's rendition of the NBR C Class, LNER/BR J36 0-6-0, and what a pleasure it is to see an R-T-R model of this popular and much loved northern engine against the seemingly endless releases of steam locomotives pertaining to southern climes.

Indeed last year saw promises of other models of prototypes connected with old railway companies north of the M62 - an LNER G5 0-4-4T and a CR Class 812 0-6-0 - and now, on page 255, Oxford Rail has revealed plans to produce an NER P3, LNER/BR J27 0-6-0. It rather looks like the manufacturers have found some northern soul at last.

It's in times like this when I am reminded of our erstwhile editor John Brewer who steered this magazine between 1978 and 2008. Had he still

been with us today, our ears would be ringing with his favourite recitation. "Who would have thought it?"

With all this northern promise and lots of mint off-the-shelf versions of unusual prototypes like the Ruston & Hornsby 48DS shunter, or the 009 Baguley-Drewery 70hp diesel - his sentiment would have been most apposite.

Speaking of "oop no-orth," it is timely to remind ourselves of the Scottish national model railway event - Model Rail Scotland - which takes place across the weekend of 22 - 24 February. This is the second biggest show in the UK organised by enthusiasts for enthusiasts and, as usual, fills Hall 3 at the Scottish Event Centre in Glasgow.

There will be more than 45 layouts on display, two of which are previewed in this issue. Firstly our Railway of the Month, Calcutta Sidings, an atmospheric portrayal of part of the old Midland main line through Burton upon Trent in the 1980s which begins overleaf on page 178, and secondly, City of Tiers, on page 220 - Aberdeen MRC's winning entry in The Great Model Railway Challenge TV show.

Representatives of both Peco Publications and Peco Model Railways will also be attending

with the firm's prestigious stand from which visitors will be able to purchase books and magazines, and view many of the firm's products together with pre-production samples of some of the new items due in 2019.

Whether you're an inveterate builder of kits and models from scratch, or a spirited user and collector of proprietary models, there's quite clearly lots and lots going on in this hobby - enjoy!

Steve Flint

You can keep up to date between issues by visiting our new website at [www.peco-uk.com](http://www.peco-uk.com) and our Facebook Page. You can also now follow Steve on Twitter @steveRMed



**Model Rail Scotland is the UK's second biggest model railway event organised by enthusiasts for enthusiasts. It runs from 22 - 24 February at the SEC in Glasgow.**

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We welcome submissions on all aspects of railway modelling, be they layout projects or suggestions, constructional articles, advice and encouragement for beginners, prototype features or what-have-you. We prefer material to be sent through the post to the address below left, on CD-R if digital images are involved, but short introductions to the article involved and a few sample photographs can be emailed in the first instance to [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

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**CONTINENTAL MODELLER**

Published on the third Thursday of the preceding month.



# Railway of the Month

Photography by Tom Silsbury

Class 56 No.56 032 Sir De Morgannwg/County of South Glamorgan working a train of SSA scrap wagons past one of the stacks of beer barrels which, confides the author, took as long to build as the typical small layout!

Full and empty merry-go-round trains cross on the main lines. No two HAA hoppers are alike; all have subtle differences.



## Model Rail Scotland

See the extended layout at at Model Rail Scotland at the Scottish Event Centre in Glasgow, 22 – 24 February 2019.

Full details in our Societies and Clubs section on page 261.



# Return to Calcutta Sidings

Doubling the length of a 4mm scale exhibition layout

A good decade on from its previous appearance in *RAILWAY MODELLER* (February 2007), **PHIL EAMES** undertook an extensive reworking of this 4mm scale layout, set in the area around Burton upon Trent. The changes were major – it was doubled in length to 40' – and minor – the gauge was widened from EM to P4 to be compatible with layouts being built by his fellow operators. The decision to employ digital control was made at the outset of this Mk.II version of Calcutta Sidings.

**A** long hard look was taken at the original *Calcutta Sidings* layout. Although it was working reasonably well at exhibitions, there were flaws in the track layout which made operating more troublesome than it needed to be. Also the baseboards, which were built to the lightweight design that was fashionable at the time of construction, were warping more than was acceptable. So, the decision was made to stop accepting any

more invitations and to start building a new layout before things got much worse.

After much discussion with friends Jim and Tim, it was decided to build a fiddle yard which would be common to each of our layouts. Jim is building a model of Birmingham New Street, while Tim is planning to build a model of Tring. As for me, there was unfinished business with *Calcutta Sidings*, with a number of rolling

stock projects that I still wanted to do. So the idea of a new version of the layout was formulated.

The baseboards are more substantial this time around, being 9mm ply tops with 6mm ply double spaced sides. The ply itself is Scandinavian birch which consists of distinct

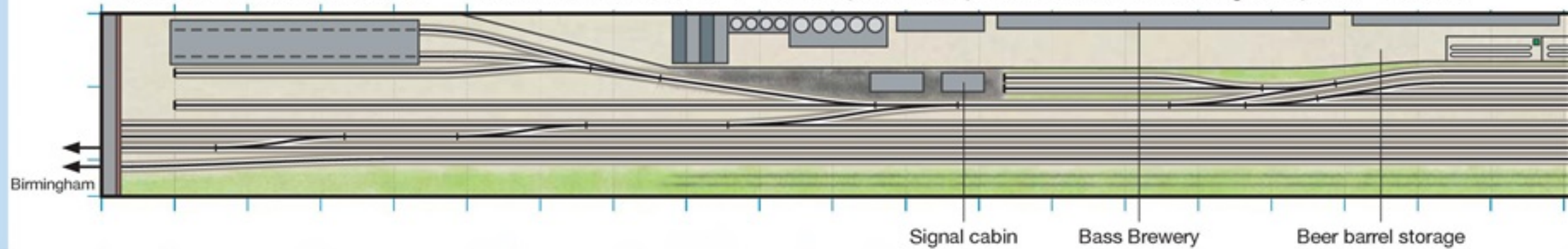
**A Railfreight Distribution Class 37 is seen at the left-hand end of the layout with mixed freight, passing a Birmingham-bound Sprinter.**





## CALCUTTA SIDINGS

Overall dimensions: 40' 0" x 2' 6", plus fiddle yards at each end. Each grid square = 1ft x 1ft.



layers of ply, rather than the run-of-the-mill ply from the DIY chains which consists mainly of sawdust and resin for the inner layer. We arranged to meet up once every month or so for a weekend of intense woodwork, and the boards were gradually constructed over a period of years. Some of the early boards are now nearly 15 years old and show absolutely no signs of warping, which is a great relief.

While doing this redevelopment the opportunity was taken to double the length

of the scenic section, as well as providing a much-needed increase in capacity in the fiddle yard. In addition, the decision was made to convert from EM to P4 gauge. There had been consideration to converting the original layout to DCC, but this was dismissed as being a waste of time due to it having a limited life. The new layout was designed for DCC from the start.

### A time and a place

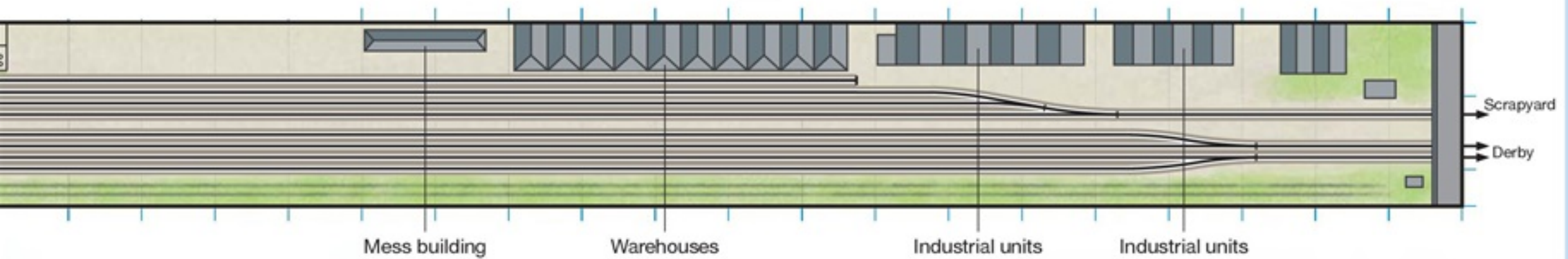
It is only the foundations that have altered;

what remains the same is the area and era that the layout depicts. This is 'somewhere' north of Burton upon Trent, but south of Derby. If you recognise the traffic that would pass through Stenson Junction, then so be it. The era portrayed is 1989 – 1991, after the demise of the Class 45s, but before the widespread use of the Class 60s due to their late delivery.

This era also shows the last years of the Speedlink wagonload service. The assumption is that the workings to the Burton breweries did not end in 1974, but hung on a little longer. Then with the creation of the Speedlink network, traffic actually increased. All the workings shown are associated with







the brewing industry and existed on the railway at the time. This traffic is serviced by a trip working from Bescot yard.

The year 1989 saw the creation of the Metalslink network. It was an equivalent to the Speedlink network for wagonload traffic in the steel sector. Tinplate for the making of beer cans is brought in from Velindre Steelworks, as well as coiled wire from ASW in Cardiff. In return there is scrap going to ASW. This traffic is serviced by a working from Cardiff Tidal sidings, which gives a great excuse to use a 37/9 loco.

It was the creation of the Metalslink network that led to the withdrawal of the 08 shunter at Calcutta Sidings as neither sector would pay for it. Consequently, all traffic is shunted by the loco from the respective incoming working. This is a blessing on the model as the locos used are more reliable than a model 08, due to having more pickups and not having shifted cranks, the latter being a very prototypical problem.

### Six different types of trackwork

When constructing the trackwork on the scenic section, which is all handbuilt, a variety of components and methods were used. The concrete sleepers flat bottom



track on the main line is by Exactoscale, which was supplied as a 'sprue' onto which the track is threaded. However, the sleeper spacing is what you would expect on a lightly-used branch line or siding, rather than a 90mph main line. So, the sleepers were cut from the sprue and threaded onto the track individually using a jig, constructed from card (which had the correct sleeper spacing cut out from it) and laminate board.

The flat bottom pointwork and plain wooden sleepers track were obtained from

▲ The signal cabin controlling access to the yard, with a bothy alongside.

the Colin Craig range, which was developed for use on the layout. These use the copperclad method of constructing track. The base of the rail is located on an etched brass spacer to give the height that a baseplate would in reality. Those for the pointwork provide the correct vertical alignment, with those on the plain track giving the rail a 1:20 inclination. Both types



▲ Full length HSTs are lost in the scale 3000' scene. Formations are the correct single first class coach for the Midland main line.





▲ Careful attention was afforded to the permanent way to replicate the correct sleeper types, spacings, chairs and other rail fixings.

were then finished off with the correct sort of cosmetic chair, which differs between use.

For the bullhead track C&L components were used on ply sleepers. Brass rivets were employed every sixth sleeper to give some

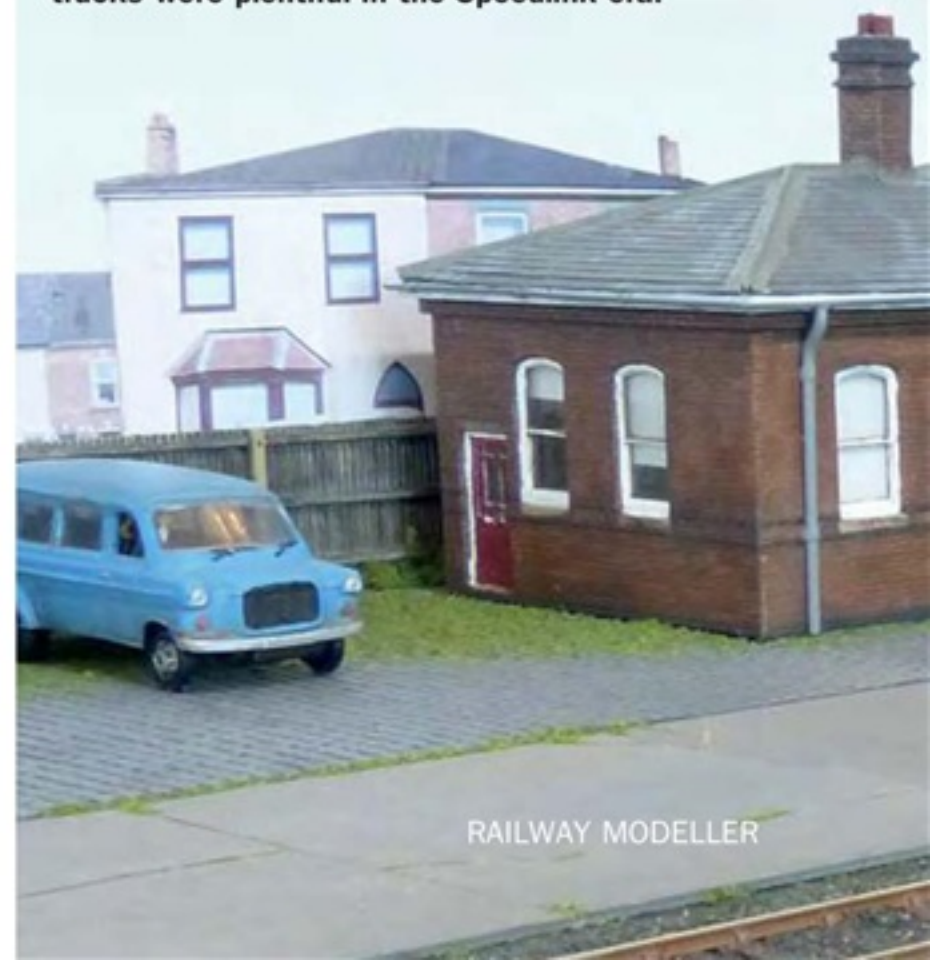
extra strength. Again, the sleeper spacing is important, with those on the goods lines being closer together than sleepers in the yard itself, as per the prototype. All the trackwork was ballasted using Woodland Scenics N gauge products and heavily weathered. In total there are six types of trackwork on the layout to try to replicate what can be seen on the real railway.

## Digital control

As you can imagine with a layout of this size, we exceeded the ad-man's cliché of only needing two wires. We split the layout into two halves with a power box for the scenic section and another one for the fiddle yard. These were connected to the command bus with one being set up as a slave to the other.

Then the scenic section was split into four power districts, three for the running lines and one for the accessories, while on the fiddle yard it is five power districts with the extra one being for another running line. Each power district has its own pair of 14 SWG bus wires, linked from board to board by push fit connectors. Under the board itself, each bus is connected to a tag strip, to

▲ A double-headed cement train thunders past the brewery plant. The yellow BR work crew trucks were plentiful in the Speedlink era.





A close-up of the barrel storage area and the brewery's pipework. Palisade fencing like this is available from the Wills Modern range.

which each section of rail's 26 SWG dropper wire is terminated. The wonderful thing about DCC is that by using the same pin numbers for the buses, any power box can be plugged into any board for testing purposes.

Circuitron Tortoise point motors mounted on Exactoscale bases are used to control the points. In turn these are controlled using Lenz LS150 accessory decoders in the fiddle yard and MERG accessory decoders on the scenic section. This was done simply due to the number of outputs each decoder had. The up and down lines each have a laptop to operate the respective routes. At an exhibition to get the train movements flowing nicely, each laptop has a signaller to set the routes, with two people to operate the trains.

There is a stand-alone operator for the yard itself. This person will select which points to operate using the hand-held controller. All the stock that uses the yard is fitted with Alex Jackson automatic couplings which have been found to be reliable in service. These are operated by electromagnets that are actuated using solid state circuitry. The MERG accessory decoder has been uprated to give a 5V output which is connected to the base of a thyristor. This switches the 12V, 5A full wave rectified dc current to the electromagnet. There is a timer on the signal of 2.55 seconds which is found to be sufficient time for the couplings to operate.

The control system used is Digitrax, which was chosen purely due to Tim having some of the equipment already. Originally, we plugged the controllers into sockets on the baseboards for control. But by accident, Jim bought some wireless Digitrax equipment. This was a revelation and soon our wired controllers were sent across the Atlantic to be converted to wireless ones. Mention must be made of Kevin at Coastal DCC who have saved our bacon on more than one occasion, thank you.

### Signalling via Arduino

The layout is controlled by multiple aspect signalling, the gantries being etched brass kits prepared 'in house' with thanks to the



person who supplied the prototype drawings. After discussion with my 'electronics mentor' Simon Stevens, it was decided to use an Arduino to control these. An Arduino is an electronics board which reads an electrical input to create an output.

Put simply the layout uses an NCE BD20 to detect that a train is in a particular section. The signal from the BD20 is fed to an Arduino, which will turn the colour lights from green to red. The Arduino can be programmed to time the lights through yellow, double yellow and back to green. Following on from this we can use two detectors, the first one to set a timer running, the second one to stop the timer and switch

the lights to red. Then depending on how fast the train is running, the lights can be cycled back to green just like the prototype.

### Operation

On the main line there is a variety of passenger and freight workings reflecting those seen passing through Stenson Junction in the era portrayed. The formations of these are modelled as faithfully as can be using my own images taken at the time, plus those from magazines, books and the internet. With a scenic section now being 40' long

The crew of No.37 238 are on their break time in the mess room. All stock performing shunting manoeuvres have Alex Jackson couplings.







◀ A two-car Class 108 rasps its way past the Bass brewery building. The real things were built just along the line, at BR Derby Works.

that some of the covers for the hopper operating gear is angular on some wagons and rounded on others. Also after 25 years of service during maintenance, different works have applied the TOPS panel in different positions. When all these variations are put together, no one wagon looks quite the same as another. This results in something resembling the prototype rather than looking as if they have just all come out of a box from China. Mention must be made of Paul Bartlett's website, without which the wagons would not quite be the same.

### Scenery, buildings – and barrels

The approach to the scenery was aimed at producing a scene in an industrial landscape typical of the Burton upon Trent area. This meant mixing the old with the new, where some of the early 20th century buildings were still in use; some had been converted for new purposes, while in some places land had been reclaimed for more modern buildings. The vast majority of buildings are from the Burton area or within a 25 mile radius of it.

The buildings reused from the previous version of the layout were released from the baseboard in minutes with the deft use of a bread knife. They had been fixed in place using clear silicone sealant which gave a good grip, while being resilient to the jolts of transport to and from exhibitions over the years. When the time came to remove them, there was a feeling of trepidation...

I am not that keen on modelling buildings and in particular bridges, as wagons are higher on my priority list. Tim came to the rescue by making a model based on the bridge on Horninglow Road for which I was very grateful. Tim also built the Bass bottling plant and the offices that are next to the bonded warehouse.

There has been a shift in technology with regards to constructing buildings. One of the last buildings to be made using traditional methods was based on the Kings Brewery and New Brewery buildings on the corner of Station Street and Cross Street in Burton. A lot of research went into that building using the Coopers Tavern across the road as a base – a real hardship!

compared to the original 20', rakes of prototypical length can now be depicted.

It was during the mid 1980s that I felt that the passenger side of the prototype railways was getting very 'samey'; this sparked my interest in freight workings and wagons. Articles by Bob Wallace and Roger Silsbury in the press outlined how the freight railway was developing. The number-crunching books from Metro which were published around the time, while not infallible, are still of use today. Hence the proliferation of freight workings on *Calcutta Sidings*; it is the passenger workings that get in the way!

The rolling stock on the layout is a veritable who's who for model railways. It would be pointless to write a list out. If someone makes an item that is suitable, it gets used. However, there is nothing running

out of the box on the layout. All the stock has been modified, weathered and renumbered using the information obtained from the various sources.

Perhaps the best example of the approach to creating a rake of wagons is the humble HAA of which there are two rakes on the layout. One of these rakes has a brake van at either end which signifies the originating point as Denby Opencast loading point, so there is a difference to start with. Then there is the rebodding programme in progress during the period modelled: variations between individual wagons results in original body wagons with a very weathered bauxite frame and some with a Railfreight red frame which ranges from very weathered to pristine.

Next there are those HAAs which have been rebodied. This is denoted by the row of rivets along the top of the body on the outside with a reinforcing plate along the inside. These wagons have frames painted in

Railfreight red that run from very weathered to pristine; Railfreight yellow with weathering moderate to pristine; and even one in bauxite livery that was extremely weathered.

Another variation to take into account is

Metalslink traffic, such as this shipment from South Wales, allows a Class 37/9 to appear. Shunting with main line power and not 08s was instigated in 1989.





A rake of HAAs has been looped to allow a faster train to pass. The brake van denotes the train as heading for reloading at Denby Opencast; marshalling a van at each end speeded up reversals.



Stock from all the main manufacturers is used, suitably rewheeled; here a Hornby Class 31 leads a Bachmann inspection saloon towards Derby.

The advent of laser cutters has revolutionised making buildings in recent years. No longer do we have to make a carcass, put an overlay of embossed/formed plastic onto it and hope the paint will take to the plastic. The carcass is now the outer layer with the brickwork pattern cut into it with great precision (providing the drawings are correct of course). This has made working on buildings much more enjoyable and I have warmed to the subject now.

There are two stacks of beer barrels on the layout which, if I had known how long they were going to take to make, the space would have been filled with something else.

After making a master of the beer barrels, a number of moulds were made. Even though the stacks only have barrels on the outside of a former, they still took a long time to cast, fettle and paint. The separators between the layers of barrels were made out of three pieces of laser-cut thin card laminated with cyanoacrylate adhesive which again took an age to do. Then everything had to be assembled. I am sure a small layout could

have been built in the same time as the barrels! The upside is that viewers of the layout make very favourable comments regarding them.

### Exhibition feedback

Exhibiting model railways is not a one-way experience, in that we as operators are not just there to go through the motions during an exhibition. It's a two-way process with feedback from the visiting public. It is surprising that the layout seems to create the same reaction that people have to Burton's second most famous product, Marmite. They either love it or hate it, there seems to be no middle ground. One of the criticisms levelled at the layout in certain places is that it is too straight. But I just think these people have never been to Burton: the railway line was built in a straight line along the Trent Valley for 6½ miles. Why would I put a curve in? It just isn't prototypical.

One well-known exhibition manager attended the DEMU Showcase event in Burton in 2015, which was held at two sites.

Instead of taking the courtesy bus, he decided to walk between the two venues alongside the railway. He said to me later "I suddenly got what *Calcutta Sidings* was about, it was if I was in the layout".

### Conclusion

For me the hobby of railway modelling is about people as much as about the modelling. The layout could not have been built without a lot of people wittingly or unwittingly helping. It would be invidious of me to mention the names of anyone in particular, they all know who they are. Whether they have only operated for one day or have attended every exhibition, it is all appreciated. But it doesn't mean I don't get a little exasperated occasionally, as it can be like herding cats now and then. But the vast majority of the time it's good fun and can be an absolute hoot in the evenings.

If you have any questions on the day please ask someone behind the layout – though be considerate if they are actually operating at the time.



# The Amlwch branch

Prolific OO layout builder

**ROB OGDEN** recently revealed in the January issue of *RAILWAY MODELLER* news of his conversion to O gauge modelling. He chose the terminus of this long-lasting branch on Anglesey as a suitable prototype and shares with us his prototypical track plan which is also suitable for the smaller gauges.

Model photography by the author

Regular readers will remember my previous layouts featured in *RAILWAY MODELLER* over the last few years, all built relatively quickly and dismantled shortly afterwards so as to start again. Not the inclination of many who build layouts themselves, I know, but I'm a restless modeller, and love to be building something.

If you read my comment piece in the January 2019 issue, you will have discovered that I am returning to a first love: O gauge. Yes my next model railway is being built in 7mm scale, the modelling scale I started out with many years ago.

The layout is under construction in my layout room – the loft of my house – and this article sets out my rationale for selecting the former branch line to Amlwch in Anglesey.

I am firmly wedded to BR/LMR and prefer ideally to model real locations. I wanted a branch line with rather more variety and operational interest than just local branch passenger trains and the daily pick up freight.



▲ Grimy Ivatt Class 2 2-6-2T No.41226 in charge of the branch train on 27 July 1964. Photograph: A J B Dodd/Rail Photoprints

Amlwch, the terminus of the Anglesey Central railway, which left the Bangor to Holyhead main line at Gaerwen, fits the bill for a number of reasons. Most of the local passenger trains ran to Bangor and, by the late 1950s, were DMU operated although there was a return to pull-push sets in the 1963 and 1964 summer timetables, as better use was found for the DMUs elsewhere.

Reference to the associated layout plan shows that the loco water supply was at the end of the former MPD siding, the shed itself having been demolished in the late 1930s. The pull-push fitted locos were marshalled at the Bangor end of the train, so the entire set ran to the water tower and back between arrival and departure. Some trains were of corridor stock and the locos for these ran round in the normal way, calling at the water tower on their own.

Freight was mainly feed, fertiliser and other agricultural supplies inwards, and produce outward. There were several decent sized

warehouses at intermediate stations along the branch, as well as at Amlwch itself. Some of the yards faced down, and others up so there were usually two daily goods trains Monday to Friday (one on Saturday) to and from the marshalling yard at Menai Bridge. As a result, one of these goods trains did little more than run round at Amlwch.

In 1953 the Associated Ethylene Company (later Associated Octel) opened a large chemical plant at Amlwch which required weekday trains of inbound chlorine in tank wagons. This was used to extract bromine from sea water in the production of ethylene dibromide, which in itself was tanked out to Ellesmere Port for use as a petrol additive in the days when leaded fuel was commonplace. A private line ran about three quarters of a mile from the BR metals, routed

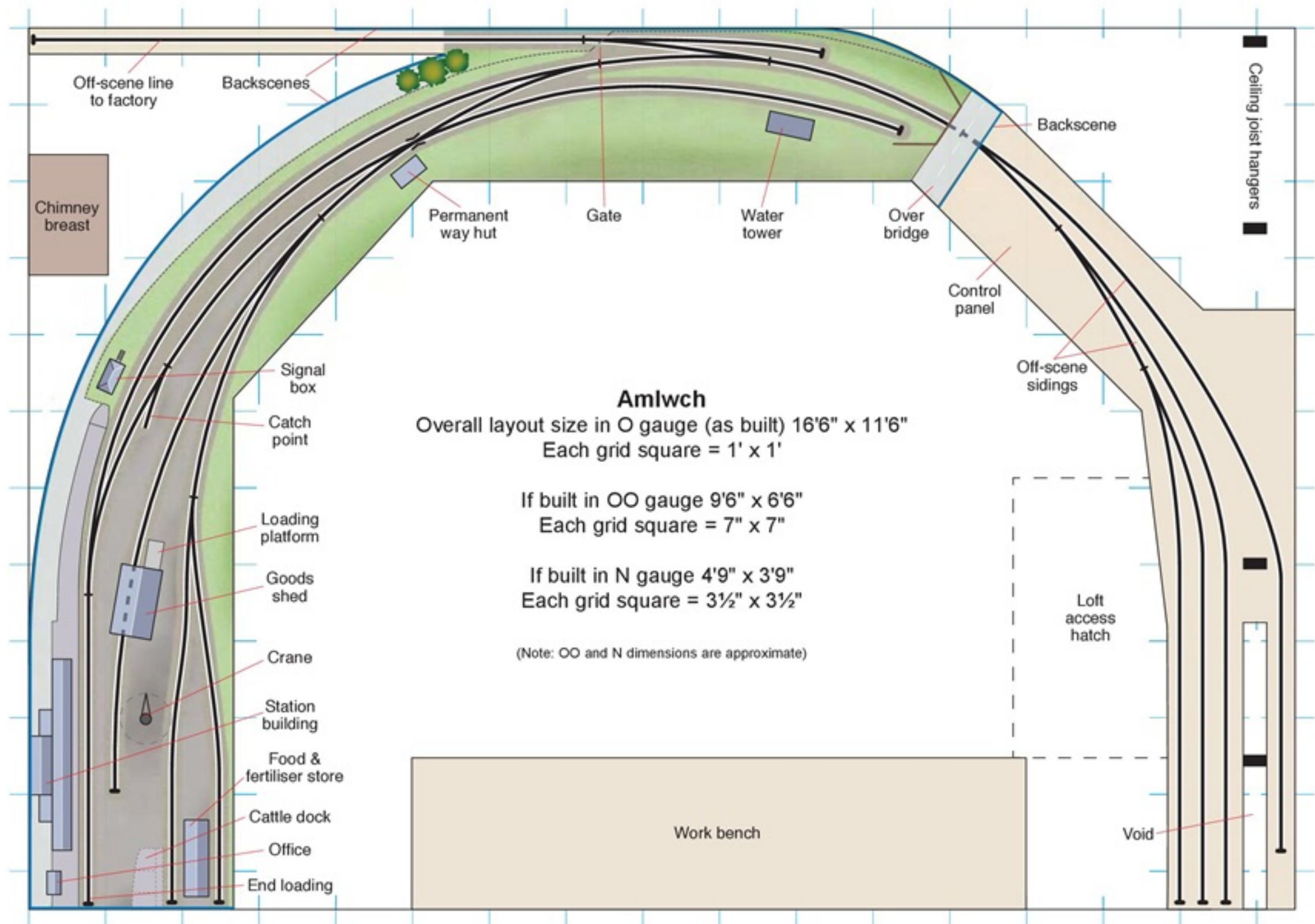


▲ The junction for the industrial branch is designed to disappear quickly off-scene with the exit disguised by some suitably located trees, as seen here. The water tower is seen in the bottom right of the picture.



▲ The goods yard area and goods shed is taking shape here. The trackwork was formed using Peco bullhead items. A painted backscene is used here to represent the town.





through the town itself. Notably steam locos were not permitted over this line. Stock was shunted to a fan of sidings adjacent to the BR station and the factory's own diesel shunter hauled vehicles to and from the plant. The traffic continued for many years even after Doctor Beeching closed the line to passenger traffic in 1964. The factory eventually closed in 2003.

### Plenty of scope for operation

Branch lines are notorious for limited operating potential, but Amlwch is different. Firstly all train departures, passenger and freight, ran outward from the platform road, the signal cabin containing the single line apparatus being adjacent to it, just prior to the starting signal. Secondly, for several years, within the late 1950s and early 1960s summer timetables, seven passenger trains ran each way Monday to Friday, interspersed with the additional freight services described above. One additional passenger train ran on Saturdays but only one goods train and there were no trains on Sundays.

On a recent visit I found most of the branch still in place, even through the town on the old industrial section to the former factory site, although most of the track was rusty and mainly very overgrown.

Amlwch station building and platform is no more, but the goods shed remains.

There have been a number of attempts to reopen the line, none of which have

succeeded to date. However, for my purposes the station and yard have remained open beyond 1964 to give me an excuse to run heritage diesel locos on BR trains as well as trains to and from the factory.

### Loco and stock potential

Regarding motive power, quite a variety appeared over the years, mainly sourced from Bangor MPD. I model mid 1950s to mid 1960s. At the start of this period Ivatt Class 2 2-6-2Ts ruled supreme until a change to the freight diagrams around 1958 meant tender engines with higher water capacity were needed. Both BR and LMS versions of 2MT 2-6-0s were the usual engines of choice but Bangor's ex L&Y Aspinall 0-6-0s were occasionally rostered.

BR class 2-6-2Ts appeared when pull and push trains returned in 1963 and, following clearance trials with a Stanier 2-6-4T in 1964, larger engines appeared on the chemical tank trains. These included other ex LMS 2-6-4T classes, Stanier Class 5s and BR Standard 4MT 4-6-0s. I have a particular liking for Collett 0-6-0s and following the transfer of lines west of Shrewsbury from WR to LMR in 1963, number 3208 of Machynlleth shed served as Llandudno Junction's snow plough engine that winter. With a bit of modeller's licence maybe it could have run on the Amlwch branch?

Diesel classes, mainly 24s and 25s, took over at the end of steam whilst Class 40s

often associated with the tanker trains appeared later.

### The plan

So, quite a heavy building programme ahead of me so that I can acquire all the locos and stock which would have run on the line in the period I intend to model: but I love the process of construction and I have made quite a bit of progress already. The layout infrastructure is hitherto in place, in the form of a large curve. The attached plan shows the layout shape, designed to maximise the radius in the available space. It is also reproduced to several scales, should anyone like to make use of the scheme themselves.

I hope to follow up with articles on progress at some stage in the future, so watch this space.

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# Little Salkeld

Modelling and words: **PAUL MOSS** Photography: **TONY WRIGHT**



It was a combination of interests that led to the building of *Little Salkeld*. These included an admiration for the Lake District and surrounding area, a growing interest in the Settle & Carlisle line, and the challenge of building an exhibition layout based on a prototype to the best standard that I could achieve. After joining the East Bedfordshire Model Railway Society – a great bunch of railway modellers who work to a high standard – I felt informed and inspired to start work in early 2016. I can thoroughly recommend joining a club if you can as it can really set you off on the right foot.

## Design considerations

The Settle & Carlisle (S&C) was a great choice with a main line route, scenic backdrop and a number of stations, many of which had straightforward track layouts. A bit of research suggested a few options, and I settled on Little Salkeld as it was in the early 1960s. This was a small rural station servicing its local village situated about half a mile away. Never one of the busiest stations on the S&C line, it did however have a goods yard which dealt with a range of mixed freight, albeit with limited movement of livestock, and a layby siding for slower running trains. Goods traffic ceased in 1963 after which the goods yard was removed. The station continued on for a few years but succumbed in 1970 when passenger traffic

**“ – N gauge ready-to-run stock has improved markedly in recent years – ”**

Paul Moss

also ceased. Today, the platforms remain in position, as does the main station building which is now a private residence.

N gauge was selected, as that enables decent length train formations to be run which I think is important on a layout depicting a line such as the S&C. In addition, N gauge ready-to-run stock has improved markedly in recent years and a little extra detailing or weathering can produce quite realistic models.







▲ Metcalfe card kits for the standard S&C structures on the line were used for *Little Salkeld*, modified with glazing scribed by a computer-driven cutting machine to create the leaded effect.

◀ General view of the station, with a Class 108 making a call at the northbound platform.

### Choice of track

Some time was spent considering which type of track was to be used, and the final choice became an important influence for my approach to a number of aspects. I opted to use finescale track as I wanted to follow the shape of the prototype closely, which is on a sweeping curve. I'm going to expand on this aspect as the use of finescale track appears to be less common on N gauge layouts.

British Finescale code 40 track components (fiNetrax) have been used together with handbuilt points in the scenic section. Peco code 55 Streamline track was also selected for reliability and speed of fitment in the non-scenic areas and the fiddle yard. Modern R-T-R stock runs fine on code 40 track, however I have found that the back-to-back wheel dimensions of all stock must be maintained for smooth running, particularly over pointwork. Tackling an exhibition layout with finescale track has certainly proved to be a challenge.

Good quality beech ply baseboards have been used and much care was taken to assemble them on a flat surface. Equally important are the baseboard joins and cabinet makers' dowels have been utilised to ensure consistent and accurate alignment. As N gauge is relatively new to me, and it is a good 30+ years since I last scratchbuilt pointwork in 4mm scale, I took the decision to engage with Keith Armes who advertises a track-building service on the 2mm Scale Association website. Keith did an excellent job converting the track layout featured in *Stations & Structures of the Settle & Carlisle Railway* by V R Anderson & G K Fox (OPC, ISBN 0 86093 360 1) to a representative Templot plan. He then built all the scenic







▲ The station master's house was situated on the west side of the line, overlooking the station and goods yard.



▲ In contrast to the stone-built waiting shelters, those at Little Salkeld, New Biggin and Langwathby were made of wood.

section pointwork to match. The pointwork employs etched chairs which are soldered onto copper-clad sleepers. I assembled the rest of the track components using a jig also supplied by British Finescale. Engaging Keith's assistance proved to be a real boon for ensuring quality and saving time.

To ensure stability further, I employed 2mm Scale Association brass sleepers soldered to copper-clad PCB material where trackwork needs to cross baseboard joins. These were screwed down into position and insulation breaks added before ballasting. They have the advantage that they can be replaced readily should any damage occur in transit or otherwise.

Ballasting required some experimentation because I find that off-the-shelf N gauge ballast material appears slightly overscale. I

eventually settled on a ratio of 70:30 chinchilla dust to N gauge ballast to gain the right effect. It works well and once secured in position using the usual diluted PVA method, sets very firm. A light coat of track colour was applied with an airbrush to finish the effect.

### Digital control

DCC locomotive control has been selected for driving trains. I settled on a Lenz 100 system with two hand-held controllers to facilitate two operators at exhibitions, however I decided to opt for a conventional mimic control panel for switching points. The decision was based on the speed at which I could flick a manual switch simply versus navigating a menu system on the controller to switch a point: the good

old-fashioned manual switch won hands-down. For greater reliability, wire droppers have been fitted to every single piece of rail, regardless of how short. Droppers are fed from a power bus created with mains cable, and wherever possible, everything is soldered. Circuitron Tortoise point motors have been used to switch all the points including frog polarity. As an exception to the rule, I have used connectors on the point motors to allow a fast change should one fail, however they have thus far proved reliable to date, and I may also solder the connections to these as well.

Gaugemaster electromagnetic uncouplers are fitted throughout the sidings in the scenic section to enable shunting.

The signals are based on a mixture of both lower quadrant semaphores and a colour aspect all shown to be in place in the early 1960s. Although non-functioning signals were initially fitted, I decided to replace those with working models fabricated from a mix of etched brass kits. These are in the process of being automated through the use of a Megapoints block detection system. When this work is completed, the signals will automatically switch depending on the location of the trains on the layout. The idea is to reduce the workload for operators at exhibitions and enable them to focus on keeping the trains moving for the audience in the knowledge that the signals on the layout will take care of themselves. The use of such a system has also enabled me to programme a satisfying bounce into the semaphore operation.



▲ Overall view of the site looking south, as a BR Standard Class Five heads for Carlisle with Mk.1 stock.



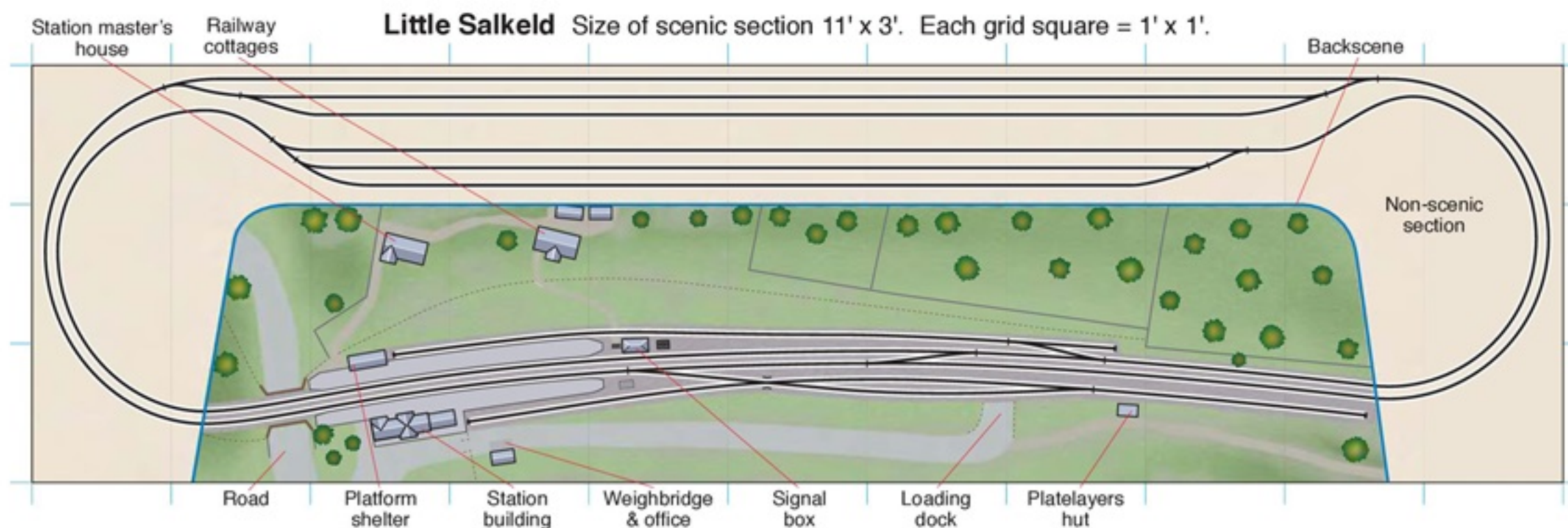


### Scenery and structures

I wanted to capture the very scenic nature of the area surrounding the station. It is certainly not as exposed as some parts of the S&C, and pictures taken at the time, and more recently, show trees and vegetation surrounding the station and the nearby station master's house. A little research also

▲ A Brush Type 2 ambles south with a local train (composed chiefly of ex-LMS stock), passing the goods yard.

The author/builder at the controls; hand-held Lenz speed controllers are used. ►







▲ An EE Type 4 rushes south over the bridge (No.297) which carries the railway over the lane leading to the village itself.

gave me a good feel for the topography around the station.

There is nothing new in the methods utilised for landscaping and these match those often described in the hobby

A northbound train of Presflos has been recessed in the siding as freight heads south.

magazines with plaster bandage laid over sculptured insulation board and various static and non-static scatter materials added.

I did find too much variation in vegetation colouring, although a light run over the greenery with a spray of Railmatch green after masking the trackwork had a great unifying effect.

Apparently life is all about timing, and the original intention was to scratchbuild all the structures for the layout, however Metcalfe recently released a range of kits for N gauge

based on the standard railway structures used on the S&C line including those at Little Salkeld. The time-saving opportunity was too good to resist, so I took the decision to use the Metcalfe kits, which are very good in their own right and provide an excellent starting point for further adaption and detailing. Window glazing has been replaced with those produced on a Silhouette cutter which simply scribes the glazing material to create a lead-light effect. Other adaptations included replacement roofs using embossed







plasticard, the addition of stone quoins to disguise exposed white card corners and other details such as gutters and drainpipes etc. I also found that the overall look of each structure could be enhanced significantly by blending it into its setting with a generous dusting of weathering powders and I've been very pleased with the effect.

### Operational aspects

Operation at home is typically a limited affair and if I am honest, it has tended to be more focused on keeping the locomotives running

whilst I have been building the layout. Operating at an exhibition is taken seriously, although not precisely timetabled, however a mix of realistic train formations and approximate sequences that I would have expected to have seen during the period on the S&C are run. These include local stopping trains as well as larger passenger or freight trains that passed through. The odd stranger is also run as the line was occasionally used as a diversion route, although not often as it could add an additional two hours to a journey time.

I have set out to keep things running wherever possible, even though Little Salkeld was not exactly inundated with stopping traffic in the early 1960s.

A number of locomotives have sound fitted although I do have mixed feelings about using that at exhibitions as it can get a bit tedious for the operators and for other co-located exhibitors who have to listen to it for hours on end. *Little Salkeld* attended its first exhibition, organised by the Kettering & District Model Railway Society, in September 2018 and I was both surprised and pleased that it won the Chairman's Award for Modelling Excellence.

Over and above finalising the wiring to automate the signals, the last significant task

▲ A Brush Type 4 rolls north with a short parcels train of typically mixed provenance.

still to complete, is to spend some time on the rolling stock. Some requires renumbering, replacement headcodes, gangway connections for coaches and final detailing etc. An appropriate level of weathering will complete the finish.

### And finally...

In concluding, I must mention the much-appreciated support received from Dave Sutton who scribed a number of the components for structures including the platform shelter. Dave also built the proscenium arch that so well presents *Little Salkeld* as a cameo layout. In addition, a huge thank you is due to Tony Wright for the excellent photographs for this article.

Over and above that, the layout is pretty much complete and will next be showing at the well-respected East Bedfordshire Model Railway Society annual club show at Stratton Upper School in Biggleswade, Bedfordshire on 16 February 2019; I hope to meet some of you there.

▼ The grounded ex-Midland van body was used as a store in the goods yard.

### Modified N gauge couplings



I am now in the process of gradually replacing unsightly N gauge Rapido style couplings with etched D&G couplings where shunting is required. In some cases, a home-grown coupling design for fixed freight or coach rakes is being installed. This utilises a small wire hook that replaces the standard N gauge Rapido couplings without the need to modify the stock permanently. Fabricated from soldered brass wire, the replacement wire hook coupling is simply pushed into the existing NEM pocket. This means that it can be swapped back to the original Rapido coupling in the future. This is ideal for coaching stock or fixed freight formations where there is little or no need for shunting.





# Dockside ships for Ealing Road

Photography by the author and courtesy  
Knickerbockerglory TV

Built by the Missenden Modellers team in TV's *The Great Model Railway Challenge*, Ealing Road needed suitable vessels for its quayside. Team member **ANDREW HICKS** describes the solution adopted and explains how you too could make them using modified 4mm scale Scalescenes card kits.

The problem with doing harbour scenes on model railways is the shortage of suitable model boats from the right period. The few that are available in more or less the right scale are mostly prohibitively expensive. There were several reasons why we wanted to include at least one on the layout:

- a model of London docklands really needs a dockside to capture the atmosphere;
- many of the Ealing comedies (the theme of our challenge layout) were set in and around London docklands;
- we wanted a layout with multiple layers, including water and wharves, with cranes and masts to give foreground interest. Team leader Barry cut a 1m x 13cm slot along the front of the middle board and gave me the job of filling it.

Scalescenes, the downloadable self-print kits manufacturer, produces a range of dockside items easily adaptable to our needs at affordable prices. These included a Dockside wall (TO16, \$3.99), a 'Clyde Puffer' coaster (TO30b, \$5.99) and a cargo ship (TO30, \$6.99). We needed a Clyde Puffer to represent *The Maggie*, the vessel featured in the 1954 movie of the same name. At only 29cm long it left a further 70cm to play with, so we decided to add a reference to the ship – the *SS Cabinet Minister* – from that other famous Ealing Comedy *Whisky Galore*.

The Scalescenes cargo ship (TO30 - see photo 1) was the right length (60cm) but wrong period – though we reckoned it would be convertible. The brief for the TV show also called for animations – working accessories in model railway parlance – so in a rash moment I promised to do either a working crane, or make



one of the derricks on the ship work.

After the show was over, Kiwi (our team's paint-meister, so nicknamed as he hails from New Zealand but now pilots full-scale container ships into Newcastle docks) said in passing that he would rather like a ship like the

*SS Cabinet Minister* for his layout. Coming from a professional seaman, I took that as pretty much the ultimate compliment, and since he had already kindly weathered a full rake of coaches and loco for me, how could I possibly refuse to build one for him as well?

▲ *SS Cabinet Minister* at the quayside on *Ealing Road*, depicting a scene from *Whisky Galore* in the Missenden Modellers' layout entry in *The Great Model Railway Challenge* TV programme.

So here is the story of two ships: the *SS Cabinet Minister* and the *SS Kiwi*. Doing essentially the same conversion twice might sound tedious, but it gave me an opportunity to learn from earlier mistakes and improve the concept. I searched the internet for images of coastal and short sea ships to help with my conversions. The best set I found is the 'transports of delight' website: <https://transportsofdelight.smugmug.com/SHIPS/British-Coastal-and-Short-Sea>

## Adapting the kit

Since Scalescenes kits are supplied as downloadable PDFs to be printed onto matt white photo paper, there are a couple of ways



in which adaptations and modifications can be made as follows.

1 Print multiple copies of the sheets and, using a scalpel, cut and trim sections to change the shape and size.

2 Make the modifications to the PDF sheets on screen using a layered graphics editor (such as Adobe Illustrator™) before printing them out.

I used method one for *SS Cabinet Minister*, and method two for *Kiwi*. Basically, for an earlier 20th century freighter the prow had to be shorter and less raked, and the wheelhouse positioned amidships with corresponding modifications to the stern section. The new central raised cabin sections gave space for the motors and servos which operate the derricks.

Only three substantive areas needed to be changed; the ship side cladding, bulwarks and deck planking. Two of these mostly involved trimming down, so method one worked perfectly well. With *Kiwi* I wanted the cabin deck sides to have squared-off tops and the bulwarks to be single continuous sections. I also decided to enlarge the upper deck of the stern section so as to move the funnel (and by implication the engine room) from amidships to the stern castle. From the photos seen on the internet, this seems to have been a more common arrangement in coastal freighters and colliers.

### Editing the PDFs on screen

Taking apart the Scalescenes PDFs may sound a daunting prospect, but actually it was surprisingly easy. If you don't have Illustrator (which is now available only on subscription), a free

program called Inkscape ([www.inkscape.org](http://www.inkscape.org)) is available for download so I used that.

There are online tutorials to help and it took me a couple of days to learn it sufficiently to make a start, after which I mostly learned as I went along. This is not the place for a full Inkscape or Illustrator tutorial, but once opened, all the elements in the PDFs become editable. You can copy, paste, resize (to a point) and trim as required.

Anyone following a similar process will find that some of the modifications are best done by trimming the prints, rather than editing on screen – this is where a bit of personal intuition will come in handy.

### Tips for printing and preparing the PDFs

Printed PDF kits are very easy, cost little, and can make superb models but have a hidden expense in that as many domestic inkjet printers drink toner and cost a fortune to refill. I use an Epson Stylus ET-2550 Eco printer which has considerably lower running costs.

The most important thing when printing is to use the correct paper. Scalescenes kits require matt white thin photo paper (not the usual thicker paper used to print holiday snaps); I use 102gsm Epson Photo Quality Ink Jet A4 paper. As the printed sheets are on this thin paper, the builder has to provide backing card and adhesive on which to mount them for added strength. Card thicknesses of 1mm and 2mm are useful, plus some light card. I used Greyboard Craft Card, sold online in packs of A4 sheets.

I did try using photo spray-

mount for gluing the sheets, but it had a tendency to peel and found white PVA hobby adhesive much more permanent. A heavy rubber roller was useful to ensure good contact and a wrinkle-free finish (see photo 2). To cut parts out from the sheets (once glued and fully dry), a steel rule and a heavy craft knife are essential.

Construction of the ships' main 'skeletons' was quite involved in that there was a considerable number of parts to cut out each time, but assembly did not require any special techniques. Most of the parts are butt joined with quick-drying PVA glue and following the Scalescenes instruction was still helpful and relevant despite my alterations. Essentially the main sections – bow, stern, cargo holds and, in the case of my ships, the midships wheelhouses – are built as individual units or 'frames' (see photo 3),

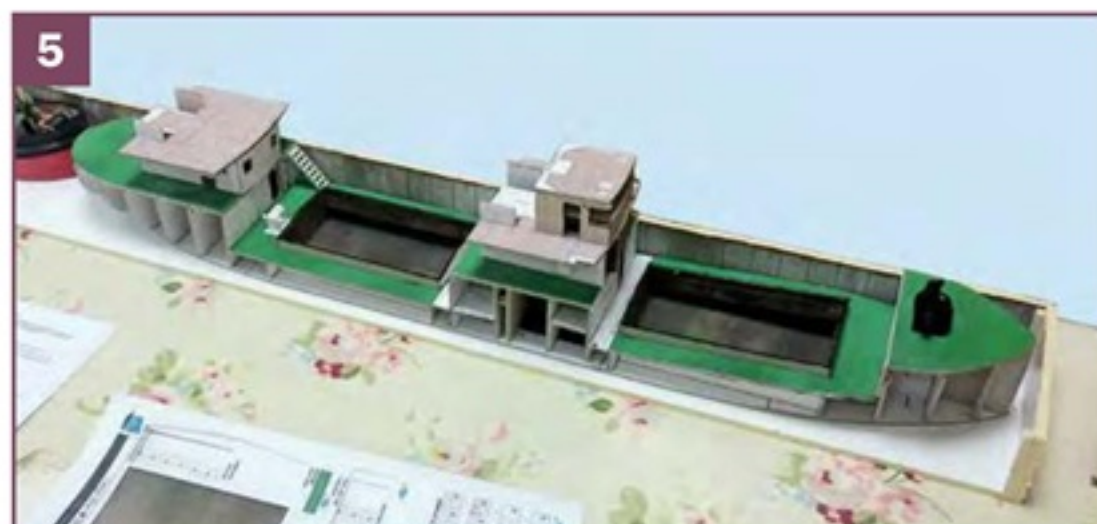
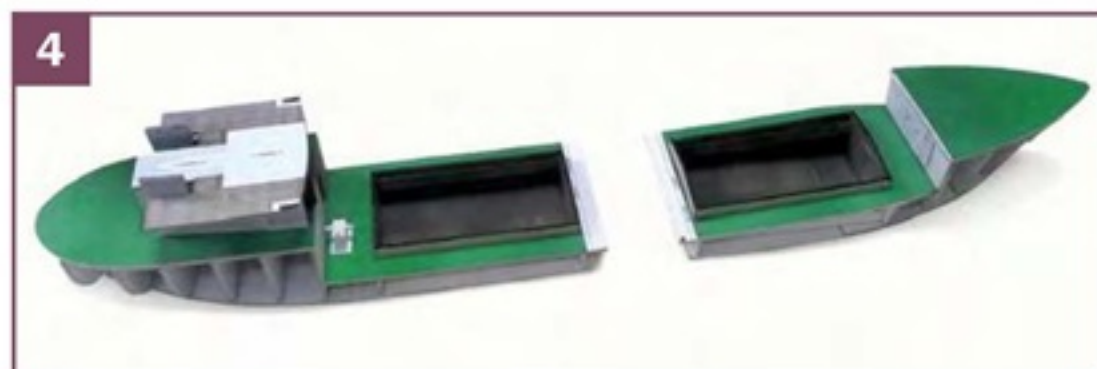
then attached together, much as real ships are built (see photos 4 and 5).

The hull sides will be glued later with impact adhesive and pegged until the glue cures. The 'fitting out' of the decks, cabins, wheelhouse, cargo holds, derricks, etc., finish off the model.

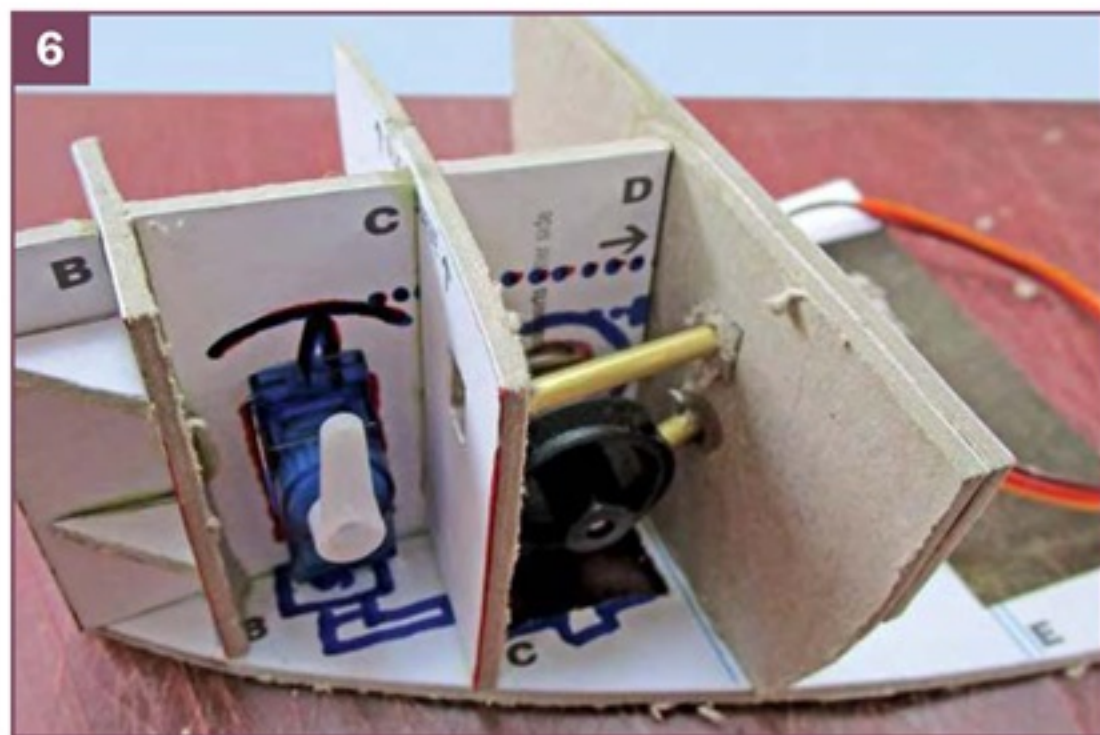
### Making the derrick rigs work

However, before assembling the individual frames sections in both my models, some surgery and bespoke construction was needed to make the derrick rigs operational. Each of the rigs uses a servo motor to swing each of the boom arms out and back, and a small N20 geared motor [Ed. – see April 2018 RM] to operate the winch.

Extra holes for cable runs had to be cut in the card crossmembers, and arrangements for







Servo Testers units. The N20s are 6V, running at 40rpm, and have 3mm spindles on which I fitted 20mm diameter plastic pulley wheels for the winch cords. A single Mini DC 5A PWM speed controller/LED dimmer (3V – 35V) is used for controlling the N20s via biased centre-off miniature DPDT switches. All the control gear is housed in a separate control case which includes a four-AA battery pack (6V) power supply. The case is a wooden 'Cargo' box I bought from Dunelm Mill for £9. It looks nice (see photo 9).

I also added microscopic LED lights using pre-soldered 0402 SMD LEDs powered via a 9V battery. These must be wired in series with a resistor; since Kiwi's ship has 10 of them wired in parallel, a 47ohm resistor in series with the whole lot works OK.

All the wiring from the ship to control box uses 10-core ribbon cable plus two extra wires for the lighting circuit, and all the soldered joints are protected via heat shrinkable insulator tubing. Small cable connectors made up using PCB header pins and sockets allow the ships to be disconnected easily from the control boxes.

mounting the servos and motors had to be devised: these were located beneath the bow deck (see photo 6), and either within the midship or stern bulkhead, depending on where the derrick masts were to be fitted. The motors are held in place with superglue, and the operating wires to the booms and winches run in conduits formed from brass tube (see photo 7).

SS *Cabinet Minister* was fitted with twin-boom derricks and therefore has two winch motors fitted per rig. *Kiwi* has simpler, single boom rigs.

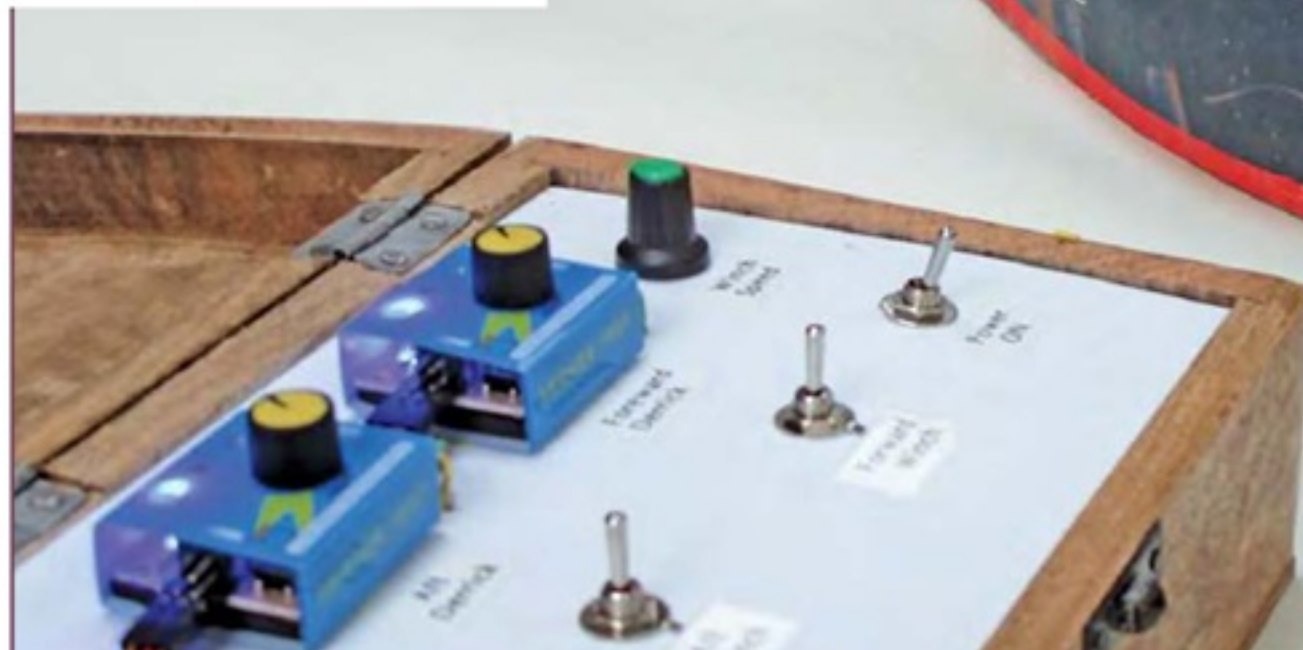
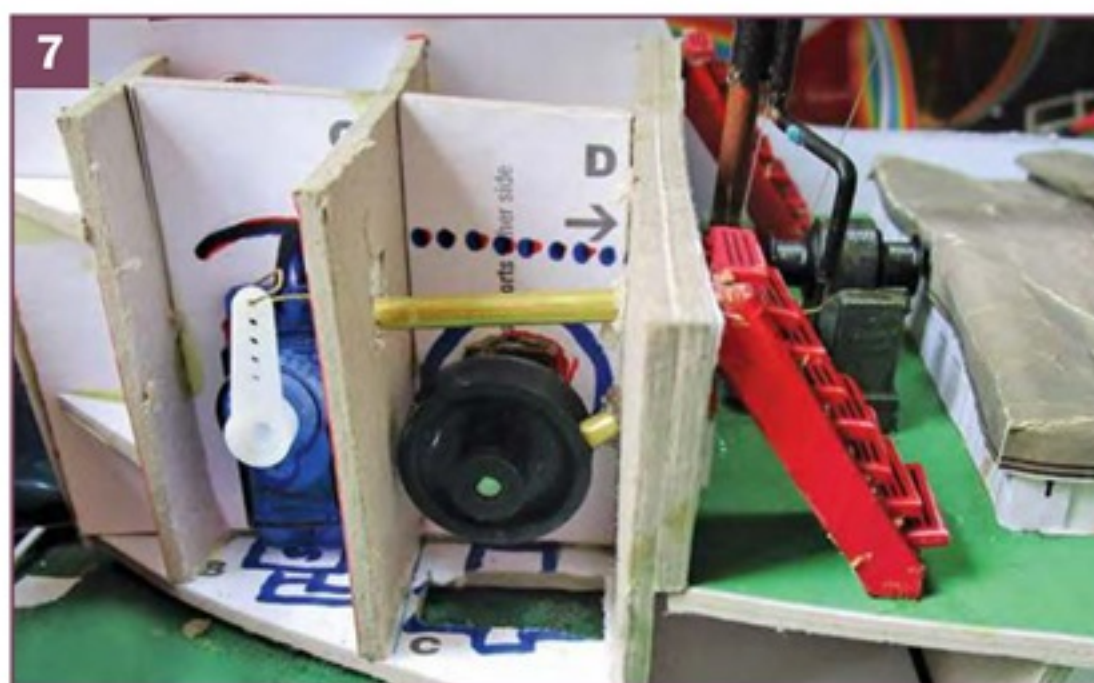
The least satisfactory parts of SS *Cabinet Minister* are the derrick masts, which will shortly be replaced. I used a set of second-hand ones which are too thick with booms that are too short. For *Kiwi*'s masts I used kebab skewers and made my own mast fittings using Albion Hobbies brass tubing.

The same tube also made the lifeboat davits as well as the pillars supporting the upper decks (held in place with shortened sewing pins, through the deck).

I used black cotton for all the winch rigging on SS *Cabinet Minister*, which looks nice but is rather hairy and prone to snagging and tangling. Lead fishing weights were added to keep the winch cords tight, but even then the system proved unreliable. When I came to do *Kiwi* I switched from cotton to fishing line which looks finer but runs more smoothly.

I added whitmetal clamshell buckets from Langley Miniature Models to *Kiwi*'s derricks, the weight of which definitely helps (see photo 8).

Most of the electrical parts I obtained online from China via eBay. The servos are SG90s controlled using ESC three-mode



## Detailing

The most visible detail on the ship is the anchor winch. Langley produces a very nice model (MB1d). Fortunately, when I went hunting, my local model shop had just bought a job lot from a shed clearance so I came away with a packet of assorted bits for £10, including a John Hayes JRH90 whitmetal 1:96 scale steam winch and some brass railing stanchions.



Most of the detailing parts I ordered from The Model Dockyard and include the working brass pulleys, 14mm bronzed ship's wheel and cabin deck skylight (see photo 10). The firm also supplied the 35mm deck-mounted davits I used for *SS Cabinet Minister*, though I made my own for *Kiwi*.

The rigging double-blocks are accessories from Billings Boats ([www.billingsboats.com](http://www.billingsboats.com)) and the ladders are from Scale Link.

Now that high quality 3D printing is becoming more widely available, suppliers like Ngeer,

Niall's Model Parts, and Model Monkey are offering increasing numbers of shipping parts for sale through internet sites such as [www.shapeways.com](http://www.shapeways.com).

The completed models at 4mm scale are quite large and fragile and delivering the finished *Kiwi* to its new namesake owner on the banks of the River Tyne proved a challenge.

Having weathered *SS Cabinet Minister* myself, I held off painting *Kiwi*'s ship: given his fame as a weathering expert, that really would be like taking coals to Newcastle!



## Main modifications to Scalesscenes PDFs

### Midships wheelhouses

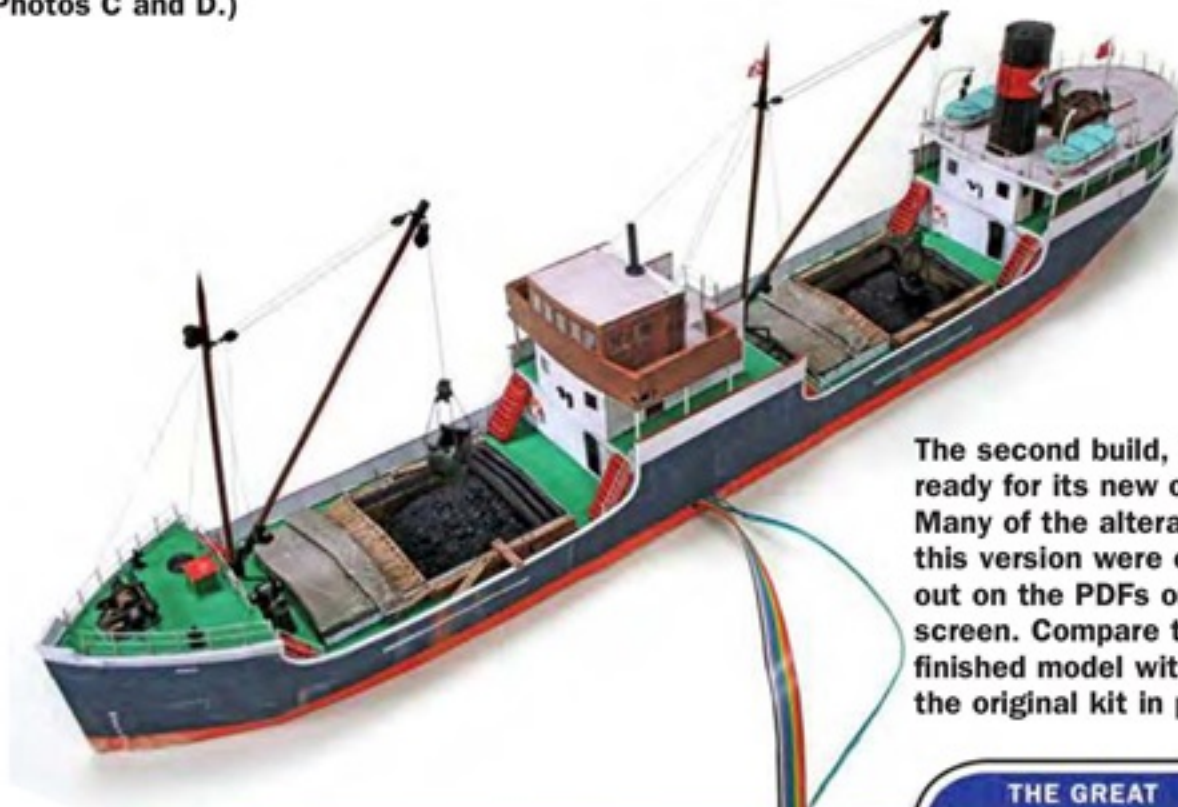
The midship wheelhouses on both ships are basically a shortened copy of the stern section from the kit, made with five squared-off bulkheads with just enough deck to fit a shortened copy of the wheelhouse cabin (the rear is trimmed to line up with the stairs). The spine has extra thickeners below where the mast-hole is to be drilled. For *Kiwi* I altered the bridge and wheelhouse cabin walls from plain white to planked wood. This was easy to do with Inkscape by copying the planking pattern from the wheelhouse door. (Photos A and B.)



### Stern cabin and deck



Just as the new midships cabin was shortened from the Scalesscenes original, so the new stern cabin was lengthened for *Kiwi*, as I planned for the upper deck to overhang all the way round. This meant adding an extra door and window. The upper deck is a modified version of the wheelhouse deck combined with the stern deck outline, and was the first job I did in Inkscape, after removing the funnel profile from the wheelhouse roof. Prototypically a 1920s ship would have had an upright cylindrical funnel. The Scalesscenes model comes with a later 'raked tear-drop' profile. For *SS Cabinet Minister* I used 22mm copper plumbing pipe and repositioned the funnel amidships. *Kiwi* specifically wanted his model to have an oval profile. I took a short length of 28mm copper plumbing pipe, put it in the vice and squeezed. (Photos C and D.)



The second build, *Kiwi*, ready for its new owner. Many of the alterations for this version were carried out on the PDFs on screen. Compare the finished model with that of the original kit in photo 2.

As seen on TV in

THE GREAT  
**MODEL RAILWAY**

CHALLENGE



A constantly-evolving layout

# Old Elms Road

Photography by Steve Flint

*This OO layout was built by members of the Bodmin & District MRC to satisfy their preferences for models of Western and Southern Region outline. **BOB HOSKINS** explains how the club developed it, using the robust fiddle yard from its predecessor, which was described in RM July 1997.*

**I**magine – it's early 1960; Elvis is riding high in the charts and Harold "you've never had it so good" Macmillan is ensconced in No.10. Meanwhile, in a busy town somewhere near the Berkshire/Hampshire border a young trainspotter, standing at the end of the station platform, cannot believe his luck at the cornucopia of trains passing before him today. There's anything and everything from main line expresses to single car DMUs and from lengthy freights to a variety of light engine

movements to and from the adjacent motive power depot... Welcome, then, to *Old Elms Road*, courtesy of the Bodmin & District Model Railway Club.

The club is based in Bodmin, Cornwall but with a high proportion of members originating from London, the south and west. It will come as no surprise then to learn that members' stock is biased towards the Southern and Western regions. The challenge was always to be able to cater for all these interests; this led to the

location of *Old Elms Road* being on the Berkshire/Hampshire border and in essence is a GWR secondary main line connecting the Western and Southern Regions.

This is a layout which epitomises evolution and hasn't really stopped changing despite its eight years or so of existence. Prior to *Old Elms Road*, *Trehayn* was our main layout from which many lessons were learnt in terms of construction and operation. This fed into the various stages of *Old Elms Road's*





creation but none more so than the decision to retain the fiddle yard from *Trehayn* which now gives us the capability to run something like 50 different trains at exhibitions. With something so reliable and robust, why change it?

### Behind the scenes

The fiddle yard uses a semi automatic route control system with two main lines entering the yard at both ends. These then split into seven lines each way using Peco code 100 for its reliability and sturdiness, two qualities paramount at exhibitions where individual points will change between 20 and 30 times an hour. A microcontroller named 'The Zapper' allows the operator to select both the exit and entry road at the push of a button. Four heavy-duty CDUs are required to handle the number of potential changes on a system which dates back some 25 years – proof indeed of its reliability.

In addition, two of the seven tracks on each side are divided into four sections to handle the short trains introduced for



▼ The future in the shape of LMS Co-Co No.10001 speeds past Modified Hall 4-6-0 No.7904 Fountains Hall, looped with a ballast train.

▲ View of the station platforms from the access road to the goods yard. Under the canopies are Wills waiting rooms.







▲ Looking across the scrap metal merchant's premises to the steam depot, Merchant Navy No.35010 *Blue Star* rolls by with a West of England line train.

authenticity and variety. These are controlled by an electronic system named 'The Shuffle' which consists of standard logic chips, a few diodes, relays and switches. Infra-red detectors are positioned at the beginning of each short section which allows the sequence to occur with no operator involvement needed. Once the first train on the line has been despatched, a switch is thrown and the three remaining trains move forward, one at a time, thereby creating a space at the rear for the first train to enter once it has completed its journey. This allows the operator to concentrate on the 'front of house' activity, knowing his next short train is all set up when required.

As far as actual running is concerned, trusted Gaugemaster controls are the order of the day, fed to the layout via four section control panels and isolating switches. Due to the layout's trackplan, should a fault ever

occur during a show, we are able to divert traffic around the problem and keep disruption to a minimum. Electronics in the form of relays and microprocessors enable operators to perform complex manoeuvres – such as crossing from the WR branch to the up main line or the SR branch to the down main line – by simply pushing a button on a dedicated special route control panel. This is invaluable during a show as it eliminates operator error and simulates prototypical operations.

We do try to avoid the 'tail-chaser' tag by having a variety of things going on around the layout to engage the viewing public. Trains tend to run in a sequence rather than to a timetable, the operator having a free hand in deciding which train runs next. This not only provides variety 'out front', but minimises operator fatigue working slavishly to a timetable. That said, however, the operation must be run 'correctly' in terms of scale speed and by avoiding running high speed expresses though the station just after a slow freight has plodded through.

### The model 'out front'

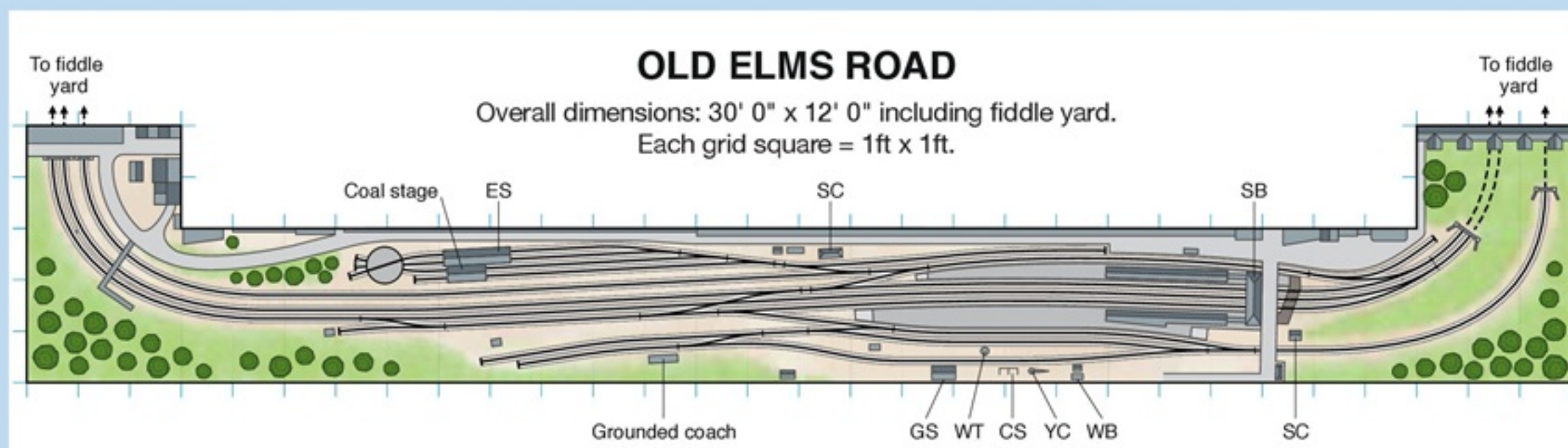
Whilst we use our tried and tested Peco point motors in the fiddle yard, on the scenic side Circuitron Tortoise motors are in control although we are now changing these for DCCconcepts Cobalt motors as the others come up for retirement.

Construction of the layout was straightforward enough using timber with plywood tops, the scenic element built using expanded polystyrene covered in plaster bandage. The usual scenic treatments followed and the whole set-up has proven to be both robust yet lightweight, a real bonus

▲ All three modes of traction in one shot: a Southern 2-HAL electric unit, an ex-GWR diesel railcar, and N Class Mogul No.31401 heading for the Bournemouth line.







Away from the hustle and bustle of the rest of the steam shed, a crewman attends to his ex-GWR 0-6-0PT.

as the layout has travelled the length and breadth of the country on the exhibition circuit.

Trackwork on the public side of the layout consists of SMP products with handmade points and crossings. Ballasting on the scenic sections was a long and arduous project with no short-cuts if the high standard intended was to be achieved. Our preference was to use Woodland Scenics OO and N scale chippings, carefully laid using a small brush and glued with diluted PVA. Various hues and shades of rust and grime were then applied using an airbrush. All scenery and ballasting is regularly 'refreshed' as it inevitably fades and deteriorates over time.

### Planning permission

The buildings on *Old Elms Road* were 95% scratchbuilt by members whose work is rewarded by having their names adorn the many shops and businesses along the high street. Members both past and present are remembered in this way. The most recent addition to the layout is the biscuit factory – Roberts Biscuits. This fulfilled the brief of having to fill a particular and in this case awkward triangular shape.



The method is initially to construct a cardboard mock-up to suit the intended building's space. This has the advantage of getting a feel for the building in terms of its scale, ensuring it does not dominate its position and surroundings. Alterations can easily be made at this stage along with determining the positions of

doors, windows and any other relevant detail.

Having agreed on the design, the building proper can commence with the walls being constructed from clear polystyrene sheet. Not only does this give strength, it provides the glazing for the windows etc. A layer of white plastic sheet is then applied with apertures already cut out for the windows and doors. From now on it is







▲ As any Western Region aficionado knows, double-heading a Hall with only eight on can't possibly be for assistance, so obviously No.D6320 has been coupled on the front to save a light engine move...

SECR H Class No.31518 propels its two-coach pull-push set away from the camera, passing WD 2-8-0 No.90441.



simply down to personal choice as to whether a brick, stone, stucco or whatever finish is applied. Window frames are affixed and details such as gutters and downpipes along with the all-important roof can then be fitted prior to painting and weathering.

By using clear polystyrene, fully detailed interiors can be installed where they are close enough to be seen by the viewing public. A bonus of course is the polystyrene gives a level of protection from prying fingers! In the case of the bakery and the workshop next door, the outward-facing rear walls were left clear to allow the public to see not only the racking and loading area of the bakery but also the joiner at work at his fully detailed saw bench surrounded by his tools of the trade.

### Working road vehicles – and TVs!

To add to the viewing experience further, gaps between train movements are filled by working road vehicles plying their way from one high street to the other. These vehicles always create a lot of

Businesses in the town carry the names of members past and present who constructed them. Spot the working TVs...

interest with the public – they know how the trains work, but cars, buses and lorries? Operators are asked countless times at a show how the magic happens – not only from the public but from other modellers.

This is because the vehicles themselves, although based on the Faller system, have all been expertly altered by one of our members into models more appropriate to the era of the layout. This involves the replacement of the vehicle bodies with an appropriate model from the Oxford range. With one bus however, this proved too much for the mechanism to cope with, entailing the rebuilding of the metal roof using plasticard and detailing, all resprayed and reassembled





An ex-Great Western twin railcar pauses for custom, as a Grange 4-6-0 arrives with a local service in the distance.

– a real labour of love!

The vehicle route, like the fiddle yard is also semi-automatic by having an off-scene terminus where four vehicles can be parked up, one behind the other, until their magnets are reactivated by switches to begin their journey along the layout. New vehicles are added to the layout over time, the latest being Fred Dibnah's traction engine complete with work van behind!

If that were not enough to grab onlookers' attention, just at the end of the high street a TV shop has a fine display of the latest 405-line sets in its window but if you look carefully, you'll see the sets actually work – this really does amaze the full scale viewers!

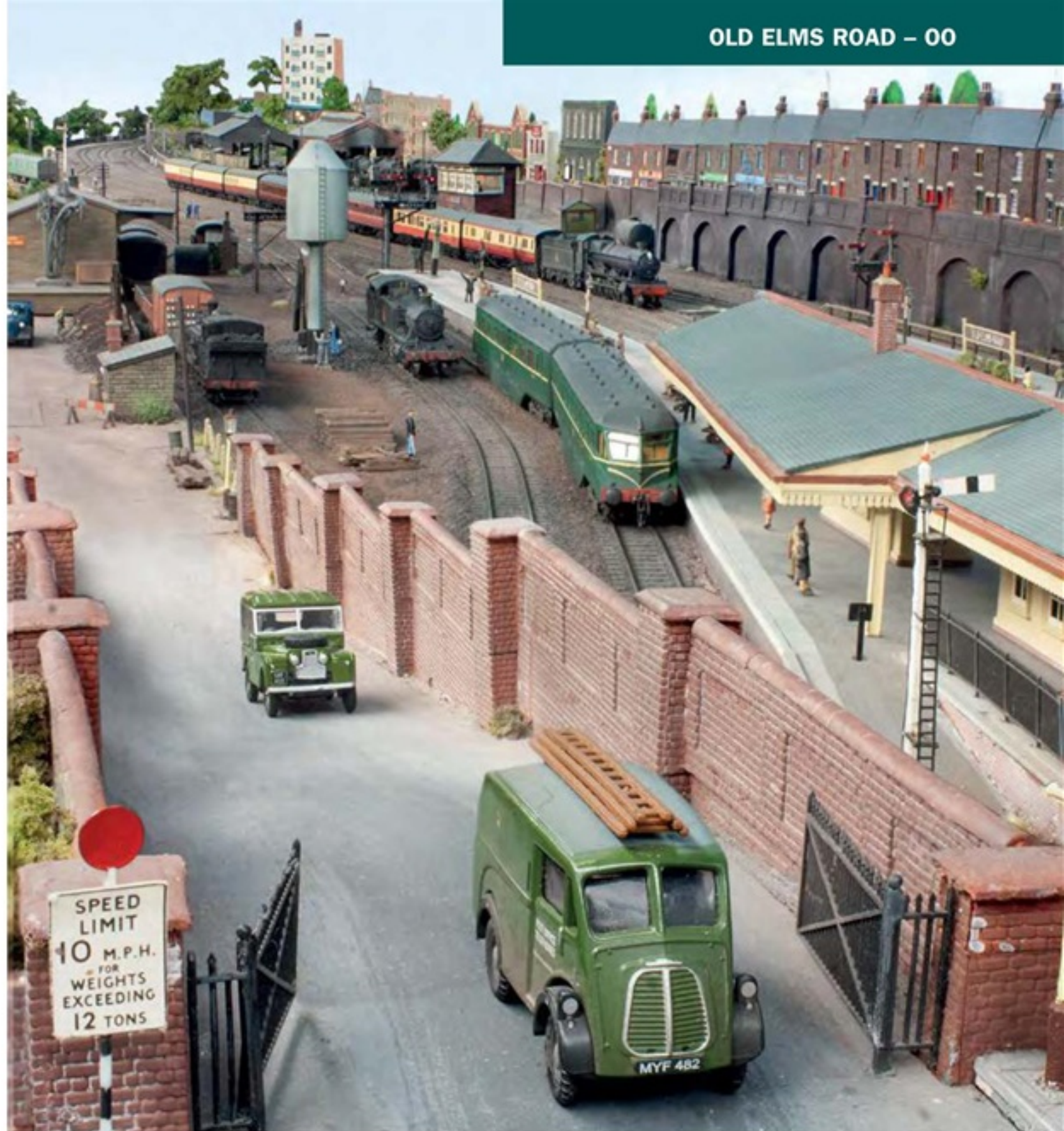
### Locomotives and rolling stock

As the layout visits various exhibitions throughout the year the stock on show changes too – all is owned by members and consists of a variety of kit- and scratchbuilt models along with those from the main manufacturers. We avoid running anything straight from the box, preferring instead some light weathering, real coal, crews and lamps etc. Heaven forbid that we run two locomotives with the same number! The newer addition to the fleet has to be renumbered, and tension-lock couplings are removed in favour of scale screw-link or Kadee couplings.

*Old Elms Road* is usually run in the timeframe 1956 to 1962 so features mainly steam but with the appearance of the new emerging diesel power. However, for a change occasionally, we base one of our show days from 1968 to 1975 which allows operators to run something a little different; blue diesels with blue & grey coaches replacing the steam age stock.

The layout has now been on the exhibition circuit for about eight years but someone seeing the layout today would notice subtle (and some not so subtle) changes even from

**Carry On Beeching** is on at the Odeon cinema this week, but the mind boggles as to how it can warrant an X certificate!



last year. The previously mentioned biscuit factory filled a site which had been just green fields and trees. We wanted something more substantial at the front of the layout, behind which trains would briefly disappear and which would add further interest for the viewing public. The level of detail offered by the interiors as already described is enhanced further by yet more detail outside the factory where the road is being resurfaced by a working steamroller, trundling back and forth (courtesy of a Tortoise motor), flattening the fresh asphalt. Another recent addition can be found by the

turntable, where a short siding was added with a static pannier tank gently simmering thanks to a hidden smoke unit under the baseboard.

As can be seen, by ringing the changes, whether they be big or small, we manage to keep the layout looking fresh; we'll continue to do so to keep the paying public entertained. Not forgetting the young trainspotter at the end of the platform either!

**Six of the *Old Elms Road* crew, from left – Phil Tombs, Bob Hoskins, Steve Saunders, Alistair Clement, Phil Simpson, Dave Savage.**





# Building a goods shed from scratch

Rather than using a printed card goods shed kit on a revamped N gauge club layout, **VICTOR HALL** chose to make his own. He says it's a great way to develop scratchbuilding experience using tried and tested techniques and readily available materials. His method is also applicable to other scales.

When the Melton Mowbray Model Railway Club refurbished *Exton*, its N gauge layout, I agreed to build the replacement goods shed. The original was a Metcalfe kit so it was important to keep within the existing footprint. However, rather than make a like-for-like copy, I drew up my own version, incorporating all the main dimensions. The model was made from plasticard using techniques that I have developed over the years.

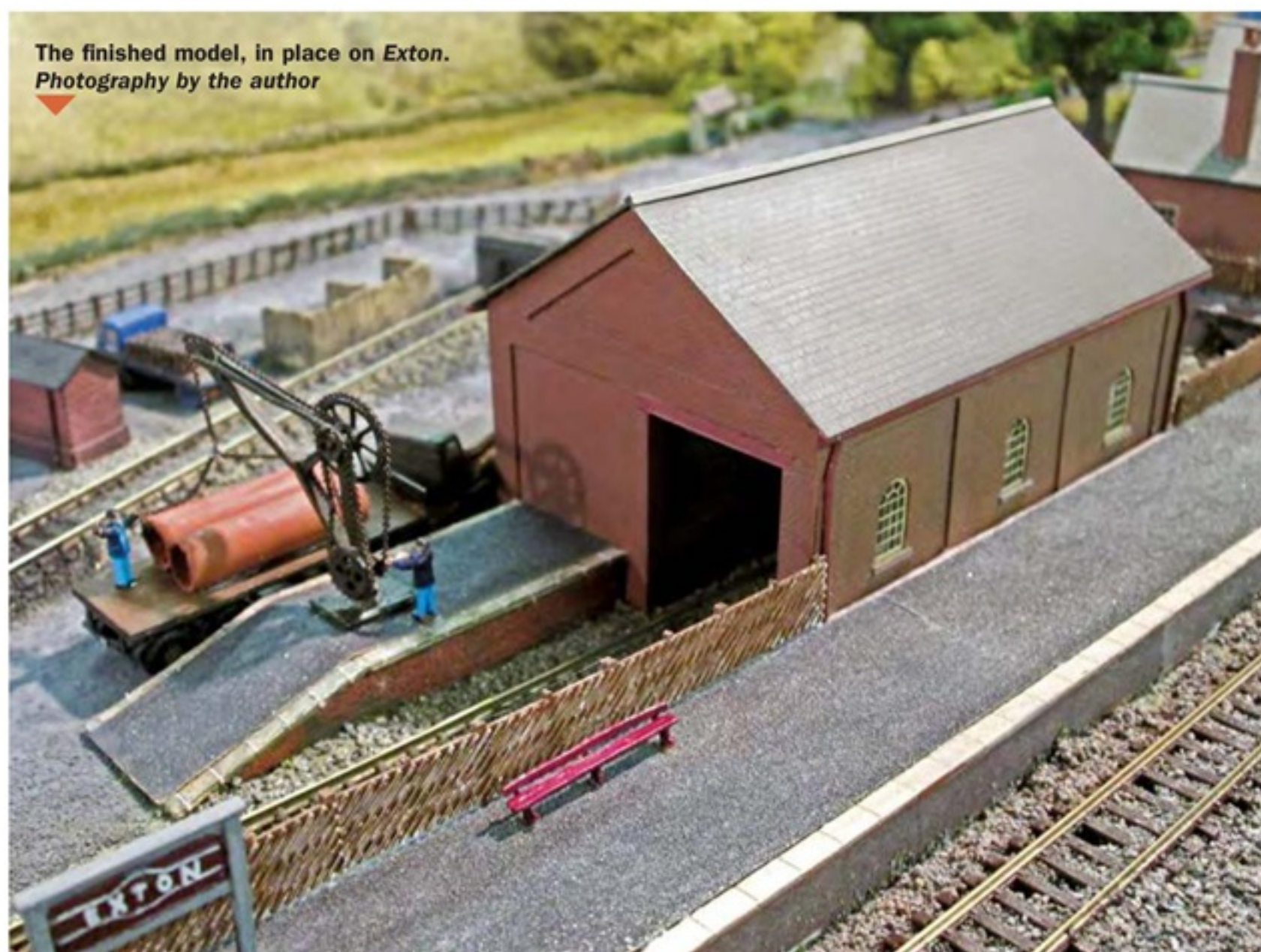
## The main components

I took a sheet of 2mm scale brick plasticard, trimmed the sides and ends to get it square, and sanded the back with fine emery paper to remove the shiny surface. This not only made it more accepting of glues and solvent, but also allowed pencil marks to be visible and durable.

I drew out the sides on the back of the plasticard, one side above the other. This ensured that the sides were the same length and made it easier to draw the vertical lines of the window and door frames. A similar procedure was carried out with the ends but in this case, one of the ends was inverted so that the door opening was on the correct side!

A feature of many brick-built goods sheds is the decorative relief panelling on the sides and ends. These were drawn separately to be superimposed over the main components, when cut out.

The doors and loading bay were cut out first, making allowance for the lintels. The technique I used was to drill holes at each corner and in the centre. The holes were joined up with a sharp craft knife and the ensuing triangles of plasticard were removed. In order to give a Midland Railway house style, I



The finished model, in place on *Exton*.  
Photography by the author

used Ratio arched window etches. Helpfully, the etch includes a template to assist in cutting out, for which I used a similar method to the doors. However, I drilled more holes and trimmed the opening with files to get the round profile and square corners.

To give adequate strength, and to avoid the risk of warping, I laminated the brick with 40thou plain plasticard. When using solvent or solvent-based glues to laminate plasticard, the adhesive can continue to eat into the plastic, long after you think it has dried, so nowadays, I use Bostik solvent free glue instead. Having joined the pieces, I clamped them together for a couple of days using two pieces of wood and a vice. The window and door

Some of the sub-components awaiting fettling.





openings were then cut out of the plain laminate and finished with a file. However, as I had used such thick backing, I cut out over-large openings before laminating, and filled in later after fitting the etched windows. The overlays for the relief features were then added and the pieces clamped in a vice again.

### Assembly

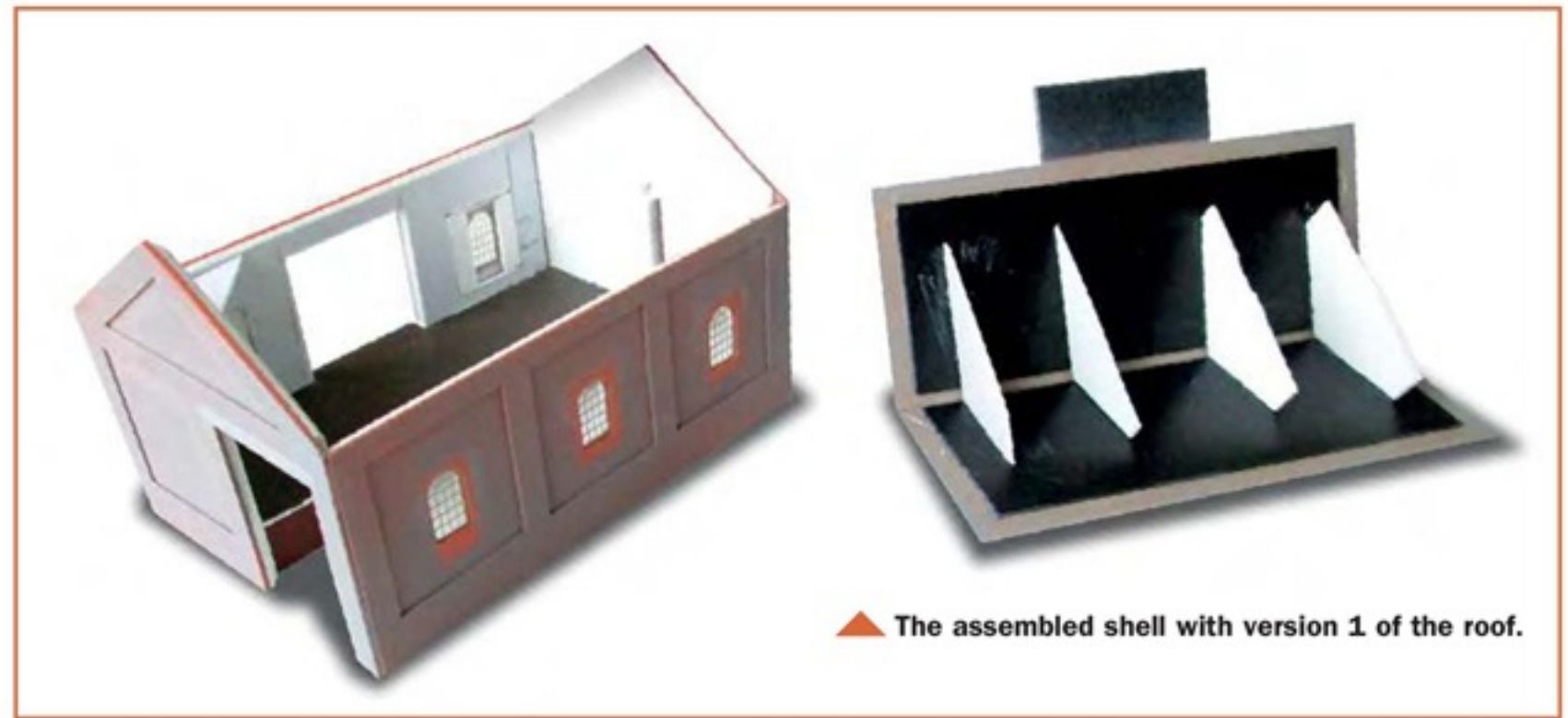
I started by joining one end to a side, with a fillet of plastic in the corner to give strength and squareness. I repeated the process thereby creating two sub-assemblies. Rather than trying to make 45° chamfers at the corners, I cut the end laminates shorter, so that the brick plasticard on the ends overlapped the sides. When dry, this can was filled and sanded as necessary to disguise the join.

The sub-assemblies were glued together to form the main building structure, adding further corner fillets. A plasticard platform was glued inside the goods shed along the loading bay side, which helped to give more rigidity. Having done this, I decided to clad the inside walls with brick plasticard, which was not an easy task. I should have planned ahead!

The roof sections were made from embossed plasticard, including an extension for the loading bay canopy, and laminated as before. In order to get the right angle for the roof, I taped pieces of 40thou plasticard inside each end of the goods shed, and the profile of the roof apex was drawn on them, marking which end it fitted. These pieces were cut and fettled as necessary then glued to the roof sections. In this way, the roof is a snug fit to the ends. Further triangular sections were glued in intermediate positions as additional roof supports. However, I then decided that it would be better if they at least resembled roof trusses, so I cut them about with snips and overlaid them with plastic strip to get the right impression (even though you can't see them).

### Finishing touches

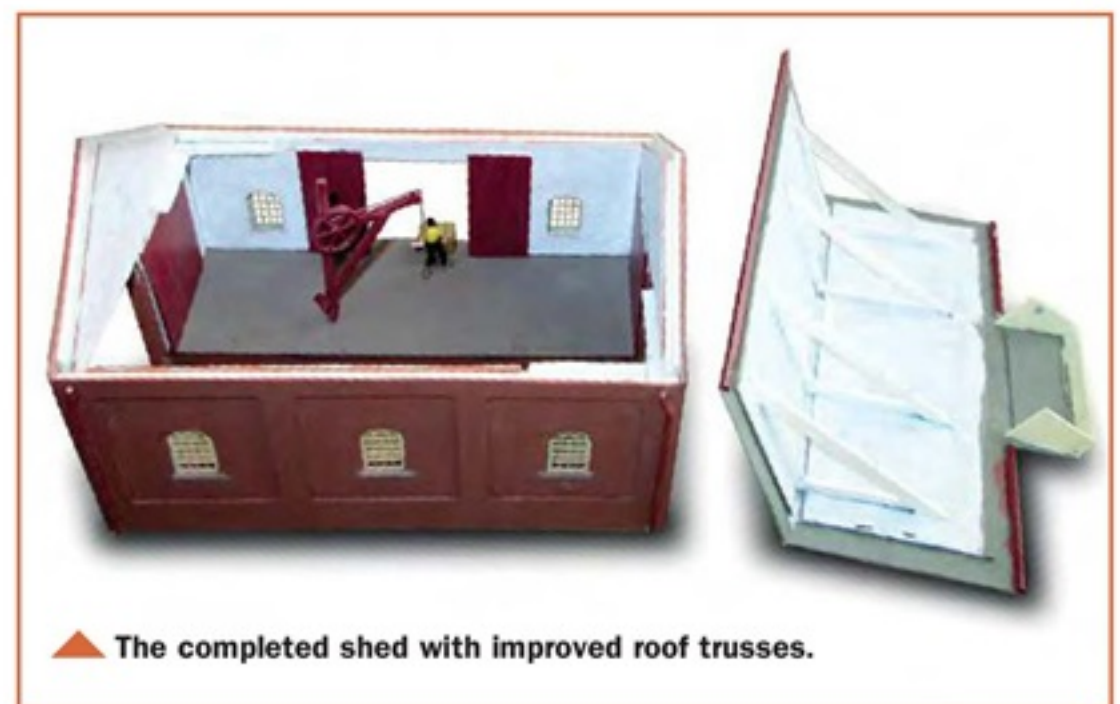
Cosmetic doors were fitted inside the end walls and the loading bay, whilst drainpipes were created from plastic rod. The loading bay canopy was finished by gluing some planked plasticard along its edge to represent the bargeboards.



▲ The assembled shell with version 1 of the roof.



▲ The hand crane.



▲ The completed shed with improved roof trusses.

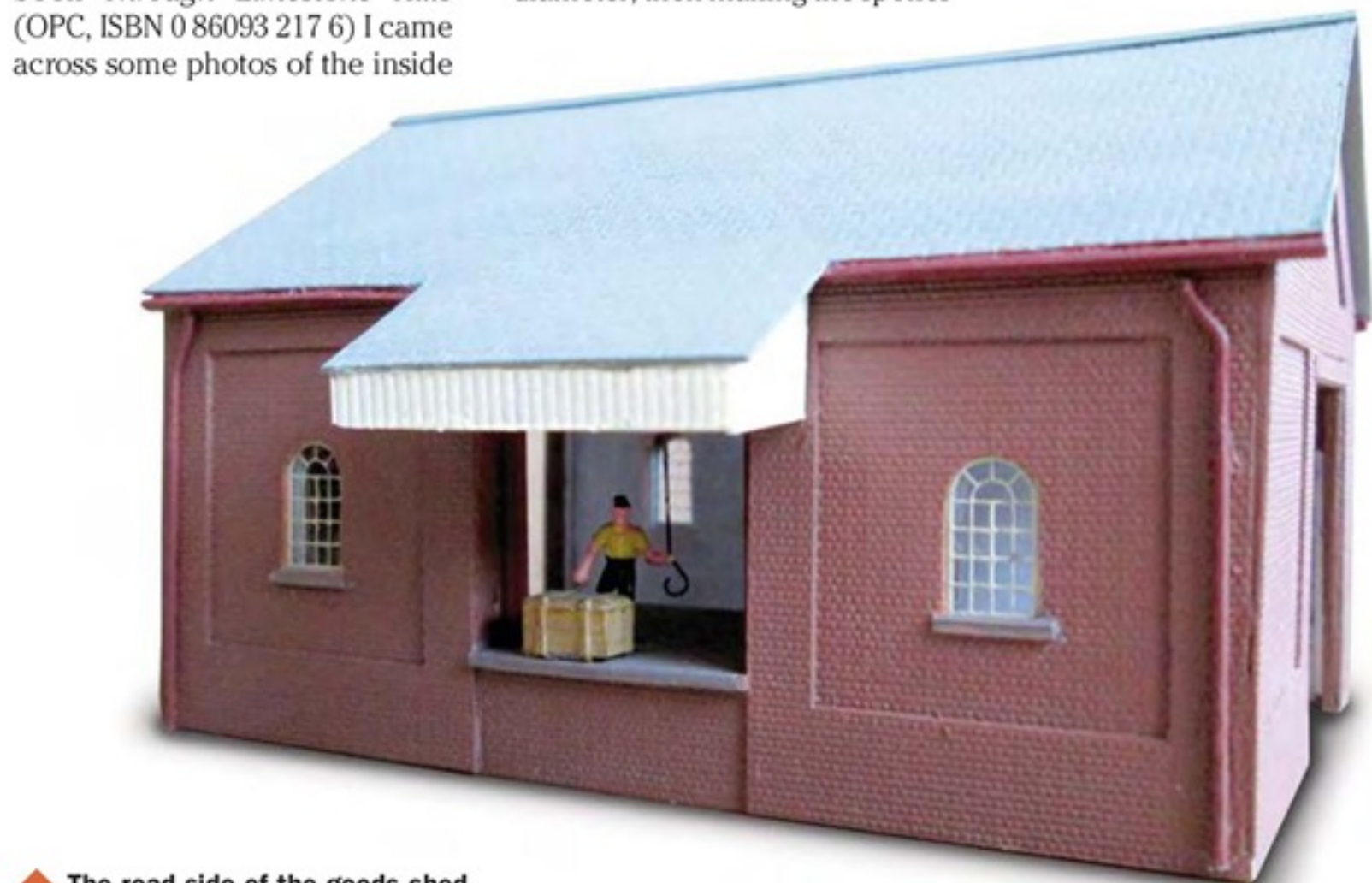
The exterior was painted with Humbrol 70, which gives a good impression of subdued brick, whilst the interior walls were given a thin coat of off-white, producing a patchy effect. Doors and drainpipes were painted maroon, with windows and the bargeboards painted cream.

Looking through Bill Hudson's book *Through Limestone Hills* (OPC, ISBN 0 86093 217 6) I came across some photos of the inside

of Bakewell goods shed which showed hand cranes mounted on the inside loading platform, and also a drawing. I made the basic structure from plastic rod, with 0.3mm wire to represent the rope. Forming the hook was fun! The wheel was made by cutting down a plastic ring from a pen, supergluing it into a smaller diameter, then making the spokes

from plastic strip. Having gone to all this trouble, I wasn't going to hide it away in an obscure corner of the shed, so I positioned it next to the side doorway. A crate and a suitable figure completed the scene.

Having completed the model, I was relieved to find that it fitted the Metcalfe footprint exactly.



▲ The road side of the goods shed.



A model railway resurrection project...

# Horton Road

Wimborne Railway Society member **DAVE ROUND** relates the tale of how the club's finescale O gauge layout gained a new lease of life after many years in the doldrums. The upgrade included converting the layout – and the sceptical operators! – to digital control.

Photography by Steve Flint

In resurrecting this 30-year-old layout from our storage, after nearly 10 years out of use and following its many visits to shows around the area, it was time to reflect on what could be salvaged – if anything – and

plan for the future. This previously popular club layout had attended many local exhibitions and had been retired as the previous operating team had split up and departed the scene.

It is designed to fit into our church hall storage room on racking at 4'6" height and measures 18' x 12' overall, thus limiting the radius of the track to 5'6".

Operated by the then contemporary analogue systems and with automated signal control, this layout with its purpose built cumbersome control cabinet, was indeed

The main terminus with a Terrier (Dapol) at the head of two MTH LMS coaches. The Heljan four-wheel railbus is in the bay, with the branch curving off behind it.





firmly stuck in the past century, using well tried and tested old style electronics.

### Choices for the future

In considering converting from analogue to DCC operation, club members set out on a voyage of discovery, to unlock the secrets of the modern day, digital control system revolution. Everything in use today revolves around 'the chip' and model railway projects are no exception.

A study was undertaken to see what could be done with the layout, enabling us to bring it back into operation and eventually to reform its operational scope, thus allowing greater train movements. This gave rise to the possibility of DCC control. This was something that had not been tried before on layouts built by our club or by our small group of O gauge modellers.



▲ The typically Southern running-in board, with integral natural display, created using small clumps of lichen.

### Pioneering

We had to get the systems operational, which became somewhat more involved than first envisaged! The layout, being stored out of use for so long in a cold, often damp store room, did not cope favourably with mid winter temperatures which did not do the baseboards any favours. We found on erecting the layout that the support legs were beyond repair and set about replacing them.

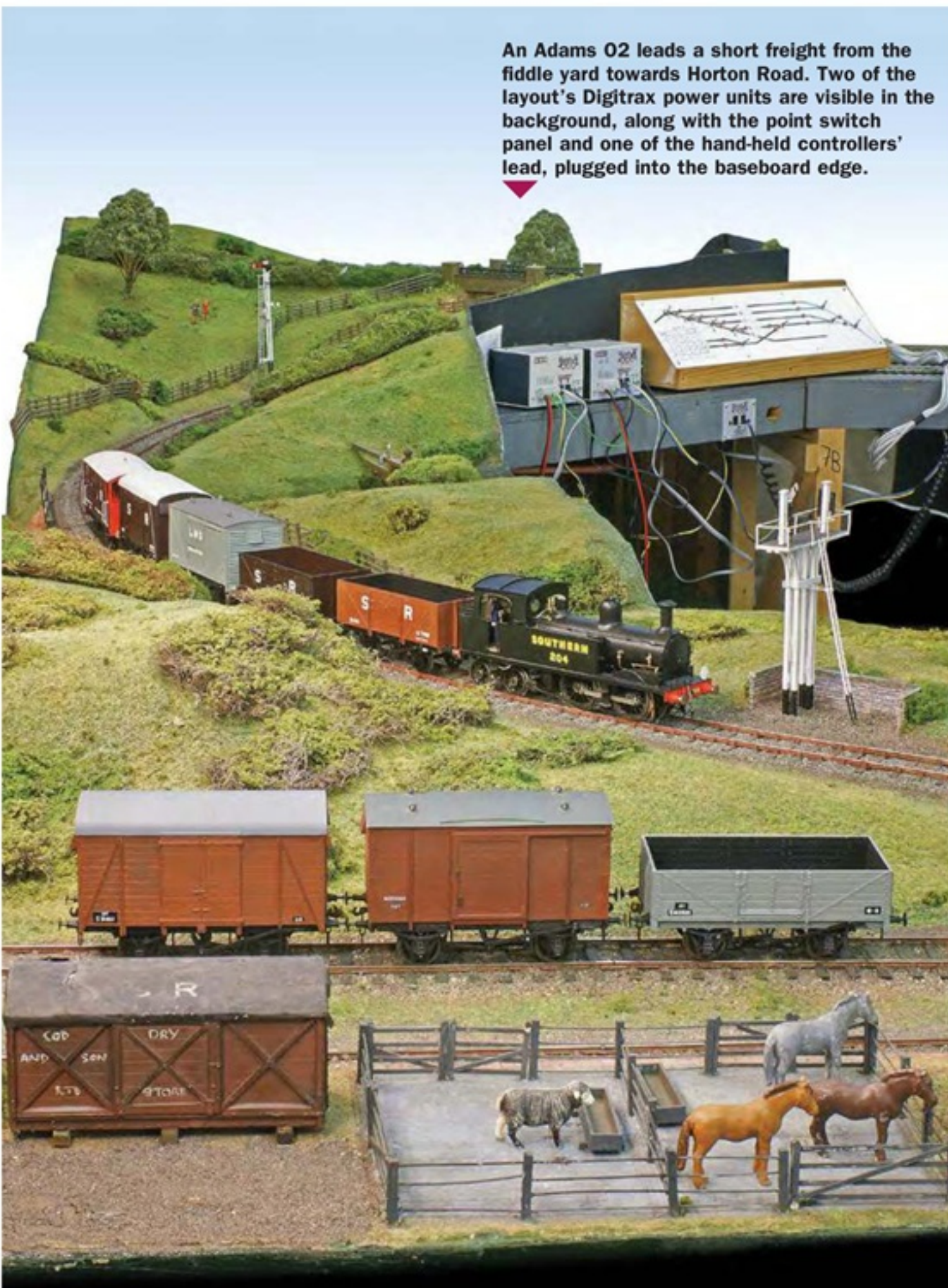
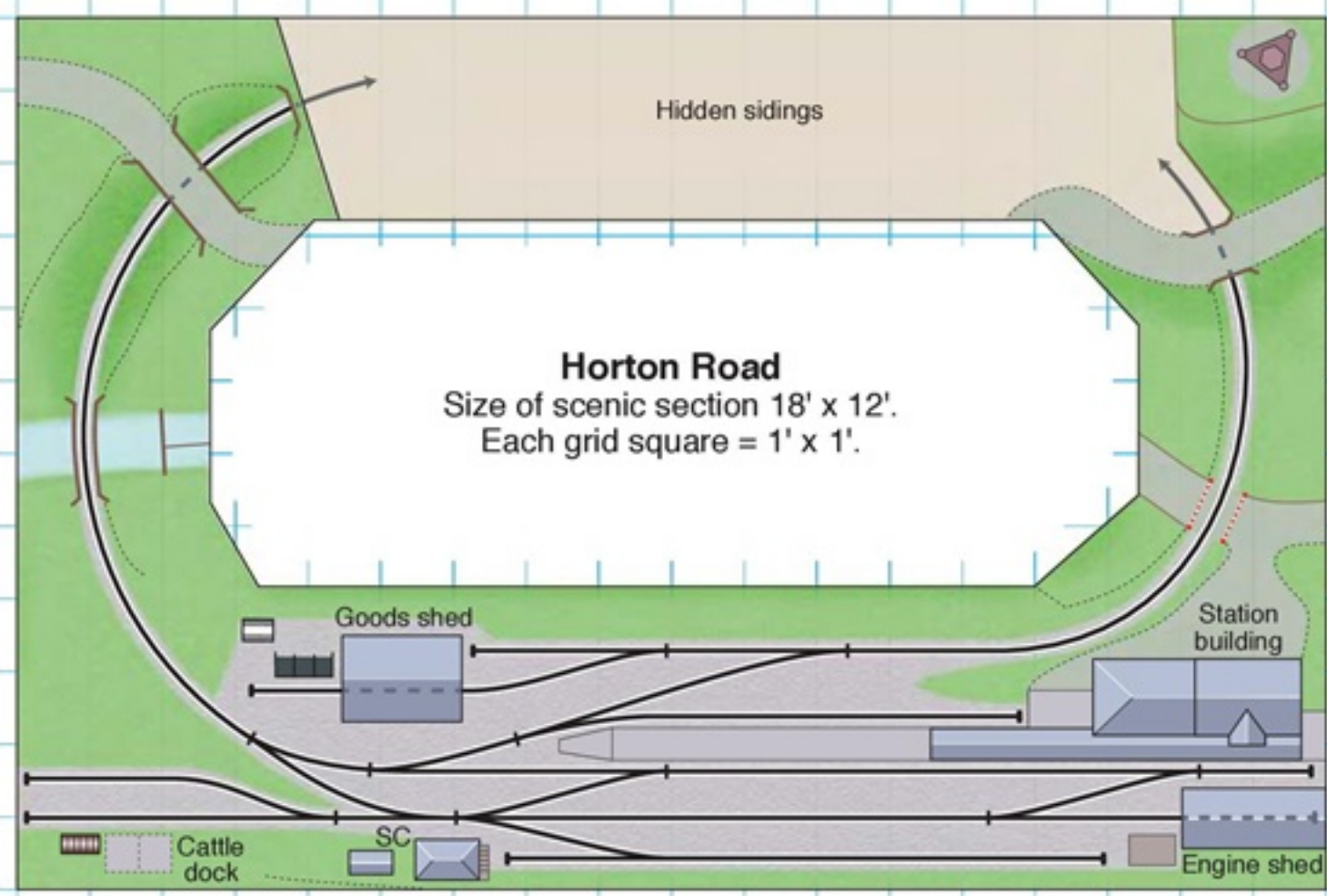
However, once everything was on an even keel, we switched the layout on and waited for things to come to life. Well, we waited and after so long out of use, about only half the layout became energised! We realised at that time that a different approach was called for!



One of the shed staff is interrupted in his work by the arrival of a Drummond M7 fitted for pull-push working.







## Transition

Being forever optimistic, we set about fault finding and got the layout to run reasonably well following a few Tuesday night sessions with a soldering iron to create a short-term fix. The old Swiss-built point motors groaned a bit due to sticky contacts so were thoroughly serviced. Our next objective was to look at the flora and fauna, buildings and track condition. The scenery had a distinctly blue hue across large areas, where the light over the years had attacked it. So nothing out the ordinary here, after so many years of being in and out of service.

It was suggested we might like to exhibit our layout at WimRail 2011 (our biannual exhibition) and this gave us a target date to aim for: there's nothing like a deadline to focus the mind! So more remedial work was undertaken, with this goal forever in our thoughts.

Our analogue controls prevailed and some scenery and track repairs were duly undertaken and, before we knew it, we were heading for the venue to unveil our layout to the great British public. Once the electrics had really warmed up over the next two days, odd things started to happen, the most serious being the failure of the main station throat point motor.

Fortunately the branch was not affected and we were able to run trains around the circuit from the fiddle yard and back to the fiddle yard, through the rear of the station. The situation was not ideal, but we scraped through to the end of the show.

## The future

Undeterred, once back in the clubroom a meeting was held to decide the layout's future and carry out a total rethink of its operating potential. A suggestion from our club techno man – who loves everything DCC and had far more knowledge at the time than any of us – gave us food for thought. Why not go DCC and get rid of section switches? With everything live, you can move a loco anywhere, having dialled it up of course, to move it. Even using a mobile phone app! Wonderful!

The sceptics amongst us old school modellers were being slowly convinced of the merits of the 'new world' and the possibilities of what it could produce. As it happened, one or two of us had been fortunate enough to be invited to a privately owned totally DCC layout controlling around 250 trains by computers and using Heathcote Electronics IRDOTs for automated signals/pathway detection. A fantastic evening, one that got even better when we were invited to participate in running the trains!

## Decision time

So armed with more knowledge, the next club night saw us meeting to discuss the

An eclectic mix of stock is in general based around prototypes local to the club's Dorset region. The Heljan Class 33 was a must-have, and now has been fitted with a sound decoder.



merits of a total upgrade and of course the likely costs involved. Plans were drawn up and the first stage was to remove the old wiring and change the point motors for Cobalts, these being smaller than the Tortoises. With no PCB connection edge strips, these were less likely to sustain damage in transit. The only drawback with these units is you cannot (gently) manually

Perhaps the driver of the ragtop 'Moggie' has joined the photographer to capture the Class 33 easing around the tight curve? Available space restricts the layout to 5'6" radius track.

Two passengers, having left from the railbus, stretch their legs. The chap with the binoculars might be about to spot trains not birds...







Private owners and BR 16T minerals are happy partners in the coal siding. This view was shot from inside the operating well.

We chose Digitrax units to power our system, after lots of research and advice.

### Scenics

One of our small group is very much of the artist fraternity and has produced a real change on the landscape. We now have hedges along the lanes, trees where there were none and bushes to replicate the hillsides around Dorset, with the scrub so much in evidence, as it is, locally.

Woodland Scenics products were widely used to improve the visual impact of the layout, and these are readily available within our locality. Fencing is one item we had to reinstate, as a lot of the previously installed fencing lines were past their best. The grass was upgraded using static grasses, applied with what can only be described as an electronic sieve!

### Trackwork

The fiddle yard was deemed not user-friendly and was subsequently lifted and renewed using Peco Streamline flexible track and home-made points, utilising Peco components, soldered to copper-clad sleepers. These are predominantly curved two- and three-way points. Smaller locomotives run well through all of it, but larger locomotives are restricted to the two outer tracks, set at 5'9" radius.

Some of the branch line tracks were also renewed, easing the curves as much as possible to give us a little more freedom with the larger engines.

### Locomotives and stock

The club has never owned its own locomotives and stock until last year; we

operate the point blades, for checking the blade throw.

So having committed ourselves to a DCC upgrade, we installed the main track feeds using heavy duty copper wire and 5A lighting

junction boxes. We also used heavy duty ¼" phono plugs and sockets to give plug-in interboard DCC power supply facilities. Once the new three-way point had been installed in the station throat, we tested everything, using an analogue controller and the 'one engine in steam' principle to test board by board. The hardest part to solve was the frog polarity issues; these really had to be addressed before attempting to use DCC power supplies, as they will trip out at the merest sign of a short circuit.

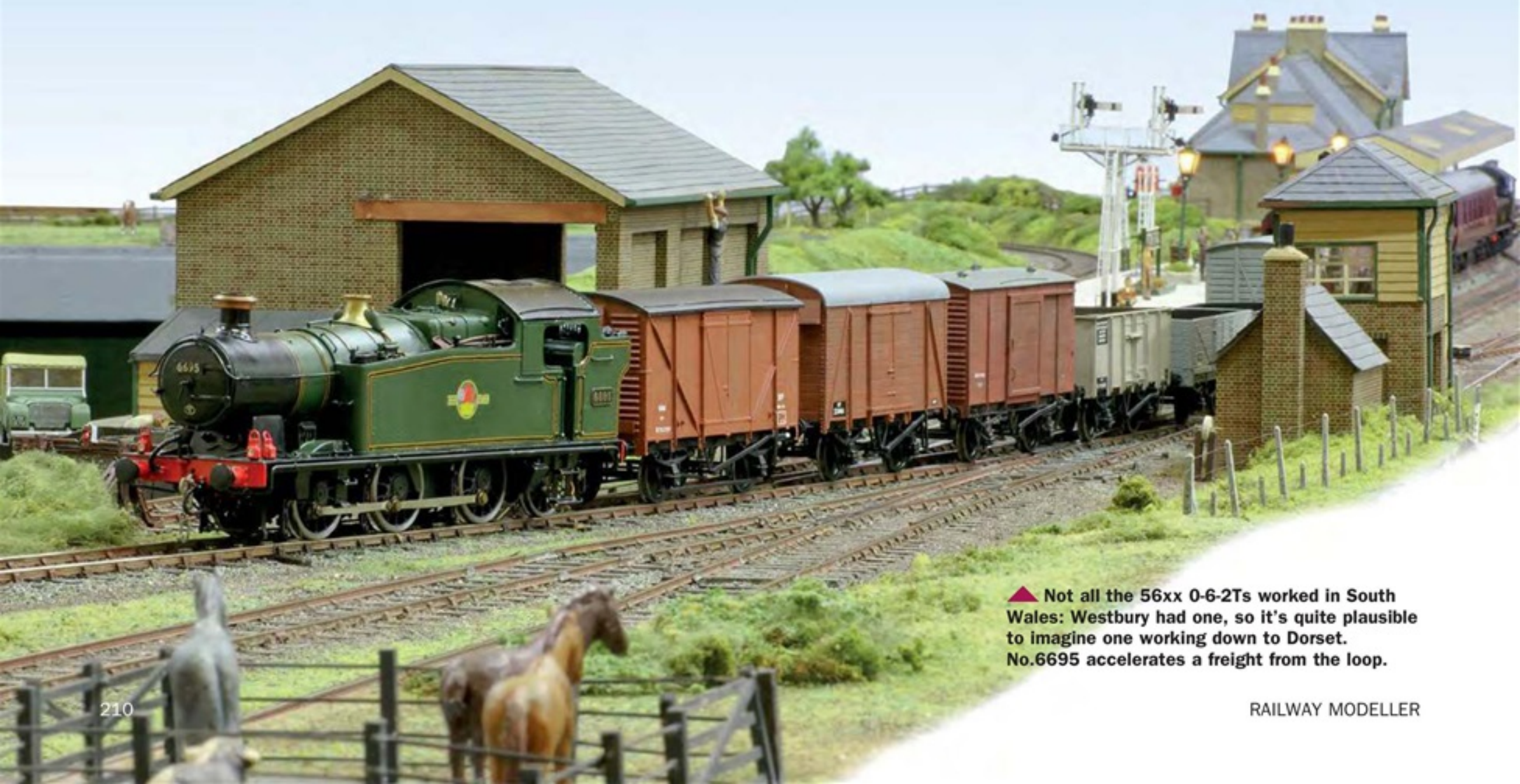
### Progress

We mounted the point motor switches on two specially made units, one for front of house and one for the fiddle yard; these operate on 12V dc independently. We felt that keeping the track supply and point motors on separate circuits would not cause us problems in the future.

The signals have been wired in and the lighting circuits too, during the last year, thus completing the revamp electrically.



Some cameos are quite perilous!



Not all the 56xx 0-6-2Ts worked in South Wales: Westbury had one, so it's quite plausible to imagine one working down to Dorset. No.6695 accelerates a freight from the loop.





have always relied on our members' own collections to operate the railway. Recently the club invested in some MTH BR maroon coaches. We would love the same manufacturer to produce some Southern green ones!

So what we run on the layout is down purely to our personal choice. The good thing is the Bournemouth/Poole area not only saw Southern engines, but LMS and GWR ones too. This is reflected in our own individual tastes. Several items are kitbuilt by our members and all have now been fitted with decoders (TCS T1s, 2A max units being my personal choice). Utilising modern motors and with a decent ABC gearbox, less than 0.5A consumption, is the norm. (One thing to watch for with DCC is the total current being drawn.)

### The next dimension

Well recent R-T-R products have really turned up the heat, with regard to new locomotives being commercially available (some with decoders fitted and sound too) from manufacturers like Dapol, Ixion and Minerva all priced very keenly; these locomotives took us into a new dimension. We had experience with sound some 10 years ago, as one of our club members has an RJH kitbuilt Class

33 with a decoder and sound. The layout was still analogue then, which meant some of the bells and whistles on board were not capable of being selected. We have amongst us a Heljan Class 33 that has also been upgraded to include sound; the future looks very bright on the locomotive front.

### Extensions

Can we reasonably extend the layout frontage, as the 18' length precludes a lot of visitors from enjoying the running of trains comfortably? My own personal layout has a scenic section some 20' long, reminiscent of the Somerset & Dorset, which could be used to make the frontage double in size. Possibilities abound!

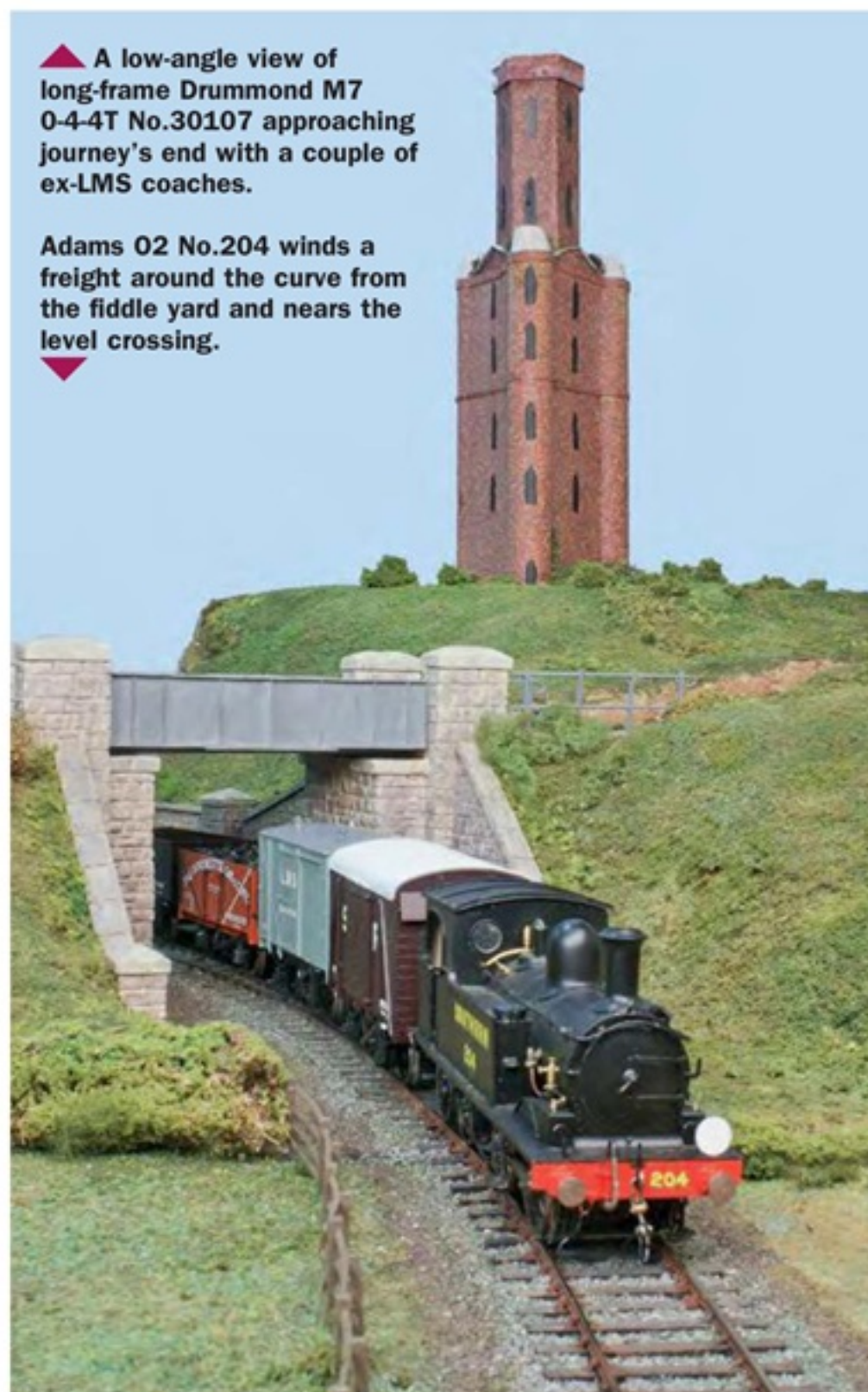
### In conclusion

So, from a sleepy Southern branch terminus situated somewhere off the 'Old Road' (known locally as Castleman's Corkscrew), this layout is slowly being changed to give greater satisfaction for operators and members of the public alike.

We hope that those who saw us at the shows we attended since the start of 2016 have appreciated our endeavours. As one local trader commented, as he took several photographs, "it's good to see it back!"

▲ A low-angle view of long-frame Drummond M7 0-4-4T No.30107 approaching journey's end with a couple of ex-LMS coaches.

Adams O2 No.204 winds a freight around the curve from the fiddle yard and nears the level crossing.





# Talking Points

Topical issues from the world of model railways

## Is your club website fit for purpose?

Gwion Davies offers some tips and advice to budding model railway society webmasters

It seems that website design and development and model railways generally don't mix well. There is hardly any crossover between the skillsets needed in each situation, but in today's digitally influenced world a really effective website can be a massive help in raising the profile of your club.

The pages of websites are created in a special code that computers translate into the digital content we see on the screens of our devices and computers. The task of writing the code (or 'coding' as it is generally referred to) is an intricate process which individuals can learn, but it is a discipline that requires a degree of technical and abstract understanding, and not everyone finds it of interest or easy to grasp. So when the Swansea Railway Modellers Group's club website was rebuilt in late 2014 it was essential that the content could be updated easily without the need for deep knowledge or understanding of coding. This was to allow other members (who generally possess average computer skills) to update content themselves with ease, but to do this we needed something called a 'content management system' or CMS.

Fortunately in the last decade or so, numerous web design CMS platforms have emerged that provide a half-way house between coding from scratch and designing pages via drag, drop and upload processes. The better-known ones are Squarespace, Wix and Wordpress, but there are many more. They are perfect for allowing multiple members to update content online without the need to know how to code. However, even with platforms like Squarespace it's still down to the designer to use good web design practices.

A fair number of railway society websites I come across use web design practices from the 1990s. Back then they would have been acceptable, but now today, look very dated and do not work well on modern hand-held digital devices such as phones and tablets. It is vital today that websites work, and are easy to read on smaller devices as well as computers. Fortunately the website building platforms do a lot of the heavy lifting for you, including automatically making your web designs responsive so that they work properly on other devices.

### Choose your platform

The first step for a budding club webmaster is to select your platform; for the Swansea site we chose Squarespace, but others are just as good. You will be required to register an account and set up a payment means, though the charges are not onerous. The platform manages all the backroom activities for you, like domain name registration and hosting, and there are usually lots of design templates from which to choose. Once past the initial registration stage, it's over to you to generate your fabulous and spellbinding content.

### Recommendations

Here are some things I would strongly recommend you consider when making your next website:-



▲ A good home page will be designed so as not to be cluttered or 'noisy'. Avoid intense luminous colours and stick to a palette of conventional or shades and tints. Use only black, dark grey or white reversed out of black for text and stick with easy-to-read sans serif fonts such as arial or verdana.



1) **Clear navigation.** Visitors don't want to guess where to go, or waste time deciphering quirky terminology and phrases, or even in-club jargon. Imagine what it's like if you used a complicated sat nav device where all the buttons had strange symbols or all the options were in a foreign language. Remember, not everyone knows what a stuffing gland is...

▶ **Love or loathe social media platforms, they are here to stay for the foreseeable future.** Lots of people use platforms like Facebook and Instagram and your club needs a presence if only to help guide them to your website. The Swansea Railway Modellers Group Facebook page includes a very up to date club status post.



2) **Use appropriate colours.** The colour scheme of your website is important, harsh/high contrasting colours can be off-putting. Stick with simple light, pastel colours and use blacks or dark greys for text, just like you would see in a book.

3) **Take advantage of Social Media sites** such as Facebook, Twitter and Instagram. These can be great for engaging with your followers, potential members, show customers. Media platforms like YouTube and Flickr are also useful for hosting your videos and photos, especially if the web hosting package has a data limit.

4) **A picture is worth a thousand words** as they say. Visitors aren't visiting to read a novel. Walls of text appear daunting and most visitors simply won't bother. By using images to convey information, text can be broken up into more manageable chunks and make the visit far more enjoyable. If you have a lot to say, add a link to an optional blog page, or add a 'read more' link to the page. And remember; spelling and grammar mistakes will make you look like an amateur, so review your work very closely, or get a colleague to proof read it for you.

5) **Keep your website up to date.** Add the latest news about the club and remove (or archive) any information that is no longer relevant. If visitors notice your content isn't current then your website will slowly lose credibility.

6) **Stay mobile-friendly.** More online content than ever is read on phones and tablets, so make sure your design caters for this. Fortunately most modern website building platforms will do this for you automatically.

**Keeping up to date with web technology is a must.** These days lots of people use mobile phones to browse digital content, so it's useful if your club website is 'responsive' ie it responds to the screen size of the device the visitor is using and adjusts the size and arrangement of the content accordingly. Many of the CMS platforms mentioned in the text will do this automatically.

## SWANSEA RAILWAY MODELLERS GROUP

SHOWS HOME PROJECTS BLOG PRODUCTS ABOUT US CONTACT US AFFILIATIONS



### - Club Projects -



St. Davids Wells



Box Station



Downton Road



Test Track v2



Kinaston



Elsbridge



Cwmfelin Parkway

Use images to help the visitor navigate around the website. Pictures are understood better than lots of words.

ically, though for more information look up 'Responsive Web Design'.

7) **Keep on testing.** Whenever adding new content or features to your website, you should make a habit of testing to see that it works. Check all the links and make sure they are linking to the correct page. It's always better to find these issues yourself, rather than have a visitor tell you.

8) **Silence is golden.** Don't use unnecessary audio, or at least audio that comes on by default. The last thing a visitor wants is unwanted audio blaring from their device if they are in a public place or noise-sensitive situation.

9) **Use easy to read fonts.** Avoid script or calligraphic fonts on websites which don't render well. Stick with simple San-Serif fonts for the easiest readability.

10) **GDPR and Cookies.** Unless your website will be designed to collect user data (eg email addresses for a newsletter) you don't need to worry about this.

11) **Avoid using animations (.gif)** in general they can make the website load slower, and can be rather off-putting.

Finally, make it easy for visitors to contact the club. Put the contact information in multiple places so it's easy to find. It should always be no more than one click away, and if you must use content forms, keep them simple. Ask the fewest questions possible, as visitors hate completing all those field boxes (don't we all?) and they may not wish to trust you with the information.

In essence a good website is a website that people are able to use easily and intuitively. Not only that, search engines like Google prioritise websites that are built and function better. With the majority of next-generation modellers using the internet as a first port of call to obtain information, having a cracking website and utilising social media will aid them to discovering your club.

Although videos and animations can cause your site to load slowly or buffer, a good place for a video is on a landing page which invites you into the site.





The Associated Joint Midland Railway

# Wren Valley

Photography by Paul Bason

Frequently thought of as a garden railway-sized scale, 16mm scale on 32mm gauge track (SM-32, representing 2' prototypes) is perfectly suitable for indoor use. **ALAN PETTITT** is our guide to his setup, based loosely on the Welsh mixed traffic lines of the mid-20th century, installed in a former garage.



**M**y modelling career began around 45 years ago after my wife noticed how fidgety and unsettled I was away from work, and that I needed something to focus on. I began buying *RAILWAY MODELLER* and remain a subscriber to this day. At first I worked in 4mm scale and over the next decade I created three layouts, the third (*Oakdale*) being loosely modelled on the LNER in north Suffolk.

My move to 16mm scale began after I read an article about Tom Cooper – aka Mr Merlin – who had developed

remote-controlled live steam locomotives during the 1980s from his base in Llanfair. As chance would have it, a holiday to Wales was on the cards, and whilst there, I visited Tom and ordered a 16mm Merlin Maestro 0-6-0 loco there and then. Soon after I sold the *Oakdale* layout and all my 4mm rolling stock to fund my move to 16mm.

Most people associate 16mm with garden railways, and my first layout was just that, running round the 33' x 20' back garden of our house in Welwyn Garden City. I was lucky to find a kindred spirit, John Hodges,

living close by and over the next 20 years, we modelled both separately and together, helping to develop *Ridgmont*, the first club layout of the Beds Area 16mm Group. We also built *Llanwelwyn*, a 33' x 7' layout that was displayed at St Albans and Stoneleigh. To be transported, it had to be broken down into 40 component parts.

In 2007 we moved with my daughter, son-in-law and their two children to rural Suffolk. My rolling stock, track and buildings were packed up and didn't see the light of day for another 18 months. Though used at





“ – a former double garage presented the perfect opportunity to build an indoor 16mm narrow gauge layout – ”

Alan Pettitt



▲ The small railbus at Somerley station.

first for storage, a former double garage, approx 20' x 16', presented the perfect opportunity to build an indoor 16mm narrow gauge layout, with much greater detail than the typical garden railway. It took a year or so to develop the plans before I got round to starting construction. In doing so I had to incorporate two workbenches and a lifting flap by the entrance door to the building, now renamed the Station House.

### Overview of the layout

*Wren Valley* was not modelled on any particular place or time, though any steam narrow gauge railway instantly brings to

mind the industrial lines of North Wales. I planned to reuse both stations from my garden layout, with a substantial goods yard and engine shed among the new additions. Running round the inside of the Station House, the layout would vary in width from just 7" where single track, to 42" for the goods yard.

The layout was built in sections and designed to be freestanding; because the walls had been dry-lined, I decided not to secure the framework as the loadbearing

◀ Roundhouse Jack Alice at Somerley station with scratchbuilt De Winton quarry locomotive pulling a short gravel train.

▼ Merlin Mayflower Carol Ann pulling a short freight train, passing Somerley station.







Scratchbuilt 0-4-0 tram engine at the level crossing near Sheden station.

Accucraft 0-4-0T *Sir Caradoc* leaving Somerley station working a mixed train.



strength of the lining was unclear. The baseboards are ½" MDF with 2" x 1" bracing and legs. The MDF was sealed with PVA on both sides before being fixed into place; this is important as live steam locos will drip

Merlin Mayflower 0-4-0T *Carol Ann* and the *De Winton* by the coal stage with the water tower behind.

slightly as they are brought up to pressure. The baseboard joins were made with locating dowels and bolts. Over the two workbenches are two tunnels, one 38" long, the other 60". The track and points, all Peco, were reused from my previous garden railway, the ballast created using fine garden centre grit, glued with PVA and sprayed with matt black car paint. Scenic structures began







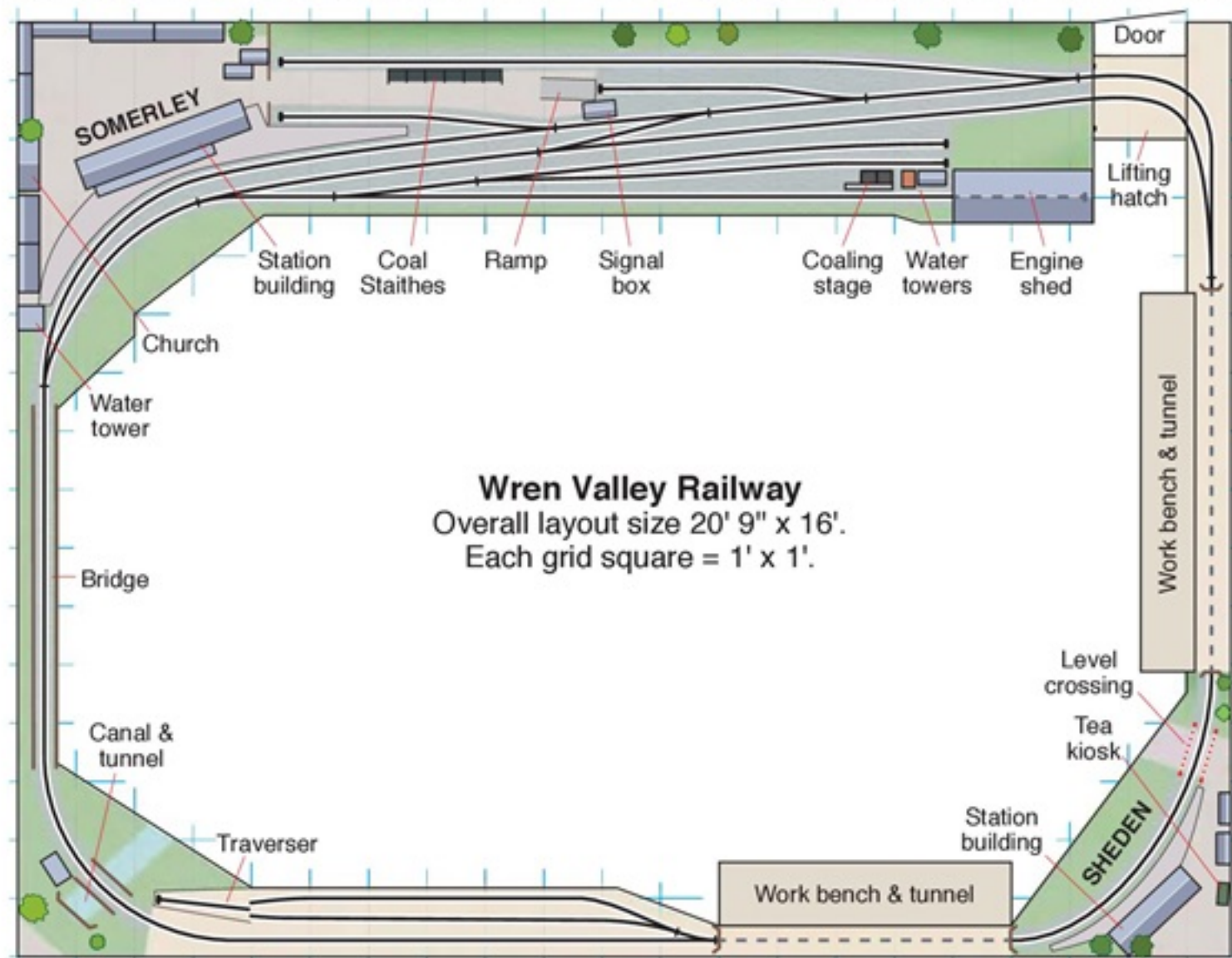
with a wooden frame, covered with card, and finished with fur fabric sprayed with Humbrol green paint to give the grass effect.

### Structures and scenics

All the buildings and platforms were scratchbuilt or adapted from kits. The Somerley station building, 17" long, is based around two Brandbright station kits at each end, joined by a scratchbuilt centre section, with detail added to the interiors. The engine shed was another adapted Brandbright kit, with the addition of a detailed workshop and inspection pit.

The village of Somerley sits at one corner of the layout and is modelled in low relief. The cottages were constructed around frames built from firework sticks gathered up after Bonfire Night; a very convenient and inexpensive way of getting materials! The walls were made from hardboard with the rough surface facing out so as to give the impression of render.

The water tower was scratchbuilt and based on one on the Welsh Highland Railway at Beddgelert. The signal box, again







▲ **Fowler 0-4-0 diesel loco Muffin pulling a breakdown train over the canal.**

scratchbuilt with a full interior, is based on an example in York Railway Museum. The two bracket signals were both kitbuilt, fully working with levers operated from the edge of the baseboard; other signals are old Hornby O gauge signals, worked from the points. The backscene, produced by ID

Backscenes, features real photographs and was pasted to freestanding panels. All the above buildings, platforms and signals can come off the layout, leaving only the track and basic scenery.

Most of the accessories have been acquired over the last 30 years. This includes some of the figures, but most of them I constructed myself to my own designs. These were made from flesh-coloured Fimo modelling clay with a metal armature inserted, dried, baked and then painted with enamel paints. For a brief time I made resin moulds from these figures and cast copies in Crystalcal plaster which I sold at railway fairs. I also made multiple sheep using this method. Trees were modelled around real cuttings, cut and shaped to scale and style. The canopy was modelled using upholsterer's horsehair, stretched over and sprayed with Humbrol spray paint. This was sealed with hairspray and sprinkled with Hornby 4mm scatter material to give the impression of leaves.

▼ **Donegal railbus approaching Sheden station. The tree on the left was formed from pieces of yarrow, taped together and spray-painted.**



## Live steam and battery power

The rolling stock, collected over a period of 30 years, is a mixture of kitbuilt and scratchbuilt. There are seven live steam locomotives on the layout, three of which are radio-controlled. These include a Roundhouse Jack 0-4-0 with scratchbuilt tender housing the electrics, and a Merlin Mayflower 0-4-0T. Five locos are battery-electric, one being radio-controlled; the rest have on-board speed controllers. The railbus is scratchbuilt, based on the examples that ran on the County Donegal Railway in Ireland during the 1940s and '50s. This includes a detachable roof to reveal the detailed interior, and is controlled by a knob modelled as the radiator cap at the front. This railbus took me two years, on and off, to construct.

The layout is maintained in a ready-to-run state, as are the battery-operated engines. To anyone beginning in 16mm scale, battery-electric locos are probably the best starting point, as live steam engines need a lot of preparation. In my preferred order, the tank is filled with butane gas, the boiler filled with de-ionised water (or rainwater), and the reservoir filled with steam oil. Before lighting up, it is important to make sure that any gas fumes have dissipated. There is then a five- or six-minute wait before pressure builds to 40psi, the usual pressure required for running.

## Closing thoughts

The world of 16mm modelling has changed a great deal over the last 30 years. Back then, off-the-shelf parts and kits were quite limited in numbers, but more recent developments, including laser-cut kits, have widened the field dramatically. My aim has been to build a 16mm layout that looks 'real' at track level, and the inspiration for this has come from reading about other layouts, regardless of scale, in the monthly editions of RAILWAY MODELLER. Ultimately I am a 'builder' rather than a 'runner.' The AJMR does not run to a timetable nor does it seek to emulate a





An overview of the engine shed area and steam-up bay, with Somerley station in the distance.

▲ Scratchbuilt 0-4-2ST *Patience* hauling coaches over the canal.

*Phoebe* is in charge of a slate train passing Somerley signal box. The slate wagons are based on those used on the Corris Railway.

particular place or time. To get the most out of the scale, it is best to join the Association of 16mm Narrow Gauge Modellers and get involved with a local club – see the website at [www.16mm.org.uk](http://www.16mm.org.uk).

Sadly my long-term modelling partner John Hodges passed away in 2015, and he was kind enough to leave me virtually all of his rolling stock, much of it being scratchbuilt. Although not from an engineering background he taught himself to work a lathe, and went on to make several steam locomotives, including two tram engines and a De Winton quarry locomotive. As well as scratchbuilding the chassis and bodywork, he also built the boilers, pistons, gas tanks – the lot, all bar the nuts and bolts. His garden layout was called the Glen Valley Railway, and I have maintained the GVR livery on John's rolling stock on my layout.





# City of Tiers

Photography by Steve Flint



As seen on TV in

THE GREAT  
**MODEL RAILWAY**  
CHALLENGE

Winners of Channel 5's The Great Model Railway Challenge 2018 – The Aberdeen Model Railway Club – unveil for the first time in print the story of the incredible layout which clinched the club the title of Britain's Best Railway Modellers. The layout can be seen adjacent to the Peco/RAILWAY MODELLER stand at Model Rail Scotland over 22 – 24 February 2019 at the SEC in Glasgow. Words by **KARL STEVENS**.

**W**ith *Clucas Bay* (see last month's RAILWAY MODELLER) safely back at the Aberdeen MRC's clubrooms following its triumph in the Waterworld episode of the Channel 5 programme, the consequences of our success began to sink in – we had now to do it all over again for the grand final of the TV series.

The first stop was another project planning evening, back at our HQ and again in secret. Unfortunately, due to work commitments, we had to substitute heat team member Mike S with a new team member, Mike O. The theme for the grand final was A Journey Through Time. We settled with tackling

▲ A 21st century Scotrail express headed by No.68 007 Valiant glides along the sky track way above the toxic clouds. Even the unintentional bubbles that subsequently appeared in the backscene print add a sinister and foreboding element to the entire scene.

The star train was a modified Hornby Class 800 five car set which ran the full 50' length of tracks spanning all five layout entries in the grand final. Above the clouds is the sky elevator, cunningly designed and created from a cut down plastic drinks bottle.

## Model Rail Scotland

See *City of Tiers* along with the club's heat winning layout *Clucas Bay* (as featured last month) at Model Rail Scotland at the Scottish Event Centre in Glasgow, 22 – 24 February 2019.

Full details in our Societies and Clubs section on page 261.







▲ Let the final commence... a member of the film crew makes final adjustments to the equipment above Aberdeen's bare baseboard.

railways in the future; rather than a historical period. Being a very different theme from the usual model railway project, it evoked our imagination and set our creative juices flowing.

### What would a future with railways be like?

Lots of ingenious ideas were sifted through until we landed on the concept of a multi-level layout which we would build in forced perspective – not towards the horizon – but in vertical planes, looking down on a future world from a lofty height. The smallest scale models would thus appear to be at the bottom of the scene.

We drew our inspiration from the movie sector in which dystopian visions of the future of our planet are often portrayed. The layout conceptualisation was derived from a mix of Hollywood blockbusters like *Blade Runner* and *Ready Player One*: movies which depict the earth laid waste by pollution, and socially divided by wealth and power. Our futuristic idea would see the star train flying across the layout in a clear blue sky, where the floating houses of the powerful hang in mid-air and look down on the lower classes of society who have to dwell in the darker and dirtier layers of the lower biosphere.

With the subject matter sorted, we again made a 3-D plan of the layout that would be the blueprint for the build. The grand final rules did not permit any pre-built models; everything would be built on set. All we could do was finalise the plan and order the materials which were to be delivered straight to the to the filming location.

So, starting from the back we needed a scene that would give the perspective of distance and the high life. Cities for the skyline would be taken from pictures around the world and scattered over the scene.

The delighted winners with their Britain's Best Railway Modellers trophy.

### Flying track

In the final all the competing layouts were going to be linked together to make one 50' long spectacle through which a special 'star train' was to travel along a specially laid through track. We wanted our section of through track to appear in gravity-defying suspension in mid air so the star train would appear to fly through the scene: but how to do it?

Team member Mike S had previously made a house sign out of thick Perspex which didn't bend so we found a supplier in Leicester, and we agreed to collect the sheets on our journey down to Fawley Hill.

After filming the Intro sessions and group shots, we got down to the job of building the layout from scratch: opening all the boxes from the suppliers.

The production team had set out a specific set of dimensions for the five teams' layouts, which would all be put together for final judging. This was done in a similar way to



▲ With filming underway Mike and Fin get to grips with the baseboard alterations for the layout's N gauge ground zero level.

modular layouts with track spacing, height etc defined. Because this was to be controlled by TouchCab (the iOS app developed by myself and Mike Sutherland), and due to Aberdeen Model Railway Club's vast experience in DCC, we prefabricated a DCC bus for each team's baseboards with parts supplied from KMS Railtech. We also supplied a detailed set of wiring instructions to each team and offered help on site to those who were not familiar with DCC.

Our layout was to have three levels. Our 'ground zero' level would be built on the surface of the baseboard provided. This would feature an N gauge circuit running in and out of high rise towers set amongst the streets. The atmosphere here would be thick, dark and toxic. The second, middle level would be a OO gauge loop with







▲ A scene from the intermediate level with a commuter train running through the smog. The futuristic circular high rise blocks were forged out of underground drainage pipes.

contemporary trains running through a less polluted atmosphere. The top level as described above, would feature trains in the cleanest of air.

Our team plan was to finish ground level on day 1, move onto the second level on day 2, then finish level 3 on the third and last day. Easy... or so we thought.

Ground level needed culverts for rain water and sewerage, bridges, roads and tower blocks together with a backscene. The culverts were easy, just strips recessed into the baseboard surface, painted, and refilled with dirty acrylic water; road inlay was sorted and track laid. Team member Colin laid his first ever N gauge track and began wondering whether he would be excommunicated from the Scalefour Society!

For the tower blocks, we had to devise a quick but effective way of representing these in the time available to us. They were made from blocks of polystyrene foam covered in printed 'housing' paper sheets. The original idea was to have a 3-D effect for balconies and cladding, but we actually ended up using coloured pens to highlight the layers. Each elevation was cut out and stuck to a side of the foam block, and even though we took turns cutting and pasting, the tedium and repetition got to team member Ewan at the end of the day.

### Keep 'em guessing

Each time a judge, presenter or a film crew came over they would ask, "So, where is the main track going?"

In the competition rules this track had to be at a pre-determined height from the floor so as to link to all the other contestants' main tracks.

"About here," we would reply, waving our hands roughly 3' higher than everyone else's track level.



▲ A ray of sunlight penetrates the noxious gloom at ground zero. This lowest level was built in N gauge to create vertical forced perspective and an illusion of great height.

They would then walk off with a bemused expression. We would smile and keep on working expeditiously.

At the end of day one, the N gauge track was down and wired, some ground work on the housing estate and a road system were also completed. We were a little way behind, no, we were far behind. Back in the hotel bar that evening we identified which beers and wines were the best, then planned how we would catch up.

### Day two and more jeopardy

At the start of the second day we still had no main track, no first level or top level, no backscene or housing estate.

We all cracked on and finished the base level. Lights went into the high rise flats and corner shops. The streets were painted and pavements went down. By mid-day we were back on track with our schedule.

The intermediate level was to portray a more desirable environment – less pollution

but still smog-bound. There would be brighter houses, a Faller road system and commuter trains to move people around.

Dark clouds were formed on the underside of the second level using glass fibre matting lightly sprayed with a black aerosol and glued around the tops of the base level buildings. Lighter 'smog' on the second level was to be formed this way with white-painted fibre glass, but now we had problem...

Plan A had been a structure that I had brought from my shed at home. It was made from parts of oil rig models which had been on display in the foyer of Aberdeen Exhibition and Conference Centre. It was going to form the buildings on the second level, but they were deemed as prebuilt, and thus disallowed.

That evening in the hotel bar we were in crisis mode. More wine and beer was consumed as we gave serious thought to the situation. All the other layouts had working main tracks and we hadn't even got the second level done: our main 'star track' wasn't even started.

### Day three

Backscene boards were built and fitted to segregate the sections and levels. The high rise buildings for level two that had been disallowed were replaced with circular ones formed from lengths of ground water drainage pipe. This was painted in various colours and lights were placed inside: the drainage pipe having factory cut holes which looked like tiny windows. With the backscene in place, Mike O was tasked with building a sky house for the top level. Meanwhile Colin was picked for the scratchbuild challenge taking him out of the main project – but it had to be done.

Wiring was completed and with only some of the buildings in place, it came to the point when we needed to get the star track to the same height as all the others. To do this we had to cut the legs off our baseboard. This looked challenging to everyone around and a film crew descended upon us, perhaps expecting a disaster? Happily it all went well, the legs were trimmed off and the entire 10' x 5' layout was gently lowered to the floor.

Our design now made sense to everyone: *City of Tiers* came into being, but we still had no star track, just the sheets of thick acrylic sheet that was to form the 'invisible' trackbed, but how to cut it to shape?

We marked out the sheet and began cutting with a jigsaw. It went well until we realised the acrylic was distorting due to heat generated by blade friction. A circular saw was deployed, a rather crude option that left a rough edge. So back to the jigsaw, this time



with lots of cold water to keep the workpiece cool. It worked and an hour later we had our star track link installed and powered.

With all our team now focused on finishing, we added lots of details and cameo scenes. A trailer park – reminiscent of that depicted in *Ready Player One* – was fitted and a working 'space elevator' devised. Electronic billboards were added to each level broadcasting propaganda in the best Orwellian tradition – the slideshows even included the judges posed as The Executioners.

### The reveal

All the teams had the morning session of the last day to finish off. Our club Lenz control system was being deployed for DCC control of the star train using an iPad loaded with the TouchCab app. Each of the layouts had brought something very different to the competition, all were very good and had been built over the last three days. But who would win?

Standing in the hall with everyone anxious and cautiously optimistic. Tim and James did their pieces to camera, Kathy and Steve put forward their reasons, then with everyone on tenterhooks Aberdeen MRC was revealed as the winner. We were stunned, then euphoric, followed by group hugs and handshakes. To be honest though, everything became a bit of a blur as interviews were filmed and the realisation sunk in.

We were a group of very happy amateur railway modellers, jubilant but also a little sad that it was over.

Our dear friend and fellow modeller Dave Clucas would have been glad to have been there.

Not everything he would have said could have been broadcast, but he was with us in spirit, we were sure.

We all feel we did him proud with both layouts, and especially *City of Tiers*, a touching wordplay reflecting our feelings when we heard of his untimely passing.

Dave, we all miss you.



▲ The ground zero trailer park scene was inspired from the 2017 dystopian movie *Ready Player One*. The 'trailers' were speedily made from thin card and part-sprayed grey and black.



◀ Fin and Karl hard at work creating the invisible sky track out of Perspex using an ad hoc cutting blade water cooling system.



▼ Is this a Virgin Galactic rocket or a Tesla SpaceX Falcon machine getting ready to take earthlings on an interplanetary trip to wonderful places? Meanwhile, the humble station platform, redolent of the travelling modes devised by transport pioneers from Victorian times, still puts on flower pot displays!



# A bridge too far



**T**his layout came about as a result of our club's participation as the Basingstoke Bodgers in the Channel 5 series *The Great Model Railway Challenge* last year.

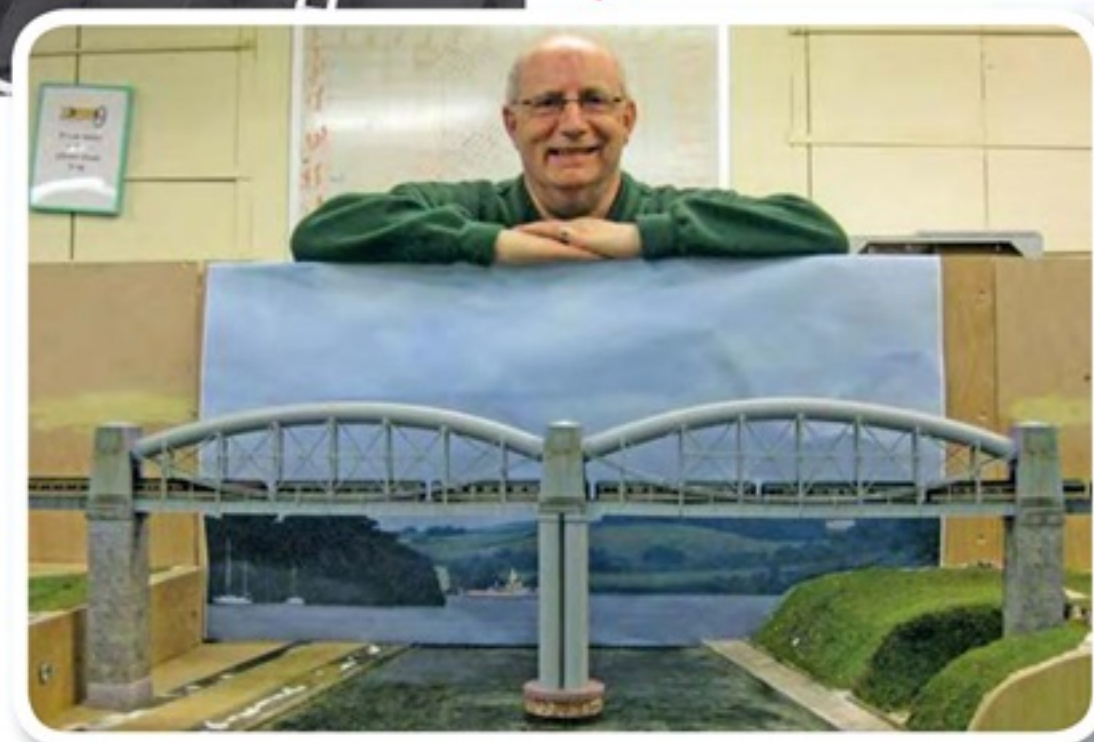
Having won our heat we then found ourselves in the final, and the theme was to be A Journey Through Time.

After kicking a few ideas around we decided that our layout would celebrate the genius of Isambard Kingdom Brunel, focusing on the period from around 1830 to 1865. We chose to incorporate a number of Brunel's achievements, including Maidenhead Bridge, Clifton Suspension Bridge and the Royal Albert Bridge. The layout was its own

journey through time as these are still in use today, and we illustrated this with the rolling stock we had running on the layout, from a broad gauge train through to an InterCity 125 high speed train traversing the Royal Albert Bridge.

Due to the time constraints of the challenge, ie producing a layout from scratch in three days, although we got two of the three bridges finished I just did not have enough time to complete the Royal Albert Bridge, though we were able to run trains over it.

The general consensus among the rest of the team was however that they would like me to complete it and have it displayed at our next exhibition.



I also wanted to finish it, but as part of a working layout - not just a static diorama.

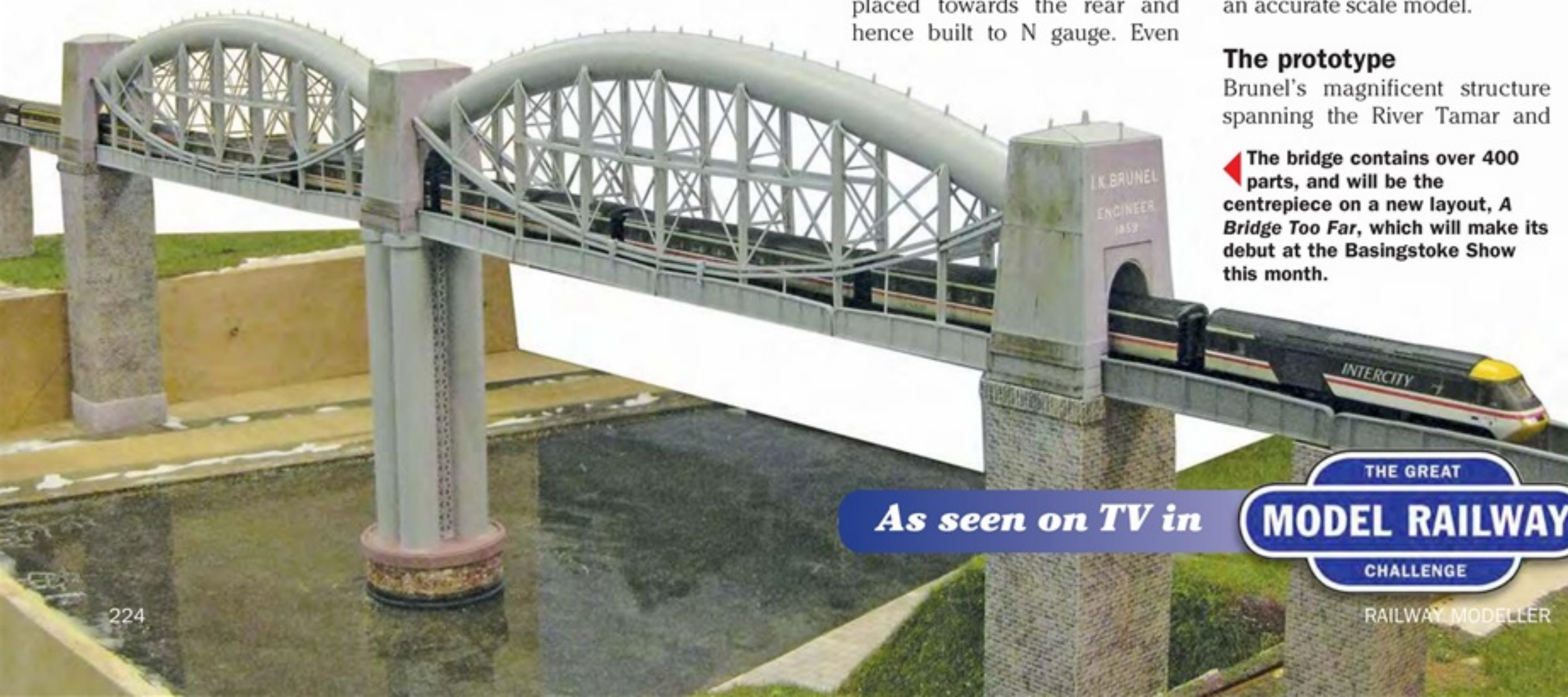
The competition layout was constructed in forced perspective with the Royal Albert Bridge placed towards the rear and hence built to N gauge. Even

**PETER THORPE** of the Basingstoke & North Hants Model Railway Society describes how building an N gauge model of Brunel's Royal Albert Bridge for The Great Model Railway Challenge became more of a challenge than he originally expected.

Photography by the author

◀ The real bridge in May 1959 sees the 'Saltash shuttle' propelled across by ex-GW Pannier No.6406. Photo: Hugh Ballantyne, Rail Photoprints

▶ The author with the finished model.



then with the competition board only 10' long, I still didn't have enough length to build a true scale 2mm scale model. Hence it is only half the length it should be; a depiction of the bridge not an accurate scale model.

## The prototype

Brunel's magnificent structure spanning the River Tamar and

◀ The bridge contains over 400 parts, and will be the centrepiece of a new layout, *A Bridge Too Far*, which will make its debut at the Basingstoke Show this month.

As seen on TV in

THE GREAT  
**MODEL RAILWAY**  
CHALLENGE

RAILWAY MODELLER



joining Devon with Cornwall took almost six years to build and was completed in 1859. Even today the bridge only carries a single track.

Brunel had plans for a double track timber structure but instead proposed a single track wrought iron design consisting of a single 850' span. As the cost of this structure would have been prohibitive, he amended the design to one of two main spans of 455' with 100' clearance above mean high spring tide; this was approved by the Admiralty and the directors of the Cornwall Railway.

At this point the River Tamar is over 1100' wide, and leading up to the main bridge spans are the 10 approach viaducts on the Cornwall side and seven on the Devon side, giving a total length of over ½ mile. I sourced some suitable drawings from the internet, and after making alterations to them to incorporate my design, printed them to the required scale (see photo 1).

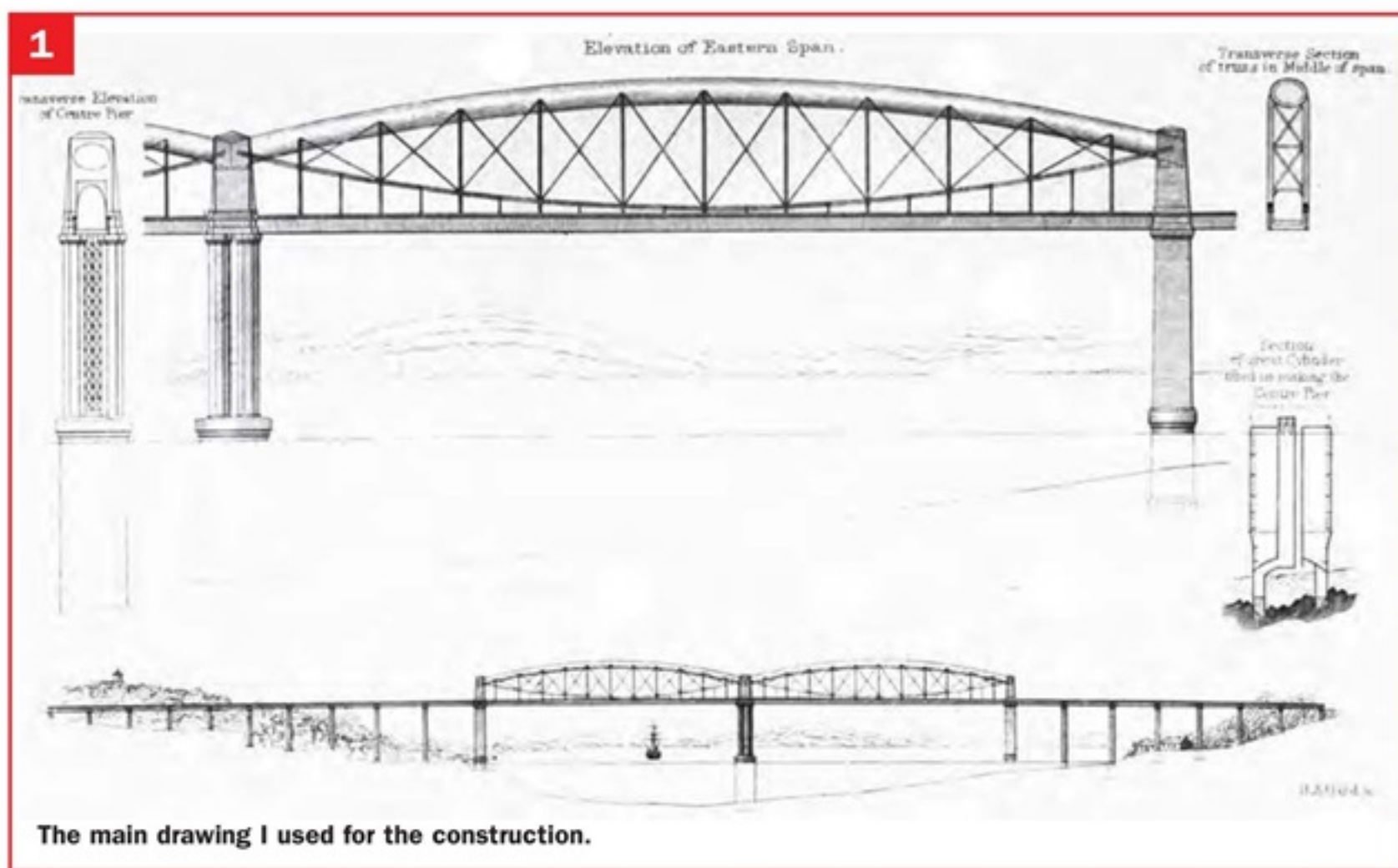
### The tubular arches

I decided to use plasticard, Plastruct and Slater's microstrip as the main construction materials for the model with Peco plate girder bridge sides, but the tubular arches required a bit more thought.

Although the arches appear to be circular, they are actually oval in cross-section, but this was a complication that I chose not to incorporate in the model.

After a quick search on the internet I found some Speedfit™ 22mm flexible pipe, which is made from high-density polyethylene. Although flexible it proved to be a bit problematical in getting it to stay at the required radius due to its tendency to straighten. Another search on the internet (how did we ever do without it in the past?) came up with the suggestion of using a heat gun. The gun enabled us to get some heat into the pipe while bending it but it was still tending to straighten out once we'd taken the gun away. However, holding it at the desired radius while it cooled did the trick (photo 2)

After cutting the pipe to the required lengths, and a couple more goes with the heat gun to make some minor adjustments to the curves, I had two almost identical tubular arches. On measuring the bridge spans later in the build I found them to be within



1% of each other, which I was very pleased with considering the relatively crude method used to produce them.

The arches were then spray-painted with Tamiya light grey primer; this was close to the actual colour required and an aerosol was going to be the easiest way to paint the model once all the cross members and bracing had been assembled later on.

### Bridge deck

This was constructed from 28mm wide strips of 1mm black plasticard with Peco plate girder bridge sides. These conveniently have a 1mm slot at the bottom in which the deck sections mated perfectly once the cut edges of the plasticard had been scraped with a Stanley knife.



Pipe bending – me demonstrating the pipe bending technique used for getting the curve in to the main arches.



Construction commences in the 'workshop area' during the TV final.

The bridge was eight plate girder sides long, and I added an additional one at each end to make it easier to line up the bridge with the approach spans. The bridge sides were painted with the light grey primer, and once this had dried the plasticard deck was fixed to the bridge sides with Contacta liquid glue, leaving a gap between each of them to enable the vertical members to fit later (photo 3).

### Cleaning the track

At this point the problem of how to clean the track on the completed bridge came to mind. It would be rather difficult (and impossible for some parts) to get to the track once the vertical members, suspension chains and diagonal bracings were in place.

The solution was solved by not fixing the track to the bridge deck. It is simply removed, cleaned, and then fed back

through one end to the other.

In order to keep the track in the centre of the bridge, I made 14 guides from the 1mm plasticard which I glued to the deck, equally spaced along its length. Each was roughly semicircular in shape to reduce the chance of sleepers getting snagged by the guides as the track was slid back along the bridge deck (photo 4).



Track guide on bridge deck – Close-up of one of the bridge deck track guides to keep the track centred.





**Close-up of the microstrip cross members attached to the vertical risers.**

## Portals

The three bridge portals were also constructed from 1mm plasticard, using a scaled photograph as a template.

The holes for where the arches enter the portals were drilled out with a flat wood bit, and then tidied up with a file.

Once the sides and tops had been cut out from the plasticard they were filed then glued together. Once set they were spray-painted with the light grey primer, and when dry the portals were positioned on the bridge deck with the tubular arches, which were fixed to the portals with two-part epoxy resin adhesive.

The finishing touch was to glue the scaled photographs of the ends to the outer faces of the end portals with PVA glue so they now bore the legend 'I.K.BRUNEL ENGINEER 1859'.

## Vertical members

The deck on the Royal Albert Bridge is slung below each of the tubular arches by 11 pairs of vertical members, but I had to redesign this aspect of the bridge due to the constraints of the baseboard size, and had just seven pairs on the model.

The verticals are cross-shaped in section, and to replicate this I used lengths of 3.2mm T-section Plastruct glued back-to-back. In order to maintain adequate clearance for trains crossing the bridge, I needed to cut away 34mm of the lower inner web of each of the vertical members. (photo 5). For the first span this was quite easy as when gluing the T-sections together I simply staggered them by 34mm, but didn't have enough lengths to do this for all of them so had to cut the vertical members for the sec-



**Just in time for judging. This shows how far I'd got with the build by the Sunday morning of the TV Challenge. Compare this with the finished model opposite, and on the previous page.**

ond span.

The first few I did I measured to ensure I had removed enough material, but I was rapidly running out of time so I ended up just judging by eye and thinking "That looks about right" – this came back to bite me later...

Also due to the lack of time I opted to make only five pairs of verticals for each span.

## Cross-members

From the drawings I had produced I was able to work out the length of each of the cross-members located between the pairs of vertical members of the trusses.

I made them from 1mm x 0.5mm microstrip.

I initially made each of the vertical trusses (comprising of the vertical member pairs and associated cross-members) as one component to be fitted to the bridge deck, but this turned out to be trickier than I had expected as I had no fixed datum to keep the pieces square to each other. In the end I only had enough time to complete one, and two others were half-done before being fixed to the deck.

At this point I glued the portals and arches to the rest of the bridge, and it was then placed on the layout on temporary columns made from pieces of wood wrapped with Metcalfe stone effect card (photo 6).

We were able to get trains running across the bridge in time for the judging, though this was far from perfect as they were catching on some of the vertical members – and of course it was some of those that I had thought "looked about right", and they obviously weren't. We had to run trains at full speed so that they had enough momentum to get

across without getting stuck, far in excess of the 15mph speed limit on the real thing!

## Aftermath

Once the TV challenge was over and I was back home with the model, the first task was to de-construct the parts of the bridge in order to rectify the clearance problems.

Fortunately most of it came apart relatively easily, and once the correct amount of material had been removed from the offending vertical members I was able to start rebuilding, continuing where I left off with the cross-members.

I made a height gauge from some blocks of plastic to keep the lower horizontal members at a uniform height of 34mm, resting them on this height gauge until the glue had set.

The pre-cut cross-members were then glued in place for each of the vertical trusses. With the vertical members fixed this was now a much easier job than my

first attempts, and I had time to construct the additional four trusses that I'd had to omit earlier.

## Suspension chains

These were made from 4mm wide strips of 1.5mm plasticard. My initial idea was to make each of these (eight in total) as a single piece with cut-outs to fit around the vertical trusses, but they were too weak at these cut-outs so I made individual pieces to go between each truss instead (64 in total). I painted these before cutting them to size and cut out notches in order for them to fit securely within the T-section of the vertical members.

Once the suspension chains had all been glued in place I then moved on to the supporting cross-bracing members between the two chords and suspension verticals which hang beneath the bottom chord to carry the railway deck. These were made from the same microstrip that I used for the truss cross-members, first painted and then cut to the required length before attaching.

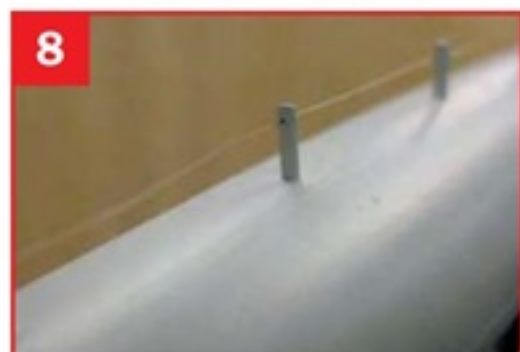
During the 1930s new horizontal bracing was added between the vertical members to strengthen the bridge further and keep the suspension chains hanging in the correct shape. Plastruct 4.8mm open web truss was a good representation for this bracing and this was cut to length with cut-outs to fit around the vertical members and side cross-bracing, and then painted before gluing to the vertical members.

When all of this had been completed I masked off the bridge deck and applied another coat of the light grey primer (photo 7).



**Once back at home I could concentrate on all the fine construction details without the pressure of a deadline. In place is the bridge deck, the vertical trusses, suspension chains and cross bracing.**





**8** Close-up of one of the microstrip handrail posts, all drilled with a No.74 hole to take the nylon thread.

### Handrails

To aid getting the handrail posts along the spines of the tubular arches as straight as possible and at equal distances apart, I produced a template of dots 20mm apart on CAD, and used this as a guide to drill 1mm holes to locate the posts.

I used the microstrip for the posts, and nylon thread for the handrail. The posts are 5mm tall with a No.74 drilled hole for the thread, and two holes in each of the posts for the handrails on top of the portals. After gluing the posts in position I tied a knot in a length of nylon thread and then passed it through each of the holes, fixing at the end with more glue (photo 8).

### Support piers

The masonry piers at either end of the bridge were constructed from 1mm card and wrapped in printed stonework paper that I had produced on my computer. Additional layers of card and stonework paper were added to the top and bottom of each pier to add some relief.

The central pier on the real bridge consists of four wrought iron octagonal columns set into a cylindrical concrete and stone base. I was unable to find any suitably-sized octagonal tube or bar, so opted for 22mm dia overflow pipe from a local DIY store.

Embracing the spirit of the 'scratchbuild challenge', the base was made from a cut-down Pringles tube, which also happened to be one of the items given to the teams in our Fire and Ice heat!

The top of the base is convex, so I had to cut out the centre of the concave lid and turn it over. I had drawn out a template on CAD showing where to drill the



**9** A cut-down Pringles tube, with holes drilled in the inverted lid for the four columns, formed the base of the central pier.

holes for the four columns, and used a 22mm flat wood bit for this (photo 9).

At the top of the columns I made the decorative octagons from two pieces of 2.5mm thick plasticard glued together and cut out using the same template used for the holes in the base. Recesses were made with the wood drill in the underside of this part in order to locate the tops of the four columns. More of the Plastruct open web truss was used for the trussing between the front and rear columns as on the actual bridge (photo 10).

All these parts were then glued together with epoxy, and once set painted light grey. To locate the bridge deck accurately on the central pier a pair of plasticard guides were added to the top of the pier (photo 11).

Finally, the top of the base was painted mid stone, and the side wrapped with Scalescenes stone dock wall paper – complete with 'high tide' mark (photo 12).

### Conclusion

Although a number of compromises have had to be made, mainly due to the constraints of the original baseboard and the time allowed, I am pleased with the way it has turned out in the end, even though it is only half the length it should be.

The layout that will feature the bridge as a centrepiece has already



**10** The completed central pier ready for painting.



**11** The deck locating guides on top of the central pier.



**12** And here it is – the central pier all finished off and in place on the part-built layout.

John Champion for their efforts in winning our heat which gave me the opportunity to build this bridge in the Grand Final, and to Dave Harris and Dave Richards who filled in for those who couldn't make the final.

### Forthcoming exhibitions

The layout will be appearing at **Basingstoke Model Rail 2019** on 9 & 10 March; **Abingdon** on 16 March; and **The International N Gauge Show at Royal Leamington Spa** on 14 & 15 September.

The new layout was constructed on the rear of the baseboard used in the TV Challenge. Can you tell it's only a half-length model?





# Hull & Barnsley Railway Class F3 0-6-2T

The only H&BR locomotive design to survive into British Railways days, drawn by **GARY BICKLEY** and described by **STEVE FLINT**.



▲ F3 No.15 in original condition seen at Springhead c.1913. It clearly shows the panelled lining and the fine trim lining (although the monochrome film does not differentiate between the ultramarine and vermilion shades). The device attached to the front buffer beam (and not shown on the drawing) is a re-railing jack which was often carried by H&BR locomotives. Photo: Mick Nicholson collection

The Hull & Barnsley Railway first deployed the 0-6-2T locomotive wheel arrangement in 1901, mostly for shunting and marshalling of heavy coal trains. Eventually three classes of 0-6-2T were in service on the line, the latter being the F3s, which under the LNER were later classified as N13s.

Although the first 0-6-2Ts of 1901, classed as F1, had been designed and built by Kitson & Co of Leeds, the two subsequent classes, the F2s and F3s, were designed by the H&BR locomotive engineer, Matthew Stirling.

Stirling's locomotive designs were typified by straightback domeless boilers and wrap-around cab roofs, a practice hand-

ed down from his father Patrick Stirling, former locomotive engineer of the G&SWR and GNR. In consequence all of Stirling's H&BR designs were built to these specifications, and the F3 was no exception.

The F3 class ran to 10 engines built in two batches by Hawthorn Leslie and Company of Tyneside and introduced in 1913 and 1914 respectively. None were vacuum fitted as they were all exclusively employed on freight workings and marshalling activities. As a result they spent most of their working lives in and around the Springhead area of Hull and the city's docks, or at the inland end of the railway around Cudworth. Ron Prattley's 1997 book *Loco-*

*motives of the Hull & Barnsley Railway* (HMRS, ISBN 978 0 902835 24 6) includes a photograph of an F3 in BR days at Neville Hill Shed in Leeds, an allocation retained after wartime locomotive relocations.

Minor detail differences were in evidence, notably the positioning of the builder's plate and at least two, LNER No.2410 (orig. H&BR No.18) and LNER No.2537 (orig. H&BR No.156), were both fitted with longer smokeboxes

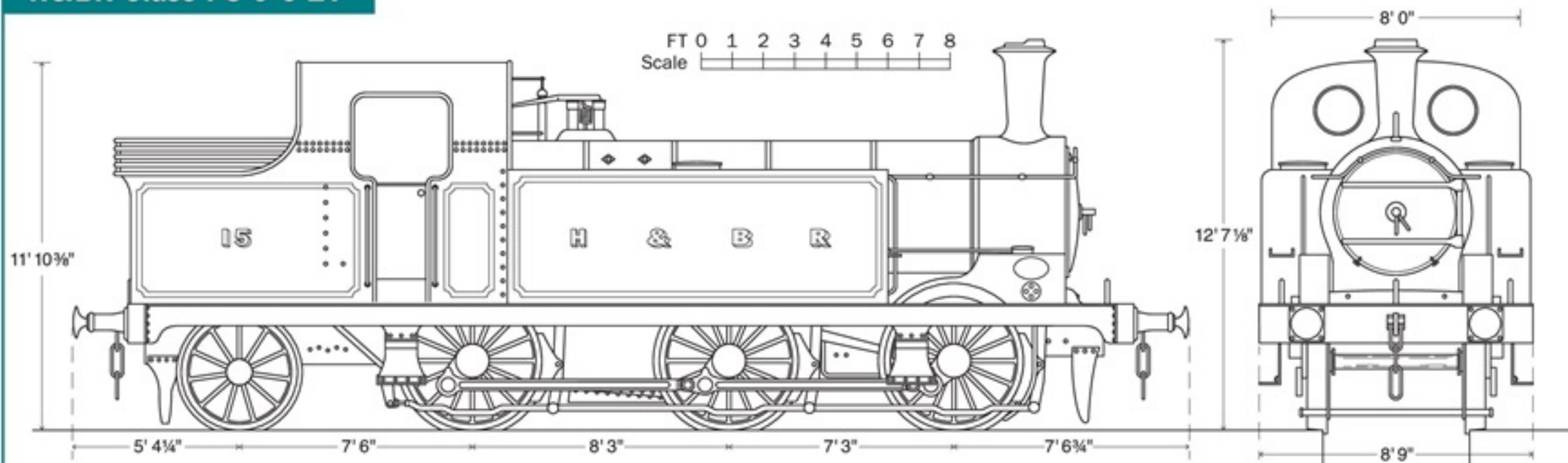
suggesting superheated boilers.

After passing into the NER in 1922, then the LNER at the Grouping, several of the class received replacement domed boilers and LNER pattern smokebox doors and chimneys. All survived into nationalisation but not all were deployed; No.9110 (orig. H&BR No.13) was withdrawn in 1948.

The remaining members of the class were scrapped commencing in 1952, with No.69114 (orig.



## H&BR Class F3 0-6-2T



Drawings reproduced to 4mm scale. Some underframe details omitted for clarity.

Detail differences on LNER No.2410 (orig. H&BR No.18) seen at Springhead in the early 1920s include the longer smokebox and builder's plate positioned on the bunker side. Photo: Mick Nicholson collection

H&BR No 27) lasting until 1956.

At introduction, the locos carried the standard H&BR very dark green 'Invisible Green' livery with black smokebox, footplate and mainframes. Decorative lining was ultramarine for the paneling on the side tanks, sand boxes, cab sides and bunker sides and rear, edged with a fine vermilion line either side. Fine vermilion lining was also applied to the footplate valances and the buffer beam ends – the beams themselves and buffer casings being vermilion.

Under the LNER and BR, plain black became standard along with the corresponding period standard lettering.



The 4mm scale drawing was prepared by Gary Bickley from an LNER weight diagram, published dimensions and photo-

graphs, and other material in the collection of the late Ron Prattley.

The author also acknowledges the assistance of Mick Nicholson

and members of the Hull & Barnsley Railway Stock Fund for their assistance with this and other articles in this short season.



▲ In LNER service at Springhead in 1933, No.2533 (orig. H&BR No.152) is seen with its original domeless boiler and smokebox door. The chimney however, appears to be LNER pattern. Photo: Mick Nicholson collection



▲ The last survivor No.69114 (orig H&BR No.27) in British Railways black livery, seen c.1956 with replacement LNER domed boiler, smokebox door and chimney. Photo: Colour Rail



# Trams for Banbury Connections

Photography by the author

**O**ur vast 35-station British railway layout *Banbury Connections*, appeared in the January 2018 RAILWAY MODELLER. Its dominating presence encompasses much of the 16m by 7.3m shed in which it is housed. Notwithstanding, one of the gems of the railway is this little-known addition known as the *Gosport Corporation Tramway*.

One might ask why I would construct a OO scale electric tramway with such a large and complex model railway to run and maintain?

The answer probably lies deep in my childhood memories of Blackpool's beautiful 1934 Balloon Trams. Despite being Australian, my first trip on a Blackpool Balloon took place when I was 16 months old and has been memorably retold by my parents as an amusing incident in which I decided to mimic the tram conductor. The tram conductor, who came up to sort out the sarcastic 'yob' upstairs, only found an innocent(?) little boy!

Following that first memorable trip, I have been on the heritage Blackpool trams numerous times as well as travelling on many other tram systems in places such as Melbourne, Zurich and Hong Kong.



▲ It began with a pair of Blackpool Balloon Tram models; here's one of them at Gosport Station Terminus.

## The collection begins

Following the release of Original Omnibus's and EFE's OO diecast tram models, at age 14, and despite my father's strong resistance, I made my first move by purchasing two Balloons and one Brush Railcoach. Secretly my father really likes trams (so much so, he was a driving force behind their initial

reintroduction in Sydney!) and within months purchased two Leeds Corporation ex-London Felthams.

This left me with five trams which each needed to be motorised by someone lacking in the skills and expertise to perform the delicate operation. Undaunted and with the confidence of youth, I ordered a couple of Bec-Kit motors (now KW Trams see; [www.kwtrams.co.uk](http://www.kwtrams.co.uk)) and, with the aid of my father, we motorised our first Balloon and Feltham tram. Naturally as time went on (and I replenished my reserves of pocket money) I gained the skills with which to finish motorising the fleet.

Dad and I then purchased an EFE Horsfield Tram from a Dr Days Bridge Junction on eBay (ironic when your father is Dr Day!). The Horsfield posed a new challenge as I had to use the cutting disc to cut through metal for the first time. Nevertheless, without too much ado, I motorised my first Horsfield which was deemed to be a simple and reliable tram. Accordingly, two more were motorised to develop a standard fleet of three. As with many real-life experiences, both subsequent motors differed from the first! However, I found that the last motor was by far the best and eventually replaced the worst performing tram with a new Mashima mechanism.

## Kiwi influences

Ironically, New Zealand is responsible for a couple of the model trams found operating on *Gosport Tramway*.

First, on a visit to the large Middleton Model Railway near Wellington, I was inspired by the owner's conversion of a Bachmann Brill Tram into a Grimsby & Immingham car. The conversion of this tram was relatively simple and required the removal of several window frames, the



▲ A pair of Feltham trams (EFE models) pass in the main street. The left-hand one is in Leeds Transport livery.





▲ My Bachmann Brill Tram converted into a Grimsby & Immingham vehicle.

construction of a new pole and a new coat of paint. Another trip down to the deep south of New Zealand, to Invercargill, was responsible for the acquisition of a Tower Trams London E3. A model shop there had a basket of Tower Tram kits and they just happened to have an E3. Yes, it's about as far away from the Blackpool home of Tower Models as you can possibly get!

This tram kit was probably the most challenging one I have worked on given it required careful construction to accommodate the motor and the scratchbuilding of two working poles. Furthermore, I was able to recycle the headlights off some of my Original Omnibus Felthams on the E3 by carefully drilling a hole in the correct position on each end. Given my liking of the London Transport livery, the E3 was shortly joined by another Feltham!

Unlike any of the British manufacturers, Cooe Collectibles in Australia produced a OO gauge Melbourne W class tram (why the company made it in OO is a bit of a mystery) which came in both a motorised and non-motorised form. Initially, I procured a bargain un-powered version from the UK which I planned to motorise. However, unable to resist a sale at my local model shop, I eventually ended up with a beautiful ready-to-run model, leaving the static tram to form part of the beer garden of a pub at the end of the line!

### Construction of the tramway proper

All in all, my tramway has finished up with 12 motorised trams which traverse the cobbled streets, reserved track and asphalt sections of the layout. The tramway provides an interesting cross-section of a British town starting at the railway station and fishing harbour before running down the main street past the shop fronts, the terraced houses, the semi-detached houses on reserved track and finally into the small dockside town from which a ferry runs. Superquick card kits will be recognisable in the background.

Construction took place in two stages with the high-street and depot ends being constructed a couple of years prior to the

terraced house and semi-detached house sections. Two operators are required to run the tramway. One operator oversees the railway station end and section up to the semi-detached houses whilst the other operator runs the section between the ferry and semi-detached houses and is also responsible for shunting trams in and out of the depot. Given the modest size of the tramway, I was able to ballast the reserved track sections and scratchbuild all my tram poles out of brass.

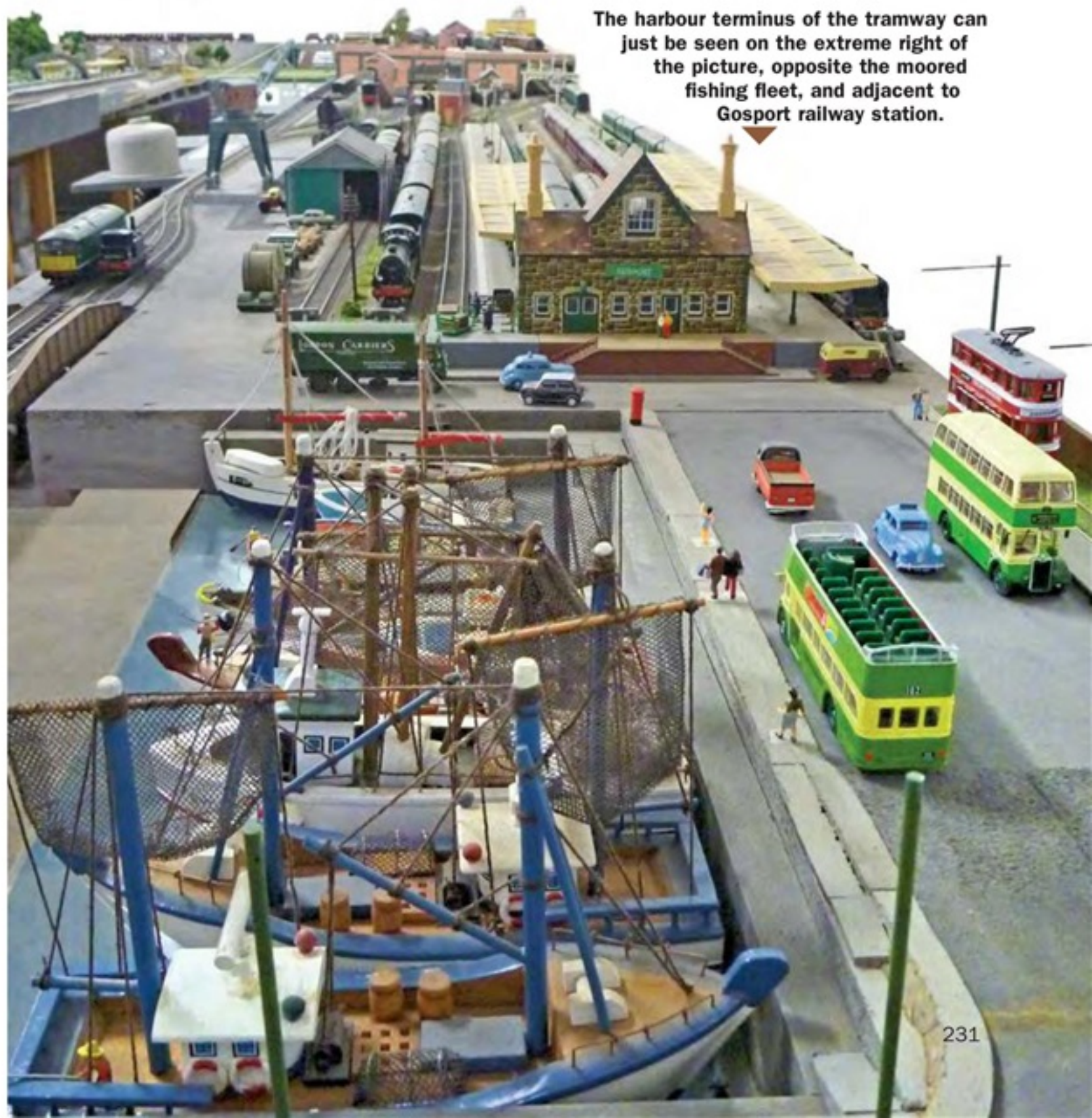
However, I have not put up any overhead wiring due to the practicalities of tramway track cleaning. Unlike the majority of model trains, model trams, given their small and fine profile wheels, seem to pick up dirt rather quickly.



▲ The Tower Models London E3 model – the kit of which I found in an Invercargill model shop – halts alongside one of the Felthams at the Chapel Street tram stop.

### Conclusion

Overall, construction of the tramway has provided me with a wide selection of skills that I would not have gained from modelling railways alone. Nevertheless, anyone considering the plunge into tramway modelling should ensure when laying their streets that the rails are slightly raised above the road surface so that the tram front and rear lifeguard grids have sufficient clearance. My earlier attempts still have the odd operational hiccup as a result of too little clearance being provided!



▲ The harbour terminus of the tramway can just be seen on the extreme right of the picture, opposite the moored fishing fleet, and adjacent to Gosport railway station.



Through The Railway Room Door...

# Hawthorn Road

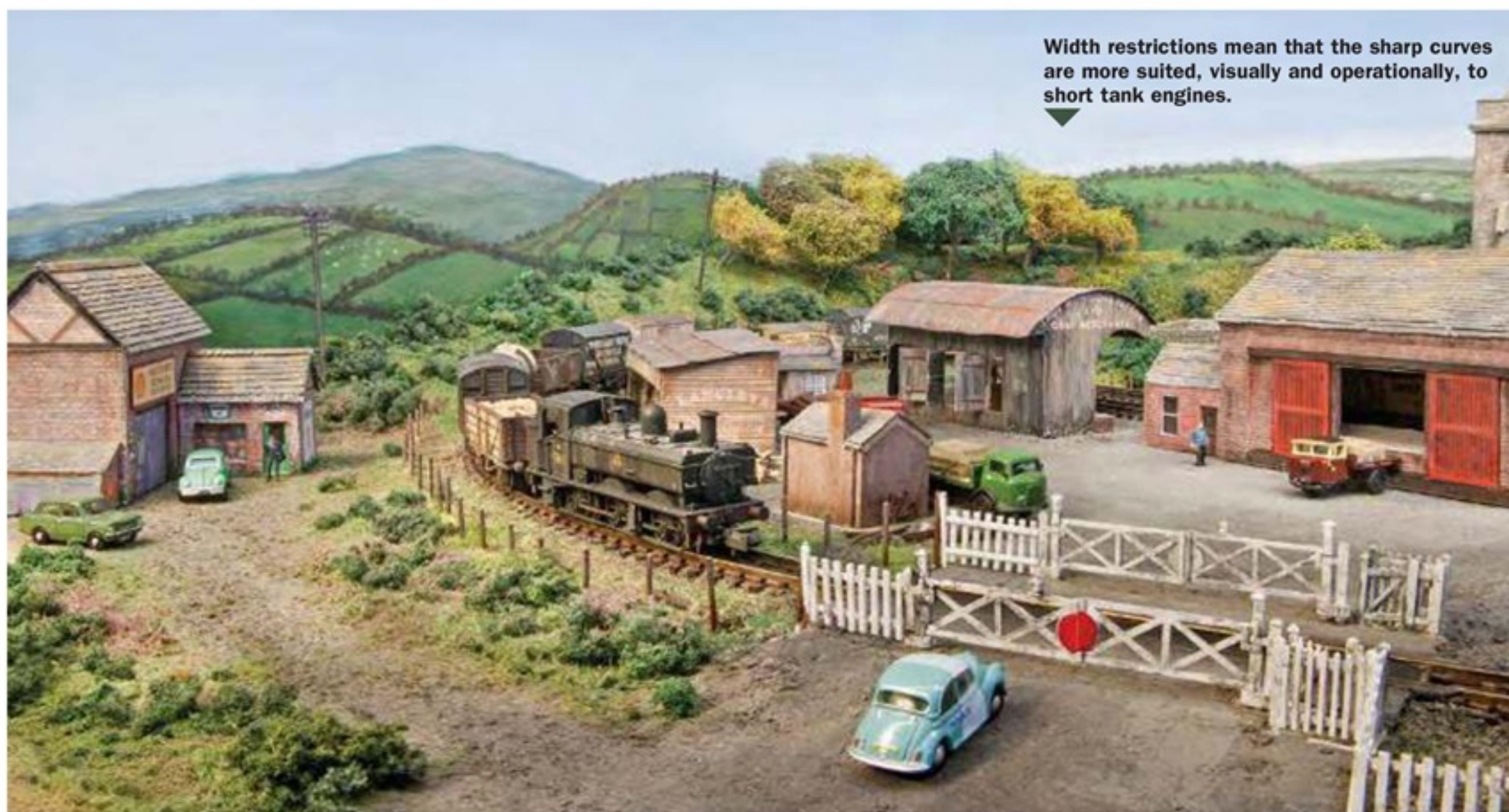
**BARRIE CANN** shows how he constructed this compact and delightful N gauge layout which, at just 4' 3" x 2' (1295mm x 610mm), is ideal for housing at home, even in the smallest of spare rooms. Set in the 1950s - 1960s, the layout uses a collection of ready-to-run locos and stock.

Photography by Craig Tiley





Width restrictions mean that the sharp curves are more suited, visually and operationally, to short tank engines.



I guess my love of railways started at Barnstaple Junction, back in the early 1960s. Grubby Western moguls, tatty Southern Ns and spam-can West Countries were the order of the day. A train set at home followed – Tri-ang TT I think – which lasted a couple of years until other things took over, like GCEs, girls, leaving school and getting a job.

Whilst the last 50 years have consisted of working, marriage, kids, and mortgages, the love of steam has simmered (sorry) in the background, kept alight by visits to various museums, and heritage railways around the country. Thankfully, my good lady wife has an interest in steam engines too, so railway visits are a shared experience. Her further approval of my interest was reinforced when, on my 60th birthday, she bought me a train driving experience on the wonderful Lynton & Barnstaple Railway at Woody Bay. A day never to be forgotten, and one which provided the impetus for my retirement plans. I now volunteer there every week, working on carriage, loco and rolling stock refurbishment.

But that's only one day a week – what to do with the rest of my free time? Again, encouragement from herself suggested the use of the back bedroom/office/computer room for a (small) model railway. Do I want to play trains again? Dunno. There's only one way to find out, so let's get cracking...

### What to build?

Measuring up, I had about 4' x 2' to sit on the desk. That'll do. I wanted something that would go round and round, so we were

◀ The station and yard are dominated by the farm on the hill, constructed from Metcalfe card kits and detailed by the author's wife.

looking at about 9" radius track. A trip to B&Q for a sheet of chipboard followed, along with an order for a Peco starter set, and a basic Gaugemaster controller. A certain internet auction site brought a Dapol GWR 0-4-2T plus autococh and a few wagons, so now we've got something to run, and something to run it on.

I am quite creative and artistic, so I liked the idea of making scenery, and trees and backscenes, so started planning it out. I needed a station, and a goods yard, so some points were needed, and a bit more track. I'm not going complicated with this, so no point motors, just one power feed and that's it. Herself is a farmer's daughter, so she wants to see a farm, with sheep and cows, so that goes on the list too.

At this point, I must mention the most useful instructional aid known to man – YouTube. Anything I have wanted to know, about anything, someone has posted a YouTube video of it. Some are good (Richard Warren and his *Everard Junction* for one, Luke Towan is another) and others are not. But as a way of giving you hints, it's wonderful, and I would not have achieved half of what I have, without some very detailed instruction from some excellent modellers.

### Trackwork

So, the track was roughly laid out, and bits of card cut out to show where roads and the station etc would go, until I had what looked right. Track was then stuck to the cork underlay

with contact adhesive (the track pins wouldn't work in chipboard). Ballast was then applied in the time honoured way with wet water, and dilute PVA through a pipette. Power was fed up through two small holes, and soldered to the track in one place only. And *voilà* – it works!

### Locos and stock

Because I used 9" radius curves, small tank engines seemed most appropriate, so I now have a couple of GWR 0-6-0PTs, a 14xx with autococh, and an SR Terrier. I also have a 2-6-2 Prairie Tank, which slips like mad going round the corners, so that doesn't come out too often. Wagons are mainly Peco, with a few odd others thrown in. A couple of nondescript second-hand Chinese coaches completes the stock list.

“ – because I used 9" radius curves, small tank engines seemed most appropriate – ”

Barrie Cann





▲ An 0-6-0PT shunting the yard. The backscene was painted *in situ* – which meant reaching over already-finished scenery...

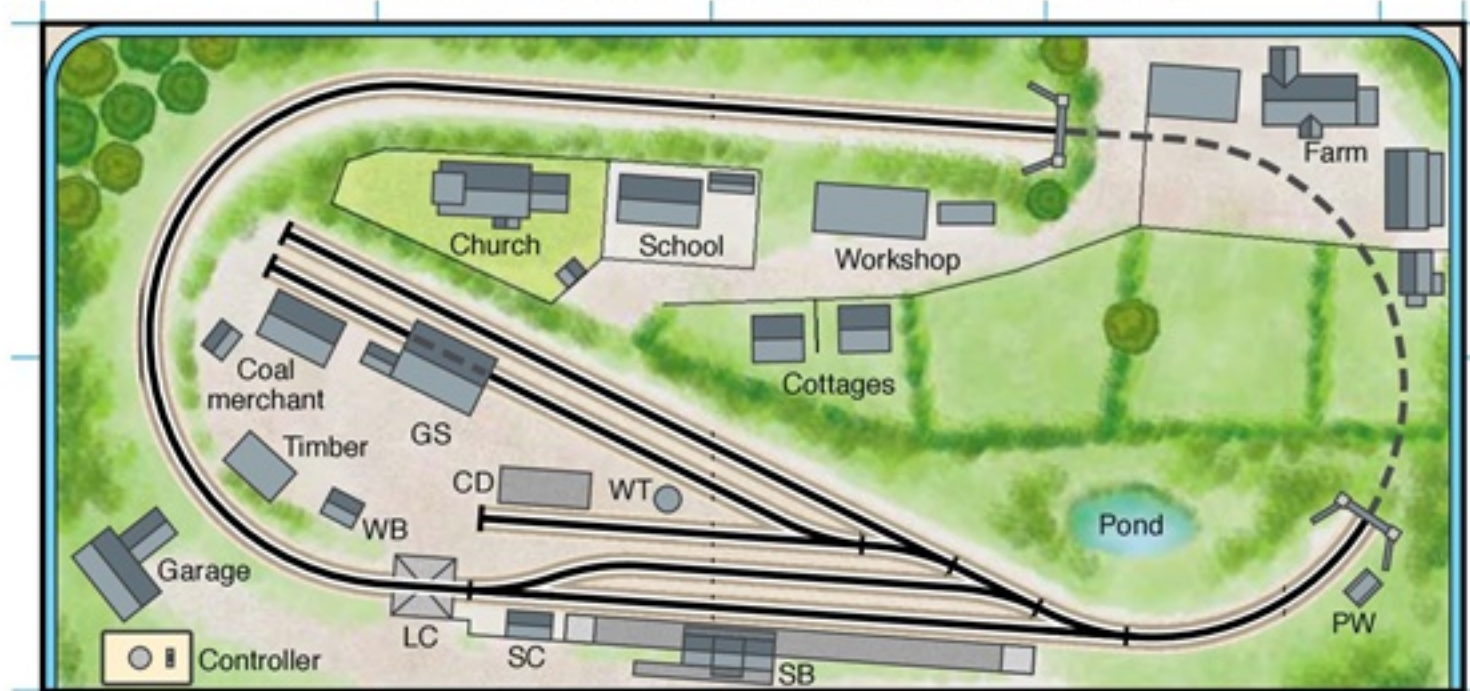
### Scenery and buildings

This is the bit I really enjoy. I think I've not so much got a model railway, as a diorama with a train running round occasionally. But it's been highly absorbing creating my little world, and has provided endless hours of creative interest.

Once the track was down, I needed a tangible view of where things would fit. I didn't want to commit to any particular buildings immediately, but needed some mock-ups to see what worked. This is where Mike Wordsworth's excellent range of free downloads came into their own (<http://www.wordsworthmodelrailway.co.uk/>). There is a vast range of cottages, stations, platforms etc, which you just download, and print on thin card. They were ideal for showing how things would look, and could provide temporary

## HAWTHORN ROAD

Overall size: 4' 3" x 2' 0". Each grid square = 1ft x 1ft.



structures, until more permanent ones were to hand.

The landscape was formed from cardboard

boxes. Two-ply corrugated was cut to form the contours for the cuttings, hill farm and tunnel. This was stuck directly to the



▲ Early days under construction: the chipboard was faced with strips of wood; track laid and tested; landscape formers have been installed.



▲ With the basic landforms in position, the building mock-ups could be test-placed until the most effective arrangement was achieved.



Trees were home-made, some using twisted wire, but string works equally well in this small scale. Ground cover was obtained from a variety of ranges, mixed for a natural look.

baseboard with a hot-glue gun. The surface was a latticework of 1" strips cut from cereal boxes. This was then overlaid with three or four layers of kitchen roll, liberally stippled on with 50/50 dilute PVA glue, mixed with brown acrylic paint. It is amazing how sturdy that is, once it's dry.

Grass is a variety of scatter from Javis and Woodland Scenics, being careful to vary colours and textures (a random mixture looks much more realistic). Hedges separate the fields – these were made from pan scourers, cut into narrow strips, and covered in scatter, then pinned and glued in place. Bushes are mainly home-made: an old foam mattress will keep me in bushes forever. I tear it in chunks, and feed it to my inexpensive food blender until it's in small fragments. This is then mixed with dilute PVA and green paint, then spread over a tray in clumps, and left to dry. It makes brilliant clump foliage.

Trees were all home-made, some with twisted wire, but because they are so small, untwisted sisal string works fine, stiffened with spray glue. Once dry, this was covered in teased out nylon fibres from an old duvet, then sprayed black. A dousing in scatter, and there – you have a tree.

Flowers have been made from a variety of dyed sawdust and coloured chalk, scraped with a knife.

I mentioned Mike Wordsworth's models earlier. Whilst they were useful as mock-ups, I have in fact re-made many of them in the final layout, albeit somewhat modified with extra profiling, detailed roofs (individual slates) and weathering.

Other buildings have come from Metcalfe, Peco and Scalescenes. The church is scratchbuilt, based on one near Cirencester. The duck pond was dribbled on, one coat of gloss acrylic varnish at a time.

One thing I have done to improve the look is to photograph the layout regularly. Blowing up a photo on a computer screen definitely reveals any blemishes, and has shown me where improvements have been needed.



### Weathering

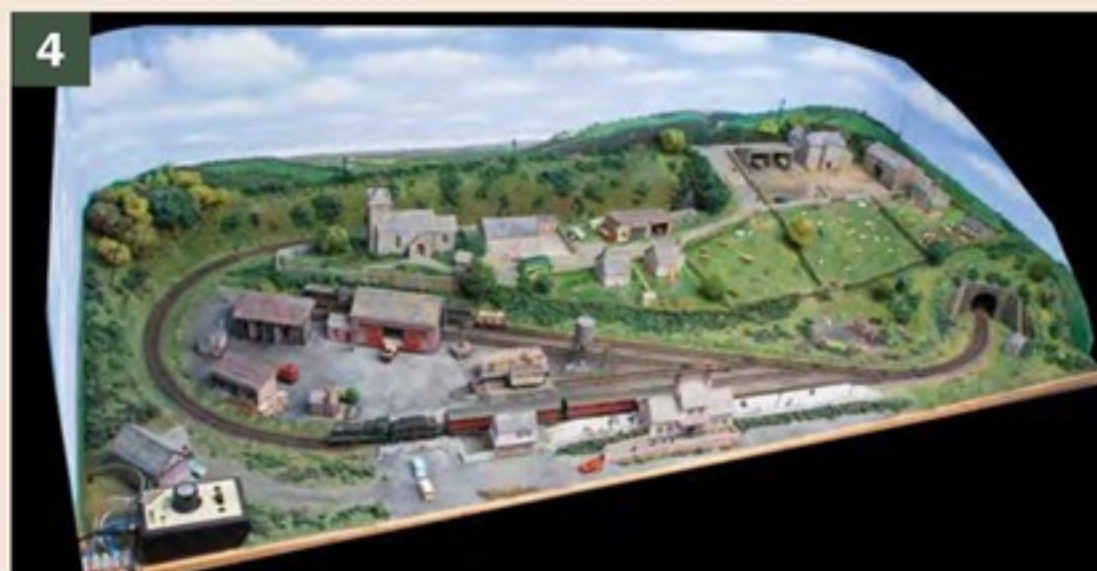
I like weathering. For me, all models should be weathered. I have seen many layouts which are beautiful, with superb models, stunning scenery, and trackwork to die for. But utterly sanitised. Not a speck of dirt to be

seen. Pristine roofs, polished tank engines, clean, bright ballast. Not for me, I'm afraid.

My chimneys have soot, my roofs have lichen, the ballast is oily, and metal things have rust. All my engines have been heavily weathered as have all the trucks and



▲ With the scenery at an advanced stage, the farm was installed and field boundaries marked out; hedges were made eventually.



▲ The finished layout. Control is simple analogue 12V dc, trains being operated by a Gaugemaster Combi unit. Construction photos by the author





▲ An auto train enters the station. The 3D scenery transits effectively into the backscene.

carriages. I purchased a Union Mills Collett Goods, which is a lovely model, but delivered in bright shiny green. To me, no working Collett ever looked like that, so mine is now dull and grimy and streaked, and looks all the more realistic for it.

### Backscene

I wish I'd painted it before I fixed it permanently and stuck all the scenery to it! Trying to be artistic at arm's length is not easy, but hopefully I have pulled it off, with a reasonable representation of a rural view. Painting clouds is definitely very therapeutic.

### People and creatures

The final part of *Hawthorn Road* was to populate it with people and animals. Painting N gauge figures is an interesting challenge for my 67-year old eyes – I tried to tone down the colours to represent roughly the era of the late 1950s and early 1960s. Planting numerous sheep, horses and cows kept the lady wife happy.

### In conclusion

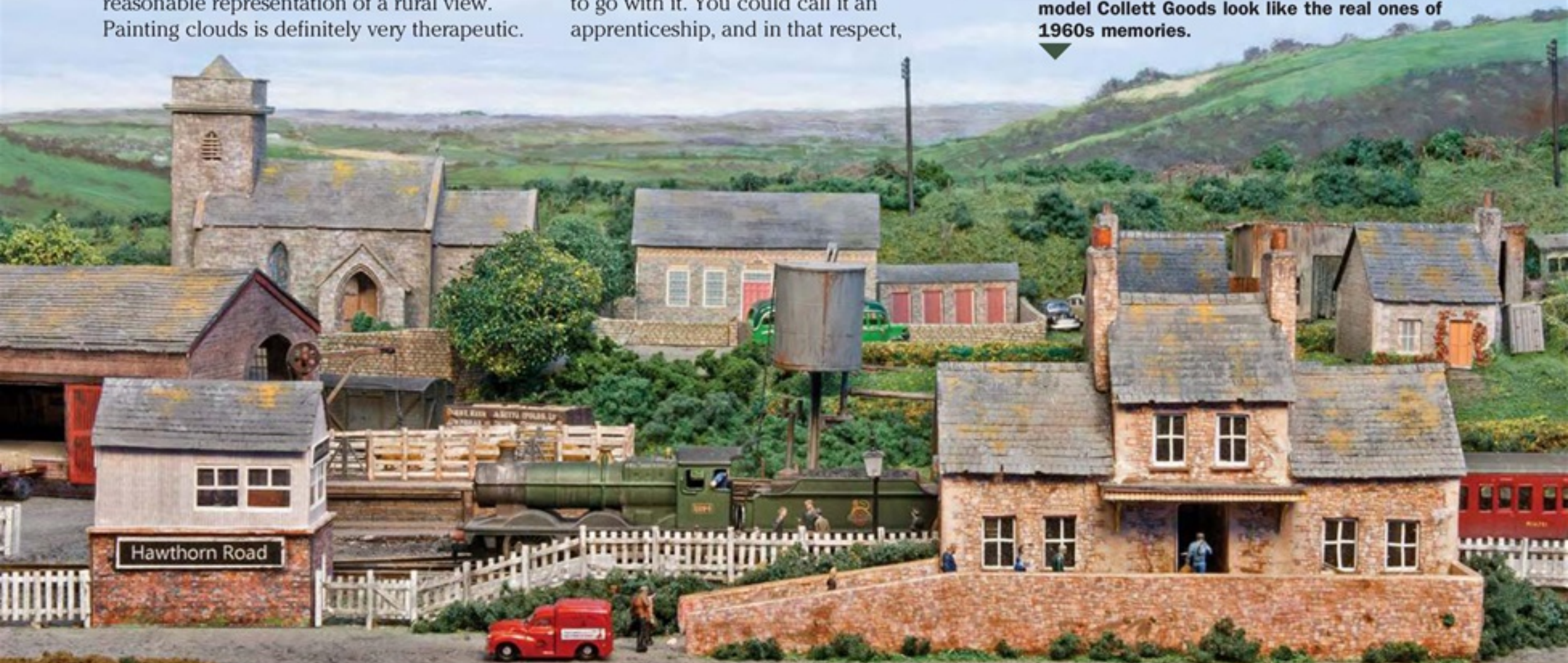
Yes, I do like playing trains. Or at least, I like building a layout. I could say it's finished; I know, they say they're never finished, but I think this one has taken me as far as I want to go with it. You could call it an apprenticeship, and in that respect,

*Hawthorn Road* has fulfilled its purpose.

My overriding aim during this process was that I wanted something that looked realistic. Within the limits of 1:148 scale, I think I've succeeded. Can you have a layout in a small space? Of course you can. Can it provide years of enjoyment? You bet it can.

I've learned loads of stuff over the past four years, and now it's time to move on to something else, with more operational interest, automation, electrification and a bit more space. Perhaps I'll be able to report back in another four years!

▼ **Weathering is paramount, especially to make a model Collett Goods look like the real ones of 1960s memories.**







# RAILWAY MODELLING *Explored*



Projects, hints and tips for beginners from *Railway Modeller* readers and staff

**THIS MONTH • Newstone • 12V dc control options • Timber traffic**

## Newstone

**MATTHEW STEVENS** recommends building a small portable layout in OO gauge when modelling time, budget and available space are limited at home.

**N**ewstone came about because I had plenty of Southern stock including an Adams O2 loco with an extended bunker, as per the Isle of Wight versions. My permanent home layout Burbage (see RM June 2018) had become a bit of a mess due to the number of times it had been modified. It was now time to demolish it and build a new portable layout in its place.

A basic terminus layout was devised with one end of a run-round loop being modelled, the other being off scene, and a coal siding and goods dock to complete a fairly typical terminus scene.

### BASIC CONSTRUCTION

I re-used the board from Burbage's fiddle yard, which is about the same age as me, having been used for multiple permanent layouts over the last 16 or so years! A set of removable timber legs were provided to allow the layout to be portable and thus exhibitable. The other board is an old household shelf with a further set of removable legs. It houses the fiddle yard and is only erected for running sessions.

The original plan was for a completely straight platform, but as I only had one right-hand and one left-hand point for the run

round crossover, I had to incorporate a curve. In hindsight I think the curved platform adds character to the layout.

Trackwork is Peco code 75 with medium radius points for the crossover and a short radius one for the coal siding. As the track was laid, the power feed wires were dropped through suitable holes drilled in the board, and insulated joiners for the electrical sections were added. The layout is fully dc analogue and the three power sections are wired on the common return basis.

Peco point motors are mounted underneath the baseboard to change the points, and both points and power sections are switched by standard Peco switches mounted in

the firms lever frame. The signal is operated by the wire-in-tube method.

### BRIDGE AND PLATFORM

The reason for building these structures before ballasting the track, was to allow the ballast granules to be laid tight up to the edge of the structures easily. Nowhere is this method more effective than the platform edge which was neatly laid using a grid of card strips to keep the platform edge at right angles and the surface level.

▼ The classic Tri-ang/Hornby clerestory coaches – soon to be released again – can easily be converted into Isle of Wight railways coaches.



Photography by Paul Bason



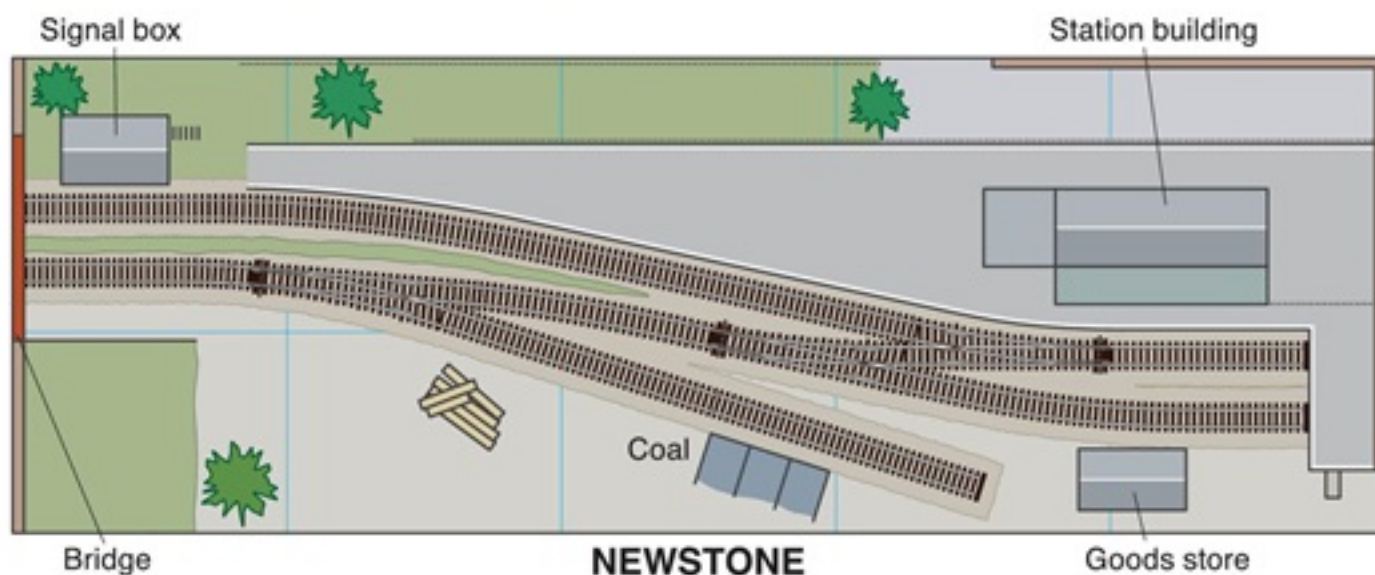
The bridge is a low-relief model to save space and is simply stuck to the end back-board. Another piece of brickpaper hides the switch panel and helps create the illusion of a full-width bridge. I think this is fairly effective and I have certainly benefited from the extra available length.

All brickwork for the platform walls and bridge is Metcalfe brickpaper, as is the platform surface. It is very realistic despite not being textured.

### THE DRY BALLASTING METHOD

Woodland Scenics fine ballast was used, with grey on the main lines and brown in the coal siding to convey a sense of dirty or older track. All the ballast was fixed using the Deluxe Materials 'Ballast Magic' kit. This involves mixing the kit's white glue powder with ballast granules in a ratio of about one part powder to six or seven parts ballast.

The mixture is laid dry between the sleepers, taking care around moving parts such as point blades, and then sprayed thoroughly with water using the fine spray bottle provided in the kit.



**NEWSTONE**  
Scenic length: 5ft. Each grid square: 1ft x 1ft.

I have found this method to be better than the wet PVA method as it gives the cleanest results and avoids the need to apply wet glue directly to the trackwork.

### THE EMBANKMENT

The small area of raised ground by the bridge is a lattice of thin card stuffed with newspaper, which is crude but cheap, and

looks the part. If I was to do it again I would probably take more care so that it wasn't such an unnatural wedge shape!

### GRASS AND FOLIAGE APPLICATION

For rough grass I use hanging basket liner, which is cheap and works well as a base. Other greenery is a variety of Woodland Scenics products fixed with PVA.

The trees are an assortment that I already had available. I am fully aware that they are different types and probably wouldn't grow in that area, but I'm no tree expert!

### ADDING BUILDINGS

The station building is a Peco Manyways kit which was very easy to assemble. I didn't want to spend too much time on a scratchbuilt station building, but I may look at doing this in the future to make the scene more personal to me and unique – for now I'm happy with the kit.

I believe the goods hut is a Wills provender store kit. It was built before Newstone for another layout. It's a very simple structure but

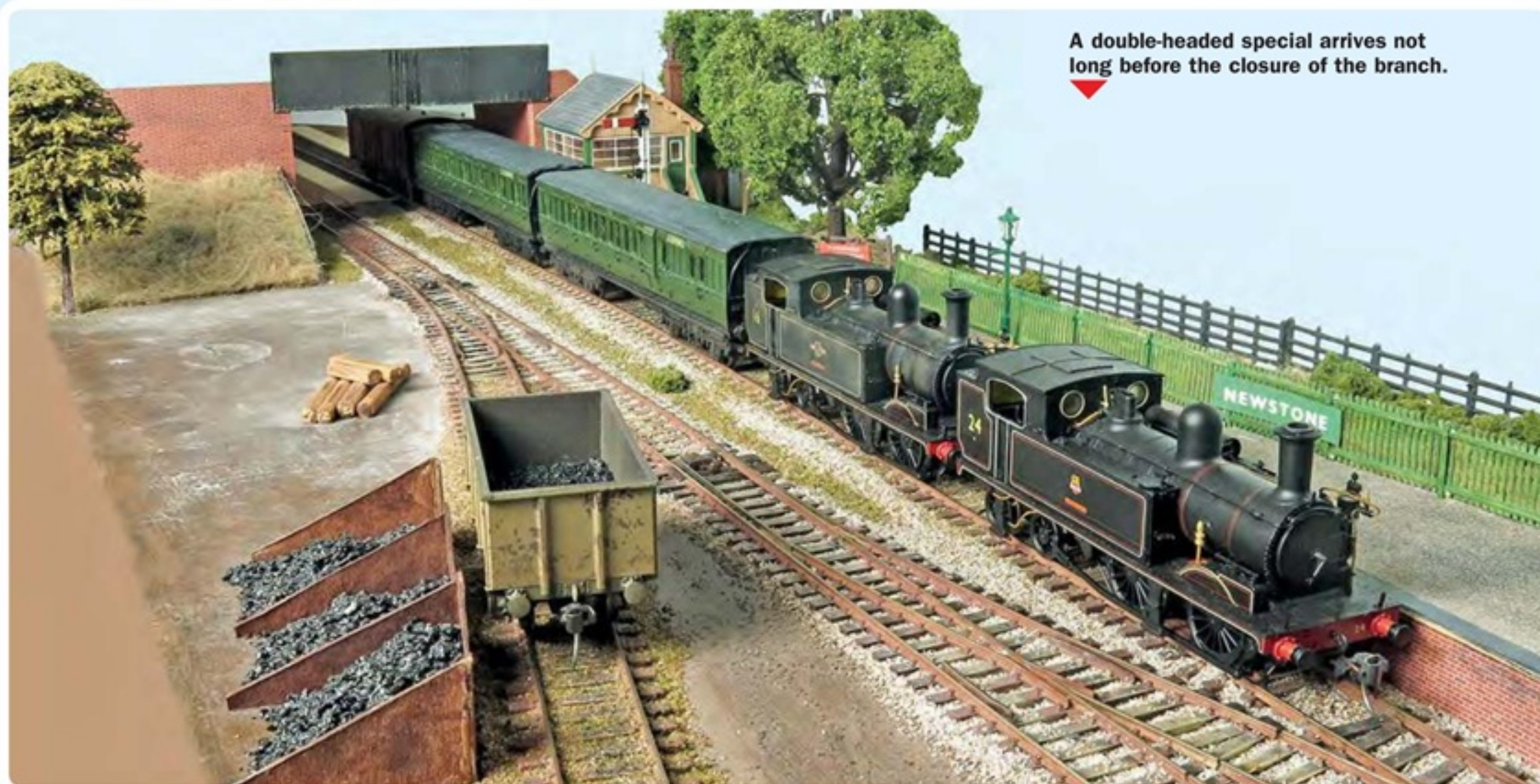


▲ My buildings' prints, which form the backscene, were produced on screen, printed out and glued onto stout card.

The small town looms above the station as the O2 and the Terrier complete their necessary duties.







A double-headed special arrives not long before the closure of the branch.

serves its purpose nonetheless. All buildings are weathered to a certain degree (although the rear of the station building is still the bright, clean colour of the moulded plastic – nobody sees that!).

The buildings on the embankment are Photoshopped images which are printed and glued onto card. I am very pleased with my successful photo editing to make the buildings unique, such as 'Newstone News'.

### OPERATING A BRANCH TERMINUS

The way this layout is operated is fairly structured and organised, particularly for exhibition purposes, and hopefully depicts how such a real station would be operated.

When a passenger train arrives, it stops at the buffers and lets the passengers alight. Once the train is empty it propels back clear of the crossover and the loco runs round the coaches. The train is gently propelled back towards the buffers, once again for new passengers to board.

When a goods train arrives, it runs from the fiddle yard into the loop and waits until any passenger train in the station has departed. The loco then uses the platform track to run round. Shunting is less structured and simply fills the quiet gaps between passenger trains.

I like to find a good balance between entertaining the public and being realistic in my operation of the layout.

All stock is fitted with Kadee Magnematic® couplings ([www.kadee.com](http://www.kadee.com)) which are easy to uncouple manually, but also use track mounted magnets to uncouple – there is one fitted part-way along the platform to assist the run round manoeuvre.

### LOCOS AND ROLLING STOCK

The coaches are modified Tri-ang clerestories from an unknown vintage [Ed. – they are to be reintroduced by Hornby in its Railroad range during 2019]. They have had their roof clerestories removed and body work painted green to depict Isle of Wight coaching stock. Unusually, the best colour for them turned out to be GWR loco green!

The two O2s that I use most are out of the box Kernow Model Rail Centre models; and are the Isle of Wight variants with extended bunkers. There is a third O2 from the same manufacturer, but this was originally a mainland version, so I scratchbuilt a new loW bunker for it. The finish on the modified model is not as good as the others, so I keep it as a spare. I also use a Dapol Terrier 0-6-0T from time to time.

### WHERE TO SEE NEWSTONE

Newstone is largely complete and made its exhibition debut in May 2018 at the Market Deeping club show at Stamford, which is local to us. At the time of writing, Newstone is booked to appear at the South Nottinghamshire exhibition in April 2019, with potentially other appearances on the horizon. My Dad, Tim, and I will be there and we look forward to seeing readers at the show.

Potential development of the layout itself might include a new scratchbuilt station building and some more scenic detailing, including non-working ground signals. Finally, I hope that Newstone will inspire readers to think about their next modelling project. Although mine is based on the Isle of Wight railways, its simple track plan at around 8' long including the fiddle yard, could be adopted for many similar style layouts.

Happy modelling.

## Basic layout wiring...

Matthew uses simple 12V analogue control with track-section wiring and Peco electrically operated point motors. For beginners wishing to know more about the basics of connecting Peco track and point motors to the power, our Shows You How Guide No.4 Wiring the Layout part 1 – First Steps is available from all good model shops or direct from ourselves. Visit [www.peco-uk.com](http://www.peco-uk.com).



▲ Me with the layout in its home position. Behind me is a potential future project in the early stages!



## Controlling your model railway - 1

New entrants to the hobby or modellers of lesser experience may feel slightly bewildered by the wide array of layout control systems available.

**CRAIG TILEY** presents this overview of the different types on offer, beginning this month with traditional 'analogue' 12V dc layout control.

*Photography by the author or as credited*

If you have just become the proud owner of a new train set, you may already be considering how to develop the basic oval of track and the rest of the contents into a complete model railway system, with additional track, multiple train operation and scenic detailing.

Something that will need to be determined early on is whether the controller supplied with your train set is adequate for the model railway you plan to build, or whether an alternative controller needs to be obtained. In this short introductory article, we will look chiefly at the equipment available for basic 12V dc analogue model railway control.

It is worth stating that analogue remains the most common method of control supplied with the OO and N gauge train sets produced by manufacturers such as Bachmann and Hornby, although an increasing number choose sets supplied with digital command control (DCC).

### 12V dc control explained

The traditional and long established method of model railway control uses a variable 12V dc speed controller. This provides a variable voltage between 0V and 12V to the track, which changes the current passing through the motor of a locomotive or powered model and thus alters its speed.

In order to provide a layout with a 0-12V dc control voltage, three items are needed; a transformer to convert domestic mains voltage from 240V ac to 16V ac, a rectifier to change the 16V ac to 12V dc, and a means of varying the dc output voltage.



Proprietary controllers, like those supplied in train sets, are usually fully integral units which come complete with the transformer, rectifier and electronic speed controller all pre-wired and safely enclosed in a sturdy casing. However, these train set controllers are only suitable for single train operation; if you want to run two or more trains simultaneously on different circuits, then a different controller will be required.



▲ Hornby's new R7229 controller (due in March 2019) is an alternative to the R8250 supplied in its analogue train sets. The R7229 has an additional uncontrolled dc output (with integral capacitor discharge unit) for the operation of solenoid point motors.

*Photo: Hornby Hobbies*

▲ The Bachmann Branchline 'Military Manoeuvres' train set (OO) comes with this basic 12V dc Speed Controller, which is powered by a separate plug-in transformer and connects to the track via the power clip illustrated.

### Cased controllers

Cased controllers are also available with up to four integral speed controllers, thus allowing control of up to four trains simultaneously on different circuits, however, a single or twin speed control version is usually sufficient.

Most controllers of this type also include an 'auxiliary output' to allow you to provide power to other electrical accessories (such as point motors) or to add on another speed controller at a later stage.

A cased controller is ideal for 'table top' set ups and model railways where the trains can be operated from a single position.



▲ Hornby also markets the HM2000 – a cased twin-track controller with integral transformer, which enables two trains to be controlled on separate circuits. The back of the unit has additional non-variable ac and dc outputs for point motors and other accessories.

*Photo: Hornby Hobbies*





▲ The twin train Gaugemaster Model D is fully cased with integral transformer and complete with mains plug. The back of the controller (above left) has screw terminals for track power connections and additional accessory outputs.



▲ The Morley Vector Zero Two Controller is equipped with two integral transformers to supply the correct voltage to each track. A cased unit, it is also supplied with an auxiliary hand-held remote controller which can be switched between the two tracks.  
Photo: Morley Controllers



▲ The Helmsman Model Rail 1A (1 Amp) single track controller is equipped with a plug-in transformer and has a pair of push-fit spring terminals for connecting to track power wires. Voltage output is controlled with a slider, rather than the rotary knob that most controllers have.



▲ The Gaugemaster Combi is a compact single-track controller (rated 1.1 Amps) with a separate plug-in transformer. The controller casing has single 12V dc (track power) and 16V ac (accessory) outputs.

## Hand-held controllers

If you need to move around the layout to uncouple trains at different locations, or perhaps to change points, then hand-held speed controllers (often called 'walkabout' controllers), which are fitted with a long cable, should be considered. These controllers are powered by a separate transformer, or from the auxiliary output of a cased controller, and do not have an auxiliary output themselves. However, the Gaugemaster 'Combi' and Helmsman hand-held controllers (see below left) do come with dedicated transformers.

## Other options

Also available are panel-mounted speed controllers, which are intended for fixing within a separate control console, or for mounting into the rear fascia board of a layout. These are more advanced options and also have to be powered from separate transformers.

Newcomers may also have heard of alternative versions of 12V dc controllers which include 'feedback' or 'inertia' functions. Feedback controllers have special circuitry to maintain a constant train speed and ostensibly improve running performance. However, they were originally developed in the 1970s long before modern mechanisms and coreless motors were fitted to model locomotives. If you possess a lot of older models from this period then a 'feedback' controller may be ideal for your purposes, but it should not be used with modern R-T-R models – in particular OO and N, which invariably are equipped with coreless motors.

The Gaugemaster Model W is a single-track 'walkabout' controller that enables trains to be controlled away from a central panel. However, it is not supplied with a transformer; a 16V ac supply is required, with the controller wired directly to a transformer.



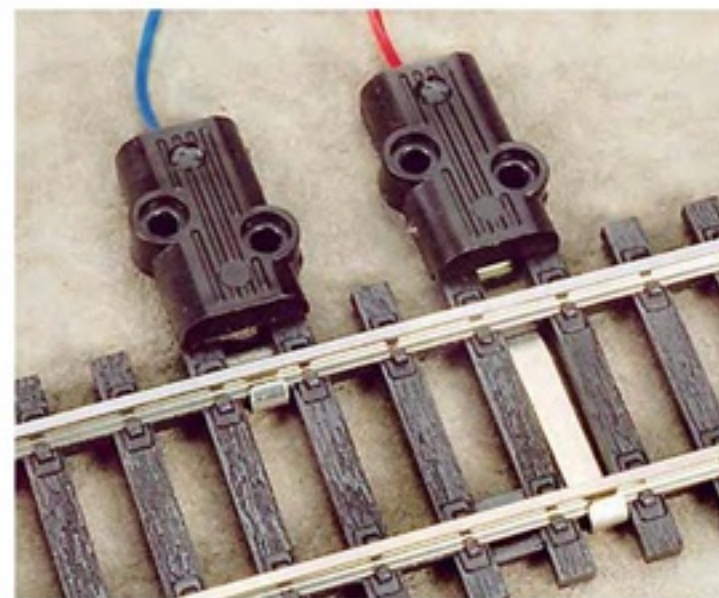
▲ If twin train control isn't sufficient for the number of trains you wish to run simultaneously on your layout, then three and four train controllers are also available; the Gaugemaster Model Q enables four train operation.

Likewise, inertia functions (sometimes called 'simulation') associated with these controllers are only suitable for older models.

## Connecting power to the track

Whichever controller you use, it will need to be electrically connected to the track. At its very simplest, the output wires of the controller are connected, one to each rail, at a position known as the track feed. With a simple layout, such as an oval of track, only one track feed needs to be installed. Power connecting clips are provided in train sets for this purpose and are also available separately (such as the Peco ST-273).

As layouts become more complex it will be necessary to install more than one track feed, together with insulated rail joiners; for more information about basic layout wiring (together with an introduction to DCC), refer to Your Guide to Railway Modelling & Layout Construction, a title in the Peco Modellers' Library (PM-200).



▲ Power connections between the controller and the track can be achieved using Peco ST-273 Power Connecting Clips, as pictured here. The wires can be connected to the clips using Peco PL-31 Push-On Terminal Connectors.

**Next month...**

Entry level DCC systems explained



## Timber traffic for a small layout

**PETER HARRISON** shows how easy it is to depict loaded and empty wagon movements realistically on a small layout, on this occasion using sawn timber. *Photography by the author*

Readers may recall my short article on the cattle train (December 2017) which passes through my model station called Lowleigh en route to the weekly livestock market at Brombury, the terminus of my fictional ex-GWR branch from Kidderminster.

As a principle I like to be able to justify every vehicle in the daily pick-up goods train which calls at Lowleigh each weekday morning. In the 'down' direction there will often be a bogie bolster wagon and an empty open heading for Brombury where there is a flourishing rail-served sawmill. This train is illustrated below.

Raw materials for the mill are readily available in the wooded hills surrounding the town and the railway is tasked with transporting prepared timber back up the branch. Each afternoon the branch freight returns through Lowleigh heading for

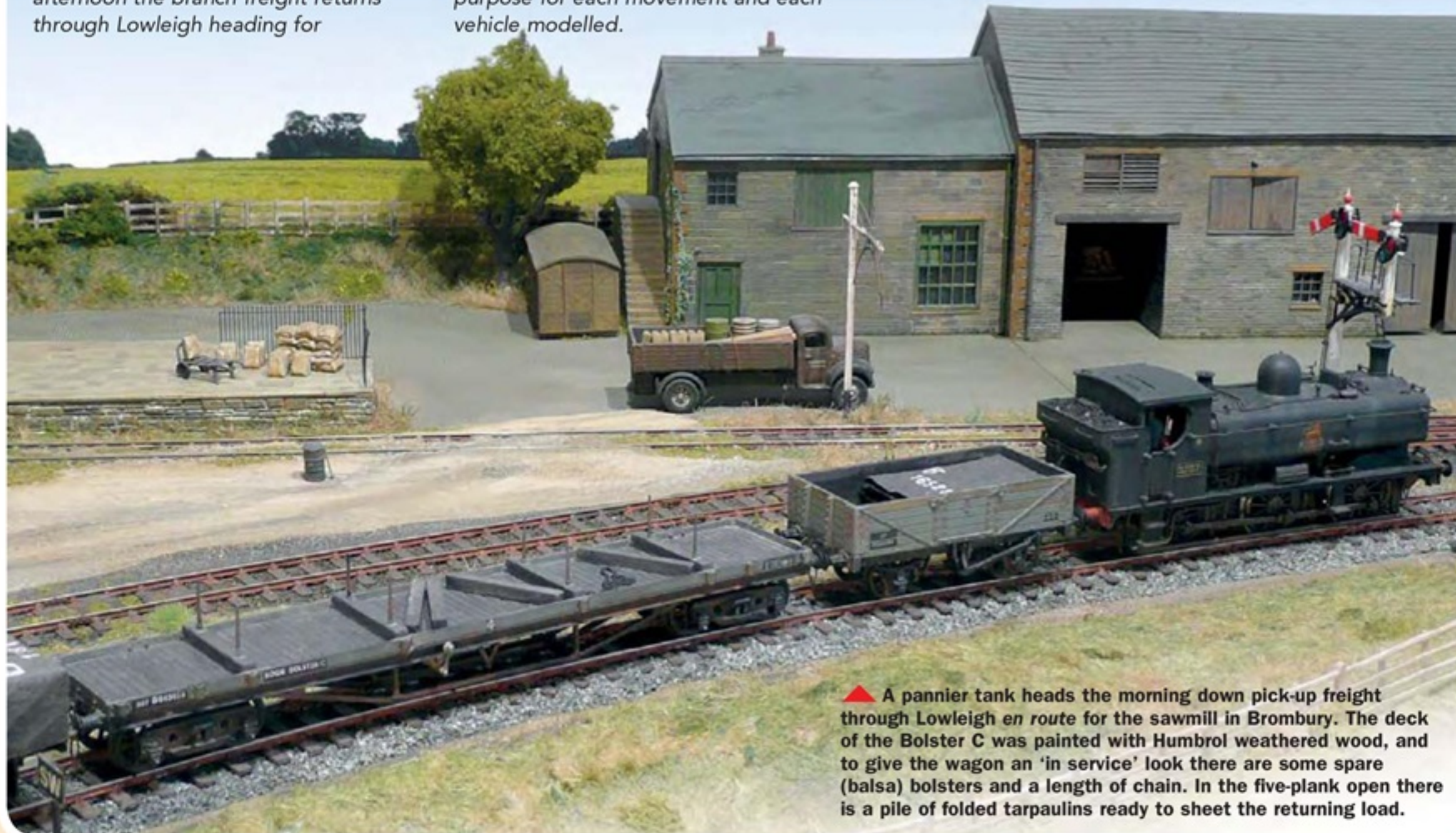
Kidderminster with a loaded bogie bolster and a half-sheeted open with supplies of timber for the firm's customers in the building trade in the Midlands and beyond.

Although the timber service runs daily, to vary the train make up on different days, I use different pairs of wagons for this service; a loaded pair from the terminus, and an empty pair to the terminus. I fit all my freight vehicles with Wizard wheels and Smith's three-link couplings and, following a little judicious weathering, everything is treated to a thin wash of my usual concoction – one part Revell earth and one part Railmatch weathered black.

So, even on the humblest of branch lines, representations of goods traffic can be brought to life with a clear purpose for each movement and each vehicle modelled.



▲ The loaded daily freight heading through Lowleigh in the afternoon. The timber loads were cut from coffee stirrers – I build up a stock of this useful material as the stirrers come in a variety of lengths and thicknesses (they are also very good as paint stirrers!). The loads were made up away from the wagons and each bundle is, in fact, hollow and contains some extra lead weight. A thin coat of Humbrol 'natural wood' gives a more realistic finish. The half-sheeted load in the BR five-plank open is covered with a tarpaulin from the Smith's range, the ropes being depicted by fine thread. The bundles of timber on the Bogie Bolster A are chained in place using fine chain obtained from Weinert Modellbau and is superglued to the moulded anchoring rings on the model. Advanced modellers may want to replace the moulded rings with something a little more realistic.



▲ A pannier tank heads the morning down pick-up freight through Lowleigh en route for the sawmill in Brombury. The deck of the Bolster C was painted with Humbrol weathered wood, and to give the wagon an 'in service' look there are some spare (balsa) bolsters and a length of chain. In the five-plank open there is a pile of folded tarpaulins ready to sheet the returning load.



# Comment

Observations and opinions on railway modelling

**Simon George**



## Don't preach to the converted

“It's often mooted that our grand hobby is only just behind fishing in the popularity stakes, yet chat to model railway exhibition managers and the majority will tell you that, just like the retail sector, the hobby is simply not attracting the numbers it once did. Time and time again, this column has posed the question of what can be done to stem the worrying downturn? Well, here's another suggestion; it's a personal one into which I'm also prepared to put some money, and it all began with an Italian sports car.

Back in 2006 I co-founded a business offering supercar driving experiences. It was initially designed to enable me to indulge in my other passion for classic supercars. To be candid I couldn't really afford the Lamborghini I owned at the time, and I'd figured in my wisdom that if I sold driving experiences in the vehicle at race circuits – complete with an instructor riding shotgun – I might just be able to reap enough cash to keep hold of it.

Initially I pitched these classic driving experiences at diehard, petrol-headed car enthusiasts in the belief that they formed the obvious market. After six months sales were simply dire, and moreover, some that did buy – being self-appointed experts – would nitpick and criticise over the tiniest issues. Sound familiar?

So, armed with literally the last £1000 I persuaded my business partner to entertain the idea of placing a supercar within a busy shopping mall and pitching the driving experience directly to the unassuming general public instead.

I recall that first rainy October day very well. It saw us sell more 'driving experience vouchers' in 10 hours than in the whole of the previous six months. Before long the vouchers were being sold via the high street retailers too – as birthday gifts and the like.

This was a clear lesson in business marketing to me: don't preach to the converted, pitch to the public at large.

By now you are asking what has all this to do with model railways. Well my own layout is based on a lineside location at which I spent many happy hours trainspotting as a schoolboy in short trousers.

I'm recreating Heaton Lodge Junction in the West Riding of Yorkshire as it was in the winter of 1983. I have also spent the last four years researching the locality and am now almost halfway towards completing the 200' long layout – it being a 7mm scale rendering of the 1½ mile length of the real site – I've never been one to do things by halves you understand.

It struck me some months ago that I could perhaps use the aforementioned lesson learnt in business marketing to target the massive railway display directly to the general public by erecting it in shopping mall car parks across the UK, rather than the well established exhibition scene to which the 'converted' attend in swathes.

***I'm recreating Heaton Lodge Junction in the West Riding of Yorkshire as it was in the winter of 1983***

To that end, from the second half of 2020 the 90 – yes 90 – 4' x 5' baseboard sections that comprise Heaton Lodge Junction (the scenic section plus the 150' long fiddle yard, and the curves at each end) will be transported in three articulated trucks to a variety of locations already popular with the general public during school and bank holidays. It will be branded – for non model railway aficionados – as *Britain's Biggest Model Railway* (yes I'm sure there are those who will state they know another, with far more sleepers or rivets!).

Together with a display of part-built layout sections included within the same giant marquee, I'm hoping it will begin to attract new blood into the hobby, given that its

impact will be with people largely unfamiliar with model railways.

'Preaching to the converted' has always been the traditional exhibition managers' route to attain sufficient

numbers, but in the long term it may not be sustainable due to many factors such as our ageing population.

As for my approach, there is an historical precedent. In the 1960s British Rail transported its very own publicity layouts countrywide to great acclaim from the public at large.

For now, I'm still in the construction phase but watch this space as I shall be reporting from time to time on progress of this intrepid 'public relations' exercise in the pages of RAILWAY MODELLER.

***Simon George is a business entrepreneur with a passion for British outline O gauge.***

**Could this be Britain's Biggest Model Railway when it's completed? The public at large will be able to judge when Simon takes it on a grand tour of the UK in 2020. In this photo a customised Heljan Class 40 heads towards the junction with a rake of oil tanks.**





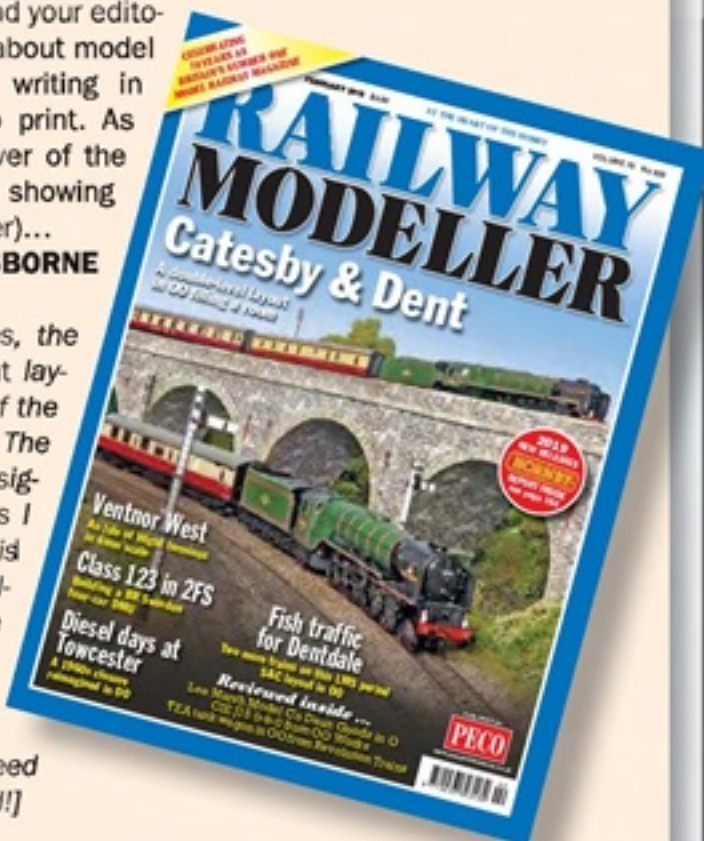
# Readers' Letters

## The flagman's behind the train...

I couldn't help smiling when I read your editorial piece in the February issue about model railway accuracy and readers writing in should a mistake make it into print. As bold as brass, on the front cover of the issue there is a photo of a train showing a SPAD (signal passed at danger)...

PETER OSBORNE

[Ed. We put this to David Bates, the builder of the Catesby and Dent layout featured on the front cover of the February issue and he replied – The signal in question is a working signal, but after extensive enquiries I have been informed that at this particular time there was a signalling fault (wire had snapped), the relevant bodies had been contacted and the driver had been notified at the previous signal box that he would have to proceed through at danger – Enough said!]



## Modelling – getting it (W)right

The Talking Points piece in the February RM by Tony Wright was thought provoking, some might even say controversial. After all, it is generally accepted by most model railway enthusiasts that Rule 1 applies: it is my layout and I will run what I want on it!

That is fine for those who wish to follow that rule. For me, railway modelling is the replication of prototype railways. I prefer to operate my model railway layout in accordance with correct operating practices which, in my opinion, enhances the layout, and gives it life. But it does require a bit more modelling discipline, and more research and background information on the topic of railway operation to get it right. For me the fun part of the hobby is 'getting it right', or should that be 'getting it Wright'?

Maybe there is a clear difference between a model railwayer and a railway modeller. And if Rule 1 applies, hopefully some will make that trip to become a railway modeller. However, be warned, it can be a frustrating and difficult journey at times.

ALISDAIR M MACDONALD

Tony Wright's interesting article gave me pause for thought. One cannot prescribe how someone should enjoy our hobby, but I will admit to agreeing with what Tony says.

When I started in the hobby, aged about 10, it was very much about aspiration and achievement. The classic route in was via a train set, reading one of the magazines, and dreaming of emulating the best modelling that was on show therein. One would progress via Superquick building kits and Airfix wagon kits, to modifying and improving ready-to-run models, using flexible track or even building one's own items

from scratch.

In contrast, the typical route now seems to revolve around ready-to-run products, and the feeding frenzy of 'product announcements'. The progress of new models through the production cycle is followed intently, as is the progress of the ship bringing the models from China.

The Baby Boomer generation is reaching retirement with money and time on its hands and I know a number of people with extravagant collections of models which spend most of their time in their boxes. I often wonder if any single purchase can ever give the purchaser as much pleasure as the satisfaction of building one's 'best model yet'.

My main observation is that human pastimes are more enjoyable if they are based on achievement and not acquisition. I do feel the hobby has lost something. It has certainly changed, and it is hard to see a true way forward.

CHRIS GARDNER

Oh dear – so Tony Wright only likes proper railway modelling!

Bob Symes-Schutzmann used to say that the mind was very flexible in terms of scale and that all sorts of inaccuracies could be overlooked if needs be. One of the all time classic layouts is the Madder Valley, where non-prototype equipment ran (indeed runs) together, but what it lacked in authenticity, it made up in spades with heart and humour.

You could really imagine going to Madderport or visiting the shops or pubs along the line. The craftsmanship was still there, but did not distract from creating a miniature community.

RICHARD STYLES

We can only consider for publication letters which are accompanied by the writer's full name and address (although we do not publish the latter except in the case of appeals). This also applies to all correspondence received via electronic means including email to: [railway-modeller@btconnect.com](mailto:railway-modeller@btconnect.com)

Correspondence for forwarding to any of our contributors must be addressed to them c/o RAILWAY MODELLER, Underleys, Beer, Seaton, Devon EX12 3NA.

All correspondence published here represents the opinions of the authors only and not RAILWAY MODELLER.

## "May the Fun be with you"

I've read with interest the responses over the last few months to my sci-fi models, Cato Pass, and the Time Travel Train made for The Great Model Railway Challenge TV show. I'm over the moon, and it keeps a modeller like me really enthused to read the positive comments, so I thank each and every one of those, and I just hope those not interested can just stay focused on their area of interest rather than feel the need to register disinterest. There is room in this hobby for all isn't there?

It is also nice to read about fantasy models based on Discworld, Emett, The Hobbit, and I have seen layouts with Mice and Minions about the shows too. Of course we had Tri-ang Battlespace in the 1960s and so

maybe that was the equivalent of A Trip to the Moon which is generally considered the first sci-fi movie.

I know my own Cato Pass has not invented the sci-fi model trains genre but hopefully, to use the same analogy, it can dare to lay claim to perhaps be a Star Wars style equivalent for model railways, bringing the idea to the masses again with its own unique style. I truly hope so, and just like that movie I have two sequels planned.

I now need to get a toy manufacturer interested just like the movies' director Mr Lucas did, and then maybe the hobby can inspire a few new members into our wonderful world of model railways. May the Fun be with you.

LAURIE CALVERT

## An emphasis on electronics please

I've noticed many discussions lately about the difficulties of attracting younger members into the hobby. I can't help but wonder if a focus on some more technical aspects would provide that encouragement, especially with programming skills now being consistently taught from primary school age upwards. I realise this isn't always everyone's cup of tea, so perhaps I'm biased – I like to think I'm at the younger end of the hobby (I'm in my 20s), and this is certainly where my preferences lie. I'm not sure I could weather a wagon to save my life, and my attempts at static grass are somewhat hilarious (not in a good way!)

I'm much more comfortable with the technical electronic side – designing and building my own DCC controller and decoders, adding automatic signalling, working on-train detection systems, directly controlling locos through wifi, automation, etc. are all things that I'm rather excited by! I've also found this area can be something some (not all!) older members tend to be less enthused by.

If there are others similar to me in this respect, then it may just be possible that advertising these sorts of tasks would see a greater interest and uptake from a younger audience.

MICHAEL BERRY

## Club wagons for charity

May I through the pages of your magazine pay tribute to the work of the Burnham & District Model Railway Club in Somerset – and in particular its member John Langley – in producing specially commissioned OO gauge wagons over the past 10 years. The club has announced on its website that it is no longer commissioning these limited edition wagons and I am sure I will not be alone when I say, "Burnham you will be missed".

Certainly my humble layout based on the GWR's Hemyock branch and the milk traffic thereon would be a poorer portrayal without these wagons.

Not to be overlooked is the club's very generous support of various charities from the profits of these wagons. The Community Support page of the B&DMRC website says it raised a very laudable £87,537.95 up until October 2018 with the last remaining stock still to be sold.

My photo shows two Duchess of Devonshire Dairymen vans awaiting loading on my interpretation of that Dairy's 'Butter Platform' which was a feature of the GWR's Tiverton Junction.

Well done to John and all at B&DMRC. Happy retirement!

DAVE WARREN





## Diagrams of the 'Swindon ghost' 4-2-4T

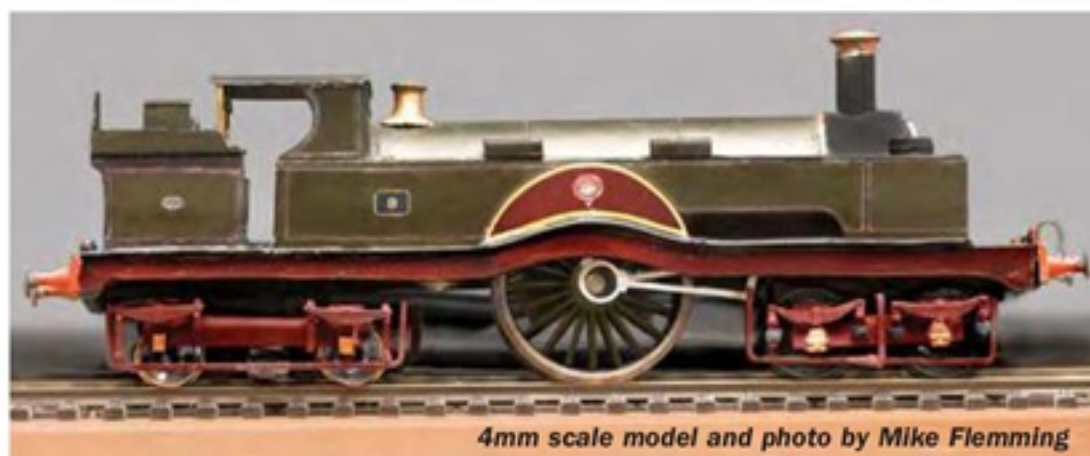
I have been reading with interest the Swindon's ghost article in the RAILWAY MODELLER Special for 2019.

A drawing of No.9 as a 4-2-4T loco is in Part 4 of the RCTS work *The Locomotives of the Great Western Railway*, on a plate between pages D24 and D25. The drawing is identified as Figure D35. The same plate has a photo of No.9 as rebuilt to a 2-2-2 tender loco. This is Figure 036.

Part 4 of the RCTS work was published in 1956; a year later, *An Outline of G.W. Locomotive Practice, 1837-1947* by Harry Holcroft was published by the Locomotive Publishing Co. Ltd. A drawing of No.9, as a 4-2-4T, appears as Fig.33 on page 48; perhaps it is a reproduction of that in the RCTS book?

There is quite a lot of information about No.9 on pages D17, D18 and D19 of the RCTS book. On page D17, the drawing (by Mr E W Twining), is described as "a conjectural reconstruction... based upon the known data".

Mr Holcroft wrote, in his preface to



his book: "Through the courtesy of Mr R. A. Smeddle, Chief Mechanical and Electrical Engineer, Western Region of British Railways, the manuscript has been carefully vetted by reference to the official records at Swindon. Sir William Stanier... has also read through the manuscript and has been good enough to signify his approval by contributing a foreword to this work."

It seems probable that the drawing

in Mr Holcroft's book is the closest we shall get to an authoritative record of the construction and appearance of No.9 in its original form.

There is some comment about No.9's design and its unsatisfactory performance on page 53 of the same book. Mr Holcroft's remarks include the sentence that "No.9 was liable to derailment and never did any useful work, but was set on one side."

**RICHARD NORTH**

## A grounded van diorama for a youngster to take home



In the article on the free kit build, December 2018, Craig Tiley asked for pictures from readers so I submit the following with a few notes.

Isaac Maxwell, my friend's son, is nine years old and generally builds in Lego. He visited me over the holiday when we operated my O gauge layout. We ran trains, looked at how things are made and had fun.

We then needed something for him to take home.

The baseboard and back are polystyrene offcuts. I had prepared the back-scene with a photo from the rear of my house, and we made the bunkers from thin plywood and the skip from plastic sheet and assembled the kit. I put the PVA glue on the hedge, tree and ground, then Isaac added the scatter

flock and static grass, put the gravel and stones into the bins and painted the road, bunker and van roof. The pipes are drinking straws and the timber stack coffee stirrers.

The skip was painted yellow with his name on the side. The overall effect is, we hope, an impression of a builders' supplies yard. We had a super day.

**ALLEN MORGAN**

## Vintage tinsplate is the hobby's historical context

I am writing to voice my support for Claire Sheppy and family's vintage three-rail Hornby O layout and commitment to railway modelling of yore (RAILWAY MODELLER November 2018 Talking Points, *Templeford* – particularly the concluding section – and the theme of your editorial).

My railway modelling involvement began with a basic second-hand Trix Twin 'Christmas Pillowcase set', in December 1941, but I was enamoured of Hornby Dublo from its arrival after the war, through my regular *Meccano Magazine* and the first post-war sets of two school friends.

Then I was given two 'mint' HD3 sets by a friend whose grandchildren were totally uninterested, an A4

Passenger and a 2-6-4 Goods set, so my search for a fully representative three-rail collection was begun.

After some 40-odd years of seeking 'affordable' second-hand runnable and repairable stock and contemporary scenic accessories, I've been able to recreate the table-top layouts of my 1950s and 1960s youth. I've also been asked "we've a box of old trains like yours, can you help in finding them a caring home?" – which I can of course, thereby conserving survivors for the future.

The semi-anonymous internet disparaging criticism of three-rail, be it O or OO by some of today's hobbyists referred to by Claire, is reminiscent of the attitude in the USA around 1940

where the 'scale' railroaders scorned the activities of the gauge O and S scale three-rail ready-made or tinsplate enthusiasts, which led to the development of 'hi-rail', the combination of toy-train equipment with scale for the enjoyment of model railroading as a family hobby, especially encouraging growing youngsters to participate and become skilled scale modellers in their own right as they matured.

I agree entirely with Claire that three-rail demonstrates the historical context of our hobby today. Her '15-year old' constructed signal box is the proof.

So, no disparagement of our historic past, but tolerance and respectful appreciation of our forebears!

**GEOFF M CALVERT**

## Snippets...

### Striven – ready for 2019

After a pleasant running session with *Striven*, Colin took the enclosed photo of me running all the trains back to their starting positions ready for the final running session of 2018 before we closed the network down for maintenance over the Christmas Holidays. Happily engineering works didn't over-run and we were ready in January for the first session in 2019.

**VAL ASHBY**



### 100% accuracy?

I note that the old canard of accuracy is being paraded. I don't believe that, in the whole history of our hobby anyone has managed to create a 100% accurate layout. I would argue that the closer you get to perfection the more boring the layout becomes. Ambience is far more important.

**FRANK DINEEN**

### Locos vaping

Given that there is sound for engines has anybody thought of equipping them with steam? I ask this as it struck me while seeing someone 'vape' with a first-iteration e-cigarette, could the technology, which involves electrical coils heating a liquid, be used within, say, a metal-bodied engine to produce a steam effect. Has this been done yet?

**MIKE GANDER**

### Accessible for all

I would like to thank the Marlow, Maidenhead and District Model Railway exhibition organisers for putting on an accessible for all model railway exhibition at the Cox Green Community Centre recently. There was plenty of room for everybody around the layouts which were at a suitable height for children and wheelchair users like me (left of photo) to see.

**SIMON JUDGE**





FROM **RAILWAY MODELLER** BY OUR EDITORIAL STAFF AND CONTRIBUTING MODELLERS

## LNER J36 0-6-0

### Brand new ready-to-run in OO from Hornby



Of the three brand new locomotive releases by Hornby just before Christmas, it might seem bizarre to concentrate on the 'ugly duckling' of them – the LNER J36 – when there are express passenger locomotives to attract our attention (the SR Lord Nelson, see overleaf, and the streamlined LMS Princess Coronation, see next month). Yet the J36 has never been available in 4mm scale ready-to-run; it represents the largest class of North British Railway locomotives; and when the last two were withdrawn in 1967, they had outlasted all other Scottish steam power... certainly worthy of the lead review we feel!

Chief Mechanical Engineer of the North British, Matthew Holmes, introduced his Class C 0-6-0s in 1888; batches were built thereafter by the NBR Cowlairs Works in Glasgow until 1900, with 30 examples being turned out (15 apiece) by private builders Neilson and Sharp Stewart in the early 1890s. In all 168 were built, used

for goods and passenger traffic, often on lightly-laid branch lines – which was the reason for their survival against more powerful but heavier machinery. Withdrawals began as early as 1925, proceeded apace through LNER days, but BR still inherited 123. One, No.65243 *Maude*, made it to preservation, but it is currently out of traffic, cosmetically restored at Bo'ness, on the Bo'ness & Kinneil Railway.

Our sample, No.65311 *Haig*, is rep-

resentative of the 26th J36 to carry a name: the other 25 were so honoured following active service overseas in WW1. The original *Haig*, BR No.65226, was withdrawn in April 1951 and the title was applied to the prototype for our sample in May 1954; the 64A St Margarets shed plate ties the model down to the two years from naming – unofficially – to transfer away to 64E Polmont in May 1956. (For completeness, the loco was built 'in

house' in March 1899 and withdrawn in November 1963 from the same Falkirk depot, by then coded 65K.)

We have remarked frequently that it was the humble and often elderly goods 0-6-0 which made a greater significance to a railway's balance sheet than the showboaty express passenger types, so to have a model manufacturer pull out all the stops as Hornby has with its J36 it's doubly significant. There's excellent representation of the inside valve gear, sprung buffers, very fine cab detail, and even a model of the rainsheet that might have made tender-first running in the wet a tad less unpleasant. Rivet detail abounds, the tender can be close-coupled to the engine (as





## VINTAGE VIEW

### LNER J36 0-6-0

No.65311 Haig at an unspecified location (believed to be Kittybrewster, Aberdeen) in the mid-1950s. Note the LNER-style ST MARGARETS depot lettering on the buffer beam, absent from the model.  
Photo: Colour Rail



here for our photos) and the hinged fallplate will prevent the doubtless-already-cast Master Piece crew figures bespoke to this engine from falling to the track. The tender coal rails are moulded in a springy engineering plastic, and the moulded coal load can be removed if desired. Within the tender body is the 8-pin dual inline DCC decoder socket and housing for a 28mm diameter speaker.

The model weighs 242g thanks to a metal superstructure; six Pullman cars were handled over the 1:48 gradients and 3' curves of the Pecorama loft layout *Peckwick Rye*, but its most impressive feature is its impeccable slow speed running at barely a whisper of transmission noise by a five-pole open-frame motor and two flywheels. For a mass-production model that's what we call quality control...

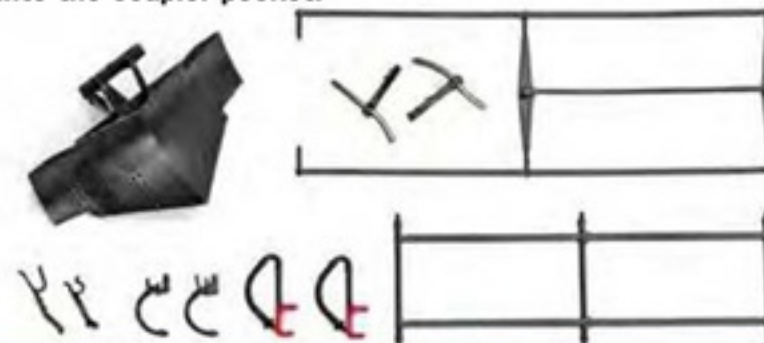
Detail parts for the modeller to fit include the miniature snowplough fitted to locos built for northern climes. All have been attached to our sample for photography.

Hornby has certainly raised the bar when it comes to pre-Grouping 0-6-0s; the choice of available follow-ups is many and varied; J6 anyone?

Tim Rayner



The miniature snowplough clips into the coupler pocket.



#### Sample supplied by

**HORNBY HOBBIES Ltd,**  
Third Floor, The Gateway, Innovation Way Discovery Park,  
Sandwich, Kent CT13 9FF. [www.hornby.com](http://www.hornby.com)

PRICE ref.R3622 - £139.99







The second of three 'headline' releases from the famous Kent-based firm to hit the streets just before Christmas is the Hornby take on the SR Lord Nelson 4-6-0, a thoroughly 21st century representation of the 16 express engines penned by R E L Maunsell, the Southern's Chief Mechanical Engineer in the 1920s. Specifically, our sample

replicates second-to-be-built No.851 *Sir Francis Drake*, as running in the 26 months between introduction to service in June 1926 and fitment of the smoke deflectors in August 1928. (The absence of deflectors, combined with the 5000-gallon bogie tender make for a particularly sleek appearance.)

The class was the subject of a

Drawn & Described feature in the February 1993 edition of RM. Placed on the late Ian Beattie's drawings, the model squared up impressively. Items such as handrails, ejector pipe and 'copper' pipework around the injectors are separately fitted parts, and there is a wealth of details supplied for the purchaser to fit, such as brake pull

(Thank the Southern's lack of water troughs for the copious amount of space inside the tender!)

Painting and lining is first class, right down to the power classification letter A on the front footplate – the Urie scheme of March 1916 for the LSWR was continued under Maunsell for certain classes, whether based on the Western Section or not; 851 was at Battersea (later Stewart's Lane) shed from new until the outbreak of WW2.

This new 'Nelson' will be warmly welcomed by Southern modellers of Railway or Region flavour – another feather in the red-box team's cap.

Tim Rayner



rods and cylinder drain cocks – all of which have been attached to our sample for photography. Some of the mouldings – the drain cocks especially – will need to be omitted if the model is to traverse sharp curves. Cab detail is exquisite, and the loco/tender gap is covered by a hinged fallplate. The draw-bar allows two engine/tender spacings; our sample displays the closer of the two. Buffers are sprung.

The loco tips the scales at 311g, which was good for hauling 18 Hornby Pullman cars around the Pecorama loft layout *Peckwick Rye*. The can-type motor has two flywheels, and drives the trailing coupled axle smoothly and quietly straight from the box. DCC users will find the 8-pin socket and blanking plug in the tender plus a housing for a 28mm diameter speaker.



Detail parts include a coupler for the front end.

## Sample supplied by

**HORNBY HOBBIES Ltd,**  
Third Floor, The Gateway,  
Innovation Way Discovery Park,  
Sandwich, Kent CT13 9FF.  
[www.hornby.com](http://www.hornby.com)

**PRICE**  
ref.R3634 – £169.99





Announced as a brand new project in 2017 (see News, March that year), Oxford Rail has delivered on its promise to release models of the Great Eastern N7 0-6-2Ts in OO gauge.

The real things were designed by Great Eastern CME Alfred Hill as a way of not only updating the company's motley fleet of older locomotives on the suburban services out of its London terminus of Liverpool Street, but to combat the increasing threat from street tramways. Unlike the electrification-minded railways south of the River Thames, the GER's solution lay in high-powered steam tanks with small coupled wheels, which were to become the backbone of the reorganised suburban workings. Inaugurated in 1920 under General Manager Henry Thornton these services swiftly and memorably became known as the 'jazz trains'.

Initially Hill caused two 0-6-2Ts to be constructed – at the company's main shops at Stratford – to compare superheated and saturated steam propulsion. In all 11 of each type were



active at the Grouping, and the design was perpetuated by the LNER to a total of 134. Also several were fitted with push-pull gear later in life once displaced from their stamping ground by electrification, newer steam power and the modernisation plan DMUs. Just one survives in preservation today.

Our sample depicts the Great Eastern K85 Class variant, which carries the very late GER period livery c1921, with the large numerals that aided identification of trains by signalmen and suchlike. These and the destination board clips, prominent on the smokebox, helped commuters recognise their train at busy times.

In addition to our sample, Oxford Rail will be releasing examples in LNER and BR early black finishes as well, as No.8011 and the preserved No.69621; see the Oxford Rail website for additional details of these and future variants.

The locomotive matches up very well to published dimensions, and has the characteristic look of these powerful machines. The sole detail parts for



Weight is an impressive 295g, thanks to a chiefly metal superstructure. This equated to a very creditable 21 Hornby Pullman cars on the Pecorama loft layout *Peckwick Rye*; the model is as sprightly as its prototypes.

It is driven by a can motor with flywheel, acting on the trailing coupled axle by a gear train. In motion the loco is very smooth and quiet, even without the manufacturer's recommended gentle running-in. Versions of each livery are also available with DCC sound factory-fitted; the eight-pin socket and blanking plug is at the bunker end of the chassis. There is ample 'rock' on the fixed trailing truck to allow for sharp curves; the N7 ran happily through a Radius 1 (371mm) reverse curve.

Oxford Rail can pat itself on the back with this little piece of the 'Swedey' – but then please start planning the quint-art sets of coaches it'll need! There'll be many more *Minories* layouts in 4mm scale to come...

Tim Rayner

the purchaser to install are the steam heat pipes (fitted here), the model having an impressive array of factory-fitted separate mouldings. The Westinghouse pump on the driver's side of the smokebox – this being one of the right-hand drive originals – is particularly well represented. The N7 is shown in dual-brake format, it being simple to detach the vacuum pipe if it offends. Cab detail is good, visible through flush-glazed windows.

Slimline tension-lock couplers are supplied fitted, but there appears to be an overly-excessive distance between the front coupler and anything attached to it – certainly greater a gap than between the bunker and a trailing load.



## Sample supplied by

**OXFORD RAIL,**  
Oxford Diecast, P O Box 62,  
Swansea SA1 4YA  
[www.oxfordrail.com](http://www.oxfordrail.com)

**PRICE**  
**£109.95**  
(DCC sound-fitted £219.95)





In the December 2017 issue we were pleased to illustrate the exquisite Great Western Diagram D20 clerestory brake third from Lee Marsh Model Co, hand-crafted by SMT of South Korea. There

are very few examples of the matching Diagram E53 and E54 composites (first/second and first/third respectively) and the C17 all third which were mentioned at the time, and shown here.

Both need no introduction in terms of museum-quality finish, interior furnishings and top drawer attention to detail. Models are available in 1898 livery with the GWR garter crest; 1912

crimson lake; fully lined brown & cream as per the 1922 period, and brown & cream circa 1927. Pictured are the E54 composite in 1898 livery, and the all-third in crimson lake. As usual with these works of art on wheels, a pair of gloves is uppermost in the sumptuous packaging to enable the models to be handled without chance of spoiling the fabulous finish. When they're gone, they're gone – Lee Marsh advises that the only passenger vehicles that will be available are the forthcoming Diagram K14/15 passenger brake vans.

Tim Rayner

## Samples loaned by

**LEE MARSH MODEL CO,**  
Unit 5, Lancaster House,  
9 Moss Way, Hillend Industrial  
Park, Dalgety Bay, Fife  
KY11 9JS.  
[www.leemashmodelco.com](http://www.leemashmodelco.com)

**PRICE**  
£1,285.00 each



# Brand new PGA hoppers arrive from Cavalex Models



New manufacturer Cavalex Models has wasted no time in delivering its promised PGA two-axle aggregates hopper, tooling samples of which were seen in the April 2018 issue. The two liveries initially available are seen here.

The prototypes for the Redland hoppers – later reliveried into the Lafarge white when the company was taken over by the French aggregates giant in 1997 – were built by various manufacturers, mostly Standard Wagon at Heywood, in the late 1970s. Other private owners and operators had similar

designs, so Cavalex has chosen its first prototype well. Livery application is good, down to the small lettering, lube charts and suchlike. The yellow axle-box covers can be 'strengthened' with a dab of paint if desired.

The bodies are excellently moulded, with the bevelled top to the hopper sides being an effective way of replicating sheet metal with injection moulded plastic. The end platforms have impressive see-through mesh, and all ribs and interior tiebar are finely recreated. The underframe detail is very well repre-

sented, most of it being all but invisible from normal viewing angles and distances. The hopper door actuating linkages are especially worthy of note, as is all the plumbing.

The models weigh 49g, and roll smoothly on fine metal wheelsets. Slimline tension-lock couplers are clipped into NEM pockets on sprung self-centring mounts.

This is a cracking debut release by Cavalex Models; we shall await its next project – see News, p260 for details – with interest.

Tim Rayner

## Samples supplied by

**CAVALEX MODELS Ltd,**  
2 Hazel Avenue,  
Black Notley, Braintree,  
Essex CM77 8GD.  
[www.cavalexmodels.com](http://www.cavalexmodels.com)

**PRICE**  
RRP £30.00 each – available  
from selected retailers





Irish Railway Models has delighted those modelling Irish railways with the release of the Tara Mines bogie wagons, supplied in packs of two wagons with individual fleet numbers and detail differences. Five different packs are available; illustrated is Pack E, fleet numbers 31020 and 31024 – with overhead live wires warning flash amidships on the upper bodyside.

The real things are a freight heavy-weight in the Irish Republic, carrying zinc from the company's mine in Co Meath to Dublin Port for onward ship-

ment to Scandinavia. Introduced in 1977 in a blue livery with the Tara Mines logo, they were later painted red oxide as per our samples. The Tara Mines trains are the heaviest on the IE system, each wagon carrying 54T of zinc. Class 071 locomotives are regularly rostered, these wagons being an ideal load for the Murphy Models release (see Reviews, June 2013).

These finely detailed models are quite heavy themselves, at 95g, but roll smoothly on 16.5mm gauge metal wheelsets. (Provision for the correct



21mm gauge, representing 5'3", has been taken care of, the supplied wheelsets being set in on the scale length axles.) The wheelsets themselves are in finely moulded Sambre et Meuse Y23 bogies, complete with rotating axle end caps. Underframe detail is excellent, even if largely invisible when the wagons are on the track.

Slimline tension-lock couplers are fitted; drawbar couplings, Bachmann Mk.I coach-style, are supplied in the packaging along with brake pipes and a supply of spare axle end caps.

The painting and finishing is excellent, and can be enhanced further with some judicious weathering.

Just 250 of each pack version have

been manufactured; IRM offers a bulk deal on all five packs of these distinctive models.

Tim Rayner

## Sample supplied by

**IRISH RAILWAY MODELS,**  
9 Crumlin Business Centre,  
Stannaway Drive, Dublin 12,  
D12 VH27, Republic of Ireland.  
[www.irishrailwaymodels.com](http://www.irishrailwaymodels.com)

## PRICES

€79.95 per pack

€375.00 for bundle of five packs



# Resin loads and 3D printed buildings from Unit Models

## 009 & 0

Unit Models has released a set of stone and coal loads in unpainted resin to suit the Bachmann models of the War Department Class D bogie open wagons (see Reviews, February 2018). The stone loads are supplied in two different versions – best to suit the Ds used on the Ashover Light Railway – and the coal as a single load. Unit Models also has examples of both with steel inserts embedded in the castings, allowing them to be unloaded easily by a magnet. If necessary they

can be trimmed to fit; take care as resin filings can be harmful if ingested.

As mentioned, the loads are supplied unpainted; a couple of painted castings are pictured to show these useful releases off best.

The selection of 3D printed buildings for O gauge has been expanded by (from left) a GWR-type lamp hut (ref.O-105P, footprint 68mm over steps x 52mm); sleeper-built buffer stop (ref.O-103P, footprint 48mm x 55mm) and coal office (with choice of signage, ref.O-102P, footprint 83mm x 80mm over front step and rear chimney). The models should be available from your local model shop, or from Frizinghall Models & Railways.

Tim Rayner



## Samples supplied by

**FRIZINGHALL MODELS & RAILWAYS,**  
Unit 8A, Sapper Jordan Rossi  
Business Park, Otley Road,  
Baildon, West Yorkshire  
BD17 7AX.  
[www.modelrailshop.co.uk](http://www.modelrailshop.co.uk)

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## Book Reviews

### From the Files – Locomotives that were ... and locomotives that weren't

Robin Barnes

This intriguingly titled book is subtitled 'an artist looks at locomotive development', and is a delightfully subjective look at some 25 more or less unlikely machines which were built and 14 – in some cases more credible – proposals which never left the drawing board. Some of those which were built perhaps should not have been, and some which were not might have worked. It is fair to say they are all curiosities, which aroused the curiosity of the artist.

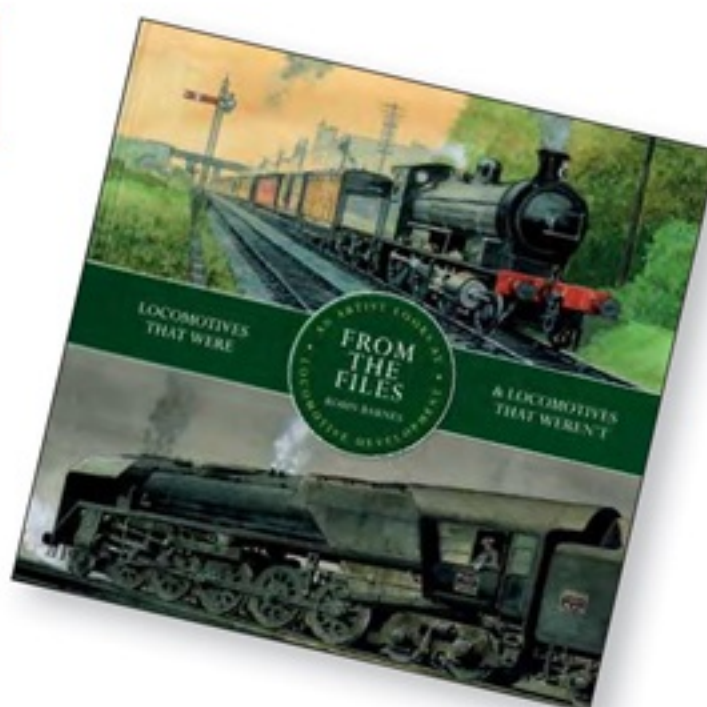
They come from all over the world, and cover a wide period, many – not surprisingly – from the earliest days of railways before the 'standard' technology was fully established, though other later proposals remind us that the technology was being continually developed in some quarters. As well as the expected steam, electric and internal combustion also feature, not to mention other sources of power (eg compressed air). And there are excursions beyond railways – how about a steam-powered flying boat?

Most of the machines built were unique, early, and short-lived, so photographs are rare, and naturally there can be none of those not built, so both are ideal fodder for artists' impressions. It takes an unusual artist with a certain technical appreciation to concoct a plausible image from sometimes scant technical information, and the author ably demonstrates his skill in this respect. (The occasional incursion of other style icons in appropriate periods, such as 1930s automobiles and fashionably-attired young ladies, may hint at his other interests and talents!)

The style is generally rather more impressionistic than photographic, but that is inevitable and indeed somehow appropriate.

The contemporary approach might be to create a virtual model in 3D CAD and then insert it into a real photo or intricately crafted digital background.

There is a double page spread for each



subject, with the main artwork illustration reproduced large and often supported by a supplementary photograph, usually of an indirectly related subject.

The descriptive text ranges widely, covering much more than the primary subject, setting the machine in context to a greater or lesser extent. Each 'entry' is concluded with a tinted background panel which details sources and suggests further reading.

There is an interlude devoted to the trams in his home city, Edinburgh, and the whole collection is prefaced with a biographical section which explains the origins of his interest in railways, and particularly the unusual and downright strange.

Most readers will find something here they did not know before. It is a dangerous book to dip into – you will almost certainly end up spending more time reading than intended!

We can only concur with the sleeve notes, which declare that the book will enlighten and entertain in equal measure.

It should also strike a chord with modellers, given their propensity for one-offs and might-have-beens!

Camden Miniature Steam Services,  
Barrow Farm, Rode, Frome, Somerset BA11 6PS.  
[www.camdenmin.co.uk](http://www.camdenmin.co.uk)

302mm x 302mm 92pp  
**Hardback £26.95**  
ISBN 978-1-909358-42-3

### 101 railway stations – a journey of variety

Jeffery Grayer

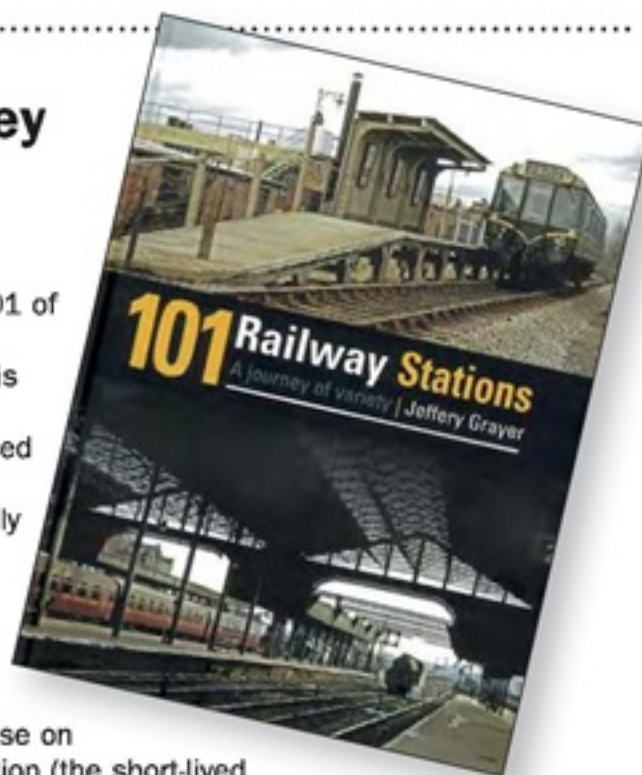
Stations, like dalmatians, are all different – and here's 101 of them, large and small, urban and rural, with a potted biography of each. There's bound to be details or the basis for a cameo scene in here for the layout.

The locations run country-wide, and depict (mostly) closed stations, although encouragingly several have been reopened – with vastly different architecture than previously – and there are also views of completely new stations, such as the one serving Cranbrook, the new town just outside Exeter on the former LSWR main line. Space is found for never-opened Lullingstone, intended to serve a development that didn't happen, and also Lymington Commando – not open to members of the public, only those on MoD business. Well-known quirks such as Boscombe Junction (the short-lived halt near Bodmin) and Templecombe Lower on the S&D are included, as are Rugby Central and many others. Several will be familiar to RM readers, such as Dent and Chard Town Goods; others less so, such as the truly remote Altnabreac.

Most of the photos are black and white, but there is a good selection of colour, including vintage postcards. OS map clippings aid locating the stations. In short, an enjoyable read.

OPC, an imprint of Crécy Publishing Ltd,  
1A Ringway Trading Estate, Shadowmoss Road,  
Manchester M22 5LH.  
[www.crecy.co.uk](http://www.crecy.co.uk)

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ISBN 978-0-86093-692-3



### Lesser Railways Around Darlington

Robin Coulthard, John G Teasdale,  
John P McCrickard and Richard V Webster

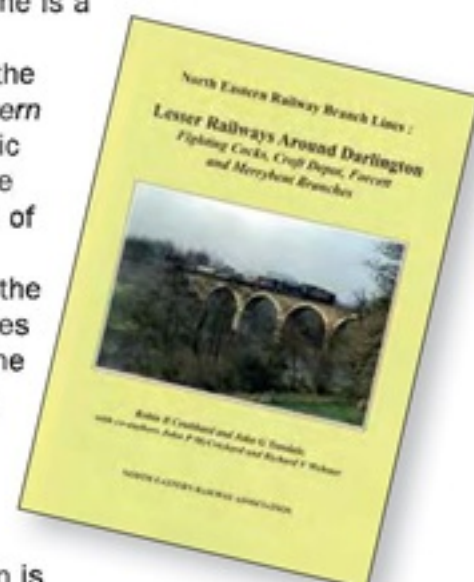
This A4 paperback volume is a compilation of articles previously published in the NERA journal *North Eastern Express* and photographic exhibitions staged by the association at the Head of Steam Museum in Darlington. It describes the history and operating lives of four branch lines in the hinterland of Darlington and includes archive photographs, tables, diagrams and line drawings. As would be expected the information is extremely well researched, written and presented and there is plenty within its glossy covers to fire the imagination of layout builders and modellers.

The lines covered are The Fighting Cocks, Croft Depot, Forcett and Merrybent branches. The first is a section of the original Stockton & Darlington route; the second, a rural goods only terminus adjacent to the east coast main line; and the last pair, one-time private mineral branches off the Stainmore line serving quarries and lime kilns.

For modellers with a liking for branch line prototypes, this 72 page book is a welcome breath of fresh air amid the seemingly endless studies of GW and SR branch lines published every other week.

North Eastern Railway Association,  
The Sales Officer, 15 Woodside Drive, Darlington DL3 8ES.  
[www.ner.org.uk](http://www.ner.org.uk)

297mm x 210mm 72pp  
**Softback £11.95 (inc P&P)**  
ISBN 978-1-911360-13-1



### Railway cranes volume 3

Peter Tatlow

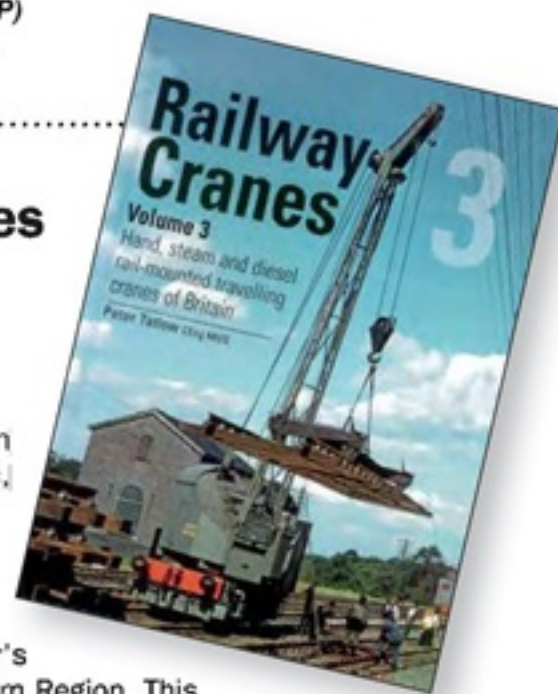
The author will be recognised instantly as an expert writer on this topic, having penned two volumes already on cranes, thanks in no small part to his service in the Chief Civil Engineer's department of BR Southern Region. This third volume looks at hand-worked cranes; smaller steam cranes (up to around 15T); diesel-mechanical, diesel electric and diesel hydraulic cranes, and telescopic-jib cranes. Within the latter chapter are several light ones and the massive 100T and 125T Kirov examples.

In addition to the clear-to-understand text, well-reproduced black & white and colour photos are accompanied by 4mm scale drawings of selected cranes, further to tempt the modeller. Photos are often taken at work sites off-limits to the general public, so the views herein are especially valuable.

Cranes are perennially popular with enthusiasts and modellers, so this book will be of great interest, opening another door into a less-highlighted part of railway operations.

Crécy Publishing Ltd,  
1A Ringway Trading Estate, Shadowmoss Road, Manchester M22 5LH.  
[www.crecy.co.uk](http://www.crecy.co.uk)

302mm x 216mm 224pp  
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ISBN 978-0-86093-684-8





## WDLR Companion

Colonel David W Ronald and Roy C Link

Subtitled 'The conception and evolution of British 60cm gauge railways on the Western Front, 1916 – 1918', this new book is intended to complement the *WDLR Album* published in 2014 – neatly book-ending the centenary of the First World War, an appropriate commemoration.

The album was based on a collection of military photographs compiled at the end of the war, which depicted the British 60cm gauge light railways used on the Western Front in France. It was thought that the photos accompanied a report, but this could not be traced at the time. However, a copy of what is believed to be the report was subsequently found in the Records Office at Kew.

This new book draws information from that report and another on the 1916 proposals to improve the movement of munitions, supplies, and men to the front lines.

At its peak there was about 1,000km of line delivering 200,000 tons of stores each week – no mean achievement.

Colonel Ronald has used his military experience to provide a clear explanation of why and how the light railway system came into existence, how it was organised, how it was operated, and how it came to be largely ignored after the conflict despite having played such an important role.

There are extracts from the report and other previously unpublished material. The drawings and diagrams featured in the reports have been reproduced here.

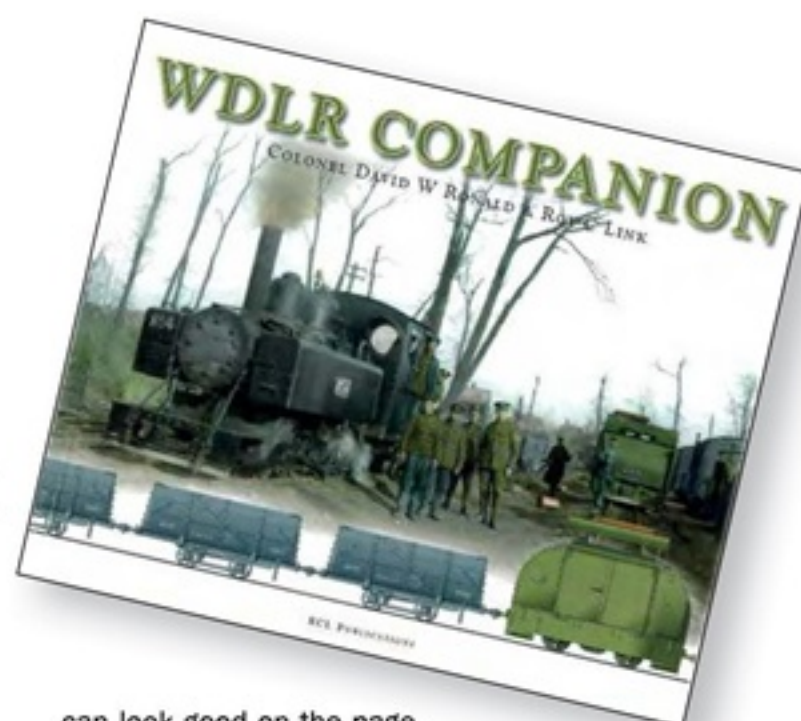
The need for improved supply lines became very apparent when the military situation became a stalemate in 1915. The Railway Operating Division had been formed to work lines for the most part taken over from French and Belgian railway companies, but something more was needed to develop and co-ordinate the use of 60cm to forward positions – hence the formation of the Directorate of Light Railways. The Canadian forces, with experience of narrow gauge in agricultural and forestry applications, made a significant contribution. After the Armistice, the light railways continued to do useful work during the clear-up and reconstruction.

There are sections on steam and internal combustion locomotives, as well as the workshops needed to maintain the motive power and rolling stock.

To augment the text and photos, there is a collection of scale drawings for modellers by Peter Foley, himself a WDLR specialist. Most have been revised for this book.

There are over 60 photographs, reproduced well, and many presented large. Most are black & white, of course, with the exception of a few facsimiles of period documents and the tastefully colourised cover image. In addition there are 56 scale drawings and 29 diagrams, maps, and plans.

Production is to a very high standard, as might be expected from this publisher, demonstrating that historical integrity and attractive design are not mutually exclusive, and that accurate information



can look good on the page.

The work is rounded off with a bibliography, acknowledgements, and an index.

The WDLR Companion is both an independent work which can be appreciated in its own right as well as an extension of the Album. Together the two offer a unique record of the War Department Light Railways.

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[www.narrowgaugeandindustrial.co.uk](http://www.narrowgaugeandindustrial.co.uk)

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## The Western Region diesel hydraulics – a pictorial observation

Robert Carroll

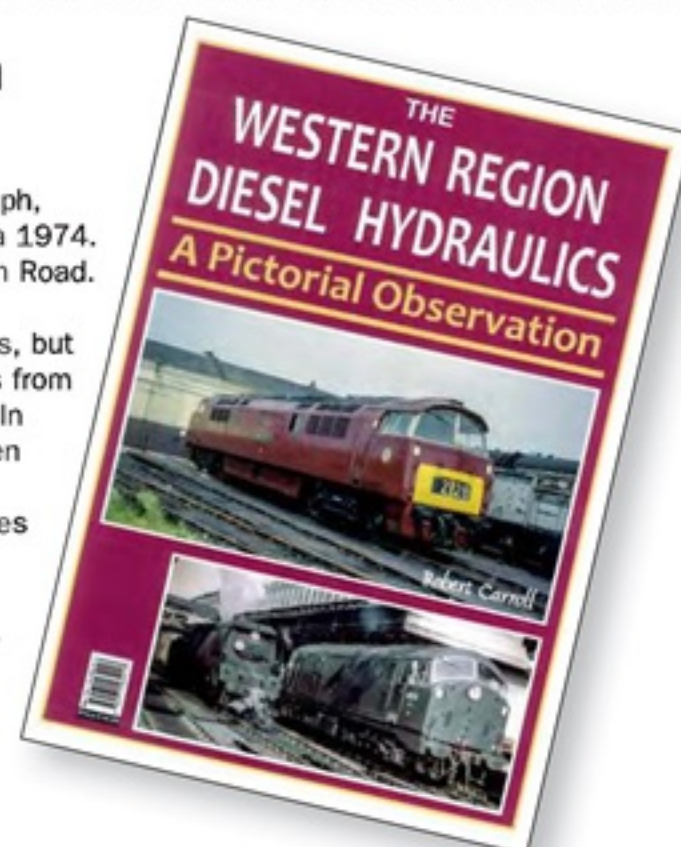
For your reviewer, this fabulous return to the days of Westerns, Warships *et al* starts with a lightning bolt of a photograph, accompanying noted lensman Tony Wright's introduction: *Western Reliance* at Liskeard with the up Cornish Riviera circa 1974. The caption notes the shorter formation, with more coaches to be added (including catering vehicles) at Plymouth North Road. Truly the stuff of our family holidays back in those pre-car days...

The colour photographs (two of the original D6xx Warships are monochrome) span the early 1960s to the late 1970s, but arranged class by class: thus the view of weary Warship No.833 *Panther* at Reading in August 1971, two or so months from withdrawal, is followed by shiny-clean D1000 *Western Enterprise* in its desert sand finish at Swindon in October 1963. In similar vein the 'Laira blue' Class 52s are followed by an out-of-the-box Hymek; and a 35 in Reading by a two-tone green 0-6-0DH shunter. Following the short life of these machines comes the NBL Type 2s to round off the fleet.

Captions are extensive, and frequently display the author's hallmark knowledge of carriage formations. These decades saw much of the new and old mixed together; a case in point being the coaches behind D802 *Formidable* at Exeter Central in 1968: blue & grey and green Mk.1s, Bulleids in green, and a Gresley buffet car in maroon. The immediate post-steam period represented by the later careers of these hydraulics are an interesting and untapped mine for layout builders, and books such as this will provide much food for thought.

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## Locomotive Traction 2019

Pip Dunn

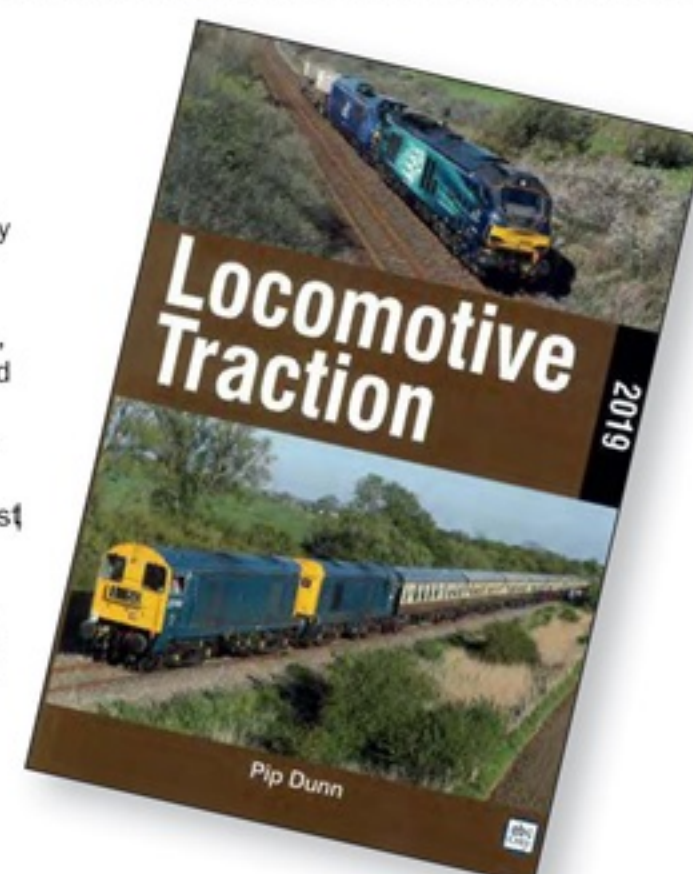
This is – in spirit and in fact – today's *abc* locospotters' guide to the railway locomotives of Britain in 2019. (The publisher took on the 'brand' of *abc* from Ian Allan.) It lists the fleet in four categories: main line – ie UK-based fully operational locomotives; spot hire and exported; preserved; and disposed, which includes everything from unclassified LMS shunters, through the extinct modernisation plan classes, to the Class 87s and sundry departmentals. All are identified by their current numbers, with former identities listed. Appendices list livery codes, pool codes, depot codes, owner codes, spot hire and owner details, and three-letter abbreviations for the preserved and heritage railways. Additionally codes for individual loco fitments within a class (RETB signalling, snowplough brackets, Scharfenberg couplers etc) are given, as are the meanings of the technical abbreviations provided at the start of each class's entry – OTMR, TWPS and suchlike.

Clear colour photographs leaven what could otherwise easily be a wall of information, mostly taken during the last two years or so.

Inevitably some typos have evaded capture: we learn that Class 09 No.09 007, based at Willesden and presumably in London Midland (LON) livery is actually listed as 'LOL' – a comment from the author on the scheme, perhaps?! The transposition of Genesee & Wyoming's name as 'Wyoming & Genesee' (p.44) is less of a laugh out loud, however. Nevertheless, this is an indispensable guide to the fast-changing power scene in the country today. Now, where's that ballpoint...?

Crécy Publishing Ltd,  
1A Ringway Trading Estate, Shadowmoss Road, Manchester M22 5LH.  
[www.crecy.co.uk](http://www.crecy.co.uk)

218mm x 148mm 144pp  
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MARCH 2019 • NEWS P.254 • NEXT MONTH P.260

## PECO unveils new easy to mount 'TwistLock' solenoid point motor, medium radius bullhead points and more items in 009

Peco used this year's Nürnberg Toy Fair in Germany (held between 30 January and 3 February) as the platform to reveal plans for several new products, including; a revolutionary new design of solenoid point motor, further additions to its code 75 00 gauge bullhead trackwork range, a number of N gauge wagons from all-new tooling and more 009 rolling stock items.

The new Peco Lectrics TwistLock Turnout Motor (ref.PL-1000, photo 1) will utilise an innovative new method of installation that promises to be both quick and straightforward to complete. The accompanying step-by-step sequence (photos 2a – 2c) illustrates how the new motor is a 'clip-fit' to corresponding locating pegs mounted beneath a layout baseboard. (Note that a pre-production item is pictured.)

The 16V ac solenoid motor (rated 3 amps) will be compatible with all Peco pointwork items (and other manufacturers' up to O gauge. The PL-1000 unit will have wires pre-attached, which negates the need for soldering, whilst the 'TwistLock' feature will enable the unit to be removed and re-fitted, whilst maintaining perfect alignment.

A new twin-microswitch attachment, which itself twist-locks to the PL-1000 housing (hence the two 'feet' that form part of the housing assembly), will facilitate a straightforward means of auxiliary frog switching; the microswitch will be available either supplied with the motor (PL-1001) or separately (PL-1005).

A surface mounting option will also be achievable with a special mount (PL-1006).

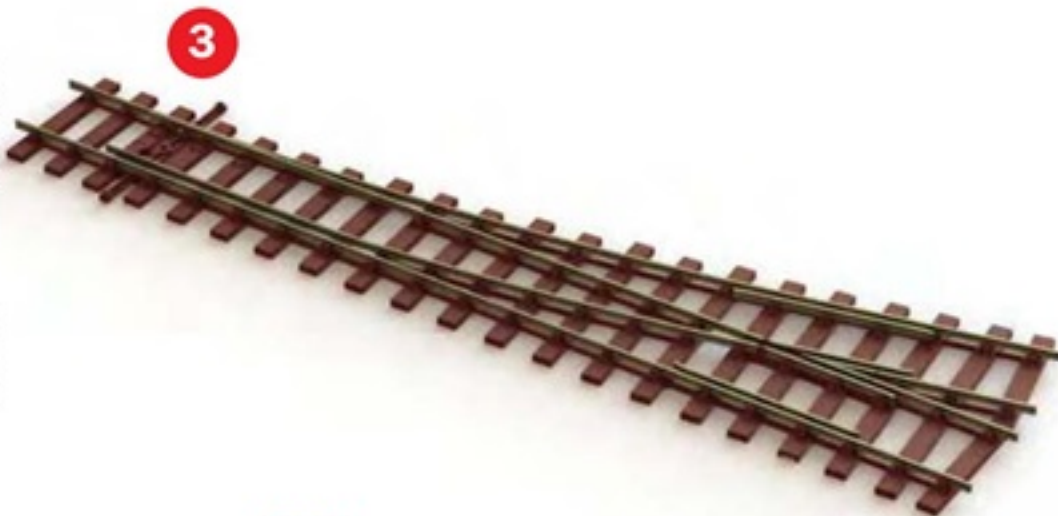


### Bullhead range additions

It has been confirmed that medium radius two-way points will join the 00 gauge code 75 bullhead range; the left- (SL-U1190) and right-hand (SL-U1196) items will mirror the geometry of the existing 00 gauge Streamline flatbottom medium radius points and will be supplied as Unifrog with solid blades. A CAD image is published here (photo 3).

### New 009 rolling stock

There will be new ready-to-run rolling stock items in 009 comprising; a Snailbeach District Railways four-wheel hopper wagon (photo 4), Festiniog Railway four-wheel quarryman coach (photo 5) and two types of Festiniog Railway four-wheel brake van (photo 6). All are illustrated here with CAD images.





### Re-tooled N gauge wagons

The long-established range of Peco N gauge ready-to-run wagons will benefit from re-tooling for the ever-popular five- and seven-plank open wagons (photo 7). The new tooling will cater for versions with and without end doors, whilst the 9' wheelbase chassis will feature NEM coupling pockets and metal wheels. A CAD image is reproduced here.

7



### And more...

The 4mm scale Parkside Models range will see the return of the LNER Bulk Grain Van; the original tooling for this kit (ref.PC13, photo 8) had become life-expired prior to the acquisition of the range by Peco, and therefore brand new tooling is being produced.

Other new announcements include Trackbed Weathering Kits in the Peco Scene range (PS-370/PS-371), sky sheets in the recently-introduced line of photographic backscapes, and a security gate kit in the Wills Modern range (SSM323, photo 9). The O gauge lineside range will be bolstered with signal box kits with brick and wood bases, utilising the cabin components from the existing ground-level version.

Look out for reviews of all the aforementioned products in upcoming issues of RAILWAY MODELLER as soon as they become available; all are planned for release during the course of 2019.

Development work continues on several previously-announced items, including the turntable motor, bullhead crossings and slips, and overhead catenary portals. A CAD render is illustrated here for the forthcoming Parkside Models O gauge kit for the BR 13T hopper (photo 10).

For further information about Peco products:

[www.peco-uk.com](http://www.peco-uk.com)

8



9



10



## LNER J27 to come from Oxford Rail in OO



At the recent British Toy & Hobby Fair, held at the Olympia exhibition centre in west London in late January, Oxford Rail announced its project to model the LNER J27 (North Eastern Class P3) in OO gauge. The firm has finished the CAD for these characterful Worsdell 0-6-0s, and will begin tooling shortly.

In all, 115 of these capable locomotives were built, the last 35 under the stewardship of CME Vincent Raven. The NER's works at Darlington constructed most; private builders Robert Stephenson, North British and Beyer Peacock accounted for the rest. Introduced between April 1906 and September 1923, one – the final example constructed – made it to preservation.

▲ J27 No.65894 at the Embsay & Bolton Abbey Railway on 24 August 1998. Photo: John Whiteley

Oxford Rail intends to cover the chief differences in the batches over time – boiler types etc – and will begin with four liveries, all pristine: plain LNER black No.1010; LNER black with red lining No.1214; and BR Nos.65837 and 65817 with early emblem and late crest respectively. Models will be supplied DCC ready and with DCC sound factory-fitted for all versions.

As yet no indication has been given as to timeframes for availability; keep an eye on the Oxford Rail website for full details in due course. [www.oxfordrail.com](http://www.oxfordrail.com)

## Hornby surprises with all-new Stroudley Terrier at 2019 range launch



Upon the public launch of its 2019 range (that took place on 7 January), Hornby made the surprise announcement of an all-new Stroudley Terrier 0-6-0T, which was in addition to the 10 new tooling projects included in our report published in last month's issue.

Details of the Terrier had, in fact, been held back from all representatives of the UK model railway press at the special briefing that took place in Margate on 11 December 2018. On the question of why Hornby had kept the release under wraps, Simon Kohler – the firm's Marketing Director and ever the showman – told the model railway press, "I like surprises, don't you?"

Six livery versions are being produced, each of which will be available either 'DCC ready' (RRP £89.99) or 'DCC fitted' (£109.99): R3767/X No.32655 in BR lined black with early emblems; R3768/X No.32636 in BR lined black with late crests, R3780/X No.655 Stepney in London, Brighton & South Coast Railway livery, R3780/X No.5 Rolvenden in Kent & East Sussex Light Railway blue livery, R3782/X No.751 in South Eastern & Chatham Railway fully-lined green livery and R3783/X No.2662 in Southern Railway green livery.

Decorated samples were on display at the public launch, with stocks of the two BR-liveried versions due to arrive with retailers in March. The other four versions will follow soon after – either late April or during May. See the manufacturer's website for more details:

[www.hornby.com](http://www.hornby.com)



# Bachmann announces 2019 ranges of new models for OO, N and 009



## Highlight report by Craig Tiley

Bachmann Europe Plc has published the latest editions of its catalogues for its Branchline (OO) and Graham Farish (N) ranges, which include details of several new tooling projects and numerous new livery versions of existing models. The manufacturer has also taken the opportunity to provide an update on a number of its previously-announced projects from new tooling that are in various stages of development.

As reported in our December 2018 issue, a new facility in Gaobu (China) has recently been fully commissioned with the manufacturing of products now underway.

2019 represents a year of further consolidation for the manufacturer (there are no new locomotives from new tooling for OO or N), as it continues to deliver a backlog of previously-announced models, whilst at the same time holding off from announcing new projects until development is at an advanced stage.

However, there is to be a programme of tooling upgrades across the Graham Farish range that will see several models become available with sound for the first time. This coincides with a change in the way models factory-fitted with sound will be described by the manufacturer (across both the OO and N gauge ranges) in the future; item numbers currently listed with a DS (Digital Sound) suffix are being phased out, to be superseded with an SF (Sound Fitted) suffix. This is to highlight the fact that future releases of Sound Fitted models will be suitable for use with both digital and analogue control systems, 12V dc users also benefitting from the sound effects (albeit played automatically and without the full controllability and customisation achievable with a DCC system).

### New diesel and wagons for 009

The manufacturer's growing range of narrow gauge models for 009 leads the way with the new tooling announcements. There will be a new locomotive in the shape of a Baguley-Drewery 70hp diesel locomotive (photo 1), based on examples built during the early 1980s for use on the network of narrow gauge lines operated by the RNAD (Royal Naval Armaments Depot). They worked trains carrying explosives and other sundries between standard gauge exchange sheds and the various hidden storage locations. As the MoD began to consolidate operations, various depots were closed, which led to a number of these locomotives entering preservation.

The Bachmann model will be presented in two liveries, both based on preserved examples; one in plain green and the other as RNAD Dean Hill DH88 in yellow with wasp stripes. The models will be supplied with six-pin DCC decoder sockets.

Alongside the locomotive releases will be a fleet of wagons from new tooling, all based on RNAD designs; a flat wagon, a flat wagon with planked ends, an open wagon (as rebuilt by the Statfold Barn Railway), an enclosed van (photo 2), an enclosed end brake van and an open-end brake van. A selection of RNAD, Statfold Barn Railway and Welsh Highland Railway liveries will be covered.

Three new versions of the Baldwin 10-12-D 4-6-0T are planned: There will be Snailbeach District Railways No.4 in weathered black livery, which will complement the forthcoming Snailbeach rolling stock items announced by Peco (see separate story); a further Welsh Highland Railway example will be presented as No.590 in lined maroon, which will also be the first of these models to be supplied



▲ New-build Patriot No.5551 The Unknown Warrior



▲ Preserved No.5 Bredgar. Photo: Andrew Burnham



▲ Ex-RNAD vans, Bredgar. Photo: Andrew Burnham



sound-fitted; and completing the trio will be a fictitious version finished as No.763 Sid in lined Southern Railway green livery (photo 3), which will appeal to modellers who have invested in the R-T-R Lynton & Barnstaple Railway models produced by other manufacturers.

### Branchline (OO)

The sole new tooling announcement for the OO range is a HKA bogie hopper wagon, for which decorated samples have been received (photo 4). Three versions are to be produced; two in weathered National Power blue and grey scheme (one de-branded), and the other in DB Schenker red. The real wagons can be defined as inners (without buffers) and outers (with buffers); therefore the Branchline models will be supplied with plug-in buffers in the accessory pack, allowing the models to be configured by the modeller as required.

Amongst the numerous new liveries utilising existing tooling will be the Wainwright C class 0-6-0 in the elaborate fully-lined South Eastern & Chatham Railway green livery as No.583 (photo 5). Other







highlights include SECR 60' Birdcage stock in Wellington Brown (photo 6) and BR Mk.IIf stock in Virgin Trains and Scotrail Saltire liveries.

The new-build LMS Patriot 4-6-0 No.5551 *The Unknown Warrior*, also features in the 2019 catalogue, which will be presented in the LMS lined crimson lake livery that will be worn by the full-size replica upon its completion (heading photo). The model will be available in two versions – with or without sound fitted. The tender will feature tooling modifications to cater for the 21st century modifications that will be evident on the full-size version.

The catalogue lists numerous new liveries/identities for diesel models that will each be available in two versions, either with or without sound fitted. For example: a Class 20/0 in BR blue and a Class 101 two-car DMU in BR green.

In addition there will be a trio of new Class 66 models, each available with or without sound; 66 413 in Freightliner G&W orange and brown scheme (photo 7), 66 789 *British Rail 1948-1997* in GBRf large logo BR blue and 66 783 *The Flying Dustman* in GBRf 'Biffa' red livery.

All three of the planned livery versions of the forthcoming Class 158 two-car DMU will now be available with or without sound fitted, as will the planned three-car Class 159 in Network SouthEast livery.

The GWR 57xx/'8750' 0-6-0PTs are amongst several models making a return to the range, including a version as No.L94 in London Transport lined maroon livery (photo 8). Other returnees include the Class 205 'Thumper' (previously a retailer exclusive), which is joining the manufacturer's Branchline range for the first time.



### Graham Farish (N)

New tooling projects will see a HKA bogie hopper released, mirroring the 4mm version. Two livery versions will be produced in N; National Power blue and grey, and DB Schenker red. Engineering prototypes have been received (photo 9).

The War Department 40T Parrot bogie wagon is to be produced in N, which follows the release of the OO version in 2018. Two versions have been announced; WD grey and LMS grey.



Models that will be making a return to the range include the LNER J39, Midland 4F, Class 42 Warship, Class 220 Voyager DEMU.

The next steam locomotive to benefit from a tooling upgrade for sound (joining the Maunsell N Class announced last year, see News, RM March 2018), will be the BR Standard 5MT 4-6-0. It will be equipped with a Next18 DCC decoder socket and factory-fitted speaker. Three livery versions have been announced, one of which will be supplied Sound Fitted (in BR lined green with late crests as No.73049).

The Class 14 and Class 60 diesel locomotives will also receive tooling upgrades for sound, which will see them equipped with Next18 DCC decoder sockets and factory-fitted speakers. One livery version for each will be available with or without sound fitted; D9522 in BR green with wasp stripes (photo 10). The class 101 and 150/2 DMU models are also to be made available with or without sound fitted – the former in BR blue and grey, the latter in Arriva Trains (Wales) scheme.



Amongst the notable new releases will be a pair of Class 66s; 66 413 in Freightliner G&W orange and brown scheme, together with 66 789 *British Rail 1948-1997* in GBRf large logo BR blue. Also joining the N gauge range will be a BR 'Highlander' coach pack (comprising a Mk.II TSO and ex-Class 101 DTCL) in green and cream livery, which mirrors the previously-released OO version, together with a 'West Highland' coach pack comprising a Mk.I TSO and Mk.I BSK in green and cream livery.

### Scenecraft

New to the range of 'ready-to-plant' resin structures will be a series of narrow gauge slate buildings and accessories, intended to complement the forthcoming Quarry Hunslet models. These comprise an engine shed (photo 11), water tower (photo 12), coal store, slate processing building, worker's cottage, footbridge, retaining walls, timber loco lift (photo 13), boiler house and chimney, tunnel portal and slate wagon loads.

There will be re-releases of several models, including (for OO) a four-road engine shed, diesel servicing point and single-road service shed, and a church in N.

### Progress with previously announced projects

For OO, engineering prototypes of the Midland Class 1532 (1P) 0-4-4T and Hawksworth 94xx 0-6-0PT are due to arrive before March 2019, whilst EPs for the Class 20/3 are scheduled to arrive during the second quarter of the year.

The accompanying table lists projected delivery dates for models that are due to arrive during the course of 2019.

The manufacturer also advises that its Research & Development department continues to work on a programme of previously announced projects,

11



12



13



together with at least 30 new projects across its product portfolio that are yet-to-be announced.

### Review of existing projects

During the preparation of its 2019 catalogues, Bachmann identified a number of previously-announced models for which it advises, "the current economic conditions do not favour further investment" and therefore the decision was taken to defer them from the range indefinitely. The items affected are; the N gauge NER Class E1 (LNER J72) 0-6-0T (all versions), N gauge DP1 Deltic diesel locomotive, OO gauge LNER Class J39 0-6-0 (DCC chassis upgrade – both versions) and the OO gauge Prestwin Twin Silo wagon (all versions).

### Latest catalogues

Copies of the Branchline and Graham Farish catalogues are available (priced £8.95 each) from Bachmann stockists.

[www.bachmann.co.uk](http://www.bachmann.co.uk)

### Current Branchline projects, OO gauge

| Item                                  | Due |
|---------------------------------------|-----|
| Western Pullman train set             | Q1  |
| 45T Ransomes & Rapier breakdown crane | Q3  |
| Class 24 diesel                       | Q4  |
| Class 90 electric                     | Q1  |
| Class 117 DMU                         | Q4  |
| Class 121 single car DMU              | Q4  |
| Class 158 DMU                         | Q4  |
| Class 159 DMU                         | Q4  |
| Class 414 (2-HAP) EMU                 | Q4  |
| North Eastern E1 (LNER J72) 0-6-0T    | Q2  |
| OO9 – slate wagons                    | Q2  |
| Parrot wagons                         | Q2  |
| ROD 2-8-0                             | Q2  |
| Warflat wagons                        | Q2  |

### Current Graham Farish projects, N gauge

| Item                         | Due |
|------------------------------|-----|
| Western Pullman six-car DEMU | Q1  |
| BR Mk.IIf coaches            | Q3  |
| Class 70/8 diesel            | Q3  |
| Class 8F 2-8-0               | Q4  |
| LNER Thompson coaches        | Q4  |
| SECR birdcage coaches        | Q1  |
| SECR C Class 0-6-0           | Q2  |
| TEA bogie tank wagons        | Q2  |



## Bookings now being taken for 2019 modelling courses at Pecorama

Pecorama, located in the picturesque coastal fishing village of Beer in Devon, is offering more of its popular two-day modelling courses in 2019.

Intended as a practical introduction to the core aspects of railway modelling, with expert tuition from the in-house Pecorama modellers, there will be three courses covering track laying and layout electronics (2 & 3 April, 4 & 5 June and 1 & 2 October) plus four focusing on scenery (7 & 8 May, 14 & 15 May, 17 & 18 September and 24 & 25 September – note all dates are Tuesdays and Wednesdays). Over the duration of the two days students will be guided through the construction of their own section of baseboard, which can be taken away at the end.

The courses include morning tea/coffee and lunch on both days, a 10% discount voucher to spend in the well-stocked Pecorama shop and ticket to access the Pecorama attractions, including train ride on the Beer Heights Light Railway.



The cost for the course is £215.00. Bookings can be secured with a 50% deposit (with the balance to be paid no later than 14 days prior to the course date). Full details are available on the Pecorama website:

**Pecorama**  
Beer, Nr. Seaton, Devon EX12 3NA  
Tel: 01297 21542  
email: [info@peco.co](mailto:info@peco.co)  
[www.pecorama.co.uk](http://www.pecorama.co.uk)

## The Andeer line returns to Pecorama

Following the success of the 'mini-exhibition' of model railways held in the Lecture Theatre/Station Gallery as an additional attraction for visitors to Pecorama last July, on Saturday 23 and Sunday 24 February 2019 there will be an operating session with The Andeer line, a Swiss HOm modular layout built by Julia & Derek Boswell.

Running 1000 – 1700 both days, visitors will be invited to help operate the layout, which employs digital con-

trol, with a central 'signal box' and trains driven individually using smart 'phones and tablets.

On request, there will also be practical demonstrations of the layout's light-weight foam baseboard construction, and the use of electrostatic grass.

Admission is included within the normal Pecorama ticket price (see website for details) and visitors can drop in for as long as they wish.

[www.pecorama.co.uk](http://www.pecorama.co.uk)



## Frizinghall N gauge wagon commissions



Frizinghall Models & Railways has commissioned from Peco a series of exclusive seven-plank open wagons in the livery of Lister & Co. Ltd of Manningham Mills, Bradford. Four are available; two in pristine finish and two weathered, each with different running numbers. Priced £9.99 each, they are available direct from the retailer:

[www.modelrailshop.co.uk](http://www.modelrailshop.co.uk)

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## LSWR Drummond K10 4-4-0 next from OO Works

Roderick Bruce has informed us that the subject of his next ready-to-run locomotive release will be the London South Western Railway's Drummond K10 4-4-0, which is due for release in late 2019. The prototypes numbered 40 in total, built between 1901 and 1902; 31 survived nationalisation, with the last example withdrawn in July 1951.

Three versions are being produced; No.347 in Southern Railway olive green livery (£295.00), No.389 in Southern Railway black livery with Bulleid 'sunshine' lettering (£285.00) and No.30382 in BR black livery with early emblems (£285.00). All will have the six-wheel tender. Prices do not include P&P. For more information or to place an order:

**OO Works**  
Tel: 01580 882 185  
email: [ooworks@btinternet.com](mailto:ooworks@btinternet.com)  
[www.ooworks.co.uk](http://www.ooworks.co.uk)



## Roger Christian

It is with great sadness that we record the passing of Roger Christian, who was widely known across the railway modelling world and in particular the OO9 Society, of which he was a lifetime member.

Born in Liverpool in 1945, Roger's grandparents on his father's side were from the Isle of Man and he retained strong ties to the island. He originally worked as a draughtsman for Cammell Laird shipbuilders before training as a teacher. He taught woodwork and technical drawing in a Liverpool school before realising his vocation and retraining to teach special needs children. Roger retired in 2006 following a period of ill health.

During his retirement he put a lot of energy into model railways and in particular OO9 where he was very active within the society. Many will recall his superb layouts including *Tan-yr-Allt*, *Crummack Valley*, *Dwyffr Uchaf*, and more recently *Prescott Aggregates* as well as his highly detailed locomotive and rolling stock models.

Roger was always willing to share his expertise with other modellers and would happily demonstrate the techniques he used in modelling to anyone who asked. He was also generous towards younger model railway fans, particularly at exhibitions where children would often be invited to operate his layouts.

Roger always maintained a positive outlook on life. He continued to attend the Oldham and Merseyside & SW Lancs OO9 Society groups where his chirpy personality was an inspiration to all.

Roger will be sorely missed in the model world, both in OO9 and other scales. He leaves behind his partner Linda.

Ed – Our thanks to Gary Whiting with his help preparing this obituary.



## Model Rail Scotland

### Free Sunday admission for kids at Model Rail Scotland 2019

This year's Model Rail Scotland event, staged by the Association of Model Railway Societies in Scotland, will be taking place on Friday 22, Saturday 23 and Sunday 24 February 2019, at the Scottish Event Campus in Glasgow (postcode G3 8YW).

The three-day exhibition – which is supported by Peco Publications Ltd, Hornby Hobbies Ltd and Bachmann Europe Plc – will bring together in excess of 50 layouts in a variety of scales and gauges. Amongst these will be *Calcutta Sidings* (pictured) – the extensive P4 gauge BR sectorisation era layout by Phil Eames that features as this month's 'Railway of the Month' (see p178).

There will also be two layouts from Channel 5's *The Great Model Railway Challenge* in action; heat-winning layout *Clucas Bay*, and winner of the grand final *City of Tiers*

(see p220), both by members of Aberdeen Model Railway Club.

Complementing the layouts will be 150 exhibitor stands including specialist trade support, societies and modelling demonstrations.

Details of admission prices can be found via the website, where a PDF voucher can be downloaded and printed to give accompanied children (aged 5 – 16) free admission on the Sunday of the show.

It has been confirmed that a free vintage bus service will run from George Square (Queen Street) to Riverside Museum (and return), stopping en route at the event venue and Central Station. See the event website for more details and timetable.

For further details about the show, including a full list of layouts and opening times:

[www.modelrail-scotland.co.uk](http://www.modelrail-scotland.co.uk)



### Accurascale receives decorated samples for PCV Cemflo and PCA cement wagons in OO

Accurascale has received decorated samples from its factory in China for the different livery versions of its forthcoming PCV/Cemflo and PCA wagon models, all of which are due for delivery during the first quarter of 2019.

The PCV/Cemflo wagon is being produced in two livery variants (pre-TOPS and TOPS), with models available individually (£24.95 each) and in triple packs (priced £69.95 each).

The PCA bulk cement wagon is being released in three livery schemes; Castle Cement, Rugby Cement and STS. There are five packs available for each livery

scheme, with each pack comprising three wagons. These triple packs are also priced £69.95 each.

The models can be purchased direct only via the Accurascale website; for more information and to pre-order:

[www.accurascale.co.uk](http://www.accurascale.co.uk)



### New wireless controller from blueRailways



The Model 602 Wireless Receiver/Controller from blueRailways is a new single track analogue dc controller containing a low-power wireless interface. It can be used as a conventional hand-held controller or it can be controlled wirelessly – using either the Model 720 Wireless Controller or an Android smartphone/tablet in conjunction with the blueRailways app (available free from the Google Play Store).

The Model 602 (which supersedes the firm's Model 601), has a 1A output, incorporates electronic short-circuit protection and has an operating range of up to 50m.

The track output can also be programmed, via the app, with the same common control variables (CVs) available on DCC; Start Value, Start Boost, Acceleration, Deceleration and Maximum Speed. These CVs are stored and retained in the 602 when the power is removed.

Locomotive models do not require any modifications for use with the Model 602 Wireless Receiver/Controller, which is available direct from blueRailways priced at £49.50 (or £89.50 when bought with a Model 720 Wireless Controller). For further details:

[www.bluerailways.co.uk](http://www.bluerailways.co.uk)

### Motor and gearboxes for O gauge from MSC Models



MSC Models (part of the Tolworth O Gauge Group) has introduced a smooth-running five-pole skew-wound motor of similar power to the now-discontinued Mashima 1833 motor. The new SM 1532 motor is available with machined single reduction gearboxes for 3/16" axles in 25:1, 30:1 and 40:1 ratios. Priced £50.00 each, the motor has a running current load of 470 mA at 8,000rpm. No load speed is 10,000rpm.

Dimensions are 15mm wide and 32mm long with shafts at each end. Flywheels are also available.

**MSC Models**  
48a Ditton Hill Road, Long Ditton,  
Surrey KT6 5JD.  
Tel: 020 8398 2415  
[www.mscmodels.co.uk](http://www.mscmodels.co.uk)

## In brief

### 25th NG North

The 25th Narrow Gauge North event will be taking place on Saturday 9 March 2019 at the Pudsey Civic Hall, Dawsons Corner, Pudsey, Leeds (postcode LS28 5TA). There will be 20 working narrow gauge layouts in various scales and gauges, together with a range of specialist society and trade stands. Opening times are 1000 – 1630. For more details including admission prices, see Societies & Clubs or go to:

[www.narrowgaugenorth.org.uk](http://www.narrowgaugenorth.org.uk)

### Midlands Garden Rail Show 2019

2019 marks the 18th year of The Midlands Garden Rail Show, which will be taking place on the weekend of 16 & 17 March at the Warwickshire Event Centre. There will be a number of layouts representing scales from O gauge upwards (including live steam), together with numerous specialist suppliers. For further details see Societies & Clubs or go to:

[www.largerscalemodelrail.co.uk](http://www.largerscalemodelrail.co.uk)

### EMGS skills day

The EM Gauge Society will be presenting the Sixth Chris Kedgley Memorial Skills Day on Saturday 9 March 2019 (1000 – 1630) at the Low Port Centre, 1 Blackness Road, Linlithgow, West Lothian EH49 7HZ. 12 modellers will be providing practical demonstrations of a wide range of skills. A small number of layouts will also be appearing. Admission is free but donations are encouraged.

[www.emgs.org](http://www.emgs.org)

### GT3 in OO planned

Keith Revell has launched a crowd-funding project to develop a OO gauge ready-to-run model of GT3 – the experimental gas turbine locomotive constructed by English Electric in 1958. Keith advises that a manufacturer has been commissioned and, providing a sufficient level of interest is forthcoming, delivery is planned for December 2019. Firm expressions of interest can be placed via the website, where further information about the project can also be found:

email: [keith@krmodels.co.uk](mailto:keith@krmodels.co.uk)  
[www.krmodels.co.uk](http://www.krmodels.co.uk)

### Northbridge NEC competition

Congratulations to Robin Sharman of Nottinghamshire, who spotted correctly the three cameos on the Warley club's *Northbridge* layout at the NEC (RM December, p988). The correct answers were 'sheds for sale', 'five carriages', and 'keep out private property'. Thanks to everyone who entered the draw.



# Coming next month

April 2019 –  
Out Thursday 14 March

**Arun Quay**  
A small-space scenic  
masterclass in O gauge

**Railway  
of the  
Month**

**Beckindale**  
A Yorkshire terminus in OO

**Merstone**  
An Isle of Wight junction station in OO

## inside ....

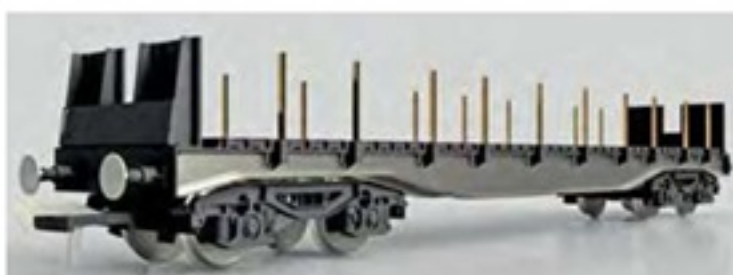
- **COTTAGE MODELLING IN N**  
Creating your own 'kits' using graphics programs and ingenuity
- **HEBBLE VALE GOODS**  
A might-have been freight depot in the BR blue era, in 4mm scale
- **BUTLEY QUAY**  
A Suffolk narrow gauge 'just supposing', modelled in 009

plus all the  
regular features ....

## Design work complete for BBA in OO from Cavalex Models

Cavalex Models – a recent entrant into the British OO ready-to-run arena – has completed the design work for its second 4mm scale project – the BR-built 50' BBA bogie steel wagon. The project is illustrated here with sample CAD renders.

The firm is also planning to produce an N gauge version of its debut OO project – the PGA hopper – see review on p250. Design work for the 2mm version is already complete and liveries have also been confirmed.



Pre-orders for both models can be placed via the Cavalex Models website until 28 February 2019. For more details:

[www.cavalexmodels.com](http://www.cavalexmodels.com)

## Download and print retaining wall kits from 3dk

Just released from 3dk Model Railway Downloadable Building Kits are two new 4mm products. The first release is a brick retaining wall kit (with red brick finish), which comprises four brick insert arches. Once the kit (ref. 00-28, price £3.99) is downloaded, it can be printed out multiple times, enabling a retaining wall of any length to be created. Measuring

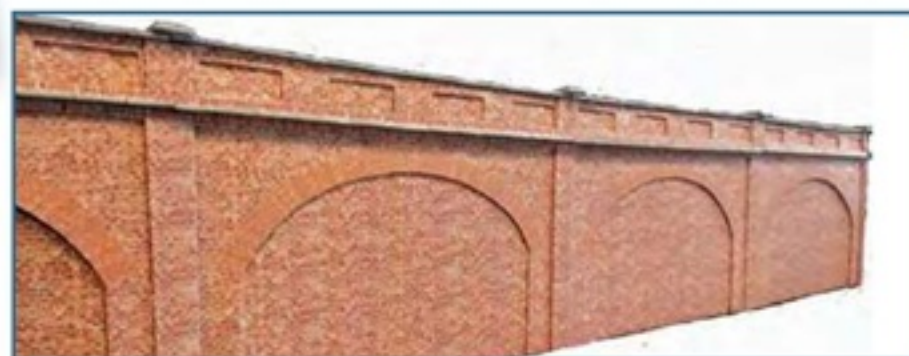
720mm (length) x 130mm (height), the kit comes with a variety of period adverts and posters.

The second release for Industrial Workshops (ref.00-27, price £2.99), which are designed to be substituted for the brick panels in the retaining wall kit. There are 14 different industrial workshops to choose from in the kit, with a variety of finishes includ-

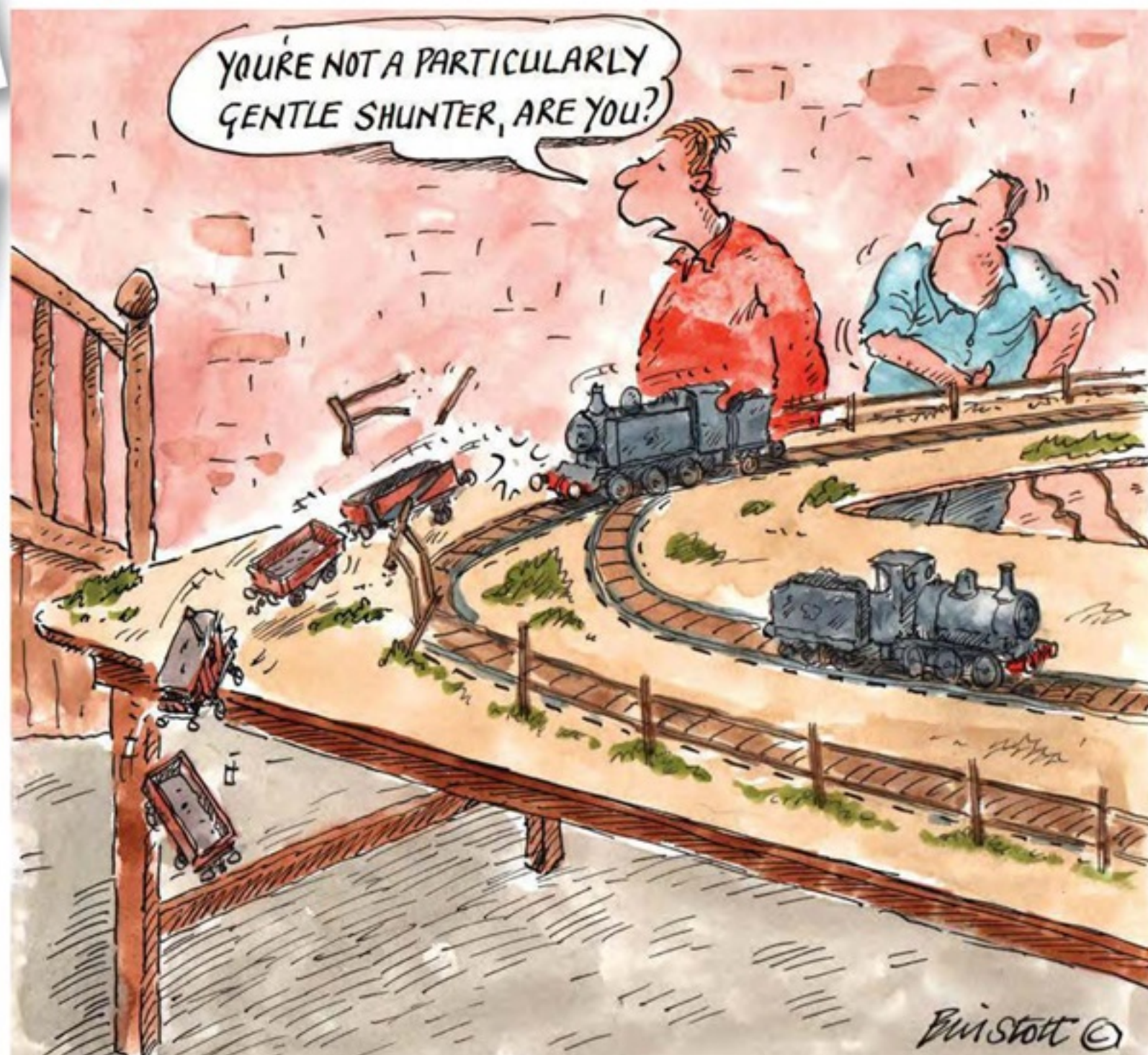
ing stone, corrugated iron and concrete. A variety of period adverts and posters are also included. Dimensions are: 150mm (length) x 90mm (height).

For more details about these latest releases and the rest of the manufacturer's range, visit the 3dk website:

[www.3dk.ca](http://www.3dk.ca)



## Laughter Lines...





# Societies & Clubs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should contact the organiser first. Entries include the organiser's town and telephone number wherever possible. The publishers cannot accept any responsibility for cancellations or errors. RAILWAY MODELLER is published on the second Thursday of the preceding month, and copy by post or via email to [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com) is required six weeks prior to publication date.

**May issue deadline: March 1st**

## EVENTS

### Friday 15 & Saturday 16 February SOUTH SHIELDS, Tyne & Wear

**Organiser:** The Salvation Army.  
**Venue:** The Salvation Army Community and Worship Centre, Wawn Street, South Shields, Tyne & Wear NE33 4EB.  
**Open:** 1300 – 2000 Friday  
1000 – 1700 Saturday  
**Admission:** adults £4.00, children £2.00, family (2+2) £10.00.  
**Features:** Model Rail 2019.  
**Contact:** Major Amanda Banner 0191 496 7511  
[www.salvationarmy.org.uk](http://www.salvationarmy.org.uk)

### Saturday 16 February HILLINGDON, Greater London

**Organiser:** Hillingdon Railway Modellers.  
**Venue:** Yiewsley Baptist Church Hall (side entrance), 74 Colham Avenue, Yiewsley, Hillingdon UB7 8HF.  
**Open:** 1000 – 1600  
**Admission:** £4.00.  
**Amenities:** refreshments will be available all day. Nearest station West Drayton. Local buses 222, 350, U1, U3 & U5.  
**Features:** Test Track Open Day – not an exhibition but a chance to meet fellow modellers and try out your O, OO or N gauge models on the club's three test tracks. 12V dc and DCC is available, but no live steam, please. Expert help is on hand from experienced modellers in case of any problems with your models, and new members are very much welcome.  
**Contact:** Peter Storey 020 8368 4090  
[www.hillingdonrailwaymodellers.co.uk](http://www.hillingdonrailwaymodellers.co.uk)

### Saturday 16 February LANCING, West Sussex

**Organiser:** Sussex Downs Group of the OO9 Society.  
**Venue:** Lancing Parish Hall, South Street, Lancing, West Sussex BN15 8AJ.  
**Open:** 1000 – 1630.  
**Admission:** adults £5.00, accompanied under 16s free.  
**Amenities:** parking, wheelchair access, light refreshments. Venue is five minutes from A27, 10 minutes walk from Lancing railway station.  
**Features:** open day. OO9 and 7mm layouts, OO9 Society sales, trade support.  
**Contact:** [contents@oo9society.com](mailto:contents@oo9society.com)

### Saturday 16 February TONBRIDGE, Kent

**Organiser:** Tonbridge MRC.  
**Venue:** The Angel Centre, Angel Lane, Tonbridge, Kent TN9 1SF.  
**Open:** 1000 – 1700.  
**Admission:** adults £6.00, children (10 – 16) £1.00, children under 10 free.  
**Amenities:** wheelchair accessible, light refreshments. Venue is five minutes walk from Tonbridge railway station.  
**Features:** 30 layouts in all scales, extensive trade support and demonstrators.  
**Contact:** Roy Prentice 01892 527199  
[www.tonbridgemrc.com](http://www.tonbridgemrc.com)

### Saturday 16 & Sunday 17 February HIGHBRIDGE, Somerset

**Organiser:** Sedgemoor O Gauge Group of the Gauge O Guild (Sedggog).  
**Venue:** King Alfred School, Highbridge, Somerset TA9 3EE.  
**Open:** 1030 – 1700 Saturday  
1000 – 1630 Sunday.  
**Admission:** adults £4.00, teenagers £3.00, family (2+2) £10.00.  
**Amenities:** refreshments, ample free parking, disabled access and facilities.  
**Features:** visiting O gauge layouts, demonstrations, trade support.  
**Contact:** D J Bowers 07808 272218

### Saturday 16 & Sunday 17 February MILTON KEYNES

**Organiser:** Milton Keynes MRS.  
**Venue:** Stantonbury Leisure Centre, Milton Keynes MK14 6BN.  
**Open:** 1000 – 1700 Saturday  
1000 – 1630 Sunday.  
**Admission:** adults £7.00 (£6.50 with valid rail ticket), children £1.00. Cash only on the days; pay by card via website.  
**Amenities:** hot & cold refreshments, free parking and level access throughout. Free Routemaster bus every 30 minutes from Milton Keynes Central station.  
**Features:** 50th anniversary exhibition: 40+ layouts plus trade support, society stands, miniature ride-on railway.  
**Contact:** [www.mkMrs.org.uk/exhibitions](http://www.mkMrs.org.uk/exhibitions)

### Saturday 16 & Sunday 17 February SILEBY, Leicestershire

**Organiser:** Syston MRS.  
**Venue:** Sileby Community Centre, High Street, Sileby, Leicestershire LE12 7RX.  
**Open:** 1030 – 1630 Saturday  
1000 – 1600 Sunday.  
**Admission:** adults £4.00, unaccompanied children £3.50, accompanied children free, family (2+2) £8.00. Donation made to charity every year.  
**Amenities:** ample parking nearby, food, refreshments.  
**Features:** layouts, trade, tombola.  
**Contact:** Chris Leach 0116 260 5760  
[www.systonMrs.org.uk](http://www.systonMrs.org.uk)

### Sunday 17 February HUBBERTS BRIDGE, Lincolnshire

**Organiser:** Glyn Halgarth.  
**Venue:** Hubberts Bridge Community Centre, Langrick Road, Hubberts Bridge, Nr Boston, Lincolnshire PE20 3SG.  
**Open:** 1000 – 1600.  
**Admission:** adults £3.00, children under 16 free.  
**Contact:** [glynhalgarth@tiscali.co.uk](mailto:glynhalgarth@tiscali.co.uk)

### Sunday 17 February MONMOUTH

**Organiser:** Monmouth Rotary Club.  
**Venue:** Bridges Community Centre, Monmouth NP25 5AS.  
**Open:** 1000 – 1600.  
**Admission:** adults £5.00, seniors & children £4.00, family (2+2) £12.00.  
**Amenities:** free parking, disabled access, refreshments.  
**Features:** annual exhibition – over 25

exhibits and demonstrations.  
**Contact:** [david\\_forbes2@sky.com](mailto:david_forbes2@sky.com)

### Sunday 17 February OLDBURY, West Midlands

**Organiser:** Warley MRC.  
**Venue:** Warley MRC Clubrooms, Unit 1f, Pearsall Drive, Oldbury B69 2RA.  
**Open:** 1100 – 1600.  
**Admission:** free, donations gladly accepted.  
**Features:** see the work of members, and an update on progress on a number of club projects and layouts, together with a bring & buy stand.  
**Contact:** [www.warley-mrc.org.uk](http://www.warley-mrc.org.uk)

### Friday 22 to Sunday 24 February GLASGOW

**Organiser:** Association of Model Railway Societies in Scotland.  
**Venue:** SEC, Glasgow.  
**Open:** 1030 – 1800 Friday  
1000 – 1800 Saturday  
1030 – 1700 Sunday.  
**Admission:** advance tickets – adults £12.00, children £5.00, family (2+2) £28.00. Advance tickets allow admission 30 minutes in advance of opening times and can be purchased via the event website or by post – to arrive no later than 15 February – from AMRSS, P O Box 9117, Shotts ML7 9AF. Please enclose an SSAE and make cheques/postal orders payable to AMRSS. On the door – adults £13.00, children £6.00, family (2+2) £29.00.  
**Features:** Model Rail Scotland 2019 – 50+ layouts, extensive trade support including RAILWAY MODELLER. Society stands and demonstrations. Free exhibition guide.  
**Contact:** [www.modelrail-scotland.co.uk](http://www.modelrail-scotland.co.uk)

### Saturday 23 February BRIGHTON, Brighton & Hove

**Organiser:** Sussex Vintage Model Railway Collectors.  
**Venue:** The Knoyle and Crowhurst Halls, Knoyle Road, Preston Park BN1 6RB.  
**Open:** 1000 – 1630.  
**Admission:** adults £4.00, children under 16 free (must be accompanied).  
**Amenities:** 'Buffet Car' refreshments. Wheelchair access throughout plus disabled toilet. Venue is just off the A23 close to main bus routes and Preston Park station. Unrestricted two-hour kerbside parking in the vicinity.  
**Features:** extensive working layouts of classic toy and model trains, including Hornby Trains, Bassett-Lowke and modern vintage-style O gauge, Meccano and much more. Trade support.  
**Contact:** 07785 567251  
[www.sussex-transport.co.uk/svmrc/](http://www.sussex-transport.co.uk/svmrc/)

### Saturday 23 February DEREHAM, Norfolk

**Organiser:** Rosemary Cooper.  
**Venue:** Memorial Hall, Norwich Street, Dereham, Norfolk NR19 1AD.  
**Open:** 1000 – 1600.  
**Admission:** adults £5.00, accompanied children free.  
**Amenities:** refreshments available, Mid-Norfolk trains running (see website).  
**Features:** layouts (some on display at MNR station), trade, societies, groups and demonstrations.  
**Contact:** [derehamexhibition@gmail.com](mailto:derehamexhibition@gmail.com)  
[www.mnr.org.uk](http://www.mnr.org.uk)

### Saturday 23 February PRINCES RISBOROUGH, Buckinghamshire

**Organiser:** Risborough & District MRC.  
**Venue:** Community Centre (adjacent to Springs swimming pool), Wades Park, Stratton Road, Princes Risborough, Buckinghamshire HP27 9AX.

**Open:** 1000 – 1700.  
**Admission:** adults £4.00, accompanied children £2.00.  
**Amenities:** light refreshments, including lots of homemade cakes.  
**Features:** RISEX 2019 offers nine layouts plus traders and modelling demonstrations.  
**Contact:** Adrian Harford 01296 415107  
[www.rdmrc.org.uk](http://www.rdmrc.org.uk)

### Saturday 23 February READING, Berkshire

**Organiser:** Kenavon Railway Society.  
**Venue:** The Warehouse, 1a Cumberland Road, Reading, Berkshire RG1 3LB.  
**Open:** 1000 – 1630.  
**Admission:** adults £4.00, seniors £2.50, children £2.00, family (2+2) £9.50. In support of the Cholsey & Wallingford Railway.  
**Amenities:** disabled access, refreshments, limited free parking. Venue is behind Mr Cod on Reading bus routes 4, X4, 13 & 15; Courtney 126, 127 & 128; Arriva 850.  
**Features:** approx 18 layouts, trade stands.  
**Contact:** Richard Standing 07760 285383

### Saturday 23 February SAFFRON WALDEN, Essex

**Organiser:** Saffron Walden MRC.  
**Venue:** United Reformed Church and Salvation Army Halls, Abbey Lane, Saffron Walden, Essex CB10 1AG.  
**Open:** 1000 – 1630.  
**Admission:** adults £3.00, concessions £2.50, children (5 – 16) £1.00. Disabled and carers free.  
**Amenities:** refreshments, disabled access to all areas. Parking (fees payable) less than 300m away in Swan Meadow car park (CB10 1BX), show venue will be signposted.  
**Features:** layouts, trade support.  
**Contact:** Julian Nettle 01799 521029  
[julian.nettle@btopenworld.com](mailto:julian.nettle@btopenworld.com)

### Saturday 23 February TAUNTON, Somerset

**Organiser:** Exmoor Coast Railway Modellers.  
**Venue:** Richard Huish College, South Road, Taunton, Somerset TA1 3DZ. Just south of the town centre on the B3170 and five minutes from Jc25 M5.  
**Open:** 1000 – 1500  
**Admission:** £2.00.  
**Amenities:** extensive free on-site parking, refreshments, disabled access.  
**Features:** Exmoor Train & Toy Fair. Model railways, diecast vehicles, books/DVDs, toys etc. Great Western Society stand.  
**Contact:** David Nelhams 01643 706736

### Saturday 23 & Sunday 24 February BEER, Seaton, Devon

**Organiser:** Pecorama.  
**Venue:** Lecture Theatre/Station Gallery, Pecorama, Beer, Seaton EX12 3NA.  
**Open:** 1000 – 1700 both days.  
**Admission:** free (within usual Pecorama ticket price).  
**Amenities:** free parking. The venue is wheelchair accessible, but note that the site is hilly and some paths cannot be negotiated by wheelchair users unaided. Garden Room licensed restaurant. Note the Beer Heights Light Railway is not running this early in the season. Exhibition and shop open (shop only Sunday).  
**Features:** running session with *The Andeer Line*, Swiss HOm modular layout, digitally controlled with locos driven by smart 'phone and tablet – visitors will be encouraged to participate in its operation. On request, practical demonstrations of electrostatic grass and lightweight foam baseboard construction.  
**Contact:** [www.pecorama.co.uk](http://www.pecorama.co.uk)



## **Saturday 23 & Sunday 24 February** **CHAPEL-EN-LE-FRITH, Derbyshire**

**Organiser:** New Mills & District Railway Modellers.

**Venue:** Chapel-en-le-Frith Leisure Centre, Long Lane, Chapel-en-le-Frith, Derbyshire SK23 0TQ.

**Open:** 1000 – 1700 both days.

**Admission:** adults £6.00, concessions £5.00, family (2+2) £15.00.

**Amenities:** free parking, wheelchair friendly, refreshments.

**Features:** 16+ layouts, 20 traders and demonstrations/displays.

**Contact:** www.nmdrm.co.uk

## **Saturday 23 & Sunday 24 February** **NEWARK, Nottinghamshire**

**Organiser:** Lincoln & District MRC.

**Venue:** Lady Eastwood Pavilion, Newark Show Ground, Newark NG24 2NY.

**Open:** 1000 – 1700 Saturday

1000 – 1630 Sunday.

**Admission:** adults £7.00, children £3.00, family (2+up to 3) £17.00.

**Amenities:** early entry (0930) for disabled and wheelchair visitors only.

**Contact:**

https://ladmrc.wixsite.com/ladmrc

## **Saturday 23 & Sunday 24 February** **TENTERDEN, Kent**

**Organiser:** Ashford MRC.

**Venue:** Homewood School, Ashford Road, Tenterden, Kent TN30 6LT.

**Open:** 1000 – 1700 Saturday

1000 – 1600 Sunday.

**Admission:** adults £4.50, seniors £4.00, children £3.00, family (2+2) £12.00.

**Amenities:** refreshments, disabled access and free on-site parking.

**Features:** 18+ layouts and trade support.

**Contact:** 01233 733416

www.ashfordmrc.com

## **Saturday 23 & Sunday 24 February** **YORK**

**Organiser:** EBOR Group of Railway Modellers.

**Venue:** Heworth Memorial Church Hall, Melrosegate, York YO31 0RP.

**Open:** 1000 – 1700 Saturday

1000 – 1630 Sunday.

**Admission:** adults £3.00, children £1.00, under 5s free.

**Amenities:** refreshments, but regret no disabled facilities at venue.

**Features:** York Against Cancer Model Railway Show. Layouts including children's interactive layout, demonstrations, trade and society stands. Sponsored by Monk Bar Model Shop.

**Contact:**

Jim Whitehead 01904 630492 (after 1900)  
https://eborgroup.wordpress.com/

## **Sunday 24 February** **TROWELL, Nottinghamshire**

**Organiser:** Ilkeston (Woodside) MRC.

**Venue:** Trowell Parish Hall, Stapleford Road, Trowell, NG9 3QA.

**Open:** 1000 – 1630.

**Admission:** adults £4.00, accompanied under 14s free.

**Amenities:** venue is just off A609, sign-posted from Ilkeston and Jc25 M1. Disabled access to all but the small stage area. Café.

**Features:** layouts in all major scales plus self-drive Thomas layout with guest appearance from the Fat Controller. Trade support, club stall.

**Contact:** 07930 909571

## **MARCH**

### **Saturday 2 March** **BECCLES, Suffolk**

**Organiser:** Norfolk & Suffolk Narrow Gauge Modellers.

**Venue:** Blyburgate Hall (formerly St John Ambulance Hall), Blyburgate, Beccles, Suffolk NR34 9TF.

**Open:** 1000 – 1600.

**Admission:** adults £3.00, accompanied under 16s free.

**Amenities:** light refreshments. Regret no access to two of the show's four rooms for wheelchair users. Parking for disabled only at hall, signposted parking nearby.

**Features:** layouts include *Bottle Kiln Lane* (OO9), *Chwarel Tegid* (OO9), *Coleford* (OO9), *Grange West Tramway* (O-16.5), *Little Ease* (1:16), *Llanfair Jamieinion* (OO9), *Nibley Knoll* (OO9), *Ruritania Eisenbahn* (O-16.5), *Thurley* (OO9), *Wensleydale* (O-16.5), *Woolthorpe Light Railway* (OO9). Plus running lines in 3½" and SM32. Trade support, modelling demonstration, society stands.

**Contact:** http://nsgnm.org.uk

### **Saturday 2 March** **COMPTON DUNDON,** **Nr Street, Somerset**

**Organiser:** Glastonbury & Street Lions Club.

**Venue:** Meadway Hall, Ham Lane, Compton Dundon, Nr Street TA11 6PQ.

**Open:** 1030 – 1630.

**Admission:** adults £5.00, children (5 – 16) £1.00, family (2+2) £11.00.

**Amenities:** refreshments. Venue is sign-posted off the B3151 between Street and Somerton.

**Features:** approx 22 layouts, trade support, demonstrations.

**Contact:** Howard Martin

01458 830542 (evenings only)

### **Saturday 2 March** **HARROGATE, North Yorkshire**

**Organiser:** Harrogate MRG.

**Venue:** Bilton Grange United Reformed Church, corner of Skipton Road/Woodfield Road, Harrogate HG1 4LP.

**Open:** 1000 – 1600.

**Admission:** adults £4.00, children £2.00, family (2+2) £10.00.

**Amenities:** refreshments available.

**Features:** club and members' layouts, sales.

**Contact:** 07710 312509

### **Saturday 2 March** **MARCH, Cambridgeshire**

**Organiser:** March & District MRC.

**Venue:** Westwood Community Junior And Infant School, Maple Grove, March, Cambridgeshire PE15 8JT.

**Open:** 1000 – 1630.

**Admission:** adults £4.00, children £2.00, family (2+2) £10.00.

**Amenities:** refreshments, ample free parking.

**Features:** 15 layouts in various gauges, trade support, demonstrations, club stand, external steam traction rides.

**Contact:** www.mdmrc.co.uk

### **Saturday 2 March** **SOMPTING, West Sussex**

**Organiser:** West Sussex Area Group of the N Gauge Society.

**Venue:** Sompting Village Hall, West Street, Sompting BN15 0BE.

**Open:** 1000 – 1600.

**Admission:** adults £1.00, children 50p.

**Amenities:** parking, refreshments, disabled access.

**Features:** open day – club layouts on show plus demonstrations and club second-hand shop.

**Contact:** 01403 266689

www.wsng.co.uk

### **Saturday 2 & Sunday 3 March** **BIRTLEY, Tyne & Wear**

**Organiser:** Birtley Railway Engineers.

**Venue:** Community Centre, Ravensworth Road, Birtley, Tyne & Wear DH3 1EN.

**Open:** 1000 – 1600 both days.

**Admission:** adults £4.00, accompanied children under 16 free.

**Amenities:** refreshments, limited disabled access via stair lift.

**Features:** 10 layouts, trade support.

**Contact:** 0191 487 1110

### **Saturday 2 & Sunday 3 March** **MANSFIELD, Nottinghamshire**

**Organiser:** Mansfield St Peter's MRG.

**Venue:** St Peter's Centre Church Side, Mansfield, Nottinghamshire NG18 1AP.

**Open:** 1000 – 1700 Saturday

1000 – 1600 Sunday.

**Admission:** adults £4.00, seniors £3.50, children under 16 £1.00, under 5s free. All proceeds in aid of St Peter's Church and local Scouts.

**Amenities:** food, cakes and refreshments available.

**Features:** 14 layouts in the major scales, plus traders and demonstration stands.

**Contact:** 01623 469068

www.mansfieldmodelrailway.co.uk

### **Saturday 2 & Sunday 3 March** **PRESTON, Lancashire**

**Organiser:** Preston & District MRS.

**Venue:** Sports Hall, Preston College, Fulwood Campus, St Vincent's Road, Fulwood, Preston, Lancashire PR2 8UR.

**Open:** 1000 – 1700 Saturday

1000 – 1600 Sunday.

**Admission:** adults £7.00, children and wheelchair users £4.00, family (2+2) £18.00. Free child ticket available from website.

**Amenities:** refreshments, ample free parking, easy access.

**Features:** layouts in various scales including *Dettingen*, *Second Hand Yard*, *Two Seasons*. Full trade support.

**Contact:** 07776 438933

www.prestonanddistrictmrs.org.uk

### **Saturday 2 & Sunday 3 March** **RAINHILL, Merseyside**

**Organiser:** Rainhill MRC.

**Venue:** Rainhill Village Hall, Weaver Avenue, Rainhill, Merseyside L35 4LU.

**Open:** 1000 – 1700 Saturday

1000 – 1630 Sunday.

**Admission:** adults £5.00, children £3.00, family (2+2) £13.00.

**Amenities:** full disabled access to all areas.

**Features:** Model Rail 2019.

**Contact:** Brian Davis 01925 226266

www.rainhillmrc.org

### **Saturday 2 & Sunday 3 March** **ROMSEY, Hampshire**

**Organiser:** Romsey & District Railway Modellers Society.

**Venue:** Crosfield Hall, Broadwater Road, Romsey, Hampshire SO51 8GL.

**Open:** 1030 – 1700 Saturday

1000 – 1630 Sunday.

**Admission:** adults £5.00, children £2.00, family (2+2) £10.00.

**Amenities:** light refreshments, fully accessible, parking (free on Sunday).

**Features:** 17+ layouts including *Castlederg* and *Chilcompton Tunnel*. Many 40" long layouts in various scales/gauges to mark R&DRMS' 40th exhibition.

**Contact:** Paul Allwood 02380 861365

www.rdrms.com

## **Saturday 2 & Sunday 3 March** **ROYAL LEAMINGTON SPA,** **Warwickshire**

**Organiser:** Leamington & Warwick MRS.

**Venue:** Warwickshire College, Warwick New Road, Royal Leamington Spa, Warwickshire CV32 5JE.

**Open:** 1000 – 1700 Saturday

1000 – 1600 Sunday.

**Admission:** adults £8.00, children £2.00, family (2+2) £18.00.

**Features:** 20+ layouts, trade support, other attractions.

**Contact:** www.lwmrs.co.uk

## **Sunday 3 March**

### **CHESHAM, Buckinghamshire**

**Organiser:** Chesham MRC.

**Venue:** White Hill Centre, White Hill, Chesham, Buckinghamshire HP5 1AG.

**Open:** 1000 – 1600.

**Admission:** adults £4.00, children £2.50, under 4s free, family (2+2) £10.00.

**Amenities:** refreshments & bar, limited parking on site (includes disabled spaces), further parking nearby.

**Features:** 11 layouts of various gauges, trade and club second-hand stalls, children's activities including a new hands-on OO layout.

**Contact:** Ron Nettleton 01494 785865

www.chesham-mrc.co.uk

## **Saturday 9 March**

### **LINLITHGOW, West Lothian**

**Organiser:** area groups of the EM Gauge Society, and Scalefour Society.

**Venue:** Low Port Centre, 1 Blackness Road, Linlithgow EH49 7HZ.

**Open:** 1000 – 1630.

**Admission:** free, donations encouraged.

**Amenities:** teas & coffees available; for lunch a wide variety of hostelrys and cafés is close at hand.

**Features:** sixth Chris Kedgeley Memorial Skills Day – a selection of shows you how demonstrations by a range of experienced modellers plus five layouts.

**Contact:** Dave Franks 01698 821272

www.emgs.org, www.scalefour.org

## **Saturday 9 March**

### **NEWTON AYCLIFFE, Co Durham**

**Organiser:** Rotary Club of Newton Aycliffe.

**Venue:** Youth Centre, Burn Lane, Newton Aycliffe DL5 4HT (opposite Tesco).

**Open:** 1000 – 1700.

**Admission:** adults £5.00, concessions £4.00, children 50p, family £10.00. Beneficiaries are Heel And Toe and The Railway Children.

**Amenities:** free parking, full disabled access, refreshments all day.

**Features:** 20 layouts in scales from N to O including self-drive *Thomas & Friends*. Traders, preservation societies, demonstrations.

**Contact:** John Burrows 07870 210269

www.newtonaycliffe Rotary.org.uk

## **Saturday 9 March**

### **NORWICH, Norfolk**

**Organiser:** Norwich Railway Society.

**Venue:** Poringland Community Centre, Overtons Way, Poringland, Norwich, Norfolk NR14 7WB.

**Open:** 1030 – 1630.

**Admission:** adults £3.00, children under 16 £1.00, under 5s free, family (2+3) £7.00.

**Amenities:** refreshments, raffle, disabled access, parking, direct bus from Norwich city centre.

**Features:** NRS annual show – layouts, trade support, local preserved railways stands.

**Contact:**

www.norwichrailwaysociety.org.uk



**Saturday 9 March**  
**PUDSEY, West Yorkshire**  
**Organiser:** Narrow Gauge North.  
**Venue:** Pudsey Civic Hall, Dawsons Corner, Pudsey LS28 5TA.  
**Open:** 1030 – 1630.  
**Admission:** adults £8.00, accompanied young people under 16 £1.00.  
**Amenities:** hot & cold refreshments, free parking, New Pudsey station 10 minutes. Disabled access except to stage.  
**Features:** 25th NGN – 20+ layouts, trade support, demonstrations.  
**Contact:** www.narrowgaugenorth.org.uk

**Saturday 9 March**  
**SWINDON, Wiltshire**  
**Organiser:** Swindon MRC.  
**Venue:** STEAM – Museum of the Great Western Railway, Fire Fly Avenue, Swindon, Wiltshire SN2 2EY.  
**Open:** 1000 – 1630.  
**Admission:** adults £5.00, children under 16 free when accompanied by an adult. Admission to the museum itself is not included in the above but not required to visit the exhibition.  
**Amenities:** disabled access, refreshments, parking at nearby designer outlet. Buses serve designer outlet, 10 minutes walk to railway and bus stations.  
**Features:** Modrail 2019 – 16+ layouts in Z, 3mm, OO and O featuring British and overseas prototypes including *Broadford* (HO) and *Tal y Bont* (EM). Trade support; full listings on website.  
**Contact:** Ian Burbidge 07880 654755  
 www.swindonmodelrailwayclub.com

**Saturday 9 March**  
**WOMBOURNE, Staffordshire**  
**Organiser:** Wombourne Spring Railx 2019.  
**Venue:** Wombourne Civic Centre, Gravel Hill, Wombourne, Staffordshire WV5 9HA.  
**Open:** 1000 – 1600.  
**Admission:** adults £5.00, concessions £3.00, children under 12 £1.00.  
**Features:** 21 layouts plus trade support and exhibition stands.  
**Contact:** 07927 059930

**Saturday 9 & Sunday 10 March**  
**BASINGSTOKE, Hampshire**  
**Organiser:** Basingstoke and North Hants MRS.  
**Venue:** Aldworth Science College, Western Way, Basingstoke RG22 6HQ.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £6.00, accompanied children £3.00, family (2+3) £12.00.  
**Amenities:** light refreshments, free parking and disabled access to all show areas. Heritage bus service between venue and Basingstoke railway station (see website for details).  
**Features:** 50+ layouts, trade and society stands.  
**Contact:** 07904 231961  
 www.basingstokemrs.org

**Saturday 9 & Sunday 10 March**  
**KEIGHLEY, West Yorkshire**  
**Organiser:** Keighley MRC.  
**Venue:** University Academy Keighley, Green Head Road, Utley, Keighley, West Yorkshire BD20 6EB.  
**Open:** 1000 – 1630 both days.  
**Admission:** adults £6.00, children £3.00, family (2+2) £15.00. Admission price includes entry into a prize draw with the chance to win a digital train set.  
**Amenities:** vintage bus service from Keighley station via town centre to venue.  
**Features:** 15+ layouts, trade support, demonstrations. Live steam ride-on railway and miniature traction engines  
**Contact:** www.keighley-mrc.org.uk

**Saturday 9 & Sunday 10 March**  
**MACCLESFIELD, Cheshire**  
**Organiser:** Macclesfield MRG.  
**Venue:** Tytherington School, Manchester Road, Macclesfield, Cheshire SK10 2EE.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1630 Sunday.  
**Admission:** adults £6.00, accompanied children under 16 free.  
**Amenities:** free parking, level access, refreshments. Free half-hourly vintage bus from the station via the town centre to venue – see website for timetable.  
**Contact:** www.macclesfieldmrg.org.uk

**Sunday 10 March**  
**STEYNING, West Sussex**  
**Organiser:** Wealden Railway Group.  
**Venue:** Coombe Court, The Steyning Centre, Fletchers Croft, Steyning, West Sussex BN44 3XZ.  
**Open:** 1000 – 1630.  
**Admission:** adults £3.50, accompanied children free.  
**Amenities:** parking nearby and refreshments all day.  
**Features:** WRG personal Layout Show – 14 layouts from N to O, British, European and US prototypes.  
**Contact:** wealdenrailway.awk@gmail.com

**Friday 15 to Sunday 17 March**  
**UTRECHT, Netherlands**  
**Organiser:** Modeltrein Expo OntraXS!  
**Venue:** Het Spoorwegmuseum (Dutch National Railway Museum), Maliebaan Station, Johan van Olden-barneveltlaan 1, 3581 XW Utrecht, Netherlands.  
**Open:** 1000 – 1700 each day.  
**Admission:** €17.50; on line in advance €16.50; weekend pass €25.00. (Prices to be confirmed; see website.)  
**Amenities:** parking; catering.  
**Features:** c.30 layouts by clubs and individuals from all over Europe, demonstrations, manufacturers, and trade, amongst the full size exhibits.  
**Contact:** www.spoorwegmuseum.nl

**Saturday 16 March**  
**ABINGDON, Oxfordshire**  
**Organiser:** Abingdon & District MRC.  
**Venue:** Abingdon & Witney College, Woolton Road, Abingdon OX14 1GG.  
**Open:** 1000 – 1700.  
**Admission:** adults £8.00, children x2 free when accompanied by an adult.  
**Amenities:** free parking, refreshments, disabled access.  
**Features:** ABRAIL 2019 – 17+ layouts, trade support, demonstrations and society stands.  
**Contact:** pthunt.mrc@outlook.com  
 www.admrc.org.uk

**Saturday 16 March**  
**TADWORTH, Surrey**  
**Organiser:** North Downs Model Railway Circle.  
**Venue:** The Good Shepherd Church Hall, Station Approach Road, Tadworth, Surrey KT20 5AH.  
**Open:** 1000 – 1700.  
**Admission:** adults £6.00, seniors/concessions/disabled £5.00, under 16s £3.00. All profits to Good Shepherd Church.  
**Amenities:** free parking, venue close to station and bus stop, refreshments, facilities for disabled visitors.  
**Features:** 36+ layouts, trade support, demonstration.  
**Contact:** Stuart Robinson 07758 458827  
 www.ndmrc.info/Home/Tadworth.htm

**Saturday 16 March**  
**WEDNESBURY, West Midlands**  
**Organiser:** West Bromwich MRC.  
**Venue:** Unit 1, Albert Street, Wednesbury,

West Midlands WS10 7JX.  
**Open:** 1000 – 1500.  
**Admission:** £1.00.  
**Amenities:** light refreshments, wheelchair access, parking outside and nearby (not free).  
**Features:** WBMRC open day – club layouts and stand plus trade support.  
**Contact:** 01902 491679

**Saturday 16 & Sunday 17 March**  
**EAST GRINSTEAD, West Sussex**  
**Organiser:** East Grinstead MRC.  
**Venue:** Sackville School, Lewes Road, East Grinstead, West Sussex RH19 3TY.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £5.00, concessions £4.00, children under 16 free.  
**Amenities:** refreshments, parking, wheelchair access. Buses 240, 281 & 291 from railway station and town centre stop outside venue.  
**Features:** layouts include *Awliscombe* (OO9), *Bee Road* (OO), *Berry Town* (N), *Birch Coombe* (OO), *Coleford* (OO9), *Cuttinglye Wharf* (O-16.5), *Dudley Road* (OO), *Holm* (OO), *Lydford* (OO9), *Moorside* (O), *Peters Street Station* (OO), *South Downs Tar* (O-16.5), *Upperton* (N), *Wenoe Quarry* (OO), *Westfield* (N). Trade support, demonstrations, society and club stands.  
**Contact:** Mick Champion 01342 314228  
 www.egmrc.org.uk

**Saturday 16 & Sunday 17 March**  
**JARROW, Tyne & Wear**  
**Organiser:** Perth Green MRC.  
**Venue:** Perth Green Community Centre, Inverness Road, Jarrow NE32 4AQ.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £6.00, children £3.00, family £14.00.  
**Contact:** www.perthgreenmrc.org

**Saturday 16 & Sunday 17 March**  
**NOTTINGHAM**  
**Organiser:** Nottingham MRS.  
**Venue:** Highbank Community Centre, Farnborough Road, Clifton, Nottingham NG11 9DG.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £4.00, children £1.00.  
**Amenities:** refreshments, level access throughout, venue is adjacent to Holy Trinity tram stop.  
**Features:** layouts including debut of the club's new 28' x 11' *Trent Lane Junction* (OO), based on the busy network of lines east of the city in the 1960s. Also *Elmfield* (N), *Heath Green* (OO), *Kozel Cement* (HO), *Mount Woodville Works* (P4), *Springfield* (HO). Trade stands and hands-on demonstration  
**Contact:** www.nottingham-modelrailway.org.uk

**Saturday 16 & Sunday 17 March**  
**Nr ROYAL LEAMINGTON SPA, Warwickshire**  
**Organiser:** Meridienne Exhibitions.  
**Venue:** Warwickshire Event Centre, Nr Royal Leamington Spa. On junction of A425 and B4455.  
**Open:** 1000 – 1600 both days (last admission 1500).  
**Admission:** in advance via website – adults £8.00, seniors £7.00, children (5 – 14) £3.00. On the door – adults £9.00, seniors £8.00, children (5 – 14) £4.00.  
**Features:** Midlands Garden Rail Show 2019, showcasing the larger scales. Specialist trade support.  
**Contact:** www.midlandsgardenrailshow.co.uk

**Saturday 16 & Sunday 17 March**  
**ROCHDALE, Lancashire**  
**Organiser:** Heywood MRG.  
**Venue:** Rochdale Masonic Hall, 2 Richard Street, Rochdale, Lancashire OL11 1DU.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £5.00, seniors £4.00, children £3.00, family (2+2) £13.00.  
**Amenities:** refreshments available, close to Rochdale Metro and railway station.  
**Features:** layouts include *Bucks Lane* (OO), *Burnaby* (HO), *Butterfly Lane* (On30), *California Dreamland* (HO), *Claremount Road* (OO), *High Lane* (N), *Knapford* (OO), *Porte d'Etioib* (HO), *Ty-Barch* (OO & OO9), *Wassup Dock* (N), *Whits End* (OO & OO9). Trade support.  
**Contact:** 07732 781989  
 www.hmrg.co.uk

**Saturday 16 & Sunday 17 March**  
**ROMANNOBRIDGE, Scottish Borders**  
**Organiser:** Tweeddale Railway Society.  
**Venue:** Newlands Centre, Romanno-bridge, Scottish Borders EH46 7BZ.  
**Open:** 1000 – 1600 both days.  
**Admission:** adults £5.00, concessions £4.00.  
**Amenities:** free parking on site, refreshments, disabled access.  
**Features:** Railways At War photo exhibition, model layouts and displays, trade stands.  
**Contact:** 01968 660980  
 www.newlandscentre.org.uk

**Saturday 16 & Sunday 17 March**  
**WIMBORNE, Dorset**  
**Organiser:** Wimborne Railway Society.  
**Venue:** Queen Elizabeth's School, Blandford Road (B3082), Wimborne, Dorset BH21 4DT.  
**Open:** 1000 – 1700 Saturday  
 1000 – 1600 Sunday.  
**Admission:** adults £6.00, children under 15 accompanied by a paying adult free.  
**Amenities:** free parking, wheelchair friendly, refreshments.  
**Features:** 29 layouts – British, continental, Canadian, N to O gauge – plus traders, demonstrators, publicity stands.  
**Contact:** 01202 673246

**Sunday 17 March**  
**ACTON, Nr Sudbury, Suffolk**  
**Organiser:** Sudbury MRC, Colchester MRC & Chelmsford MRC.  
**Venue:** Acton Village Hall (behind the Crown public house), High Street, Acton, Nr Sudbury, Suffolk CO10 0AT.  
**Open:** 1000 – 1600.  
**Admission:** adults/concessions £3.00, children £1.00. Raising funds for the 1st Cavendish Guides.  
**Amenities:** refreshments, raffle, children's activities.  
**Features:** layouts include *Ashurst Light Brickworks* (OO9), *Copsey* (OO), *Dawes Lane* (N), *East Dunnet* (O-16.5), *Leadson Street* (O), *Penny Lane MPD* (OO), *St Saviours Street* (N), *St Tugdal* (Nm), *Treblecombe* (OO, OO9 & Z), *Triang Three* (TT), *Wensleydale* (O-16.5). Trade support.  
**Contact:** Alan 07961 258299

**Saturday 23 March**  
**LEICESTER**  
**Organiser:** Train Collectors Society.  
**Venue:** Trinity Methodist Church Hall, Narborough Road, Leicester LE3 2RD.  
**Open:** 1030 – 1600 (members' only AGM begins at 1500).  
**Admission:** adults £4.00, TCS members £3.00, children under 14 free.  
**Amenities:** cafeteria with hot food, free street parking, easy access (3 miles) by car from Jc21 M1, good public transport from city centre.



**Features:** 10+ vintage toy train layouts plus trade support and club bring & buy.  
**Contact:** Bryan Pentland 07801 818764  
www.traincollectors.co.uk

#### Saturday 23 March

##### SETTLE, North Yorkshire

**Organiser:** Scoutrail.

**Venue:** Settle Primary School, Bond Lane, Settle BD24 9BW.

**Open:** 1000 – 1700

**Admission:** adults £3.00, children £2.00. Free entry for members of the Scout Association in uniform.

**Amenities:** refreshments. Venue is near railway station.

**Features:** layouts and traders, show in aid of Scout funds.

**Contact:** Dave Burrows 07580 120951

#### Saturday 23 March

##### TRIMLEY ST MARY, Suffolk

**Organiser:** Felixstowe Area Group of the N Gauge Society.

**Venue:** The Welcome Hall, High Road, Trimley St Mary, Suffolk IP11 0NT.

**Open:** 1000 – 1600

**Admission:** free.

**Amenities:** light refreshments, wheelchair friendly, free on-street parking.

**Features:** several layouts including the club's modular layout, *Trimley Station and Broadlands*. Trade and second-hand stalls.

**Contact:** Chris Shum 01349 277211

www.felixstowengauge.org.uk

#### Saturday 23 & Sunday 24 March

##### HOWDEN, East Yorkshire

**Organiser:** Carlton Railway Society.

**Venue:** The Shire Hall, 13 Market Place, Howden, East Yorkshire DN14 7BJ.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday.

**Admission:** adults £5.00, concessions £4.00, children (11 – 16) £2.00, under 11s free if accompanied by an adult.

**Features:** 15 – 20 layouts.

**Contact:** 01405 762476

#### Saturday 30 March

##### BELPER, Derbyshire

**Organiser:** Belper MRG.

**Venue:** Strutts Centre, Derby Road, Belper, Derbyshire DE56 1UU.

**Open:** 1000 – 1630.

**Admission:** adults £4.00, children £2.00.

**Amenities:** light refreshments, free parking at the venue and wheelchair access.

**Features:** layouts in a variety of scales plus trade support.

**Contact:** James Longden 07866 689004

#### Saturday 30 March

##### RAINFORD, Nr St Helens, Merseyside

**Organiser:** Rainford Narrow Gauge Show.

**Venue:** All Saints Parish Hall, Church Road, Rainford, Nr St Helens WA11 8HE.

**Open:** 1000 – 1630.

**Admission:** adults £5.00, accompanied children under 15 free.

**Amenities:** free parking at the hall and surrounding car parks, refreshments, wheelchair-friendly venue. Free vintage bus trips around Rainford courtesy of the Transport Museum in St Helens.

**Features:** layouts, trade support, society stands and demonstrations. The OO9 Society AGM will take place during the afternoon in a separate hall.

**Contact:** Stan Williams 01744 25361

## MEETINGS

#### Friday 1 March, 1930hrs

##### RCTS – Bristol

Branch AGM and members' evening.

**Venue:** St Peter's Church Hall, Church Road, Filton BS34 7BX.

**Contact:** Ronnie McAdam 0131 669 7503

#### Friday 1 March, 1930hrs

##### RCTS – Carnforth

*Rail Traffic For Power Generation* by Steve Chapman.

**Venue:** Station Hotel.

**Contact:** Ronnie McAdam 0131 669 7503

#### Friday 1 March, 1900hrs

##### SLS – Middlesbrough

Centre AGM.

**Venue:** Newport Community Hub, St Paul's Road, Middlesbrough TS1 5NQ.

**Contact:** 01642 780658

#### Monday 4 March, 1930hrs

##### Peak Rail Association – Sheffield Branch

*Trains And Terrains* by Geoff Proctor.

**Venue:** The Harlequin, 108 Nursery Street, Sheffield S3 8GG.

**Contact:** Dave Sharp 0114 274 5478  
www.peakrail.co.uk/sheffieldbranch

#### Monday 4 March, 1930hrs

##### Plymouth Railway Circle

*Plymouth's Hidden Railways*

by Paul Burkhalter.

**Venue:** St Edward's Church Hall, Home Park Avenue, Peverell, Plymouth, Devon PL3 4PG.

**Admission:** visitors £2.00

**Contact:** Stuart Hammond 07814 821116

#### Monday 4 March, 1400hrs

##### RCTS – Birmingham

*Next Train Gone* by Adrian White.

**Venue:** Christ Church Community Hall, Burney Lane, Ward End B8 2AS.

**Contact:** Ronnie McAdam 0131 669 7503

#### Tuesday 5 March, 1900hrs

##### RCTS – Watford

*An Evening With Father's Slides* by David Cross.

**Venue:** Beechen Grove Baptist Church, Clarendon Road WD17 1JJ.

**Contact:** Ronnie McAdam 0131 669 7503

#### Tuesday 5 March, 1930hrs

##### SEG/LRTA – Eastleigh

*More From My Tram Ciné Library*

by John Laker.

**Venue:** Eastleigh Railway Institute, Romsey Road.

**Admission:** £3.00

**Contact:** www.southernelectric.org.uk  
www.lrta.org

#### Wednesday 6 March, 1900hrs

##### Association of Shrewsbury Railway Modellers

*Loco Construction In The Smaller Scales* by Sandy Harper.

**Contact:** Nick Coppin 01952 884398

www.shrewsburyrailwaymodellers.co.uk

#### Thursday 7 March, 1930hrs

##### Lutterworth Railway Society

AGM and photo competition (meeting for members only).

**Venue:** United Reformed Church, George Street, Lutterworth LE17 4EF.

**Contact:** David Hanger 01455 203307  
www.lutterworthrailwaysociety.co.uk

#### Thursday 7 March, 1930hrs

##### RCTS – Milton Keynes

*East – West Railway Update*

by Stephen Barker.

**Venue:** The Methodist Church Hall, Silver Street, Stony Stratford MK11 1BE.

**Contact:** Ronnie McAdam 0131 669 7503

#### Thursday 7 March, 1930hrs

##### RCTS – Shipley

Branch AGM.

**Venue:** Saltaire Methodist Chapel, Saltaire Road, Shipley BD18 3HJ.

**Contact:** Ronnie McAdam 0131 669 7503

#### Thursday 7 March, 1915hrs

##### SLS – Kendal

*Preston To Oxenholme In The Last 10 Years Of Steam* by Noel Machell.

**Venue:** St John's Ambulance Centre, Sandes Avenue, Kendal LA9 4LL.

**Contact:** 01539 725858

#### Thursday 7 March, 1930hrs

##### The Great Central Railway Society – Sheffield Branch

*The Route Of The Master Cutler Part 2*

by Chris Youett.

**Venue:** The Harlequin, 108 Nursery Street, Sheffield S3 8GG.

**Contact:** Bob Gellatly 01909 565763  
www.gcrsociety.co.uk

#### Friday 8 March, 1915hrs

##### Great Western Society – Swindon

*Fast & Slow On The Great Western – A Look At Some Key Infrastructure Projects* by Phil Deaves.

**Venue:** Lawn Community Centre, Guildford Avenue, Off Windsor Road, Lawn, Swindon SN3 1JE.

**Contact:** 01793 495976

#### Friday 8 March, 1930hrs

##### Purbeck Railway Circle

*The Railways of Yeovil* by Brian Jackson.

**Venue:** Harmans Cross Village Hall, Haycrafts Lane, Harmans Cross, Dorset BH19 3EB.

**Admission:** free, donations invited.

**Contact:** mhwalshaw@talktalk.net

#### Friday 8 March, 1900hrs

##### SLS – Newcastle upon Tyne

*15 Inch Gauge Railways And*

*Sir Arthur Heywood* by Anthony Coulls.

**Venue:** The Conference Room, 1st Floor (access via Black Swan Courtyard), Newcastle Arts Centre, 67 Westgate Road, Newcastle upon Tyne NE1 1SG.

**Contact:** www.slsnewcastle.org.uk

#### Saturday 9 March, 1900hrs

##### RCTS – Exeter

*Peter Gray's Westcountry – Part 2*

by Amyas Crump.

**Venue:** Conference Room, Great Western Hotel, by Exeter St David's station, EX4 4NU.

**Contact:** Ronnie McAdam 0131 669 7503

#### Saturday 9 March, 1400hrs

##### SLS – Kidderminster

*Incidents, Accidents And Anecdotes*

by Chris Wilkinson and Ian Tipper.

**Venue:** Kidderminster Railway Museum, adjoining Severn Valley Railway station, Kidderminster DY10 1QX.

**Contact:** 01905 749526

#### Monday 11 March, 1930hrs

##### Lincoln Railway Society

*Distant Trails 2* by Ted Parker.

**Venue:** St Hugh's Church, Harewood Crescent, North Hykeham, Lincoln LN6 8JG.

**Admission:** visitors £3.00

**Contact:** 01522 698085  
grahamlightfoot1948@gmail.com

#### Monday 11 March, 1930hrs

##### RCTS – Croydon

Branch AGM, photographic competition and members' slides and digital images.

**Venue:** Small Hall, East Croydon United Reformed Church, Addiscombe Grove, CR0 5LP.

**Contact:** Ronnie McAdam 0131 669 7503

#### Monday 11 March, 1930hrs

##### RCTS – Ipswich

*Swindon Works – The Golden Years – Plus (From 1914)*

by Rev Canon Brian Arman.

**Venue:** Greenfinch Church Hall, Greenfinch Avenue IP2 0SX.

**Contact:** Ronnie McAdam 0131 669 7503

#### Monday 11 March, 1400hrs

##### RCTS – Northampton

*Ramblings Around The UK 1962/63*

by Brian Holland.

**Venue:** Weston Favell Parish Hall, Booth Lane South, Weston Favell.

**Contact:** Ronnie McAdam 0131 669 7503

#### Monday 11 March, 1900hrs

##### RCTS – Sheffield

*Ex BR Shunting Locomotives At Home And Abroad* by John Wade.

**Venue:** The Sheffield Scout Headquarters, 60 – 68 Trippet Lane S1 4EL.

**Contact:** Ronnie McAdam 0131 669 7503

#### Monday 11 March, 1930hrs

##### SEG – Sussex Branch

*To Tashkent And Back* by Godfrey Gould.

**Venue:** The Deall Room, Southwick Community Centre, BN42 4TE.

**Admission:** non-members £3.00

**Contact:** stuart.hicks90@gmail.com

#### Monday 11 March, 1930hrs

##### SLS – Ardrossan

*A Lifetime Of Railway Photography* by Ian Lothian.

**Venue:** Lauriston Hotel, 15 South Crescent Road, Ardrossan KA22 8EA.

**Contact:** 01294 822303

#### Tuesday 12 March, 1945hrs

##### Great Western Society – Bristol

*Island Treasures (Isle Of Wight)*

by Colin Brading.

**Venue:** BAWA, 589 Southmead Road, Filton, Bristol BS34 7RG.

**Contact:** 01454 324230

#### Tuesday 12 March, 1930hrs

##### RCTS – Cambridge

*Railways Of The Isle Of Man*

by Geoff Brockett.

**Venue:** The Swift, Haggis Gap, Fulbourn CB21 5HD.

**Contact:** rctscambridge@gmail.com

#### Tuesday 12 March, 1930hrs

##### RCTS – Hitchin

Branch AGM and members' presentation evening.

**Venue:** Tilehouse Street Baptist Church, Upper Tilehouse Street SG5 2EE.

**Contact:** Ronnie McAdam 0131 669 7503

#### Tuesday 12 March

##### RCTS – Merseyside, Chester and North Wales Branch

*Observations at Doncaster*

**Contact:** Alan Turton 01606 854227  
Merseyside@rcts.org.uk

#### Tuesday 12 March, 1930hrs

##### RCTS – Nottingham

*Through Kilton Tunnel – Sheffield To Cleethorpes Part 3* by Stephen Gay.

**Venue:** Nottingham Mechanics, 3 North Sherwood Street, NG1 4EZ.

**Contact:** Rodney Allan 0115 972 4184  
rodney43allan@hotmail.co.uk

#### Tuesday 12 March, 1930hrs

##### Wells Railway Fraternity

*Crossrail* by Patrick Griffin (provisional).

**Venue:** Wells Town Hall, Market Street, Wells, Somerset BA5 2RB.

**Contact:** Andrew Tucker 01749 830695

#### Wednesday 13 March, 1930hrs

##### RCTS – Cardiff

*From Railways To Royalty*

by Jack Boskett.

**Venue:** Old Church Rooms, Park Road, Radyr CF15 8DF.

**Contact:** Ronnie McAdam 0131 669 7503

#### Wednesday 13 March, 1430hrs

##### SLS – Godalming

*Organising Special Trains in the 1950s*



by Hugh Davies.

**Venue:** The Scout and Guide Headquarters, Godalming GU7 1HR.  
**Contact:** 0208 390 5375

**Thursday 14 March, 1930hrs**  
**LCGB – St Albans Branch**

*Freight Trains Around Britain In The Last Century* by Geoff Brockett.

**Venue:** The Chiswell Green United Reformed Church Hall, Watford Road, St Albans AL2 3HG.

**Contact:** John Green 01727 861839  
www.lcgb.org.uk

**Thursday 14 March, 1945hrs**  
**Nuneaton Railway Circle**

*A Selection Of Diesel & Electric Images From My Archive* by Bob Sweet.

**Venue:** Chilvers Coton Conservative Club, Bridge Street, Nuneaton CV11 5UD.

**Admission:** non-members £3.00  
**Contact:** 01827 895961

**Thursday 14 March, 1930hrs**  
**RCTS/SRPS – Edinburgh**

*Sketching Scotland By Rail* by Leo Du Feu.

**Venue:** Quakers' Meeting House, 7 Victoria Terrace EH1 2HE.

**Contact:** scotland@rcts.org.uk

**Thursday 14 March, 1900hrs**  
**RCTS – Newcastle**

*The Peter Bland Collection Part 1* by Bryan Cross.

**Venue:** The Arts Centre, 67 Westgate Road, Newcastle upon Tyne NE1 1SG.

**Contact:** rctsnewcastle@gmail.com

**Thursday 14 March, 1900hrs**  
**RCTS – Peterborough**

*A Programme Of Steam On DVD* by John Baxter.

**Venue:** St Luke's Church, Mayors Walk, PE3 6EZ.

**Contact:** Ronnie McAdam 0131 669 7503

**Thursday 14 March, 1430hrs**  
**RCTS – Woking**

*40 Years Of Preserving The Legendary Deltics* by Murray Brown.

**Venue:** Woking United Reformed Church, White Rose Lane GU22 7HA.

**Contact:** Ronnie McAdam 0131 669 7503

**Friday 15 March, 1930hrs**  
**Great Western Society – Taunton**

*East Of Penzance* by Roger Marsh.

**Venue:** Village Hall, Stoke St Mary, Nr Taunton, Somerset TA3 5DE.

**Contact:** 01823 334188

**Friday 15 March, 1930hrs**  
**Launceston Railway Circle**

*Geoff's Photographic Competition* followed by members' films.

**Venue:** Dingley Hall, Central Methodist Church, Castle Street, Launceston PL15 8BA.

**Admission:** non-members £2.00

**Contact:** Roger Webster 01566 779896  
rogerwebster@hotmail.com

**Saturday 16 March, 1830hrs**  
**Great Western Society – Torbay**

*The Great Western Trust Collection* by Peter Rance.

**Venue:** Parish Centre, Church End Road, Kingskerswell, Torbay TQ12 5LD.

**Contact:** 01803 326329

**Monday 18 March, 1930hrs**  
**Plymouth Railway Circle**

*Roaming Through Wessex* by Peter Triggs.

**Venue:** St Edward's Church Hall, Home Park Avenue, Peverell, Plymouth, Devon PL3 4PG.

**Admission:** visitors £2.00

**Contact:** Stuart Hammond 07814 821116

**Monday 18 March, 1930hrs**  
**RCTS – Chester**

*55 Years Of Railway Photography Part 2*

by Les Nixon.

**Venue:** The Town Crier, City Road/Station Road (opposite station entrance).

**Contact:** Ronnie McAdam 0131 669 7503

**Monday 18 March, 1400hrs**  
**RCTS – Coventry**

*Steam Around LNER Lines In Scotland* by Steve Armitage.

**Venue:** The Coventry and North Warwickshire Sports Club, Binley Road CV3 1HB.

**Contact:** Derek Morris 01926 855069

**Monday 18 March, 1930hrs**  
**RCTS – Didcot**

*Heritage Railway Infrastructure Part 2* by John Sreeves.

**Venue:** Didcot Civic Hall, Britwell Road OX11 7JN.

**Contact:** Andrew Jenkins 01793 783749

**Monday 18 March, 1400hrs**  
**RCTS – Roade/Milton Keynes**

*1960s Steam In The East Midlands Area* by Michael Clemens.

**Venue:** Roade Village Hall, Bailey Brooks Lane NN7 2LS.

**Contact:** Ronnie McAdam 0131 669 7503

**Monday 18 March, 1930hrs**  
**RCTS – Shenfield**

*Main Line Railtours* by Matthew Hills.

**Venue:** Shenfield Parish Hall, 60 Hutton Road CM15 8BL.

**Contact:** Ronnie McAdam 0131 669 7503

**Monday 18 March, 1900hrs**  
**The Friends of Swindon Railway Museum**

*The Railway Navvy: Prince Of Labourers* by David Gilks.

**Venue:** STEAM – Museum of the Great Western Railway, Fire Fly Avenue, Swindon SN2 2EY. Free parking available in front of the museum: email the address below for the access code.

**Admission:** free, donations to The Railway Children requested.

**Contact:** friendsofswindonrailwaymuseum@gmail.com

**Tuesday 19 March, 1930hrs**  
**RCTS – Cheltenham**

*An Eighth Colour-Rail Journey* by Paul Chancellor.

**Venue:** The Victory Club, Burlington House, Lypiatt Road GL50 2SY.

**Contact:** cheltenham@rcts.org.uk

**Tuesday 19 March, 1930hrs**  
**RCTS – Hull**

*Favourite Images* by Eddie Parker.

**Venue:** Upstairs, The Highway Public House, Willerby Road HU5 5LH.

**Contact:** Ronnie McAdam 0131 669 7503

**Tuesday 19 March, 1400hrs**  
**RCTS – Redhill**

*Trainspotter's Odyssey* by Rev Canon Brian Arman.

**Venue:** Redhill Methodist Centre, Gloucester Road RH1 1BP.

**Contact:** Ronnie McAdam 0131 669 7503

**Tuesday 19 March, 1900hrs**  
**SLS – Middlesbrough**

*Update On The G5 New Build Project* by Bernard Morris.

**Venue:** Langdon Square Community Centre, Coulby Newham, Middlesbrough TS8 0TF.

**Contact:** 01642 780658

**Tuesday 19 March, 1930hrs**  
**The Grimsby & Cleethorpes Railway Group**

*A Rhapsody In Blue – The GNR Of Ireland* by Ken Grainger.

**Venue:** The No.1, Station Approach, Cleethorpes DN35 8AX.

**Contact:**

Stuart Blackburn 01472 505853

www.gcrsociety.co.uk

**Wednesday 20 March, 1930hrs**

**RCTS/Mid-Hants Railway – Eastleigh**  
*And Now For Something Different (Nostalgia)* by Mark Ellis.

**Venue:** Eastleigh Railway Institute, Romsey Road SO50 9FE.

**Contact:** Ted Vaughan 023 8084 9533

tedvaughan449@yahoo.co.uk

**Thursday 21 March, 1930hrs**  
**Aln Valley Railway**

*Cross Country Trains* by Alex Bray.

**Venue:** The Old Waiting Room, Alnwick Station.

**Contact:** Roger Jermy 01665 606168

**Thursday 21 March, 1930hrs**  
**Lutterworth Railway Society**

*British Railways In Transition 1950s – 1970s: Photographs* by Cedric & John Clayson by John Clayson.

**Venue:** United Reformed Church, George Street, Lutterworth LE17 4EF.

**Contact:** David Hanger 01455 203307

www.lutterworthrailwaysociety.co.uk

**Thursday 21 March, 1915hrs**  
**Marlow & District Railway Society**

*UK Steam Preserved, 1969 To The Present Day* by Ken Livermore.

**Venue:** Bourne End Community Centre, Bourne End, Bucks. SL8 5SX.

**Contact:** 01494 638090

www.mdrs.org.uk

**Thursday 21 March, 1900hrs**  
**RCTS – Darlington**

*American Wanderings – Heading West* by Gordon Davies.

**Venue:** Darlington Cricket Club, South Terrace, Feethams DL1 1SG.

**Contact:** Brickjames94@gmail.com

**Thursday 21 March, 1930hrs**  
**RCTS – Shipley**

*1975 – A Good Year* by Robin Lush.

**Venue:** Saltaire Methodist Chapel, Saltaire Road, Shipley BD18 3HJ.

**Contact:** Ronnie McAdam 0131 669 7503

**Friday 22 March, 1900hrs**  
**SLS – Newcastle upon Tyne**

*The Bahamas Renaissance* by John Hillier.

**Venue:** The Conference Room, 1st Floor (access via Black Swan Courtyard), Newcastle Arts Centre, 67 Westgate Road, Newcastle upon Tyne NE1 1SG.

**Contact:** www.slsnewcastle.org.uk

**Saturday 23 March, 1400hrs**  
**SLS – Manchester**

*Manchester To Cleethorpes On Ex-GCR Routes* by Phil Lockwood & Enid Vincent.

**Venue:** The Friends' Meeting House, Mount Street, Manchester M2 5NS.

**Contact:** 0161 973 1165

**Monday 25 March, 2000hrs**  
**Nantwich & Market Drayton Railway Society**

*Shrewsbury & Welshpool Railway* by Bob Yate.

**Venue:** King's Head Inn, Shrewsbury Road, Market Drayton TF9 3EH.

**Contact:** johnslynn147@virginmedia.com

**Monday 25 March, 1930hrs**  
**RCTS – Maidenhead**

An evening with Mark Hopwood in celebration of the branch's 100th meeting.

**Venue:** Cox Green Community Centre, Highfield Lane, Cox Green SL6 3AX.

**Contact:** Ronnie McAdam 0131 669 7503

**Monday 25 March, 1930hrs**  
**RCTS – Northampton**

*Stored For Scrap* by Colin Home.

**Venue:** Weston Favell Parish Hall, Booth Lane South, Weston Favell.

**Contact:** Ronnie McAdam 0131 669 7503

**Monday 25 March, 1900hrs**  
**RCTS – Sheffield**

*The Calder Valley From Summit And*

*Todmorden To Normanton*

by Steve Armitage.

**Venue:** The Sheffield Scout Headquarters, 60 – 68 Trippet Lane S1 4EL.

**Contact:** Ronnie McAdam 0131 669 7503

**Tuesday 26 March**  
**RCTS – Merseyside, Chester and North Wales Branch**

*West Yorkshire Day Ranger*

**Contact:** Alan Turton 01606 854227

Merseyside@rcts.org.uk

**Tuesday 26 March, 1930hrs**  
**RCTS – Nottingham**

*SARUK Sibiu – Agnita, Romanian Narrow Gauge Railway* by Frank Cooper.

**Venue:** Nottingham Mechanics, 3 North Sherwood Street, NG1 4EZ.

**Contact:** Rodney Allan 0115 972 4184  
rodney43allan@hotmail.co.uk

**Tuesday 26 March, 1400hrs**  
**RCTS – Welwyn Garden City**

*1000 Eggs On A Bicycle*

by Colin Boocock.

**Venue:** The Methodist Church, junction of Ludwick Way and Cole Green Lane, AL7 3PN.

**Contact:** Ronnie McAdam 0131 669 7503

**Tuesday 26 March, 1915hrs**  
**RCTS – Woking**

*Clan Line* by Chris Meredith.

**Venue:** Woking United Reformed Church, White Rose Lane GU22 7HA.

**Contact:** Ronnie McAdam 0131 669 7503

**Wednesday 27 March, 1930hrs**  
**RCTS – Chichester**

*Strictly Freight Only Part 2*

by Brian Ringer.

**Venue:** Council Meeting Room 2, County Hall, West Street PO19 1RQ.

**Contact:** Ronnie McAdam 0131 669 7503

**Thursday 28 March, 1945hrs**  
**Nuneaton Railway Circle**

*The Story Of Freightliner*

by Peter Graham.

**Venue:** Chilvers Coton Conservative Club, Bridge Street, Nuneaton CV11 5UD.

**Admission:** non-members £3.00

**Contact:** 01827 895961

**Friday 29 March, 1930hrs**  
**Great Western Society – Oxford**

*The Final Years Of Western Region Steam* by Ralph Ward.

**Venue:** Pauling Human Sciences Centre, 58 Banbury Road, Oxford OX2 6QS.

**Contact:** 01865 243526

**Friday 29 March, 1930hrs**  
**RCTS – Preston**

*21st Century Miscellany*

by Tom Heavyside.

**Venue:** Fulwood Lawn Tennis Club, Highgate Avenue PR2 8DY.

**Contact:** Ronnie McAdam 0131 669 7503

**Saturday 30 March, 1400hrs**  
**T&LRS – East Midlands Area**

*Steam Trams* by Paul Abell.

**Venue:** Sixth Beeston Scout Hut, Middle Street, Beeston, Nottingham NG9 1GA.

**Contact:** David Hanger 01455 203307

## CLUB NEWS

**Daventry Model Railway Club** has moved to new clubrooms: Unit 1, Bridge Ground Workshop Premises, Staverton NN11 6BG. See www.daventrymrc.org.

**Worthing Model Railway Club** has announced the appointment of its new Exhibition Manager, Mark Stevens, who has succeeded Chris Sweeney. Mark may be contacted via the club's website at www.worthingmrc.co.uk



# RAILWAY MODELLER

## Opportunity Yours

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## Trade

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**PASTIMES, GLASGOW** - Buy and sell obsolete and new railways and diecast. 126 Maryhill Road, Glasgow, G20 7QS. Tel: 0141 331 1008.

**COLWYN BAY, CLWYD MODELS.** Opening times: Thursday, Friday + Saturday 10-1pm, 2-5pm. Second hand model railway books, videos, layouts, diecast bought, sold and exchanged. 447, Abergele Road, Old Colwyn. Tel: 01492 518 709.

**PLUS DAUGHTERS OF BASILDON ESSEX.** N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. PLUS DAUGHTERS, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon, Essex, SS13 1EU. Tel: 01268 726 211 [www.plusdaughters.co.uk](http://www.plusdaughters.co.uk)

**MODEL EXCHANGE.** Tel: 07580 065 017  
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**RHYL MODEL RAILWAYS,** 44 Wellington Road, Rhyl LL18 1BN. Opening times, please check website. Tel 01745 798460.  
[www.model-railways-rhyl.co.uk](http://www.model-railways-rhyl.co.uk)

**SHREWSBURY AND TELFORD - COME TO MODELSCAPE FOR YOUR MODEL RAILWAY NEEDS.** Open Monday 10-5, Wednesday 2-5, Friday 2-5 and Saturday 10-12:30. Unit 8bc, Rodenhurst Business Park, Rodington, Shrewsbury, SY4 4QU. On B5062 between Roden and High Ercall. 01952 771 115 [www.modelscape.net](http://www.modelscape.net)

## Trade Sales Products

**O GAUGE ETCHED KITS.** Locomotives, Carriages and Wagons. Connoisseur Models on line catalogue, [www.jimmcgeown.com](http://www.jimmcgeown.com) or send 2nd class address label for free printed catalogue. 1 Newton Cottages, Nr Weobley, Herefordshire, HR4 8QX.

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## Trade Sales Books

**SPECIALIST IN OUT OF PRINT RAILWAY** books, timetables, photographs, official publications and relics. Lists welcome. Visitors by appointment. BARRY JONES, 28 Marine Crescent, Worthing, BN12 4JF. Tel/Fax: 01903 244 655.

**MIDDLETON PRESS - PUBLISHED ON 16TH FEBRUARY.** Ilkeston to Chesterfield, just published Loughborough to Ilkeston £18.95 free P+P (UK) mail order service. Cheque, Visa or Mastercard. Write or telephone for latest brochure. MIDDLETON PRESS, Easebourne Lane, Midhurst, West Sussex, GU29 9AZ, telephone: 01730 813 169.  
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## Trade Services

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[www.tuebrooktrains.co.uk](http://www.tuebrooktrains.co.uk)

**O GAUGE AND GAUGE 1 MODEL BUILDER - OVER 30 YEARS EXPERIENCE.** Contact: PAUL ALLEN, telephone: 01773 741 194 (after 5pm).

## Wanted (Trade)

This section is reserved for the use of traders only.

**WE VALUE YOUR USED MODEL RAILWAYS** very highly. Best prices paid for good condition modern OO and N Gauge second hand items. Send us your list of surplus items in any gauge or make (s.a.e. please) for our quotation by return post. We also buy tinplate of all kinds, Dinky Toys, Corgis and Lesneys. We will travel anywhere to collect. RAILWAY ROUNDABOUT, The Walnut Tree, Egremont Street, Glemsford, Suffolk, CO10 7SA. Tel: 01787 280 452, email: [bill.railwayroundabout@gmail.com](mailto:bill.railwayroundabout@gmail.com)

**MODEL RAILWAYS WANTED** - cash waiting for British & American N & OO/HO scales plus Continental HO model railway equipment. British and American O-Scale also considered along with interesting items of railwayana. (Closed Mondays). 53A Models, 430 Hessle Road, Hull. 01482 227 777. [www.53amodels.co.uk](http://www.53amodels.co.uk)

**AIRFIX/FROG/REVELL** and other makes of unmade Second-hand plastic kits wanted. Aircraft especially wanted. Any quantity. KINGKIT, Unit 8, Cedar Court, Halesfield 17, Telford, TF7 4PF. Tel: 01952 586 457.

**WANTED - TRAIN COLLECTIONS O, OO, N.** Always calling in areas SOMERSET, DEVON, CORNWALL, S-WALES, WILTS, HAMPSHIRE, most places in UK covered. Will dismantle layouts any size. Interested in British N gauge Farish, Mintrix, Peco, OO Bachmann, Hornby, Mainline, Lima, Wrenn, Tri-ang. Damaged, scrap locos, Peco points, track work, buildings, cars, people, books, diecast, EFE buses.



Cash paid, friendly service offered. *R.FOSTER 15 Rosebery Avenue, Yeovil, Somerset BA21 5LW. Tel: 01935 424165 email [russelltrains@aol.com](mailto:russelltrains@aol.com)*

**MODEL RAILWAYS ALWAYS WANTED**, including Hornby Dublo, Wrenn, Trix, Lima, Bachmann, Steam, Modern Image, OO, O, N gauge, kits and kit built. Send lists or give me a call. *BARRY JONES, 28 Marine Crescent, Worthing, BN12 4JF. Tel/Fax: 01903 244 655.*

**MODEL RAILWAYS WANTED** - All Gauges including N, OO, O Gauge. Large collections to surplus items. British/American and Continental boxed or unboxed, no problem. Spares and track also purchased. We will also buy any diecast. *Send list or telephone: MJH MODELS, S'Agaro, Goring Road, Woodcote, Reading, RG8 0QE. 01491 681 262 email: [mjhtoy@aol.com](mailto:mjhtoy@aol.com)*

**WE CARE A GREAT DEAL MORE!** We realise that your collection large or small is a prized possession, so we will always give our highest price, we will respond quickly to receiving your list and collect, dismantle and remove your items efficiently because we care. Whatever you have to sell from single items to large collections. Give us a call and prepare to be amazed! *S&J MODELS. Tel: 01606 872 786.*

*Email: [sjmodels@tiscali.co.uk](mailto:sjmodels@tiscali.co.uk)*

*Website: [www.sandjmodelrailways.co.uk](http://www.sandjmodelrailways.co.uk)*

**THINKING OF SELLING YOUR MODEL RAILWAY COLLECTION?** If so contact me for an immediate evaluation, distance no object, collections large or small. *PAUL CULL, telephone: 01302 481 274 or mobile: 07767 356 890 or send list to: The Bungalow, Dentons Green Lane, Kirk Sandal, Doncaster, DN3 1JP, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)*

**HORNBY & BASSETT-LOWKE O GAUGE TRAINS**, wagons, coaches and accessories. Anything considered. Top prices paid for single items and/or complete collections. *Tel: 01993 840 064 (Oxon) 0797 991 0760.*

**MODEL RAILWAYS WANTED - ANY GAUGE, ANYWHERE. ALWAYS BEST CASH PRICES PAID.** Layouts dismantled. *Telephone: 01924 824 748, anytime. Send your lists to: GOING-LOCO, 38 Potovens Lane, Lofthouse Gate, Wakefield, WF3 3JF email: [goinglocomodels@hotmail.com](mailto:goinglocomodels@hotmail.com)*

**ALWAYS WANTED BY TRI-ANGMAN**-collections, layouts, seeking, Tri-ang, Hornby, Dublo, Wrenn, Bachmann etc. Will collect across most of the UK. More than fair prices paid. *Tel: 07966 333 605. email: [Laurence@tri-angman.co.uk](mailto:Laurence@tri-angman.co.uk) [www.tri-angman.co.uk](http://www.tri-angman.co.uk)*

**WANTED-MODEL RAILWAY ITEMS, FROM ANYWHERE IN THE UK**, single items to whole collections, any gauge, we pay cash and will beat any genuine offer on collections. We also buy diecast models + vintage toys. *Tel: 01209 718 562 or 07876 457 114, email: [harvey.instance@sky.com](mailto:harvey.instance@sky.com)*

**HORNBY DUBLO WANTED - LOCOS, COACHES, WAGONS AND ALL RELATED ITEMS.** From single items to whole collections, cash paid, distance no object. *TONY COOPER. Tel: 01254 234 017.*

**MODEL RAILWAY COLLECTIONS URGENTLY REQUIRED - SMALL & LARGE**, any scale and any condition. British, European and American collections all required. Nationwide collections, distances no object. Contact us today for a quick and friendly service. *Telephone: 01302 371 623, mobile: 07526 768 178, email: [anoraksanonymous@googlemail.com](mailto:anoraksanonymous@googlemail.com)*

**WANTED WANTED MODEL RAILWAYS, ALL MAKES, ALL SIZES** from N Gauge, OO scale to G Scale and larger, will travel all UK. We also dismantle layouts, over 30 year's experience. *Send lists to DEREK BARNETT, 6 Vale Gardens, Penkridge, Staffs, ST19 5LQ. Tel: 01562 750 076 daytime, or text me 07721 333 521 anytime and I will ring you back, email [footplate@btconnect.com](mailto:footplate@btconnect.com)*

**WANTED TOP PRICES PAID! BUY-SELL-EXCHANGE ANY GAUGE, MAKE OR AGE.** Also Specialists in diecast Lorries, cars, buses and railwayana. Friendly, fast, professional service from our family run business, established for over 40 years. "We are a real shop not just a phone number" Distance no object, from single items to lifetime collections, instant settlement by your preferred payment method. See our main advert. Deal with confidence from a name that you can trust. *Rails of Sheffield 21/29 CHESTERFIELD ROAD, SHEFFIELD, S8 0RL. Tel: 01142 551 436. [www.railsofsheffield.com](http://www.railsofsheffield.com)*

**WANT TO SELL YOUR TRAINS - ALL GAUGES, ALL MAKES?** Hornby/Bachmann, OO, O, and N gauge. Top cash prices paid, collected, layouts dismantled. *Please send list to CLAIRE CLARKE: 14 Stourbridge Road, Lloyd Hill, Wolverhampton WV4 5NE email: [claireclarke@aol.com](mailto:claireclarke@aol.com) telephone: 01902 342 722.*

**MODEL RAILWAYS WANTED - N GAUGE, OO GAUGE, O GAUGE - ANY MAKE.** Free valuations, home visits, layouts dismantled. Fast friendly, courteous service. *Contact MARTIN: 01623 473 376, lists to: [modelpower@sky.com](mailto:modelpower@sky.com)*

**ANY MODEL RAILWAYS WANTED, ANY CONDITION, WORKING OR NOT**, (spares etc). Collections bought small or large. Hornby, Lima, Bachmann, Heljan, Dapol, Farish, Vi-trains, PECO, any gauge, OO, O & N etc. Accessories bought, layouts dismantled. *Send lists to: KEITH'S MODEL RAILWAYS, 2 Holyrood Drive, Countesthorpe, Leicester, LE8 5TR. Telephone: 0116 277 8634 email: [shak.marge@yahoo.co.uk](mailto:shak.marge@yahoo.co.uk)*

postage. *Mr W Sladen, telephone: 01274 818 102, 17a Moorclose Lane, Queensbury, Bradford, BD13 2NS.*

**MÄRKLIN SLR 700 LNER £1700, 442GE SIGNAL £1350.** *Tel: 01279 505 306.*

**OO GAUGE CLEARANCE. KITBUILT LOCOS, FAIRBURN 2-6-4T £69**, Stanier 2-6-4T £75, 9F with portescap motor £125, Nucleo Jinty £59, Gem Super D £65, Genesis. Diesels 10000/10001 £95 each, Grange £40. Also Bachmann standard class 4 £32, unrebuilt Scot £45, Mainline rebuilt Jubilee £40, J72 £19, black 5 £30, Lima Western £20, Hornby Ivatt Class 2 £29, Airfix Scot Hornby motor £30, unrebuilt BB £32, Hornby Class 110 3-car DMU boxed, 2 available £40 each. Wagons, various makes individually priced or bargain 10 for £30. PECO points code 100 £5/£6. A few coaches. *Phone for list, telephone: 0121 688 4504 (answerphone).*

## Property for Sale

*Cut the expense of moving house by advertising your property in the RAILWAY MODELLER or CONTINENTAL MODELLER for just 15p a word its great value for money!!*

**MID WALES 3 BEDROOM SEMI DETACHED FARM HOUSE** - Calor Gas central heating, wood burner in dining room, 2 extra model railway rooms, poly tunnel, green houses, caravan port, summer house in a 1/5th of an acre, also 1+1/4 acres of private land with mainline railway crossing with shooting and fishing rights. £325,000. *Tel: 01650 511 551.*

## Clubs and Societies

**GREAT EASTERN RAILWAY SOCIETY.** Railways in East London and East Anglia from the 1800's to today. Quarterly Journal and News magazines, publications and meetings. *SAE for details to: GERS, 24 Bacons Drive, Cuffley, Potters Bar, EN6 4DU, web site [www.gersociety.org.uk](http://www.gersociety.org.uk)*

## Wanted Clubs and Societies

**NARROW GAUGE STOCK WANTED BY THE 009 SOCIETY**, collections, loco's, rolling stock, readymade BEMO, LILLIPUT, ROCO etc or kit and scratch-built. *Contact: 009 Society, 5 Quarry Road, Colehill, Wimborne, BH21 2NP. Email: [members-sales@009society.com](mailto:members-sales@009society.com) For society details see [www.009society.com](http://www.009society.com)*

## Private

### Private Sales

**HORNBY OO LOCOMOTIVES - ALL NEW, UNUSED.** R3618 B.O.B Royal Observer Corps, 34050. R3658 Class 50 Glorious, N.S.E Livery 50033. R3298 Harrods Limited Edition, Bucklebury Grange, 6803 - only 200 made. Any loco £119.99, including

## Wanted Private

(This column is reserved for readers who are private Model Railway enthusiasts or collectors and require items purely for their own personal use).

**TRIX TWIN AND TRIX EXPRESS.** Avid collector. TTRCA No26, anything will be considered. Please phone anytime, will collect. *Tel: 01736 754 200. Mobile: 07969 499 586.*



**COLLECTOR PURCHASES QUALITY BRITISH OO COLLECTIONS**, plus 009 ON30 kit-built O fine-scale LMS/LNER. Phone for a friendly discussion: 0114 250 8009, email: [ellnitty@hotmail.com](mailto:ellnitty@hotmail.com)

**RAILWAY PHOTOGRAPHS**-original 35mm slides (or negatives) any railway subject, steam to blue diesel era. Good price for quality material. **JOHN TURNER**, 01482 227 777 or 01652 632 370 (evenings).

**CHESHIRE, NORTH WALES & MERSEYSIDE.** Wanted Model Railways preferably Hornby & Trix for my private collection, layouts dismantled. Email lists to: [cdo14@icloud.com](mailto:cdo14@icloud.com) or call 07974 116 014.

**N OR OO GAUGE**, British outline Model Railways sought by collector, Wrenn, Hornby, Bachmann, Farish etc. Will travel to collect. Tel: 01702 461 214 after seven, 07399 73 44 94, daytime. Send lists to: [psdavis17@talktalk.net](mailto:psdavis17@talktalk.net)

**HORNBY O GAUGE** LMS Princess Elizabeth & Southern L1. Both 20 volt electric. Tel: 01993 840 064 (Oxon) or 0797 991 0760.

**RAILWAY RELICS LOCOMOTIVE PLATES.** Station signs, clocks, posters, hand lamps, signalling, would collect. Top prices. Dale, East Barn, Loads Road, Holymoorside, Chesterfield, S42 7HW. Tel: 01246 569 263.

**WRENN RAILWAYS WANTED, LOCOS, COACHES AND WAGONS.** Single items or whole

collections, distance no object. Immediate cash settlement. **TONY COOPER**, 26 Warmden Avenue, Baxenden, Accrington, Lancs BB5 2PR. Tel: 01254 234 017.

**WANTED TRIANG TT AND LONE STAR TREBLE O ELECTRIC BY PRIVATE COLLECTOR.** Good price paid, will travel. Tel: 01980 862 387 or email: [hamptonrob1951@outlook.com](mailto:hamptonrob1951@outlook.com) (Wiltshire).

**WRENN 1966 TO 1992**-items wanted by collector preferably boxed, single or complete collections. Please telephone: 0773 0957 800/01582 401 053 or visit my website [www.wrennspecialist.co.uk](http://www.wrennspecialist.co.uk)

**CASH PAID ON COLLECTION OF YOUR UNWANTED MODEL RAILWAYS**, small or large collections OO or N gauge. Hornby, Lima, Bachmann, Farish etc. We will come to you and dismantle if needed. Please contact **STEVE** on: 01909 487 938 or send lists to: 27 Prince Charles Road, Worksop, Nottinghamshire, S81 7ES. Email: [steve37425@hotmail.co.uk](mailto:steve37425@hotmail.co.uk)

**KIT ITEMS, WHITEMETAL, BRASS OO & O, DJH,** Wills, Millholme, Alan Gibson etc. Engines, coaches, wagons, unmade, part made, engines working or not. Kit spares - will travel. Tel: 01302 481 274, mobile: 07767 356 890, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**LIMA DIESELS WANTED & ROLLING STOCK.** Tel: 01302 481 274 & mobile: 07767 356 890, email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

**G SCALE AND OTHER GARDEN RAILWAYS WANTED.** Gauge 1, SM32, Bachmann, LGB, Piko, Accucraft, Roundhouse etc. Locomotives, rolling stock, track, points and accessories all required. Tel: 01302 371623 email: [sophiecul99@gmail.com](mailto:sophiecul99@gmail.com)

**YOUR UNWANTED RAILWAYS, SINGLE ITEMS OR QUANTITY, ALL GAUGES -** immediate cash. Tel: 0208 715 5304, (South London).

**HORNBY DUBLO AND WRENN RAILWAYS WANTED, LOCOMOTIVES, COACHES, WAGONS** and all associated items, from one item to large collections. Happy to travel, immediate cash settlement. Jason Roger, send lists to 7 Broomways, Great Wakering, Southend on Sea, Essex SS3 0DP or email: [jaycossie@googlemail.com](mailto:jaycossie@googlemail.com) or telephone: 01702 749 183.

**IS THERE CASH IN YOUR ATTIC?** I will collect and pay best cash prices for model railway items, from single items to complete collections. Call **GEOFF**, for a no obligation chat on: 07986 936 058 or email me on: [cash4trains@live.co.uk](mailto:cash4trains@live.co.uk)

**O GAUGE MTH SCALETRAX TRACK, W.H.Y?** Telephone: 01259 220 972. Email: [iancatling2@msn.com](mailto:iancatling2@msn.com)

**WANTED LIMA O GAUGE MK1 COACHES, 33 DIESEL, HELJAN DAPOL LOCOS AND STOCK, O GAUGE TRACK.** Tel: 07711 631 800, [kevin.gill1@btconnect.com](mailto:kevin.gill1@btconnect.com)

# Swapmeets • Sales • Fairs

The following announcements are included in good faith from information provided by the organisers. As cancellations are sometimes inevitable, anyone travelling a long distance should telephone before doing so. All advertisements include the organiser's town and telephone number. The publisher of RAILWAY MODELLER cannot accept any responsibility for cancellations or errors.

## February 2019

**12 February (Tues) 7pm-9pm (early entry 5pm £2)**  
**BARNSELY TOY AND TRAIN FAIR**, Elsecar Heritage Centre, Elsecar S74 8HJ, space for 150 stalls, lots of free parking for stallholders and visitors. Adults £1.50. 5 minutes M1 junction 36. Tel: 01226 744 425 [www.newcomenairs.co.uk](http://www.newcomenairs.co.uk) [sales@newcomenairs.co.uk](mailto:sales@newcomenairs.co.uk)

**15 February (Fri) 7pm-9pm**  
**ALFRETON LEISURE CENTRE**, DE55 7BD. Superb range of rail and diecast, free parking and refreshments. Next event: 26th April. Contact: MALC. Tel: 07951 072 790.

**23 February (Sat) 10.30am-2pm**  
**DERBY (TRAINS ONLY) SWAPMEET**, Our lady of Lourdes Parish Centre, 36 Uttoxeter Road, Mickleover, Derby DE3 9GE. Admission £1, free car parking, refreshments available. Contact: PETER

**STANTON.**  
Tel: 01773 279 407 or 07798 858 532.  
Email: [pstanton1000@sky.com](mailto:pstanton1000@sky.com)

**23 February (Sat) 10am-3pm**  
**EXMOOR TRAIN & TOYFAIR TAUNTON**, Richard Huish College, South Road TA1 3DZ. Admission £2. 100+ tables, free parking, refreshments. Contact: DAVID. Tel: 01643 706 736.

**23 February (Sat) 10.30am-2pm**  
**NORWICH -** The Willow Centre, 1-13 Willowcroft Way, Cringleford, Norwich, NR4 7JJ. Admission £2, under 16 free, early entry 9am - £4pp. Good parking, disabled access and refreshments. **JOHN'S TOY FAIRS.** **JOHN PARSONAGE**, telephone: 07881 555 283, email: [john.parsonage@btconnect.com](mailto:john.parsonage@btconnect.com)

**23 February (Sat) 10.30am-2pm**  
**WORCESTER**, Perdiswell Leisure Centre, Bilford

Road, WR3 8DX (off A38 Droitwich Road), M5 junction 6. Admission £2. **TONY OAKES FAIRS.** Tel: 01270 652 773 & 07825 631 323.

**24 February (Sun) 10.30am-3pm**  
**COVENTRY - 150 STALLS**, The Connexion, Ryton on Dunsmore, Coventry CV8 3FL. Lots of stalls full of new and used model railways - many at bargain prices. Adults £3, Seniors £2.50, Children £1. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**24 February (Sun) 10am-2pm**  
**RAYLEIGH, ESSEX**, The Swayne, Park School, Sir Walter Raleigh Drive, off London Road, SS6 9BZ. Next event: 14th April. **SRP TOYFAIRS.** Tel: 01322 662 729 or 0773 999 8012 [www.srpoyfairs1.co.uk](http://www.srpoyfairs1.co.uk)

**24 February (Sun) 10.15am-3pm**  
**WIMBORNE MODEL TOYS AND TRAINS COLLECTORS FAIR**, Queen Elizabeth School (QE). Wimborne minster, Dorset BH21 4DT. Large free car



park, refreshments. Admission: £1.50. Enquiries to DAVE.

Tel: 01202 590 158 or 07808 484 414.

### 24 February (Sun) 10.30am-2pm

**WIRRAL**, Hulme Hall, Port Sunlight CH62 5DH (in the beautiful tourist attraction village of Port Sunlight). Up to 65 tables. Adults £2, Children 50p. BARRY STOCKTON.

Tel: 0151 334 3362 [www.barrystocktonfairs.co.uk](http://www.barrystocktonfairs.co.uk)

## March 2019

### 2 March (Sat) 10.30am-3pm

**SANDOWN PARK – 500 STALLS**, Europe's No 1 Toy Collectors Fair, Sandown Exhibition Centre, Sandown Park Racecourse, Esher, Surrey, KT10 9AJ. Adults £6.50, Senior Citizens £6, Children £2. Hundreds of stalls selling everything you want in model railways, come and see why Sandown is Britain's best loved toy show.

Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

### 3 March (Sun) 10am-2.30pm

**EASTLEIGH**, Barton Peveril Sixth Form College, Chestnut Avenue, SO50 5ZA. Less than 2 miles from our Fleming Park venue.

Tel: 01380 725 322, anytime.

### 3 March (Sun) 1pm-4pm

**FALKIRK** – Graeme High School, Callendar Road, Falkirk FK1 1SY. Entry £1, under 12 free. BRIAN McLAREN

Tel: 01324 624 102 [www.mclaren-models.com](http://www.mclaren-models.com)

### 3 March (Sun) 10.30am-2pm

**HAYDOCK PARK RACECOURSE**, Exhibition centre WA12 0HQ. (2 minutes from M6, junction 23). Up to 85 tables. Adults £2, Children 50p. BARRY STOCKTON.

Tel: 0151 334 3362 [www.barrystocktonfairs.co.uk](http://www.barrystocktonfairs.co.uk)

### 3 March (Sun) 10.30am-2pm

**KIDDERMINSTER**, Gilt Edge Leisure Centre, Zortech Avenue, Kidderminster DY11 7DY. Admission £1.50. TONY OAKES FAIRS.

Tel: 01270 652 773 & 07825 631 323.

### 3 March (Sun) 10am-2.30pm

**\*LONG MELFORD-NEAR SUDBURY - NEW TOYFAIR\***, 50 Tables. Long Melford Old School, Hall St, CO10 9DX. Light refreshments. Adults: £2.50/concessions £2.00/£5.00 early entry.

For Bookings contact PHIL COOPER: 01787 372 559 or BILL BOURNE on 01787 280 452.

### 3 March (Sun) 10.30am-3pm

**NEEDHAM MARKET – SUFFOLK**. Stowmarket Railway Club Train and Modell Collectors Fair, Needham Market Community Centre, School Street IP6 8BB. Adults £2, accompanied Children free. Wheelchair access, free parking, refreshments. Model Railways, diecast cars, buses and lorries, toys, railway and transport books and photographs.

Tel: 01449 672 698, email: [stowrailclub.org.uk](mailto:stowrailclub.org.uk)

### 3 March (Sun) 10am-2.30pm

**YORK RACECOURSE**, the Knavesmire Stand, York Racecourse, York YO23 1EX. 100 pitches, admission £2.50, Seniors £2, 1st Child £1.50. Organised by Lorraine for J&J FAIRS.

Tel: 01522 880 383 [jandjfairs-virginmedia.com](http://jandjfairs-virginmedia.com)

### 6 March (Wed) 7pm-9pm

**HERTFORD**, Richard Hale School, Hale Road, Hertford, Herts SG13 8EN. Close to Hertford East and North Stations and Bus Station, M25 and A10. Admission £1. Contact: JOE LOCK.

Tel: 07866 641 215.

### 9 March (Sat) 10am-2pm

**ROMFORD**, North Romford Community Centre, 32 Crockhouse Lane, Collier Row, Romford RM5 3QJ. Next event: 21st September. SRP TOYFAIRS.

Tel: 01322 662 729 or 0773 999 8012

[www.srptoyfairs1.co.uk](http://www.srptoyfairs1.co.uk)

### 10 March (Sun) 10am-2pm

**CARNFORTH**, Crag Bank Village Hall, LA5 9UH. Refreshments, free parking, wheelchair friendly.

Tel: 0161 620 0321, 0779 279 5512.

Email: [quadsquad0@gmail.com](mailto:quadsquad0@gmail.com)

Website: [www.cragbanktoyfairs.webs.com](http://www.cragbanktoyfairs.webs.com)

### 10 March (Sun) 10.30am-3pm

**DONCASTER RACECOURSE**, The Lazarus Exhibition Centre, Leger Way, Doncaster DN2 6BB. More than 300 stalls of toys, trains and models. Adults £4, Seniors £3.50, Children £1.

Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

### 10 March (Sun) 10am-3pm

**NEWTON ABBOT TRAIN & TOY FAIR**, Newton Abbot Racecourse, Devon TQ12 3AF. Admission £2. Superb venue, free parking, refreshments. 100+ tables. Organised by RAY HEARD.

Tel: 01823 480 097.

Email: [rayheard66@btinternet.com](mailto:rayheard66@btinternet.com)

### 10 March (Sun) 10.30am-2pm

**SHERINGHAM**, Sheringham Community Centre, Holway Road, Sheringham, Norfolk, NR26 8NP. Admission £2, under 16 free, early entry 9am - £4pp. Good parking, disabled access and refreshments. JOHN'S TOY FAIRS.

JOHN PARSONAGE, telephone: 07881 555 283, email: [john.parsonage@btconnect.com](mailto:john.parsonage@btconnect.com)

### 10 March (Sun) 10.30am-3pm

**WALSALL WOOD** – Oak Park Active Living Centre, Coppice Road, Walsall Wood.

Details: TRANSTAR PROMOTIONS.

Tel: 01922 643 385.

### 10 March (Sun) 10am-2pm

**WORTHING – SUSSEX**, The Charmendean Centre, Forest Road, Worthing West Sussex BN14 9HS. Next event: 5 May. SRP TOYFAIRS.

Tel: 01322 662 729 or 0773 999 8012

[www.srptoyfairs1.co.uk](http://www.srptoyfairs1.co.uk)

### 12 March (Tues) 6.30pm-8.30pm

**ODIHAM/HOOK**, Community Centre, Ravencroft, Hook RG27 9NN. Second Tuesday monthly.

Tel: 01380 725 322, anytime.

### 13 March (Wed) 6.30pm-8.30pm

**BURTON ON TRENT**, Town Hall, DE14 2EB. Always a good selection of rail and diecast.

Next event: 15th May. Contact: MALC.

Tel: 07951 072 790.

### 16 March (Sat) 10am-3pm

**BEXHILL ON SEA**, St Barnabas Parish Church, Sea Road/ Cantelupe Road, Bexhill on Sea, TN40 1JG. Community Café on site serving hot food. Next event: 1st June. SRP TOYFAIRS.

Tel: 01322 662 729 or 0773 999 8012

[www.srptoyfairs1.co.uk](http://www.srptoyfairs1.co.uk)

### 16 March (Sat) 10.30am-3.30pm

**CREWE TOY & TRAIN COLLECTORS FAIR**, Crewe Heritage Centre, Vernon Way Crewe CW1 2DB. 6ft tables £18. Enquiries: 93, Merlin Way, Crewe, CW1 3YP.

Tel: 01270 505 781 (before 8pm).

### 16 March (Sat) 10am-3pm

**GATESHEAD INTERNATIONAL STADIUM**, Neilson Road, Gateshead NE10 0EF (Just off Felling by pass, A184) Admission £3, Concessions £2. Future date: 11th May.

Tel: 0750 403 5955, email: [jim\\_corr73@hotmail.co.uk](mailto:jim_corr73@hotmail.co.uk)

### 16 March (Sat) 10.30am-2.30pm

**\*GLOUCESTER – POSTPONED\***, Churchdown Community Centre, Parton Road, GL3 2JH.

Tel: 01380 725 322, anytime.

### 16 March (Sat) 10.30am-3pm

**HEREFORD TOY & TRAIN SALE**. Hereford Leisure Centre, Holmer Road, HR4 9UD. 80 tables, parking, café. BUY\*SELL\*EXCHANGE. Admission £2.

Tel: 01643 702 757 & 07966 694 579

[www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)

### 16 March (Sat) 10.30am-3pm

**RUGBY VINTAGE – 100 STALLS**, The Benn Hall, Newbold Road, Rugby, Warwickshire CV21 2LN. Adults £3.50, Senior Citizens £3, Children £1. A must for collectors of Old toys and trains.

Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

### 17 March (Sun) 10am-2pm

**ASHFIELD** – Festival Hall, Kirkby in Ashfield NG17 7DJ. Good access, free parking, refreshments. Always a good selection of rail and diecast. Next event: 12th May. Contact: MALC.

Tel: 07951 072 790.

### 17 March (Sun) 10am-2pm

**ASHINGTON TOY & TRAIN COLLECTORS**, Ashington Community Centre, Foster Lane, Ashington, West Sussex RH20 3PG.

Telephone for further information PETE: 01424 846 676 or DAVID: 07742 609 865.

Email: [modelcarmart@gmail.com](mailto:modelcarmart@gmail.com)

Website: [www.modelcarmart.com/ashington](http://www.modelcarmart.com/ashington)

### 17 March (Sun) 10am-2pm

**\*CHARTHAM – NEW EVENT\*** Chartham Village Hall, Station Road, Chartham, near Canterbury CT4 7JA. Next event: 26th May. SRP TOYFAIRS.

Tel: 01322 662 729 or 0773 999 8012

[www.srptoyfairs1.co.uk](http://www.srptoyfairs1.co.uk)

### 17 March (Sun) 10.30am-3pm

**DEVIZES**, Corn Exchange, Market Place SN10 1HS. Toy and Train Fair.

Tel: 01380 725 322, anytime.

### 17 March (Sun) 10am-3pm

**IVANHOE MRS – DIECAST TOY & TRAIN FAIR**,



The Rothley Centre, Mountsorrel Lane, Rothley, Leicestershire LE7 7PR. Admission £1.50. Refreshments and free parking available. Website: [www.ivanhoe-mrs.co.uk](http://www.ivanhoe-mrs.co.uk)

**17 March (Sun) 10.30am-3pm**

**LLANELLI TOY & TRAIN SALE.** Llangennech Community Centre, off Hendre Road, Llangennech SA14 8TH. Just off J48 of M4. 45 TABLES, FREE PARKING. Great café and great atmosphere! BUY\*SELL\*EXCHANGE. Admission £2. Tel: 01643 702 757 & 07966 694 579 Website: [www.chrisdyerfairs.co.uk](http://www.chrisdyerfairs.co.uk)

**17 March (Sun) 10.30am-3pm**

**POTTERS BAR,** Elm Court Centre, Mutton Lane, M25 J24. EN6 3BP. Adults £1.20. Refreshments. Tel: 020 8205 1518.

**17 March (Sun) 10am-2.30pm**

**SPALDING,** Springfields Events Centre, Camelgate, Spalding PE12 6ET. 150 pitches, admission £2.50, Seniors £2, 1st Child £1.50. Organised by LORRAINE for J&J FAIRS. Tel: 01522 880 383 [jandjfairs-virginmedia.com](http://jandjfairs-virginmedia.com)

**17 March (Sun) 10.30am-3pm**

**STAFFORD – 250 STALLS,** The Prestwood Centre, Stafford County Showground, Weston Road, Stafford, ST18 0BD. Adults £4, Seniors £3.50, Children £1. There are many stalls selling OO and O gauge trains. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**19 March (Tues) 7pm-9pm**

**WOOTTON BASSETT,** Memorial Hall, Station Road SN4 7EE. Toy and train Fair. Third Tuesday monthly. Tel: 01380 725 322, anytime.

**24 March (Sun) 10am-2.30pm**

**LINCOLNSHIRE,** The Exhibition Centre, Lincolnshire Showground, Lincoln LN2 2NA. 150 pitches, admission £2.50, Seniors £2, 1st Child £1.50. Organised by LORRAINE for J&J FAIRS. Tel: 01522 880 383 [jandjfairs-virginmedia.com](http://jandjfairs-virginmedia.com)

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**SHEPTON MALLET,** Bath & West Showground, Shepton Mallet, BA4 6QN. 300 stalls, restaurant, free parking. Admission £4. BULLDOG FAIRS. Tel: 01373 452 857 or 07917 125 641 [www.bulldogfairs.com](http://www.bulldogfairs.com)

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**26 March (Tues) 6pm-9pm**

**GARSTANG TOYFAIR,** Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON. Tel: 01282 439 009.

**30 March (Sat) 10am-2.30pm**

**BRENTWOOD, INTERNATIONAL HALL,** Brentwood Centre, Doddinghurst Road, Brentwood, Essex, CM15 9NN. 200 pitches, admission £3, Seniors £2.50, 1st child £2. Organised by LORRAINE for J&J FAIRS. Tel: 01522 880 383 [jandjfairs-virginmedia.com](http://jandjfairs-virginmedia.com)

**31 March (Sun) 10.30am-3pm**

**BUXTON – 150 STALLS,** The Pavilion Gardens, St John's Road, Buxton, Derbyshire SK17 6XN. Adults £3, Senior Citizens £2.50, Children £1. Many stalls full of OO and O gauge trains. Tel: 01604 846 688 [www.bpfairs.com](http://www.bpfairs.com)

**31 March (Sun) 10am-3pm**

**PUDSEY CIVIC HALL,** on Leeds to Bradford Ring Road LS28 5TA. STEELE PROMOTIONS. Tel: 0161 283 1255.

## June 2019

**29 June (Sat) 10am -2pm**

**\*DITTON – NEW EVENT\*** Ditton Community Centre, Kilbarn Road, Ditton, Kent ME20 6AH. Next event: 11 August. SRP TOYFAIRS. Tel: 01322 662 729 or 0773 999 8012 [www.srpoyfairs1.co.uk](http://www.srpoyfairs1.co.uk)

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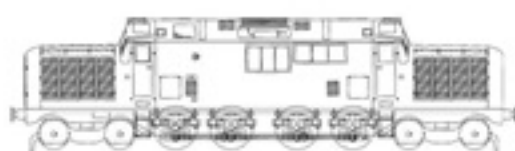
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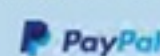
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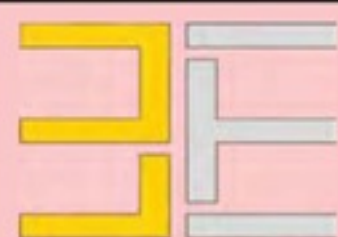


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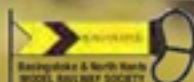
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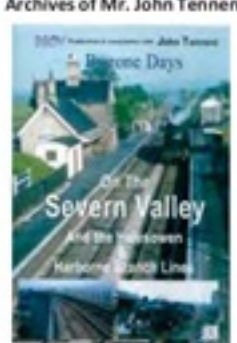
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Sat 10a.m. to 5p.m. Sun 10a.m. to 4p.m.

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Euxton Junction. Photo courtesy Tony Wright

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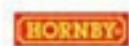
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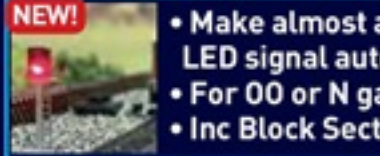
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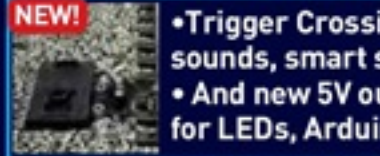
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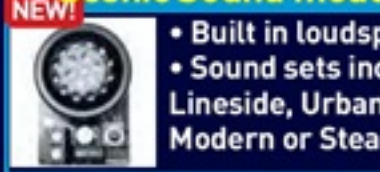
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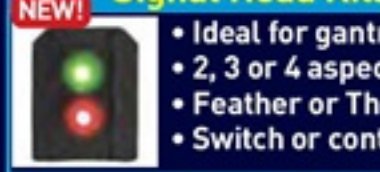
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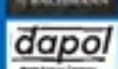
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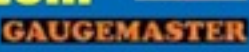
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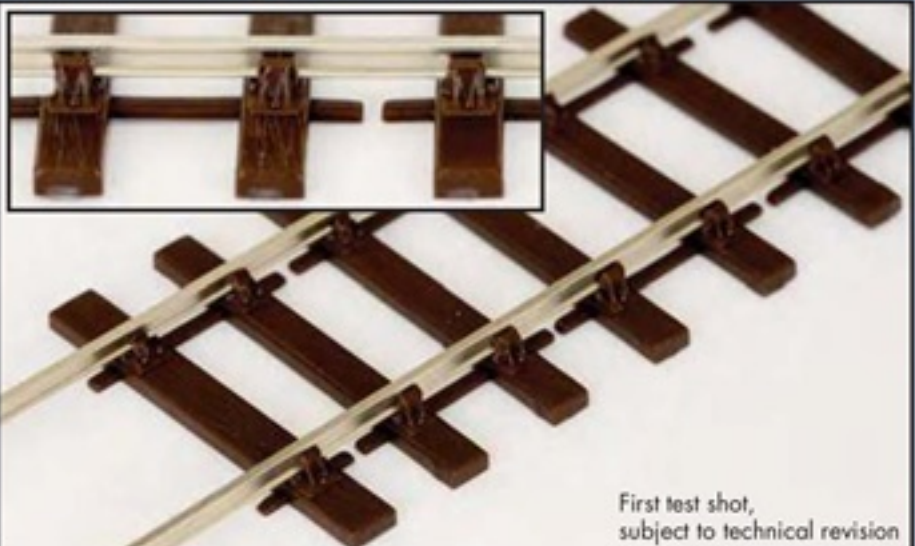






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## 7mm Scale / 0 Gauge



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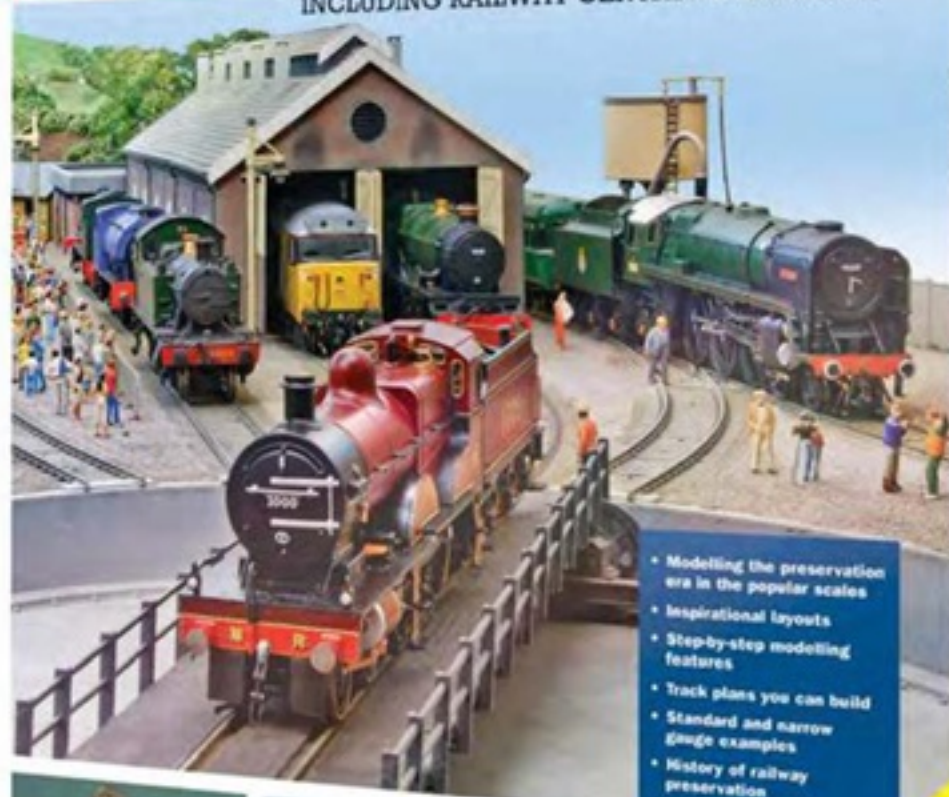
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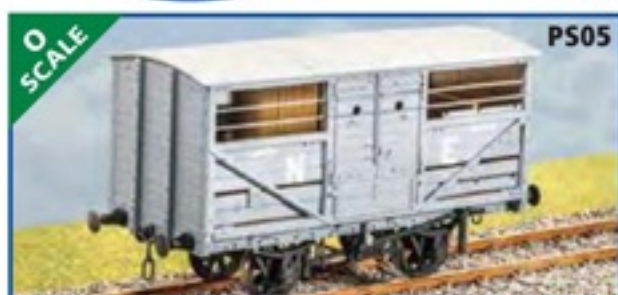
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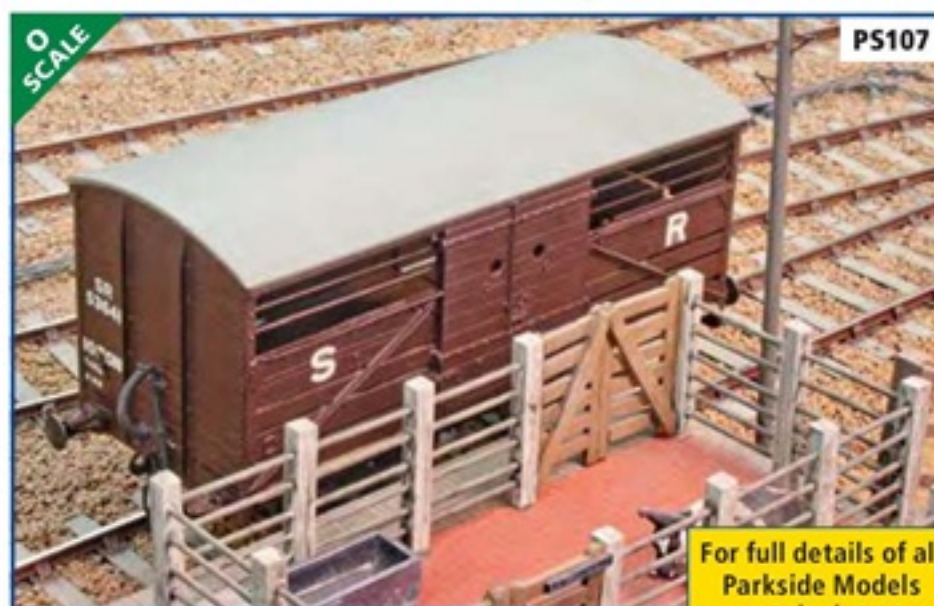
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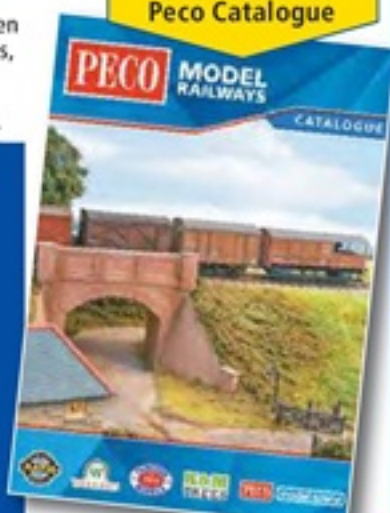
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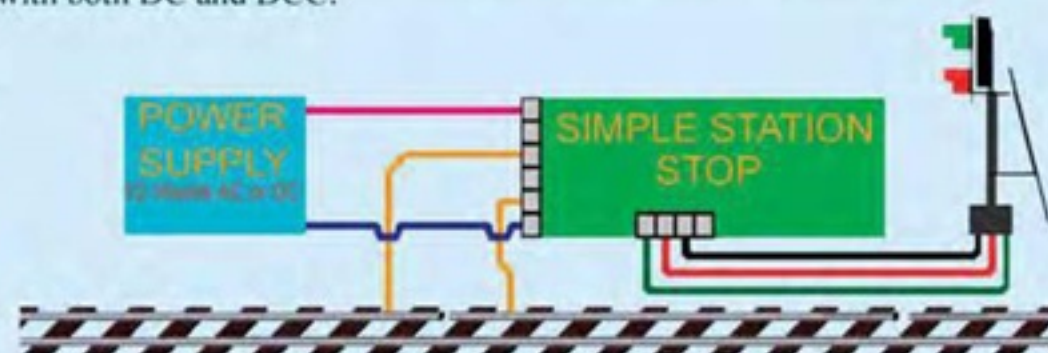


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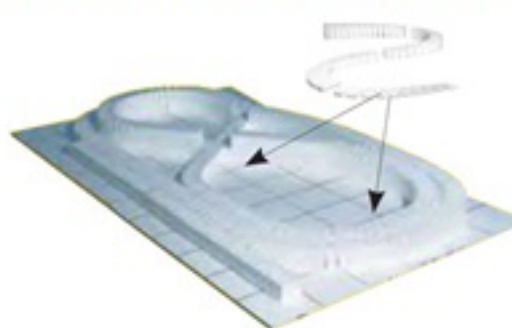


|         |           |
|---------|-----------|
| WST1406 | 1/2" (x4) |
| WST1414 | 3/4" (x4) |
| WST1407 | 1" (x4)   |
| WST1408 | 2" (x4)   |
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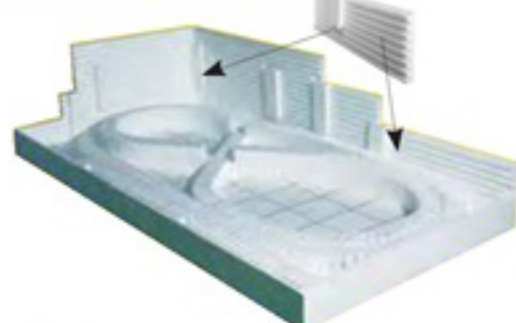


|         |                 |
|---------|-----------------|
| WST1410 | 2% Set (x8)     |
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|         |           |
|---------|-----------|
| WST1422 | 1/4" (x1) |
| WST1423 | 1/2" (x1) |
| WST1424 | 1" (x1)   |
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