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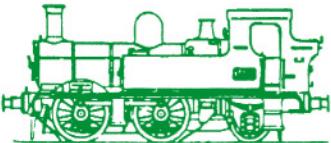
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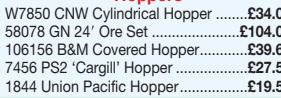


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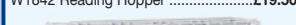
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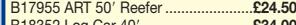
A34790 D&RG Hopper £15.50



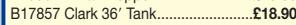
A4678 Amtrak 52' Gondola £21.95



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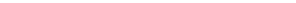
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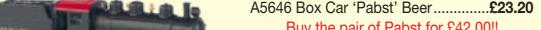


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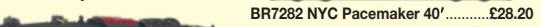
MT961 40' PFE Reefer £33.70



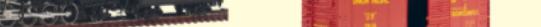
BW38164 Cyl Hopp £29.35



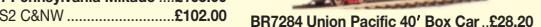
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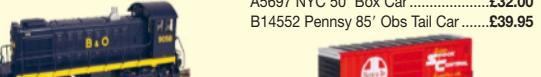
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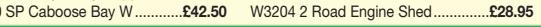
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CONTINENTAL MODELLER

August 2025

Volume 47

Number 8

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RAILWAY MODELLER

For all modelling British railway practice.
Published on the second Thursday
of the preceding month.



Cover

Deutsche Reichsbahn Neubau 2-10-2T 99 1777-4 hauling a mixed train which includes a transporter wagon across the spectacular Greifenbachtal bridge in the direction of Thum.

Photo: Helge Scholz.

Jools Holland visits Peco

We were delighted to welcome music legend Jools Holland to Peco recently. With concerts with his Rhythm and Blues Orchestra in Weymouth and Torquay on successive evenings, his route between the two would pass very close to Beer. So an invitation for an informal private visit was issued – and accepted.

Arriving mid-morning, after meeting the team responsible for the magazines and other office staff, Jools (along with his tour manager Steve Taylor) was taken through the Peco factory by Ben Arnold (Managing Director) and Michael Pritchard (Chairman), accompanied by the Editors of RM and CM. He happily chatted with staff in all departments about modelling and music, and said he was very impressed by the well-organised production facilities from design through to packing – and the fact it was all done on site in Britain.

He was then given the opportunity to drive 'his' locomotive – Beer Heights Light Railway 0-4-T No.12 *Jools*, which he had formally named during his previous visit in June 2018. The loco is frequently used for full day Driver Experience Courses on the BHLR.

After lunch (at his request in The Dining Car – the staff canteen – rather than the *Orion* Pullman car where significant guests are usually entertained!), he made time to see the extensive exhibition of model railways and the model shop. Then it was off to the next gig.

Since we featured his layout (January 2019) Jools tells us that he has made further developments, which we hope to report in due course.



Left
Jools is shown a test model of a new laser-cut wood kit by Ben Arnold.

Below left
Jools inspects work in progress on one of the multi-colour printers.



Below
Jools and Jools! Under the expert guidance of Bob Pearce and Derek Boswell, Jools then took to the driver's seat.

Photos: Craig Tiley.



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Photo shows a scene on the stunning
French HO layout *Le Quai des Deux Ponts*,
created and photographed by Christian Navello.
As seen in Continental Modeler May 2024.



The legendary DR diesel express train makes its come back!

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Icons - Option

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Using N Scale Track

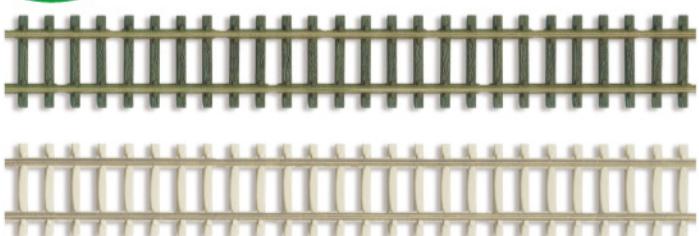


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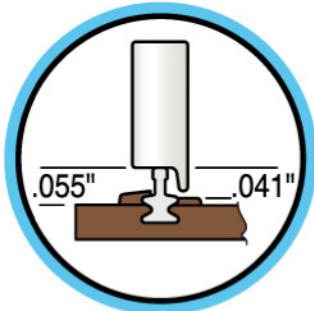


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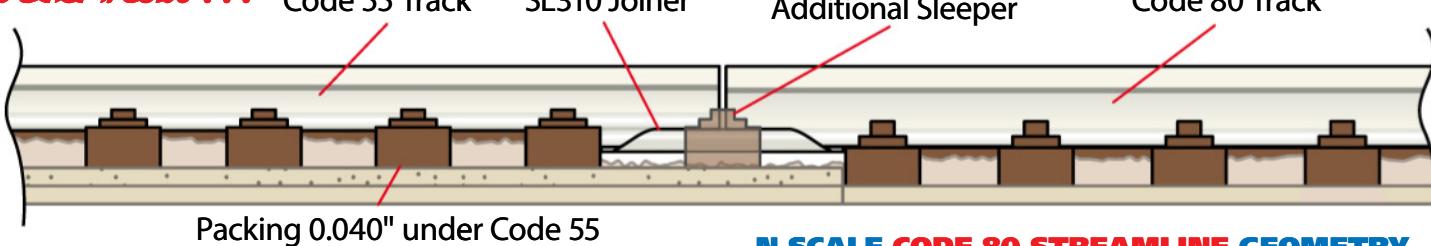


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Curved Turnouts Nominal Radii – Outside: 914mm (36in), Inside: 457mm (18in)

N SCALE CODE 55 STREAMLINE GEOMETRY

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Medium Radius Turnouts Nominal Radius: 457mm (18in)

Medium Y Turnouts Nominal Radius: 610mm (24in)

Large Radius Turnouts Nominal Radius: 914mm (36in)

Curved Turnouts Nominal Radii – Outside: 914mm (36in), Inside: 457mm (18in)

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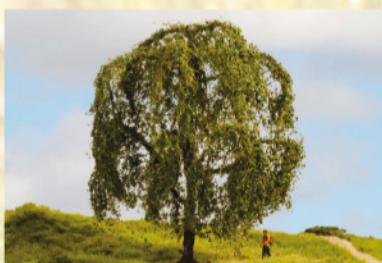
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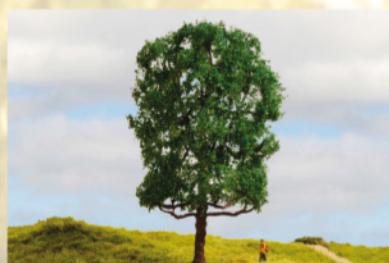
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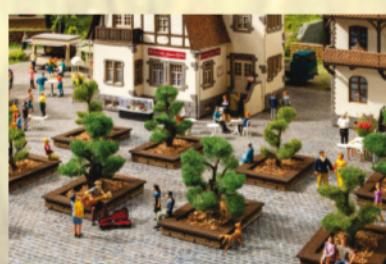
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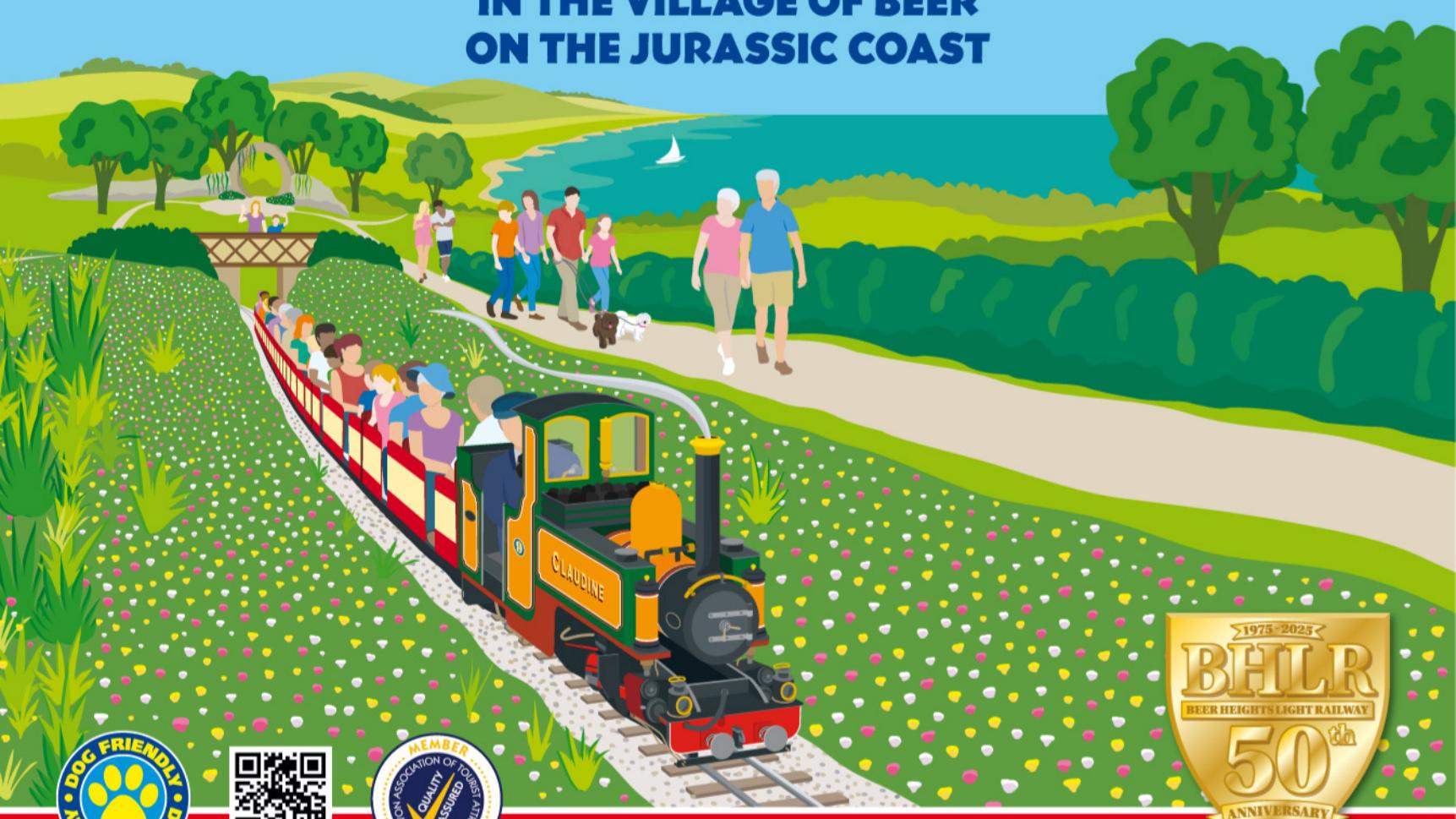


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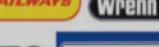
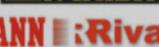
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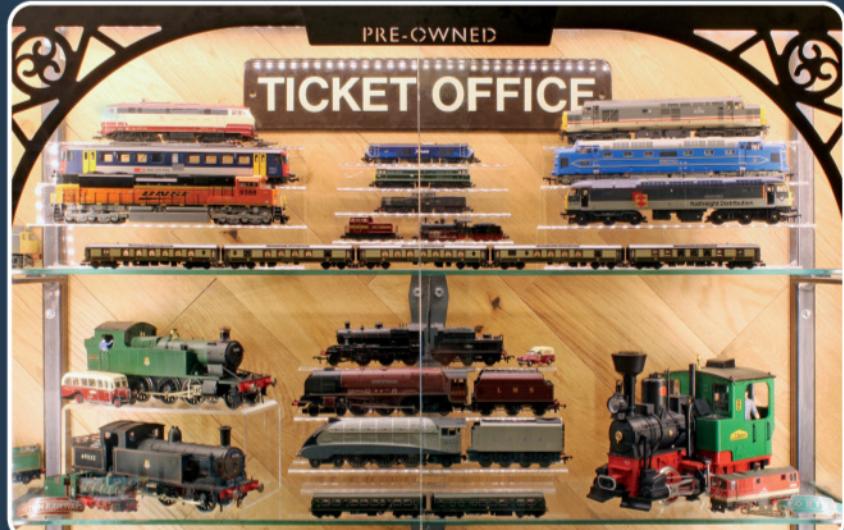
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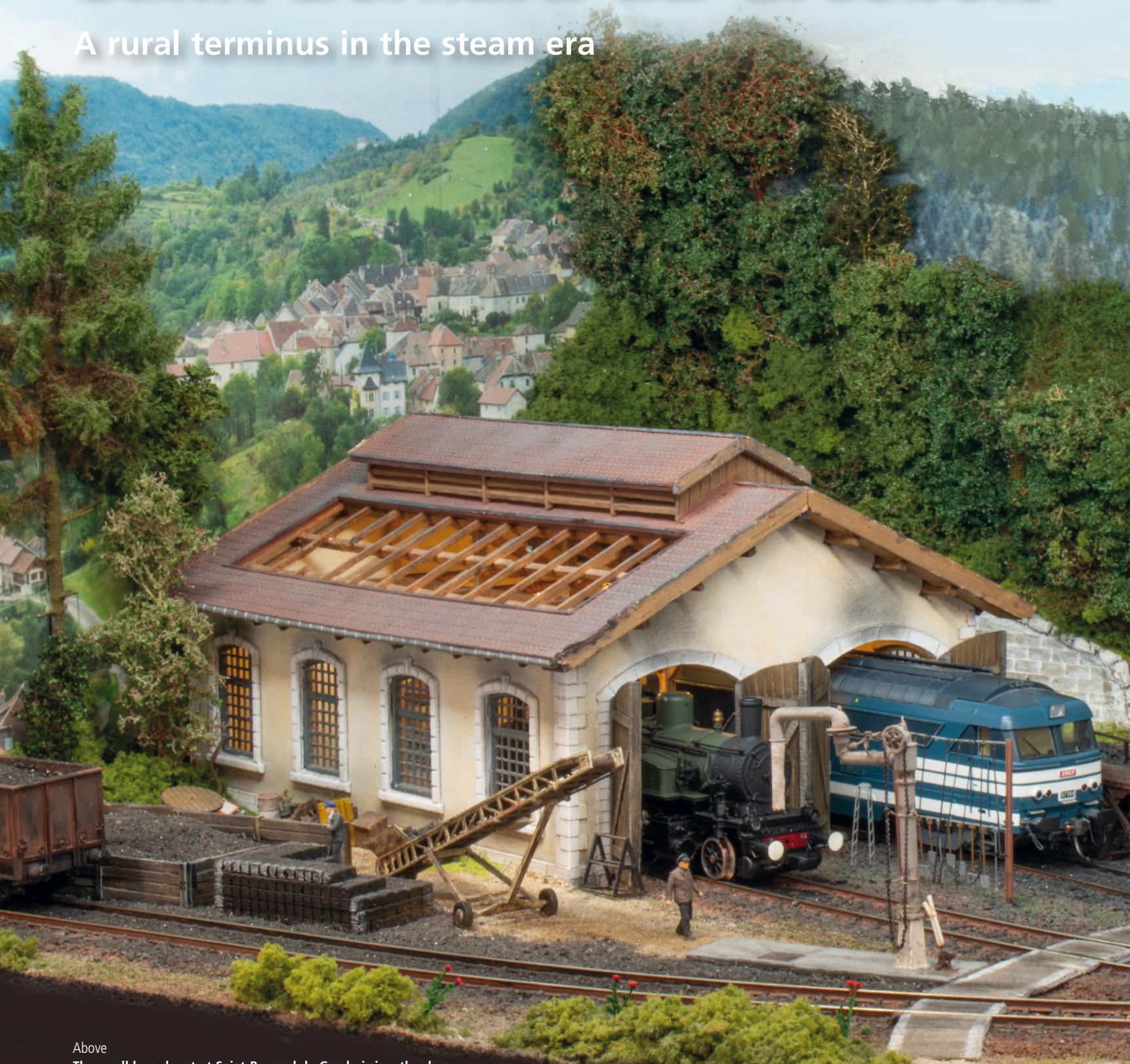
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Thomas Bossonet presents his popular French exhibition layout.

Saint-Bernard du Grosbois

A rural terminus in the steam era



Above

The small loco depot at Saint-Bernard du Grosbois is rather busy, hosting a 130TB (former Prussian T9³), BB67368 (a sign of things to come), 050TA 22 (former Prussian T16), and a 140C having the smokebox cleaned of ash. The short turntable was adapted from an item made for N gauge.

Photographs by the editor.

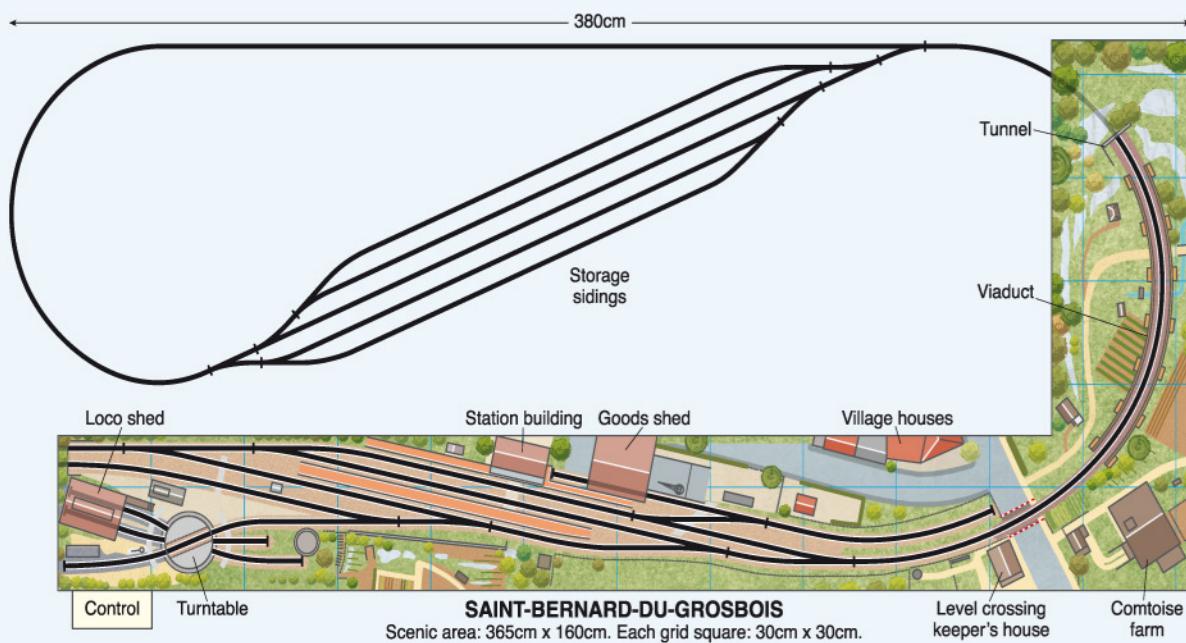
This attractive layout is fictitious but largely inspired by the Paris-Lyon-Méditerranée stations of Lods (a terminus) and l'Hôpital-du-Grosbois on the Loue Valley line in Haut-Doubs, in the Doubs department in eastern France. The chosen period is the 1950s-1960s. The layout has been shown at around thirty exhibitions in France over the past five years, culminating in major events such as Meursault (December 2022), Chambéry (October 2023) and even Intermodellbau in Dortmund (April 2024).

The layout measures just 4m x 1.6m and represents a single track standard gauge secondary line leading to the terminus of Saint-Bernard-du-Grosbois.

The location

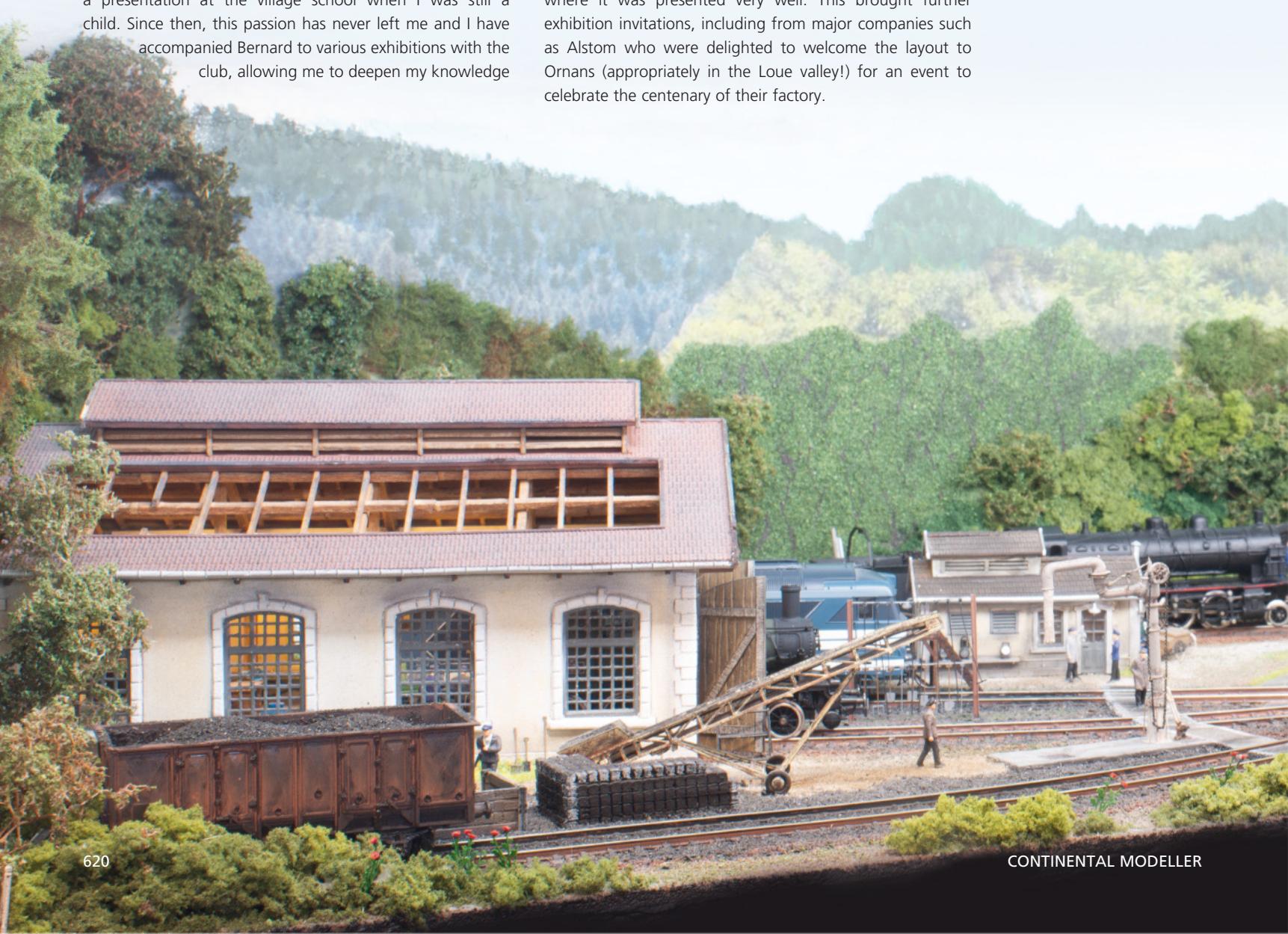
The name comes first from the l'Hôpital-du-Grosbois which inspired the model and whose main elements (station building, goods shed, loco shed and turntable, as well as the track plan) were arranged to provide diverse and varied operation. The 'du-Grosbois' part of the name was retained to be reminiscent of a country village.





'Saint-Bernard' comes from Bernard Sauvaget, who I think of as my mentor. This magnanimous man was a talented modeller recognised in the Doubs department and far beyond. A founding member of the Rail Miniature 25 club, he passed his passion for trains on to me during a presentation at the village school when I was still a child. Since then, this passion has never left me and I have accompanied Bernard to various exhibitions with the club, allowing me to deepen my knowledge

and improve my techniques. Following his unexpected death in 2018, his wife asked me to replace him at an exhibition in which he was due to participate. So in October 2018 *Saint-Bernard-du-Grosbois* had its first outing. It was successful and was featured in the local newspaper (*Est Républicain*) where it was presented very well. This brought further exhibition invitations, including from major companies such as Alstom who were delighted to welcome the layout to Ornans (appropriately in the Loue valley!) for an event to celebrate the centenary of their factory.





Track and the turntable

The track is Roco-Line, painted rust-coloured, ballasted, and weathered. The point check rails were painted white, characteristic for the time.

I converted a Fleischmann N turntable to HO. Several decades ago Fleischmann did produce a small diameter turntable (with a 17m bridge suitable for secondary depots, about 20cm in HO) but this is now difficult to find.

The N gauge bridge translates to around 22m long in HO, typical of secondary lines. I added a deck, the track, and a purple square stop signal.

Tortoise motors ensure slow movement of the point blades in the station. The points behind the scenes are operated by MTB motors because the modules are not as deep (to allow easier storage and transport in a private car).

Above
A short goods train is being assembled in one of the platform roads.



Above
Now 141TA308 is on the turntable while BB66105 is stabled by water tower. The layout can accommodate stock from different periods.



Scenery and structures

To make the bushes and small trees, I used spirea branches and 'sea foam', covered with Busch foliage and natural lichen for the small vegetation.

The station building, the goods shed, and the level crossing keeper's house were made from Bois Modélisme kits. The locomotive shed, the village houses, and the Comtoise farm with its *tuyé* (large chimney used for salting and smoking

Above
The diesel railcar with a four-wheel coach as a trailer is of German origin rather than a true French *autorail*.

Left
The modest station building, deemed ample for the traffic on this rural branch.



Right
030TU3, one of the many American-built switchers which arrived with the liberating forces, waits on the headshunt accessing the goods shed. A village festival takes place around the wine press.



Above
050TA22 has left the depot and is ready to depart with the short goods train. Beyond we can glimpse a Renault ABJ autorail.

hams and Morteau sausages, which is very typical of the Haut-Doubs region) and its *méchoui* (a functioning spit – all that is missing is the smell!) were all scratchbuilt.

In the village square, you can admire the festival around the wine press and the still. You can also see a multitude of small activities carried out by the peasants of yesteryear.

On leaving the station and before entering the tunnel which leads to the storage sidings behind the scenes, trains cross a valley on a long curved viaduct.

I designed this myself and had it laser cut by Bois Modélisme. It is surrounded by scenes of rural life – ploughing the fields, growing vines, etc.





Stock

The rolling stock comes from most of the major brands, mainly Fleischmann, Roco, REE, and Jouef.

Most trains are hauled by tank locos which can be turned on the turntable in order to return with the smokebox facing forward. However, if a larger (tender) loco is employed, it can leave tender first.

How the layout works

The layout consists of the terminus station connected to a return loop and five storage sidings behind the scenes. These work in an automatic sequence. The station is controlled manually (analogue DC) with Gaugemaster controllers – they give perfect slow running, close to real in my opinion.

To facilitate uncoupling, I use Kadee couplers – a 'must' for this style of operation – with magnets judiciously placed under the track. Stock with NEM coupler pockets is easily converted.

The mechanical signals are from the Spanish manufacturer Raphaël Sanchez and I made them functional to regulate the traffic realistically. Their operation also adds interest for the viewers.

Above
The goods train departing. The crossing keeper has already opened the barriers!





Left

A large Comtoise farmhouse occupies a prominent place on the front right corner, with many local details.

Right

A train of old German clerestory roof compartment coaches on the viaduct hauled by a venerable 030T.



Below left

The rolling barriers at the level crossing move but are not interlocked with train movements.

Below

The other side of the farm, with a goods train leaving, just coming onto the viaduct.





When a train arrives at the station (either track 1 or 2), the loco (usually steam) is uncoupled, pulls forward, reverses onto track 3, then runs to the shed. There, it passes successively through the pit to empty the ash and clean the smokebox, the turntable, the coal yard, and the water crane to refuel.

The water crane is operated manually to show how it was done, and the whole sequence is explained to the viewers as it happens. The loco thus prepared returns to the station to the limit of shunt (LM) sign located just before the level crossing. This sign protects the level crossing and allows it to remain open despite the arrival of the loco because it does not pass the sign and cross the road. The level crossing barriers only close when a train enters or leaves the station.

In addition to the loco preparation manoeuvres, the station includes a loop so goods trains can be broken up and re-assembled with wagons shunted to and from the goods shed, often using a small shunting loco.

During these 'performances', I share my passion for the rural railway theme in the steam era – with stationmaster's whistle in hand and cap on head!



Helge Scholz provides the background to an impressive model.

Photographs as credited.

Greifenbachtalviadukt

The largest narrow gauge viaduct in Saxony

The largest bridge on the narrow gauge in Saxony (*De gruße Brick* in the local dialect) was 180m long and spanned the Greifenbach Valley near Geyer at a height of 36m. It was first used in timetabled service on 1st May 1906.

After the first section of the Schönfeld – Meinersdorf (SM) line was opened to Geyer in 1888, the section on to Thum had to wait another eighteen years before operations began.

The Keller & Hildebrand company from Großluga (now Dresden-Niedersedlitz) built this bridge at kilometre 10.661. Work began in 1904 with the clearing of areas on both sides of the stream. After the space had been created for building, the abutments were built first on the Geyer side and later on the Thum side of the valley, plus sixteen concrete base foundations for the four lattice pillars and four platform foundations for the cantilever supports of the two plate girders from the abutments to the first lattice pillar in each case.

The construction of the four lattice pillars and two cantilever supports began in April 1905. The long Erzgebirge winter of 1904/05 did not allow an earlier start. After the lattice pillars and cantilever supports had been erected, the auxiliary structures were begun in mid-1905. The whole structure was riveted together. Timber for the scaffolding did not have to be transported to the construction site but was felled locally.

To deliver the components, the builders used the track that had already been laid from Geyer to the site, on which construction trains were propelled to around 100m from the Geyer end bridgehead.

The last task before commissioning was the completion of the wind protection fence made of wooden slats on an iron frame, including four openings on both sides. The clearance between the fences was designed to allow the operation of standard gauge wagons on transporters.

Right

The construction of the fishbelly girders is under way. Auxiliary structures can be seen between the lattice pillars. The first fishbelly girder has already been started.

In the middle of the picture is the Greifenbach, which is small enough to jump over but a larger obstacle for the railway.

Photo: H.Scholz collection.



Above

After clearing the ground, work began in 1904 with the pouring of the base foundations under the abutment. This is a view over the formwork from the Thum side across to Geyer, where the concrete bases and the first pillar have already been installed.

Photo: Häupel collection.



Right

As the last step in the approval to operate, the bridge was subjected to a load test on 19th March 1906 with a IVK (weight c.29t), a IK (16t), and the bridge test car weighing 6.3t when loaded.

The bridge load test was carried out on 19th and 20th March 1906. The permissible deflection of the three fish-belly girders and two plate girders was measured using the load of IK No.24, IVK No.140, and the ballasted bridge test car K.1. The bridge was designed for a load-bearing capacity of 7.25t/m. The components weighed a total of 276t, and construction costs amounted to 130,109 Marks.

Freight traffic in particular developed positively with the change from transporter bogies (*Rollböcke*) to platform wagons (*Rollwagen*). The infrastructure had to keep up with the higher loads caused by transporting wagons with increasingly heavy loads. Upgrading the track to take higher loads was one thing, but enabling the civil engineering structures to follow suit was another matter. The bridge had to be upgraded in 1927 to handle the higher loads. For this, a construction site with an office and stores buildings was set up in the valley. There were three significant changes. The two plate girders received a second support. This required the construction of eight new base foundations. The next step was to provide the lattice pillars with additional bracing. The two central pillars were given a vertical strut in the form of a ladder in the middle on both sides of the head ends. This strut was only installed on the outer lattice pillars on the valley side. Internal supports were riveted into the struts of the three fish-belly supports, which also guaranteed more stability.

Right

After opening, the bridge became a popular tourist destination and was depicted on several postcards.

Below

The structure was repainted once before it was reinforced.

At that time, occupational safety was not a major issue.

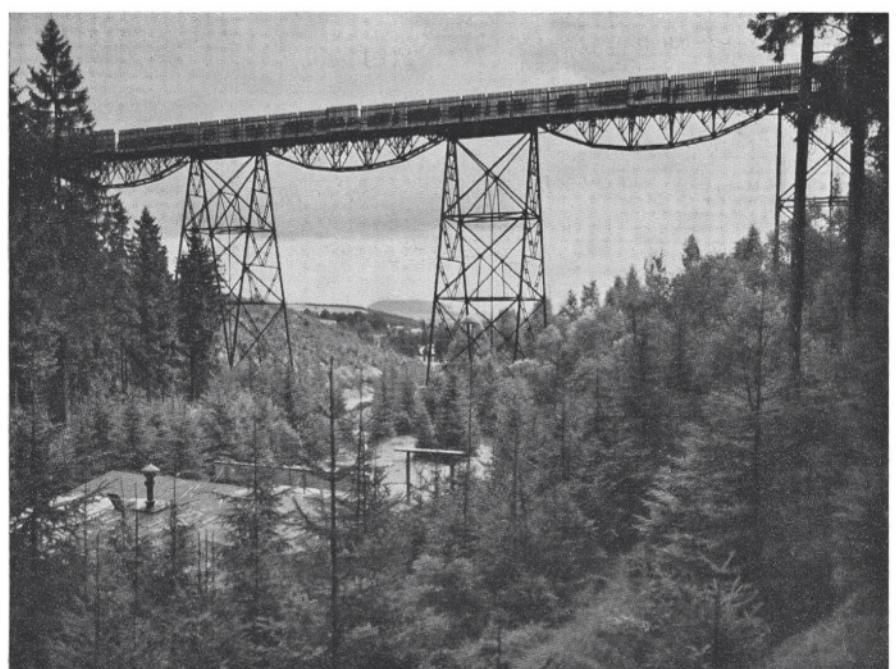
Rungs on the corners of the pillars made it possible to climb up.

Right

A building site was set up in the valley for the reconstruction.

The lattice pillars do not yet have the vertical central struts and so the picture can be dated to the start of work in 1927.

Photos (4): H.Scholz collection.





Above

Train P3008 from Thum to Geyer passing over the bridge. The windbreak fence only ever allowed a silhouette view. Photo: Günter Meyer.

Below

The last passenger train to Thum on the Greifenbachtal bridge on 15th August 1967. The moment was captured by many photographers on special train 03016. Photo: Günter Meyer.

Nothing more changed until the line between Schönfeld and Thum was closed on 15th August 1967. Then the bridge, with an estimated value of around 1.18m Marks, fell into a deep sleep but was not dismantled.

On 13th June 1970, 2-10-2T 99 1777-4 was the last loco to cross the bridge from Thum to Geyer (and later back) with the stock intended for the Geyer museum display. The loco pulled two coaches and pushed a wagon in front as protection. Members of the model railway groups from Geyer and Crottendorf travelled in the coaches.

The bridge was only used for those two crossings. In 1971/72 the track on most of the line was dismantled but left on the bridge.

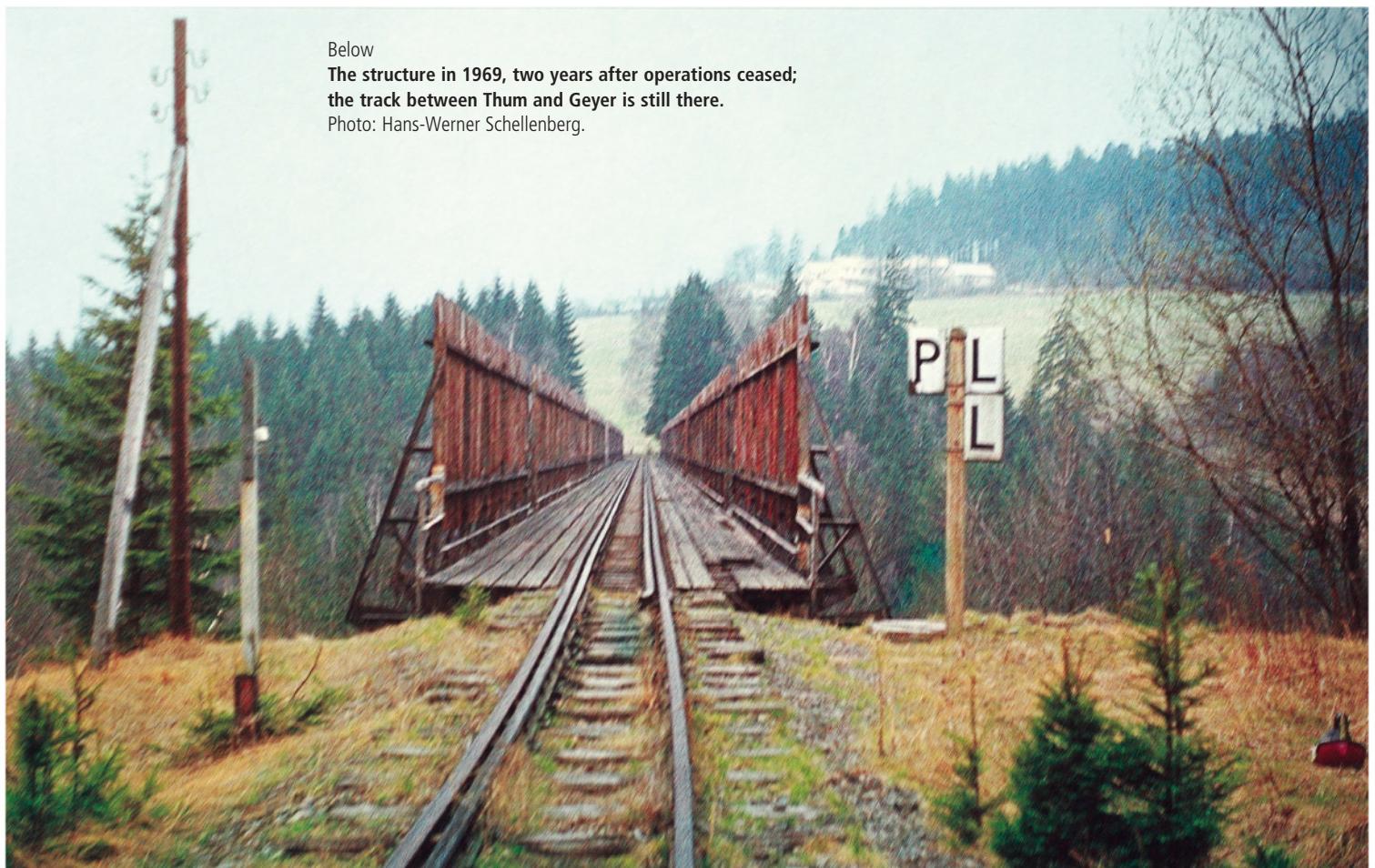
On 6th October 1977 the bridge was demolished. Targeted cutting separated and weakened the structure so that it could be moved with a bulldozer and scrapped on site. Today only the concrete base foundations in the valley bear witness to the bridge.



Left and below

Two detailed views of the lattice pillars. On the left, a view along the line through the two outer supports to the lattice pillars. Below, one of the support feet with an adjustment bolt. All supports and pillar feet had these on both sides for possible tilt correction. Photos (2): Hans-Werner Schellenberg.



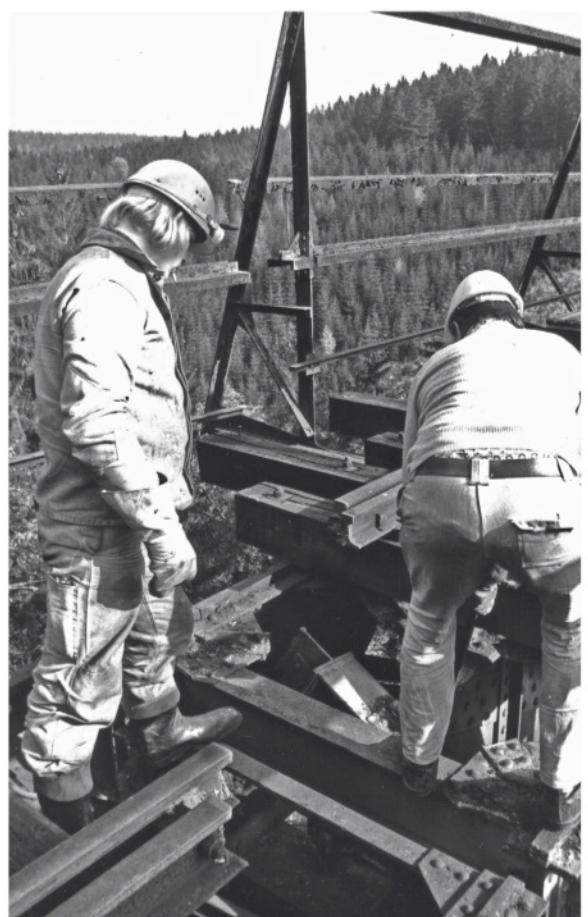


Below
**The structure in 1969, two years after operations ceased;
 the track between Thum and Geyer is still there.**
 Photo: Hans-Werner Schellenberg.

Below
A view of the bridge from the museum stock transfer train on 13th June 1970.
 Photo: Siegfried Bergelt.

Right
 The demolition work begins at the start of October 1977. First, the superstructure and the track that was still there were separated. You can see the separation of the railings at the transition point between the plate girders and the girder on the lattice pillar top.
 Behind this is a wind fence support. All pillar bases were also cut.

Photo: D.Hertzsch.





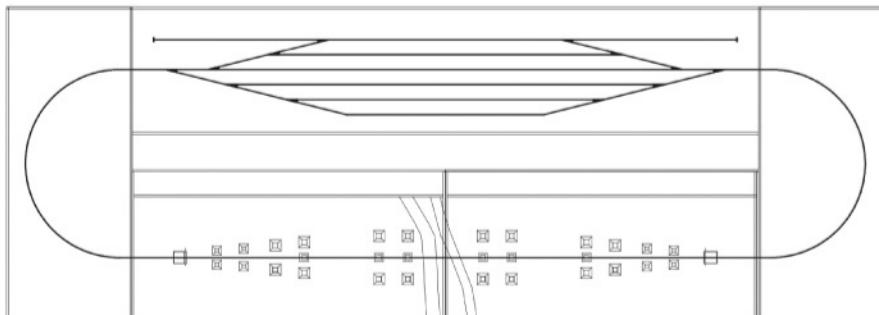
The late **Jochen Klinger** and **Jens Petermann** created a super display showing what was once the highest narrow gauge railway bridge in Saxony. *Report and photographs by Helge Scholz.*

Greifenbachthalbrücke

A masterpiece in 1:87

The well-known modeller of the Saxon narrow gauge Jens Petermann got to know Siegfried Seidl and Jochen Klinger when they exhibited their HOe layout of Ehrenfriedersdorf station in the town hall of that mountain town in the Erzgebirge where they lived. They talked about the modelling possibilities of Thum network. One possible project was the Greifenbachthal bridge, but Jochen Klinger only saw the difficulty – not the structure itself, but the many trees! Master tree modeller Jens responded that that was the least of the problems, and so a firm modelling friendship was formed.





Above left and left

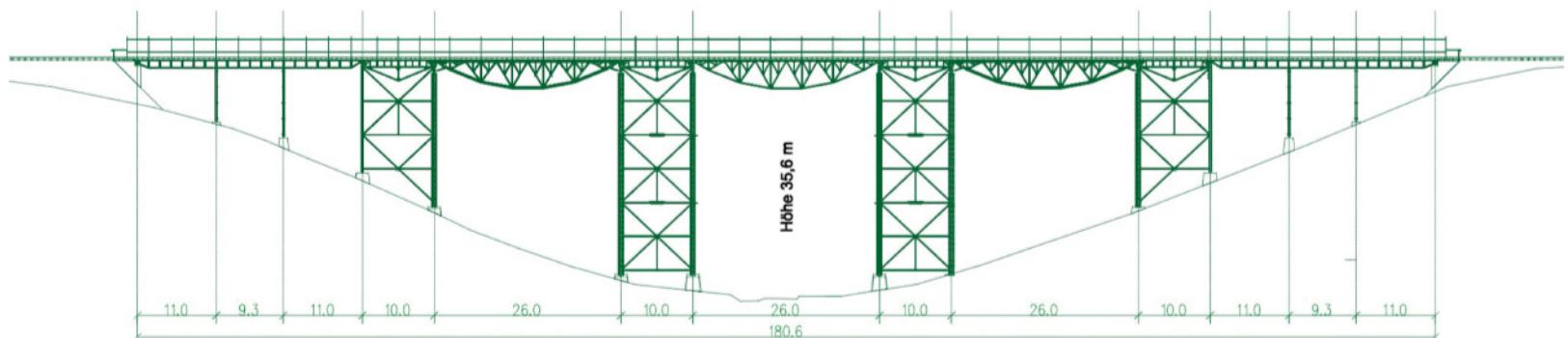
Neubau 2-10-2T 99 1777-4 hauling a mixed train with transporter wagons across the bridge in the direction of Geyer. In the model, part of the wind screens on the bridge was left open to provide a better view of the passing trains.

Above

The same loco with a return working, a similar mixed consist.

They intended to model the Greifenbachthal bridge exactly to 1:87. Jochen had already worked intensively on and perfected the drawing of files for laser cutting components for the *Schützhausbrücke Ehrenfriedersdorf* HOe layout, so this technology was chosen to produce the parts. In weeks of work, Jochen developed the specifications using an architectural drawing program based on a surviving (but unfortunately not reproducible) diagram made when the bridge was reinforced, in conjunction with as many photos as he could collect. Lasercut Hofmann of Chemnitz cut everything out of 0.25mm thick cardboard, and the parts turned out to be a perfect fit with almost no tolerances.

The parts supplied were assembled with superglue. Siegfried Seidel made metal jigs for some assembly steps. This made work easier, for example when attaching the windbreak fence supports and aligning other assemblies.







It took around half a year to assemble the viaduct. In parallel, Jens Petermann developed the diorama. The structure is divisible so it can be transported, with boxed return curves and storage sidings behind which incorporate the background scenery.

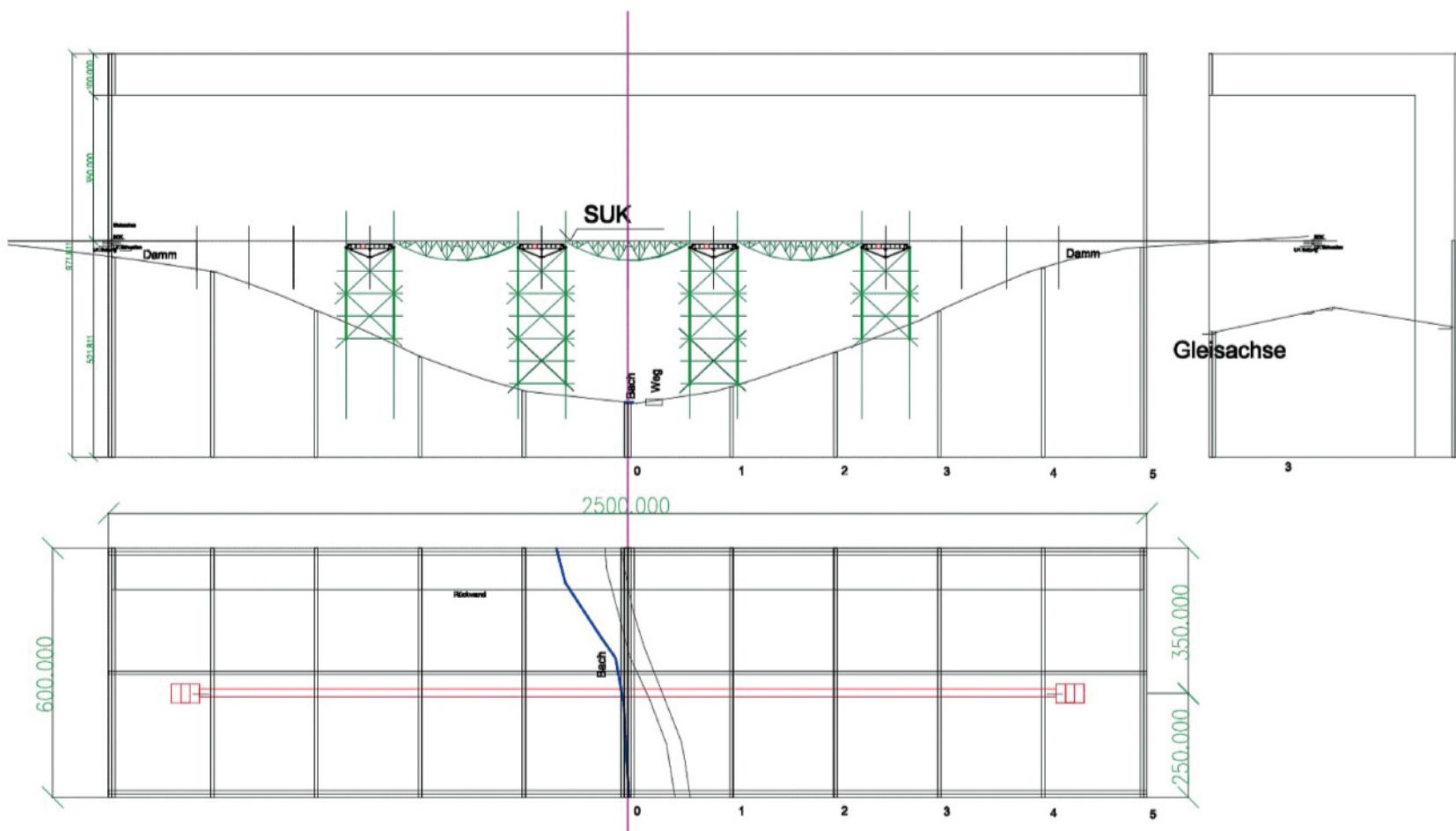
The diorama has lighting (with day/evening effects) built in to the cover. All told, a perfectly designed and well constructed bit of work which is suitably robust.

The foundations and pier supports are moulded parts inserted into the base. The terrain was made with Styrodur foam and the abutments were inserted.

Then the stream was poured and the empty slopes of the valley were planted – there are 270 trees in the background. After the bridge was assembled, work began on positioning the foreground trees and completing the landscaping with small trees and shrubbery.

The last step was to connect the rails on the bridge deck, held with Künzel brass type K rail fixings, to the approach tracks either side. With that the wonder was completed.

Without a doubt a masterpiece.







It was clear to the builders from the start that operation would be limited to passing trains. But even if no crossings or connecting operations are possible, the view of trains rolling over the structure is impressive.

By adopting the standard modular construction, the bridge can also be incorporated into the Bimmelbahner HOe modular layout.

Above right

The gaps between the re-growing spruce trees allow glimpses of the trains, as in reality.

Below

A family excursion on the forest path alongside the Greifenbach itself, far below the railway.

Below right

Another train, headed by 2-10-2T 99 1787-3. The locos are Bemo 'Metal Collection' models.



David George eventually created an exhibition layout that he had promised never to build ...

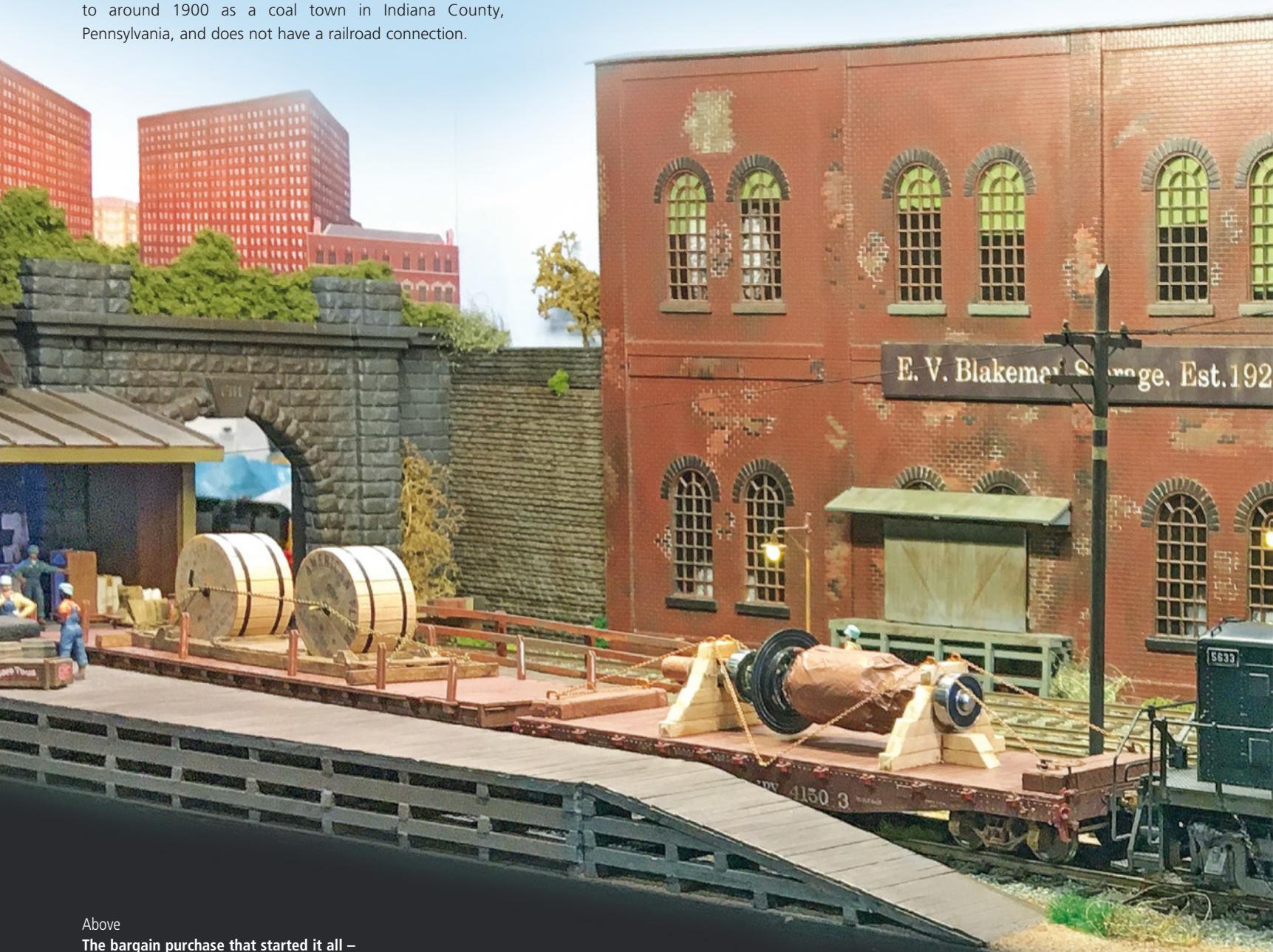
Arcadia PA Terminal

A fictitious short line freight yard

Location and layout background

You may well ask why this layout is called *Arcadia Pennsylvania Terminal*, especially given that Arcadia is a small township of approximately one hundred people, going back to around 1900 as a coal town in Indiana County, Pennsylvania, and does not have a railroad connection.

The layout was never intended but in early 2016, during a visit to my local model shop, Arcadia Models, I happened to see an immaculate Atlas O Pennsylvania ALCO RS-1 diesel locomotive on the so-called 'bargains table'.



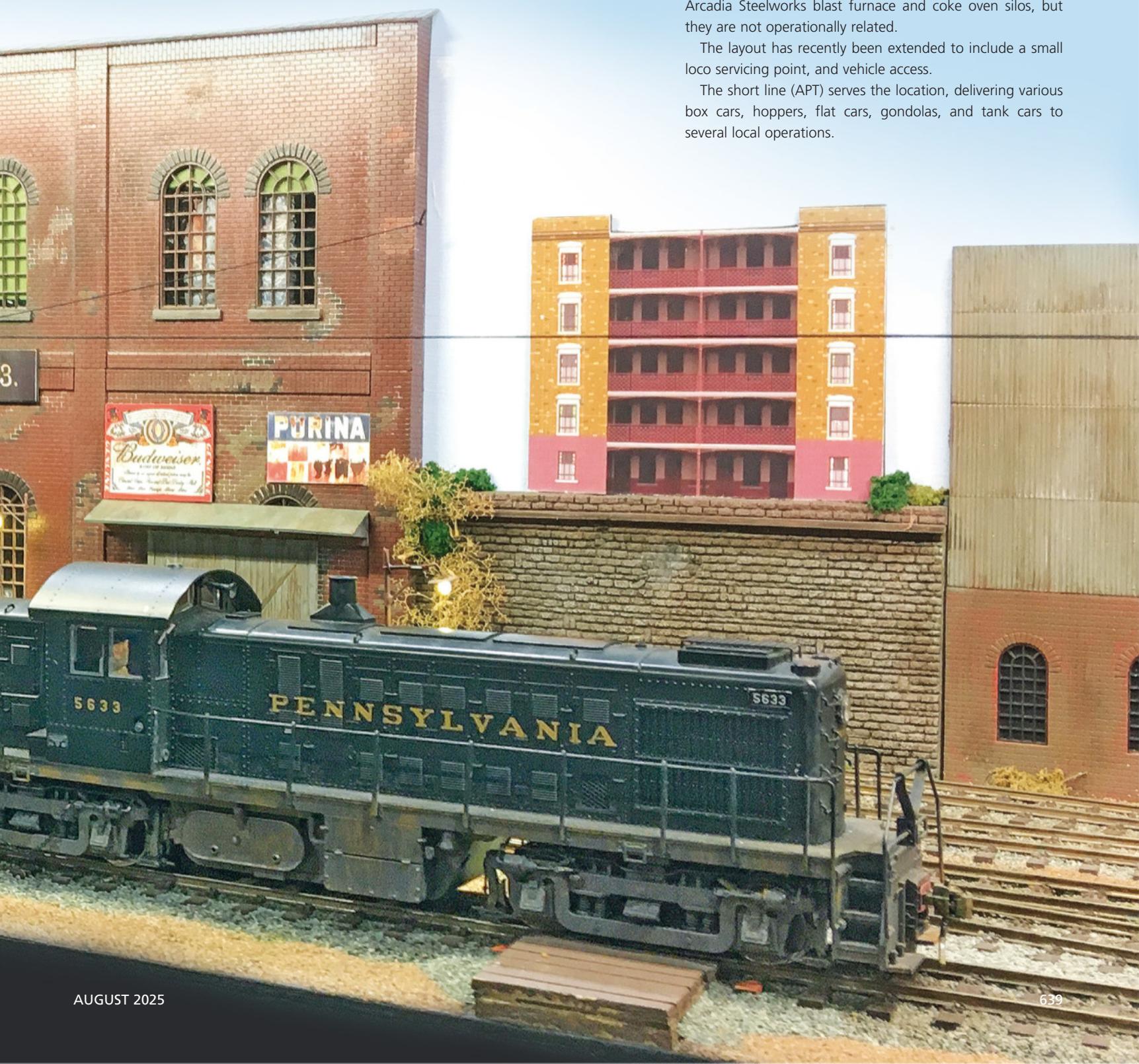
Above

The bargain purchase that started it all –
an Atlas ALCO RS-1 lettered for the Pennsylvania RR –
spots a couple of interestingly loaded flat cars at the loading dock.

Photographs by the editor, unless otherwise noted.

At the time, my main interest was focused on European HO/H0m and American HO, and the gradually improving British OO stock was also beginning to appeal. So there was absolutely no interest haunting me about O. Indeed, my wife, who jointly operates at shows, said: "No way, we cannot have another layout, especially one in that scale". Being sensible, I duly promised the ALCO would never be more than an interesting feature to display on a shelf, on a piece of track. Well, history has proved me unreliable and wrong!

Four months later, at the Perth show, I saw but resisted the opportunity of acquiring an Atlas CNJ GM-EMD SW9 switcher. After spending a great day at the exhibition, we began to make our way to leave. Alas, the lure of O again proved too strong and before we left it was in the bag!



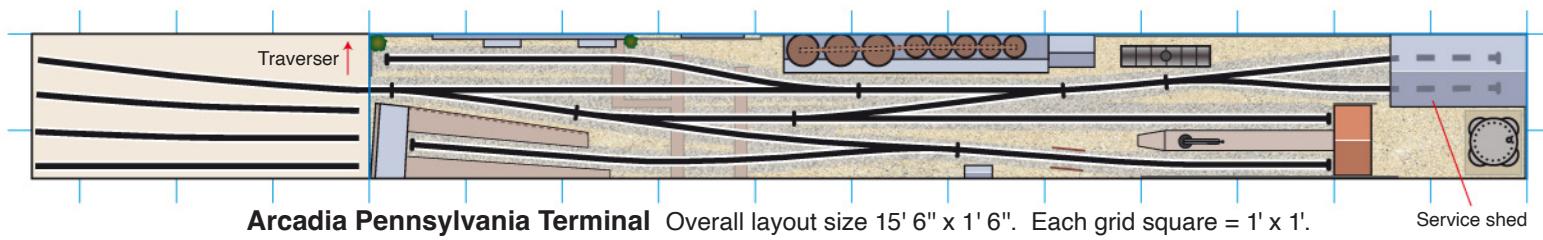
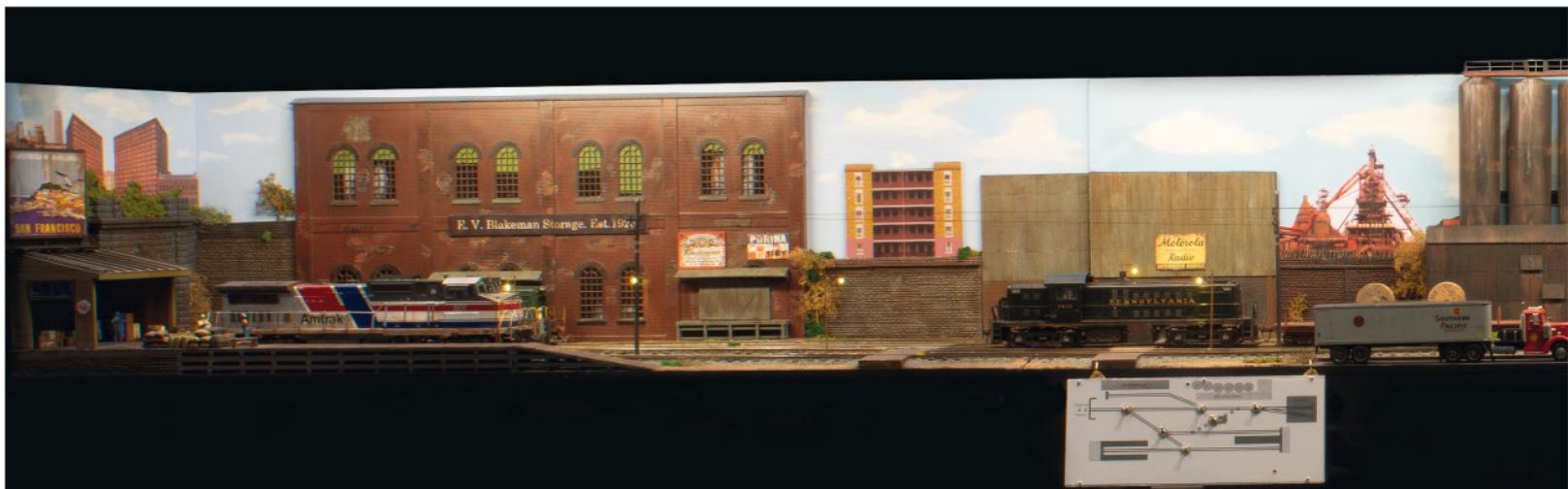
So there were now two excellent, high quality, heavy, and appealing models to admire on the shelf, plus some second-hand box cars quietly stored in a cupboard.

So, yes, it was inevitable! In late 2016, I started to build baseboards and in mid-2017 a work-in-progress layout was presented at the Trent Valley North American show, TVNAM, in Armitage, Staffordshire. As you may have already deduced by now, the layout name was formed from the combination of model shop and the ALCO's railroad. A year on, I spotted a Louisville & Nashville GM-EMD SD35 and an Amtrak GE Dash 8-32BWH; both are Atlas models, and it was clear that they had to have a home to be exhibited at shows.

The layout is freelance, a boxed theatre style 'time-saver' that depicts a fictitious small industrial railroad freight terminal. The terminal has, as a backdrop, the declining Arcadia Steelworks blast furnace and coke oven silos, but they are not operationally related.

The layout has recently been extended to include a small loco servicing point, and vehicle access.

The short line (APT) serves the location, delivering various box cars, hoppers, flat cars, gondolas, and tank cars to several local operations.



Motive power, rolling stock, and architecture represents the 1960s to 1970s period but there may be cameo appearances by late 1980s diesels.

The freight yard's resident loco is a leased Central Railroad of New Jersey GM-EMD SW9 #1089 (to be branded APT). In addition, other motive power includes a 'Pennsy' ALCO RS-1 #5633, a Louisville & Nashville GM-EMD SDP35, and an Amtrak GE Dash 8-32BHW – these three are dual-motored, with 'China-drives'.

Baseboard

The layout is 15'6" by 18" and is formed with three 4' boards and a 3'6" four-track traverser. Backscenes are 15" high.

As it was an O gauge layout, I used 12mm plywood, but it gradually dawned on me that my 'Victorian' structure approach was unnecessary. I then resorted to drilling 50mm diameter holes along the board sides to reduce the weight for transport to shows.

Board alignment is achieved using disc-type 25mm brass pattern makers' dowel sets, with baseboards held together with latch-type toggle clamps.

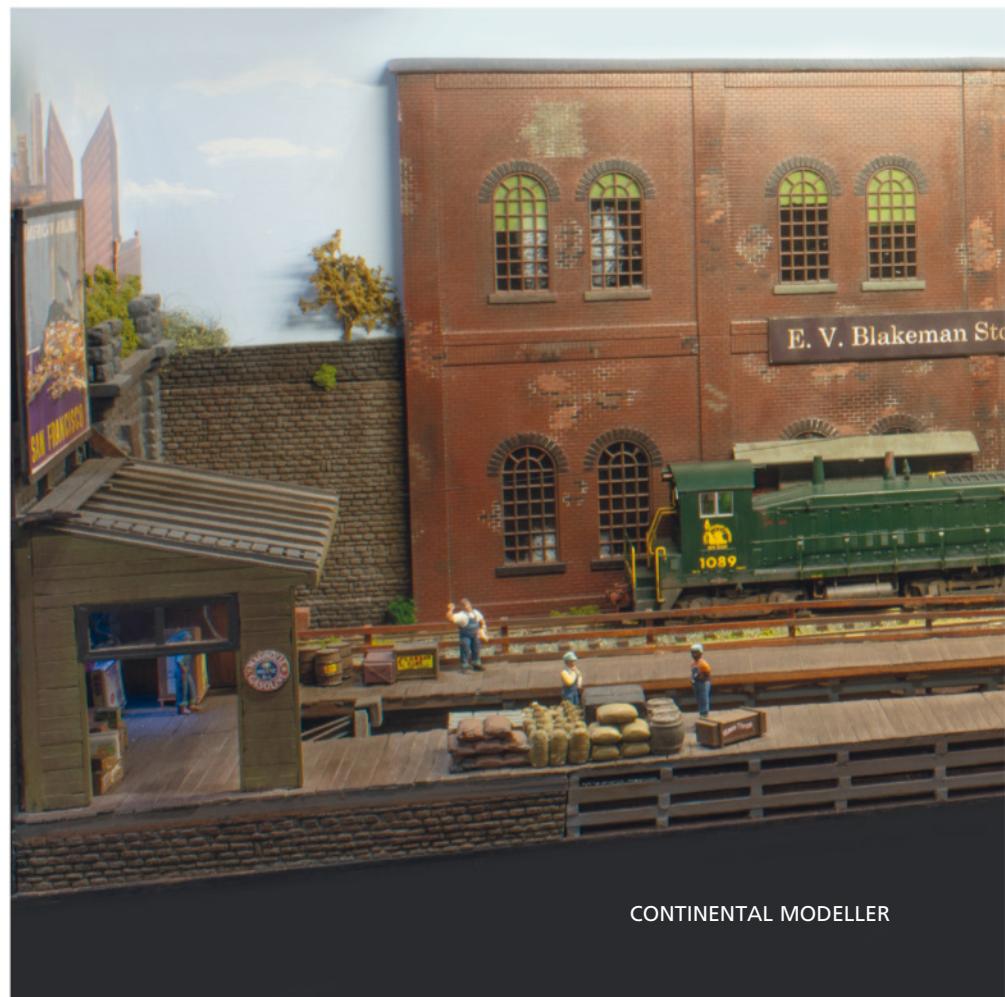
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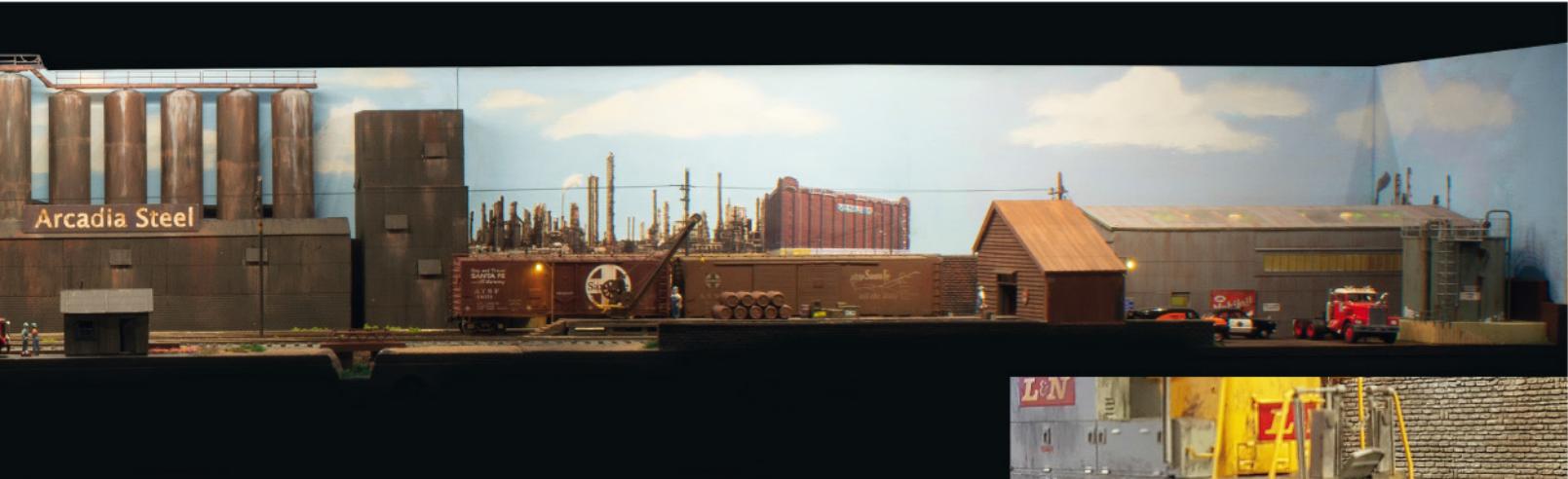
One of the baseboards.
Note the holes (to save weight)
and the alignment dowels.
Photo: author.



Track plan and operation

The track and operational design are loosely based on a 'timesaver' layout, which serves as a switching puzzle. Various websites explain the concept (for example, www.wymann.info/ShuntingPuzzles/sw-timesaver.html). Our layout has been an extended with the benefit of two extra points to allow for an extra siding and access to the service shed.





Above
The whole of the scenic area.

Right
Small red squares indicate the location of the magnets which work as uncouplers.
 Photo: author.

Below
GM-EMD SW9 CNJ #1089 waits on the Blakeman spur.

The layout is mainly operated from the front, with traffic generated from the four-track traverser through a short tunnel into the freight yard area. This is a flat yard, i.e. no humps.

All four of the Atlas locos have been DCC sound fitted.

The layout is powered by an expanded NCE 5-amp booster system and power is also protected with an NCE 3-second circuit breaker.

Track and point control

I decided to use Peco Code 143 flat bottom track with medium radius points (c.16.5" long). This was a compromise to avoid expensive American track, which was over twice the price. It delivers what is needed, but I accept it is not for the purists. I have considered using Atlas track in future.

The points are controlled by a combination of Tortoise and Cobalt slow-motion motors. They work well together, although I found Tortoise motors particularly smooth and quiet.



The yard route selection is based on a simple diagrammatic control panel, to manage the seven points with toggle switches.

Uncoupling is achieved by using Kadee #811 magnet blocks laid between the tracks, with yard workers' crossings made of beechwood strips placed either side. The blocks are then painted and ink-marked to match the beechwood strips. A small red square is then painted on the adjacent sleepers to denote the uncoupler section, and to avoid implying that other crossings have uncouplers.





Left
PRR RS-1 #5633 spots
box cars at Blakeman's.

Right
CNJ SW9 #1089 carefully
positions a tank car.

Below
The points control panel
is separate, hung on hooks,
and connected to the layout
by a multi-core cable.



Kadee couplers are used throughout on the locos and rolling stock – I find them excellent to work with. Some have Kadee #745/6/7s series (plastic draft gear box) units, but the majority are Kadee #805 (metal box) couplings by preference.

Freight yard structures

The E.V.Bakeman warehouse is a kit-bashed low relief structure created from an old British O scale steam shed kit.



The walls were merged to form a two-storey building with unloading bays. Most of the work was focused on painting and highlighting brickwork, to represent repointing activity, typically found on old/neglected buildings.

There are three other freight platform locations that serve local businesses. The loading platform bases were created with $\frac{1}{4}$ " square beechwood strips with the platform floor formed by using coffee stirrers. These were all then stained and weathered.

The 2-ton yard crane was assembled from a whitemetal Skytrex kit and weathered.

Arcadia Steel was based on elements of steelworks found along Pennsylvania's Monongahela River. These were typified by Scottish emigrant Andrew Carnegie's 'Carnegie Steel Company' and subsequently the US Steel Duquesne blast furnaces. They provided the inspiration for the layout background, of a defunct industry, by focusing on now old and dilapidated coke oven silos, fronting rusting blast furnace background images.

The silos were created using a section of drainpipe cut into appropriate lengths, filled with foam, and capped with Woodland Scenics plaster. The silo domes were shaped by using a sliced-off section of a toilet system plastic ballcock!

Like virtually all of the buildings, the loco servicing shed is a scratchbuilt two-track facility adjacent to a diesel fuelling point. The large storage tank is a 'Made in Manchester' structure that added further detail.

Telegraph poles and wires are made of dowel and black elastic cotton.

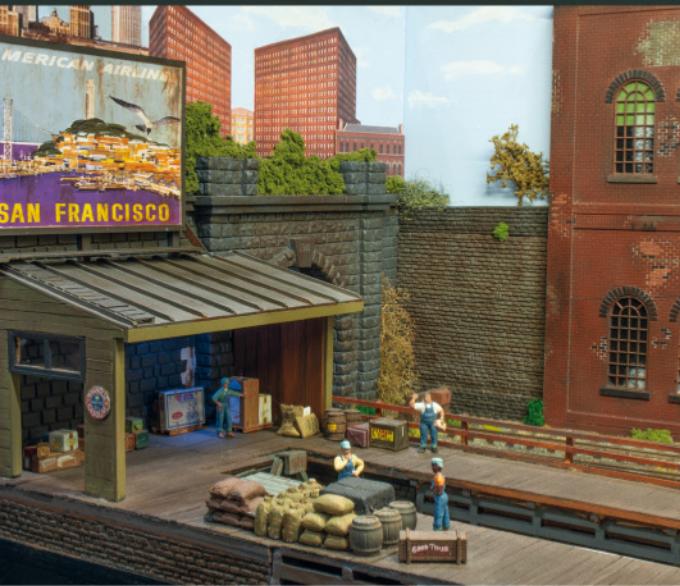
Background scenery

Images of oil refineries were used to portray additional industry; they were printed on 200gm card and the individual towers carefully cut out to provide a degree of detail and relief to the scenes.

American railroads

To put APT into perspective, there are around seven hundred freight railroads in the United States, and they are assigned to Class I (National), Class II (Regional), or Class III (local), according to their annual revenue. There are just six Class I companies: BNSF Railway, CSX Transportation, Canadian National Railway, Canadian Pacific, Kansas City, Norfolk Southern Railway, and Union Pacific Railroad.

Class II comprises 21 companies, and there are over 600 in Class III. APT is presented as being in the Class III category.



Above
The tunnel to the traverser is well screened by the lean-to freight shed.

Left
Just delivered or waiting to be loaded?

Below
Box cars form the bulk of the freight car fleet.

Motive power roster

The layout still has just four locomotives, all Atlas models with die-cast steel chassis, truck side frames, and fuel tanks. All were acquired secondhand, in pristine condition; they appeared to have never been used. All are now DCC sound enabled and controlled using NCE PowerCab and ProCab handsets.

ALCO (American Locomotive Company) RS-1, 1,000hp, built between 1941 and 1960. #5633 is twin motored, with a 'China drive' on each truck. 21-pin ESU LokSound V5 L decoder. Weight 5lbs.





GM-EMD (General Motors – Electro Motive Division) 1,200hp SW9 #1089. These switchers were produced from 1950 to 1966 with only minor detail changes, and used by virtually every major railroad in the US and Canada. The model has a dual flywheel equipped precision five-pole can motor and NCE D408SR motion decoder, with sound provided by a 'piggy-back' MRC Sounder chip – as basic a sound installation as you can get. Weight c.4.5lbs.

GM-EMD SD35 2,500hp six-axle road switcher, with 360 built between 1964 and 1966. The model has a 'China drive' on each truck and is managed by an ESU LokPilot V5 L 21-pin decoder, again with a 'piggy-back' MRC Sounder chip. Weight is 6lbs.

GE (General Electric) Dash 8-32BHW 3,200hp built in the late 1980s, with some still in service today, although relegated to yard duties. The model has twin 'China drives' with a QSI Sound decoder delivering excellent sound and control. Weight is c.6lbs.

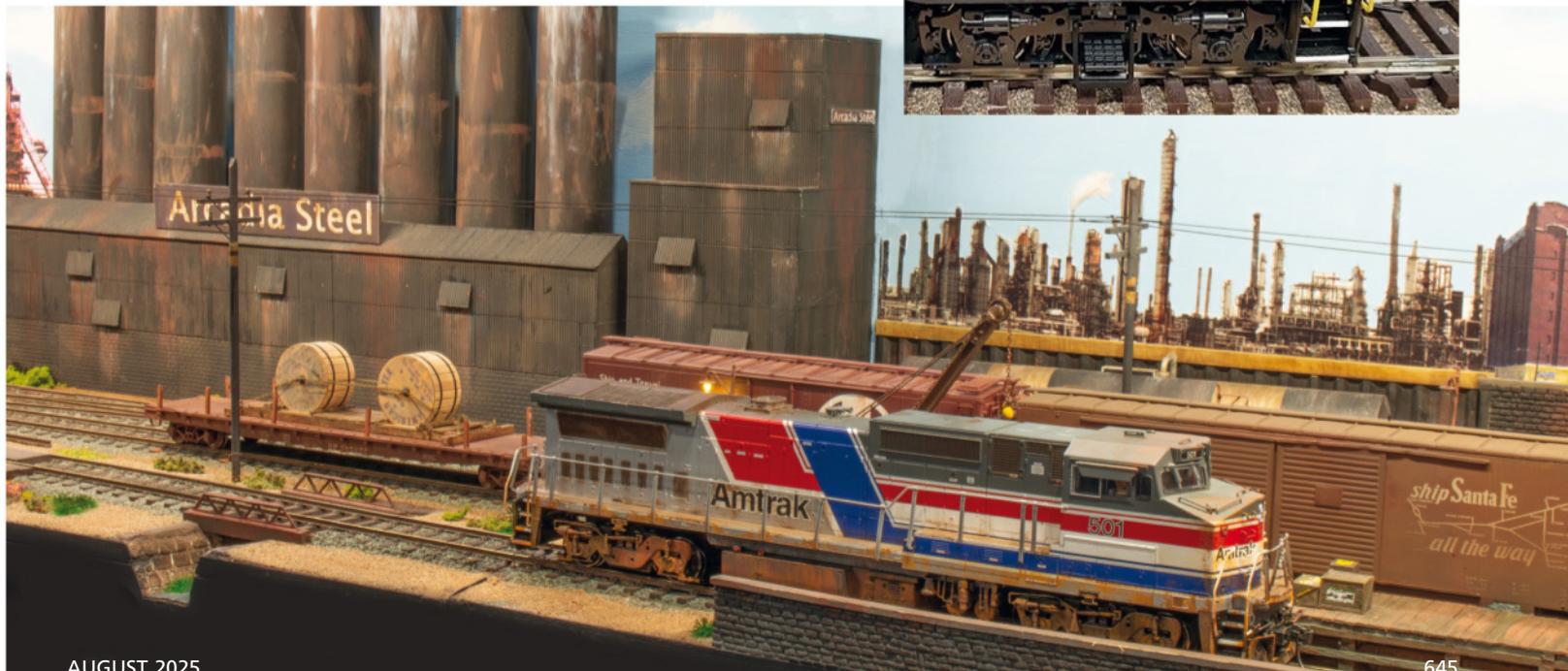
Above
The wooden foot crossings for yard workers conceal the uncoupling magnets.

Right
An Atlas 'China' drive self-contained power truck.
 Photo: author.

Below
The Amtrak General Electric Dash 8-32BHW is perhaps a little out of place in a small freight yard.

In general is it is best to wire motors on two trucks in series to minimise the competition between the motors, particularly at low speeds, typically in a freight yard.

Discussions with a number of British O scale exhibitors revealed that, in their experience, ESU V5L LokPilot/LokSound decoders were considered very reliable in managing the demands of dual motors. The intention will be to upgrade the SW9 and SD35 decoders.





Rolling stock

The layout uses a small fleet of freight cars, mainly manufactured by Atlas, Weaver, and Red Caboose. As with the locos, all of them were secondhand 'finds'. They are from a host of operators, notably the following:

Box cars: mainly 40' with some 50', representing Santa Fe, L&N, WP, Erie Lackawanna, SP Lines, Rock Island, ATSF, Wabash, B&O Sentinel, and PRR.

There are also some reefers (Union Refrigeration) and stock cars (Union Pacific).

Hoppers: two-bay and center flow, Lehigh Valley and Wabash.

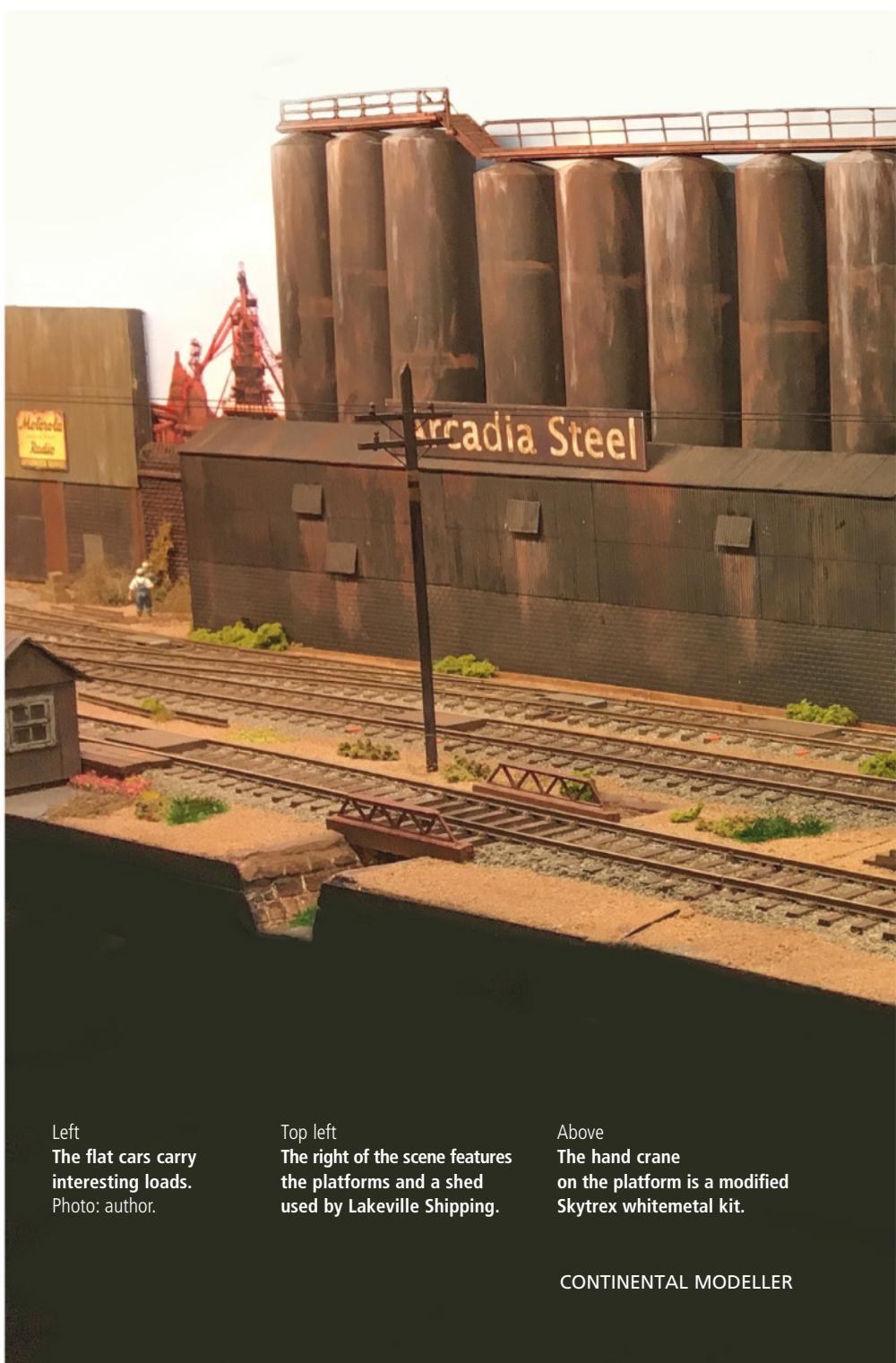
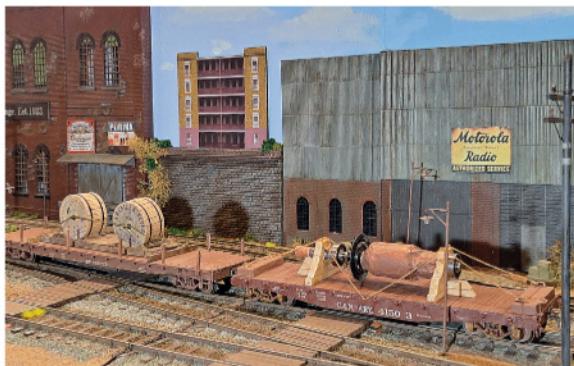
Flat cars: Chicago & North Western and Southern Pacific. These are Weaver kits, adapted to carry hand-built deck frames, to support cable drums (made by Brian Davis of Rainhill MRC) and an electric generator component. (This load was in fact created from my burnt out Dremel armature!)

Tank cars: Red Caboose UTLX 10K fuel tank kit, and a Rollin J. Lobaugh Southern Pacific crude oil brass kit.

Gondolas: so far just an old Burlington Northern 'fishbelly' made from a wooden kit.

The collection is mainly representative of build dates in the 1950s and 1960s, with a just few lingering from the 1930s and 1940s. The Americans do like to sweat their assets, and they add character to the layout.

The stock runs very freely, and it has been necessary to add weights to counter coupler separation when passing slowly over magnets. Consequently, all the cars have been weighted, on average to 400gr.



Left

The flat cars carry interesting loads.
Photo: author.

Top left

The right of the scene features the platforms and a shed used by Lakeville Shipping.

Above

The hand crane on the platform is a modified Skytrex whitemetal kit.

Conclusion

I hope this gives some useful background to *Arcadia Pennsylvania Terminal*.

The development of the layout has been somewhat serendipitous, with periodic enhancements during its seven years of operation, but so far it has proved to be the most popular of our layouts at exhibitions.



Forthcoming exhibitions

Harrogate	August 2025
Fleetwood	September 2025
Leeds	October 2025
Falkirk	November 2025
Preston	March 2026
Leigh	September 2026



Top right

Behind the diesel fuel tanks
is a large shed for servicing
the diesel locomotives.

Norman Raven describes his family's latest exhibition layout.

Megsdorf

Southern Germany in the snow



This layout is tinged with not a little sadness as it was built and named for my wife Margaret (Meg to most of us) who passed away in July last year. Meg was an integral part of our layout building and exhibiting. She loved snow scenes and thus the setting of Megsdorf could not be anything else.

The general picture

The layout depicts a small station and freight yard on a truncated branch line somewhere in southern Germany. An additional source of traffic is provided by a small brewery accessed by a private gated siding.

It is certainly an exercise in minimalism, measuring just 2.1m x 0.4m (approximately 7' x 1'6") on two boards. The main scenic area is a laser-cut plywood canopy board from Grainge & Hodder. The second board provides the storage area and a narrow scenic section incorporating the aforementioned brewery and its dedicated siding. This is a standard 0.9m x 0.4m (approximately 3' x 1'6") flat board again from Grainge & Hodder. I modified this to provide the narrow scenic area and a storage area with a separating backscene.

Below
0-6-0D V60 1218 stands ready to depart from Megsdorf station with the branch train, just one coach and a van.

Photographs by the editor.





The configuration

As can be seen from the track plan, I have adopted the common space-saving ploy of the storage road forming one end of the run round loop. In addition the single track which houses the local service railcar is a simple extension of the bay on the scenic section.

The personnel

As ever, the layout is a joint effort between my self and son Simon, who provides technical expertise and patience! Daughter Melissa offers ideas and encouragement, while the other two members of the group, Arion the horse and Phoebe the dog, just let me get on with it so long as they are fed!

Trackwork

The track is Peco code 100 salvaged from previous layouts. Turnouts are all medium radius electrofrogs operated by MP1 point motors which also change the frog polarity. The motors are operated through Digitrax DS52 decoders.

The track was laid on Copydex glue with the odd track pin to hold any of the tighter curves and the track at the baseboard edges. It was then sprayed with 'Leather Brown' from the Army Painter acrylic range. Ballast is chinchilla dust which is used by those beasties to clean their fur. A £10 bag has already done four layouts (albeit small ones) with plenty left for future use. The ballast is secured with dribbles of

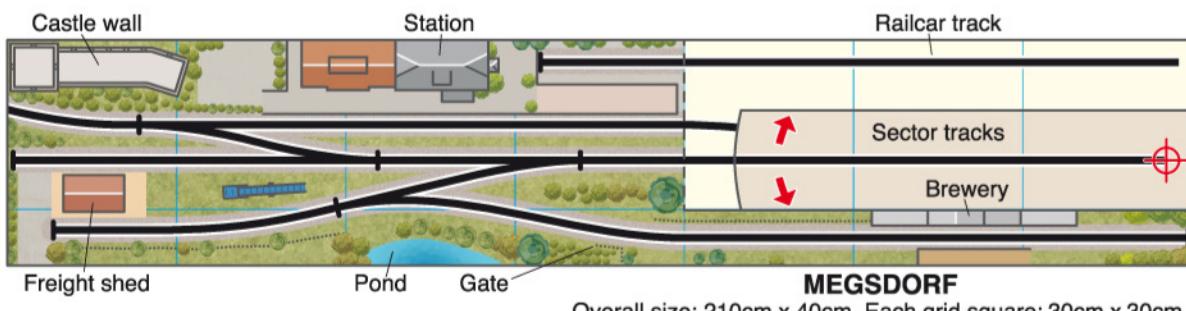
Woodland Scenics 'Scenic Glue' after wetting with the usual spray of a mix of water and a drop or two of washing-up liquid.

Sector track

I have already mentioned that the storage area incorporates a moving sector track to store the next train on scene and to form one end of the run round loop. This is a very simple affair. I think that is the best way to describe it although other less flattering descriptions could be applied! The track itself simply slides on top of the baseboard being secured at one end with a humble track pin and describing an arc at the other end to line up with the two tracks of the loop. That arc is limited by the simple expedient of two wood strips so that pushing and pulling to the limit each way will automatically give the correct alignment. The push/pull action is achieved by a brass rod let into a groove in the baseboard surface. One end terminates in a wooden knob on the layout fascia. The other end is bent up and hooks under a panel pin superglued across the outside of the track sleepers. Hopefully, the accompanying diagrams will show this to better effect. I said it was simple!

This does mean that stock has to be placed and taken off the sector track but the generally slow operation of the layout does not make this too much of a problem even when operating on one's own. The locos are admittedly somewhat delicate but some protection is provided by using Peco loco lifts for their transfer.

Above
The remains of the castle still tower over the village, depicted on the backscene.



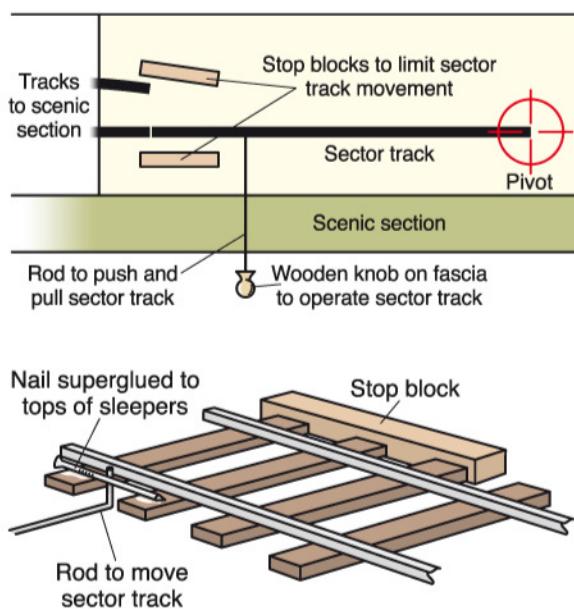


Electrics

Operation is by DCC. The main controller is our ESU ECoS unit. This is connected to the layout and sits on a shelf purpose-built by Simon to include programming tracks in various gauges. The ECoS unit is linked wirelessly to an Edimax router and thence to either a LoDi-Con remote handset or to iPhones or iPads which use the WiThrottle app. Because of the configuration of the canopy baseboards, we have to operate from the front and the wireless system allows us to roam around and chat easily to the spectators.

Scenery

Although this is very obviously a snow scene, the scenery follows well established procedures. Essentially, the scenery is applied as if snow were not present. Ground cover is our usual mix used tea leaves, coloured scatters, and ground foam, the latter two being mainly from Woodland Scenics.



Above
The station building and adjacent goods shed on the fringe of the village.

All this is held in place by the customary wetting spray and 'Scenic Glue', taking care to avoid gumming up the turnouts – the voice of experience! Simon then applies static grass to appropriate locations which gives a pleasing bit of height and texture to the scene.

Larger bushes are ground foam held in place by applications of hot glue, taking care to keep fingers away – again the voice of experience!

I always like to have decent-sized trees on a layout to give more height to the scene and also hide the holes in the sky through which the trains come and go. Very visible holes in the sky is a particular bugbear of mine. (A close second is a very high gloss finish on vehicles.) The trees are a mix of home-made efforts, mainly from sea moss, and excellent specimen examples from Primo Models.

I always aim to have water somewhere on a layout. In this case it is a small duckpond at the front of the scene with the ducks waiting expectantly for food from the dog walker. The water is represented simply by black paint and several coats of gloss varnish.

Snow

The final element of the scenery is the snow itself. This is applied over the existing scenery to give the effect of a fairly normal snowfall rather than a blizzard. White paint is applied where I want the snow to lie, including building roofs. I then sprinkle on a mix of Woodland Scenics 'Snow' bulked out with baby powder and given an extra sparkle with very fine glitter powder. Most of the snow will adhere to the wet paint but I then go over it with the wetting spray and 'Scenic Glue' to fix it place just like the ballast. Where the snow has to be on trees or bushes it is held in place by applications of cheap, sticky hairspray.

Structures

Given the small size of the layout, there are not too many three dimensional buildings. The station is a Faller kit which I made a long time ago for a layout which never materialised.



Left
0-4-0T 'Glaskasten' 98 301
has arrived with two wagons
for the brewery.

Right
Diesel railcar VT75 902
and trailer VB140 042
may look modern
but date from 1932!
The trailer has no driving cab
so the railcar must run round.

Below
The loco may be modern
but the stock is still classic.
Note the animated features
such as the burning bonfire,
the welder by the conveyor,
and the man chopping wood.

Below right
The gate on the siding
serving the brewery
is worked by a servo motor.



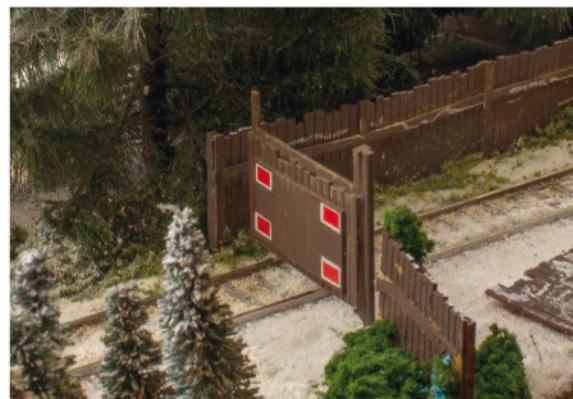


The freight shed is a Wills kit (a wooden shed is a wooden shed!). The small brewery buildings are Petite Properties laser-cut kits; the bricks on these are individually applied. The family say I need to get out more (!) but the process is quite therapeutic once you get going. The bricks themselves are the waste pieces from old style computer punch cards. I get these ready-coloured from Bob Dawson at Acomb Models.

The remains of the castle walls (Metcalfe kits) indicate that Megsdorf was once a fortified settlement.

Background

I painted the backscenes using chalk pastels for the basic sky, mountains, and trees. The buildings on the backscene are copied from kit catalogues, painted onto thin paper, and then stuck to the backscene. The trick is to get the size and perspective right. The roads leading away from the station car park and uphill from the small factory area provide an opportunity to blend together the three dimensional foreground elements and the two dimensional backscene.



Additional fun

Megsdorf has some operating functions in addition to the trains. A fire burns near the freight shed. This is a random flickering unit which is wired directly to the layout's power bus through a smoothed 12v supply. In this way, when we switch on ECoS to power the layout the fire should light up. If it does, all is well. If it does not, then we have problems!

The siding for the brewery is controlled by a gate. This opens and closes courtesy of a servo powered by a Tam Valley servo decoder. This was all installed and wired up by Simon.

The other features are, in best modern terms, 'interactive' in that they are activated by the spectators. A small button on the layout fascia triggers one worker to start welding the conveyor and another to start chopping wood in the field at the front of the layout. Both these functions are wired through an adjustable time switch which stops the action after about ten seconds. The only slight problem we had here on the layout's first outing is people's interpretation of





Left

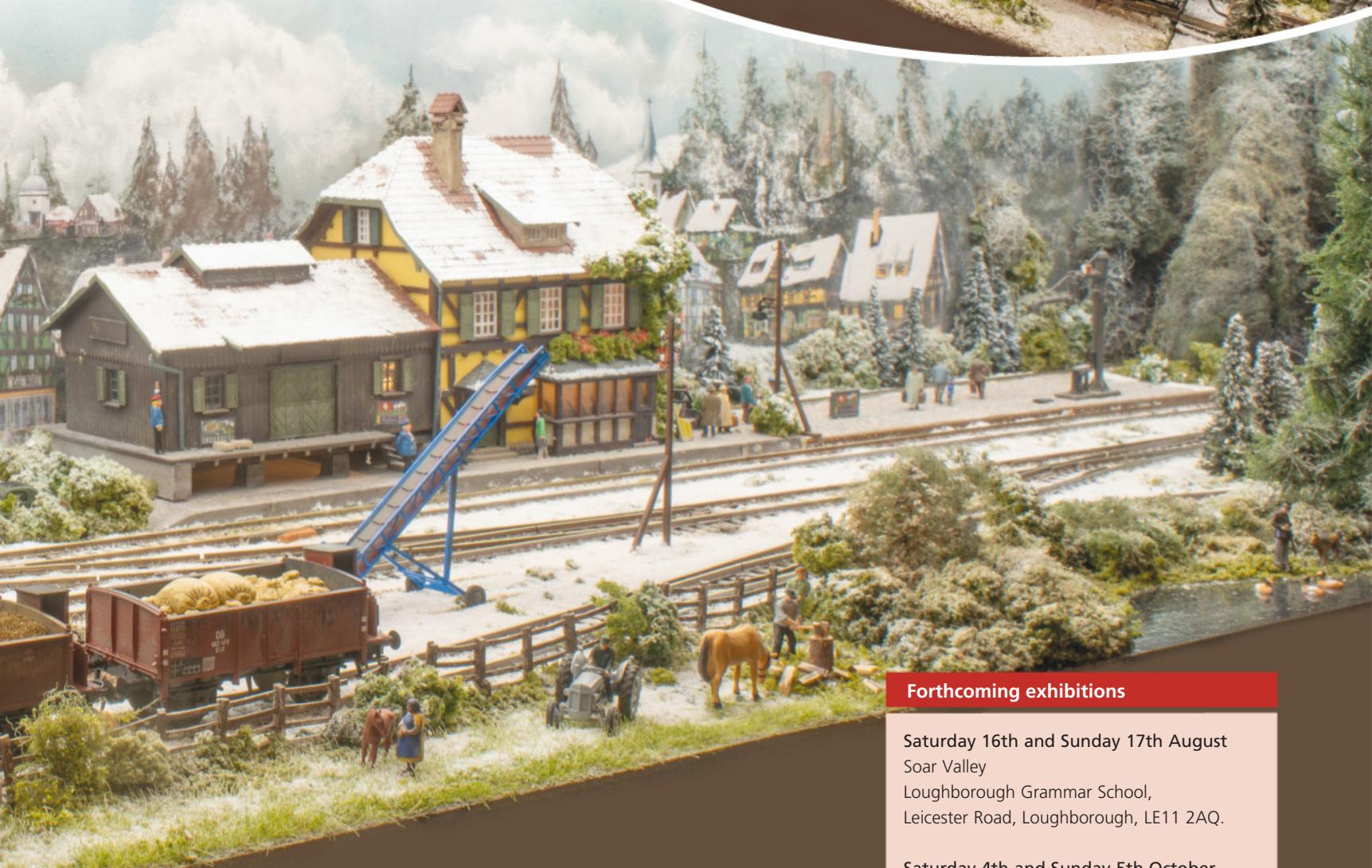
No DB Epoch III branch layout would be complete without 'the saviour of the branch lines', a VT95 diesel railbus.

Below

The 'Glaskasten' 0-4-0T (former Bavarian PtL2/2) was designed for one man branch train operation. 98 301 prepares to propel two wagons to the brewery.

Right

With the gate to the private siding opened, the shunt can proceed with suitable care.



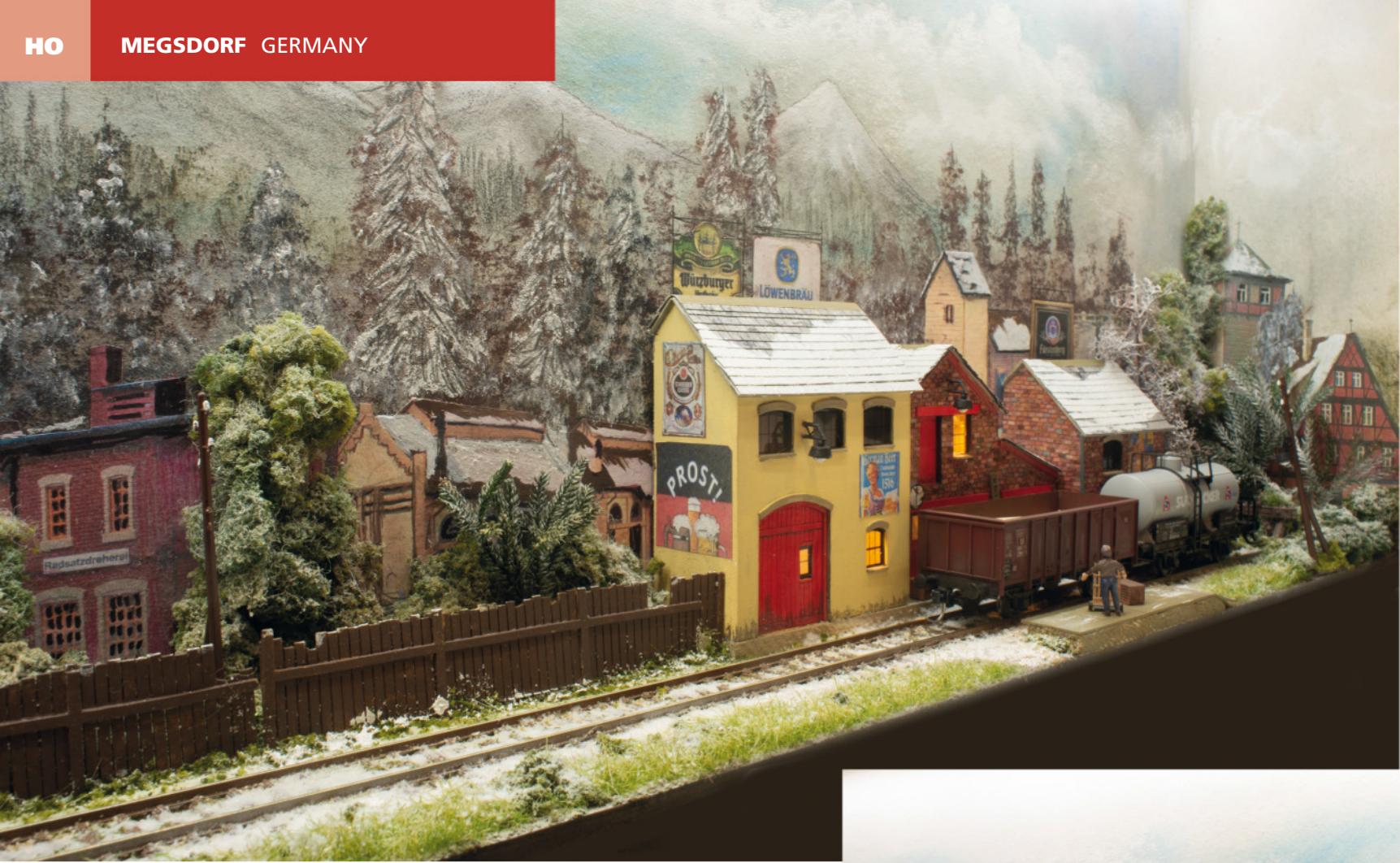
Forthcoming exhibitions

Saturday 16th and Sunday 17th August
Soar Valley

Loughborough Grammar School,
Leicester Road, Loughborough, LE11 2AQ.

Saturday 4th and Sunday 5th October
Locomotion Museum
Dale Road Industrial Estate, Shildon, DL4 2RE.

Saturday 25th and Sunday 26th October
Great British Model Railway Show
British Motor Museum, Gaydon, CV35 0BJ.



the word 'click' in the instructions beside the little red button. Most people managed it but some were so enthusiastic as to render the layout in danger of toppling over! We had to resort to adding to the instructions a 'gently' exhortation. These features have, however, produced a lot of fun for those watching the layout and we have no problem in retaining it for future appearances.

Playing trains

Operationally, the layout is pretty simple in that trains appear, shuffle around a bit as appropriate, then disappear. Freights are interspersed with local railcars shuttling in and out of the station. The ultimate aim is to have a schedule to regulate the train movements to make things a bit easier for the operators.

Above and right
The brewery is located at the end of the siding. This narrow scene masks the sector plate and storage.

Below
The V60 runs round the short branch train. The sector plate (off stage) completes the loop, replacing one point.



Shunting is carried out with the assistance of the Kadee couplers (mainly #18 and #19) fitted to all the stock. Conversion is a fairly easy process in most cases as it is just a question of pulling the existing couplers out of the NEM pockets and replacing them with the Kadees.

They are uncoupled by three 6mm x 3mm cylindrical neodymium magnets hidden in the ballast on each side of the track.

The trains

Stock is a mixture of Fleischmann, Brawa, Roco, and Trix. All the locomotives and the railcars are sound-fitted, which adds another dimension to operation.

Conclusion

So that is a brief account of our little layout. I like to think Meg would have approved.

If you see us at an exhibition, please have a chat.

If exhibition managers are interested, I can be contacted via fluff48@zoho.com



Olivier Taniou reports on a French club layout which depicts a local prototype.

Photographs by the author.

Malansac

On the southern Brittany main line

Malansac is a small town in Morbihan (*département* 56). Its wayside station is on the southern Brittany main line between Vannes and Redon. This line was opened in 1862 by the Paris-Orléans company, initially as single track, which was doubled around 1900. The line was electrified in the early 1990s. The original station building, made of alternating rows of brick and stone, was designed by the architect Phidias Vestier, as were the other buildings on this line.

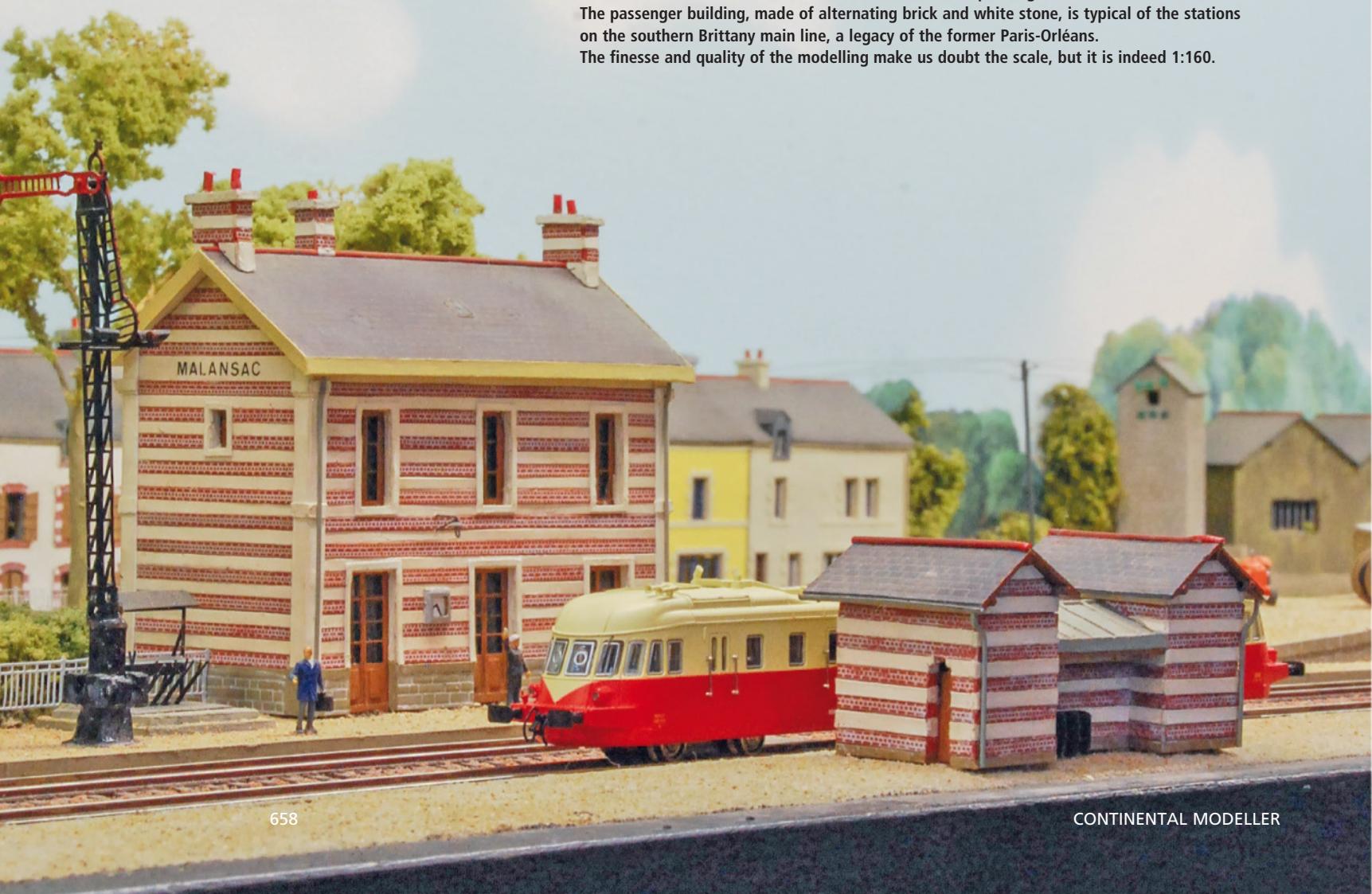


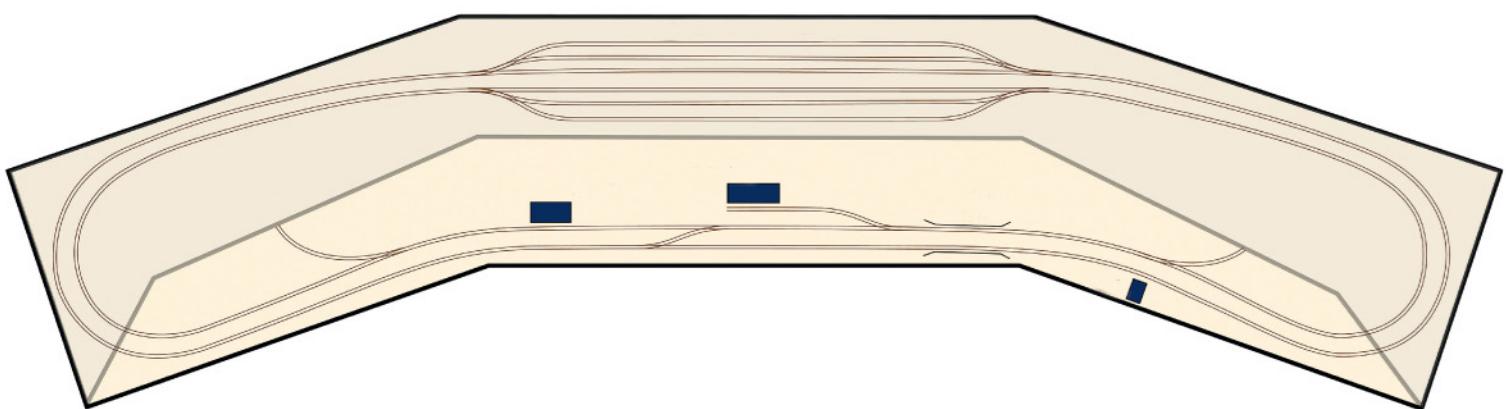
Above

040 D 582 leaves the single track secondary line with an all stations stopping train. Many such lines crossed Brittany from north to south to connect the main east – west routes.

Below

A Renault ABJ arrives at Malansac station where a few passengers await the next train. The passenger building, made of alternating brick and white stone, is typical of the stations on the southern Brittany main line, a legacy of the former Paris-Orléans. The finesse and quality of the modelling make us doubt the scale, but it is indeed 1:160.





Top
A general view of the
Malansac station module.

Below
Railways in Brittany
were not electrified
until the early 1990s.
CC72000 diesels took over
from electrics in Rennes.

Why not N?

The Rail Miniature d'Allaire club have premises in a simple building with a basement, rented from the local authority.

Members decided to reproduce some local sites on the southern Brittany main line in N, to facilitate transport and reduce costs, which are often a barrier for exhibition organisers.

The choice was strongly influenced by the avalanche of new N products from various manufacturers in recent years and by two members of the club: Jean Cantaloube, a committed N modeller, and Michel Lecouronnais, a former N fan now converted to Oe (see CM December 2020). Other club members easily transitioned from HO to N for this project, mixing real and imaginary sites.

Nowadays in Malansac only two electrified tracks remain. FRET and TGV trains pass through non-stop, leaving only a few TER trains to serve passengers at this modest wayside station. The passenger building is now used by local associations. The goods shed, now a storage facility, sits proudly in the middle of the former yard ... now surfaced as a car park.

While in reality few trains now stop at the station, in the model, despite a simplified track layout, things are different! Connections are provided between express trains and autorails.

The new blue diesels have taken over most of the traffic on this line, but the station is still home to the last steam locos, assigned to lesser services.





Above
040 D 582 (ex Prussian G8) on the 'omnibus', the all-stations stopping train.

Below
The line saw many 141Rs from depots at Vannes, Auray, Rennes, Nantes, and more.

Malansac station and the various sites reproduced are not far from the clubrooms. For the layout, the track plan was slightly modified. A site visit allowed for a thorough understanding of the significant elements to be reproduced, to take measurements, and to take the necessary photos. Thus, the passenger building, the goods shed, and the station street, all still in existence, could be modelled. For the platform shelter, it was necessary to go further along the line

to Saint Jacut-les-Pins, to find the only remaining railway element of this disused station.

The sawmill, which is imaginary, was added for interest.

In a club, everyone's skills are utilised. Alain Niol and Christian Maire worked on the carpentry and the technical aspects of the box girder baseboards, while others handled the scenery. Everyone found their place in the development of this project.





Above
A CC72000 diesel passes Malansac with an express. The station still sees many trains passing through, but (as is the case today) only a few stop.

Below
The tree-lined courtyard still has its white concrete boundary fences.

A two stage layout

When it was first designed, the entire structure was shaped like a dog bone. Two return loops, with imaginary decor, surrounded the station. The layout was a false double track – the two circuits were independent, with no interchange possible from one track to the other, and the three points at the station were not motorised.

Soon tired of this limited operation, the members extensively transformed the plywood structure and the electrical wiring. The return loops were modified, and a removable sliding traverser, with a bundle of sidings, was installed behind the scenic section. A block system manages traffic behind the tracks.

A miniature camera monitors traffic in the station, which is well illuminated by fluorescent tubes.

A module with the Vacherie Bridge, which spans the Arz River, and is actually several kilometres away, was added, doubling the length of the layout. The whole thing became a large double-track oval. The former exits from the return loops became imaginary junctions, no longer connected to the storage sidings.

Scenery

A change of scale is not always easy, but this club includes some modellers with experience in N. So, this aspect was entrusted to Jean, assisted by Michel and Marcel Loriot.



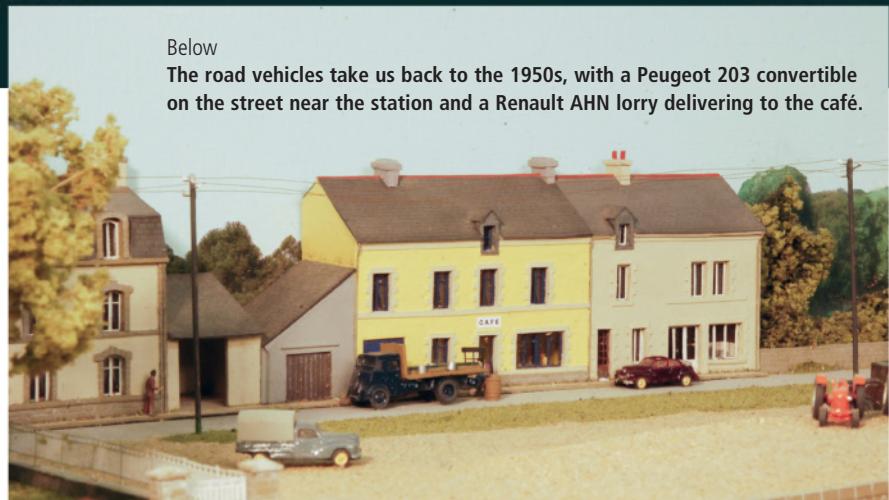


Above

A BB67500 diesel rolls through with a train of four-wheel vans.

Below

The road vehicles take us back to the 1950s, with a Peugeot 203 convertible on the street near the station and a Renault AHN lorry delivering to the café.



The polystyrene foam used to make the landscape was covered with painted plaster strips, then with flock and fine sand.

A printed photo was cut out and glued onto a sky blue painted background.

The buildings are cardboard constructions covered with plastic sheet, following the shapes indicated in photographs.

The road vehicles are from Dominique Pion's range and set the scene in the mid-1950s.

Many accessories produced by specialist suppliers, such as the station barriers and the bridge railings, give a remarkable finesse to the whole.





Rolling stock

What do we see passing through the station? Everything! The variety of rolling stock used on the line and the wide selection of N products allow for a period spanning from the early 1950s to the mid-1980s. However, as with many club layouts, at exhibitions other iconic French railway vehicles may appear for viewing pleasure.

N allows long trains to be run over a fairly extended period, and the club makes the most of it. The latest Piko products have greatly contributed to the variety of vehicles running on the layout display. We also see the Roco BB63000, Minitrix CC72000, and more.



Left

Road has not yet taken over from rail completely for freight. The activity around the shed is intense and requires several employees to ship or receive oil, wine, sacks of grain, etc.

Below

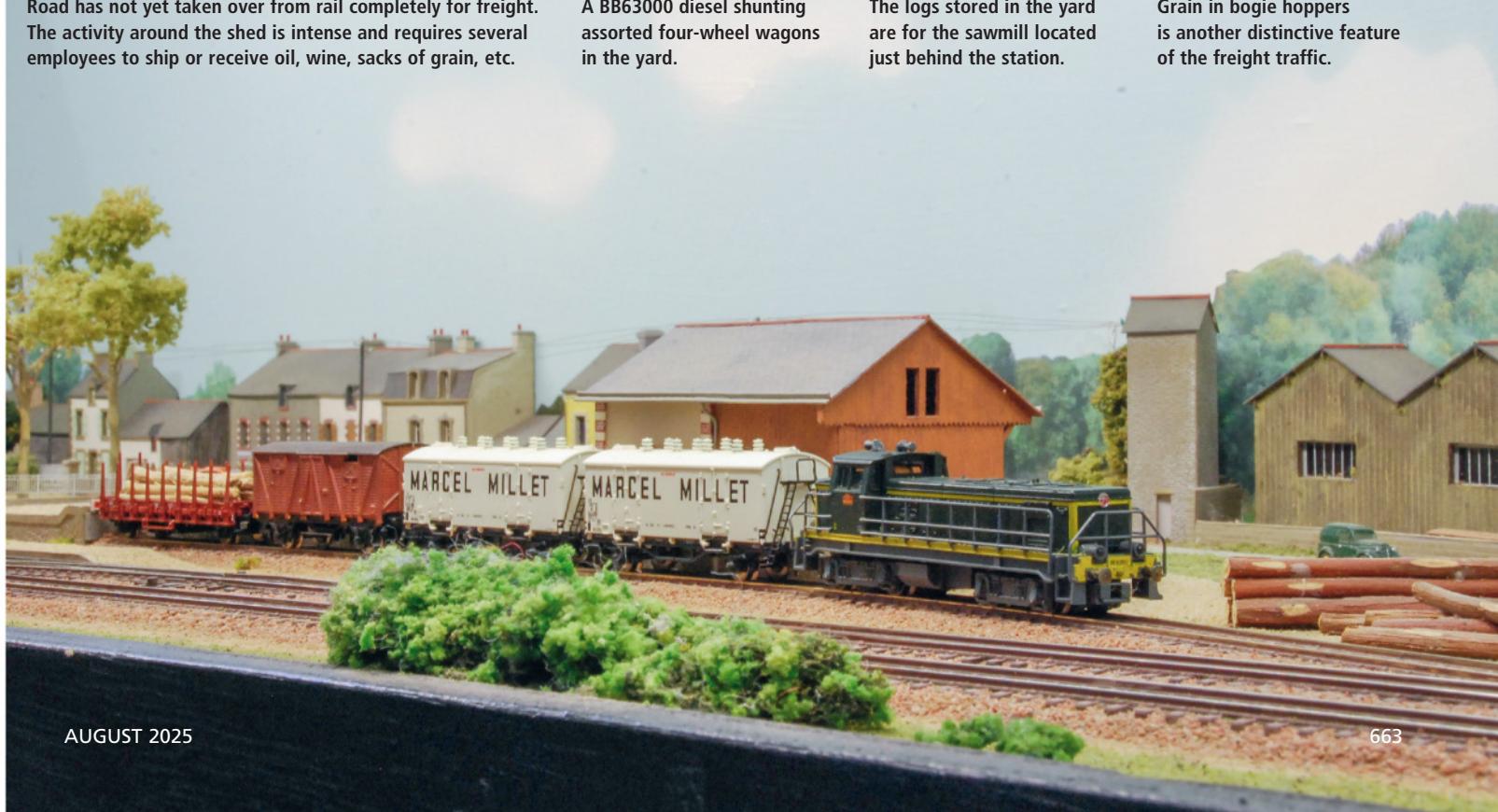
A BB63000 diesel shunting assorted four-wheel wagons in the yard.

Above

The logs stored in the yard are for the sawmill located just behind the station.

Top

Grain in bogie hoppers is another distinctive feature of the freight traffic.



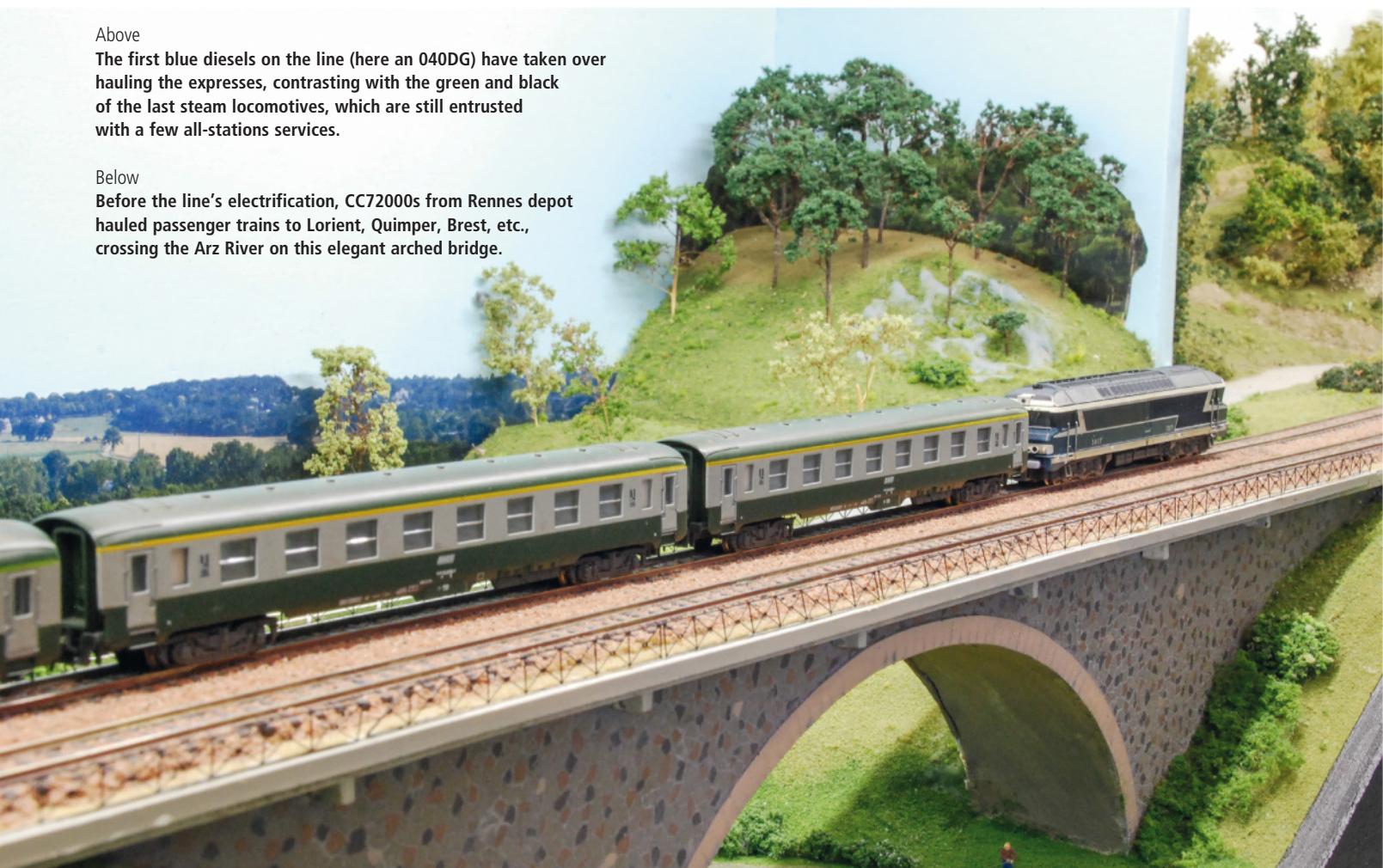


Above

The first blue diesels on the line (here an 040DG) have taken over hauling the expresses, contrasting with the green and black of the last steam locomotives, which are still entrusted with a few all-stations services.

Below

Before the line's electrification, CC72000s from Rennes depot hauled passenger trains to Lorient, Quimper, Brest, etc., crossing the Arz River on this elegant arched bridge.





Above

Behind the embankment,
wedged between the railway and the road,
stand a large house and a smaller cottage.



Above

After crossing the Arz River bridge,
CC72071 speeds its express past a remote level crossing.

Above

On the right of the layout, before entering the tunnel,
the double track runs along a valley. Brittany is far from flat!

Each club member can bring their own stock to run on the layout. There is even one who builds his own rolling stock entirely out of brass.

Current status

Malansac is now effectively finished, and has been displayed at various events in recent years.

The club is currently working a new HO layout, a sort of return to its roots ... the Arz, of course.

Bob Whetham was inspired by visiting a narrow gauge coal railway with unusual motive power. *Photographs and drawing by the author.*

Jianghe

A Chinese coal railway represented on a table top

Small petrol or diesel powered locomotives, or 'critters', may be homely, even ugly to some, but for me they have a certain appeal.

The Jianghe coal railway in central China had a fleet of home-made machines adapted from road vehicles and, captivated by the equipment as well as the setting of the railway itself, I knew that one day I would have to attempt a model.

The choice of motive power was not its only unusual feature. Most of China's narrow gauge railways were constructed during the post-revolutionary period and were of 760mm (30") gauge. Departing from the norm, Jianghe was 2' and from an earlier era, having been constructed by a British entrepreneur before the First World War.

Another unusual feature was its track. The ties or sleepers were made of hand cut stone with holes drilled to accept wooden pegs. Normal track spikes were then used to hold the rails in place.

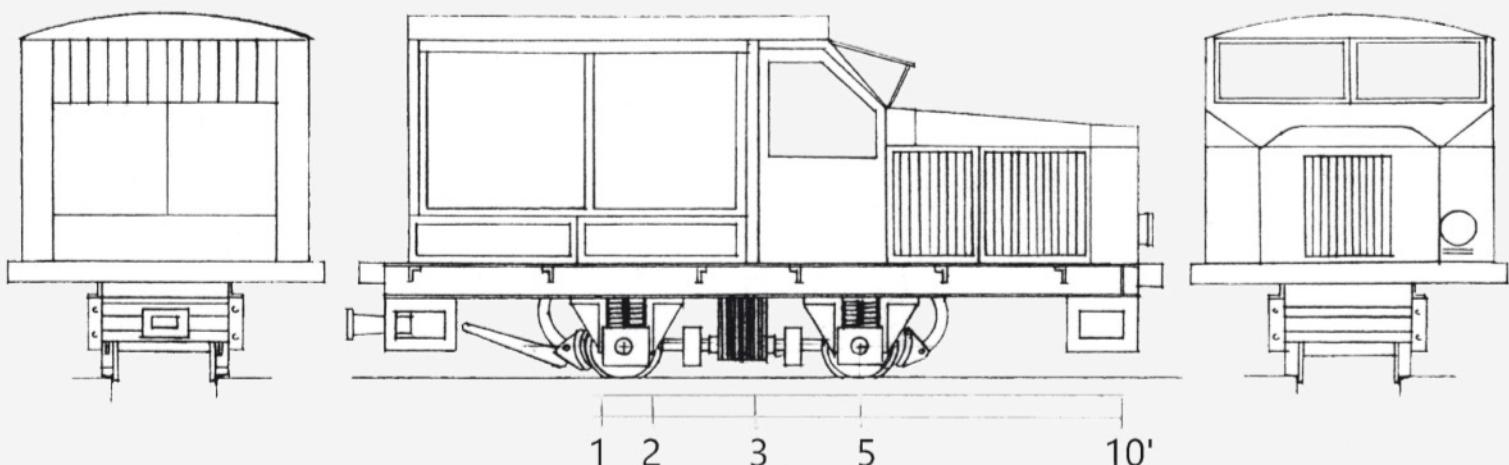
The line was between 12 and 15km in length and originally ran from a mine to a coking plant. By the time of my visit in 2003, the plant had closed and coal was being offloaded to trucks for onward transport. Operations were also somewhat unusual with three or more trains running close together in convoys.

Below
Photos of the prototype plus a few known dimensions were used to prepare a sketch plan for constructing the model. Reproduced at 1:43.5.



Above
The layout's much condensed village scene was inspired by this view taken at approximately the mid point of the line.

Left
Rails were spiked to wooden pegs inserted into hand-cut stone ties.



Right
An early stage of construction, showing the unpainted carved foamboard hills and an outline for placing the track.

Initially I worked up a set of drawings for a loco and coal wagons from photographs and a few known dimensions, but the project languished for some years while other activities intervened. One of the things that held me back was the lack of a suitable mechanism. Eventually I was able to source a four-wheeled N gauge Tomytec power truck from Japan which closely matched my drawings. After testing, I found it to be a surprisingly smooth runner so, newly motivated, I began to design something to run it on.

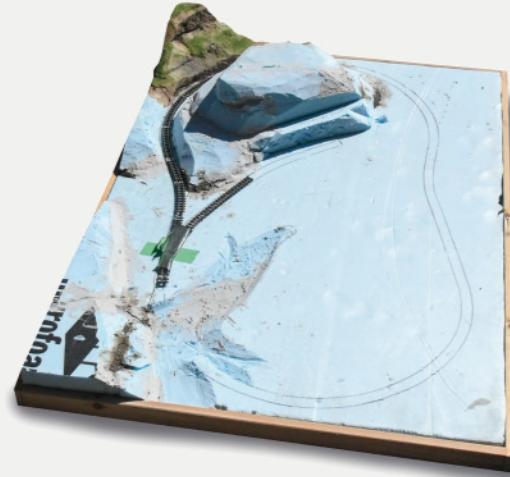
From the outset, my intention was to construct a small portable display layout using mostly salvaged materials. Some of these I had not used previously, which opened the door for experimentation, adding to the challenge and enjoyment of the project.

For practical reasons I would use N gauge track and components. This would represent approximately 30" in 1:87 rather than the 2' of the prototype, but I felt it was a worthwhile compromise.

The layout needed to include street running through a village as well as a coal mine but otherwise my only goal was to try and capture an impression of the real railway and its environs. With this in mind, greater reliance was given to colour and texture than exact detail to achieve the desired effect.

The dimensions of the layout were determined by the size of a discarded panel of foamboard insulation and the radius of some vintage sectional N gauge track that somehow wound up in my possession. Ultimately, I was able to fit an irregular shaped loop and single siding into a space of 43½" x 31" including the frame.

The widely spaced ties of the Lone Star track were not very realistic for N but adapted for HO and painted a dirty grey colour they very closely resembled the stone ties used on the narrow gauge Jianghe. Cuts made to the tie strip allowed flexibility to vary the curvature and helped to create an illusion that it had been laid down to fit the scenery rather vice versa.



My box of track included a single manual turnout but it was badly oxidised and had a toy-like appearance that could not easily be disguised. The solution was to custom build a new one using scraps of brass and rails cut from surplus track sections. I did take a cue from the designers of the commercially manufactured version and used a similar closed frog design, necessary for the short wheelbase wagons to negotiate the curve without derailing.

In keeping with the intention for the layout to be portable, it also had to be light, so blocks of foamboard were glued together and carved with a sharp knife to form hills and a low mountain with exposed rock faces. Because of their tendency to cling to everything they touch, small detached pieces were difficult to clean up so I tried as much as possible to avoid producing them while doing this work. Once steep areas and cuttings had been scribed to represent exposed rock, they were coloured using washes of diluted acrylic paints. Roads and other terrain were painted with a variety of blended earth tones. Diluted white glue was then brushed onto grassy areas and commercial landscape materials sifted over them using several different colours to avoid a uniform "golf course" look. Finally, irregular shaped foliage clusters were glued down in crevices and areas where moisture would likely accumulate. Overall, I was quite pleased with the result and will likely use similar materials on future projects if the opportunity arises.



Right
The motive power might appear crude but seemed up to the task. The locos had a belt drive to a longitudinal shaft which was geared to the axles. The short wheelbase made them hunt from side to side as they made their way along sometimes indifferent track.



The scenery on my model is more open than on the prototype but I did need some trees. Searching for appropriate materials, I discovered a few long forgotten and somewhat flattened commercially made examples at the bottom of a box of scenery supplies. Although not ideal, they were much improved after teasing out the branches and repainting the trunks. Additional trees of varying sizes were built up using twisted wire coated with a layer of craft glue for the trunks and clumps of coloured foam for foliage.

Unfortunately, I did not have enough foam board to complete the low hill in the centre of the layout and had to resort to a more traditional method. First, a supporting framework was made of stiff paper drywall tape cut into narrow strips and hot glued to form a lattice of the desired shape. Irregular pieces of torn newspaper were then dipped into a mixture wet plaster and applied to the surface. A final layer of plaster textured with an old paint brush and a few rocky areas carved into the soft plaster with a knife completed the job. Once painted and covered with vegetation, it fits well with the rest of the scenery.

Carefully tended vegetable gardens were a feature seen almost anywhere a patch of arable land could be found. For these, I used corrugated cardboard with the paper removed from one side and cut to the size and shape of garden plots. A base coat of Mod Podge craft glue helped to keep the cardboard from curling before being painted an earth colour. Once dry, a bead of diluted glue was applied along the tops of the rows and contrasting landscape materials sifted over them to give the appearance of crops. This was done over a tray so that surplus materials could be collected for further use. The finished plots were then glued to the layout and the edges blended onto the base using filler made from dry plaster mixed with craft glue.

Probably the single most expensive purchase was the corn plants. While exploring some online offerings I came across some packages of hand-crafted dried corn stalks. Carefully touched up with a mixture of diluted green acrylic paints they made very realistic living plants and provided a contrast to the other low profile crops.

Apart from the wall and freelance mine buildings, which were scratchbuilt using brick patterned styrene, the rest of the structures were made from poster board reinforced with wooden strips. Although photos were used as a guide, the buildings do not pretend to be exact replicas but rather an attempt to capture the character of those found in the area. One of the challenges was finding suitable material for the tile roofs. Unable to locate anything available commercially, I settled on the card holders for takeaway coffee. The spacing of the corrugations was right, and once scribed at intervals of approximately one scale foot, painted and weathered, they looked quite convincing at my threshold for viewing of approximately three feet. As a bonus, their uneven surface resembled the somewhat organic appearance of the roofs on the real buildings more closely than would have been achieved if a rigid, flat material such as styrene had been used.

Above
The completed layout.

Below
Most flat areas not occupied by buildings have been put to use as garden plots. The figure in the centre started off as a dapper looking European businessman but after judicious use of a file, painting, and the addition of a typical hat, emerged as a local farmer.





Construction of the loco and wagons turned out to be less demanding than building the layout. With a working mechanism at hand, all that was needed to complete the loco was a fairly uncomplicated body and chassis. Delving into my box of scraps and using my drawings as a guide, I built these from styrene except for the roof and cover over the motor which were made from thin brass.

The cars were also made from styrene, taking advantage of scraps with a scribed wood pattern left over from a previous project. Wheelsets were salvaged from N gauge freight car trucks, modified only by having the sharp axle ends filed down.

For loads I used actual coal pulverised from a stray lump found beside a road near some abandoned mine buildings. Once reduced to scale size, the coal was glued to carved foam blocks which press fit into the cars. The home-made horn hook couplers were leftovers saved from some HOn3 rolling stock that I had since converted to Kadees.

No layout is ever really finished and *Jianghe* is no exception. It would benefit from more figures, a vehicle or two, and possibly a mechanical ox, all of which will have to be scratchbuilt or adapted from commercial products. In the meantime it has provided many hours of rewarding modelling.

While I did not keep exact records, building the layout falls well within the budget category, and its small size can be used to demonstrate that neither cost nor space need to be barriers to the enjoyment of our hobby.

We do not have scheduled train shows where I live in southeast British Columbia but we do have an active group of volunteers who build and maintain a working model railway exhibit at the Cranbrook History Centre. One of our aims is to encourage more people to enter the hobby and I hope to be able to assist in this endeavour by displaying *Jianghe* at one of the museum's family day events.

Right
A short train trundles through the village, crossing the road and passing an abandoned branch line.

Above left
The line curves though this deep cutting carved from foambord and coloured with washes of diluted acrylic paints.

Above right
The hand-worked coal mine with its sloped entry was based loosely on a facility seen from roadside.

Right
Crude and battered, the completed loco on a test run.



Friedel Helmich describes his portable loco depot layout.

Bw Klappstadt

An ingenious fold-away layout

After my operational diorama *Bw Rheinbrück* (see www.bw-rheinbrueck.de) had become outdated, I wanted to build a new one, preferably a steam depot, as I have a large collection of HO steam locos.

I also wanted to reduce the effort required for transport and assembly/disassembly for exhibitions. Thus, the idea of building a foldable layout that could fit in the boot of a mid-size car matured.



Above

4-6-0 38 3715 with tub tender has just arrived on shed and is being coaled.

Ahead of it, 0-10-0T 94 1730 takes water as the firebox is cleaned of clinker and ash while 2-10-0 50 2840 waits in the shed for its next assignment.

Layout photographs by the editor.



Above
Visiting enthusiasts are keen to photograph
preserved Prussian T12 2-6-0T No.7726
as it shunts a lowside wagon loaded
with spares for the workshops.





1.
The bare boards laid out on the lightweight tables that will support the layout.

3.
Success! The folded layout.

2.
Folding up the bare boards as a test of the theory.

The resulting layout is 3m long and 50cm wide. It consists of three 80cm x 50cm and three 30cm x 50cm baseboards, each 19mm thick, connected by hinges. When folded up, it measures 80cm x 50cm x 60cm.



4.
Planning in progress.
The boards were set up in the loft so the track plan could be tried out with the buildings placed.



5.
During construction.
The layout is operated with analogue DC and section switches are positioned conveniently along the front edge.



6.
Ready to move.



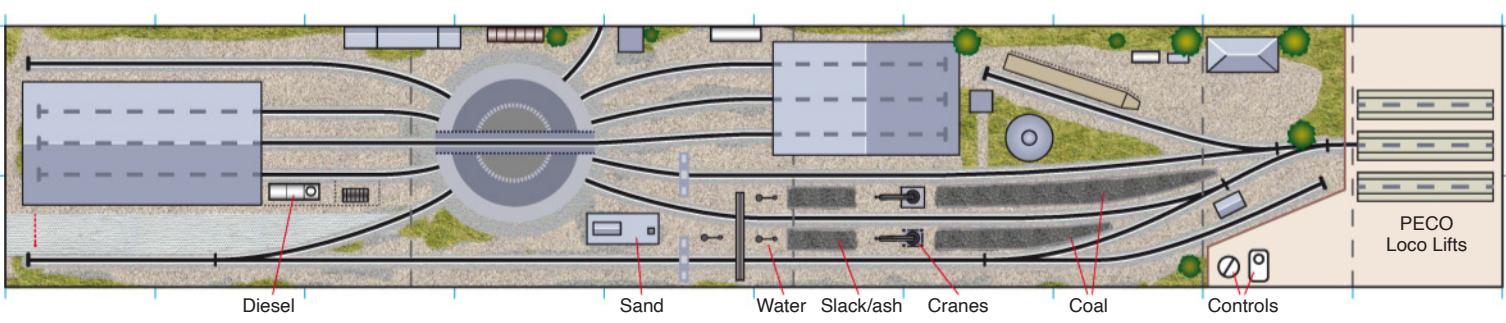
7.
In the car. A perfect fit.



8.
The assembly process:
tables ready to receive.



9.
Folded layout in position.
10.
Boards unfolded.



Bw Klappstadt Overall size 3000mm x 500mm. Each grid square = 300mm x 300mm.



The subject is a locomotive maintenance facility in the Epoch III-IV period. A Fleischmann turntable is located between the two loco sheds, which was rarely seen in real life. The only one I know of with this arrangement was Lauda, where the 13m turntable between rectangular sheds was replaced by points with the advent of the longer BR58 locos at the beginning of the Reichsbahn era.

The left-hand shed was made from a Faller carriage shed (ref.190069), and the right-hand structure was built using parts from the Kibri 'Ottbergen' loco shed.

Fleischmann Profi track was used.

Operation imitates the processes in a steam depot. Arriving locos pass the coal bunkers and are coaled by cranes lifting narrow gauge tubs.

They then take water from the water cranes while the firebox slag and smokebox ash is emptied into pits in the track. The locos then travel to the sanding towers, where their sandboxes are refilled.

Once fully serviced, locos run to the turntable.

A special feature of the front track is that the locos must first advance to the end of the track and then reverse over a point to the turntable.

Above
2-10-0 50 622 is well kept with polished boiler bands.

Above right
After the coal, water, and ash/clinker facilities we find two sand towers. Sand is dried in the brick hut.

Below
Signs of modernisation – a diesel fuel tank has been installed by the other shed.



Right
Behind the first shed are the water tower, offices, and coal bunkers.

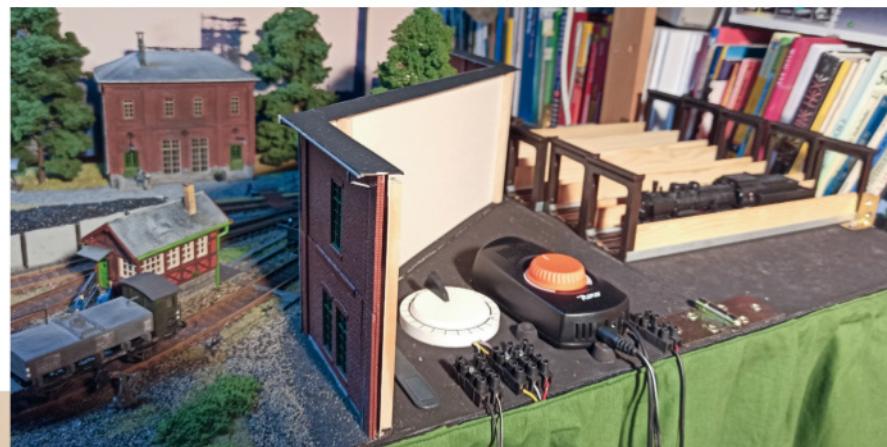


Below
Peco loco lifts are used
in the small off-stage area
next to the controls.

The turntable distributes the locos to the sidings leading to the sheds. Sections can be isolated using small switches in the layout. Control is analogue, with the electrical supply to the sections by cables with plug connections.

After a period on shed, locos leave for their next assignments. By way of variety in operation, small shunting locos occasionally bring wagons loaded with supplies for the workshops within the sheds.

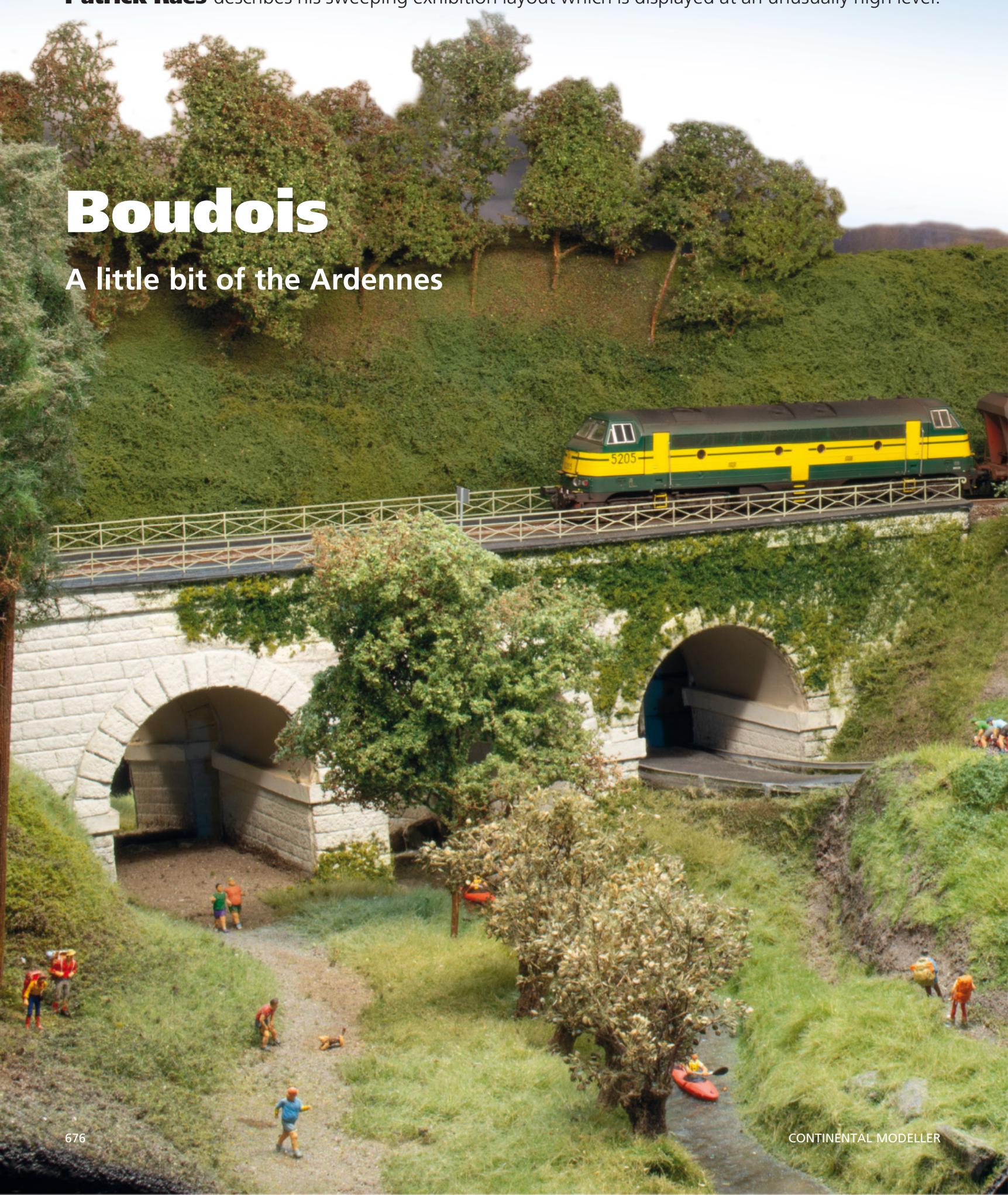
At the right-hand end of the layout, several Peco loco lifts are used to place different locos on the short live track leading onto the layout.



Patrick Raes describes his sweeping exhibition layout which is displayed at an unusually high level.

Boudois

A little bit of the Ardennes



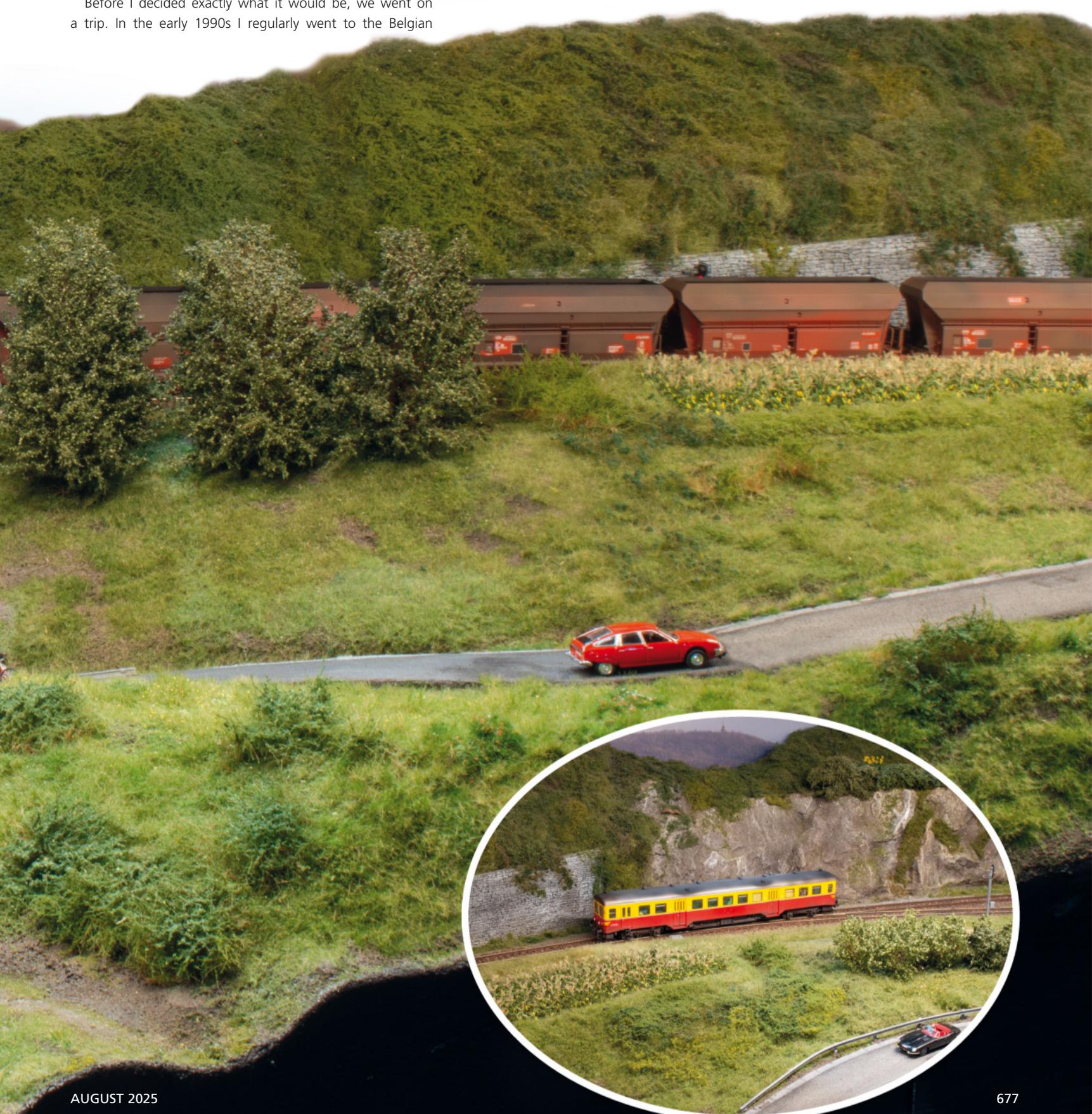
When you are designing a layout, you are immediately faced with various choices. One of them is the choice between a fixed installation or portable sections. After having built a fixed few layouts when I was younger, when a house move was announced work that had taken years was unfortunately lost. You learn from experience. So the next project would be in sections.

Before I decided exactly what it would be, we went on a trip. In the early 1990s I regularly went to the Belgian

Ardennes – what a beautiful region. I often came to quiet rural places where the peace was suddenly disturbed by the roar of large, powerful diesel locomotives and their beautiful sound. I saw how they pulled heavy coke trains at a fairly low speed on gradients, sometimes double heading. This was a 'goosebumps' moment. The choice was made quite quickly to build a layout set in the Belgian Ardennes.

Below
The peace of the countryside is shattered as 5205 rumbles by with empty coal hoppers.

Inset
A class 45 railcar provides the local passenger service.

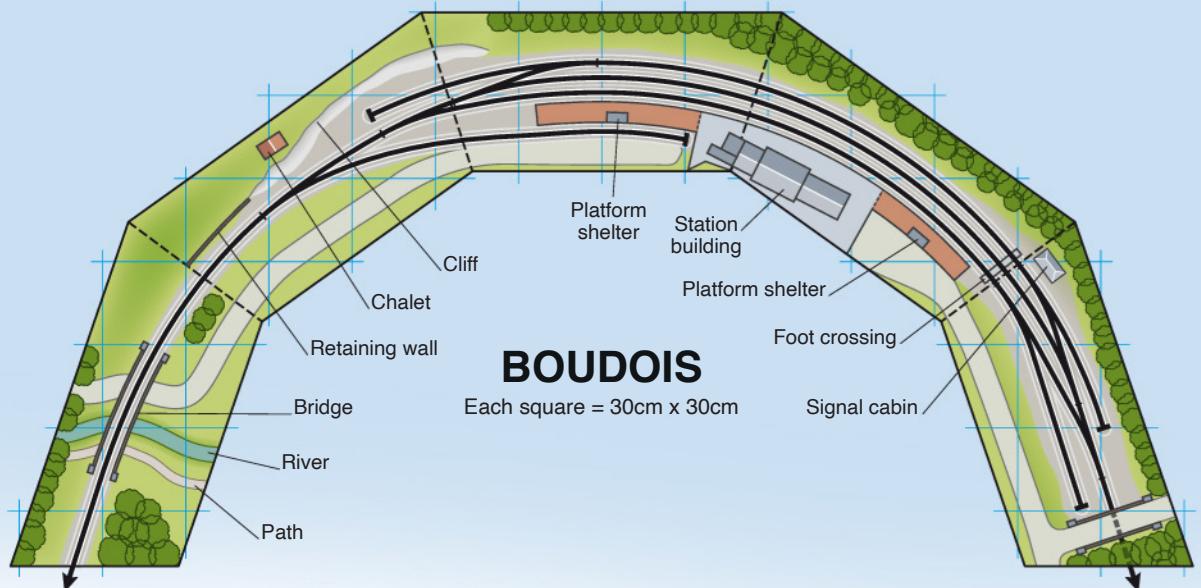




Above
The scenic section is arranged
as a sweeping curve.

Below
Bogie diesel railcar 4509
on the scratchbuilt viaduct.

Photographs by the editor
unless otherwise noted.





Above
5507 with a mixed rake of type M4 stock.
Photo: author.

Below
Timber was an important freight traffic in this region.

In building this layout I looked for a lot of information about the prototype, in books and on line. After all, you have to know and understand the feeling of the real railway first. Only then can you convert this into a model.

The track plan would be quite simple. A single track line with a station where a passing loop was provided to allow trains to cross or to allow a train to wait giving priority to

a following faster train. I also thought it was important to provide a third track. This siding is used to hold a number of wagons, specifically for the transport of tree trunks – a theme that was certainly found in the south of the country. Just like in reality, the end of this siding has been out of service for years and is overgrown with weeds. There is also a van that has been here for years used as a store.





Trains are hauled by heavy main line diesels such as the AFB 'round noses' (series 52, 53, 54) but also the series 55. These were the draft horses in the hilly country at the time – we are some time in the 1990s. Good times! Unfortunately this is a thing of the past, but certainly not in model form.

The track plan is completely fictitious, as is the name of the station.

Because I like to see gentle curves with super-elevation, the rail traffic really comes into its own when you look at it

Above
5205 and the empty hoppers held in the loop at Boudois.

Below
The class 41 twin railcars in the 'new look' livery were supplied by Alstom from 2000.





Above
The station building, deliberately placed this side to break up the view so the layout seems longer.

Left
An international train with Swiss stock passes through the station.
 Photo: author.

Below
International working of a rather different kind – a veteran French X3800 'Picasso' autorail.

at eye level. I am quite tall so the layout sits somewhat higher than most you will see at an exhibition.

There is almost no straight track, and the layout is built in a curve with an inner diameter of 3m – something else you rarely see at exhibitions.

There is fiddle yard at each side so alternating traffic is possible.

The track is from Peco. I know from experience that this is good and the range includes large radius points. It was good planning to let the points and track flow as smoothly as possible. I chose code 100 as I have a lot of older stock and it would mean a lot of work and expense to replace all the wheels. But once the track is painted and ballasted, the rail height is much less noticeable.

The points are of the electrofrog type, with no insulated sections where small locos sometimes stall. They are worked by Tortoise motors.

The landscape was constructed from insulation material, namely Styrodur sheets. This is known in the construction industry and is ideal for creating a beautiful landscape. It can be easily processed with a hot wire cutter, saw, or rasp.

The rocks are actually impressions that I made of real stones which I found during walks in the Ardennes. I made rubber moulds with products from Voss Chemie so I could cast a number of plaster pieces to assemble the rock formation. The colouring was done with washes and dry brushing.

The goods yard as well as the asphalt roads and the platform are made of foamboard with the top layer removed and scribed or impressed as necessary. For the goods yard I pressed the stones with a profile. For the asphalt I used a wire brush to get some texture.

The platform was covered with gravel from Woodland Scenics.





The viaduct was also made of peeled foamboard; here again the necessary stones were scribed in.

The high-stemmed firs are made of dowel sold at DIY stores. These trunks were rounded off at the top. For the branches I worked with sea foam that was treated with grass fibres and then painted with a spray can of pine green.

The corn plants are from Busch, removed from the sprue one by one.

The field itself consists of corrugated cardboard that was painted brown and provided with a layer of real earth. (First put it in the microwave to remove any bugs.) Then each plant was planted separately in the 'furrows'.

Above
The type M4 stock includes driving trailers.

Photo: author.

Below
The signals are now lights but the signal box remains.





Below
5525 and a classmate double head a heavy coal train.
 Photo: author.

Above
Railcar 4509 passes the end of the headshunt where an old van has rested for some years.

For the pollard willows I chose products from ER-Decor. They make beautiful realistic trees. The shrubs come from the MBR range, the greenery and the ground covers are from well-known brands such as Heki, Woodland Scenics, and MiniNatur. The latter have beautiful grass fibres that look very natural with the right colours.

I wanted to soak up the atmosphere of the beautiful General Motors powered locomotives and the models are equipped with sound decoders. In this way, a piece of history is revived in model form, which is certainly an added value. On this type of line you do not usually run different trains at the same time, which would become too much and spoil the sound effect.

The trains are controlled digitally, but the points and signals are analogue.

The signals were made by Gustaaf Demeester. The 'crocodiles' (track circuits) come from the PB range and are 3D-printed.

As far as operation is concerned, we try to approach reality as closely as possible: a blue series 55 with three type M4 coaches in Bordeaux red or a CFL 1600 with Wegmann coaches forming the Liège – Luxembourg train; a series 54 as rebuilt with isolated 'floating' cab with M2 coaches, sometimes as a push-pull set; or a heavy steel train. All these and more which were familiar sights in the early 1990s.



Nigel Hurst has developed his interest in an unusual system.

Photographs by the author, unless otherwise noted.

Return to the

Montréal Métro

Long term readers of CM may recall the February 1997 edition with my article on producing the rubber-tyred Montréal Métro MR-63 cars in HO.

As a former resident of Montréal, regular Métro rider, and life-long railway modeller, it was inevitable that I would construct a model of the Paris-inspired Métro system. The article covered my attempts at experimenting with home-made resin components, and how I eventually turned to whitemetal casting. I fabricated master components and then made rubber moulds to enable me to cast multiple whitemetal pieces, to produce the components to construct two three-car elements.

Time moves quickly in urban transit, and the 52-year-old Montréal MR-63 cars (*Matériel Roulant conçu en 1963*, or rolling stock designed in 1963) were withdrawn and scrapped between 2016 and 2018, being replaced by the new MPM-10 Azur trains from 2014.

Shortly before the last MR-63 cars were withdrawn, in 2017 I was visiting Montréal to see what had changed on the Métro system. Plenty of new modelling opportunities presented themselves, and so it was time to return to the Montréal Métro in every sense!

MR-63

While my two MR-63 elements (an element is a three car set with a motor driving cab at each end and a non-powered centre car) were constructed on my workbench in 1997 and painted in the original 1963 livery, my visit back to Montréal convinced me that it was time to refresh them and bring them up-to-date.

Modelling techniques have moved on with the times, and while whitemetal casting was once a valuable method of building models, 3D-printing has revolutionised the way I approach my model making, creating new opportunities for repetition and accuracy. Coupled with my experience and joy of building railway models with plasticard, a sharp knife, and filler, I have also embraced 3D printing as a powerful new tool.

I decided that this long completed modelling project was ready to be updated with new technology. This decision was also confirmed following an unfortunate accident, dropping of one of the whitemetal driving cars onto a hard wood floor, which resulted in the instant separation of almost every component back into the constituent pieces! After twenty-five years, contact adhesive glued whitemetal components were no match for gravity!



While this car was glued back together and the gaps refilled and smoothed, I decided it was time to repaint it into the second livery applied to the MR-63 stock during the 1991-1993 refurbishment. The second livery is similar to the original but with a lower and deeper bodyside white band which uniformly wraps around the driving cab. The oval headlight clusters were also painted into a black rectangle on the front, which alters the car's appearance.

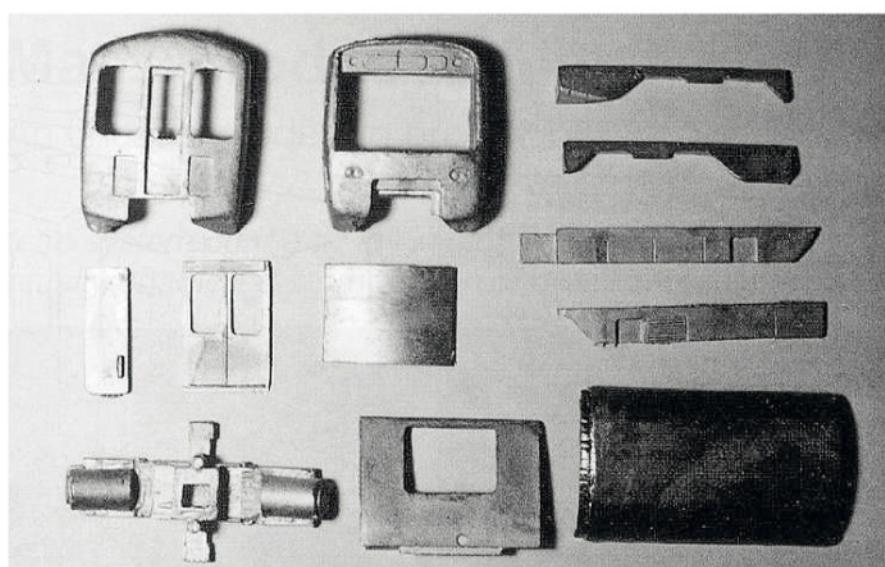
All three cars of one of my MR-63 elements (including the rebuilt car) were repainted into the new livery.

I was never happy with the original 'rubber' tyres on my MR-63 model, as back in 1997 there was very limited choice of obtaining accurate tyre representations in HO. I decided it was also time for a full bogie refurbishment, and so the whitemetal bogies were removed and a replacement designed on SketchUp before being 3D-printed and painted.

Top
The refurbished models of the MR-63 stock in the 1963-1991 (left) and the 1991-2018 (right) colour schemes.

Above
A refurbished MR-63 model in the later livery.

Below
The whitemetal parts made in 1997.





I also designed a new tyre on SketchUp which would align perfectly with the new metal rail wheels which I was also going to use on the model refresh. As I wanted the tyres to be as perfectly round as technologically possible and without the need for any post-3D-print clean-up work (as there were going to be forty-eight tyres in all for the six cars), I opted to have them 3D-printed in resin.

While on a visit to Zagreb in Croatia, I found a local Cosplay enthusiast who offered 3D resin printing services and so I returned home with a box of beautifully smooth, and ready-to-use resin wheels, which simply needed a coat of matt black paint to represent the tyre itself.

While resin 3D-printing is more expensive and less common than FDM (Fused Disposition Modelling – also known as FFF, Fused Filament Fabrication) printing, the results are quite stunning in terms of very high definition and minimal post-print clean-up.

With a new front dummy coupler design also 3D-printed, both MR-63 elements were then finished, re-glazed, and ready for service. One is in the original 1963-1991 livery, the other in the refurbished 1991-2018 livery.

MPM-10

The newest, third generation, rolling stock is the MPM-10 Azur, which was built by the Alstom-Bombardier consortium and commenced testing in April 2014.

Seventy-one of the new MPM-10 (Montréal Pneumatic Material 2010) nine-car sets entered service in early 2016, replacing the MR-63 stock on a one-in, one-out basis.

Unlike the existing MR-63 and MR-73 trains which run up to nine cars formed of three three-car elements, the MPM-10 is a fixed rake nine car train with a continuous through passage. When compared to the older stock, there is a very clear modern step change in terms of design, technology, and ride quality.

The MPM-10 general arrangement drawings were available online so I imported these into SketchUp to commence turning the 2D drawings into a 3D model.

Like most modern rolling stock, each car is a very straightforward modular design in terms of the repetition of standard sized components: each non-driving car has two different sized passenger windows which repeat along each side, with three sets of identical sliding doors. All I had to virtually model was one window (which could be stretched to create the other) and one pair of sliding doors, plus half of a roof.

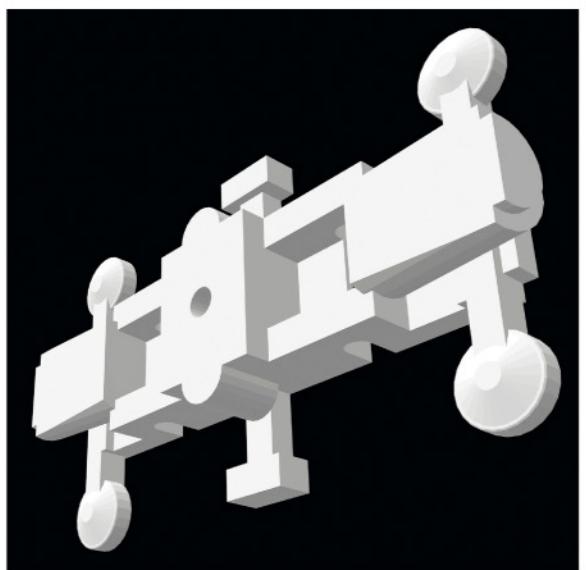
This is same approach as I took when making the physical MR-63 master components in 1997, which were then cast as whitemetal pieces. This time with the MPM-10 it was virtual

Above
Refurbished models of the MR-63 stock in the 1963-1991 (left) and the 1991-2018 (right) colour schemes alongside a new MPM-10 cab car.

Above right
3D-prints in resin for the wheels and tyres.

Right
Bogie artwork with the guide tyres incorporated front and back.

Right
New 3D-printed bogie.



Below
One of the MPM-10 sets in service.

Photo: Benoît Clairoux, STM.





component model making, duplicated to create a full car. With relatively little effort, each car was quickly created using SketchUp and exported as an STL file ready for checking and printing.

Cabs

The two end driving vehicles were slightly different as the cabs were a little more complicated to model as they featured quite a curvy design and a windscreens. I created a new file and modelled half of the cab, split precisely down the centre line. The cab, and indeed the entire train, is completely symmetric along the length, so it is much faster to model one side of the cab and then mirror it.

I admit that virtually modelling more complex three-dimensional curves is still at the limit of my SketchUp skills, and so my goal was to create something which looked as good as I could on the screen and then make physical adjustments once printed. This is precisely what happened, as the test prints of the driving cabs confirmed that I had not fully captured the curves at the top of the windscreens as it should have been. Both test-printed cabs had their window profile re-adjusted with thin strips of plasticard, stuck on in layers, then filled with Milliput and smoothed. The area around the coupling was also slightly too large, so again this was modified with plasticard and filler. These changes made the shape look much more accurate.

Above
MPM-10 sets in the depot.
 Photo: Benoît Clairoux, STM.

Below
Cab test print.
Some adjustments were required.



Left
One of the MPM-10 cab cars in the works yard.
 Such a view of the bogies and underframe features is impossible from the platforms.
 Photo: Benoît Clairoux, STM.

Underframe

The underframe was also modelled to fit into the car body, with suitable holes and screw fixing points for the bogies. With the Montréal climate being extremely harsh in winter, the entire system was built completely underground, including the maintenance garages. This means that the only time an MPM-10 was ever seen outside in full daylight, was the day it was delivered from the manufacturer on a lorry. As the entire underframe of the train is painted dark grey, and spends its entire life underground, I found researching the underframe detail next to impossible. Any published photos taken at stations, including my own photos, are always too dark to really get an idea of how the underframe equipment is arranged. There are clearly multiple underframe equipment boxes, yet it was difficult to understand exactly how they were positioned.

So I reached a compromise and simply modelled blocks of equipment below the floor as best as I could from the available information. The underframe captures the look of the prototype, which is the most important thing in HO. If I ever get more accurate information in future, the underframes could be redesigned and reprinted.

Seating

I also virtually modelled an interior, using the General Arrangement drawings to set out seating positions. From other projects, I had virtual seats in HO which were easily imported into SketchUp and duplicated. The seats were all joined together on runners at the outside edge of each car, meaning that once printed, the one-piece seat component – which included all car seats – could be simply glued to the underframe, and would guarantee a fast and accurate installation.

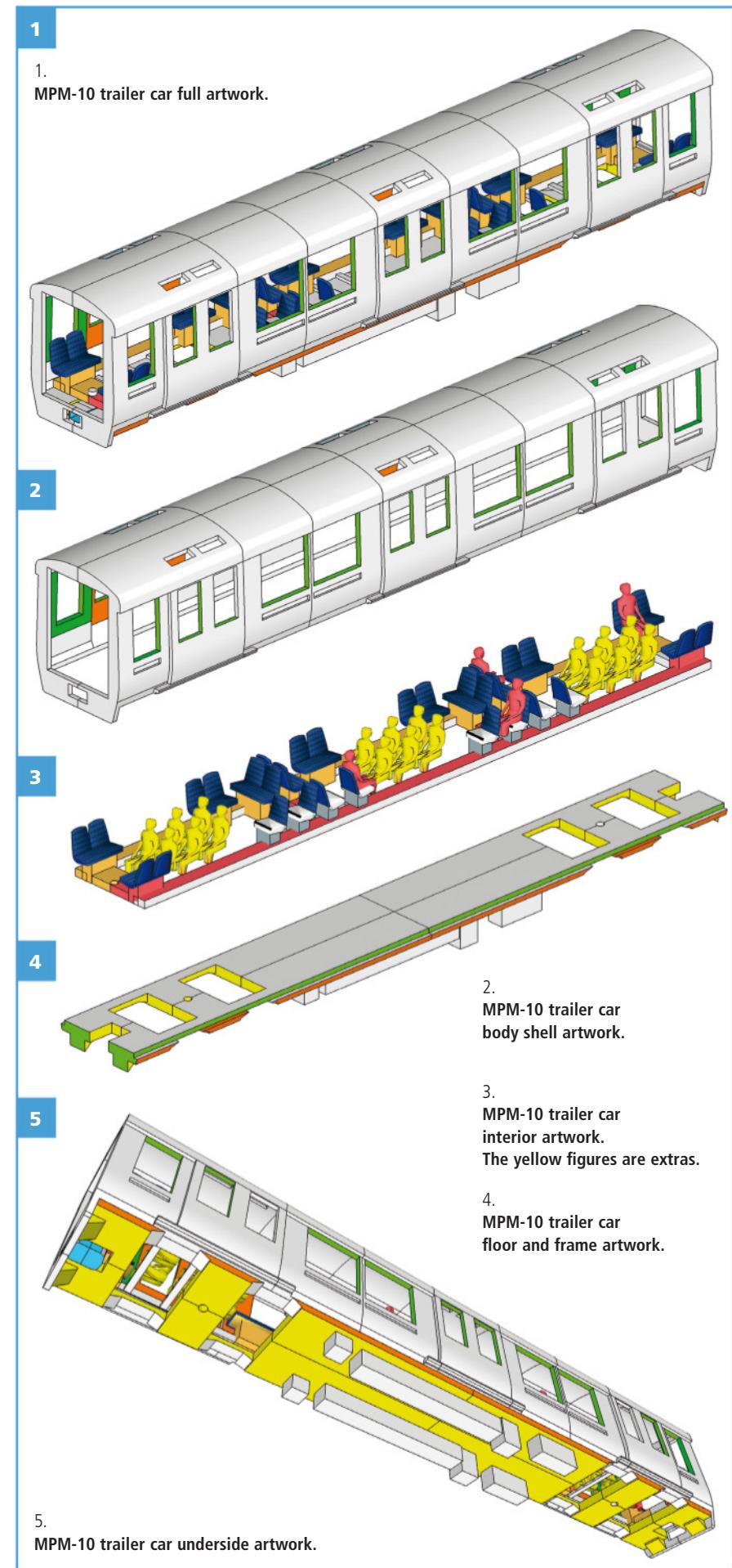
I also took four virtual sitting people and 'sat' them into the virtual seats on the computer to create seated passengers. I did not want all cars to have the same people seated in the same positions, and so numerous additional 'loose' figures were added to the seats, which allowed me to cut out and spread more people around the train randomly, with more crowding towards the centre of the train.

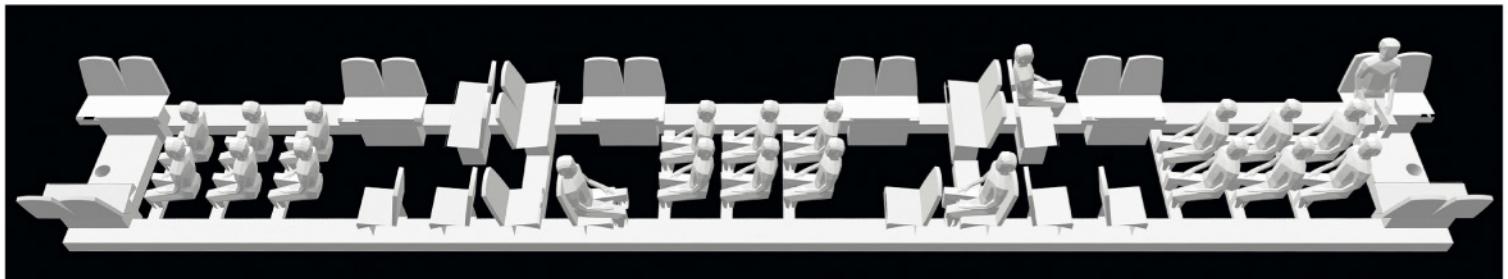
Printing

I decided to model my HO MPM-10 as a six car unit instead of the prototype nine. This was mainly due to lack of space, as the nine MPM-10 would be 1.75m long as opposed to the more manageable 1.17m for six cars.

I used an online print on demand service and had six complete car shells, underframes, and seats printed in white PLA. The results looked good, though with FDM 3D-printing curved roofs always show the print 'steps' and this was no exception. Fortunately, the prototype MPM-10 roofs are smooth with no details, and so Milliput, wet-and-dry paper and patience was the solution to gently remove the steps and create a smooth roof. There was a lot of stringing too on the prints, so a sharp knife and sandpaper was used to clean these up.

Two of the cars were chosen to be adapted to receive the previously printed cabs to become the driving cars. The final passenger window of each chosen car was cut off and the 3D-printed and modified cabs glued on, filled, and smoothed.



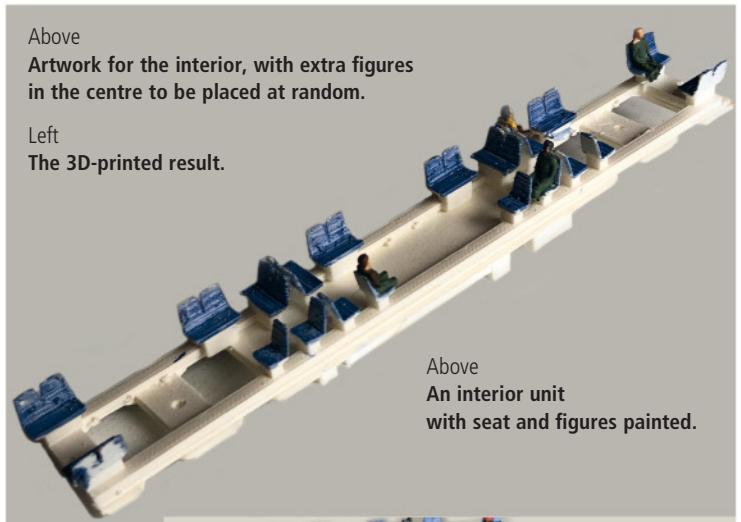


Above

Artwork for the interior, with extra figures in the centre to be placed at random.

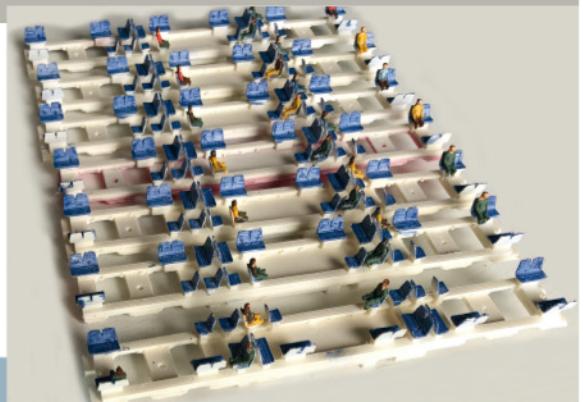
Left

The 3D-printed result.



Above

An interior unit with seat and figures painted.



Bogies

The bogies and tyres were taken from the MR-63 refresh project, as they are very similar. Just like the underframe – being dark grey, they difficult to see clearly and even more difficult to obtain any accurate prototype information. I accepted that I would be less than completely accurate in this area, as long as the finished result looked convincing. And they do!



Above

A 'production line' of painted interior units.

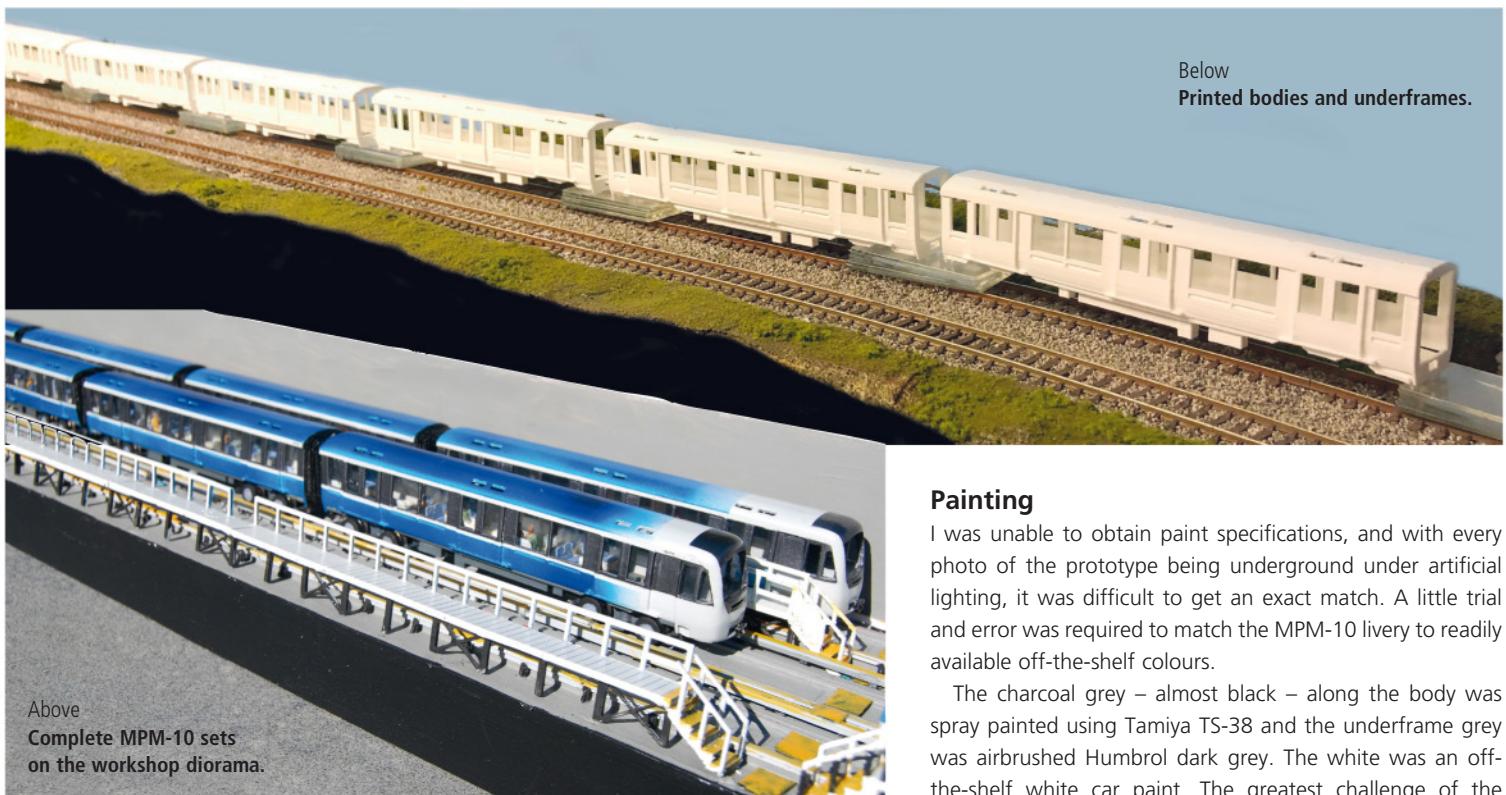
Left

Freshly printed trailer car body shell on painted chassis.

Below

The completed six car set.





Norman Timpe proves that a satisfying layout can be created in a small space.

Zechenbacher Feldbahn

A small rural idyll

Never heard of the Zechenbacher Feldbahn? That is probably because it does not really exist. But it could be an accurate impression of a light railway running in eastern Germany in the 1980s.

Not everything is fake. The name 'Zechenbach' is a real part of my home town in south-western Saxony. It literally means 'pit stream', named after abandoned iron mines in this area. This local content is reflected on the layout by an old overgrown dump.

Now something about the story of this layout. I am a member of the group of narrow gauge modellers well known in Germany as *De Bimmelbahner*. For a long time I only helped running other layouts, but I wanted to build something myself. The main intention was to make something with small dimensions in a manageable time. The basic conditions were: it should be able to be transported in the boot of an ordinary car, and can run as a stand-alone display without the need for a table.



Above
A loaded train leaves the cutting
and approaches the level crossing.

Photographs by the editor.



The dimensions were defined as length 75cm, depth 48cm, and height 43cm. This is not much space for HO ... but a narrow gauge light railway, as supplied by Busch, was the partial solution. Partial? Unfortunately they only offer only set curves. This was not an acceptable option as my motto is 'Build something, but never use it as it came out of the box'. And so most of the elements were scratchbuilt or at least much modified commercial products.

I started with the wooden framework. To check the dimensions, and that what I planned would fit, the whole box was first mocked up actual size in cardboard. I decided the exterior should have a unique appearance. It was not painted, so that the natural wood look was retained.

The problem with the fixed dimensions of the Busch track components was solved by customising flexible HO (6.5mm gauge) track from the former Technomodell company: only the sleepers were used, combined with code 70 rail from Micro Engineering.

The Busch Feldbahn locos contain a magnet and the track has a corresponding metal strip: the attraction is intended to ensure good electrical contact and enhance adhesion so the very light locos can pull a reasonable load. To replicate this function, a flexible magnetic strip meant for the Faller Car System was used below the track.

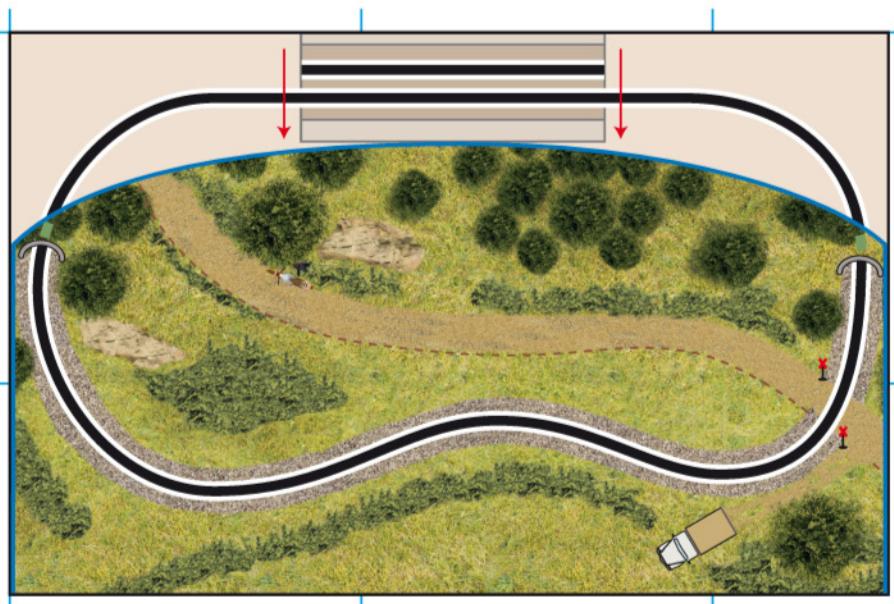
I planned to have loop sidings at the back of the layout for changing trains, but the points needed too much space, so I built a traverser. It is manually operated, fixed in the

middle with a simple screw – very basic, but it works absolutely reliably. Other modellers have reported problems with the Busch points, so this was a lucky alternative.

The landscape was built with well-known methods. As a special feature, the rocks are real stones. Large pieces were broken up so the new surfaces appear more colourful and shiny. The rock is called 'phyllite', a kind of slate, which has a very fine structure and is perfect for downsizing to 1:87.

Above
The scene in its frame, designed to allow viewing from two sides.

Overall size 750mm x 480mm.
Grid squares 300mm x 300mm.





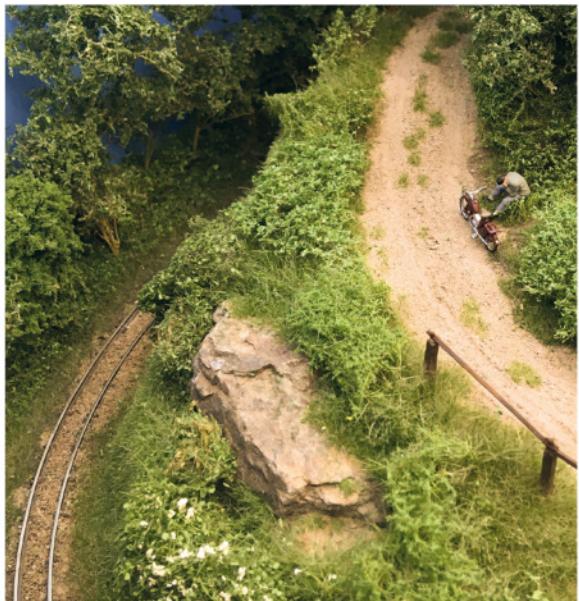
Left
The roof of the box includes LED strips which can be adjusted for brightness and colour temperature if required.

I deliberately kept the trees lower than the full height of the backscene.

Right
The rider of a moped takes a rest on the steep hill where the lane climbs above the railway in the cutting.

Below
A train of empty skips returning to the quarry.





Static grass was applied with the Greenkeeper by RTS.

Bushes and deciduous trees were scratchbuilt. The trees are based on armature kits from MiniNatur/Silhouette. They were cut into pieces, re-assembled, and got new leaves from the Czech firm Polak.

The pine trees were crafted with the wire method. Branches were made with short pieces of thread (cotton or coconut), extended with static grass and finished with leaves from Polak and Heki.

The background was simply made using two sheets of 2mm plastic. There should not be sharp corners, so the sheets were formed with a heater to make round edges. They were then painted by airbrush, and some clouds were added.

A cluster of LED strips was mounted on the underside of the top plate. This means there are no shadows of the pine trees on the backscene, which is better for the effect of more depth.

As important as what was used, and how, is where every element found a place in the overall scene. There is no actual prototype but my idea was to arrange all the components in a way that could have been in reality. I had some images in my mind that I wanted to reproduce, and the rest was arranged around them. The basics were a small farm track alongside the line and a simple level crossing.

In short, it should be a 'stage set' for the small trains in



which visitors could become deeply immersed.

Trains are controlled by a DCC system with a Roco MultiMaus. The Busch locos as supplied have a 3 volt motor, so they had to be changed for new 12 volt ones as well as being equipped with a decoder and a stay-alive. This was quite a challenge in such small models but the combination allows the small locos to run very smoothly and slowly, just like a real field railway.

Most of the skip wagons are from Technomodell. They are very realistic but fragile. Unfortunately production of the Feldbahn range has ceased under the new owners.

All the stock is heavily weathered for realism, showing the daily hard use.

Altogether the time from beginning until completion was about seven months.

The layout has now been shown at several exhibitions and the audience response has generally been (in a word) 'consistent' or 'coherent'. This is exactly what I intended.

I also wanted to show what is possible in a small space. Not having enough space available is often the main reason given for not starting to build a layout. This small creation should prove the opposite.

Finally, I should mention that this is my first really finished layout and another intention was to gain experience for further projects.

Above
The loaded train has beaten the horse and cart to the level crossing.

Below left
Some local road traffic is still animal powered. Perhaps for the best ...

Below
A tractor has been enlisted to assist a loaded lorry that has come off the road.



Reviews

Evaluated by our own specialist staff



N ASF battery-electric shunter new from Arnold

For shunting in wagon workshops and for moving 'dead' locomotives in depots, the Lokomotivbau Elektrotechnischen Werke (LEW) in Hennigsdorf began delivering the first series of EL16 battery-electric shunters (Akkuschleppfahrzeug, ASF) to the Deutsche Reichsbahn in 1966. Based on the proven EL8 mining locomotive, these compact workhorses quickly gained popularity due to their low cost, minimal maintenance requirements, and emission-free operation. Just over five hundred had been built by 1990, in four slightly different series. There were a standard feature in all DR maintenance and repair facilities and also widely used in East German industry, and remained essential even after the founding of Deutsche Bahn AG. Today, these shunters can still be found in major workshops across Germany, including ICE maintenance depots — some even sporting classic white livery replacing the original green and later orange. With the closure of DB facilities and industrial plants, many found new roles in other workshops, private railways, and railway preservation groups. Only a handful have been scrapped to date.

In addition to the DR, many industrial plants with rail connections and foreign customers, especially from Poland, were among the customers for the ASF.

The sturdy sheet metal body with a central cab is bolted to a welded box frame. Two battery boxes are attached either side of the cab. The box lids can be opened for easy maintenance and battery removal, with four maintenance doors each side. The control panel with all necessary controls and gauges is installed in the centre of the cab.

At each end the solid sheet metal buffer beams have two hardwood discs as buffers and a sliding coupling which is operated by a pedal in the cab.



The wheels are just 650mm diameter and run in roller bearings. Each wheelset is driven by a series-wound traction motor via an intermediate gear. Each motor has an output of 8.5kW. To ensure consistent tractive effort, the wheelsets are connected by an automatically adjustable roller chain. Top speed is 6km/h and weight just 12t.

The ASF has a mechanical brake, with four brake blocks acting on the wheels each side. While the first series was equipped with a foot brake, subsequent production features a spindle brake that can be operated from either end of the cab using a hand crank. The foot brake proved unsuccessful due to its force being dependent on the operator's weight and the weather. There was no separate parking brake — the foot brake was locked with a ratchet, the crank brake was simply tightened. With a force of 50kp applied to the handwheel, 78% of the vehicle's mass is braked. The brakes could be adjusted using turnbuckles located in the brake rods below the cab ladders, easily accessible.



The vehicles have foot-operated sanders for each direction. Sandboxes are mounted at each of the four corners behind the end beams. When the button is released, it is pushed upwards by return springs, closing the sand box openings. Controlling the movement a large heavy locomotive must have required some skill.

Arnold have recently released a brand new model of this diminutive machine, their smallest powered product to date. The length over buffers is just 19mm — exactly to scale.

Our sample is in the white InterCity livery; the red stripe and the logos are clearly printed, as is the tiny black number (404 001-0) and yellow warning plates. DR green and orange versions are also offered.

The model has a moulded plastic body with separate handrails and flush glazing.

The side frames are detailed with the sandbox covers at each end and the roller bearing axleboxes.

Not only is it equipped with directional lighting but there is even a NEM 651 six-pin socket for a digital decoder. The model can also be obtained factory fitted with a Zimo multi-protocol four function decoder, though in this case only the lights are switched.

Both axles are driven by reduction gears, and all four wheels collect current. There are no traction tyres! As supplied, in DCC the speed range is from slow to almost imperceptible but even with a wheelbase of only 10mm the model did not falter over Insulfrog points. Remarkable.

Two minuscule scale couplers are provided to replace the practical but necessarily large model hooks.

By any standards, this is a tiny technical marvel for a mass-produced model.



Manufactured for

Arnold (Hornby Hobbies Ltd.),
Westwood, Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICE DC ref.HN2640 £118.49.
DCC ref. HN2640D £158.99.

The Royal Prussian State Railways introduced the type G8.1 superheated 0-8-0 in 1913 as a development of the G8. With almost 5,200 constructed up to 1921 by many different manufacturers, for Mecklenburg and Alsace-Lorraine as well as Prussia, this robust and powerful machine became one of the most numerous steam classes ever built. Classified BR55²⁵⁻⁵⁶ by the DRG from the 1920s, they were in service with both German and some other European railway administrations (including Belgium, Poland, Sweden, and Italy) until well after the Second World War. The Deutsche Bundesbahn inherited around five hundred and it was not until 1972 that the last was taken out of service. The East German Deutsche Reichsbahn still had 150 in use in 1968.

The locos ran with Prussian tenders of types 3 T 16.5, 3 T 20, or 2'2' T 21.5.

Rivarossi announced the BR55 as new tooling in 2022, in DRG, DR, and DB versions with the three dome boiler (in fact one dome and two sandboxes). Our sample is a DR machine with Epoch III markings, all excellently reproduced (in three colours) on the satin black of the body.

55 7254 was allocated to Bw Vacha in the BD Erfurt area; the last service date is 1954.

The body is made up of a metal boiler plus a plastic cab and details: it is accurate in major dimensions and detail, according to published data and photos, and embellished with many separately applied fine parts: generator, blower, feed pump, and pre-heater; valves, pipes, and handrails, of the correct diameters, notably fine on the cab, which has flush-fitting windows and added side windshields. The backhead has moulded features, with regulator and reverser added, and cab curtains.



The metal-bodied tender is equally well detailed.

The valve gear and motion is metal, with plastic eccentric cranks and crossheads.

The motion brackets and some of the pipework under the cabs are made of coloured resilient plastic.

The model is powered by a five pole skew-wound motor with flywheel concealed in the boiler, with a gearbox in the firebox driving onto the rear axle; the side rods couple the other wheelsets. The motor is maintenance-free.

Current is collected by wipers on the backs of all drivers and both sides of the outer axles of the tender.

The loco runs smoothly and very quietly, from a reasonable minimum to a realistic maximum.

All the driving axles are allowed some side play, so the minimum radius is 356.5mm (R1).

Two traction tyres on the driven axle ensure adequate pulling power.

The printed circuit board with a 21-pin MTC socket (NEM 660) for a digital decoder is in the tender, which also has a mount for two rectangular loudspeakers. (Factory fitted sound is an option.)

The model can be easily converted three-rail AC operation; a pick-up shoe is included.

The loco and tender are linked by an eight-pin plug and socket on flexible mounts, length adjustable for prototypical distance.

The model has working directional headlights using warm white LEDs.

The firebox glows red-orange and flickers realistically under digital control.

The front buffer beam is detailed with a coupling hook, brake pipes, and steam heat hose; the rear buffer beam comes with brake hoses fitted and a scale screw coupling is supplied as an alternative for display.

The model has NEM pockets front and rear and is supplied with standard hook & loop couplings, one fitted on the rear of the tender and one provided for the front.

Length over buffers is 210mm.

The model is very well packed in the now customary shaped wrap-around in clear plastic, with a clear plastic sleeve around that, in a foam-lined cardboard box. Loco and tender are packed separately.

Although this model is only recently released, the Hornby International website already advises 'Last Few' – so if you are interested, do not delay!



Manufactured for

Rivarossi (Hornby Hobbies Ltd.),
Westwood, Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICE DC ref.HR2893 £345.49.
DCC sound HR2893S £454.49.



In the late 1970s RENFE had begun planning to modernise its fleet of shunters; initially upgrading the class 303 with new engines was considered and one was converted, but the experiment was not continued. In 1982 bids were invited for a contract for fifty locomotives of up to 1,100hp (820kW). Maquinista Terrestre y Marítima offered its existing model DH-700, an asymmetrical six-wheel diesel-hydraulic with 385kw (516hp) MTU V6 engine and Voith L3r4U2 transmission built under license: this offer was successful and in July 1983 RENFE placed an order for twenty. The modern design offered improved comfort, visibility, and ease of operation, quickly establishing it as an essential locomotive for shunting duties across Spain. Weighing 54t, the top speed is 50km/h, and they are equipped with hydro-dynamic brakes. The first were delivered in 1985 and were accepted into service in 1986 after correction of some technical problems.

After the creation of Adif, these locos were divided between Mercancías (formerly Cargas freight) and Adif (infrastructure services). By 2010 about half had been repainted in the grey and red livery. They were used in the Basque country, Miranda de Ebro, and Cantabria. Two are still in service, six have been sold to industrial users, and one has been preserved.

Electrotren have recently released modified versions of their popular model, with a detailed moulded plastic body shell that is accurate according to published drawings and photos. It is offered in three Epoch VI livery versions - 'Adif' green/white (illustrated), 'FESAVA' blue, and 'Captrain' grey & red.

The cab has a fully detailed interior, including an independently lit control desk and a driver figure.

The finish on our sample is excellent, with well defined colours and very clearly printed markings. Note such small touches as the red caps on the recessed sandboxes.

All the etched metal handrails and grabs are separately applied, and beautifully fine – they could be susceptible to damage, so take care.

Behind the detailed frames, the fine sandpipes align with the wheel treads.

The model comes with one buffer beam fully detailed and at the other end a working model coupler and attenuated fittings; accessories are supplied to enable both ends to be equipped either way.

Also provided are exquisitely printed maker's plates, though there is no information on where to fit them - there is a recess above the front footsteps. (Plates seem to have been removed from the repainted machines.)

A cast metal chassis gives stability and good adhesion; power comes from a small five-pole skew-wound open frame motor with a flywheel, with spur gear drive to all wheels. Running is smooth and quiet, with a realistic speed range. There are no traction tyres, for optimum current collection, and the weight of 139g should give fair hauling capacity.

The model is 16.5mm gauge, not 19.1 (= 1,668mm broad gauge), though the frames seem wide enough to accommodate true to scale gauge.

There are NEM coupler pockets on close coupling mounts at both ends.

The minimum recommended radius is 356.5mm.

The model has directional head and tail lights using LEDs plus cab interior lighting and even an illuminated control desk; all lights can be controlled separately in digital mode.

A 21-pin (E24) socket is provided in the cab floor for a decoder, and above it is a mounting for a loudspeaker.

Analogue DC and DCC models are available, the latter with function couplers and a built-in power bank for improved current pick-up. There is also a sound option.

Higher numbered functions may require programming to re-assign useful and wanted functions to available 'buttons' even on relatively recent and capable controllers.



Manufactured for

Electrotren (Hornby Hobbies Ltd.),
Westwood, Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICE

DC ref. HE2027 £181.99.
DCC ref. HE2027D £222.99.
DCC sound ref. HE2027S £290.99.

New figures and accessories from Preiser



Recently released are more new items announced as part of the 2024 and 2025 programmes.

In the HO exclusive series are four variations of a 'Corsair' type twin sail sailing dinghy, with different coloured hulls, different registration numbers on the main sail, and different crew figures. (refs. 10676, 10677, 10679, and 10681, each £23.25).

Also in the HO exclusive series are a range of mobile sales stands, differently decorated and with appropriate figures and accessories. The shutters can be positioned closed or open.

ref.10747 "Doner Kebab"

ref.10748 "Confectionery"

ref.10749 "Snack Point"

Each £35.50.

The HO Exclusive range has also gained a number of diverse fairground stalls, again supplied as ready-made models with figures and accessories.

ref.24692 "Toys"

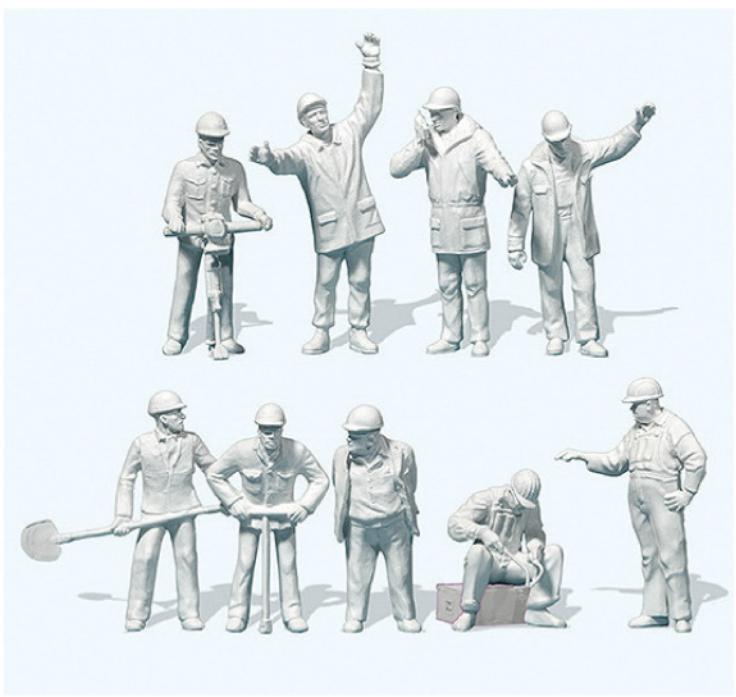
ref.24693 "Porcelain"

ref.24694 "Shooting Gallery"

A rather more mundane market stall, and probably dating from an earlier age, is represented by ref.10742 "Potato Sales", which features two farm workers selling their wares from a horse-drawn wagon, with a table, a chair, sacks, a set of scales, and a sign. (£35.50)

Also available for detailing good shed and platforms is a pack of forty assorted postal parcels (ref.17703, £14.50).

All these new HO items are finely formed in plastic and carefully painted by hand.



For Gauge 1 (1:32 scale) there is a new pack of nine unpainted plastic figures of railway and track workers (ref.63003, £23.25). Some accessories are included, and some of the figures have separate and interchangeable heads and limbs to enable unique poses to be created.

At the other end of the scale, quite literally, there is a new pack of eighteen unpainted miniature figures for 1:200 architectural model making, "Standing Women and Men, Workers" (ref.80992, £5.25).

All these figures are finely formed.



Manufactured by

Paul M.Preiser GmbH
Am Ruhbach 2,
D-91628 Steinsfeld, Germany.
www.preiserfiguren.de

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www.gaugemasterretail.com

PRICES in text.



Loco-Revue**Les Dossiers Thématiques 08****Entre rail et asphalte**

Edited by Alexis Avril

Éditions Loco Revue,
BP 30 104, F-56401 Auray Cedex, France.
www.locorevue.com297mm x 210mm 116pp
Softback €16.80
ISBN T.B.A. ref.L11659
French text

The leading French model railway magazine *Loco-Revue* continues its now quarterly additional offering with another themed collection with the emphasis on practical modelling.

This eighth issue (dated June – July – August 2025) is concerned with the links between rail and road, and as usual for this series shows prototype practice and modelled examples.

The material is presented in three sections:

1. From road to rail
 - loading road vehicles onto rail wagons, notably the characteristically French UFR and 'Kangarou' systems of trailers on flat wagons, with dedicated equipment;
 - loading double-deck bogie car carriers with not only cars but also motor bikes;
 - a movable loading ramp for the above, made from a brass kit;
 - a loading dock for UFR trailers made in card;
 - loading agricultural vehicles and machinery on TWA low-floor flat wagons;
 - the transhipment of goods between rail and road, from hand cranes to container portals.

2. Road crossings

- the many types of level crossings;
- making a working type SAL 2 crossing;
- track inset into a cobbled surface;
- road markings;
- road signs;
- period road signs in HO.

3. Tyres and steel wheels
 - fitting a working drive to a static Atlas model of a Floirat *autorail*;
 - weathering road vehicles to a level comparable with rail rolling stock;
 - using the Faller Car System;
 - making a Brimont dual road-rail tractor for shunting wagons from a 3D-printed kit.

The coverage goes into considerable detail. Modelling projects have actually been carried out and are generally illustrated with step-by-step photos, plus examples drawn from layouts previously published in the monthly magazine. Tools and materials required are listed for each project. Commercial products are identified, and a list of suppliers rounds off the text.

While all of the modelling is HO, most of the techniques would be valid in other scales/gauges.

The articles are by a number of contributors. They are all substantial and range in length from two to twelve pages, with many sub-sections.

The work is very well illustrated, with clearly drawn diagrams and step-by-step photo sequences. Bar a handful of historic images, the photos are all in colour and well reproduced on good quality silk paper.

As usual for *Loco-Revue*, the presentation is clear and straightforward. The occasional 'tip' is highlighted separately, while lists of tools and materials are on tinted panels.



A reasonable ability in French will be required to get the most from the text but the illustrations make it useful for any modeller of the French scene.

The book includes just two and a half pages of advertisements for other LR Presse publications and products.

**Modellbahn-Kurier 58****Digital 2025**

Edited by Ralph Zinngrebe

EK-Verlag GmbH
Munziger Straße 5a, D-79111 Freiburg, Germany
www.eisenbahn-kurier.de
280mm x 210mm 84pp
Softback €13.90
ISBN 978-3-8446-1761-0
German text ref.1761

This new publication from *Eisenbahn Kurier*, dated spring 2025, is the annual survey of digital control systems and components.

This issue opens with a large digitally controlled American HO layout, around 450m of track, which uses ESU decoders, Digitrax and Uhlenbrock control equipment, and Train Controller software.

Brief reports of recent new products occupy four pages, but more detailed reviews and consideration of specific products include:

- the Zimo app and updates;
- a new central unit from RailBox, economical and innovative;
- yet another digital supplier: from Digikeyi emerges Yamorc;
- the DB V100 B-B diesel in HO from ESU.

There is also a survey of digital central units without controls or screens, and consideration of model railway apps for Windows, Android, and IOS devices.

There are articles on fitting decoders (not just for beginners); using digital control to facilitate an operating concept rather than just operation; layout control with ESU equipment; expanded functions of the Märklin CS3, which already has block control built in; automatic operation quickly achieved – step-by-step to block control using Rocraill software with a Märklin CS3; route selection with Uhlenbrock components; and the Soundtraxx 'Blunami' control system, wireless via Bluetooth, applied to American locos.

The collection concludes with a look at the special considerations of using digital control for a garden railway – there is more to it than just needing higher power capacity.

The articles are from various expert contributors as well as EK staff, and range between two and eight pages. As can be seen, many relate to particular brands and even specific products while others are more general.

Overall, an interesting mix of background information, practical applications, and product analysis, well illustrated with photos, screen shots, and diagrams.

There is the equivalent of ten pages of advertising, many for other EK publications but with some for relevant digital products and suppliers.

This should be a useful reference both for anyone already familiar with digital operation and those considering it, helping to keep up with the latest products and developments. However, good German will be needed to get the full benefit as much of the content is inevitably technical.

Kursbuch der deutschen Museums-Eisenbahnen 2025

Compiled by Sabine Ressel and Sebastian Werner

Eisenbahn Kurier Verlag,
Munziger Straße 5a, D-79111 Freiburg, Germany
www.eisenbahn-kurier.de
210mm x 140mm 180pp
Softback €8.50
ISBN 978-3-8446-6845-2
German text ref.6845

This handy guide to museum railways in Germany is published by Eisenbahn Kurier in collaboration with the Verband Deutscher Museums- und Touristikbahnen (VDMT). Over the years it has grown in size and scope, and the level of detail, to reflect the number and variety of the railway museums and preserved lines in Germany: the VDMT now has over a hundred members.

The core of this work are the timetables of all active museum railways, along with information about the route, location, access, operating days, prices, a summary of the motive power in use, and contact details. Around two hundred and fifteen are listed. Almost

all entries also include a QR code which will connect appropriate devices to the relevant internet site.

Despite the increased availability of information on line, this useful guide remains popular. The reason is clear – in a convenient compact form, it provides the enthusiast with all they might need to know about societies, museums, preserved railways, and railway companies with any preserved and/or historic stock.

The information is arranged Land by Land, roughly from north-west to south-east. An overall map at the start explains the regions, then specific locations are noted on a map at the opening of each regional section. A comprehensive contents list also aids finding

Modellbahn-Kurier Special 52

Faszination Spur 1

Ausgabe 32, 2. Quartal 2025

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297mm x 210mm 82pp
Softback €19.90
ISBN 978-3-8446-1965-2
German text ref.1965

This edition of the series devoted to Gauge 1 begins with the 'Einheitslok-Exot' (five pages) by Thomas Montaperti, a detailed look at the new BR87 tank engine from Dingler. Eleven colour photos show the wealth of detail to the cab, boiler, and running gear. Six variations of the model are offered, in photographic grey, in DRG black with red running gear with a Hamburg-Wilhelmsburg allocation plate, in the same livery but with an Essen Hbf. allocation, and in early Reichsbahn livery of green and brown.

'Wilchingen: Schweiz in 1:32' by Hans Jürgen Götz (seven pages) is the second part of the description of Kurt Hablitzel's Swiss loft layout.

Over four pages Oliver Strüber takes a detailed look at the ÖBB Breuer shunting tractor from Fine-Models.

Seven pages report the latest products available for Gauge 1, including a set of six *Edelweiß* coaches from Märklin, the ET403 'Donald Duck' Inter City electric unit from Wunder, and a Deutsche Bundesbahn V90/BR290 diesel-hydraulic from KM1 in various liveries. More short news items and notes occupy another two pages.

A list of model railway events with Gauge 1 interest taking place across Germany covers the period from mid May to September 2025.

In 'Märklins VT88.9', Peter Pernsteiner looks at the new model of the so-called Wismar 'Schweineschnäuzchen (pig's snout) type VT88.9 four-wheeled railbus in Deutsche Bundesbahn crimson red..

A visit by the magazine's editorial team to the Dampfbahn Fränkische Schweiz (DFS) in Ebermannstadt is reported over three pages.

'Rangier-Bügeleisen' by Thomas Montaperti (six pages) examines the new model from Wunder of a DB E63 electric shunter, with prototype background.

In 'Wechselladung' (five pages) Frank Mäule describes how he created exchangeable loads for his wagons, with no fewer than 40 colour photos.

'Kleine "Bockerl"' (seven pages) is a review by Thomas Montaperti of the new model from BMB of a 'Glaskasten' 0-4-0T in DB form and as No.3 of the Vereinigte Chamottewerke Marktredwitz in green.

In 'Helvetia', Thomas Montaperti gives an account of a diorama featuring a section of main line in Switzerland by the Leuvense Spoorren Vrienden club in Belgium.

'Vorbild: Zeche Holland' (three pages) with text by Ralph Zinngrebe and photos by Frank Zarges showing how he made a façade as a background for a scene using a kit for a mine building from Bünning, based on an original building in Gelsenkirchen.

Thomas Montaperti is back with 'Kohle-44er der DB' (three pages) looking at the new model of a BR44 2-10-0 from Spur 01 Modellbahnen.

In the penultimate section, 'Eisenbahn auf der Straße' (seven pages), Markus Schmidt describes and illustrates the second part of modelling railway lines through the streets.

The final feature, 'Akku-Rangierbörcke' (four pages) is a review by Thomas Montaperti of the models by Dingler of Deutsche Bundesbahn battery shunting locomotives of classes Ks, Ka, and 381.



A good knowledge of German will be required to get the best from it.

Numerous advertisements from model manufacturers are included, from the likes of Märklin, BMB, HEGOB Modellbahn GmbH, ASOA, GK Glaskasten, Spur 1 Austria, Wunder, Spur 01 Modellbahnen, MO-Miniatur Modellbau, and KM1. There are also a few advertisements for model shops in Germany and for other products available from Eisenbahn Kurier Verlag, namely books, magazines, and DVDs.

Zeunert's Schmalspur Bahnen Band 54

Edited by Dirk Endisch

Verlag Dirk Endisch
Yorckstraße 12a, D-39576 Stendal, Germany
<https://verlag-endisch.de>
240mm x 170mm 96pp
Softback €18.50
ISBN 978-3-947691-54-8 German text

This issue is not dated, but the most recent news items date from 25th March 2025.

The lead article is a substantial feature (twenty-four pages) by the editor about the metre gauge Steinhuder Meer Bahn (StMB) between Wunstorf and Rehburg Stadt, one of the most famous light railways in the Federal Republic, using photos taken by and from the collection of the magazine's founder, the late Wolfgang Zeunert. The use of diesel locos and railcars could not prevent closure in August 1970.

There are seven pages presenting brief news items from all over Germany, including the DEV, the Döllnitzbahn, the Molli, the Öchsle, the Preßnitztalbahn, the Pollo (Prignitz), Rügen, the SDG, the SOEG, and the Schwarzbachbahn. Despite reporting modest increases in passenger numbers, the serious consequences of the reduction of state subsidies to the Saxon lines is causing concern and cut-down timetables.

In addition there are four pages devoted to news from the Harz, notably the temporary closure of the

Harzgerode branch for track maintenance, the return of diesel 199 861-6 from overhaul, and the unfortunately necessary demolition of the rotten wooden station building at Benneckenstein.

There are no reports from Austria or Switzerland in this issue.

The other main articles are:

– goods traffic on the Loßnitzdackel (Radebeul-Ost – Radeburg) (twelve pages) documented by the editor with the aid of old timetables and photos from his collection.

– memories of the metre gauge Biasca – Acquarossa line in southern Switzerland (ten pages); Dr Markus Strässle recalls this 13.8km line in the Blenio valley with archive images and a good range of colour photos taken on a visit in September 1973 by R.Schmidt.

– prototype and model: extensive (twenty-four pages) background information (including scale drawings) about 2-10-2T 99 222, the only survivor of the three original standard locos built in 1931 for the Eisfeld – Unterneubrunn line in Thüringia, on the Harz system since 1966 as a prelude to a thorough review of the new ILM (1:22.5) model by Kiss/Fine Models (seven pages).

There are five further pages devoted to modelling, featuring locos, rolling stock, and accessories from manufacturers large and small (Bemo, Busch, Fine Models/Kiss, LGB, pmt, Roco, Tillig, and Weinert) compiled by Heinz Hofmann, mostly using supplied illustrations.

any specific line. Lines which cross a state border are listed only once, under the principal location.

The information was correct at February 2025, but it is noted that things can change.

The book includes a request for all preserved lines to send information for the 2026 edition as soon as possible.

The work is illustrated with occasional black & white photographs, placed in relevant places to balance the content, all well reproduced on good quality paper. There is colour on the covers and just a few pages within, mostly for selected advertisements. The most recent illustration is from December 2024.

Basic German will be adequate to understand the information, which is essentially all in tables.

There is the equivalent of around twenty-seven pages of advertisements, most for other EK publications, but some from the lines and institutions featured, distributed through the book, placed as far as possible where relevant.

In short, a mine of information, both for planning railway visits in a particular area and checking to see what may be nearby and when it will be operating. It represents amazing value for the amount of data presented.



As usual, the work is copiously illustrated with black & white and colour photos, all well reproduced on high quality paper. The earliest colour dates from July 1970; the most recent images are March 2025.

The photo content means that even those with only basic German should be able to appreciate this book.

There is a page of advertising for other Endisch publications and half a page previewing the next issue.





Eisenbahn Video-Kurier 164

Schwerpunkt: 25 Jahre Baureihe 185

The main topic of this looks at the Bombardier TRAXX class 185 electric locomotives, which have now been in service for twenty-five years. It begins with views of the first of the class, 185 001, at the InnoTrans exhibition in Berlin in 2000. Further footage shows 185s used by DB Cargo and Railion seen from the lineside at the head of a variety of heavy freight trains.

In Nürnberg and Mannheim, 185s are seen on shed. Examples used by SBB Cargo of Switzerland (classified 482) are also featured filmed from the lineside on heavy freight workings, including along the Rhine Valley, where both car and container trains are seen. 185s in use with private operators such as Rail4Chem and Captrain are then shown, and Inter Connex's 185 515 is seen on a passenger service entering and leaving Marburg. In the Bombardier factory 185s are shown under construction, including Railpool's 185 682 which is later seen passing through a washing plant. 185s in service with DB Cargo are seen on a variety of different freights filmed from the lineside, including 185 277. Some aerial views are also featured. Examples sporting advertising vinyls are then featured along with others in use with private companies such as hvle, MRC, Metrans, and TXL.

The next feature is 'Diesel-Power im Kohlenrevier' and here we see Deutsche Reichsbahn 'Taigatrommel' 120 274 (leading) and 'Ludmilla' 132 158 double-heading a heavy photographers' special coal train of bogie hopper wagons. Later 120 274 is seen at a depot being turned on the turntable and the driver is interviewed in the cab. Views inside the engine compartment are also featured. Later scenes show this train headed by 120 274 with 132 158 assisting at the rear.

The next feature shows standard 750mm gauge 2-10-2T 99 787 as rebuilt with 'light-oil' firing undergoing test runs on the Zittauer Schmalspurbahn.

EK-Videothek DVD c.58mins + 45min Bonus ref.8564 €19.80

That is followed by a look at 'Blue Tiger' class 250 diesel-electric locos. First, RAG 250 001 is seen emerging from a depot shed, with some scenes filmed inside the cab as the loco moves forward light engine to couple up to a heavy train of bogie hopper wagons. Later scenes of this train were filmed from the lineside and in the cab. OHE 'Blue Tiger' No.3300 is then shown at the head of a train of empty container flats, and 250 00 is observed at the InnoTrans exhibition in Berlin in 2002. Later we see this loco in service on a train of silo wagons with 250 010 at the rear. We then see an mkb example filmed from the lineside on a train of bogie hopper wagons, and an hvle machine is seen on the Rübelandbahn running light. Another of this company's 250s was filmed from the lineside with bogie hopper wagons, and ITL Captrain examples are also shown.

The *Erinnerungen* section has colour film from 1990 of 2-6-0T 74 1230 at Berlin-Charlottenberg departing from the station with a special passenger train, passing a 'Ludmilla'-hauled express as it accelerates away. Further scenes were filmed on the footplate and from the lineside. The train is formed of seven four-wheeled coaches. At Wustermark depot, the locomotive is seen running light onto the turntable; it is turned and moves off smokebox first into the shed.

Bahn News shows NEB two-car BR563 units on local passenger services at Erkner, south-east of Berlin, followed by views at Berlin Hbf. of the first day of operation of the new ICE service to Paris Est.

In the final *Rückblick* section we are treated to archive black & white film of steam locos at Bw Hamburg-Altona depot, including Pacific 03 263.

A certain amount of background music is used on this production, mainly in the *Rückblick* section, but it is not too intrusive.

Trailers for other DVDs from Eisenbahn Kurier Verlag last for about ten minutes.

The 'Bonus' section features an old report from Die Odenwaldbahn between Darmstadt and Eberbach, with steam locos of classes 01, 23, 50, and 62 and diesel-hydraulics of classes 215 and 218 – for your reviewer an unexpected pleasure!

The Eisenbahn Kurier railway video library is published by EK-Verlag, Munziger Straße 5a, D-79111 Freiburg, Germany. www.eisenbahn-kurier.de



Die DR-Baureihe 243/143

Universal-Ellok aus Hennigsdorf

The BR243 Bo-Bo electric locomotives were delivered to East Germany's Deutsche Reichsbahn (DR) in the 1980s. This successful class was later used throughout Germany after the country's re-unification, classified 143. They saw extensive use on Deutsche Bahn AG DB Regio passenger trains, particularly on the Black Forest (Schwarzwald) and Höllentalbahn lines, and also on S-Bahn services. Withdrawals began in 2008. This programme tells the story of this dependable

class with film of their construction at LEW Hennigsdorf, and their service with DR and DBAG in the 1990s. We see them on a variety of both passenger and freight workings, including those which are now in use with private operators. Also not forgotten is a look at the transition to the classes 112 and 114.

It begins with film shot from the lineside of 143 922 in DBAG traffic red livery at the head of a DB Regio service formed of double-deck coaching stock. Other members of the class are seen on similar workings, including at Königstein, plus some on a variety of freight duties in their original DR Bordeaux red colour scheme. Also shown are some 143s now in use with private operators. At Chemnitz Hbf. 143 157 is seen on a DB Regio service formed of double-deck stock, followed by a similar service in the Saale Valley, where 143 098 is seen at Rudolstadt. 143s on DB Regio services formed of silverfish (*Silberlinge*) coaching stock are also featured, including at Stuttgart Hbf., where we see 143 109 entering the terminus. At Nürnberg Hbf. 143 022 is seen departing with a DB Regio service and in Köln 143 259 is shown working an S6 S-Bahn train to Essen Hbf.

This is followed by lineside footage of 143s at the head of a variety of different freight services. We see a driver climb aboard 143 904 with views filmed from in the cab and equipment room. The loco moves off shed light engine to couple up to a train of steel coils waiting in the sidings, and we see this train accelerate away.

Next is archive DR publicity film of new 243s with DR, including film shot in the cab on passenger services at stations and from the lineside. Film of these locos under

EK-Videothek: Baureihen DVD c.58mins ref.8675 €22.80

construction at LEW Hennigsdorf is shown before we see examples in original DR Bordeaux red at the head of a variety of different freights filmed from the lineside, including along the Elbe Valley line.

In post-reunification days we see 143s on Inter-City and Inter-Regio passenger services, with some views in the cab. In the snow we see 143s on passenger services on the Frankenwaldbahn and on the Schwarzwaldbahn. In Nürnberg orange & pebble grey liveried examples are seen on S-Bahn services with matching coaches. At Witten Hbf. an S-Bahn service is seen arriving and departing with orange & pebble grey 143 060 at the rear. Other similarly liveried 143s are observed from the lineside with double-deck stock on the Höllentalbahn and in Neustadt (Schwarzwald) station, where 143 679 is seen.

We move to 143s in the orient red colour scheme, including at Eberbach, where one is seen arriving on a train of double-deck stock. 143 272 is seen in this livery in ex-works condition. In Werk Dessau we then see 143s being overhauled, including traffic red 143 117 and 143 558. Further scenes of 143s in this livery are featured, working DB Regio services of double-deck and silverfish stock on the Höllentalbahn, including 143 332 and 143 810, and at Geislingen (Steige) station.

The prototype 243, the so-called 'White Lady', in white with AEG lettering on the body sides and carrying the number 143 001, is seen at the head of a special of green DR and red MITROPA coaches. This loco has since been renumbered 243 001.

This is followed by film from the lineside of privately owned 143s working a variety of freight services, including 112 001 of the Erfurter Bahn Service double-headed with a Bordeaux red 143 on a train of bogie tank wagons. Later scenes show just 112 001 at the head of a train of motor vans, and brand new 112s are shown in orient red livery, so too 114 017 in traffic red while 112 121 is seen on a train of DB Regio double-deck coaches. Two yellow DB Bahnbau 143s are also shown.

The programme ends with further lineside scenes of 143s on freights and on DB Regio services double-deck coaching stock, including 143 932 in the snow at Demmin, south of Stralsund.

The commentary, which is only available in German, is both clear and informative. Very little background music is used.

Around ten minutes of trailers for other EK DVDs follow the programme.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Munziger Straße 5a, D-79111 Freiburg, Germany. www.eisenbahn-kurier.de



75 Jahre Uerdinger Schienenbus

EK-Videothek: Baureihen DVD c.58mins ref.8678 €22.80

Legendärer Rettet der Nebenbahnen

The first prototype VT95 diesel railbus entered service with the Deutsche Bundesbahn 75 years ago, in 1950, having been developed by Waggonfabrik Uerdingen. Two years later series production of the VT95 (from 1968 BR795) commenced and they were seen as being the *Retter der Nebenbahnen* (saviour of the branch lines). A few years later they were followed by the larger and more powerful VT98 (from 1968 BR798) and by 1962 over a hundred of these vehicles were in service. Their deployment ended around 2000, but some remained in service on the Chiemgaubahn until 2014.

This programme tells the story of these iconic rail vehicles using both historic and present day film material. Their use on preserved railways is also covered.

The programme begins with scenes at Gerolstein on the Köln – Trier line where a preserved VT98 railbus and trailer is seen departing the station with a service to Kaisersesch on the Eifelquerbahn. Further scenes of this working were filmed from the lineside. Further preserved VT95 and VT98s are seen from the lineside, including around the Nördlingen area in Bavaria, and VT95 465 is seen at the Technical Museum in Berlin. Later this vehicle is seen filmed from the lineside and on arrival at Basdorf. Views inside show the passenger accommodation.

We then get archive black & white film of VT95s in service with DB and archive colour film shows the later VT98 type (with buffers and screw couplings) including at Tübingen Hbf. In Garching a BR798 and trailer is seen departing with a service to Mühldorf (Oberbay.). Further scenes of this working were filmed from the lineside. Film from the 1990s at Neuenkirchen shows a 798 and trailer departing the station. At Siegen we see 798s on shed and being checked over by the maintenance staff. A 798 is seen being refuelled, leaving the depot, and being coupled to a trailer. Further scenes show it from the lineside. At Tübingen Hbf. a set of three 798 and trailers is seen and followed from the lineside, plus views inside the passenger accommodation and from the cab. The last working of a 798 and trailer is then shown departing Tübingen Hbf. in spring 2000. Further views of this service were filmed from the lineside and from the cab.

We then pay a visit to the Chiemseebahn where at Prien am Chiemsee a mint green, turquoise, and grey 798 and trailer is seen departing. Further scenes show this train from the lineside, from the cab, and on arrival into Aschau, the terminus. This is followed by sad scenes of withdrawn 798 railbuses and trailers awaiting their fate.

Derivatives of the 798 railbus are then shown, such as the 701 *Turmtriebwagen* for overhead catenary maintenance. 701 156 is seen in DB crimson red (RAL 3004) livery performing these duties. Yellow examples are then seen in main works undergoing overhaul. Examples of other similar specialist vehicles include the *Messwagen* classes 725, 727 and 728. At Zittau a preserved former DB VT98 is seen departing with further scenes filmed from the lineside and from the cab and in the passenger compartment.

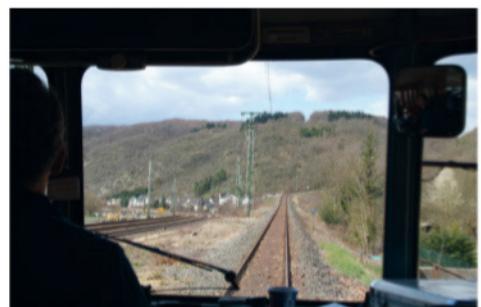
On the Prignitz system (PEG) at Neustadt (Dose) we see ex-DB 798s departing the station, and a 798 and trailer in the blue PEG livery are filmed on board and from the lineside. At Pritzwalk a single PEG 798 is seen arriving and departing. A crimson red example is also shown on this railway, filmed from the lineside and from in the cab. Laaske and Putlitz stations are also shown. At Putlitz the 798 is seen terminating.

Next we see examples of VT98 railbuses in Austria, followed by a preserved 798 in DB crimson red leaving Linz (am Rhein) on the short but steeply graded branch to Kalenborn. Further scenes of this were filmed from the lineside and on arrival at and departure from Kalenborn. This is followed by scenes of a Dessau-Görlitzerbahn 798 and trailer filmed from the lineside while at Nördlingen a 798 and trailer is seen departing on the last day of semaphore signals at this Bavarian town. Further scenes of this working were filmed from the lineside and at Dürrenzimmern. At Aufhausen a single 798 is seen passing through the station, followed by film of a 795 and trailer at Königs Wusterhausen, south-east of Berlin. The programme ends with scenes of a 795 and trailer at Friedersdorf being passed by a MIREO three car set and then departing.

The commentary, which is only in German, is both clear and informative. There is some background music but it is not intrusive.

Around ten minutes of trailers for other EK DVDs follow this programme.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Munziger Straße 5a, D-79111 Freiburg, Germany. www.eisenbahn-kurier.de



Right
798 760-5 leaving
Linz am Rhein and climbing
the steeply graded
Kasbachtal branch
to Kalenborn
on 13th April 2013.
Photo: editor



Die Baureihe 58³⁰

Die starke Reko-G 12 der Deutschen Reichsbahn

Around 1,500 of the legendary Prussian G12 three-cylinder compound 2-10-0s were built between 1917 and 1924 for use on heavy goods trains in the Mittelgebirg mountainous areas of Germany. On the Deutsche Bundesbahn none remained in service after 1953, but in East Germany fifty-six were rebuilt with newly developed type 58E boilers and smoke deflectors, becoming class 58³⁰. This programme shows these Reko G12 locomotives on enthusiasts' tours, photographic specials, and scheduled services in their old areas of operation with scenes filmed both from the lineside and from the footplate. Preserved 58 3047 is featured prominently.

The coverage begins with scenes filmed from the lineside showing 58 3047 on a train of green Deutsche Reichsbahn coaches. Later at Bw.Zwickau the loco is seen moving onto the depot's turntable. This is followed by footage on the footplate and from the lineside on a freight. At Bf.Amerika (Sachs.) it is seen passing another freight hauled by a DR BR228 diesel-hydraulic. 58 3047 is then seen in a DR colour film on scheduled goods trains filmed from the lineside. 58 1111 is also seen at Bw. Schwarzenberg moving off the turntable, with further film shot on the footplate and from the lineside heading a heavy train of hopper and open wagons.

Next we see archive black & white film taken at Neusörnewitz, near Meißen. There is film of these locos being built and receiving new boilers. Colour film then shows further members of the class on a variety of different freight services

EK-Videothek: Baureihen DVD c.58mins ref.8676 €22.80

This is followed by more recent film of 58 3049 and 58 3047 alongside 58 201 at a depot. In fading early evening light 58 3047 is seen running light, tender first, and coupling up to a timber train. The train is later seen moving off double-headed, with 58 3047 leading 58 111. Further scenes were filmed from the lineside and on the footplate of 58 3047.

Another double-headed freight formed of bogie hoppers is then seen, this time with 58 3047 leading and 52 8154 with tub tender as the train engine. The train is observed moving off from a stand with further scenes from the lineside. Later 58 3047 is seen alone on another freight service and then double-heading another freight.

At Reichenbach 58 3047 and a BR52 are seen passing through on another freight train, with further scenes of just 58 3047 at the head of a train of vans and later on bogie open wagons, with a BR50 assisting at the rear.

Then there are scenes at a loco depot. The driver and fireman are seen checking over 58 3047 as a DR BR219 diesel-hydraulic passes by light engine. Later 58 3047 is seen working tender first on a heavy mixed goods, and at the head of a seven coach passenger train (smokebox first) en route to Neuenmarkt-Wirsberg. At Bw.Glauchau the loco is seen on shed in the company of some DR BR228 diesel-hydraulics.

Next 58 3047 is seen again from the lineside at the head of a train of bogie silo wagons, then moving off double-headed with bogie hoppers leading a BR50. Some scenes of this train were obtained on the footplate. The programme ends with film taken in the snow of the same loco on another seven coach passenger train.

The commentary, only available in German, is clear and informative. There is little or no background music, used mainly towards the end and during archive scenes, but it is not intrusive.

Around five minutes of trailers for other EK DVDs follow this programme.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Munziger Straße 5a, D-79111 Freiburg, Germany. www.eisenbahn-kurier.de



Darjeeling stock ready-to-run in OO9 from Precision Model Works



Precision Model Works, devoted to the creation of high quality scale models and miniatures, offers a wide array of Indian Railways models that bring history to life in intricate detail. Each model, fabricated using high resolution 3D printers and then handcrafted with the utmost care by a team of skilled craftsmen, captures the essence of legendary locomotives, rolling stock, and accessories, making them a delight for enthusiasts.

They have two processes – small batch production and 'made to order' items. They typically take pre-orders for small batch productions and deliver the whole set of models in one go after the completion of the production process, irrespective of when the orders are placed.

For 'made to order' items they usually ship an item within 60-75 days of receiving the order.

They provide some customisation options depending on capacity, such as realistic weathering and installing additional features (at extra cost).

Following the success of their accurately modelled range of Darjeeling Himalayan Railway kits, they are now working on completed versions of these beautiful miniatures that depict the modern DHR as ready-to-run train sets and train packs as well as individual locomotives, coaches, and wagons in OO9.

The models are 3D-printed in high definition and professionally painted and detailed with accurate livery and lettering. Many fine details are separately added.

The train sets feature a basic oval of Peco OO9 Setrack, a DC controller, and a re-railer along with the NDM6 diesel and a choice of modern bogie coaches and classic four-wheel wagons.

The locomotive features a high quality Kato 11-109 mechanism and has a fully detailed cab interior complete with a driver figure and openable door/

Coaches have a fully visible interior and the option of fifteen pre-installed figures.

There will also be train packs without track and controller.

The range includes the NDM6 diesel (with a choice of numbers), AC coach, second class coach, luggage/brake coach, and four-wheel vans and opens. The stock is fitted with Peco OO9 couplers and runs on metal wheels.

Sample prices – loco + two coach pack Rs19,196.43; loco Rs9,598.21; coach Rs4,687.50; vans (2 pack) Rs2,901.79; opens (2 pack) Rs2,678.57. (This makes the loco c.£82 at the time of writing.)

For international customers, Indian GST will be deducted at checkout. Customs duties and local taxes are included and the customer will need to pay the same at the time of delivery.

The models can be pre-ordered now and are expected to be delivered in autumn 2025, approximately six months from the time of placing the order, subject to the availability of raw materials and supplies.

To protect the finest details from damage during shipping, some separately added details will be sent unassembled and will require the customer to add them to the model. These would be very minor and all you need is a pair of tweezers and some superglue.

Note that these are 3D printed models and despite all efforts it is impossible to avoid some print lines and 3D printing artifacts; these are not defects. The detail parts are also fragile and will not survive rough handling.

These models can also be produced for HOe (1:87), running on 9mm gauge track.

For the complete selection and to place an order, see the website.

<https://precisionmodelworks.co.in>

Kaeserberg

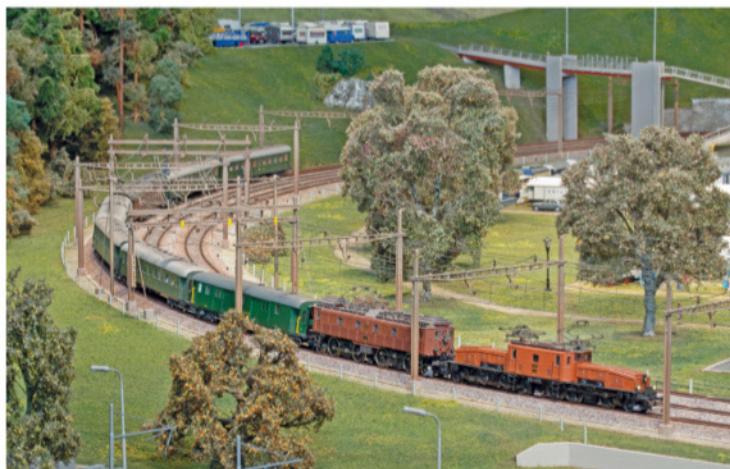
train of the month

SBB Ce6/8^{II} of 1920 + Be4/6 of 1921, both in brown livery, with a train of seven heavy bogie coaches, with a maximum speed of 65km/h.

This express from the 1930s has survived to this day – it has been part of the SBB Historic fleet since 2001. It runs sporadically on the Kaeserberg layout, as it does in real life. It commemorates the trains that were run for the 100th anniversary of the Gotthard line. The fact

that a 'Crocodile', intended for freight, makes an appearance adds to the fun, even though it would have been rare during the heyday of electric traction. Both locos were added to the historic rolling stock collection in 1975/76.

Kaeserberg
Impasse des Ecureuils 9,
CH-1763 Granges-Paccot,
Switzerland.
www.kaeserberg.ch



Latest from Cités Miniatures



HO
PLM Cooperative at Paray-le-Monial
This building, from the railway town of Paray-le-Monial (Saône-et-Loire), boasts quite remarkable architecture. Abandoned for many years, it has recently been superbly renovated and converted into holiday cottages.



When the railway town was active (1930s-1940s), this building was the town's 'supermarket'.

This kit (ref.BV-022-HO) goes perfectly with the Villeneuve St.Georges railway town structures released earlier this year.

N
Shelter for diesel fuel pumps
The umbrella shelter for a diesel depot fuel point developed for HO has been adapted to 1:160 (ref.ED-091-N).

Cités Miniatures
145 Avenue Henri Ginoux,
F-92120 Montrouge, France.
www.cites-miniatures.com

New from American Z Line



American Z Line is pleased to announce the release of an all new body style in the shape of the General Motors – Electro-Motive Division FP7, initially decorated for the Western Pacific with two road numbers, 804A and 805A.

The models come DCC-ready and feature the AZL 7mm motor, dual flywheels, prototype specific details, metal stirrup steps, directional LED lighting, blackened metal wheels, optional front truck with mounted coupler, and AutoLatch™ couplers.



The GM-EMD SD50 is offered with two new Union Pacific road numbers, 5050 and 5056

The models have directional LED lighting, blackened metal wheels, traction tyres, AutoLatch™ couplers pre-installed, front pilots/ploughs, optional truck with coupler, and prototype specific details.



The GM-EMD SD40-2 is available with two new Wisconsin Central road numbers, 6002 and 6005.

The models represent early production with 81" nose, nose headlights, brake wheel, extended roof, prototypical aerials, bay windows, Nathan K3LA air horn, dynamic brakes, open grid radiators, original fans, small width anti-climber, low snow plough, 3,200 gallon fuel tank, and HTC trucks.

All AZL SD40-2s include directional LED lighting, blackened metal wheels, traction tyres, AutoLatch™ couplers, and optional pilots/ploughs.

The ALCO RS-3 gets two new Boston & Maine phase III road numbers, 1506 and 1510.



www.americanzline.com

New from Halling



Rack railway coaches for HOn3z

Halling are replenishing stocks of the 10.5mm gauge rack railway models from Ferro-Train. They have also taken the opportunity of expanding the range. In response to popular demand, there are now also two different open observation cars to match the Schafbergbahn passenger coach.

All are painted in a uniform purple-red colour with white stripes. Numbers 5 and 6 are of wooden construction and are equipped with a new weight which allows all the new vehicles to run much more smoothly due to the low centre of gravity.

Berlin S-Bahn stock for HO

The class 481/482 are electric railcars for third rail operation on the S-Bahn. With around five hundred quarter trains (two car units), it is now the most common type on the S-Bahn Berlin. The first quarter train was presented to the public in 1996. As part of the ongoing refurbishment, it is planned to be in service until the 2030s. Since 2019, S-Bahn Berlin has been overhauling and modernising over three hundred quarter trains. This involves completely dismantling the vehicles, removing corrosion, repairing the solebars if necessary, and then painting them in a new 'redesign' livery based on that of the class 483/484 series. Four hundred sets have now been overhauled.



Halling are giving their models a similar makeover. The new editions of the class 481/482 models have been modernised from the ground up. They are second models to be converted to the ALX underfloor drives, and all the electronics have been moved to the driving trailer. The space that was freed was filled with a huge weight. The resulting extremely low centre of gravity and the two powered bogies in the motor car, coupled with the slow and quiet but very powerful bell-type armature motors, transform the model.

The new edition has new car numbers, and Halling have again made sure that a half train consisting of two quarter trains has consecutive car numbers.

But what really deserves the 'redesign' title in the model is the new light board, which replaces the strange light guide construction from the previous version. Now micro-LEDs illuminate both headlights evenly. It will be possible to retrofit older models – a kit will be offered as soon as the series has been completed.

Halling Modelle
Leopoldigasse 15-17,
A-1230 Wien, Austria.
www.halling.at

Rokuhan train approach alarm for Z

Rokuhan has announced a new Z scale trackside accessory, a train approach alarm. In the USA, these are similar to ground signals or searchlight signals. The Rokuhan version (ref.S073-1) connects to the feeder slot on straight sections of track. The signal flashes when current flows to the track.

There are three colours to choose from – orange, blue, and green (all included in the package).

The alarm was expected to be available in July.

www.americanzline.com



Exhibition Diary

Dates, events, and information

Saturday 19 July

CRAWLEY, West Sussex

Clubroom Open Day

Organiser: Seaboard Southern Model Railroad Group.

Venue: Hut 13, Tilgate Park Recreation Centre, Tilgate Drive (off Brighton Road), Crawley, RH10 5PH.

Open: 1000-1600

Admission: free.

Amenities: free on-site parking; wheelchair access; complimentary tea/coffee.

Features: members showcasing extensive HO and N layouts, also On30 models on display. Secondhand items for sale.

Contact: Malcolm Dickie (Secretary) 07915 068816.

E-mail: Malcolm.g.dickie@gmail.com

Website: www.seaboard-southern.org.uk

Saturday 19 & Sunday 20 July

CHATHAM, Kent

Organiser: Chatham & District MRC supported by Bachmann Europe plc.

Venue: Historic Dockyard, Slip 5, Main Gate Road, Chatham, ME4 4TZ.

Open: Saturday 1000-1700

Sunday 1000-1600

0930 entry for advance tickets.

Admission: adult advance tickets £9.00, on the door £10.00. U16s free with paying adult.

Amenities: parking at Historic Dockyard Chatham and Dockside Outlet Centre – note all parking facilities have tariffs. Chatham railway station is a 35-40 min walk from the public Historic Dockyard entrance, also served by local Arriva buses. Dockyard museum entry at half price is available with show tickets. Dockyard has food and drink outlets.

Features: 40+ layouts including *Obermatt* (Swiss HOm), *Mont Blaireau* (French HO), and the Kent G Scale Group. Demonstrations, trade.

E-mail: info@chatham.show

Website:

www.chatham-mrc.co.uk/exhibition

Saturday 19 & Sunday 20 July

CUPAR, Fife

Organiser: Cupar & District MRC.

Venue: The Corn Exchange, St.Catherine's Street, Cupar, Fife, KY15 4BT.

Open: 1000-1630

Admission: adults £7.00, accompanied U16s free.

In aid of MacMillan Cancer Support.

Features: 18+ layouts including *St.Adrian* (Swiss Z, CM February 2003) and *Michaelerplatz* (Austrian HO, CM August 2013). Demonstrations, trade.

Website: www.cuparmrc.co.uk

Saturday 26 July

BATH

LARKRAIL

Organiser: Titfield Thunderbolt Bookshop

Venue: New Oriel Hall, Larkhall, Bath, BA1 6RA.

Open: 1030-1630

Admission: adults £8.00, accompanied children free. Proceeds to Julian House (helping the homeless in Bath) and Parkinson's UK.

Amenities: limited parking nearby; free minibus link from Tollbridge Studios at BA1 7DE or take First Bus service 7 from Bath bus station. Wheelchair accessible. Refreshments from Kim's Kitchen (famous for cakes!).

Features: light railway and branch line model miscellany. Demonstrations, trade.

E-mail: simon@titfield.co.uk

Website: www.titfield.co.uk

Saturday 26 July

HITCHIN, Hertfordshire

Organiser: Letchworth MRS.

Venue: The Priory School, Bedford Road, Hitchin, SG5 2UR.

Open: 1000-1630

Admission: adults £5.00, U16s free.

Amenities: parking; disabled access; refreshments.

Features: 16+ layouts including *Sorrento Park* (US TT, CM August 2023), *Kamiak Falls* (US HO, US HO, CM April and July 2022), and *Gare de Brindille* (WV1 SM32, CM November 2019).

Website: www.letchworthmrs.org.uk

Sunday 27 July

ALDRIDGE, West Midlands

Models in the Museum

Organiser: Aldridge Transport Museum.

Venue: Aldridge Transport Museum, Shenstone Drive, Northgate, Aldridge, Walsall, WS9 8TP.

Open: 1000-1600

Admission: adults £6.00, children £2.00, U5s free, family £15.00. Includes entry to museum.

Amenities: free bus every half hour – see website. Free parking on site, plus park & ride.

Features: c.20 layouts, including *Le vieux hameau* (French HOe, CM July 2025). Trade, museum shop.

Website: www.amrtm.org

Saturday 26 & Sunday 27 July

NORTH SHIELDS, Tyne & Wear

RAILEX NORTH EAST

Organiser: Blyth & Tyne MRS. Sponsored by Durham Trains of Stanley.

Venue: Parks Sports Centre, Howdon Road, North Shields, Tyne & Wear, NE29 6TL.

Open: Saturday 1000-1700

Sunday 1000-1600

Early entry for advance ticket holders – book tickets via show website.

Admission: adults £10.00, children (5-16) £5.00, U5s free, family (2+2) £22.00. Card payments accepted.

Amenities: bus from North Shields Metro station; free parking; disabled parking, access via ramp and lift.

Features: 40 layouts including *Shasta* (US Z, CM March 2014), *Iron Flatts* (N), and *Kiyomi* (N). Demonstrations, trade.

Contact: Brian Taylor, 07957 1234317.

E-mail: exhibitions@btmrs.co.uk

Website: www.railexne.co.uk

Please send details for possible inclusion to:

Peco Publications & Publicity Ltd., Beer, Devon, EX12 3NA.

Telephone: 01297 21542

E-mail: clubs@railwaymodeller.co.uk

Saturday 26 & Sunday 27 July

STOKE ON TRENT, Staffordshire

Venue: St.John's School, Trent Vale, Stoke on Trent, ST4 6SB.

Open: Saturday 1030-1630

Sunday 1030-1600

Admission: adults £7.00, children free with accompanying adult. Proceeds to charity.

Amenities: ample parking; disabled access; light refreshments.

Features: layouts include *Bregenbach im Schwarzwald* (German N, CM September 2022).

Contact: 07747 087050.

E-mail: jrfcox93@gmail.com

Saturday 26 July

BARNSTAPLE, North Devon

Organiser: Barnstaple MRC

Venue: Christ Church, Bear Street, Barnstaple, Devon, EX32 7BU.

Open: 1000-1600

Admission: adults £5.00, children (6-16) £1.50.

Amenities: Seven Brethren and Fairview long stay car parks, charge £2.50. Barnstaple station, on the Tarka Line from Exeter via Crediton, is 6 minutes walk. Disabled access. Refreshments.

Features: layouts include *Nevaro Springs* (US HOon3). Demonstrations, trade.

Contact: 07974 785840

or 01271 858727.

E-mail: hayesclass50@gmail.com

Website: [https://barnstaplemrc.weebly.com/exhibitions.html](http://barnstaplemrc.weebly.com/exhibitions.html)

Sunday 27 July

SEVENOAKS, Kent

Organiser:

Dawn Quest, Model Railway Quest.

Venue: Sevenoaks Primary School, Bradbourne Park Road, Sevenoaks, TN13 3LB.

Open: 1000-1600

Admission: adults £7.00, seniors and U16s £5.00, family £15.00, Rail/TFL staff £5.00. (Discounts if pre-booked.)

Amenities: free parking on site and in adjoining roads. Full wheelchair access. Café.

Features: 19 layouts and 12 traders.

E-mail: modelrailwayquest@gmail.com

Website: www.modelrailwayquest.com

Saturday 2 & Sunday 3 August

BEER, Devon

Venue: Station Gallery/Lecture Theatre, Pecorama, Underleys, Beer, Devon, EX12 3NA.

Open: 1000-1600

Admission: included in Pecorama special 50th anniversary entry prices: adults £7.25, concessions (ages 3-17 & 65+) £6.25, family (4) £24.95, family (5) £29.95, under 2s free, dogs £1.00. Additional Beer Heights Light Railway train rides £4.00.

Amenities: parking; disabled access (N.B. hilly site); refreshments.

Features: visitors are invited to help operate modular digitally controlled Swiss HOm layout *The Andeer Line*. (Full instructions will be given.)

Website: www.pecorama.co.uk

Saturday 9 August

BEXHILL-ON-SEA, East Sussex

Organiser: Bexhill MRC

Venue: St.Richard's Catholic College, Ashdown Road, Bexhill-on-Sea, TN40 1SE.

Open: 1000-1700

Admission: adults £6.00, children £2.00, family £14.00.

Amenities: free parking on site. Refreshments available.

Features: layouts include *Neustift* (N), *Rybnik* (N), and *Out West* (US On30, right, coming in CM). Societies, trade.

Website: www.bexhillmrc.com

Saturday 16 & Sunday 17 August

CRICH, Derbyshire

Organiser: National Tramway Museum.

Venue: National Tramway Museum, Crich Tramway Village, Crich, near Matlock, Derbyshire, DE4 5DP.

Open: 1000-1730 both days.

Last admission to museum site 1600.

Admission: pre-booked online (at least 24hrs in advance): adults £23.00, children (aged 4-15) £14.00, family (2+ up to 3 or 1+ up to 4). On arrival: adults £25.00, disabled adults £19.00, children £15.00, disabled children £13.00, family £55.00. Free admission for one carer per disabled person. Free return admission for 12 months on full priced tickets.

Amenities: car and coach parking. There are access ramps or lifts to all visitor facilities. A 'smoothway' provides a route for wheelchairs and buggies, and there are disabled toilets, but unfortunately the line's disabled-accessible tram is awaiting replacement. Refreshments at several venues on site.

Features: nine layouts including *Hampshire Hill Interurban Trolley* (below right). This displays the rural way of life in New England connecting the small towns of Concordia and Vermontville, the latter the site of the car barn and sub-station. At Hampshire Hills, midway between the two, the line interchanges with the Boston and Maine Central.

Contact: 01773 854321.

Website: www.tramway.co.uk

Saturday 16 & Sunday 17 August

ST.ANDREWS, Fife

Organiser: East Neuk MRC.

Venue: St.Andrews Town Hall, Queen's Gardens, St.Andrews, Fife, KY16 9TA.

Open: Saturday 1000-1730

Sunday 1030-1730

Admission: adults £6.00, children £3.00, family (2+2) £15.00. Cards accepted.

Amenities: St.Andrews Park & Ride (located at the western edge of the town) allows visitors to park and take a bus into the town centre, reducing congestion. Venue does have disabled parking (payable) and is fully accessible. Nearest railway station is Leuchars, on the Edinburgh to Dundee & Aberdeen line approximately five miles from St.Andrews. Stagecoach bus No.99 also runs to Dundee city centre. Buses X60 and X59 run from Edinburgh to St.Andrews Central Bus Station, a short walk from the venue.

Coming next month

out 21st August



Features: around ten layouts, some with a local Scottish flavour, plus demonstrations and trade.

Contact: 07552 122484.

E-mail: Contact@eastneukmrc.co.uk

Website: www.eastneukmrc.co.uk

Saturday 23, Sunday 24, and BH Monday 25 August

BEER, Devon

Venue: Station Gallery/Lecture Theatre, Pecorama, Underleys, Beer, Devon, EX12 3NA.

Open: 1000-1600

Admission: included in Pecorama special 50th anniversary entry prices: adults £7.25, concessions (ages 3-17 & 65+) £6.25, family (4) £24.95, family (5) £29.95, under 2s free, dogs £1.00. Additional Beer Heights Light Railway train rides £4.00.

Amenities: parking; disabled access (N.B. hilly site); refreshments.

Features: visitors are invited to help operate modular digitally controlled Swiss HOm layout *The Andeer Line*. (Full instructions will be given.)

Website: www.pecorama.co.uk

Bank Holiday Monday 25 August

HARROGATE, North Yorkshire

Organiser: Harrogate MRG

Venue: Constance Green Hall, St.Aidan's School, Oatlands Drive, Harrogate, HG2 8JR.

Open: 1000-1600

Admission: adults £7.00, children £2.00, family (2+2) £15.00.

Amenities: free parking. Local buses. Nearest railway station is Hornbeam Park, c.15 minutes walk. Easy access for those with impaired mobility. Cafeteria.

Features: layouts include Arcadia, PA. Terminal (US O, in this issue) and Karolina Falls (US On30, CM September 2014). Demonstrations, trade.

E-mail: contact-us@harrogate-model-railwaygroup.org.uk

Website: www.harrogate-model-railwaygroup.org.uk

Saturday 30 August

TADWORTH, Surrey

Organiser:

North Downs Model Railway Circle.

Venue: The Good Shepherd Church Hall, Station Approach Road, Tadworth, KT20 5AE.

Open: 1000-1700

Admission: adults £6.00, seniors/disabled £5.00, U16s £3.00.

All proceeds to Good Shepherd Church.

Amenities: free parking; refreshments.

Features: 27 layouts including Neumarkt (German HO), Five Mile Siding (US HO), Lafayette Spring (US HO), and Willis Bluff (US HO). Trade.

Contact: Stuart Robinson, 07758 458827.

E-mail:

stuart.robinson27@ntlworld.com

Website: www.ndmrc.info



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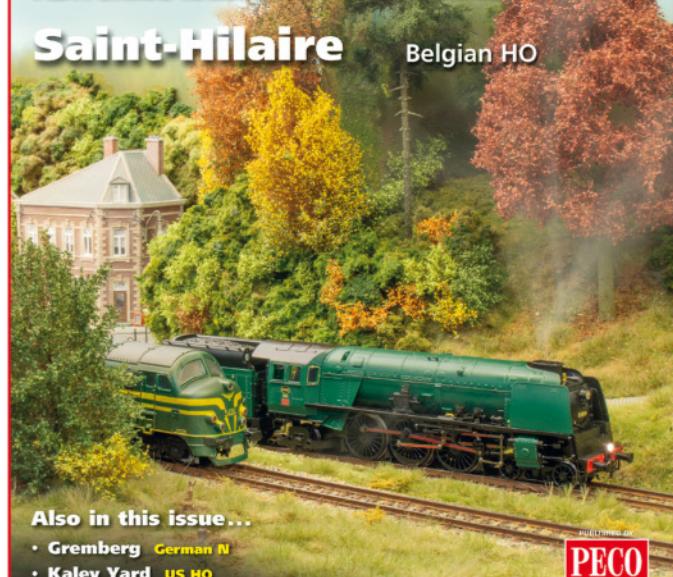
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Saint-Hilaire

Belgian HO



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- Gremberg German N
- Kaley Yard US HO
- grain elevator Canadian HO

... and much more

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• Saint-Hilaire

Steven Deruytter presents his Belgian HO layout and explains how it was planned and built with a simple track plan for exhibition display.

• Gremberg

Dries Reubens describes his first exhibition N layout, the loco depot for a large freight yard near Cologne, originally created as a competition entry.

• Kaley Yard

Trevor Smith introduces the second level of his large American HO permanent home layout; this new section is set in Orlando, Florida.

• Terminus

Belgian diesels at the end of the line – Rudi Nelissen crafted an evocation of industrial decay in HO.

• Grain elevator

Andrew Eastabrook has provided a new source of freight on his unusual Canadian HO layout, The Pas & Northlands, which can also be used as an independent display.

... and much more!

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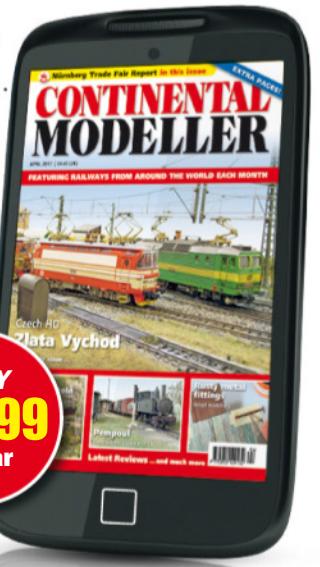
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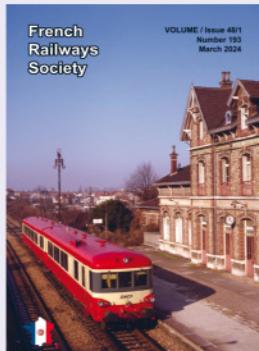
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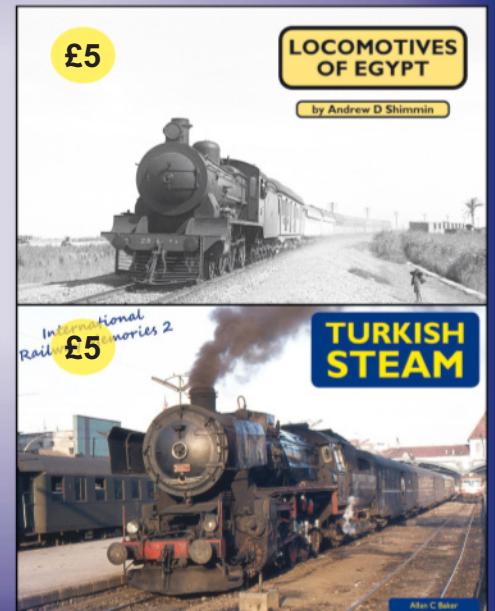
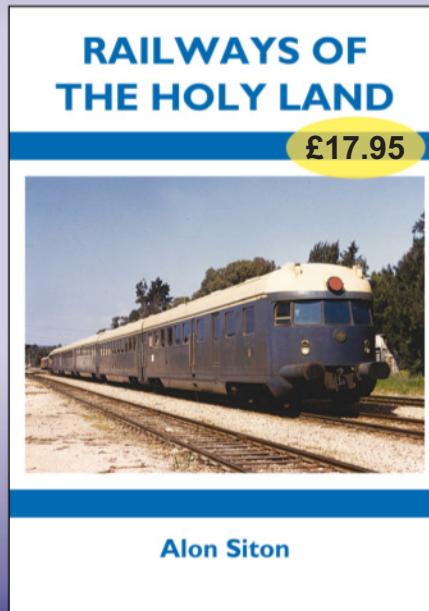
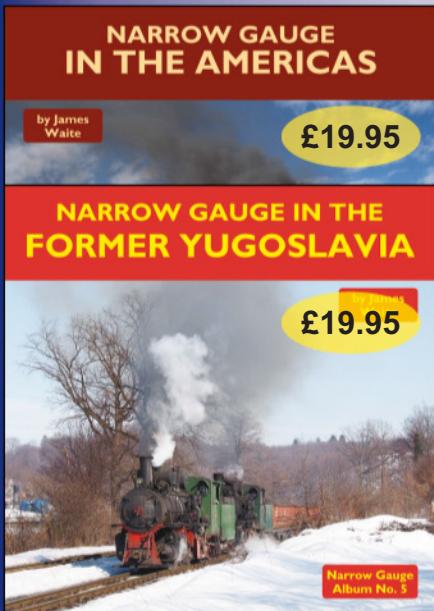


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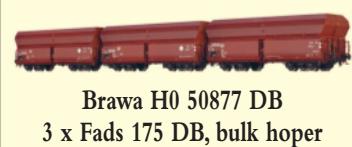
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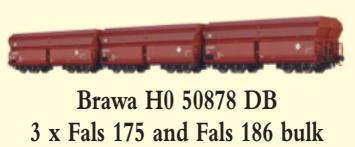
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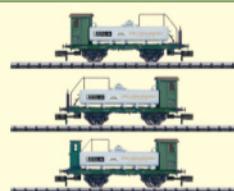
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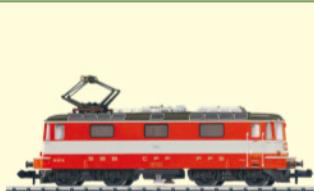
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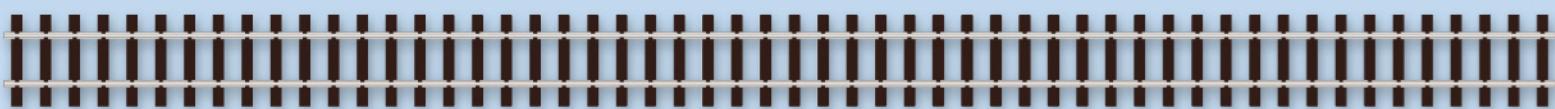
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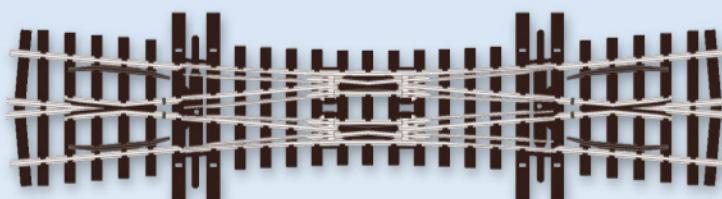
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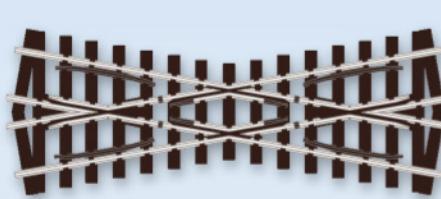
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