

CONTINENTAL MODELLER

JULY 2022 £5.99 (UK)

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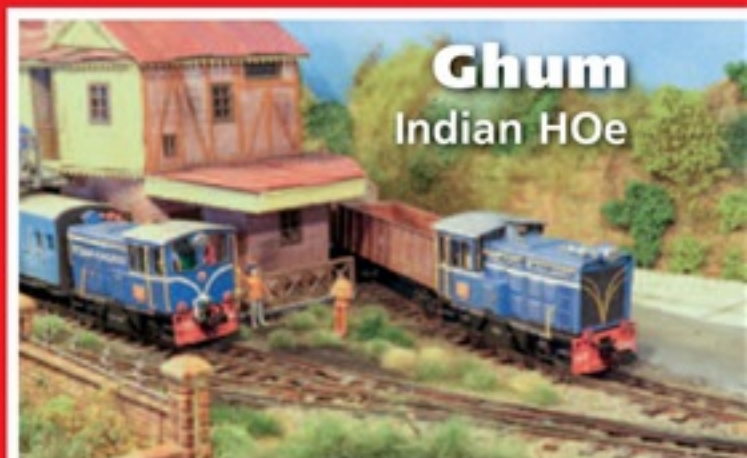
Japanese N

**Also in this issue...**

US HO
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French 1:35



Ghum
Indian HOe

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10475 GP9 SPSF£138.00

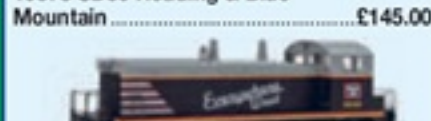


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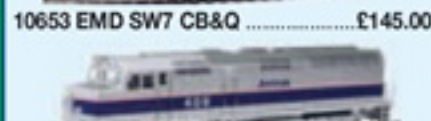
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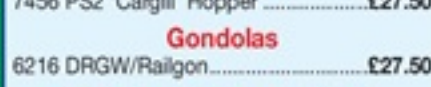
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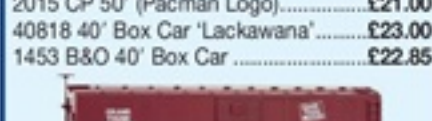
1198 Northern Pacific 40'£27.50



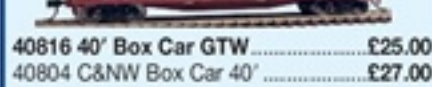
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40816 40' Box Car GTW£25.00

40804 C&NW Box Car 40'£27.00

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1124 Union '76 3 Dome£22.50

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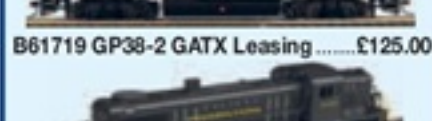
41211 40' North West Reefer£28.00



41218 SF 40' Wood Reefer£27.70



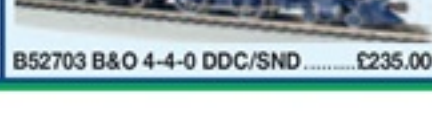
A3028 Pennsy RS3 DCC Ready£150.00



B61719 GP38-2 GATX Leasing£125.00



B52703 B&O 4-4-0 DCC/SND£235.00



B19806 40' Grand Union Reefer£29.95

B19805 Reefer 'Pure Carbonic'£24.00

B19803 40' ART Reefer£27.00

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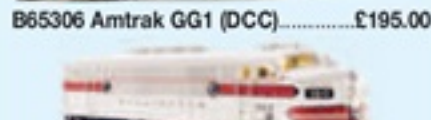
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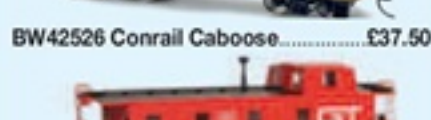
BR6262 Southern Pacific Cab Fwd£590.00



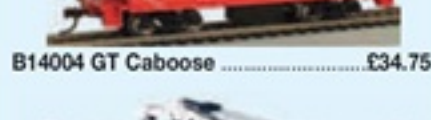
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BW42526 Conrail Cabooses£37.50



B14004 GT Cabooses£34.75



B16149 Boom Crane & Tender (unlettered)£58.95



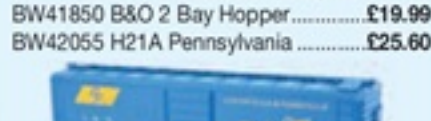
A5807 Reefer 'Nuckolls'£38.00



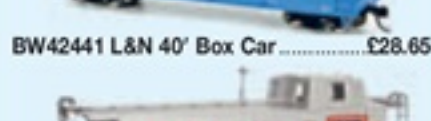
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BW41850 B&O 2 Bay Hopper£19.99

BW42055 H21A Pennsylvania£25.60



BW42441 L&N 40' Box Car£28.65



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1753 Burlington Northern£28.75

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L612888 Railroad Crossing Kit£49.95



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Lattice Footbridge£27.95



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B45978 Water Tower£25.70

AMB494 Dry Goods Store (laser cut wood)£46.20

A6916 Steel Water Tower£44.10

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HO Scale

Broadway 6368 Pennsylvania GG1

Single Stripe, Keystone Logo£399.00

Walthers Trainline post & packing £4.20



101 GP9 Burlington Northern£69.00



102 GP9 Union Pacific£69.00



103 GP9 Santa Fe£69.00



113 GP9 AT&SF£69.00



2503 GP15 CSX£78.00



451 GP9 Boston & Maine£69.00



1501 Burlington Northern£24.00



1503 Santa Fe£24.00



1505 CSX£24.00



1520 BNSF£24.00



1756 Union Pacific Track Cleaner£28.75

1482 D&RG Track Cleaner£28.75

1753 Burlington Northern£28.75

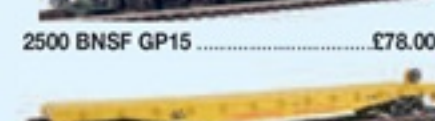
1100 Spare Pads for Track Cleaner£5.95



2505 Union Pacific GP15£78.00



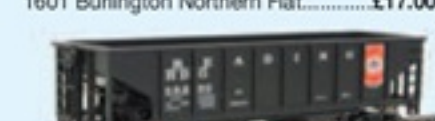
2500 BNSF GP15£78.00



1603 UP Flat£17.00

1462 D&RG Flat Car£17.00

1601 Burlington Northern Flat£17.00



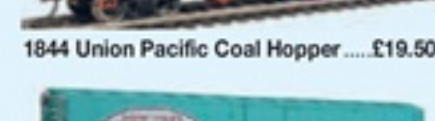
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1426 Illinois Central Hopper£17.50

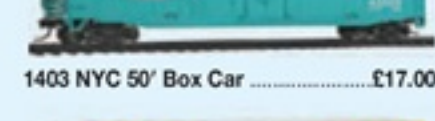
1655 Offset Hopper N&W£17.50



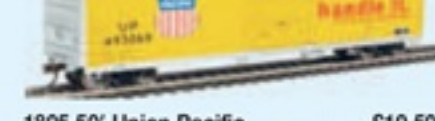
1844 Union Pacific Coal Hopper£19.50



1403 NYC 50' Box Car£17.00



1805 50' Union Pacific£19.50



1441 BCOL Tank£17.50



1444 Santa Fe Tank£17.50



1612 Gulf Tank£17.50

CONTINENTAL MODELLER

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Volume 44
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RAILWAY MODELLER

For all modelling British railway practice.
Published on the second Thursday
of the preceding month.



Cover
An Odakyu Railway 60000 MSE
(Multi Super Express)
six car unit (by Micro Ace)
crosses the bridge on the
approach to Ishinaka station.
Photo: editor.

The benefits of modelling

We are constantly delighted by the range of subjects which interest and enthuse our contributors and (hopefully) our readers. This issue is another excellent example of the typically varied mixture: Germany, France, The Netherlands, America, Japan, India, and South Africa. Some make use of proprietary products, with various degrees of adaptation or detailing, others would require scratchbuilding – though that does not seem to deter creative modellers.

It has also been encouraging to find that article material continues to arrive, suggesting that the hobby is in a healthy state. There is considerable evidence that people have made good use of their time in lockdown, whether to make progress with an existing project or to undertake a diversion into a different scale, gauge, or subject, often as a compact or 'micro' layout – and they do not come much more micro than Ian Arkley's HO of *Feldbahn*! We were also particularly struck by a remark in Ranjeev Dubey's article: "Time is not fungible". We should make the best use of it.

It has been good to see exhibitions and events happening again, and to get out to some of them. The diary is filling up nicely – indeed, we are already finding that some events clash, and we expect that will happen more later in the year. It may take time for attendances to return to pre-pandemic levels, for various reasons: many people are still sensibly cautious, while travelling is more expensive for visitors, exhibitors, and traders.

The situation is not so settled, it seems, in mainland Europe, even though international travel is now possible again: OntraXS! in Utrecht and Intermodellbau in Dortmund were cancelled again, in March and April respectively, though economic prudence may be as significant a factor as official restrictions. We have also heard that the major Modellspeer bi-annual show planned for Leuven/Louvain in October cannot take place as there is still uncertainty over when the hall used will be released from service as a vaccination centre, which poses a problem for planning.

Then just as it seems we are at least beginning to get over the pandemic, another problem looms with price rises, in large part as a result of the situation in Ukraine and its impact on world economics. Of course models and raw materials have not been, and will not be, immune.

Moving manufacturing of models to China was to benefit from lower costs, but for some while now costs there have been rising and manufacturers have been looking for alternatives. Roco, for example, while never making much use of Chinese sources, do now have a sizeable plant in Vietnam. Until now it was mainly used for rolling stock and budget locos, so the ÖBB Rh1923 Vectron reviewed in this issue may be among the first full range products to be made there. Some components (e.g. motors, decoders) are presumably sent out – though these are not large in bulk compared to the finished items. But as well as the production, the cost of shipping around the world, not to mention the environmental impact, will sooner or later become a real concern for our hobby: we play with things made out of plastic and powered by electronics which use rare and valuable minerals, etc. It seems likely that there may come a time when the costs associated with these, and the transport, exceed the advantage of the cheap labour. This no doubt benefits the local economy to some extent, but as the standard of living rises as a result, as it generally has in China, the workforce will become less competitive.

Participation in any hobby depends on the availability of spare time, space, and disposable income. And a secure environment – whether the threat to that comes from economic pressures, international aggression, or a global pandemic. Many of us came to appreciate our pastime during lockdowns due to Covid-19. We should count ourselves very lucky.

CONTINENTAL MODELLER

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406 SIMPLE BUT SATISFYING – PLAN OF THE MONTH

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**430 GHUM**

A station on the Darjeeling Himalayan Railway in HOe: Ranjeev C.Dubey was diverted from his long allegiance to the Kalka – Shimla with an extra project – undertaken with equal effort, attention, and commitment.

438 BESCHIEDEN FELDBAHN

Ian Arkley has created an ingenious micro layout – a German industrial line using Busch HOe equipment on a Rokuhan 'Z Shorty' track base.

442 PRODUCT REVIEWS**450 BOOK REVIEWS****452 VIDEO REVIEWS****20a NEWS and FORTHCOMING EVENTS**

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A Comprehensive Model Railway System

PIKO

Offering great value for money, PIKO is definitely the range to look at for beginners to continental railways and existing collectors alike, with a variety of starter sets on offer.

NEW 2022



Expert DB BR194 Electric Locomotive - Epoch IV
PK51470 - DC - £315.00
PK51472 - DCC Sound Fitted - £415.00

NEW 2022



Expert DB E10 1270 Electric Locomotive - Epoch III
PK51812 - DC - £190.00
PK51813 - DCC Sound Fitted - £295.00

NEW 2022



Expert DBAG BR755 025 Electric Locomotive - Epoch V
PK51721 - DC - £195.00
PK51723 - DCC Sound Fitted - £305.00

NEW 2022



Expert DB BR181.2 Mosel Electric Locomotive - Epoch IV
PK51355 - DC - £205.00
PK51356 - DCC Sound Fitted - £315.00

NEW 2022



Expert OBB Rh1041 Electric Locomotive - Epoch III
PK51895 - DC - £205.00
PK51896 - DCC Sound Fitted - £325.00

NEW 2022



Expert CD Rh1216 Electric Locomotive - Epoch VI
PK59844 - DC - £190.00
PK59845 - DCC Sound Fitted - £310.00

HO

The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert. The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC-Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology. The HO Scale A-Track has clear, simple geometry and a realistic appearance.

NEW 2022



DRG E16 Electric Locomotive
PK40354 - Epoch IV - £215.00

NEW 2022



Lineas G1206 Diesel Locomotive
PK40482 - Epoch VI - £170.00

NEW 2022



DB BR191 Electric Locomotive
PK40540 - Epoch IV - £275.00

N

The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits. The locomotives run smoothly, with accurately printed liveries and DCC compatibility. This range includes many passenger multiple units that run around Europe. This allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.

NEW 2022



DB Glass Train EMU
PK37331 - Epoch IV - DCC Sound Fitted - £1,425.00

NEW 2022



DB Lufthansa Airport Express Diesel Locomotive
PK37443 - Epoch VI - DCC Sound Fitted - £825.00

NEW 2022



DBAG BR218 PIKO/Marklin Diesel Locomotive
PK37511 - Epoch IV - £475.00

G

The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors and is great value for money. The locomotives feature directional lighting, and some even have sound and smoke. Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.

NEW 2022



DR BR55 Steam Locomotive
PK47107 - Epoch III - £225.00

NEW 2022



Railpool BR151 Electric Locomotive
PK47207 - Epoch VI - £110.00

NEW 2022



DR V15 Diesel Locomotive
PK47308 - Epoch III - £135.00

TT

TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train. A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale. New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.

These products are distributed in the UK by Gaugemaster Controls Ltd. and are available from your local model shop, or, in case of difficulty, direct from ourselves.

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tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com www.gaugemaster.com E&OE.

N and HO Scale Continental Specialists

TRIX



The Trix range covers the majority of Europe in HO Scale, but especially Germany, spanning multiple eras and embracing DCC sound technology. A full range of locomotives, rolling stock, and track is available.

NEW 2022



HF Wiebe V320 001-1 Diesel Locomotive
M22434 - Epoch VI - DCC Sound Fitted - £480.00

NEW 2022



DB BR290 090-0 Diesel Locomotive
M25903 - Epoch IV - DCC Sound Fitted - £360.00

NEW 2022



SBB Ae3/6 I 10703 Electric Locomotive
M25360 - Epoch III - DCC Sound Fitted - £455.00

NEW 2022



SBB Ce6/8 II 14253 Crocodile Electric Locomotive
M25595 - Epoch VI - DCC Sound Fitted - £530.00



DB BR44 Steam Locomotive
M22989 - Epoch VI - DCC Sound Fitted - £490.00

NEW 2022



DB BR065 001 Steam Locomotive
M22664 - Epoch IV - DCC Sound Fitted - £460.00

Minitrix produce an extensive range of N Scale European locomotives and rolling stock, from the steam age to the modern era. A track system is also available.

MINITRIX

NEW 2022



DB BR103 237-4 Electric Locomotive
M16345 - Epoch IV - DCC Sound Fitted - £440.00

NEW 2022



DB BR212 372-7 Diesel Locomotive
M16126 - Epoch IV - DCC Sound Fitted - £310.00

NEW 2022



My Hobby DB V160 003 Lollo Diesel Locomotive
M16162 - Epoch III - £150.00

NEW 2022



KBayStsB D II Tank Locomotive
M16331 - Epoch I - DCC Fitted - £275.00

NEW 2022



Press BR218 054-3 Diesel Locomotive
M16824 - Epoch VI - DCC Sound Fitted - £285.00

NEW 2022



LLC (GtF) BR110 459-5 Electric Locomotive
M16267 - Epoch VI - DCC Sound Fitted - £275.00

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'Tradition and Passion' is the long-standing motto of Fleischmann, whose model train products are well known for their high performance, as well as their visual and mechanical excellence. All of their models are durable yet detailed.

Fleischmann is a full product range with locomotives, rolling stock, control equipment, and track available in one system, with optional innovative DCC technology and accessories such as point control and signalling.

There are Starter Sets available to get you going, whether you choose to run DCC or DC control. The PROFI track system is pre-ballasted which makes laying your track quicker and easier.



NEW 2022
KBayStsb Pt 2/3 Steam Locomotive - Epoch I - N Scale
FM707008 - DC - £235.00
FM707088 - DCC Fitted - £275.00



NEW 2022
DB BR065 001-0 Steam Locomotive - Epoch IV - N Scale
FM706504 - DC - £190.00
FM706574 - DCC Sound Fitted - £285.00



NEW 2022
FS Gr460 010 Steam Locomotive - Epoch III - N Scale
FM715504 - DC - £190.00
FM715584 - DCC Fitted - £235.00



NEW 2022
DB E10 1311 Electric Locomotive - Epoch III - N Scale
FM733809 - DC - £175.00
FM733879 - DCC Sound Fitted - £270.00



NEW 2022
DB ET91 01 Electric Railcar - Epoch III - N Scale
FM741103 - DC - £230.00



NEW 2022
DB BR103 174-9 Electric Locomotive - Epoch IV - N Scale
FM737812 - DC - £185.00
FM737882 - DCC Sound Fitted - £280.00



NEW 2022
DB F-Train Hans Sachs Coach Set - Epoch III - N Scale
FM881910 - £170.00



DB EC145/148 Frans Hals Coach Set - Epoch IV - N Scale
FM881915 - £180.00



NEW 2022
DB BR221 Diesel Freight Starter Set - EPOCH IV - N Scale
FM931902 - DCC Fitted - £329.95

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Big in Detail and Technology

Roco

Roco have always produced a large range of HO Scale German Railway locomotives and rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras! In recent years, their range has expanded dramatically and now includes locomotives from across Europe and around the world.

The HOe range is also growing in both size and popularity, with Narrow Gauge layouts being built on their own as well as part of existing HO layouts, as they are the same scale. A starter set is the ideal place to begin, with extra track also available to extend your Narrow Gauge line.

HO and HOe track systems are available with accessories to match, including the well known Rocoline range.



NEW 2022



OBB Rh86 Steam Locomotive - Epoch III - HO Scale
RC73030 - DC - £290.00
RC73031 - DCC Sound Fitted - £385.00

NEW 2022



USATC 2610 Steam Locomotive - Epoch II - HO Scale
RC72154 - DC - £350.00
RC72155 - DCC Sound Fitted - £445.00

NEW 2022



OBB Rh1293 200-2 Electric Locomotive - Epoch VI - HO Scale
RC71975 - DC - £235.00
RC71976 - DCC Sound Fitted - £330.00

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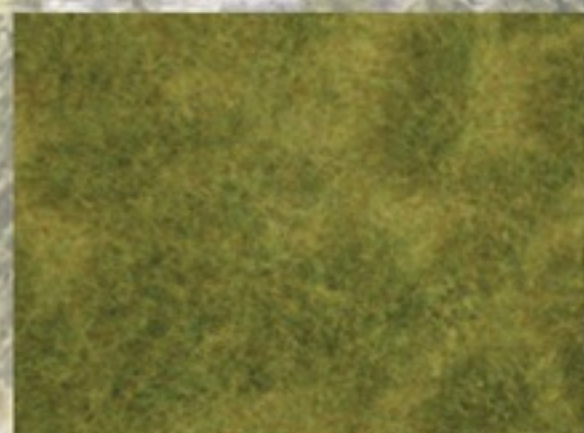
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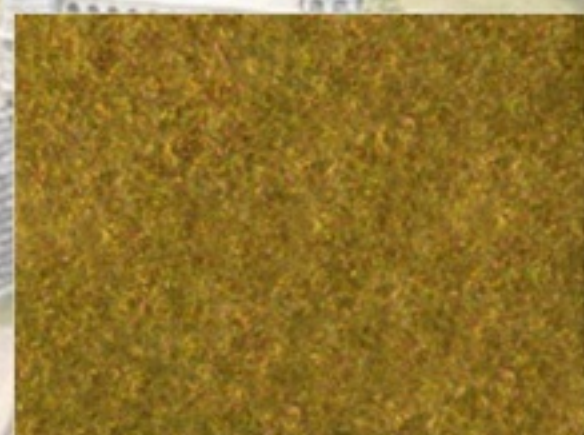
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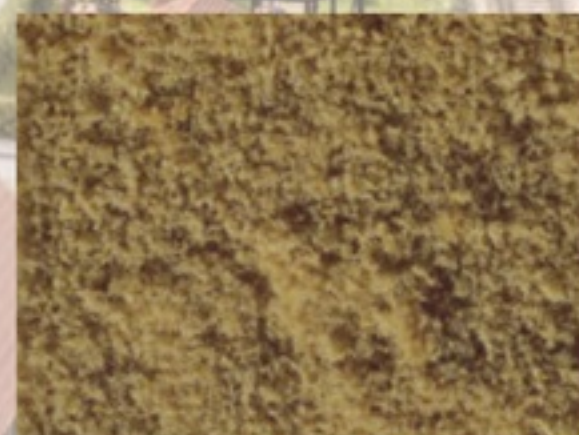
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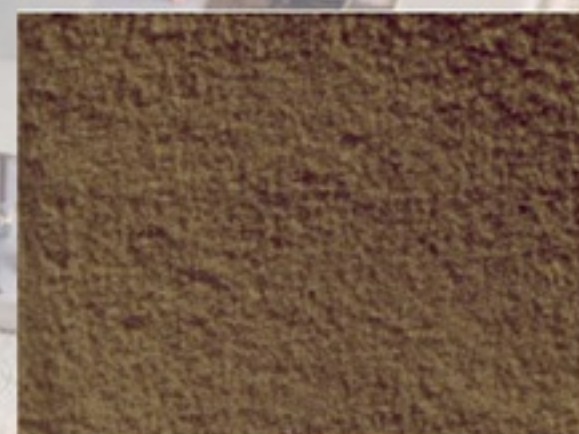
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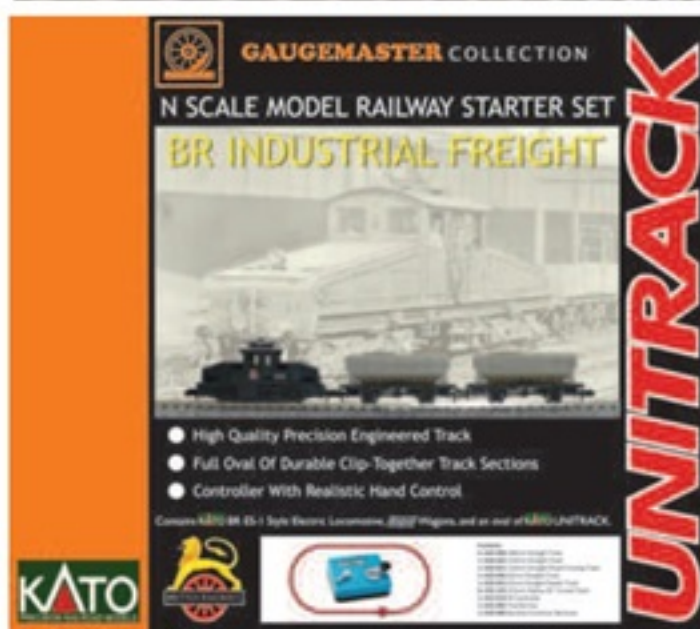
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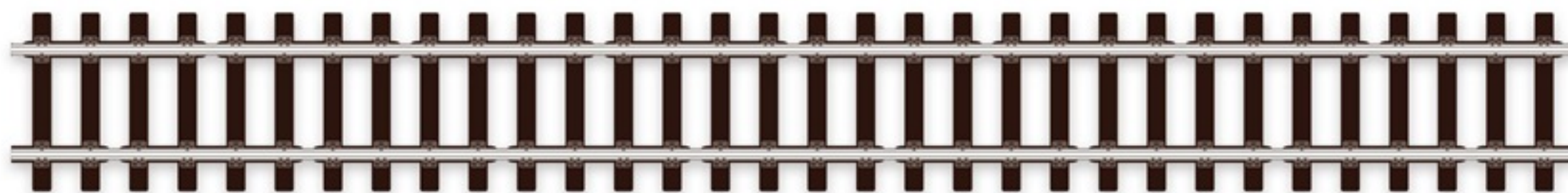




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Norman Raven reveals the latest layout by Raven Miniatures – another foray into Japanese N.
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Ishinaka

Before the world became bent out of shape by a virus (or, perhaps more exactly, by the reaction to a virus) an invitation to exhibit at The International N Gauge Show (TINGS) in September 2020 prompted thoughts of another Japanese layout. I say another because we have built a selection of such layouts, beginning with *Katami*, then *Katamitu* (CM December 2002) and *Katami Mittsu* (CM November 2009) followed by *Shinsu Mizu*, a Z scale effort (CM November 2013).



'We' as usual means wife Meg, daughter Melissa, son Simon, Phoebe the dog, Arion the horse, and myself, collectively known as Raven Miniatures.

Of course, TINGS 2020 was cancelled even before I got started with a layout. Fortunately, TINGS 2021 was able to go ahead and the layout made its debut there.

While my thoughts had initially been on a continuous run, I decided to scale back my ambitions to make use of some existing baseboards, track, and odds and ends (I am half Scottish and half Yorkshire!).

What has resulted is a fairly standard terminus to storage yard effort. The layout therefore uses a plethora of existing stock, a veritable cornucopia of Kato Unitrack, and a collection of Grainge & Hodder laser-cut baseboards held in hand for any speculative layout which came to mind. As I always say, my mind is a cosmic whirl of creative invention, although others sometimes describe it differently!



Above

A DD54 diesel (a Tomix model, one of my first Japanese N acquisitions) crosses the river bridge with a short train of Seibu Railway coaches (also by Tomix).

Photographs by the editor.

Background

The layout is entirely fictitious but is generally supposed to represent a town where the discovery of healing spa waters has generated a thriving tourist trade and consequently a busy train schedule with an eclectic mix of stock.

Truth be told, we have collected a fair amount of Japanese stock over the years, much of it on the basis that Meg liked the colour! Simon also plays his part by pointing me in the direction of many possible purchases, a few of which I manage to resist!

The busy train schedule is simple: it is an exhibition layout and we and the customers like to see trains running. Accordingly, trains run intensively, side by side with other stock that normally would not see the light of day together.

Baseboards and lighting

The baseboards are our now accepted standard canopy style boards where you look into the modelled scene rather than down on it, almost like a theatrical setting. The backscene and canopy top are integral parts of the board, making storage and transport easier. It also means that I do not suffer the embarrassment of forgetting things like the lighting gantry bolts, for example!

Lighting of the scenic sections is by means of six 12v LED strips on each board. They are stuck to the underside of the canopy roof and provide a relatively even lighting of the modelled scene.

Adjoining boards are aligned with 25mm pattern maker's dowels and held together with M6 bolts.

Everything is prevented from falling to the floor by homemade legs which slot into pockets in the baseboards; a grandiose name for two wooden battens which hold the legs tight against the baseboard cross members. The wood for the legs was salvaged from a couple of previous layouts which were broken up. I do not have any luck with selling layouts which are life expired and always envy those who put



a 'for sale' sign on a layout and have it gone by 10.30 on the Saturday morning of a show. One advantage, however, is the ability to recycle a fair amount of stuff for new layouts. For example, some of the green scenery on the layout has seen service in Japan, Germany, and the USA!

Trackwork

For those who have no experience of Kato Unitrack, it is a sectional system of ready ballasted pieces which makes tracklaying relatively quick and easy. We have used it on all our N gauge Japanese layouts. The geometry is admittedly fairly limited; the similar Tomix Finetrack system is more extensive.

Below

The modern residential and retail area by the station, where an ET 122-8 railcar (Kato) and a 500 Type EVA (Tomix) are waiting.





Below
The park and retail area near the station.
A 500 Type EVA (T) waits to depart.

Below
A 6000 MSE four car unit
(Micro-Ace) pulls in
while the 500 Type EVA
prepares to depart.



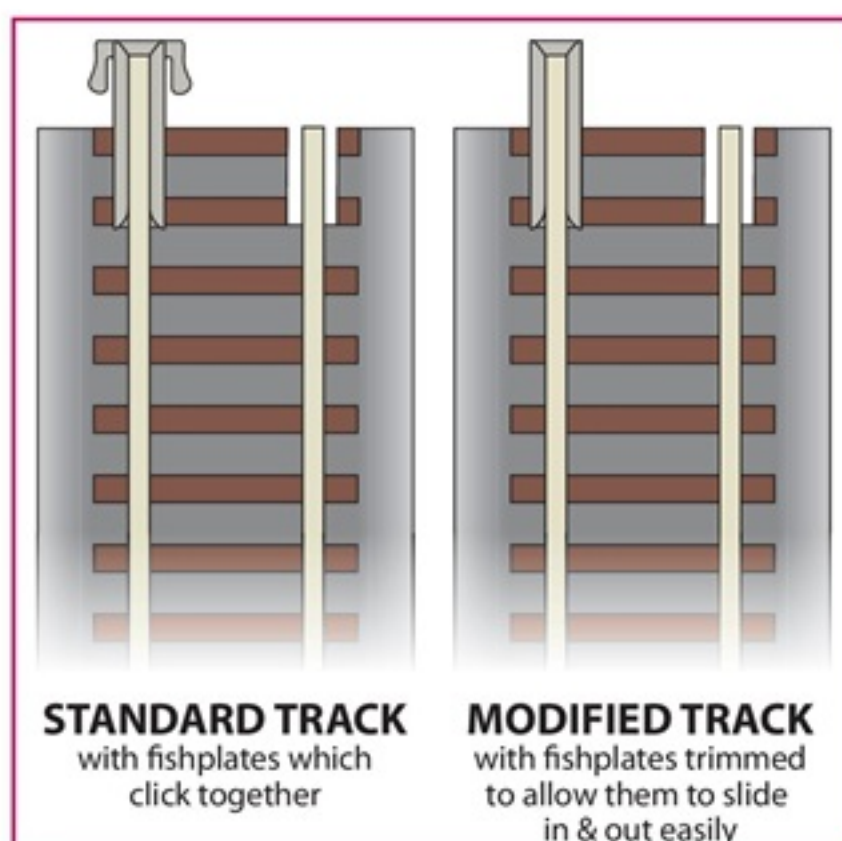


One useful advantage is that the points come ready fitted with the motor enclosed within the ballast and operated with an accompanying switch, albeit in a dreadful bright blue colour which we have painted a more sophisticated black for the layout.

The relatively high ballast shoulder in shiny plastic can look a little toy like and this is remedied somewhat by a judicious (fairly!) application of brownish/greyish paint and by bringing the surrounding ballast and scenic treatments up to the side of the track shoulder.

Another useful element of the track system is the extender track, a piece which slides to adjust its length. This is inserted between the track ends on adjoining baseboards to provide a physical and electrical connection without trailing wires, plugs, etc. A slight modification is needed. The Kato track sections click together by means of small locking pieces on the rail joiners. These need to be removed on both the extender and receiving tracks to allow the extender to slide on and off the fixed track (see diagram).

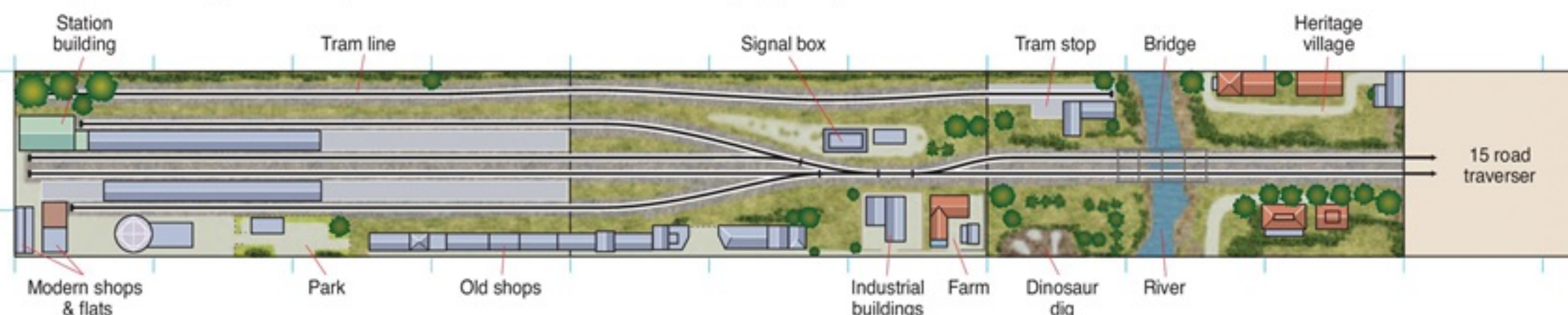
The track layout is fairly straightforward with three points providing four platform roads and a further point splitting the main and branch lines. A little bit of interest is added by the inclusion of a slightly elevated tram track along the rear of the layout. This goes from behind the station to a small tram stop where passengers disembark to visit a nearby shrine (offscene) and a samurai heritage village. This line is formed with Tomix Finetrack which also uses an extender track system to bridge the board joins.



Above
Type 258-0 *Sunrise Express* sleeper train (by Kato) passes the signal tower on its way into the station.

The points on the railway line are #4 with a radius of 481mm. By moving a couple of small screws in the bottom of the point, the frog can be made live or dead and can be configured as 'non power routing' or 'power routing', meaning that power can be passed the way the point is set or down both arms permanently, for DCC for example.

The main and branch lines head off out of the scenic section onto a fifteen road traverser. The roads on the traverser are also aligned and powered by the extender tracks which again obviates the need for bolts, switches, etc. Two of the traverser roads are sectioned to hold multiple smaller trains. This is achieved simply by means of Peco switches connected to the track by copper tape.



Ishinaka Scenic section - 3,000mm x 400mm. Traverser board - 1,200mm. Each grid square - 300mm x 300mm.



Right
E 655 Series *Nagomi* (Kato)
draws into the station
as a Kiha 58 railcar (Kato)
waits at the bay platform.



Structures

The structures on the layout are a mix of new and old. The station is made up of Kato platforms and a small station building. The urban buildings by the station are Tomytec kits. Along the front of the scene is a long row of traditional style Japanese shops, also from Tomytec. All these structures are weathered a little to kill the inevitable plastic shine.

The girder bridge over the river is from Kato and was salvaged from a previous Japanese layout. On the other side of the river is the aforementioned samurai village, a local tourist attraction. This is a collection of resin cast buildings which first appeared on our original Japanese layout, *Katami*, many years ago. I found them in a box while looking for something else – serendipity in action.

Scenery

The green scenery is mainly from Woodland Scenics and mostly salvaged from previous layouts. Ground cover, ballast, etc., is all stuck down with the Woodland Scenics scenic cement, except near the station where I had to resort to the good old PVA glue, water, and a drop of washing-up liquid. This was forced on me by the simple problem of running out of scenic cement! Bushes etc. are stuck down with hot glue. Trees are made from sea moss strengthened with a skewer, spray painted brown/grey, and covered with green scatters applied with cheap, sticky hairspray. Pine trees are skewers with rubberised horsehair, again covered with green scatters. Taller trees are important to the general appearance since they give height to the overall scenic picture.

Above

The colourful 500 Type EVA accelerates past the signal tower.

Below

The row of traditional shops near the station – a contrast to the modern retail area.

Right

The timber yard and the adjacent farm.





Above
More traditional shops with the signal tower behind. The hand-painted backscene is shown to good effect.

Below
A tram (Kato) has just arrived at the tram stop near the heritage village.

The other main element of the scenery is the river. There are many clever water effects now available but on this occasion I resorted to another old technique, forming the water surface and waves from PVA glue and toilet paper. This is then painted with black, blue, and white gloss paint. The rocky channel in which the river flows is made from cork bark painted with suitable 'rocky' colours and this is also used for the rocks at this end of the layout. The waterfalls at the rear are the old standby of cotton wool teased down the rock faces.

The backscene is, as usual, hand painted by myself. At the station end of the layout some cut outs from cityscape photographs represent the town of Ishinaka.

One unusual feature on the layout is found near the river. An archaeological team from the local university has unearthed a large dinosaur skull and leg bone. In reality these were bones unearthed by me in the garden! However, it does make for something a little different.

Operation

Operation is simple. Stock runs on and off the scenic section into the station, waits a while, and then returns to the storage area. The trains are predominantly electric and diesel multiple units which, conveniently, run in both directions. There is no run-round facility at the station so loco-hauled trains would be a problem. However, a couple of trains are powered by a single loco with driving trailer at the other end. Another train has a loco at each end; I remember a similar configuration running between Edinburgh and Glasgow. All gloriously unprototypical but Rule 1 applies.





Above
A twin unit diesel railcar (a Tomix model)
passing between the tram stop and the dinosaur dig.

Trains from the branch run onto the scenic section and can be held at the semaphore signal before proceeding towards the station. The signal is a fully operational Tomix effort which I have had kicking around for a long time. It may be joined by other signals in due course.

The trains themselves are a mixture from Kato, Tomix, and Micro Ace. A couple of the trams are from Modemo. The stock includes a three car diesel set and a DD54 locomotive which were my first Japanese stock purchases more years ago than I care to remember. As a testament to the running qualities in general, these items still run beautifully.

Right
A DD51 diesel loco (Tomix)
rumbles onto the bridge
hauling tourist coaches
(also Tomix).

Below
The ET 122-8 diesel railcar
in Three Cities Flowers livery
(Kato) crosses the bridge
and heads up the branch.



CONTINENTAL MODELLER

For the younger spectators (and, indeed, some of the not so young!) *Thomas* and *Percy* top and tail a short train consisting of *Annie*, *Clarabel*, and the red Sodor van.

Control

Although the layout is plain analogue DC, it is controlled wirelessly. This is achieved by the Blue Railways system. The track is wired to a receiver at the back of the layout, powered by a 12v adaptor power plug. This receiver communicates wirelessly with a handheld controller enabling us to walk around the layout and engage in our preferred front operation.

Conclusion

That then is a brief account of our latest layout. If we ever get to exhibitions again, please come and have a chat. We are happy to travel anywhere people cry out for justice and entertaining layouts ... if any exhibition managers are interested, we can be contacted at fluff48@zoho.com

Above

Part of the heritage village which helps to conceal the exit to the traverser.

Forthcoming exhibition

Ishinaka will be at the Perth model railway show on Saturday 25th and Sunday 26th June.
www.smet.org.uk



Emmanuel Nouaillier

shows more of his techniques for highly detailed structure modelling.
Photographs by the author.

Weathered render

On two typical structures

I used polyurethane foam to make the core of these two structures. The first was commissioned by a cognac producer, and I will just summarise the important stages here. The second was for myself, and in this article I will mainly describe the weathering process.



The distillery façade that served as inspiration has characteristic elements, and typical weathering. It was not very complicated to reproduce, except for the rather original typography. I got round this by using another style of lettering which was readily available.

Step 2

To create the lettering on the façade, I used alphabets of block letters from Slater's and Yen Models.

I made a trial layout on graph paper first in order to define the adequate spacing between each letter, and then I put them in place using slow-setting cyanoacrylate glue.

2a



1



Step 1

The structure was made from foam board.

For the cut stone, the foam was simply scribed and then flecked with a small brush.

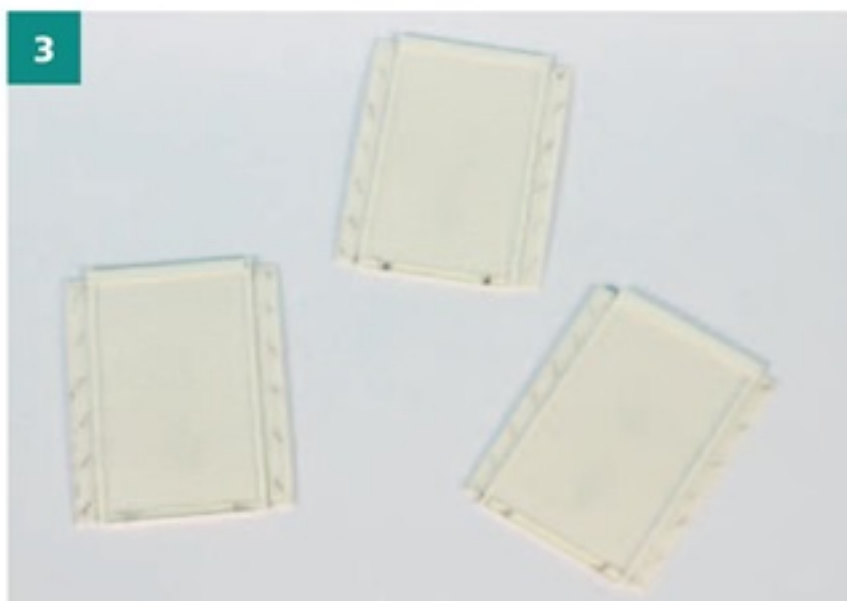
For the render, I applied a layer of filler, dabbed with a stencil brush after smoothing. Once dry, light sanding accurately imitated the surface in a state similar to that observed in reality.

2b



Step 3

The old wooden slatted blinds were prepared using Evergreen grooved sheet, rubbed with a wire brush to imitate the grain of the wood. A few scraps of flat strips and 0.12mm plasticard completed the three almost identical assemblies.



Step 4

Other features were initially placed without being fixed to check the arrangement and fitting.

Step 5

The first colouring used enamel paints, ranging from very light grey to beige, mainly by dabbing with a sponge over the entire rendered area. The dressed stones were painted more uniformly with a large flat brush. The aim is to obtain a varied background.

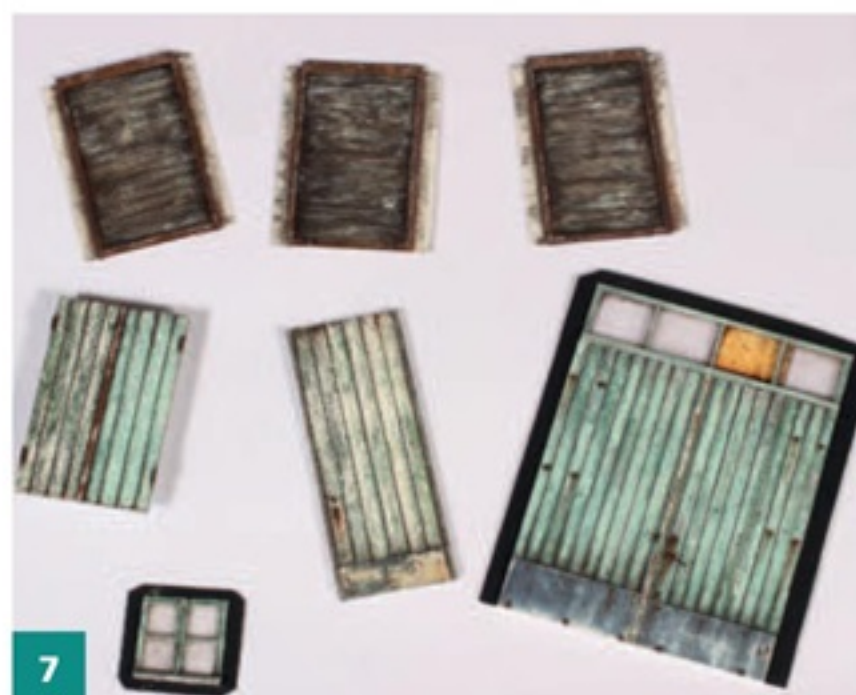


Step 6

Once dry and after the application of chipping medium, the façade was dabbed again with other acrylic shades then chipped using an old fine makeup brush and a wooden stick.

Then as usual I toned it down with different washes, and applied AK enamels to reproduce all kinds of oozing and dripping.

Step 7



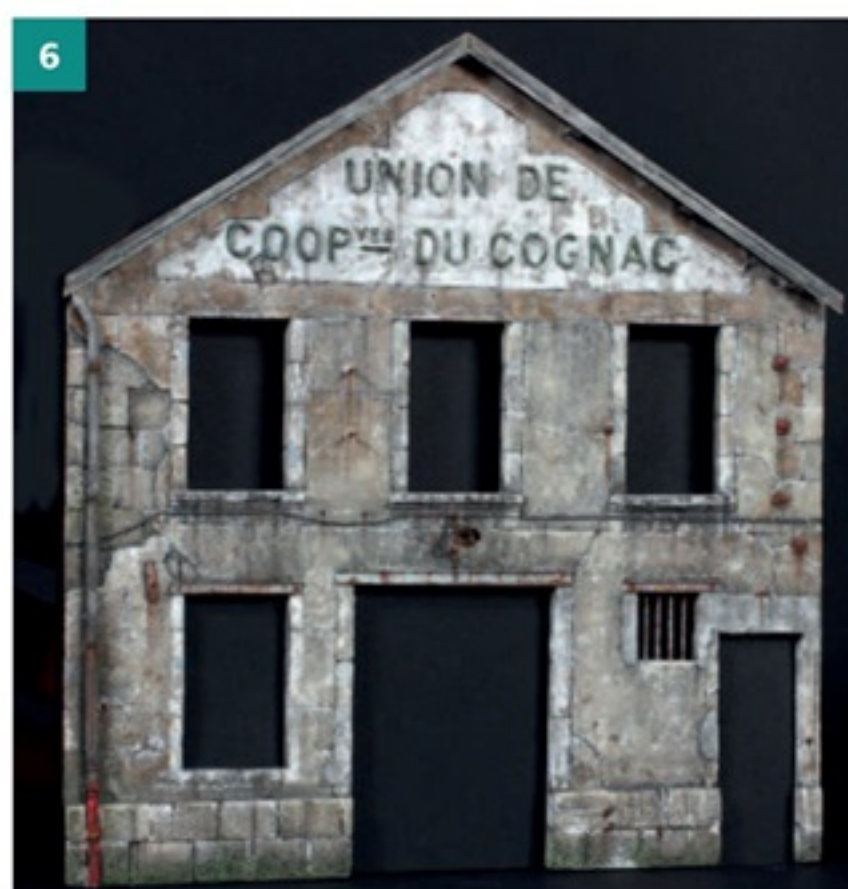
The other doors and windows were made meanwhile using plasticard, and joined the wooden shutters for a fairly extensive weathering session.

On some of these components I was able to use the 'weathering pencils' from AK Interactive, among other things, to reproduce certain *trompe l'oeil* effects.

4



6



Step 8

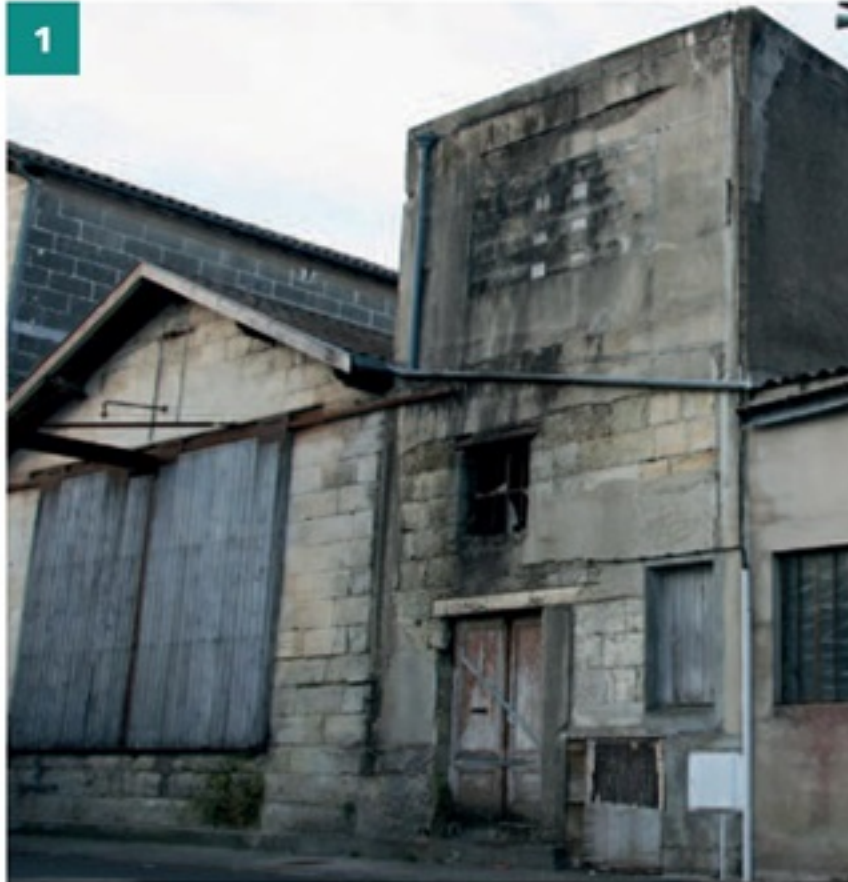
The framed finished model will be exhibited not far from the place that served as inspiration.

8



Step 1

Another example, part of a distillery seen from the street side. The central section immediately inspired me. It is near a railway station, and the Calvet establishments were formerly served by rail. The style of the central building, with its rough and blackened render, is perfect for an exercise in detailing and weathering to create something different, adding a few elements to personalise it.



Step 2

A little further on, I noticed this small access platform to another building, which I wanted to add to my project.



Step 3

As for the previous example, I decided to add cast letters to the façade, this time in the Art Deco style, dating from the 1930s. This was derived from the lettering seen on a very old agricultural silo.



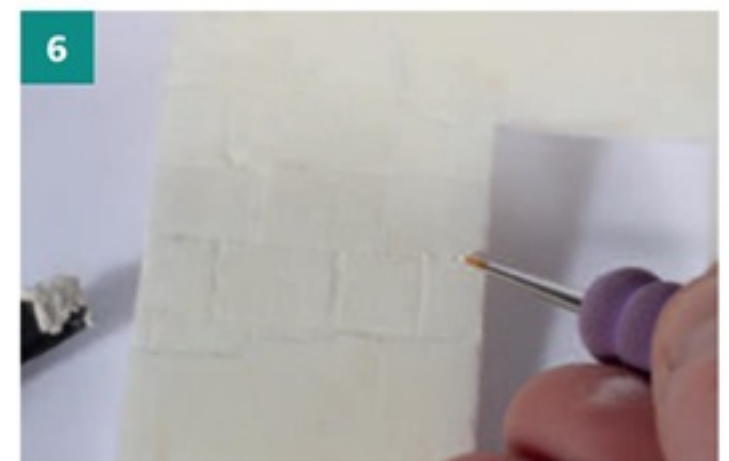
Step 4



The façade I designed is smaller than the real one. It was cut out of 5mm foamboard and the card covering was removed on the visible side.

I sketched in pencil suggested places where the covering render is missing and reveals the cut stone beneath, which was scribed with a metal point.

Steps 5 & 6



This time I went back to the classic method of more or less accentuated stippling with the wire brush, emphasising more strongly to reproduce the texture of eroded cut stone, the foam being left as it is to imitate the render. To go a little further in realism, some traces of mortar between the stone joints were simulated using filler.

Step 7

After arranging some Yen Models 'Art Deco' letters, I continued with the intricate detail of the structure, in particular by reproducing sealing strips glued to the tar paper roof (imitated with sections of recycled aluminum foil), a gutter downspout (Evergreen plastic rod), and also masonry anchors, tie rods, and other small metal details specific to this style of construction.



Step 8

Once the façade was ready, I moved on to the small access platform. This was reproduced by stacking peeled sections of 5mm and 10 mm foamboard and adding detail with angles, irons, etc. Based on observations made from reality, I made a quick plan before construction as it had to fit perfectly into the opening in the façade.



Step 9

Many details have been put in place and other features added, such as the electric cable (twisted copper wire), railings, exposed concrete irons, etc.

The aim is not an excessive accumulation of detail, just the right one, in the right place!



Steps 10 & 11

I used the same colouring method as for the first façade, with enamel paints, but this time added a dark grey tint to the panel to highlight the darker parts from the outset.

Once dry, the surface was brushed with a matt black acrylic wash, and I accentuated this effect even more in places with a flat brush. The structure was then set aside for 48 hours to dry.



Step 12



I then rubbed the entire surface vertically using 400-grit abrasive to partially remove the pigments left by the wash on the surface and reveal certain runs more strongly. The texture of the rendering is already particularly obvious at this stage and would satisfy many modellers.

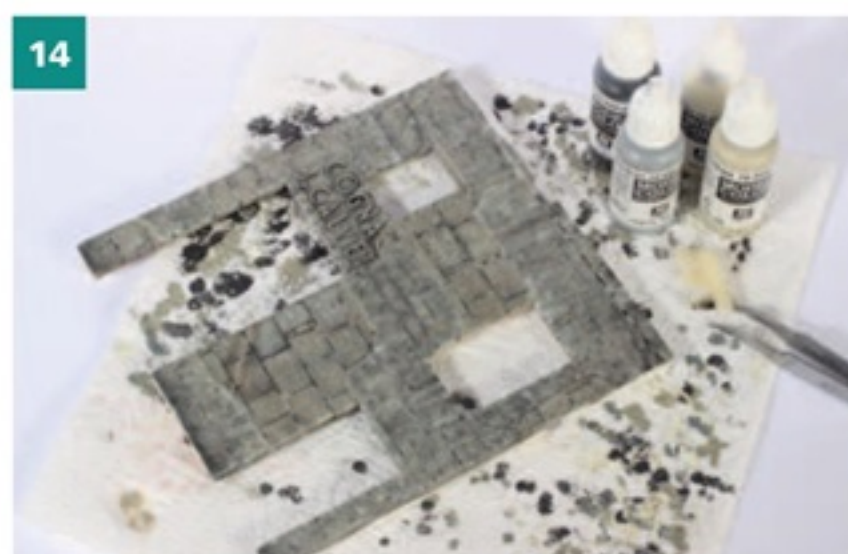
Step 13



But I wanted to go further, highlighting the joints, cracks, and hollows with a wash of pure Vallejo matt black acrylic, applied with a fine brush. The overflows were immediately erased with a brush moistened with clean water.

I continued by dabbing with a sponge to create a multitude of variations as typical of this material when altered by time. I used no fewer than four shades: dark grey, light grey, stone grey, and ivory.

Step 14



Steps 15 & 16

The rest of the ivory paint was then mixed with matt white and used to create some more subtle cracks and to underline certain cracks or edges. For the latter, I use the old dry-brush technique, with a flat brush previously wrung out on a sheet of paper towel.



Step 18

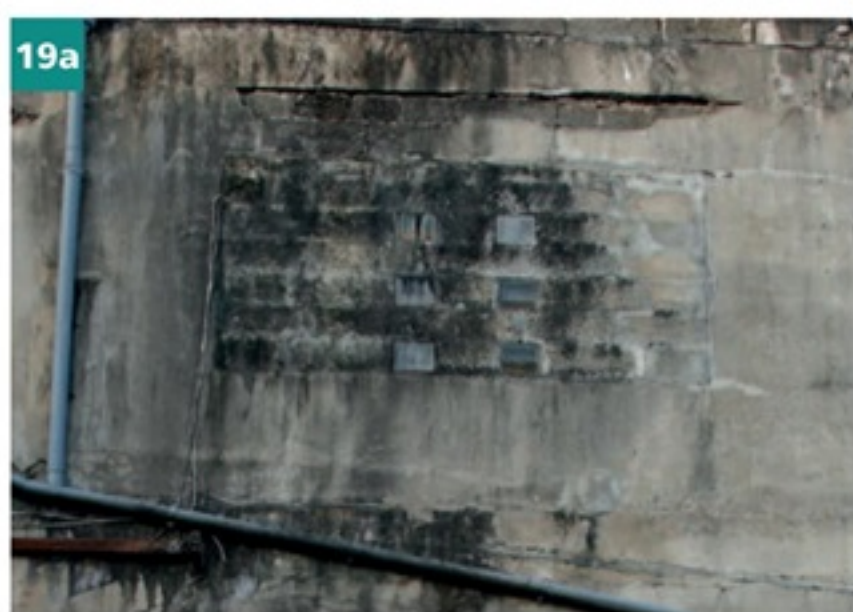
The rest of the work on the structure itself came down to the colouring of the stones, by simple dry brushing of stone grey and ivory, then, once dry, applying a dark grey wash, attenuated to a greater or lesser extent with a clean cloth dampened with clean water.



Step 19

Using the same dark grey wash along with matt black, always referring to real examples, I continued by refining the effects of black drips and oozing visible over the entire structure.

The remaining stone grey and ivory paint was used at this stage to enhance the grouting and mortar repairs.



Step 17

I continued by imitating the whitish speckle often seen on this style of old surface. The technique of splattering with an old paint-soaked brush worked well for this. I worked with a mixture of light grey and ivory, systematically restricting the area to be affected with a paper mask for perfect control.



Step 20

I then moved on to the imitation of tar around the roughly glued sealing strip. To reproduce this texture, I had the idea of making a mixture of water-based plaster plus matt black paint, applied with a fine brush. The result is quite stunning in this scale. Once applied, the aluminium look was imitated using Pébéo 'silver' acrylic paint.



Step 21

21



Step 23

The other door was based on one photographed on another building of the same style. It is not very complicated and was made from plasticard (*above*). Some work with a scribe imitated where the metal had been eaten away in places.

After multiple stages of weathering, the door was complete, with various signs. The whole thing took a while, trying different layouts, but I had a lot of fun with it.



23

All that remains is to take care of the cast letters, on which I applied the chipping technique, with different shades of green, finally completed with a little targeted work with a sponge.

22. The access door was made from plasticard and Evergreen strips, with some added details compared to the real thing. The handle was imitated with the head of a small nail, painted glossy white to give the illusion of porcelain, while the enamel 'office' sign was created on the computer and printed out on glossy paper.

Step 22

22



Step 24

With doors and windows in place, all that remains is to detail the surroundings with a multitude of carefully worked details: dirty rags, jugs, boards, broken boxes (from Cognac Calvet, of course!), old bottles, an old billboard, and other things.

24



Paul de Groot shows how he was inspired by a magazine article to construct a Dutch HO layout to encourage starters – and re-starters. *Photographs by the author.*

Simple but satisfying

Built to a budget – almost

During the *OnTraXS!* exhibition in Utrecht back in 2015 I came across the LR Presse stand. I already knew some of the magazines from this publisher, such as *Loco Revue* and *Voie Libre*, but I had not seen their small B5 format *Clés pour le train miniature*, which is very easy to read (even in French!) and is intended for beginners or re-starters. It is published six times a year and each issue is 40 pages. It explains almost everything you need to know about making a model railway.

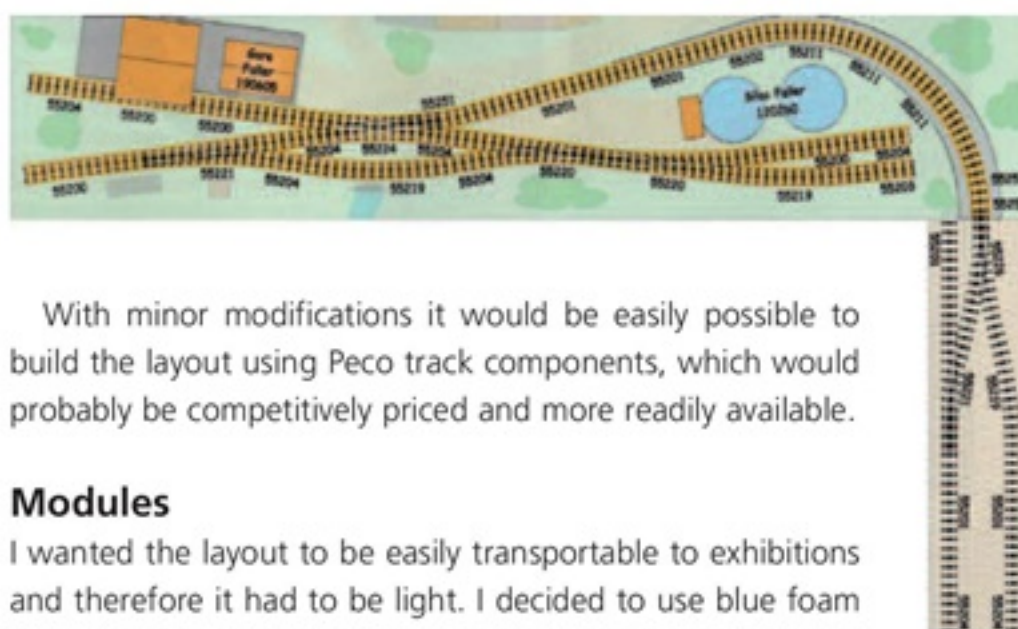
In the centre there is usually a light card sheet on which is printed a building plan in HO or N which you can assemble.

In one issue I found a very nice track plan for a small shunting yard. Of course, all the buildings were French but I thought I could make it with Dutch structures and trains to demonstrate to local modellers what could be done.

I also wanted to build the layout as economically as possible and decided that the budget should not exceed €500.00.

Track plan

The plan was designed using Piko A track, and the article gave an exact list of the pieces needed, with prices, which then added up to €182.80. As it happened, at the time I was giving workshops in landscaping at the Bentink Modelspoor shop in Apeldoorn and the owner kindly donated the required track.



With minor modifications it would be easily possible to build the layout using Peco track components, which would probably be competitively priced and more readily available.

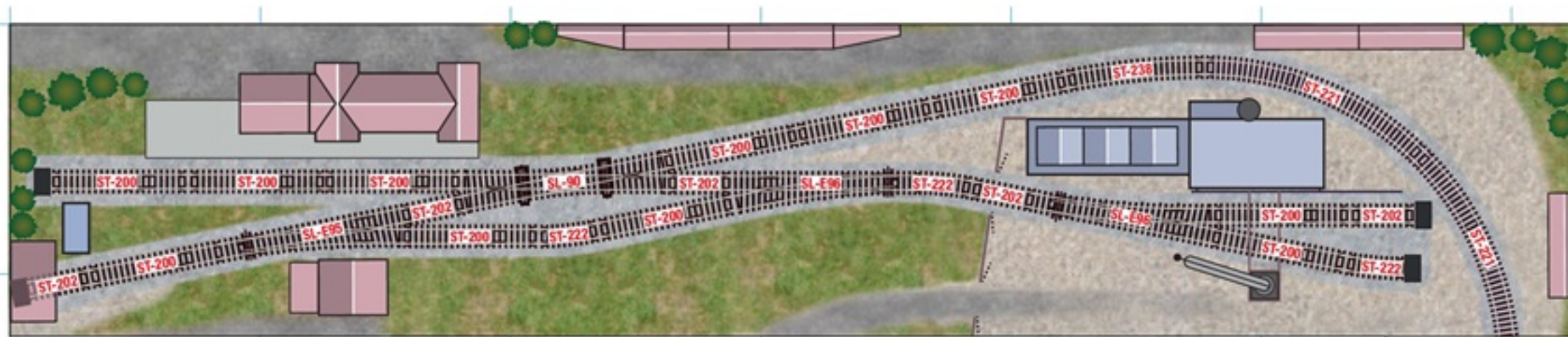
Modules

I wanted the layout to be easily transportable to exhibitions and therefore it had to be light. I decided to use blue foam sheets and three-ply poplar plywood. In many DIY shops the blue foam is sold in packages, but you can also get it in larger sheets. For the three modules I needed two pieces each 100cm x 40cm and one piece 80cm x 20cm. Normally a sheet is 120cm x 60cm, so I needed only two, in total €11.50.

The price for poplar plywood depends on the supplier, so I first decided what I needed. It mostly comes in 250cm x 122cm sheets, so with two I could make the whole base for the modules including the backdrop. Two sheets cost €58.60 and I paid €10.00 for having them cut to the correct sizes. At home I glued and screwed the plywood pieces together and placed the blue foam within the frames.

Above
The original track plan
for a French scene
using Piko A track.





Scenic section 6' 3" x 1' 6". Fiddle yard 9" x 2' 6". Each grid square = 1' x 1'.

Above
The track plan amended
to use Peco components.

Peco track components

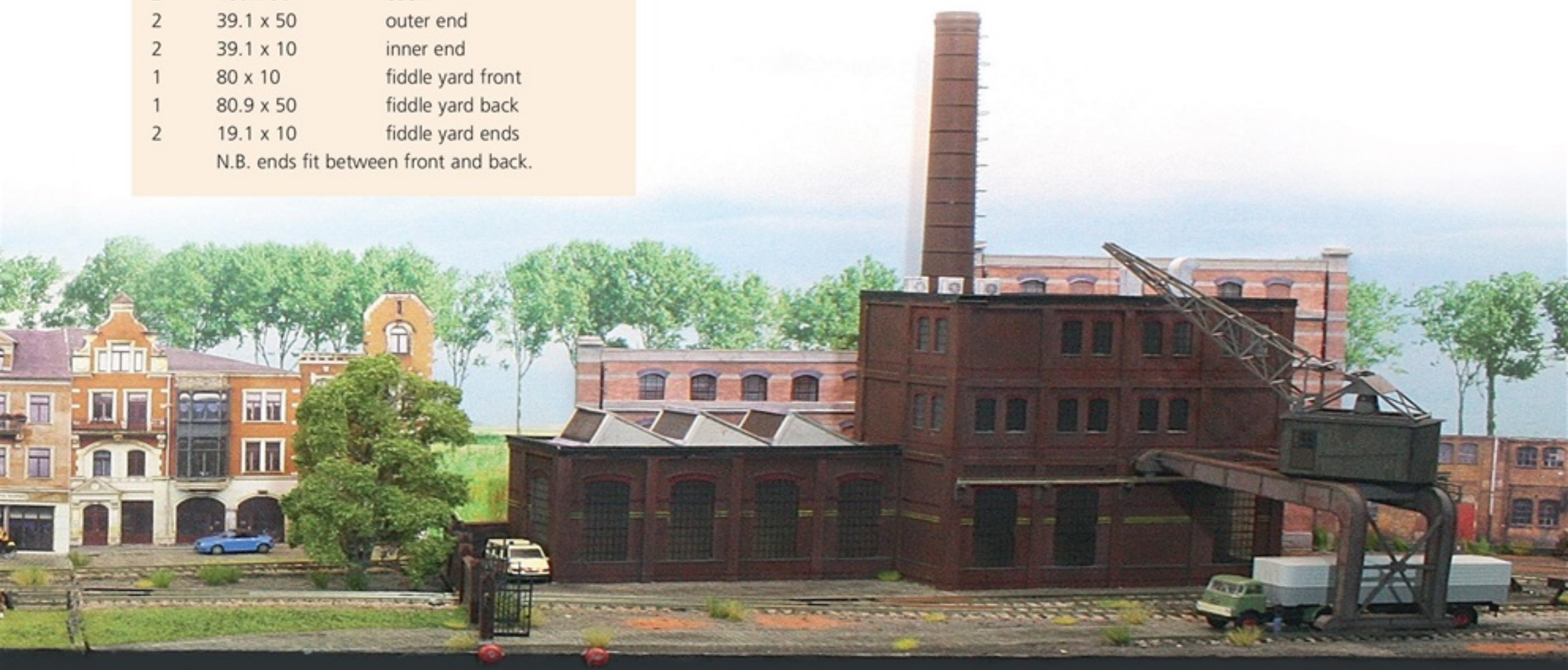
Qty.	ref.	Description	Price each	Total
Setrack				
17	ST-200	Standard Straight	£1.80	£30.60
5	ST-202	Short Straight	£1.55	£7.75
2	ST-221	No.1 Radius Double Curve	£3.10	£6.20
3	ST-222	No.1 Radius Half Curve	£1.80	£5.40
1	ST-238	Special Curve	£2.50	£2.50
7	ST-270	Buffer Stop, sleeper-built	£2.10	£14.70
Streamline				
3	SL-95	Medium Radius Right Hand Point (Insulfrog)	£14.75	£44.25
or	SL-E95	Medium Radius Right Hand Point (Electrofrog)	£14.85	£44.55
2	SL-96	Medium Radius Left Hand Point (Insulfrog)	£14.75	£29.50
or	SL-E96	Medium Radius Left Hand Point (Electrofrog)	£14.85	£29.70
1	SL-90	Double Slip (Insulfrog)	£46.15	£46.15
Total				£187.55

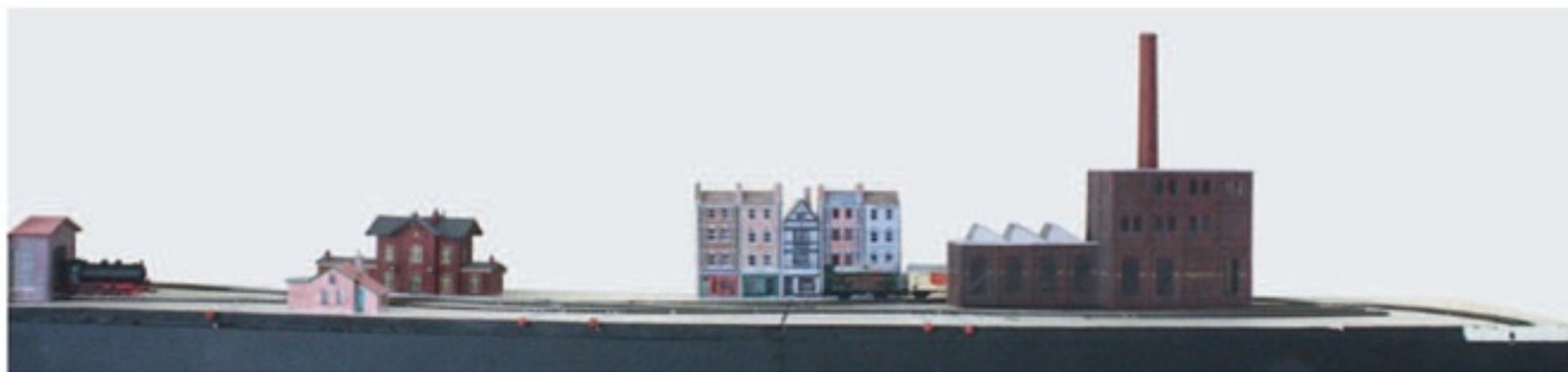


Baseboard parts – 9mm plywood

Qty.	Size (cm)	Description
2	100 x 10	front
2	100 x 50	back
2	39.1 x 50	outer end
2	39.1 x 10	inner end
1	80 x 10	fiddle yard front
1	80.9 x 50	fiddle yard back
2	19.1 x 10	fiddle yard ends
N.B. ends fit between front and back.		

Below
The eventual result,
converted to a Dutch scene
with alternative structures.





Left
The back has been fitted and the structures are positioned to check the arrangement.



Left
The Dutch backscene image has been printed out and applied to the backdrop, with curved corners.

Background

Before laying the track, I had to fix the backdrop. Before doing that I decided that the backdrop should be curved in the back corners. To do this I made a few right angle triangle pieces of 5mm foam board with the long side as the required radius curve and glued them horizontally in the corners. Then I cut out two pieces of 3mm foam board 40cm high (from the blue foam to the top of the background) and with the length matching the curve. I incised a groove every 5mm along it so the board could be bent and glued into the corners. A 100cm x 70cm sheet of 5mm foam board cost €9.95 and a sheet of 3mm cost €6.95. The 5mm sheet can also be used to make the valance for the layout.

On my computer I already had a file with a typical Dutch background from www.modellbahn-hintergrund.de. A short trip to the print shop with this file and I had a 350cm

x 40cm photo backdrop for €75.00. You might think this was expensive and it might have been sufficient to paint a backscene, perhaps a plain sky or a simple landscape. But the photo backscene helps to establish the character of the layout, and could not have been added later.

Now it was time to glue the background to the layout. This was done with thick wallpaper glue. Watch out that you do this very carefully, because you do not want to have bubbles in the background. To begin, the first module was laid down on the left side. The background was glued on. Then the module was laid on its back and the background was glued onto the curved corner and the back. Only then was the background sheet cut at the end of the module. The next step was to glue the background to the back of the second module in the same way, but starting from the join to sure a neat match.

Below
All the track pinned in place but not yet ballasted. The small red beads on the ends of the point operating rods are clear against the black fascia but not too obtrusive.





Track laying

The modules were held together with clamps so I could position the track. All the pieces were laid down in their places. I decided that the points would be worked manually. For this I used a small brass tube with an inner diameter of 1mm. Inside I placed 0.8mm brass wire that was bent up at 90° to engage with the hole in the tie-bar. The other end of the brass wire was brought to the edge of the layout, and I glued on an 8mm red bead as a handle. This cost €2.50 for fifty red beads (only seven needed!) and €8.00 for the brass tube and wire.

After the points were in place, I glued the track to the blue foam with wood glue. Watch out that you do not glue the point blades! After the glue had set I cut the rails at the edge of the modules with a Dremel handheld drill fitted with a cutting disc. Be sure to wear eye protection during this operation.

Now it was time to bring in the ballast. For this I used Busch ref.7127 at €1.95. First I applied the gravel by hand. After that I spread it with a small paintbrush. The gravel was glued down with Noch ballast glue (ref.61134, €5.79).



Above left

The loco shed on the left, only partly modelled. The doors are by Auhagen. The Sik shunter is by Roco, the coal wagon by Artitec.

Above

The station building is a kit based on Gasselternijveen.

Buildings

Since the original layout plan was French, I had to search for alternative buildings as economically as possible. I planned a low relief loco shed. I also needed a station and some low relief houses in the background on the left. The right side would be industries, also with low relief buildings behind.

The Belgian firm Train Service Dankeart donated the station building kit, a model of the Dutch station at Gasselternijveen designed by the famous architect E.Cuyper who was also responsible for Amsterdam Central station. Since this was donated it cost nothing, but I would have had to pay about €49.95 for a similar laser-cut kit from MKB (www.mkb-modelle.de).

The low relief loco shed on the right was made from scratch. A PDF from Scale Scenes (www.scalescenes.com) with a weathered brick wall pattern cost just €2.19 – you download the file and print it out on your own printer on a sheet of 120gram paper for the best result. I also re-scaled the file, because Scale Scenes produce it in 1:76 for OO. The way to do this is to open the file in Adobe Acrobat. In the print dialogue box make it 87% instead of 100%, and in 'characteristics' select 'best quality'.

For the doors and the windows of the loco shed I used the Auhagen planning book for their BKS modular building component system. This is free for use and you can find it on the internet (www.auhagen.de). I took the measurements of articles 80505 (brick walls with large green doors) and 80509 (brick walls with industrial windows). I also used Auhagen refs. 2342N (brick walls with industrial windows) and 2326A (large doors); total cost €15.20.

The base of the loco shed was made of 2mm thick cardboard. A 100cm x 70cm sheet cost €3.95. This was also used for the other low relief buildings.

For the background behind the station I bought low relief houses from Auhagen (refs. 42501, 42502, and 42506) for €53.70. A few of the buildings I scanned in and printed out at 120%.





Above
Right opposite the station is a small farmhouse, scratchbuilt in card with overlays downloaded from Scale Scenes.

Below
The left side of the layout. Even though there are only three wagons, making up a train is a bit of a challenge as none of the sidings is long enough for all three.

The little farmhouse at the front of the layout was also scratchbuilt from cardboard with overlays from Scale Scenes. The windows of the farmhouse come from a box filled with all the parts left over from other building projects.

All the buildings were glued onto 1mm card and then assembled. This gives a little bit of three-dimensional effect to the background.

Now I had the left side ready and could start with the buildings for the right side. Here industry is the main part. As I already had the low relief industry building from Auhagen, I needed another low relief building. This was ordered as a PDF file from Scale Scenes (ref.T029, €4.39). This was also printed at best quality at 87% of the original. The parts were glued onto 1mm and 2mm cardboard and cut out. After that

all the parts were brought together and the two low relief factories were placed on the background.

The main factory was built from Auhagen BKS components with a cardboard back and roof. The big chimney at the back of the factory is also from Auhagen.

Now I had all the buildings and the track was ready, so I could start on the rest of the landscape.

Scenics

Along the track I first made some light gravel from shell sand mixed with chalk powder. I used black, grey, and rust colours mixed with the shell sand. Also the whole industrial area was sprinkled with coloured shell sand.

Most of the grass comes from Noch. As I do workshops on landscaping, I had enough material in stock to do the greenery on this layout.

With the Noch Gras-Master I first applied a layer of 2.5mm grass fibres into wet glue, directly followed by a layer of 6mm fibres in several colours. At the end I added the fine details with 12mm fibres and some foliage.

For the street I used Vollmer cobblestones which were cut into strips so it was easy to make curves. There is a special method to make very nice streets. Let me explain: all the Vollmer sheets have a millimetre grid printed on the back. First measure the width of the street and cut out what you need from the sheet. Then make an incision every 2mm up to 1mm or 2mm in from the edge, depending on the curve you have to make. Now the street material is a little bit flexible and you can glue it in place easily.

Because I had to have some trees on the layout, I used some scratchbuilt trees from a good friend of mine, Jaap Vriend. He makes a lot of different trees as a hobby and sells them only to friends.



Finally

The last tasks were the legs, and the top valance with LED lighting.

For the supports I used two low trestles like those used for market stalls except that these are wooden and cost only about €3.50 each. The fiddle yard is supported by a single post. The three modules are bolted together with M8 bolts and nuts.

The top of the layout was made from 3mm white foam board, glued to the tops of the back and side walls. The valance was made from 5mm black foamboard. All the plywood was painted black.

I bought a long string of warm-white LEDs with a transformer and remote control for under €12.00.

The only thing needed was a controller. Piko offer a simple handheld controller with mains adaptor for just €20.00.

Rolling stock

I wanted this to be a Dutch railway, so I needed Dutch rolling stock. Most modellers would have suitable stock already. I have modelled many things but oddly not my own local prototype! I decided on an Artitec DE1 diesel-electric railcar in blue and a Roco class 200 diesel (the so-called 'Sik') for shunting the three coal wagons (also from Artitec). The diesel loco and the wagons were fitted with Kadee couplers which I find much better for shunting.

As the stock is not an integral part of the layout, and will have other uses, I have not included it in the budget.

Above right

More industrial buildings were modelled in low relief.

Right

The layout has a valance which houses the LED lights, and stands on a pair of simple wooden trestles.

Below

A DE1 'Blue Angel' diesel railcar (by Artitec) provides the passenger service, on an automatic shuttle when required.





Above

The factory, assembled from Auhagen modular components. Note each siding will hold only two wagons.

Below

The low relief industrial buildings were mounted on card to give a bit more depth.

Right

The printed photo backscene gives a real sense of place.



Summary

track	€182.80	donated
wood	€68.60	
blue foam sheet	€11.50	
foam board 5mm	€9.95	
foam board 3mm	€6.95	
background	€75.00	
red beads (50)	€0.35	only 7 needed
brass	€8.00	
wood glue	€3.39	
ballast glue	€5.79	
5mm card	€3.95	
Scale Scenes stone	€4.38	
doors and windows	€15.20	
low relief houses	€53.70	
station building kit	€49.95	donated
Scale Scenes low relief factory	€4.39	
Vollmer cobblestones	€8.00	
Noch materials	€25.00	from stock
Busch ballast	€1.95	" "
Preiser figures	€19.95	" "
trees	€20.50	" "
foamboard 3mm	€13.90	" "
support trestles	€7.00	" "
bolts and nuts	€8.25	" "
LEDs	€12.00	" "
Piko controller	€20.00	
Total	€590.70	

Operation

The railcar runs from the fiddle yard to the station and back on an automatic shuttle; it is only used when I take a coffee break and no visitors (usually children) are shunting wagons with the diesel using the handheld controller.

First they bring the three wagons from the fiddle yard to the station, then shunt two to one of the factory tracks and uncouple. Then move forward and bring the third wagon to the other factory track. Then the loco moves to the shed. The next thing they have to do is bring the three coal wagons back to the fiddle yard. So they have to pick up either the one or two wagons from the factory track. The train is assembled on the short track between the points. The catch is that none of the sidings will hold all three wagons ... so you have to work out how to do it and get the loco back on the front! After that the train can be moved back to the fiddle yard. Job done!

Conclusion

I exceeded my nominal budget by almost €100.00, if you include all the materials that were donated or came from my existing stocks, but not the rolling stock. What did I do wrong? Very simple: I have high standards and I wanted to do the best could. It was worth it as the layout was intended to inspire and instruct.



WHAT IS AVAXHOME?

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Peter Dale relates his experience exploring the Cape narrow gauge in search of the last steam.
Photographs by the author.

The Patensie branch

plus a glimpse of the *Apple Express*

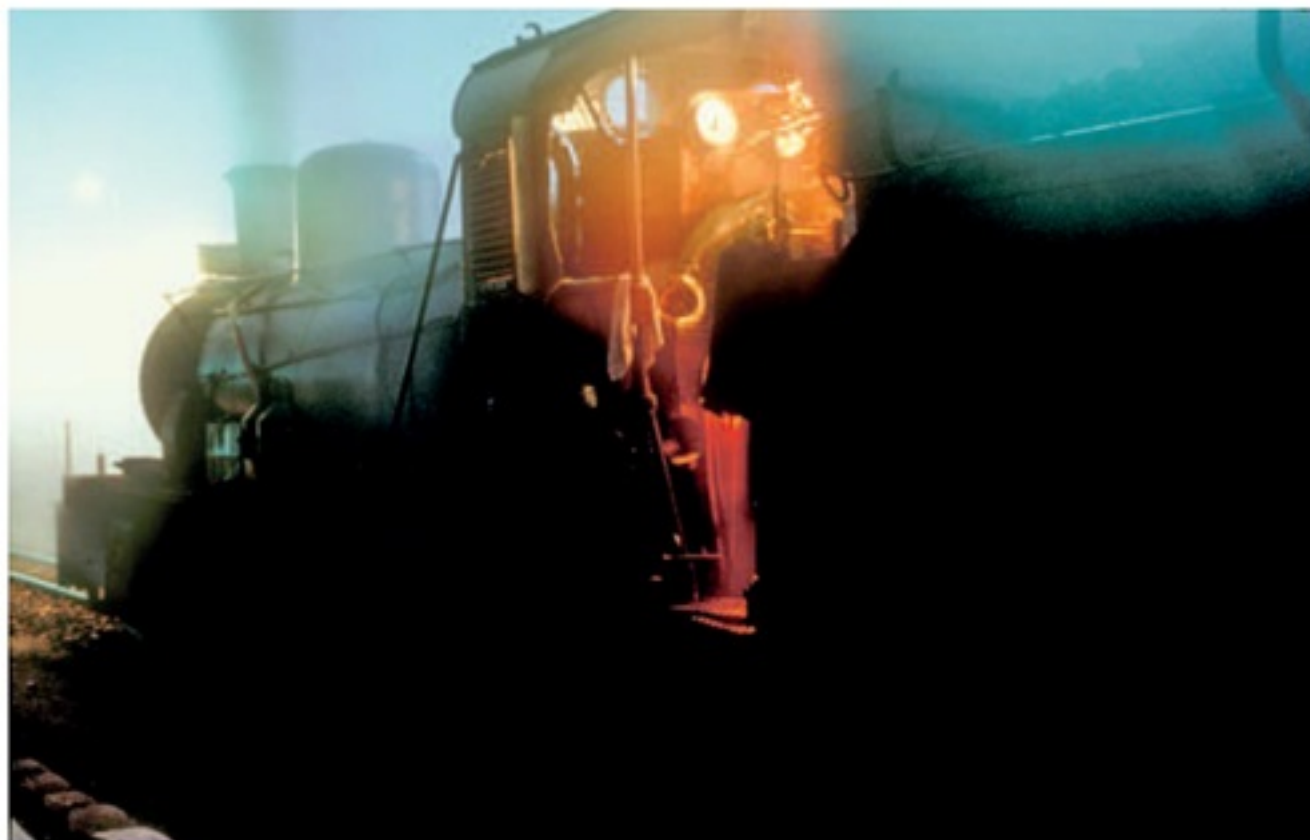
In 1990 I was living in Cape Town, but by then sadly South Africa was no longer the mecca for steam it had been. Regular steam had finished on the 3'6", except for the George to Knysna line still using class 24s. Of the 2' gauge, the Alfred County Railway in Natal was fighting a brave rearguard action and the Loerie – Patensie branch of the Avontuur line clung to steam.

The first lines in South Africa, both in Natal and Cape Colony, had been built to standard gauge but it was soon realised that a narrower gauge would be better suited for the development of the difficult terrain and large distances in southern Africa. A committee advised the government of Cape Colony to adopt 3'6" gauge as the standard for future construction. It was a wise decision. However, there were still areas where the available traffic would not be enough to give a return on the construction costs but a railway would greatly assist the development. A light narrow gauge railway could be built far more cheaply enabling a return on capital. This led to a recommendation to use 2' gauge (although 2'6" may have been more suited to the length of some of the lines and the traffic carried, but was regarded as too close to the 3'6" main lines).

Below NG15 148 and NGG16 131 about to leave Loerie, 07:05 5th July 1990.



Below NG15 148 at Loerie.



The region west of Port Elizabeth into the Langkloof was one such area, and in 1899 £445,000 was made available for the construction of a line from Port Elizabeth to Avontuur, a distance of 178 miles. It opened in early in 1907. By 1990 the main line was dieselised with the huge class 91 diesels.

The Gamtoos Valley branch (which, when extended, became the Patensie branch) was first surveyed in 1899. It was to be an important part of the line as it was estimated that half the traffic to Port Elizabeth came from the Gamtoos Valley but, at first, a launch with a lighter in tow provided a service to carry produce down river to the main line where a crane loaded it on to the next train. However, in addition much was moved by ox wagon so an Act was passed for the construction of a line from Gamtoos to Patensie, 19 miles, which opened on 3rd April 1914. It had gradients as steep as 1 in 40 in places so impressive working could be seen. Passenger services ceased in 1939 although it seems some passengers may have been conveyed in the guards van.

Freight ceased over the whole system in February 2011 but there have been moves to revive the *Apple Express*.

In the line's early days, motive power was provided by the B class 4-6-0s, built by Bagnall, despite their somewhat American appearance. They were assisted by class NG9 4-6-0s built by Baldwin during the First World War and class NG10 4-6-2s also built by Baldwin, in 1916. The NGG13 class 2-6-2+2-6-2 Garratts were introduced in 1928 and were followed by the similar NGG16 class in 1937. When the 2' gauge was widened to 3'6" in South-West Africa, the NG15 2-8-2s were transferred to the Avontuur line, replacing the last of the NG10 class.

In July 1990 I had some days off due, so my thoughts turned to the Patensie branch. I was not sure if it was still steam and it was a long way to go to find it had been dieselised! I telephoned South African Railways in Port Elizabeth and was told "Yes, it is still steam – but you had better come this week"! Fortunately there was nothing urgent at work but it was already Wednesday. I went home and packed and drove over in the late afternoon to find a place to stay.

Next day I was up bright and early. It was not far off mid-winter in South Africa but I was still surprised to find a light frost on the car. It was barely light as I drove to Lorie depot, for although the actual junction with the main line was at Gamtoos, the locos were shedded at Lorie, so that was the first stop, to see the preparations. There two locos, NG15 148 and NGG16 131, were the centre of attention. When ready they ran coupled together light to Gamtoos where there were stabling sidings and the trains were made up.

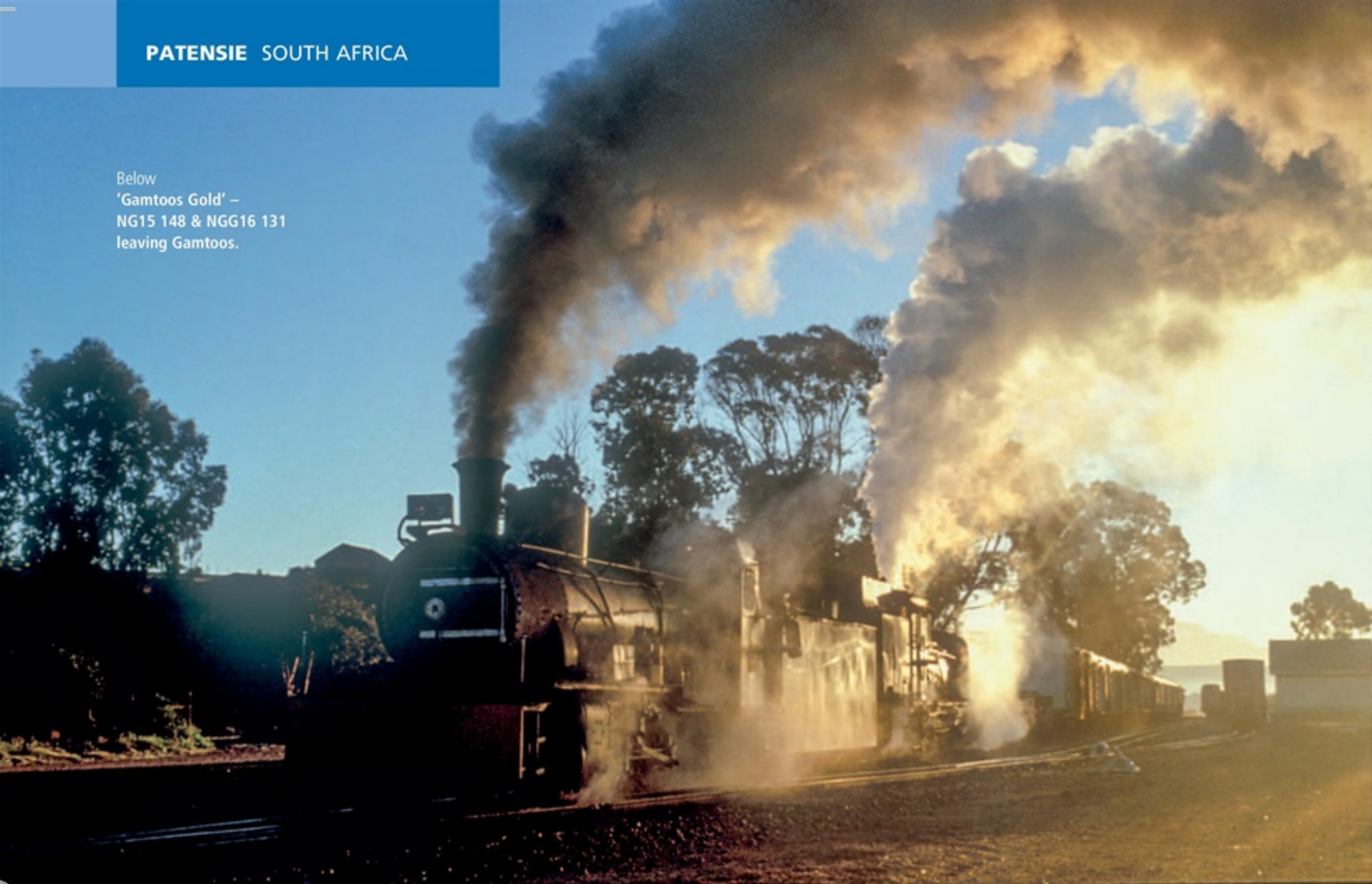


Above
NG15 148 and NGG16 131 between Lorie and Gamtoos,
Left
NG15 148 and NGG16 131 at Gamtoos, 08:00 6th July 1990.



Map – Wikimedia Commons.

Below
'Gamtoos Gold' –
NG15 148 & NGG16 131
leaving Gamtoos.



Below
NG15 148 and NGG16 131
climbing out of Hankey.





Above
NG15 148 crossing a road
near Patensie.

Right
NG15 148 entering Patensie.

Below
Improvisation – 'borrowing'
parts from NGG13 81
plinthed at Patensie.

There followed a day of exploration (as the line was new to me) and photography. While the main line carried a lot of deciduous fruit (hence the name *Apple Express*) for export out of Port Elizabeth in season between January and May, the citrus fruit harvest from the Gamtoos Valley extended from May to September so at the time of my visit there was a lot of traffic. In years gone by there had also been considerable limestone traffic to a cement factory near Port Elizabeth.

The first working of the day was a double-headed train of empty fruit vans – narrow gauge super power: at 85% boiler pressure, the combined tractive effort of the locos was comparable to that of a 9F!







Above left
NG15 148 and NGG16 131
departing Patensie,
5th July 1990.

Quite a bit of shunting was needed at Patensie to propel the empties up the upgrade to a warehouse to load the oranges. As well as slides I was also taking video and the footage shows that the NG15 had two unsuccessful attempts and I think that the train was split and taken up in two sections. Then a loaded train was assembled for the return run.

At the end of the day it was back to the hotel.. The next day was much the same again, except that in the early morning I stopped at Gamtoos to see the first train marshalled and the bright early morning winter sun truly lit the scene.

It transpired that this was not the last steam freight working on the branch – that came a week later, on 13th July.

Left
NGG16 131 climbing out of
Patensie, 6th July 1990.

Above
NGG16 131 leaving Hankey,
afternoon of 6th July 1990.



Right
NGG16 131 shunting
at Patensie, 5th July 1990.



Left
91 007 shunting the stock for the *Apple Express* at Humewood Road, early in the morning of 7th July 1990. The large modern diesel towers over the old stock.

Below left
NG15 124 backs down onto the train at Humewood Road.



The *Apple Express*

After a couple of days chasing steam on the branch from Loerie to Patensie, I was not far from Port Elizabeth (the interchange with the Cape gauge) and Humewood Road, the 2' gauge station and depot.

I took the opportunity to ride the *Apple Express*, operated for the tourist market and worked by a pair of NG15s highly decorated for the purpose, one in green livery, the other in red, and named after locally-grown apple varieties.

Besides steam haulage, the big attraction for tourists was the Van Staden's bridge (claimed to be the highest narrow gauge bridge in the world) at 254' above the gorge of the same name. A stop was made there for photos before going on to Loerie where the loco ran round for the return trip.



Left
NG15 124 working the return *Apple Express*.

Right
NG15 124 and *Apple Express* on Van Staden's Bridge.



Building a layout to meet an exhibition deadline – what could possibly go wrong?

Antony Quinlan finds out ...

Kamiack Falls

Part 2 – the technicalities

Continued from the April 2022 issue.



Above

Burlington Northern GM-EMD GP15-1 #1376
has left a single hopper at the grain elevator.

Photographs by the editor.

Kamiack Falls came about as a result of a discussion I had at the NMRA Seaboard Southern show with Dan Spalding, the exhibition manager. He mentioned that he was looking to book some new American layouts for the following year. Then, in what can only be described as an out of body experience, the following words just seem to blurt out of my mouth: "No problem – I will build you one." There was a full twelve months to go, so what could possibly go wrong?

It turns out – quite a lot.

Baseboards

The first thing to go wrong was my schedule at work. I was extremely busy professionally and construction had to be put off for a full six months, by which time the clock was ticking so I purchased some pre-assembled laser-cut plywood baseboards. The layout comprises two scenic boards with fiddle yards at each end. Overall, I was very pleased with these baseboards and I am likely to go down this route again for future layouts.



Above

The local turn rolls into Kamiack Falls.





Left
The underside of one board, showing the sandwiched beams used to strengthen the base and the hinged legs.
Photo: author.

Below
One train is held on the passing siding under the road bridge as another rolls by on the main. Spurs serve the team track, the grain elevator, and a warehouse.



However, the second thing to go wrong was that the wider scenic baseboards seemed prone to a slight amount of twisting/torsion. This would not be a problem with a normal home layout but not optimal for humping in and out of exhibition halls.

I strengthened the two scenic boards drawing on my normal method of baseboard construction – sandwiched beams. I added some softwood fillets and attached a few strips of MDF laterally to form mini-beams. It seems to have done the trick and there have not been any issues with the boards since.

The baseboards were further adapted by attaching a pair of hinged fold-under legs.

In total, the baseboard adaptations added another three weeks to the project. Time was ticking and by this time, seven of the twelve months seemed to have simply vanished into thin air and all I had to show was a set of bare baseboards.

Trackwork

Track is entirely Peco Streamline code 83, glued in place on Woodland Scenics foam underlay. Turnouts are Peco electro-frog. I find Peco track to be very robust for the rigours of exhibiting and it also looks very good, especially when dressed and weathered. The plastic sleepers were painted with weathered sleeper enamels from Precision Paints, picking out individual ties, and sprayed sporadically with track dirt. The sides of the rails were painted with Woodland Scenics Rusty Rail Track Painter. Check rails and wing rails around the frogs were chemically blackened before being painted a dark rust colour. The chemical blackening is simply to ensure that the rail remains dark in case any of the rust-coloured paint comes off when cleaning the track. Switch stands are Rix Products. These are a bit clumsy and will be replaced when I can source some suitable etched brass versions.

Peco 'Power Feed Joiners' were used throughout – these are fishplates with dropper wires pre-soldered to the underside. This keeps everything simple, provides for very good electrical connection, and avoids any unsightly wires coming through the baseboard soldered to the side of the rails (a pet hate of mine).

Points are all operated by Tortoise stall motors. I have never had a Tortoise motor fail and have found them to be 100% reliable.

Finally, when the track was fully wired and tested, it was ballasted with N-gauge granite ballast and sprayed with a bit more track dirt.

Electrics

I use the NCE Power Pro DCC system. It generally works really well. The few 'sticky' moments we have experienced at shows have always been fixed by the age-old method of turning off the power and then turning it back on again. Not very technical but it seems to work.

However, I would advise NCE users to wire additional circuit breakers into their layouts – I learnt this the hard way. Indeed, this was the next big thing to go wrong and impact on my exhibition deadline. An intermittent short-circuit literally resulted in flames shooting out of my command station. Ouch. I sent the unit off to Coastal DCC who were wonderful and managed to get the thing repaired (in



Above
Spokane Portland & Seattle
GM-EMD F7A #9754 arrives
with a very light loading
heading east to Palouse.

America) and returned unbelievably quickly. Full credit to them and they have my sincere gratitude. I made sure to purchase a number of Tam Valley DCC circuit breakers from them to avoid a repeat incident.

Wiring is very simple and fairly standard with a single DCC bus for the track and a separate isolated accessory DCC bus for the point motors. If we forget to switch a point and drive a locomotive into the frog thereby causing a short circuit, the isolated accessory bus allows us to switch the turnout immediately and without the whole layout being short-circuited. The Tortoise motors are controlled using Tam Valley accessory decoders with integral mimic buttons built into the rear of the baseboards.

We have traditionally used tethered DCC cab throttles, plugged into fascia panels. However, the new toy is a TCS UWT-100 wi-fi wireless throttle. It gives us the freedom to move about without needing to worry about plugging it in

anywhere. This has been a game changer on a switching layout such as *Kamiack Falls* and I can thoroughly recommend it.

The TCS wireless throttle is used in conjunction with a Wi-Fi Trax NCE interface. Initially, I was apprehensive about connecting products from three different DCC manufacturers (TCS, Wi-Fi Trax, and NCE) and doubted how well they would work together – if at all. This was because there are no common DCC standards for cab buses. However, my worries were entirely unjustified. The whole thing was up and running in well under fifteen minutes including unboxing, changing the Wi-Fi Trax default cab address, and putting the batteries in the throttle. It could not have been simpler.

There has been another big advantage in buying the TCS UWT-100 and that is that I can use it on my friends' layouts – even those that have different DCC systems (providing they have a suitable wi-fi interface).

Below
Northern Pacific F7A #710
at the head of a coal train.





Scenery

This is by far my favourite part of the construction process. It reminds me of building layouts when I was a kid – only now I can make as much mess as I want (well, up to a point – as I was to find out). Scenery is traditional plaster bandage on polystyrene, painted in earth-coloured emulsion paint, and finished with Woodland Scenics flock, all stuck in place with scenic glue. I had purchased various packs of static grass to put on top of the flock but found that I did not need it. When I experimented with the static grass over the flock base, to me it looked a little over-scale. So, I did not bother with it. I did, however, add individual tufts of grass from the 'Army Painter' range.

The trees are perhaps the most popular topic of conversation at shows. Originally, there were only about twenty trees on the layout but on its first outing a friend commented that the layout did not look quite finished. He was absolutely right, but adding more buildings would have lost the feel of the place I was trying to capture. The area I model is generally fairly barren, but it is typical to see deciduous trees in abundance lining the watercourses. As the layout is named Kamiack Falls, the obvious remedy was to put in another 120 or so trees. This resulted in eating take-aways for the best part of a week whilst the kitchen was converted into a tree-making factory.

The trees were made from a product called 'Super-Trees' by Scenic Express. It is a natural plant, the twigs of which are naturally tree shaped. It only took a box and a half for the entire layout and so works out extremely economical. I soaked the twigs in water to straighten them, soaked them in PVA glue, and then hung them upside down to let them dry. I added weights to the bottom of the twigs to keep them straight whilst drying. I then sprayed the twigs a dark 'trunk' colour with cheap spray paints.

I used Woodland Scenics flock to form the foliage and leaves. Some people use hairspray to glue the flock to the tree branches, but this did not seem to work for me at all. And the house stank like a hair salon. What with the take-aways and a house resembling Toni & Guy's, I was fast



Left
Burlington Northern #1970 is a former Spokane Portland & Seattle GM-EMD GP18 on an eastbound freight. The saloon bar stands right at the end of the block. Tom, Charlie, and Lindsay are knocking back the beers.

Below
Railway Street reflects the general state of decay in the once prosperous town of Kamiack Falls. BN GM-EMD GP38-2 #2087 rolls another freight east.

becoming unpopular at home. I quickly resorted to using Peco Scene PSG-11 Layering Spray aerosols in place of the hair spray. Various greens and autumn-coloured flocks were used and mixed in various proportions for the different trees. Tea leaves were used to represent fallen leaves around the base of the trunks. (Twinings Earl Grey, if you are interested.) I have yet to add any fir trees. I think I will wait until the smell of the hair spray is but a dim and distant memory!

General earth and dirt tracks were made using Treemendus Earth Powder. The road was formed from Woodland Scenics Smooth-it Road system, spray painted. Although I have modelled patches of asphalt that have lifted, there is still much detail work to be done in ageing and weathering the road surface.

Buildings

The race was now on as the exhibition deadline loomed and so I started work on the buildings while the DCC command station was away receiving specialist 'medical' treatment somewhere in America.

All of the buildings are constructed from heavily modified kits, mainly of the craftsman laser-cut wood type. I had not built any wooden kits before and received some very good

advice from the master of craftsman kit building, Mike Carter. Working on his advice, my priority was to strengthen the wooden walls so that they would not warp. I used lolly sticks as they are cheap and freely available from work. Glue is also an important consideration and again, on Mike's advice, I used a range of glues – all from Deluxe Materials but particularly their Canopy glue and Glue'n'Glaze.

The wooden walls were stained with Indian ink and left to dry. To capture the peeling paint effect, I applied masking fluid with a toothpick and then brush-painted the walls using acrylics. I rubbed over the walls with fine emery paper when the paint had dried to reveal the stained wood underneath. I varied the pressure on the emery paper to produce a general faded effect on the paint.

Railroad Street has three buildings that were clearly built by the same contractor, all in exactly the same style – based on a wonderful and very economical kit produced by Model Junction in Slough which I highly recommend, particularly for anybody starting out with laser-cut wooden kits. The idea is that over the years, the three buildings have been adapted and re-modelled by their various owners. The Saloon Bar



Right
Junk is beginning to accumulate behind the grain elevator.



at the end has barely changed at all. Nick and Mick's Guns and Bait store has been totally remodelled with the removal of the balcony and a side staircase being added so as to be able to rent out the rooms on the first floor. The building in the middle – Keith's Hardware and Mercantile Store – has definitely seen better days. I have modelled it as a burnt-out shell. (Keith got a little careless with his storage of fireworks and the rest is history.) The burnt-out building has a fully burnt-out interior with charcoal floor beams, staircase, and stud walls, together with a collapsed roof. The original idea was to have it at the front of the layout so the public could peep over and see the interior detail through the burnt-out roof. However, the scene was reconfigured as the layout developed and the shops ended up being positioned so that it is not actually possible to see the detail inside.

I created a range of custom decals for the various buildings using Photoshop and decal paper. These include the cowgirl on the side of the saloon bar and the lettering on the grain elevator, with the faded lettering of the previous sign just peeping out from behind.

The grain bins are plastic kits, painted as though a few panels have been replaced over the years. Like the faded sign on the elevator, I had seen this effect whilst carrying out general research on the internet. The grain shed to the left of the elevator is a standard Walthers kit with individual corrugated sheets glued over the moulded timberwork. Sheeting-over is a process that regularly happened to these buildings in real life and I wanted to incorporate it on the layout.

Lighting

I am all too aware that lighting can make a layout look gimmicky – but who can resist? I know I cannot. I have used Woodland Scenics lighting products to create various night-time cameos, including a number of old street lamps, LEDs in various buildings, and I just could not resist a Woodland Scenics automobile complete with working LED headlamps.



We do not often get a chance to show the full benefit of the lights at exhibitions but when we are located in a side room, we really enjoy operating the layout in the dark, especially with all the DCC lighting and sound effects on the various locomotives.

Rolling stock

Rolling stock is mainly ready-to-run with the occasional kit-built item. Locomotives (and some of the freight cars) have DCC sound on board. I feel that this really brings the sights and sounds of American railroading to life – especially the sound of the horns and bells.

All stock is weathered using an airbrush, powders, and washes. I try and obtain photos from the 1970s of the actual locomotives that I run on the layout, sourced from books and the internet. What really surprised me was how quickly the newly-painted Burlington Northern Cascade green would weather and become so grubby.

Top
SP&S F7A #9754 passes the end of Railway Street where the hardware store is boarded up but the bar is still open ...

Above
... at all hours!
The layout lighting can be dimmed to demonstrate the lights in the locomotives, road vehicles, street lamps, and structures.

Right
'Extra 8014 West' comes off
the passing siding with
a motive power transfer
of a pair of new but already
lightly weathered SD40-2s,
the pride of the fleet.



Notes on exhibiting

I live in west London and van hire is getting expensive. *Kamiack Falls* was designed specifically to fit in my car – just. I mocked-up a three-dimensional 'cuboid' to experiment and see what could be crammed in. The original idea was to have just two baseboards, bolted together between two end plates to form a box for transport. Then I realised that I could also squeeze in a couple more narrower baseboards alongside. This is when things got out of hand and a simple little two-baseboard terminus to fiddle yard layout suddenly fell victim to mission creep and became twice as long as originally intended and with twice as many baseboards.

A first for us on this layout was the use of a cassette storage system in the fiddle yards. Initially we could not get the locos to run on the aluminium cassettes. This was the final issue encountered during construction and it took a week or so to eventually realise what the problem was. It became apparent that the aluminium I had purchased was anodised and it turns out that anodised aluminium does not conduct electricity. I quickly purchased some non-anodised aluminium angles which were glued and then screwed to plywood bases to form the cassettes.

The exhibition lighting gantry is made from lightweight anodised aluminium angle (I happened to find myself with quite a bit of anodised aluminium angle lying around!) with low voltage LED strip lighting. The lighting gantry stands proud, in front of the layout so that scene is lit from the front, thereby avoiding awkward shadows and back-lighting issues.

Conclusion

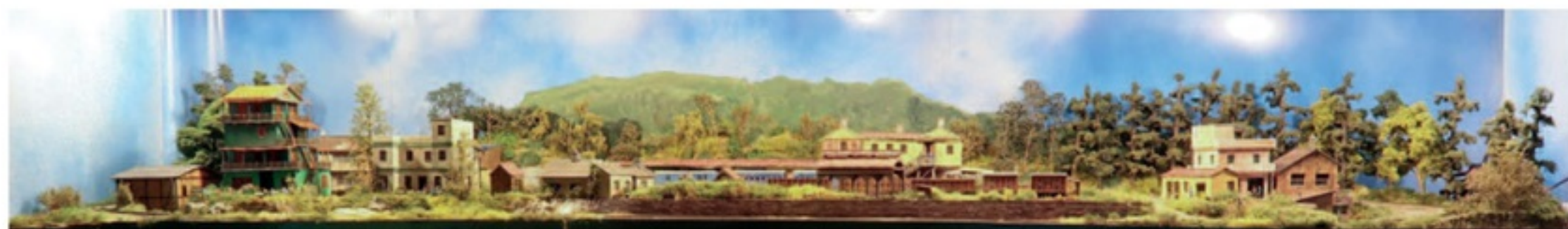
That just about concludes the story of building *Kamiack Falls*. As ever, I am indebted to Graeme Wheeler who takes charge of operations, logistics, and also provides excellent advice during the construction and planning stages. I would like to thank Mike Carter for his advice and encouragement in attempting my first craftsman laser-cut kits.

The layout was built in just under six months, give or take 120 trees, and became an exercise in things that can go wrong when building to meet an exhibition deadline.

Next time, I will make sure that I have a layout fully built and operational before I even think of offering it for exhibitions.

Below
Former Northern Pacific
GM-EMD GP18 #1997
spots a couple of hoppers
by the grain elevator.





Ranjeev C.Dubey was diverted from his long allegiance to the Kalka – Shimla with an extra project – undertaken with equal effort, attention, and commitment. *Photographs by the author.*

Ghum

A station on the Darjeeling Himalayan Railway

It all started with the mandatory 'bucket list'. If you model Indian, building at least one Darjeeling Himalayan Railway station is probably on your wish list, or should be! But which would you want to build?

The way the mountain man in me saw it, Siliguri or Sukna were disqualified simply because they are in the plains, even though the latter has some appeal. Rangtong, Tindharia, Gayabari, Mahanadi, and so on were excluded because there was not enough to them.

That left three real choices. First there was Darjeeling, which features a beautiful station to build, especially if you picked a period before 1934 when the charming original structure came down in an earthquake. I would not dream of building anything later, being no fan of art-deco buildings!

On reflection, I preferred a through station to a terminus and that brought me to Kurseong. It was certainly tempting, but it was way too spread out to make a convincing small layout. Besides, such a model would really be about building a shanty town: charming from a certain point of view, but not quite what I was in the mood for.

So Ghum hit the sweet spot by default. It had everything you would want in a through station – two passing loops, several sidings (now a museum), and an engine shed too (tracks regrettably lifted a long time ago). Bingo!

My wish list turned into an intermittent obsession as I proceeded to build the lifetime project Kalka Shimla Railway (CM October 2020 to June 2021) in HOe. One of the most under-described miseries of ambitious life-time prototypical projects is their mind-numbing repetitive routines. You are going to have to build multiple examples of similar stations, identical locomotives, coaches, and wagons all from scratch. As one year turns to the next and the railway inches up the mountain, the charm tends to wear thin. Such was my fate as well. By 2017, I was ready to give it up. Somewhere between the completion of Homage Hill and the commencement of Shimla station, Ghum launched itself as a pleasant intermittent diversion from the main project.

The model builder's angst

But why HOe? It would be perfectly logical to model the DHR in OO9. The scale/gauge worked out, and by then there were kits and parts and 3D-prints out there so it really was not that hard, especially if I was willing to model the diesel era. But human beings are fundamentally irrational! 1:87 was my drug of choice so I was going to build it in HOe.

Then came the critical mis-step. A track gauge of 9mm would be too far off: would not 6.5mm be the way to go? I suppose I could not resist the exotica. I knew Z gauge mechanisms ran too fast for convincing HO of and besides, where would I source a four-wheel mechanism? The penny dropped when I started to look at the Busch HO of system. It was irresistible. We had the mechanism, we had the track-work complete with usable turnouts, and we had the exotic appeal. I was going to do digital HO of with Busch because it had never been done before! How cool would that for the pioneering spirit?

The problem is building model railways is a lot like having children: you never quite know how they might turn out! I simply could not make digital Busch work. I had the track down, the loco mechanisms wired with DCC decoders, the turnouts connected for mechanical remote control, the track ballasted, the main station built and installed, but still smooth running eluded me. A year later, bruised and beaten, I told myself that I was not the first man to be felled by too much hubris. I admitted defeat and went back to Shimla station on the KSR! I abandoned one promising prospect to find solace in another.

But the memory remained, as did the wistfulness! Shimla was eventually built and after October 2019 I was at a loose end again. My relationship with Ghum had never found closure. That was one old flame I would have to meet again. The project re-started in about December 2019. At that point, I had a baseboard, a ready built station building, and about four other buildings. Why not get real, make the compromises, and build the layout in HOe?

Above
A panoramic view
of the whole layout.



Above
Ghum station, with canopy,
footbridge, and engine shed,
plus a colonial bungalow
on the left.
The tracks in the foreground
are the 'hidden' return loops.

Why not indeed? I had depicted Indian broad gauge in HO using 16.5mm track (see *Ritu Valley Railway*, CM August and October 2006) and I had not been lynched! Out came the Busch HO of track (at those prices, too!) to be replaced by Peco main line HOe. My hand-built HO of NDM6 locos over Busch mechanisms were junked. Meanwhile, the hand-built coaches lost their Märklin Mini-club Z gauge bogies to be replaced with N gauge bogies. In no time at all, the layout was functional again.

The prototype

For those not initiated into the Order of Darjunkies, Ghum on the Darjeeling Himalayan Railway is, at 7,404' above sea level, the highest railway station in India. The tracks trudged up the mountain for seven long years before finally arriving at their crest on 4th April 1881. It was at the time a small market town like so many others around it, but was already distinguished by a beautiful Gelukpa (yellow hat) monastery named Yiga Choeling, which had been built in its current



Right
NDM6 No.604 at Ghum
with a passenger train
ready to head south ahead
of NDM6 No.603 on goods.



Left
NDM6 No.603 at Ghum
with a northbound train.

Below
No.604 brings a goods train
into the loop at Ghum
to pass a southbound train.
The large white house
functions as a view block.

Foot of page
No.603 waits on shed
as No.604 retrieves
a rake of coaches
from the sidings
behind the colonial bungalow.

form in 1850 under the stewardship of the Mongolian astrologer and monk Sokpo Sherab Gyatso. Even today, it is much admired for its 15' high statue of Maitreya Buddha. The station building has survived as built, largely unchanged, to our times.

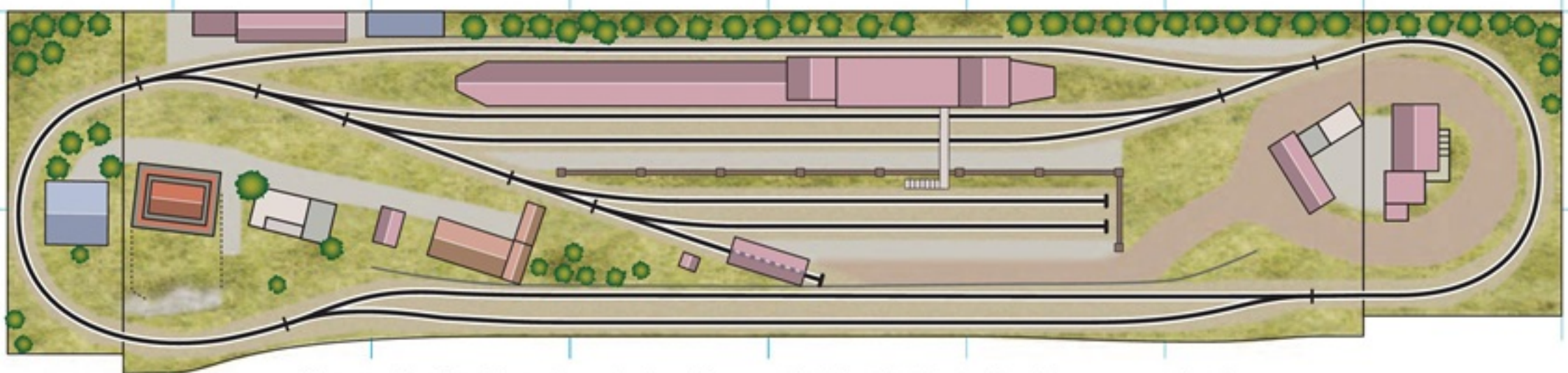
Ghum meanwhile has changed much. It now features two other monasteries, several educational institutions, a multiplicity of government offices, and vastly expanded commercial activities. Much of the old shanty town has been replaced by concrete structures inspired by our unique post-modern chocolate-cake Indian baroque! I for one could not bring myself to build what has become of Ghum's environs and this is one of several reasons that anachronisms abound on my layout. Entire hills that have now been cut into and flattened to make Himalayan 'high rise' buildings are still the hills of old on my layout. Time has moved selectively in my version of Ghum, picking the plots it wanted to advance on. More on that shortly.

The baseboard and track plan

The baseboard remains true to my philosophy of rudimentary minimalism. I took sheets of 1" thick blue foam and laminated them together to make a 66" x 18" foundation for Ghum for most of its length with a 22" bulge at one end, leaving enough of a hollow below to accommodate the 'frog juicers'. The outer perimeter of the board was then edged with 3.2mm styrene sheet, and finished with spray paints. In use, this lightweight baseboard is designed to sit on a flat table, with no legs or integrated lighting provided. Ghum is my first truly portable layout, should I ever be inspired to make a public showing of it.

Being a train watcher more than an operator, continuous running was key to my plans. I have built Ghum within a continuous loop. The main baseboard houses not just the main station but also layover sidings that would usually be hidden behind a backscene in an exhibition layout. These sidings are built at a level about 1" below the station, making them less obtrusive. This eliminates the need for a backscene. Visually, it is all quite appealing, though prototypical it is not!





Ghum, Darjeeling Overall size of layout 7' 10" x 1' 10". Each grid square = 1' x 1'.

The main baseboard does not have turnback loops, so I have built those at either end as separate fully scenic sections. The end modules are really small – approximately 7" and 11" wide respectively – making the layout truly portable. These loops are used to drop the track steeply from the station level to the siding level. Since the board is only 18" to 22" wide, the widest loop has a radius of a mere 8". Fortunately, the DHR does not run anything larger than four-coupled locos. The bogie coaches seem to track well too.

So far, there is no plan to expand this layout, not least because the DHR does not lend itself to flat baseboard treatment. Should I want to build a second station, it would be a simple matter to insert a module between Ghum and the turn back loop at either end of the layout as appropriate.

How was I to connect smaller end modules with the turnback loops to the main board? I have been reading model railway magazines since 1984, so if a method has been out there, I have probably heard of it. That does not mean I was ready to implement a precision alignment solution to this project. In the end, I decided to use short sections of Kato N Unitrack at the end of each module. Yes, it is N and it is all wrong but I did what many modellers have done before me: dropped ballast over the sleepers and obliterated its identity! Would it be possible for rail joiners alone to hold lightweight blue foam baseboards together? I held my breath as I waited to find out. So far, the track sections seem to click into place and I have had no problems. Worst case scenario: if it all falls apart, I will create bridge track sections the way they do with N-Trak modules and I will be back in business.

Which brings us to the track plan. Ghum has always featured two passing loops and two sidings, formerly the goods shed and now the museum. That was the easy part.

Below

The turnback loop at the left end of the layout is on a board just 7" wide. The tall green building and the timber-framed shed help to conceal the corner.

Below right

The turnback loop leads to a pair of tracks at the front which act as storage tracks 'hidden' in plain view.

I also wanted an engine shed. Where was I to place it and what era would that make my layout? From the modeller's viewpoint, Ghum's design is critically dependent on the era one chooses. The goods shed started off in one place, travelled to the diagonally opposite end of the station, and then disappeared entirely before being resurrected as a museum. I can do no better than refer you to Terry Martin's definitive *Iron Sherpa* for detailed information on all things Ghum. In the end, I chose to have my goods shed where the railway museum now stands.

More accurately stated, I first visited Ghum (camera in hand) in the summer of 1989. Everything I can spot in my photographs dating back to that trip is now on the layout. There are two significant exceptions, the first being the engine shed. We know the location of the shed but it did not exist in 1989 when I was there. I was happy to live with the anachronism. I could not source a photo of the shed so I have built a representative generic minimalist shed instead. This will have to do till I am able to update my database. The urban clutter is the other exception. Ghum became a slum in the 1980s and it has been downhill from there. In a concession to my delicate sensibilities, I eliminated the roadside shanty shops, restoring the atmosphere Ghum had back in the 1930s and 1940s.

Which side of Ghum would be its front? If I modelled it from the east, one pair of passing loops, the goods sidings and the engine shed would be at the back, making it visually less appealing and hard to operate. Decision taken: I modelled it from the west, engine shed in front. This has offered great spin-off benefits. The exit turnouts on either side of the station are close to the road, now at the back (see the track plan). Logic dictated that my turnback loops should bring the tracks to the front of the layout such that my 'hidden storage tracks' are right under my nose. Because I dropped this track an inch below the station, I have not had to build a hill over the passing/storage loops to make it all believable. This also works because buildings and undergrowth visually separate the station from the staging. Thanks to the drop in level, hands working the station do not knock stock sitting in the 'hidden' staging.



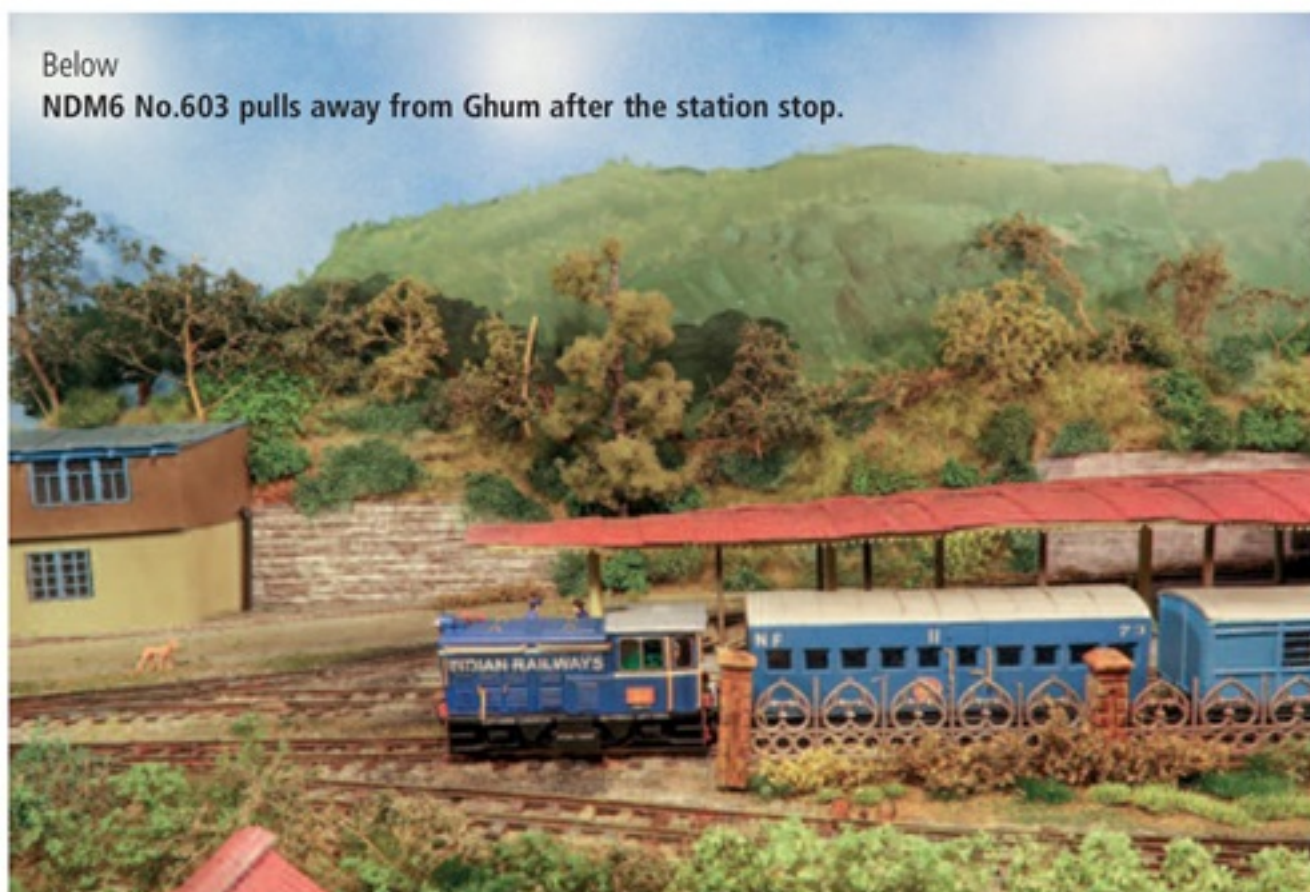


Above
A goods train heads north from Ghum.

Below
A northbound passenger train pauses at Ghum.



Below
NDM6 No.603 pulls away from Ghum after the station stop.



Motive power and rolling stock

What about the locomotives and stock? In 2017, I had built as many DHR goods carriages and coaching stock as I would ever need just before the arrival of excellent 3D-printed material that came on the market. It was a cruel blow since time is not fungible. Still, it is what it is and I will live with my imperfect hand-crafted stock. So far, I have built eighteen coaches across three eras. When I finally decide on the date of my railway, I will run the ones that seem appropriate for the time. Call it future proofing the past! I have also built several other types of four-wheel and bogie freight stock.

Not so the locos. This project remains prototypically unsatisfactory because I have not fixed my layout date. Ideally, I should have started by building a B class 0-4-0T. I could not think of a way to get it done. The NDM6 diesels just seemed easier to work with and I decided to have a few until I could figure out the challenge with the B class.

Two of these diesel locomotives were built by San Engineering and supplied to the line in 2000. They worked well and two more were acquired in 2016. I have models of all four on my layout, which dates my railway to 2016 at its earliest while my track plan dates to the 1950s at its latest (depending on when the engine shed disappeared). Hopefully, I will be able to resolve this absurdity in time.

My original scratchbuilt NDM6s were tailored to the Busch HOf mechanism. As I converted to HOe, I had no motivation to hack into those locos when I could buy new 3D-prints for very little more! I obtained my prints from Precision Model Works (a pioneering – and brave – Indian venture) and had talented Indian modeller Kaustav Chatterjee jazz them up for me. He tore into it with murderous ambition. These locos have DCC sound, a stay-alive, and added weight too in that tiny frame. Visually, they are brilliant. Kaustav has supplied all four locomotives for this layout – and we are still friends.

I must admit I am dissatisfied with the running of the NDM6s. We looked at Japanese mechanisms but I was sceptical about plastic gears so we custom ordered heavier mechanisms from a specialist UK supplier. He even kindly assembled them for me. They ran passably well as delivered but under actual operating conditions the results have been mixed.

Tinkering with the DCC settings has not worked as well as I had hoped. In time, this too will be fixed, I am sure. Meanwhile, I run non-prototypical stock when no one is looking. This includes a variety of 3D-printed bodies atop standard Fleischmann and Minitrix mechanisms of various vintages.

Couplings

A word about couplings is in order. Traditionally, all my HOe layouts have used standard Rapido N gauge couplers. These work well for me because they look neater, are easier to uncouple manually, are very forgiving of changes in gradient, and should you wish to lift a wagon out of its rake, you can simply pluck it out. Fixing the odd derailment is so easy! The Kalka Shimla Railway was the first time I tried Bemo couplings. It allowed me to employ proprietary locos and stock during the ten year long build. Now that the layout is finished and I have started operating it regularly, I am pretty unhappy with those fiddly things. Coupler height is much

too critical and uncoupling manually can be really difficult sometimes. I have added Bemo couplings to my long list of supposedly convenient things that I will never do again.

Somewhere along the way, I also added uncouplers to my list of undesirable gizmos. The early stations on the Kalka Shimla Railway used Peco OO9 uncouplers. These work with Bemo – sort of – as they do with Rapido style couplers. In time though, I found it just as convenient to drill a hole and stick an all-purpose drawing pin to a chopstick and uncouple wagons with a giant hand action. The DHR stock on this layout works that way now.

Structures

The civil construction story started with Ghum station. It is a straightforward box structure, easily made with styrene sheet. That cannot be said of the platform canopy. I created a substructure of styrene H girders, etc., and then used Slater's corrugated sheet to finish the building. It took some cutting and fitting, but it was rather an easy if fiddly build. The fun started about a year afterwards. I do not know why but the canopy keeps distorting with each passing year. I suppose I should be grateful: model railways do not get more prototypical than that!

The other buildings around the station are inspired by actual buildings found on the prototype. I picked mine from material published by Terry Martin, and duly supplemented by other pictures I have collected over the decades. All are made from styrene sheet with windows obtained from various proprietary manufacturers. While prototypically more or less representative of the original, most buildings have not found their home where they are in the real world. I relocated some because the baseboard space did not exist and others because they looked better that way. Whatever my excuse, the end result has been aesthetically pleasing even if it offends the railway historian.

Two footnotes are necessary here. First, I did build the corrugated roof platform canopy over the goods sidings. It was moody and prototypically accurate, but in the end it was only a variation on the station platform canopy. It hampered visibility and impaired operation. As time went on, it seemed like too much canopy for so small a station. That was it. I took it off. It is my layout and I choose how much ugly prototypicality I want to live with! Second, there is that brown wooden corner building within the looping road north of the station to deal with. This combined house and auto workshop was a very late addition to the layout.

Scenic elements

There is not so much to be said of the landscape 'texture'. The basic landscape has been created using real dirt over bare blue foam, all held down with white glue. A variety of European manufacturers have contributed foliage, ground cover, and bushes. Deciduous trees are mainly sea moss sprinkled with ground foam. I also added some thicker deciduous trees using Woodland Scenics tree trunks, duly supplemented with additional wire branches. All of this is tried and tested by many, indeed by me many times and truly, none of it is rocket science any more.

About the only new thing I did this time around was to make roads using Monte Mart modelling paste. It is some



sort of plastic product and works very differently from my usual plaster of Paris. It went on easily enough but did not dry nearly as smoothly as I would have liked. This is when I found out that it is far harder than plaster of Paris! I spent a long time sanding it down and the love did not grow between us.

Painting was no better. Plaster of Paris requires simple staining, but in this case, the process was far closer to painting a dense surface which means you have to get the road colour right. I would not use it again.

Ghum is capable of being exhibited, which is why it did not strike me that I should add a backscene. Eventually, the layout came to rest in one of my many internally lit layout display cases. Obviously, a wood finish background would not do, so I added plain blue sky along the back. That was better but I decided some backing scenery would be even better. I inserted some foam landforms covered with bushes and static grass. That looked even better but, clearly, there was a need for trees. These drifted on to the layout in the weeks that followed with the result that I now have proper

Above
The unusual footbridge allows access from the offices on the first floor of the station building direct to the goods yard.



Below
A northbound train arrives at Ghum station.



Left
Arranging the front
of the layout as the west
placed the loco shed
in the foreground.

Below
I wanted to model a period
when there was still
a reasonable amount
of goods traffic.

Foot of page
NDM6 No.604 on shed.

background scenery sitting independently at the back, quite separate from the layout board.

All conifers were scratchbuilt using satay sticks and bits of wire, then covered with a paste of real dirt and white glue. Once I had the final shape dried and coloured right, the foliage was easily added using a static grass machine.

In a very late addition to the layout, a properly painted backdrop also appeared. Ghum now looks much prettier and more complete with proper background mountains and a thick mop of trees at the back. Regrettably, most of the pictures were taken before these late additions.

It is now possible for me to remove Ghum and transport it should the occasion arise, leaving the scenery behind. This is why I have been able to flip the layout and take pictures from both sides ... readers will find that in some pictures, the back is the front and vice versa!



Endnote

Ghum is now complete. It lacks sufficient detail but it is hard to get that done when the layout date is not entirely clear. In any case, trackside details are a 'moving target' and that is my main defence!

The roads feature no vehicles, mainly because I run radio-controlled HO vehicles over them – to the bitter disappointment of my serious model railway comrades. My 'driving' is not quite up to scratch, but this is okay because there are not that many details to knock over!

The layout date continues to haunt me. It cannot be resolved until I finally identify a mechanism suitable to build the B class locomotives in HOe (with or without half decent valve gear). When that day comes, back dating the layout to eliminate all anachronisms will be a breeze. But that said, it would have made sense in the first place to build it in O-16.5 where all the problems would have disappeared entirely!



Right
The walled goods yard is a distinctive feature and the sidings also add operational interest.

Below
Two southbound trains – passenger at the platform and goods in the loop behind the station.

Foot of page
The house and auto workshop were a late addition. The roads are used for radio-controlled vehicles.



What comes next? Two radical changes seem to be looming. One, I am about done with hardcore prototype modelling. You can have too much of a good thing. Bear in mind that I got into this game at a time when most people fell about laughing helplessly when I struggled to build Indian Railways in India on an Indian budget with limited talent. That is history. India now has some seriously smart new players, with ambitions to match. It is easy to find them on YouTube. It is time to pass the baton on to the new generation of model builders, people who work seamlessly with 3D programs and plastic-spewing machines. Unless my mind does a back flip again, prototype freelancing is where I am likely to head. That apart, long term projects are beginning to be exhausting. What remains of my life is getting shorter and event horizons need to be closer. Why not try one of those charming micro layouts that seem to be quite the rage these days?



Ian Arkley has created another micro layout.
Photographs by the author.

Bescheiden

A Feldbahn using
a Z Shorty base

Introduction

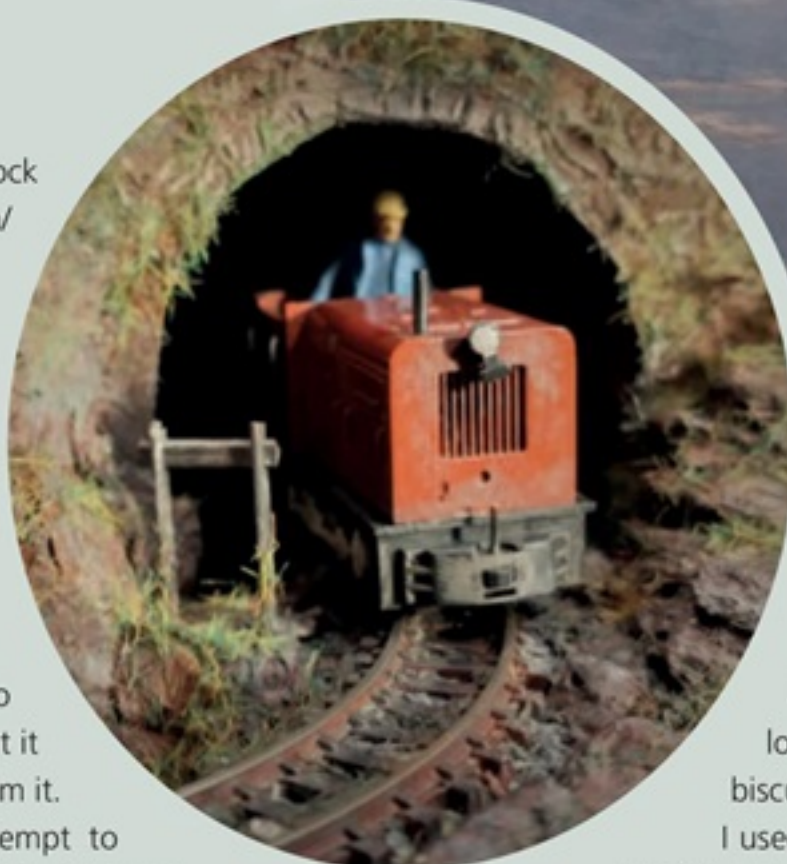
I already had some narrow gauge stock from the Busch HOf Feldbahn/Grubenbahn range and a few years ago constructed *Herbst Feldbahn* which was featured in CM in September 2020.

I recently purchased a Rokuhan Z Shorty set from Plaza in Japan for the grand sum of £25.00. The set is tiny, works well, and is great value for a first Z gauge setup. The problem is that a tiny oval with a railcar dashing round and no scenic breaks is very toy-like, but I bought it for fun and to see what I could make from it.

The idea for *Bescheiden* was to attempt to make the smallest 1:87 scale layout possible with even the tiniest amount of believability. It took around four days to make, on and off, and is the smallest micro layout I have ever attempted, measuring just 210mm by 165mm.

Construction

The layout could have been made even smaller (160mm x 110mm) but I wanted to keep the Z Shorty plastic layout base as it contains the battery holder (one 1.5v AA) and the direction switch. All I would have had to do was unscrew the track from the base, snip the wires, lift the track, and add longer wires.



The only slight complication came because I wanted to add steel strips under the tracks to attract the magnets which aid pick-up and adhesion with the Busch locos. For this I just cut up a biscuit tin with scissors.

I used a Dapol N stock box to lift the track level under the tunnel, and the foam innards from the same box to form the embankments. The track was glued in place with the tin strips underneath. The stone bridge and the tunnel mouths were made from card which were lightly smeared in smooth filler and scored to represent stones. The path was made from card to the correct shape, and the rocky, grassed areas from ripped up Sundeala board and kitchen roll paper. The wooden bridge was made from matches and wood offcuts, with a railing made from Auhagen dummy Feldbahn track. I also used a bit of the dummy track for the sign near the tunnel.



Above
The whole scene is just
210mm x 165mm.

Left
The tunnel and bridge
clearances only allow
the cableless Ns2f diesels
and the battery mine loco
to run on this micro layout.

Below, left to right
The scenic base of the scene
gradually takes shape.





The layout was framed by 3mm ply cut to shape and I made the back to fold down on gaffer tape hinges with small magnetic holders for ease of essential maintenance. Extra framing was made from coffee stirrers.

The winding of the path was inspired by a YouTube video of another Z Shorty set someone had made into a scenic layout.

Scenery

The scenery was made from a fairly small range of scatters and static grass from Peco, Busch, etc., and the water was made from clear nail varnish. This was also used for a puddle next to the stone bridge which has a makeshift pair of wooden planks to cover it to keep the non-existent inhabitants dry. The small stones by the stream are painted cork pieces. After the rockfaces were painted in a coat of artists acrylic burnt sienna, I added some weathering using a Tamiya Weathering Master set B with a little water on the brush and white acrylic dry-brushed on. Various scatters were then applied.



Above left
A battery electric mine loco hauls a basic manrider.

Above
The four-wheel Ns2f diesels have no problems with the sharp curves.

Right
Different levels help to make the scene look larger.

Below
The stonework of the bridge was scribed into plaster.

The track was painted and lightly ballasted using the usual method. After this the Tamiya set B was used again to help blend things together with a mix of brown, black, and white. Some real dead foliage was used for the wintry trees (as well as the tree included in the Z Shorty set, with some scatter added!) and I painted the backscene with acrylics. Other bits and bobs like fencing, were constructed from scrap wood.

After scenery was completed, a thorough clean-up of the track was needed. I did revisit the scenery after a few weeks to refine the look slightly, and because I added more weathering to the track I needed yet another thorough clean-up using a vacuum cleaner and isopropyl alcohol.





Operation

The Busch locos run at a nice scale slow speed on 1.5 volts and are much less noisy than when operated on the usual 3v supplied by the Busch battery controller. You can even double-head two matching LKM Ns2f locos with no loss of speed. Because of the slow running, they disappear for a while into the tunnel before re-emerging and it does not look as silly as it would otherwise.

The Rokuhan Z scale railcar can still be used as there are no structures that would make the layout strictly 1:87, but it runs a little bit faster. I have two of these, one from the set and an earlier separate purchase. They may be useful to convert to something HOf.

The four-wheel LKM Ns2f locos have a central jackshaft drive and look a bit like 0-6-0s from a distance. The Busch model mechanisms are really well done and they are quality items with beautiful looks, if a little expensive. They are bizarre as they can even run upside down because of the magnets which help with performance, but things need to be kept spotlessly clean (track, wheels, and pickups) as they are all 0-4-0 with absolutely tiny wheels.

Once the locos are going they will run for hours without needing to change the AA battery under the layout base. After a while a clean-up will be required again. The locos can be very fussy at times while at others they just keep going without any issues. I think just one speck of contamination can cause problems, and my house does have some high humidity at times being in a lush valley in rural Wales.

My green Ns2f has been running for hours today without any issues at all.

All locos and some of the wagons have been weathered.

The thing I avoided on this layout were points which were not a good experience on *Herbst Feldbahn* (neither was the crossing).

The Deutz diesel and the Decauville 0-4-0T cannot be run on *Bescheiden* as I made the clearances only just tall enough to clear the drivers' helmets on the Ns2f locos.

The B360 battery powered mine loco is the best and most consistent runner, slightly faster than the others. In real life it is even smaller and this is a great model but is just not as interesting to watch without the coupling rods on small fly cranks, as on the diesel locos. I have generally painted the coupling rods/cranks in weathered white as they are hard to see otherwise and they are amazing to watch!

To pull stock, a 5mm piece of guitar string is used between the loco and wagons. This works because the couplings on the locos and mine wagons are magnetic.

I mainly use the miniscule mine wagons on this layout but I am experimenting with other stock. Everything will go round the tight curves but some couplings had to be extended. The mine wagons have magnetic couplings and magnetic adhesion, like the locos, and despite their tiny size they work perfectly.

Above left

The wooden trestle bridge makes a good central feature.

Left

One of the Ns2f diesels squeals round the tight curves with a rake of mine tubs.



Above
The Rokuhan Z Shorty railcar which came with the set still appears now and then!

Above right
The green Ns2f diesel with a works train. The jackshaft drive and coupling rods are a delight to watch as these locos trundle round.



Conclusion

I am really pleased with this project. It was just started as a bit of fun, but has turned out better than I hoped. It is a challenge to make a layout with no buildings and no fixed figures or road vehicles look interesting, but this does have a certain character.

I wish that Busch had put a sprung pick-up foot each side of the loco (as on some large scale models). This would have helped pick-up a lot and also helped keeping the track clean. It would not have been too noticeable between the wheelsets.

The magnetic attraction would keep it in contact with the track. But it may have been impractical on something so small.

I also wonder if the locos could be converted to work from a battery in an adjacent wagon. This would certainly solve any issues with pick-ups.

There is a video on my Elvinley YouTube channel which shows the layout working.

Is there a smaller convincing continuous run layout in 1:87?



Above
The tiny mine tubs have magnetic couplings which work very reliably.



N

Deutsche Bundesbahn BR191 electric new from Piko



The class E91, later BR191, are among the most distinctive German electric locomotives. The first order was placed by the Deutsche Reichsbahn in 1922 for thirty electric locos with a C+C wheel arrangement for heavy goods trains on mountain lines. They were built by Krauss (mechanical elements) and WASSEG (electrical equipment) in 1925 and 1926. Sixteen were allocated to Bavaria, numbered EG 5 22 501 – 516. The other fourteen were deployed to Silesia as EG 581 to EG 594. From 1927 they were classified E91. In 1927 a further four were delivered.

The three-axle bogies had inside frames carrying two motors driving via a jackshaft and Winterthur diagonal side rods. The body was in three sections, the ends fixed to the bogies and the centre section suspended between them, with the connections protected by bellows. The Bavarian locos had an additional door between the two cab front windows.

The machines were to be able to haul goods trains of 1,200 tonnes at 35km/h on a 10‰ incline and passenger trains of 500 tonnes at 45km/h.

Some of Silesian machines were transferred to Ulm in the Stuttgart division in 1933. In 1943, 88 and 94 were moved to Bavaria, and 82, 92, and 93 followed in 1944.



The remaining Silesian machines were transferred to the Soviet Zone in autumn 1945. In 1946 they were given to the USSR as war reparations but returned in 1952–53. They remained mothballed until retired in 1962 and scrapped in 1965 – planned use on the Rübeld line did not occur when this was electrified at 25kV 50Hz.

Seventeen remained with the Deutsche Bundesbahn in 1950 and were modernised between 1958 and 1960, all the electrical equipment being renewed. The cabs were changed (gangways and window shields removed and the third window replaced with a larger one). They were reclassified 191 in 1969. In their final years they were not only deployed in south German depots but also at Oberhausen in the Ruhr. Due to the low top speed, they were mainly used for shunting. They were retired between 1969 and 1975.

Following the success of their HO model, Piko have created a brand new model in N, initially as DB 191 098-3, with Epoch IV markings (very clearly printed) and the relevant body modifications.

The moulded plastic body is very well detailed with grilles, beading, and rivets, and enhanced with many added parts: handrails, windscreen wipers, electrical components on the roof (cabling, switchgear, and insulators), and fine sprung pantographs. Brake hoses and scale couplings are supplied for the modeller to install if required. The detail on the chassis includes steps, sandboxes, speedometer drive, pipework, and the jackshaft drive casings. The keeper plate incorporates brake shoes in line with the wheel treads.

The model has LED lights, with directionally controlled white head and red tail lights. Additional lighting (cabs and

equipment room) can be switched under digital control, for which a Next-18 NEM 662 decoder socket is provided.

Mechanically, the model follows proven design principles: a can-type motor with two flywheels housed in the middle section is connected to the outer axle of each bogie via cardan shafts and worms. The coupling rods provide the drive to the other four axles. Current is collected by wipers on all wheels. The result is excellent running, with a minimum recommended radius of 192mm. One wheel of each middle wheelset is fitted with a traction tyre to increase the haulage capacity; the 104mm long model weighs 90g.



Manufactured by

Piko Spielwaren GmbH,
Lutherstraße 30,
D-96515 Sonneberg, Germany.
www.piko.de

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE DC ref.40540 £275.00.
DCC sound ref.40541 £385.00.





In January 2017, the ÖBB (Austrian Federal Railways) concluded a framework agreement with Siemens for two hundred new multi-system Vectron locomotives under the class designation 1293. They are to be used in more than ten countries in eastern and south-eastern Europe as well as Germany and Italy. The first and second batches have country packages for Austria, Germany, Italy, Hungary, the Czech Republic, Poland, Slovakia, Croatia, and Slovenia. Delivery of the third series (sixty-one units) started in March 2020. Twenty-eight (number range 1293 173 – 200) will also be equipped for operation in The Netherlands and Belgium.

The latest tooling variant of the popular model by Roco, licensed by the ÖBB, reproduces these features, with

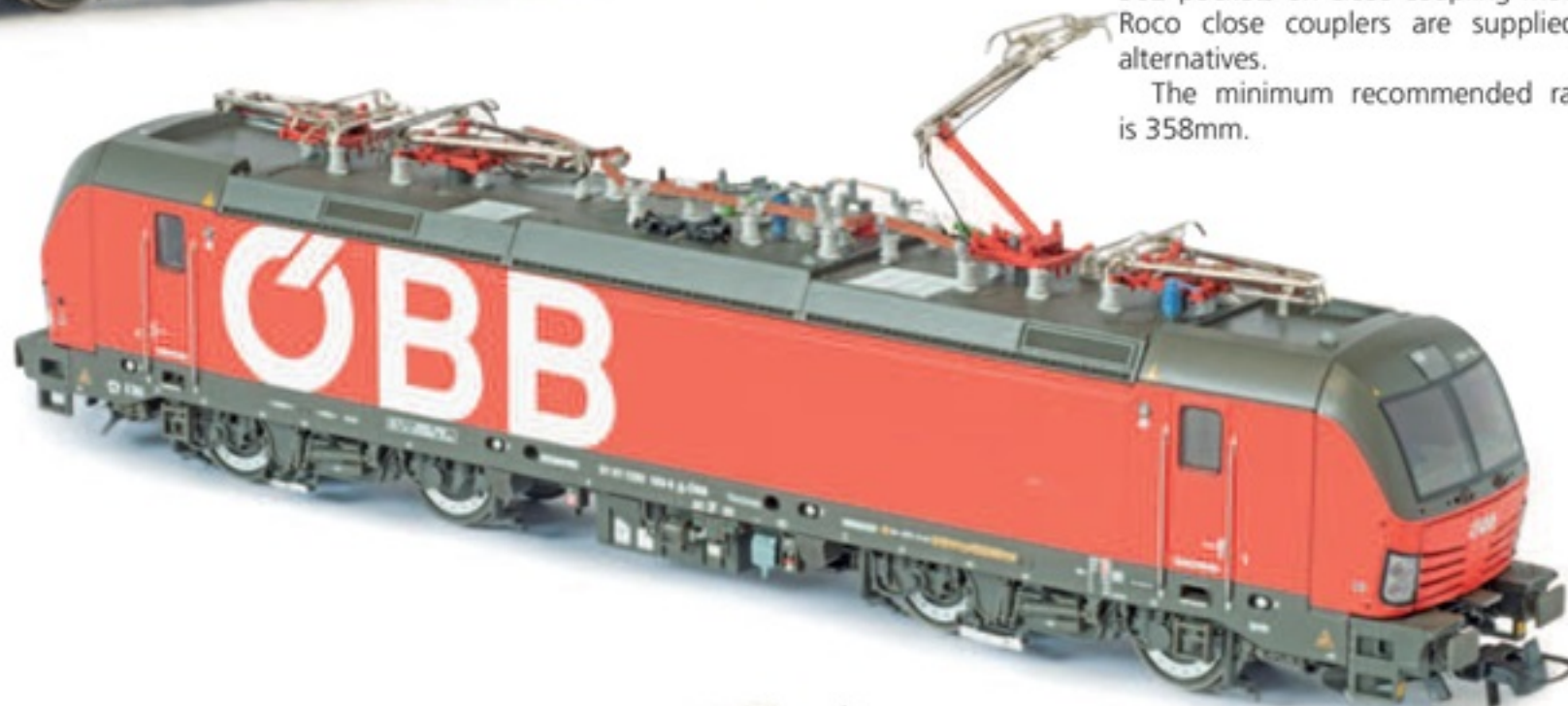
be partially or entirely switched off with a DIP switch.

A PluX22 socket allows for a digital decoder, and digital sound version is also available.

The model is built around the customary mechanism – a cast metal chassis carries a central skew-wound motor with two flywheels, driving via cardan shafts and gear reduction in each bogie. All wheels are driven and collect current, and there are traction tyres on both wheel of the inner axle of one bogie. A weight of 455g should ensure good pulling power.

Standard couplers are fitted in NEM 362 pockets on close coupling mounts; Roco close couplers are supplied as alternatives.

The minimum recommended radius is 358mm.



authentic modifications to the roof and underfloor equipment.

The plastic body shell is accurate according to published dimensions and the moulded features are enhanced with many separately added detail parts, notably cab door handrails and the equipment on the roof – horns, aerials, cabling, insulators, and switchgear. The pantographs have different contact strips, as per prototype. They are sprung but not wired for current collection from the overhead.

Parts for the modeller to add include windscreen wipers, nose handrails, multiple unit sockets, and various optional signalling/safeworking devices (LZB, Mirel, Euroloop) to fit to the underframe, plus the usual optional scale couplings and full brake hoses to replace the cut-down versions if working model couplers are not required, along with full buffer beams/air dams.

Lighting is by LEDs, with three head and two tail lights, dependent on the direction of travel. The headlights can

The model is already noted as sold out at Roco but retailers may still have stock.

The box label states that the model was made in Vietnam – Roco have had a facility there since 2016 capable of moulding, painting, and assembly, producing some rolling stock and budget/hobby locos but this is the first 'proper' loco we have noted.

Manufactured by

Roco,
A-5101 Bergheim, Austria.
www.roco.cc

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE
ref.71958 DC £235.00
ref.71959 DCC sound £330.00





Urgently needing more passenger stock, as an interim measure while new designs were being developed the Deutsche Bundesbahn created new vehicles from the early 1950s by converting old compartment coaches. Commonly referred to as *Umbauwagen*, the underframe and running gear was taken over from the donor vehicles, technically revised to standard dimensions. The bodies were completely new, of welded construction. The central axle of the six-wheelers was not braked but could move laterally to achieve good running characteristics. Also with a view to better running at higher speeds, but also for the electrical equipment, from 1955/6 the six-wheelers were run in close-coupled pairs. Any combination except two baggage types was possible.

They were built as first and third class composites, third class, and third class with baggage compartment. With the class reform of 1956, third class became second and the vehicles were classified AB3yg (1,118 built by 1958), B3yg (4,737 by 1959), and BD3yg (683 by 1958).

The coaches had air vents at the ends and only the composites had an additional 'cuckoo'-type vent in the centre of the roof.

These rebuilt coaches were indispensable in local traffic. The last examples were not taken out of service until the mid-1980s, and many were subsequently used as construction vehicles or passed to museum railways.

These coaches are now being produced as all new Expert series models by Piko.

They are marketed in close coupled pairs, as per prototype, initially with Epoch III markings: AB3yg + B3yg, B3yg + B3yg (choice of two running numbers), and B3yg + BD3yg. Markings are sharply printed, even on the ends and fall plates. Destination boards are provided for the modeller to fit.

The moulded plastic bodies are accurate according to published dimensions. They feature separately applied door handrails. Door handles are picked out in metallic paint, and the steps have a textured surface.

Other added details include the electrical connections and 'rubber' surrounds to the corridor connections which have roller shutter doors within.

The flush fitting windows are clear, and reveal the interior detail.

Interior lighting (ref.56297) and tail lights (ref.56299) are optional extras, easily installed.

The underframe is also well detailed, with separate battery boxes, toilet out-fall, cables, pipework, and brake linkages as well as black wire shunters' grabs, plus coupling hooks. Full dummy couplings, brake hoses, and steam heat hoses are supplied for the modeller to fit if working model couplers are not required.

There is precise representation of the long leaf springs, shackles, and plain bearings, with brake shoes in line with the wheel treads.

The centre wheelset can move sideways, allowing a minimum radius of 358mm.

The models roll freely on blackened metal disc wheels.

The vehicles are prototypically spaced, with close couplers at the inner ends and regular couplers at outer ends of each pair, in NEM pockets on close coupling mounts.



Manufactured by

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D-96515 Sonneberg, Germany.
www.piko.de

DISTRIBUTED IN THE UK BY
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Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.co.uk

PRICES
ref.58240 AB + B £115.00.
ref.58242 B + BD £115.00.



In the early 1960s, the Deutsche Reichsbahn put into service the first compartment coaches that largely corresponded to the international UIC-Y standard. This vehicle family is characterised by the standard length of 24.5m, a side corridor, and corridor connections with rubber surrounds. Initially, coaches of both first and second classes had six seat compartments. They were often referred to as "Bautzener Wagen" because of their place of manufacture, and were to be found in various forms. Between 1966 and 1970 22 firsts, 66 composites, and 142 seconds were ordered. Towards the end of the 1960s, the design was adapted in numerous respects to the then current state of the art and then referred to as Y/B70. These vehicles were produced both for the DR and for export to other eastern bloc countries. In addition to ordinary coaches, dining, couchette, and sleeping cars were also built. Initially, the Y stock was only used on premium services, especially on international routes, but when more modern material became available in the 1980s, these coaches were also used in ordinary express trains. They were phased out quickly after the 1994 rail reform in Germany but many are still in service elsewhere.

At the end of 2021, Piko produced not one but three brand new models of the former DR Y stock – first, composite, and second – for the first time in HO, exactly to scale. Several types enable the formation of a wide variety of authentic train consists.

In addition, two three car sets representing sections of train D300 München – Berlin in the 1970s have just been issued: one includes a new model of the type WLAB sleeping car in 'Mitropa' red livery which is not yet available separately.

The Epoch IV markings are clearly printed, down to the small data.

The moulded plastic bodies are well detailed with grilles and panels, notably at the roof ends with ventilators, maintenance hatches, and correctly positioned air intakes, and are enhanced with a number of added details such as the handrails, corridor connections, and buffer heads.

The windows sport neat metallic surrounds, and the handles on the upper edge are a neat detail.

The interior is prototypically reproduced, with individual compartments, toilets, and vestibules.

Specific lighting kits (ref.56270 for coaches and ref.56271 for couchettes) with suitably coloured LEDs are available to create an authentic atmosphere. There is also an optional rear red light (ref.56296) for even more realism.

The underfloor is also finely detailed with various assemblies represented – battery boxes, electrical compartments, and brake equipment, with controls



neatly painted.

The buffer beam can be completed with optional full brake hoses (included).

The Görlitz VI bogies feature the axleboxes, suspension, shock absorbers, and brake shoes in line with the wheel treads, plus the dynamo where appropriate.

The models roll freely on blackened metal disc wheels on pinpoint metal axles, insulated one side by a plastic centre bush.



Standard couplers are fitted in NEM pockets on close coupling mounts. With the alternative short couplings (ref.56048), the corridor connections can be kept particularly close.

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PRICES

ref.58220 set of three:
Ame first, ABme composite,
and Bme second £200.00.
ref.58221 set of three:
two Bme seconds and
one WLAB70 sleeper £210.00.

ref.58550 Ame first £65.00.
ref.58551 ABme composite £65.00.
ref.58552 Bme second £65.00.





The ETR 610 electric train sets, built by Alstom Ferroviaria in Savigliano (Cuneo), belong to the fourth generation of the well-known Pendolino tilt train family.

Since 2006, fourteen trains of seven cars (four powered units and three trailers) were built for the Italian-Swiss company Cisalpino AG to operate high speed services between Switzerland and Italy. They are tri-current sets that can run at up to 250km/h on lines powered at 3kV DC, 15kV AC 16.7Hz, and 25kV AC 50Hz. The total installed power is 5,500kW. The train is 187.4m long and can accommodate 430 passengers, plus two wheelchair users.

When Cisalpino joint venture ceased in 2009, the trains were divided equally between the Swiss Railways (SBB) and the Italian Railways (FS Trenitalia), initially retaining the same classification and liveries. These were later modified: the FS sets were branded 'Frecciargento' and the SBB were reclassified RABe 503.

The Swiss subsequently ordered twelve more sets (eight in 2012, four in 2015), with minor modifications: headlights, single windscreen wiper, and coupling cover.

Similar sets, though without the tilting facility, have been supplied to Poland, Spain, and China.

Arnold are making the models as a four unit base set (two driving ends and two second class power cars with



two pantographs). Also announced is a three car extension pack (first, second with pantograph, and restaurant with pantograph).

The vehicles are supplied in separate plastic boxes within a card outer wrapper.

The moulded plastic body shells have good panel and grille detail, with inset light lenses, windscreens, and tinted passenger compartment glazing which all but conceals the interior fittings.

Nose details on the Italian and Swiss versions are different, as per prototype.

Bogie side frames are moulded in deep relief with excellent detail, and anti-roll bars added.

In the four car sets, one end car is motorised, driven by a five-pole skew-wound motor with flywheel. All wheels are driven and collect current. Two traction tyres are fitted, on one outer wheel

of each bogie, to ensure adequate grip for moving the whole set in addition to the weight of the metal chassis.

The model runs very smoothly, but on plain DC the top speed is very fast.



Swiss EW IV restaurant car new from Fleischmann

Fleischmann have recently expanded their range of Swiss EW IV standard coaches with a new model of the type WRm restaurant car, without pantograph, as per prototype, in the current InterCity livery. The colours are well defined and the markings neatly printed.

The EW IV standard coaches were delivered from 1981 and, with over five hundred of various types built, form one of the largest groups of Swiss coaches. The body is made of welded lightweight steel. The coaches are very popular with travellers because of the spacious interior with face-to-face seating and the smooth running even at high speeds. With various modernisations over time, the EW IV coaches are still in use practically everywhere in Switzerland.

The model has a detailed moulded plastic body shell with flush glazing and interior fittings. The only added parts appear to be the corridor connections, roof access platforms, and brake wheels.

An interior lighting kit is available as an optional extra (ref.946901, £25.50).



Underframe components are moulded in deep relief.

The bogie side frames are nicely detailed, with representations of the anti-roll dampers.

The model rolls freely on blackened metal disc wheels on pinpoint metal axles, insulated one side by a plastic centre bush.

Standard couplers are fitted in NEM

355 pockets on close coupling mounts on the frame.

Length over buffers is 165mm.

The model is noted as already sold out at Fleischmann but retailers may still have stock.

Fleischmann also offer EW IV first and second class coaches and the control trailer.

Manufactured by

Gebr.Fleischmann,
A-5101 Bergheim, Austria.
www.fleischmann.de

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.co.uk

PRICE ref.890325 £48.50.



The internal circuit board includes a Next-18 socket (NEM 662) for a digital decoder and space for a loudspeaker;

A DCC sound version also offered, using a LokSound V.5 decoder by esu.

Up to thirty-one functions are available, depending on the controller used. Sounds include the running noises, high and low horns, pantograph raised/lowered, opening/closing nose cover, coupler clank, opening/closing cab door, opening/closing passenger doors, air release, compressor, sanding, vigilance device (enforced braking), curve squeal, rail clank, guard's signal, and various station announcements (in either Swiss-German or Italian).

The non-powered end car has a NEM 651 socket for an accessory decoder to control the head and tail lights.

There is no provision for interior lighting.

There are no couplings on the outer ends of the train – a shame as the Swiss sets are now usually operated in pairs. The links between the cars are by small coupling bars connecting pockets on close-coupling mounts, and the corridor connection bellows are lightly sprung.

The minimum radius recommended is 192mm.

Various liveries are offered – SBB Cisalpino, FS Cisalpino, SBB RABe 503, FS Frecciargento, FS AV Frecciargento, and even RENFE S-114 (as a four-car set only, with different second class cars).



Manufactured for

Arnold (Hornby Hobbies Ltd.),
Enterprise Road,
Westwood Industrial Estate,
Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICES

ref.HN2470 SBB DC £286.99.

ref.HN2474S FS DCC sound £391.49.



VTG four-wheel gas tanks new from Arnold

Arnold have recently released a new model of a modern four-wheel pressurised gas tank wagon with heat shield.

It is available and proposed with several different identities.

The models are supplied in pairs, packed in a standard plastic box. This set is decorated for the well-known VTG transport company, registered with the Deutsche Bundesbahn and based at Maschen yard in Hamburg.

The models have a good matt finish, with markings well reproduced, correct for Epoch IV-V. They are printed not only on the flat data boards and the curved tank side but also on the rounded ends.

The brake and load controls are also neatly picked out in red on a white background.

The moulded plastic body is detailed with ribs and the maintenance access



hatch at one end. The steps and grabs are formed as part of underframe, as is most of the brake gear. Separately-applied components include the heat-shield, an etched metal end platform, with fine handrails, and the handbrake wheels.

The model runs on blackened metal disc wheels on pinpoint metal axles, insulated one side by plastic centre bushes.

Standard couplers are fitted in NEM pockets on close-coupling mounts.

192mm is given as the minimum radius.

Manufactured for

Arnold (Hornby Hobbies Ltd.),
Enterprise Road,
Westwood Industrial Estate,
Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICE ref.HN6477 £56.60.



RENFE type JPD sliding sidewall vans new from Arnold



Arnold have recently released a new model of a Spanish type JPD long wheel-base four-wheel van.

These are 14.02m over buffers and similar to the common European type Hbis sliding sidewall design but with distinctive panelled sides, with two ventilator grilles inset high at either end. They were rebuilt from older J1 vans between 1985 and 1987. They are intended primarily for palletised loads and can carry 23.9 tonnes. Maximum permitted speed is 100km/h.

The model is available and proposed with several different identities.

They are mostly supplied in sets of three, individually packed in standard plastic boxes within a card outer wrapper.

This set are in RENFE red oxide livery, with Epoch IV markings.

They have a good matt finish, with all markings well reproduced. The brake and load controls are also neatly picked out in red on a white background.

The moulded plastic body and detailed with the ribs and louvres. Separately-applied components include metal handrails.

The steps are formed as part of underframe, as is most of the brake gear. The brake shoes are in line with the wheel treads.

The model runs freely on blackened metal disc wheels on pinpoint metal axles, insulated one side by plastic centre bushes.

Standard couplers are fitted in NEM pockets on close-coupling mounts.

The minimum radius is quoted as 192mm.



Manufactured for

Arnold (Hornby Hobbies Ltd.),
Enterprise Road,
Westwood Industrial Estate,
Margate, Kent, CT9 4JX.
www.hornbyinternational.com

PRICE ref.HN6527 £77.49.



DB E91 Epoch III
ref.51544 £285.00.



ÖBB Rh1010 Epoch IV
ref.51772 £220.00.

Piko have been particularly prolific in recent months and produced a number of variants of existing popular models in the Expert series with more or less modification to the tooling to create interesting new items.

Space precludes us from including full reviews but as the technical specifications and the standards of detail, finish, and performance are in line with the rest of the range (some of which are fully reviewed elsewhere in this issue), we wanted to draw them to your attention.



ÖBB Rh1144 Epoch VI
ref.51624 £230.00.



DBAG BR360 Epoch V
ref.52832 £190.00



Manufactured by

Piko Spielwaren GmbH,
Lutherstraße 30,
D-96515 Sonneberg, Germany.
www.piko.de

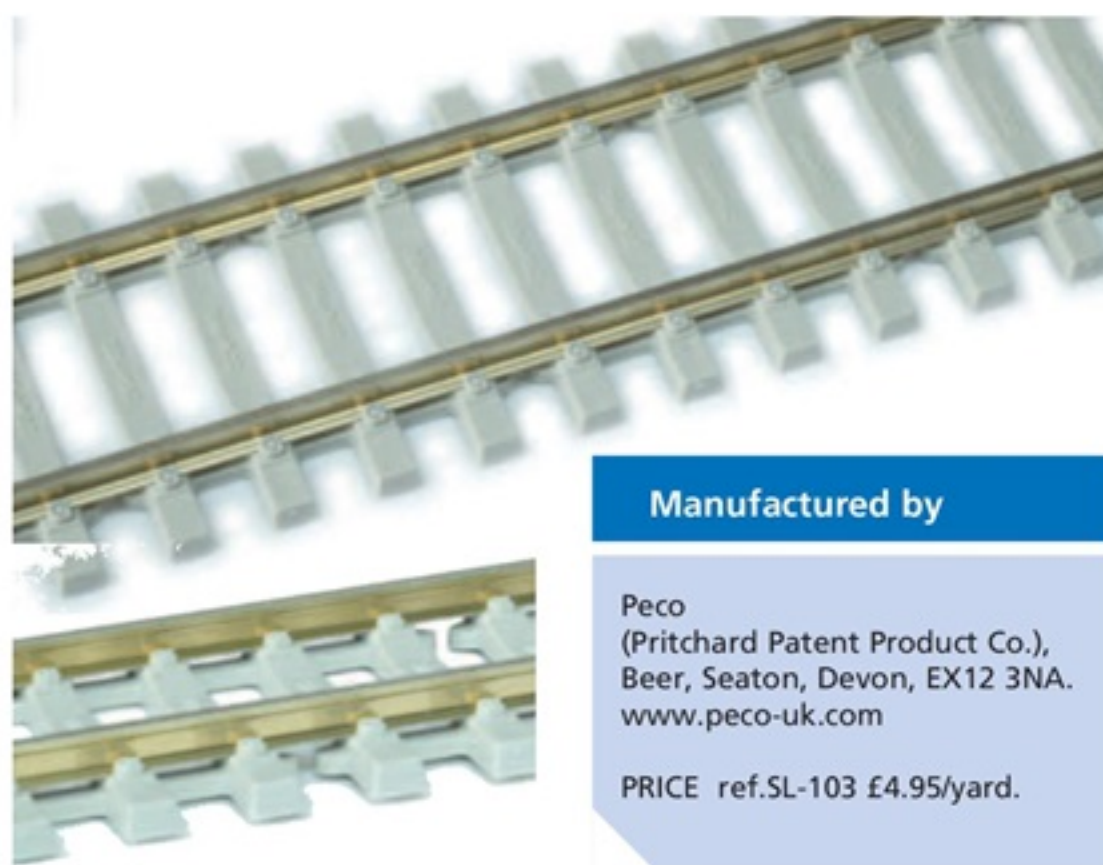
DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.co.uk

When you produce as much Streamline flexible track as Peco, even the substantial hardened steel tools used for moulding the plastic sleeper bases wear eventually and need to be refurbished to maintain the high quality for which the brand is renowned.

When the Code 100 concrete sleeper track moulds became due for maintenance recently, it was decided that it would be worth making completely new tools to current standards of precision and detail.

This includes much finer and more accurate Pandrol-style rail fixings (which still hold the nickel-silver Code 100 flat bottom rail securely) and the Dow-Mac manufacturer's name 'cast' into the concrete on the top of each characteristically shaped sleeper.

Accordingly, the reference number has been changed from SL-102 to SL-103 to distinguish the old and new products.



Manufactured by

Peco
(Pritchard Patent Product Co.),
Beer, Seaton, Devon, EX12 3NA.
www.peco-uk.com

PRICE ref.SL-103 £4.95/yard.

Modellbahn-Kurier 55 Digital 2022

EK-Verlag GmbH
Lörracher Straße 16, D-79115 Freiburg, Germany.
www.eisenbahn-kurier.de
280mm x 210mm 94pp
Softback €12.50
ISBN 978-3-8446-1758-0
German text ref.1758

This new publication from *Eisenbahn Kurier* is the annual survey of digital control systems and components. (The modelling series now seems to consist of just this and the Gauge 1 specials).

The contents include brief reports of recent new products (three pages), including a detailed description of the stereo sound in the latest KM1 steam locos.

More detailed reviews and consideration of specific products include:

starting in digital with z21, using a premium start set of Fleischmann N equipment;

the new wireless handset from Lenz;

the new XP5.1 decoder from Piko;

the new Z21 signal decoder from Roco, utilising an extension of the DCC protocol which is as yet little known but offers new possibilities;

Railcom for the Uhlenbrock Intellibox 1;

Uhlenbrock's Mfu module and WLAN interface;

the latest (2021) version of WinDigiPet software, with many new and improved functions;

Railcontrol, free layout control software for use with any digital system;

Clever Train Control, a wireless system aimed at large scale and garden railways;

a new versatile set of Loco-Net modules from Deloof, designed for self-assembly;

and the small Deutz KG230B four-wheel diesel shunter from ESU in HO, packed with the usual high-tech features.

Other articles deal with operating points with servos via the Märklin CAN-Bus; a survey of background



sound modules for the layout; and the basics of measuring techniques, with the necessary equipment.

A highlight is an account of an unusual and extensive HO private layout developed over many years which now runs through several rooms of a house and a neighbouring double garage; it is now digitally controlled using Railware software.

The articles are from various expert contributors, and range between one and nine pages.

Overall, an interesting mix of background information, practical applications, and product analysis, well illustrated with photos, screen shots, and diagrams.

There is the equivalent of thirteen pages of advertising, many for other EK publications but with some for relevant digital products and suppliers.

A good knowledge of German will be required to get the full benefit.

Loco-Revue Hors-Série 84 La Patine en pratique pour tous

Edited by Alexis Avril
Éditions Loco Revue,
BP 30 104, F-56401 Auray Cedex, France.
www.locorevue.com
300mm x 210mm 100pp
Softback €15.00
ISSN 0024 5739 French text

This new special from *Loco-Revue*, dated 4/2022, is titled 'Weathering in practice for all'. It aims to encourage modellers to create more realistic scenes by weathering locos, stock, structures, and accessories.

An informative introduction deals thoroughly with materials and equipment, and then there are sections on a steam loco, classic and modern diesel locos, an electric loco, wagon buffers, a ventilated van in the steam era, a flat wagon with a wooden deck, an oil tank wagon, small containers, four-wheel grain hoppers, intensively used bogie ballast hoppers; a lightly weathered steel express coach, flaking paint on carrier bogies, an old water tower (round steel tank on rendered stone base), using gouache to weather buildings, techniques for weathering track (sleepers and rails), paved surfaces, and chassis and tyres on road vehicles.

The subjects are, naturally, specifically French but the general techniques are widely applicable, and all examples are very nicely done. The results are impressive, and likely to encourage modellers to have a go.

Each topic is presented as a separate article, with title and credits. Some are by L-R staff, others from a handful of regular contributors. They range in length from two to eight pages.

All the modelled examples are HO, but the techniques could be applied to any scale.

The collection is rounded off with a list of suppliers, and a QR code links to a video presentation.



The presentation is in the usual style, clear and straightforward, with many step-by-step photo sequences. The pictures are clear and well reproduced.

There are panels with lists of required tools and materials or supplementary details, plus a few tips highlighted on tinted panels for with a coloured flash.

Some knowledge of French will help to get the most from this work, but there is much that can be learned from the copious illustrations.

The book includes just three pages of advertisements, for some relevant products and other LR Presse publications.



Kursbuch der deutschen Museums-Eisenbahnen 2022

Compiled by Sabine Ressel

and Sebastian Werner

Eisenbahn Kurier Verlag,
Lörracher Straße 16, D-79115 Freiburg, Germany.
www.eisenbahn-kurier.de
210mm x 140mm 180pp
Softback €7.90
German text ref.6842
ISBN 978-3-8446-6842-1

This handy guide to museum railways in Germany is published by *Eisenbahn Kurier* in collaboration with the Verband Deutscher Museums- und Touristikbahnen (VDMT). Over the years it has grown in size and scope, and the level of detail, to reflect the number and variety of the railway museums and preserved lines in Germany: the VDMT now has over a hundred members.

The core of this work are the timetables of all active museum railways, along with information about the route, location, access, operating days, prices, a summary of the motive power in use, and contact details. Over two hundred and twenty are listed. Almost all entries also include a QR code which will connect appropriate devices to the relevant internet site.

In a convenient compact form, this useful guide provides the railway enthusiast with all they might need to know about societies, museums, preserved railways, and railway companies with any preserved and/or historic stock.

The information is arranged Land by Land, roughly from north-west to south-east. An overall map at the start explains the regions, then specific locations are noted on a map at the opening of each regional section. A comprehensive contents list also aids finding any specific line. Lines which cross a state border are listed only once, under the principal location.

As usual the book includes a request for all preserved lines to send information for the 2023 edition as soon as possible.

The work is illustrated with occasional black & white photographs, placed in relevant places to balance the content, all well reproduced on good quality paper. The presentation is straightforward.

Basic German will be adequate to understand the information, which is essentially all in tables.

There is the equivalent of around twenty two pages of advertisements, most for other EK publications, but some from the lines and institutions featured, distributed through the book, placed as far as possible where relevant.

In short, a mine of information, both for planning railway visits in a particular area and checking to see what may be nearby and when it will be operating. It represents amazing value for the amount of data presented.

As the Covid-19 situation improves and restrictions on travel to and within Germany are eased, we hope that it will be possible to make use of some of the information presented here before the end of the season.

Modelling the Belgian Vicinal

by Martin Petch

Heathfield Publishing
38, Mulberry Way, Heathfield,
East Sussex, TN21 8YN.

www.capitaltransport.com
316mm x 222mm 128pp
Hardback £35.00
ISBN 978-1-854144-68-3

Belgium once had a comprehensive standardised secondary light railway network, mostly laid to metre gauge, known as the Vicinal. Development started in 1885, and eventually employed over a thousand steam tram locos and a similar number of electric trams, as well as diesel railcars. Most parts of the country were served, but nearly the entire network disappeared between 1950 and the early 1990s – only the well-known coastal route and some lines around Charleroi survived, along with several museum operations.

Beyond a small but dedicated band of devotees, few British enthusiasts were aware of the Vicinal and this book is an attempt to redress this. It reviews layouts based on the Vicinal in various scales (N, TTm, HOm, Sm, O, 1:32, G, and even larger), and provides useful data and drawings for potential modellers.

Several of the layouts featured will be familiar to CM readers – going back to *Fontaine Rebecq*, created by the author's brother, and winner of the CM Cup for 1992. More recent appearances include the excellent *Halte Tombroekstraat* (which provides a worthy cover image), *Bouillière*, *La Roche en Ardennes*, and *Vicinal à Vresse* (all by talented local modellers).

While some items of stock and structures are available in HO, there is virtually nothing in other scales, so scratchbuilding is required – and some superb examples are illustrated.

The author's own large scale garden railway project is documented.

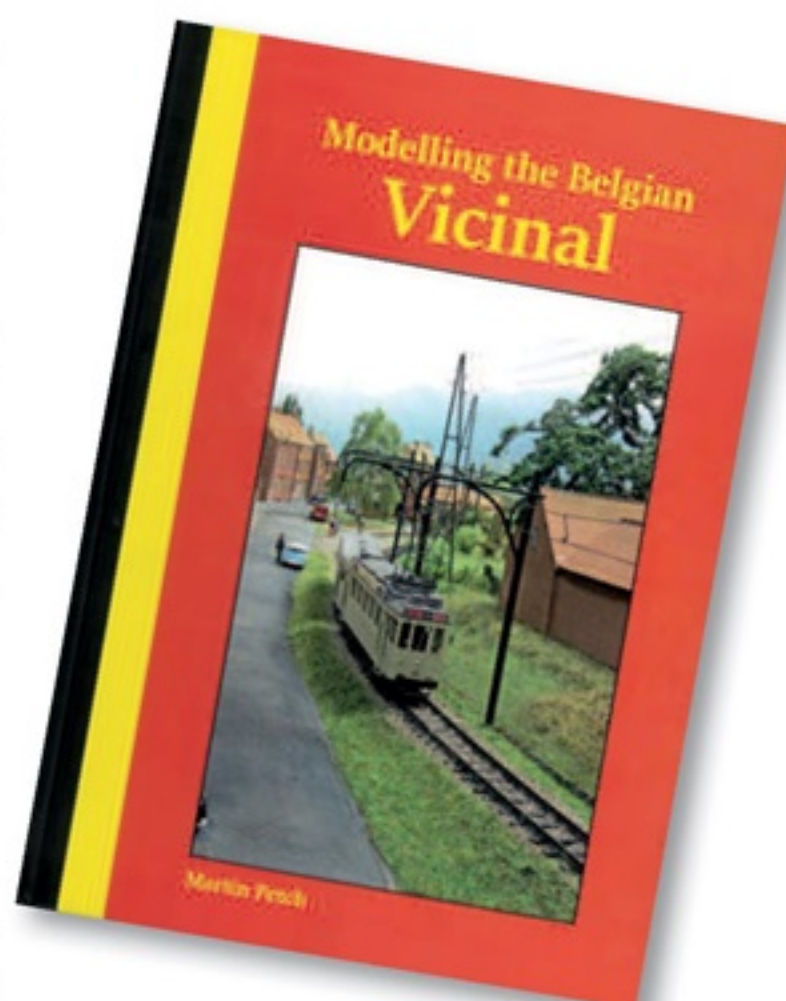
The photographs and drawings are from various sources but all are clear and the reproduction is good. Most are presented large, with informative captions adjacent. Most are credited.

The page design is simple and clear.

Appendices provide a wealth of scale drawings of stock and structures, a list of preservation sites, a survey of commercial models past and present, and models of Belgian buildings. The work is rounded off with a comprehensive bibliography.

This is both an objective review and a personal record, and as such makes an interesting read as well as an informative source. Recommended.

We understand the initial print run is limited but more can be produced if there is demand.



Model Railroading – the ultimate guide 2022

Edited by Carl Swanson

Kalmbach Media
20127, Crossroads Circle, P.O. Box 1612,
Waukesha, Wisconsin 53187-1612, U.S.A.
www.ModelRailroader.com
271mm x 219mm 84pp
Softback US\$9.99
ISBN 9781627009188

This special issue of *Model Railroader* aims to showcase the latest techniques to inform and improve your modelling. In combination with the printed step-by-step features, there are related video presentations on line.

There are seventeen sections, with subjects including: a 23' x 27' HO layout based on a railroad hauling iron ore; building a helix; repainting a GP38 for the

MR&T in N; fixing a brass loco; installing a PowerPack from ESU in a Rapido diesel; modelling a typical small town; modelling a metal foundry; getting started with paper structures; making a paved roadway; making a lake; telling stories with details; operating tips for loco allocation from the prototype; Amtrak at 50; and tips for better model photography.

Most of the modelling described is HO.

The features range in length from two to ten pages. Most are in the form of step-by-step photo sequences. Most of the photos are in colour, and all are clear and well reproduced. Diagrams and plans are computer-generated artwork, to a high standard.

There is some excellent modelling here and much useful information. Although aimed at the American market, much may have wider appeal.

There are four pages of advertisements, some for other Kalmbach publications, some from relevant suppliers (Bachmann and Peco).



Die Baureihe 50⁴⁰

by Dietmar Schlegel and Gerald Groß

Eisenbahn Kurier Verlag,
Lörracher Straße 16, D-79115 Freiburg, Germany.
www.eisenbahn-kurier.de
297mm x 210mm 192pp
Hardback €45.00
ISBN 978-3-8446-6049-4
German text order no.6049

This substantial publication, the latest in the authoritative series of class portraits from *Eisenbahn Kurier*, is concerned with the BR50⁴⁰ 2-10-0, the new standard goods locomotive built for the Deutsche Reichsbahn in East Germany after the Second World War. The first was delivered for evaluation in 1956, while the last, 50 4088, in service in 1960, was the last standard gauge steam loco built new for the DR, though a programme of rebuilds – some of which were effectively new but defined otherwise for book-keeping and political purposes – continued for some time thereafter.

Although only in service for around twenty years, the type proved powerful and well suited to the intended duties, and was used principally in northern areas (Griesswald and Schwerin); only a handful spent any time further south.

However, the high maintenance associated with the plate frames and the increasing use of diesels saw them replaced by the 1980s; the last was withdrawn in January 1981. Several survived a while longer as steam heating boilers at various depots and industrial installations. One (50 4073) has been preserved.

As the class designation suggests, the design owed much to the DRG standard 2-10-0 but used modern construction methods and the latest improvements.

The book covers the technical details, including the new features, and the similarities with the contemporary 23¹⁰ (later 35¹⁰) 2-6-2, designed to use as many common components as possible.

It lists the depot allocations and records of service, with representative working timetables to show the typical duties.

There are selected maintenance records, and a summary list of accidents and incidents, plus some notes on special workings and exhibitions which featured examples of the class. Alterations over time are also noted – very useful for modellers.

The work is rounded off with a bibliography, and supported with around two hundred and eighty-five illustrations. Most of the photographs are black & white, with colour images in a sixteen page section at the end of the book (and on the back cover).

Most of the images are presented large, two or at most three to the page, with several full page.

They range from classic roster records and close-ups of details, many under construction or in works, to views of the locos on shed, at stations, and out on the line. Some have been drawn from official railway archives, others from enthusiasts. The standard of reproduction is excellent, and every image is fully captioned and credited.

In addition there are many old documents (technical data, timetables, working diagrams, etc.) reproduced in facsimile, and many technical drawings.



Although a relatively small class with a limited life-span and a restricted range of operations, it gets the standard work of reference treatment from EK.



Eisenbahn Video-Kurier 152

EK-Videothek DVD c.60mins + 45mins ref.8552 €19.80

Normalspurbahnen im Harz

The main topic of this programme looks at the standard gauge lines in the Harz region of Germany. It begins with scenes of Abellio operated diesel unit 1648 830 in Goslar. Further sequences show this train departing the station before we see Erix operated LINT diesel units, including one on a service to Bad Harzburg. At Bad Harzburg we see the station building, its impressive stained glass window, and the semaphore signal gantry and signal box. In

Wernigerode we see a class 232 'Ludmilla' diesel-electric

entering the station with a passenger service to Dessau, with a sister

loco at the rear. At Walkenried we see the now abandoned station building with a DB Regio LINT Class 648 diesel unit arriving and departing on a local service.

Moving on to Ellrich, a DB Regio class 648 unit is seen on a service from Nordhausen to Göttingen, with another 648 crossing in the opposite direction. At Quedlinburg station and at Thale Harz-Elbe-Express (HEX) local services are seen. At Blankenberg more Abellio locals are featured worked by 1648 units, and preserved 2-10-2T 95 027 is seen on a special formed of Deutsche Bundesbahn *Silberlinge* (silverfish) coaches. There are further scenes of this train in the snow before a visit to the Rübelandbahn where we see hyle electric 146 640 at the head of a train of calcium carbonate. Archive film from the days of the Deutsche Reichsbahn (DR) then shows 171 015 in Bordeaux red on a train of DR green bogie coaches bound for Blankenberg. Later we see this loco running round its train at Königshütte and then departing. 171 013 is then seen in the latest traffic red (*Verkehrsrot*) livery at the head of a train of bogie tank wagons with further scenes of freight workings filmed from the lineside.

The next subject is 'Taigatrommel 120 274 wieder in Betrieb' with film of this preserved DR diesel-electric which is fully operational again. The loco is seen being started up and we see inside the engine compartment and the cab as it moves off shed. Archive black & white film shows other 'Taigatrommels' on freight services. Some colour footage is also shown, and film of these locos with private operators and others which have survived into preservation.

The next section looks at the railway centre of Hannover with scenes of the main station (Hauptbahnhof) featuring both local and Inter-City services, including the latest ICE 4 trains. Away from the main station, scenes filmed from the lineside depict DB Regio class 424 electric units on local services and a Captrain 146 electric on a freight. At Seelze Rbf., DB Cargo diesel-hydraulic 294 771 is seen running light.

The next topic is the DB Fernverkehr class 605 diesel ICE TD units, filmed from the lineside and at Lindau Hbf., Zürich Hbf., and Hamburg Hbf. Further scenes in the cab and from the lineside along the Vogelfluglinie to Puttgarden. At Puttgarden a 605 is seen creeping onto the ferry for the crossing to Rødby in Denmark, and later we see this train on Danish metals on its way to Copenhagen. The 605 converted into a test facility, the Advanced Train Lab (ATL), is also featured in the snow.

The *Erinnerungen* (memories) section has film of the legendary Prussian P8 4-6-0 38 1772, at first powering a Christmas special, filmed from the lineside and from the footplate. Also seen is archive colour film of this loco in DB service, including at Freudenstadt in 1974.

In *Bahn News* Berlin Gesundbrunnen station is seen with a new Stadler battery FLIRT unit for services in Schleswig-Holstein, filmed from the lineside in the snow and from inside the cab. Next we see the new digital automatic coupling and a demonstration shows them on a train of bogie open wagons. Archive black & white film briefly shows how different things used to be with conventional screw link couplings before the train of wagons with digital couplings moves off behind Voith 'Gravita' diesel-hydraulic 261 018. Following this we see Rail Adventure's electric 111 120 hauling preserved Köln type diesel unit VT 137 856; the vintage vehicle is in poor external condition but is to be fully restored.

The final *Rückblick* (look back) section presents archive black & white film of the Bodenseegürtelbahn in 1925, with trains filmed from the lineside and the footplate.

A certain amount of background music is used on this production, but it is not too intrusive. The commentary is in German (only), clearly delivered.

Trailers for other DVDs from Eisenbahn Kurier Verlag last for about five minutes.

The Bonus section features 'Dampf am Rochlitzer Berg', an autumn 1997 event with steam-hauled passenger and freight services using 03 204, 50 3616, 52 8154, 58 3047, 64 1491, and 86 1001.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. www.eisenbahn-kurier.de



Die Baureihe 5710

EK-Videothek: Baureihen DVD c.58mins ref.8615 €19.80

Die preußische Güterzuglokomotive

Between 1910 and 1925 a number of builders delivered almost 2,700 examples of this 0-10-0 goods locomotive, Prussian class G10, later Deutsche Reichsbahn BR5710-35. This programme shows many historic scenes from almost all their years in service, with examples of the last members of the class with both Deutsche Reichsbahn and Deutsche Bundesbahn, as well as those outside Germany,

such as 657 2770, a 1938 Romanian-built machine preserved by the ÖGEG in Austria since 2003.

In fact scenes of this loco on a mixed freight open the presentation, followed by lineside views of a special passenger service. This loco is then seen being turned on a turntable before working another passenger special, this time tender first.

In Germany we see 57 2770 with DB emblem with a train of four open wagons on its way to Passau in Bavaria. Some scenes were filmed on the footplate and on arrival at Garching. The fireman is interviewed and tells the history of this machine. The train then departs, seen from an overbridge, from the footplate, and from the lineside.

Next a 5710 is seen in action on a freight in Romania, numbered 50.881. 50.227, another Romanian loco, is seen at the Bayerisches Eisenbahnmuseum (BEM) in Nördlingen in Germany.

This is followed by archive black & white film of G10s under construction and later in service at the head of a variety of different freights in DB and DR service in the 1950s, with some scenes filmed on the footplate. 57 1569 and 57 2089 are among the locos featured.

This is followed by colour film taken at Griez showing 57 3297 being shunted by 346 571; 89 6009 is also seen running light.

At Dresden-Altstadt 57 3297 is seen moving off the turntable.

Moving to Turkey, we see a preserved G10 running light at Usak depot, with further scenes from the footplate and from the lineside heading a heavy freight chartered by photographers. Further scenes show this train being piloted by a Prussian P8.

Back in Germany we see archive black & white film of DB 5710 locos on freights, including 57 020 at Bestwig en route to Winterberg. Colour film shows 57 3088, first statically preserved at Hattingen and then in Bw.Siegen being moved by a Köf diesel shunter. 57 2770 is then shown at the head of a photographers' special freight filmed from the lineside, from the footplate, and at Trostberg station, where the loco runs round to work the train back tender first. Further scenes from the lineside show the loco running smokebox first and later double-headed with a Prussian P8 (BR38). 57 2770 is then seen at Neuenmarkt-Wirsberg, and later banking a heavy passenger train double headed by a DR BR015 Pacific and a DB BR23 2-6-2 over the Schiefe Ebene incline. The programme ends with more sequences showing 657 2770 in Austria.

Very little background music is used, and it is not too intrusive. The German commentary is clear and informative.

Trailers lasting approximately five minutes promote other DVDs available from Eisenbahn Kurier.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. www.eisenbahn-kurier.de



Left
57 3088 (built by Rheinmetall in 1922), with 3T20 tender and typical DRG extended chimney, preserved at the Südwestfälische Eisenbahnmuseum, Siegen, movable but not operational.
Photo: CM collection.



F-Züge

EK-Videothek DVD c.58mins ref.8614 €19.80

Der Fernverkehr der Deutschen Bundesbahn Von D-Zug zum Fernschnellzug

The 'F' stands for *Fernschnellzug* (long distance express train), introduced to the timetables from 1958. Already at the beginning of the 1950s the young Deutsche Bundesbahn had begun renewing and developing its long distance express passenger network. It was not until 1971, with the introduction of the Inter-City system, that F-Zug operations ceased. This programme tells the story of the F-Zug services using

historical film sequences of such named trains as the *Hanseat*, *Merkur*, *Schwabenpfeil*, *Roland*, *Domspatz*, and *Blauer Enzian* with locomotives of classes E10, V200, 01¹⁰, 03¹⁰, 01 and 03, as well as the VT.08 diesel units.

It begins with scenes of DB Fernverkehr class 101 electrics at the head of today's Inter-City services, together with some footage of the latest double-deck stock. This is followed by film of Pacific 01 118 on a special recreating an old DB F-Zug at Bullay. The train of period stock is seen arriving and departing the station en route from Koblenz to Trier. Further scenes were filmed from the lineside along the Mosel valley.

Another recreation of an F-Zug is seen from the lineside behind Pacific 01 1066 before archive black & white film of Pacific 03 263 on the turntable at Bw.Hamburg-Altona shed, followed by scenes of F-Zug passenger services, including the *Hanseat*, arriving at Köln Hbf. Further scenes show services filmed from the lineside in the snow, followed by colour film of 03 1001 at Hannover plus still colour and black & white images of diesel and electric units used on F-Zug services in the 1950s. More colour film then features the *Glückauf* train headed by 03 246 at Essen Hbf, with further scenes inside the passenger accommodation on this train. We are also shown a map of the different F-Zug train services. F-Zug coaching stock is observed being externally washed and internally cleaned before diesel-hydraulic V200 002 is seen backing onto the *Blauer Enzian* service to München in Hamburg. The train is seen departing Hamburg Hbf., from the lineside, and from in the cab of the V200. A VT.08 diesel unit is also seen the lineside on an F-Zug working, with further scenes showing the

passenger accommodation. V200 002 is then seen again, from the lineside and in the cab before arriving at Fulda with the *Blauer Enzian*. At Würzburg Hbf. the train is seen arriving, connecting with the *Glückauf* behind a class 01 Pacific. Track maintenance work is then seen being carried out along the main line before V200 002 and the *Blauer Enzian* pass at reduced speed. As it passes we get a good look at the rear coach, an observation car. At München Hbf. the train is seen on arrival.

In Passau the *Glückauf* observed earlier is seen behind a former Bavarian S3/6 Pacific and we watch the customs formalities being carried out before the train's onward journey across the border into Austria, this time behind an ÖBB class 1010 electric.

This is followed by black & white film of the VT10.5 diesel units *Senator* and *Komet* and more colour film of V200.0 locos working F-Zug services, including scenes at Hamburg Hbf. and in the Black Forest in the snow. The later and more powerful V200.1 type is also featured on F-Zug services.

Then it is the turn of electrics on F-Zug workings. A preserved E18 in green is seen at the head of a special. Black & white film from the 1930s shows the famous *Rheingold* express along the Rhine valley, followed by colour film of this service in the 1960s being worked by E10 locos. Further footage, black & white and colour, shows TOUROPA and Trans European Express (TEE) services using VT11.5 diesel units, including one arriving at Frankfurt (Main) Hbf. More F-Zug trains are seen here, including one behind E10 211. We then see class E03 electrics on TEE services and further F-Zug workings in the hands of the E10 (later 110).

This is followed by specials recreating the heyday of F-Zug services with 01 Pacifics and E10 electrics, including 01 1100 at Konz and 01 202 at Trier Hbf. As a contrast, we then see present day Inter-City services worked by DB Fernverkehr 101 electrics and ICE sets. The presentation ends with scenes of an 01 Pacific departing with an special formed of DB blue-liveried coaches.

Virtually no background music is used on this production. The German commentary is clearly delivered.

Trailers lasting approximately five minutes advertise other programmes available from Eisenbahn Kurier.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. www.eisenbahn-kurier.de



Kriegslokomotiven

EK-Videothek DVD c.58mins ref.8616 €19.80

Die Baureihen 42 und 52

The urgent wartime need for goods locomotives led in 1942 to a curiosity of loco building. From the standard BR50 2-10-0 came the BR52, not a new development but instead a dramatically simplified version to save material and speed assembly time. By 1945 almost 7,000 had been produced, in various factories. In 1943 the BR42 was introduced for heavier duties, again with many simplifications. This programme looks at the history of these classes with scenes from almost all of their years in service, including

some outside Germany. These *Kriegsloks* (war locomotives) were designed to be expendable with an intended service life of just five years yet they today form the largest group of surviving steam locos. The last operational examples are shown on preserved railways and at *Plandampf* events.

The programme opens with scenes of preserved 52 7596 at the head of a special passenger service in the snow at Eyach. The train is seen arriving and departing with further footage from the lineside. This is followed by scenes of 52 6106 at the head of another passenger special on the Eifelbahn, then 52 4847 is observed on a depot turntable and later from the lineside with another passenger train.

52 7409 is then seen departing with a passenger train from Darmstadt Hbf, with further scenes from the lineside. In the Deutsches Dampflokmuseum (DDM) in Neuenmarkt-Wirsberg 52 5804 is seen in the Tarnanstrich (camouflage livery) and similarly liveried 52 3879 is seen working tender first in Austria at the head of a chartered freight filmed from the lineside. In Trier Hbf. in Germany, CFL No.5519 is seen departing the station on a special with further lineside scenes.

52 4966 is then shown inside the Deutsches Technik Museum in Berlin and curator Alfred B.Gottwaldt tells the history of the type.

This is followed by archive black & white film showing *Kriegsloks* being built, including 52 4866 and 52 6172.

We then see these locos on freight workings on both the Deutsche Bundesbahn (DB) and Deutsche Reichsbahn (DR), including some troop trains, with some scenes filmed on the footplate.

Colour film then shows 42 768 at the Bayerisches Dampflokmuseum in Nördlingen while Austrian 42 2708 is seen in poor external condition awaiting restoration in Vienna. We then see examples in Poland, classified Ty2 and Ty3, on freight and passenger services. Examples depicted include Ty2 406 and Ty2 953, and in Wolstyn depot Ty2 148 is seen being turned on the turntable before taking a passenger service to Poznan in the snow. Another Ty2 is depicted on a similar working and a Russian example, T3 59 33, is also shown.

Heading next to Turkey we see *Kriegsloks* working on the state railways (TCCD), including freights filmed from the lineside and from on the footplate. In Austria, ÖBB examples are shown running light engine and on the Semmeringbahn 52 1198 is observed at the head of a heavy freight formed of bogie hopper wagons. At Straßhof we see 52 100 and others, including 52 1198, 52 3517, and 52 7102.

The next section presented archive colour film of DR BR52s on passenger services. 52 6666 is seen hauling a restaurant car, filmed from the lineside, with further scenes from the footplate. Later this loco is seen on an special formed of DR bogie suburban coaches and on shed in Berlin-Schöneweide. 52 5448 is seen stabled at Leipzig Hbf., 52 1360 is noted at Bebra on another special, this time with DB stock, and 52 8131 and 8047 are observed running light. Another special is seen headed by 52 8195 with a further example, 52 8154, at the head of a freight.

The programme ends at Hechingen where 52 7596 departs with a special passenger train and is observed from the lineside in the snow. Finally another member of the class is observed on a freight being passed by a passenger train behind a BR01 Pacific.

The commentary, which is in German (only), is both clear and informative. Little background music is used, mainly during the archive black & white sequences.

Around five minutes of trailers for other Eisenbahn Kurier DVDs follow this programme.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. www.eisenbahn-kurier.de



Classic OO9 layout honoured



Organised by Wessex Narrow Gauge Modellers, Narrow Gauge South 2022 was staged at Barton Peveril College, Eastleigh, on Saturday 23rd April.

There were 54 stands in all, including 34 layouts in various scales and gauges. The standard of modelling on display was excellent, which made selecting "best in show" (sponsored by Peco) quite a challenge. The award was presented to Dick Wyatt for his classic *Dovey Valley* OO9 layout on its final appearance after over 150 shows in

45 years or so, in recognition of the influence it has had on so many OO9 modellers; it even featured on television in a Two Ronnies musical sketch.

Above

Left to right: Andrew Burnham (Editor, *Continental Modeller*, representing Peco), Tim Couling (Exhibition Manager), Dick Wyatt (*Dovey Valley*), and Patrick Collins (Chairman, Wessex NG Modellers). Photo: Mick Thornton.

The end for a layout?



Christian Wells writes:

I am fortunate to have built, with help, the *Homage to the Alps* HO layout which was featured in CM in May and July 2003 and won the CONTINENTAL MODELLER Cup for 2003.

Less fortunately, advancing age means that my wife and I need to move soon to a smaller home. As it is unlikely I will find a buyer able to accommodate the whole 7m x 5m layout, which is not in portable sections, it will have to be dismantled.

I would be happy to offer the layout free to anyone willing to take it on despite the extensive rebuilding that would be needed. I would also be happy to help with its dismantling, but time may be relatively short.

The layout is located in the Godalming area.

edward596wells@btinternet.com



Photographs by Len Weal.

Exeter show returns



Exe Model Railway Society is pleased to be able to return to its familiar venue, Matford Livestock Centre, Matford Park Road, Marsh Barton, Exeter, EX2 8FD, on Saturday 25th and Sunday 26th June.

The exhibition will feature around thirty layouts in various scales and gauges (including a dedicated OO9 area), modelling demonstrations, societies, and traders.

A number of foreign outline layouts are booked to appear, among

them *Bw.Dügelshausen* (German HO, CM March 2016), *Gleisbau Bitterfeld* (German HO, above, coming in CM), *Norge* (Norwegian HO, CM May and June 2020), and the modular *Port Dominic Branch*. There will also be *The Andeer Line* (Swiss HOm), which offers visitors the chance to drive the trains using smart 'phones.

The venue is noted for its catering.

Further details on the website:

www.exemrs.co.uk

More narrow gauge by Halling



ÖBB 2093.001-2 in HOe

The ÖBB narrow gauge diesel 2093.01 is unique. Built in 1927 by the Grazer Waggonfabrik, it is still in service today. It was mainly used on the Ybbstalbahn between Waidhofen and Kienberg-Gaming or Ybbsitz. Later, it was used for shunting at St.Pölten-Alpenbahnhof for a long time until it was finally retired. Then it was carefully restored to original condition for museum operation on the Ybbstalbahn mountain line.

In the past Halling made several versions but a green ÖBB 2093.01-2 as used on the 'Krumpe' (Mariazellerbahn) was simply overlooked – until now.

The new model will come with new accessories. Fine metal handrails replace the previous plastic parts. The printed signs on the body can be exchanged for 'cast' plates and fine windscreen wipers visibly enhance the model. Some details are included both on the etched sheet and as moulded parts so you can decide for yourself if you want the features on your model to be more three-dimensional or really robust.

New rack railway open coaches

The new edition of the popular four-wheel open coaches for the former Gerard HO3z (10.5mm gauge) rack railway system combines modern small series production with traditional craftsmanship. The models are based on those that the model railway visionary Gerhard Riedel produced as kits over thirty years ago. The superstructure is now laser-cut from high-quality wood and printed in fine detail thanks to the modern UV printing process. An abundance of fine accessory parts is included and allows individual detailing of the model.

The red & white version is ideal for the Achenseebahn; there are also blue & white and green & white freelance models.

Optional accessory set ZRS1025 allows current to be collected via the wheels either for interior lighting or to connect to a loco for enhanced pick-up.

Halling Modelle,
Leopoldigasse 15-17,
A-1230 Wien, Austria.
www.halling.at

Chris Deith

With deep sadness Meridienne Exhibitions has announced that Chris Deith passed away suddenly on 12th May at the age of 76 whilst enjoying a holiday in Italy with his wife.

He will be remembered by many in the model engineering and hobby world as the entrepreneur who created TEE Publishing, Engineering In Miniature, Meridienne Exhibitions, and the Warwickshire Event Centre.

He is survived by his wife, daughter, daughter-in-law, and three grandchildren.

He achieved so many things during his life and his daughter, who worked with him for many years, will now continue his legacy in his honour with the support of family and the team.

Our condolences to his family, friends, and colleagues.

Forthcoming events

Saturday 18 June DIDCOT, Oxfordshire

Globalrail

Organiser: German Railway Society

Venue: Didcot Civic Hall,
Britwell Road, Didcot, OX11 7JN.

Open: 1030-1600

Admission: adults £6.00,
children £3.00, family £12.50.

Amenities: free parking; disabled
access; cafeteria.

Features: layouts include *Brunswick*
(German N), as yet un-named German
N from Paul Smith, *Rennsteig* (East
German TT), Austrian TT from Bevis
Wearing-King, *Nove Mesto na*
Nedostatku (Czech TT), *Köln Draußen*
(German HO, CM October 2018),
St.Pankraz in Steiermark (Austrian
HO, CM February 2021), *Halta Nerau*
(Romanian HO), *St.Pierre (Canal)*
et la rue Perrin (French 1:34e, CM
September & December 2002), *Naples*
Street (US TT, CM July 2018), *Blue*
Heron (US HO, CM June 2022),
Underpass (US HO, coming soon),
and *Lunisolar Peat Tramway* (16mm
scale ng micro). Societies, trade.

Contact: <https://grs-uk.org>

Saturday 18 & Sunday 19 June NORTH SHIELDS, Tyne and Wear

Organiser: Blyth and Tyne MRC
(charity event)

Venue: Christ Church, Preston Road,
North Shields, NE29 0LW.

Open: Saturday 1000-1600

Sunday 1200-1600

Admission: adults £4.00,
children under 5 free.

Amenities: 125 yards from Metro
station; bus stop outside; free parking
in adjacent school yard; refreshments.

Features: 5+ layouts; Contikits trade.

Contact: trevor3489@hotmail.com

Website: www.btmrs.co.uk

Saturday 25 & Sunday 26 June EXETER, Devon

Organiser: Exe MRS.

Venue: Matford Livestock Centre,
Matford Park Road, Marsh Barton,
Exeter, EX2 8FD.

Adjacent to Matford Park & Ride.

Open: Saturday 1000-1700

Sunday 1000-1600

Admission: adults £7.50, accompan-
ied children free.

Amenities: free parking; disabled
access; restaurant.

Features: layouts include *The Andeer*
Line (Swiss HOm), *Bw.Dügelshausen*
(German HO, CM March 2016),
Gleisbau Bitterfeld (German HO,
coming in CM), *Norge* (Norwegian
HO, CM May & June 2020), and
Port Dominic Branch (HO modular).
Demonstrations, trade.

Website: www.exemrs.co.uk

Saturday 25 & Sunday 26 June PERTH, Scotland

Organiser: Perth MRG.

Venue: The Dewars Centre,
Glover Street, Perth, PH2 0TH.

Open: 1000-1700 both days

Admission: adult £10.00, child
£4.00, family (2+2) £20.00.

Amenities: parking; disabled access;
buffet and bar.

Features: c.40 layouts in vari-
ous scales and gauges, includ-
ing *Bahnbetriebswerk* (German N),

Wurzbach (East German TT), *Dobris*
(Czech HO), *Neu Orle* (US N), *Whyte*
Hills Pass (US N), *Williamson River*
(Canada HO, CM October 2020),
Lewis Lake, Wyoming (US On30, CM
June 2022), *2nd (hand) Yard* (US O),
US O standard gauge display, and
Ishinaka (Japan N, in this issue).

Demonstrations, societies, trade.

Contact: 07803 737652

Website: www.smet.org.uk

Saturday 25 & Sunday 26 June HARDEN, BINGLEY, Yorkshire

Organiser: Keighley MRC.

Venue: Woodbank Garden Centre
& Nurseries, Harden Road, Harden,
Bingley, BD16 1BE.

Open: Saturday 1000-1600

Sunday 1000-1600

Admission: adults £6.00, children
£3.00, families £15.00.

Amenities: vintage bus shuttle from
Keighley and Bingley railway stations.

Features: 17 layouts including
Parazare (French HO). Trade.

Website: www.keighley-mrc.org.uk

Saturday 2 & Sunday 3 July PENARTH, Vale of Glamorgan

Organiser: Barry & Penarth MRC.

Venue: St.Cyres School, Sully Road,
Penarth, Vale of Glamorgan, CF64 2TP.

Open: Saturday 1000-1630

Sunday 1000-1600

Admission: adults £5.00, children
(under 14) £3.00, under 5s free.

Amenities: heritage bus from Cogan
station; free parking; disabled access
throughout; catering.

Features: over 15 layouts including
the debut of *Kleine Albula* (Swiss
HOm). Trade.

Website: www.bpmrc.club

Saturday 9 July BEER, Devon

Organiser: Pecorama.

Venue: Station Gallery/Lecture
Theatre, Pecorama, Underleys, Beer,
Devon, EX12 3NA.

Open: 1000-1600

Admission: included in Pecorama
entry (adults £12.95, children 4-13
£10.95, under 4s and over 80s free).

Amenities: parking; disabled
access (N.B. hilly site, unsuitable
for three-wheel mobility scooters);
refreshments.

Features: *The Andeer Line* – visitors
are invited to help operate this Swiss
HOm layout – instructions will be given!

Website: www.pecorama.co.uk

Saturday 23 & Sunday 24 July CUPAR, Fife

Organiser: Cupar & District MRC.

Venue: The Corn Exchange,
St.Catherine Street, Cupar, KY15 4BT.

Open: 1000-1630 both days

Admission: adults £5.00,
accompanied children (5-15) £1.00,
under 5s free. Cash or card payments;
colour show guide free with adult
admission.

Amenities: wheelchair accessible;
light refreshments sold in aid of
MacMillan Cancer Support.

Features: over 15 layouts include
St.Adrian (Swiss Z, CM February
2003). Demonstrations, societies,
trade.

Website: www.cuparmrc.co.uk

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Franklin

US On2



Also in this issue...



• Franklin

Bob Harper explains how he came to model
a typical small branch terminus in On2
inspired by the famous Maine two-footers.

• Barcola

Davide Raseni and Alessandro Rusin created
this HO tram layout representing the western terminus
of the Trieste line 6, which closed in March 1970.

• Signals in the garden

Jim Trotman shows how he scratchbuilt semaphore signals
for his 45mm gauge garden layout *Waldheim*
which was featured in our June 2022 edition.

• Vischkaai

Lamenting the outcome of the Brexit referendum,
Neil Rushby built a small Belgian HO quayside layout.

• 7th Street Terminal

Nick Dibben describes his compact American N layout,
an interurban station in the 1950s
with plenty of operational interest.

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Saturday 17th September 2022

Open to the public: 10.30am to 5.00pm

The H Suite, 100 Icknield Road, Edgbaston, Birmingham, West Midlands B16 0AA

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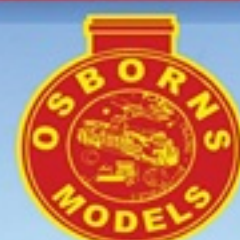
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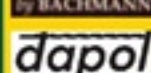
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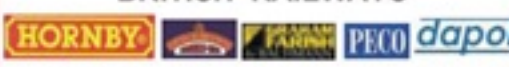
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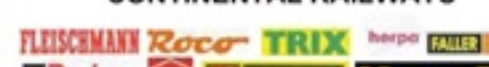
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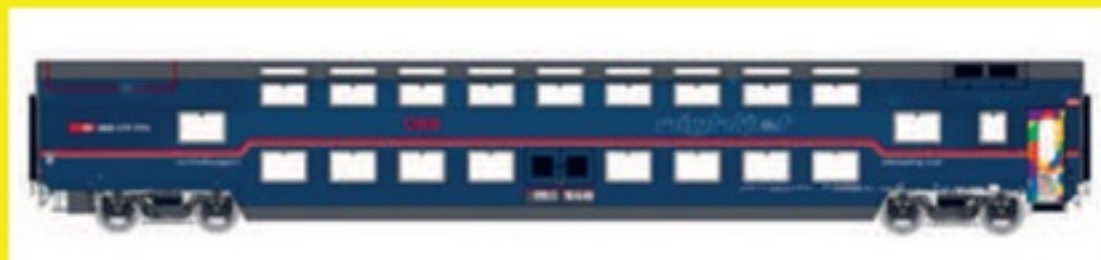
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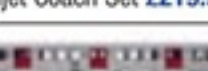
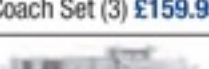
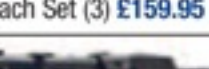
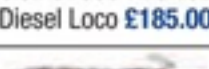
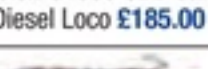
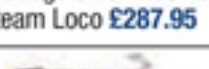
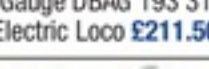
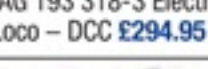
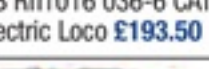
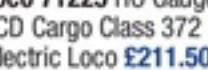
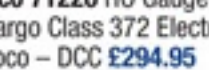
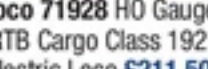
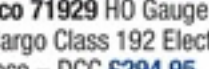
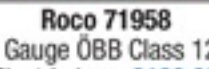
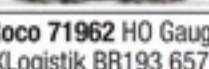
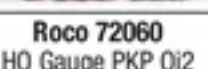
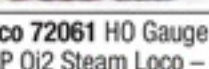
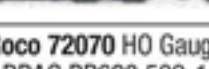
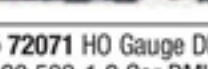
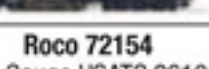
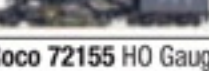
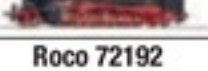
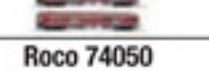
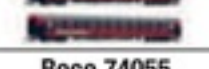
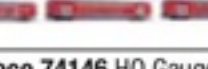
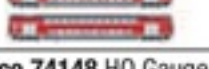
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