

CONTINENTAL MODELLER

FEBRUARY 2021 £5.20 (UK)

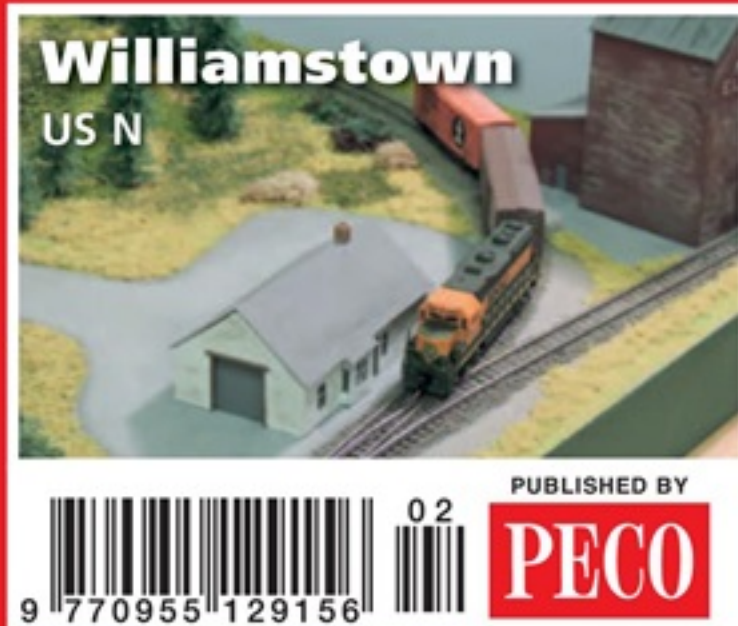
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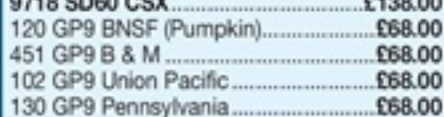
10418 GP9 C & NW DCC Ready£138.00



20403 GP9 BN (S & DCC)£165.00



9718 SD60 CSX£138.00

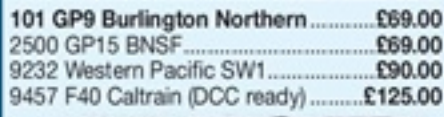


120 GP9 BNSF (Pumpkin)£68.00

451 GP9 B & M£68.00

102 GP9 Union Pacific£68.00

130 GP9 Pennsylvania£68.00



101 GP9 Burlington Northern£69.00

2500 GP15 BNSF£69.00

9232 Western Pacific SW1£90.00

9457 F40 Caltrain (DCC ready)£125.00



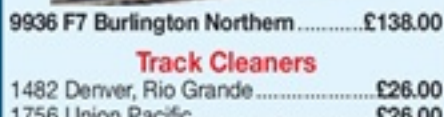
9713 SD60 Burlington Northern£138.00



10361 SD50 Union Pacific£138.00



9716 BNSF SD60£138.00



9936 F7 Burlington Northern£138.00

Track Cleaners

1482 Denver, Rio Grande£26.00

1756 Union Pacific£26.00

Caboose



1503 Santa Fe£24.00

1520 BNSF£24.00

1502 Union Pacific£24.00

Flats

104511 ACL 50' Pulpwood£21.50

104505 MP 50' Pulpwood Flat£21.50



1603 Union Pacific Flat Car£17.00

1462 D & RG Flat Car£17.00

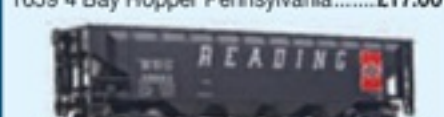
1601 Burlington Northern Flat Car£17.00

Hoppers



1423 Rock Island Hopper£17.00

1659 4 Bay Hopper Pennsylvania£17.00



1422 4 Bay Hopper Reading£16.50

1655 Offset Hopper N & W£17.50

Tanks



1124 Union '76 3 Dome£22.50

1441 Br. Columbia Railway£17.50



1442 Cooks Paints£17.00



1615 Bakers Chocolate£17.00

Box Cars

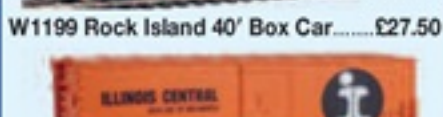


2318 50' Santa Fe 'Shock Control'£23.95

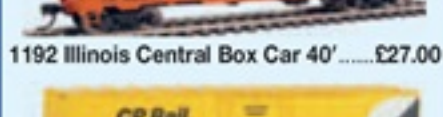
2825 50' insl. Box Car Union Pacific£24.80



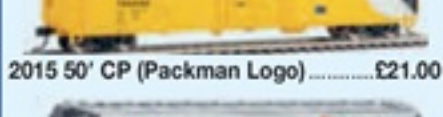
W1199 Rock Island 40' Box Car£27.50



1192 Illinois Central Box Car 40'£27.00



2015 50' CP (Packman Logo)£21.00



1310 40' Southern Pacific£24.50

2711 40' Union Pacific£20.50



1169 Penn Central 40'£27.00

40804 C & NW Box Car 40'£27.00



1198 40' Northern Pacific£27.00

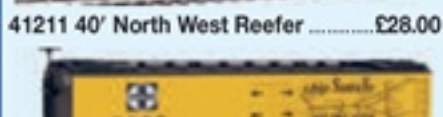


1163 GN 'Big Sky Blue' 40'£27.00

Reefers



41211 40' North West Reefer£28.00



41218 Santa Fe 40' Wood Reefer£27.70

Opening Hours

Mon & Tues 10 a.m. to 5 p.m.

Thurs & Fri 10 a.m. to 5 p.m.

Sat 10 a.m. to 4 p.m.

Tel 01639 731005

ALL items IN STOCK at time of going to press!

American HO Scale Locomotives/Freight all brands



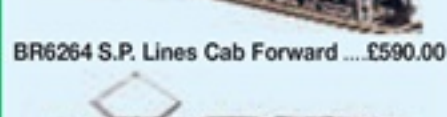
A3208 GP38 Bangor & Aroostook£165.00



BW24805 SMS 4-4-1000£130.00

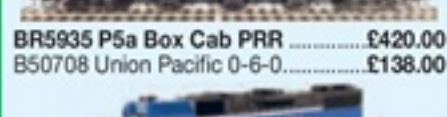


BR6264 S.P. Lines Cab Forward£590.00



BR5935 P5a Box Cab PRR£420.00

B50708 Union Pacific 0-6-0£138.00

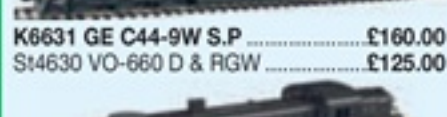


B61719 GP38-2 GMTX£115.00

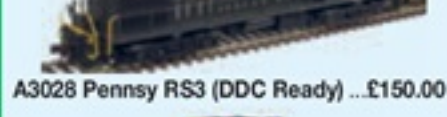


K6631 GE C44-9W S.P.£160.00

S14630 VO-660 D & RGW£125.00



A3028 Pennsy RS3 (DDC Ready)£150.00



B17702 AT & SF Bay w. Caboose£28.80

B17725 36' D & RGW Caboose£20.00

BW41642 X32 Box Car Virginian£23.50

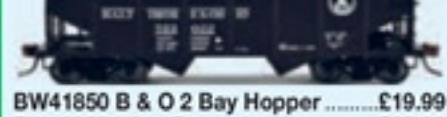


M4062 Monarch Foods Reefer£38.60



BW41850 B & O 2 Bay Hopper£19.99

BW42055 H21a Pennsylvania£25.60



BW42432 40' GN Box Car£28.60



A3984 Libby's Ref Van£39.95

B16402 Blacksmiths Car W & A£45.00

B16149 Crane Car & Boom Tender£58.95

B17947 50' Tropicana Reefer£25.95

CC19304 3 Bay Hopper Southern£18.25

CC523 3 Bay Hopper BNSF£18.25

B19806 40' Grand Union Reefer£29.95

B19805 Reefer 'Pure Carbonic'£24.00

A3529 50' Box 'Cotton Belt'£25.95

B19803 40' ART Reefer£27.00

'O' Gauge buildings p&p £4.99 (BU - Built up)



Victorian Station Kit (pre-coloured) 380mm x 120mm£42.45 whilst stocks last!

B33165 12 Seated People£16.50

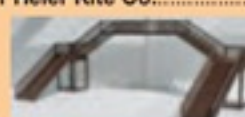
A6902 Island Platform x 2£35.95

MTH9097 Gate Tower BU£29.70

L612888 Railroad Crossing Kit£49.95



DPM951 Heier Kite Co.£58.40



Lattice Footbridge£26.95



Signal Box£27.95



A6902 Platform x 2£35.95

AMB49 Company House (laser cut wood)£29.65

A6916 Steel Water Tower£44.10

B45979 Coaling Tower£31.20

American N Gauge post & packing £3.99



B63560 GP40 CSX£89.00

B63566 GP40 Conrail£89.00

B63565 GP40 N & S£89.00



A3754 GP30 D & RG£120.00



K9212 SDP 40F Santa Fe£105.00

B51572 Southern 2-6-2£99.00



A3952 Union Pacific SD60£135.00



RP35 GMD-1 CN 1400£150.00



K053 PA1 AT & SF DCC£150.00



FV513 Coal Gondola 'Fayette Power'£27.50



A4684 52' UP/CNW Gondola£22.00



A2540 50' MKT Box Car£22.68



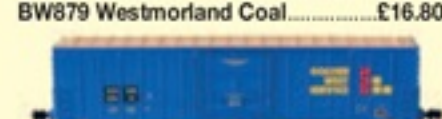
B145654 85' Str. Obs Undec£120.00

B14754 85' Str. Coach Undec x 2£120.00

B14654 72' Baggage Undec£120.00



BW879 Westmorland Coal£16.80

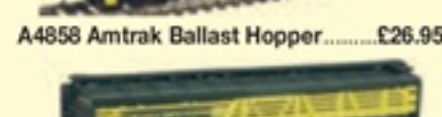


WOT194 D & RGW Box Car£25.40

WOT201 T & P Box Car£25.40

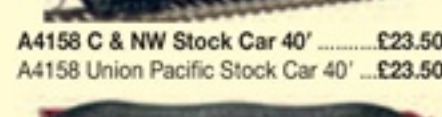


A4858 Amtrak Ballast Hopper£26.95

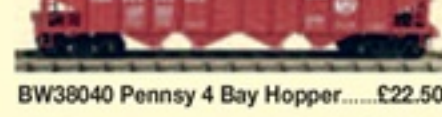


A4158 C & NW Stock Car 40'£23.50

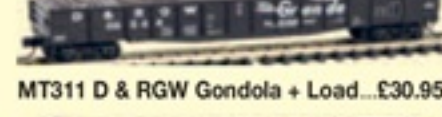
A4158 Union Pacific Stock Car 40'£23.50



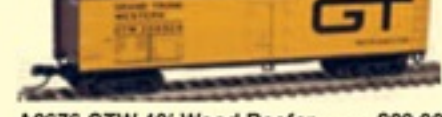
BW38040 Pennsy 4 Bay Hopper£22.50



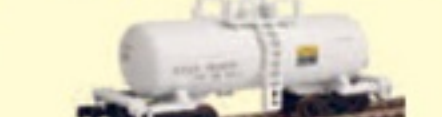
MT311 D & RGW Gondola + Load£30.95



A2676 GTW 40' Wood Reefer£23.90



A3202 Single Dome 'Exxon'£16.90



A2780 40' Finlay Premium£16.25

A2848 Maywood Station£30.15

W3240 Clarksville Depot£32.00

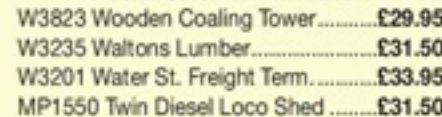
MP1546 Holland Iron£19.95

W3823 Wooden Coaling Tower£29.95

W3235 Waltons Lumber£31.50

W3201 Water St. Freight Term.£33.95

MP1550 Twin Diesel Loco Shed£31.50



M9210 Hickson's Depot£31.50

SCENIC MATERIALS



A variety of grass tufts, scatters and trees to build up realistic vegetation on your layout. Just blend them together to get the desired effect.



MODEL FIGURES

Noch produce a comprehensive range of pre-painted and unpainted figures to populate your layout, from passengers to the emergency services.



LASER-CUT PLANTS

Made from Laser-Cut paper, these kits simply fold together to create realistic plants and vegetables to plant onto your layout.



DECO SCENES

A complete themed scene in a pack, ready to add life to your layout - including the new Sound Scenes range which adds an extra dimension.



Preiser

Figures for Every Scale



Preiser produce the most comprehensive range of model figures on the market. There is a vast array of different subjects available, allowing you to populate every area of your layout.

Railway Scales covered:
Z, N, TT, HO, OO, O, G, and
Gauge 1.



A selection of other scales
useful to architects and
modellers are also included
in the range.



Useful scenic items and accessories
are also available, allowing you to add
the realism of street furniture and
clutter to your layout.

Most of the range are
pre-painted and ready
to add to your layout,
with unpainted packs
available for those
who want a bit more
customisation.
A range of single
figures are available
in HO Scale.



Preiser also produce value
packs, allowing you to cover
your layout with people for a
keen price.
You're sure to find what you
need in the Preiser range.

Roco



Item Nr.: 73035

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60

ROCO has been your loyal partner in the world of model trains for more than 60 years! Our railway models are available in the H0 and H0e track gauges, no matter whether you are interested in historical steam locomotives or modern multiple unit trains. Of course ROCO also offers everything that goes along with your model trains. Our innovative Z21 controls enable a classic operation with a hand control unit or via a smartphone or tablet. The ROCO Line track system offers a wide and diverse range of options to design your setup.

H0: Steam locomotive class 58, DB



Item Nr.: 71922

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3/2

- Anniversary model - „60 years of Roco“
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H0e: Steam locomotive Mh.4, NÖVOG



Item Nr.: 33272

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4/1

Item Nr.: 33273

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4/1

- Precise realization of the Mh.4 in the current operating state
- Model with colour-contrasted boiler rings

If you would like to find more about our Z21-Systems,
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TRIX



N and HO Scale Continental Specialists

MINITRIX N 1:160 Scale

Minitrix produce an extensive range of German locomotives and rolling stock, from the steam age to the modern era.

N Scale gives you the flexibility to build a complex layout in a relatively small area.



TRIX HO 1:87 Scale

The Trix range covers the majority of Europe in HO Scale, but especially Germany, spanning multiple eras and embracing DCC sound technology. A full range of locomotives and rolling stock is available.



HO 1:87 Scale C-Track

The Trix C-Track range is a robust, pre-ballasted track system that simply clips together giving a firm, reliable electrical contact between track sections - simply clip together and get your locomotives running.



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tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com www.gaugemaster.com/trix



Faller are world famous for their plastic building kits. With a huge range in different scales of Continental-style buildings, any layout will benefit from the inclusion of Faller kits. To compliment the buildings, Faller also produce a range of figures and accessories to enhance the atmosphere of any street scene, as well as trees and other scenic items to add to town and countryside.

CAR SYSTEM

The CAR SYSTEM range allows you to have moving vehicles on your layout with no visible tracks or wires. Automate junctions, traffic lights, and even speed cameras!



FAIRGROUND



The Faller Fairground range is produced in N and HO Scale, and is almost a separate hobby it itself! You can add sound and movement to your layout with interesting rides and attractions.

marklin

Marklin are one of the oldest model railway companies in the world.

They have over a century of expertise, and currently produce models in Z, HO, and Gauge 1 Scales.



Z 1:220 Scale -

One of the smallest commercially available model railway scales and ideal if you only have a small space, but don't want to compromise on detail.



HO 3-Rail 1:87 Scale -

Marklin produce a unique 3-Rail system in HO Scale, with locomotives and stock spanning the eras from the dawn of railways to the present day.



Gauge 1 1:32 Scale -

The Gauge 1 stock produced by Marklin is breathtakingly detailed. Extra realism is provided by MFX-Sound decoders and smoke generators in the steam engines.

Marklin Central Station 3

The CS3 is an innovative digital controller for both DC and AC layouts. It has a high resolution touchscreen display, and many possibilities for networking. It has two locomotive controllers and a central track diagram allows simple, easy control of locomotives and the layout.



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- Together, we can surmount -



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If you require any help or advice please call or email us and we will be more than happy to help.



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LINEAS, electric locomotive class 186, period VI

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N
1:160

SNCF Diesel railcar class X 73500


HJ2390

SNCF, diesel railcar class X 73500 in "Bretagne" livery, period VI

£ 189,50
H0
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FS Gran Confort Coaches 85/88


HR4274

FS, set of 4 Gran Confort Coach 85/88 series in XMPR livery with restaurant coach 87 series with ribbed roof with second logo type, period Vb

£ 161,99
H0
1:87

RENFE "Chartren" original train


E18043

RENFE, "Chartren" original train, 4-unit pack, 15000 coaches, period IV

£ 207,99
H0
1:87

April 2021
May 2021



Photo courtesy of GWR by Ron Westwater



Photo courtesy of LNER

N **GWR CLASS 800/0**
Scale 1/148 #10-1671 5 Car Set

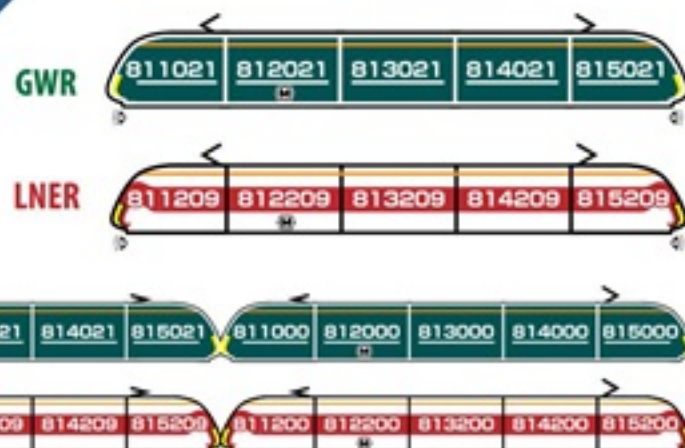
N **LNER CLASS 800/2 "AZUMA"**
Scale 1/148 #10-1674 5 Car Set

The Class 800 is a bi-mode multiple unit built by Hitachi for Great Western Railway and London North Eastern Railway. They primarily use electric power via pantographs but also have diesel engines for use on unelectrified routes.

3D Prints



Formations



Fitted with prototypical couplings within the 'nose' for operation in multiple

Class 800 Features Include:

- Running Numbers: 800 021 (GWR) & 800 209 (LNER)
- 1:148 Scale. Minimum Radius R282mm
- DCC Ready (Use Kato EM13 & FL12 Decoders)
- Directional LED Lighting with head/tail lights & marker lamps
- Driving Cars fitted with couplers to enable 10 car operation
- Flywheel Motor
- Prototypical Disc-Braked wheels with no traction tyres

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Distributor



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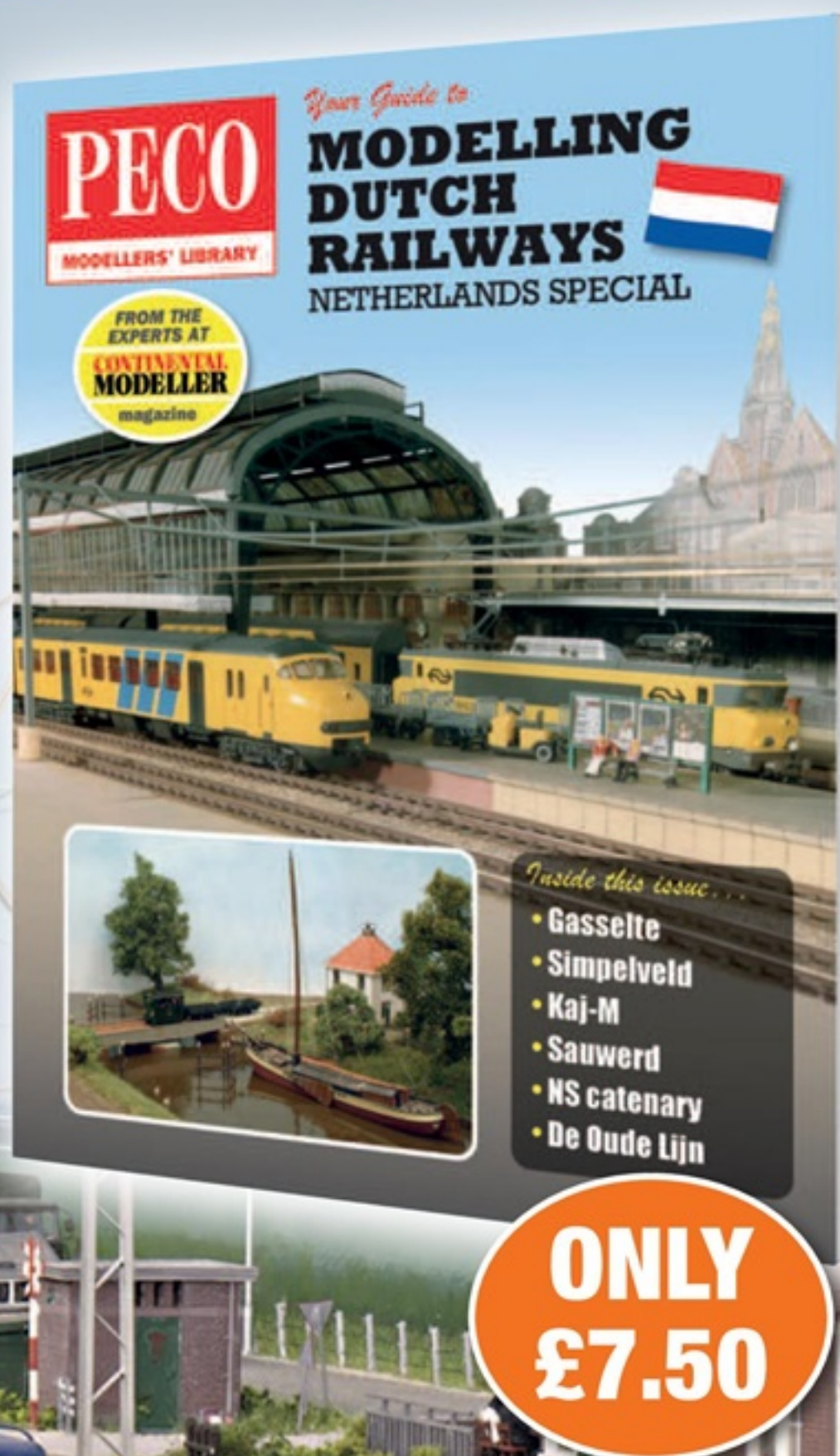
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Photo: Gerard Tombroek

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CONTINENTAL MODELLER

February 2021

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Number 2

From the Editor

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RAILWAY MODELLER

For all modelling British railway practice.
Published on the second Thursday
of the preceding month.



Cover

Two Berliet X3300 autorails
pause briefly at Pontempeyrat
on a cross-country line
high in the Haute-Loire.
The ballast wagons in the loop
indicate there is trackwork
in progress nearby.

Photo: editor.

From a Christmas cracker ... ?

Q : Why is the CM editorial like the Brexit deal talks?

A: Because both only get completed just before the deadline!

About as amusing as the joke in a typical Christmas cracker. It could just as well have been: "Why is the editorial *unlike* the Brexit deal?" "Because you can rely on there being an editorial ... "(even if it does not amount to much). Ah – another possible similarity!

At the time of writing – between Christmas and New Year, such are production lead times – at least there is the basis of an agreement, even if it is still to be formally ratified by the various national assemblies on both sides, and no doubt the details will keep civil servants occupied for some time to come. But it is certainly a relief that there will be no tariffs on goods moving in either direction, though there is likely to be more paperwork both for importers/exporters and carriers, so it is probable that we will see delays and some increase in prices, wherever the models are actually made.

Incidentally, we congratulate the Peco sales department who achieved an export award in 2020 in what have been challenging conditions. (See page 134.)

The virus situation has of course caused many delays to production and delivery schedules all over the world.

In the absence of the Nürnberg trade fair, it seems that most major companies will still announce their plans early in the year, as usual. The first programmes are just becoming available.

Even with vaccines in prospect, as so many places experience a serious second wave, it seems it will still be some time before we get back to any kind of 'normal'.

Many modellers have been using the enforced isolation to complete stalled projects. Among outstanding tasks may be repairs which need replacement parts, and spares are often not readily available as a result of modern 'just in time/only just enough' production methods. Spares are just that – the components left over when the specified quantity (or as many as possible) has been assembled. Those 'exploded' diagrams with lists of components are there not so much for the user to order replacements as for the producer to collect all they need for assembly; they are an incidental (if these days essential) part of the design and manufacturing process, not primarily for customer service, even if an order form is included, or there is an on-line facility which seems to accept an order – but nothing results.

Also, parts are often grouped under one reference so it is not possible to obtain just the component you need – this is reasonable for mouldings, which are formed together on sprues, but less so for items such as coupler springs or fixing bolts (which are quite likely to get lost). It should be easy enough not only to predict what might wear with reasonable use (traction tyres, brushes, or indeed complete motors in the case of sealed units) but also make provision to supply other vulnerable parts that might get broken or lost, even if this means ordering extras (from the sub-contractor if relevant) at the time of production. We recognise that the price would have to take into account not only the material but also storage and administration, and of course we appreciate that major manufacturers are not in business to supply bits to kit-bashers. But if a spares service is advertised, it should be efficiently run – even if that means sending honest "out of stock – delivery date unknown" responses. Some companies do well in this regard; others need to improve.

CM Cup competition 2020 – reminder

There should still be time to send your choices for the Cup competition before 31st January – we would like to know which six articles you found most inspiring. All articles are eligible. Full details and an entry form can be found in the January issue (p.67).

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Michel Gachet describes his French exhibition layout.

Pontempeyrat

Beautifully simple and simply beautiful

Prototype

The station of Pontempeyrat is at km46.4 on the cross-country line between Bonson and Sembadel, at an altitude of 814.14m above sea level in the Haute-Loire département, south-east of Clermont-Ferrand and west of Lyon. The line has to double back on itself to reach the plateau, and this requires a continuous series of gradients and reverse curves of just 300m radius.

The Paris Lyon Méditerranée sited a passing station with a standard class four building here because of the steep gradient – 1 in 40 (25mm/m) from the exit point.

The layout

In effect, the layout is a long scenic diorama through which pass typical trains. It could be the perfect formula for an exhibition layout, and has proved popular when it has been shown.

To scale, the site would have been 5.7m long, but the space available in my hobby room dictated that this had to be compressed to 3.6m.

The scenic section of the layout is built on three baseboards each 1.2m long with integral backscene and lighting valance.

Below

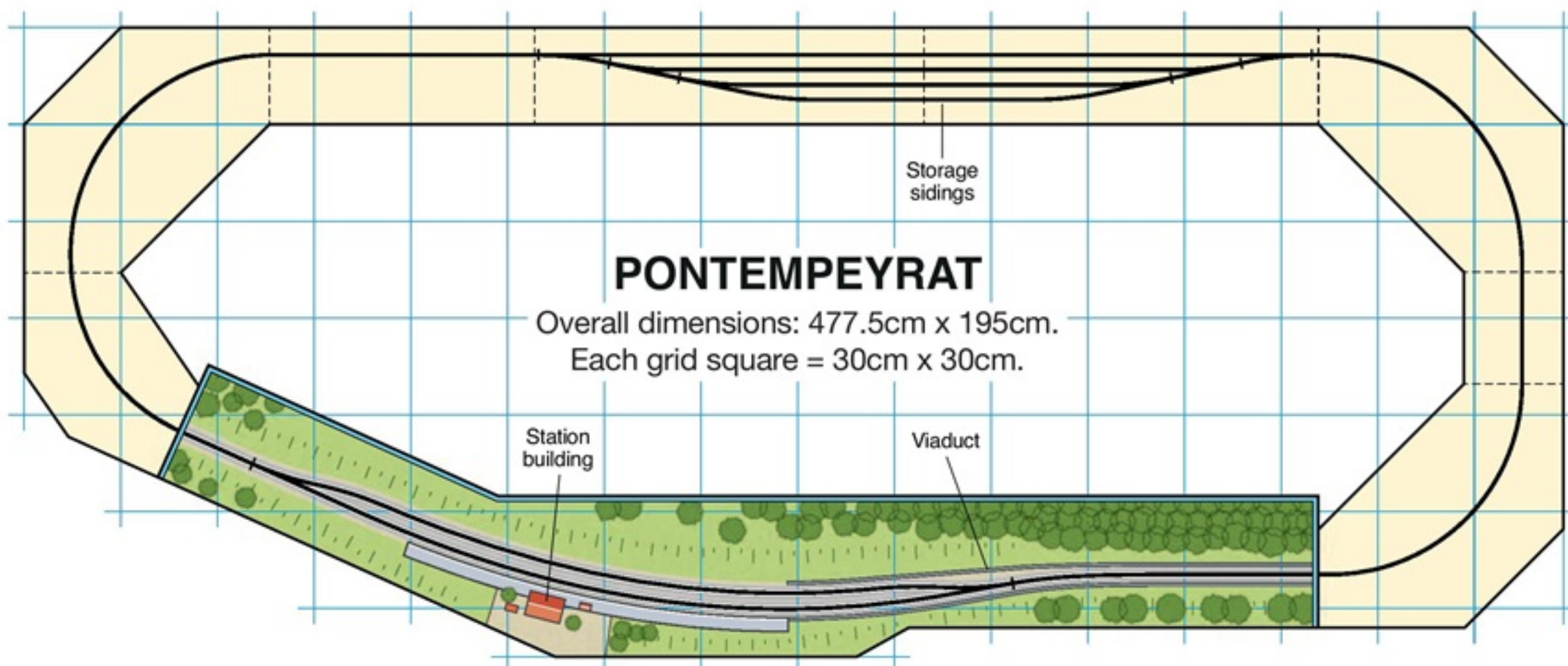
A pair of Berliet X3300 autorails pauses briefly at Pontempeyrat; no-one alights, and no-one boards. The station agent will soon be able to get back to his well-tended garden.

Photographs by the editor.



Right
An X2800 *autorail* stops at the station.
The public toilets are in a separate building,
as usual in France.

Below right
The station agent works the points.
The models have been detailed with authentic French features.



Curves at each end lead to a four-track storage yard. The construction is conventional, from 10mm plywood, with L-girders and cross beams.

The main line track is Tillig code 83, with Peco code 75 with metal sleepers for the loop.

The points were detailed to make them more authentically French, and have working point levers. They are operated by Conrad motors.

Control is analogue, using a Gaugemaster hand-held controller plugged in to the switch panel.





The station building was scratchbuilt from foamboard with a covering of plaster; the brick corners were added using Redutex structural foil, with window frames and doors from the Architecture de France range.

The small adjoining buildings were made from plasticard.

All the retaining walls and arches were scratchbuilt in plaster.



Below

With little local passenger traffic anticipated, a standard class 4 station building was deemed adequate.





There may not be many structures on the layout, but there are a lot of trees – in the second half of the 19th century they were widely planted in this region to provide timber for the mining industry. There are over a hundred, all hand-made, with a tapered wooden trunk into which holes are drilled to take finer branches onto which commercial foliage material is stuck. Gaps in the trees allow glimpses of the trains, which makes the layout seem larger. One area shows the stumps of recently felled trees.



Top
An X2800 *autorail*
arriving from Bonson.

Above left
More passengers must be expected on the line today as the *autorail*
has been replaced by coaches hauled by 040 DE 407.

Above
The Berliet *autorails*
depart toward Bonson.





Left and below left
The snowplough train
 returning to its depot.

Above
The 'reinforced composition'
 departs for Bonson.

Below
Berliet autorails in the forest.

The period is set in the 1950s to mid-1960s, which allows for a mixture of steam and diesel motive power.

Rolling stock in use includes a Roco X2800 *autorail*, a pair of Berliet X3300 *autorails* made from a kit by RPI (no longer available), and a BB63000 diesel (originally 040DE) (Roco) with four-wheeled coaches by LS Models, a so-called 'reinforced composition' deployed if an *autorail* is deemed insufficient or has failed. The snowplough train is powered

by a BB66000 diesel (Hornby Jouef) with scratchbuilt wagons. The ballast wagons were adapted from kits for British 16-ton mineral wagons from Parkside Dundas.

All rolling stock is realistically weathered with pigment powders.

As there is no opportunity for shunting, the stock is coupled with miniature screw couplings. The trains are assembled behind the scenes in the storage sidings.



David Cox returns to a familiar theme.

Photographs by the author.

Weidenstein



A small town in Bavaria

Most of the layouts I have built over the years seem to repeat the same pattern. The prototypes which inspired them were all secondary minor railways (indeed, half of them were narrow gauge) and they were all set in largely rural surroundings. Most have been termini, the design of which were fairly similar: about half of the scenic area had a representation of a small town in half relief along the rear of the baseboard, whilst the other half was more bucolic in nature with fields or woodland where the track departed off stage to the fiddle yard.

My current effort is no exception, being yet another branch line terminus, this time set somewhere in Bavaria between the mid-1950s and the early 1960s. Previous incarnations had been representations of Austria (HOe), the Eifel region of Germany (N) and in UK (OO). The latest version in HO has been christened *Weidenstein*.

Whether the design indicates a deep lack of imagination on my part or a determination to achieve the perfect combination of model railway, townscape, and scenery whilst pursuing the same theme over a lifetime, I have no idea!

Scratchbuilding

Given the vast range of plastic and laser-cut kits as well as the 'ready-to-place' cast resin buildings now available, the question may be asked as to why bother making your own models. I have always preferred to scratchbuild, largely because I usually cannot find exactly what I want. Also, many kits for HO seem to be to a smaller scale than 1:87 and some, particularly older plastic ones, do not go together very well. These considerations apart, making buildings does give a layout rather more individuality, apart from which I quite enjoy the process.

Nevertheless, I must admit I have been tempted by what the manufacturers produce and indeed, have yielded on a number of occasions in the past. Sometimes, rather than building a kit as was intended, I have modified it to look like something else, with varying degrees of success. Only one plastic kit will appear on *Weidenstein*, however, and that is part of the loco depot.

Inset

The unmetalled track at the end of the station looking towards the Gasthof zum Hirschen.

Above

The larger of the two barns alongside the gable end of a row of shops and houses. Note the trees disguising the fact that both buildings are low relief.

Right

Typical of Bavarian village architecture, this plain and simple structure is a bakery with living accommodation above. I have included a fairly basic window display with loaves and cakes made from DAS clay, but viewers can hardly see them. A small bush will hide the broken fencing.

Small towns in Bavaria

It has been many years since I have been to Germany; indeed, the last time I was there was before the internet became available to ordinary mortals. To plan *Weidenstein*, it was fairly easy to find thousands of prototypes which were worth modelling simply by sitting with my laptop in the living room and browsing through images of as many Bavarian villages and small towns as I could find.

A few of those I came across showed stations at or very close to the time they were opened. All those I found appeared to be in the middle of nowhere, surrounded by fields, with only a few buildings visible in the background – if any at all. Clearly, a lot of *Lokalbahn* stations were constructed at the edges of towns, presumably for the same reason many of their UK contemporaries were: land was cheaper and it was not necessary to knock any houses down. More recent images, however, often revealed housing and industry in the background. By and large, *Lokalbahnen* were built to encourage economic growth and where this happened, the towns they served expanded towards the railways with businesses developing alongside as well.

Below

Next to the bakery is the Gasthof zum Hirschen. The builders wanted to attract customers so it is a little more decorative than its neighbours. Despite being close to the station, it seems to have fallen on hard times and needs a repaint. The photo was taken early in the construction of the layout.





Above

The stationmaster's garden has come on very well, and a large tree has been placed to block the view down Bahnhofstraße.

Below

Two identical houses at the rear of the station, the prototypes of which can be found in Grafenwohr. They were modelled as if they had recently been renovated.



So what sort of buildings were to be found around the edges of small towns near railway stations? Housing and commercial premises seemed to be fairly plain, sturdy structures with little or no decoration. They were usually plastered, with timber framework and plain stone or brickwork little in evidence. All those plastic kits of attractive half timbered and/or elaborately decorated prototypes that feature heavily in tourist brochures of Germany seemed to be confined to the much older centres of towns and were rarely seen near stations. Doubtless there are exceptions, but so far I have not found any in Bavaria. In one respect, this made life easier: less ornate buildings are a lot easier to make and take up considerably less modelling time. Perversely, I think this adds to the realism of resulting scene. It has sometimes been remarked that what makes some models more appeal-

ing than others is the sheer ordinariness of what is depicted. Nevertheless, I have included a couple of buildings which are more decorative, but only in a fairly understated way!

The other noticeable thing about small town Bavaria these days was that although the buildings were largely plain, they were often brightly coloured and well maintained. There were few structures in a poor state of repair. But would this have been the case during my chosen period?

The few photos I found from the 1950s were almost all black & white, so little could be ascertained from them. However, the history of Germany from 1945 onwards suggested what small towns probably looked like.

Unless they had been fought over in the latter months of the war, most would have escaped the heavy bombing and shelling of the major cities, so there would have been little, if any, signs of damage. However, in the immediate aftermath of the War, times were very hard and it took several years for the German economy to recover. The *Wirtschaftswunder* or 'economic miracle' only really manifested itself from the mid to late 1950s. So, I felt it was reasonable to assume that a mixture of restored and moderately dilapidated buildings would be typical for the period I was modelling.

Using commercial parts

Whilst I like to base whatever I make on a prototype, I am not overly fussed about reproducing it exactly. Provided the result would not look out of place in a small town in Bavaria, then I will be satisfied. After all, Weidenstein does not exist, so I can include whatever structures I like provided they look appropriate.

There are a number of reasons I do not attempt to make accurate copies. Firstly, I often need to alter the dimensions so that the model will fit the space available.

Right
H.Lindner & Son's premises behind the station building. Lindner & Son claim to be agricultural suppliers, but their yard (on the right of the building) contains building materials (courtesy of the Auhagen packs which were free with CM one year) rather than anything agricultural. The prototype is at Olching, near Munich, and was one of a pair I thought were worthy of modelling. Now, I wish I had built the other one, a small industrial plant of some kind with tall factory chimney and a lucam (a structure in the roof that projects out from the building to allow a hoist to winch up sacks clear of the wall and give protection from the weather) which was rather more interesting. I may replace Lindner's with it sometime in the future.

Below right
A different view of Herr Lindner's premises.

Below
Two types of Auhagen windows as they come (top) and modified (below).



Secondly, without scale drawings, I could not reproduce a building exactly anyway and making a special trip to measure up my chosen prototypes might have been fun but was just not practical!

Finally, I now use or adapt commercial scale windows, either plastic or etched, from both UK and European manufacturers because they do not warp like scratchbuilt ones cut out of styrene or card. Inevitably, it is well nigh impossible to find exactly what is needed in the size required, so some minor adjustment of dimensions is necessary. Previously, I have used Dornaplas windows, but have also utilised any suitable ones discovered at exhibitions, even occasionally some intended for a different scale. I built a pair of houses based on some in Grafenwohr with N scale windows, but as they were supposed to be for use in industrial buildings, were larger than usual and pretty much the right size for HO housing. More recently, I came across the accessory packs produced by Auhagen as part of their modular range. A pack

of thirty industrial type windows (ref.80222) was purchased which I adapted in several different ways by cutting them down as required. One of the photos illustrates some of the permutations possible.

Given how free I am with prototype dimensions, experience shows that it is all too easy to end up with a model in which the proportions are completely askew. Consequently, I usually make drawings of what I am planning to scale to see if the overall proportions look realistic or if there are any obvious errors, such as doors being too small or windows too large. Checking with the dimensions of my own house helps with this.

Apart from windows, I often use what the trade has to offer for roofing materials as well. In days gone by, I tackled roofs by laying down strips of plastic or paper to get the effect of overlapping slates or tiles. Whatever adhesive I used to stick them down, they sometimes warped with the strips lifting in a most unrealistic way.





For the station building, toilet and wash house, and goods shed described in earlier issues of CM (July 2016 and October 2018), I used tile sheets produced by Noch which I think are made of ABS plastic. They were of a reasonable size (more or less the same as a sheet of A4 paper), so there was no need to butt sheets together and then try to disguise the join as there often is with similar UK manufactured products. They also cut easily and looked pretty realistic when painted and fitted in place. Alas, Noch ceased to produce them a while back, but a few structures in Weidenstein were roofed with the material before it became unavailable. However, the last time I visited my local model railway shop in Ipswich, I discovered an equally good British made product by a firm called Tasma. Their tile sheet is made of styrene, looks as good if not better than the old Noch stuff, and is significantly cheaper.

In order to introduce a little variety into Weidenstein, I also tried a Vollmer roof plate (ref.46026) which represented scalloped shaped tiles. Unfortunately, this had its problems. Cutting it with a scalpel or a Stanley knife usually resulted in the plate shattering. There has been some discussion on German model railway forums about the quality of the plastic in the ranges of building kits now produced by Viessmann which I wish I had read beforehand. After having had to discard a couple of damaged plates, I found a way of overcoming their brittleness. Instead of trying to cut them to size, I wrapped the plate tightly with masking tape as close to where the cut was to be made and then used a fine toothed saw very carefully.

Disguising low relief buildings

All of the non-railway buildings had, of necessity, to be low relief. A lot of German buildings were built with gables facing the street. Reproducing this in 1:87 creates problems, at least to my eyes. Looking at such models from any perspective other than square on exaggerates the fact that they are half relief. It is impossible to represent the missing part of the model convincingly by painting it on the backscene, and it is also impossible to disguise the sloping roofs with trees.

I only found this out the hard way, having completed what I thought was a rather interesting model of a small brewery which I had weathered really rather nicely! It was only when placed on the layout that the visual problems created by modelling only a few centimetres of the real thing were appreciated!

The only solution is, of course, to choose prototypes which could be represented 'side on', with the half relief gable ends mostly hidden by vegetation. Thus the town of Weidenstein is represented wholly by such buildings, with one exception.

That exception is the gable end of a house or shop located at the very end of the layout. It was on the opposite side of the street to almost all of the other town buildings and in theory at least, represented the end of a block of adjacent houses or shops. As such, it acted as a view block to the rest of the street leading from the station itself into the town and it therefore seemed logical reproduce the gable end. The rear of the building, i.e. the low relief side facing the front of the layout, could be masked by a tree or two whilst view blocks elsewhere (more trees) could hide the other side. The photos should make clear how this was achieved.

Construction

The methods employed to make buildings have been outlined in these pages on a number of occasions previously, so I will not repeat them in any detail here. Suffice to say that 1mm thick styrene sheet is used for all walls and floors, both internal and external. To prevent warping and to ensure joints, both vertical and horizontal, were at right angles, I used square section tubes, either plastic or brass, of the kind readily available in most model shops. Tube tends to be more accurately square than solid plastic or metal as well as being lighter, but I always check that it is so before using it.

The photos illustrate how the sides, ends, floors, and room dividers all go together. Liquid solvent, such as Mek Pak or Plastic Weld, is used to assemble the structure, with an impact adhesive, like Evostick or similar, being applied when gluing plastic and metal.

Above left

Looking up Bahnhofstraße to the town centre, which is represented by the backscene.

Apart from the mountains, which are heavily modified Faller, the images of the onion-domed church and the roofs of the rest of Weidenstein were downloaded from the internet. In front of these were pasted some of my own creations.

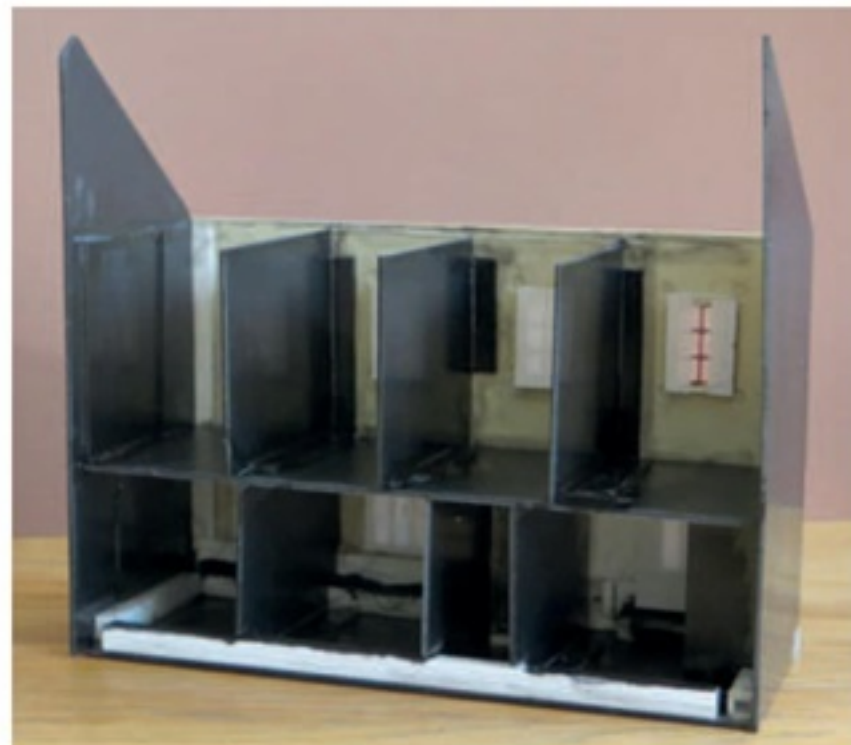
Above

This view shows the tree installed and how my artistic limitations have been disguised. Part can still be seen, so there is a hint that the rest of Weidenstein exists, but without the shortcomings being apparent.

Right
Rear of a low relief building during construction with the ground floor room dividers in place.



Far right
Rear of the building with the first floor base and room dividers added.



The barns

One of the interesting features of small towns in Bavaria (and probably elsewhere in Germany) is the intermingling of the normal kind of housing and shops one would expect to find alongside strictly agricultural structures, such as barns and even farms. Weidenstein includes two such buildings, both barns, but quite different.

The small barn is the other (slightly) ornate building on the layout. The bulk of it is of natural stone with a little half timbering above. I found the prototype on an estate agent's website, but unfortunately, I cannot remember where it was, other than it was somewhere in the Oberpfalz region! I chose it because it contrasted with the rest of the buildings, both town and station, but only in a restrained way!

The construction of the basic shell was straightforward, being the usual 1mm thick styrene sheet. The random stone is yet another Slater's product, but with this stuck to the exterior, the walls would have consisted of two layers. In order to avoid any distortion, an additional layer of plain styrene of the same thickness as the random stone (0.5mm) was added to the inside of the walls. The random stone itself required careful fitting as the corners and the edges of the

stonework on either side of the door were coursed. This was reproduced with individual stones cut from strips of plain styrene 0.5mm thick as the two types of stone were flush on the prototype. The strips were of various widths between 2mm and 5mm as the courses were different. Obviously, the random stone had to be cut out to accommodate the coursed stone.

Once complete, the stonework was painted using Humbrol enamels with a mix of light grey and dark earth, with individual stones picked out in different shades. When completely dry and hardened, the whole lot was given a wash of diluted matt white to represent the mortar.

The timberwork came next and was made from strips of 1mm thick styrene. Before fitting, these were pre-painted dark brown, again using Humbrol enamels. The plaster in between the timbering was filled in with DAS modelling clay. It was necessary to ensure this was laid as flat as possible and that the pre-painted styrene strips projected just a little. This was achieved using very small screwdrivers and water. The idea behind this was to avoid any bleeding of paint between the DAS and the timberwork. Once dry, the DAS was soaked in liquid solvent to ensure it stuck to the styrene shell.

Below
The part stone, part timber barn, the original of which came from the Oberpfalz region of Bavaria.





The larger barn is at the end of the station, behind the buffer stops. In fact, it's big enough to accommodate the odd cart or two and possibly also livestock. It is quite plain, but the upper storey is clad in timber, courtesy of Slater's embossed planking. I used the variety with 2mm wide planks, although 1mm or even 3mm could have been employed and still been to scale.

The Gasthof

Apart from track, locos, and rolling stock, the other essential which cannot be excluded from a layout is a pub! Quite what this says about modellers is something I occasionally wonder about.

Weidenstein is no exception and the *Gasthof zum Hirschen* lurks in a corner of the layout sufficiently near the station to be the first place passengers leaving the station encounter. It's also just out of sight of any DB official who might object

to staff and crews sinking the odd *stein* or two whilst on duty. When it was first built, the proprietors did specify that it should look a little more impressive than the more usual style of building. Whilst almost all of the structure was plastered over, the window and door surrounds were constructed of stone, as were the corners. Over the years, these were painted over, but were still distinctive even after the place began to look rather run down and seedy.

The decorative elements, such as they are, were taken from a building listed as being of historical interest by the Bavarian government. It was actually a house, not a Gasthof, but I lengthened it, added the name (The Deer Inn) along with a street sign. The door and window surrounds were from thin Evergreen strip about 1mm wide whilst the blocks at the corners came from Slater's embossed planking sheets, but cut so that the 'joins' between the planks were horizontal and looked like courses between stone blocks.

Above

By far the largest structure on the layout is the BayWa warehouse. Some more foliage needs to be added on the right-hand side of the main building.

Below left

The larger and plainer barn, beyond the buffer stops.

Below

The Gasthof zum Hirschen located on Bahnhofstraße has seen better days and needs some redecoration.





Above
The *Lagerhaus* from the
entrance to the goods yard.

The BayWa warehouse

I also felt it was essential to include some hint of how the local population earned its living. The town was situated in what was clearly primarily an agricultural area, so what was modelled on the layout should reflect that. That apart, I wanted a reason to add an extra siding to increase the shunting potential.

A fair number of branch lines in Bavaria had warehouses for agricultural produce, machinery, animal feed, etc., situated alongside stations and often with their own private sidings. The two largest organisations involved were the Bayerische Warenhandlung (or BayWa) and Raiffeisen, although there were others as well. I chose the former simply because the signage was easier to reproduce on a laptop. When did BayWa adopt the distinctive green style of signage? I hope it was before the end of the 1950s.

Faller produce a kit for the rather attractive half timbered structure at Stadtlauringen, which was typical of several BayWa warehouses found at other *Lokalbahnen* stations. However, it had clearly been shortened and moreover, was commonly seen on German layouts. The internet again provided me with a prototype from Falkenstein, which was to a different, rather less picturesque, design, but which nevertheless seemed likely to fit in with the rest of Weidenstein. The resulting model is the largest building on the layout, but construction was exactly the same as described previously.

Weidenstein

My tiny 1:87 bit of small town Bavaria is by no means perfect and I would not claim that the structures are anywhere near Pendon standards. However, they do show what can be achieved with simple tools, such as a ruler, a Stanley knife, scalpels, and a few files, and materials which can be bought at any good model shop. They are also different to what anyone else has done with their layouts and, perhaps most importantly, I have had a lot of fun constructing them.



Above
Two BayWa employees wait for the DB crew to shunt a van
into their siding so they can load the sacks on the platform.

Below
The van has arrived but the men
have yet to begin work ...



Kevin Smith describes another 'catch up during lockdown' project. Photographs by the author.

NS 8811

A Hunslet Austerity 0-6-0ST scratchbuilt in 1:32

Before going into the construction of my latest 'let's get it finished in lockdown' project, I thought I would give some background on the 'whys and whats'.

Many years ago, long before I went fully into Z, I used to exhibit two large Gauge 1 layouts. One was based on a typical British coal mine (*Mardy Colliery*) and the other (*Gottersee*) was a Bavarian branch line based on Frasdorf. As the show team got older and we found ourselves living further apart geographically, it became apparent that lugging 10m long heavy layouts about was getting too much so they were both retired and sold.

The colliery used mainly small British industrial tank locomotives and here the saga of the Austerity tanks begins. The design was a development of a standard Hunslet 550050 class modified and simplified to meet the requirements of the War Department commission for a loco that could be built in large numbers relatively easily for use in the UK and Europe after liberation to replace the thousands of locos lost in the conflict.

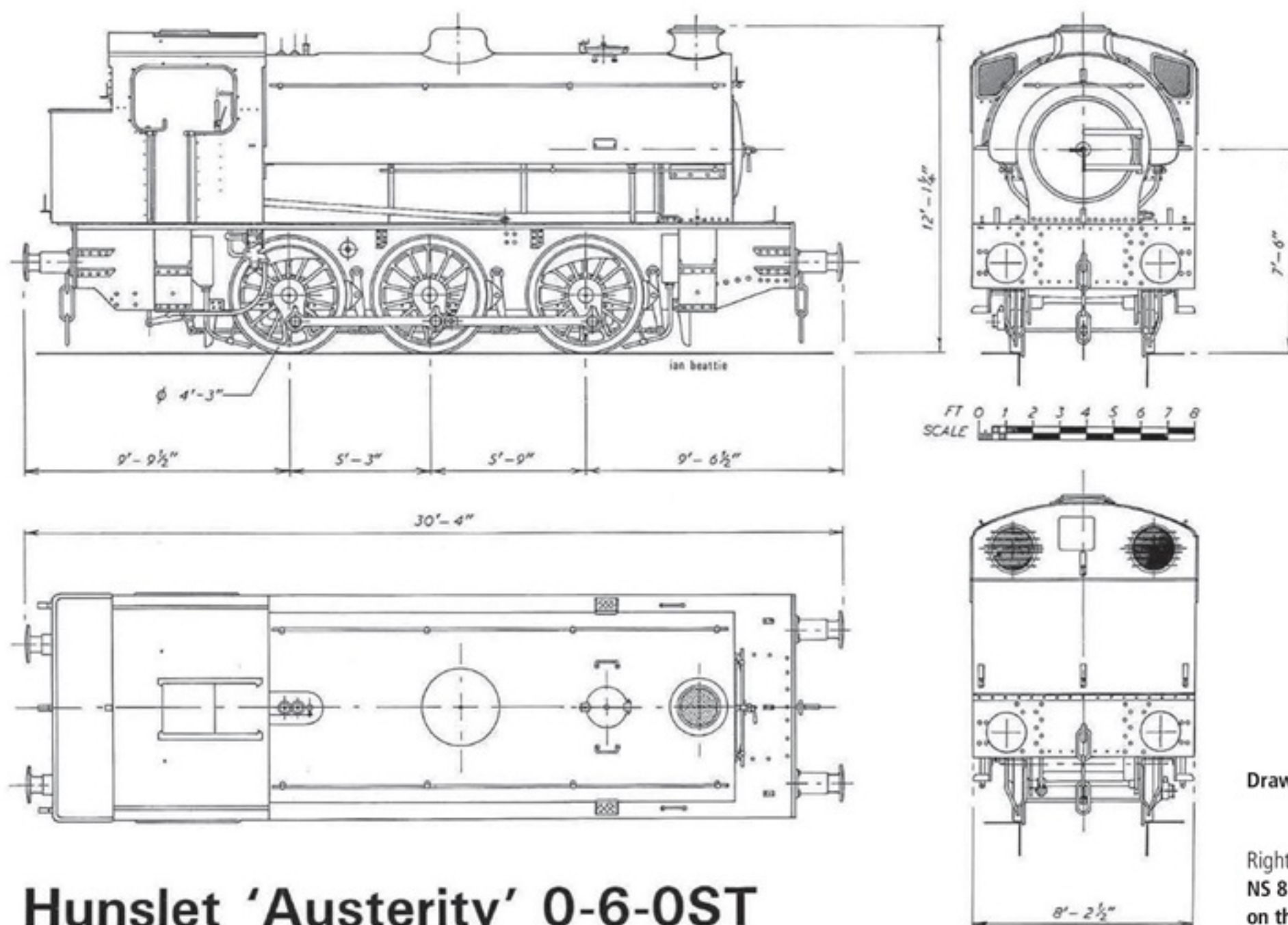
No less than 485 were eventually built by Hunslet and other manufacturers, and so rugged, reliable, and powerful was the design that construction carried on until 1964!

Above
The finished model.

Top right
NS 8826 at the colliery at Laura-en-Vereeniging, 1970.

Above right
NS 8825 at Feyernord, c. 1956.

Right
NS 8811 in September 1970 at Laura-en-Vereeniging colliery.



Hunslet 'Austerity' 0-6-0ST

Drawing reproduced to 1:87.

Right
NS 8811 at Simpelveld on the Zuid Limburgse Stoomtrein Maatschappij.

I started with the 550050. Construction was a nickel-silver superstructure on a ground flat stock (gauge plate) steel chassis with wheel castings by Locosteam; power was a Bühler motor mounted vertically in the firebox. All axles were sprung, and made from 1/4" silver steel. The wheels were turned to 4' foot diameter with the spokes insulated on one side, and I turned the safety valves, whistles, and buffer heads at the same time. The loco had the characteristic deep buffer beams and sloping back to the bunker. It was finished in an industrial livery and named *Topham*.



The next one was 68011 – a standard model that went into British Railways stock after the war, and was eventually preserved on the Kent & East Sussex Railway. I used the Ian Beattie drawing from an old *RAILWAY MODELLER* (January 1986). Construction was the same as *Topham* but the wheels were 4'3" diameter. It had working inside valve gear and was a very powerful, smooth runner. It became the loco of choice to bring the empties up the incline into the colliery yard on the layout as the four-wheelers usually needed a banking engine to get up the hill.

Lurking in the drawer were two more sets of driving wheel castings. A friend wanted an Austerity that had the unique cut-down cab of the examples that ran on the Lambton, Hetton and Joicey Colliery system in the north-east coalfields, and so I started two more models, the fourth being another

standard example. This time the superstructure was brass but in all other respects the design was as before. I got the Lambton cab one finished after a major fight with the shape of the roof.

The last one fell off the radar and was dispatched to the loft in 2000. Every now and then it was taken out and dusted off, but my heart really was not in it, so it has languished for quite a while. Now I am working from home due to the lockdown and there no show deadlines for the Z layouts it seemed the obvious next one to finish after the recently completed Thai railways C56 2-6-0. (See CM October 2020.)

The first thing to decide was what to finish it as. Another British one did not appeal to me but then I remembered that twenty-seven had been sold to the Netherlands Railways after the war.

Several of the Dutch ones survived in traffic until the early 1970s, principally at the Laura and Vereeniging colliery on the Dutch – German border. Substantially unaltered, no fewer than three of these have survived into preservation to join the 67 other examples that have survived elsewhere. That is right, there are 70 Austerities still surviving today and the design has been a mainstay of preserved lines in the UK for decades, being powerful and easy to maintain.

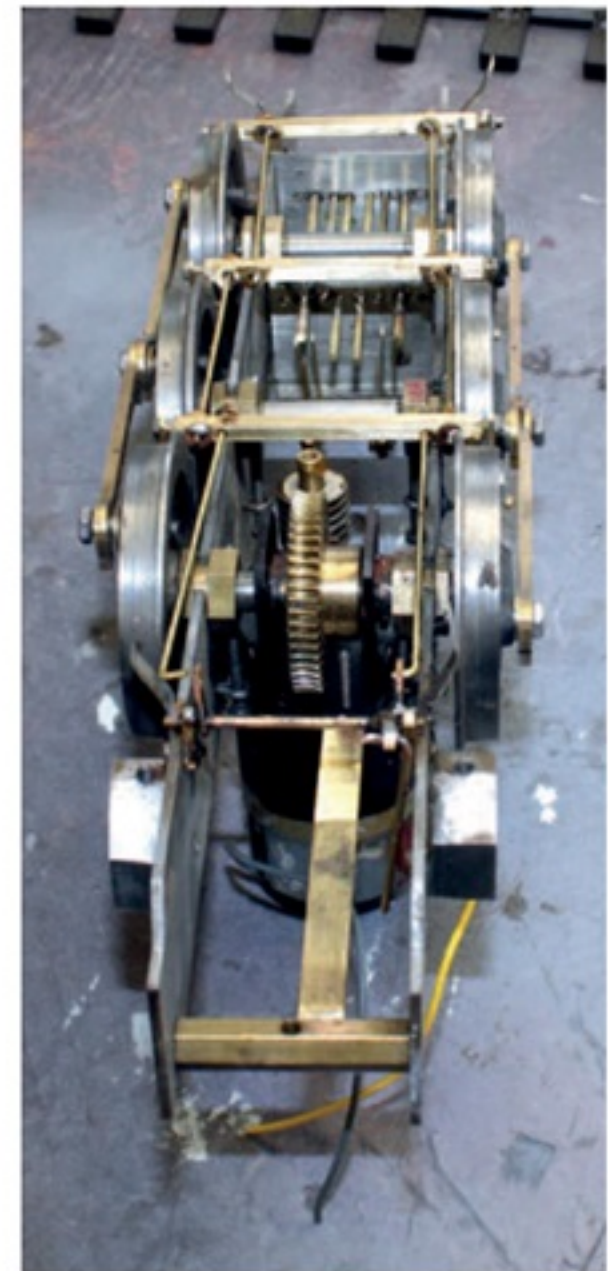
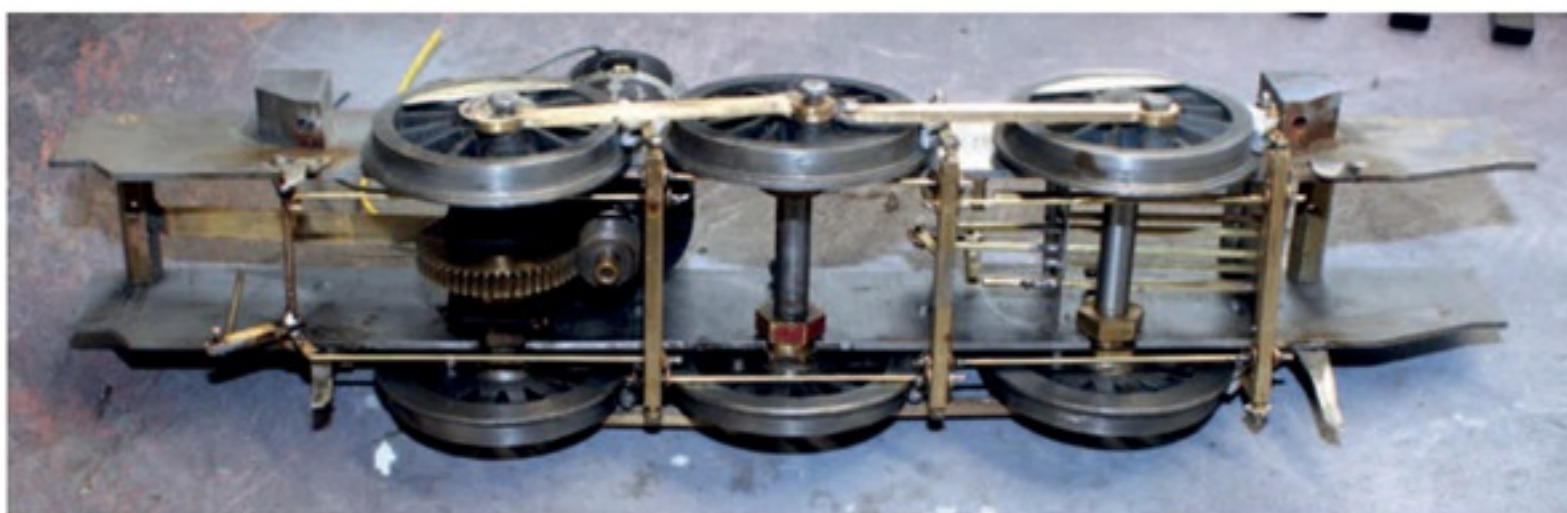
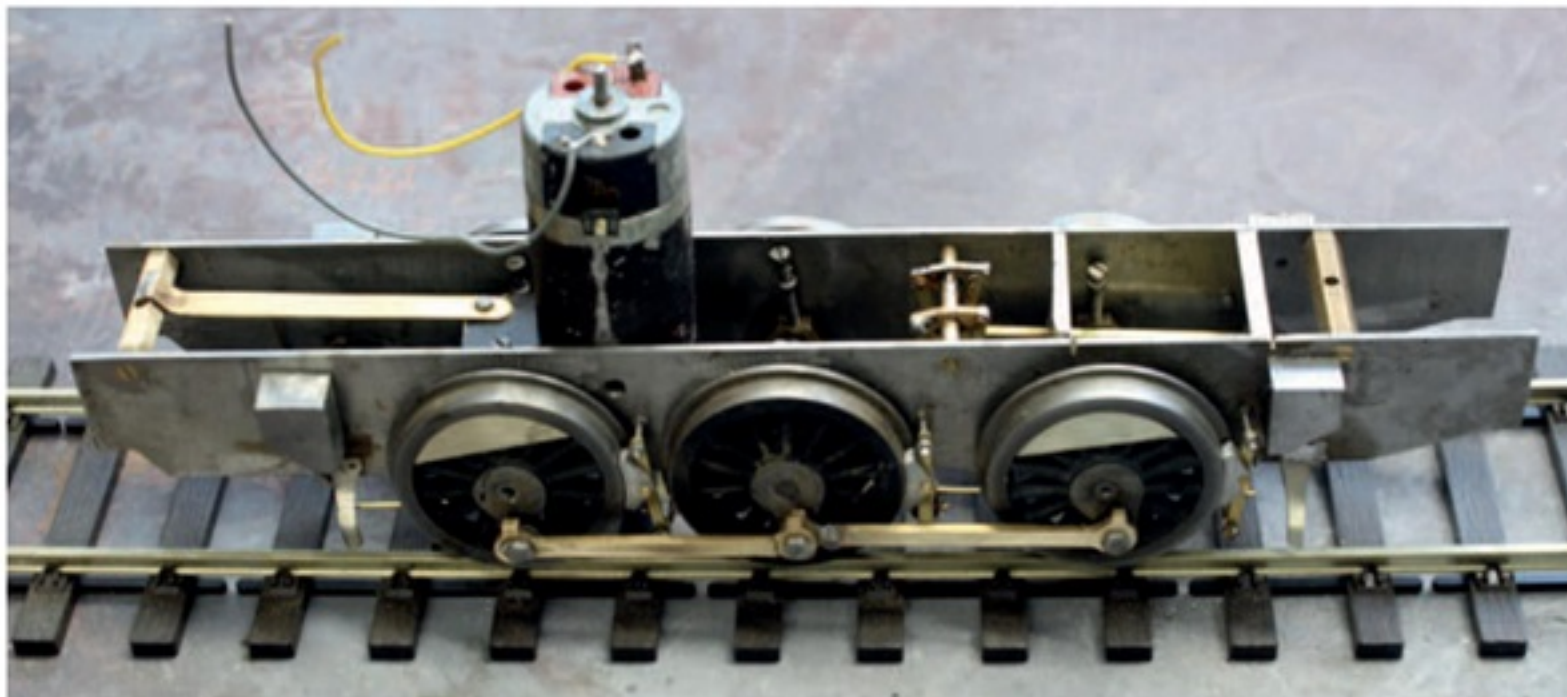
The existing Dutch ones are:

NS 8811 – a Hudswell,Clarke-built example (HC 1737 of 1943) returned to steam in 2010 at the Stoomstichting Nederland museum in Rotterdam.

Hunslet-built NS 8815 (HE 3315 of 1943) has made the move back across the North Sea and is now *Walkden* on the Ribble Steam Railway in Preston.

NS 8826 (HE 3165 of 1944) is preserved on the Zuid-Limburgse Stoomtrein Maatschappij (ZLSM) based at Simpelveld.





I decided to do NS 8811. It is not an exact replica as the original has been modified in preservation a couple of times but it should be fairly close, finished in WD green with the characteristic continental lamps.

Laying all the bits out it, was apparent it was not far off being finished. It had donated one wheelset to the Lambton one due to a quartering issue, and the side rods needed tweaking. Also, I could not find the firebox backhead anywhere so would have to make another.

A start was made by making the torque arm that holds the motor upright but allows the back axle to move up and down on the springs and also the inside valve gear. 68011 had full working inside valve gear but on 8811 it will be static, just there to fill the big empty void between the frames. The chassis, with the middle wheelset re-engineered, was then run up and down with the gear wheel disengaged to check for tight spots. One crankpin hole needed easing slightly and it was fine.

The brake rigging was constructed from brass bar and rod and added. This drops out so I can push the brake hangers forward to clear the wheelsets so they could be dropped out for painting. Brake hangers and blocks are from Tenmille.

The sandboxes are solid lumps of steel shaped to the correct profile. The sand pipes were added right at the very end.

The injectors are real pigs to make and involve a bit of finger burning but they are very prominent on the real thing so you cannot really miss them off. The mechanical lubricator was a lost wax casting.

After a good clean up, the chassis and wheelsets were primed using red acrylic primer. The driven axle and motor just had the working bits masked off.

The cab and footplate needed the additional steps and

grab rails adding to the bunker, and a lot of dirt had accumulated on them, particularly in the bunker and the inside corners of the cab. They were grit blasted (with a full size spot blaster, not a model one) to get it cleaned up. The cab roof is held on by 12BA countersunk screws and is easily removable.

The brake rigging, rods, backhead, reversing stand, and cab roof, being mainly brass, were cleaned up and then dunked in white vinegar for about 30 minutes to etch them. They were then rinsed and dried off and the sprayed with Hycote aerosol etch primer, which I discovered recently. It is not cheap but it sure seems to work.

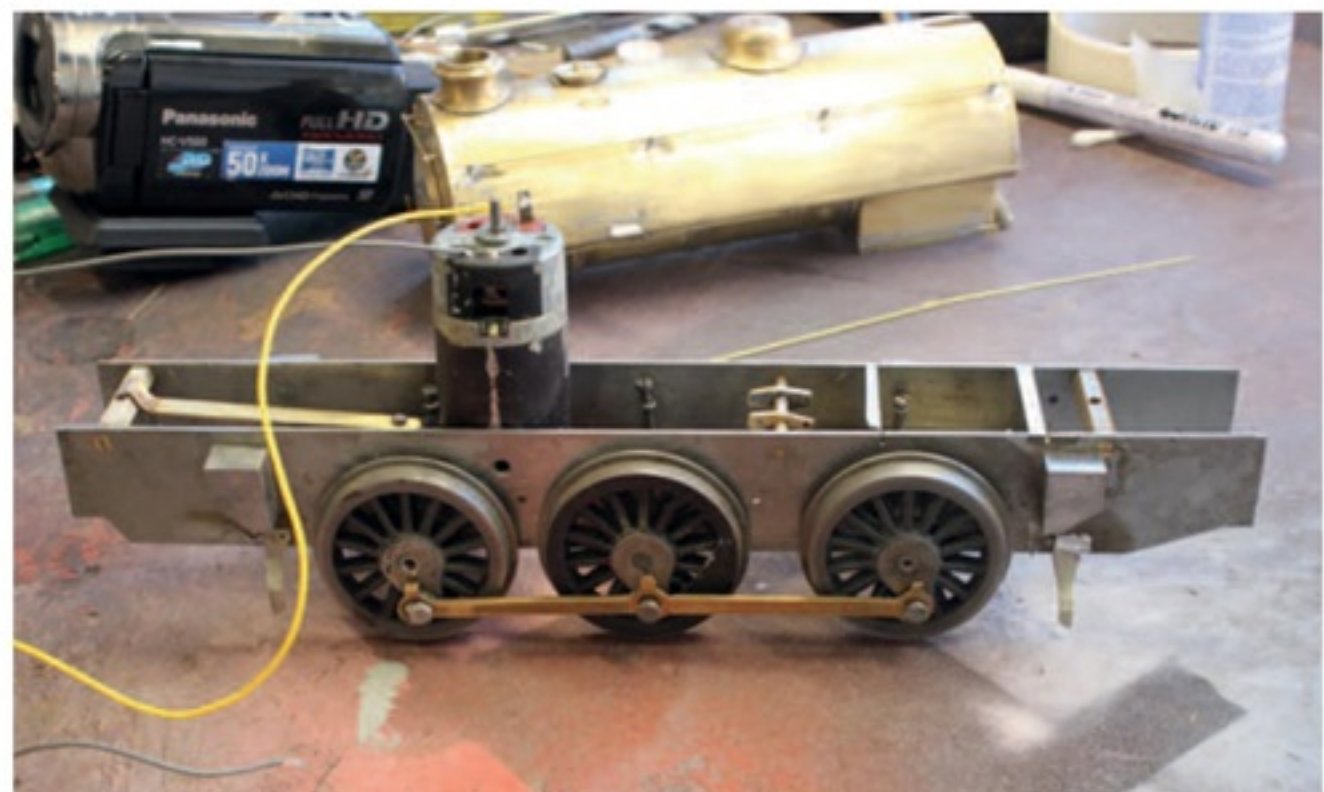
The backhead had the bits I wanted to keep polished metal masked with Maskol fluid which can be peeled off to reveal the bare metal once the black top coat is on.

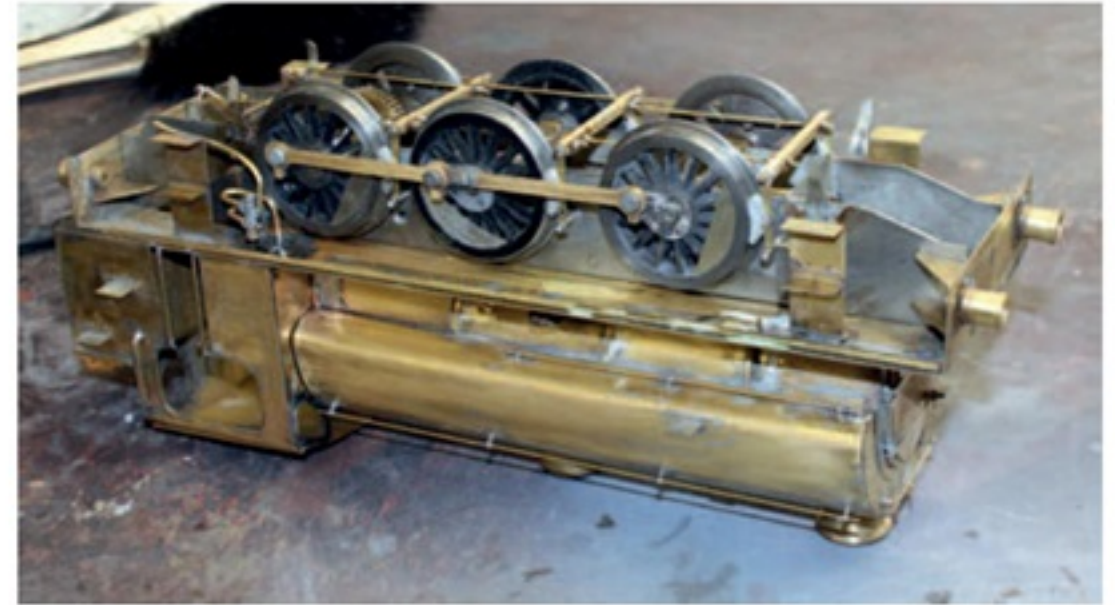
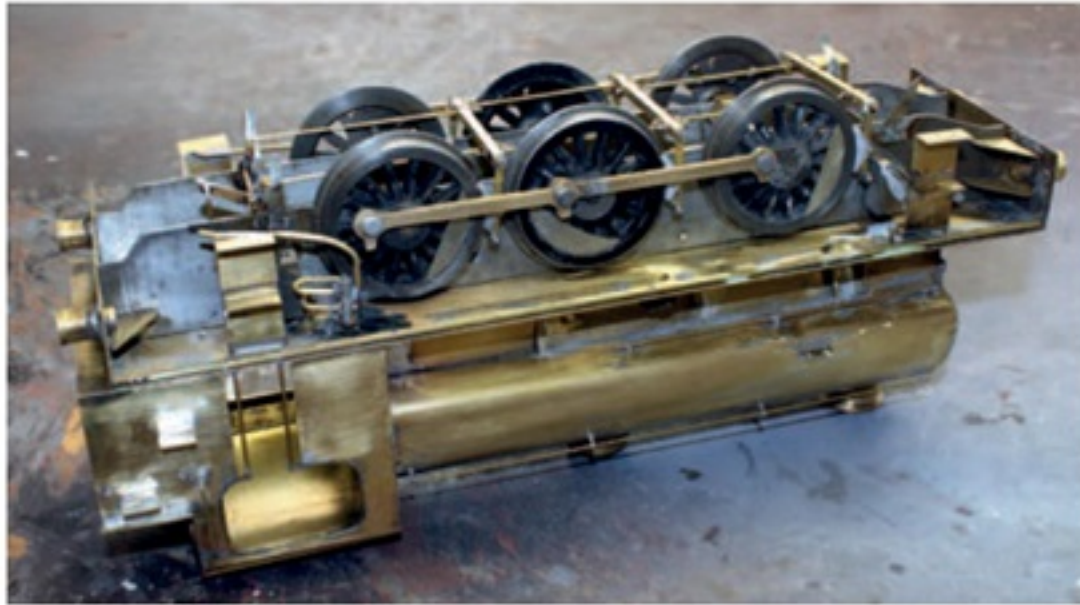
Above (3)

The chassis fully assembled for test running. On this model the inside motion is cosmetic, but in previous builds I have made it working.

Below

The Bühler motor (which is concealed in the firebox) and the torque arm.





Above (2)
The brake gear in place.



The boiler/saddle tank was also dunked in white vinegar to etch it even more. I agitated it every ten minutes for about forty minutes so the inside of the boiler got a good clean as well as the inside of the chimney.

I made a start on painting the inside of the frames and dummy valve gear in Tamiya gloss red.

Once the black had cured, I popped the wheelsets back in the lathe and polished the paint off the treads and flanges. The crankpins were polished up and the coupling rods painted burgundy.

The suspension coil springs and axle retaining screws were re-fitted and the brake rigging put back on.

The pickups are phosphor-bronze strips soldered to small pieces of copper-clad strip bonded to the inside of the frame on the insulated side and bearing on the back of the wheel rims.

The saddle tank, cab, and footplate were etched primed and left to cure. I had bought some Hycote 'British Racing Green' paint which looked to be very useful as 'Brunswick Green' and I thought I would do a test shot to see how it looked. Aaargh – it was metallic! I should have read the label. Fortunately I had only misted one side of the tank so I let it dry while I rooted through the paint cupboard to see what else I had got in stock. I found some Hycote 'Rover Brooklands Green' which actually looked right.

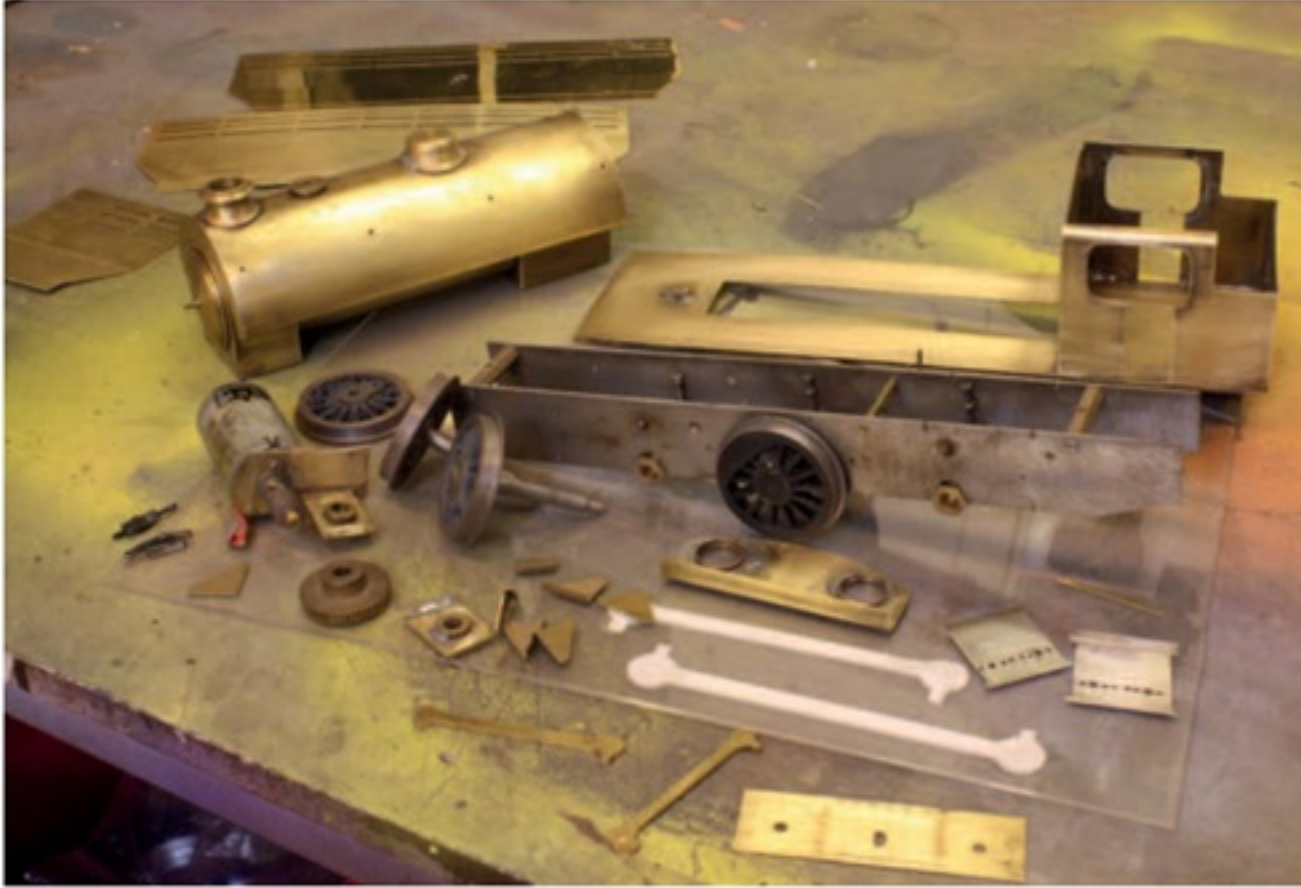
The cab roof and footplate were sprayed Hycote gloss black and the other black areas were brush painted with Tamiya gloss black.

Right
The cab and bunker fixed in place on the footplate. Although the body parts will be green and the footplate black, they are easy enough to mask but is definitely worth keeping the boiler and saddle tank unit separate for spray painting.

Above left
The boiler and saddle tank assembly being etch primed in white vinegar.

Above (2)
The boiler/saddle tank assembly clean and ready for painting
Below
The coupling rods, brake gear, backhead, reverser, and cab roof etch primed and ready to be painted black.

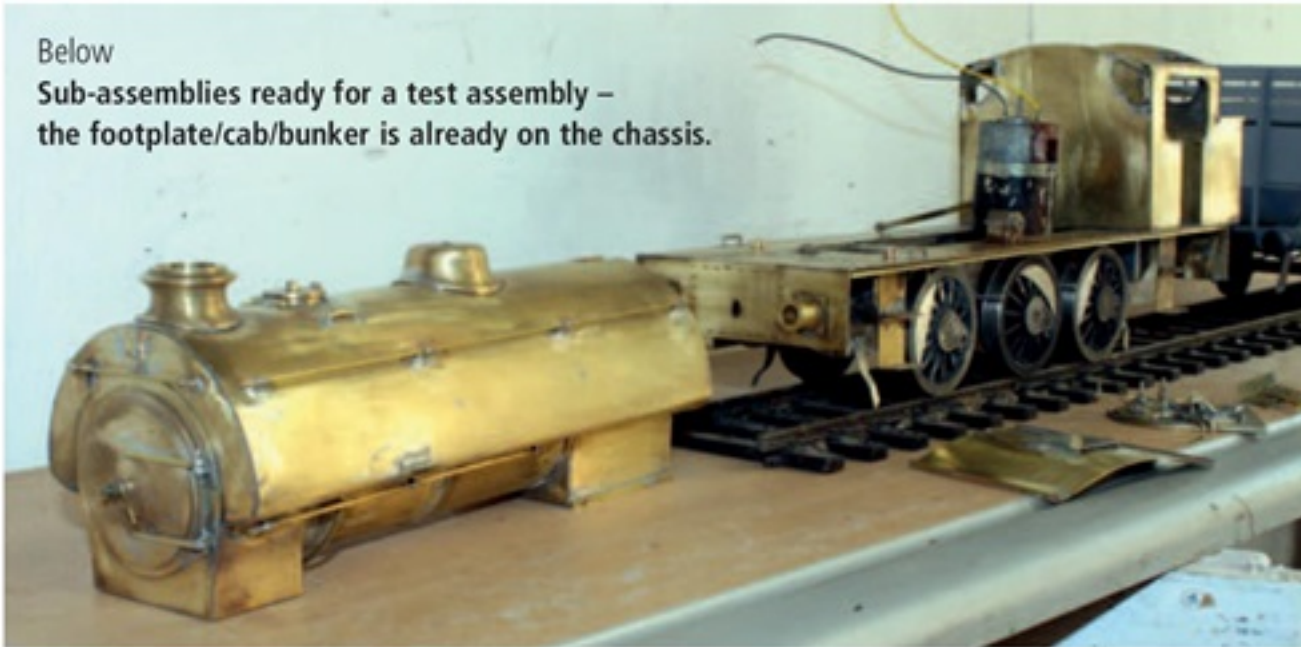




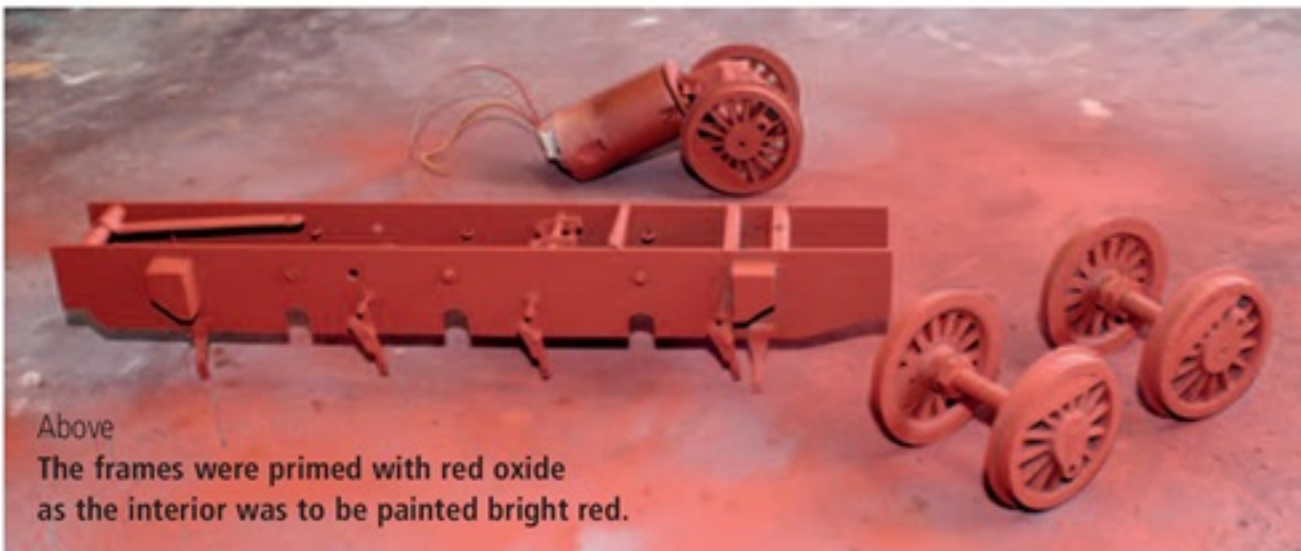
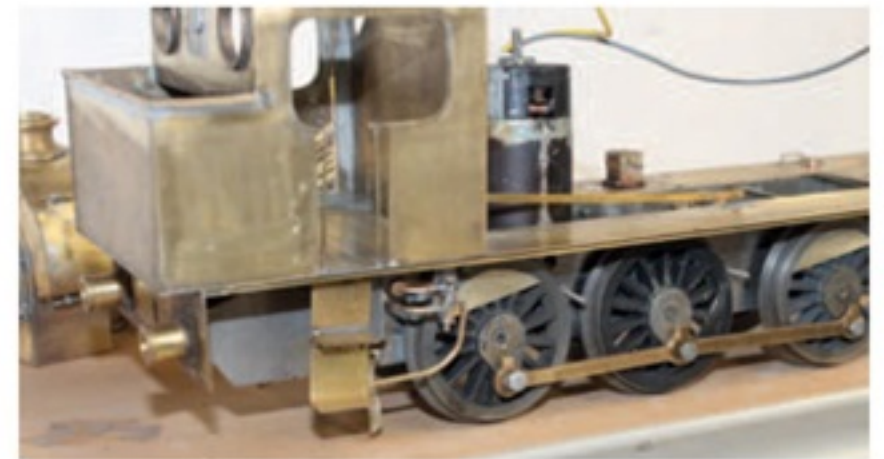
Left
All the parts ready for painting.

Above
The cab roof, backhead, and reverser were kept separate to make it easier to paint them black.

Below
Sub-assemblies ready for a test assembly – the footplate/cab/bunker is already on the chassis.



Below
Detail of the injector under the right-hand side of the cab.



Above
The frames were primed with red oxide as the interior was to be painted bright red.

There is a bolt that comes out of the back of the boiler through the front of the cab and then through the backhead. A nut on the inside of the cab pulls the three together. A similar one comes out of the bottom of the smokebox through the footplate. The chassis is held to the body with 6BA screws that engage with captive nuts on the top of the footplate.

The sprung buffers were polished up and fitted, as were Tenmille screw couplings. These are lost wax nickel-silver components which you have to assemble yourself and add to the laser-cut steel drawhooks.

The fancy lamps were modified from some resin cast

Below
The black parts have been painted (and the masking removed to reveal the bare metal) and the body components are in grey primer.





Left
An ornate lantern as carried
by Dutch steam locomotives
and my replica in 1:32.

Right
In this scale the cab needs
to be detailed – and crewed.
Just the coal to add!



Below
The inside of the frames
was painted bright red.
The valve gear is cosmetic.



ones I made when I was scratchbuilding German locos for Gottersee. With a bit of modification they look pretty close to the Dutch ones.

Looking through the decal stock I did not have any waterslide transfer close to the Dutch markings, so I reverted to a technique I used to use a lot in the old days. I had the right size in rub-down Letraset (remember that?) which I applied to Microscale clear waterslide sheet and then sprayed with clear acrylic lacquer to seal it so the characters can be cut out and applied like a normal decal and thus be positioned accurately on the model.

The assembled superstructure was then given two coats of Hycote matt lacquer.

After all the painting and lettering was done, the safety valves and whistle were added and the cab windows glazed. The crew are German figures repainted to match the Dutch uniforms.

The only thing still to do is to get some Hunslet plates etched to finish it off.



Roderik Vanderkelen describes his entry for the *Modelspoor* micro layout competition.

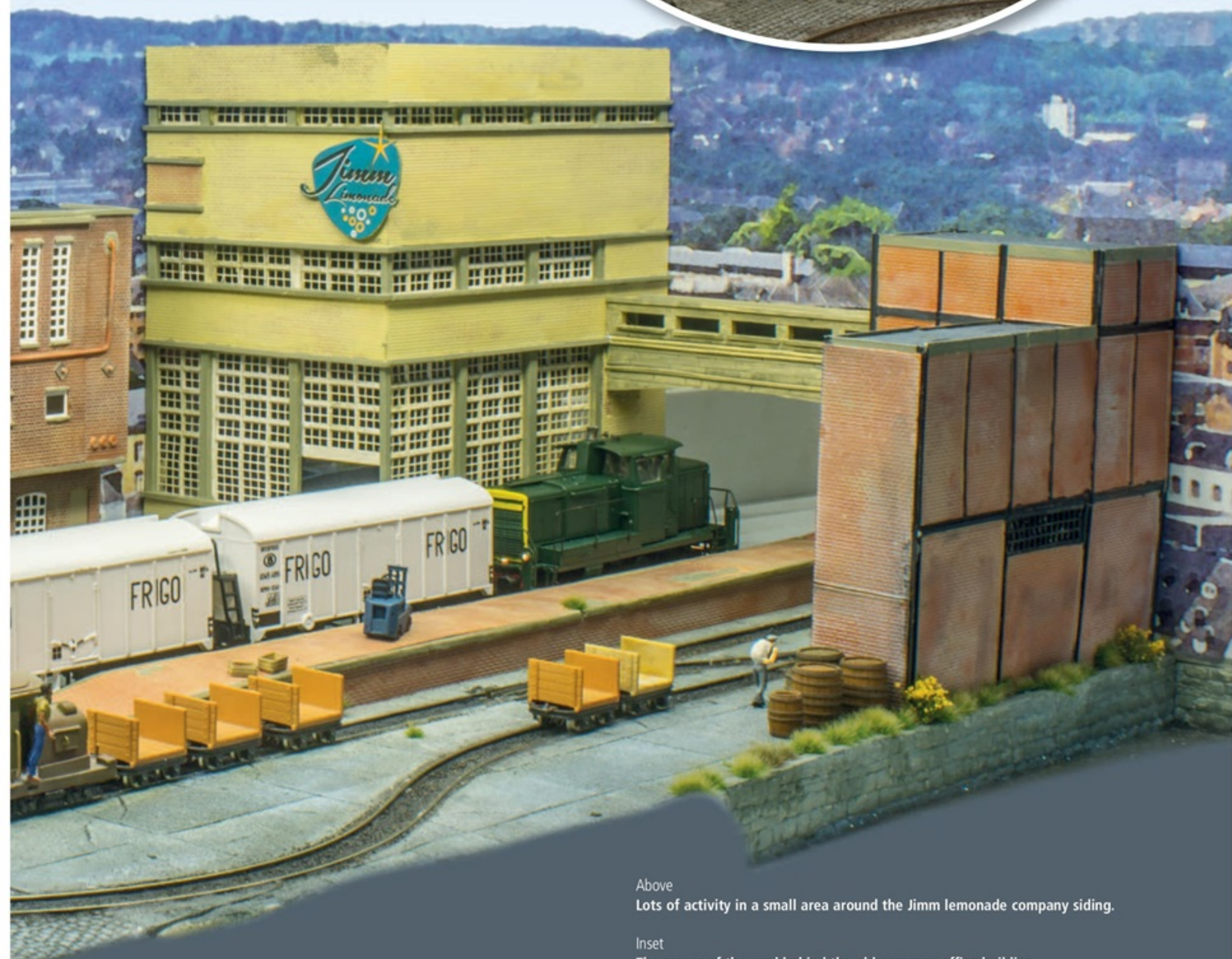
Jimm Lemonade

A factory with internal narrow gauge and a standard gauge siding



To make this micro layout as an entry for the *Modelspoor* competition, I was inspired by the track plan of an English layout, *The Loop*, by Giles Favell. He had modelled a quarry scene in O that had a central piece of standard gauge track and further loop of narrow gauge around it. I liked the idea, but it took a long time to think of a Belgian theme; eventually I thought it might suit a brewery, but because I am not a beer drinker, I opted for a lemonade factory.

The name Jimm is based on the beverage manufacturer 'Jim' which made different types of lemonade in Ninove. Older residents of the town probably remember a 'Jimmeken', which was the local name for the rather pale yellow-green coloured lime flavour variety, which was actually not too bubbly.



Above

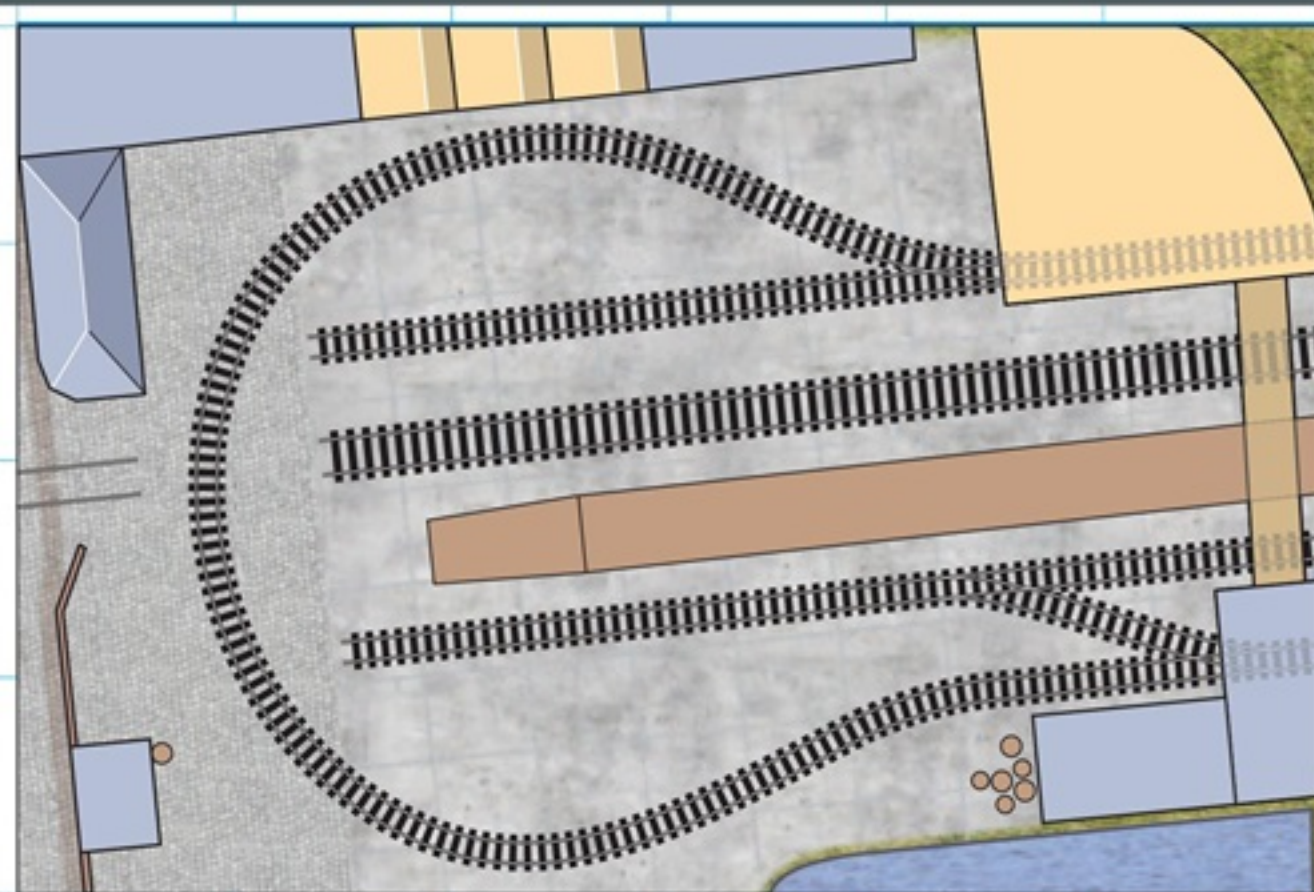
Lots of activity in a small area around the Jimm lemonade company siding.

Inset

The corner of the yard behind the old company office building.

Photographs by the editor.

Above
The narrow gauge forms a tight loop around the end of the loading dock.



Jimm Overall layout size 600mm x 400mm. Each grid square = 100mm x 100mm.

Jimm Limonade is a fictional soft drink producer somewhere in Flanders which uses a nearby connection with the SNCB/NMBS to transport the lemonade away. In the past, the siding ran right across the yard and even further, crossing the street to more companies. Now only the remnants can be seen in the road.

The track plan seemed well suited to a very limited area, just 60cm x 40cm, as specified in the competition rules, with a few nice challenges, such as very tight curves, and a credible 'exit' for the standard gauge locomotives.

During the 1950s and 1960s, soft drinks really took off, hence the choice of the 1960s, when these factories were in their peak years.

Despite the very limited area, a continuous run (on the narrow gauge) is possible.

Construction

The construction is very straightforward. The base is made of 9mm lightweight plywood with a cork layer on top. The buildings are plasticard with a brick surface. The windows were cut with a Silhouette Cameo. The buildings had to be very realistic, so I collected a lot of photos of all kinds of breweries from all over Belgium – because they are easier to find than lemonade factories. Buildings from breweries such as Roman, Rodenbach, Mort Subite, and Slaghmuylder were used. The bridge under which the standard gauge runs to leave the scene was inspired by that at Cristal Alken.

At the front right is a river.

The background is a photo found on the internet.

The simulated concrete slabs were cut from architect's cardboard, one side of which was peeled off. Then they were broken in specific places, as often happens with real concrete. Different aerosols were used for the concrete colour. By holding the aerosol at about a meter from the cardboard, larger paint particles are created that represent the characteristic stones in the rough, unpolished concrete structure.

The track is from Peco and Tillig.

The lighting is by LED strips.

Operation

The rolling stock consists of a pair of narrow gauge locomotives, one of which was transferred from my *Janakpur* layout (see CM January 2012, itself a previous entry in this competition). This was scratchbuilt from plasticard. A second machine came from Jelly Models, an Italian manufacturer of very nice narrow gauge locomotives. It is a Badoni, and consists of a combination of moulded plastic, 3D-printed, and whitmetal parts.

Smaller SNCB/NMBS shunting locomotives were chosen for the standard gauge. For the exhibition Geert Descamps adapted a type 260, making it correct for the period with a complete repaint and some headlights. During the exhibition, a type 230 from Van Biervliet was acquired.

Only four refrigerated vans are used, regularly exchanged in the off-stage sidings. Just like the locomotives, they do not have couplings because they are only ever pushed.

At shows, I work with a sequence that displays as much of the rolling stock as possible and also ensures the most movement. The simplest scenario is for the 230 to run up the yard and wait for the 260 to propel two empty wagons in and leave them behind. The 260 disappears under the bridge and after a short pause the 230 pushes the supposedly full vans out again, after which everything starts again.

In the meantime, one of the narrow gauge locomotives keeps running on the circuit around the freight yard while the second locomotive shunts some wagons back and forth on the front tracks.

The track is digitally controlled with the Lenz system. Decoders come from Zimo and ESU, all with additional 'stay-alive' power packs or self-built capacitor packs.

The turnouts are operated with Conrad motors, by means of a few buttons on the left of the layout.

All the digital control and electrical equipment is built in under the layout. Only one plug needs to be connected for the control system and one for the LED lighting.

Below

The road entrance to the works. The former rail connection can still be seen between the paving stones.



Finally

Jimm Lemonade was only finished just before the exhibition. A few intense days, evenings, and even nights ensured that everything was 'presentable'. Despite many hours of work, the narrow gauge section did not run properly, and one building even lacked a roof, but few comments were made about that.

All buildings will eventually have interiors and many more details. The entrance gate is still needed, and two large water tanks will be placed on top of the large main building at the back, filled with water from the underground well.

Since the show, I have added an overhead line in the yard and two electric narrow gauge locomotives have been built – after a comment from an alert exhibition visitor who pointed out to me that it is not such a good idea to run diesel locomotives where foodstuffs are made.

Even a layout of this modest size can be quite a challenge and it taught me a lot about how to be creative with a track plan and the necessary scenery. That is what made building it so interesting. But what I am most satisfied with is the atmosphere and tranquillity that it radiates despite the small space.



Giles Barnabe has a novel suggestion for a layout – a beach tramway in western France.

Photographs by the author.

Île d'Oléron

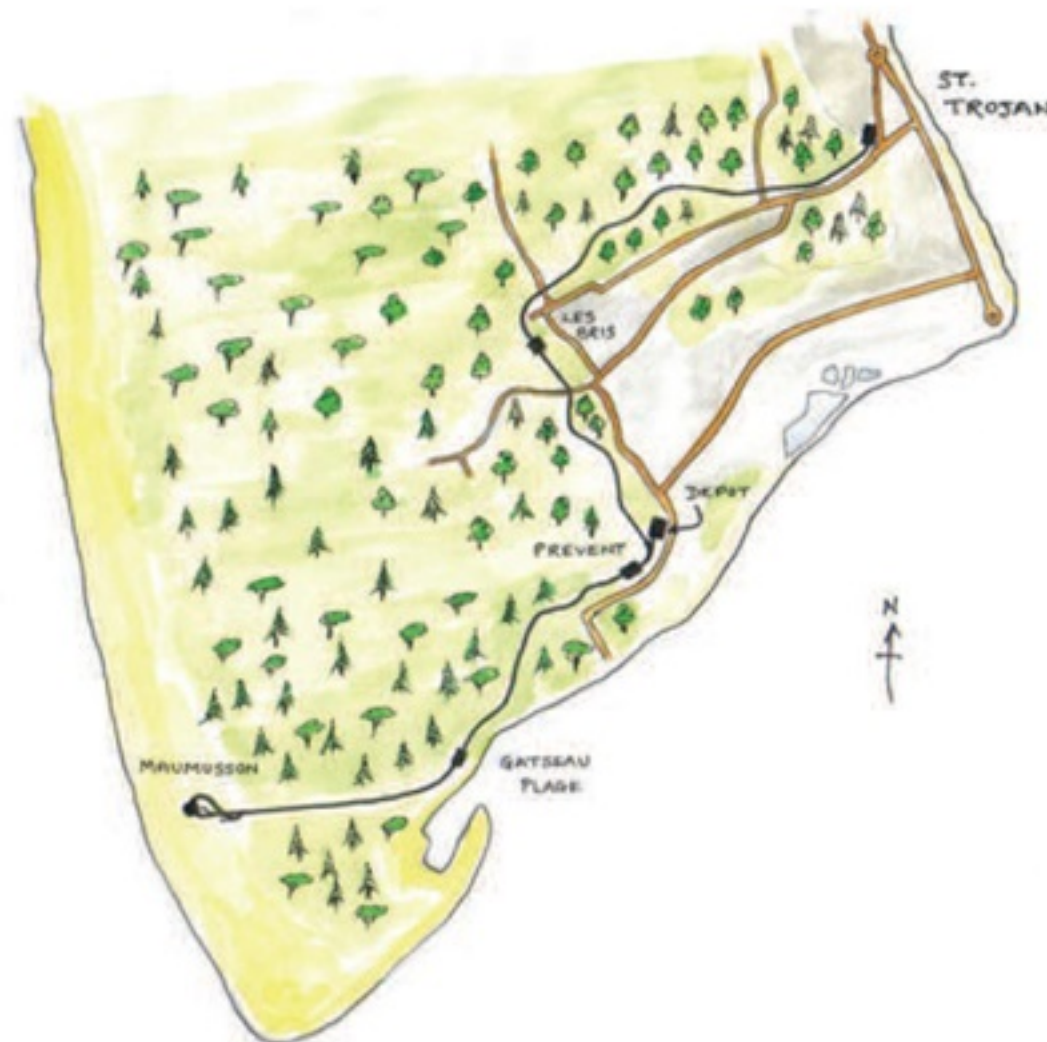
60cm to the beach

There is a long French tradition of laying small tramways of 60cm gauge to connect a seaside town or standard gauge railhead with the nearby beaches. Apart from the long defunct lines in the Somme region connecting the Nord company at Quend station on the line to Calais with Quend Plage and Fort Mahon Plage (closed in the early 1930s), there were also similar lines on the west coast of the country linking St.George de Didon, Royan, and Ronce les Bains; however, these closed around the time of the Second World War. Happily, there are two broadly similar lines still in existence in the area, one at Cap Ferret and also on the Île d'Oléron. The latter spot was previously served by a metre gauge line – also now long defunct – but that is outside the scope of this article.

A history of the Oléron Tramway

After the Royan tramway closed a local railway enthusiast, Doctor Pol Gala, came up with a plan to restore the line between the resort and Ronce-les-Bains. Alas, the building of a new road along the coast and through the forest put paid to this scheme. However, the idea was transferred to the nearby Île d'Oléron where a suitable site was identified based on the seaside resort of St.Trojan, situated at the southern end of the island on the sheltered east-facing coast. After three years of planning and administrative work, construction began in 1962, the line opening the following year from St.Trojan to the beach at Gatseau.

Over the next couple of summer seasons it was extended through the woods to a new terminus on the Atlantic side of the island at Pertuis de Maumusson, the extension opening in 1965. Here the beach is 11kms long and is a natural attraction for holidaymakers who make use of the railway in some numbers.



From St.Trojan the line originally ran along the side of the road as far as the riding stables at Les Brys, but this later became inconvenient owing to the increasing road traffic. In the 1970s a new route was built running further inland to reach Les Brys then continuing through the forest to Gatseau, and the original route was lifted. The new route is more picturesque but includes a steep climb over a hill with gradients of 1 in 50 in both directions.

The total length of the line is 6km and with its wild surroundings the trip provides an opportunity to see some wildlife on the way to the beach.

The route described

The description here is based on notes taken during a visit in 2009 – there may have been changes since.

St.Trojan station is not far from the centre of the town, but is not too well signposted – arriving from the mainland

Below far left
Old railcar trailer acting as
the ticket office at St.Trojan.

Below left
No.8 waiting to depart
from St.Trojan station.





Above
No.7 at Prévent passing loop heading for St.Trojan.

Right
No.7 at Prévent again, heading for the beach, seen from a returning train.



Right
A German Heeresfeldbahn 0-8-0T stored at Prévent.



you need to turn right at the mini-roundabout by the shops opposite the old harbour, but there is parking at the station, which has a single platform used only for departures, with a partial bay alongside. The main line ends with a coach serving as a 'temporary' ticket office parked at the buffers, beyond which is a snack bar. At the far end of the platform the line turns sharply to the right and there is a run-round loop where returning passengers are set down, and a tight turning triangle for the locomotive.

The ride starts with a short section alongside the public road, but soon penetrates the pine woodlands that cover this side of the island, running on its own right of way. Despite the ballast being sand or earth the ride is not as rough as you might suppose. After a short while the line negotiates PN (level crossing) No.1, beyond which there is a passing loop near the riding stables at Les Bris. The train curves round the edge of the paddock and continues through the trees and suddenly there are some sidings full of spare stock as the train passes the line's workshops and the mid-way stop at the Gare de Prévent.

In high season the train waits here for the incoming service that arrives a few minutes later, the drivers exchanging the single line token, a thin paddle painted in yellow and white stripes. With the line clear we are off again, the line leaving the road and passing through some dense woodlands, occa-

sionally hooting for crossings where footpaths cross the line, and slowing down at the R signs that warn of sudden descents or sharp curves. The woods here are mixed deciduous with a few pine trees set in sandy soil with steep hillocks, through which the railway has to pick its way.





This mixed vegetation gradually changes to pine woods, and before long we pull up at Gatseau Plage station, with the beach visible between the trees about thirty metres away. After a few minutes wait it is off again, round a bend and up onto a long straight embankment that levels out the wooded dunes rolling along on either side. As the kilometre markers (strictly speaking 100m markers) pass by we see we are almost at journey's end.

There is a short passing track with a stub siding at the top of the dunes, where what looks like a generator wagon has been parked, and then a balloon loop in the sand with a dead-end siding in the middle. This is the daily resting place for the inevitable refreshment car, or *Bar-wagon* to the locals. The station is marked out by a stockade of rustic timber fencing, and although the wide expanse of beach is visible beyond, the top of the dunes are lined with scrubby pine trees, blasted by the winds and partially burned from a past forest fire – an ever-present risk. Originally the terminus was further on, but such is the speed of erosion and coastal change hereabouts, that the station tracks have had to be re-laid twenty times in the last forty-three years – a hazard not encountered on many lines!

Locomotives

For a relatively modest line the tramway has an interesting collection of locomotives. The original motive power was provided by three Deutz 0-4-0 machines with two-cylin-

Above
No.7 returning from the dunes.
The train is made up of open and semi-open coaches.

der engines, the gearbox having four forward speeds and reverse. They date from 1932, and only two are still in use, the third being plinched elsewhere. Number 1 is kept for use on the line's fire-fighting train, while number 2 is used to haul the *Bar-wagon* to and from the seaside terminus, where it usually spends the day. Although these machines are no longer used for passenger duties, their 'Wild West' silhouette – the bodies date from 1965 – are still used on the line's publicity material.

These days the train services are in the hands of a collection of five Billard diesels dating from 1937 and originally intended for work building defence works on the Maginot Line. From here they went on to careers in industry, and eventually came to St.Trojan from a local brickworks and a sand pit in northern France. There are two different types: numbers 6 and 9 are type T75P machines fitted with 75hp Panhard engines, while 7, 8, and 10 have been rebuilt with Deutz 100hp engines and are classified T75D.

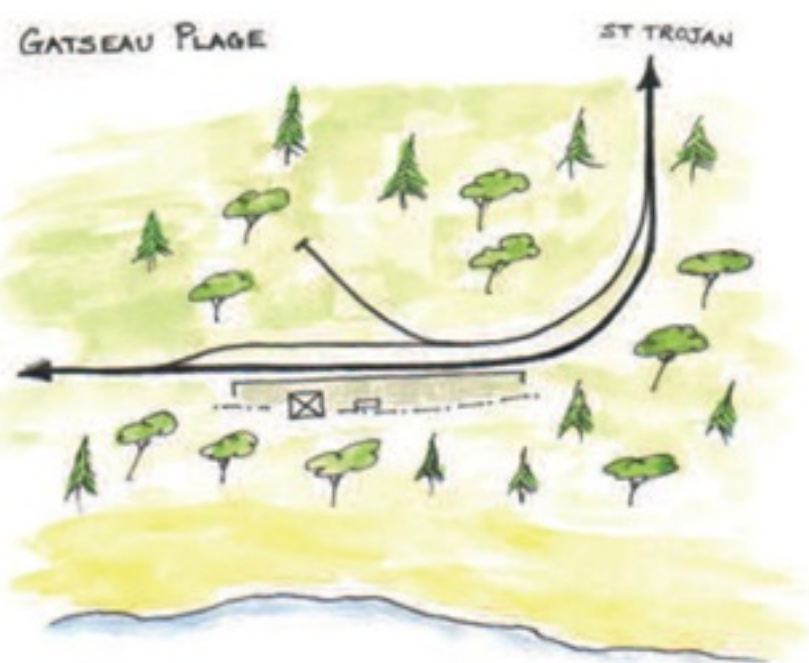
The line possesses one steam locomotive but the hazards of operating this among pine woodlands mean that it is merely on display. It is a 150hp Koppel built in 1918 for Feldbahn service. A study is being undertaken to see if it could be converted for gas firing, which if successful would certainly be a novelty.

The last piece of motive power is also preserved out of use. It is a Crochat-style railcar, built under license by Decauville in 1926. Its early life was spent on the CF de Savoie before it passed to the Calvados system in 1934, and it was also used briefly on the Pithiviers – Toury line before passenger services ended there. In 1952 it gravitated to western France to work on the Cap Ferret line, and at various other times was used at Pithiviers (again), the St.Eutrope pleasure line, and at Meyzieux. This well-travelled machine was in a very poor state when it arrived at St.Trojan and is presently held awaiting a complete refurbishment, which may be accomplished by means of an association with a technical college, whose students would use the process of recreating the mechanism as a practical project in electrical engineering.

To go with the railcar there is a matching trailer. This was re-bodied by the GIRAGR company in 1998 and is now used as a booking office at St.Trojan station, whose original building was destroyed by arson in 1997.

Coaches

The line makes use of some bogie chassis by Koppel dating back to 1914 and 1916, which were given semi-open *baladeuse* bodies fitted with transverse seating for the line's opening. Because of the aggressive weathering from



Right
Gatseau Plage station.



windblown moisture, salt, and sand the entire collection has undergone a programme of rolling replacement over the last ten years. The underframes come in two lengths, seating either 32 or 40 passengers. Some coaches have a more enclosed body with seats arranged round the perimeter of the vehicle, and these are used for an evening 'Dinner Train' service as well as in normal service by passengers in wheel-chairs, or by those with small children in buggies. They are also handy towards the end of the season when the weather can become a little unpredictable and their semi-enclosed bodies offer a little protection from the rain.

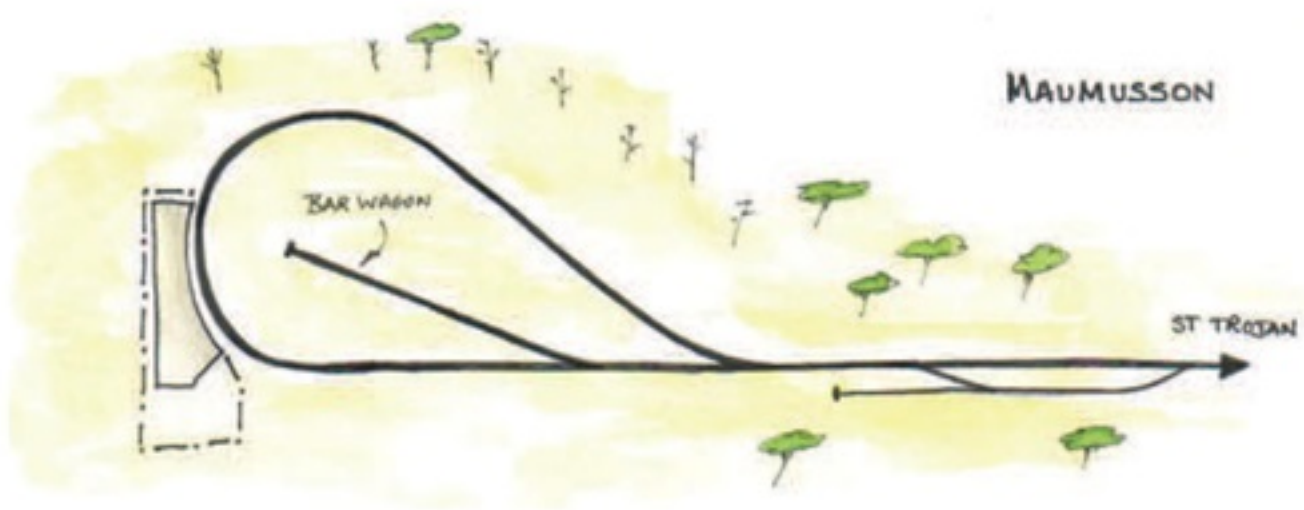
Another product of the line's workshops is the *Bar-wagon*, essentially a kitchen in a bogie van body, which has side hatches that can be raised to reveal a serving counter. In high season this is taken to the seaside terminus by Deutz No.2, but in September the loco returns to the depot around lunch-time and the *Bar-wagon* returns attached to the last passenger train of the day.

Since 2002 the *Bar-wagon* train has also included another unusual vehicle – a wagon adapted to transport windsurfing boards, which are unloaded by means of an adapted quad at the seaside terminus.

Photo possibilities

The station at St.Trojan is an obvious spot for photographs, as is the short roadside section – about 500m long. Beyond here a dirt track turns off to the stables and there is a good level crossing vantage point. If you follow the signs marked 'Thalassothérapie' along the main road you will reach the depot and station at Prévent, though the surrounding chain-link fence makes clear photography somewhat difficult.

This is about the last place on the line that can be reached by car and you will need to be on foot or bicycle to explore the forest trails that cross the line, or else make the journey by train to the beachside terminus at Maumusson.



Above
No.8 waiting to return from the beach.

Left and right
No.8 at the beach station.

Below left
The Bar-wagon.

Below right
'Wild West' Deutz loco and surfboard wagon at the beach.



Myles Munsey brought to life memories of a trip to Romania in 2005 and created this bucolic scene.

Photographs by the author.



Sânpetru

A remote station in the Banat, near the Hungarian border

Background

After a ten year dalliance with modelling the railways of Alaska, my interest in European modelling has been re-awakened. Not that it ever went away. It is just that I found some Romanian coaches which I bought long ago, never used, gathering dust and in need of a suitable layout.

When in 2005 I travelled in the Banat (the part of western Romania near the Hungarian border), I saw a way of life seemingly stuck in a time warp. Horse and cart held sway as a means of conveyance. Likewise the steel wheel supported rolling stock of a similar antediluvian nature. That part of Romania appeared to have barely entered the machine age.

I have harboured a desire to model this little-known railway system for quite some time. A country which – to the best of my knowledge has not featured in CM before. Finally I am there!

The whole area was delightfully rustic. I resolved to capture elements of this scene in a little diorama. The aim was to model authentic rural Romania. The unrepresentative single coach trains do not really matter. What matters is the portrayal of a decaying branch line just before the swathe of mass closures in this area.

This particular flight of fancy is faithful to the prototype, but does not revolve around one single place. Instead ideas were drawn from several locations in order to capture the atmosphere of the Banat whilst resuscitating a couple of cameos that are still fresh in the memory.

Below

A DA 060 Co-Co diesel built by Electroputere of Craiova (model by Albert Models) arrives hauling just a single double-deck coach (Piko).





The district round Timisoara and Arad was punctuated by a spider's web of remote branch lines where normally only four or five trains a day were provided. Sadly most rolling stock was in a pretty dilapidated condition. Stations were often distant from the communities they were meant to serve and frequently set amongst fields. Characteristically they featured low level platforms, grass strewn track, and telegraph poles leaning at crazy angles. Freight traffic was scarce but none the less evident with small goods yards still served by wagonload traffic. There were small settlements with geese, chickens, and pigs running amok and farm transport very much reminiscent of a bygone age. Such things were scenes I wanted to recreate in miniature.

The task now was to turn memories into a model.

With the small collection of Romanian models I had acquired (and are now virtually impossible to obtain), I set about modelling a small country station. The name Sănpetru (pronounced Sinpetru) was chosen as it is a genuine station not far from Arad, so very much in the correct area of the Banat. Furthermore, it has a nice Romanian sounding name and is next door to Periam, on which the station building is actually based. I made my version a terminus, whereas the real place is a through station. Other than style, there is little similarity between the two and I repeat that my model is exactly that – an impression.

Baseboards and electrics

The layout was built on a 32" long shelf unit onto which a small extension had to be attached so that a loco and the longest coach will just stand clear of the points leading to the goods siding. The restricted length is forced by the requirement to fit everything onto a 6' long folding table, which will be the normal means of display when we get back to exhibiting.

Power connectors were wired in at the heel of the point from a simple Gaugemaster Combi controller. A simple on/off switch in the platform line enables a locomotive to be isolated once detached from its train.

Trackbed

Normally where a bit of landscaping is required I use foam risers. On this occasion I used foam sheets to support the track. This gave me a little height to play with, in order to create a small stream and pond. I then inserted strips of card-board and cork directly under the sleeper web.

The track itself is Peco code 100, painted with MIG 'old rust'. It was ballasted using Legacy grey blend ballast.

There is a single right hand short radius point operated by wire in tube.

The buffer stop on the main line is Peco whilst the siding is merely equipped with rail stops.

Above

For most passenger services a single deck second class coach is quite sufficient.

Below

Sometimes a couchette second in day mode is used as an alternative.



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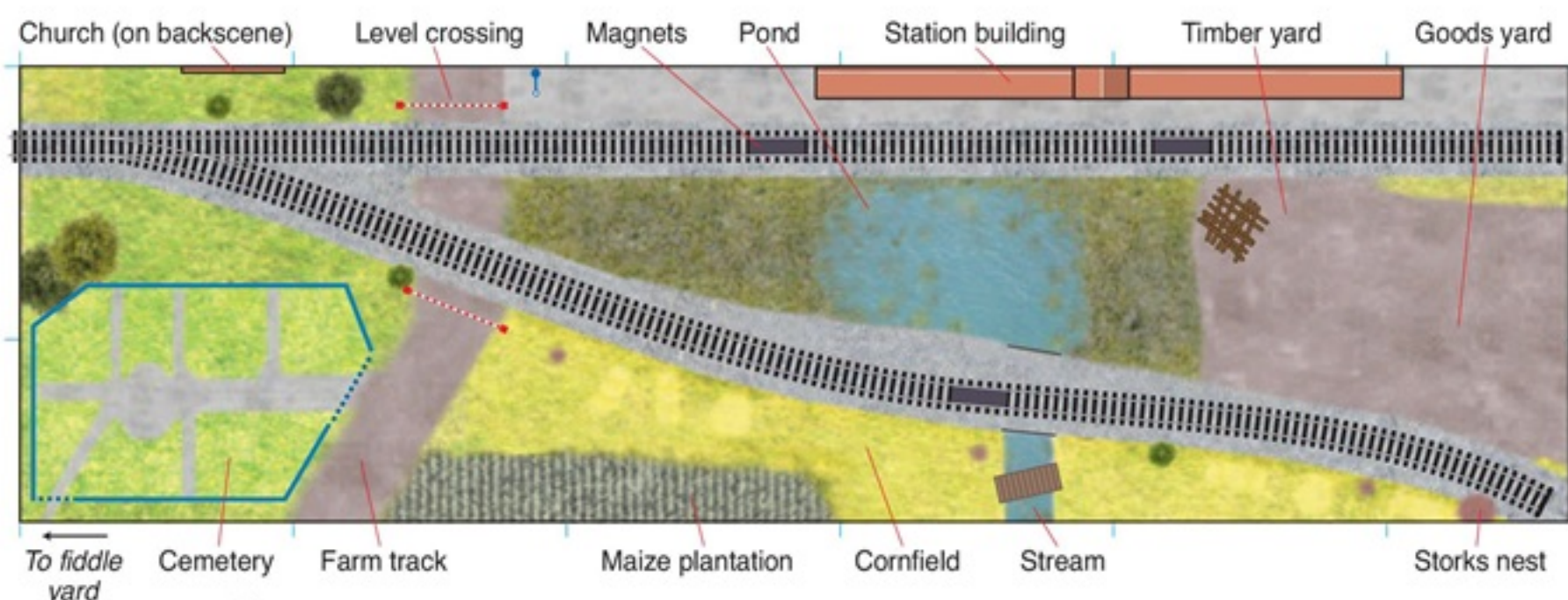
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Sânpetru Size of scenic section 34" x 10". Each grid square 6" x 6".

Track plan and operation

On a board a just 34" long a run-round loop was out of the question so a two-prong 'tuning fork' requiring just one point was chosen. More track would have impinged too much on the scenic area. Limited space means single coach trains but this drawback is immaterial if prototype fidelity is otherwise observed. There is just a main line which is angled slightly away from the front edge to add interest and a timber/carriage siding which is laid on a reverse curve.

Whilst in Timisoara in 2005 I bought a copy of the *Mersul Trenurilor*, the CFR public timetable (CFR = Caile Ferate

Române, Romanian State Railways). Whilst primarily an aid to travelling, it provided great insight into the pattern of train services, especially in far flung parts of the system. It was evident that branch line services were sparse and irregular, so I have relied on these in general, without adhering to any one in particular, to shape my timetable.

The standard service revolved around the 'Tren Personal' – an excruciatingly slow all stations train that served every little village along the way. The 'Personal' often ran at very unsocial hours to fit work shift patterns. The real Sânpetru had five trains a day to and from Arad.





Above
A two coach train
is a rare occurrence!

When the single loco and coach train arrives, the class DA 060 detaches (and is isolated) and the second (freight) loco is engaged to remove the solitary coach and place it in the siding. Several shunt moves later the main line loco is now at the head of the coach in the platform (that coach having been propelled right back to the buffers) and is ready once more to depart.

Below
The smart CFR Marfa livery
on the locomotive contrasts
with the rough condition of
the opens loaded with logs.

The timber train likewise pulls into the platform and a second loco will withdraw the wagons and place them in the goods siding, the difference being that on this occasion the wagons are detached and placed as far down as possible for loading. The two locos will then swap over so that the freight loco is able to haul the pair out of the siding and away.

There are three magnets to assist coupling and uncoupling, all stock being fitted with Kadee couplers. The magnet in the siding is solely for detaching timber wagons. A loco and coach can lay over in this siding but the coach must remain attached – it is not possible to re-couple owing to the curvature.

I accept that the track plan could hardly be simpler, but operation is still pleasing and meaningful, if a little contrived. It does give an excuse for some shunting – something I enjoy.

The branch line train alternates between a double-deck coach and a couchette operating in day mode: a bit of modeller's licence perhaps but it fits in well with the second class only policy and levels of branch line service.





Above

Just a couple of passengers awaiting the next train as work goes on in the field.

Locomotives

The locos are class DA 060 Co-Co diesels, built by Electroputere of Craiova in Romania. Both are by Albert Models and were obtained from Trains Addicted of Cluj-Napoca.

91 53 0 601328-3 is in blue/grey CFR Calatori (passenger division) and is in charge of the local commuter train service.

92 53 0 600934-9 is in the colourful red & grey livery of CFR Marfa (freight division) and works the timber train.

There are minor detail differences between the two locos. Most noticeable are the roof mounted cooling units for the driver on the passenger version.

Rolling stock

CFR 50 53 26-26 088-0. There are two of these second class double-deck coaches with Era IV/V markings by Piko which are now virtually impossible to obtain so I am very pleased I purchased them when I did. The coaches are labelled as 'Tren Personal', supposedly running between Arad and Oradea, so are exactly right for this application.

CFR 51 53 59-50 024-3 and 51-53 59-50 016-9 (both blue livery) plus 51 53 59-50 012-8 (wine red livery) These are second class Vagon Cuceta couchettes by Sachsenmodelle and again are now virtually impossible to obtain. Although nominally couchettes they are entirely suitable as ordinary second class sitting carriages. The two blue examples feature the older style CFR logo with the couchette wording expressed in French, German, Italian, and Russian along the roof band and in Romanian above the solebar.

All coaches are weathered with Tamiya weathering powders in a mixture of rust and grime, the double-deck ones particularly so as these were consistently grubby when I visited.

Below

The driver of DA 060 92 53 0 600934-9 may be taking a bit of a risk with the loco this far along the poor track of the siding.



The freight rolling stock consists of a pair of open wagons for timber transport. These started out as Hungarian but have had the MÁV logos removed and the CFR Marfa logo applied. Each has been heavily weathered with old rust to give a truly well-worn appearance. The logs are natural – twigs gathered from countryside rambles and cut to length with a hacksaw.

Portraying the scene

With a layout this small, much time and effort was expended on correctly creating an authentic landscape, high on detail, invoking the character of the area. There was also the period element – avoidance of modern machinery was desirable.

I relied quite heavily on memory, plus a few pictures I had taken that proved invaluable. Much of this part of Romania is flat, given over to hay making and punctuated by drainage ditches.

A good portion of the scenic area is taken up by the cemetery and the cornfield. I had to get these right and make sure that they fitted into the limited space available whilst leaving room for other features. It was all about balance.

Cemetery and farm track

It seems as though every village in Romania has a cemetery. As a result I was obliged to include one and managed to fit it into the only possible location, by the points.

The wall is by Busch but looked rather dowdy so was repainted in white with blue capping. Note the irregular shape. One side follows the alignment of the farm track which gives more interesting viewpoints. The trees inside the cemetery are Noch poplars.

The grave markers are a selection from the Woodland Scenics tombstones (1856) pack.

Pathways were cut from a sheet of paving stone by Aneste. Light green grass was again used to fill the spaces between the pathways. As befits a cemetery, the area is rather overgrown. A Catholic priest stands outside the iron entrance gate.

The farm track itself was created with plaster cloth to give a gentle rise to the simple barrier level crossing. Study of level crossings in these parts suggests the common use of red and white striped poles as barriers.

The old lady (in black) berating the stubborn pig is a throwback to a vivid memory I have of a young girl chasing a hog down a country lane. The geese (Faller) waddling down the farm track is another memory – also a common sight in these parts.

The cornfield

The cornfield was made using Woodland Scenics 12mm static straw grass. I found that this looked good without the need for an applicator and is indicative of a dry and dusty landscape. The round hay bales were by Woodland Scenics Scenecraft, whilst the ovoid Romanian type corn stooks were homemade using a pair of finger stalls to which 'straw' was glued. The central pole is simply a bit of stiff wire. My cornfield is worked by hand so modern machinery is absent. I therefore added some manual hay workers (complete with pitchforks and scythes) by Noch.

At the front edge of the cornfield is a strip of maize consisting of 72 individual plants by Tasma Products (ref.00680). The original quantity was doubled as the plot

looked rather threadbare. Maize grows as a very tightly bunched crop so the doubling up exercise made for a much better visual effect.

Sânpetru station

The station building is an adaption of the nearby one at Periam. Similar in style, but with different colouring, it was constructed primarily from mount card with gutters, drain-pipes, etc., mainly made out of plastic odds and ends. It is compressed somewhat from the original (given that this is a micro layout) and is therefore proportional to the operable length of the platform, which can only accommodate a single coach plus loco. I feel I have captured the correct style and proportions, and it sits very nicely in the scene.

The platform itself was made from 5mm foam sheet, the low profile being exactly in keeping with the height of most wayside stations in Romania where a big step down is normal.

Benches and dustbins (in Romanian national colours of red, yellow, and blue) are by Noch, whilst the platform lamps are fashioned from the Walthers utility poles set.

Elsewhere

The stork's nest atop its pole and the solitary example on the chimney pot (a familiar sight all over eastern Europe) is by Faller. Telegraph poles are by Rix Products, and deliberately have a distinct lean of about 10°. The cross arms are all askew as well.

The goat by the level crossing is by Penduke Models.

Ground cover is a mixture of Woodland Scenics light green 12mm grass, light green lichen, plus a few clumps of brown briar patch and light green underbrush.

The pond was cut out from the foam sheet, sloped at the edge, then choked with Tasma Products sedge (ref.00713). The light green grass was applied liberally to many parts of the layout, including between the rails – coverage along the siding was intense, along the platform less so.

The goods yard is very rudimentary and I saw fit to only equip it with a pile of logs waiting collection plus a Volvo grab appearing sporadically when the timber train is about.



Above
DA 060 91 53 0 601328-3
right at the end of the line.



Above
A stork nest on a pole
was an essential detail
to establish local authenticity.

Below
The hayricks are based on
examples observed.

This near-empty space works well in contrast to the rather more crowded left and middle sections. Detailed though this scene is, I was careful not to overdo things and fill corners just for the sake of it.

Figures are mostly from a Preiser pack of undecorated figures. These have been painted and include a few passengers, the Catholic priest outside the cemetery, and the old lady on the farm track.

The backscene is blue mount card, plain and simple. However, I could not resist superimposing an orthodox church (cut from a picture taken on my travels) to reinforce the Romanian identity.

Conclusion

So there you have it – a foray into a distant corner of eastern Europe. Given the scarcity of models for former Eastern bloc countries, this was quite a challenge but highly rewarding.

There are other eastern European countries which I think have never been attempted as layouts. I do have a small collection of Bulgarian carriages, so something featuring the Cyrillic alphabet, perhaps? You never know!



Andrew Knights chanced on a novel approach to finding space to accommodate a layout. *Photographs by the author.*

A home for your layout

– or a layout for your home?

Most articles dealing with the subject of a home for a layout look at the merits of using a spare bedroom, the loft, the garage, or a shed in the garden, but the corona virus pandemic and subsequent cancellation of model railway shows has presented me with a slightly different dilemma.

I have been a keen exhibitor of layouts for some twenty years or so now. When asked at shows, I have always said that the layouts I build are really home layouts that I take to shows, with even the largest being small by exhibition stan-

dards at around 12' by 5' at the most, which in a hall full of club layouts seems small in comparison. For many years these layouts all followed the general plan of a boxed scenic board with a flat and simple fiddle yard, sitting to one side or the other of the scenic box, normally beside the operator controlling the layout from the front, giving easy access to the yard.

I decided, early on, that my layouts would not be reliant on tables provided by the exhibition venues but would be self-supported on my own system. Initially this was a collapsible table with the legs folding up within the depth of the top. As the show layouts became more varied in length and design, a more adaptable system was developed with four three-legged trestles and loose layout support beams constructed from the usual 2 x 1 timber which are screwed together as required on site. Easy to store, transport, and set up ready for the layout to be positioned on top – and easier than trying to turn over a 4' long board with bolted-on legs by yourself.

Left
All the layout elements ready to erect – legs and beams, fiddle yard wings, fiddle yard, and layout box (on end).

Below
One set of legs and beams to support a fiddle yard wing in place, and the main beam showing the centre join.





4' long? A change here, early in the show career. I reduced my main board size, generally, to 1' wide by 1m long. Yes, mixed units, but the timber yard I use to supply my needs can cope easily with either. This size is important for it is the largest that I have found that can be carried easily in front of a person, fitting through doorways leaving their knuckles intact and being able to see where they are going. Also, boards of this size should readily fit into most saloon or hatchback cars.

Keeping the baseboards as light as possible, much use is made of 4mm plywood plus DIY store tongue and groove wall cladding, which minus both tongue and groove is thin, very light, readily available, and cheapish. This makes up the frame for the front and one end of the plywood baseboard, with the back and other end made from 15" high 4mm plywood supporting the lid with provision for layout lighting. A pair of these baseboards are hinged on the front edge and when folded together and protected with a ply end board give a boxed layout suitable for transport or storage.

The fiddle yards are also framed with the cladding timber, but not boxed, to be as light and rigid as possible.

After a number of years exhibiting British, German, and American outline end-to-end layouts with their rather limited out-and-back operation, often only shunting movements, I felt that it would be great for a change to see the trains running through a scene. This would require a new type of fiddle yard and if it could be made adaptable enough to accommodate different scenic boards this would make the most of the investment involved. Thus a 'Universal Fiddle Yard' was planned out and constructed to use the existing trestles for support.

Two 4mm plywood boards 4' by 2' were framed top and bottom where possible with inch square timber and

Above
The fiddle yard wings ready to be put in place – at the top are the yard tracks, below the entry roads for the various layouts.

Right
The wings are now in place. The fiddle yard is unfolded, ready to put in position.



equipped with a set of Peco Setrack curved points leading to five storage tracks at the back and three tracks at the front for access to the various layouts. These boards stack together for storage and transport with the track, point motors, and switches safely protected inside. All the points on these wing boards are powered and the DC track power is fed to the apex of the curve and then to the relevant tracks on the storage board and the layout, using the points to direct power to the selected route.

The similarly constructed rear storage track board is hinged for ease of handling and to protect the five individually powered one metre long tracks on each half. When unfolded and held rigid with a piece of steel angle, it is suspended from the trestle-supported wing boards in a similar manner to the layout scenic boards which drop into place at the front.

Track power to the layout is fed by simple USB leads and all other connections are made with pluggable terminal blocks from the control panel and between boards.

Power is fed independently to each wing board and from there to the attached half of the layout and the storage tracks, keeping wiring to a minimum for the layout and giving a possible ten storage spots in the yard. None of the layouts feature power feeds across the baseboards within their scenic sections.



So far three layouts have been constructed to use this yard: *Summer Springs on the New Cut* (RAILWAY MODELLER October 2017) based on the lines out of Norwich Thorpe to Great Yarmouth; an American HO short line *Eastwood, Vermont* (CM September 2017), and a German outer suburban line and freight yard *Köln Draußen* (CM October 2018).

Having retired *Glasgow Emerald*, my previous Scottish HO fiddle yard to terminus layout, its replacement is a double-track 'trains in the landscape' layout, set somewhere on the western side of Scotland, also constructed to use the universal fiddle yard.

In all cases the layout, supports, fiddle yard, and rolling stock boxes all fit into a small family hatchback, leaving room for an operating crew of two, in relative comfort.

Construction was well under way with the new layout, *Mertonford 1983*, when everything changed dramatically. All of a sudden there were no exhibitions to go to and little prospect of such popular public events for some time to come. This leaves me with a number of layouts and nowhere to show them, but having always told people that my layouts were really home layouts taken to shows, now they were home layouts in reality, so something had to be done. So for a while, my various exhibition layouts will need to truly become capable of being put up in the home, which is what I have been telling people at shows while trying to encourage them to build a layout themselves.

In an exhibition hall these layouts seem small, but in a 1930s bungalow space is at a premium and clear floor space even more so. I have in the past put up one of the layouts in the sitting room for testing and maintenance using the

trestle supports but this involved clearing all the furniture outside onto the verandah, setting up the layout to check that everything joined up correctly or play trains (operate!) for a while, and then reversing the process to put everything straight again. Not an ideal solution! Occasionally I have even hired the village hall for an afternoon to save all the fuss!

What was needed was a set of supports that did not require the emptying of whichever room the layout was to be erected in. The sitting room was still the room of choice as it has the least obstructive furniture, such as a bed, wardrobe, or similar immovable item. It does have a 1930s fireplace and mantel shelf but as the room was not a dedicated 'railway room' nothing was to be bolted to the walls or other furniture. Whatever was decided on, had to be quick and easy to assemble, rigid in use, and just as easy to remove and store away afterwards.

Using the same wall cladding timber as in the layout framing, I made up a beam, L-shaped in section, in two parts which plug together and all but span the full width of the

Above
The legs, beam, wings, and yard await the addition of the layout box.

Below
For safety, the yard is clamped to the mantel shelf.





Above
Köln Draußen in position,
awaiting the addition
of the freight fiddle yard
(propped at the far end).

room. Four outriggers made of an inverted L section and a shade over 4' long were made. These are designed to clamp or bolt to the outer ends of the beam to carry the fiddle yard wings, supported by simple gate-style legs at the other end.

As much use as possible has been made of simple one-handed pump clamps for ease of assembly and these hold two of the outriggers in place, fix the legs to the outriggers and, most importantly, fix the whole assembly's main beam to the mantel shelf, giving a stable and rigid support, despite its lightweight construction.

The height of the mantel shelf means that the bottom of the beams are clear of the armchair furniture and the track is around 4'2" from the floor, giving a good height for viewing the supported layout.

Now I have this support system, as there is no need to move any furniture, I can erect it in under twenty minutes and set up the fiddle yard in around half an hour from leaving its loft home. A few more minutes fitting in whichever layout scenic box is required and the whole thing is set up and ready to operate in just over an hour.

At shows we have often been told when chatting with visitors that it would be great for them to have room for a layout, not just a shelf shunting layout, but they would like a 'proper layout' with trains running round through the landscape, though this is just a pipe dream as they do not have the space. If something such as the above can be set up quickly and with very little, or no, domestic disturbance, this may be the way to make those dreams a reality. The change in circumstance has allowed, or forced me, to do just that ... find the room for a layout.

To make such a scheme workable, lessons need to be taken from the world of exhibiting. Unlike a fixed home layout, a temporary home layout needs to be constructed as though it was going out. All the boards need to be as light and easy to move as possible, while boxing the main board means that there are no worries about damaging the layout during storage, moving, or erection. Alignment of the boards should be as simple as possible, and I tend to use hinges minus their pins which are screwed to the board frames, one pair vertically on the backscene and another pair horizontally beneath the boards themselves, again using clamps to hold the boards together.

Wiring for power and control using pluggable terminal blocks and USB leads affords quick and easy connection.

All the buildings and scenery need to be firmly fixed in place, with as little needing fitting as possible to bring the layout into full operation.

Should you want to have more than one layout with alternative themes, for example either different regions or different periods, this could be easily accommodated by just dropping in the required scenic boards, though changing the stock in the fiddle yard would take longer.

So, maybe you do have room for a layout, if not a room for your layout.

Now I have another life for my exhibition layouts, which does not depend on loading the car late at night and early morning starts for long journeys to the venue. I suppose bacon butties can still be had, even if I have to produce them myself! But I will miss the banter with other exhibitors and chatting with visitors.

Making a start with a micro layout

Having long considered the possibilities of American N, **Josh Williams** took the plunge and built this small layout as a test project.

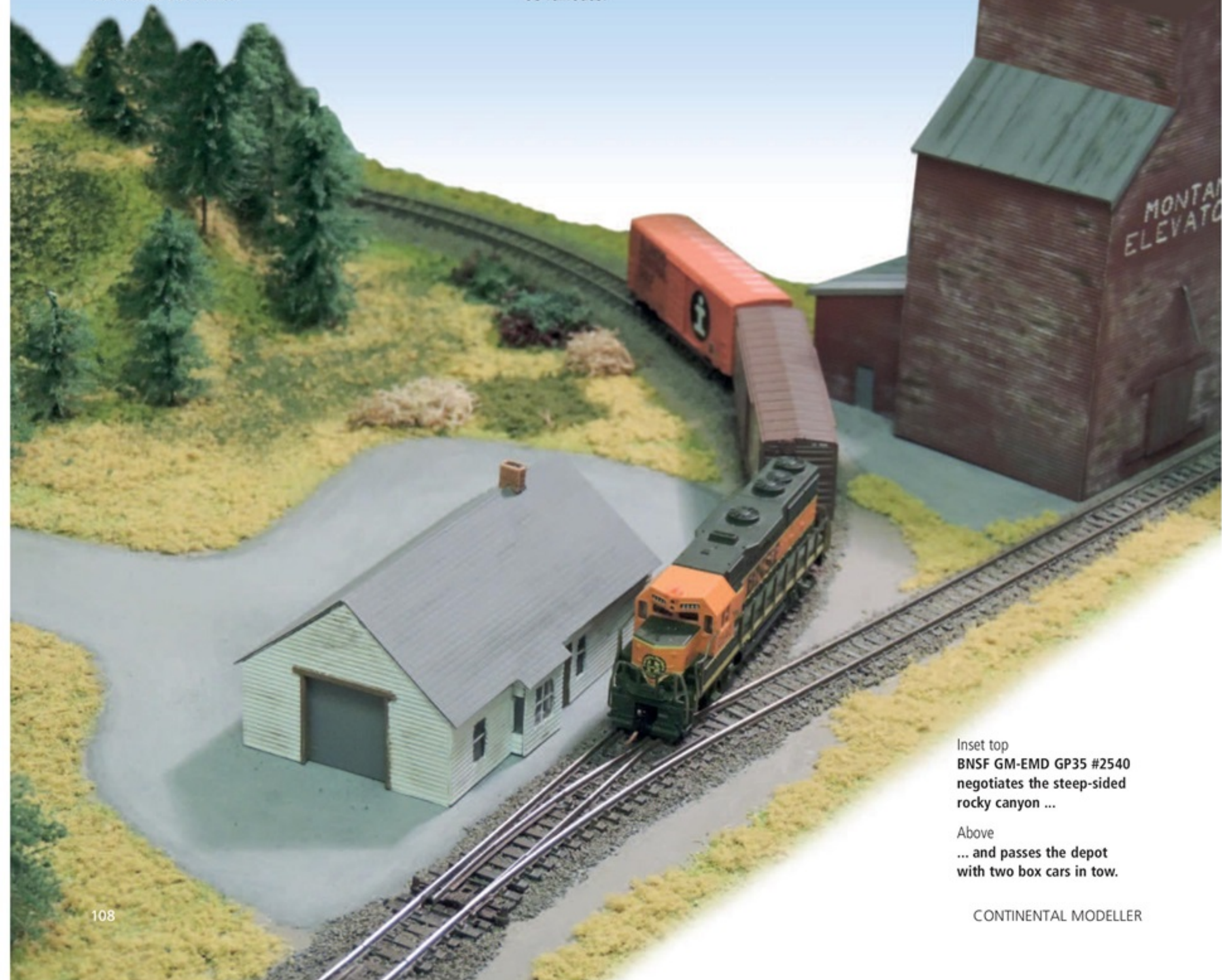
Photographs by the author.



Williamstown, MT

I had often thought about the possibilities of modelling the American scene in N without ever taking the plunge. I had read about the quality of the rolling stock in various modelling media, and eventually I decided to give it a go. I have not looked back since.

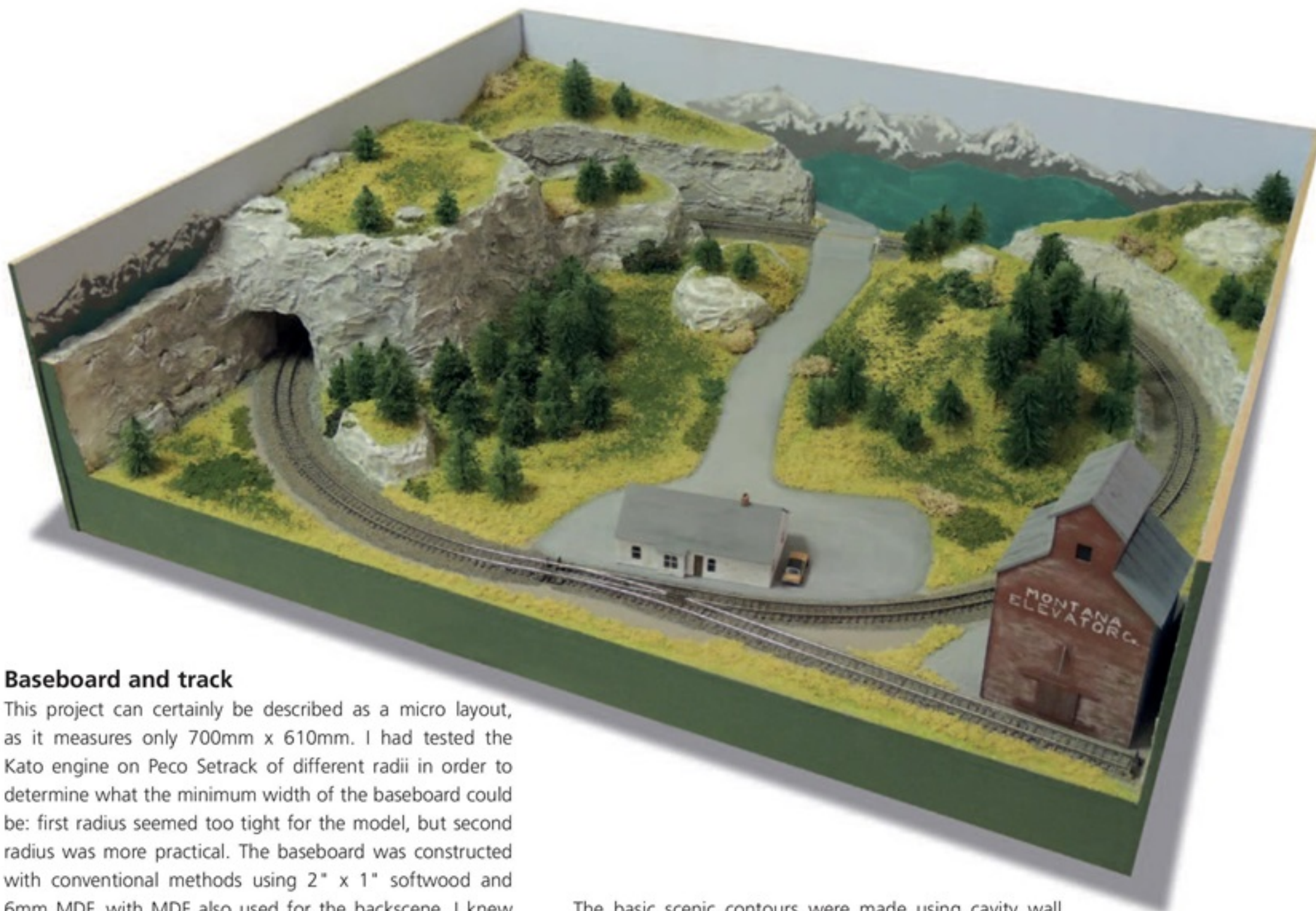
It started with the purchase of a Kato SD45, and the quietness and smoothness of its operation along with the fine level of detail meant that I was immediately hooked. I felt that I had to build a small layout as a test bed for modelling US railroads.



Inset top
BNSF GM-EMD GP35 #2540
negotiates the steep-sided
rocky canyon ...

Above
... and passes the depot
with two box cars in tow.

CONTINENTAL MODELLER



Baseboard and track

This project can certainly be described as a micro layout, as it measures only 700mm x 610mm. I had tested the Kato engine on Peco Setrack of different radii in order to determine what the minimum width of the baseboard could be: first radius seemed too tight for the model, but second radius was more practical. The baseboard was constructed with conventional methods using 2" x 1" softwood and 6mm MDF, with MDF also used for the backscene. I knew from the outset that I wanted to include a tunnel, so I cut an access hole in the section of backscene adjacent to this area to make track cleaning easier.

The layout was built to stand atop a small flat-pack table, although I might eventually build some legs to raise the viewing height.

The whole baseboard surface was painted with brown emulsion, and then Peco code 80 track was laid down. I use Copydex for this, as it is easy to lift the track prior to ballasting if necessary. In hindsight, I should probably have used a form of trackbed to create more of a ballast shoulder as befits the US prototype.

After applying some sleeper grime paint, the track was ballasted with Green Scene fine grade material. I also used Copydex (diluted 50:50 with a drop of washing-up liquid) for this instead of the usual PVA, as it seems to reduce the levels of noise transmission from the rolling stock to the baseboard.

Power to the track is provided by a Gaugemaster Combi 12v DC unit.

Scenery and structures

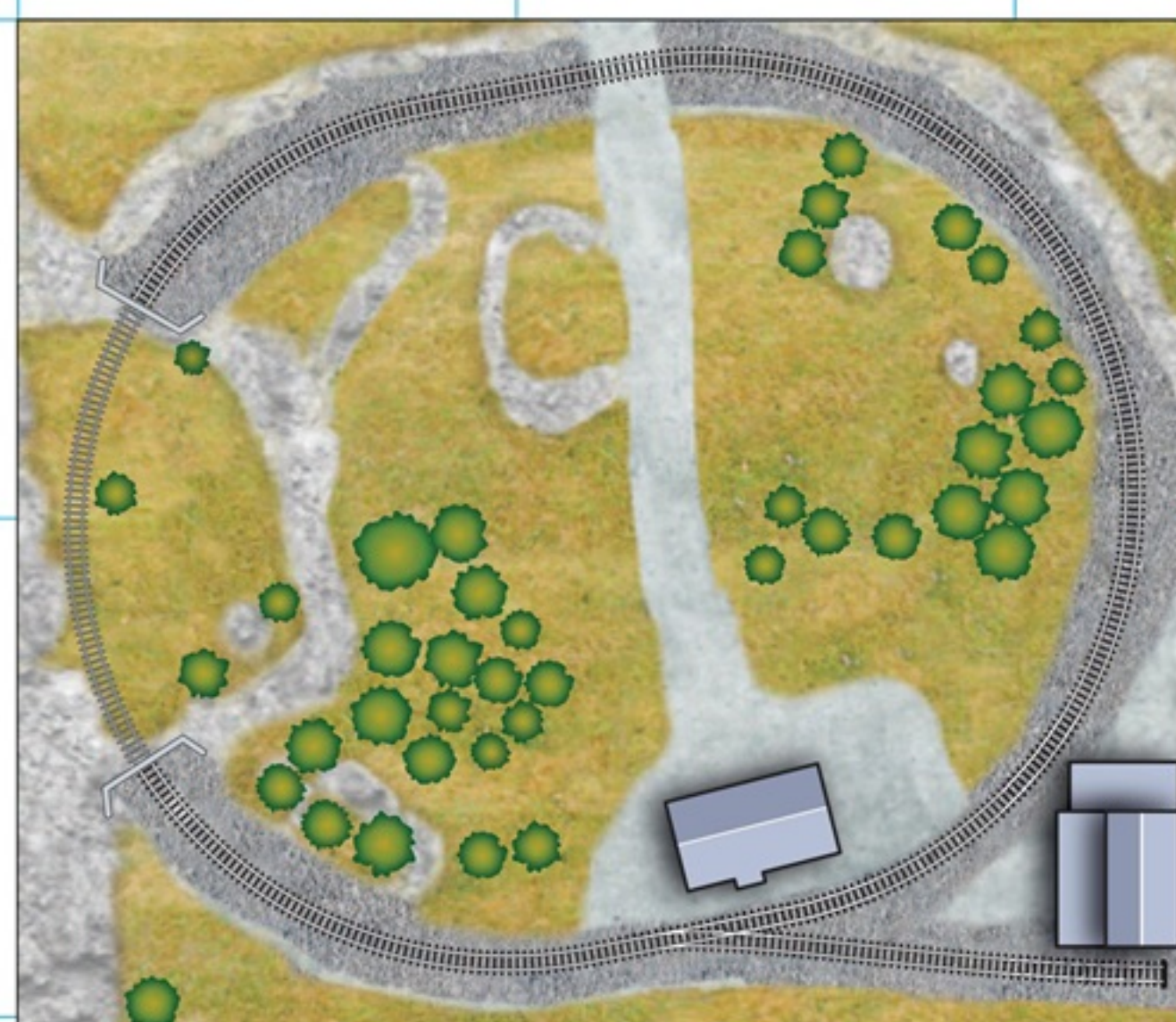
This was where I could really begin to delve into the possibilities of modelling the American scene. I searched the internet for inspiration, and the type of scene that appealed most to me was the landscape of the Rocky Mountains, particularly in the state of Montana. I wanted to try to create an image of this wilderness despite having such a small layout. I felt that this layout could perhaps represent a short line diverging from the BNSF's Chicago to Spokane route. I took inspiration from locations such as Eureka, Libby, and Essex, which are situated near to the border with Canada, along with some of the places along the Montana Rail Link route, and in so doing I came up with the basis for this fictitious location.

The basic scenic contours were made using cavity wall insulation board, which was easy to use but quite messy. Once it had been shaped to my liking, I created a hard shell using DIY filler.

I wanted to try to distract the eye from the train set like nature of the layout by partially hiding the sharp curves, so I included features such as a canyon and a cutting along with the tunnel.

Williamstown

Overall size 700mm x 610mm
Grid squares 300mm x 300mm





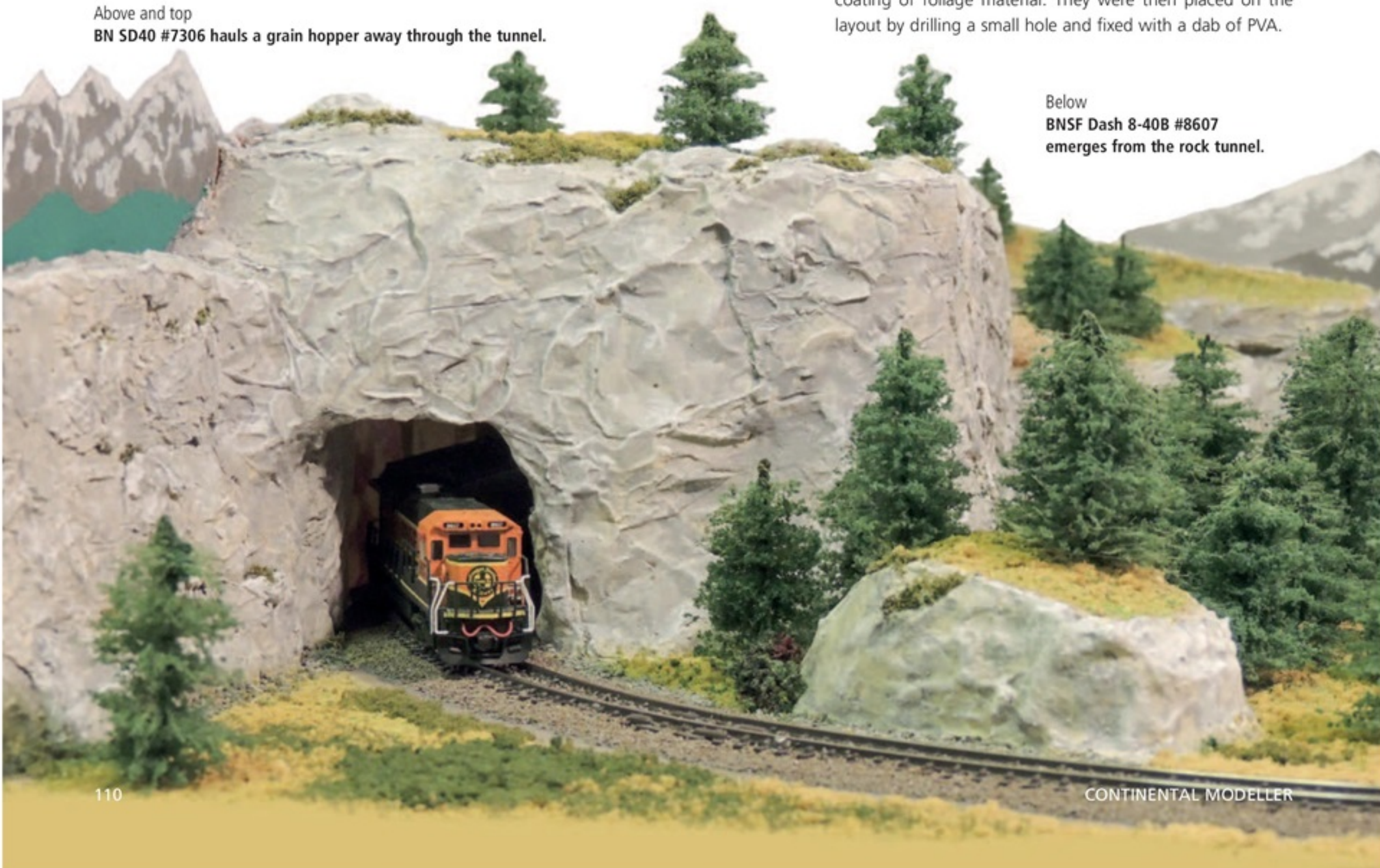
Above and top
BN SD40 #7306 hauls a grain hopper away through the tunnel.

I had not modelled rocks before, so I wanted to give this a go. At first, I used crumpled-up foil to create impressions in the filler, but I soon realised that I could achieve a similar effect just by applying the filler in a haphazard fashion.

Once dry, I gave the rocks a coat of grey acrylic before brushing on a variety of colours to get the effect that I wanted. I am not sure how convincing the results are, but I think this is something that could be improved with practice.

The rest of the landscape was covered with scenic materials from Jarvis and Woodland Scenics. I have attempted to create quite a wild landscape with large areas of coarse yellow grass. The trees were all homemade; I used florists wire and Woodland Scenics field grass strands to create an armature, which was then covered in hairspray and given a coating of foliage material. They were then placed on the layout by drilling a small hole and fixed with a dab of PVA.

Below
BNSF Dash 8-40B #8607
emerges from the rock tunnel.



Left

A short freight moves through the rock cutting and approaches the grade crossing.

I had not attempted to paint a backscene previously, so I tried to depict a mountainous backdrop using acrylics over a blue emulsion base coat. I am satisfied with the results, but I don't have the artistic skills to really create an illusion of depth.

There are only two structures on the layout, and they are both scratchbuilt from plain and embossed plastic sheet.

The depot is based on a style that is common in the state of Montana, featuring a timbered exterior painted white and a grey tiled roof.

The grain elevator has been modelled in a near-derelict condition with faded paintwork. This imposing building fills the corner of the layout quite nicely and further helps to draw the eye away from the sharp curves.

Stock

I have been impressed with all of the American stock that I have acquired. The motive power comes from Kato and Atlas, and the freight cars are mainly by Micro-Trains. All of the stock has been bought secondhand from reputable sellers and at very reasonable prices, which means that American modelling is achievable on a budget. Every locomotive has directional LED lighting, but I have replaced the factory-fitted LEDs with warm white variants as this seems to create a more life-like headlight glow.

There will not be any two-mile long intermodal consists running on a layout of this size, of course, so instead I try to depict short freight movements to local businesses, such as the grain merchant. I have aimed to create a 1990s scene, in which the old Burlington Northern is being incorporated into the new BNSF system. This means that I have avoided running some of the newer locomotives now found on the network, and instead I have opted for older and smaller prototypes such as an EMD SD40 and a GE Dash 8 40-B. All are brilliant runners, as well as being easy to service and maintain, and as a result I think it is unlikely that I will return to British modelling any time soon.



Conclusion

This quick project has given me a good insight into the potential of modelling US railroads. I think that there are great possibilities to continue developing my scenic skills by modelling the dramatic landscapes that the country has in abundance, and I can see myself developing a keen interest in the Montana scene. I also feel that the characteristics of N scale make it ideal for modelling this subject, especially if you have the space to really capture the essence of heavy freight workings. I would highly recommend giving it a go if you are considering it.

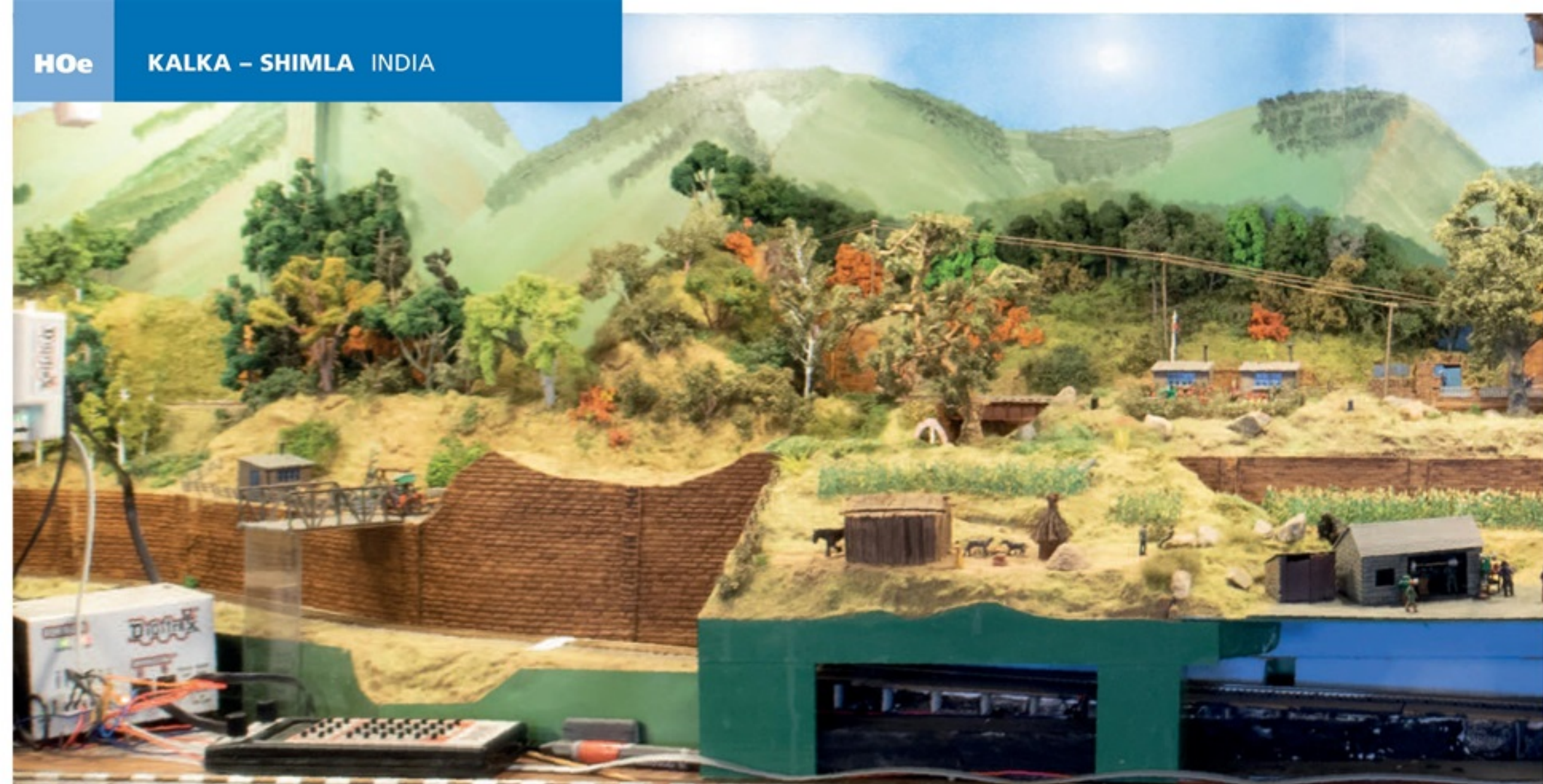
Below

The engineer of #7306 cautiously approaches a grain hopper which is ready for collection.

Above

A train disappears into the cutting. The skyline behind is dominated by the snowy peaks of the Rockies and vast conifer forests.





Ranjeev C.Dubey continues the description of his layout which reproduces scenes along this famous narrow gauge hill railway. *Photographs by the author.*

Kalka – Shimla

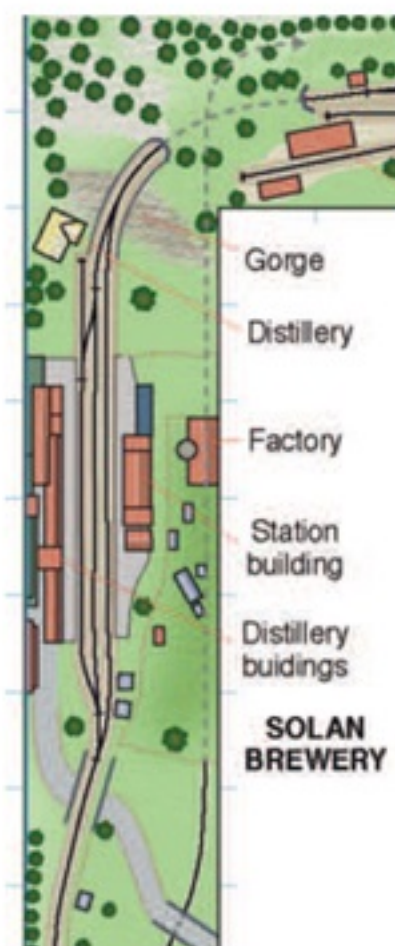
Part 5: Solan Brewery

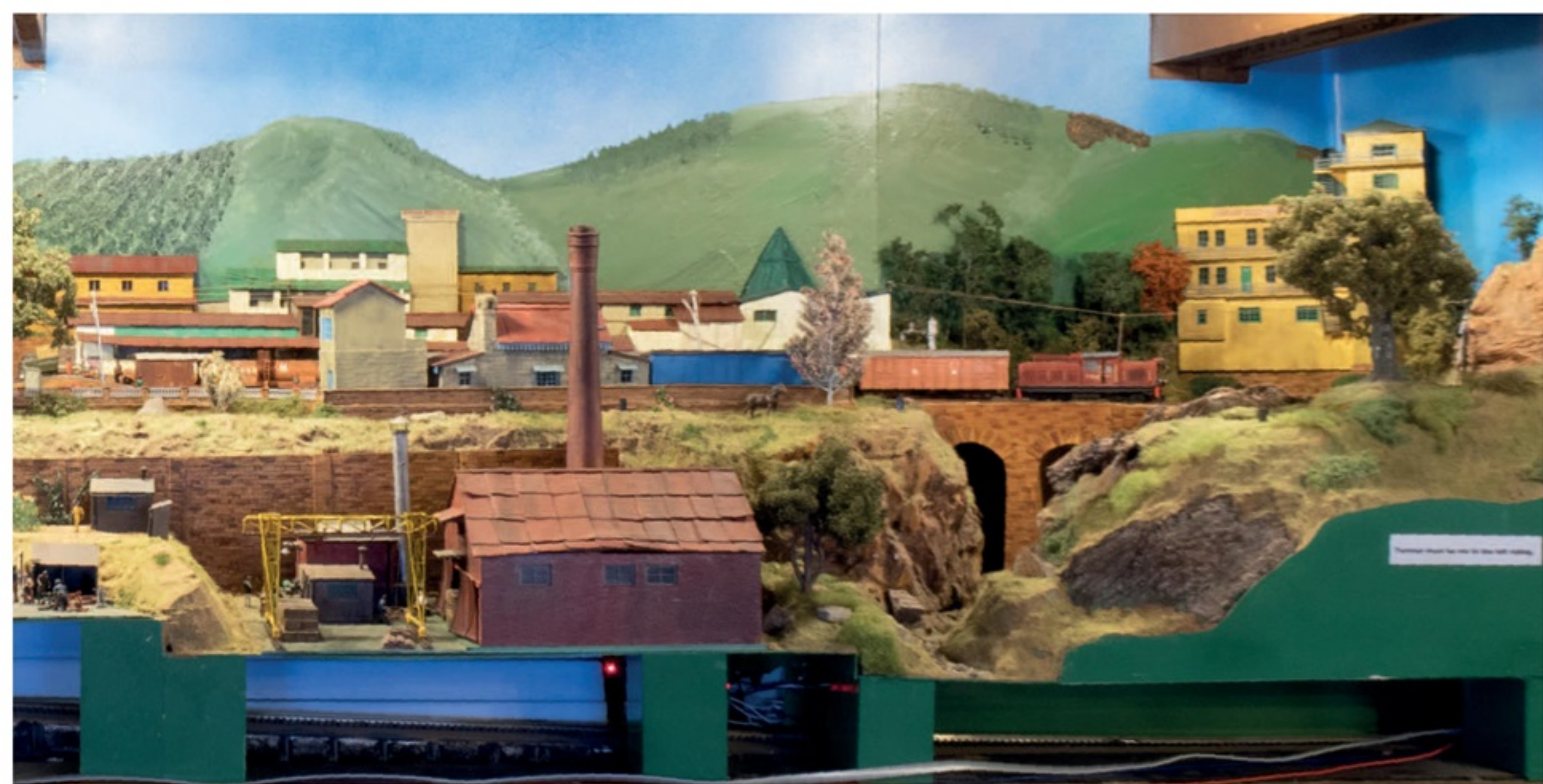
In the late 1820s, Edward Abraham Dyer, father of Colonel Reginald Edward Harry Dyer, who later achieved notoriety for the Jallianwala Bagh massacre, moved from England to set up India's first brewery in Kasauli, less than 10km from Dharampur. He soon launched Asia's first beer, 'Lion', to wide acclaim; one famous poster featured a satisfied British 'Tommy' declaring "as good as back home"! By 2001, Lion sales had dwindled to nothing in India, though I understand the brand is still the segment leader in Sri Lanka. In time, the brewery became a distillery, offering a variety of popular Indian whiskies of the modern era: Diplomat Deluxe, Colonel's Special, Black Knight, and Solan No.1. By then, Lion had been replaced by their own Golden Eagle Beer. In the decades that I have been over the age of consent, Old Monk, their vatted Indian rum, blended and aged for seven years, remained the gold standard. It is dark, with an alcohol content of 42.8%, though the Indian army gets it with 50%. Through the 1990s, I routinely had to supply bottles of the divine potion to a range of my Nordic clients! How can one possibly avoid modelling a place with a heritage this rich?

It was the first station I built when I started on my life-time KSR project. Solan Brewery is a suburb of Solan now but it was a lonely hamlet in its heyday, the road to Solan infested with bears and leopards. Travel was hazardous, so the company offered local accommodation to its workers at the perimeter of the works. It is a quaint little place, a bit run down, but the bridge across the chasm is irresistible, buildings towering up both steep slopes of the stream that runs right through it and provides water to the works.

They removed all sidings after rail traffic ceased from the distillery in the late 1990s. The works and its support structures still survive today, a place forgotten, with a single through track traversing its length. I have built my model as it existed in 1970. I have presented the main works in relief, but could not find the space for the substantial workers quarters in front. Again, the station faces the 'wrong' way and we see only its back, but it is indeed modelled in its entirety. Someday, I will have a gizmo that will project the correct view on a screen or some other clever scheme.

... to be continued.





Above
Solon Brewery station
and surroundings, from
Picnic Point to the viaduct.
The lower line (partly hidden
under the factory) climbs
from Dharampur up to Solon.

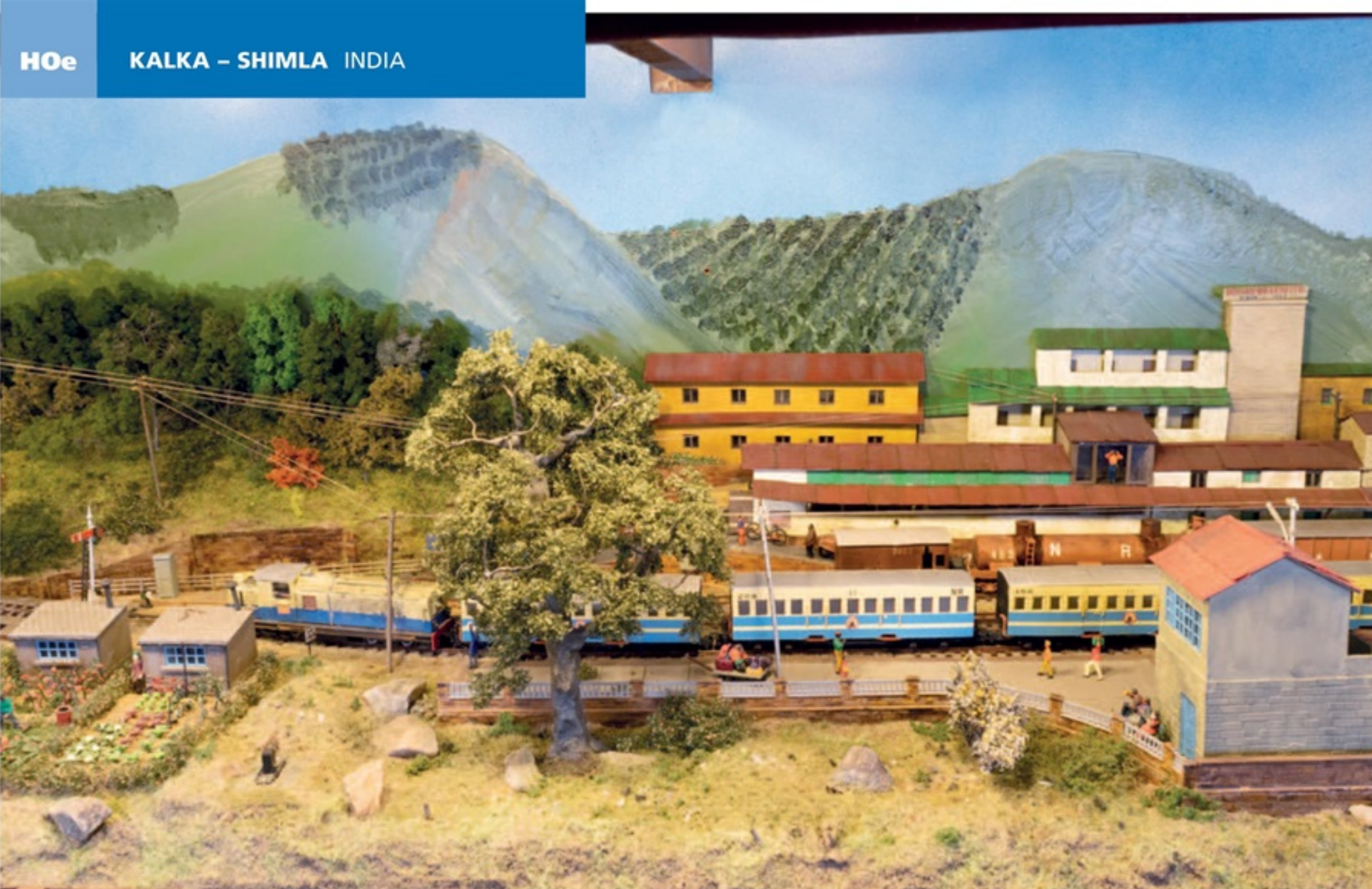


Right
ZDM3 No.155 heads
an express south to Kalka.

Right ZDM2 No.132 rolls a freight train into Solon Brewery.

Below Picnic Point, between Solon Brewery and Solon.





Top
ZDM3 No.155 hauls the afternoon express from Shimla through Solan Brewery station.

Above
2-6-2Ts ZF No.119 and KC No.521 leave Solan Brewery station leading a rake of vans.

Above right
Much activity in the factory workshops below the station as the express pauses to set down and pick up passengers.

Right
2-6-2T ZF No.118 waiting in the loop with a freight.

Opposite top
Railcar No.14 at Solan Brewery station.

Opposite lower
ZDM1 No.704 passes ZDM2 No.132 on the viaduct.



After constructing a large exhibition layout

Michael Upton was looking for something more portable for his new project.

Photographs by the author.

St. Pankraz im Steiermark

Compact, but not compromised

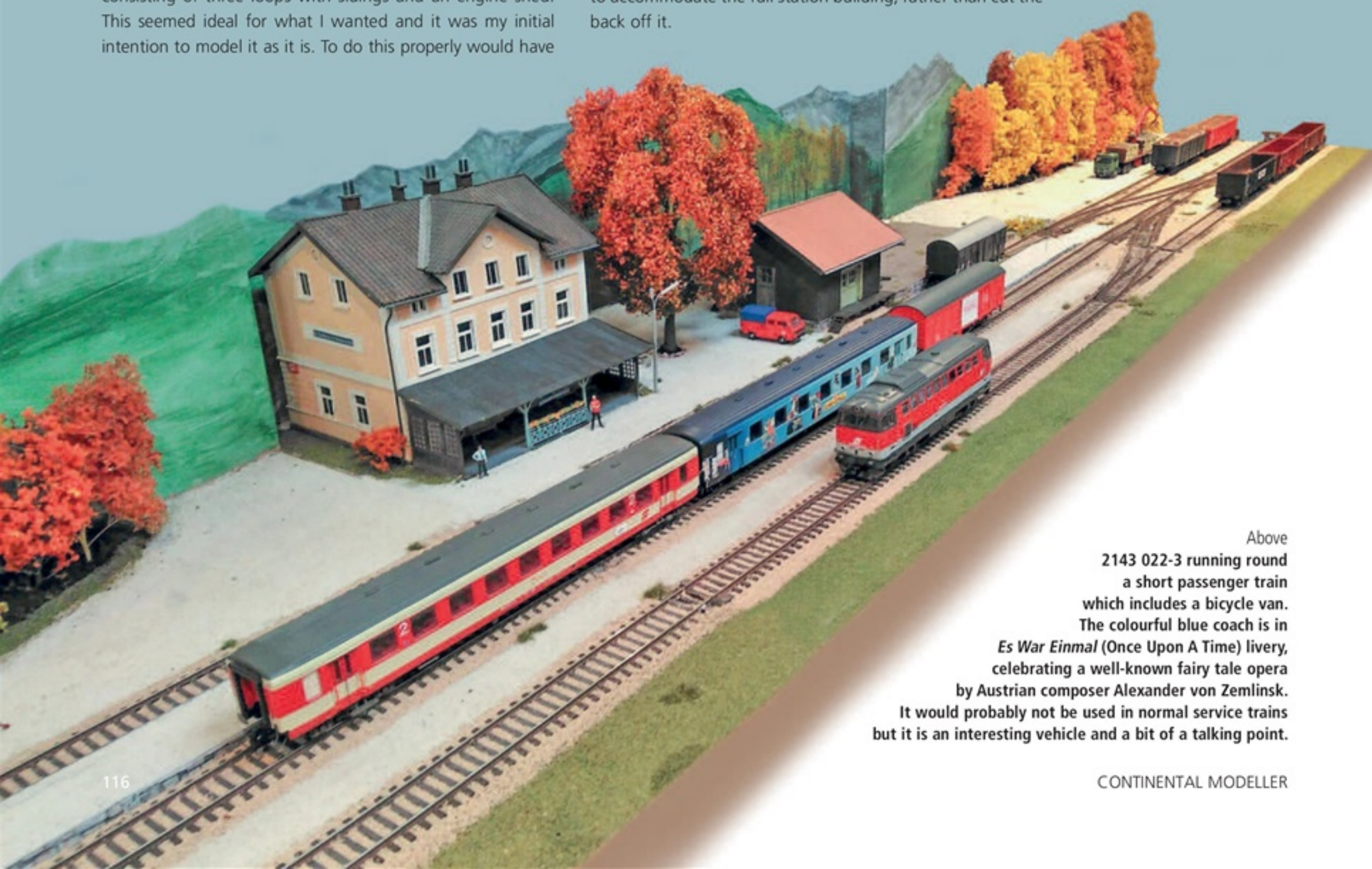
Having previously built a large Austrian exhibition layout (*Michaelerplatz*, see CM August 2013), I was looking around for a new, smaller, layout project which was more portable and would give me an opportunity to build something more intimate in nature where I could concentrate on getting the finer details right while not being so small as to make it operationally boring.

The publication of a book of Austrian station track plans by the Austrian Railway Group, and the discovery in B&Q of some small sheets of MDF which would make ideal baseboards for such a layout, gave me the inspiration I needed. Flicking through the pages I came across the plan for Grunau in Almtal, the small terminus of a branch line from Wels in Upper Austria. It showed a typical Austrian branch station consisting of three loops with sidings and an engine shed. This seemed ideal for what I wanted and it was my initial intention to model it as it is. To do this properly would have

required a layout around 20' long by 2' wide. However, this would not have fitted in the car when boxed for transport without folding the seats down and removing the parcel shelf, which was something I wanted to avoid, and one of the appeals of a smaller baseboard in the first place. As it is, the six boards give a total layout length of 16' by 16".

With the size imposed by the MDF sheets I therefore had to re-arrange the loops and loco shed siding and change the position of the buildings to make it fit while not looking cramped. The only thing I did not do was compress the length of the loops and associated sidings – they are still to the lengths given in the book, only the loco shed siding is shorter.

As it happened, one board needed 1½" added to its width to accommodate the full station building, rather than cut the back off it.



Above
2143 022-3 running round
a short passenger train
which includes a bicycle van.
The colourful blue coach is in
Es War Einmal (Once Upon A Time) livery,
celebrating a well-known fairy tale opera
by Austrian composer Alexander von Zemlinsk.
It would probably not be used in normal service trains
but it is an interesting vehicle and a bit of a talking point.

As the plan was no longer that of Grunau, I decided that another name was required. Looking though an Austrian road atlas for something suitable I chose St.Pankraz as a bit of fun, as it a small terminus, not the large one people may expect, and moved it to Steiermark to differentiate it from the real Austrian village of that name which is just down the valley from Klaus on the Pyhrnbahn, but does not have a station of its own.

The small sheets of MDF from B&Q at 810mm x 405mm (32"x 16" in old money) were an ideal size for a small, light, easily portable, layout. Each sheet required a single 2.4m length of 1½" x ½" to frame it, making for very little waste. Legs were made up from lengths of 1½" x ½" cut in half then fitted with two cross struts. The legs are removable, being fitted into sockets in each baseboard. This gives a track bed height of 40" or 1.2m, which is about perfect for me at 6'5". Small bracing pieces were fitted near the top of each leg on which the base frames sit to hold the legs square instead of separate diagonal cross braces; they also help with board alignment. It worked reasonably well but in the end a single extra diagonal cross brace was fitted to aid longitudinal stability.

Track is Peco code 100 with medium radius points and Peco point motors used throughout.

As underlay I used a 6mm foam for laminate floors which came in a pack of twenty sheets and was not very expensive. Handily, it is green, and is easily cut with a scalpel. I used double sided sticky tape to fix it to the boards, being cleaner and more instant than using PVA. I also wanted to avoid the solid joint that PVA gives, helping to improve noise damp-

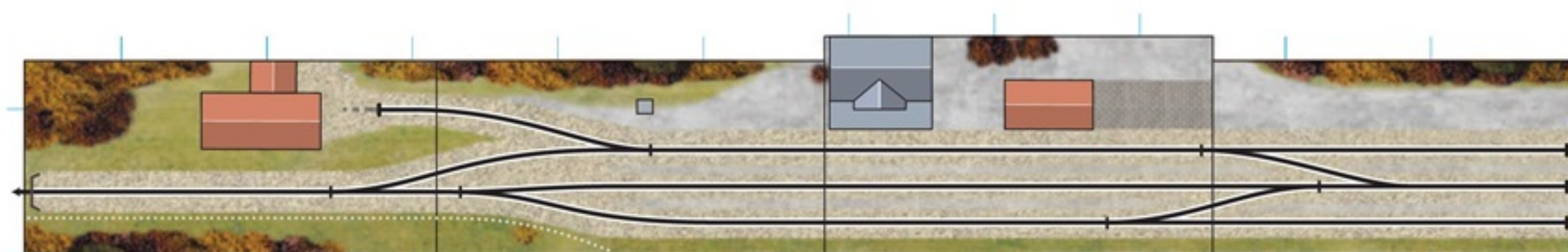


ening. The tape was fitted around the edges and down the centre lengthways of each board to prevent the middle of each sheet lifting. You only get one go at it when using this method as lifting it to realign will result in cutting and fitting a new sheet and tape. In hindsight I need not have put the bit of tape down the middle as the track does a good job of holding it down.

Where necessary, holes for point motors were cut in the underlay using a scalpel before cutting the baseboard with a jigsaw. Before laying track the underlay was painted grey to seal it from the track colour spray that would be used as part of the weathering process.

Above
The station building is from a K-B Modell kit with extra details by Ferro Train. The chap in the red shirt looks rather familiar.

Below
An ÖGEG railtour arrives hauled by 2-8-2T 86 501. This is a Fleischmann model to which extra detail and paintwork has been added.



St.Pankraz im Steiermark Scenic section 10'8" x 1'4" (Station board 1'6" wide). Fiddle Yard 5'4" long. Each grid square 1' x 1'.





Above
2-8-2T 93 1421 (a Bachmann Liliput model) belonging to railtour company Brenner and Brenner runs round its train of four-wheel coaches.

I have a love/hate relationship with wiring layouts. On the one hand I enjoy working out what is required and making as neat a job as possible of installing it. On the other, even for a small layout like this, it takes ages, even when kept to a minimum – but it is very satisfying when it all works.

I use analogue control, finding it to be the most consistently reliable means of operating layouts.

There are five main sections on the scenic part of the layout, one for the main line in and out, one for each of the loops and its associated siding, and one for the loco shed road. I almost did not wire the loco shed road or its point as it is supposed to be out of use but in the end I did as it will probably come in handy.

There are five roads in the fiddle yard, with the track power to the fan of points being part of the main line section.

Points are changed by push-to-make switches, and frog polarity is controlled by Peco motor mounted accessory switches. Track feeds use, where possible, the same connection as the point frog switching wires via a chocbloc connector; this reduces the number of holes needing to be drilled in the baseboard.

I use what I term the 'octopus' method of wiring across board sections. In this method a whole section is wired from the section switch on the panel regardless of how many boards it crosses. This is done by wiring from the section

switch to a common connection in the panel and then out from there via the cable connections to all the relevant boards for that section, giving the control panel the appearance of an octopus with many legs and therefore the name (even though there could be more or less than eight). The exception to this is in the fiddle yard which has one feed from the panel to the first board for points and power and is then wired with a cross board connection from one board to the other for simplicity.

The panel is mounted to the rear of the layout with two bolts and has a single leg which slots in to support it, which also helps with lateral support of the layout.

Power comes from a KPC controller fed, along with the CDU for the points, by a transformer pack on the floor.

Once I finished the wiring I could move on to the part I enjoy the most, the scenery and buildings

I began by painting the track, first with Railmatch sleeper grime from a rattle can and then the rail sides with Humbrol 113 rust.

After that I built up some contours around the entrance to the fiddle yard and along the back of the layout using some of the left-over foam underlay which was then covered in brown wood filler – which actually dries a sort of terracotta colour but is better than white.

The platform was made up from thin card faced with Noch stone sheet and covered in Noch fine sand to represent the track level type common in Austria until swept away by modernisation. The pedestrian crossing outside the station building was added using plasticard. I also glued in some fouling point markers from Ferro-Train to mark a scale 3.5ms between the rails.

I ballasted the track using Noch fine buff ballast.

Summoning all of my GCSE grade E art skills, I painted a mountainous landscape on the backscene using water colours on a base of satin blue for the sky. The results are not quite Bob Ross standard but will do for what I want, and most of it is hidden behind the trees anyway.

Woodland Scenics fine turf of various shades was used for grassy areas, with decaffeinated Earl Grey tea (a better use

Below
ÖGEG 4-6-2T 77 28 departs. This is a Klein Modellbahn product which has handrails and smokebox detail picked out in red as per the prototype.





Above
Diesel railcar 5022 016-9 (Piko) at the platform.

Below
A previous generation of diesel railcar, 5146 205-9 (a Bachmann Liliput model) arrives at St.Pankraz.

Below right
An even older diesel railcar, 5042 08 (Roco).

than drinking it ... it tastes horrible) used for areas which were going to be covered in trees. The road areas were laid using more Noch fine sand. MiniNatur fine tufts were dotted around the layout to give a bit of an unkempt look to the trackwork and roads.

You seldom see layouts set in the autumn, which I think is a shame as it is such a colourful time of year, giving us rich and vibrant yellows, reds, and browns rather than the usual wall of green. With this layout I set out to explore this option, inspired partly by a couple of trips over the Semmering and along the Danube in late October and marvelling at how beautiful it all looked.

The trees were made up from seafoam with foliage from Footpath and Woodland Scenics stuck on with a watery mix of PVA. I used a mix of yellows, reds, and oranges to give the seasonal feeling I was after.

The only tree I did not make was the large maple between the station building and the goods shed. This rather magnificent specimen is from MBR, a Polish company; in the UK they are available from A&H Models of Brackley.

I have yet to work out how to satisfactorily replicate drifts of fallen leaves, some of my attempts being far from adequate. Possibly one of the reasons this season is rarely modelled ...





Above
A SteiermarkBahn Rh2016
'Hercules' diesel (Roco)
arrives with empty wagons
for the wood siding.

Below
The disused loco shed.

Below right
2143 044-3 (Lima) runs in
passing the old loco shed
as a couple of cows look on.

There are only four buildings on the layout and I decided to use the prototypes at Grunau. The loco and goods sheds are my first attempts at scratchbuilding actual prototypes, while the station building and the small hut are laser-cut kits, also a first for me. I like to widen and improve my modelling skills and knowledge with each new layout.

During an internet search for pictures of Grunau I managed to find sufficient photos on various websites to give a good idea of each building's appearance. Google Earth usefully provided a nice aerial view with a class 5047 railcar in the station. Knowing that it was just over 25 metres long gave a useful starting measurement for working out basic floor plan dimensions for each building. These measurements were then used to work out other dimensions from photos, with varying degrees of success.

The loco shed was tackled first, in two parts, the main building and the tower. The basic shells were made with 5mm foamboard walls with the window and door openings cut over-size. These walls and infill pieces for the roofs were pinned using dressmaking pins to hold the basic structure together, the corners also having masking tape to hold the pins in. The outer walls were cut to size from 1mm plasticard, with openings cut to the correct size for the window frames and doors, for which I used some left-overs from a Walthers kit. The window openings were given an extra thickness of plasticard to give more depth to the wall. These were then attached, along with the main roof pieces, to the shells using double-sided tape. The corners of the plasticard were then glued together using liquid poly. The corners on the main part of the real building are raised and adding





these pieces, along with additional strips along the top of the walls below the eaves, made the joints stronger. Further pieces of plasticard glued under the eaves gave the roof its extra thickness and helped to hide any imperfections at the join between the walls and the roof. Window sills are square section Evergreen strip.

The main doors were made using a large factory window from the Walthers kit which, in hindsight, I should have cut it down a bit as the shed is now a bit wider and taller than the prototype. The railings and cage for the fire escape were soldered up from brass wire. The whole building was then given a coat of Halfords white primer before a colour called 'On Deck' from Wilkinsons emulsion match pots was brushed on. This gave the slightly off-white look I was after as well as giving it a bit of texture. The wooden areas were painted using artist's acrylics burnt umber.

The roof tiles are Noch pantile self-adhesive sheets; they are the wrong colour for the prototype but looked good so I decided not to repaint them. The roof vent was made from a piece of plastic tube, the top being one of those plastic screw head covers used on flat pack furniture. The ridge tiles are Plastruct U section, a bit of careful paint mixing blending them in quite well.

The window frames were painted Humbrol 80 'Beige Green' before being added.

The windows were glazed with thin clear sheet fixed in place using Johnsons Clear (Future) floor polish. This is a wax which dries clear and strong and unlike glue does not make a mess of the window if it ends up in the wrong place. Black paper was fitted inside to prevent the windows being see-through. Guttering and down pipes were added from Plastruct.

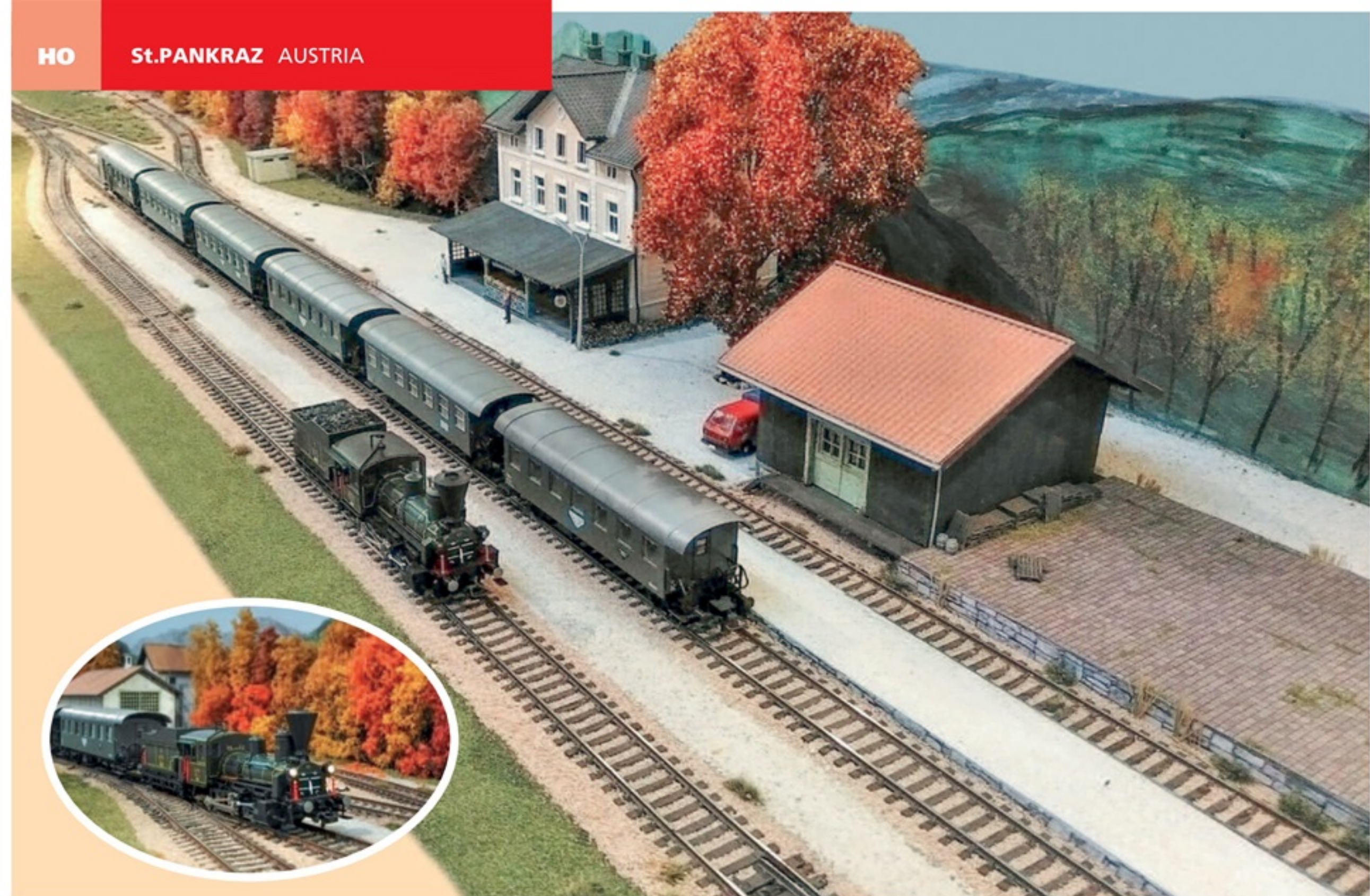
I weathered the building using some very dilute burnt umber, touching the brush to the top of the walls and letting it run down the side to give a streaky appearance.

The goods shed was built using the same basic principles and products as the loco shed. I scored the plasticard wall sections to look like wooden planks before I attached them to the foamboard walls. Again, the building was given a white primer coat then painted burnt umber. Once touch-dry, some areas were rubbed back a bit with a damp cotton bud to allow the white to begin to show though giving an aged weathered look. The same method had been used on the loco shed door. The roof is again Noch pantiles but in this case the colour is correct. The loading dock platform had been built up from the foam underlay sheets and covered with Noch stone and cobble sheets before the ballasting.

Above
2045 07 (Roco) assembling wagons for the wood yard. The goods shed is little used now so it is rare to see a van in the loading dock.

Below
The station building and goods shed. The large maple is by MBR.





Having built the loco and goods sheds I thought that I was going to have to scratchbuild the main station building as well. However this time when working out the dimensions from the photos on the internet I was less confident of my measurements. While looking for some more photos via Google images I scrolled down far enough to find a photograph of a model of the station building. Clicking on this took me to the website of K-B Model, a Czech company which offers a number of laser-cut card kits of Czech/Austrian prototypes, most notably k.k.s.t.B and ONWB, as well as other useful looking buildings. An order went off

and soon I was the recipient of a kit for a standard station building type 16/H of the K.k.s.t.B.

The kit is well made and thoughtfully laid out, all of the joints being hidden behind other pieces such as the cornices and other details.

The instructions in the kit were, of course, in Czech, so I download the German version from the website and then use Google Translate to get them into English. Once I had done this construction was very easy. I also modified it a bit by blanking two doors as per the photos I had of the one at Grunau.

Above

Graz Köflacher Bahn 0-6-0 No.671 with a rail tour using a rake of four-wheel coaches. Classed as an ancient monument in Austria, this is the oldest steam loco still running that has never been officially withdrawn from service. The charming model is by Bachmann Liliput.



Left

2-8-2T 93 1421 approaches the station hauling another ÖGEG railtour.

CONTINENTAL MODELLER



When the basic construction was complete it was given an overall coat of Halfords white primer to seal it prior to the white areas of the finished building being over painted with an artist's acrylic white top coat. Colour applied to the walls was again Wilkinsons match pots using a mix of the 'On Deck' used on the loco shed and one called 'Warm Days' which is an orangey colour. The resultant mix came out a pinkish shade which was a surprise but it still sort of matched the pinkish yellow that the building appeared to be in the photos. This was applied using a Humbrol No.1 brush so the paint could be carefully worked into all the corners without going over the white.

The kit comes as standard with an Eternit style roof which is a diamond pattern tile. I needed the 'Bobrakova' or pantile style which is an alternative you can request at the time of ordering at no extra cost. I did this but still got the Eternit style. Not wishing to mess about complaining, I ordered some more Noch pantile roof from Gaugemaster, luckily finding one close to the colour I needed at a sale price. This was added to the roof and painted to match the real roof. The flashing in the gullies where the roof sections meet was made using chocolate bar foil wrapper, brush painted Humbrol 56 'Aluminium' on the shiny side. Chimneys, barge boards, gutters, drains and ridge tiles, again from U-section Plastruct, finished the main construction. The water fountain is from Ferro-Train. ÖBB signs came from a sheet of decals by T-L Modellbau placed on a piece of plasticard then cut to shape, with the edges painted red. Posters came from a sheet produced by the ARG some years ago. The station name boards were made up on the computer.

The veranda comes as a separate kit which I painted before assembly. My mistake here was not to put a thicker piece of card between the two roof sheets supplied, so it sags a bit.

The railings are an etched kit from Ferro-Train, painted Humbrol 65 'Aircraft Blue', with the flower box added along the top from U-section Plastruct.

When it came to detailing it was important to get the prototype details right as I feel this is where some of the atmosphere of a layout comes from. Most of the railway signal signs are made up using another sheet of decals from T-L Modellbau using plasticard and brass wire, the exception being the wait sign (the large yellow W) which is also Ferro-Train, and were then positioned appropriately. The cows in the field are also Ferro-Train. Vehicles are a mix of Wiking and Kibri; as the wood lorry has appeared on a number of previous layouts it was given a refresh with a coat of paint. There are only a couple of figures on the layout so far, the chap in the red polo shirt (from Modelu) looking particularly familiar. More details and figures will be added as appropriate items are found as I do not want to overdo it and create a cluttered look by adding detail for the sake of it.

Above
2143 044-3 runs in to the platform with a service train.

Inset
The loco shed siding hosts a rare visitor – a *draisine* used by track workers.

Below
The farmer greets the driver of vintage diesel 2045 07 arriving with empty wagons to be loaded with logs.





Operationally the layout could be set any time from the early 1980s to the present day and a variety of stock from each era is available, making a rolling change of period possible. To that end the stock list consists of ÖBB class 5022, 5042, and 5146 railcars as well as some loco hauled stock for passenger workings.

Freight traffic is mostly in the form of timber, heating oil, and general goods. Motive power comes in the form of class 2016, 2043, 2045, 2048, and 2067 diesels. Occasionally for added interest a rail tour will traverse the line, consisting of green four-wheel coaches hauled by a preserved steam locomotive.



Top

The timber lorry is equipped with its own hydraulic crane.

Above

2048 022-4, a former DB V100 (Roco), shunts a tanker of household heating oil.

Left

Loading almost complete in the yard, the Rh2067 shunter arrives to move the wagons.



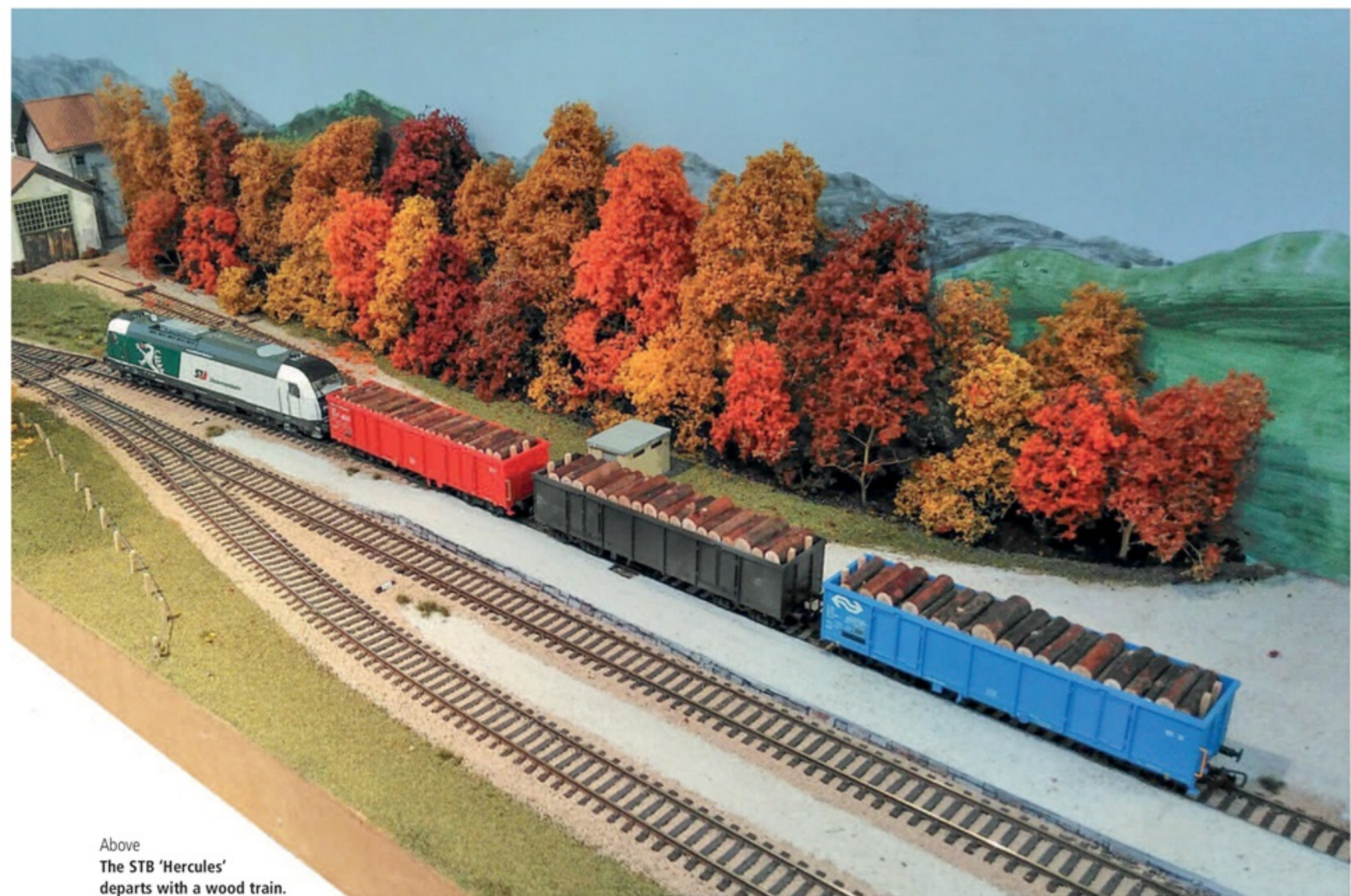
I started layout in mid-2018 with the aim of having it ready for the Austrian Railway Group's 2019 AGM exhibition. Due to circumstances beyond my control this did not happen and as a result it was rescheduled to the 2020 AGM, but this and subsequent bookings were postponed due to the corona virus situation, so at the time of writing the layout has yet to attend a show. It has, however, given me a great deal of pleasure to build and I look forward to when I will be able to present it to the public in person.

I would urge anyone who is interested in the railways of Austria to consider joining the Austrian Railway Group. Details can be found at

www.austrianrailwaygroup.co.uk



Above
 0-6-0D 2067 091-5 (Lima)
 shunts loaded Eanos wagons
 into the loop,
 ready to be collected .



Above
 The STB 'Hercules'
 departs with a wood train.

James Hilton shows how he created a model of an industrial diesel widely used in Hungary.
Drawings and photographs by the author, unless otherwise noted.

Hungarian C50

Designing and building a model of a small loco

As a narrow gauge modeller I am always looking at new ready-to-run locomotives in N and 009 for their potential to be modified into something else. When Bachmann USA announced the range of 'Thomas' themed models, many British modellers rejoiced at the fine running near-scale models of *Skarloey* and *Rheneas*, perfect for adapting to their real counterparts on the Talylyn Railway, *Talylyn* and *Dolgoch*. However, it was sad that *Rusty* the diesel arrived resembling the animated model rather than a real Ruston shunter – but I looked at the potential of re-using the well-engineered chassis under another model.

The Hungarian C50 diesel is a small 50hp locomotive built for both 600mm and 760mm gauges; there are also 950mm and metre gauge examples. Over two hundred were built between 1953 and 1966 at the 'Északi Főműhely' (Hungarian Railways Northern Vehicle Repair Ltd.) in Budapest. Almost all forestry railways in Hungary have one or more of them, which makes it, I think, the most prolific of all narrow gauge types in the country. Another twenty were exported to North Korea.

Above right and right (3)
5727 on shed at Balatonfenyves.

Below

303 in the shed at the Budapest Children's Railway.

Below centre

3788 stored at the Budapest Children's Railway.

Below right

404 in the shed at Gyongyos.

Photos (6): Blair Hobson, September 2011.





It is simple and robust. Powered by a Csepel D413 diesel engine of 50hp (originally 38hp) built under licence from Steyr, with mechanical transmission and a maximum speed of 30km/h, the light axle loading made it suitable for the lightly laid and often poorly maintained track found on forestry and secondary narrow gauge lines across the rural parts of the country. Today many survive operating on the remaining forestry services and tourist lines – for example at Balatonfenyves, Felsőtárkány, Gemenc, and Hortobágy – and it seems these locos will be in use for years.

The engine was the same as used in many lorries, so it was easy to repair them at small workshops.

Some have had the engines totally renewed in recent years, and in some the mechanical transmission was changed for hydraulic.



Above left and above
5733 on the tourist train at Balatonfenyves in May 2003.

Photos (2): Myles Munsey.

Below left and below
5713 on the tourist train at Balatonfenyves, September 2011.

Photos (2): Blair Hobson.



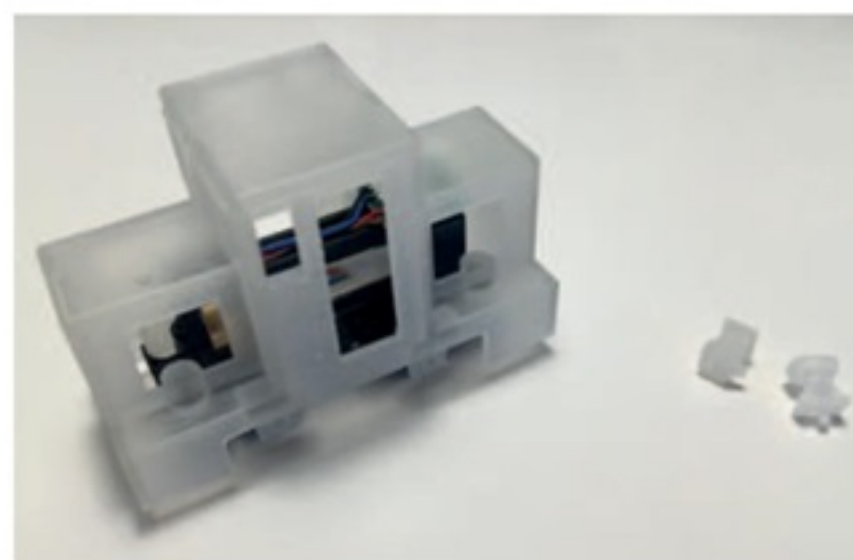
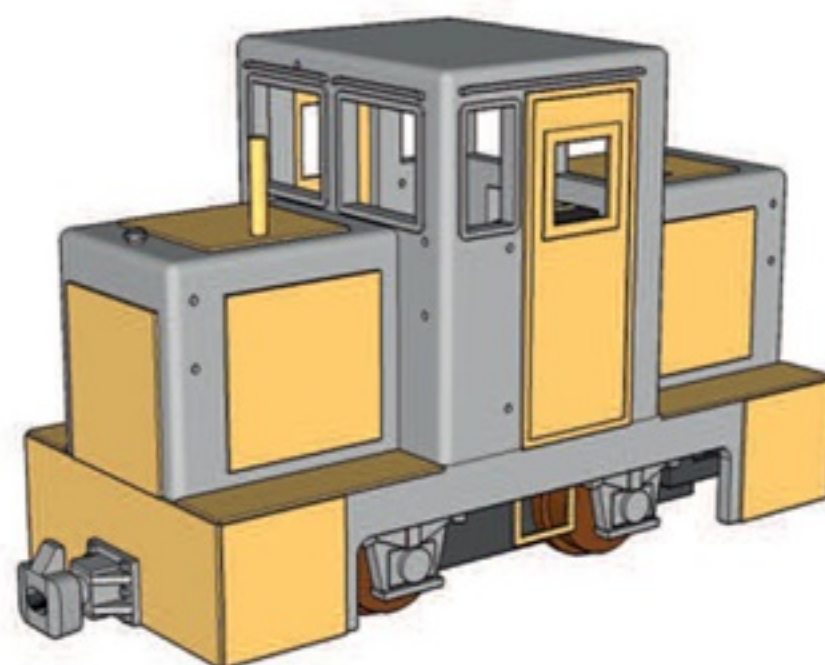
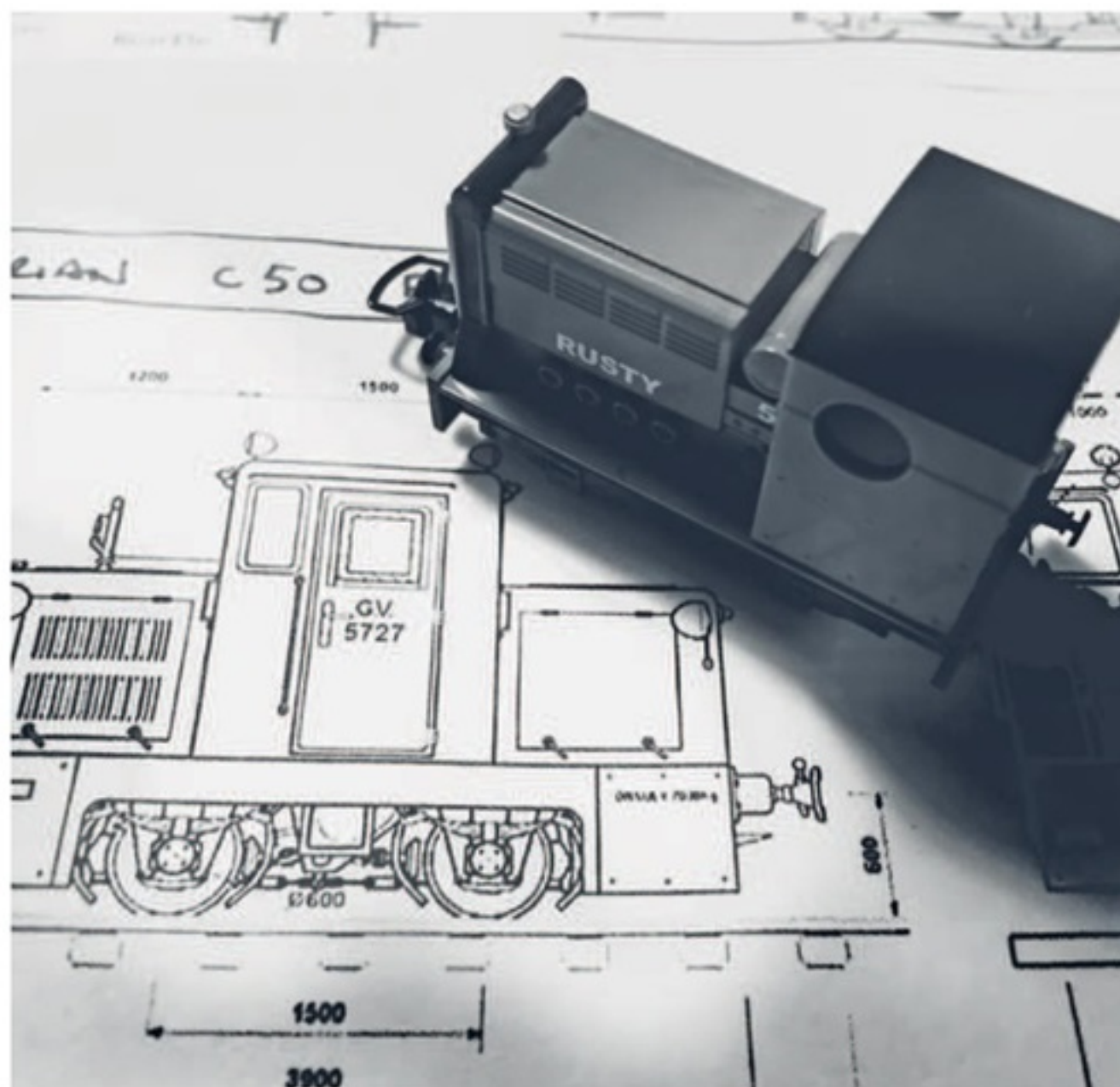
Left
5710 at Balatonfenyves, September 2011.

Right
The cab interior of 5713.

Below
The engine of 5713.

Photos (4): Blair Hobson.





The model

I have a soft spot for Eastern European models and have built a few in the past, as well as designing kits for EuroNG, usually using Minitrains products as a basis. I felt that the Bachmann *Rusty* mechanism offered the chance of modelling the diminutive C50 in 1:76 (OO9 rather than HOe), my preferred scale. The wheelbase is perfect, and the body allows the motor and drive to be completely hidden below the window line.

So I fired up the computer and drew the chassis unit in 3D CAD, and then drew up parts around this for the bodywork. I added the parts I intended to have etched to the 3D CAD, so that the whole model could be visualised and checked

Above

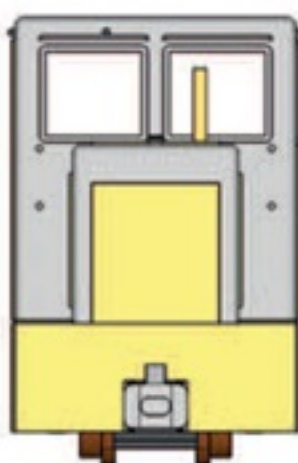
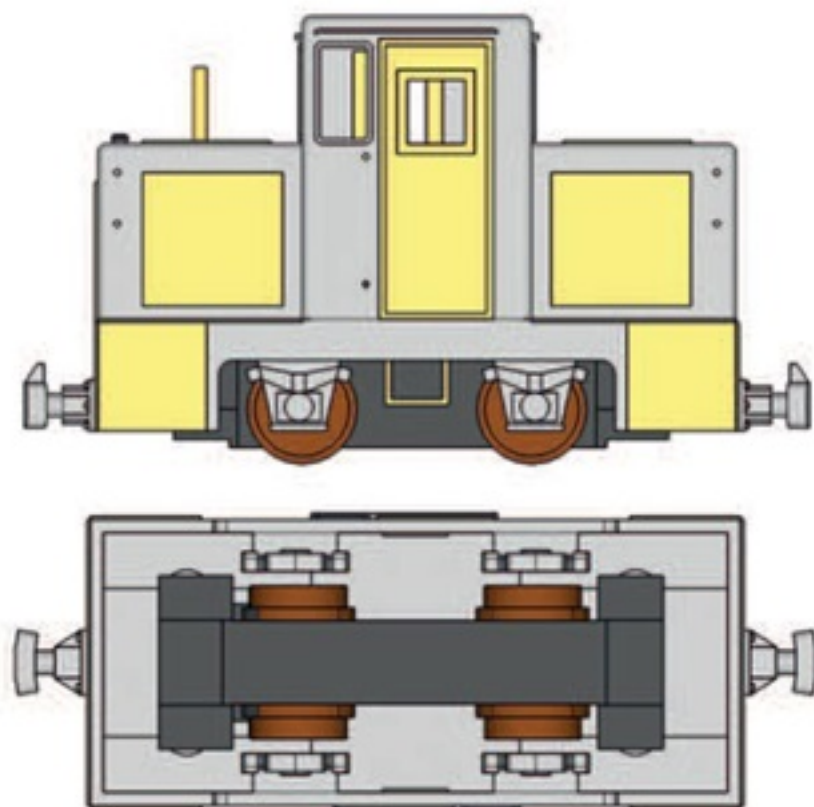
The donor Bachmann model with the prototype drawing, the starting point of the project.

Above right

The 3D-print as delivered is checked for quality and the fit of the Bachmann chassis.

Below

The CAD allows you to confirm the fit before any parts are fabricated.



Right

The etched metal parts fitted to the 3D-printed core. Brass rod and wire was used for various details such as the exhaust pipe, grab rails, and door handles.

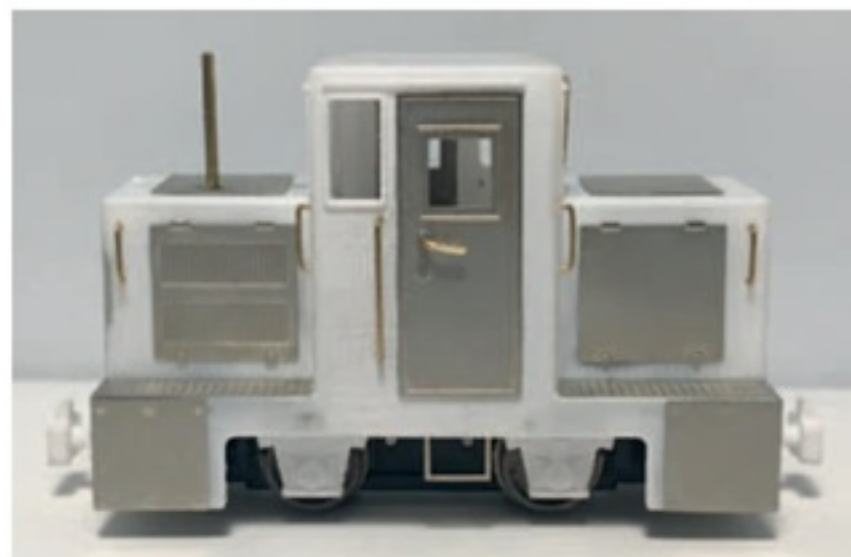
before sourcing the 3D printed core and 2D etched metal is undertaken.

The 3D CAD is used to generate an STL type file that can be printed through services like Shapeways, and I used their Smooth Fine Detail plastic.

The 2D body panels are exported as PDFs into a vector graphics package and detail added to allow the etch artwork to be mastered (preparing both front and back images) and finalised (adding tabs) for the etched metal supplier.

The beauty of modelling this all in the computer first is the knowledge that, at least in theory, it should all work as a model.

I dismantled *Rusty* and ran the chassis in before cleaning up the 3D-printed body shell and checking it fitted as intended. The body uses the same mounting screws as the Bachmann original, and once I had removed a little material from each end of the inside of the print the body was a good fit. It was then removed to allow further work to be undertaken.



Left
The CAD image showing the 3D-print (light grey), etched metal (yellow), and Bachmann chassis (dark grey). At this stage the etched metal parts are not detailed, and show just the size and position.

Right
The assembled model in grey primer.

Right from top (3)
The finished model, painted and lightly weathered.



The next step was to cut each part from the etch carefully, removing the tab with a small needle file and then checking its fit on the model before securing with superglue. I was particularly pleased with how well the etched chequer plate came out for the footplates either side of the bonnet panels.

Once all the parts were added, I re-mounted the body on the chassis to allow the couplers to be fitted at the correct height to match my other stock before the body was primed. This is an important step and allows any blemishes or marks to be addressed before painting.

A simple satin green from the Humbrol range was applied with an airbrush, then I hand painted the red side frames and black underframe. The buffer beams were sprayed yellow, then carefully masked before spraying black to represent the wasp stripes seen on many of the prototypes. The finished model was then very gently weathered to represent a unit in service, based on photos in Paul Engelberts's wonderful book *Forestry Railways in Hungary*.

The model is now available as a kit through the Light Railway Stores (formerly Narrow Planet) marketplace:

www.narrowplanet.co.uk/products/eng-011

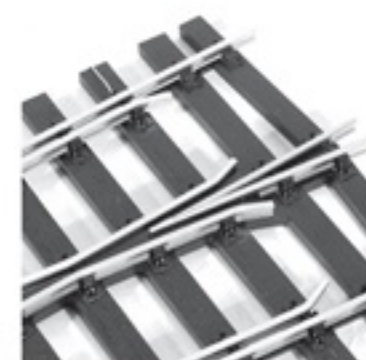
More of my modelmaking can be seen on my blog, updated several times a week:

<http://paxton-road.blogspot.co.uk>



Below and below right
The finished model in service on my forestry layout.





HO

Kirow EDK 750 rail slewing crane new from Roco



The EDK 750 is a six-axle diesel crane built by VEB Schwermaschinenbau Kirow in Leipzig from 1972 and delivered all over the eastern bloc. The drive is diesel-electric, and the telescopic boom is hydraulic. It can be used under horizontally under catenary, in tunnels, and under bridges. The crane can travel up to 100m/min under its own power, and a speed of 100km/h is permitted when being hauled in a train. With the appropriate supports, it can lift loads of up to 125 tons. These crane are often used for railway construction, tasks such as removing and replacing turnouts and track panels, as well as salvaging derailed rail vehicles. The EDK 750 is still in use today by various construction companies and railway administrations, including in Poland and the Czech Republic as well as Austria and Germany. The ÖBB have only one; the DBAG examples were inherited from the DR.

The prototype of the model tested is in the ÖBB (Austrian Federal Railways) colour scheme (ref.73036); it was built in 1975 and is still in use today. The model is also due in yellow for the DBAG (ref.73035). Both versions are also available for operation on three-rail AC track (refs.79036 and 79035 respectively).



The German version is not just a livery change – physical differences include the work lights front and rear (spots rather than tubes), while the work platforms alongside the operator's cab are straight, not tapered.

Roco have recently been delivering the brand new high-tech digital-only model. It has 21 functions, including rotation of the crane body, inclination of the boom, telescopic jib length, and raising/lowering of the hook, along with suitable sound effects. In addition, the model is self-propelled.

The model is very faithful to scale dimensions. While the modern enclosed body and telescoping jib lends itself to a working model, that is not diminish the technical achievement. The jib extension and retraction is by gear wheel and teeth, rather than cable and multiple pulleys.

The model must be operated with a digital controller according to DCC standards. Operating the crane with the Roco Z21 digital control system is even more convenient, working wirelessly using a smart 'phone with the (free) Z21 app which now includes a dedicated 'cab' for the crane. It can also be worked from a PlayStation console connected to the DCC system (either by cable or wi-fi).

Older digital controllers (Motorola protocol) can only access the basic crane functions (F0 to F8).

The box contains the crane, the match truck (carrying outrigger supports), four large and four small support blocks (resembling stacks of timber baulks), the counterweight, the lifting beam, six lifting eyes or buckles (as one etched metal fret, to be assembled), lifting sling material (80cm to be cut as required and threaded through the buckles), and a fine screwdriver (for adjusting the threaded jacks in the ends of the outriggers). U-clips (one large and two small) are provided to fix the lifting beam to

the hook, and the slings to the beam – both types are small, a bit fiddly, a firm fit (as they must be), and no spares are provided. We can envisage them being lost easily.

The bright orange material for the lifting slings looks rather wide (4mm) and thick (c.1mm) but may be reasonably to scale – we are advised they are substantial!

There is also the usual selection of buffer beam accessories and alternative couplings for both crane and match truck – they come with Roco close-couplers fitted and the standard type are also supplied.

Also included is a length of Roco-Line ballast-base track – one long plus one short piece, not electrically connected, despite rail joiners on outer ends – and a pair of end caps to make a display.

The crane and match truck main frames are die-cast metal. The crane body, frame, and bogie side frames are well detailed, with handrails, railings, and steps added, along with the cosmetic hydraulic pistons. Even the cab interior is detailed, with control desk and driver.

The paint finish is excellent, with very clearly printed hazard warning 'wasp stripe' panels and markings, legible (with a magnifier), revealing an overhaul date of 29 February 1992.

Packed into the body of the crane are four tiny geared motors, a microprocessor, and a backup capacitor. The DCC sound decoder is by Zimo.

The body will rotate 360° without a stop thanks to contacts through multiple concentric slip rings around the main pivot.

There is a fifth motor, for movement along the track, in one bogie, driving onto just one pair of wheels, which are fitted with traction tyres. Top speed is a scale 8km/h.

The gearbox can be disengaged (by a manual level under the frame) to allow it to be hauled in a train. Access from the side might be awkward when the crane is on a layout; it is perhaps a shame that this could not have been a digital function, but apparently it is mechanical on the real thing.

The weight (c.436gr, plus 36gr for the counterweight), ensures good adhesion, and good electrical contact. The match truck weighs 42gr.





Both vehicles run on blackened metal wheels, with relatively fine flanges (c.1mm high).

The ensemble is 235.5mm long (crane 136.5mm + match truck 98mm). The minimum recommended radius is 358mm (R2).

The match truck carries the counterweight, the lifting boom, and the outrigger supports.

The hook and pulley block rest in a recess in the counterweight when it is on the match truck for travelling.

The decoder functions are as follows:

- 0 lights on/off
- 1 sound on/off
- 2 turn body OR drive on track, using controller knob
- 3 lift rope winch (stops automatically at top)
- 4 retract boom
- 5 extend boom
- 6 lower rope winch (must be stopped manually)
- 7 lower boom
- 8 lift boom
- 9 half speed (F3 - F8)
- 10 work lights
- 11 rear light off
- 12 short horn
- 13 continuous horn
- 14 mute
- 15 very short horn
- 16 compressor
- 17 "switch ratchets" (only with F1 and whilst moving on track) – seems to be track noise
- 18 go to working position
- 19 go to transport position
- 20 end position shutdown – an automatic stop on the centre line when the body is turned.

The crane has working lights, on the front and rear of the body, and under the end of the jib. In this version, the jib and rear work lights simulate fluorescent tubes, which flicker just like the real things as they come on – a neat touch.

The rear light is blocked by the counterweight, hence the separate off switch F11).



The engine sound seemed loud in comparison with the other noises, but can be adjusted by programming.

All movements have a soft start and stop, and run at a sensible speed, with the half speed function (F9) for even finer control.

The software will allow multiple simultaneous movements.

The one-touch functions (18 and 19) for 'into operation' and 'stow for transport' are very good.

The cables are automatically adjusted as the jib moves – very clever, which saves snarl ups.

We did not test to see how far below track level the hook could be lowered (F6).

The maximum jib elevation is 30°.

The crane will work without the outriggers within 15° of the track centre line.

With the supports in place, it will lift 130g at maximum reach and 300g at the minimum; correspondingly less without the supports and the counter-

weight. The capacity in each position is clearly explained with diagrams in the very informative illustrated instruction booklet, in three languages (German, English, and French).

The major limitation is the amount of manual intervention required between transport and working – engaging the drive, swinging out the outriggers, placing the bases and supports, screwing down the jacks, attaching the lifting beam (with securing clip), attaching the lifting slings (likewise), and mounting the counterweight. This may reflect the reality but it is quite a process on the test track, and might be very awkward on a remote part of the layout. It was no accident that pre-production samples were shown at Nürnberg on a layout in two set scenes – laying track (with the crane mobile) and retrieving a derailed vehicle (with the crane static).

There are interesting and useful videos on YouTube which can be accessed via the manufacturer's website:

In short, this is a very clever bit of kit, ingeniously designed, beautifully engineered, and carefully manufactured, and the performance is amazing. Such quality naturally has a price, but at about double the cost of a typical DCC sound-equipped locomotive it does not seem unreasonable.

Manufactured by

Roco,
Modelleisenbahnen GmbH,
A-5101 Bergheim, Austria.
www.roco.cc

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE ref.73036 £720.00.

BLS car shuttle driving trailer new from Hobbytrain

Recently released under the brand name of Hobbytrain, European models made for Lemke, is a long-awaited new model of the Bern Lötschberg Simplon type BDt driving trailer coach used on the car shuttle trains through the Lötschberg tunnel between Kandersteg and Goppenstein. The usual motive power is an Ae4/4 at the Kandersteg end of the train. At the beginning of the 1990s this type of driving trailer replaced the various ageing type Bti. First, a driving trailer which was no longer needed in regional passenger traffic was converted into BDt 946; three more followed shortly afterwards. They have a large compartment for motor cycles, and some seating was retained for the riders. (Car occupants stay in their vehicles for the 20 minute journey.) In contrast to the newer BDt 942 – 945, however, these coaches do not have any side entrances, which makes loading the motor cycles more difficult – they must go through the (enlarged) end door onto the loading wagon. Different markings have been applied over time – the original yellow BLS lettering was later replaced by the BLS Lötschbergbahn logo (as modelled here), and more recently still by the new BLS logo.

The model should also be available in Epoch VI grey and lime green (ref.H23942, £130.00).



The model is designed to be operated with the BLS eight wagon car carrying train (ref.H23941) (six car transporters and two entry/exit wagons). This reflects the original composition of the trains, which were later extended to fifteen transporters.

The model was announced in 2018, to accompany the car-carrying flats which have been available for some time.



The livery is nicely rendered, with clear distinction between the colours and markings clearly printed.

The one-piece moulded plastic body shell has a ribbed roof and the non-standard wide door at the train end, with the fall plate added.

Other separately added components include the antenna, ventilators, doors, mirrors, buffers, brake hoses, and very fine handrails.

A small packet of spares of some of these parts is included.

The glazing fits flush, and there are internal fittings.

The model comes as standard with interior lighting and working directionally controlled white head and red tail lights. The internal printed circuit board has a Next18 NEM662 socket for a digital decoder.

Current collection is by wipers on the backs of all wheels.

The underframe has the major components (battery boxes, brake system, toilet outfall) moulded in deep relief.

The bogie side frames are well detailed, and carry added door steps (at both ends); there is brake rigging within and brake shoes in line with the wheel treads.

The bogie at the cab end has the skirt attached.

The model runs on blackened metal disc wheels.

A standard coupler is fitted on a close-coupling mount on the frame at the non-cab end, steered by the bogie rotation. There is no provision for a working model coupler at the cab end.

The model is 151mm long, and the minimum recommended radius is 230mm (R2).

A rather specialised vehicle with a limited sphere of operation, but very nicely reproduced.

Manufactured for

Lemke Collection,
Schallbruch 34A,
D-42781 Haan, Germany.
www.lemkecollection.de

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE ref.H23940 £120.00.

East German T170 loader kit new from Auhagen

The T170 self-propelled loading crane was produced in East Germany from the mid-1950s and was used in agriculture and on construction sites. With its own drive it could only move at 3.5km/h, but it could also be towed at up to 20km/h. The 6.3m long jib was made of two tubes and could be raised and lowered with cables. With the 10hp, later 13hp, engine, up to 0.8t could be lifted.

The kit is an exact replica. The body can be rotated 360° on the chassis, which is equipped with 'rubber' tyres and a steerable front axle. A red & white towing bar is included. The jib is variable in inclination, and can optionally be equipped with a simple hook, a bucket grab, or tines. Once assembled, these cannot easily be exchanged. Because these parts are relatively light, the cables are not under tension, which detracts a little from the final effect.

This is an intricate kit with a lot of small parts. There is no flash, and the fit is excellent, as might be expected from Auhagen. For a few small components, such as the headlights, there is no positive location, which is unusual for them.



The instructions (on two folded A3 sheets) are a series of eighteen clear diagrams with the parts numbered; there are drawings of the sprues so they can be identified.

Care must be taken with several sub-assemblies, leaving parts free to move, but eventually the cable lengths are fixed so the jib can no longer be raised or lowered. Some of the holes through which the 'cable' must be threaded are very tight, and will only stand a little easing. The end of the cotton thread may fray with repeated attempts, which also does not help!



The steerable front wheels are a nice touch but the axle assemblies are fiddly! Note the wheel hub should not be glued to the stub axle but retained with the washer - the instructions do not mark this step with a 'do not glue' symbol.

With some dexterity and even more patience, the satisfying result is a very typical DDR machine.

Manufactured by

Auhagen GmbH,
OT Hüttengrund,
D-09496 Marienberg/Erzgebirge,
Germany.
www.auhagen.de

DISTRIBUTED IN THE UK BY
Golden Valley Hobbies
Unit 1, Pontrilas Business Park,
Pontrilas, Herefordshire, HR2 0AZ.
www.goldenvalleyhobbies.com

PRICE ref.41659 £28.70.

Loco-Revue – Hors Serie 76 L'art et la manière de manoeuvrer

by Jean Cuynet

Éditions Loco Revue,
BP 30 104, F-56401 Auray Cedex, France.

www.locorevue.com

297mm x 210mm 100pp

Softback €15.00

ISSN 0024 5739 French text

The title of the latest special from the leading French model railway magazine *Loco-Revue*, dated December 2020, can be translated "the art and method of shunting". It aims to explain why shunting is necessary, and how many specific tasks are achieved.

Beyond the running of trains, shunting is a key element of railway operation – changing a loco (perhaps due to type of traction), re-arranging the train composition (coaches to different destinations), and collecting or dropping off wagons.

So once the layout is 'complete' and the trains are running, some shunting is going to be essential to avoid the same rakes circulating ad infinitum. Operating like the real thing can renew enthusiasm.

Indeed, layouts have been built for the sole purpose of shunting, and can be well suited to limited space, though some railway installations where shunting takes place are far from compact; large marshalling yards are beyond the scope of the collection.

Modellers need to design the track plan to allow for the required shunting, the power supply and switching must be planned in advance (unless digital control is employed), and the track laid well so that slow running is possible.

The book begins with sections on passenger trains – splitting/combining, changing locos, and *autorails* with trailers.

The characteristics of different locations are considered – through stations, junctions, and termini, before a look at some typical industrial sidings, including specialised sites such as a colliery, a dockside, and a military depot.

There is a brief look at the CFTA, a company operating minor lines which you might say was devoted to shunting, with both steam and diesel locos.

A challenge to reproduce (working) in model form would be moving wagons by capstan and cable, not to mention the use of horses or manpower!

Another feature looks at the necessary safeworking and signalling equipment – point levers, stop blocks, and signals.

There is a section on model couplings – the American magnetically operated knuckle type is shown in detail, and there is some mention of digitally controlled devices, but not much on how to get the best from the standard hook & loop.

The collection is rounded off with a short look at shunting locomotives – just a small selection as the choice is huge.

The author is an experienced modeller and a prominent member of the Club Ferroviaire de Franche-Comté (CFFC), a club with a long tradition of excellent layouts such as *La Bosse* (see CM December 2012, January, February, and March 2013) and *La Maurienne* (as featured in a recent *Loco-Revue* special), which are paradoxically mostly 'watching the trains go by' style. But suitable layouts have been found to provide relevant illustrations, either drawn from the author's and publisher's archive or taken for the purpose.

The work is copiously illustrated with photos and diagrams, many specially drawn, others taken from reference sources; other historic documents are reproduced in facsimile. Illustrations of the real thing are in black & white and colour; the model photos, many specially staged on layouts to illustrate particular manoeuvres, are technically competent and clear; all



are in colour and well reproduced on good quality glossy paper.

The presentation is clear and straightforward, with restrained use of different fonts and colours for the chapter titles and subheadings. There are occasional tinted panels with supplementary details, plus a few tips or points of interest highlighted on tinted circles.

Some knowledge of French will be required to get the full benefit but much should be apparent from the strong visual element. Modellers will find noting the authentic small details worthwhile.

The book includes just three pages of advertisements for other LR Presse publications.

Although based on French practice there is much here of general application. The shunting puzzles should keep anyone occupied!

The Railway to Adventure

by Charlie Lewis David Payling

The Ffestiniog and Welsh Highland Railways

Harbour Station, Porthmadog, LL49 9NF.

www.festshop.co.uk

282mm x 217mm 266pp

Hardback £37.50 + £3.95 p&p

ISBN 978 0 901848 25 3

The Adventure referred to in the title is the remote South African hamlet of Avontuur and the railway is the 177 mile 2' gauge line running west from Port Elizabeth, which includes amongst its structures the spindly steel Van Stadens trestle viaduct. For over a hundred years the line served the deciduous fruit growing region known as the Langkloof. Much closer to Port Elizabeth an important branch to Patensie tapped the fertile vegetable farms and citrus groves of the Gamtoos valley. During the 1920s quarries were established near Patensie by the Eastern Province Cement Company (EPCC) and limestone was hauled to Chelsea siding from where a twelve-mile private line ran to the cement factory at New Brighton. This provided the railway's base traffic for over seventy years, remaining on rail even after the fruit was lost to road. This is serious narrow gauge, with long trains of fruit in vans in season and limestone trains of eight well-loaded bogie opens which required banking over the steepest sections. The density of traffic is remarkable, and it required hefty motive power.

Several of the locomotives featured here are now working in the UK and elsewhere in the world, including three NGG16 Garratts on the Welsh Highland Railway – with a NG15 2-8-2 under long term restoration. The work done at Sandstone elsewhere in South Africa should also be noted.

The line was first described in detail in Sydney Moir's seminal book *Twenty-four Inches Apart* (Oakwood Press, 1963). More recently the story has been covered

in eight parts of the authors' *Soul of a Railway* series on line. Since being put on line, there have been many requests for the material to be published in book form and this is the response. It brings the story up to date and focuses on fascinating new information about the railway, particularly the political shenanigans that plagued its turbulent latter years. These are recounted by Charlie Lewis, Regional Engineer responsible for the line from 1981 to 1986 – and one of South Africa's most prolific and accomplished railway photographers.

The book is dedicated to the memory of David Payling (long time supporter of the Ffestiniog and Welsh Highland Railways and author of *Garratts and Kalaharis* and *Fairlie Locomotives of North Wales*), who sadly passed away early in 2020 after a long illness. He instigated the project to produce a modern successor to *Twenty-four Inches Apart* and provided inspiration and much of the material for *Soul of a Railway*.

The names of the authors alone should be recommendation enough to anyone with an interest in the railways of South Africa.

The text is complemented by a fine selection of photos, many previously unpublished and the majority in colour (a considerable number from as early as 1959), forming a definitive record of the railway in its latter years. They include contributions from many well-known photographers. These are augmented by much historic archive material, naturally in black & white. All images are excellently reproduced. Rarely have we seen such a consistently high standard of photography and reproduction.

The work was edited by John Hunt, Dick Manton, and John Middleton.

The format is mostly as a picture album with long and very detailed captions. There are some sections of text, often as chapter or sub-section introductions. In some places the narrative flows from caption to text and back to caption, which can be a bit confusing, and very occasionally there is a page of just text.



There are excellent maps through the work, culminating in a three-page fold-out inside the back cover showing the whole route, with a gradient profile.

The presentation is straightforward, with clear fonts and no use of fancy graphic gimmicks or superfluous colour.

While the locos feature prominently, there is no dedicated technical section (and no drawings).

All proceeds from the book will go to support the FFR/WHR.

Spectacular scenery and steam super power is a winning combination! Wonderful inspiration for modellers – with enough space!



Peco gains export award

Peco was proud to receive the prestigious Devon & Plymouth Chamber of Commerce 'Export Business of the Year' award at the 2020 Business Awards ceremony held on line in December, in recognition for its achievements in exporting products around the world.

Exports represent a significant part of overall sales for Peco. The range of model railway track and accessories can be found across the world and is well known to enthusiasts as reliable, high-quality products. With a resurgence in hobbies generally, business has been going from strength to strength.

With so much focus being placed on growing export sales, it was especially gratifying to receive this recognition, given all the challenges that 2020 has brought, with disruption caused by the virus pandemic and uncertainty for the future due to the unresolved arrangements for business after Brexit. The sales team has worked hard to make sure that the company is in the best possible position as we enter a new phase of trading following departure from the European



Union. Accreditation as an Approved Exporter, and the use of best practices for exporting, puts the company in a strong position for growth in the future. This award is an endorsement for all that has been done so far.

The company is now eligible to enter the British Chambers of Commerce regional awards in 2021.

16mm NGA digital subscription

The Association of 16mm Narrow Gauge Modellers has launched a new digital subscription service.

It is initially being offered as an option for membership outside the UK and continental Europe due to huge increases in postage to overseas locations.

The option to change to a digital subscription for existing overseas members will apply to renewals in 2021.

Digital subscription will be open to all membership categories, including residents of the UK and Europe, as soon as the system changes to support this are complete.

The digital subscription option makes membership affordable and instantly available worldwide.

The Association will also continue to offer all current and future members the traditional option with full colour printed publications.

Members also have access to an extensive online resource which includes downloadable, fully indexed, and searchable digital copies of almost the entire back catalogue of magazines and special publications produced since the Association started, with new editions added at the same time as the hard copy is delivered.

Non-digital subscription rates in the UK and continental Europe are not expected to change for 2021.

www.16mm.org.uk

Chrezo may continue MTH O stock

Philip Morgan, whose garden and garage layout was featured in our August and September 2020 editions, reports that French O gauge specialist Chrezo Modélisme Ferroviaire has been offered the opportunity to acquire the tooling for the CIWL and OCEM-RA coaches produced by MTH (from 2009 and 2012 respectively) when the owner of that concern retires in spring 2021.

Before committing to such a major investment, Chrezo would like to get an idea of the level of possible demand and are asking interested modellers to contact them (without obligation).

<http://chrezo.fr>



New from Cités Miniatures



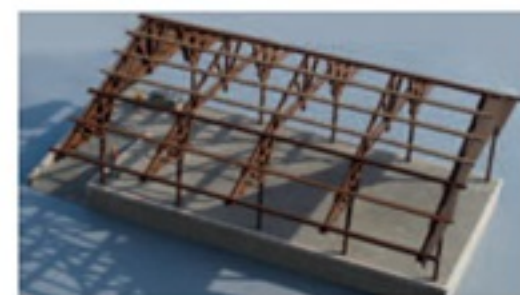
A covered goods platform has been added to the HO range of railway buildings. This kit was inspired by structures built before the war on the PLM network (at Toulon, Nice, Cannes, etc.), most of which were demolished in the 1960s and 1970s. However, a few are still standing today, often having been heavily modified and converted in the 1970s and 1980s to serve Sernam and Calberson, but are now dormant.

It is this later configuration, with a fibre cement/eternit roof, that the kit represents (ref.BF-008-HO).

An open goods dock is also available:



New in N is a low relief factory (ref.BV-018-N). This model was developed three years ago as part of specific layout project but due to lack of time the kit was never made into a commercial item and included in the range.



Following a request from a modeller who saw the project featured recently in *Loco-Revue*, the kit is finally in the catalogue, with two options in terms of depth: 20mm (backdrop) or 50mm (half relief).

Cités Miniatures
145 Avenue Henri Ginoux,
F-92120 Montrouge,
France.
www.cites-miniatures.com



South African electrics in HO



Richard Bevan writes:

It was nice to see an article on a South African subject (The Blue Train) in the January 2021 edition. Having worked for GEC Traction since 1978, I have a personal interest in South African Railways as I worked on many contracts in SA, from 6E1 to 10E1 locos.

I have many Lima HO SAR models. The Class 34 diesel (seen in the article) is very good but the 5E electric is not so accurate, as I found when trying to convert a body shell to a 6E1.

Thankfully a fellow enthusiast in New Zealand, after a few e-mails and the

supply of drawings and photographs, came to the rescue and produced a very accurate 3D-printed body shell and bogie frames for the particular 6E1 that I wanted for my layout.

He now has 3D files for all the various 5E, 5E1, 6E, and 6E1 designs.

I think 3D-printing is the way forward for producing models that are not covered by the main manufacturers.

The photos show my 6E1 before and after painting. It is my first attempt at painting a loco so hopefully my next attempt on the second shell will be better.

New in Nm from AB-Modell



Before the turn of the year, Nm specialist AB-Modell delivered their fourth and last version of the Rhätische Bahn Ge4/4II loco in advertising livery. The model has been elaborately painted and decorated to replicate the prototype as faithfully as possible. Since December 2018, No.616 has carried advertising for the restoration of the first steam locomotive on what was then known as the Landquart – Davos Railway.

The 1:160 scale 6.5mm gauge model is equipped with a Faulhaber motor and a large flywheel. It has a Zimo MX616

decoder for optimum driving characteristics even on analogue layouts. The milled brass chassis provides enough weight to haul appropriate trains.

Free-standing handrails, additional front sockets, with contrasting black handles, enhance the body and stand up to comparison with larger scales. All windows are individually glued in from the inside to fit flush. The corner windows are made of rounded foil, so that the magnifying glass effect often visible in large-scale products is completely absent.



A few of this strictly limited edition model are still available to order; the price is €874.00 and delivery time approximately twelve months.

For each model sold AB-Modell will donate 50.00 SwFr to the reconstruction fund for Rhätia.

Announced for the beginning of 2021 is the BDT low-floor driving trailer of the 1750 series, which was procured by the RhB from 1999. The price is €319.00.

The nickel-silver body replicates the modern appearance with a high snowplough and wide light grey trim lines. Raised features include door opening push buttons, hooks for destination boards, etc., along with free-standing handrails, multiple unit sockets, and connecting hoses.

The front carries a cosmetic buffer. The usual Micro-Trains coupler is mounted on the back.

The customer has a choice between two running numbers and six train destination displays. Other numbers and destinations can be created on request.

In addition, seated passengers and/or a driver can be installed (at extra cost) to individualise the model.

Enthusiasts who like to build themselves can order the model as a kit (€119.00); delivery is expected in the second quarter, after preparation of the usual very detailed instructions.

AB-Modell

Anja Bange Modellbau

Im Stuckenhahn 6,

D-58769 Nachrodt, Germany.

www.n-schmalspur.de

Latest from American Z Line

AZL are pleased to announce a new body style, in the form of the R-70-20 reefer. The first road name to be offered is Great Northern, available as a four-pack, two-pack, and singly.

These cars were built by Pacific Car and Foundry in Benton, WA. The designation indicates a reefer with 70 ton capacity, and the twentieth design in the builder's series.

The cars are a direct derivative of the PFE reefer design. They represent the last major release of this design of cars. In 1969, 1,200 were delivered, with another 1,200 in 1970. Initially they carried Pacific Fruit Express lettering.

Alco RS3 – GN

Great Northern prototype details include: Phase I body, corner stacked number boards, dual bulb headlight, three chime air horn behind the cab, transverse exhaust, 800 gallon fuel tank, and AAR trucks. There is a choice of three numbers: 221, 222, and 223.

Alco RS3 – D&RGW

Denver & Rio Grande Western prototype details include: Phase I body, body mounted number boards, dual bulb headlight, one single and three chime air horns, lengthwise exhaust, 1,400 gallon fuel tank, and AAR trucks. Three numbers are available: 5202, 5203, and 5204.

The RS3 models feature a 7.5mm coreless motor, dual flywheels, traction tyres, built in pilot, directional LED lights, and body mounted couplers. The coupler box is screwed in place.

EMD GP38-2 – ATSF

The GP38-2s are back! The body shells are from the first run, but the chassis is the improved 2020 version. There is a choice of four numbers – 2372, 2374, 2379, and 2380.



Over the years, the cars were sold off and repainted for new owners.

The 1980s saw the biggest change as the Southern Pacific and Union Pacific split the PFE operations. The SP retained the trade name and their cars were let-

tered SPFE. The UP chose to completely repaint their cars in the armor yellow colour scheme. Later they became all white after the SP and UP merged in 1996.

The second largest operator of these

cars was the BNSF. The BNSF would build their collection through the absorption of roads such as the Burlington Northern and Great Northern.

These cars are still in current use.



The models have a coreless motor, dual flywheels, traction tyres, standard DCC-ready PCB board, directional LED lights, and AutoLatch™ couplers.

The Gunderson MAXI-I five car articulated sets now come with Southern Pacific reporting marks, loaded with five Capital 40' containers, with a choice of three road numbers.



The packaging includes slots for ten 40' containers. The cars feature a metal chassis with etched metal details and add-on parts, roller bearing trucks, and AutoLatch™ couplers. <http://ztrackdistribution.com>

New HO from Halling



New edition of Vienna C1 tram and c1 trailer

After bad experiences with the high capacity C-c sets, the Viennese public transport company decided to order 58 sets of a DÜWAG design which SGP built under license between 1955 and 1959. The trams were numbered 102 – 159. The trailers have been renumbered and revised several times. They were originally numbered 1202 – 1259, later 1502 – 1559, and finally 1802 – 1855.

The vehicles are linked with a Scharfenberg coupling and so cannot be used with other types.

The first use in regular service was on 11th September 1955 on line 46 from Ottakring station. They finished their service life in Vienna on the same line on 20th December 1996.

In 1971, tram 102 was equipped with the equipment to tow a c1 trailer without a conductor – numerous modifications and improvements were made for this. In addition to some changes in the driver's cab, indicators, door lights and line signals were fitted. Based on this example, a further 31 were converted between 1971 and 1973.



The C1-c1 sets have been used on lines D, J, 10, 29, 45, 45/46, 46, 49, and 71 throughout their lives, as well as being used as reinforcements for large events on line 8. C1s were also used alone on line 42 from 1976 to 1980.

With the new edition of the models, care was taken to ensure that they were delivered in compatible sets. For the first time, however, you can also choose the exact period of the exterior! Thanks to a small set of decals, you can decide in which operating condition the vehicles should be reproduced. The high quality decals also enable operation on different lines.

The motorised models of the C1 are now factory-fitted with a NEM651 socket for decoder.

An accessory kit is available to convert the model with directional lights.

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Work trams are special vehicles that are not used for passenger or freight transport but are reserved for maintenance and repairs of the systems. Often converted vehicles are used, which due to their age or design are no longer suitable for use in regular traffic.

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Mini World Côte d'Azur to open

Four years after the inauguration of its first display at Carré de Soie in the Lyon metropolitan area, Mini World is opening a second exhibition of animated miniatures near Toulon, within the shopping and leisure centre of Avenue 83, located in La Valette-du-Var.

The opening was initially scheduled for October 2020 but was delayed by the health crisis. On 3rd February the site should finally be able to welcome its first visitors.

It is a reproduction in 1:87 of the main places of the French Riviera, from Marseille to Nice. The Old Port, the Calanques, the Promenade des Anglais, the Esterel massif, Saint-Tropez, and even Toulon harbour, in c.2,000m².

As in Lyon, humour and enchantment are the order of the day, with tens of thousands of scenes from life, hundreds of vehicles and trains traveling autonomously, all with spectacular and entirely new scenes reproducing the landscapes of Provence and the French Riviera by day and by night using projections, lights, and sound, and even scent.

This new display features new animation techniques never seen before. One



of these reproduces an animated sea with moving ships, while another major attraction is the A8 motorway overlooking the city of Nice; it has dozens of vehicles running independently without any battery or motor.

Visitors will trigger many of the effects themselves using buttons installed throughout the display.

A team of fifteen builders worked for eighteen months in the Lyon workshops then on site in La Valette-du-Var to bring together and finalise the gigantic jigsaw. 70,000 hours of work were needed to bring all this to life.

www.miniworldcotedazur.com

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An impressive recreation of scenes on the Rhätische Bahn in HOm by three talented modellers in The Netherlands.

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Roberto Macchi shows how to fit a digital sound decoder and the necessary loudspeaker in the very limited space available in the diminutive Hornby Rivarossi HO model.

• Canal lock construction

The MTD Treinenclub depicted a novel scene in N as an entry for a competition with unusual conditions – the micro layout had to be built inside a suitcase!

• Fort Whiting

A small OO9 layout built to display a growing collection of colonial narrow gauge locomotives and rolling stock by the prolific Charles Insley.

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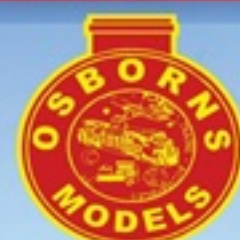
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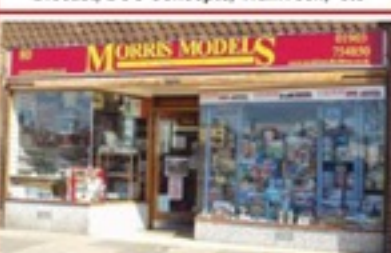
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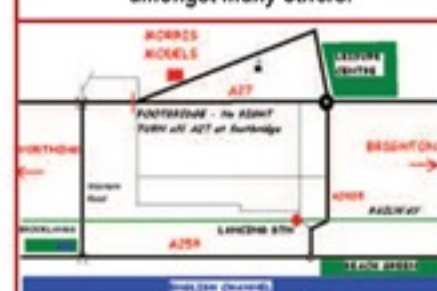


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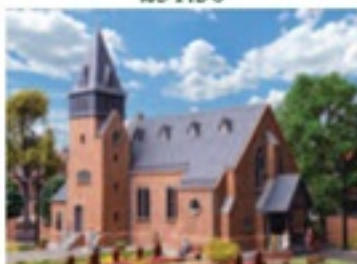
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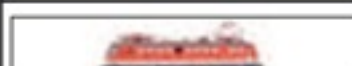
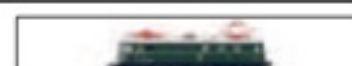
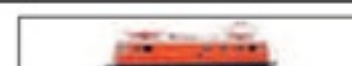
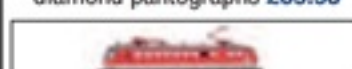
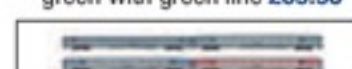
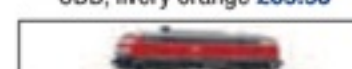
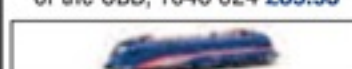
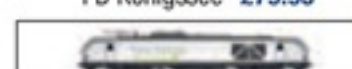
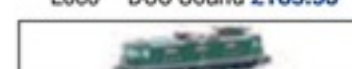
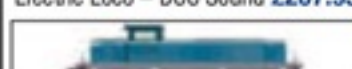
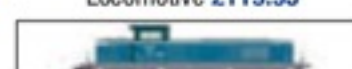
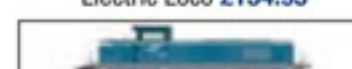
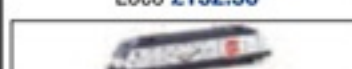
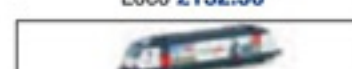
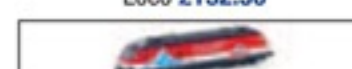
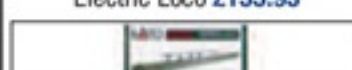
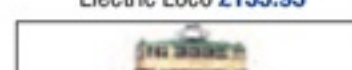
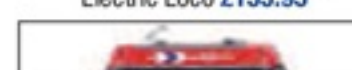
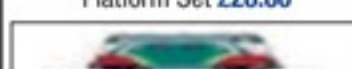
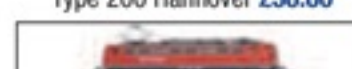
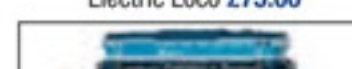
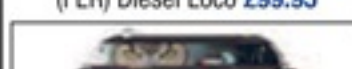
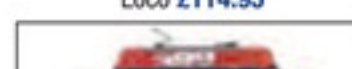
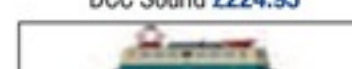
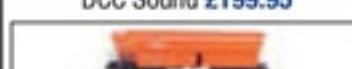
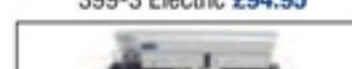
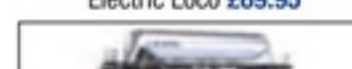
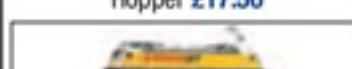
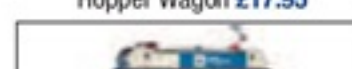
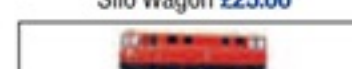
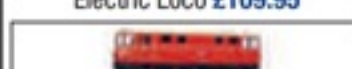
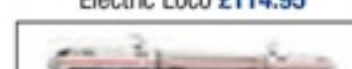
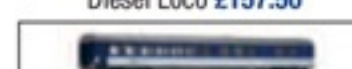
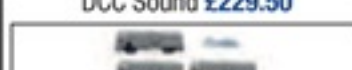
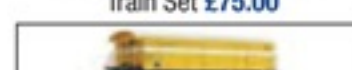
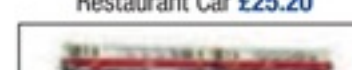
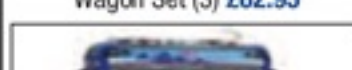
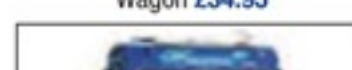
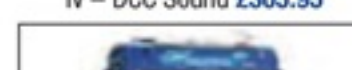
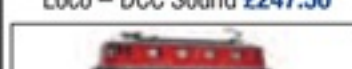

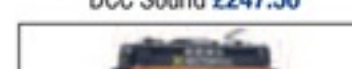
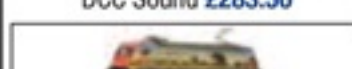
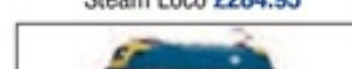
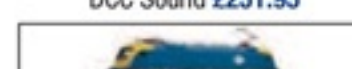
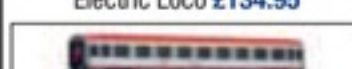
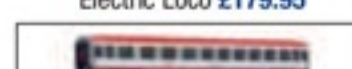
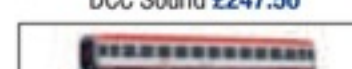
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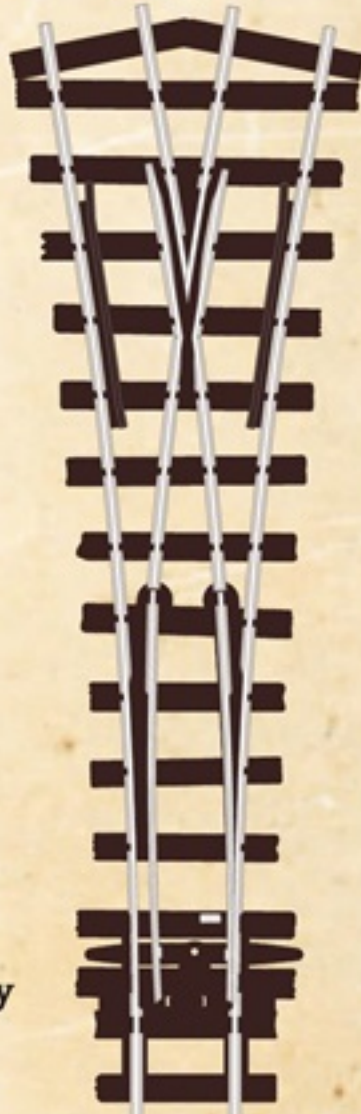
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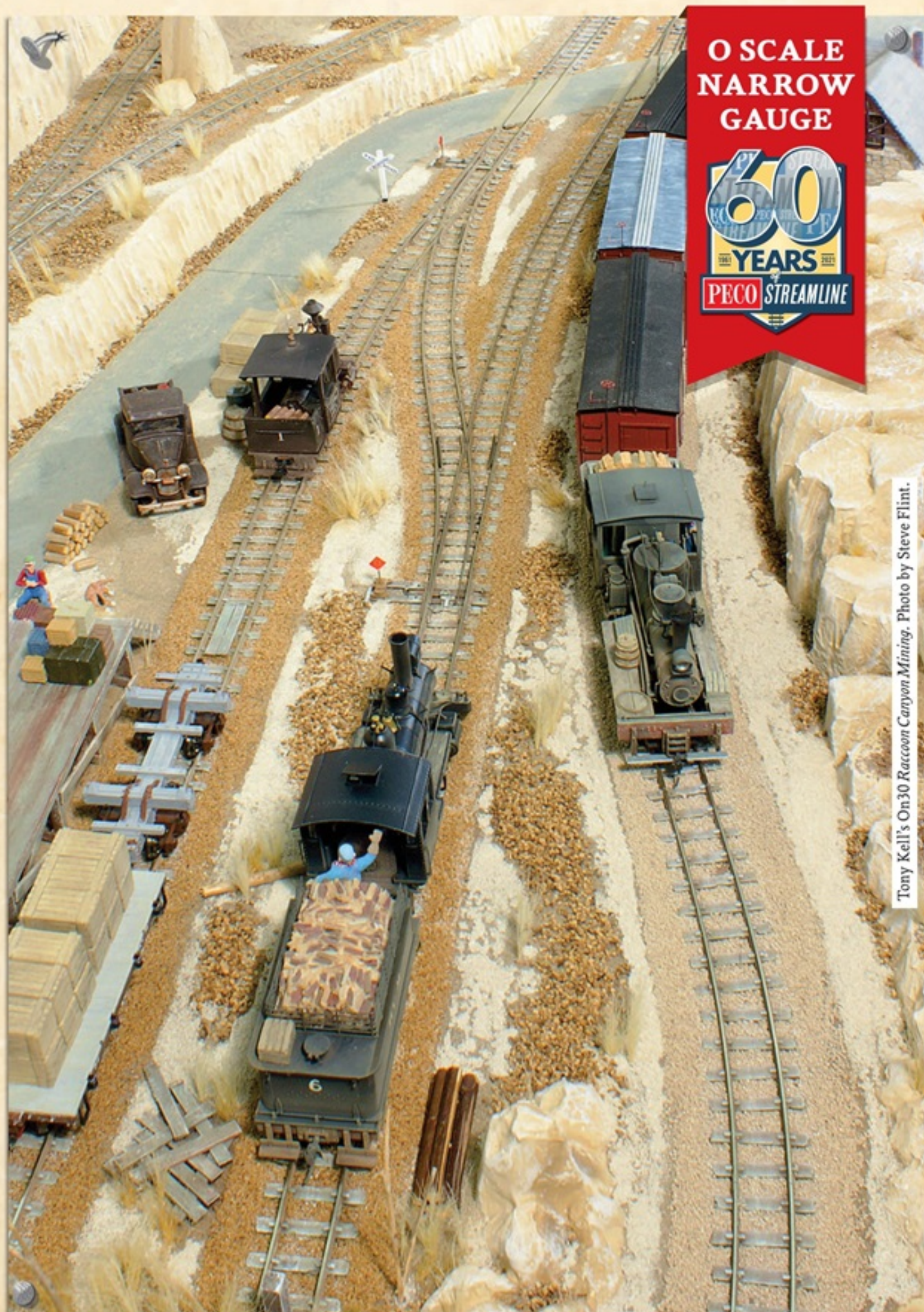


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74511 DR 2nd, orange/cream	£20.00	£23.00
14028.1 DRG 2nd 4th green	£10.00	£3.50
14027 DRG 4 x 4 coach set, green	£60.00	£40.00
75126 DRG Railbus, cream, epl	£60.00	£30.00
14015 SBR D Deck SZU x 2, epl	£60.00	£34.00
14013 Special Plankton IV coach set, green	£20.00	
74168 CSO 3 x 4 coach set, epl	New £55.00	£40.00
75419 CSO KRP 4th tanker, epl	New £22.00	£15.00
75309 DB ESSO 4th tanker, silver, epl	£15.00	
75219 DB Refrig van, Kaiser Friedrich, yellow	£10.00	
15181 DB Refrig Bogie tanker, grey	£40.00	£15.00
17887 DB long Bogie Interregio van, blue/white/weather	£18.00	
76131 DB Interregio transport wagon (one wheel set)	£7.00	
15189 DBAG Tamsa Rolltop bogie open, brown	£10.00	£5.00
76375 DR 4th BR/US zone G101 van, brown	£3.00	
15183 DRG Homann 4th tanker + brake hnd, yellow	£10.00	
15026 DRG Refrig van, Mtkani-Waren van, white, epl	£3.00	
16264 DRG Homann van, yellow	£3.00	
76281 DRG Landkron Beer van, white, epl	£12.00	
76435 DRG Parcel van, green	£12.00	
15044 DRG beer tub Kalk wagon, brown	£3.00	
76358 SBR UETIKOW Acid Carboxy wagon	£12.00	

USA HO ACCURAIL

2554 C-O USRA SFT Hopper	£7.00
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8061 CONRAIL SD-50 #8609, blue/weather	£38.00
8049 NS S200 #8517, black	£45.00
16474 Penn Central SD40, black #8277 (DC fitted)	£52.00
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ATLAS

8702 SF Alco S-2 #2352, blue/yellow	£35.00
8828 UP Alco S-3 #1250, yellow	£35.00
8751 MLW Rd S-4 #817, orange/black	£35.00
15973 Southern MP-150C #2408, black	£50.00

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8071 N-W C-25 #1005, black	£38.00
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New Haven RSD #501, custom paint, orange/black	£45.00
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7311 L+N Ge U30C #1557, grey	£35.00
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20000347 B-M Evans Gondola #9001, black/weather	£10.00
1600 B-O GSC Pulwood Flat, open ends, wood load	£10.00
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13151-1 Chicago Central #199049 St Cupola Caboose, black/yellow	£16.00
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1616-1 ILL Cent Gulf Flat #101367, wood load	£14.00
1849-2 JC PS-2 Cov Hopper #846, grey	£10.00
11356 JC TM Cupola Caboose, #91539, red/white	£12.00
20002717 Karlsruhe beer 40' Wood Reefer #3215, yellow	£15.00

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1606-2 SF GSC Pulwood Flat #92995, wood load	£14.00
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BACHMANN

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18001 UP One car, brown	£8.00

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1324 Austin + Texas Central LH Alco RSD-15 #442	£150.00
082 NYC JE 4-6-4 #5395, Big Four, (DCC sound)	£178.00
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1473 GACX 53'18" Wood Express Refrig car MKT 2 car set, Pullman, green	£34.00
1473 GACX 53'18" Wood Express Refrig car MKT 2 car set, Pullman, green	£34.00
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5018 C-O 1-2-10-4 #5023 (DC sound/DCC ready)	£148.00

BROADWAY BLUELINE: ALL DCC CHIP FITTED 50 DC/DC SOUND

5080 AT SF #815 RSD-15 low hood Zebra stripe	£135.00
5159 AT&SF 4-8-4 #3552 (DC Sound)	£136.00
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P/M30223 B-O C-16 Express van, blue #1880 Wagon Top/Flat door	£20.00
P/M30234 B-O C-16 Express van, blue #1884 Wagon Top/Flat door	£20.00
P/M30230 B-O C-16 Express van, blue #1918 Wagon Top/Flat door	£20.00

GENESIS

G5401 BNSF SD-10MAC #8887, Heritage R, orange, DCC/Sound	£125.00
G5012 CN USRA 2-8-2	£75.00
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INC

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R40-2 Annular Woodside Reefer Budweiser, white/graphics	£18.00
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PROTO 1000

30298 NP RS11 #9116, green/yellow (DCC sound)	£85.00
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920-32001 AT&SF 2-8-2 #1734 (Sound)	£196.00
30029 Virginia #508 USRA 2-8-4	£135.00

PROTO 2000 (BLUE BOX)

8106 AT-SF GP26 #3167 DCC fitted, blue/yellow	£48.00
30528 BN GP18 #1993 DCC fitted, green	£48.00
30545 BNSF #6702 GP40, blue/yellow	£35.00
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23723 GM-O #661 S1, purple/gray	£45.00
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 MKN Digital/Marklin Repairs19a
www.mkn-digital-train-repairs.com
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www.morris-models.co.uk
 Mount Tabor Models, Matlock22a
 email: mount.tabor@btopenworld.com

N

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www.nmrabr.org.uk
 N Scale CH18a
www.nscalech.com

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www.osbornsmodels.com

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R

Rails of Sheffield24a & 25a
www.railsofshffield.com
 Rowsley MRG18a
 email: aak2008@hotmail.co.uk

S

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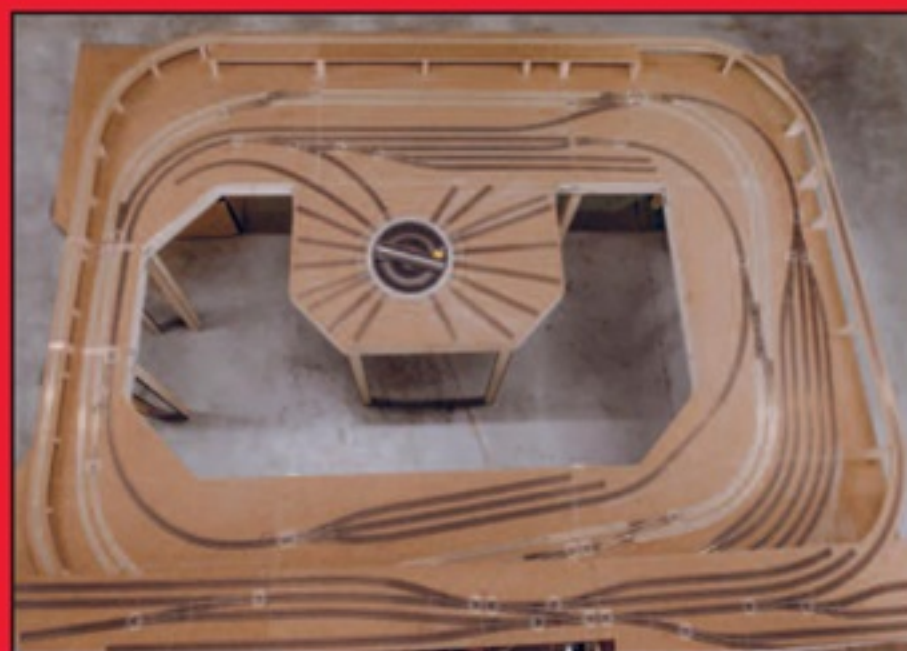
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Offering great value for money, PIKO is definitely the range to look at for beginners to German railways and existing collectors alike, with a variety of starter sets on offer.



PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors and is great value for money. The locomotives feature directional lighting, and some even have sound and smoke. Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert. The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology. The HO Scale A-Track has clear, simple geometry and a realistic appearance.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train. A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale. New modellers should consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits. The locomotives run smoothly, with accurately printed liveries and DCC compatibility. This range includes many passenger multiple units that run around Europe, not just in Germany. This allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.

