

# CONTINENTAL MODELLER

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FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

## RIMKOV

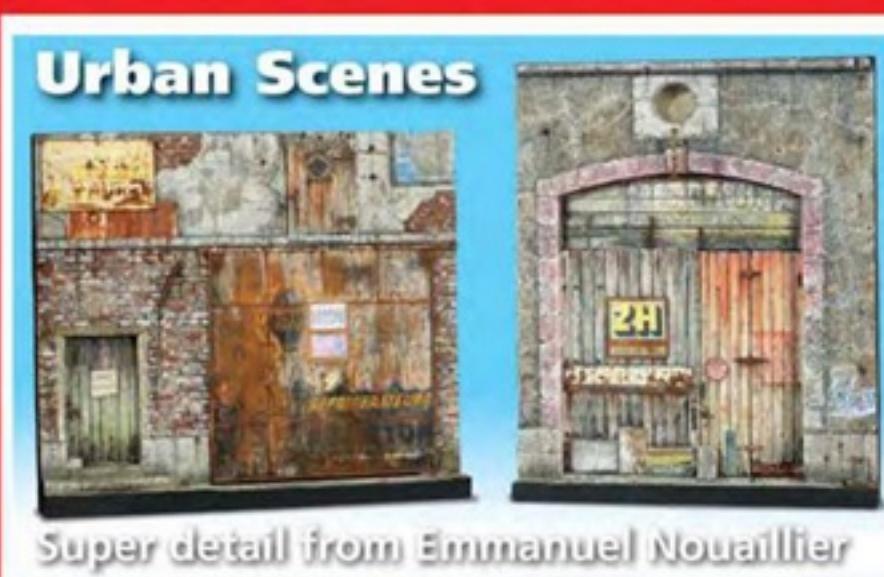
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German Gauge 1



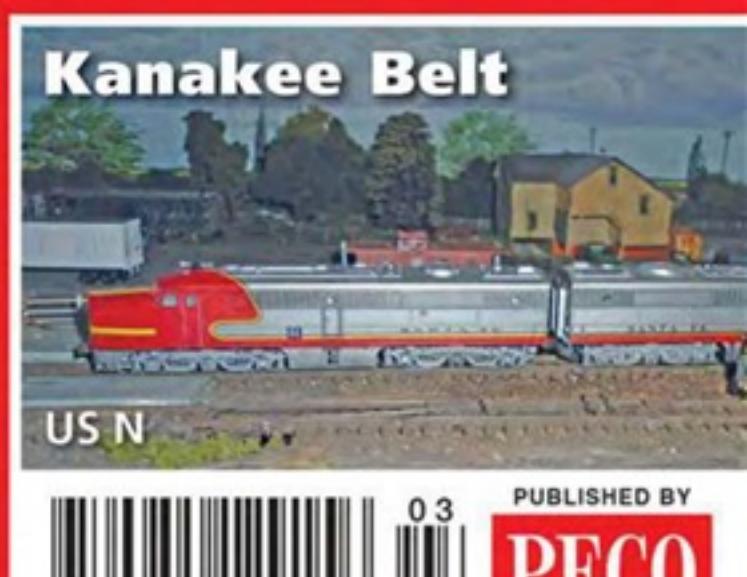
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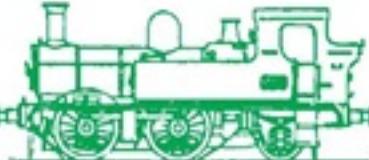


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# CONTINENTAL MODELLER

March 2020  
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Number 3

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## RAILWAY MODELLER

For all modelling British railway practice.  
Published on the second Thursday  
of the preceding month.



**Cover**  
RIMKOV GmbH – a Gauge 1 model of a static testing station for steam locomotives created for a contest by Rik Martens and Koen Vermeulen.  
Photo: editor.

## 2020 vision

We have just returned from our annual visit to the major international toy and hobby trade fair in Nürnberg. Our comprehensive report will begin with HO in the April issue, and continue with other scales and gauges in the May edition.

Some manufacturers, large and small, have already posted their plans on the internet, which poses the question of whether a trip to the trade fair is worth while for dealers, importers, and the press. On balance we believe it still is – published plans are not always complete when posted, and things will be announced and revealed that were not ready earlier. It is also useful to try and determine, both by carefully inspecting samples on display and enquiring of the representatives, which projects are at what stage – some may be almost ready, while others may well only be suggestions put out to gauge potential.

### **Modelltrein Expo OntraXS! in Utrecht, The Netherlands**

This popular exhibition will take place once again between Friday 13th and Sunday 15th March at the Spoorwegmuseum (Dutch National Railway Museum) in Utrecht. This impressive institution combines a significant collection of historic artefacts, large and small, from locomotives to archive documents, with modern styles of interactive presentation that have made it appeal to a wider public. The annual model railway exhibition manages to achieve a similar balance between the potentially competing interests of dedicated enthusiasts and general visitors, particularly families. Some of the best quality layouts, of different sizes and subjects, from all over The Netherlands and well beyond, will be displayed amongst the real rolling stock, and there will be some serious modelling demonstrations and workshops alongside various activities for children. Last year these were sensibly concentrated in a separate area, which seemed to work well.

In addition some major manufacturers will be represented as well as some specialist suppliers, though (perhaps surprisingly given the marketing thrust) there are no general retailers, and no second-hand stalls. For that kind of event in The Netherlands, and indeed also in or near Utrecht, RAIL (February) and Eurospos (October) are already well established – and worth attending.

The layouts are due to include *Palmoliebaan (Keretapi Minyak Sawit)* by Henk Wust, *Braubach* by Walter Smit, *Franzburger Heidebahn* by Franz Rittig of MIBA, *Gbf.Unterhaltung* by Vincent Scholtze, *Lichtenhain* by Ton Janssen, *Station Heide* by Yannick de Vinck, *Lidanfurth* by Hans Konink, *Straßberg* by Sebastian Schmidt, *Distelbach Ost* by Alexander Losch, *Den Overslag* by Frans Peeters, *Moulin Margot* by Claude Fandel, *Le Trevois* by Patrice Hamm, *Hoch über dem Fluss Albula* by Daniel Schoeren, *Ett stykke af Odsherreds* (Denmark) by Steef van der Straeten, *Station Zielona* (Poland) by Marcin Turko, *Leverda-Pairau* by the Escadrille Saint Michel, *La carrière à Baurion* by Bernard Junk, *Ga Long Bien* by Thomas Schmid (CM September 2019), *Achtung! Feldbahn kreuzt* by Marcel Ackle, *Canada's Finest* by Evan Daes, *The Escondido Row* by Elmar Manfroid, *York Rail* by Denis Arafut, *A piece of American countryside* by Christian Collet, and *South Creek* by Kim Marsh.

Some of the modellers will be familiar to regular readers, but most seem to be presenting new layouts here. The scales and gauges and subjects are, unfortunately, not specified, but we are sure the variety and quality will be well up to the accustomed standard. As in previous years, CM 'roving reporters' will be attending the exhibition, so you can expect to see articles about some of the layouts shown there in future editions. Indeed, several of those found there have gone on to win the CM Cup, which must be some measure of the standard, and underlines the appeal of the event to enthusiasts.

For further details, see *Exhibition Diary* (p.208).

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Jan Martens has created a convincing H0m impression  
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### 145 SELECTED EXAMPLES

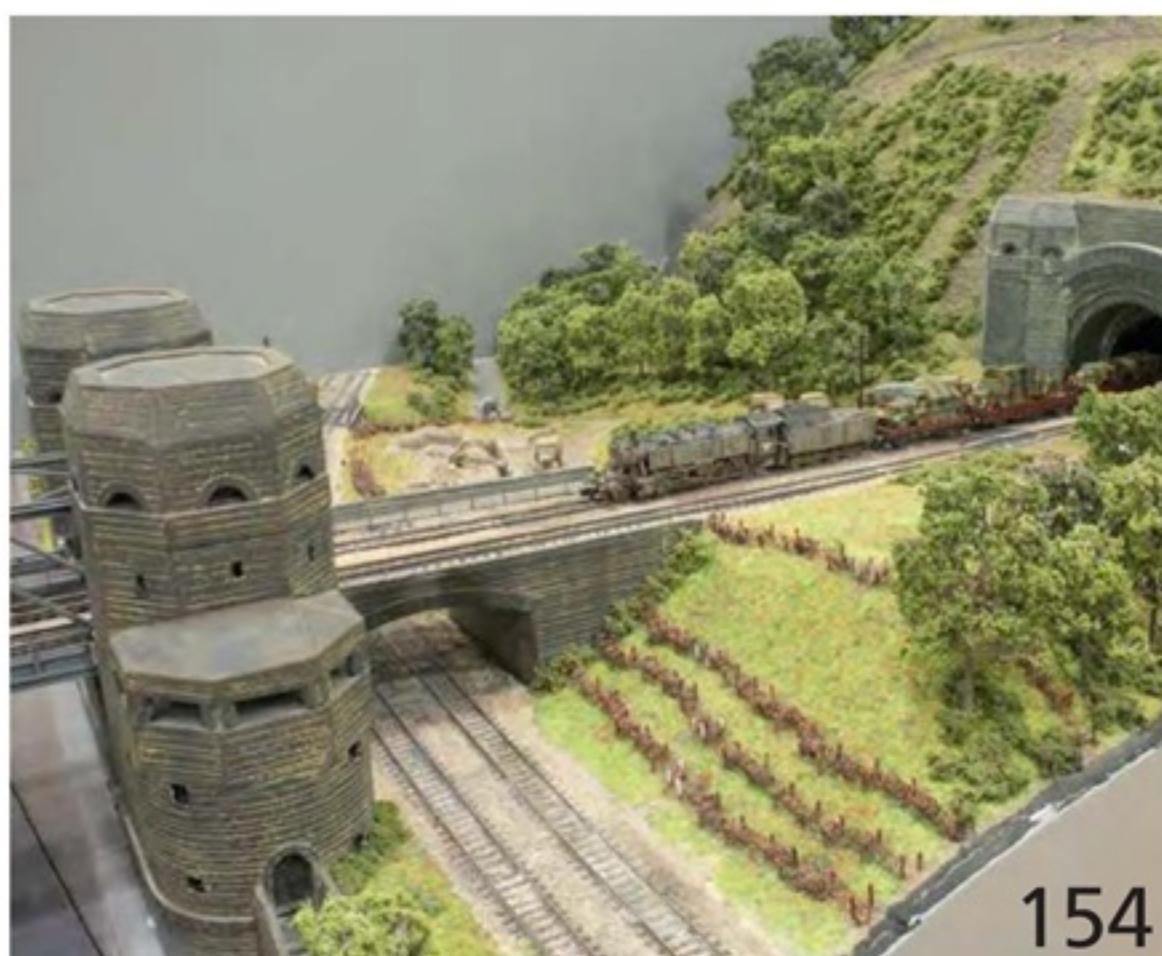
Emmanuel Nouaillier illustrates more of his techniques  
for highly detailed and realistic scenic modelling,  
creating characteristic elements of the French urban scene.

### 154 THE BRIDGE AT REMAGEN – 2

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with an impressive collection of authentic train consists.

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Rik Martens and Koen Vermeulen worked together  
and built a steam loco testing station in 1:32, winning  
the mini layout contest at Modelspoor in Leuven in 2018.

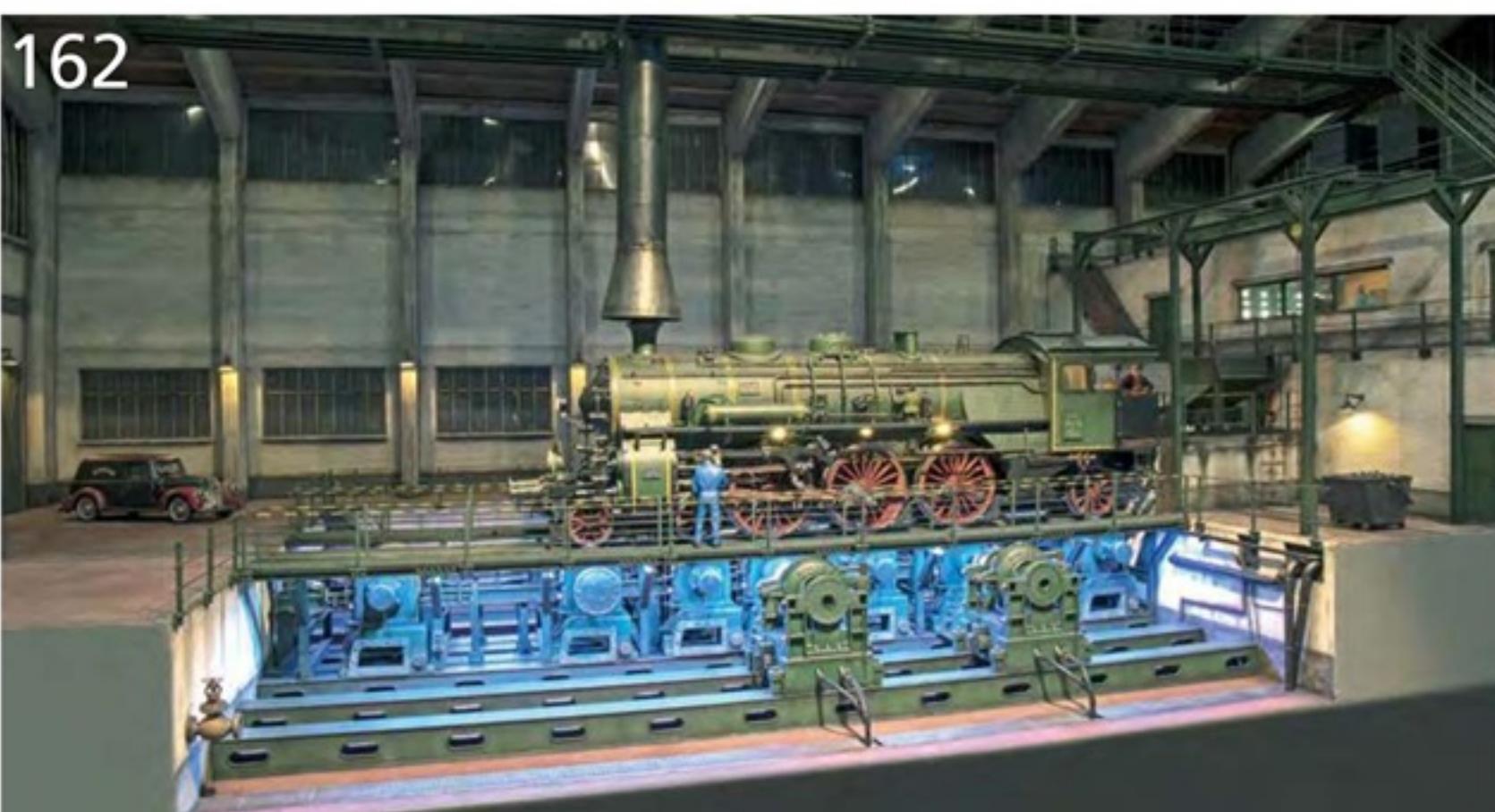


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Mike Carter fills a gap on his new Norwegian HO layout with a distinctive scratchbuilt structure.

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John Grey transferred a British N layout to the plains of the mid-West, representing a run-down Chicago by-pass.

**180 SNCF CC72000 – SCALE DRAWINGS**

To augment his feature in the recent CM French Special, Gordon Wiseman offers more illustrations of a widely used high power mixed traffic diesel.

**186 BERLIN PANKOW**

A room full of East German HO – Phil Sharp has a new layout in a spare bedroom.

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# TRIX



## N and HO Scale Continental Specialists

### MINITRIX N 1:160 Scale

Minitrix produce an extensive range of German locomotives and rolling stock, from the steam age to the modern era.

N Scale gives you the flexibility to build a complex layout in a relatively small area.



### TRIX HO 1:87 Scale

The Trix range covers the majority of Europe in HO Scale, but especially Germany, spanning multiple eras and embracing DCC sound technology. A full range of locomotives and rolling stock is available.



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The Trix C-Track range is a robust, pre-ballasted track system that simply clips together giving a firm, reliable electrical contact between track sections - simply clip together and get your locomotives running.

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# märklin



Marklin are one of the oldest model railway companies in the world.

They have over a century of expertise, and currently produce models in Z, HO, and Gauge 1 Scales.



### Z 1:220 Scale -

One of the smallest commercially available model railway scales and ideal if you only have a small space, but don't want to compromise on detail.



### HO 3-Rail 1:87 Scale -

Marklin produce a unique 3-Rail system in HO Scale, with locomotives and stock spanning the eras from the dawn of railways to the present day.



### Gauge 1 1:32 Scale -

The Gauge 1 stock produced by Marklin is breathtakingly detailed. Extra realism is provided by MFX-Sound decoders and smoke generators in the steam engines.

## Marklin Central Station 3

The CS3 is an innovative digital controller for both DC and AC layouts. It has a high resolution touchscreen display, and many possibilities for networking. It has two locomotive controllers and a central track diagram allows simple, easy control of locomotives and the layout.



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# Preiser

## Figures for Every Scale



Preiser produce the most comprehensive range of model figures on the market. There is a vast array of different subjects available, allowing you to populate every area of your layout.

Railway Scales covered:  
Z, N, TT, HO, OO, O, G, and  
Gauge 1.

A selection of other scales useful to architects and modellers are also included in the range.



Most of the range are pre-painted and ready to add to your layout, with unpainted packs available for those who want a bit more customisation. A range of single figures are available in HO Scale.



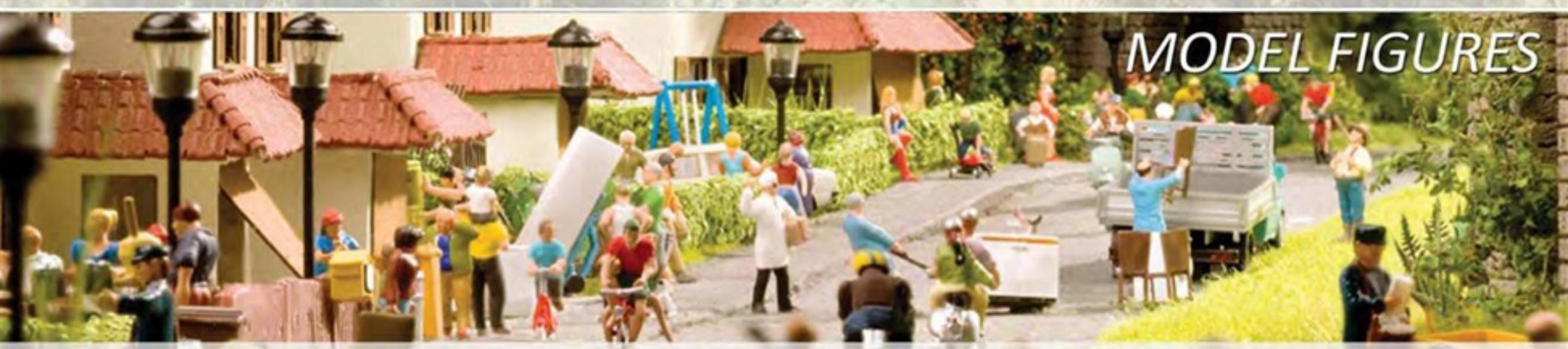
Useful scenic items and accessories are also available, allowing you to add the realism of street furniture and clutter to your layout.



Preiser also produce value packs, allowing you to cover your layout with people for a keen price. You're sure to find what you need in the Preiser range.

**SCENIC MATERIALS**

A variety of grass tufts, scatters and trees to build up realistic vegetation on your layout. Just blend them together to get the desired effect.

**MODEL FIGURES**

Noch produce a comprehensive range of pre-painted and unpainted figures to populate your layout, from passengers to the emergency services.

**LASER-CUT PLANTS**

Made from Laser-Cut paper, these kits simply fold together to create realistic plants and vegetables to plant onto your layout.

**DECO SCENES**

A complete themed scene in a pack, ready to add life to your layout - including the new Sound Scenes range which adds an extra dimension.



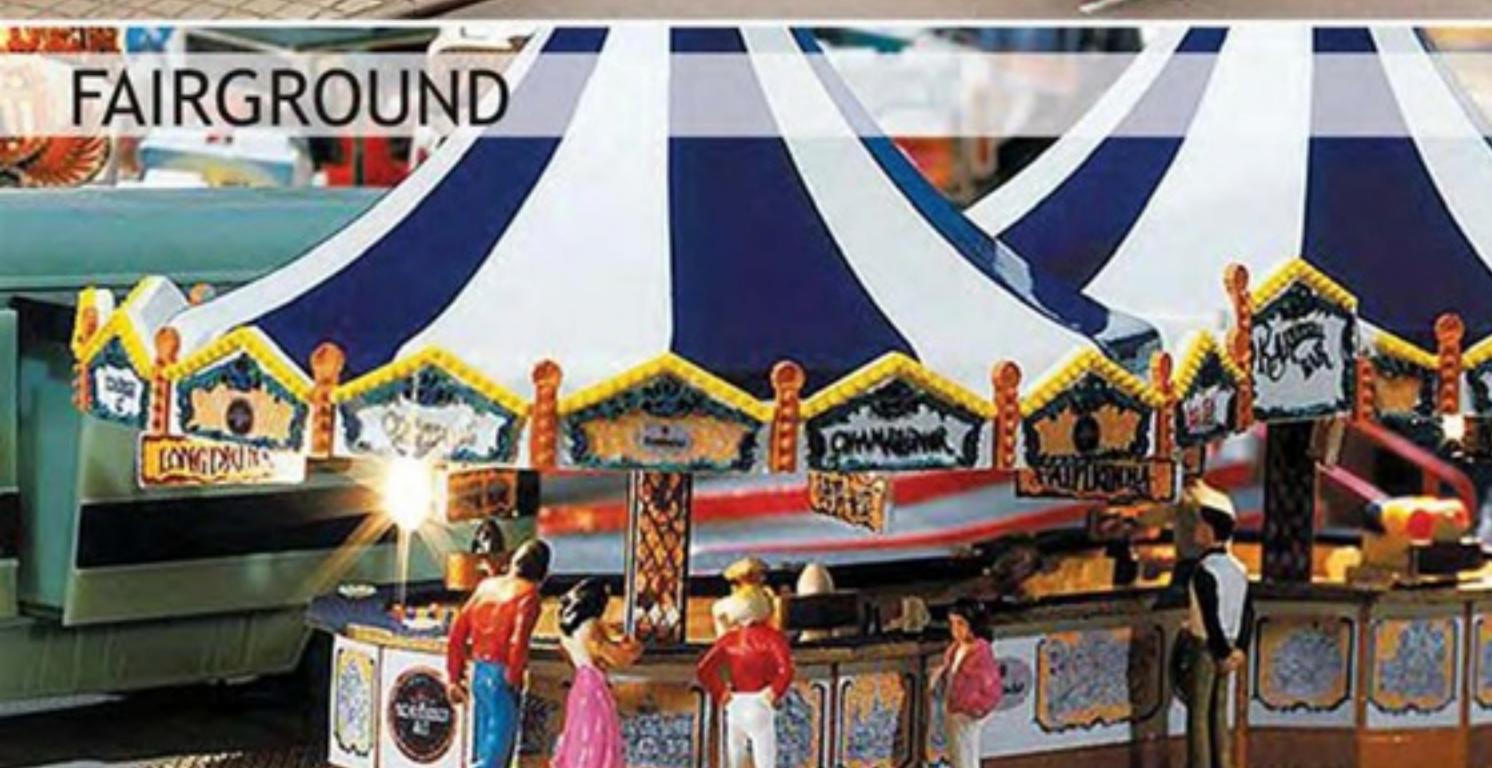
Faller are world famous for their plastic building kits. With a huge range in different scales of Continental-style buildings, any layout will benefit from the inclusion of Faller kits. To compliment the buildings, Faller also produce a range of figures and accessories to enhance the atmosphere of any street scene, as well as trees and other scenic items to add to town and countryside.

## CAR SYSTEM

The CAR SYSTEM range allows you to have moving vehicles on your layout with no visible tracks or wires. Automate junctions, traffic lights, and even speed cameras!



## FAIRGROUND



The Faller Fairground range is produced in N and HO Scale, and is almost a separate hobby in itself! You can add sound and movement to your layout with interesting rides and attractions.



# A Comprehensive Model Railway System

# PIKO

Offering great value for money, PIKO is definitely the range to look at for beginners to German railways and existing collectors alike, with a variety of starter sets on offer.



The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert.

The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology. The HO Scale A-Track has clear, simple geometry and a realistic appearance.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits.

The locomotives run smoothly, with accurately printed liveries and DCC compatibility.

This range includes many passenger multiple units that run around Europe, not just in Germany. This allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.



The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors, and is great value for money.

The locomotives feature directional lighting, and some even have sound and smoke.

Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train.

A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale.

New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.





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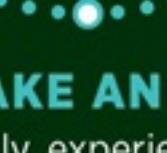
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STEP  
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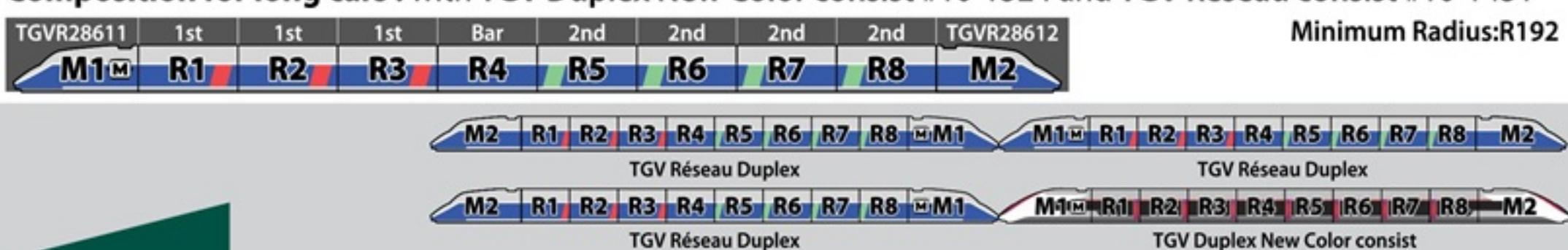
# TGV Réseau Duplex



- The prototype for the scale model is a train compatible with AC/DC, 601 to 612, 616 to 619. It has the No. 606 printed on it.
- The painting scheme is different with TGV Réseau consist #10-1431
- 20 car consist can be made like the real train by removing the coupler cover and using the draw bar supplied in the product package.
- Ok with DCC. 6 pin socket decoders can be installed in M1 and M2. Then CV29 of the decoder in M2 needs to be altered to exchange head and tail lights.

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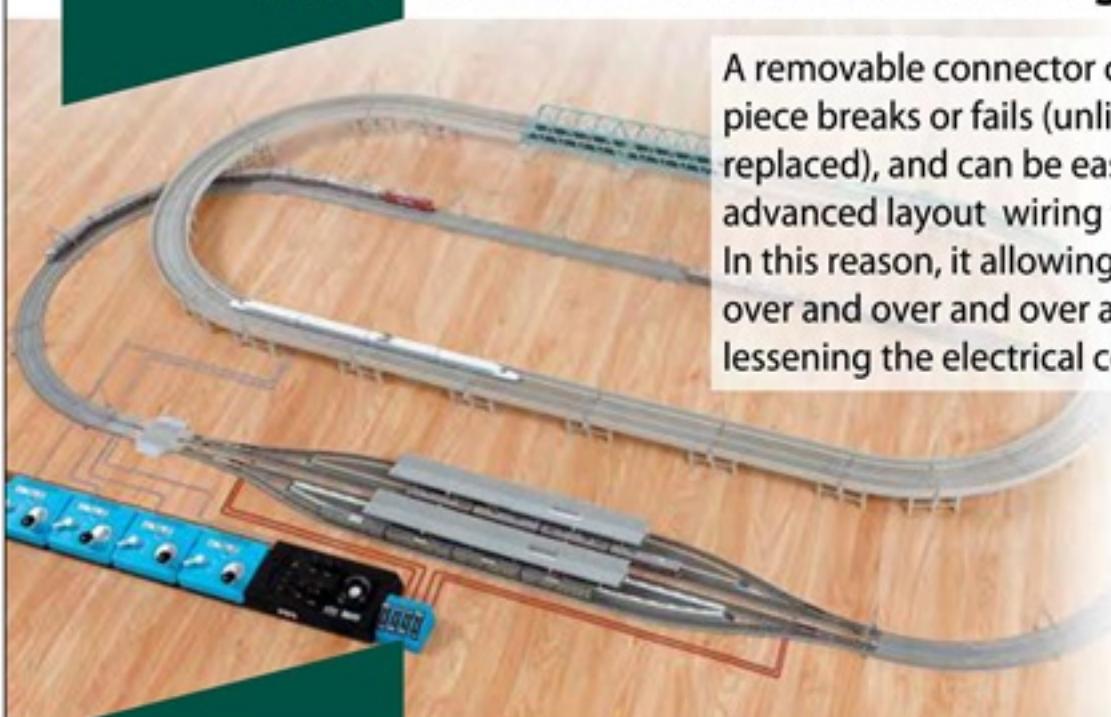
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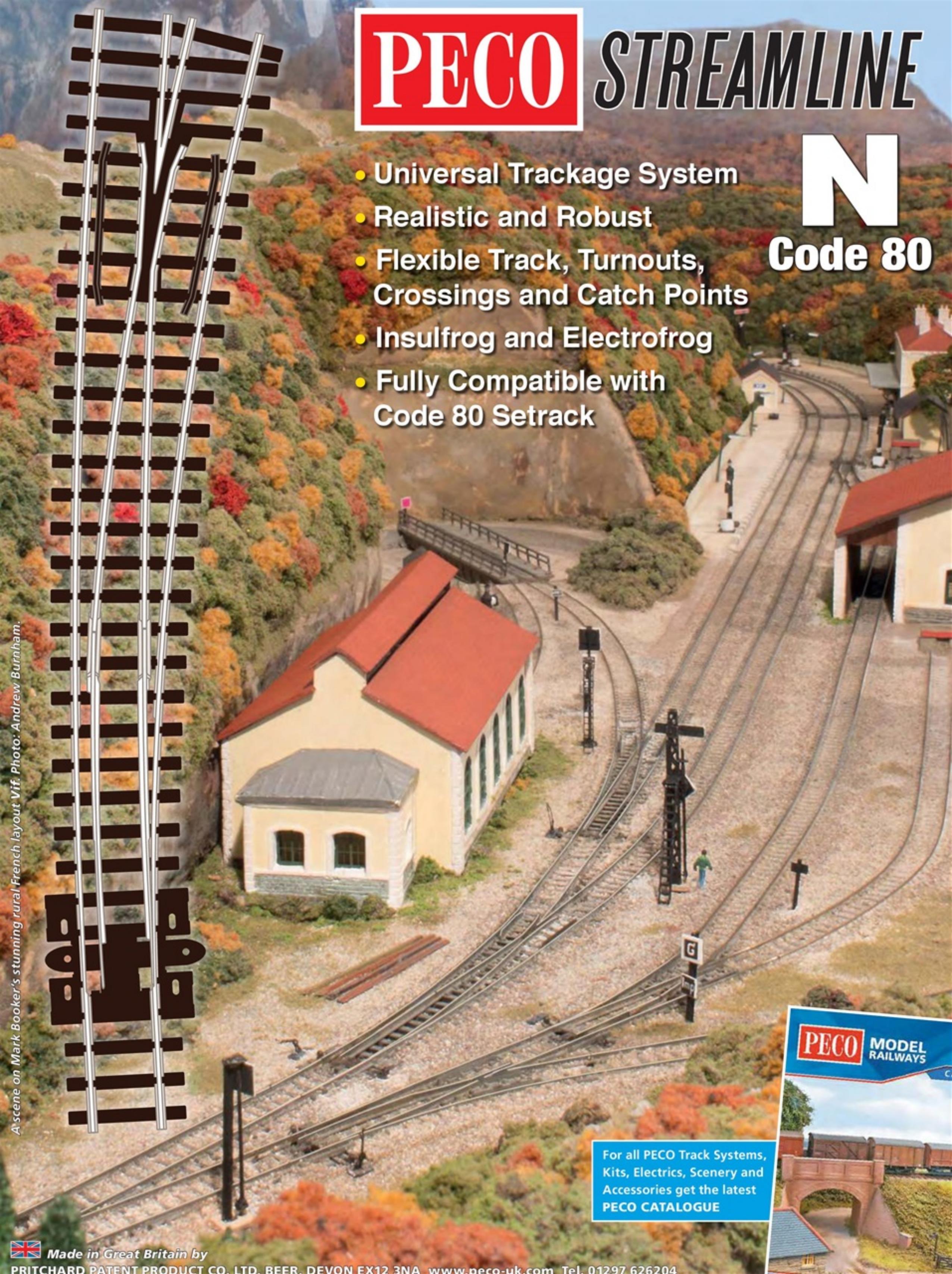
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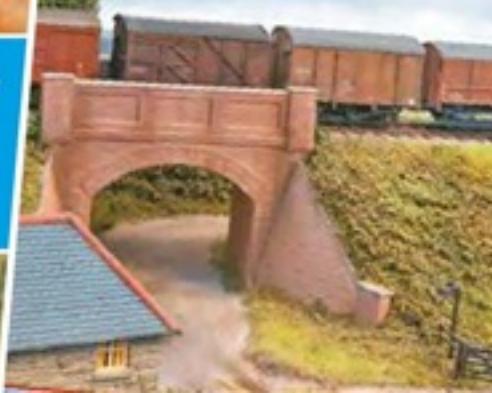
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A scene on Mark Booker's stunning rural/French layout *Vif*. Photo: Andrew Burnham.

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**Jan Martens** has created a convincing impression of a typical Vicinal metre gauge line within a compact layout that is practical to transport and exhibit.

# Le Vicinal à Vresse

An evocation of SNCV/NMVB line 553

**T**his layout was inspired by Vicinal line 553 in the Belgian Ardennes, which has an eventful history, to say the least.

A tramway was projected between Gedinne and the Semois valley as early as the beginning of the 1900s. In 1907, the SNCV received approval to build the line. Construction, by the Brussels firm Kirckmann, began in 1909 and ended in 1913. On 16th June 1913, the first tram reached Vresse, and on 28th July of

that year, Membre was reached in the section along the Semois. Operation was granted to the Semois Local Railways company. On 27th July 1914, the tramway reached Alle-sur-Semois.

The First World War then put a stop to the development of the line. The occupiers removed the rails along the banks of the Semois. It was not until 1921 that the SNCV restarted operation and the service along the Semois was resumed. Work on the





Top  
A timber train rolls through the forest, hauled by diesel tractor AR 221.  
Above  
The timber train has reached the village where wagons are being shunted for the sawmill.



Above  
AR 220, the 10 o'clock tram to Vresse.

extension from Membre resumed in 1930. The creation of this line required the construction of two bridges and the longest tunnel on the country's local railways.

On 15th May 1935 Bohan was finally connected to the line.

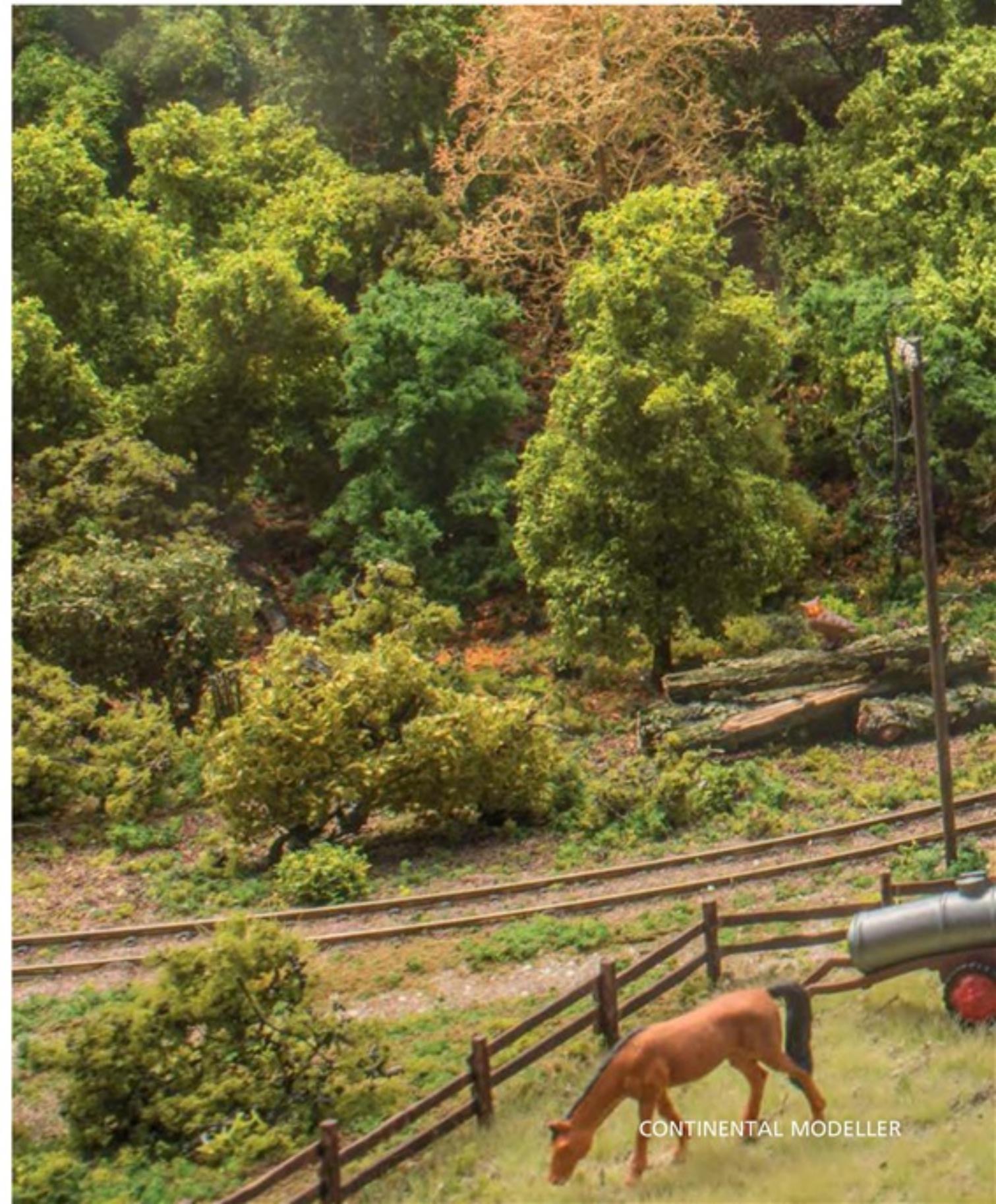
Diesel *autorails* were introduced in 1935 and gradually took over the services from steam tram locos, which finally disappeared in 1947.

On 17th October 1938, Belgian *autorails* reached Sorendal station in France, where they connect with the departmental railway from the French Ardennes to Monthermé. However, this connection to France was of short duration, since it disappeared again in September the following year.

The Second World War was a catastrophe for the entire line. Due to damage to the bridges in Membre, Bohan, and Alle, temporary bridges were put in place and attempts were made to ensure a minimum service. The situation became very chaotic on the route to Membre and Bohan and the trams disappeared from there in September 1944. On 14th May 1950 buses took over the whole route; they had been used since from 1947 on the sections which already been dismantled. For the time being, goods transport remained on the rails. In 1954 the freight service between Vresse and Alle was stopped on 21st October. On 4th May 1955 the goods traffic between Gedinne and Vresse ceased.

The line was eventually dismantled in 1956.

There is almost no archive material and very few pictures of this line. On walks in the region you can find some relics, such as the old station and tram depot in Alle-sur-Semois, which is now in use as a fire station. (However, a new one is being built, so the old SNCV buildings are threatened with demolition.)



Because there is not much to be found in the archives, this of course gave the opportunity to let my imagination run wild during the construction of the layout. The layout is therefore more an impression of "how it could have been" than a representation of "how it once was". The intention is to show the atmosphere of the Ardennes in the 1930s with the tram in the leading role.

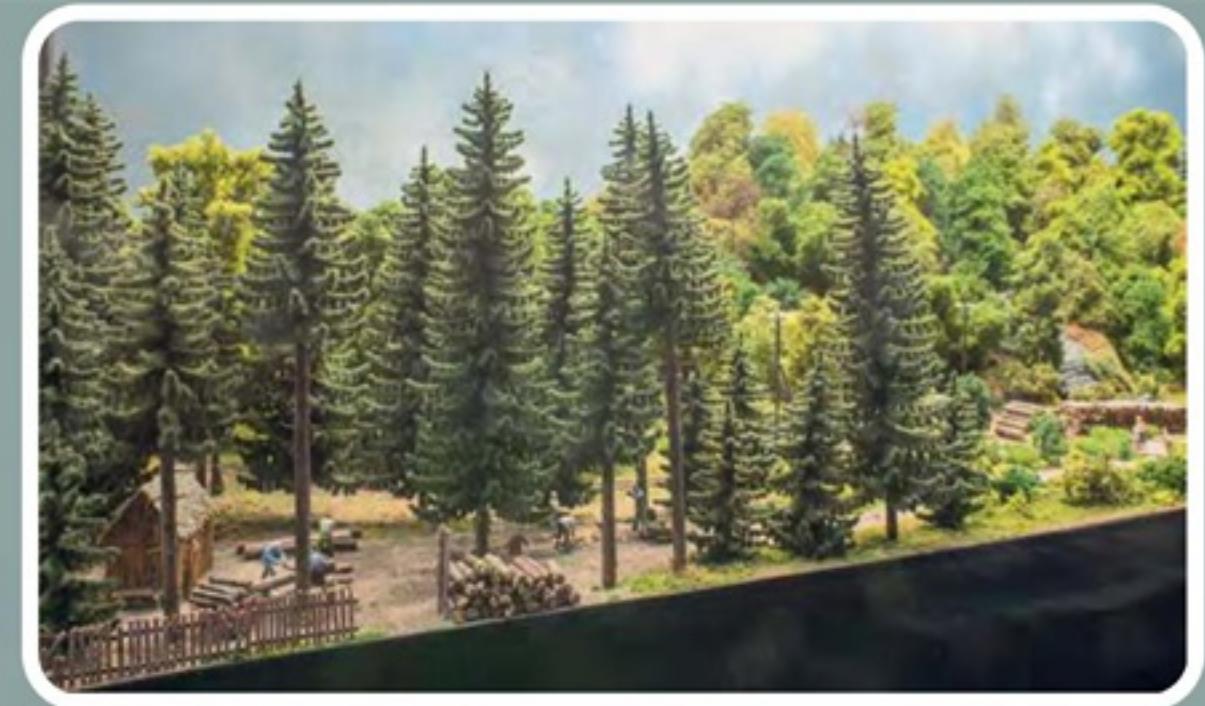


### The layout

The layout has an area of 1.5m x 1m and is transportable, so it can be exhibited, with a valance above carrying fluorescent lighting and a skirt below. It consists of two parts, the forest on one side and the village on the other, separated by a painted background with light clouds. (The weather is good that day in Vresse and you should take advantage of that in the Ardennes.) The line runs in an oval from one scene to the other and back again, with trains working in both directions. There is no off-stage fiddle yard, only functional track. The visitor can walk around the layout and view both scenes.

The display also includes copies of the few available photos of the real line.

Above  
The freight tram just coming out of the tunnel.



Above  
Hard at work in the forest.



Left  
Overall view of the village side.  
AR 220 is in the station while  
AR 147 shunts some wagons  
to the sawmill.



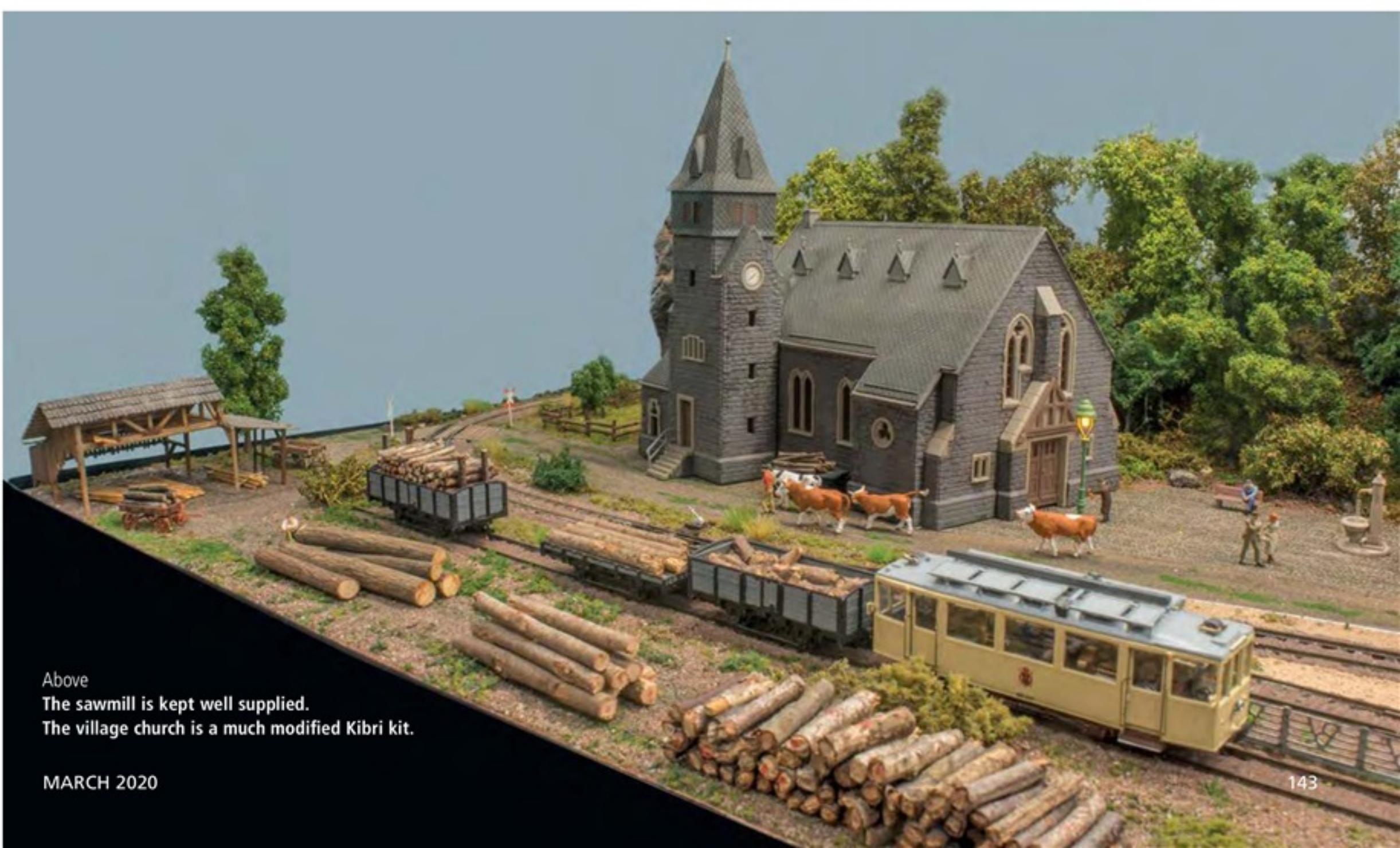
Above  
**Visiting tram ART 297 is ready to depart with a special working for Bohan. Built in 1947 at the Andenne workshops, this vehicle actually worked in Marloie, Tournai, and Forville. Like the other models, this was built from a Ferivan kit.**

On the forest side, a single track line runs through the forest with a small bridge over a stream that flows into the Semois (somewhere beyond the layout).

The village consists of a tram stop and a siding serving the sawmill. There is a loop where the trains can cross. Usually I run a freight in one direction and the passenger tram in the other. At the front of the village another freight tram shuttles back and forth to the sawmill, independent of the running line.

Most of the freight transported was wood, and this is replicated on the model. The wood loads on the freight wagons and the piles of wood that you see find all over the layout are real wood that was brought from the region of Vresse-sur-Semois.

The forest is a mixture of scratchbuilt trees based on sea foam covered with scatter material in different shades of green (it is spring), like in a real forest. For variation, some trees from Heki have also been purchased and 'planted'.



Above  
**The sawmill is kept well supplied. The village church is a much modified Kibri kit.**

The firs are from Busch and the shrubs are different varieties of foliage from Woodland Scenics.

The buildings in the village are alpine houses from the Kibri range that have been adapted, re-painted, and fitted with Redutex slate roofs. The village church is from Kibri, which has also been completely adapted and re-painted.

The trams in use are AR diesel trams from Jocadis, bought from Saroulmapoul. They are numbered AR 147, AR 148, AR 220, and AR 221 – these are the ones that were actually used on the line. The models are powered by mechanisms from PMT, and run very well.

The vans and open wagons are also kits from the former Jocadis range.

The track is controlled by analogue DC from a Gaugemaster Combi hand-held unit.

If you should see the layout at an exhibition, do not look for direct representations of what it was really like but just let yourself be immersed in the atmosphere of the Vicinal!



Top

**The timber train has to wait until the track ahead is free, when the passenger service has arrived safely in the station.**

Above

**Although the sidings are connected to the main line, at exhibitions the timber wagons are simply shuttled back and forth. Note the two bolster wagons out of use and slowly becoming overgrown. Beyond the village the line curves through the woods to the forest side of the layout.**

Photographs by the editor.



## Emmanuel Nouaillier

reflects on his approach to the detailed modelling of urban architecture.

*Photographs by the author.*

# Selected examples

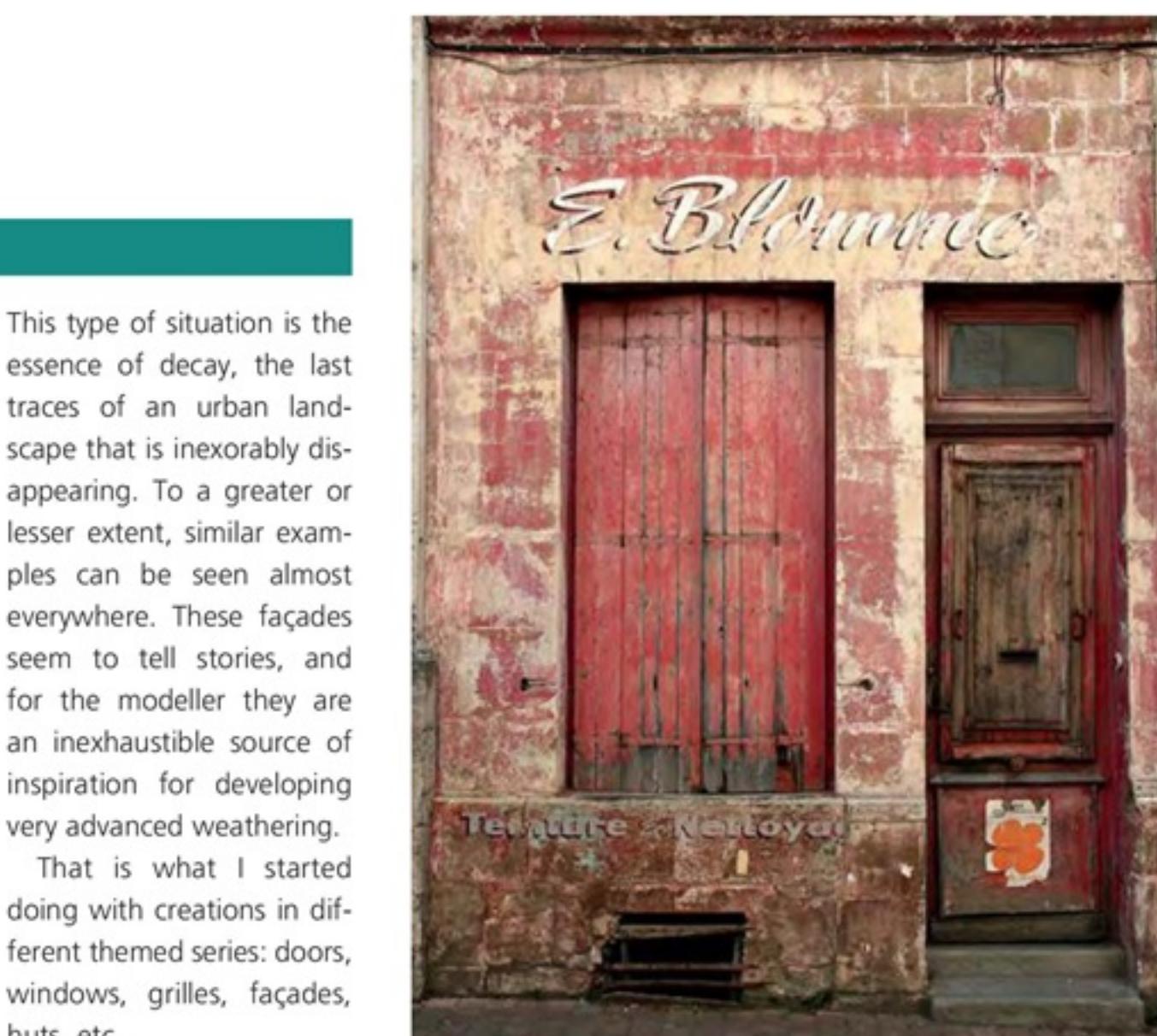
Beyond the usual model railway ambit

During the years of being published in this magazine, I have developed a certain approach, which has been refined over time. I have become increasingly focussed on creating an atmosphere and illustrating particular techniques and effects with small scenes and dioramas. These allow me not only to show ideas on specific topics in a reasonable time, but also to show them in places (e.g. art and craft galleries) beyond the usual model (railway) exhibitions. My philosophy may not appeal to those modellers who are only interested in moving trains among basic scenery, but others may find it useful to know how the structures were made and detailed, and how they create a concentrated atmosphere in just a few square centimetres.

The results have been shown in print, and also at many exhibitions and on line in my Instagram pages and blog.

Here are illustrations of some urban 'fragments' that might not necessarily be seen at a railway-centred exhibition. I will return to some of them in more detail in forthcoming articles.

### Prototype inspiration



This type of situation is the essence of decay, the last traces of an urban landscape that is inexorably disappearing. To a greater or lesser extent, similar examples can be seen almost everywhere. These façades seem to tell stories, and for the modeller they are an inexhaustible source of inspiration for developing very advanced weathering.

That is what I started doing with creations in different themed series: doors, windows, grilles, façades, huts, etc.

## Examples 1 and 2

A little featherboard, plaster, plasticard, or Forex ... though the main ingredients for the general construction have remained constant over the years, the painting and weathering techniques and materials have evolved considerably, allowing us to reach an extremely advanced degree of finish. An important aspect is what I call weathering even before painting, that is, intentionally altering parts of the structure to show some effects of decay.



## Example 3

Weathering, both in the structure and the finish, has become my speciality, and I am constantly experimenting and developing my techniques, often in response to the release of new products.

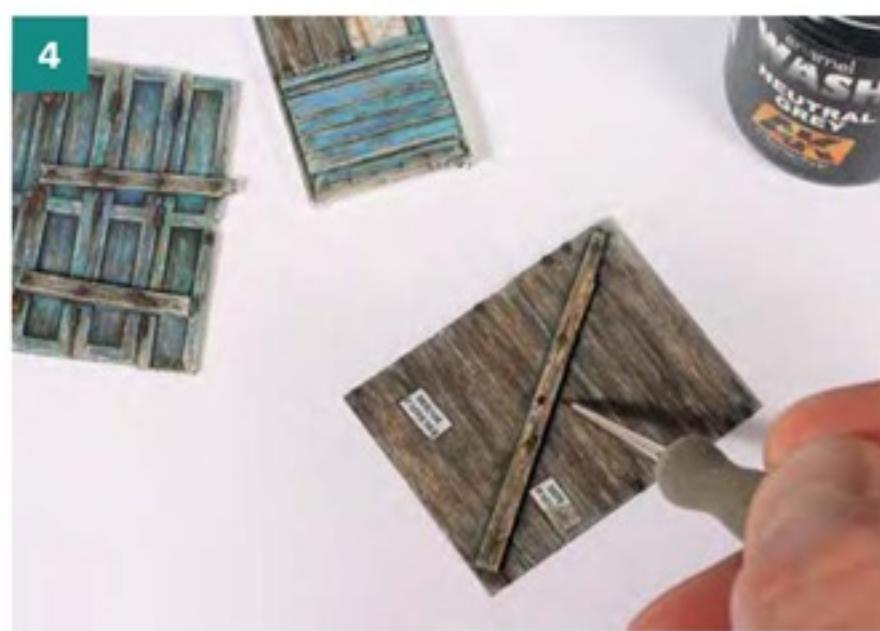
Here is a stucco effect over a brick structure.



## Examples 4 and 5

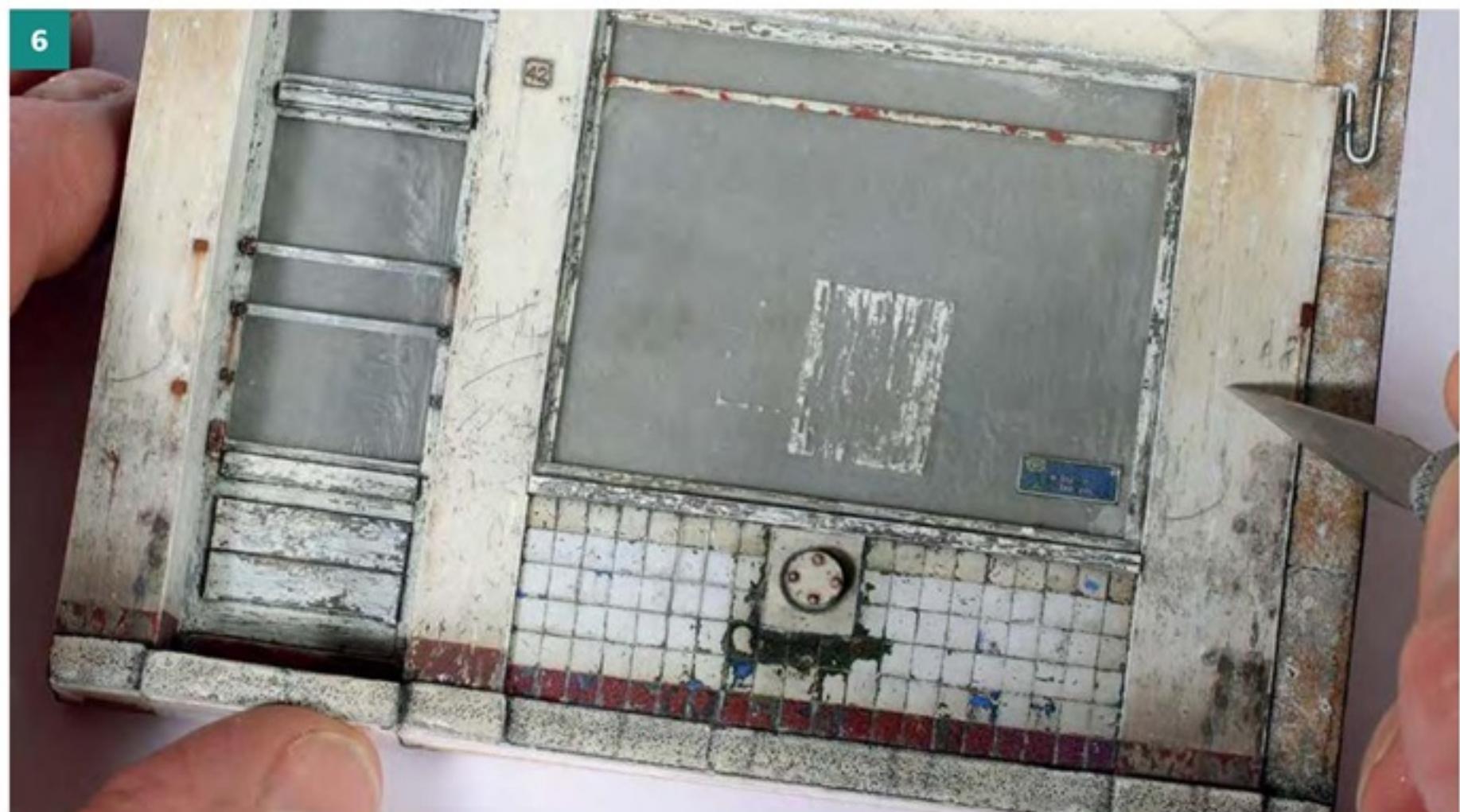
Weathering the different components of the structure uses the methods I have already described, as for these wooden elements, but not limited to them, depending on the case.

I have also been combining different techniques and testing some unlikely methods to achieve even more variety, without any time limit.



## Example 6

A perfect example of my previous comments: on this shop front I combined almost all the techniques to achieve different effects on different areas, working by 'feel' without a precise order until the desired result was achieved.



## Examples 7 and 8

With this particular urban theme, developed in a large scale, I have constantly tried to push the limits of what is possible, particularly with regard to painting freehand, using my past experience painting figures. This, plus the experience acquired from a lot of micro-painting in 1:87, allows me to paint letters and artwork following actual examples.



## Example 9

A careful use of paper, worked with a sponge and the micro-painting process also makes it possible to reproduce signs in which the elements or layers blend completely.

This topic was discussed in a previous article (April 2018). This one of the most recent examples.

## Example 10

Paper can also be very useful for imitating enamel signs with particular lettering with various scratchbuilt elements added. Since the original model was already weathered, I just needed to layer the weathering effects.



### Examples 11 and 12

I have put a lot of effort into making signs and indeed whole shop fronts appear as degraded as possible, though sometimes not as much as the examples that inspired them – some real situations are so extreme that reproducing them accurately in miniature could appear excessive.

12



11



12



11



## Example 13

The private entrance doors to old shops can give rise to innumerable interpretations with all kinds of details. They can be developed considerably with additional parts, commercial and scratchbuilt. It is easier in larger scales.



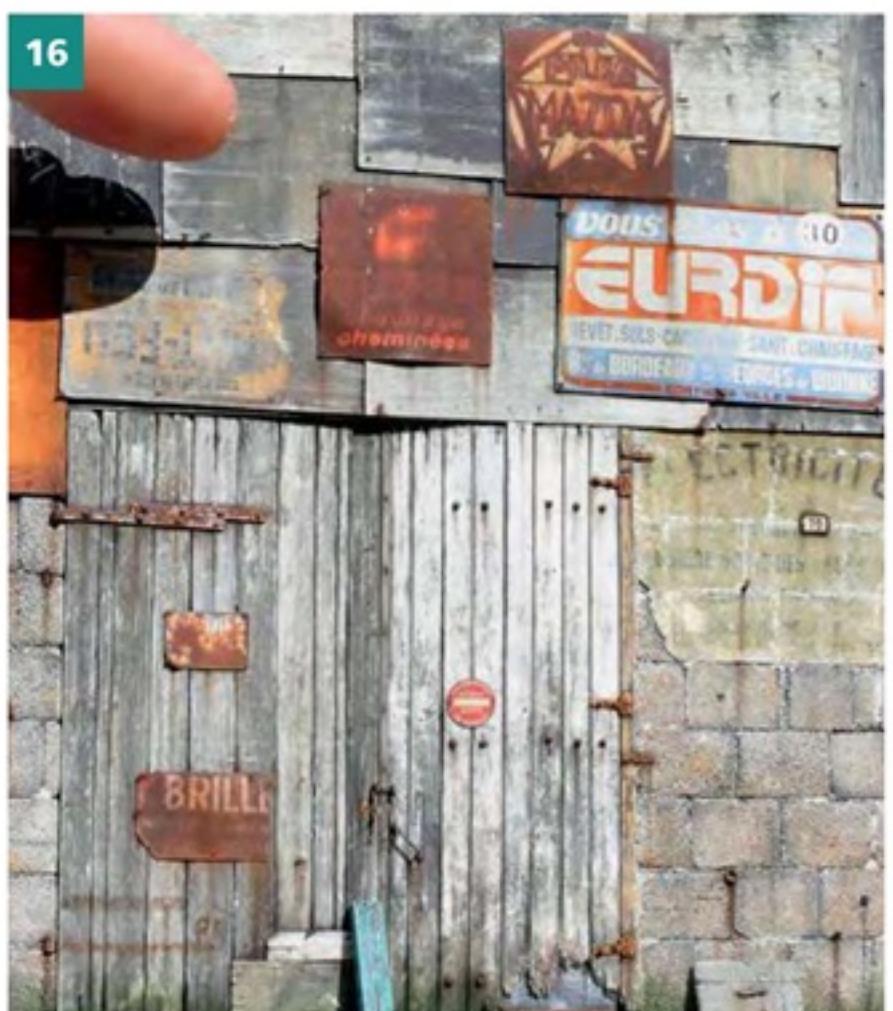
## Example 14

The glass blanked with Meudon whitewash (a kind of crushed chalk), the patched planking, the broken tile, an old half-torn sticker ... many everyday details have been carefully added to perfect the appearance of this door.



## Examples 15 and 16

Façades like this allow you to add a lot of details without systematic planning which bring the whole thing to life.



## Prototype inspiration

To me, features like these doors cry out to be modelled, never mind the rest of the structure.



## Prototype and model – examples 17 and 18

I have tried many different variations around this theme, particularly highlighting the work on weathering the wood – bare, bleached, chipped, split, and riddled with wood-worm, with all kinds of different details – added patches, nails, nail holes, rust streaks, etc.

This door is closely based on a real example, but adapted to fit a different façade.

I almost always start with a real example but with a certain freedom to adapt it as suits the specific scene.

This workshop door is close to completion.

The ability to show the models with very close-up photos has induced an unhealthy attention to the smallest detail ... the spectator's attention can be focussed on the smallest element.

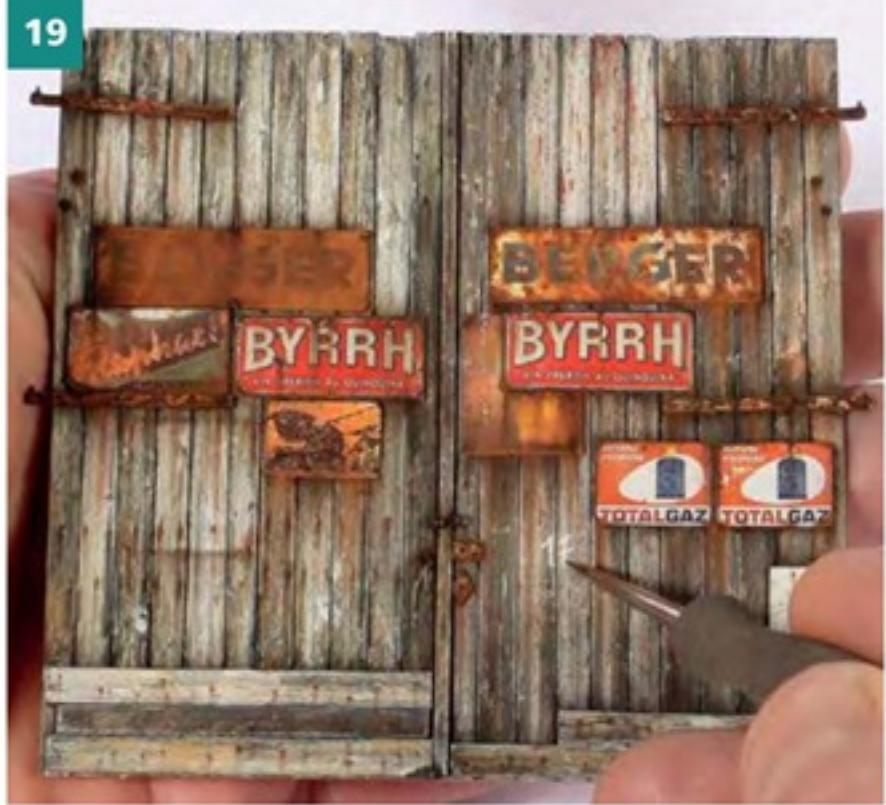


## Examples 19 and 20

Two more examples, in which I have almost reached the limit of detail – for the moment!

Note the layers of advertisements on the second example.

19



20



## Examples 21 to 24

Such intense weathering of one feature must be carried over to the surroundings, with an accumulation of details which may seem individually insignificant but are important for the overall effect. These should be treated with the same care, and especially be arranged to give the whole a natural aspect.

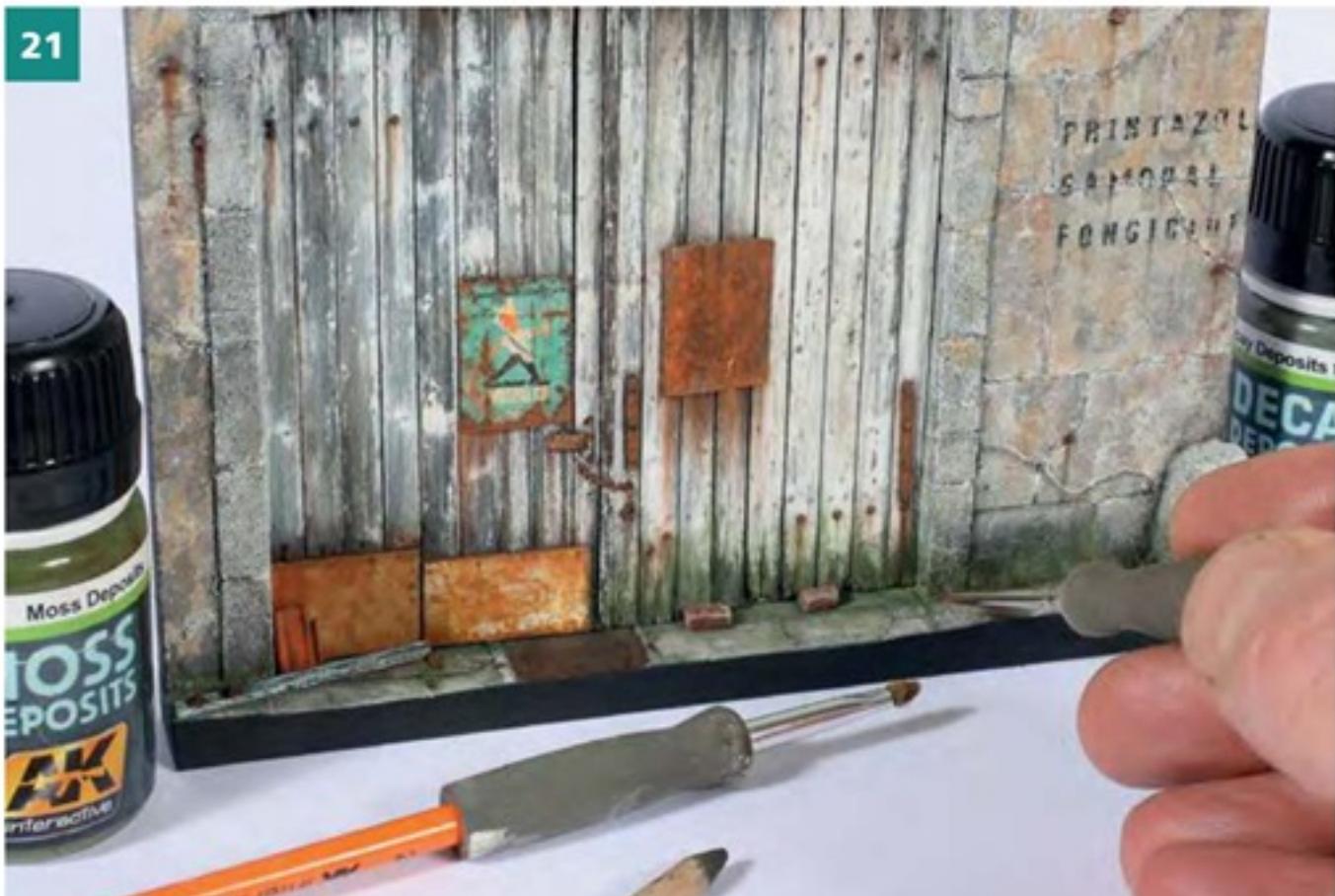
I started this approach with micro scenes in smaller scales, which to some may have seemed like a challenge at the time.

The mixture of methods is now the rule, using various unconventional techniques tried and found successful. Only the result counts.

To see more and follow me:

[www.instagram.com/wallsfromthepast](https://www.instagram.com/wallsfromthepast)  
[www.blogspot.com/manuelnouaillier](https://www.blogspot.com/manuelnouaillier)

21



22



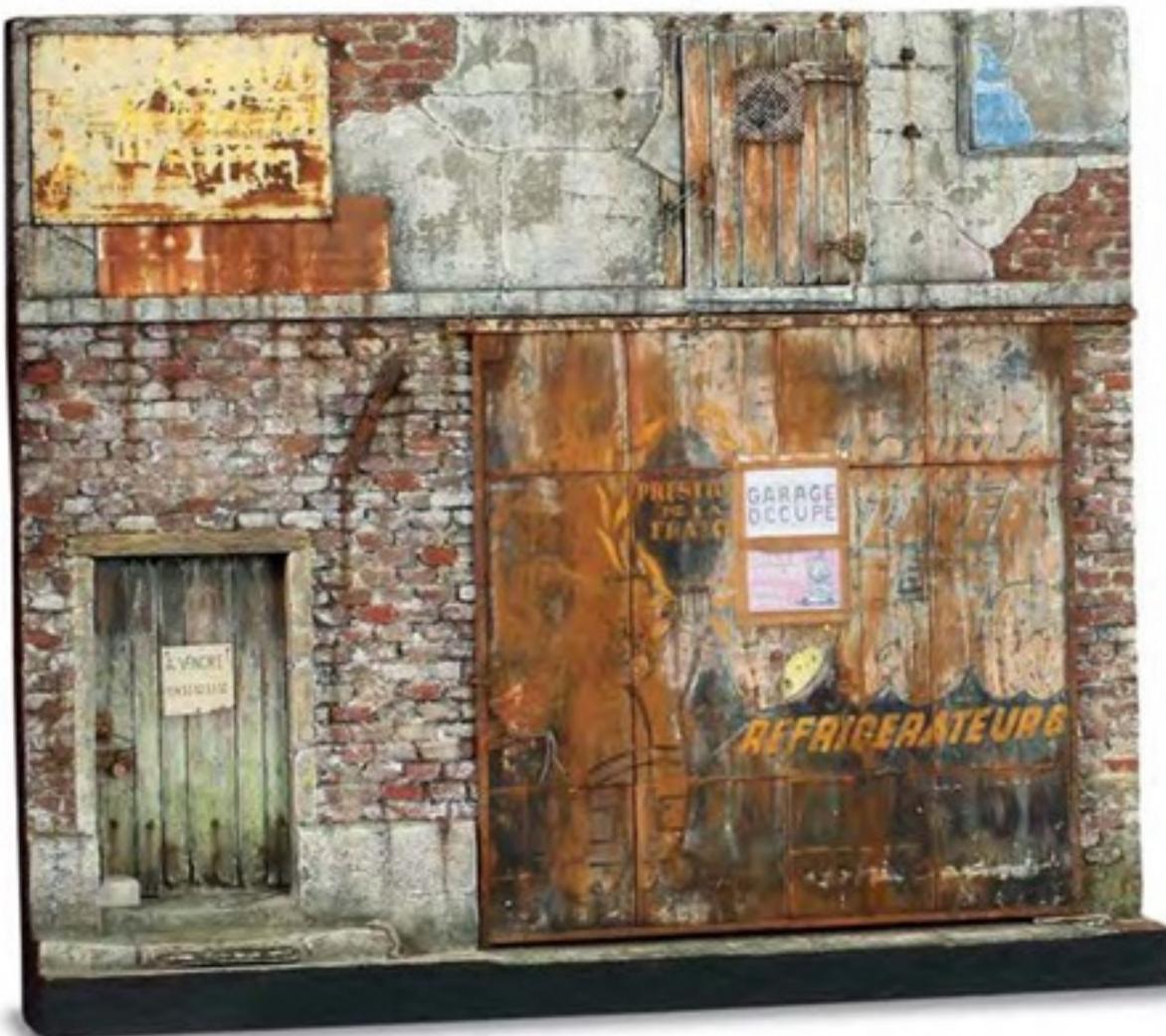
23



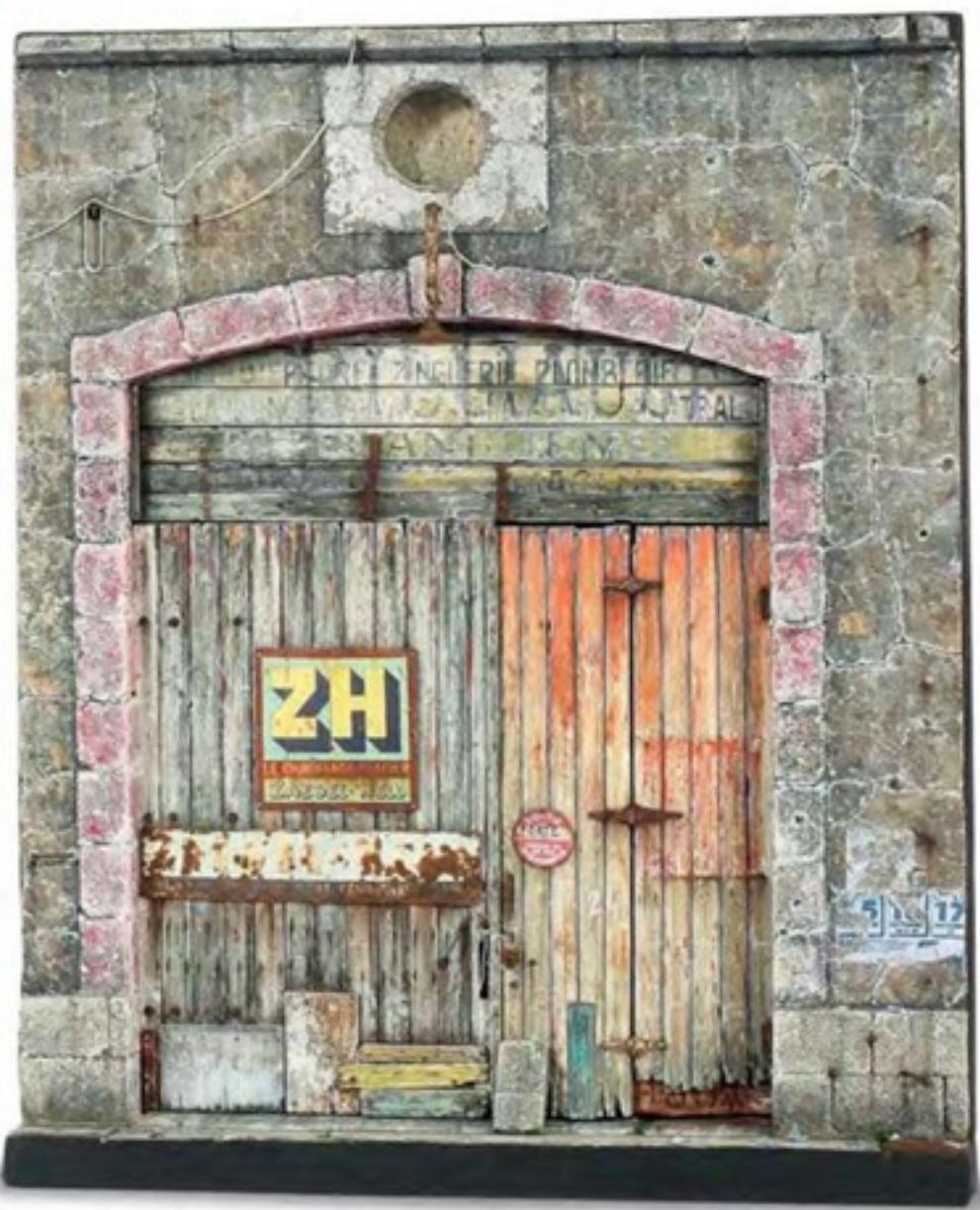
## Examples 25 and 26

25

Here are a couple of my latest works, in which I was trying to push the limits of the possible effects while remaining realistic.



26



24



## Example 27

To finish, an example of how I sometimes now present the finished models, like a painting or a photo, in a smart frame, ready to be hung on a wall or placed on an easel on a piece of furniture.



**Al Turner** was inspired by a well-known feature film to create an accurate scale model of the Ludendorff Bridge across the Rhine, and has built up an impressive collection of authentic train consists to run over it. *The layout is due to be at Model Rail Scotland, Friday 21st to Sunday 23rd February.*

# The bridge at Remagen

Braced for the Allied attack – part 2



## The trains

The majority of the stock started life as ready-to-run models, but all have undergone some degree of modification, be it a simple application of Deutsche Reichsbahn lettering and weathering, or a complete rebuild, such as the semi-streamlined BR05 and armoured railcar. Each train has been specifically researched to be as accurate as possible within the operating constraints of the layout.

Several items of rolling stock, such as the K5 rail gun and Karl-Gerät mortars, were converted from static models, while others, such as the *Schienenwolf* train and the Armoured Train have been extensively converted and scratchbuilt.

### Karl-Gerät train

The Karl-Gerät train is made up of two of the massive 600mm mortars suspended on Schnabel heavy load wagons with accompanying flats and vans. The mortars, *Eva* and *Loki*, are Dragon 1:144 static models that have been converted to run with 3D-printed five-axle wagon chassis. Information on the train make-up is a bit sketchy so a logical approach has been taken with *Munitionpanzers*, *Wirbelwind* anti-aircraft guns, and other support vehicles occupying the flat wagons, and tool vans and crew coaches making up the rest of the train.

Below

**West of the bridge,**  
a BR52 *Kriegslok* at the head  
of the Karl-Gerät train waits  
for clearance to head west,  
while in the east-bound loop  
the *Schienenwolf* train  
powered by two BR38s  
is passed by a BR94  
on a passenger train.  
The armoured car  
towing a domestic caravan  
is based on a photograph.

Photographs by the editor.





#### Vampir Panther train

This train's load is an armoured assault company from the 116th Panzer Division, mainly flat wagons carrying Sdkfz251 half-tracks, accompanied by a platoon of Panther tanks. Each of these vehicles has been specially modified with scratchbuilt infra-red night-fighting equipment and are being deployed to the Western Front where the supremacy of Allied air power makes daytime operation hazardous.

#### Operation Greif train

This is the Panzer Brigade 150 unit famously used on the opening day of the Battle of the Bulge to infiltrate the American lines with the objective of capturing the key Meuse bridges. The original concept was to have the brigade equipped with captured Allied equipment, but the numbers

Above

**Heading onto the bridge**  
is a camouflaged BR52  
hauling Panzer Brigade 150  
disguised as American  
armour, as famously used  
on the opening day  
of the Battle of the Bulge.





available fell woefully short of what was needed, so Panther tanks were modified to look like American M10 Tank Destroyers, and other vehicles repainted in olive drab with Allied markings. The unit failed in its objectives, with the majority of the armour knocked out within the opening hours of the offensive, but it did plant a huge amount of distrust among the American troops which produced a massive increase in 'friendly fire' incidents.

#### *Schienenwolf* train

The *Schienenwolf*, or rail ripper, needed much more extensive work, being scratchbuilt onto a heavy Trix chassis. These wagons were used extensively by the retreating Germans to destroy railway lines to prevent them being used

by the advancing Allied forces. They were crude devices consisting of a large metal blade that was lowered into the 4-foot and dragged by a couple of locos to sever the sleepers and destroy the trackbed formation. The model is loosely based on the example preserved at the Longmoor museum. It is coupled to a couple of BR38s, one of which has had an armoured cab added and carries camouflage paint. Ahead of the locos is a three-axle tool van which houses the troops and equipment; with the constant threat of air attack, a Flak 38 quad 20mm gun has been mounted on it.



#### Top

A BR52 *Kriegslok* waits for clearance to head west, while the *Schienenwolf* train is held in the east-bound loop.

#### Above

A BR52 with condensing tender hauls the *Vampir* Panther train west over the Remagen – Kripp road.



#### Above

**SVT137 railcar set heads east in the service of the Wehrmacht.**

#### *The Flying Hamburger*

Once the flagship of the Deutsche Reichsbahn high speed diesel fleet, this SVT137 used to thunder between Berlin, Hannover, and Cologne, and at one time held the world speed record for diesel trains at 205km/h (127mph). Now under the control of the Wehrmacht, it is looking a little worse for wear as it ferries various Party officials.

#### **The K5 gun train**

This train illustrates the formation of the rail gun as it would be while being moved between sites. The camouflaged section, in the middle, is made up of the gun carriage, a generator wagon, munitions wagons, and a BR36 diesel shunter. The rest of the train is made up of support wagons, with tool vans at the front and crew coaches to the rear.

The K5 is another Dragon static model which has been modified with four ready-to-run three-axle bogies to turn it into a running model.

#### Below

**A BR05 4-6-4, stripped of its streamlined casing, hauls the K5 rail gun train west.**





#### The Messerschmitt train

In the later war years, partly through the lack of pilots and partly due to fuel restrictions, new aircraft were moved from the factories by rail. This train is carrying a consignment of new Messerschmitts to a dispersal area – Me109 fighters, Me262 jets, and a couple of Me163 Komet rocket planes. Under construction are three Me110 twin-engined night fighters that will shortly join the rake, on suitable wagons.

With regard to loads, several of the goods trains are works in progress like this.

#### *Von Ryan's Express*

Continuing the theme of war film references, I could not resist including *Von Ryan's Express* – it must have overshot the Alps! In the film the loco is an Italian Gr.735, but there are hardly any Italian steam locos available in N so this has been converted from a Minitrix BR56, a 2-8-0 with similar dimensions. The conversion was carried out just to give an impression of the loco in the film, so it was limited to adding red smoke deflectors and removing one of the two cab side windows.

Above

**Heading west off the bridge** is an armoured BR52 with a rake of stake wagons carrying new aircraft.

Below

**Von Ryan's Express** comes off the bridge and into the Erpeler Ley tunnel while a camouflaged BR44 waits for clearance to proceed.





Above  
A train double-headed by a BR85 2-10-2T and a BR94 0-10-0T.

#### Armoured Train 24

Armoured Train 24 is a freelance anti-partisan train based on several photos of early armoured trains that operated on the Eastern Front. The loco is a Minitrix BR24 2-6-0 which has had armoured plates added to either side and a second water tender attached to the front. The armour plating only covers the sides, leaving the top exposed. The two accompanying wagons are captured Russian vehicles and have been partly re-equipped with German quad 20mm flak guns alongside the Russian T34 and BT3 turrets. The full rake also consists of a two flat wagons carrying captured French S35 tanks and two pusher wagons. Unfortunately the Minitrix BR24 suffers from poor traction and is unable to propel the full formation up the layout inclines and curves, so for now the train sits on one of the non-running sections until the loco can be rebuilt onto a better chassis.



Below  
Armoured Train 24 on the main Bonn to Koblenz line waits alongside a BR38 with a rake of vans.





#### The *Rheingold*

Officially the *Rheingold* trains were suspended in 1939, but the stock was conscripted to transport high-ranking Party officials. This set is normally an eight coach rake from Arnold, but is seen here reduced to six coaches and a van. Originally it was hauled by an Arnold BR18 decked out in Party regalia, but this has now been replaced with the better Minitrix model, though this is still in K.Bay.St.B. (Bavarian) livery and awaits repainting and weathering.

#### The V2 rocket train

This train is still in development. It started out as a rake of stake wagons transporting V2 rockets but has slowly been expanded to include launch support vehicles. It will eventually include two experimental rail mounted launch platforms for the V2 rockets and a Köf II shunter.

While the layout is now mostly complete, apart from the Apollinaris Church and the endless potential for details, the process of modelling authentic trains will continue.

Above  
Former *Rheingold* stock, now under Party control, heading east.

Below  
A BR50 2-10-0 hauls a short mixed goods train onto the bridge approaches.



**Rik Martens** and **Koen Vermeulen** collaborated to create an unusual scene, and won the mini layout competition at Modelspoor in Leuven in 2018. *Photographs by Gerard Tombroek.*

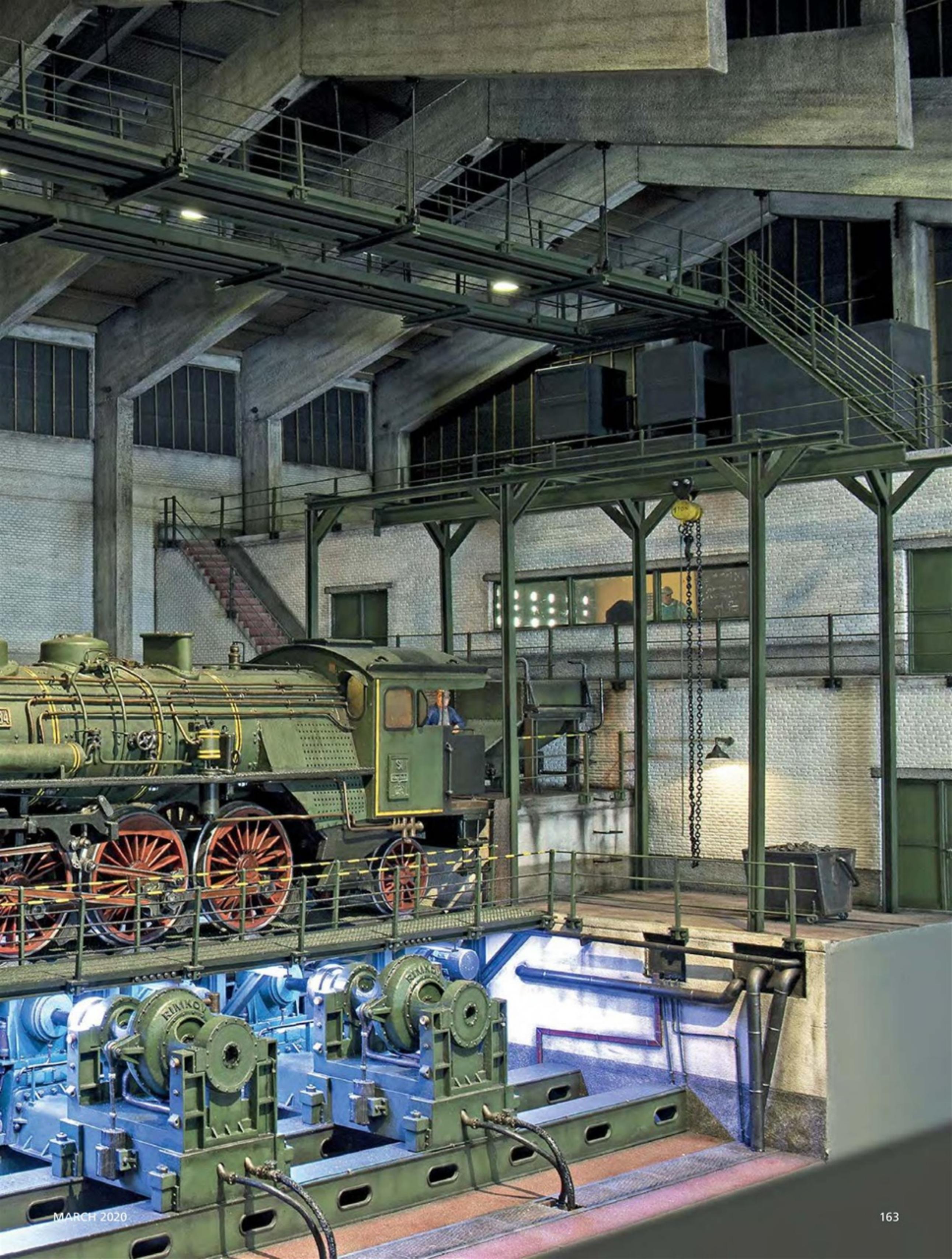
# RIMKOV

Locomotive testing station

Right:

The elegant Bavarian S3/6 Pacific stands on the RIMKOV test bench. Before an extensive test can start, all parts of the motion are lubricated. Dynamic water brakes are used which can be adjusted for any wheel arrangement. The water must be sufficiently cooled before it can be used again.





decided to participate in the Modelspoor mini layout competition again, but this time as a team with my friend Koen Vermeulen. I went back to an idea I had been considering for some time, largely inspired by and based on the locomotive testing station at Rugby in England – a test bench for steam locomotives from the middle of the last century. The name RIMKOV is a fusion of our names.

## Background

For some time I had been playing around with an idea that I wanted to work out. However, fate decided otherwise and it turned out that this idea was not eligible for the competition in 2018. I was not worried, as in the meanwhile another plan that had been in my mind for a while came to occupy my attention. A few months earlier I had found a video on the internet that had struck me with astonishment – a roller test bench for steam locomotives. This was ultimately what I chose to make.

I had planned to compete alone, as before, but the choice of modelling a roller test bench with the related technical equipment made me change my mind. To realise such a project as this in a limited time it seemed sensible not to do it alone. The new competition rules offered the opportunity of participating as a team. My fellow club member Koen Vermeulen already had extensive experience in the field of 3D-printing and laser cutting, and the necessary know-how of electronics. Together we could form a strong team and each could use his particular talents to bring this project to a successful conclusion.



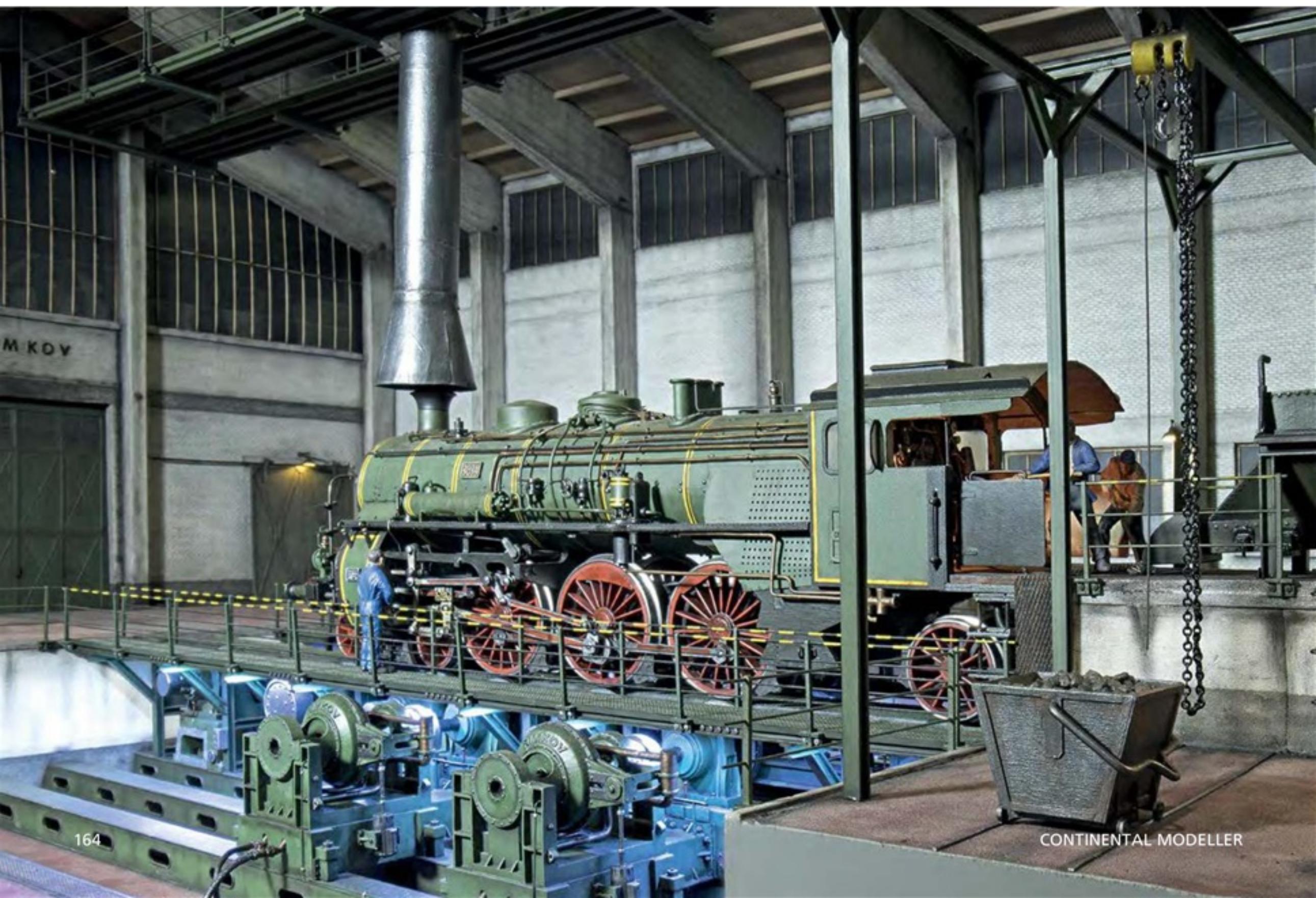
## The inspiration

We have frequently been asked where we got our inspiration. In this case, the answer is easier than you might think. For us, the internet is the biggest source of both inspiration and research material.

We are also often asked how you start to work out something like this. It was clear from the start that the best way to bring the roller test bench to life would be by using 3D-prints, supplemented with parts laser-cut from cardboard.

Above  
The shed itself forms the presentation cabinet. That way all the space can be used to contribute to the atmosphere.

Below  
Coal is delivered to the loco under test by small skips.



The first task was to carefully study the available black and white films. Sketches were made of various parts and components, then we started drawing in detail with accurate shapes and dimensions.

To be able to 3D-print the main parts of the test bench a third step was necessary before we could start construction: the three-dimensional scale drawings. Koen managed to convert the elements of the complicated design into something it was practical to print.

## Presentation

An original idea or an interesting piece of history is one thing, but for a successful diorama or layout the visual effect is equally important.

Originally we would have chosen to work in HO, as we were familiar with that, but it soon became more interesting to use a larger scale for this subject. We knew we had to make the test bench the main subject of the proposed layout. We decided it was important to place it centrally, with the shed in which it was housed forming the 'viewing box', framing the image. If we did it this way in 1:87, everything would probably have been too small and not clear enough.

Everything depended on finding a suitable model in a larger scale, and at an affordable price. As the inspiration was British, we went looking for a British steam loco in O or 1, but it soon became clear that nothing suitable was readily available. In Gauge 1 there certainly are proprietary models, but usually of German prototypes, and quite expensive.

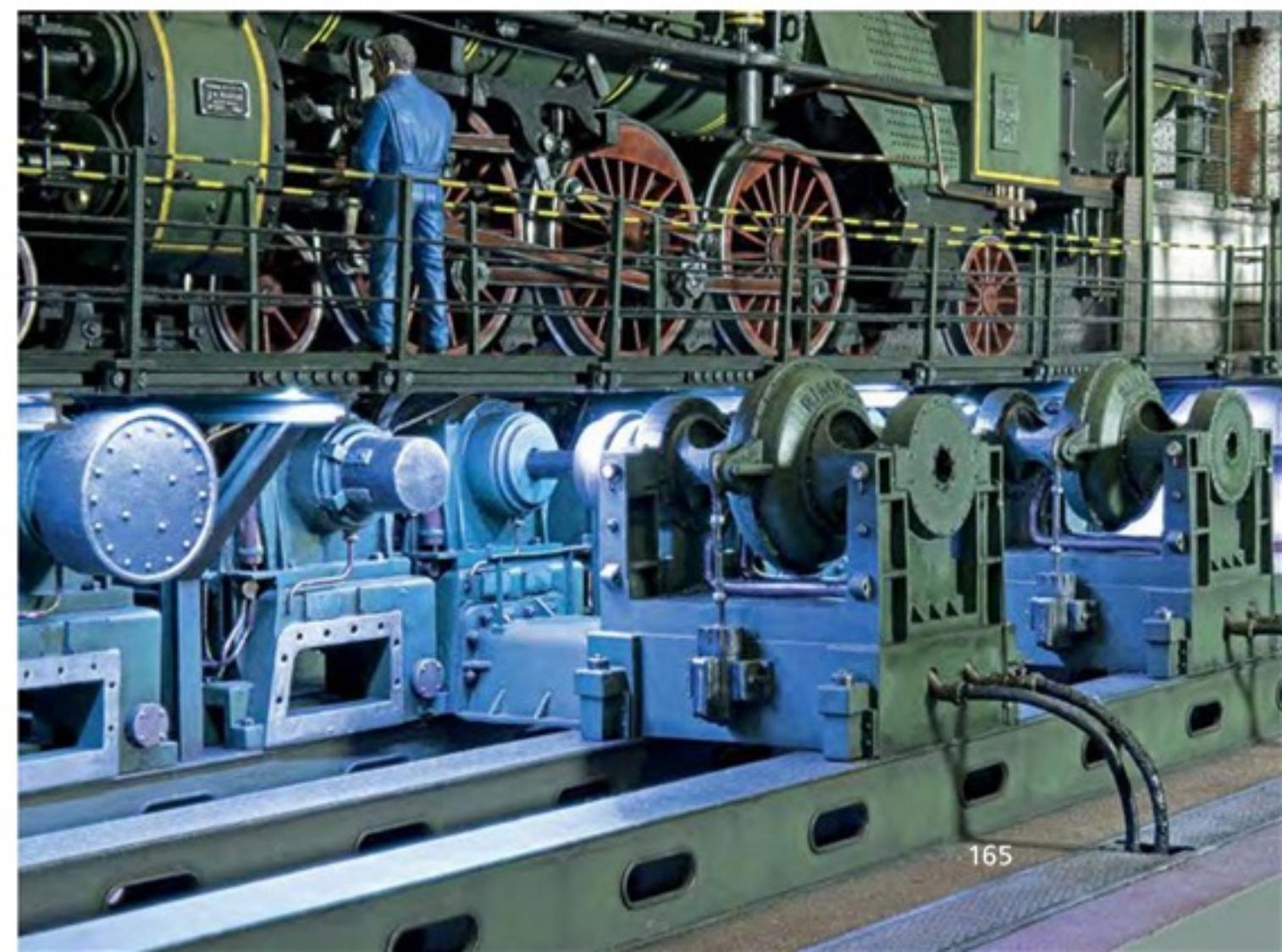
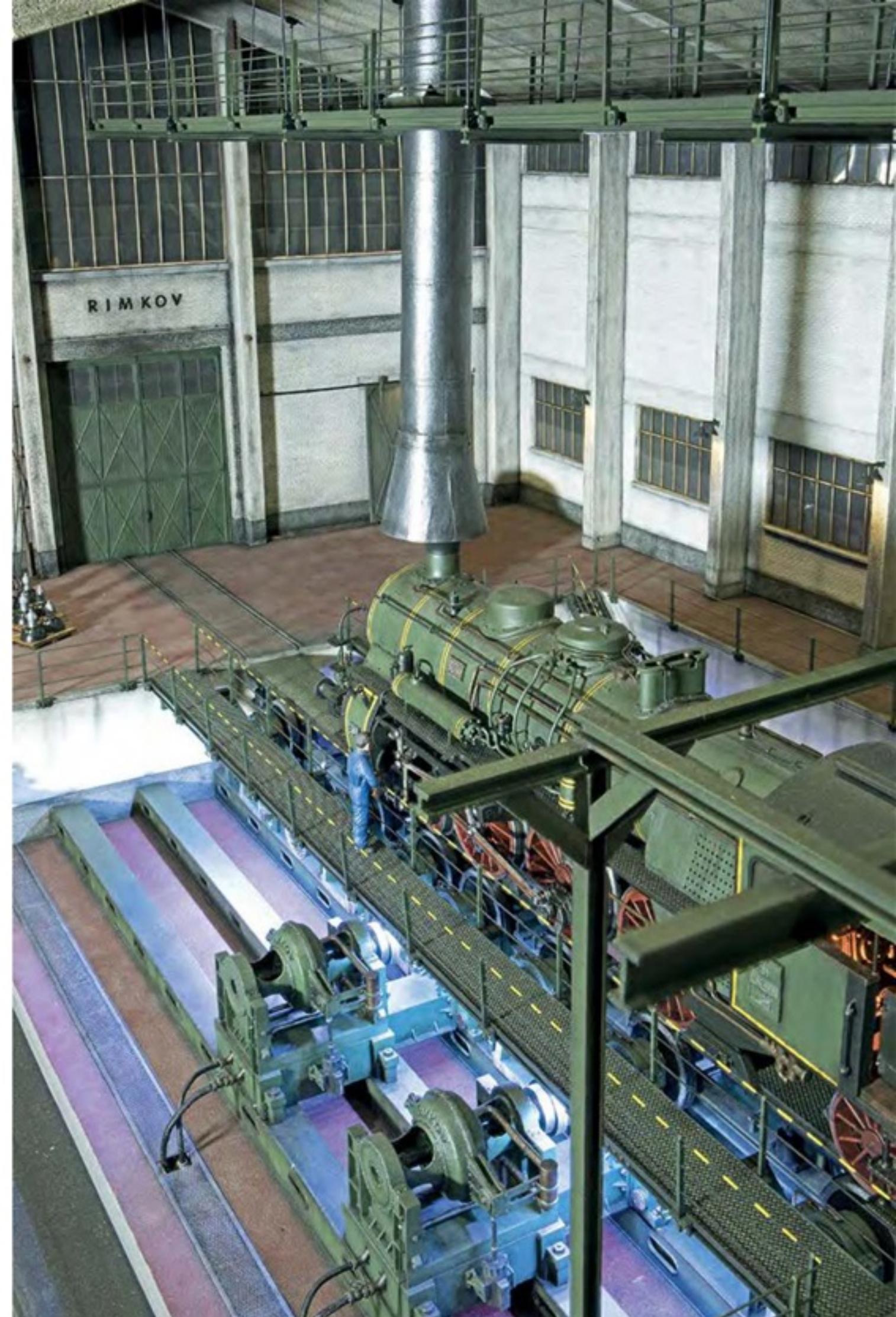
Besides the price there were some other stumbling blocks that stopped us buying a ready-made steam locomotive. Most of the available models have the drive in the tender, and this was not interesting for us for the simple reason that they are not run with a tender on the test bench.

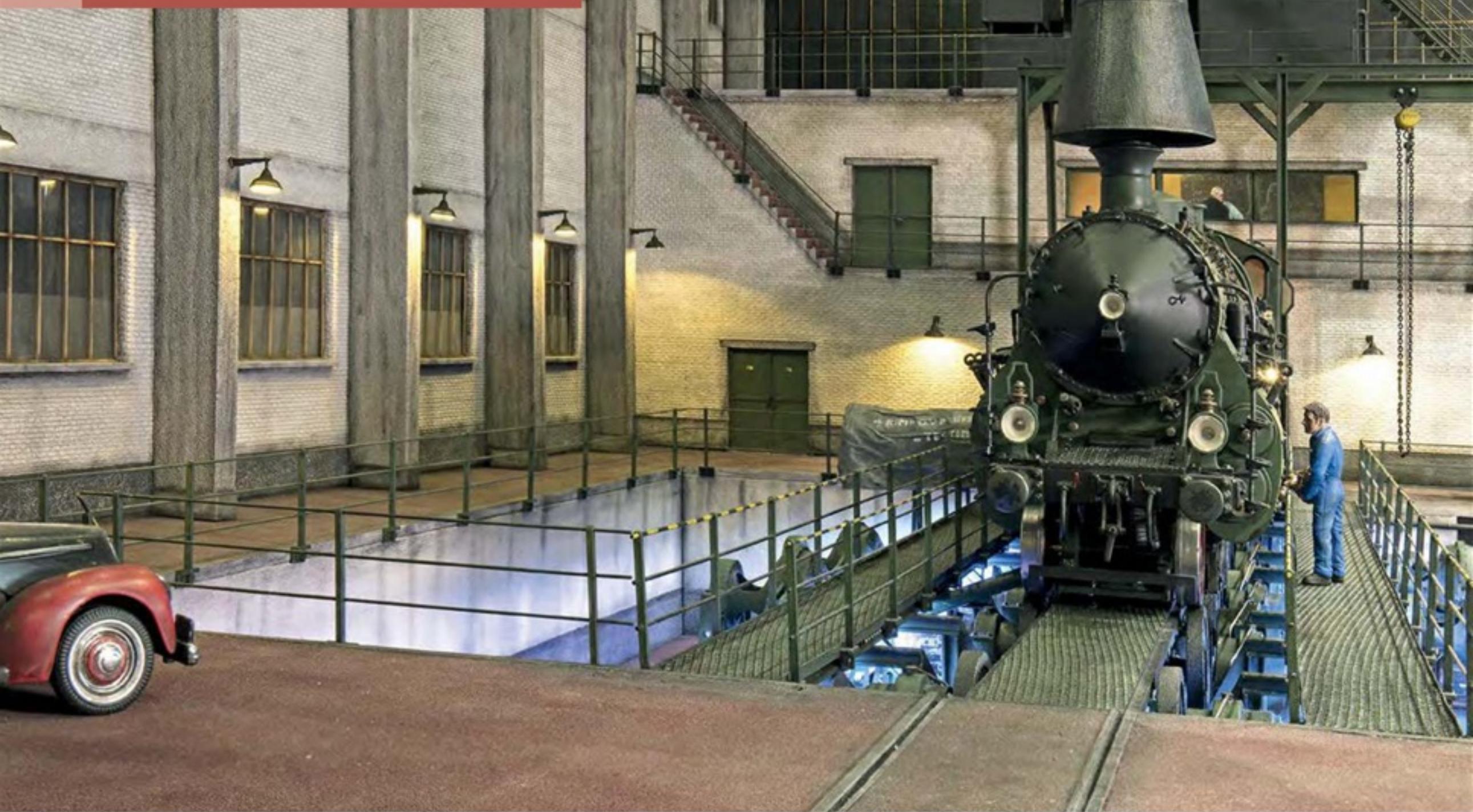
A kit for a Bavarian S3/6 Pacific by the Spanish company OcCre offered an affordable solution. This is a striking and attractive loco, and the kit is very neat, largely laser-cut wood with metal detail parts, though it did need some refinement. Among other things, the wheel flanges were turned down and various parts were filed and sanded; also, certain parts were replaced with nicer ones, some scratchbuilt, some as spares from commercial models. We cannot compare the resulting locomotive with the beautifully detailed models that are available, but this model is quite presentable – it looks good with the necessary painting and weathering.

## Construction

The choice of the S3/6, with the same 4-6-2 wheel arrangement as the locomotive seen in the black and white film, meant we could start planning the test bench. The rest of the layout is completely scratchbuilt. Most of the time spent on this project went into construction of the test bench itself.

The work started by making sketches of every part of the test bench that was seen in the video and photos. If this could be interpreted as one coherent whole, we could start producing a CAD drawing. This way the dimensions and proportions were determined. I say an interpretation because we did not have a proper drawing with dimensions. Once we had the necessary side, end, and top views, Koen started making the 3D drawings. With the purchase of a second-hand printer, it was not long before the first results became literally and physically tangible. This gave Koen useful experience for manufacturing parts for locomotives and wagons.





Once all necessary parts of the test bench had been printed, the support sprues had to be removed – a time-consuming and fiddly task. The further cleaning, painting, and weathering of all the parts of the machine took approximately six months to complete.

Then phase two could be started. Before we were able to start assembling the test bench, we first had to put together the kit for the loco. This took around three months, then it was time to make a start on construction of the base of the test bench – now the axle spacings were known, we could place the various components.

After finishing the entire test bench, pit, and locomotive, we came finally arrived in more familiar territory – making the shed itself with accompanying structures. This was to take another five months. We applied trusted techniques where we had experience. Foamboard, a very reliable material, was much used in this construction process.

For practical reasons the shed was built as two modules, the main part of shed in one and the control room with associated workshop in the other. That way we were able to work on two parts of the project at the same time. Koen handled the wiring of the test bench, locomotive, and con-

Above  
**Smoke must be removed, as the locos on are driven by their own power so performance can be measured – the test bench does not have electric motors, as people sometimes think.**



Left  
**On the left are the doors for locomotives and staff. Pallets of spare shed lamps have just been delivered.**

trol room, while I continued working on detailing the control room and workshop, as well as the walkway suspended under the ceiling of the shed, the movable chimney, and the handrails.

Three months later the moment arrived to bring the two modules together. We then entered the final phase, finishing the platform behind the locomotive cab and installing the coal supply track and gantry crane.

With two weeks remaining before the exhibition, the last job was to make the base on which the layout would be presented.

### Extras that make the difference

To bring the whole thing to life, we used an ESU sound decoder equipped with the original sound of an S3/6. Extra sound effects that can be operated with push buttons provide the right atmosphere in the shed. To make this all sound better (and louder!), an amplifier with a subwoofer bass loudspeaker was set up under the layout.

"No steam locomotive without steam" was our slogan and we decided to put a smoke generator under the layout to provide the necessary 'steam'.

We also consciously opted for a night scene to show off the working lamps in the shed. The effect in the control room is also enhanced. With the accompanying wisps of smoke in the shed, the atmosphere is complete.



Above

A movable chimney that runs along a suspended bridge is manually placed in the correct position to take care of the exhaust.

### Thank you

The new rules for the mini layout competition allowed us to participate as a team. That gave us the opportunity to divide the tasks according to our best abilities. Our story would not have been possible otherwise. I would like to thank Koen for this special co-operation.

Unfortunately still photos do not do justice to the movement of the wheels, but on YouTube you can find videos of our model and compare them with the archive footage of the real thing,



Above

The control room, where all measurements are carefully monitored and recorded.



Right

A workshop is essential for such an installation. A shut-off valve receives necessary maintenance.

**Peter Dale** has modelled a small town station on an *intérêt local* metre gauge line.

Photographs by the author.

# Cap Sacré

More Tramways de la Corrèze than Vivarais



This layout came about as a result of a chance purchase from the OO9 Society sales stand of a resin body for a De Dion NC/ND autorail – so this is a warning about the results of what you may buy! I brought it home and put it away in a drawer that already contained some Billard A80D resin bodies and had the passing thought: “That will not see the light of day again”. However, there was something about that little *autorail* body that nagged away and early the next year it came out again to be built.

Then the question was whether it should be to 9mm or the correct 12mm gauge. As I had nothing else in 12mm, I decided that 9mm would be the better option, and by the time of the Shepton Mallet show in February it was finished.

Then of course the next thing was to finish one of the A80Ds. This sparked a rising interest in French metre gauge railcars helped by the purchase in France of one of the Magazine des Tramways à Vapeur et Secondaires (MTVS) periodicals (No.51) with a large section on various metre gauge railcars, including some drawings. Some appeared on the OO9 sales stand – I could not afford them all at once but at subsequent events they were still there, and were added to my growing collection.

About this time we started going to what used to be Expométrique near Paris, and one or two books were bought about French metre gauge lines. Naturally the best ones are in French, so many years after French O level I started French evening classes – what a wonderful hobby that takes you into these interesting byways!

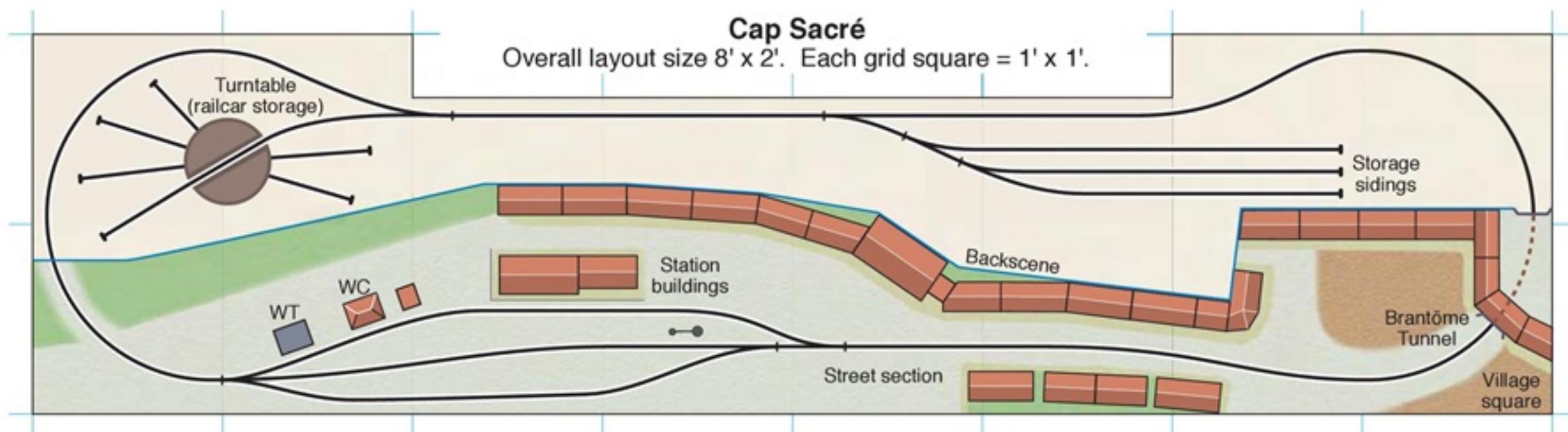
Looking at the books, one of the things that struck me was how the railway was an integral part of the community, with much roadside track, often running in the streets in villages and towns, and sometimes the station was there as well. The interest grew beyond just railcars into a more general interest in French metre gauge lines, of which there were in excess of 20,000km. I started to find French manufacturers of metre gauge models, although Mougel and Gecomodel were no longer available.

To use some of these models, the decision had to be made to change to 12mm gauge, and the railcars were re-gauged. It also meant it was time I started a layout and I wanted to include some street running. But how to bring the railway into the street? I achieved this by going straight through a building – you may think this is very unlikely, but the ‘prototype for everything’ rule triumphs again! I refer to the CFD de l’Yonne at Chablis station – which may give a clue to the contents of the barrels!

Above  
A Pinguely 0-6-0T brings a goods through Cap Sacré station as a De Dion NC *autorail* awaits passengers. The gents urinal just behind the water tower is based on a prototype at Chaves.

Above right  
The Cotes du Nord Blanc Misseron loco.  
The scene was inspired by the cover picture of a MTVS magazine showing the CFD de l’Yonne at Chablis station – which may give a clue to the contents of the barrels!

Right  
The Saurer *autorail* entering the village square.





Left  
The Pinguely 0-6-0T on a short passenger train; the tri-composite coach means all three classes can be accommodated. All three vehicles are from Trains d'Antan kits.

Below  
Shades of the Lozère – two De Dion NC autorails pass in the station.

de la Dordogne and the station at Brantôme – my buildings are based on those at Brantôme. Someone has modelled Brantôme, and if I had been aware of it when I started I might have been tempted down the same route.

There are two baseboards, each 4' long by 2' wide. Baseboard construction uses laminated side pieces to reduce weight and 2" x 1" softwood cross members.

The layout is a continuous circuit with storage sidings at the back and a turntable for *autorail* storage – which is already not big enough!

The scenic section consists of a typical continental three track station at one side and a small village square at the other which are linked by a short section running through the street. Originally the street was going to be open at the front but I later decided to put buildings along the front as well. As it is short and slightly curved towards the front of the layout at each end, not much is hidden, and it is possible to look into the street at each end. The buildings on this side have alleyways between them and the back of one is open allowing the viewer to look through a furnished living room to see the street through the window.



Track is Peco HOM, except for the street running sections where code 75 rail soldered to copper-clad sleepers is used and the gaps are later filled in.

Conventional analogue control is used, and the layout is divided into a number of sections with a dedicated control



Left  
Locotracteur X runs down the street with a goods train.



Above  
**Sarthe tramway locomotive and train in the station.**

panel and colour coding for the various sections. A diagram of the wiring and a record of the colour coding was made as I went along, as I shall certainly not remember it in the future!

The buildings are about 50% kit-built and 50% scratch-built. I approached scratchbuilding with considerable trepidation as this is the first railway layout of mine to reach the scenic stage (although there have been two tram layouts)! Apart from those from Brantôme, two other buildings are based on prototypes: the goods shed is based on the one at Tortoli in Sardinia (there were similar ones in France) and the gents urinal is modelled on one I saw at Chaves in Portugal – it had to be done as I doubt I will ever model Portuguese metre gauge. The main modelling material is mounting board but the goods shed is scribed basswood.

### Locos and stock

Stock was not easy to get – there was not a lot available, and this is still the case. At first I had not really decided my preference for the *lignes d'intérêt local* and bought what I could get.

The first loco was a DJH/Model Loco kit for the Réseau Breton 4-6-0T – I thought this would be good as the instructions would be in English. Not so – it was a commission for a French firm and the instructions were only in French!

The Blanc-Misseron tram loco from the Côtes du Nord is a 3D-print developed by Paul Titmuss, detailed, and running on a Halling chassis.

The Pinguely 0-6-0T is from a Trains d'Antan kit, as is the locotracteur X.

The Sarthe tram loco is from an Aubertrain kit.

Below  
**A De Dion ML autorail makes its way along the street, but it looks as though the competition is just ahead.**





Above  
The De Dion JM4 autorail  
has just run down the street.

Below  
The Pinguely on a goods  
entering the village square.

The autorail collection has grown considerably. Some were resin bodies that were built up and motorised, others were complete kits. Halling 12mm gauge chassis were used to motorise bogie cars where necessary.

Naturally there are several Billard autorails.

The A50D and A50DL are from Maquettes Kitway.

The A80D and trailers and the A150D2 are resin bodies, painted and motorised.

The A150D is from a Worsley Works scratch aid brass kit, finished and motorised.

There are several De Dion cars – hardly surprising considering the number they built.

The JM4 was on the OO9 sales stand "needing TLC". Despite stripping it down, cleaning it, and re-painting, it remained a poor runner until re-motored.

The ML is a 3D-print from Ivan Industries (via Shapeways), complete with bicycles on the back carrier as on the Corrèze.

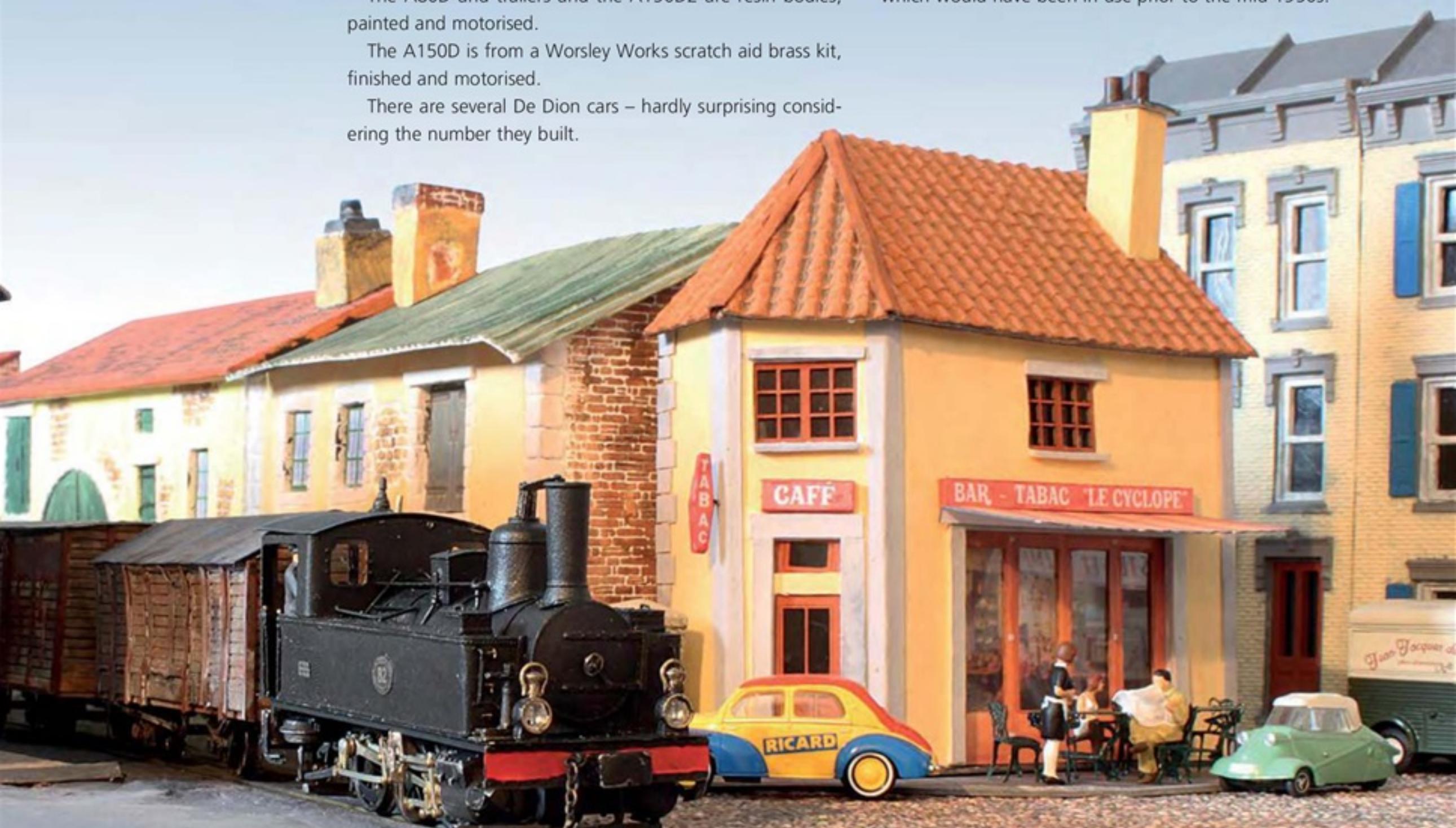
The NC has already been described.

There is also an OC1 from an Apocopa resin casting which is huge.

Passenger stock includes some large bogie coaches from the Vivarais and Réseau Breton. There are four Tramways de la Sarthe four-wheelers built from Trains d'Antan kits. Other coaches more in keeping with the *intérêt local* theme are under construction or waiting to be built.

Most French metre gauge lines had goods stock of three basic types: vans (*couverts*), open wagons (*tombereaux*), and flats (*plats*). Some of these are available as kits and 3D-prints, but as my model is not based on a specific line I decided to build wagons and vans using the ideas from an article in *Voie Libre*; however, instead of card and plastic strips for the vans and plasticard for the open wagons I have used scribed basswood for both. After cutting out this is weathered to emphasise the planking, then sealed before final painting and weathering. A number of flats have also been built in the same way. They all run on Worsley Works brass underframes. A Corrèze Tramways *fourgon* (brake) has also been built using etched basswood.

Road vehicles are mainly French but there are one or two German and Italian vehicles. I have tried to have only vehicles which would have been in use prior to the mid-1950s.



Right

A Billard A80D *autorail* emerges from the tunnel. This may look unlikely, but there was a prototype, at Brantôme in the Dordogne.



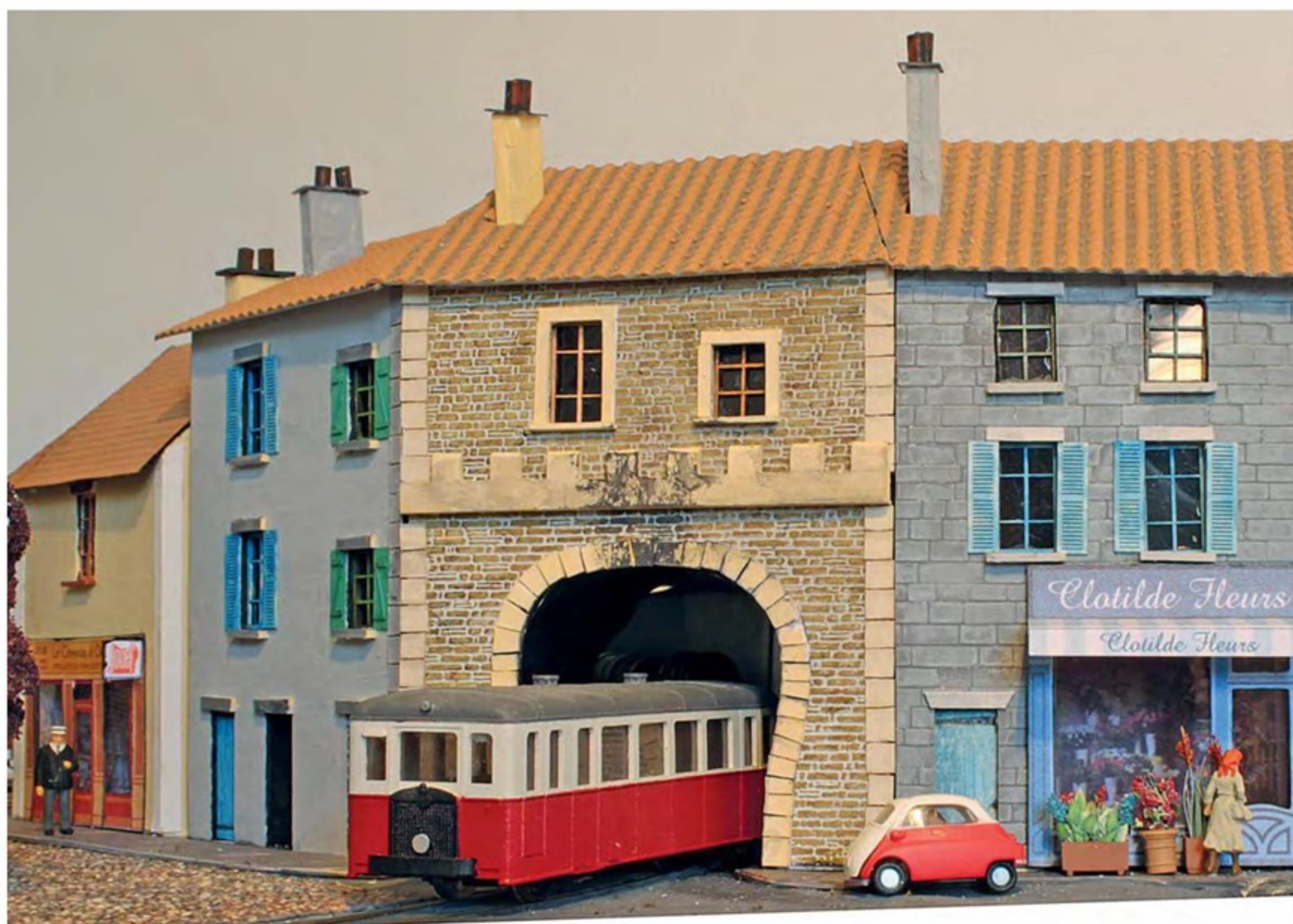
Below

A De Dion ML autorail leaves the tunnel and prepares to cross the village square.

### Conclusion

So one chance purchase of a resin *autorail* body has led me to a whole new branch of the hobby: new kits and books and re-learning French (my old French master must be spin-

ning in his grave!), and visits to the Baie de la Somme line and the MTVS museum at Butry-sur-Oise as well as various exhibitions in France. I look forward to learning even more about the subject.



**Mike Carter** fills a prominent gap on his new layout  
*Norge – a taste of Norway.*  
Photographs by the author.

# Kjeungskjaer lighthouse

A distinctive structure scratchbuilt

**O**n a cruise in Norway, about three hours after departing Trondheim, our ship passed by the iconic Kjeungskjaer lighthouse. This very distinctive structure is located on a tiny island. It is 57' tall, made of stone in an octagonal shape, and painted red. In my opinion it is one of most beautiful lighthouses along the Norwegian coast.

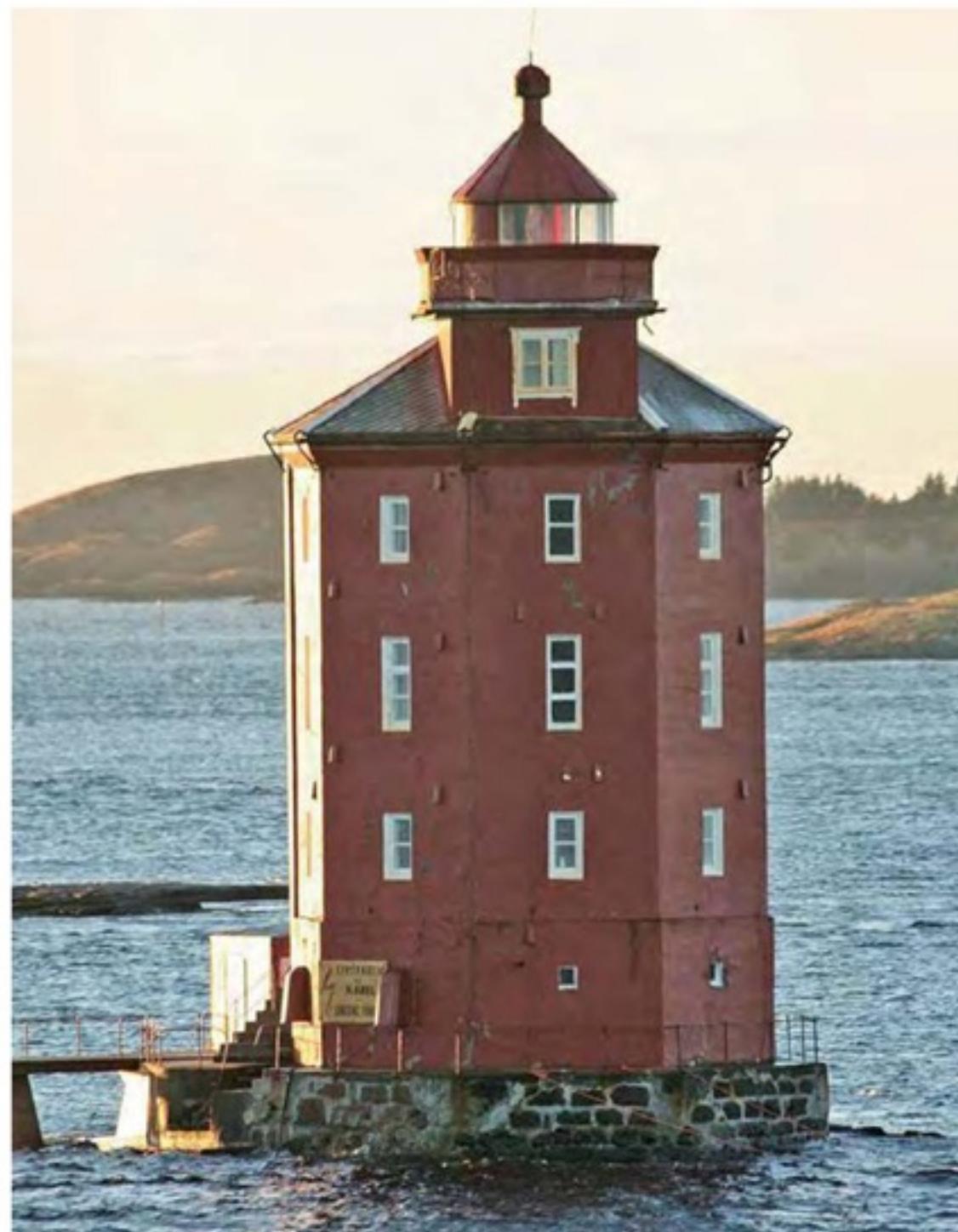
Having scratchbuilt the Bryggen in Bergen (see September 2019) for my new layout, I had left a space on the harbour for a lighthouse. My Red Hook Bay American layout (see

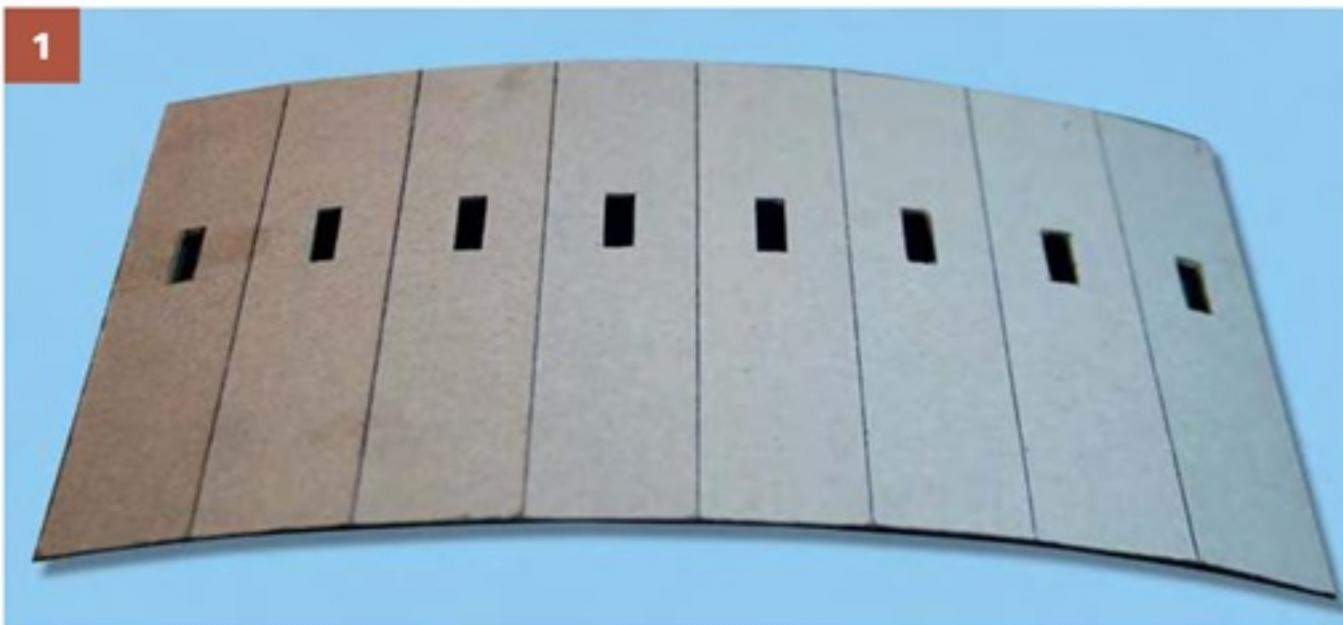
November 2014) has the beautiful Thomas Point Shoal lighthouse, which is very attractive, but this was made from a kit and, as far as I know, there are no kits for Norwegian lighthouses. So I decided to attempt another scratchbuild.

In HO the lighthouse would scale out at 200mm tall. I felt this would overpower the scene, so I decided to reduce it to about 80% of its scale size which would make it just about 150mm tall on the dock and around 205mm higher than the seascape.

Above  
**The completed model**  
in place on the layout.

Below  
**The prototype.**





I had taken several photos of the lighthouse as our ship sailed past. I printed these off and reduced them in size on a photocopier to 83% to aid construction.

I thought hard and long about what material to use for the body of the structure. Originally thin card was tried but when I sprayed it with red primer the surface became very gritty. In the end thin 0.25 mm styrene sheet was found to be ideal. Having realised the card would not work, I carefully laid a sheet of styrene over the card walls and used them as a template to cut the styrene.

There are several windows on each wall, with the middle one being the most prominent. I found a window in the Tichy range that was close to the right size. The smaller ones were made by cutting larger windows in two or removing mullions to achieve a reasonable result. After careful measuring, I proceeded to cut out holes in the walls for the windows.

The floor or base of the lighthouse was made from thick 3mm card. I made each of the eight faces of the octagon 1" long and cut a base using a 45° angle to form the octagon. I then cut eight small pieces of scale wood 1" long and glued them to the top of each edge. I repeated the exercise to form the ceiling for the tower.

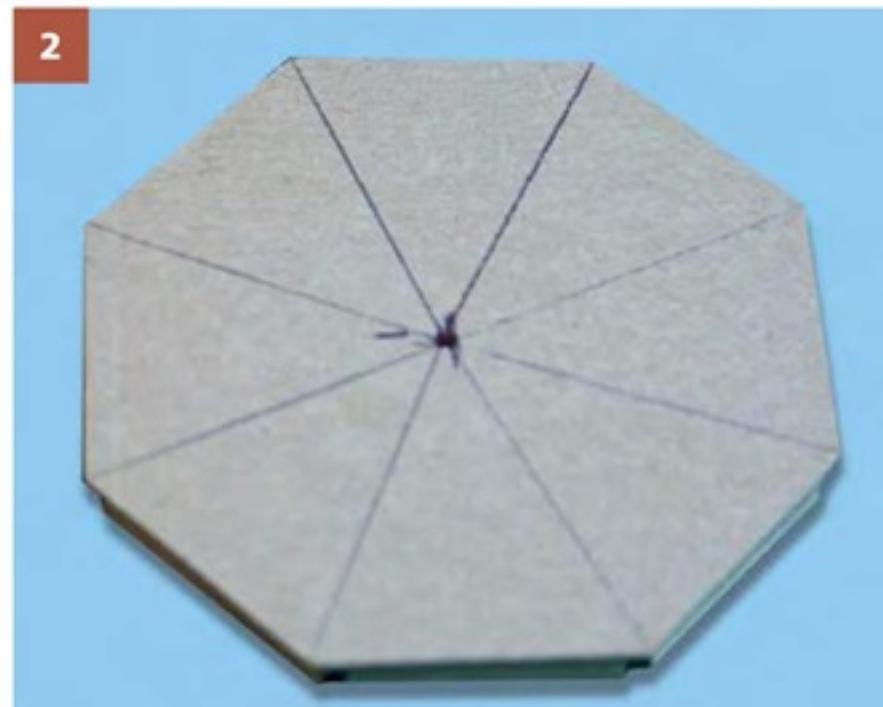
The lighthouse has quite a complicated roof line. Having previously made an octagon like the base for the top I used spare card to make the formers for the roof. Using a photograph, I measured the height of the roof line and then cut card to the correct angle. These pieces were then glued to the roof. I omitted two as there is a square 'shed' below the actual light that needs to be allowed for. The roof panels can then be cut individually to fit.

To construct the little shed, I worked from photographs and used a calculator to gauge the measurements. I cut three pieces of card as templates for the wood. Having rummaged around, I found some suitable basswood off-cuts and cut the shape of walls required.

When cutting out the window openings on such a small wall, I used masking tape on the reverse of the part to avoid splitting the wood. I added the shed walls to the roof.

Using scrap card, I cut six panels for the roof prior to adding shingles. Two smaller panels were cut to go each side of the shed. These were duly covered in grey fish-scale shingles from Northeastern.

For the light I found an acrylic bottle cap the right scale size. It was the right width but unfortunately too tall. However, by mounting it on the sub-roof within the shed the height would be correct. I cut a shed roof to surround the



1. The substructure was initially made from one piece of card, scribed to make folding easier.

2. The octagonal roof base, with wooden strips below to engage with the sides.

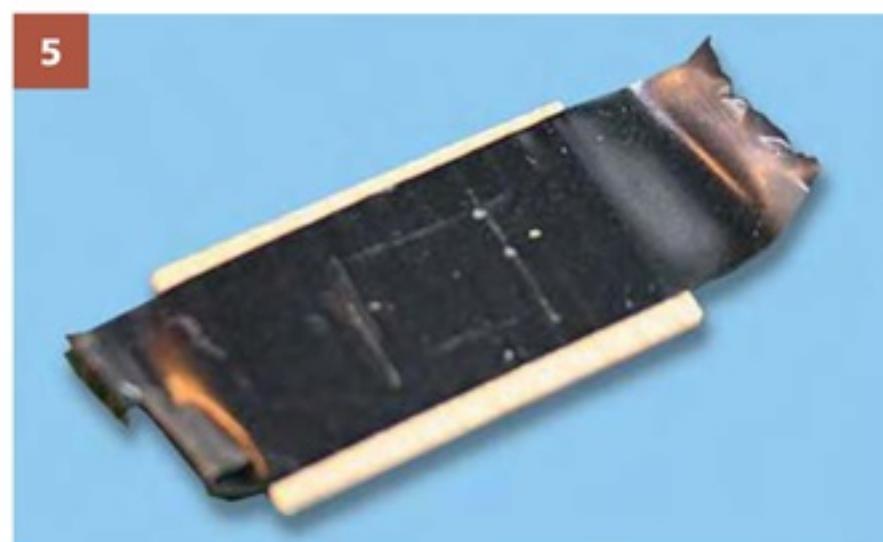
3. The triangular roof supports and roof pieces were cut from scrap card.



4. The card sub-structure for the 'shed' on the roof.

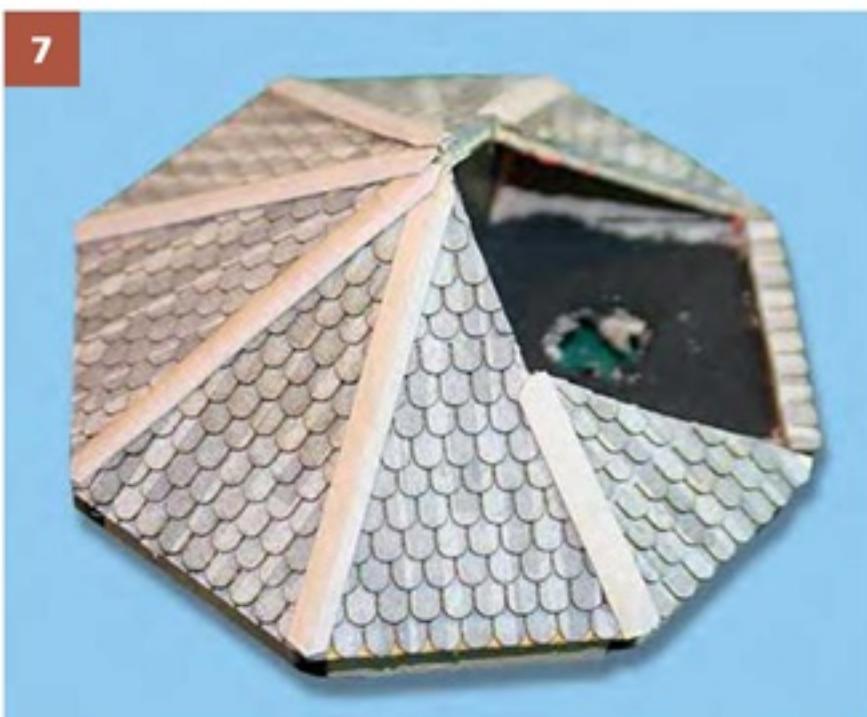


5. Masking tape was applied to the wooden cladding to stop splitting when the window opening was cut.



6. The shed with planked sides mounted on the roof base.





light (bottle cap). I then measured the size and angle of the light housing roof, cut eight card formers, and glued them on top. I then used some DAS clay to form the roof cap, filling in between the card formers.

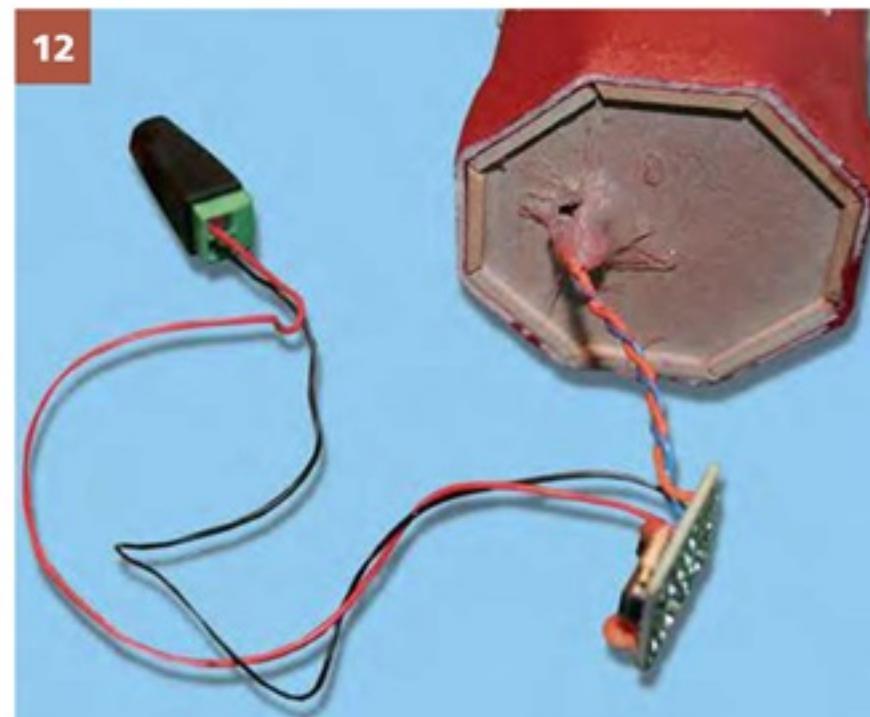
The metal addition on top was a lucky find in a box full of castings.

The fence around the light was made by cutting down some Javis OO Mesh Fencing and backing this onto masking tape. This was then spray painted.

I added the LED light to the structure. I tried using a plastic straw as a conduit to hold the light cable in place, but I could not find one thick enough for the LED. Then I had a eureka

moment. I do not throw anything away (according to my wife). TePe interdental brushes have a clear plastic sheath which I tend not to discard as they might come in useful one day – this was that day! The LED fitted in them, so I trimmed three using my NSWL Chopper and with superglue and masking tape formed a tube long enough to fit completely within the building. So it would remain within the building and not slip out of alignment, I hot glued the wires to the base of the lighthouse.

The LED light blinks every six seconds, worked by a light-house simulator from Layouts4u which it operates on 6 to 20 volts smooth DC or from a 9 volt battery.



7.  
The roof sections covered with grey fish-scale shingles from Northeastern.

8.  
The light housing was made from a bottle cap, again with triangular card pieces to form the roof shape

9.  
The roof cap itself was made with DAS modelling clay.

10.  
The fence around the light was made from cut-down 4mm scale mesh fencing.

11.  
A strip of beading was added to the top of the sides under the roof.

12.  
The flasher unit for the light hangs below the baseboard. A plug allows for removal if necessary for maintenance.

13.

The thicker base was made by adding double-sided tape.

14.

The finished model, ready to install on the quay.



The top of the body of lighthouse was then superglued to the base of the roof.

I added some half-round strip styrene to the top of the walls just under the roof line.

Around the base I added some thick double-sided tape (from Poundland) to represent the thicker stone walls at the base of the lighthouse. I deliberately weathered this by removing the paper from the tape and touching it at various places before spraying with Halfords Red Primer paint.

*Norge* is due to appear at the following exhibitions:

2nd May 2020 – South West Herts., Watford

30th May 2020 – Globalrail, Didcot

3rd and 4th October 2020 – Railex, Fareham



15.

In place on the quayside. Beyond is the row of buildings described in a previous article.

**John Grey** transferred a British layout set in East Anglia to the plains of the mid-West.  
Photographs by the author.

# Kanakee Belt

## A run-down Chicago by-pass

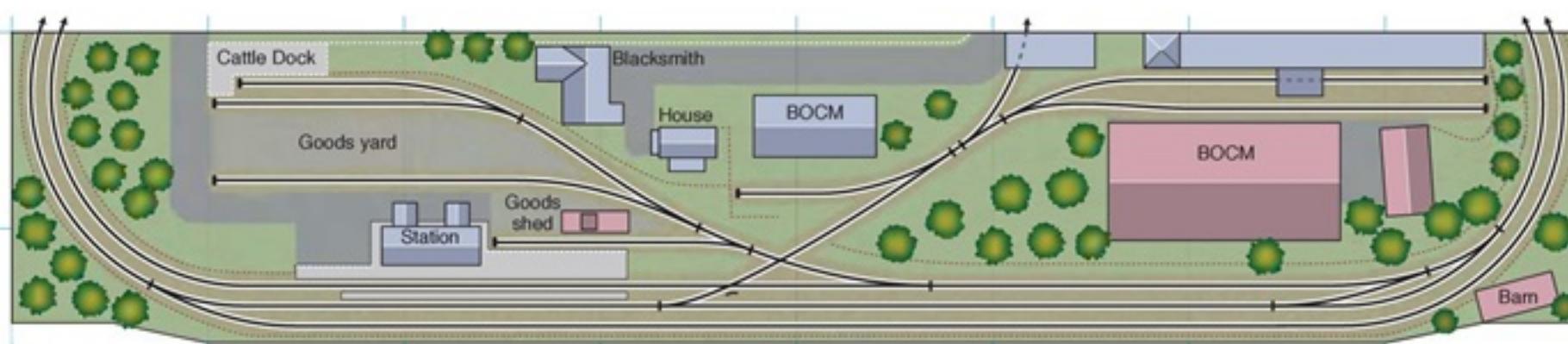
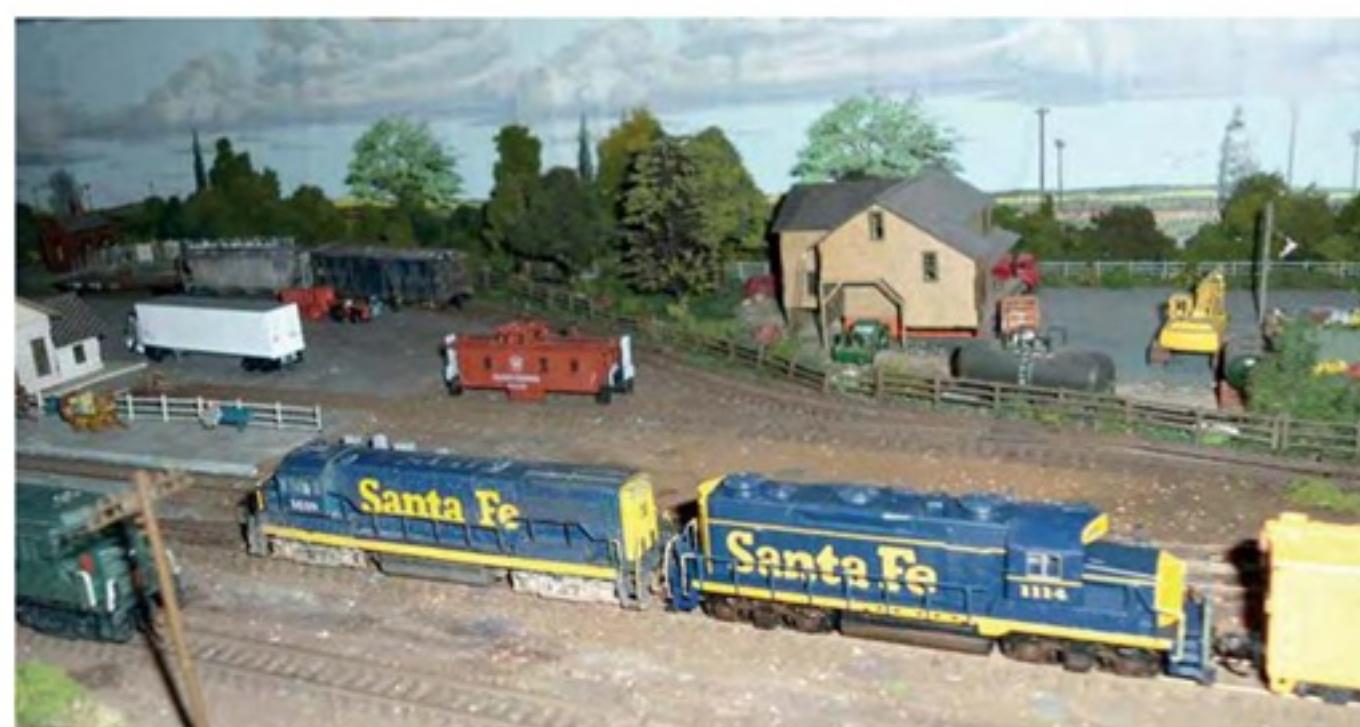
In February 1969, *Trains* magazine published a feature on a New York Central branch line known as the Kanakee Belt. This had been drifting towards closure when it found a new purpose as a Chicago by-pass. At the time the article was written, there were two jointly operated freights running on the line – pool trains for the Penn Central/Santa Fe and Penn Central/Rock Island, plus a Penn Central train serving local industries.

My existing layout was an East Anglian secondary line but it occurred to me that flat country was flat country, and it could just as well be in the Mid West. The B.O.C.M. animal feed factory went from being British to Best Oil & Cake Mill but apart from that not much had to be changed, apart from installing telegraph poles with wide cross-arms. (The station already had light railway style ground level platforms.)

In February 1980 *Model Railroader* had a feature on authentic short passenger trains. I modelled a couple of these – the Santa Fe train number 23 *The Grand Canyon* and the Wabash RR *Banner Blue*. In the late 1960s train 23 was hauled by Alco PAs, which ensured it was well photographed even though it consisted of just a baggage and a couple of cars, plus eventually a buffet. Like many of the short trains described in the article, it started small and accumulated more cars as it progressed along its route. Neither of these trains actually ran over the Belt line but their routes did cross it.

As I had a spare short siding in the fiddle yard I added a fictitious NYC loco-hauled passenger service.

Many of the locomotives and freight cars on the layout have been repainted to fit the time and place. In 1970 the financially shaky Penn Central was obliged to absorb the already bankrupt New Haven RR. The whole mess filed for receivership in 1971. Meanwhile the New York Central



Kanakee Belt Overall layout size 8' x 1' 7". Each grid square = 1' x 1'.

Top  
Santa Fe GE U25B #1618 +  
EMD GP20 #1114 roll  
a freight through Kanakee.

Above  
NYC Alco RS-3 #5348 is busy  
switching the team track.



Above left  
**Santa Fe Alco PAs at the head of a passenger train.**

Above right  
**A Pennsylvania EMD GP30 leads New York Central Alco RS-11 #8000 and Penn Central GE U25B #2558.**

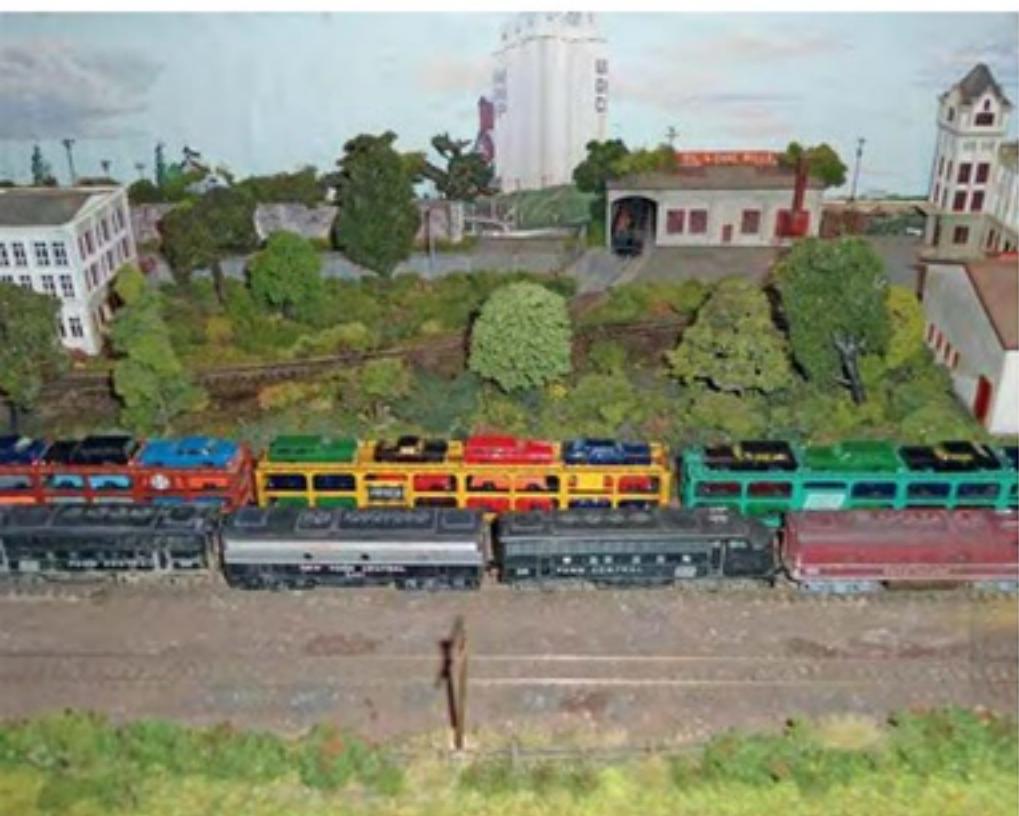
Right  
**NYC RS-3 #5348 delivers a PFE reefer to the outlying B.O.C.M. building.**

Below left  
**A very mixed lash-up of old PC, RI, and NYC F7 A and B units contrast with modern auto racks.**

Below right  
**Mixed motive power from the Pennsylvania, New York Central, and Penn Central is common on this route.**



Because of space limitations, the layout has much tighter curves than I would have liked – down to just 9" radius, but surprisingly modern 85' hi-cubes, auto-racks, and piggyback flats will go round these curves providing the couplers can move freely. Obviously they would look better on more generous curves, but you have to make the best of what you have.



To augment his feature in the recent French Special, **Gordon Wiseman** offers more illustrations of a widely used class. *Photographs by the author.*

# SNCF CC72000

## A high power mixed traffic diesel

The CC72000 diesels belong to the SNCF 'Nez Cassées' (broken nose) design era. They were delivered between 1967 and 1974. According to the UIC wheel arrangement notation system, the 72000s are C-C, not Co-Co, as they have monomotor bogies. This often confuses viewers of the SNCF scene, not least because for years SNCF lettered locos with 'BB' and 'CC' to designate the number of axles regardless of the actual drive mechanism.

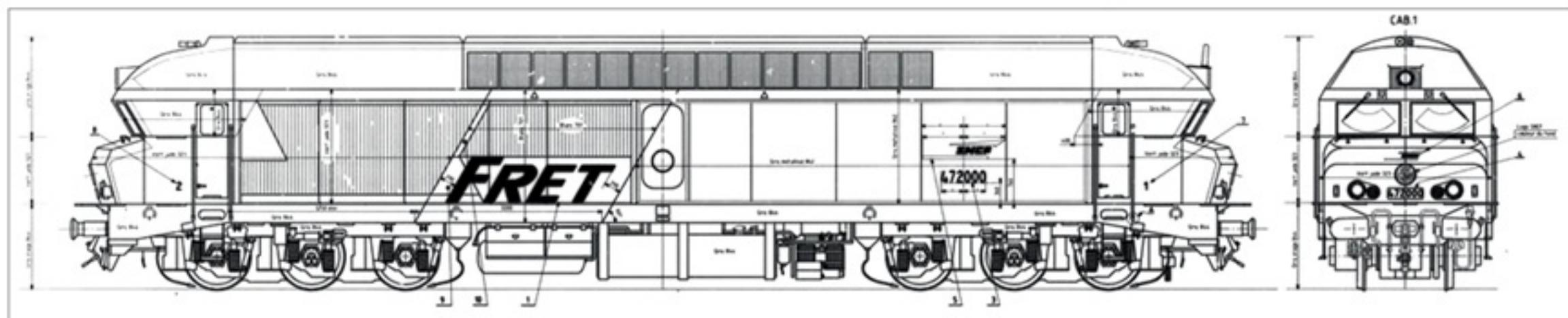
The class took over top link trains on the major non-electrified SNCF routes – nearly all of which have since been electrified.

Their presence and power has always endeared them to enthusiasts – apart from die-hard steam buffs, who decried them as replacements of the famous SNCF 241P Mountains.

For about twenty years they were unchallenged on express trains and on some specific freight routes where their power and route availability was useful.



Builder	Alsthom, SACM
Build date	1967 – 1974
Total produced	92
Wheel diameter	1,140mm
Length	20.19m
Weight	114 tonnes
Fuel capacity	5,500L
Prime mover	SACM AGO V16 ESHR
Traction motors	two TAO 656 B1
Maximum speed	140km/h (72001-16, 18-20) 160km/h (72017, 21-92)
Power output	2,650kW





Ordered in December 1965, the first of ninety-two was commissioned on 20th December 1967 at Rennes depot, and the last of the fleet was delivered on 21st June 1974.

Like the CC6500, the 72000 had monomotor bogies with gear selection.

The first twenty had a top speed of 140km/h, while the rest were geared for 160km/h. Both had the lower gear maximum speed of 85km/h designed for freight haulage but also used when hauling passenger trains over difficult routes such as the sinuous and steeply-graded Ligne des Sauvages on the Lyon – Nantes route and the Ligne des Alpes, which for a brief period in 1972 saw 72000s on a Grenoble – Marseille service via the 'mountain' route.

Above left  
72062, with the original red-backed emblem and number plates.  
Nantes, June 2005. Note the TGV in the background.

Left  
72054 at Vierzon.

Top  
72082 passes near Vesaignes sur Marne on 'Line 4' in the Marne valley, from Paris via Mulhouse to Basel, in 1997.

Above  
72017 pauses at L'Arbresle with the 17.02 Lyon – Roanne peak hour extra on 8th June 2005

Right  
72020 heads south over PN (level crossing) 61 near Soissons with a Laon – Paris Nord express in August 1995.

Left  
From an official SNCF diagram which has been annotated to show the application of the FRET livery.





Introduced to replace express steam on trunk routes such as Le Mans – Nantes / Rennes – Brest/Quimper and the Paris – Clermont Ferrand Bourbonnais route, the CC72000s were allocated to just four depots through their lives – Rennes (1967-1992), Chalindrey (1969 – ), Vénissieux (1968-1992), and Nevers (1989-2012). However, despite their 'prestige' aura, their most high profile work only lasted around twenty years, between introduction in 1967 and the late 1980s.

The zenith of the 72000s was arguably on the *Trans Europ Express (TEE) Jules Verne* Paris to Nantes (395km) from September 1975, and on the *TEE Arbalète* Paris-Est to Basel from 1969 to 1979. They also held sway for twenty years on the Bourbonnais route which included haulage of the named trains of central France, most famously the *Thermal Express* serving the spa resorts of Vichy and Le Mont Dore. For some years they also reigned on the Nantes – La Rochelle – Bordeaux main line down the west coast of France, where they were replaced in the mid-1990s by pairs of BB67300 or 67400 diesels. For a period 72000 were used throughout between Grenoble and Nantes via Lyon, Bourges, Tours, Angers, and Saumur, a distance of some 760km.

They gradually lost out to ever-expanding electrification, particularly in the 1987–1990 period when the Brittany routes went over to TGV as part of the TGV Atlantique, and Paris – Clermont was electrified. For a while they also hauled trains to the English Channel coast from Amiens.

Above  
72067 stabled at Roanne in the late 1990s  
with stock used on the peak hour trains to and from Lyon.

Above left  
72023 arrives at Roanne in on 9th June 2005  
with the 18.07 peak hour extra from Lyon.

Left  
72028 on a Paris – Basel express in 1997.



In their latter years, having lost work to electrification and TGVs, they became better known by enthusiasts for work which they were already doing but to which more attention was then paid. This included heavy freights south from Luxembourg, and cross-country freight from Dijon to Nevers and Clermont-Ferrand, while passenger services included Paris – Laon (until 2002) and cross-country trains, some covering long distances such as Bordeaux – Lyon over the scenic central French route via Ussel and Brive-la-Gaillarde, and Reims – Dijon via Chalons-en-Champagne. They were the regular traction on the Genève – Valence via Grenoble route and appeared several times a day at Basel on passenger trains from Paris Est via Mulhouse, and on freight.

Having lost passenger work, on the introduction of business sectorisation by SNCF in 1999 many 72000s were allocated to the Fret sector and repainted in the green livery. Six of these were later sold to Morocco where they run alongside equivalents delivered new to ONCF. The ex-SNCF locos are still running in Fret green but with logos removed.

In the meantime time three of the class regained prestige status when 72061, 72062, and 72064 were equipped with special couplings (as were the TGV sets) in 2000 to pull Paris – Les Sables-d’Olonne TGV Atlantique trains beyond Nantes to the Vendée coast. These ‘TGV Vendée’ survived until 11th December 2004. The hauled TGVs were replaced by cross platform connections at Nantes. The 72000 still hauled the replacement services, albeit now formed of moribund 1960s coaches of the UIC and USI types in their distinctive green and grey livery. This developed for a time in to a small network of 72000-hauled commuter services between Nantes and the coast.

The same combination of loco and stock was used for a good few years on long distance peak period services west of Lyon where their power was useful for hauling well-loaded commuter trains in hilly territory, notably on the runs to and from Roanne via two different routes. For a time this made Lyon – Roanne something of a hotbed of 72000 use, with

the commuter trains joining 72000-hauled inter city trains on the Lyon – Tours – Nantes route.

The last daily regular use of CC72000s was on remnants of this once important cross-France route, on Tours – Lyon services, and some associated shorter commuter-orientated hops such as Tours – Nevers. Because of their reduced daily mileage, the three ‘TGV Vendée’ machines were among the last four 72000s in commercial service, which ended on 13th December 2009.

The three remaining 72000s (049, 074, and 084) were all allocated to Chalindrey, and to the ‘Materiel’ (traction/rolling stock) sector. They are kept in reserve and used occasionally for moving stock from one place to another around France. 049 and 074, which also received ‘En Voyage’ livery in their latter days as passenger locos, still occasionally appear on 72100 turns. 72084 is more often to be seen on ad-hoc materiel duties, and appears regularly at heritage events, often accompanying and/or acting as a reserve for steam locomotives.

Above  
72034 on a Paris – Basel express in summer 1999.

Below  
72042 at Lyon Perrache after arrival from Tours in the last days of CC72000 passenger operations.





Above

'Celebrity' 72084, which was retained in original livery with raised plates and numerals, powers through Longueville (Seine et Marne) with ballast hoppers in 2011.

Below

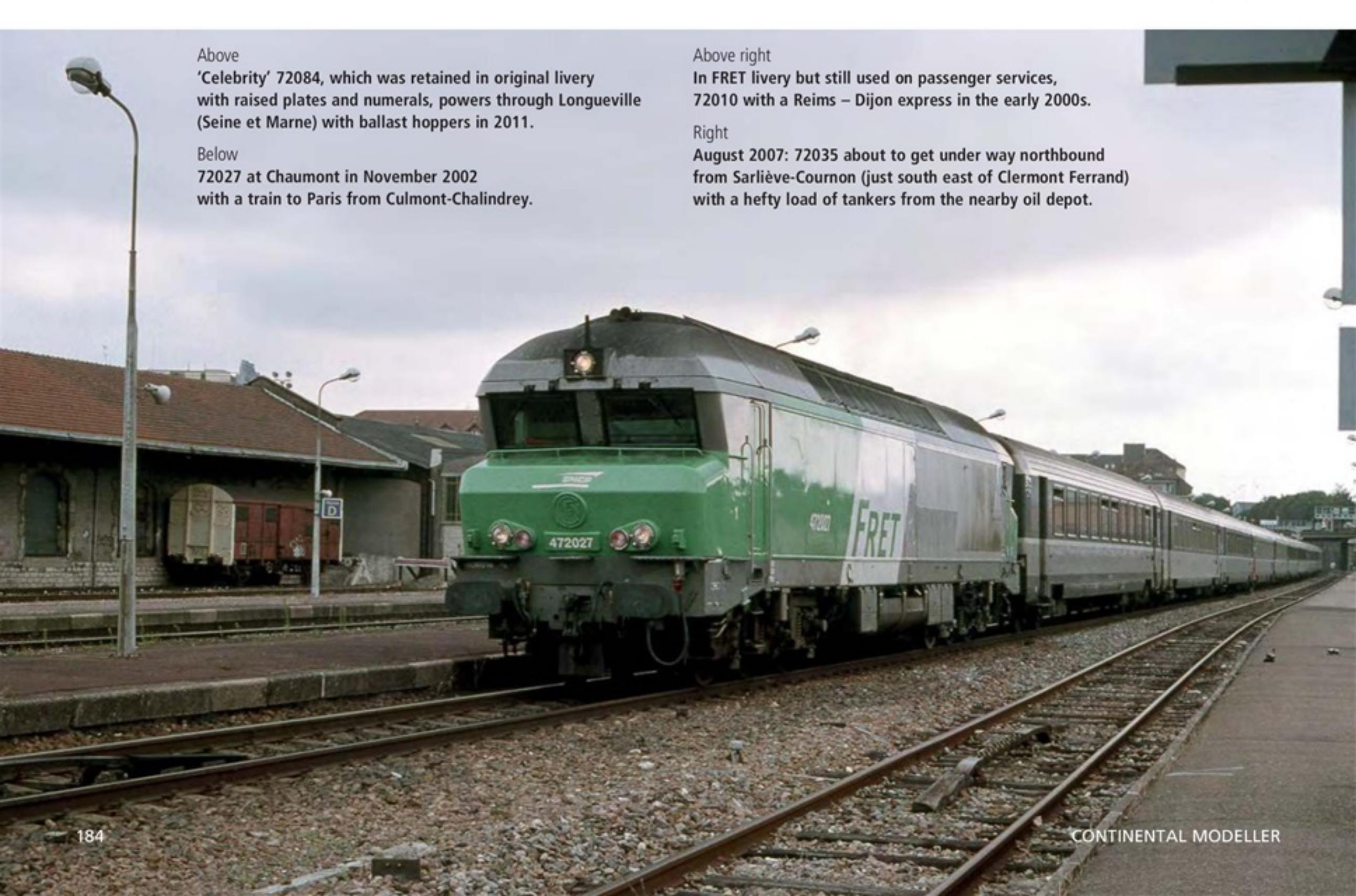
72027 at Chaumont in November 2002 with a train to Paris from Culmont-Chalindrey.

Above right

In FRET livery but still used on passenger services, 72010 with a Reims – Dijon express in the early 2000s.

Right

August 2007: 72035 about to get under way northbound from Sarliève-Cournon (just south east of Clermont Ferrand) with a hefty load of tankers from the nearby oil depot.





**Phil Sharp** has a new layout in a spare bedroom.  
Photographs by the author.

# Berlin Pankow

A room full of East German HO

**T**his layout replaced *Berlin Ost*, which was featured in the June 2016 issue. That was built in my loft and due to the extreme summer heat it suffered from many problems: most traction tyres perished or expanded, points

and double slips could cause the locos to derail (I used Fleischmann Profi track and the double slips in particular would rise in the middle), and it was often just too hot to operate the layout!



Right

A BR44 three-cylinder 2-10-0 gets a freight under way.

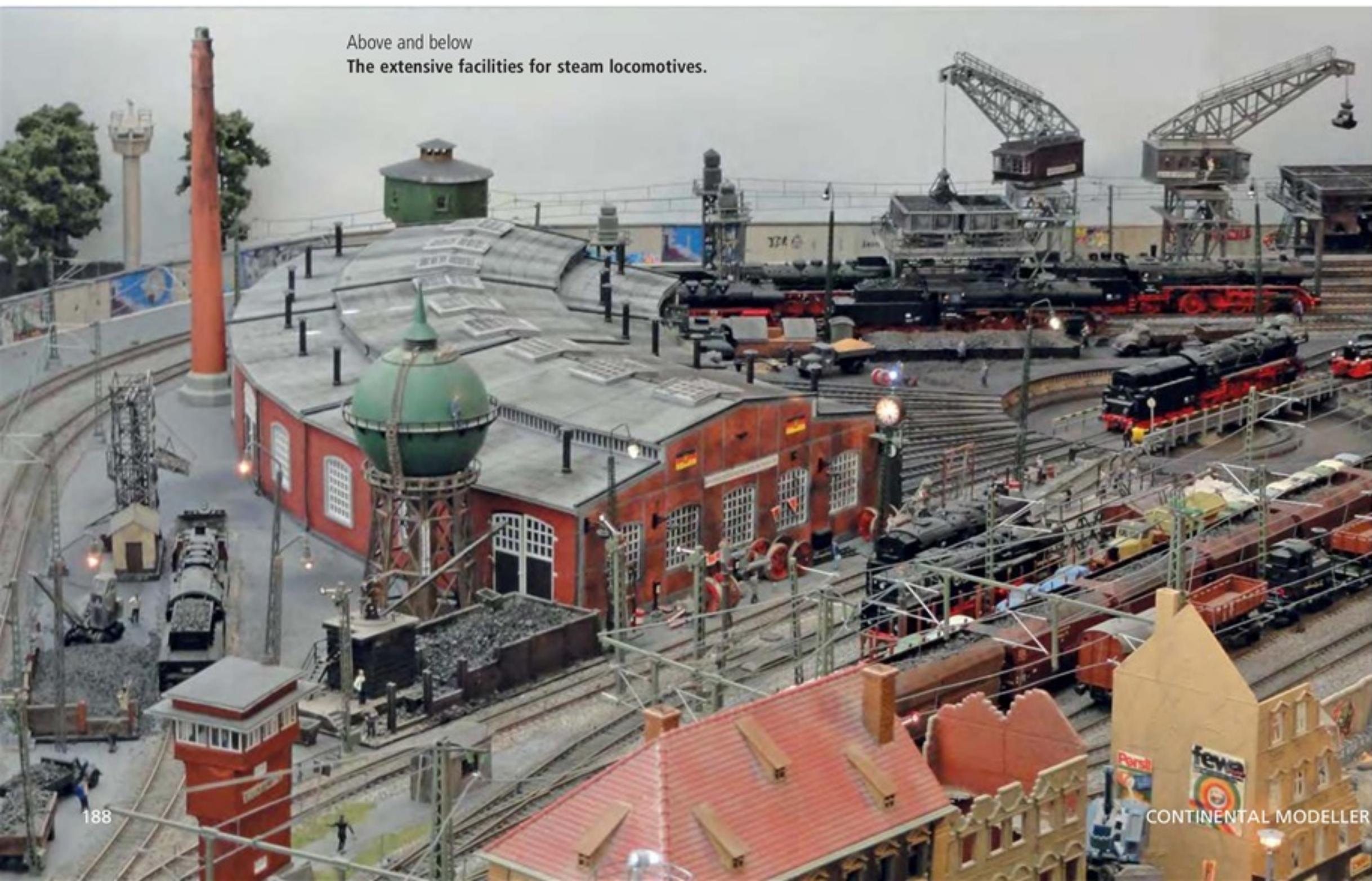
Below

The station throat area, looking towards the border and the American checkpoint.





Above and below  
The extensive facilities for steam locomotives.





Above and left  
The diesel depot, fitted in  
to a convenient recess.

Below  
Looking towards the shed  
over the central part of the  
main station, packed with  
characteristic double-deck  
coaches and S-Bahn stock.

My parents, who live very close to me, agreed to let me have one of their bedrooms for a new layout.

After careful consideration I commissioned White Rose Modelworks from Bedale in Yorkshire to build me a layout. The friendly and professional staff agreed to visit me at my parents' house to take precise measurements and to discuss my requirements.

The room (approximately 14' x 14') is smaller than my loft but they managed to fit in all my requirements, which included a station, a steam depot, and a traverser.





Above  
**An over-track signal box controls this end of the station, where the police are dealing with trespassers.**

Berlin Pankow is modelled with a flavour of East Berlin in the 1970s and 1980s during the height of the Cold War. The layout includes Checkpoint Charlie, the Berlin Wall, and watch towers.

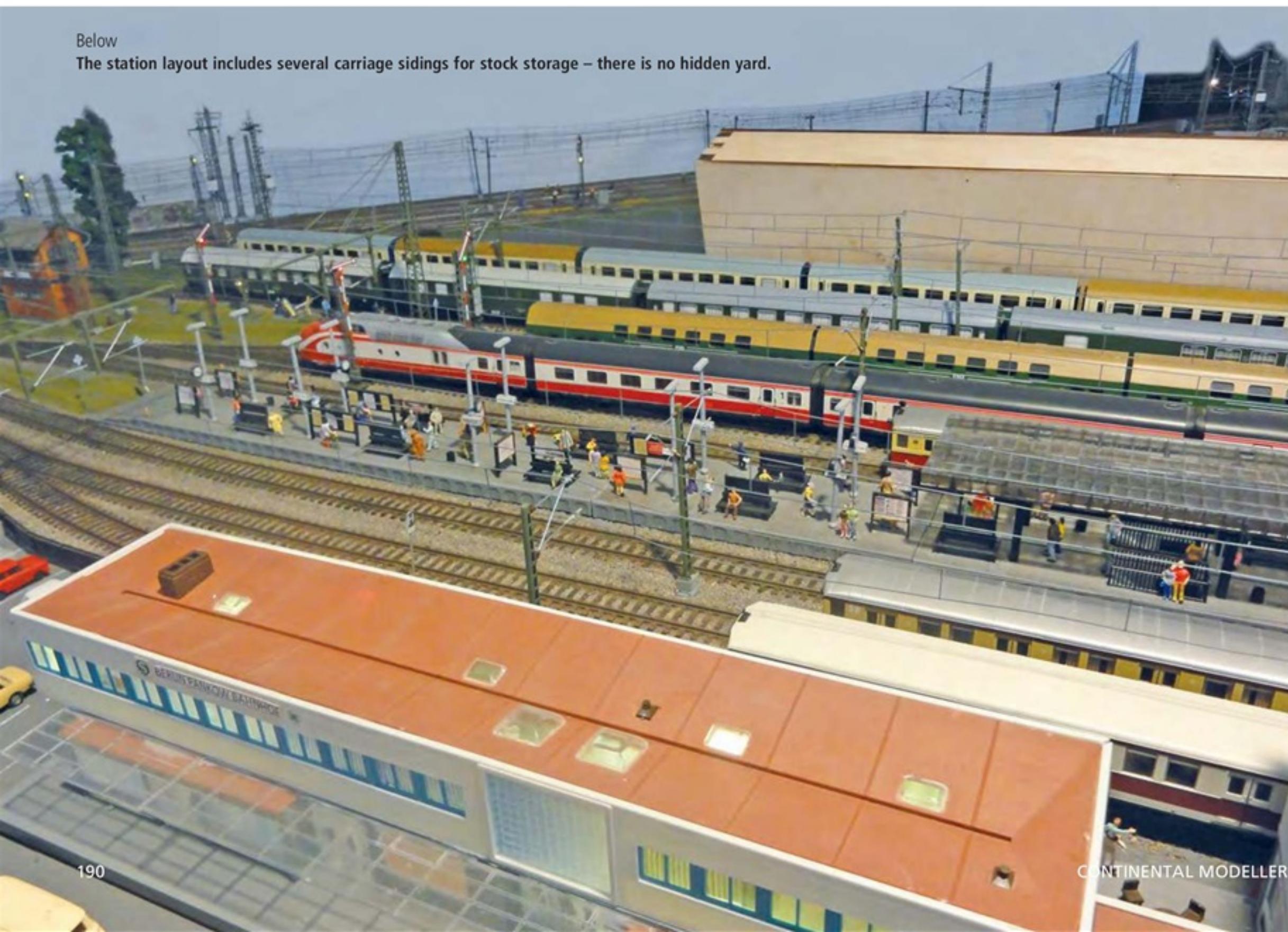


White Rose built the baseboards, designed the track layout, laid and weathered the track, and undertook the wiring. Peco track was used along with their point motors and control switches. They also built the superb control panel. All my locos (Gützold, Piko, Trix, Roco, Fleischmann, and Liliput) ride the track well without any derailments. The turntable is Fleischmann and the traverser is Brawa.

I have two outer circuits, which I call express lines, and two inner circuits, local lines. The local lines are mainly used for the Berlin S-Bahn which is a favourite of mine.

There are six lines running through the station, plus storage sidings for passenger coaches and goods wagons.

Below  
**The station layout includes several carriage sidings for stock storage – there is no hidden yard.**



Left

To make it Berlin, there had to be S-Bahn trains.

Control is analogue DC using Gaugemaster panel mounted controllers.

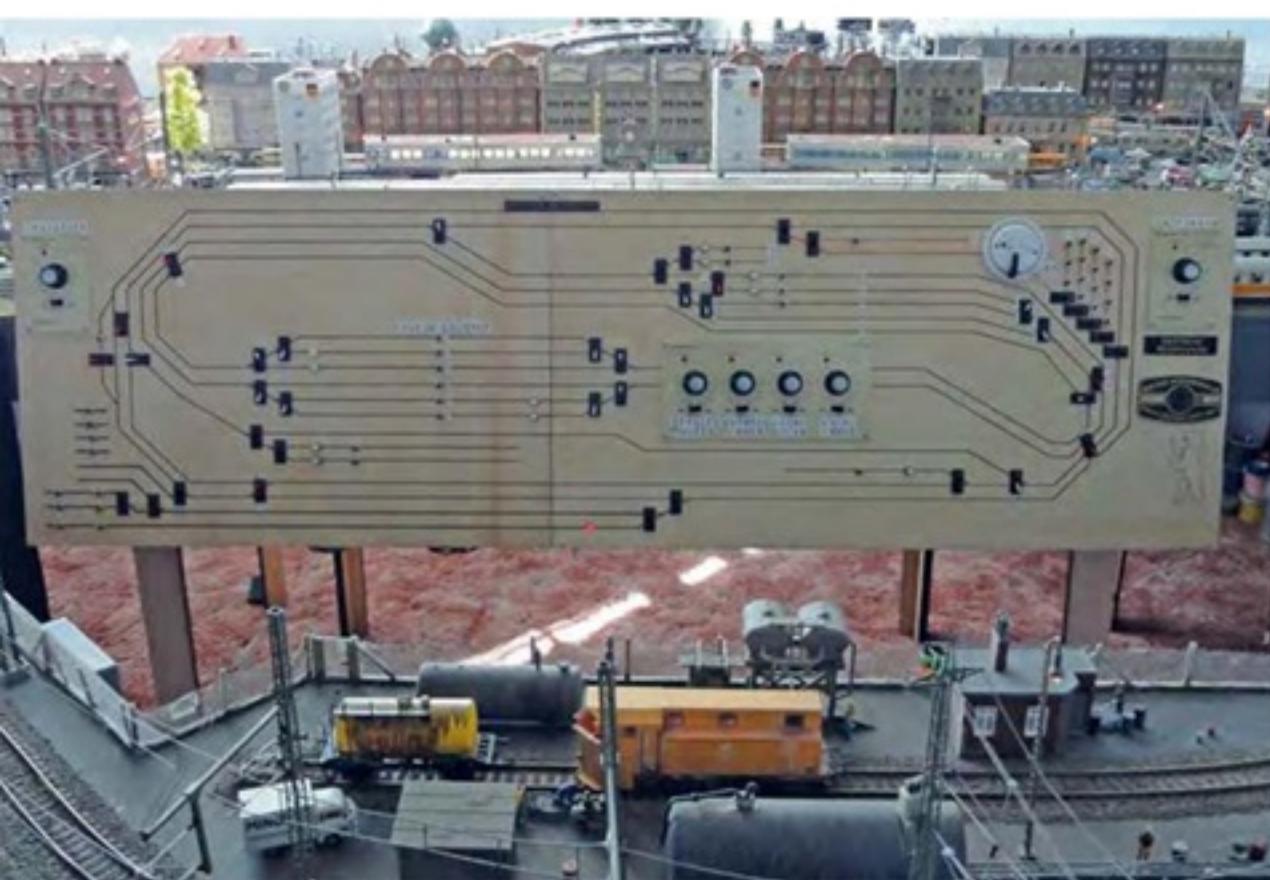
Viessmann semaphore signals were installed and move when the point switch is thrown. These signals are superb but very delicate!

The Viessmann catenary (mostly installed by me) is not live but I believe it is a must if running electric locos.

Viessmann and Brawa lights have been used.

Many buildings, vehicles, and figures came from my old layout.

It is now so much nicer to view and operate the layout in the comfort of a room, and I am sure it will give me many years of pleasure.



Below

To get the maximum length, the station is situated diagonally across the room.

Above

The schematic control panel.



Above

Star Pacific 18 201 departs with an express as workmen assemble new catenary masts.



The modular layout being developed by **Club Proto 87** will be one of the attractions as part of the Proto 87 convention at this year's RAIL show in Houten, The Netherlands, from Friday 21st to Sunday 23rd February.

# Docks Ceinture

A suburban belt line serving docks and industries



Members of Club Proto 87 have shown this developing layout several times at the RAMMA exhibitions in Sedan and at RAIL in Houten. In fact, a small team of four set out originally to build an industrial scene, the Rue de la Glacière, which was later expanded to include part of the belt line. Made to Proto 87 fine scale standards, it has taken several years – and work continues.

For more information, see [www.club-proto-87.com](http://www.club-proto-87.com)

Right

The *Train Automoteur Rapide* (TAR) is by Fulgurex.

Below

040 DA diesel (model by Electrotren) with a goods train.

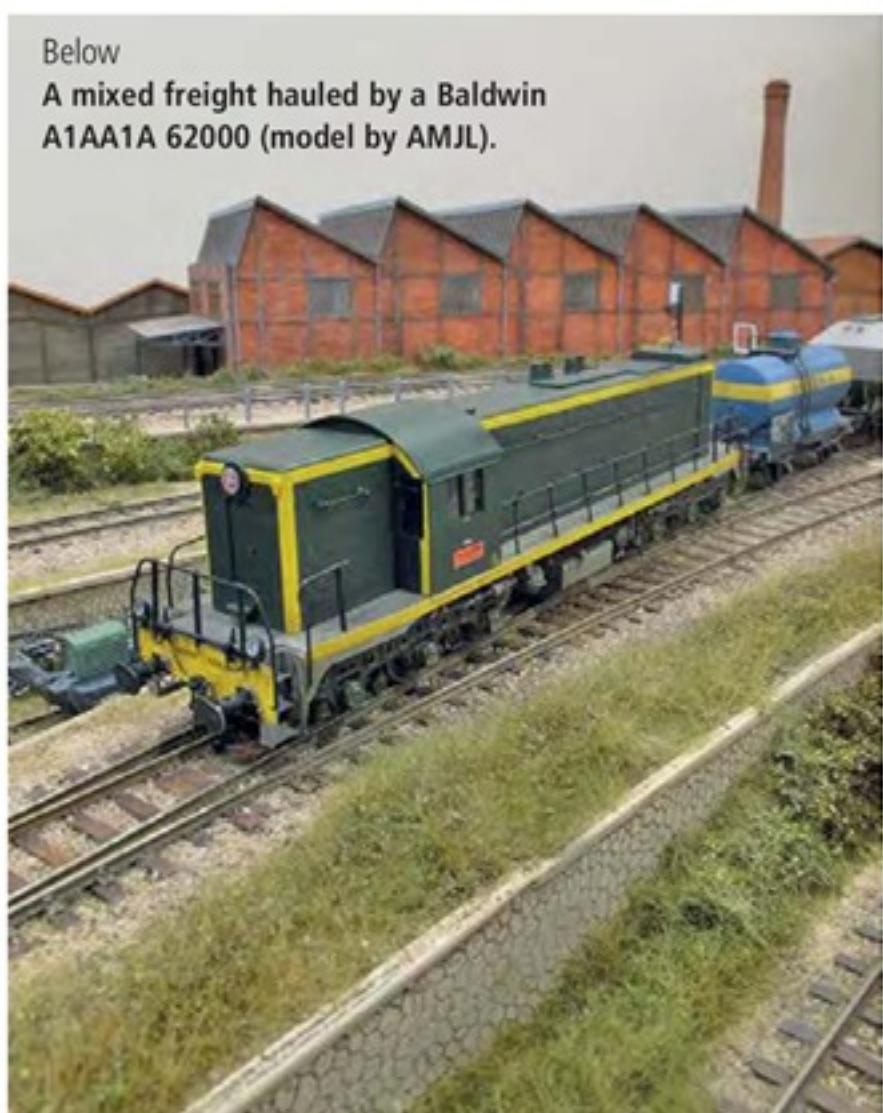




Above  
The Rue de la Glacière section  
includes some structures by Emmanuel Nouaillier.



Right  
030 TU (model by REE) passing the ice house.



Below  
A mixed freight hauled by a Baldwin  
A1AA1A 62000 (model by AMJL).

Left  
The figure is Preiser, the wooden toy train was made using a laser cutter.



Below  
The dock area itself is still work in progress.



# Reviews

Evaluated by our own specialist staff



## HO Deutsche Bundesbahn BR181.2 electric new from Piko



The BR181.2 Bo-Bo electric was a dual frequency machine (15kV 16.66Hz and 25kV 50Hz) designed for cross-border working into France and Luxembourg. Twenty-five were built in 1974-5; rated at 3,300kW and with a top speed of 160km/h, they were suitable for both passenger and freight services and used on the then recently electrified Mosel line, from Koblenz to Trier, and beyond. The first was withdrawn in 2002, but the last survived until 2019. The class leader has been preserved, and is (appropriately) at the DB museum's Koblenz Lützel base.

New in the Expert series from Piko is an excellent model of this technically and visually distinctive type. It is very accurate in all dimensions and details, according to published data and illustrations.

The colours are clearly defined, and all markings are clearly printed, identifying the machine as 181 205-6, based in Saarbrücken, with an overhaul date of 1982. The makers' emblems are apparent, and the metallic beading, lamp surrounds, and window frames are delightful.



The body boasts subtle moulded features, such as the reinforcing ribs and grilles with louvres and bolted frames.

Separately added details include the handrails, windscreens wipers, multiple unit connectors, horns, aerial, walkways, and the insulators, cabling, and switch-gear on the roof. The grille over the braking resistors is etched metal.

The type SBS67 pantographs are fine, and sprung, but not wired for current collection from the overhead. They carry different width contact strips for the different networks, as per prototype.

The cabs have flush glazing and there is interior detail in the cabs and the machine room, the latter visible through the characteristic bodyside windows.

Dummy scale couplings, brake hoses, and electrical connectors are provided for the purchaser to fit in place of the working couplers if the model is only to be displayed.

The head and tail lights change according to the direction of travel; in DC they need about 4 volts to illuminate, at which level the model is moving at

c.40km/h. Under digital control, cab and machine room lights can be switched separately.

The bogie sideframes exhibit very crisp moulded features, and added details such as the cab steps and sandboxes with delivery pipes (which are in line with the rail head), air lines, brake rod, and horizontal and vertical shock absorbers. Behind the frames, and hardly visible, details of the traction motors are represented. The wheel faces are also correctly differently detailed each side – a remarkable level of detail.

The underframe is well detailed with the major features cast or moulded in place, enhanced with added small components such as controls, switches, and guard irons.

The model has a cast metal frame carrying a can-type motor with two flywheels, driving both axles in both bogies via cardan shafts and reduction gearing. Current is collected by wipers on all wheels.

Two traction tyres (one on each inner axle) and a weight of c.500g ensure

more than enough pulling power.

The only deviation from NEM standards are the slightly deep flanges.

Performance is quiet, smooth, and controllable, with the top speed a little higher than the prototype but within NEM tolerances.

The model has a NEM 658 PluX-22 socket for a digital decoder, with provision for also installing a loudspeaker and a 'stay-alive' capacitor.

Standard couplers are fitted in NEM pockets on close-coupling mounts on the frame.

The minimum radius recommended is 358mm.



Another first class offering from Piko, representing excellent value for a unit with a relatively restricted area of operation in real life.

We can no doubt expect blue/beige, orient red, and traffic red versions in due course.

### Manufactured by

Piko Spielwaren GmbH,  
Lutherstraße 30,  
D-96515 Sonneberg, Germany.  
[www.piko.de](http://www.piko.de)

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[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
ref.51340 DC £175.00.  
ref.51342 DCC sound £265.00.

**HO**

## BR86 2-8-2T new from Roco



The BR86 2-8-2T was one of the Deutsche Reichsbahn Gesellschaft *Einheits* (standard) designs, introduced in 1928. They were supplied by various builders, and 776 had been delivered by 1943 when war conditions curtailed production, even though it was one of the few types authorised for continued manufacture in the early part of the war.

It was designed as a powerful mixed traffic branch line machine, especially suited to heavier freights on steep gradients, with an axle load under 15t. It was rated to haul 650t at 70km/h on the level, and while capable of 100km/h was only allowed 80km/h in either direction. The bar frames helped to cope with track in less than perfect condition.

There were various alterations during construction, notably the increased use of welding, to save weight and material. From 252, the tanks were welded. From 230, a steel firebox was fitted (to save copper) and the frames were 100mm longer. At about the same time the braking was significantly improved, with clasp brakes on all drivers and carrying wheels replacing simple shoes on the leading edge of just the drivers. The motion bracket was modified from 358. Many later machines had Krauss-Helmholtz trucks, equalised with the outer axles, instead of simple Bissel trucks. From March 1942 they were delivered without a cab side window as a wartime austerity measure. Disc wheels were also used in the pony trucks.

There were many other variations over time, both during construction and as a result of maintenance and exchange of parts.

The DB inherited 378 machines; they were used until 1974. On the DR a handful lasted until 1978, and one even to 1990!

Despite inheriting a respectable for its time model of this class from the Fleischmann HO range, Roco have created a completely new item. Among the initial releases is a Deutsche Bundesbahn Epoch III machine, representing the ori-

ginal form with the short cut-out at the front of the water tanks (only as far as the rear of the cylinders), a smokebox door without central lock (which suggests a later build), and fitted with DB reflector lamps. Having different size lamps on the front footplate and smokebox door is prototypically correct

The model is very accurate, according to published dimensions.

It is numbered 86 257, retaining broad DR-style numbers, based at Kempten in the Augsburg administrative area. The overhaul date is 6.10.60.

As optional extras there are etched metal plates for the DB emblem and numbers, to be stuck carefully over the neatly printed markings; the plates have to be burnished to reveal the raised metal characters.

The body is a combination of plastic and metal parts, though you cannot tell which is which from the colour: the tanks and bunker are metal, the cab and boiler plastic. The main components are detailed with many separately applied parts, including turned brass whistle, generator, both brake pumps, and feedwater heater, along with most handrails, conduits, and pipework (all bar the smallest lubrication lines). It is quite tricky to remove the body (in two stages), with several fine pipes and handrails to be disengaged.

The rivets are subtle, almost scale size. The tanks are the original pattern but have no rivets.

The open front deck with fine steps and struts, and chequer plate surfaces, is particularly notable.

The coal bunker access ladders are resilient plastic.

There is an unobstructed view through the cab, though the lower part is full of the mechanism so crew figures would have to be legless.

Supplied accessories include piston tail rod covers plus full and attenuated brake hoses and dummy scale couplers so either end can be fitted out for operation or display.

The wheels are metal, and have the correct axle ends (as usual for Roco).



The fine motion is made up of metal and plastic parts, with jointed coupling rods, and return cranks correctly at different angles each side – commendable. The keeper plate includes the brake shoes, in line with the wheel treads, with the rodding added.

The metal chassis carries a small five-pole motor with flywheel horizontally in the boiler, with double reduction worm and gear drive to the rear axle; the other drivers are coupled by the rods. The second wheelset has some vertical play, the third is allowed lateral movement.

The minimum recommended radius is 358mm, or 419.6mm with the piston tail rods fitted.

With a weight of 355g and traction tyres on the rear pair of driving wheels, haulage capacity should be more than adequate.

The model runs smoothly and quietly, controllable from a crawl to a realistic maximum. The digital version is exactly matched to the prototype's top speed.

All wheels collect current – there are wipers on the drivers, and the pony trucks are equipped with split axle wheelsets.

A PluX-22 decoder socket in the cab floor allows for a digital decoder. The

model is also available with a Zimo sound decoder factory fitted, with two small speakers under the cab floor and a chunky 'stay-alive' capacitor in the boiler. A smoke generator can also be installed.

The lighting is by LEDs, which change according to the direction of travel.

Standard couplings are fitted in NEM pockets, flexibly mounted on the frame. Roco close couplings are supplied as alternatives.

In contrast to the wrap-around vacuum-formed blister packaging so common now, the model arrives in a simple card box with a shaped expanded foam insert and a clear protective cover.

The model is also available as a Deutsche Reichsbahn Epoch IV example, with long water tank cut-outs.

A very smart model indeed, fully justifying the decision to start from scratch and employ the latest production techniques.

### Manufactured by

Roco,  
A-5101 Bergheim, Austria.  
[www.roco.cc](http://www.roco.cc)

DISTRIBUTED IN THE UK BY  
Gaugemaster Controls,  
Gaugemaster House, Ford Road,  
Arundel, West Sussex, BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
ref.73022 DC £265.00.  
ref.73023 DCC sound £340.00.





To enable the Deutsche Reichsbahn to replace steam-hauled passenger trains and pre-war railcars on branch lines with light traffic, in 1955 Waggonbau Bautzen started designing a new series of lightweight railcars; the first two prototypes were built in 1959 and with five pre-production sets entered regular service in 1962. Following this 63 more sets were delivered, making a total of 70, and in 1964 for the first time there were driving trailers, to avoid the need for running round, a series of fourteen classified VT 2.09.1. It was not until 1968/69 that the DR procured further revised examples, now designated 2.09.2. From 1965, 89 driving trailers (VS 2.08.1 and VS 2.09.2) were acquired so that the sets with just one motor car no longer had to be run round at termini.

The original classification VT 2.09 refers to the general type of vehicle (VT = Verbrennungstriebwagen = railcar with internal combustion engine), the power output (approximately 1% of the engine horsepower) and the top speed (approximately 10% of the value in km/h). The trailers were numbered as VB 2.08, the driving trailers as VS 2.07.

The type had several nicknames, among them *Ferkeltaxe* ("piglet taxi") due to it being commonly used on rural lines, or *Blutblase* ("blood blister") because of the original dark red paint.

They were intended for branch line services and could be seen almost all over East Germany, except on lines with steep gradients. With a top speed of 90km/h they could also be used for stopping train services on main lines, in particular north of Berlin. They also saw regular use on suburban services around Berlin and Leipzig, and on staff trains.

The first of the class had curved front windows around the corners of the cabs. To make production easier, later vehicles were equipped with flat glass windows with pillars at the cab corners.

Starting in 1965, later units (which became class 172) were equipped with multiple-unit train control, allowing trains of up to six to be run.

To avoid damage by the buffers of other stock, auxiliary rubber buffering pads were fitted.

In 1970 they were renumbered into classes 171/172, and in 1992 into classes 771/772 when the Deutsche Reichsbahn and Deutsche Bundesbahn pooled their rolling stock. Many were rebuilt and repainted into the banded mint green and white regional livery.

They were withdrawn from 2000, though the last 772 was not retired from regular service until January 2004. Several railcars and trailers have been preserved or survive with independent operators. Some were sold to Romania, Spain, Cuba, and Brazil.

The brand new model of the VT 2.09 from Piko represents the later (more numerous) variant with pillars at the cab corners, in original Epoch III condition.

The finish is excellent, with very clearly printed markings, which reveal that the unit is based at Templin in Brandenburg with a last overhaul date of August 1969. Door handles, handrails, and the bodyside bands are metallic – the latter also subtly raised, and with the black centre line perfectly printed. There is interior detail, and the flush glazing has neat black or silver frames.

Separate parts include the windscreen wipers (correctly different), electrical connectors, buffer pads, and imitation Scharfenberg automatic couplings. The underframe features even more added detail – brake rigging, magnetic track brakes, brake reservoir, exhaust, oil filter, toilet outfall, and more. If the coupling link bar is not required, there are blanking plates and detail parts.

The railcar is driven by a powerful miniature motor, with flywheels for perfect running, acting on both axles



(compared to just one in the real thing). It has a factory-fitted 'stay-alive' buffer capacitor for optimal operating without interruptions, especially important if a sound decoder is installed. The small size of the motor and drive means there is a clear view through the passenger compartment above the waist line. No traction tyres are fitted, and the weight of c.250g should enable the railcar to haul a couple of trailers with ease.

The railcar and driving trailer are coupled together by a current-conducting link bar on close-coupling mounts. Links are provided to connect railcar, driving trailer, and intermediate trailer, as well as an adaptor to standard HO couplers.

The lighting functions of both vehicles can be switched separately. The railcar independently registers whether a driving trailer is attached and then switches off the lights on the ends between the vehicles. The interiors are also illuminated. Lighting is by LEDs.

The model has a PluX-22 socket for easily fitting a digital decoder, and there is provision for installing a loudspeaker enclosure (for which the connections

have to be soldered). It is also available with a sound decoder factory fitted.

The analogue model tested runs smoothly and quietly, controllable from walking pace to a maximum only a shade over the prototype's 90km/h.

Another winner from Piko. No doubt versions with further period liveries and markings will follow in time.

#### Manufactured by

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Lutherstraße 30,  
D-96515 Sonneberg, Germany.  
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[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
ref.52880 DC £225.00.  
ref.52882 DCC sound £315.00.





The 1100 class Bo-Bo electrics were acquired by the Nederlandse Spoorwegen as part of the extensive post-war rebuilding and 1,500v DC electrification programme. Ordered from Alsthom in 1948, they were derived from and similar to the SNCF BB 8100 class. They were built in France, the first fifty between 1948 and 1952, then ten more in 1956.

The first were delivered in the NS turquoise livery but this proved a problem to keep clean so they were repainted dark Prussian blue in the mid-1950s. In the 1970s they got the grey & yellow livery.

With a top speed of 130km/h they formed the backbone of loco-hauled services for many years. They were eventually rebuilt with crash-protection cab fronts, which changed the appearance radically; all surviving machines (58) had been done by 1981. In the course of the 1980s they were ousted by the new 1600 class.

New in the Expert series from Piko is a model of this distinctive type, in original form (before the ends were rebuilt) with three headlights, in the classic blue livery with metal beading and cast plates but with the new NS logo added.

It is very accurate in all dimensions and details, according to published data and illustrations.

Separately added details include the cab door and nose handrails, lamp housings, windscreens wipers, multiple unit connectors, and brake hoses, plus the insulators, cabling, and switchgear on



the roof. The raised centre section of the roof and the walkways are also added parts.

The pantographs are fine, and sprung, but not wired for current collection from the overhead.

All glazing is flush with neat frames, and there is interior detail in the cabs and the machine room.

Dummy scale couplings and brake hoses are provided for the purchaser to fit in place of the working couplers if the model is only for display.

The head and tail lights change according to the direction of travel. Under digital control, cab and machine room lights can be switched separately.

The bogie sideframes exhibit very crisp moulded features, with added

details such as the sandboxes and cab steps, realistic springs, and brake gear, plus accurate representations of the buffer beams on the ends of the bogie frames, as per prototype.

The loco has a cast metal chassis carrying a can-type motor with two flywheels, driving both axles in both bogies via cardan shafts and reduction gearing. Current is collected by wipers on all wheels.

Two traction tyres (one on each outer axle) and a weight of c.330g provide more than enough pulling power. The rear towers 'float' within the side frames

to accommodate uneven track.

Performance is quiet, smooth, and controllable, with the top speed slightly more than the prototype.

The model has a NEM 658 PluX-22 socket for a digital decoder, with provision for installing a loudspeaker.

Standard couplers are fitted in NEM pockets flexibly mounted on the bogies.

The minimum radius recommended is 358mm.

Another quality product from Piko.

We can no doubt expect original turquoise and later grey & yellow livery versions in due course.



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[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
ref.51360 DC £205.00.  
ref.51362 DCC sound £300.00.



The Deutsche Reichsbahn class V180 (from 1970 BR118, later DBAG BR228) was the largest diesel locomotive built in the DDR, with a power rating of 1,800hp. They were constructed by Lokomotivbau Karl Marx (LKM) in Babelsberg.

In 1959, LKM constructed two prototypes, V180 001 and 002, but since East German industry was not at that time able to manufacture all the components for a large diesel locomotive, they used Voith hydraulic transmission (imported via a subsidiary in Austria) and multiple unit controls from Brown, Boveri & Cie. That first pair were too heavy so not adopted by the DR and were eventually scrapped at the Babelsberg plant in 1965 and 1966. In 1961, the state planning commission approved the funds for the construction of 128 machines intended to replace various steam locomotive types in the performance range above the V100. Two more test locos were built in 1962, followed by a small series of five, then the first series production, up to V180 087, between 1963 and 1965. They had a steam boiler for heating passenger trains. The axle load was 19.5/19.8t, designed for main line operation. The top speed was 120km/h, which was quite sufficient on the main routes in the DDR at the time.

The next sub-series had a more powerful (1,000hp) version of the engine. The DR was very satisfied with this and ordered a further 82. The first series were subsequently converted with the more powerful engine and renumbered to the 180.5 series.

To reduce the axle load to 15.6t and give wider route availability, even on branch lines, a six-axle version was created, classified as 180.2 to 4. 206 were produced until 1970.



The class was also permitted to work over steep sections.

When a 1,200hp engine became available, the C-C machines began to be uprated, but not all were rebuilt; the running was increased by 400 to distinguish, as 118.6 to 8.

The V180 was used throughout the DDR, and particularly around Berlin, where important expresses and freight trains had to be worked.

Three locos (548, 550, and 552) were assigned to the DDR government trains.

In 1995 the last of this type were withdrawn by DBAG.

Many were sold to private operators for use on freight traffic and infrastructure work.

A few have been preserved, some in working order.

The brand new model from Fleischmann replicates the four-axle variant, in the so-called 'Sparlack' economy livery with Epoch V markings.

The plastic body shell is accurate according to published dimensions, and has most detail moulded integrally, with well defined grilles, vents, and panels. Windows fit flush, with interiors in the cabs and machine room. Handles and handrails are painted silver. The only added parts seem to be the buffers and multiple unit cables.

Supplied accessories include front skirt blanking plates and brake hoses if working couplers are not required. The brake hoses are small and very fine.

The bogie side frames exhibit axlebox and suspension detail, with fine sandpipes that could almost be added parts.

A cast metal chassis carries a five-pole open frame motor with flywheel which drives via worms and reduction gearing in each bogie to all four axles. There are two traction tyres, one on each of the inner wheels, and a weight of 100g should provide ample pulling power.

Current collection is from all wheels, transmitted to contact pads under the frame by sliding wipers.

The model runs smoothly and quietly, controllable from walking pace to a reasonable maximum.

Triple white LED headlights and twin red tail lights change according to the direction of travel; either end can be isolated by small switches on the internal printed circuit board. There are also lights in the cabs, and all are separately switchable under digital control.

The model is fitted with a Next18 decoder socket.

The digital sound version comes with a load-sensing Zimo MX658N18 decoder loaded with true to the original sound by Henning.



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[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE  
ref.721402 DC £160.00.  
ref.721472 DCC sound £240.00.



In the early 1970s railway administrations across Europe collaborated intensively on the development of new standard passenger coaches. The intention was to achieve economies of scale by building in quantity, to raise standards of comfort, with soundproofing and improved suspension, and to harmonise equipment to facilitate international through running. In France, the result was the 26.4m long Corail (from 'comfort rail') series of coaches, placed in service from 1975, incorporating electric train heating and air conditioning.

They were designed by Roger Tallon and built mainly by Franco-Belge (absorbed by Alstom in 1982) in Raismes, in the Nord département. Alstom also participated, finishing second class and bar cars. In total, 3,780 vehicles were constructed between 1976 and 1988, in both saloon (Vtu) and compartment (Vu) variants, running on type Y32 bogies.

Configurations included firsts, seconds, and second buffet; there were also driving trailers, couchettes, and full and half baggage vans.

They were delivered in two-tone grey livery. One distinctive feature was the inset doors, painted bright orange. Later modifications included folding leaf doors and then Faiveley sliding doors.

The colour scheme was modified in the 1990s to match the TGV Atlantique (anthracite grey with light grey window band), with colour-coded doors – red for first, turquoise for second, and yellow for catering. Subsequently they have appeared in various regional liveries. They have undergone two renovations: Corail Plus in 1995 and Téoz in the 2000s. The majority are still in service.

Roco first produced scale length models of the Corail Vtu open saloon coaches in 2007; they have recently released visually revised versions which feature correct execution of the weld seam on the body, in several forms: type A10tu first (ref.74530), type A10rtu first (ref.74531), type B11tu second (ref.74532), type B11rtu second (ref.74533), type B10tu second (ref.74534, *illustrated*), and type B5rtux second buffet (ref.74535).

The finish is excellent, with dense and even main colours, well defined along the waist, and clearly printed markings.

The doors are separate parts (no doubt to facilitate the production of later variants), and the handrails are added.

The scale length model (303mm over buffers) has flush glazing and interior detail, and comes ready to accept an optional interior lighting kit.

The underframe has also been enhanced with finer detail; note, for example, the toilet outfalls. Full and attenuated brake hoses and dummy scale couplings are provided for either display or operation, along with electrical connection cables.

The bogie sideframes have good moulded features, with shock absorbers added; the brake shoes are in line with the wheel treads.

The model runs freely on blackened metal disc wheels, insulated one side with a plastic centre bush.

The bogie pivots are inboard of the centre line, to permit a minimum radius

of 358mm. Door steps are provided for the modeller to install if curves allow (they will limit the bogie rotation).

NEM 362 coupler pockets are fitted on close-coupling mounts on the frame. The model comes equipped with Roco close couplers.

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Gaugemaster House, Ford Road,  
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[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE each £53.00.



It is good to see this Australian specialist returning to activity after quite a while, during which time the ready-to-run market for Australian prototype has grown remarkably. Although proprietary models today are much better detailed, there is still scope for accessories to modify and improve the commercial offerings, or to fill the gaps.

For those modelling the earlier periods of New South Wales Government Railways, SJM offer a pair of scale three-link screw couplings.

The components are very fine brass castings – hook, loop, two link bars, and

central screw link with turnbuckle, cast in two groups – plus fine wire to make the pivots.

The modeller must clear the holes to take the wire.

Take care when soldering to leave the parts free to move – the instructions suggest putting a sliver of thin paper between them, and this works well.

The small parts are fiddly, but assembly would no doubt become easier with practice!

The turnbuckle does not actually tighten, but the loop will engage with the hook.



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Stephen Johnson Models  
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[www.stephenjohnsonmodels.com.au](http://www.stephenjohnsonmodels.com.au)

Price A\$7.00.  
Postage and packing extra.



Powerline Models has recently released a second, updated, batch of the Victorian Railways T class Bo-Bo diesel, representing the Series 3 (T4) variant with high cab and low nose, in the classic blue and gold livery. Four running numbers have been produced – 370, 371, 381 (illustrated), and 383.

The upgrade includes: MTC 21-pin decoder socket, loudspeaker ready fitted, metal knuckle couplers as standard, blackened wheels, and golden white LED lighting.

The T class is a General Motors G-8 export design built in Australia under licence by Clyde Engineering. VR eventually had a fleet of 99, built in batches from 1955 to 1969, with technical and detail differences. Although there have been withdrawals, many are still in service, some with private operators. As a result they have carried several liveries in their long careers.

The body of the model is injection moulded plastic, accurate in major dimensions and details, enhanced with various separate detail parts such as lift rings and horns, the correct pattern exhaust, plus metal handrails and finely etched brass grilles, open mesh step treads, and mirrors.

The cab is flush glazed, with some windows open, and fine moulded plastic windscreen wipers added. The interior is fully fitted, though no crew figures are present. The roof has the extra dimples resulting from the later fitting of the vigilance controls.

Sandboxes and pipework are present on the main frames.

The buffer beams carry metal coupler cut bars, and brake hoses with detailed connectors.

The fuel tank and battery boxes are cast in metal to add adhesive weight. (The model weighs just under 300g.)



The bogie sideframes feature added roller-bearing axleboxes, brake cylinders, actuating levers, and pipework.

The chassis is cast metal, and power comes from a five-pole skew-wound motor fitted with twin brass flywheels. Wheels are to RP-25 110 profile; all are driven, and all collect current. There are no traction tyres. Running on analogue DC, the model worked smoothly and quietly, controllable across the speed range from a minimum of 5mph to a maximum of c.60mph, reflecting the prototype's performance.

The model is also available ready fitted with a Loksound V5 decoder by ESU loaded with authentic sounds by DCCSOUNDS, replicating the 8-567CR prime mover and the RVB Hurricane three-chime horn. It has a comprehensive set of functions:

- F0 directional headlights on/off
- F1 not used
- F2 playable air horn
- F3 coupler
- F4 headlight dimmer
- F5 drive hold
- F6 independent brake
- F7 shunting mode (half speed)
- F8 sound on/off
- F9 run 8 (full throttle)
- F10 coast
- F11 sanding valve
- F12 radiator fan sound on/off
- F13 air conditioning off/on
- F14 auto brake set/brake release off/on

The decoder will work on 14, 28, or 128 speed steps, and all the usual facilities are programmable – two or four digit addresses, acceleration and deceleration, braking, momentum, maximum and minimum speeds, and speed



curves. It accepts both direct and operations mode programming, and has two function outputs rated at 100mA each, with various optional lighting effects.

The model is fitted with directional lighting, not only the headlights but also the marker lights, using golden white LEDs. The markers are illuminated via fibre optic guides and these are shrouded with mirror foil to prevent leakage.

The model comes fitted with Kadee® metal shank knuckle couplers with scale #158 heads.

The finish on our sample is excellent – the yellow (a difficult colour) is dense and even, and well defined. The wings, logo, numbers, and plates are sharply printed.

The original model (reviewed in our July 2008 issue, p.447) was very good, and with the present modifications Powerline have kept it right up to date and in line with current expectations.



#### Manufactured for

Powerline Models Pty. Ltd.,  
P.O.Box 2100, North Brighton,  
Victoria 3186, Australia.  
[www.powerline.com.au](http://www.powerline.com.au)

SAMPLE SUPPLIED BY  
Trainworld Pty.Ltd.,  
290, Bay Street, East Brighton,  
Victoria 3186, Australia.  
[www.trainworld.com.au](http://www.trainworld.com.au)

PRICE  
DC A\$335.00.  
DCC sound A\$435.00.  
Plus postage & packing.

**HO**

## transformer control centre building kit new from Auhagen

Auhagen have produced a kit representing the control centre for an electrical sub-station and transformer installation, as part of a series of structures and accessories on this theme.

The plastic parts are moulded in seven different colours – light grey, medium grey, dark grey, light brown, dark brown, stone, and brick red. Light weathering would help to bring out the detail.

Clear foil is provided for glazing the windows, with a printed insert to mask the interior.

The components are very finely moulded, with a good level of detail, and the feeds are not very thick, so can be easily cleaned up. There is virtually no flash, and the parts go together very well.

Gutters and downpipes are included, along with accessories such as the fire extinguisher device panel, tools, exterior lights, siren and control box. The clock on the roof is particularly attractive, made as a one-piece moulding. The



cross struts are delicate and the result looks somewhat vulnerable. Care must also be taken to use a minimum of adhesive when attaching it to the roof; there are no marks to determine the precise location. The clock faces are printed on pre-cut plastic discs which are a perfect fit in the clock

The major parts are derived from the ingenious modular 'Bau-Kasten' system from Auhagen, so the building lends itself to adaptation or extension using these components.

As the kit is composed of standard sprues, there are some spare parts – which the creative modeller will no doubt be able to put to good use!

Built as intended, the finished struc-



ture occupies an area 143mm x 66mm; its overall height is 100mm.

As usual, the instructions are in the form of a sequence of clear 'exploded' diagrams on both sides of a folded A3 sheet, with the parts numbered and identified on a separate A4 sheet, so there should be no language difficulties. The sprues are shown separately, with parts numbered to aid identification. Construction is presented in ten steps. Liquid solvent is recommended as adhesive.

### Manufactured by

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D-09496 Marienberg  
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Golden Valley Hobbies  
Unit 1, Pontrilas Business Park,  
Pontrilas, Herefordshire, HR2 0AZ.  
[www.goldenvallyhobbies.com](http://www.goldenvallyhobbies.com)

PRICE ref.11459 £23.60

**009**

## freelance diesel body kit by Budget Model Railways

New from Budget Model Railways is a freelance diesel body kit, designed to clip onto a Kato ref.11-103 or 11-104 N gauge chassis, as well as the newer more readily available ref.11-109 chassis. (No doubt the base could be adapted for other mechanisms – this would be best done before assembly.)

It makes quite a large machine for narrow gauge – the body measures 70mm long over buffer beams, 25.5mm wide, and 36mm high. The parts are 3D-printed in PLA plastic. commendably, there is very little flash but the back of the components can be rough, and in some places this is critical to the fit – some parts will need a good deal of filing and sanding.

Conversely, some components are under size and the gaps will need some filling. These include the bonnet top, which unusually has a simulated chequer plate finish, and the centre of the cab roof, where the printed layers are apparent.



The parts are designed to be glued together, using cyanoacrylate (super-glue) sparingly; the material is not suitable for fixing with liquid solvents as used for styrene.

The upper cab fronts overlap the bonnet sides, which seems unlikely.

No glazing is included – as the cab sides are quite thick, the panes would be best cut to size and fitted individually.

Features such as the grilles, louvres, bonnet doors, hinges, and catches look rather prominent.

The door handles and handrails are moulded in place, as is the lettering on

the front above radiator grille. The solid cab steps are formed as part of the sides.

While the body will require a certain amount of careful work for a satisfactory result, it does offer plenty of scope for detailing and customising – the builder could drill out the headlights and fit working lights, for example.

The instructions (to be downloaded from the website) take the form of a series of perspective CAD drawings, and are quite easy to follow.

Not shown are the optional buffers (supplied), though these can be seen in photos on the website.

### Produced by

Budget Model Railways  
56, Ringwood Road,  
Eastbourne,  
East Sussex,  
BN22 8TB.  
[www.budgetmodelrailways.co.uk](http://www.budgetmodelrailways.co.uk)

PRICE £10.00.

## MIBA Anlagen 1/2020

### Die grandiose Modellbahnanlage von Rolf Weinert

by Markus Tiedtke

VGB – Verlagsgruppe Bahn GmbH, MIBA-Verlag,  
Am Fohlenhof 9a,  
D-82256 Fürstenfeldbruck, Germany.  
[www.miba.de](http://www.miba.de)

295mm x 210mm 116pp  
Softback €12.00  
ISBN 978-3-89610-720-6  
German text ref.15087339

The latest layout special from MIBA is devoted to a remarkable private HO layout, being built by Rolf Weinert. He will be known to many as the designer and producer of a range of high quality kits for locomotives, rolling stock, lineside accessories (notably signals), and scenic items, primarily in HO but with some in other scales. He is a master of casting in whitemetal and brass, and photo-etching in brass and nickel-silver, and subsequently also got involved in plastic injection moulding. In recent years he has also developed the *mein Gleis* range of authentic German pointwork, while for some time his company (in business for just over forty years) has been the distributor for Peco products in Germany. His commitment to quality, accuracy, and detail is legendary, so it should come as no surprise that his own layout is of a similar high standard. Located in the loft space above the business premises, it is fairly large by domestic standards but still achievable (given time, space, and resources!) and not in the same category as, for example, Miniatur Wunderland or other commercial ventures. Modellbundesbahn, now at Brackel, might be the nearest comparison (on several levels) though even that has much more room in its new home.

The basis was a plan by Rolf Knipper, developed in response to Weinert's requirements as far back as 1989. In fact what is seen in this book is only half of the project, depicting the Deutsche Bundesbahn station of Syke, around fourteen metres long and less than two metres wide, with storage sidings below, accessed by long ramps behind the backscene.

Syke was chosen as the prototype as it had the desired elements, and was also relatively local. The brief was for the relatively flat rolling rural landscape

of northern Germany – snow-capped mountains with dramatic waterfalls were never envisaged; there should be a small town, not a big city, and some local industries to provide goods traffic – for example, brickworks, brewery, coal merchant, oil depot, and a large livestock loading dock, reflecting the mainly agricultural setting.

The other side of the U-shaped layout, of similar size, will be more urban and with a dockside area.

The period is late Epoch III.

This is a long term project – thirty years and counting! The layout is described as a life's work – and conversely one has to wonder how much of that life's work has had the layout ultimately in mind!

It has been made with the assistance of some of the best modellers in Germany – following Rolf Knipper, sadly deceased far too young, it currently involves Michael Butkay, Jörg Spremberg, and the noted model photographer Markus Tiedtke, as well as Rolf Weinert himself, all with their particular areas of interest and expertise. The workmanship is of the highest standard, professional in every sense.

The layout is operated under digital control using Lenz equipment, run from a computer using Train Control software by Freiwald.

The book opens with a selection of impressive single images presented large across double pages. In some cases the details become larger than actual (HO) size, but both modelling and photography stand this treatment.

All aspects and areas of the scene completed so far are shown in detail, with a logical combination of wider views and close-ups. There are many small scenes, never crowd-pleasing gimmicks, but the overall impression is spacious and uncluttered.

As well as the standard gauge, there is a metre gauge local line, the Hoya – Syke – Asendorf, with interchange facilities.

Each section has some introductory text, but the coverage is primarily achieved by photos with captions. Many of the products used are identified (not surprisingly, a lot of Weinert!), with some explanation of techniques and tips, but in general this is not a 'how to' guide.

Ironically quite a lot of the stock shown was borrowed back from customers for the photo sessions as Weinert's own models are in pristine condition for display on their trade stand, not (yet?) realistically weathered for use on a layout.



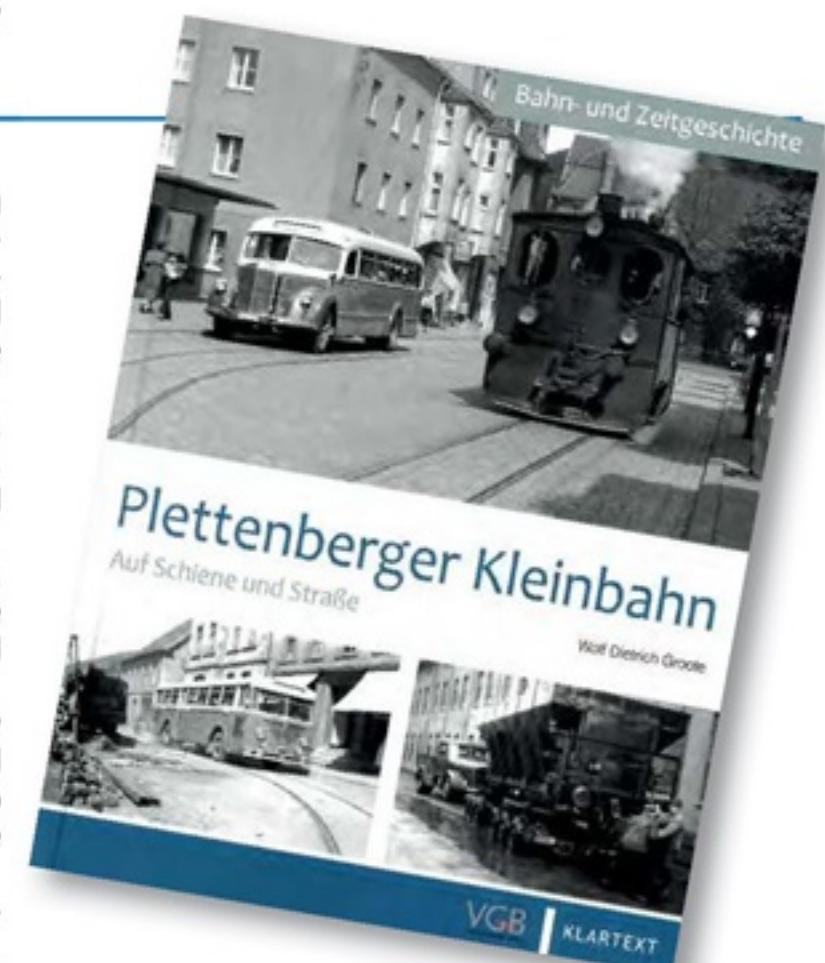
The work is illustrated by over 150 excellent photographs by Markus Tiedtke, all in colour, and all very well reproduced on good quality paper. A number of clear plans and diagrams show the layout as planned, as built, and what is still to come. Some operational aspects are also explained (e.g. the arrangement of the goods yards). It is clear that little has appeared by chance.

Presentation is in the usual clear, straightforward MIBA style with no extravagant use of fonts, different type sizes, or colours, just the occasional tinted background information panel or separate text box for clarity.

Some knowledge of German will be needed to get the most from the text, but the high pictorial content makes it well worth a look for anyone modelling a German prototype of this period.

The issue includes six pages of advertisements for other MIBA and VGB publications plus some from relevant suppliers.

Warmly recommended. But be prepared to be envious!



## Plettenberger Kleinbahn Auf Schiene und Straße

by Wolf Dietrich Groote

VGB – Verlagsgruppe Bahn GmbH,  
Am Fohlenhof 9a,  
D-82256 Fürstenfeldbruck, Germany.  
[www.vgbahn.de](http://www.vgbahn.de)

295mm x 223mm 272pp  
Hardback €49.95  
ISBN 978-3-8375-1965-5  
German text ref.581808

This substantial new book deals with an interesting metre gauge line in the Sauerland. Although there were plans from the late 1870s, construction did not start until 1895. The aim was to link industrial premises in the side valleys with the main line network and so improve their competitiveness. The system developed gradually, and although at its maximum it was only 13km long, it performed an essential transport task. With many sidings into works and factories, it was noted for street running with sharp curves and some very tight situations. Such sections were inevitably incompatible with increasing road transport after the Second World War, and the system was closed in 1962. It made considerable use of first transporter bogies and then transporter wagons to carry standard gauge wagons through the streets, and was also noted for enclosed steam tram locos, still active long after they had disappeared elsewhere in Germany. It survived long enough to attract the attention of enthusiasts, so its final years are comparatively well recorded.

The book begins with a brief history of Plettenberg and the development of the light railway before offering a full description of the route, with all its complexity. An excellent initial map aids comprehension, and old documents are reproduced in each section as we trace the routes.

There is detailed examination of the locomotives, railcar, passenger stock, goods vehicles, service stock, and transporter wagons, with many specially produced scale drawings (side and end elevations).

Further sections cover the operating procedures, traffic carried, and selected accidents, as well as the road vehicles operated by the company (buses and lorries).

There is brief mention of surviving stock, principally the steam tram Plettenberg at Bruchhausen-Vilsen and the diesel railcar at Hüinghausen – now back not so far from its former home – and a note on some large scale models.

The contents are rounded off with a summary timeline and a list of sources.

The work is illustrated with a superb collection of around 650 photographs, mostly black & white but with some colour (the earliest from January 1956), all of good quality and well reproduced. Many are presented large – the one across p.56-57 perhaps over large! They have long and informative captions. They are from a wide range of sources – historic, official, and enthusiasts, among them some British visitors in 1957 and 1958. Many are previously unpublished.

The contents are simply but attractively laid out, with restrained use of typefaces. There are maps, track plan

diagrams, timetables, tickets, and company letterheads (of the enterprises served by sidings), etc., several reproduced in colour.

This book shows an aspect Germany long lost to road transport, and as such provides an important historical record.

A fair knowledge of German will be needed to get the most from the text, but the story comes through in the photos. So many scenes to inspire modelling!

Thoroughly recommended.

## Modelling the standard gauge *Spirit of Progress*

by Ian Black

Published by the author.

295mm x 210mm 36pp  
Softback A\$15.00  
No ISBN.

This interesting and informative booklet is dedicated to exactly what the title proclaims.

Although the *Spirit of Progress* was numbered as a New South Wales service (trains 3 and 4), it was essentially a Victorian Railways train. It followed the all first class non-stop sleeper *Southern Aurora*, calling at a number of intermediate major towns.

When built in 1937, the broad gauge SoP stock set new technical and aesthetic standards copied from American practice – the use of special steels, air conditioning, streamlining, and fixed consists. When standard gauge passenger services were extended from Albury (on the state border) to Melbourne in 1962, two sets of cars were transferred and modified: most were SoP vehicles, augmented by some 1950s-built Z cars plus stainless steel NAM sleepers and PHN power vans.

The booklet opens with a summary history and outlines the train composition, with reference to what is – or has been – available commercially to assemble an authentic representation of the train, and what is therefore required to make up the shortfall.

It shows making the VBK, VFK, and VHN baggage van from Powerline AS and BS models, modifying the VFS, VFX, VFR, and VAC to match, and building the unique VAM sleeper which served Canberra and was added to/detached from the SoP.

As well as the specifics for each vehicle type, there are general sections on interiors, windows, gangways, underframes, adjustable coupler mounts, bogies, rivets, and painting and decals.

As an extra, there is a section on modelling the unique SoP Parlor Car (round end tail observation) even though this remained on the broad gauge.

This material was originally conceived to assist modellers in upgrading older Powerline AS and BS coaches to match current products (right – see review

in CM December 2019). It could have been published as (necessarily given the amount of material) a series of articles in one of the Australian modelling magazines, like some of the author's previous projects, but he felt it would be more useful to other modellers to have it all available at once in one convenient package, and so decided to publish it himself.

While much of the information is very specific, some of the techniques developed by the author and explained here could have much wider applications. Two of these, the sprung corridor connection gangways and the adjustable coupler mounts, were discussed in his article in the recent issue of the *Australian Journal of Railway Modelling* (see review in CM December 2019). The third innovation is the use of polycarbonate sheet for glazing, to overcome the various failings of both injection moulded and vacuum-formed 'flush' glazing.

All this is based on real experience and illustrated with examples of the author's work – this is no theoretical treatise but a very practical guide. Some of the work is relatively simple, some more ambitious, but the guide inspires confidence.

This booklet clearly demonstrates that even with the ever-increasing quality and detail of ready-to-run models, there is still scope for individual creativity improving older models to match, adding specific detail, ameliorating compromises, correcting errors, and creating variants not (yet) offered by commercial suppliers – it is likely there will always be gaps in the range which it would not be cost effective to fill.

The presentation is simple and straightforward, without use of colour, fancy fonts, or graphic devices. The photographs are all in colour, and reproduced well, generally just inserted at the appropriate point in the text at a sensible size. A few of the images have been 'cut out' to a white background; most are sensibly



photographed against a plain background which reproduces more or less white, and are clear enough, but it would have raised the presentation a notch if they could all have been cut out.

The material is supported with excellent plans and line diagrams, reproduced actual size and fully dimensioned to assist modelling.

The work concludes with references to relevant books, magazines, and websites.

The booklet has been successfully sold in model and specialist book shops in New South Wales and Victoria.

By post, copies can be obtained from:

Stephen Johnson Models, P.O.Box 31, Red Rock, New South Wales 2456, Australia.

<https://stephenjohnsonmodels.com.au>  
Airmail postage for a single copy is A\$5.00.  
Payment can be via PayPal.



## ALe *Automotrice Leggere Elettriche*

by Lucca Berardocco, Giuseppe Lippolis, and Emiliano Maldini

TG-Trains,  
Casella Postale 8, I-21046 Malnate (VA), Italy.  
[www.tg-trains.com](http://www.tg-trains.com)  
210mm x 297mm 354pp  
Softback €60.00  
Italian text  
ISBN 979-12-200-5314-3

This substantial landscape format book presents the history and current situation of part of the FS electric railcar fleet; the first of two planned volumes, it covers the vehicles equipped with electro-mechanical rheostatic control equipment. The second will deal with electronic control machines. The coverage encompasses prototypes and specials as well as series production, and the relevant trailers are also included.

The story goes back as far as 1901, when initial attempts were essentially existing coaches with motors fitted to the bogies and driving cabs installed in more-or-less modified ends, with pantographs added to the roof; some of the stock of this style lasted a long time, into the mid-1980s.

Later came purpose-built body shells, designed with lightweight construction, and often with semi-streamlined ends, though the 790 and 880 were not symmetrical, having one streamlined and one 'flat' end, for working back-to-back in pairs with connecting doors but often seen singly and with the flat end leading.

Sections of the book deal with early examples, mostly built in small quantities, culminating in the E.624;

the first generation, from the 1930s – ALe 790/880/781, ALe883 and Le883 trailers; the second generation, in the post-war period – 840 and trailer, 660, 540, and trailers 640/680; and the third generation, from 1960 onwards – 601, 603, 841, 803 and trailer, 801/930 and trailer 108.

Coverage is completed with a section on experimental units and those allocated to special duties.

Following the main photo album, a substantial section provides official diagrams and/or scale drawings of almost every class featured. Some are clearly derived from existing material, but many appear to have been specially drawn. All are dimensioned, but most show only the side elevation, though many also give a plan view and internal layout.

The collection is rounded off with comprehensive tables showing the allocations of every unit and a list of when built, by whom, and subsequent fate or present situation.

The account is illustrated with an excellent collection of photos, many previously unpublished, with images from throughout the period. A good mixture of portraits, in stations and on shed, and views of trains in the landscape, they show railcars in use all over Italy, and in all weathers, from summer heat to winter snow.

The pages with photos are not actually numbered – no type spoils the images, which quite literally fill the available area – which can make finding a specific section interesting!

There is a short bibliography.



While there are significant sections of text, the book primarily consists of photos with concise captions (giving unit, location, service, and date, and crediting the photographer). Most are presented full page, with the occasional half page vertical alongside text. The majority are in colour, with some early black & white views. The standard of photography and reproduction is generally good; only a handful should perhaps have not been presented quite so large.

A passable knowledge of Italian will be required to get the full benefit of the text, but as this is photo album rather than a technical treatise it can still be appreciated.

## MIBA-Spezial 123

### Nicht nur Bahnhöfe: Hochbauten

Edited by Martin Knaden

VGB – Verlagsgruppe Bahn GmbH, MIBA-Verlag,

Am Fohlenhof 9a,

D-82256 Fürstenfeldbruck, Germany.

[www.miba.de](http://www.miba.de)

295mm x 210mm 108pp

Softback

€12.00

ISSN 0938-1775 German text

ref.12012320

The latest special from MIBA is concerned with major structures – and, as the subtitle indicates, not just station buildings. Railways also need signal boxes, block posts, crossing keepers' huts, goods sheds, stores, workshops, loco sheds, huts, and also retaining walls and tunnels. But while highly detailed locos and stock are easily available ready to use, the majority of buildings for our layouts, in whatever scale, still have to be assembled from kits, of one type or another.

This collection of individual articles examines various aspects of a wide and varied topic. They include:

- techniques for assembling laser-cut card kits, with particular reference to how they differ from plastic;
- assembling a standard Austrian wayside station building from a laser-cut kit by Igra Modell;
- scratchbuilding the station building at Hasenfelde on the Oderbruch line in HO, with attached goods shed; the building still stands but is no longer rail-served.
- assembling and detailing a two-track loco shed in TT by Auhagen; the structure could be easily adapted for narrow or metre gauge in 1:87.

- building a *Bahnmeisterei* railway maintenance depot from a resin kit with all its attendant small detail, plus notes on suitable stock;
- making a *Bahnmeisterei* signals depot from a laser-cut card kit;
- making a small and relatively simple block post to develop CNC design skills for cutting out the parts;
- a large curved single track stone arch viaduct built using 3D-printed parts;
- creating a small module in HO showing a short tunnel with different portals either end through a rocky outcrop;
- making concrete retaining walls by casting plaster in moulds.

All of the ten are substantial and detailed accounts, between six and twelve pages. All show actual examples, modelled to a high standard, and often with reference to prototype pictures, with logical step-by-step sequences clearly showing the procedures described.

The work is illustrated by over 300 photographs, most in colour, all very well reproduced on good quality paper, supported by a number of drawings and diagrams.

Presentation is in the usual clear, straightforward MIBA style with no extravagant use of fonts, different type sizes, or colours. There are some summary information panels, and occasional use of tinted backgrounds to define separate sections for clarity.

A certain facility in German will be need to get the full benefit of the text, but the high pictorial content makes the collection well worth a look. While most of the subject matter is German, many of the techniques could have wider applications.

The issue includes around eleven pages of advertisements, including one – most unusually – on the



contents page; most are for other MIBA and VGB publications, but some promote relevant suppliers and retailers.

The success of such a book may be measured not in how long it holds the reader's attention but by how soon they are inspired to put it down and get on with some modelling!

## Lok 2000

by Maurizio Tolini

TG-Trains,

Casella Postale 8, I-21046 Malnate (VA), Italy.

[www.tg-trains.com](http://www.tg-trains.com)

210mm x 297mm 334pp

Softback

€60.00

Italian text

ISBN 979-12-200-5312-9

This large landscape format book looks at the history and current situation of the Bo-Bo electric locomotives classified Re460 by the Swiss Federal Railways (SBB/CFF/FFS) and Re465 by the Bern Lötschberg Simplon.

The first 460 entered service in 1992, with the last (of 119) delivered in 1996; the BLS acquired eighteen slightly more powerful versions between 1994 and 1996. All were built by SLM of Winterthur, though some bodies were sub-contracted to Krauss-Maffei in Munich.

The book begins with the background. As early as 1978 the SBB was seeking a successor to the Re4/4<sup>II</sup> even while these were still being built (until 1984, a total of 277); four prototype Re4/4<sup>IV</sup> were delivered in 1982, using the then novel thyristor control. Although successful, and eventually providing part of the foundation for the Re460, no more were ordered and as a non-standard class in 1994 the SBB exchanged them with the Süd Ost Bahn for their four regular Re4/4<sup>II</sup>s.

There is brief mention of the Re4/4<sup>V</sup> (later Re456) built for private lines and the Re450 single ended type for the Zürich S-Bahn before we come to the 460/465.

Single and dual supply variants were initially envisaged – the latter designated Re462, for 15kV AC and 3kV DC, for working into Italy, but never built. The original orders were for twelve passenger and twelve freight locos, but they were effectively universal or mixed traffic machines, equally capable of high speed and heavy loads.

There is a comprehensive technical section describing all aspects.

The type was notable for the first application of advertising liveries, carried by many but by no means all machines, and some have been seen in several guises. All are illustrated in a gallery of miniature portraits, sixteen to the page (4 x 4) over eight pages.

As well as the locos, the associated IC 2000 inter-city coaches, single- and double-deck, with driving trailers, are covered.

Not forgotten are the export orders – the El18 for Norway (22), Sr2 for Finland (46 built locally for broad gauge under licence), and two for Kowloon-Canton (Hong Kong). The double-deck coaches were also tested in Denmark from 1999 to 2001.

There is a full account of a cab ride from Olten to Locarno, hauling a very special service, the School and Discovery Train, through the new Gotthard base tunnel before official opening, with the comments of the experienced driver.

The book continues with the upgrade with IGBT control equipment, and the revamping of the coaches, to give another twenty years service.

There is brief mention of major accidents, and extensive notes on the ZUB and ECTS train control and safeworking systems, with diagrams.

A section of photos is dedicated to special events, among them SBB 460 036 carried to naming ceremony on the metre gauge CF du Jura using *Rollwagen* – a very unusual load.

The account is illustrated with a fine selection of photos, many previously unpublished, with images from throughout the period. There is a good mixture of portraits, in stations and on shed, and views of trains in the landscape, in all weathers, plus detail, interiors, and technical equipment. The most recent is from March 2019. Each section also features detailed drawings, including internal layouts, but with no scale bar or dimensions marked.



The work is completed with extensive tables – a full construction list, including those for export, overhaul dates and details, and the advertising liveries carried. There is also a short bibliography.

The author is well qualified as a rail photo journalist and video producer of long experience, with wide interests but a special concern for northern Italy and Switzerland.

While there are significant sections of text, the book is mainly photos with concise captions (giving loco, location, service, and date, and crediting the photographer). Most are presented full page, with the occasional smaller image amongst the text. The majority are in colour, with a few early black & white views. The standard of photography and reproduction is very good throughout. The pages with photos are not actually numbered – the images fill the available area – which can make finding a specific section interesting!

For those with enough Italian, this must be close to a standard work of reference. Recommended.

# Digitale Modellbahn 1-2020

Edited by Tobias Pütz

VGB – VerlagsGruppe Bahn GmbH,  
Am Fohlenhof 9a,  
D-82256 Fürstenfeldbruck, Germany.  
www.digitalemodellbahn.vgbahn.de  
297mm x 210mm 84pp  
Softback €8.00

ISSN 2190-9083 ref.652001 German text

The new edition of the magazine devoted to digital technology in model railways, produced by specialist staff and contributors from MIBA, Eisenbahn Journal, and Modelleisenbahner, celebrating ten years of publication, opens with a brief survey of recent new products and then presents some detailed reviews: the TCS central unit from Zavavov; the small MX10EC central unit by Zimo; the products of the Bühler company, LSdigital and now also LDT; the Lodi system from Lokstore digital; the Traintronic TR8051 point motor from Itelec; specialist reporting modules by Blücher; the multi-function System Detector 7 from KM1; and the OC32 multi-function control from VPEB.

Other topics include an interview with Dr Peter Ziegler, founder of Zimo, to mark the company's forty years in business, and a report from a digital rail summer school.

Practical projects include: installing a sound decoder in a Trix HO V3201; Christmas market stalls decorated with LEDs; LED chains for advertising signs; adding LED lights to a Märklin wagon; using VSDM feedback modules in a Märklin system; working platform signs (by Dale Schultz, already featured in CM August 2019); part three of a DR signal panel project; a scratchbuilt traverser controlled by an Arduino; programming in Pascal; and notes on electrical safety.

The layout portrait in this issue presents Modellbahn '65, a 140m<sup>2</sup> HO Epoch III layout by a club in Stuttgart which has been converted to digital control by computer – no small undertaking, given its size and complexity.

DiMo aims to provide features which appeal to beginners and experienced modellers alike, offering practical advice.

The work is illustrated with over 150 photos, diagrams, screenshots, circuit diagrams, and tables, all well reproduced on high quality glossy paper.

The presentation makes some use of coloured headings and background tint panels, in summary tables and side bars, which generally assist clarity.

Reasonable knowledge of German will be required to get the full benefit from the text, which is inevitably technical.

There is the equivalent of around eleven pages of advertisements, some for other VGB publications and several from relevant suppliers.



## Le Train – spécial 100 4/2019

### Les 140C

by Olivier Constant

Editions Publitrain eurl,  
CS80010, F-67660 Betschdorf, France.  
www.letrain.com

297mm x 210mm 100pp  
Softback €16.50  
ISSN 1267-5008 French text

This new special from *Le Train* is the first for some years to be devoted to a steam class. The 140C 2-8-0 was built in quantity – 334 were placed in service between 1913 and 1920, built by SACM, Schneider, Fives-Lille, Nasmyth Wilson, North British, and Vulcan Foundry. 270 were ordered by the État and 70 by the military for hauling heavy artillery. (Six were lost at sea.) They were mainly used in the East, West, and South East regions. Examples were the last working steam on the SNCF, in November 1975. Eight have been preserved, with two currently in working order on tourist lines.

The author had access to much previously unpublished material from the SNCF archives centre, and help from eminent railway historians and enthusiasts.

After examining the origins, there is a full technical description, notes on the livery, a record of major modifications, and examination of maintenance records. Their service is described region by region – west, south-east, and east, plus limited use in the north – with allocations by depots. There is a separate section on their role during Second World War.

The final chapters detail the tenders used, and list the preserved examples.

The work concludes with the official specification sheet which includes a line diagram but not a detailed scale drawing.

As usual, the work is built around an excellent collection of 137 photographs, black & white and colour, all very well reproduced.

The captions are informative, and mostly placed adjacent to the relevant image. The page design is straightforward, with frames and tinted backgrounds for the tables and information panels to aid clarity.

There are eight pages of advertisements for other *Le Train* publications.

The publication is perfect bound, using high quality glossy paper between heavier varnished covers.

Reasonable French will be needed to follow the text, but the captions and tabular information should be



clear enough and the publication can be appreciated for the illustrations.

## Swiss Standard Gauge

by Jason Sargeson

Charaxes Publications,  
17, Muirfield Park, Hull, East Yorkshire, HU5 3JF.  
www.jasonsargeson.uk  
295mm x 210mm 40pp  
Softback £10.00 incl.p&p  
ISBN 978-0-9575048-9-9

This book is a guide to standard gauge railways in Switzerland today, concentrating on passenger lines. For each there is a summary description of the route, mention of significant features, and the principal motive power and rolling stock. Service frequency and journey times are detailed. The SBB is described as eight key routes and associated local services, and the principal private companies are included – Bern-Lötschberg-Simplon, Südostbahn, CF du Jura, Orbe-Chavornay, Oensingen-Balsthal Bahn, Rigibahnen, Rorschach Heiden Bergbahn, Sihltal Zürich Uetliberg, Pont Brassus, Transports Régionaux Neuchâtelois, Transports Publics Fribourgeois, and Transports de Martigny et Régions.

Heritage or historic services running trains are also mentioned, along with brief coverage of other national operators running services into Swiss territory – SNCF, DB, ÖBB, and FS.

Each entry is completed with relevant flags and civic coats of arms.

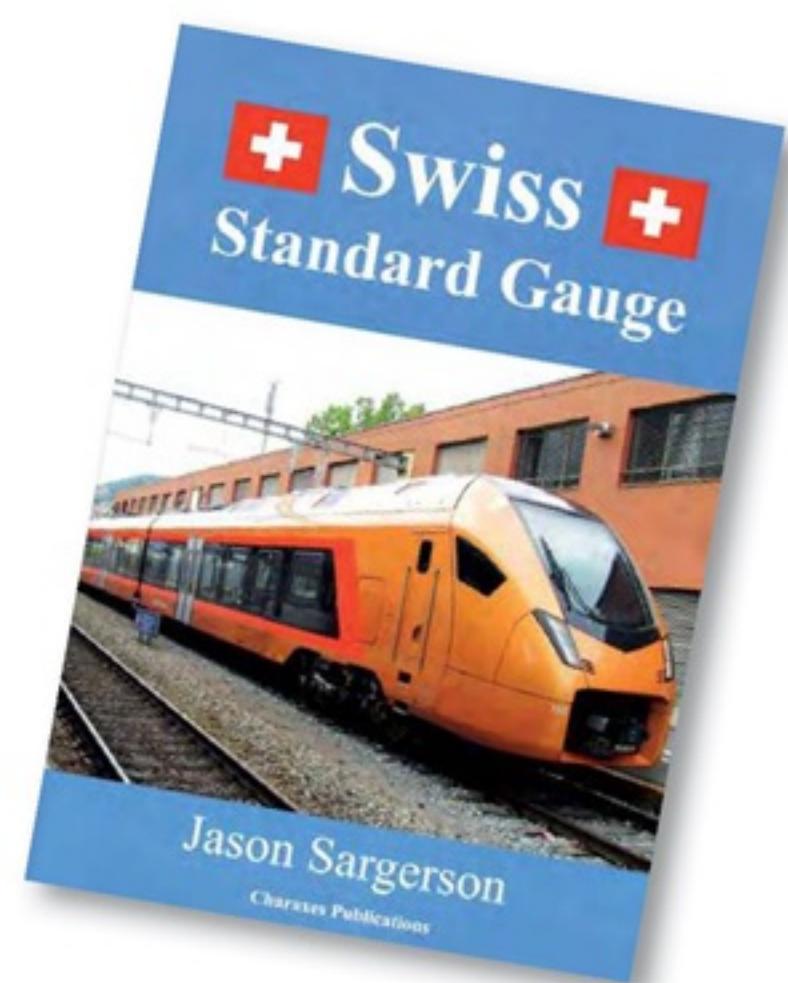
There are around 140 photos, the great majority by the author, augmented by contributions from Alan Mitchell and Georg Trueb; it is an impressive collection. Most were taken in stations, but some scenic locations are illustrated, including Brig, Martigny, Kandersteg, Chur, Sargans, Spiez Einigen, Unterterzen, Rapperswil, Arth-am-See, and Schaffhausen. They range in date from April 2008 to August 2019, most from recent years, and almost all in good weather. They are all in colour, and well reproduced to a reasonable size, between three and six per page, depending on the amount of accompanying text. Four pages in the centre each offer two large scenic views, and there are a couple more of these elsewhere.

Simple captions give the railway (by abbreviation, listed at the start), location, motive power, and date.

Inside the back cover is a clear rail map of the country; the featured lines are of course shown but not highlighted. The scale is not large enough to show all the locations. Schematic route diagrams alongside each account would have iced the cake.

There are recommendations for further reading, both authoritative printed works and useful websites.

Like the author's previous coverage of Swiss metre gauge, this is straightforward, attractive, informative,



well produced – and excellent value.



## New modelling tool sets from Peco

Peco has announced two new packs of modelling tools, a *Tracklayer's Tool Set* and a *Kitbuilder's Tool Set*.

The *Tracklayer's Tool Set* has all the tools required to lay items of Peco track in any gauge. It consists of a pair of rail cutters, modelling knife, cutting mat, set of four needle files, track pins, pin pusher, and marker pen. The pack will also include a copy of the Peco 'Shows You How' guide *Laying the Track*.

The *Kitbuilder's Tool Set* contains tools useful for constructing kits in plastic, card, and laser-cut wood. The set consists of a modelling knife, steel rule, cutting mat, set of four needle files, side cutters, fine tweezers, and two brushes (Nos.0 and 4). A copy of the latest Peco 'Shows You How' booklet, *Kitbuilding for Beginners*, will also be included.

Available soon from Peco stockists.  
[www.peco-uk.com](http://www.peco-uk.com)

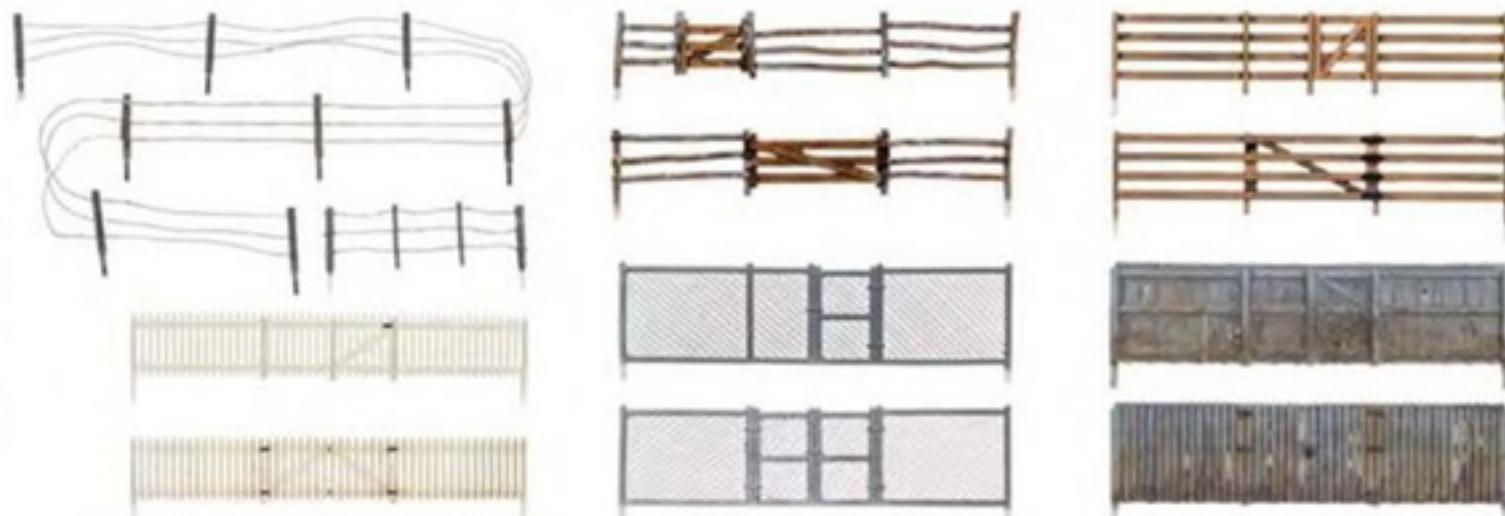


## New fencing from Woodland Scenics

The noted American supplier of scenic accessories, Woodland Scenics, has released a range of hand-painted and realistically weathered fences for N, HO, and O. Supplied in sections, the fences feature gates with hinge detail and are complete with planter pins for simple installation.

Each pack includes sufficient parts for 192 scale feet of fencing. Six types are available: barbed wire, rough log, wooden rail, picket, chain-link, and privacy.

[www.woodlandscenics.com](http://www.woodlandscenics.com)



## Rare opportunity to acquire successful German exhibition layouts

Our regular contributor Norman Raven informs us that as part of a downsizing programme he has to sell two of his larger layouts, *Klinkerhofen* (German O) and *Eisengebrau* (Prussian HO). Both have been featured in CM – September 2016 and September 2014 respectively. Each has been exhibited and won several 'Best Layout' and 'Best Scenics' awards.

*Klinkerhofen* is a terminus to storage yard, wired for DCC (including turnout control). It has professionally built baseboards with integral folding legs and box covers, hand painted backscenes, and a lighting gantry. It measures 22' x 2' (6.7m x 0.6m). It uses Lenz track in the scenic section and Peco for the traverser/storage. Working features include a smoking chimney, welder, and flag waving stationmaster. The price will include all necessary board and electrical connectors, but not the controller and stock.



*Eisengebrau* is also terminus to storage yard format, DCC wired. It too has professionally built baseboards with integral folding legs and box covers, hand painted backscenes, and a lighting gantry. It measures 20' x 18" (6.1m x 0.5m). It uses Peco code 75 track with

electrofrog turnouts operated by DCC. It has working features such as smoking charcoal burners and a hunter with gun flash and sound, plus appropriate period (1910) figures and vehicles. All necessary board and electrical connectors will be included but not controller and stock.

Full instructions will be given for assembly and operation!

Each of these layouts packs so it can be transported in a large estate car.

For further information, Norman can be contacted at:  
[fluff48@zoho.com](mailto:fluff48@zoho.com)

## Andeer Line at Pecorama again

Following the success last year, as an extra attraction for visitors to Pecorama on Saturday 23rd and Sunday 24th February there will be an operating session in the Lecture Theatre/Station Gallery with *The Andeer Line*, the Swiss HOm modular layout built by Julia and Derek Boswell which depicts a section of the metre gauge Rhätische Bahn and uses innovative lightweight foam baseboards for ease of storage and transport.

Visitors will be invited to help operate the layout, which employs digital control, with a central 'signal box' computer and trains driven individually using smart phones and tablets.

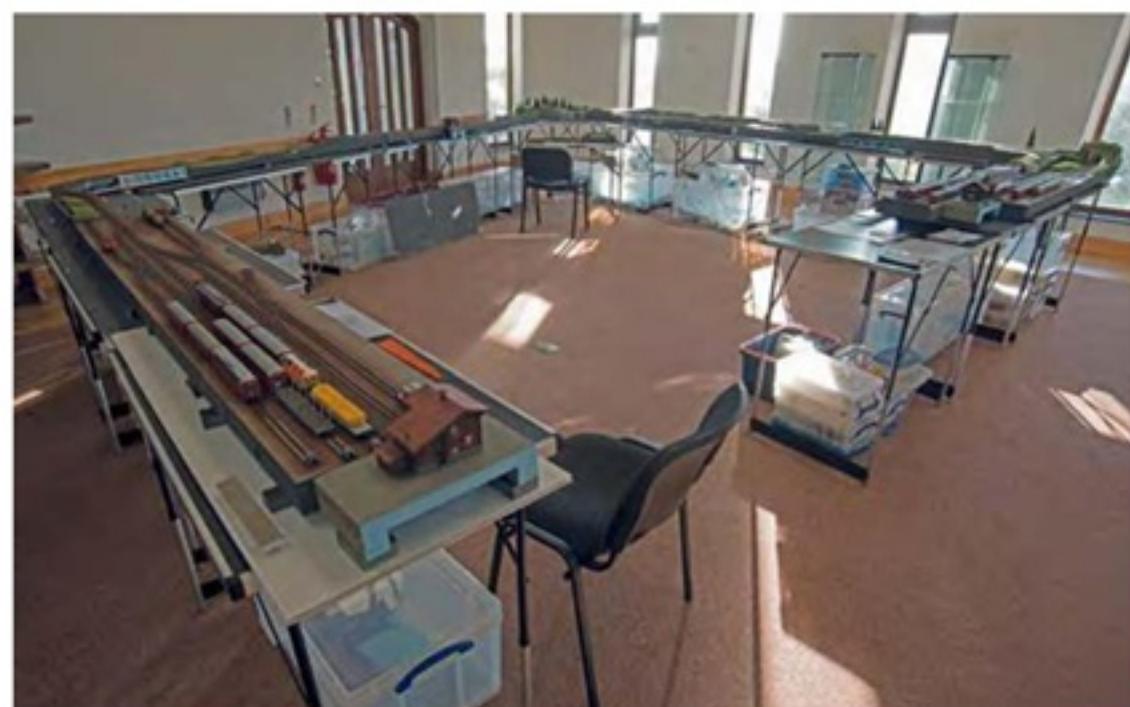
The session will run from 10.00am to 5.00pm both days. Drop in for as long as you wish.

Admission is included within the normal Pecorama ticket price.

Note the gardens will not be fully open and the Beer Heights Light Railway will not be in operation; also the model shop will only be open on the Saturday. Catering will be provided in The Garden Room, and The Play Station will be open.

See [www.pecorama.co.uk](http://www.pecorama.co.uk) for more details.

There should be further Andeer running sessions in the course of the year.



## New Nnoch O gauge figures



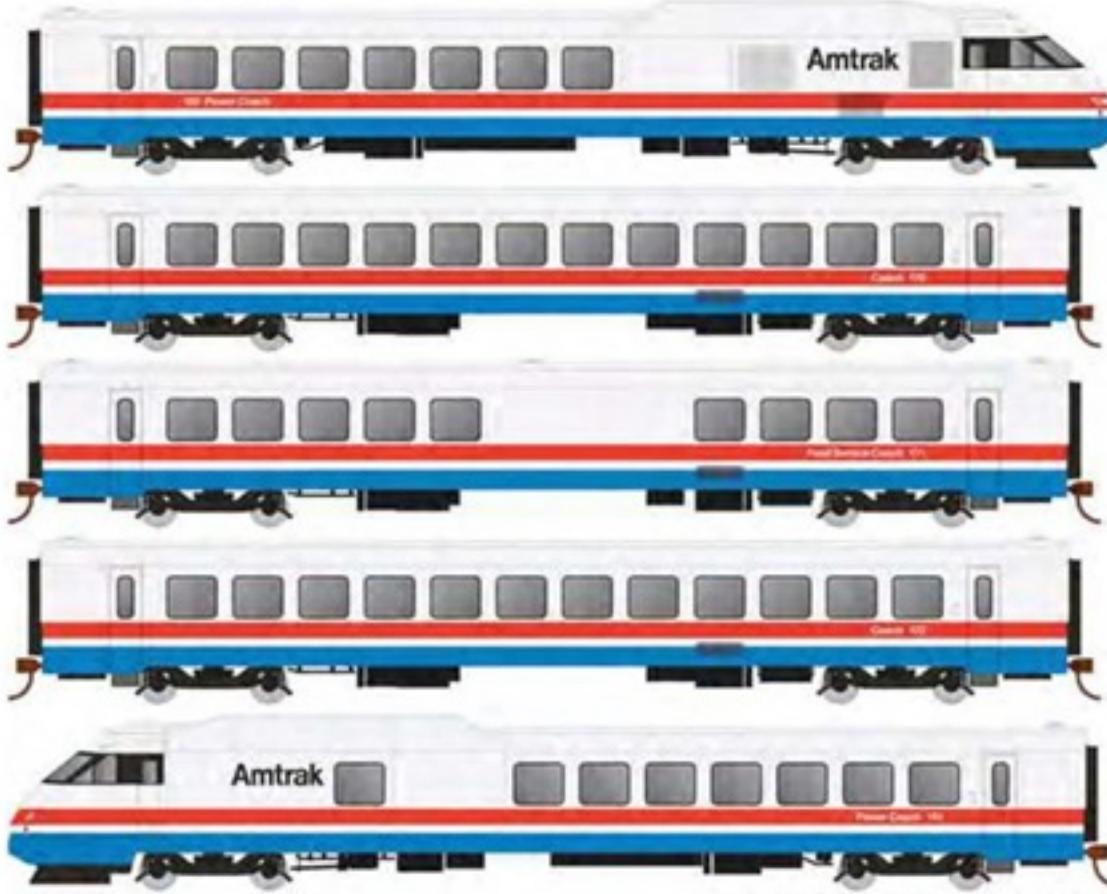
The German scenic specialist continues to expand its range of figures for O scale. New additions include a set of three fishermen (ref.17870), a cyclist (ref.17871), a set of three climbers (ref.17872), a set of wedding figures (a man kneeling proposing to a woman plus a newly wed bride and groom, ref.17880), a set of four black and white cows (ref.17900), and a set of three sheep plus shepherd and sheep dog (ref.17901). [www.noch.com](http://www.noch.com) [www.gaugemasterretail.com](http://www.gaugemasterretail.com)

## Narrow Gauge North

This year's Narrow Gauge North will take place on Saturday 14th March at the Pudsey Civic Hall, Dawsons Corner, Pudsey, Leeds, LS28 5TA. There will be a large number of working narrow gauge layouts in various scales and gauges, including *A Taste of Alber* (Czech TT, CM March 2019), *Vals* (Swiss HOm, coming next month), and *Köszeg Old Town* (Hungarian HO and HOe, CM August 2018), together with a range of specialist societies and traders. Opening times are 10.30am to 4.30pm.

For more details, including admission prices, see *Exhibition Diary* or: [www.narrowgaugenorth.org.uk](http://www.narrowgaugenorth.org.uk)

## Rapido plans Turboliner in HO



The Canadian model manufacturer has announced plans to produce HO scale models of the Rohr Turboliner. Entering service from 1976, these trains were operated by Amtrak on most of the Empire Corridor services out of New York City. They also worked to Montreal, Buffalo, Detroit, and occasionally Toronto.

They are offered in five-car sets, with additional cars available separately. Production is dependent upon sufficient support being received; reservations made before 17th March will be eligible for a 5% discount.

For further details, including prices, liveries, and a full technical specification, go to: [www.rapidotrains.com](http://www.rapidotrains.com)

## O gauge show in Gießen

On Saturday 21st and Sunday 22nd March Messe Gießen (Ausstellungszentrum Hessenhallen, D-35398 Gießen, An der Hessenhalle 11) will again host the International O Gauge Show, the largest event of its kind in Germany.

The show is now organised by Lenz, and supported by *Spur Null Magazin*. It will be considerably larger this year: instead of the previous 3,000m<sup>2</sup>, 4,500m<sup>2</sup> of exhibition space will be available. In place of the small Hall 5 the much larger Hall 1 will be used, giving much more space for exhibitors, layouts, and visitors.

This will allow some exhibitors who could not get a stand last year to participate, and there will be no fewer than 20 layouts, among them Cambach vor der Höhe, our Railway of the Month in the February 2020 issue.

Opening hours are Saturday 10.00am – 6.00pm and Sunday 10.00am – 4.00pm. The entrance area will be open from 9.00am.

Admission costs €12.00 for a day ticket (cheaper than last year) or €20.00 for the weekend. Children under 140cm tall are admitted free. On Sunday (only) a family ticket (two adults with children) is offered for €16.00.

The venue has parking on site and catering facilities. [www.internationale-spur-0-tage.de](http://www.internationale-spur-0-tage.de)

## Coming next month

out 19th March



Nürnberg Trade Fair Report in this issue

EXTRA PAGES!

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### Vals

Swiss HOm

Also in this issue...



Schweizer Kleinstadt



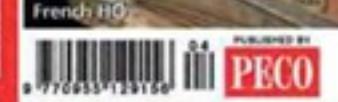
Great White River



Rue des épinettes



Latest Reviews ... and much more



PECO

#### • Vals

Roger Epps and Nathan Bridge chose to model in HOm a station in the upper reaches of the Valser valley on a line proposed but never built by the Rhätische Bahn.

#### • Great White River

The Great White River Navigation, Extraction, and Steam Packet Company operates a line serving a small town somewhere in the Pacific North West, as depicted in the new On30 layout by the Raven family.

#### • Schweizer Kleinstadt

Chris Atkinson has built a new Z gauge exhibition layout representing a typical small town in Switzerland.

#### • Rue des épinettes

Urban decay on Spruce Street – Jean-Michel Tete created a compact but realistically detailed industrial scene in HO.

#### • Nürnberg report – part 1

The first part of our customary comprehensive coverage of the model railway section of this major international toy and hobby fair, dealing with HO.

... and much more!

# Exhibition Diary

Dates, events, and information

## Friday 21 – Sunday 23 February

### GLASGOW

Model Rail Scotland

**Organiser:** Association of Model Railway Societies in Scotland.

**Venue:** SEC, Exhibition Way, Glasgow, G3 8YW.

**Open:** 1030 – 1800 Friday  
1000 – 1800 Saturday  
1030 – 1700 Sunday

**Admission:** On the door – adults £14.00, children £6.00, family (2+2) £30.00. Free exhibition guide.

**Features:** layouts include *The Bridge at Remagen* (N, CM February 2020 and in this issue), *Charlieville* (N), *California Surf Line* (N), *Forththal Schmalspurbahn* (HOe), *Smierweida* (TT), *Irrgarten* (HO), *Jackson Yard* (O), and *A Journey in Switzerland* (G). Societies, demonstrations, trade.

**Contact:**  
[www.modelrail-scotland.co.uk](http://www.modelrail-scotland.co.uk)

## Friday 21 – Sunday 23 February

### HOUTEN, The Netherlands

#### RAIL 2020

**Organiser:** Alex Fichtinger.

**Venue:** Euretco Center, Meidoornkade, Houten, near Utrecht, The Netherlands.

**Open:** 1000 – 1700 Friday  
1000 – 1700 Saturday  
1000 – 1630 Sunday

**Amenities:** free parking; shuttle bus from Houten station (charges apply).

**Admission:** adults €17.00, accompanied children up to 12 free.

**Features:** a very large event with a first class selection of layouts from all over Europe, modelling demonstrations, and specialist traders. Large market section. Includes fifth Proto87 Convention.

**Contact:** [www.rail.nl](http://www.rail.nl)

## Saturday 22 February

### PRINCES RISBOROUGH, Buckinghamshire

**Organiser:** Risborough & District MRC.

**Venue:** Community Centre, Wades Park, Stratton Road, Princes Risborough, Buckinghamshire, HP27 9AX.

Adjacent to Springs swimming pool.

**Open:** 1000 – 1700

**Admission:** adults £4.00, accompanied children £2.00.

**Amenities:** light refreshments.

**Features:** layouts include *Naples Street* (US TT, CM July 2018), *Klapping* (Austrian HO, CM October 2018), and *Fictionville* (French 1:35 on 45mm). Demonstrations, trade.

**Contact:** Mark Bacon 07943 496652  
[www.rdmrc.org.uk](http://www.rdmrc.org.uk)

## Saturday 22 February

### SAFFRON WALDEN, Essex

**Organiser:** Saffron Walden MRC.

**Venue:** United Reformed and Salvation Army Halls, Abbey Lane, Saffron Walden, Essex, CB10 1AG.

**Open:** 1000 – 1630

**Admission:** adults £3.50, seniors £3.00, children £1.00, under 5s free, disabled with carer free.

**Amenities:** charged-for parking at Swan Meadow CB10 1BX; access for disabled to all areas; refreshments.

**Features:** layouts include Questa (US On30), Club second-hand stall, trade.

**Contact:** Julian Nettle 01799 521029  
[julian.nettle@btopenworld.com](mailto:julian.nettle@btopenworld.com)

## Saturday 22 & Sunday 23 February NOTE CHANGE – EARLIER DATE

### CROMFORD, near Matlock

**Organiser:** Mount Tabor Models.

**Venue:** Cromford Community Centre, 9, Market Place, Cromford, Derbyshire, DE4 3RE.

800 yards from station.

**Open:** 1030 – 1630 Saturday  
1000 – 1600 Sunday

**Admission:** free.

**Amenities:** free parking 200 yards.

**Features:** Nürnberg new items show – brochures from the major trade fair. Layouts include *Keinenamebahn* (HOe), *St. Etienne en Caux* (French HOe, CM October 2010), *Brünshafen* (German HO, CM January 2015), *San Demetrio* (Italian HO), and *Eu (Le Tréport)* (French O, CM November 2018), plus a Minitrains HOe layout and stand, a Gaugemaster support stand, European Railways Association, French Railways Society, Italian Railways Society, and Mount Tabor Models sales.

**Contact:** 01629 822294  
[mount.tabor@btopenworld.com](mailto:mount.tabor@btopenworld.com)

## Saturday 29 February & Sunday 1 March

### FULWOOD, Lancashire

**Organiser:** Preston & District MRS.

**Venue:** Sports Hall, Preston College, Fulwood Campus, St. Vincent's Road, Fulwood, Preston, Lancashire, PR2 8UR.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday

**Admission:** adults £8.00, wheelchair users £4.00 (carers free), children £4.00, accompanied under 15s free.

**Amenities:** ample free parking; easy access; refreshments.

**Features:** layouts include *Ulvaryd Strand* (Swedish HOe, CM October 2019), and *Purgatory Peak* (US On30, CM November 2011). Trade.

**Contact:** 07776 438933  
[www.prestonanddistrictmrs.org.uk](http://www.prestonanddistrictmrs.org.uk)

## Sunday 1 March

### CHESHAM, Buckinghamshire

**Organiser:** Chesham MRC.

**Venue:** White Hill Centre, White Hill, Chesham, Buckinghamshire, HP5 1AG.

**Open:** 1000 – 1600

**Admission:** adults £5.00, children £2.50, under 4s free, family (2 + up to 3) £12.00.

**Amenities:** limited parking on site (but includes disabled spaces), plenty of further parking nearby. Refreshments.

**Features:** 10+ layouts including *Obuom Junction* (African N) and *Salinas Valley* (HO). Club second-hand stall, trade.

**Contact:** [www.chesham-mrc.co.uk](http://www.chesham-mrc.co.uk)  
Ron Nettleton 01494 785865

Please send details for possible inclusion to:

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Telephone: 01297 20580 Fax: 01297 20229

e-mail: [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com)

## Saturday 7 March

### WOMBOURNE, near Wolverhampton

**Organiser:** Midarc.

**Venue:** Wombourne Civic Centre, Gravel Hill, Wombourne, WV5 9HA. Regular bus from Wolverhampton with stop outside venue.

**Open:** 1000 – 1600

**Admission:** adults £5.00, concessions £3.00, children under 12 £1.00, under 5s free.

**Amenities:** free parking; full disabled access; refreshments.

**Features:** layouts in various scales including *Kato Tramway*, *St. Fayre-Rhianne* (French HOm), and *Montreal East*. Societies, trade.

**Contact:** 07927 059930

## Saturday 7 & Sunday 8 March

### ROYAL LEAMINGTON SPA, Warwickshire

**Organiser:**

Leamington & Warwick MRS.

**Venue:** Royal Leamington Spa College, Warwick New Road, Royal Leamington Spa, Warwickshire, CV32 5JE.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday

**Admission:** adults £8.00, children £2.00, family (2+2) £18.00.

**Amenities:** free parking; station shuttle bus; refreshments; rest areas.

**Features:** layouts include *Neuburg* (East German HO, CM March 2018) and *Bear Creek Junction* (US HO/HOn3/HOn30).

**Contact:** [www.lwmrs.co.uk](http://www.lwmrs.co.uk)

## Friday 13 – Sunday 15 March

### UTRECHT, The Netherlands

**Modeltrein Expo OntraXS!**

**Venue:** Het Spoorwegmuseum (Dutch National Railway Museum), Maliebaan Station, Johan van Oldenbarneveldlaan 1, 3581 XW Utrecht.

**Open:** 1000 – 1700 each day.

**Admission:** €17.50.

**Amenities:** parking; catering.

**Features:** c.25 layouts (list not available by press date). Demonstrations, manufacturers, and trade, amongst the full size exhibits.

**Contact:** [www.spoorwegmuseum.nl](http://www.spoorwegmuseum.nl)

## Saturday 14 March

### LEEDS, West Yorkshire

**Narrow Gauge North**

**Venue:** Pudsey Civic Hall, Dawsons Corner, Pudsey, Leeds, LS28 5TA.

**Open:** 1030 – 1630

**Admission:** adults £8.00, accompanied children under 16 £1.00. Proceeds support Sue Ryder Care – Wheatfields, Leeds.

**Amenities:** plentiful free parking; disabled access to all areas except the stage; refreshments.

**Features:** layouts include *A Taste of Alber* (Czech TTe, CM March 2019), *Vals* (Swiss HOm, in this issue), and *Köszeg Old Town* (Hungarian HO and HOe, CM August 2018). Trade.

**Contact:**  
[www.narrowgaugenorth.org.uk](http://www.narrowgaugenorth.org.uk)

## Saturday 14 March

### SWINDON, Wiltshire

**Organiser:** Swindon MRC.

**Venue:** STEAM – Museum of the Great Western Railway, Fire Fly Avenue, Swindon, Wiltshire, SN2 2EY.

**Open:** 1000 – 1630

**Admission:** adults £5.00, accompanied children under 16 free. Admission to STEAM not included but not required to enter exhibition. Discount entry to museum for show visitors.

**Amenities:** parking at the Designer Outlet car park; frequent bus services; 10 minutes walk from railway station; disabled access; refreshments.

**Features:** 16+ layouts from Z to O, including *Sankei Valley* (Z, CM October 2013), *Yurakucho* (N), *Kamahashi Kikanku* (N), *Snake Pass* (N), *Jäger Straßenbahn 2* (HOm), *Schwarzsee* (HO), and *Riverside Yard* (HO). Trade.

**Contact:**  
Ian Burbridge 07880 654755  
[www.swindonmodelrailwayclub.com](http://www.swindonmodelrailwayclub.com)

## Saturday 14 & Sunday 15 March

### BASINGSTOKE, Hampshire

**Organiser:**

Basingstoke & North Hants MRS.

**Venue:** Aldworth Science College, Western Way, Basingstoke, Hampshire, RG22 6HQ.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday

**Admission:** £6.00, accompanied children free.

**Amenities:** free parking; heritage shuttle bus from the railway station; disabled-friendly with level access to all areas; light refreshments.

**Features:** 50+ layouts in a variety of scales and gauges including *California Coast* (US Z) and *Eight Dollar Canyon* (US HO). Societies, trade.

**Contact:** 07904 231961  
[www.basingstokemrs.org](http://www.basingstokemrs.org)

## Saturday 14 & Sunday 15 March

### JARROW, Tyne & Wear

**Organiser:** Perth Green MRC.

**New venue:** Primrose Community Association, Lambton Terrace, Jarrow, Tyne & Wear, NE32 5QY.

**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday

**Admission:** adults £6.00, children £3.00, family £

children £3.00, family (2+2) £15.00. **Amenities:** vintage bus service between station and venue via town. **Features:** layouts include *Yatagawa* (Japanese N) and *Ridgeville* (US HO<sub>3</sub>). Demonstrations, trade. **Contact:** [www.keighley-mrc.org.uk](http://www.keighley-mrc.org.uk)

### Saturday 14 & Sunday 15 March MACCLESFIELD, Cheshire

**Organiser:** Macclesfield MRC. **Venue:** Tytherington School, Manchester Road, Macclesfield, Cheshire, SK10 2EE. Free half-hourly vintage bus from the station via the town centre (timetable on website). **Open:** 1000 – 1700 Saturday 1000 – 1630 Sunday. **Admission:** adults £6.00, accompanied children under 16 free. **Amenities:** free parking; level access; refreshments. **Features:** layouts include *Hakuho* (Japanese Z), *Hironocho* (Japanese N, CM December 2016), *Gresten* (Austrian HO<sub>e</sub>, CM September 2012), *Bastogne Rue de Clervaux* (Belgian HO, CM May 2015), *Winziges Industriegebiet* (HO), and *Megantic* (US On2). **Contact:** [www.macclesfieldmrg.org.uk](http://www.macclesfieldmrg.org.uk)

### Saturday 14 & Sunday 15 March NOTTINGHAM

**Organiser:** Nottingham MRS. **Venue:** Highbank Community Centre, Farnborough Road, Clifton, Nottingham, NG11 9DG. **Open:** 1000 – 1700 Saturday 1000 – 1600 Sunday. **Admission:** adults £4.00, children £1.00. **Amenities:** some on-street parking, or use Clifton Nottingham Express Tram park & ride at NG11 8BF. NET stop – Holy Trinity. Level access throughout; refreshments. **Features:** layouts include *Tamaro* (Swiss N), *Antwerpse Viskaai* (Belgian HO), and *Blue Heron Paper* (US HO). Demonstrations, trade. **Contact:** [www.nottingham-modelrailway.org.uk](http://www.nottingham-modelrailway.org.uk)

### Saturday 14 & Sunday 15 March ROYAL LEAMINGTON SPA, Warwickshire

Midlands Garden Rail Show **Organiser:** Meridiene Exhibitions **Venue:** Warwickshire Event Centre, The Fosse, Fosse Way, Radford Semele, Royal Leamington Spa, Warwickshire, CV31 1XN. **Open:** 1000 – 1600 both days. (Last admission 1500 each day.) **Admission:** in advance via website – adults £8.00, seniors £7.50, children 5–14 £3.50. On the door – adults £9.00, seniors £8.50, children 5–14 £4.50. Advance tickets available until midnight Tuesday 10 March. **Features:** layouts in O and upwards, plus specialist trade support, societies. **Contact:** [www.midlandsgardenrailshow.co.uk](http://www.midlandsgardenrailshow.co.uk)

### Saturday 21 March ABINGDON, Oxfordshire

**Organiser:** Abingdon & District MRC. **Venue:** Abingdon College, Wootton Road, Abingdon, Oxfordshire, OX14 1GG. **Open:** 1000 – 1700. **Admission:** adults £8.00, two children free when accompanied by an adult.

**Amenities:** free parking; disabled access; refreshments. **Features:** 17 layouts, including *Jäger Strassenbahn* (HO) and *Auerbach Yard* (US HO, CM October 2010). Demonstrations, societies, trade. **Contact:** [www.admrc.org.uk](http://www.admrc.org.uk)

### Saturday 21 March EGHAM, Surrey

**Organiser:** Egham & Staines MRS. **Venue:** Strode's College, High Street, Egham, Surrey, TW20 9DR. **Open:** 1030 – 1700. **Admission:** adults £6.00, concessions £5.00, children £3.00, family (2+2) £15.00. **Amenities:** refreshments. **Features:** layouts include *Schwungischerplatz* (N), *St.Frazal d'Ardèche* (France Nm), *Somewhere in France* (WWI OO9, CM June 2019) and *Santa Agueda* (Portugal HO, CM August 2017). **Contact:** [www.esmrs.weebly.com](http://www.esmrs.weebly.com)

### Saturday 21 March TADWORTH, Surrey

**Organiser:** North Downs Model Railway Circle. **Venue:** The Good Shepherd Church Hall, Station Approach Road, Tadworth, Surrey, KT20 5AE. Venue is close to station and bus stop. **Open:** 1000 – 1700. **Admission:** adults £6.00, concessions/disabled £5.00, under 16s £3.00. All proceeds to The Good Shepherd Church. **Amenities:** free parking; facilities for disabled visitors; refreshments. **Features:** 30 layouts, including *Seehof* (Bavarian HO), *Neumarkt* (German HO), *Zone Industrielle* (French HO), *Klimkówka – Kolej Doliny Ropa = Ropa Valley Railway* (Polish HO), *Lafayette Spring* (US HO), *Five Mile Siding* (US HO), *Pratrea* (US HO), and *Oregon* (US G). Demonstration, trade. **Contact:** [www.ndmrc.info](http://www.ndmrc.info) Stuart Robinson 07758 458827

### Saturday 28 March PITSTONE, Bedfordshire

**Organiser:** Tring & District MRC. **Venue:** Pitstone Memorial Hall, Vicarage Road, Pitstone, Bedfordshire, LU7 9EY. **Open:** 1015 – 1630. **Admission:** adults £3.00, children £1.50, family (2+2) £7.00. **Amenities:** free parking (venue will be signposted locally); access for disabled visitors; refreshments. **Features:** eight layouts from N to G, including *Achalraj* (Darjeeling 009, CM March 2019). Trade, preservation. **Contact:** 01525 850703 [www.tringanddaylesburymrc.co.uk](http://www.tringanddaylesburymrc.co.uk)

### Saturday 28 & Sunday 29 March GATESHEAD, Tyne & Wear

**Organiser:** North East of England Group of the N Gauge Society. **Venue:** The Masonic Hall, Alexandra Road, Gateshead, Tyne & Wear, NE8 1RB. Short walk from Gateshead metro station. **Open:** 1000 – 1700 Saturday 1000 – 1600 Sunday. **Admission:** adults £3.00, children £1.50, family (maximum 4 people) £6.00. **Amenities:** dedicated free parking; disabled access; refreshments.

**Features:** layouts include *Lötschbergbahn* (Swiss N, CM Swiss Special November 2018). Trade. **Contact:** Neil Walker 0191 482 1682, Ian Holmes [i.d.holmes@btinternet.com](mailto:i.d.holmes@btinternet.com)

### Saturday 28 & Sunday 29 March NAILSEA, Bristol

**Organiser:** Nailsea & District MRC. **Venue:** Nailsea School, Mizzy mead Road, Nailsea, Bristol, BS48 2HJ. **Open:** 1000 – 1700 Saturday 1000 – 1600 Sunday. **Admission:** adults £6.00, children £2.00, family (2+2) £14.00. **Features:** layouts include *Somewhere in France* (WWI OO9, CM June 2019), *Klapping* (Austrian HO, CM October 2018), *Karolina Falls* (US On30, CM September 2014), and *Laramie Engine Terminal* (US O, CM November 2017). **Contact:** [www.nailsea-district-mrc.co.uk](http://www.nailsea-district-mrc.co.uk)

### Saturday 28 & Sunday 29 March SHEFFIELD, South Yorkshire

**Organiser:** Sheffield Model Railway Enthusiasts. **Venue:** Birkdale School, Oakholme Road, Sheffield, South Yorkshire, S10 3DH. **Open:** 1000 – 1700 Saturday 1000 – 1630 Sunday. **Admission:** adults £6.00, children £3.00. **Amenities:** limited disabled access; refreshments. **Features:** 19 layouts including *Republic Steel* (US Z, CM October 2016), *Ulvaryd Strand* (Swedish HOe, CM November 2019), *Menabrea* (Italian HO, CM December 2015), *Hadleyburg* (HO), *Tramway de Balaigne* (Oe), and *Canaan Valley* (On30). Trade. **Contact:** Kevin Rayworth 0114 296 2403 [www.sheffieldmodelrailwayenthusiasts.com](http://www.sheffieldmodelrailwayenthusiasts.com)

## Meetings

### Monday 2 March – 1400 RCTS – Wellingborough

*The Railways of Zurich* by Ken Follett. **Venue:** The Wellingborough Museum, Castle Way, NN8 1XB. **Contact:** Ronnie McAdam 0131 669 7503

### Tuesday 3 March – 2000 The Enfield Transport Circle

The 2020 Malcolm Alderman Tribute Presentation: *Giant Steam Locos of the USA* (video) by Kenneth Crane. **Venue:** First Floor Meeting Room, The Drill Hall Sports Club, 1, Old Park Avenue, Enfield, EN2 6PJ. **Admission:** £4.00. **Contact:** [k.h.crane@hotmail.co.uk](mailto:k.h.crane@hotmail.co.uk) Kenneth Crane 07596 142117

### Wednesday 4 March – 1915 Swiss Railways Society – Manchester

*RhB Green Turns To Red* by Phil Donbavand. **Venue:** Friends' Meeting House, 6, Mount Street, Manchester, M2 5NS. **Contact:** [manchester@swissralsoc.org.uk](mailto:manchester@swissralsoc.org.uk)

### Monday 9 March – 1930 SEG – Sussex

*A Visit to Switzerland with the help of a Rail Pass* by Godfrey Gould. **Venue:** The Deall Room, Southwick Community Centre, BN42 4TE. **Admission:** non-members £3.00. **Contact:** [stuart.hicks90@gmail.com](mailto:stuart.hicks90@gmail.com)

### Tuesday 10 March – 1930 Wells Railway Fraternity

*The 'Berliner' British Military Train* followed by *The Royal Train*, both by Douglas Beazer. **Venue:** Wells Town Hall, Market Street, Wells, Somerset, BA5 2RB. **Contact:** Andrew Tucker 01749 830695

### Wednesday 11 March – 1900 Swiss Railways Society – London

*Swiss Alphabet – My Digital Slide Collection* by Martin Fisher. **Venue:** Meditation Room, YMCA King's Cross 1, 120, Cromer Street, London, WC1H 8BS. **Contact:** [london@swissralsoc.org.uk](mailto:london@swissralsoc.org.uk)

### Friday 13 March – 1930 Swiss Rly Soc – Bristol & Bath

*Swiss Mélange* by Roger Payne. **Venue:** Jack Pugh Room, Bath Scout Headquarters, 7, Grove Street, Bath, BA2 6PJ. **Admission:** £2.00 donation to help cover the cost of the room hire. **Contact:** [henderson@timicomail.co.uk](mailto:henderson@timicomail.co.uk)

### Saturday 14 March – 1100 Swiss Rly Soc – West of England

*Pulsating Poznan* by Richard Green. **Venue:** The Keyberry Hotel, 17, Kingskerswell Road, Newton Abbot, TQ12 1DQ. **Contact:** [wofe.branch@swissralsoc.org.uk](mailto:wofe.branch@swissralsoc.org.uk)

### Monday 16 March – 1930 RCTS – Didcot

*Swindon To Saigon And Beyond* by John Sreeves. **Venue:** Didcot Civic Hall, Britwell Road, OX11 7JN. **Contact:** [tv-fixtures@rcts.org.uk](mailto:tv-fixtures@rcts.org.uk) Andrew Jenkins 01793 783749

### Tuesday 17 March – 1930 The Grimsby & Cleethorpes Railway Group

*California Dreaming* by Graham Lightfoot. **Venue:** The No.1, Station Approach, Cleethorpes, DN35 8AX. **Contact:** [www.gcrsociety.co.uk](http://www.gcrsociety.co.uk) Stuart Blackbourn 01472 505853

### Monday 23 March – 1930 RCTS – Maidenhead

*A Taste of Japan* by Gordon Davies. **Venue:** Cox Green Community Centre, Highfield Lane, Cox Green, SL6 3AX. **Contact:** Ronnie McAdam 0131 669 7503

### Wednesday 25 March – 1930 RCTS – Chichester

*The Berlin U Bahn* by Brian Hardy. **Venue:** Council Meeting Room 2, County Hall, West Street, PO19 1RQ. **Contact:** Ronnie McAdam 0131 669 7503

### Wednesday 25 March – 1930 Swiss Rly Soc – West Midlands

*January – March European Ramble (Poland, Hungary, Czech Republic, and Austria)* by Richard Shaw. **Venue:** Kidderminster Railway Museum, Station Approach, Comberton Hill, Kidderminster, DY10 1QX. **Contact:** [west.mids@swissralsoc.org.uk](mailto:west.mids@swissralsoc.org.uk)

### Thursday 26 March – 1900

*Swiss Rly Soc – East Midlands* *This And That, And A Bit Of The Other* by Glyn Jones. **Venue:** Brunswick Inn, Railway Terrace, Derby, DE1 2RU. **Contact:** [east.mids@swissralsoc.org.uk](mailto:east.mids@swissralsoc.org.uk)

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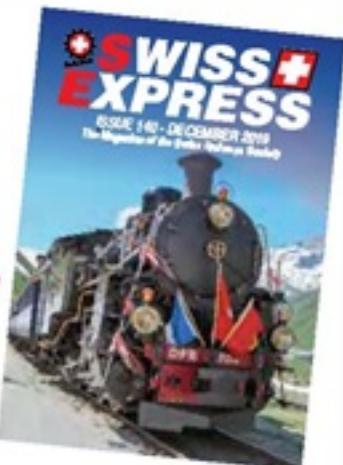
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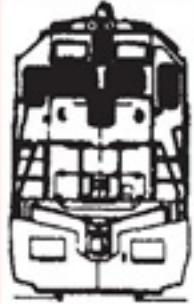
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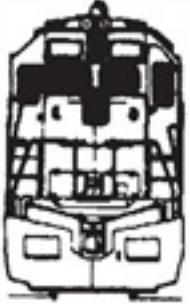
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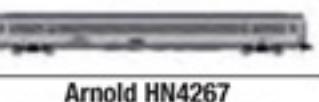
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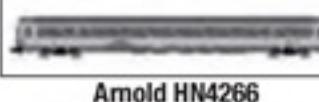
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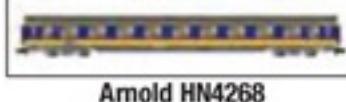
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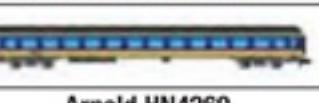
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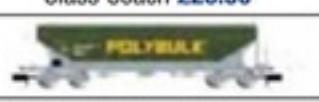
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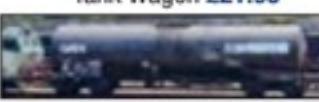
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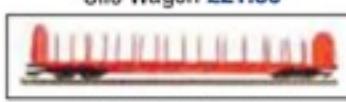
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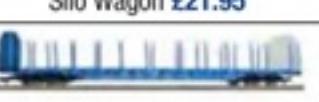
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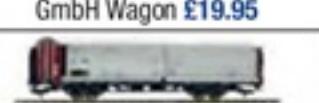
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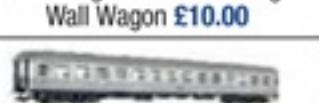
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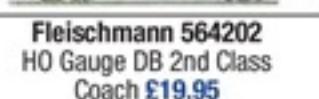
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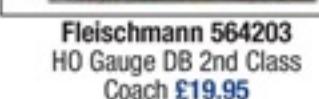
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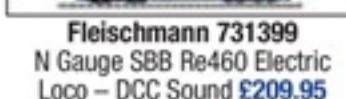
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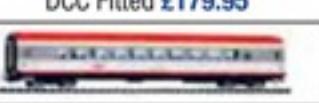
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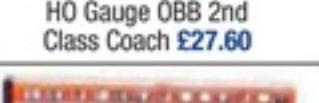
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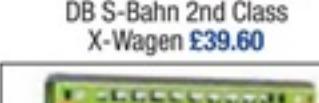
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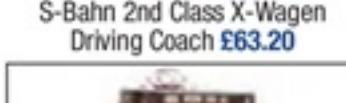
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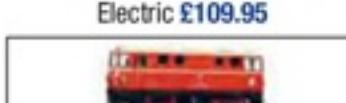
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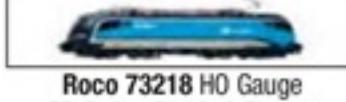
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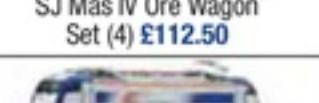
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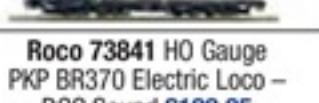
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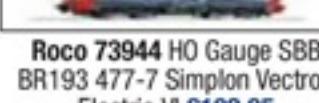
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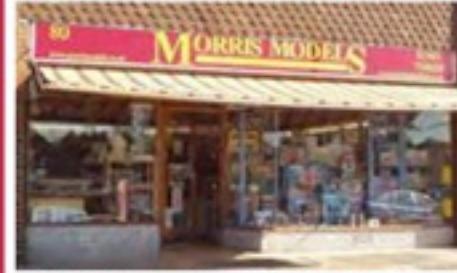
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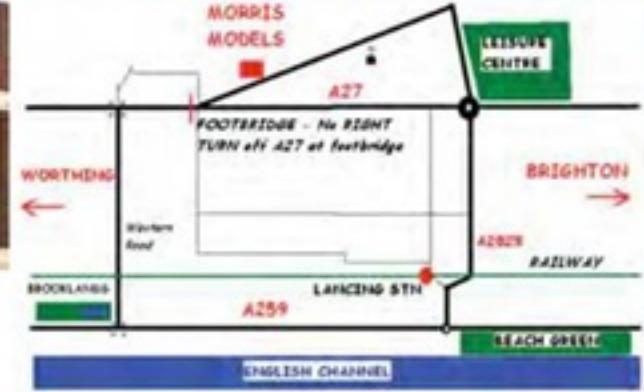


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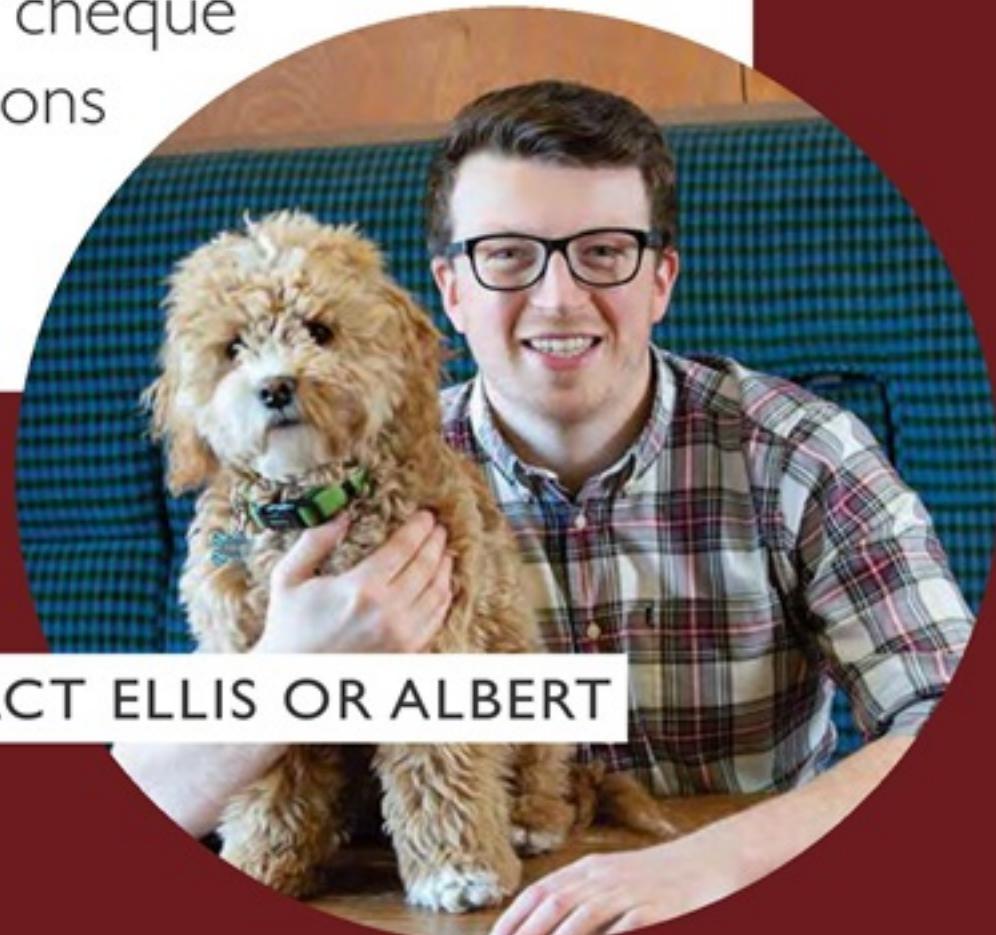
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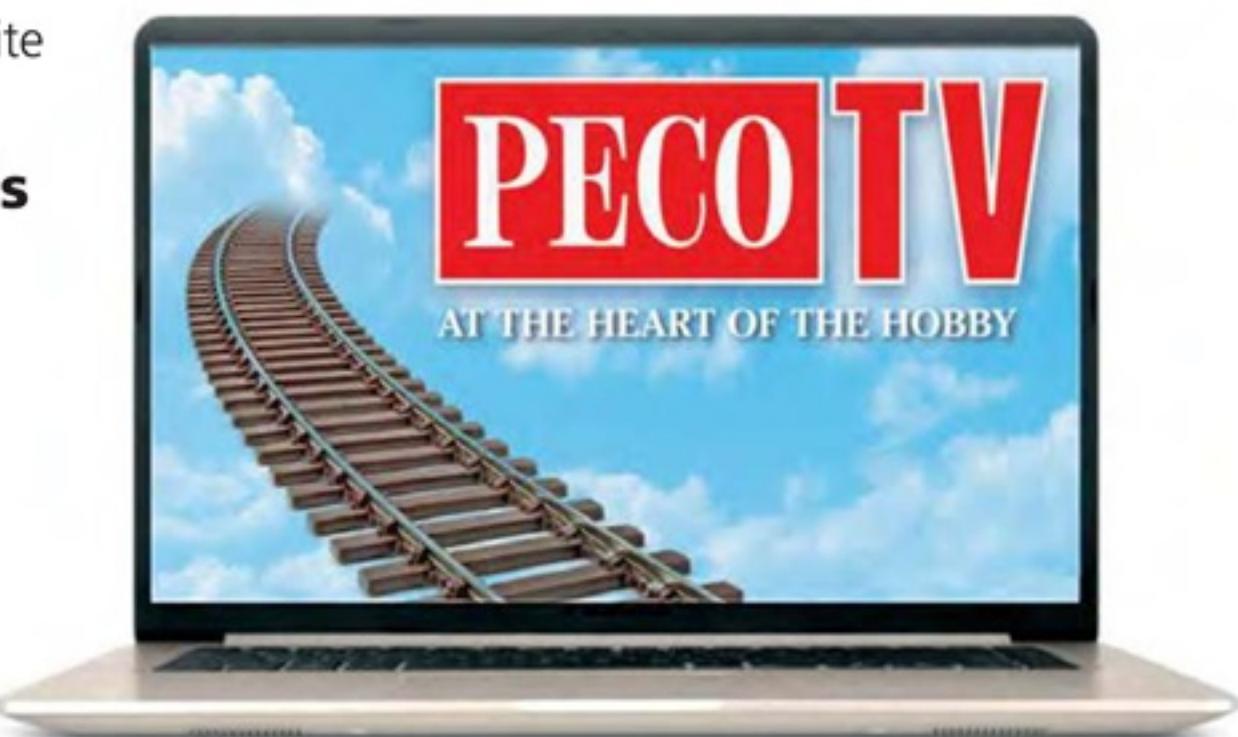
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# MODELMAKER



BY BACHMANN

The latest product to be added to the ModelMaker range of modelling tools is the incredibly versatile **LED Work Light** (MM025).

This compact foldable LED lamp comes with five light settings, magnetic base and an ergonomic design. It provides portable cool, shadow free light for a variety of modelling, craft and DIY tasks.

## Features

- 24 COB LEDs producing 5000 – 6000K light
- Energy efficient & sustainable, using only 3W
- 5 settings provide soft & bright white light, flashlight, red light & SOS mode
- Shadow free, cool light that cuts glare and helps reduce eye strain and headaches
- Foldable design with magnetic base
- Portable & lightweight with textured handle and hook
- Built-In rechargeable battery
- Run time up to 12 hours
- Includes 1m USB cable for charging



# LED Work Light

MM025



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Model • Collect • Create



## HO Scale

Roco produce a large range of HO Scale German Railway locomotives nad rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras!

A HO track system is also available, including the Rocoline pre-ballasted track.



## TT Scale

TT Scale is 1:120 Scale and is a good compromise if you want the detail of HO but only have the space for a smaller layout and don't want to go down to N Scale.

Roco produce a small but growing range of items in this size, so they are will worth exploring in more detail.



## Z21 Digital System

It can be programmed to change locomotive speed and points at the touch of a button, and as it is a free app, anyone can join in as long as they have a supported device!

The Z21 digital system is an innovative way of controlling your layout. Utilising any Apple or Android phone/tablet, you can control trains and accessories on your layout using the Z21 app.