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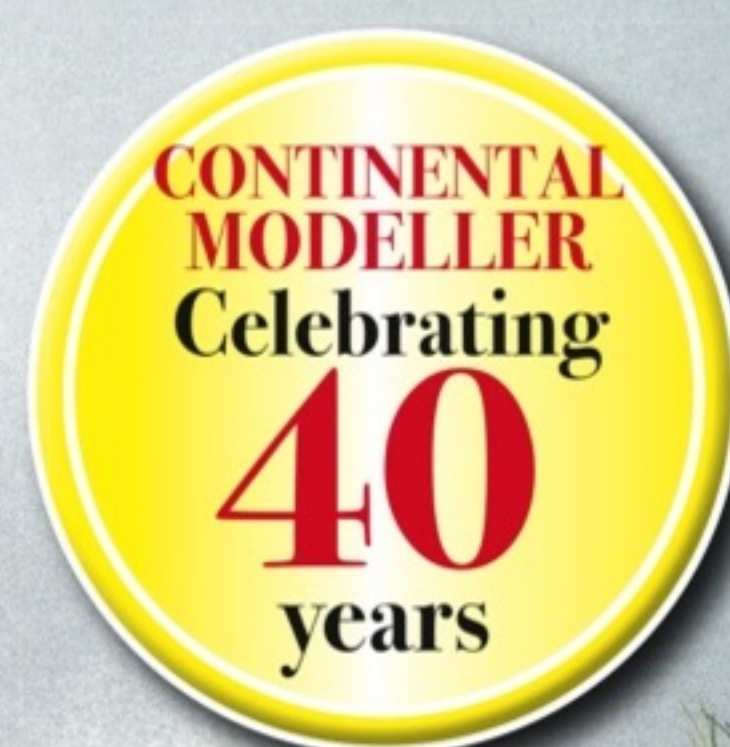
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FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

VOLUME 41

Graafstroom

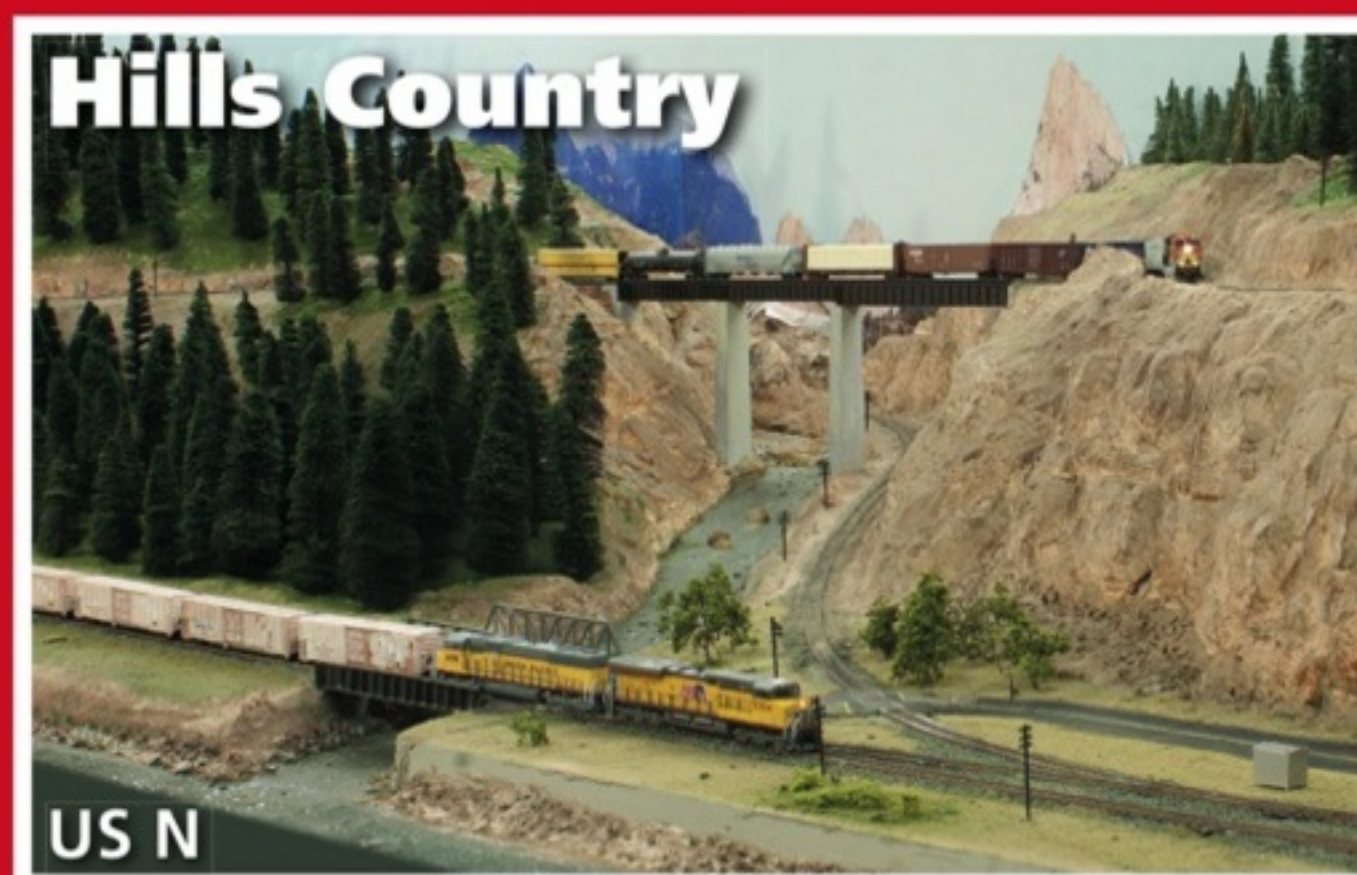
Dutch P87n3.5



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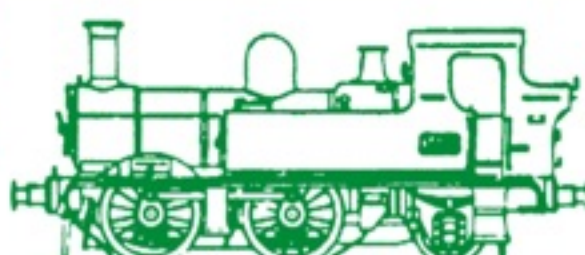


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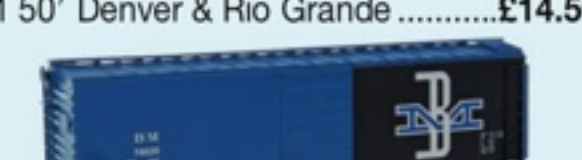
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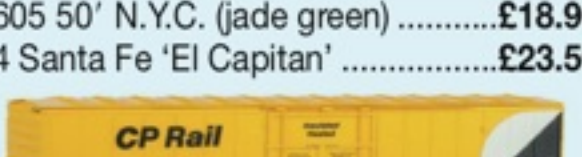


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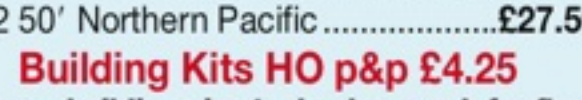
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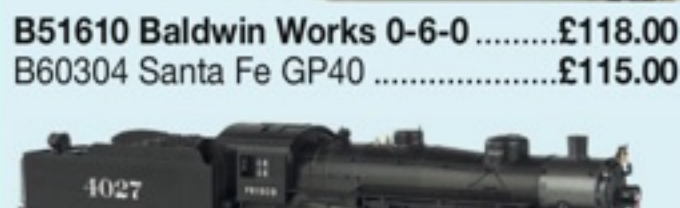


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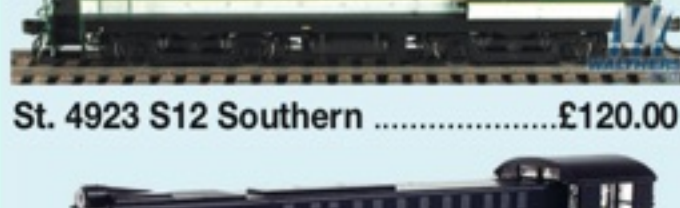


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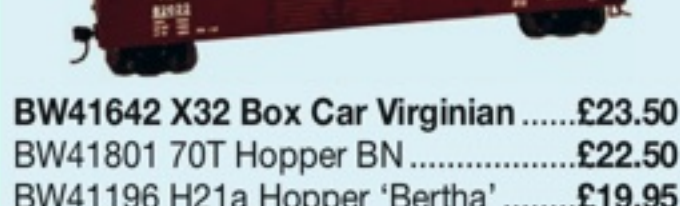
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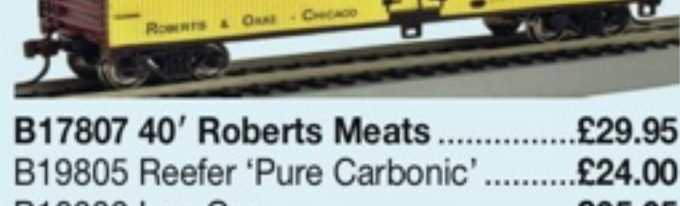
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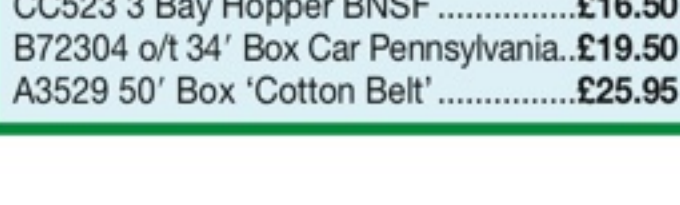
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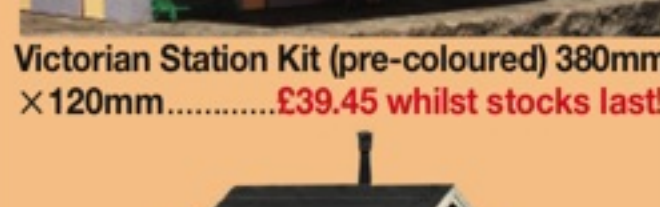
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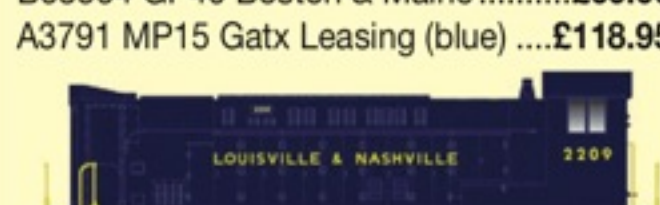
K402 FEF Union Pacific£190.00



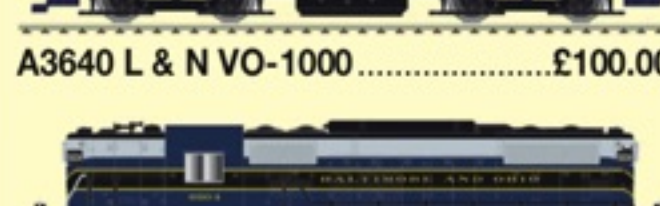
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B63564 GP40 Boston & Maine£66.00

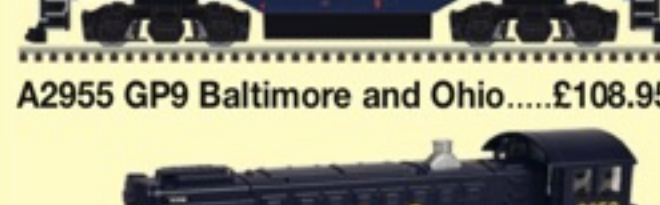
A3791 MP15 Gatz Leasing (blue)£118.95



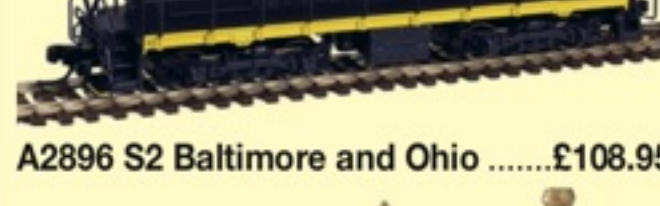
A3640 L & N VO-1000£100.00



A2955 GP9 Baltimore and Ohio£108.95



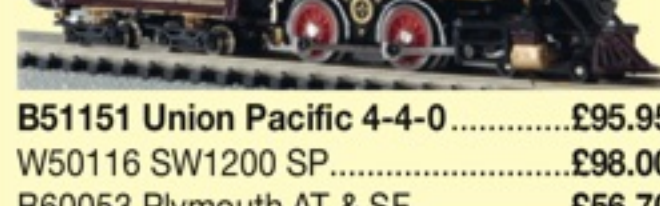
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AN2220 GE U25C Conrail£101.95



K7035 C & NW AC4400CW£108.00

K4367 NW2 CB & Q£100.00

HN2317 GE U28C BN£100.00



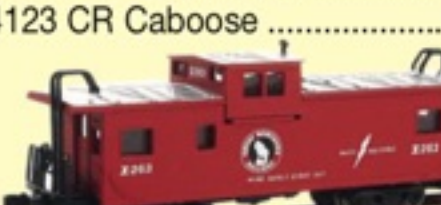
CC14152 Pennsylvania Caboose£17.50



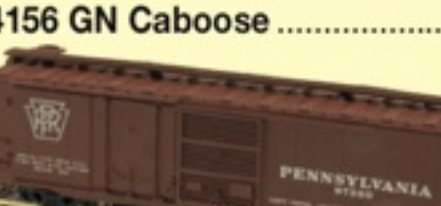
A3322 C & O Caboose£18.50

CC14126 CSX Caboose£17.50

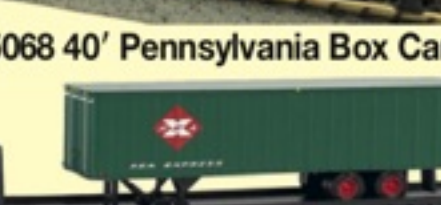
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B16752 B & O Flat and Trailer£29.95



A1808 Mountain Pine Box£14.50

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October 2019

Volume 41

Number 10

From the Editor

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**Published on the third Thursday
of the preceding month by**

Peco Publications & Publicity Ltd.,
Beer, Seaton, Devon, EX12 3NA, England.

Telephone: 01297 21542. Fax: 01297 20229.

E-mail: cm-editor@btconnect.com

Website: www.pecopublications.co.uk

*Distribution to the model trade, & direct subscriptions
(Home & Overseas): Pritchard Patent Product Co.Ltd.
(address and telephone as above)*

*Distribution to the newsagency trade (Home & Overseas):
Marketforce (UK),
Second Floor, 5, Churchill Place, Canary Wharf, London, E14 5HU.*

*Printed by
William Gibbons & Sons Ltd., P.O.Box 103,
26, Planetary Road, Willenhall, West Midlands, WV13 3XT.*

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ISSN 0955 – 1298

*Annual subscription (12 issues):
United Kingdom £48.06 (paid by Direct Debit), post free.
Overseas (including Eire) £79.00.
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RAILWAY MODELLER

For all modelling British railway practice.
Published on the second Thursday
of the preceding month.



Cover

The lifting bridge is raised to allow the tjalk to pass as the Zuid Hollandsche Tramweg Maatschappij (Z.H.T.M.) mixed train pauses to pick up a few passengers at the halt by De Graafstroom. This scene could almost be a Dutch Old Master painting.

Photo: Len de Vries.

Crossing the Channel

Our Railway of the Month, *De Graafstroom*, is coming over from The Netherlands to the Uckfield show this month. We urge you to go and see it if you can – it is as good as it looks in the excellent images provided by our good friends Len de Vries and Gerard Tombroek.

Incidentally, our best wishes to Len for the future following his retirement from the post of Modelling Editor for the Dutch *RAIL Magazine*. His ability as a modeller, his artistic talent, and his skill with a camera will be much missed. We hear he has moved into more spacious accommodation with a dedicated railway room, and hopes to make progress with his own layout at last!

This is a timely reminder – as we peruse the list of layouts due to be presented at the Warley show at the National Exhibition Centre next month – that it is not just such major events which have hosted layouts from the continent: Chatham had a long tradition of doing so, and it is not the only one (the CMRA show in Stevenage this year was another example). But it is uncertain whether it will be feasible for such links to continue – in either direction – when Brexit (in whatever form) takes effect. Moving a layout across the Channel will almost certainly involve more paperwork (if the current situation with Switzerland is any guide) and this will deter some exhibitors and some exhibition managers; they are hobbyists, not professional importers/exporters.

Whatever your views on Brexit (and we do not intend to allow politics to intrude here), it seems likely there will be problems in the short term, and even our hobby is not immune.

Great Model Railway Challenge – series two

The first episode the second series of Channel 5's *The Great Model Railway Challenge* was due to be transmitted at 8.00pm on Friday 13th September, six days before this issue was officially on sale. Unfortunately, the transmission dates were not confirmed by Channel 5 in time for us to publish the information in our September issue.

This second series has been considerably expanded and consists of eight 75-minute programmes – five heats, followed by two semi-finals, and culminating with a grand final, due to be broadcast on Friday 1st November.

Following the transmission of each episode, the programmes will also be available to watch on My5 – Channel 5's online catch-up service: www.channel5.com

A special companion publication to complement the television series has been published by Peco to coincide with the transmission of the second series. This 64-page A4-sized 'bookazine' allows readers to meet the teams competing in the second series, take a brief look back at highlights from the first series, and have a peek through the railway room door of some great British model railway layouts. There are also plenty of hints and tips for those who are inspired by the television series to embark on their own railway modelling projects.

Priced at just £4.99, copies will be available from newsagents, major supermarkets, through Peco model railway retailers, and direct from Peco:

www.pecopublications.co.uk



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TRIX



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MINITRIX N 1:160 Scale

Minitrix produce an extensive range of German locomotives and rolling stock, from the steam age to the modern era.

N Scale gives you the flexibility to build a complex layout in a relatively small area.



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The Trix range covers the majority of Europe in HO Scale, but especially Germany, spanning multiple eras and embracing DCC sound technology. A full range of locomotives and rolling stock is available.



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The Trix C-Track range is a robust, pre-ballasted track system that simply clips together giving a firm, reliable electrical contact between track sections - simply clip together and get your locomotives running.



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Roco

Big in Detail and Technology

HO Scale

Roco produce a large range of HO Scale German Railway locomotives and rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras!

A HO track system is also available, including the Rocoline pre-ballasted track.



H0e Scale

The H0e Scale range is growing in both size and popularity, with Narrow Gauge layouts being built on their own as well as part of existing HO layouts, which are the same scale.

A starter set is the ideal place to begin, with extra track also available to extend your Narrow Gauge line!



TT Scale

TT Scale is 1:120 Scale and is a good compromise if you want the detail of HO but only have the space for a smaller layout and don't want to go down to N Scale.

Roco produce a small but growing range of items in this size, so they are well worth exploring in more detail.



Z21 Digital System

The Z21 digital system is an innovative way of controlling your layout. Utilising any Apple or Android phone/tablet, you can control trains and accessories on your layout using the Z21 app.

It can be programmed to change locomotive speed and points at the touch of a button, and as it is a free app, anyone can join in as long as they have a supported device!

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A Comprehensive Model Railway System

PIKO

Offering great value for money, PIKO is definitely the range to look at for beginners to German railways and existing collectors alike, with a variety of starter sets on offer.



The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert. The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology. The HO Scale A-Track has clear, simple geometry and a realistic appearance.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits. The locomotives run smoothly, with accurately printed liveries and DCC compatibility. This range includes many passenger multiple units that run around Europe, not just in Germany. This allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.



The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors, and is great value for money. The locomotives feature directional lighting, and some even have sound and smoke. Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train. A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale. New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.



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Faller are world famous for their plastic building kits. With a huge range in different scales of Continental-style buildings, any layout will benefit from the inclusion of Faller kits. To compliment the buildings, Faller also produce a range of figures and accessories to enhance the atmosphere of any street scene, as well as trees and other scenic items to add to town and countryside.

CAR SYSTEM

The CAR SYSTEM range allows you to have moving vehicles on your layout with no visible tracks or wires. Automate junctions, traffic lights, and even speed cameras!



FAIRGROUND



The Faller Fairground range is produced in N and HO Scale, and is almost a separate hobby it itself! You can add sound and movement to your layout with interesting rides and attractions.

SCENIC MATERIALS

A variety of grass tufts, scatters and trees to build up realistic vegetation on your layout. Just blend them together to get the desired effect.

MODEL FIGURES

Noch produce a comprehensive range of pre-painted and unpainted figures to populate your layout, from passengers to the emergency services.

LASER-CUT PLANTS

Made from Laser-Cut paper, these kits simply fold together to create realistic plants and vegetables to plant onto your layout.

DECO SCENES

A complete themed scene in a pack, ready to add life to your layout - including the new Sound Scenes range which adds an extra dimension.

marklin

Marklin are one of the oldest model railway companies in the world.

They have over a century of expertise, and currently produce models in Z, HO, and Gauge 1 Scales.



Z 1:220 Scale -

One of the smallest commercially available model railway scales and ideal if you only have a small space, but don't want to compromise on detail.



HO 3-Rail 1:87 Scale -

Marklin produce a unique 3-Rail system in HO Scale, with locomotives and stock spanning the eras from the dawn of railways to the present day.



Gauge 1 1:32 Scale -

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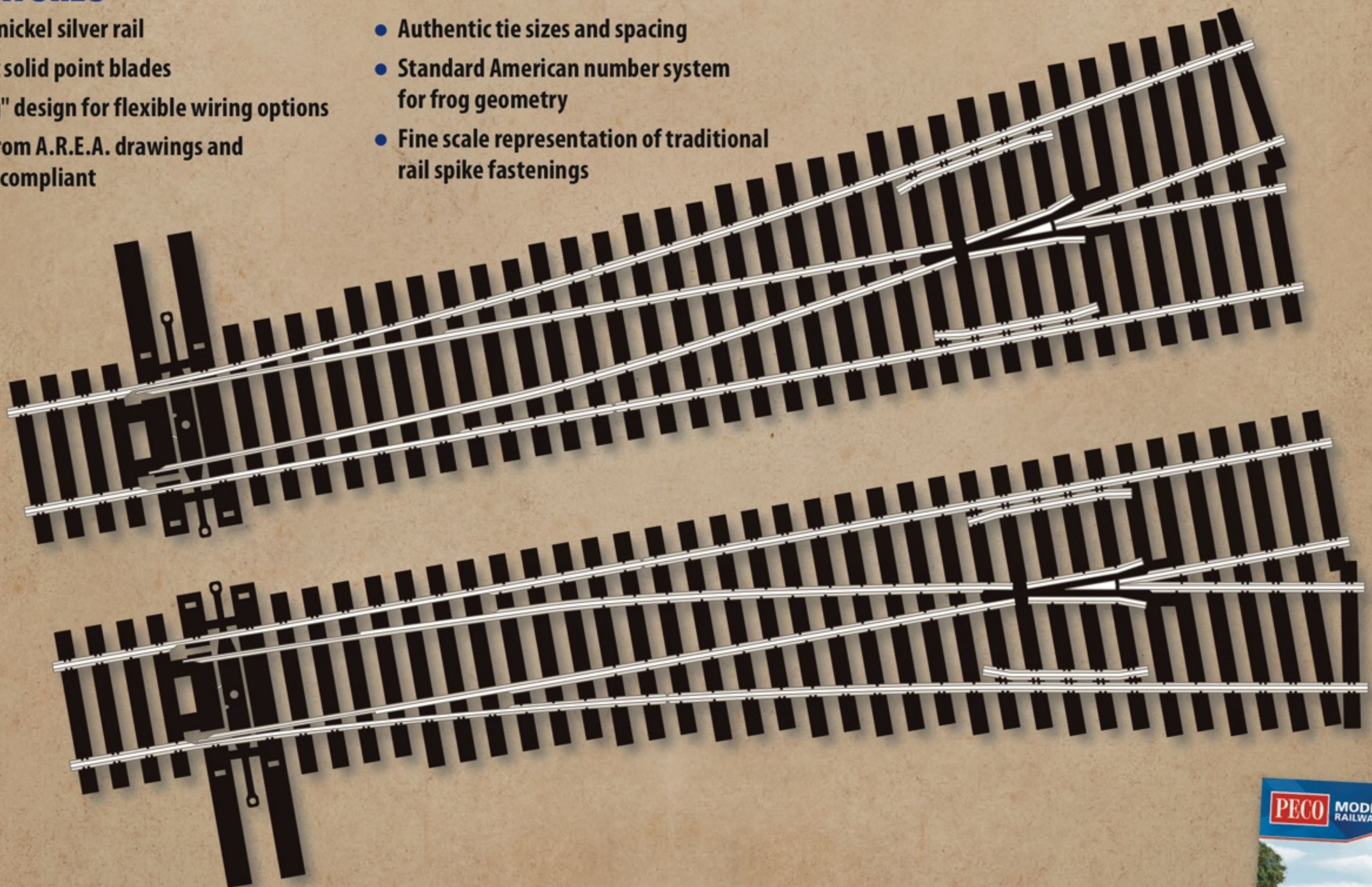
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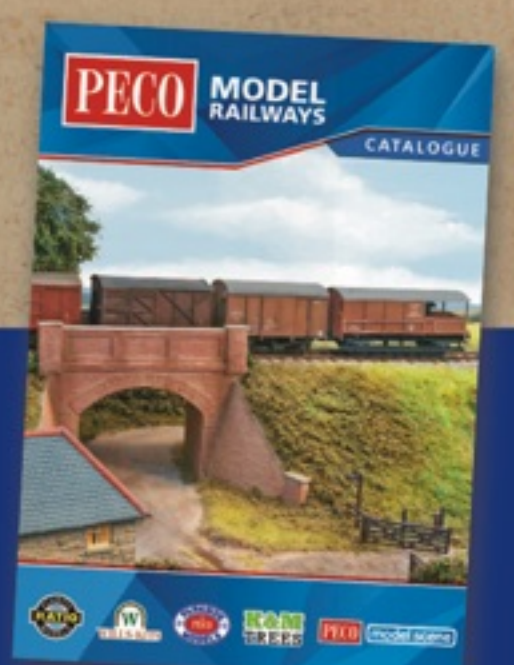
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Gerard Tombroek introduces an exquisite scene with a 1,067mm (3'6") gauge tramway modelled in P87n3.5 by noted fine scale modeller **Vincent de Bode**.

De Graafstroom

A South Holland scene set in 1947

De *Graafstroom* is a new layout by noted fine scale modeller Vincent de Bode, very well known in both The Netherlands and the UK for his British P4 layout *Flintfield*. With the help of his railway modelling friends Peter van der Kooij and Claude Moinier, Vincent has created an atmospheric layout which incorporates a lot of his family history.

The project started about twenty-five years ago with the production of the tjalk (canal sailing barge) by Artitec. At the sight of this model vessel, Vincent felt that it was a bit ‘bald’: it should have a sail. And it should actually move.

From an early age, Vincent had had the idea to build a Dutch prototype layout featuring the mill where his father grew up, the dairy where his uncle worked all his life, and the special landscape of the Alblasserwaard. And ideally with a moving tjalk ... well, you have to plan. And now here was the tjalk from Artitec in 1:87.

The first thing was to get the boat sailing, or at least to suggest that the boat is sailing. The water would consist of a beautifully painted surface. The basic idea is based on the Faller Car System. The principle is fairly simple: a steel guide wire is embedded in the road surface, or in this case

Below
A classic Backer & Rueb 0-4-0 steam tram, ZHTM No.43, approaches the halt with a typical mixed train. The siding on the left is alongside the canal; that on the right leads to the dairy.
Photo: Len de Vries.





Above
Steam tram against sailing barge is not a fair contest.
 Photo: Len de Vries.



Below
The decommissioned mill by the Alblas where Vincent's father grew up. It was one of a set of three which worked together to keep the polder farms dry.
 Photo: Gerard Tombroek.



the water surface. The vehicles are equipped with a motor that drives the rear wheels and there is a magnet on the front axle which follows the guide wire and so the vehicle automatically runs along the road. Unlike with cars, where the front axle is steered, for boats the direction is controlled by the rudder at the rear. In other words, the stern deviates first. In order to move the boat over the water in this realistic way, after the necessary sawing and milling to make room, two 'tricycles' were placed in the tjalk that follow a steel wire in the 'water'. The front wheels are powered by a zoom lens drive unit from an old video camera, supplied by a battery. The rear set just rolls along.

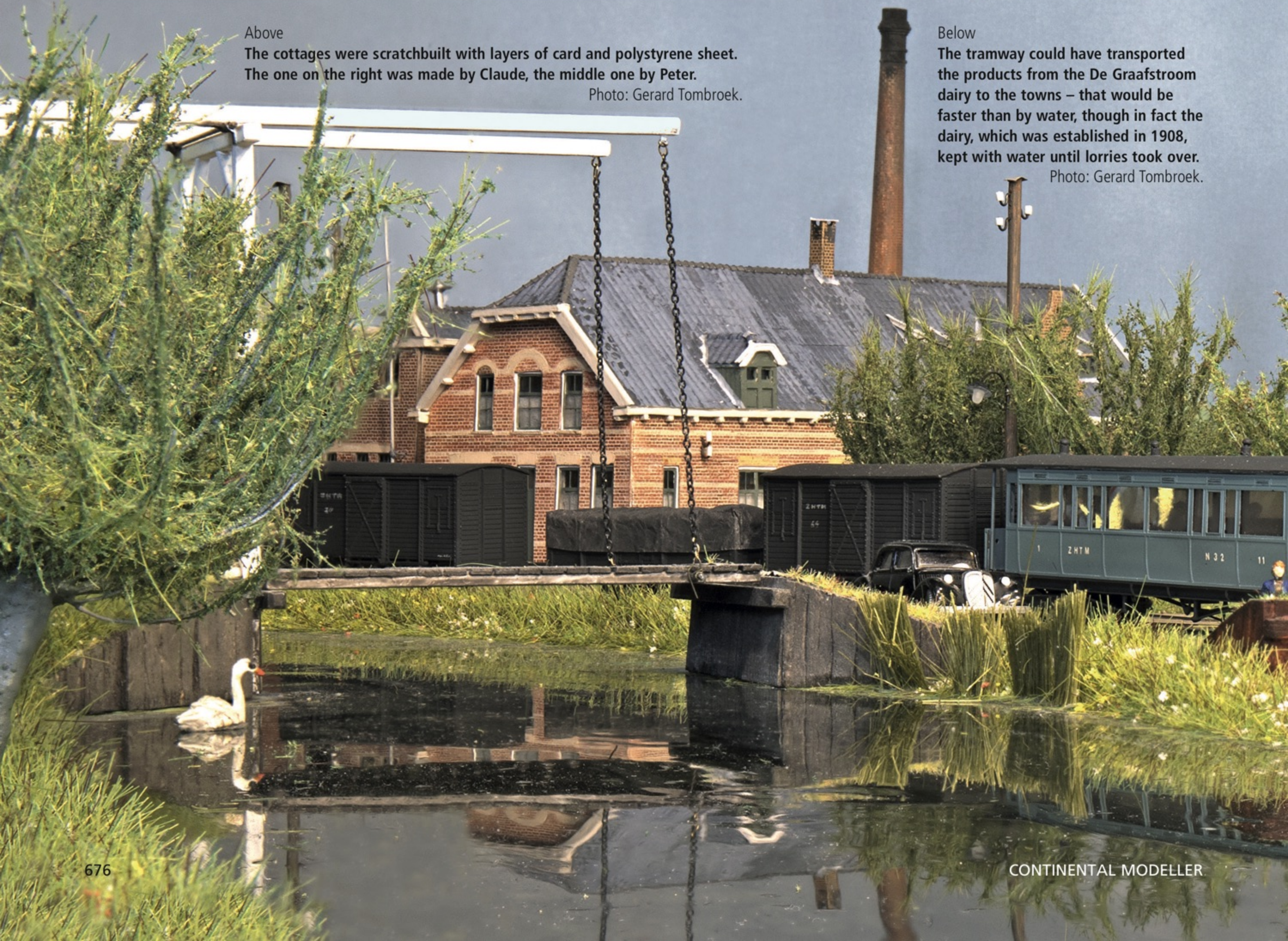


Above
The cottages were scratchbuilt with layers of card and polystyrene sheet.
The one on the right was made by Claude, the middle one by Peter.
Photo: Gerard Tombroek.

A row of mills

With the moving boat a central feature, the landscape was designed with the river Alblas as the starting point. This 10km long river in South Holland, between Bleskensgraaf and Oud-Alblas, starts as the Graafstroom and changes its name just outside Bleskensgraaf, near the Wijngaardeseweg, becoming the Alblas. On the layout it can be seen that the river is higher than the surrounding land. From the twelfth century, the land along the Alblas, which consists of clay on peat (peat bog), was exploited and kept dry by draining at low tide. This caused the drying peat to settle so far that draining was no longer possible and the water had to be pumped away with windmills. Between Bleskensgraaf and Oud-Alblas there was a line of three mills. Their function was taken over around 1900 by a steam pumping station built on

Below
The tramway could have transported the products from the De Graafstroom dairy to the towns – that would be faster than by water, though in fact the dairy, which was established in 1908, kept with water until lorries took over.
Photo: Gerard Tombroek.





the site of the first mill. The other two mills were shut down and served as homes for the driver (Vincent's grandfather) and the stoker. Thus one closed mill became the birthplace of Vincent's father, which Vincent had always wanted to model.

The second building in Vincent's family history is the 'De Graafstroom' dairy which was located in Bleskensgraaf. His uncle worked there, and wrote a book about it. Thanks to that book, it was fairly easy to see what the dairy looked like in 1947, the period in which the layout is set.

The buildings on the layout are all authentic, but arranged with creative freedom to make an attractive composition.

Dimensions

For a layout of any size to be transportable, it has to be built in segments. Moreover, the whole thing had to be able to be taken up the stairs to the attic.

Making the joins invisible is one of the major challenges. The river had to be as long as possible without a join. This determined that the visible part could be at most 2.4m x 1.12m; it consists of two parts, unusually one behind the other: the water and the railway tracks are on the front part, while the rear part is pure landscape. Together with the slightly curved background this gives the effect of enormous depth.

Above

The positioning of all the structures has been adapted with the necessary freedom to obtain an attractive arrangement on the layout.

Photo: editor.



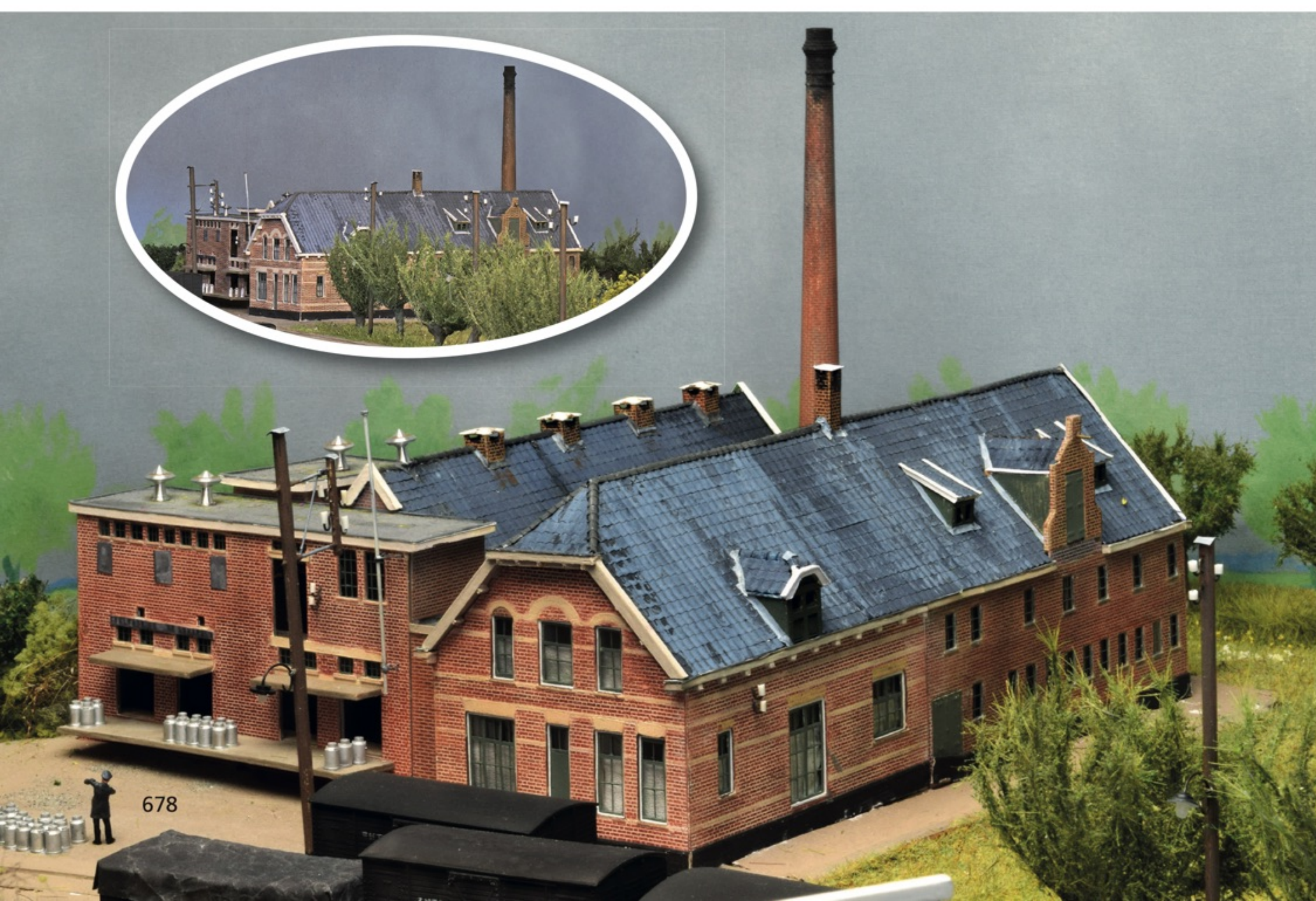
Above

The layout as exhibited.

Photo: Len de Vries.

On each end, behind the backscene, there is a flat space c.50cm square on which there is a loop of the guide wire to allow the boat to turn. At one end there is also a single track turntable for turning a complete train, while at the other end there is a connection for interchangeable cassettes carrying different trains.





Above

The boat really seems to be sailing round this bend in the river, as Vincent has been able to simulate the movement in great detail. Attentive viewers will also observe the swan swimming through the water.

Photo: Len de Vries.

Left

Vincent's uncle worked at the Graafstroom dairy, which was why Vincent wanted to recreate the building as it looked in 1947.

Photo: Len de Vries.

Inset: Gerard Tombroek.

Right

The fictional narrow gauge line is the 'Zuid Hollandsche Tramweg Maatschappij' (Z.H.T.M.). It would have been established in 1910 and remained in use until 1948.

Photo: Gerard Tombroek.

CONTINENTAL MODELLER



Cape gauge

There never was a railway along the Graafstroom and the Alblas – all transport went by water. Even the farms faced the water, not the road. But a railway modeller naturally wants to have working trains, and of course for Vincent they have to be fine scale. The first version used standard gauge track as an NS class 8800 steam locomotive and a van had already built in accordance with P87 standards and skilfully weathered by Len de Vries.

When Vincent showed this first setup to a model railway friend, he commented quite pertinently that narrow gauge would actually be more appropriate for this theme, and to be something really special it should represent the Cape gauge (3'6", 1,067mm) track commonly used in The Netherlands for local light railways. Vincent accepted the challenge, but rather than adopting HOm (12mm gauge), like most modellers in The Netherlands, track was hand laid to 12.26mm gauge (exactly accurate in 1:87) with points built up from code 40 rail (just 1mm high). To keep the rails to the correct gauge and with the right gaps at the pointwork, he first had to make the necessary track gauges. It required a bit of calculation, reducing the P4 standards.





Forthcoming exhibition

The layout is coming over from The Netherlands and will be at the Uckfield MRC exhibition on Saturday 19th and Sunday 20th October. See *Exhibition Diary* for more information.

Below

The Artitec model of the tjalk is hardly recognisable. The main sail is attached to a cable and, as it should be, the seam was pierced by sail eyes and a line wrapped around the mast. The boat sails past the spectators at almost eye level so to minimise the view under the boat, vertically moving baffles have been made in the middle of the hull that slide over the 'water'.

Photo: Len de Vries.



Above left

The bridge is down. Reed contacts under the 'water' ensure that the motor (from a video camera) is switched on in time to lift the bridge.

Photo: Gerard Tombroek.

Rolling stock

Drawings of suitable rolling stock were found in the book *De Betuwsche Stoomtramweg Maatschappij (BSM)*, and extra equipment was brought in from other companies, as in real life.

The locomotives and coaches are scratchbuilt, the freight wagons come from the Jocardis range. The wheels are from England, and were originally intended for TT but have been filed front and rear so they have the correct profile dimensions. The flanges are only 0.5mm high. The locomotives have compensation, the carriages have working suspension, and the wagons have some slack – this appears to be just enough not to derail.

The couplers are adapted Alex Jacksons, as used on *Flintfield*. The adaptation consists of an inverted nose and a steel wire of 0.2mm instead of 0.3mm – this makes them much shorter than the normal Jacksons, 30mm instead of 60mm.

By the way, the NS 8800 and van ended up in the display case of the friend who had suggested that the proper gauge should be used!

Help

There was not much progress with the building for a long time. At an exhibition in 1995, Vincent and his friends Peter van der Kooij and Claude Moinier decided to complete the layout together, but even after that it did not progress quickly. Railway modelling is a hobby, after all, and Vincent has other pastimes, including flying scratchbuilt radio-controlled model aircraft. But after a few encouraging telephone calls, the layout was tackled with renewed enthusiasm in mid-2017 with the result that it was completed in summer 2018.

Left

The bridge has been raised to allow the tjalk through. Just as with the tramway, the tjalk was operated according to a fixed timetable. Both means of transport are now history.

Photo: Len de Vries.

Below

There are more than seventy willows on De Graafstroom. They had to be made and re-made a few times before they would stand the test of criticism, first by the team of three, then later Vincent had another go. The trunk is made of six 1mm iron wires and finished with bark paste. Each crown consists of approximately two metres of 0.3mm iron wire, formed into around thirty branches. The foliage is a combination of 3mm and 6mm electrostatic fibres.

Photo: Gerard Tombroek.



Emmanuel Nouaillier

continues his campaign to encourage detailed modelling of architecture.

Photographs by the author.

Corrugated iron gates

A common feature

Some time ago I described my original techniques for this type of material in 1:87, but since then what I have achieved in larger scales has prompted me to redefine somewhat my approach to weathering. This has been accentuated in the meantime by the appearance of new products.

As a generic example here is an old gate made of corrugated iron sheet to which I will apply different weathering.

Whether on agricultural sheds, industrial warehouses, or other urban structures, reality offers a wide range of varied examples of the use of this material.

This can also be an interesting option for customising commercially available industrial buildings, replacing the doors provided in the kits with such elements and thus giving more character.



Step 1

These doors can be easily made from embossed aluminium foil such as that supplied by Campbell or Builders in Scale.

For this example in 1:48, I will concentrate on a gate made with these sheets, forming part of a building.

I detailed this gate with hinges made from Evergreen strips and Grandt Line bolts, with copper wire for the handles. Note how I distorted and damaged the sheets in places to give it a degraded look.

Before continuing, I apply a coat of Humbrol neutral grey primer.



Step 2

The gate in its opening. I protect the already weathered edges of the wall with masking tape before the application of a first layer of Humbrol ref.98 and ref.100 mixed together directly on the surface. The variations in colour and texture obtained this way are immediately apparent.



Step 3

Once dry, I rework this basic corrosion with acrylic paints from the AK Interactive 'Rust Effect Colors' set, this time using a sponge to simulate coarse variations in the rust. Remember that it is necessary to wring out the sponge on a paper towel to keep a minimum of paint to achieve the desired effect.



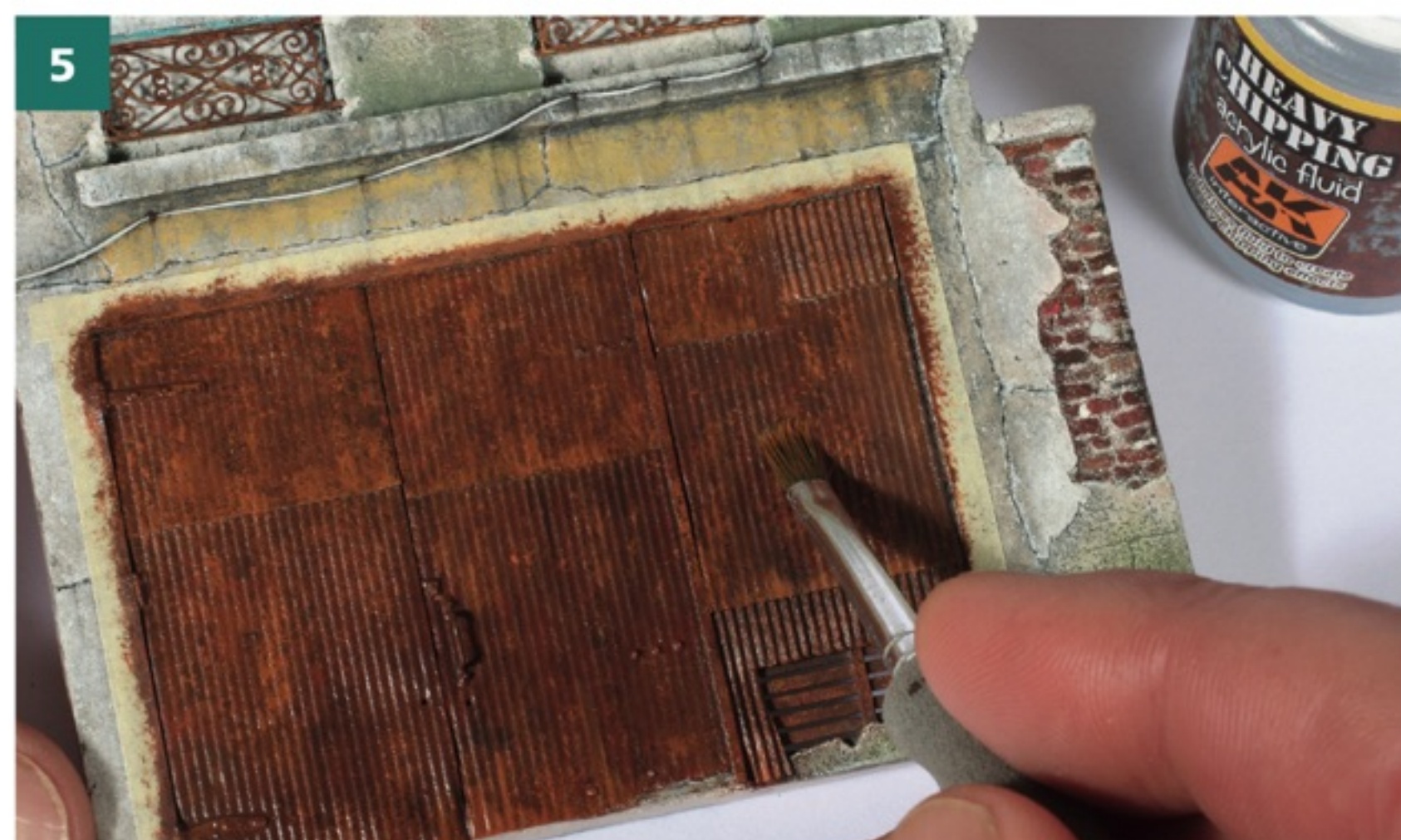
Step 4

I then quickly refine the effect with 'light orange rust' which highlights places where corrosion may appear more pronounced. For this I use a very fine retouching brush with short bristles (made by Kolinsky) for perfect control. At all costs avoid cheap long haired brushes.



Step 5

After a full day drying, the whole is soaked with AK 'heavy chipping' or 'worn effects' fluids, it does not matter. If you do not have these products, you could use hair spray based on natural resin, which also works.



Step 6

About ten minutes later, I apply Vallejo light and dark grey acrylics, in the same way as previously for rust, by mixing the paints directly on the surface. For this, I work with two different width brushes, No.02 and No.04.

Note the separate area covered with orange rust.



Step 7

The surface is moistened with clean water, then the chips are made with a wooden stick, an old brush, etc. – but nothing metallic! I also use a hard-bristle brush that I salvaged from my wife's make-up kit when she was no longer using it.



Step 8

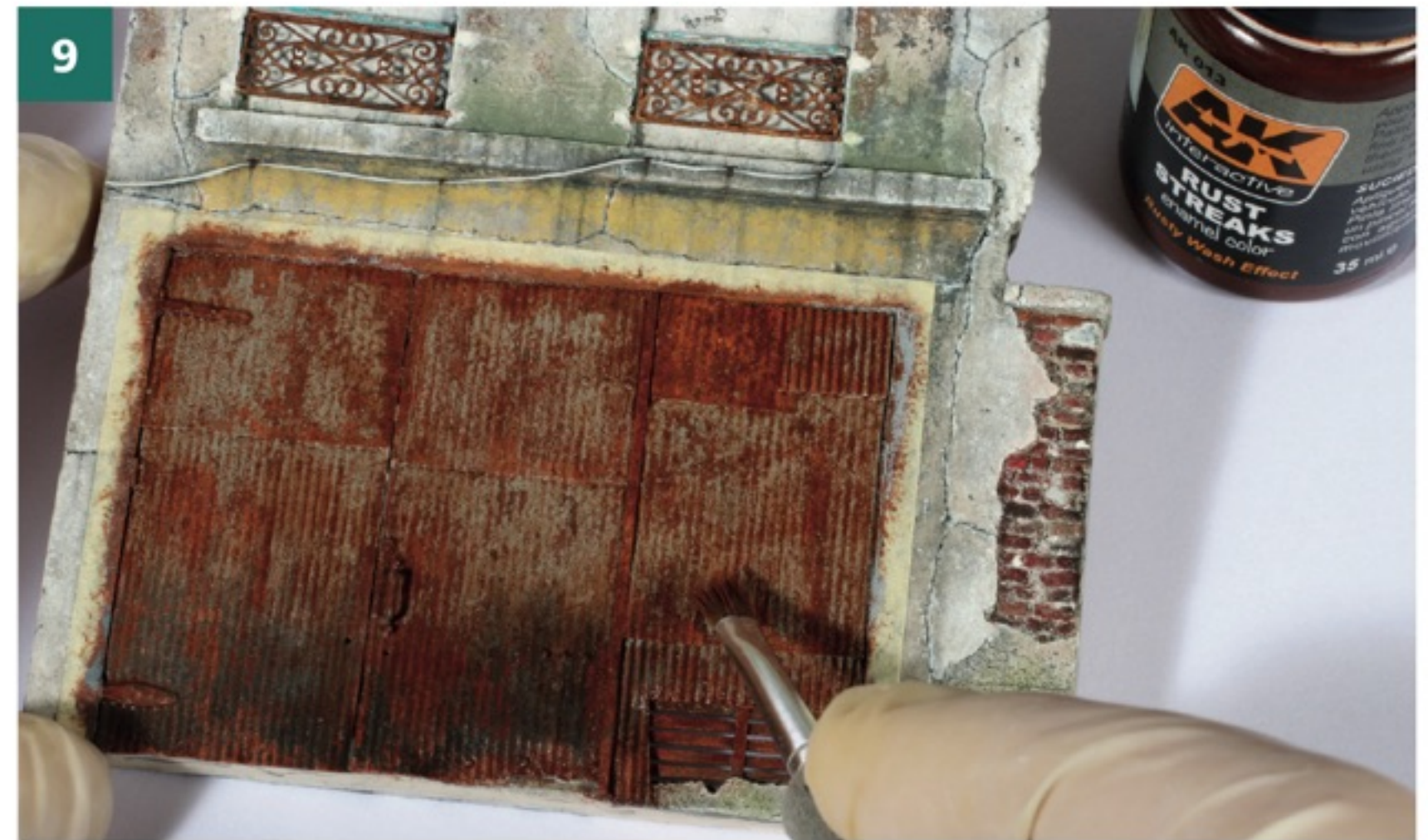
To speed up the drying process and see the result faster, I dry the surface with a soft clean cloth or a paper towel.

At this stage, I can see if some changes are necessary while there is still time to go over it; after a good day's drying the process is set in the final state.



Step 9

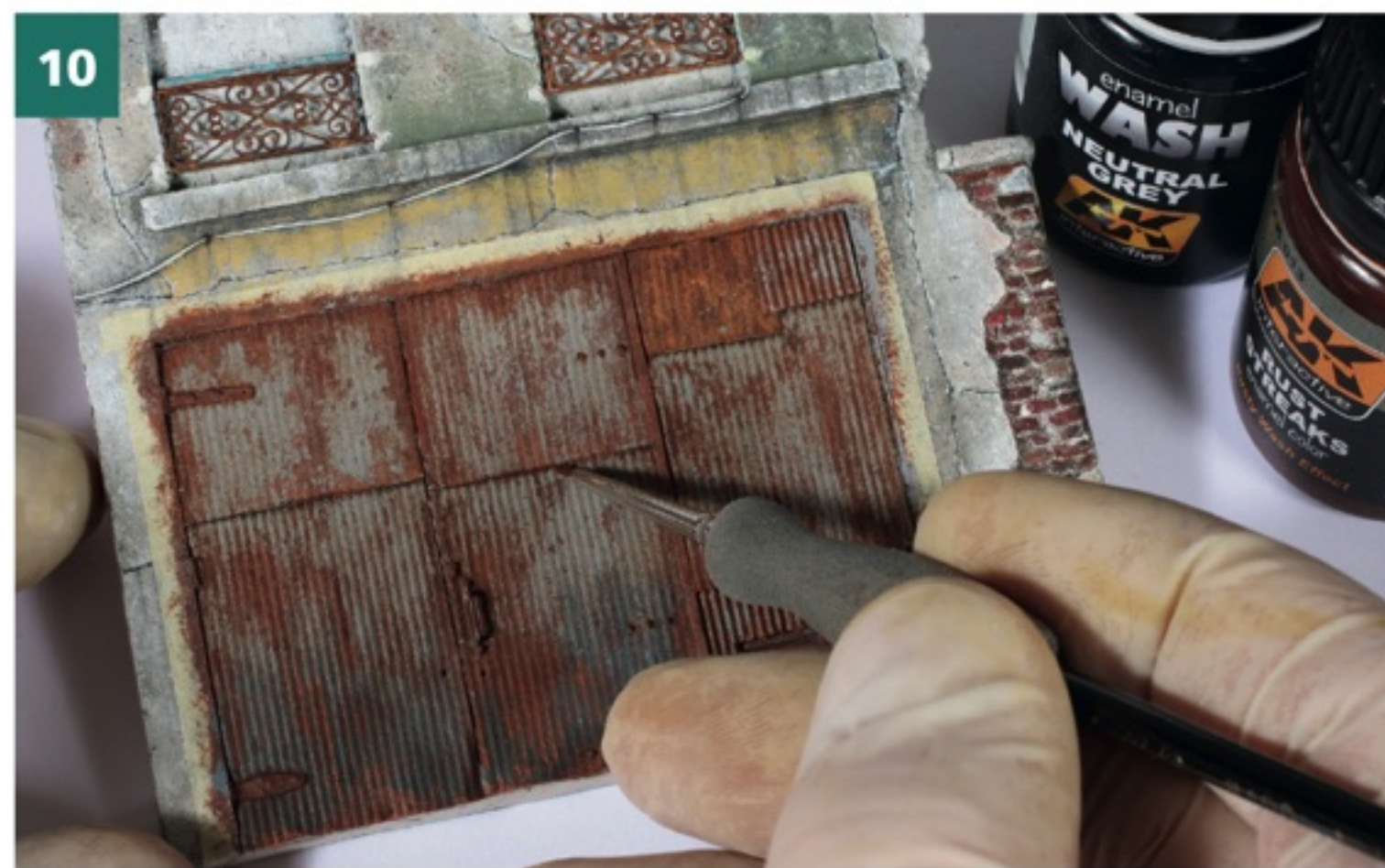
I then go further with the effects of corrosion by applying a coat of AK 'rust streaks' all over the surface. This also helps to unify the rusty appearance of the whole thing while at the same time highlighting some tiny points of corrosion for the best effect.



Step 10

Now to emphasise and perfect the effect. I start by using a wash of AK 'neutral grey' to emphasise the joints between each sheet. Once again I use a fine retouching brush to draw

a thin trickle of liquid which is then blended into the mass using a Tamiya weathering sponge soaked in thinner. If you do not have this kind of tool, you can also use a flat No.2 brush.



Step 11

Hinges and hollows can be washed with AK Interactive 'crusted rust deposit' with a fine brush, leaving a crisp rust outline.



Step 12

I take this opportunity to stamp the lower part of the door irregularly with the same liquid. Everything here is a matter of taste and the effect required. In many cases, a light touch may be enough.



Step 13

The edges of the sheets are then enhanced using the 'Rust Effect Colors' acrylics. Use 'medium rust' or 'light rust' to create strong variations on these narrow bands compared to the rest of the surface ... this is the best effect on such corroded structures.



Step 14

To add more texture to certain places, I then use the good old method with the pigments, first 'wetting' the surface to be treated with mineral spirits.

This is also a good way to bring out shades of rust and obtain even more subtle contrasts.



Step 15

To complete the weathering, you can later add dust to simulate dirt on the lower part, but take care to avoid overpowering the work done previously. To obtain such an effect here, I used a minimal dose of AK 'city dirt' pigments, applied with a very fine brush.



Step 16

To conclude the weathering, I use graphite powder and a black pencil to emphasise the metallic appearance, irregularly on some protruding parts. This must be done without too much emphasis to remain subtle.



Step 17

To personalise the result even more, I finish by adding various enamel signs, e.g. the company name and parking prohibitions, produced as described in a previous article. You could add graffiti, tags, nailed-on boards, posters, etc.



Step 18

Finally, the gate is ready and blends particularly well with the rest of the carefully detailed structure.



Hills Country



It started with a discussion on the way home from a day scouting for layouts for the Colchester exhibition in 1997. We considered the various American N gauge layouts we had seen and felt we could produce something that would take full advantage of the scenic modelling possibilities that N allows.

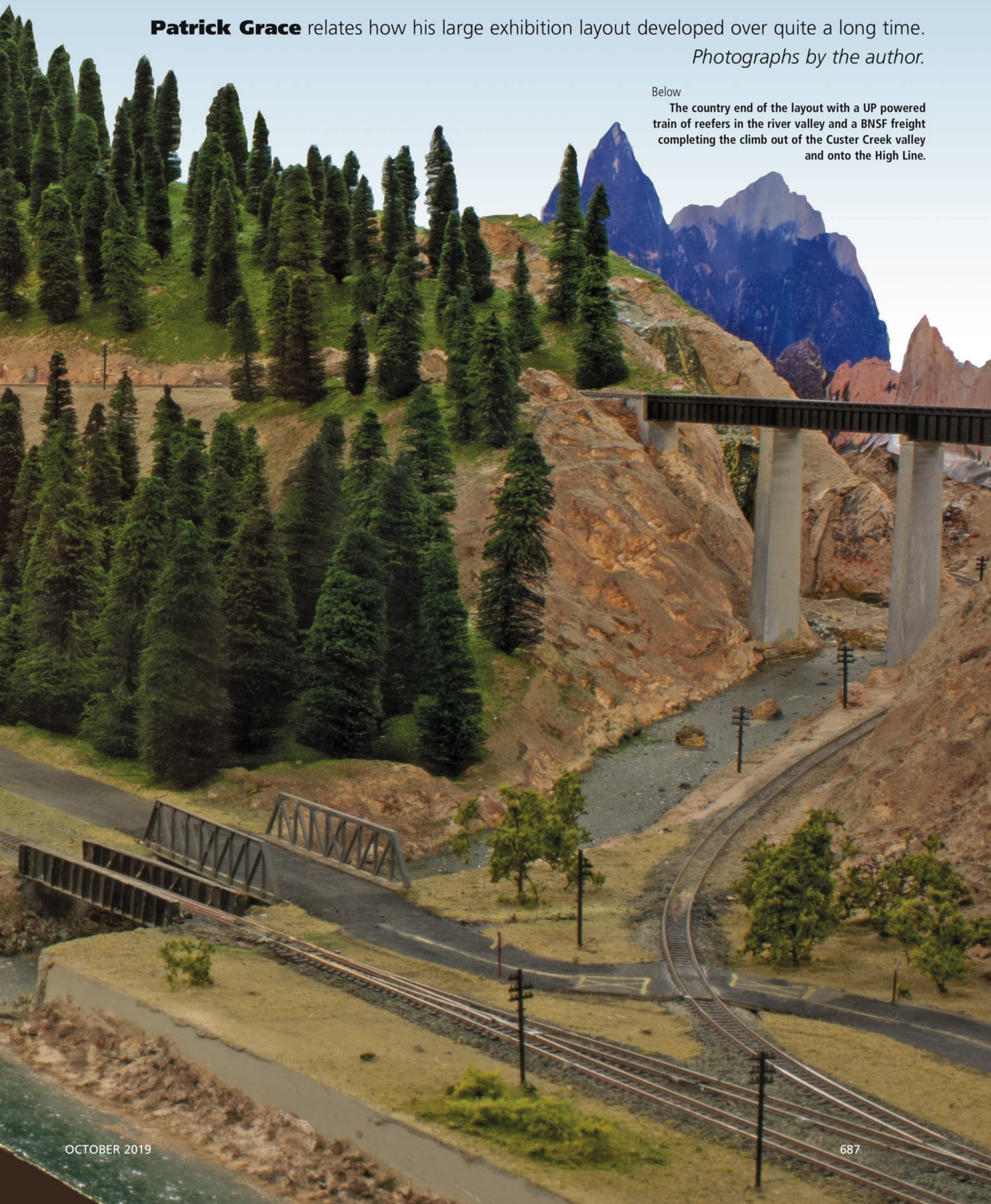
During the next year we developed the first design for the layout, taking ideas from a number of magazines and combining them to form the track plan, using the following criteria:

1. Single track main line at a lower level.
2. Branch line climbing a mountain via a spiral and a large trestle/bridge.
3. Suitable size town yard to feed the two lines.
4. Switching area in the town.
5. Loco depot to enable both loco changes and static display of locos and M-o-W stock.
6. Suitable size reversing fiddle yard with a helix linking to the high level branch.
7. Set somewhere in Colorado.
8. Minimum radius 18" at the front and 12" at the back.
9. To fit into a long wheelbase Transit van.
10. Set in the period 1960 – 1970.

Patrick Grace relates how his large exhibition layout developed over quite a long time.
Photographs by the author.

Below

The country end of the layout with a UP powered train of reefers in the river valley and a BNSF freight completing the climb out of the Custer Creek valley and onto the High Line.





We went through ten designs, using Winrail, and in the process of developing the plan presented it as a project to the Colchester MRC in 1998. Once approved by the committee, construction started and was sufficiently advanced for the layout to be exhibited at the Milton Keynes N gauge show in 2001.

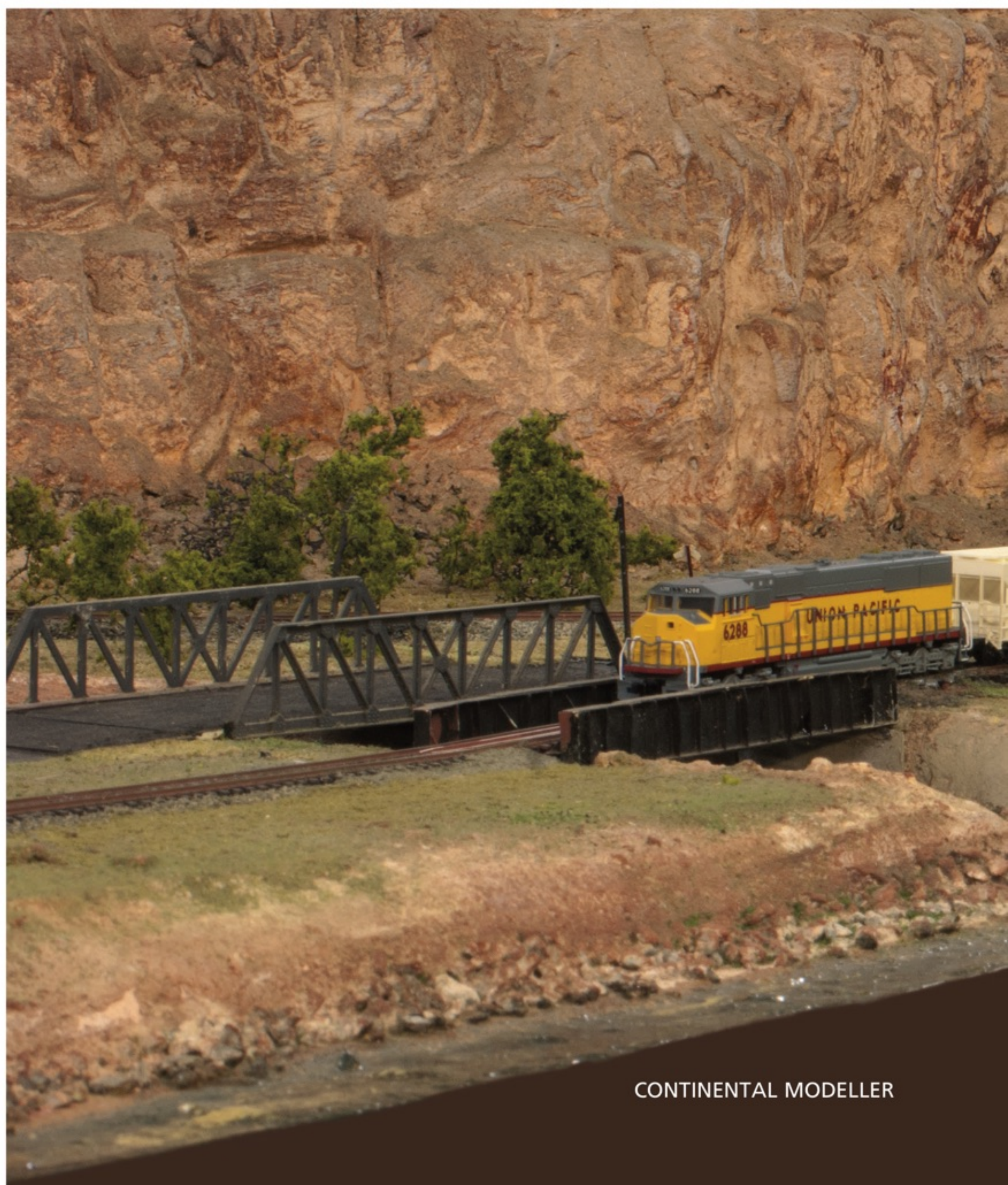
The Mark 1 version of the layout was E-shaped with a scenic loco depot at one end of the layout, the helix necessary to get up/down from the upper level line in the middle, and the fiddle yard / reversing loop at the opposite end at the back; all this was in an area of 21' x 10'.

As constructed the layout was built for analogue DC with a complex power routing cab control panel using six ECM controllers.

Experience over the first couple of years showed that the original design with a couple of reversing loops on the back was less than ideal to operate – in fact it was not possible to set up the switches controlling the reversing loop to operate logically as it was possible to enter the reversing loop from both directions. So we decided to change the layout to remove the issues with the reversing loops. Conversion to DCC happened a couple of years later.

Mark 2 was a straight 24'6" x 5' with a new board added to the end of the layout to create space to install the helix inside a large mountain. The high level line was realigned and taken along behind the backscene to connect to the repositioned helix.

The period was revised to take advantage of the large Atlas and Kato Co-Co locos that were available and relatively easy to fit with decoders; currently the layout is set in the post-1990 period, and in fact really post-2000.



Since then there have been two sets of changes, the first bringing it to its final Colchester MRC condition as Mark 2A, which involved modernising the industrial area by removing some industries and adding a supermarket and parking lot, along with improvements to how the new mountain connected to the rest of the layout scenically, and splitting one of the three tracks on the lower level at the back to make two shorter (9') stabling loops.

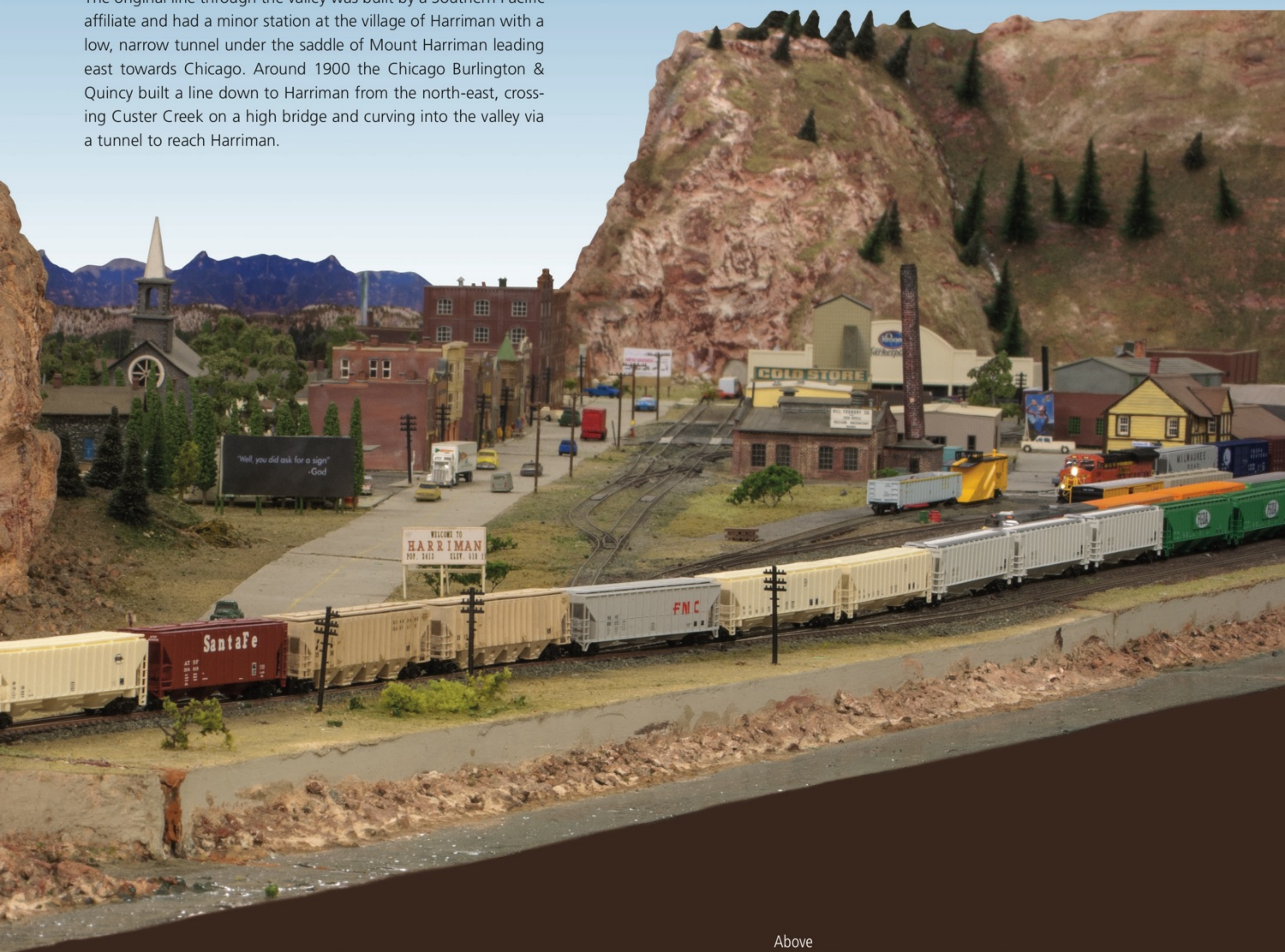
The layout moved with me to Aberdeenshire in spring 2019 and changes have been undertaken, thus creating Mark 2B. These include further changing scenery and structures to reflect the present day, and adding DCC point control to the town yard as we were never able to use the town trackage satisfactorily with the point controls at the back and the train operator in front of the layout. We are now able to use Lenz handheld controls or a wireless connection and Touch Cab on iPhones and iPads.

A little fictional history

The original line through the valley was built by a Southern Pacific affiliate and had a minor station at the village of Harriman with a low, narrow tunnel under the saddle of Mount Harriman leading east towards Chicago. Around 1900 the Chicago Burlington & Quincy built a line down to Harriman from the north-east, crossing Custer Creek on a high bridge and curving into the valley via a tunnel to reach Harriman.

As a result there was a major reorganisation of the facilities at Harriman. A new yard, station, and a loco depot were built on land reclaimed from the river and the SP also realigned their line to the east on an easier grade. The original line through town was retained along Main Street to access a new industrial area developed between the yard and the town. A few years later the CB&Q continued their line via another tunnel under Mount Harriman taking their line in a south-east direction.

The loco facilities were removed with the end of steam in the late 1950s, with the industrial area expanding to cover most of the former railway land. Facilities were kept to stable a local road switcher and snowplough at the west end of the depot. The siding at the east end was used by the Railway Express Agency until they ceased trading and is kept currently as the stabling point for a tourist train which runs in summer along part of the former SP route under an agreement with BNSF. Currently the town is served by Union Pacific and Burlington Northern Santa Fe for freight, both roads using the yard as a crew change point, and Amtrak a couple of times per week using UP metals. Local switching is handled by the local Harriman Short Line using an elderly road switcher which they also use as a backup to the vintage railcar used on excursions and to serve a couple of smaller mines in the area for the larger roads.



Above left
Using running powers on the BNSF, a Canadian Pacific freight headed by AC4400CW #8541 is coming down from the high bridge.

Above
UP SD60M #6288 on a grain train leaving Harriman and entering the single track section along the valley, with the town behind, looking along the 'original' main line.



Baseboard construction

The eight main 5' x 3' baseboards were originally constructed using lightweight beams, made up of ply webs with softwood spacers. All the boards have a scenic depth of 4'6" with 6" behind the backscenes.

For lightness we used 4mm ply as the basic material, with 120mm deep webs. Baseboard tops and trackbed were also constructed from 4mm ply. The backscenes are also 4mm ply, with plastic used to make the curves at each end.

The beams are very strong in the vertical plane (stiffer than 2" x 1" PAR), and the only problem was fixing the baseboard tops to the edge of 4mm ply. This was overcome by using a thin layer of expanding foam filler down the beams.

2" x 2" legs were then permanently attached to each board using locking hinges; however, we quickly found that they do not lock that well, so we also have to use cable ties to make sure that the legs stay up once we have folded them away. We also fitted adjustable feet to each board.

Left

A Canadian Pacific through freight leaving Custer Creek canyon and about to enter the yard at Harriman.





Above
The yard at Harriman, with the industrial area between the town and the railroad. Leased KCS SD50 #718 is about to depart.

Left
A Union Pacific reefer train entering Harriman yard with a BNSF freight on the High Line having just crossed the Custer Creek bridge.

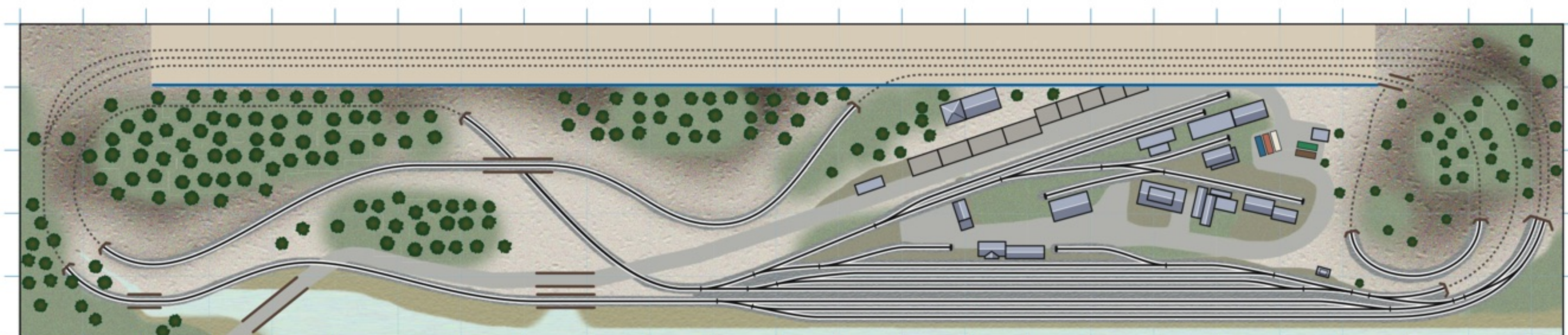
Alignment along the board edges was achieved using four pairs of brass dowels. The boards are connected together using large over-centre toggle clips, with smaller case clips on the top of each backscene.

The legs are braced to the boards using colour coded metal rods that were repurposed from some old exhibition barrier steadies.

On rebuilding to Mark 2, the new board for the helix was constructed using 9mm ply for the sides to which a top was fitted, the helix attached, and the top then removed from inside the helix.

Track and electrics

The track is mainly Peco code 55 fine scale with some code 80 in the fiddle yard. The track was fixed to the baseboards using Thixofix contact adhesive. Thixofix has a ten minute setting time which means that there is sufficient time to get the alignment right. Tracksettas were used to ensure the alignment. On straight sections the Tracksetta was pushed along the track, straightening it out. A similar technique was used around the curves. The track can be lifted if necessary by knocking it sideways, which shears the glue. Generally the points were not glued down.



Hills Country Overall layout size 24' 6" x 5'. Each grid square = 1' x 1'.

Board joints were dealt with by using “No More Nails” glue to stick down pieces of copper-clad stripboard to which the rails were soldered.

The electrical system is now fairly standard for a DCC layout though it was slightly unusual at the time for a DC layout in that each track on each board was individually fed via a Single Pole Double Throw Centre Off switch, selecting which controller supplied it. The control panel was linked to the boards using 25-way D-connectors. There was a separate common return wire from each board to the control panel. Power to the points was via a separate cable.

Conversion to DCC simply required the installation of the DCC power bus taking the supply to each board. The only connections to the panel were for the points.

The control panel was made from Perspex and has two sections, each dealing with four baseboards.

The points are powered by Peco and SEEP point motors via a stud and probe arrangement with a single Capacitor Discharge Unit driving the whole system, with the exception of the industrial area which is operated by a Lenz LS150.

Scenery

The scenery was constructed using expanded polystyrene sheets glued together with “No More Nails” as a central core. This was then shaped using a hot wire. The polystyrene core then had pre-coloured rock castings added using a mixture of finishing plaster and PVA. The plaster/PVA mix was also spread over any areas that were not covered by castings and held together using scrim. A wet paintbrush was used to shape the plaster and make sure it blended into the castings. When dry, the whole lot was painted with a standard poster paint mix for Colorado stone, and then a “zip texture” coat of dry plaster was sprinkled on top. Once this had dried, artists acrylics were used to weather the scenery, and finally a couple of coats of dilute Indian Ink was sprayed on along with a variable amount of Woodland Scenics green scatter as the Colorado mountains are quite green. The scree was made using O gauge ballast and was added as part of the plastering process.

The rivers were first painted using acrylics and then given a coat of gloss medium, but this went matt quite quickly and has been replaced by numerous coats of clear varnish.

The roads are made from cardboard strips that were stuck down and then painted as either tarmac or concrete.

The two girder bridges on the main line were scratchbuilt and are as close to scale as possible. The high level bridge with its concrete piers was built using a variety of kit parts.

The buildings come from any suitable source and run the gamut from secondhand plastic kits through American laser-cut kits to scratchbuilt. The church is a laser-cut kit. The buildings along Main Street are Heljan, Faller, DPM, or scratchbuilt. The diner on Main Street was cobbled together using an old passenger car and some bits from the junk box. The depot was scratchbuilt from plans for a SP No.22 depot from an old *Railroad Model Craftsman*. The feed and seed with attached lumber yard was built from several kits that were produced by LSWR Models many years ago. The large modern supermarket was an eBay purchase after I had identified the local regional brand.

The trees are either products from Graham Avis or manufactured from Set Scenes tree kits in bulk – the local Colorado Blue Spruce happens to look like a bottle brush tree! There are at least three hundred on the layout, mainly made by members of the group. My wife has since very kindly helped me to tidy up and refurbish many of the homemade trees, resulting in a better looking and more natural forest at the country end of the layout.

The advertising hoardings are simple plastic structures with posters culled from the internet.

Stock

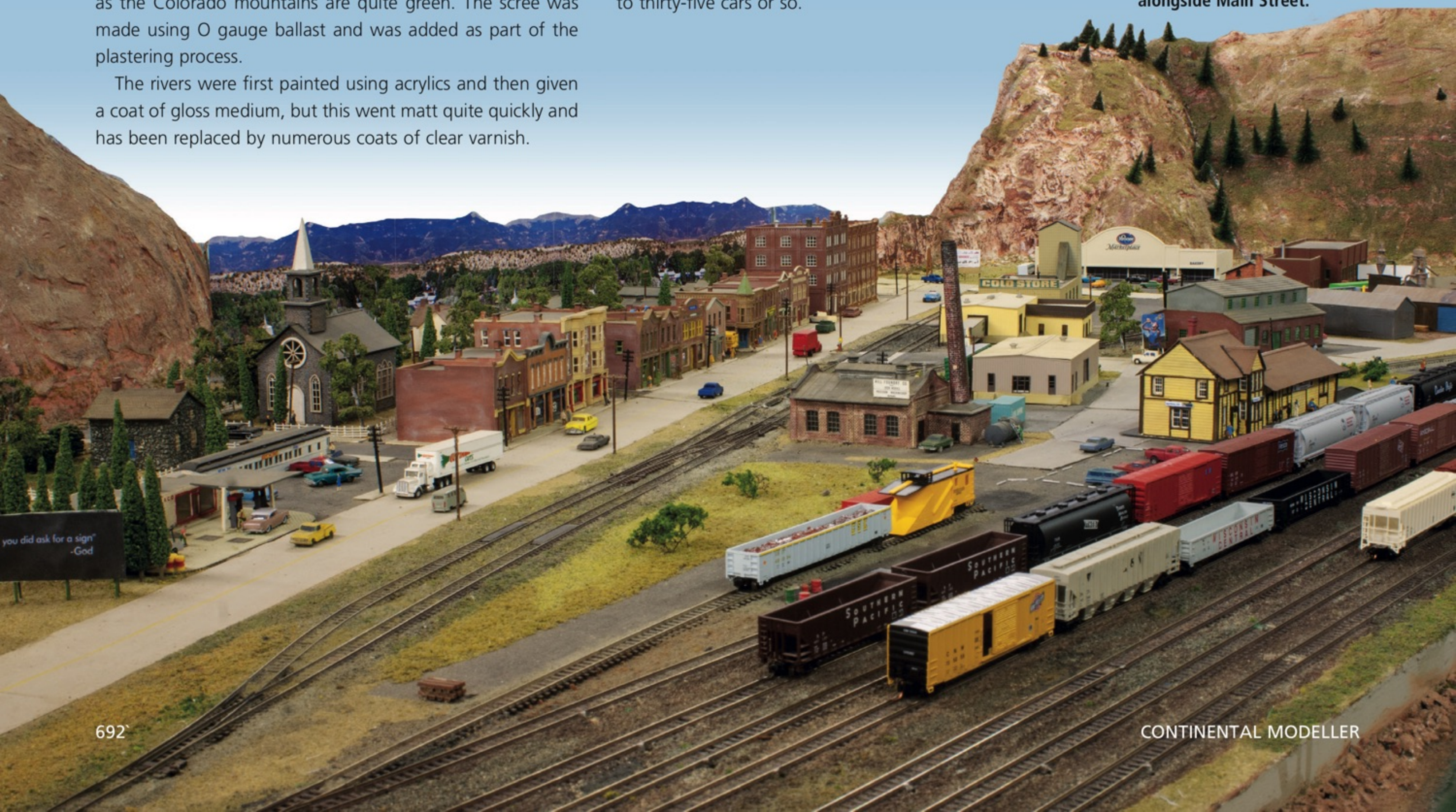
We discovered quite early on that American freight cars are not as free running as we anticipated and accordingly the high line is usually served by a couple of shorter (seven car) freights. The lower level is home to a number of longer trains, with length constrained by loco availability and siding length; a single loco is usually good for fourteen cars. The length of the passing loops in the town limit the coal trains to thirty-five cars or so.

Right

Two Union Pacific freights in the holding loops, headed by #5354 and #6288, awaiting clearance to depart with BNSF #3999 beyond. On the spur, a snowplough awaits winter duties.

Below

The town, industrial area, and railroad yard, looking towards Mount Harriman; the ‘original’ route is now the industrial trackage alongside Main Street.





Passenger services consist of a 'doodlebug' or RDC on the excursion train and a short Amtrak train on the lower level. Sometimes an F unit A-B-A consist with the eleven car *California Zephyr* replaces the Amtrak train – it is out of period, but it does look good! It is capable of going round the high line.

Double stacks and autoracks are confined to the low line due to clearance issues – just like the real thing!

Operation

The analogue layout was originally designed for at least two operators, one handling east-bound departures and one west-bound, but we found that it was more constructive to have one operator and a second person available to talk to the public.

The industrial area is equipped for using Micro-Trains couplers, but this rarely got used at exhibitions as it required an extra operator.

As converted to DCC, the layout ideally needs four operators, a dispatcher and two engineers (drivers), one for each line, along with a further operator at the front of the layout for the industrial area and chatting to the public. As a minimum two operators are needed, one to run a very reduced service (one train on the high line and two on the low line) and one to chat to the public.

The seven track yard at the front stretches over several boards and each road is at least 8' long; all are connected to the low line but only four to the high line. There are further loops on the back of the layout providing two 9' roads on the lower level and a 6' road on the high line.

The high line is effectively single track apart from the short passing loops so has a maximum capacity of two trains.

The low line is double track for three quarters of its length and accordingly can handle more trains. At scale speeds it takes at least two minutes to do a complete circuit.

All change

I acquired the layout from the club in 2018 and moved to pastures new in Scotland in 2019. This means that there will be a slight change in the mixture of stock; in particular, the *Zephyr* is not going to feature as often as it did.

OCTOBER 2019

Right
BNSF GE ET44C4 #3999
just leaving a tunnel
on its way down the spiral,
with the double track main
line tunnel mouth below.



Below
CP #8541 arrives in the yard
as another freight prepares
to depart on the low level
main line. The depot is now
a museum, hosting excursion
trains at weekends.



Acknowledgements

I must thank members of the Colchester MRC for their help and assistance, not only in moving the layout at times but also for operating when needed. Thanks also to Katherine Evans, Kathryn Child, and Fiona Reynolds who were more than willing to help operate at many exhibitions. I would also like to thank members of the NMRA-BR who have helped by operating over the years, my wife Katherine for helping to prune some three hundred trees, and members of my new club in Aberdeen for help with the Mark 2B changes.

Doug Tompkins describes his new compact exhibition layout.

Dügelskirchen

A small diesel depot

Following on from *Bw.Dügelshausen* (CM March 2016), it was time to build something new. I had discovered with *Dügelshausen* that despite its small size it was still possible to have a working layout in a space of just 4' x 2', and using just one baseboard, making it easily transportable in the back of my VW Polo.

I am always looking at ideas and plans for small layouts, particularly locomotive depots, and the track plan for my new layout was largely inspired by an article by George Dent.

Construction commenced in September 2018 with the ordering of a plywood top baseboard from Model Scenery Supplies of Northrepps in Norfolk. I am not good at carpentry

Below

B-B diesels 216 118-0 and 216 134-7 have taken over the old steam shed, while 0-6-0D 260 593-9 collects a van which has delivered spares to the new shed.



and so having something ready-made to start laying track on is a bonus.

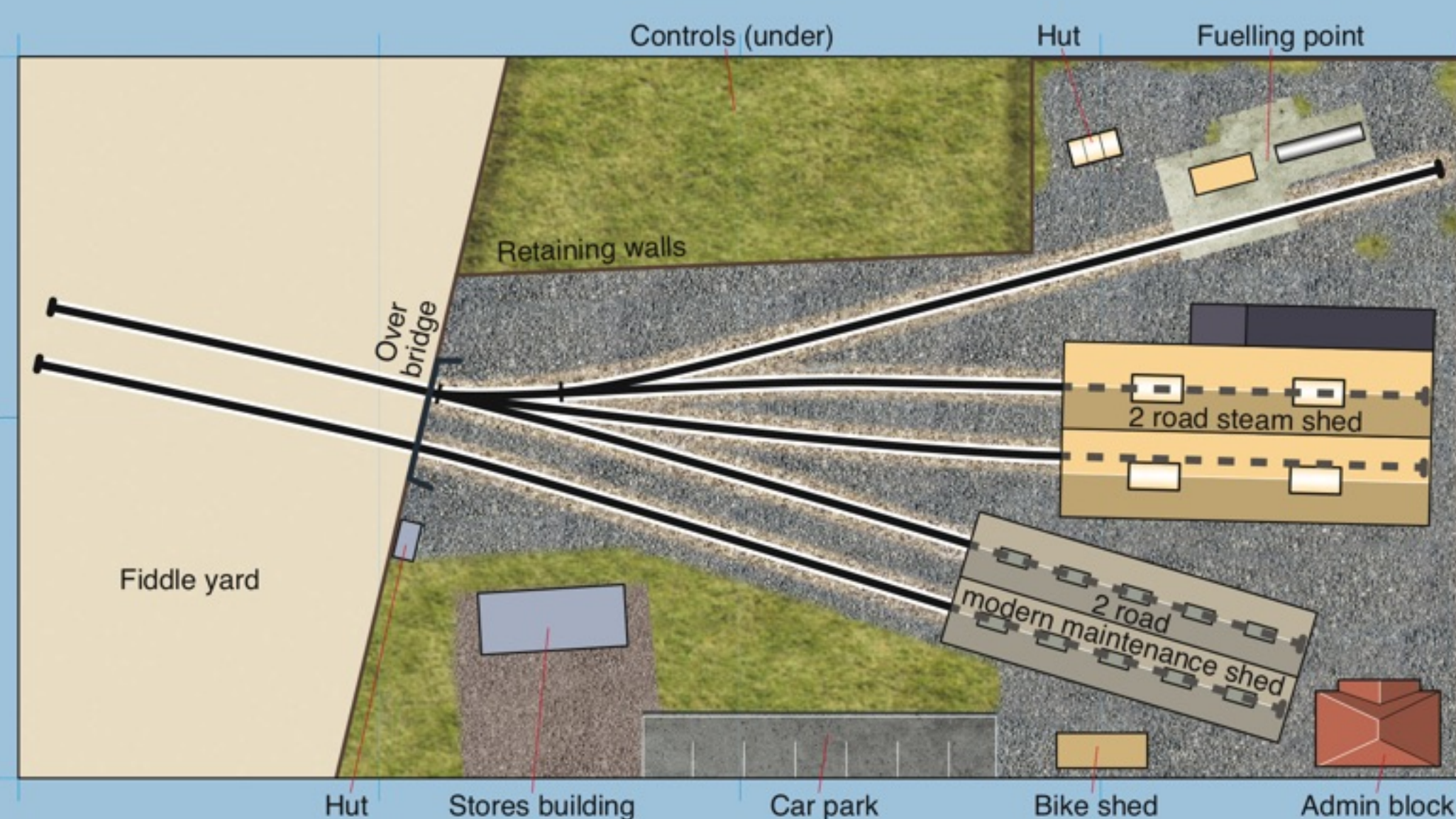
I first painted the top matt black as I do not like working on a plain wood surface. Once the paint had dried I applied a coat of white PVA glue and then sprinkled on a ground cover of black and grey scatter material and small amounts of imitation grass from Woodland Scenics and Heki before laying the track, all Peco code 100. This was stuck down using PVA contact adhesive as I find this preferable and easier than pinning into plywood.

Once the track had been laid it was wired up and tested, then the sides of the rails were painted with Revell matt 84 dark brown enamel. Then the laborious but ultimately worthwhile job of ballasting began. I prefer not to use any ballasting device and, being a locomotive depot, it was not necessary to have a ballast shoulder as on main running lines.

The ballast comes from Woodland Scenics and was laid dry around the sleepers with a small spoon and then positioned carefully into place using a small paint brush, taking care not to block any moving parts on the points. Once this was done the whole area was misted with water and then a 50/50 mixture of white PVA glue and water with a few drops of washing-up liquid was carefully dropped on to the track using an eye dropper. Once this had all dried and hardened, the excess ballast was vacuumed away and the tops of the rails thoroughly cleaned before a second test of the electrics.

The plan was altered slightly from the original in that I chose to use a three-way point at the depot entrance plus one Y-point, but the idea of having two separate locomotive servicing sheds was retained. All the points are operated manually using Peco surface mounted point motor bases onto which cut-down map pins have been inserted to pro-





Dügelskirchen Overall layout size 4' x 2'. Each grid square = 1' x 1'.



vide a knob to throw the tie bar. The workings of the three-way point are hidden from view in the fiddle yard and that for the Y-point behind a retaining wall. Point rodding to the Y-point was covered over by a raised platform using Wills embossed cobble stone plastic sheets and Plastruct fencing and includes an authentic German point lantern and lever from Auhagen.

Despite all the points being insulfrog (I am not brilliant with electrics either!), I have not encountered any problems with slow running of locomotives; even my short wheelbase Piko Köf I four-wheel diesel shunter will run through smoothly without stopping.

Above right

221 147-2 (left) leaves while 211 070-8 arrives to be refuelled.

Below

The stores building, car park, bicycle shed, two separate loco sheds, and the administration building (*Lokleitung*). Note the DB 'Keks' emblem by the door, plant pots, and wheelie bin.

Structures and details

There are two separate locomotive sheds on the layout, an old steam era *Lokschuppen* from Vollmer plus a more modern affair from Jouef. Both have two roads and inside these sheds flooring has been installed using plastic sheets, painted concrete colour. A certain amount of repainting and weathering was applied to both of these structures to give them a more realistic appearance.

The *Lokleitung* (administration) building is from Kibri and has received some weathering and a DB 'Keks' (biscuit) emblem by the entrance door. The flower pots outside this building come from Noch (ref.14011).



The tall yard lights and the two wall mounted lights by the fuelling point are from the excellent Viessmann range, whilst those on the walls of the locomotive sheds and on the administration building are from Auhagen.

The fuelling point is from Faller (ref.120196). This is sold as appropriate for Epochs V and VI and so was adapted as all my layouts are set in Epoch IV. The nearby corrugated shed is from Noch.

Figures inside the buildings and around the layout come from Preiser and Noch.

The car park was created using a Faller self-adhesive mat with pre-printed white marker lines. Cars come from Brekina, Herpa, Wiking, and Schuco. All have been carefully chosen to suit a West German Epoch IV layout and have been stuck down using superglue. All are fitted with German number plates, some being water-slide transfers from Truckline.

The bicycle shed comes from Faller (ref.180 584), whilst the brick-built flat roof building nearby is from Peco. Waste bins come from both Faller and Bachmann.

The retaining walls are hard foam and come from the Noch range. These are very realistic in appearance, very light in weight, and are easy to work with. The depot entrance has an overbridge using Noch hard foam stone walling and Wills Vari-Girder girder bridge sides (ref.SS57).



The clump foliage around this bridge and hanging down from the retaining walls is from Woodland Scenics. Peco self-adhesive summer grass tufts (ref.PSG 50) have also been applied in places to replicate weeds. The grey hut backing onto the retaining wall is from Peco.

The fiddle yard has a plywood surround (for which I have to thank my brother-in-law for his carpentry skills) and the scenic background was created using Peco sky paper onto which photocopies of catalogue pictures of Faller high rise flats were stuck in place – simple but effective.

Above
0-6-0D 260 593-9 propels a tools van into the depot while the stores building receives a visit from the window cleaner. Unusually, there are no Volkswagen Mark 1 Golfs in the car park.





Control

Control is analogue DC, using a Gaugemaster Combi controller. The siding into road No.4 in the modern shed can be switched on or off as required.

Although I have operated digital layouts belonging to friends and have particularly been impressed with sound-equipped models, analogue control is a technology I am happy with and can easily understand.

There are no working signals as all locomotive movements are controlled by the yellow *Sprechsäule* (literally 'speaking columns') placed at each exit road. These are from the Noch range (ref.13621) and I feel that these are now the best HO models of *Sprechsäule* on the market. Also from Noch (ref.13623) is a stand for locomotive steam heating pipes – a very realistic feature, which has been placed just in front of the old steam shed.

Above

218 499-2 has just arrived for refuelling.

Above right

Köf 323 187-5 arrives on road 2. Note the rack of steam heating pipes between roads 1 and 2, and the *Sprechsäule*.

Below

220 025-1 runs into road 4 as a couple of engineers are assessing the damage to a lattice post yard lamp.

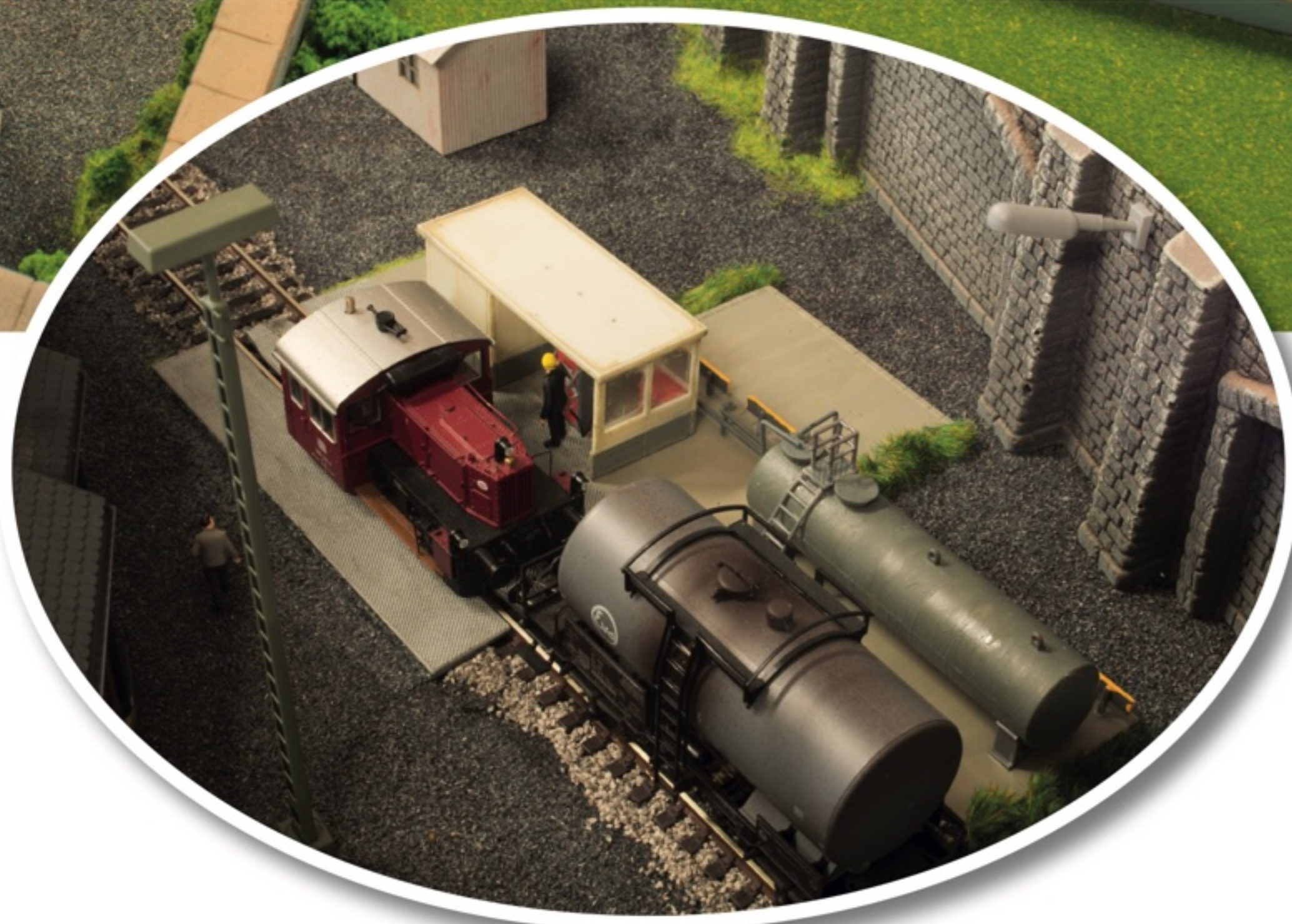


Setting and stock

The town of Dügelskirchen is purely fictitious and is set somewhere on the former Deutsche Bundesbahn (DB) in West Germany between 1978 and 1985. I have previously located my layouts in Schleswig-Holstein in the north of Germany as this is the area I first visited in 1979 to photograph and travel behind the DB class 220 and 221 diesel-hydraulics. However, as I now have quite a collection of DB diesels that were more commonly seen in the south of the country, such as classes 210, 215, and 280, with this layout I have been less area specific.

I have a good selection of DB and Deutsche Reichsbahn diesel locomotives to use on my layouts, mainly from Piko, Roco, Fleischmann, Brawa, Trix, Liliput, and Gützold. At two day shows I tend to choose DB locomotives in the older crimson red livery on the first day, and sometimes a visiting steam





locomotive might also appear! On the second day the later ocean blue & beige colour scheme is dominant.

For delivery to the depot of fuel supplies and engineering spares, etc., all the shunting locomotives have retained their couplings. However, all the main line types have had these removed to allow for the buffer beams to be detailed.

A number of my DR locos have been quite heavily weathered. On my visits to Germany in the early 1980s where I saw them on cross-border services I can never remember seeing a clean example.

Conclusion

Bw.Dügelskirchen does have a rather cramped appearance. Would I have done anything differently? Well, perhaps. The viewing options from the front are certainly more limited than on my previous layouts and in retrospect it may have been better to have positioned the two loco sheds at a different angle with the fuelling point at the front of the layout. However, the layout is small enough to allow viewers to easily observe movements from above at both the fiddle yard and depot buildings end, and overall I am very happy with it – it shows what can be achieved in a small space. It was never rushed and it took just over six months to finish. I am always heartened when appearing at exhibitions to hear that people did not realise that something could be built in such a small space on just one baseboard. It might not be everyone's cup of tea but there is no excuse for saying you do not have space for a layout.

Above
Köf 323 187-5 arrives at the fuelling point with a tank wagon.

Top
290 110-6 is being fuelled while 221 147-2 arrives and runs into shed road 4.

Photographs by the editor.

Finally, I would like to thank my brother-in-law Peter for his help with the supply and fitting of the wood surround to the fiddle yard, my friends and fellow German Railway Society members for their suggestions, and to Paul Tucker and Rob Stevens for their help with transport to and operation at exhibitions.

Forthcoming appearances

Saturday 21st September – Culm Valley, Willand.
Saturday 5th October – South Devon MRS open day, Abbotskerswell, near Newton Abbot.

Davide Raseni and **Paolo Visintini** present a diorama built by **Daniele Arbore** which commemorates a small station on the border between Italy and former Yugoslavia.



Draga/Sant'Elia

Above

An Italian Gr.875 2-6-0T shunting in Sant'Elia. Some policemen are watching. Near the door are the Italian flag and signs in Italian and Slovenian 'Valico Ferroviario N°.8' (railway border), but the name of the station is only in Italian.

Photo: Davide Raseni.

The Herpeljebahn through Val Rosandra – a victim of the Cold War

From Stettin in the Baltic to Trieste in the Adriatic, an iron curtain has descended across the continent. In a speech on March 5th 1946, Sir Winston Churchill described the beginning of a difficult era that affected the lives of millions of people around the world, particularly in Europe, for the following forty-three years.

At that time the city of Trieste, at the north end of the Adriatic, saw the umpteenth change of administration since the end of the First World War: it was the main Austro-Hungarian harbour until 1918, under Italian military administration until 1919 (Treaty of St.Germain), and then Italian until 1943. After the Italian surrender it became for a short time part of the German Third Reich, until 1st May 1945; then there was forty days of Yugoslav military administration followed by the Allied Military Government (covering Trieste – Gorizia – Pola). From February 1947 until October 1954 under the Memorandum of London the creation of the T.L.T. (Territorio Libero di Trieste – Free Territory of Trieste) divided the territory around Trieste into an A zone (Anglo – American administration) and a B zone (Yugoslav administration). After 1954, Zone A reverted to Italian administration and the B zone passed to Yugoslavia. Not until 1975, with the bilateral treaty of Osimo between Italy and Yugoslavia, was the Italian administration of Zone A definitively confirmed – although without UN agreement!





After 1954 three railways connected Trieste to Yugoslavia: the double track line to Ljubljana (the former Südbahn, through to Vienna), the single track line to Jesenice (the former Wocheinerbahn), and the single track line to Herpelje/Pula (connection to/from the Istrianerbahn). This once important international line is almost unknown today. It climbed from Trieste through the wild and spectacular Rosandra valley (locally it was known as the Val Rosandra line) and in only about 20km reached the northern part of the Istrian peninsula.

A short history of the Herpeljebahn

The construction of the Austrian Herpeljebahn, as it was titled in the design and technical documents of the time, was in part the consequence of the decline of the port of Trieste, particularly apparent in the 1870s, as a consequence of competition from the Mediterranean and northern European ports, but even more due to the monopoly of the company managing the only railway connection to the port: the private Südbahn. The problem of providing a second railway connection to the port of Trieste, under state control, to break the private monopoly, remained topical for several decades, only to be solved as late as 1906 with the completion of the Wochein railway – in Italian known as the Transalpina – which brought Trieste back to a prominent position among the outstanding European ports until the outbreak of the First World War. Nearly half a century passed after the opening of the Südbahn (1857) before Trieste got an alternative rail connection under state control (k.k.St.B).

Between these two milestones in the history of Trieste, the Herpeljebahn was built. Work started in October 1885 and the line was inaugurated on 5th July 1887, preceded by a long controversy about the choice of the new route. The long-expected second rail connection would prove to be a patched-up solution.

The result was a very steep line just 20km long which, climbing up the Rosandra valley and overcoming the Carso gradient, reached the existing station of Herpelje, so making a connection with the existing Istrian Railway (Divaca – Pola), state-controlled since 1876. For some years the line was one of the steepest in Europe, with a maximum gradient of 33/1000.

Above

During the last years of the line Fiat ALn56 diesel railcars were often used for passenger services. A very shiny example has just arrived from Trieste. Photo: Davide Raseni.





Left
A local train has just arrived from Trieste hauled by a Gr.728 2-8-0, a former Austro-Hungarian Rh270.
Photo: Paolo Visintini.

Below
The Gr.728 clearly shows its Austro-Hungarian origins.

The line effectively became a short cut for goods from the port of Trieste to Divaca, as it avoided the only previous route, via Grignano, Aurisina/Nabrežina – Opicina/Opcine and Sežana, which was 38km longer and more expensive because of the charges levied by the Südbahn. For nineteen years the Herpeljebahn played an important role, carrying an enormous quantity of goods.

However, after 1906, when the new state-controlled Wochein railway opened, the Herpeljebahn suffered a heavy setback: it lost its function as a bypass and was downgraded to a mere regional connection between Trieste and Pola. This was maintained after 1918, when it was taken over by the Italian State Railways. But it was the political change resulting from the Second World War that dealt the final blow to the Val Rosandra railway.



Left
728.012 runs round, preparing to return to Trieste tender first as there is no turntable.
Photo: Paolo Visintini.



A lot of borders in a few metres

In early May 1945 the Yugoslav army entered Trieste, and for forty days there was a tense situation with the Allied forces.

We may imagine how perilous it was to travel by train from Trieste Campo Marzio to Herpelje between 1945 and 1947. About one kilometre from Draga/Sant'Elia, near the ruins of the Fünfenberg castle, the train skims the border, just one metre from the track. Near here in 1948 some FS trains were shot at by Yugoslav soldiers, and the service was limited for six months to the station of Boršt/Sant'Antonio; then the train passed the village of Draga/Sant'Elia and stopped at the station, at a higher level beyond the village. Here the Polizia Civile (trained and equipped by the British) effected the first check of documents and luggage. After leaving the station in the direction of Yugoslavia, trains stopped a few metres beyond the last point (km15+209). Here a patrol of Yugoslav soldiers carried out a second check.

The border between the military zones was so close that the distant and home signals on the Yugoslav end of the station were on the Yugoslav side of the border! For this reason maintenance was too dangerous for FS personnel and these signals were set permanently at danger.

The train restarted in the direction of Herpelje but after about only a kilometre (km16+485) it came to a second border, between the military zone and Yugoslavia. A third check

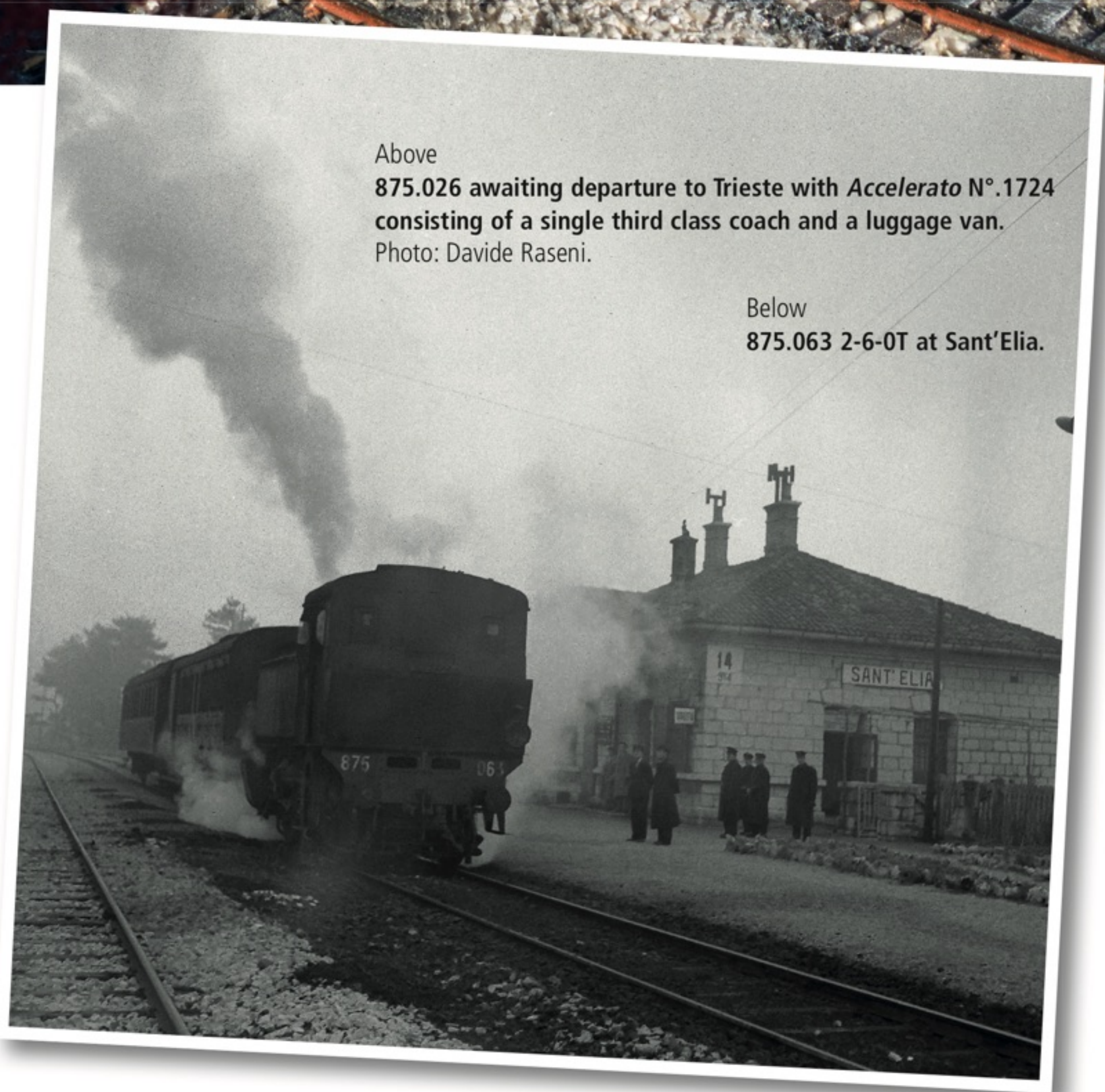
Above

875.026 awaiting departure to Trieste with *Accelerato* N°.1724 consisting of a single third class coach and a luggage van.

Photo: Davide Raseni.

Below

875.063 2-6-0T at Sant'Elia.

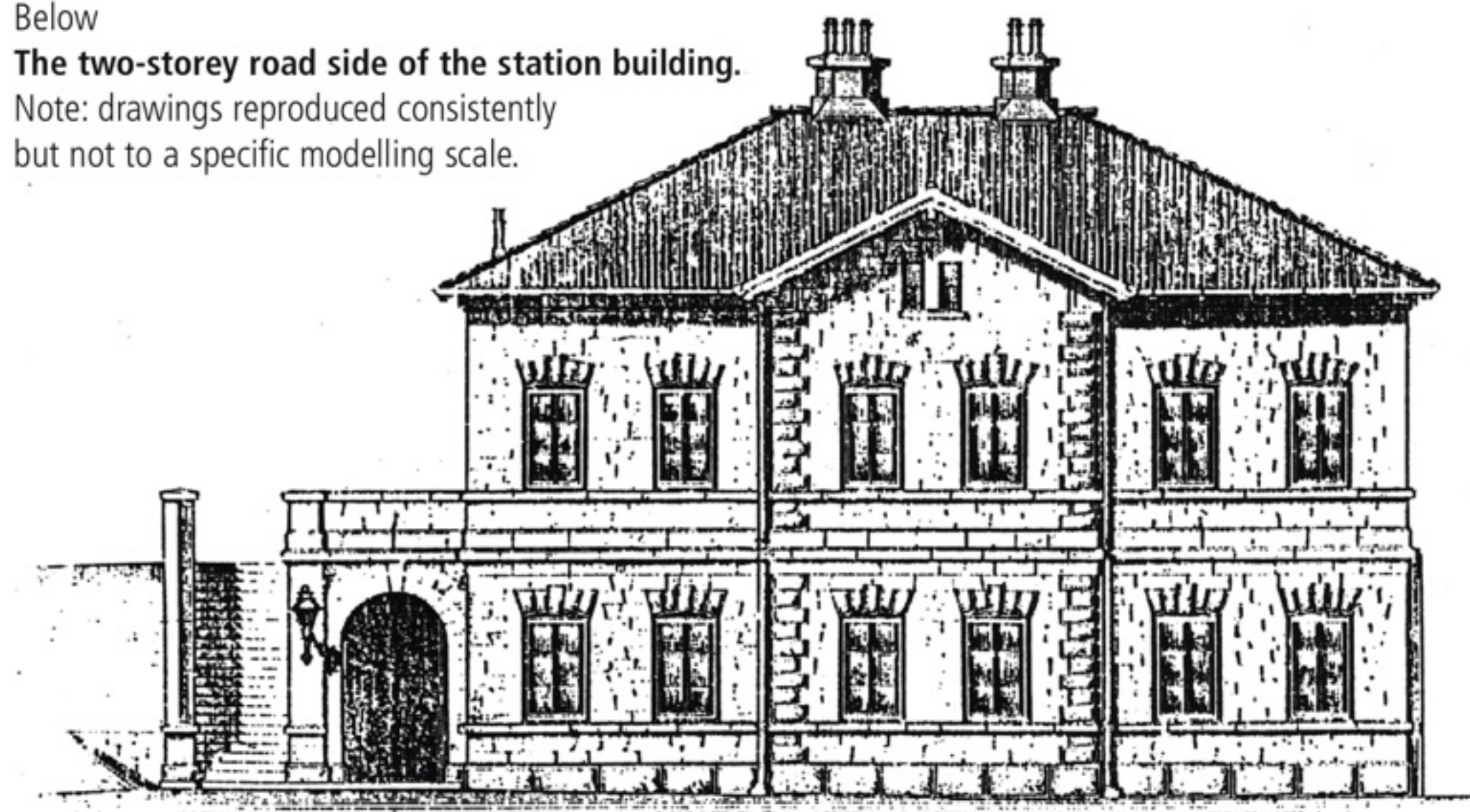




Below

The two-storey road side of the station building.

Note: drawings reproduced consistently but not to a specific modelling scale.



was needed, and often a military escort climbed up onto the locomotive until Herpelje, where the FS train stopped. Here the passengers bound for or coming from Pula/Pola in south of Istria on a Yugoslav passenger train were controlled again! Often this check was not very pleasant ...

The FS steam loco was not permitted to use the turntable so it had to run round the coaches in a very short time in order to return to Trieste, running in reverse.

The charming railway station of Draga/Sant'Elia (Draga in Slovenian, Sant'Elia in Italian, though bilingual signs were never used) was an unusual structure built in Istrian stone with two storeys on the road side and one facing the tracks. At the end of the Second World War it became the last station in the T.L.T. and with the return to Italian administration one of the four FS border stations with Yugoslavia. The main part of the trains ended there and only a single coach proceeded to Yugoslavia.

There is no evidence of a regular freight service; often a tank wagon transporting drinking water was added to passenger trains in order to supply stations and crossing keepers' houses along the line in the hot summers. The tank wagon remained in Draga/Sant'Elia until it was empty.

The station had only two tracks and a siding. There was no turntable so steam-hauled passenger trains had to return to Trieste tender first.

In the last years of FS services, class ALn56 diesel railcars were used.

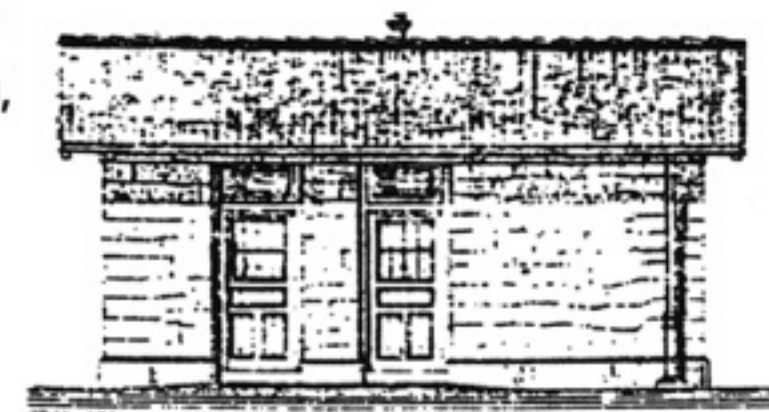
Left

The two-storey road side of the elegant station building, built in Istrian white stone.

Photo: Paolo Visintini.

Right

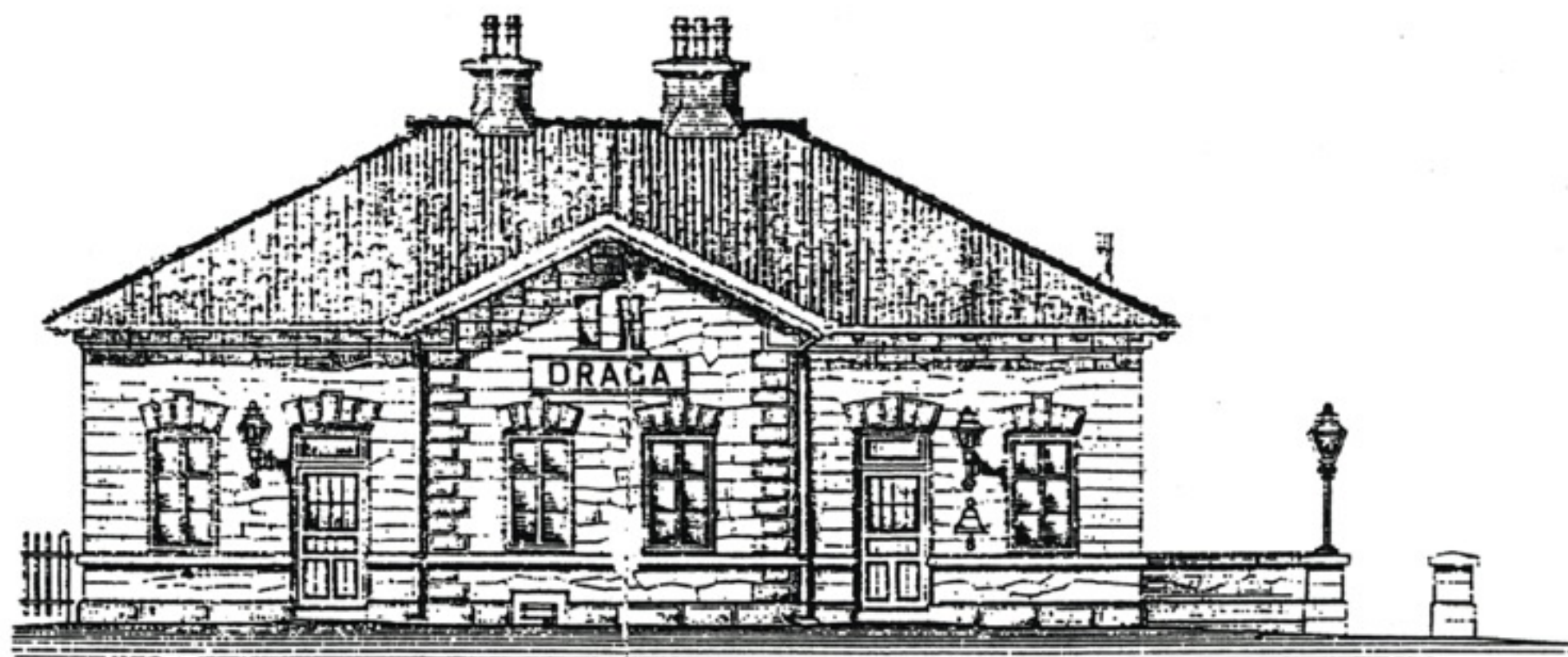
The separate toilet block.



Left
A Gr.875 2-6-0T in Sant'Elia,
by the flight of steps
which descend to the road.
Photo: Davide Raseni.



Right
Front view of a Fiat ALn56
railcar on a sunny morning.
The large Italian flag
indicates that the scene
is after 1954, when Trieste
was returned to Italy.
Photo: Davide Raseni.



The end

On 31st December 1958 a lone ALn56 forming the evening train N°.1730 to Trieste left the station of Draga/Sant'Elia for the last time. Although Yugoslavia had abandoned the eastern bloc ten years before (with the break between Tito and Stalin) and became non-aligned, political tensions between Italy and Yugoslavia remained due to what had happened in this region after 1918, exacerbated by the tragic events of the Second World War. A line with poor traffic in such a delicate border zone was not interesting for either FS or JŽ. The Yugoslav railways soon dismantled the track in their short section, while in Italy they remained in place until 1966.

Above
The single storey rail side
of the station building.

Drawings and historic photos
from the authors' collection.



Right
The single storey rail side
of the station building,
which was no doubt ample
for the level of traffic.
Photo: Davide Raseni.



Today all is gone: not only the railway, but Yugoslavia is no more and both Italy and Slovenia and Italy are in the Schengen area, at last with no borders and a climate of friendship and co-operation. What happened in the past is difficult to explain to any young Italian or Slovenian riding a mountain bike along the route of the old railway surrounded by the natural beauty of the Rosandra valley. It is a great shame that line did not survive as it could have been an extraordinary tourist railway line between the two countries.

The diorama

Daniele Arbore built this little diorama after a lot of trips to the real station to take measurements and photos of the buildings and the original track area. The dimensions are just 117cm x 38cm. The structure is a box of 10mm plywood and the sides follow the shape of the terrain. The scene only shows the central part of the station with the interesting building and part of the through tracks. The siding is not modelled, as it is too distant.

Above

875.018 has just coupled to the short train.
Some farm women have left the train after a day selling milk at the market in Trieste, but they still have a long way to go on a steep path before they get home.

Photo: Davide Raseni.





The track is Peco code 75, laid diagonally across the board.

The building was made from scratch with the aid of some original Austro-Hungarian drawings, using 3mm plywood covered by modelling clay which was lightly scribed to simulate the stonework on the façades.

As at the real location, the station is some way from the village, so no other buildings are represented on the diorama, except for a concrete hut and a small stone-built toilet block near the main building.

The vegetation is a very important aspect of the real location, with a large number of trees and bushes. The black pines were made using MiniNatur 'forest pine' foliage glued onto a wire 'skeleton'. The many bushes and small trees were made with sea foam (teloxys aristata) painted red and yellow to give the right look for autumn.

The models used in the photos are a combination of commercial and handcrafted products. There is a Gr.875 2-6-0T (Roco), a Gr.728 2-8-0 (formerly k.k.St.B. Rh270, heavily modified from a Klein Modellbahn model), and an ALn56 railcar (by HF). The coaches are a mixture of Roco, Laser, and A.C.M.E. models. The four-wheeled tank wagon is by Sagi.

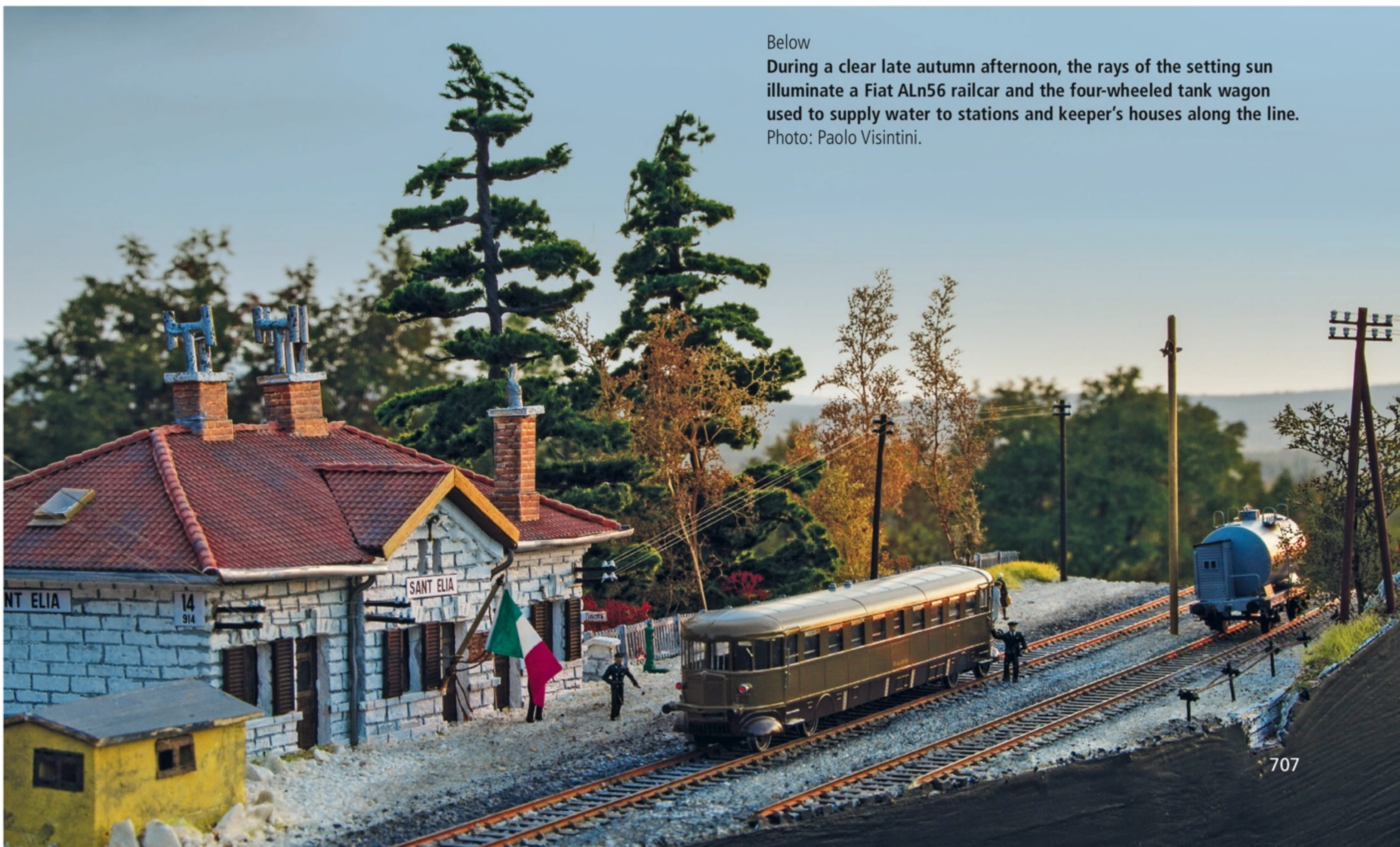
This diorama is a personal tribute to this much missed scenic railway and its timeless atmosphere.



Above

FS railway staff pose near the Fiat ALn56 in pristine condition which is ready to depart to Trieste.

Photo: Davide Raseni.



Below

During a clear late autumn afternoon, the rays of the setting sun illuminate a Fiat ALn56 railcar and the four-wheeled tank wagon used to supply water to stations and keeper's houses along the line.

Photo: Paolo Visintini.

Chris Hales describes his unusually themed layout.

Photographs by Greg Pye.

Sóller

Inspired by a holiday in Mallorca

The Ferrocarril de Sóller was opened in 1912 linking the town of Sóller to the capital, Palma. Built to 3' gauge, the line winds its way up from Sóller, going through ten tunnels and crossing the impressive Cinq Ponts viaduct before finally passing through the 2.8km long Tunel Major beneath the Sierra de Alfàbia mountains. From there it continues its comparatively uninteresting journey to Palma.

I was lucky enough to holiday in Sóller a few years ago and fell in love with the line and was struck by how much of a gift to the modeller it would be.

Right

A steam-hauled tourist train weaves up the hill over the Cinq Ponts viaduct.

Below

A diesel hauled service has arrived at Sóller station.

The steam special in the upper loop must wait there to pass the other special ready to leave the station.





Above
A track maintenance train
stabled in the sidings at Sóller.
Just visible through the trees above,
a steam special waits in the upper loop
to pass an uphill train bound for Palma.

Having recently moved house and inherited several sheets of fibre-board from the previous owners, everything seemed to be in place to return to railway modelling after a thirty year hiatus.

I had always been enchanted by the smaller scales, having dabbled in N as a youngster, and this seemed the perfect opportunity to realise a long-held ambition to build a Z scale layout.

What I really wanted to do was to take advantage of the small scale to give a sense of spaciousness and present an uncluttered layout. Operationally, Sóller may not have much to offer, but I hope it does evoke the charm and atmosphere of the real place.

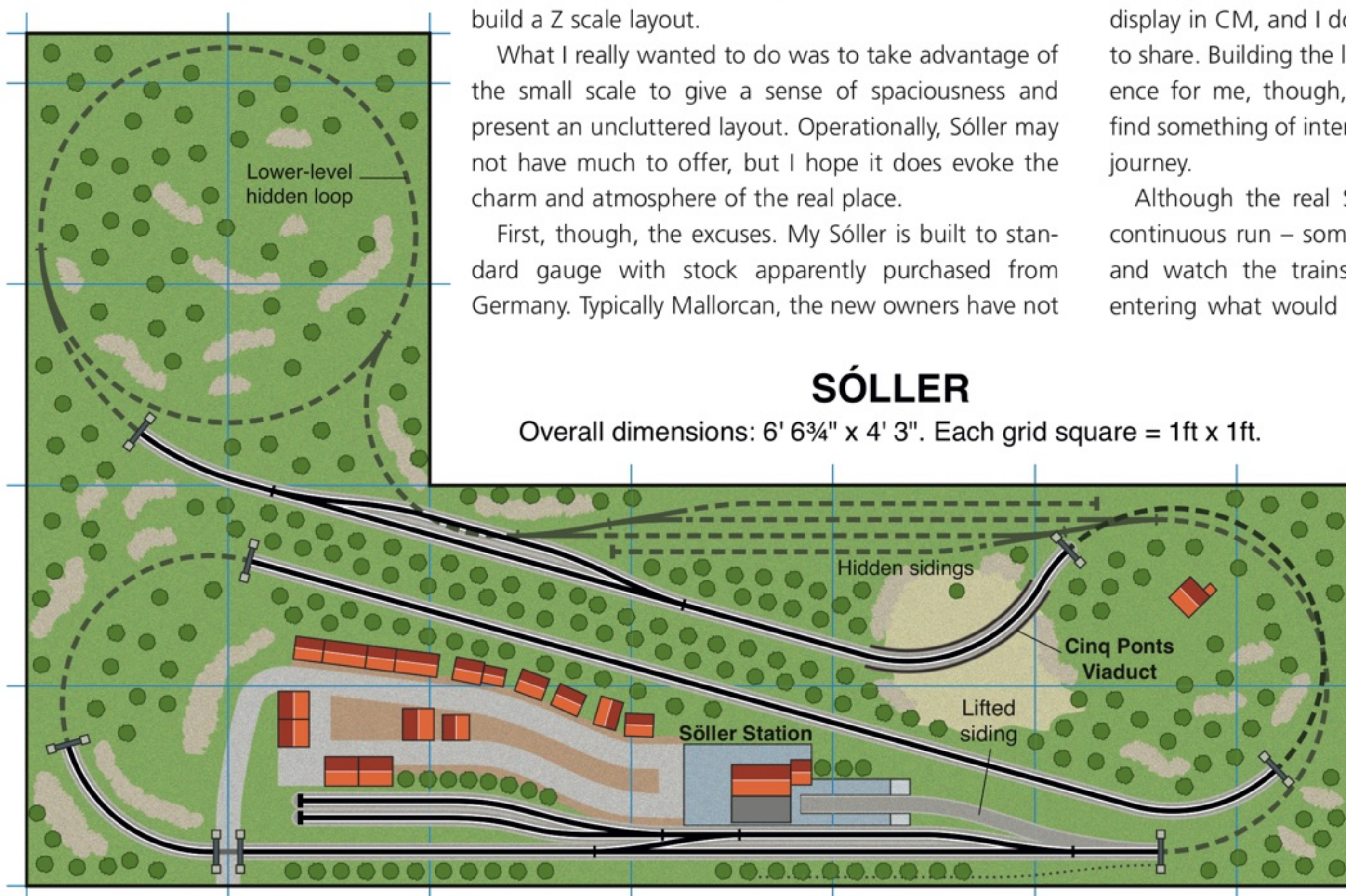
First, though, the excuses. My Sóller is built to standard gauge with stock apparently purchased from Germany. Typically Mallorcan, the new owners have not

even repainted it yet; I expect it will be done *mañana*. And whilst the prototype was electrified from the outset with overhead catenary, I felt that would be too much to ask of my clumsy fingers so the layout has had to do without. As a final conceit, my line operates steam-hauled services for the many tourist visitors.

I am often humbled by the quality of modelling on display in CM, and I do not think I have much expertise to share. Building the layout has been a learning experience for me, though, and perhaps other readers will find something of interest, or at least amusement, in my journey.

Although the real Sóller is a terminus, I wanted a continuous run – sometimes you just want to sit back and watch the trains go past – so I have the line entering what would be the north-east of the station

through a tunnel. The station has a couple of sidings and a passing loop, although I have yet to devise a satisfactory method of uncoupling to allow locomotives to be uncoupled and run round their train. From the station the single track line winds its way upwards, splitting into another passing loop before entering the tunnel that takes the line down a helix to the four track fiddle yard.





I spent quite a bit of time planning the layout, getting the gradients and track layout right, and I am glad I did. I had originally planned to have much steeper gradients, but some experiments soon showed these would be impractical, so I ended up with a maximum 3%. Looking at the completed model, I think anything more would have looked too steep, so that all worked out for the best.

A thirty-year-old Peco catalogue I still have recommended baseboard construction of Sundeala on a wooden frame. I do not know if the pin-boards I took down from my son's bedroom were Sundeala, but what I have learned is – make sure your baseboard is rigid! Mine was not and I only discovered this after I had laid most of the track and then noticed that everything had bowed. I think I have managed to correct this by adding a much more substantial timber frame, but I am still not confident in its stability with temperature and humidity, and given the time again I would definitely do it differently.

Being new to Z, if not entirely to the hobby, I elected to use Märklin track. Inevitably this turned out to be a mixed blessing. I committed not to use anything less than their largest 220mm radius curves, but even these are far too tight for my liking. I really like Märklin turnouts, though, as they can be 'run through' against the point, which is really helpful in passing loops. On the other hand, nothing much can dis-

guise the enormous switch mechanisms. (In fairness, I have seen a couple of layouts which camouflage them admirably, so that is maybe something for future improvement.)

With the exception of the helix, where the track is glued down with impact adhesive, the track is simply pinned in place. This was mainly due to my lack of confidence that I would not want to rip it all up! Having said that, the Märklin track pins are very unobtrusive and I think the appearance is not bad at all.

I still have nightmares about ballasting from my youth, and so far I have found several plausible excuses not to bother on this layout. I did have the foresight to paint the trackbed grey – and I think this has made an enormous improvement – but

Above
The 2-6-2T and its train have been stabled in the yard

Below left
Special and service trains pass in the station.

Below
The overbridge allows easy road access to the town.



there is no doubt the track would look much better ballasted. For now, though, I am not ready to risk rendering all of my pointwork inoperable or to endure the countless evenings of tedium ballasting!

A book I had in my youth (*The Project Club – Build a Model Railway*) advocated using Mod-Roc to build terrain. That was something well beyond my budget at the time, but the thought stuck with me and was the technique I used on Söller. Typically, the Mod-Roc was applied to a supporting structure of aluminium gauze, though in some places I went a bit mad with expanding foam filler by way of support. Whilst I am generally happy with the results, I was totally unprepared for the amount of mess it would make and how much plaster would get onto the trackwork, despite diligent application of masking tape. In retrospect, I should have laid the track afterwards, though I expect this would have resulted in other problems.

I found the instructional videos by Woodland Scenics very helpful and followed them as closely as my patience allowed for the colouring and texturing of the terrain and for the rock outcrops. The latter were cast in Woodland Scenics silicone moulds and coloured as per their instructions. A couple of areas needed larger and more bespoke rock outcrops and for these I developed my own technique, of which I am inordinately proud. Moulds were made out of crinkled kitchen foil misted with diluted detergent and filled with mixed casting plaster. The plaster was allowed to nearly set before placing on the layout, allowing it to set fully, and then carefully removing the foil mould. I coloured these again following the suggestions of the Woodland Scenics videos.

The rock outcrops and formations are just about the only things on the layout not to be dry-brushed, that being a technique I learned about some time later.

I dotted some pre-made grass tufts here and there and some of the areas nearer the front of the layout have been given the static grass treatment. I think it is fair to say that is another technique I have yet to master.

Looking up from the town of Söller to the surrounding mountains, and indeed walking there, one is struck by how many trees there are. Lots of olives, but also citrus and locust bean. Being a cheapskate, I bought some inexpensive small model trees and spent countless evenings applying more realistic leaf cover. It is somewhat frustrating that I got much better at this with time, and you can easily see the improvement from one side of the layout to the other. Nevertheless, I am pretty happy with the general appearance, at least from a distance, which does seem to closely match that of the real thing.

The layout does not yet have a backscene but the way the mountains rise to the rear of layout makes this less of a necessity, especially when displayed at a reasonable height.

Cinq Ponts viaduct was the first structure to be built for the layout, constructed from 2mm styrene sheet and embossed plasticard. The arches were punched out with an inexpensive craft punch and the finished article given a wash of various neutral colours, this being another construction before dry-brushing. Needless to say, given my time again I would do it differently, but I am pretty satisfied with the result.

Tunnel mouths are Noch products treated with filler or dry-brushed to emphasise the pointing. I am often surprised that otherwise excellent layouts are spoilt by 'gaping hole' unlined

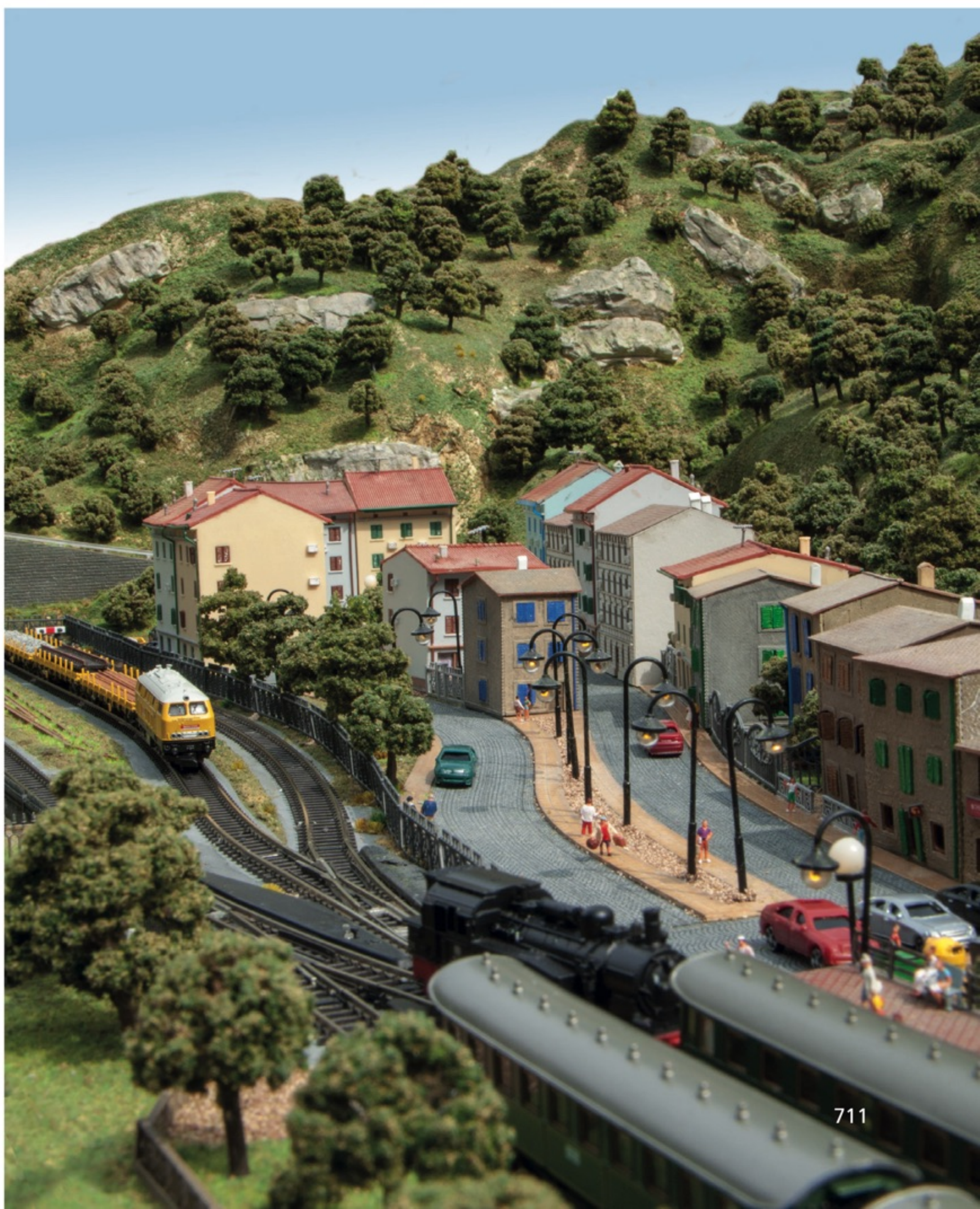


Above
A steam-hauled tourist train is crossing the viaduct and about to enter the tunnel.

tunnel mouths so I have gone to some trouble to line the tunnel mouths with stone, and add black shrouds to minimise the amount of light showing through. (These can be removed to ease the frequent chore of track cleaning.) To my eye, this has made an enormous improvement to the look of the tunnel openings.

The first buildings I added to Söller town were modified Kibri kits – reducing the roof pitch to more Mediterranean levels and, in a couple of instances, reducing the height. The walls have been matt varnished to get rid of that plastic sheen and the roofs dry brushed, both of which, I think, have improved the appearance quite a bit. I have also added a few extra details like graffiti, air conditioning units, and so forth.

Below
Söller town was made up with modified Kibri kits.





Above
The 0-10-0T working across the viaduct on the run up from Sóller.

Above right
With the Palma bound train safely stored in the loop, the inbound working emerges from Tunel Major.

My friend Colin Barker – an extremely talented modeller – offered to 3D-print items for me and most of the later buildings were designed in 123d as ‘flat’ parts, kindly printed by Colin, and painted and assembled by myself.

I am no painter, so I like to be able to paint, for example, doors and window frames separately before assembling them into the walls.

The roofs are cut from pantile textured plasticard as I reasoned even I could cut a rectangle of styrene with reasonable accuracy. (If you look closely, you can see I was mistaken!)

Colin also introduced me to the concept of dry-brushing and I increasingly used this technique to bring out the texture of the models and, indeed, of the texture created by the 3D-printing process.

Some of the buildings (the station, for example) are based on real buildings in Sóller. Originally I had a Kibri kit for the station but it looked a bit out of place in the otherwise Mediterranean setting so it was replaced with this scratch-built alternative. For some reason, I retained the original platform layout, something I now wish I had changed. The platform was 3D-printed and then capped with embossed plasticard to get the desired paving effect. The station building itself was 3D-printed as flat panels, as elsewhere. I have to confess to being quite proud of the station and particularly the station bar. Over the years I have had plenty of opportunity to study bar interiors and I have reproduced those blurred memories here ...

I have to thank Colin for having been so supportive and so generous with his advice, expertise, and plastic. It would be a very different, and far less attractive layout without his contribution.

Below
Sóller station building, assembled from custom-made 3D-printed panels.



Even at this small scale, I think it is important for buildings to be more than just shells, so I have made some attempt to model interiors. The ticket office at Sóller station, for example, has a ticket booth, and the bar interior has a counter, till, and bottles. The nearby fashion shop has clothes racks with items hanging from them. It is not necessary to model this stuff in great detail, but I do think it is important to give some life to building interiors. Fortunately, being a Mediterranean town, most windows have shutters so I have been spared having to do this for all the buildings. They are all illuminated, though, and I have taken care not to have lights behind every window.

The road surfaces are Noch self-adhesive tape, the appearance of which is surprisingly good. This also seems to have suffered from the instability in the baseboard, though, or else has shrunk itself – something I will watch out for next time. It was only when the town centre was nearly complete that I tackled the problem of getting the traffic in and out. The real Sóller is served by a tunnel and that would have been an easy solution but I did not feel either that or a level crossing would look right, so I ended up building the bridge that takes the road over the railway and to the edge of the scene. This did require a bit of re-landscaping, but I am sure it was the right thing to do.

It is a shame that many manufacturers seem to be pulling out of Z scale so there is little choice of road vehicles, even secondhand. I did find some (slightly over scale) cars on the internet. They were improved quite a bit by trimming off the flash and picking out details like headlights. I have plans to add number plates, but that is not a priority at present.

The streetlights are again inexpensive online acquisitions. They are somewhat over scale and, despite my best efforts, few are vertical, but I think the layout benefits from having them.

I have tried to add interesting and authentic details to other areas. Tables and chairs in the gardens, swimming pools, barbecues. Recycling points here and there, and a couple of skips loaded with all the usual things – broken glass, door and window frames, and even vinyl records. If you look really carefully you can see the covers of my five favourite records. Clearly the locals have no taste! Once again, Colin very kindly 3D-printed these items for me. I have placed a few lengths of rail by the track here and there, suitably weathered and overgrown, to give the impression of a real railway.

I imagine it must always be early morning as my layout is very sparsely populated. I intend to buy some more figures, though there are a limited number available that would suit the setting. Some of the figures were bought

unpainted, which would have been quite cost-effective had I not then spent a small fortune on paints. Despite my imprecision with a paint brush, I am fairly satisfied with the standard of decoration and this has allowed me to bring more variety to how they are dressed.

The track is split into several different sections which can be allocated to either of two home-built analogue controllers in the control console. In principle, you can operate two different trains in different directions on the visible part of the layout.

With the exception of a couple of turnouts in the fiddle yard, they are all electrically operated.

I have already made my excuses about the inauthenticity of the rolling stock. If I like the look of a model, I will buy it, and if I have it, I will run it! I have a suspicion I might be collecting for a future layout. Having said that, I have tried to acquire stock that is sympathetic (if not authentic) to the prototype. Most of the trains run are passenger services, predominantly steam-hauled, with the occasional track maintenance train for a bit of variety. Ironically, most of those seem to include loads of ballast.

I did not really envisage exhibiting the layout, but it is small enough to take out now and then – I hope people will feel it is worth the effort.

I think a model is never finished – there are always small details that can be added to enhance the realism and sometimes compromises made early on become intolerable and require more drastic action. For now, though, I have



got Sóller to a state I am fairly happy with. I would like to add some typically Spanish external electrical wiring to the buildings, some solar panels on roofs and, of course, more figures, but it is now in a state where I can enjoy just running trains. Once I have cleaned the track, of course ...

For all the inaccuracies and compromises, I think I have been successful in capturing some of the charm of Sóller and its railway.

Forthcoming exhibition

The layout will be at the Oxford MRC exhibition (which incorporates Zedex, the specialist Z event) on Sunday 13th October.

See *Exhibition Diary* for more information.

Above

A regular passenger service to Palma climbs from Sóller. As on the real thing, historic coaches are used on ordinary trains.

Below

The track maintenance train tackles the gradient on the middle section of the line.



Andrew Knights introduces his latest exhibition layout.

Photographs by the author.

Ponts Mill, Vermont

A small American switching layout

This layout was unplanned. That is not to say it was not designed, it was just that it did not feature in my plans to build an N scale North American model. But at one of the Wealden Railway Group shows I purchased from a fellow member the original model of this name, which was on a board some 4' long by 4" deep, set in Cornwall. The idea was to move the setting to Vermont and use the layout as a small switching scene test piece using the magnetic knuckle couplings fitted to the American stock, which involved adding a siding to the mill and a Micro-Trains® magnet for uncoupling. Operationally speaking, the model worked well, though its sinuous curves, which made the small layout look good, defeated reliable operation of the couplers. I took the layout to a couple of one day shows, which proved that the idea of a small switching layout in N scale was workable.

Doodling commenced. During this stage the plan was turned around in order to have the main switching area next to the operator, as I prefer to operate layouts from in front and sitting at the left-hand end, with access to some sort of added external fiddle yard.

A year or two later, I had some plywood sheets cut up for another project which resulted in four scrap sections 4' long and 9" wide. I realised that these could be a home for the proposed replacement *Ponts Mill*, giving a layout 8' long and 9" deep. The track plan was dragged out and, with a little adjustment, fitted the new space easily.

I wanted to keep the truly rural aspect of the original model and make a layout which although small was rather different from those I had constructed before. My previous layouts, built in OO or HO, were normally based around a 2m by 12" scenic box with or without fiddle yards as appro-

Above

A local passenger service waits in Hickling depot. The yarners are in full swing at the waterside.

Below

The RS1 bringing box cars from Ponts Mill has to wait for the Pine Tree bound RDC departing Hickling depot.





priate. This layout, however, would be narrower and longer – well over twice as long in scale terms as HO – and N excels in showing trains in the landscape, probably more easily than in the larger scales. The top of the boxed layout's proscenium arch would be lower too, at around 8" from baseboard to lid. This meant no mountain scenery! Keeping with the rural theme, and having a liking for water on a model, I wished to maximise the amount on the layout, which has resulted in the 8' of *Ponts Mill* having just over 4' of girder bridge.

The setting for the layout harks back to the Ahern County Railroad (ACR), my original HO loft empire, which like all my American layouts is to be found in a quiet corner of Vermont and is a short line supporting communities and various industries along its spreading route.

The track plan for Ponts Mill supposed a connection to a branch of this system where the main line has been truncated and a new station being provided to serve the settlement of Hickling. A much earlier truncated route stops at the eponymous mill itself, serving a couple of other lineside industries. The junction is kept so passenger trains from the nearby town of Pine Tree run directly into the depot. Freight is handled in the plant by hired-in power stabled as required at a small stub siding provided in the yard. Freight trains run into the depot and are then backed into the plant where any switching required is done by the plant switcher. Departing cars are then towed back to the depot, from where the main line loco hauls them off to Pine Tree and beyond.

Construction

A pair of baseboards were made up using the four 4mm ply offcuts to give the 8' by 9" scenic area – two pieces made the backscene and the others were the baseboards themselves. 1" square timber was used to make a simple frame which gave the 4mm ply some rigidity, with alignment achieved using hinges less the pins, the whole thing being held together by a clamp.

At this stage the layout was constructed on these L-profile boards until being finished with the addition of a top for each box containing the LED lighting strips and hinged front protecting flaps-cum-information boards to provide security in transit and in storage.

The track plan was drawn onto the plywood and the areas deemed to be 'water' were given three coats of acrylic gesso. Once this was dry, mixtures of acrylic paints were used to depict the water itself, mucky green in the middle fading to a murky brown towards the edges. Once this had dried, six coats of polyurethane yacht varnish were applied, two coats per day, morning and evening, over the previous dry but not fully hardened layer. After this the boards were left alone and perfectly level for all of this to set hard.

The main land areas were then covered with 5mm Foamcore with another layer of Foamcore for the trackbed, the factory yard area, and depot, then Peco Streamline Finescale Code 55 N gauge track was laid on top of this and glued into place.

Above
Fishing is clearly popular as the short freight rumbles over the bascule bridge en route to Hickling depot.

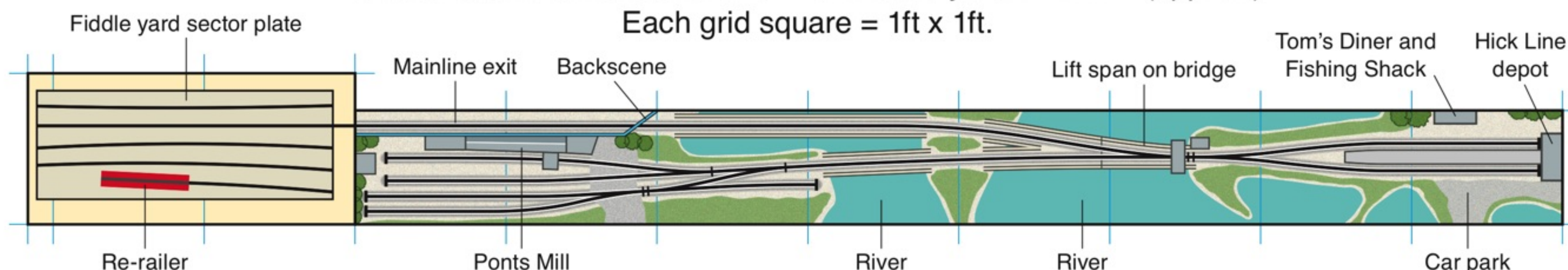
Below
A freight for Ponts Mill draws into Hickling depot. Tom's Diner and Fishing Shack has a few customers. The local bus seems to have problems – a rail replacement may be needed!



PONTS MILL, VERMONT

Overall scenic dimensions: 8' 0" x 9". Fiddle yard: 2' 2" x 1' (approx.)

Each grid square = 1ft x 1ft.





Above
The freight has arrived at Pont's Mill yard, and the train engine departs.

Below
Switching completed, the leased power, an RS1, drags the new cut of cars out to Hickling depot.

The track was wired and point motors fitted, and all was thoroughly tested before painting the sleepers and rails using acrylic paints from the Games Workshop 'Citadel' range.

Ballasting was next, just a fine Peco N scale ballast on the main line and a muckier version for the branch and factory. This was fixed using a watered-down acrylic varnish mix, which I prefer to the more usual diluted PVA glue.

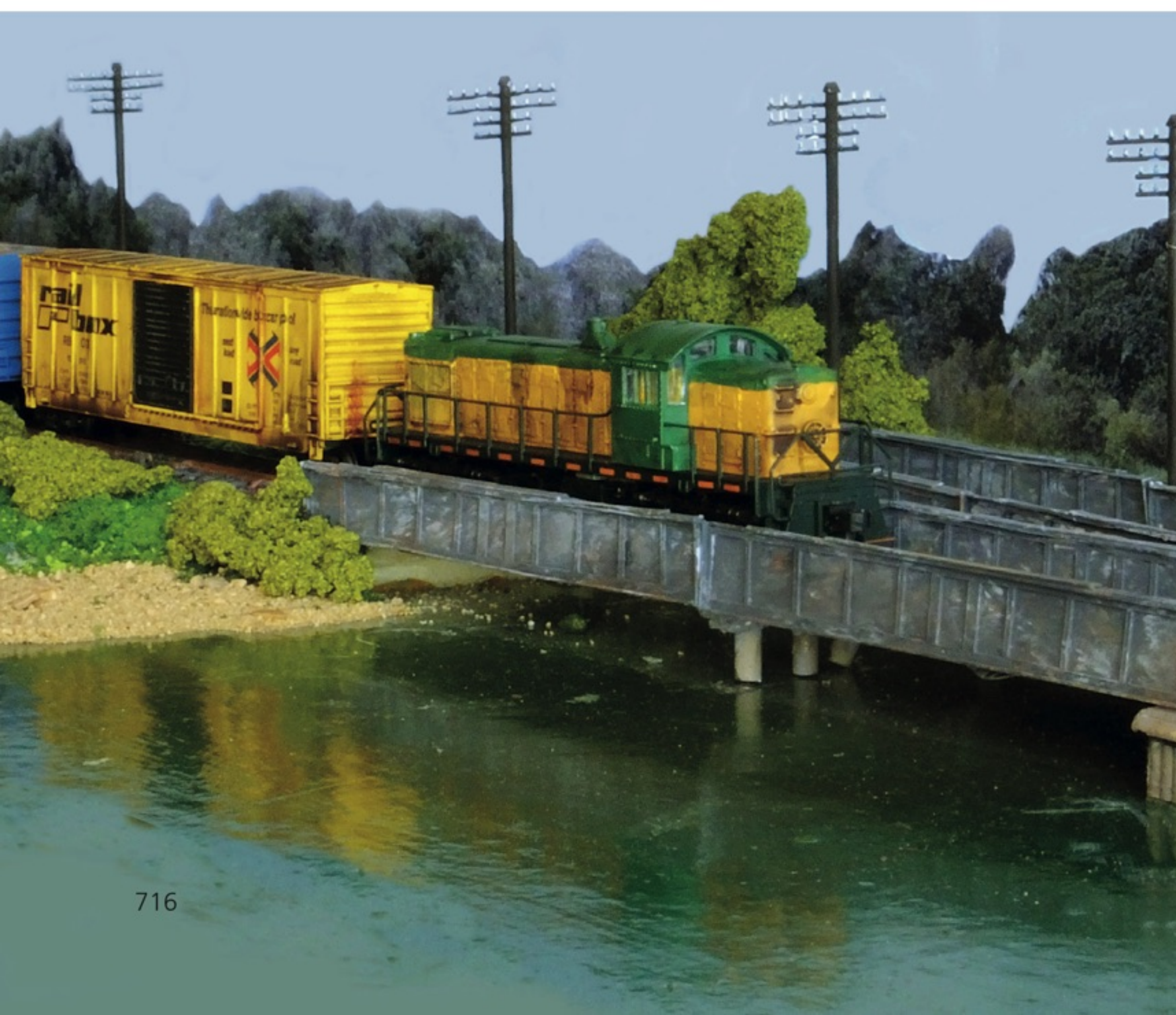
The acrylic varnish was also used to sculpt the rest of the land formations, as the adhesive in a form of papier-mâché. I wanted to avoid using plaster in the scenery and keep the weight as light as possible and came across this technique on a YouTube video by a Swedish modeller who used toilet paper to make sea water *in situ*. The process is simple and exploits the low wet strength of the toilet paper. Basically the area to be worked on is painted with neat varnish, thickly

applied, and then toilet paper, quilted or otherwise, is stippled into place using a stiff brush. The stippling is continued until the texture of the toilet paper breaks down and the area is covered in a thick malleable 'goo'. More layers of paper and varnish are added until there is the desired amount of material in place, when the paper pulp is then moulded into shape and left to harden, which in the winter could take about a week – quicker if you lean the boards against a radiator! The result is very light, very solid, and best of all there is no risk of dropping dry plaster powder onto finished ballast, where I have always found it all but impossible to remove.

Once everything had dried, cheap acrylic paints were used to colour the stark white Foamcore and toilet paper scenic foundation, with areas for grass and foliage painted green and the basic ground and road areas many sorts of grey. This was just to provide a base, and to give a chance to evaluate the overall effect before anything too drastic, or expensive, was done.

Bridges

The girder bridges make up the entire centre section of the layout and are thus somewhat of a feature. I made the girders using a Silhouette Portrait Two cutter and laminated 10thou plasticard to form beams some 50 scale feet



Right
A new cut of cars is hauled away from Hickling depot by the road engine (a GP9).

Below
Pont's Mill itself. "Ahern County's Own" what, one wonders?





in length. Twenty-odd were constructed, with two being modified to make up the lifting span which is the main part of a Scherzer rolling bascule bridge. It is quite a large span as it handles the divergent track of the junction. Possibly two bridges would have been more correct, but hopefully one is enough for effect. Once assembled the girders and the bascule bridge superstructure were painted and then weathered with a mixture of paints and weathering powders.

Next I had a setback. The whole layout is a scene of rough grassland, brush, scrub, and water. As a result, once all of the ground foam was down, the board was rather soggy with acrylic varnish. Before the scenic clumps and buildings could be fixed into place this had to be dried out; being winter this could take a long time. So I used a small fan heater and after an hour or two most of the board was dry. Except, however, the area beneath the fan heater. So, I moved it to the other end of the layout, set it going, and had lunch. On my return, the board was all nice and dry and the foam quite rigid. However, the two spans and lift span were some 5" beneath the track, which had bowed up this far from the river. The bridges themselves were fairly easy to remove, as they were crumpled and only about six inches long. When I moved the heater, I had inadvertently set it to its 2kW setting. The bridge had been the focal point of this heat!

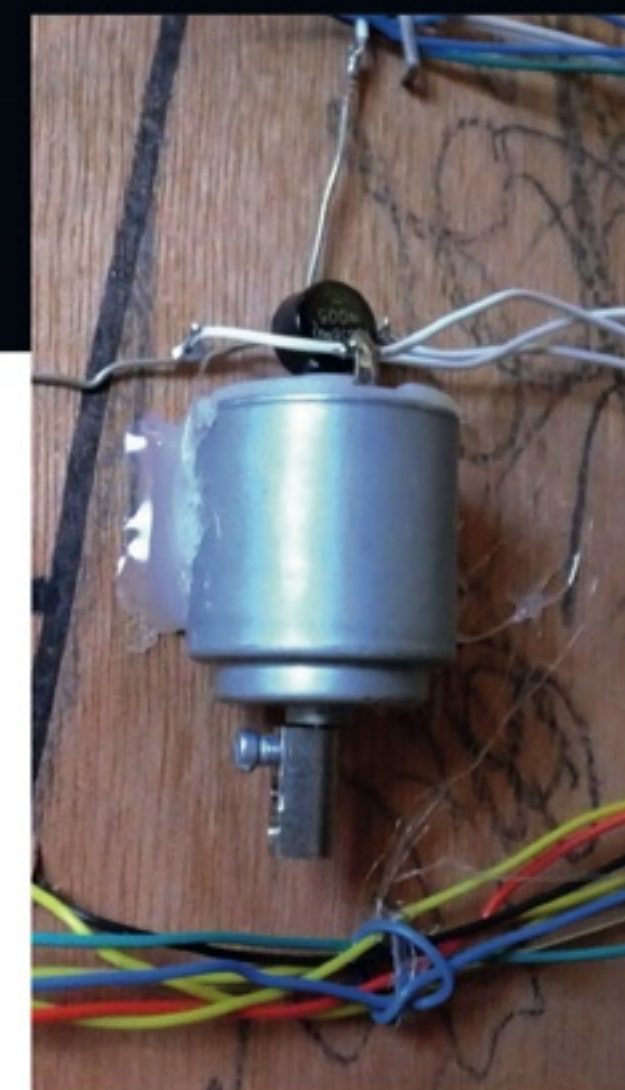
Fortunately having used the Silhouette cutter to make the girders in the first place, a new set were cut, assembled, painted, installed, and weathered. By late that night all was back nearly as it had been just before lunchtime.

Wiring and control

The layout is wired for 12v DC operation. The control panel, incorporating a throttle and capacitor discharge unit (CDU), sits on the operator's knees, being attached to the layout only by its cabling. The panel also has the DPDT toggle switches to actuate the point motors. These toggle switches are of the on-on variety so they indicate the direction of the point. The centre contact of one side of the switch has been wired to the base of the toggle of the switch itself with the other wires from the same side going to each coil of the solenoid. The other sides of the DPDT switches are used to determine the crossing polarity, though this does give a potential short circuit until a simple probe is used to flash current onto the switch and change the points themselves – a simple system which should cope with the high currents these motors draw.

A small electric motor has been firmly attached to the underside of each baseboard using a hot gun glue. These motors each have the metal insides of an electrical connector screwed to their shafts. When the motors are actuated, being off balance they vibrate the boards – hopefully freeing a stuck locomotive. These baseboard vibrators are activated via a push button on the panel.

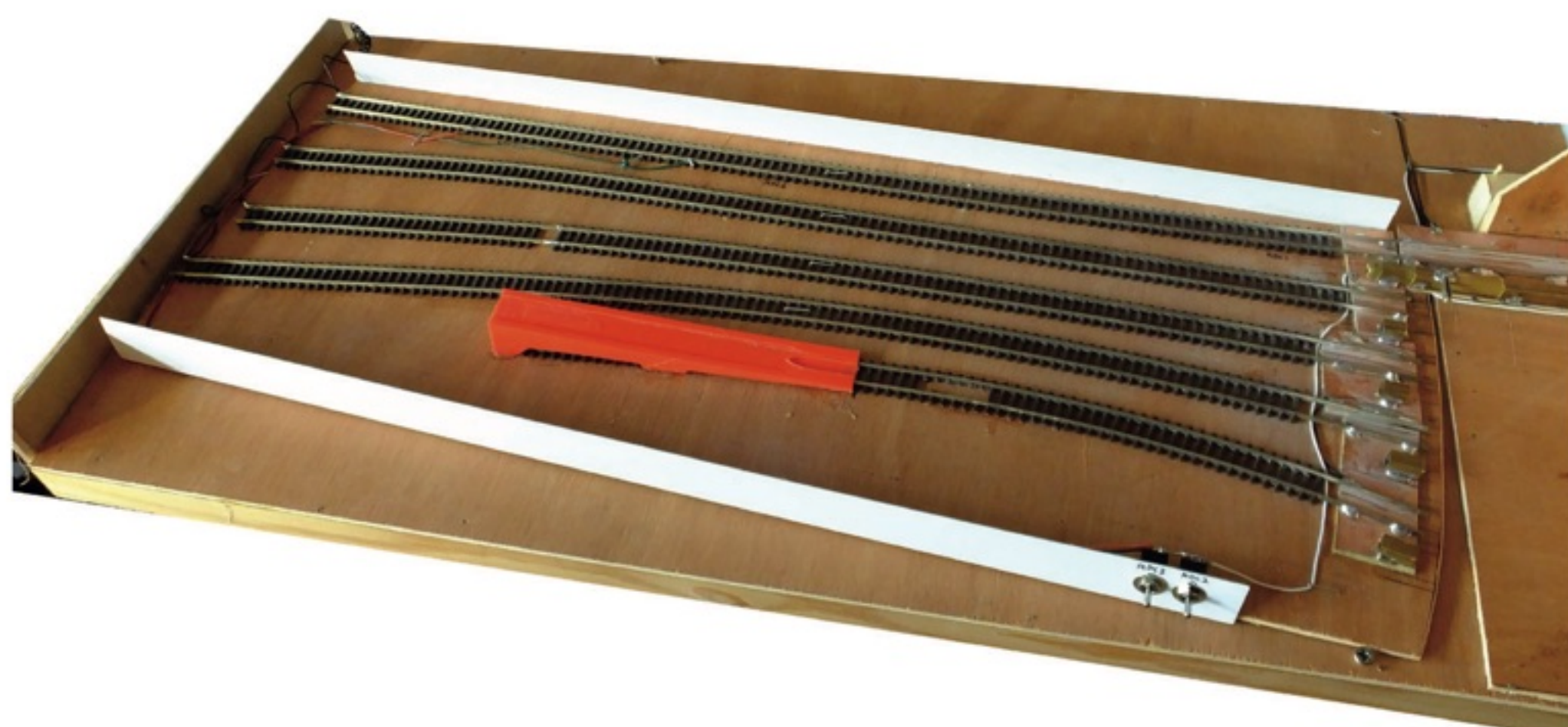
After trying a couple of different point motors, I reverted to my favoured H&M solenoid machines, ignoring the built-in switch, which can be unreliable on these old point motors. So that these do not hammer the points to pieces I made up a linkage involving an arm ending in a loop being connected



Above
A baseboard vibrator along with its rectifier (supplied from the AC feed to the CDU which drives the points).

Below left
The sector plate fiddle yard, showing the switches for the rear road, which can hold three RDCs, and the re-railer track for top and tailing returned trains.

Below
Fiddle yard and baseboard alignment by pin-less hinge. Track location and power feed is by a simple bolt.



to the shaft of the motor. The loop allows the wire inserted into the point tie bar to push the point over, but loses any excessive throw, all with no bending springs or omega loops and little force on the tie bars too, in theory at least.

Buildings

The buildings are all printed using a computer from images found on Flickr and Google searches; they were adapted and manipulated using a photo editing program.

The main part of the mill building was from an O scale industry prepared for my On30 loft layout, and a factory on my *Eastwood, Vermont* HO layout provided the file for the side extensions for the mill and the other industrial unit. These were modified on the computer and several copies printed and glued to thin card. Different elements of each layer were cut out to give the basic structures some relief when fixed together.

It was the same for the buildings at the far end of the layout. A site dealing with prefabricated buildings allowed me to construct the 'new' depot building at Hickling. "Tom's Diner and Fishing Shack" was an image of a containerised Post Office on a Scottish Island which has been rearranged more than slightly to fulfil its new rôle. The bridge control cabin was from a Flickr picture of a tatty and derelict scrap shed.

Backscene

The backscene provided the name for the depot itself. I made this from a series of panoramic shots of Hickling Broad. Such areas of water are ideal for backscene photographs – long runs of trees with only sky and/or water viewed from some quarter of a mile or so and taken with a 50mm-70mm lens. The pictures were matched by hand and a couple of other scenes blended into the chosen tree run to give the main navigation channels. Similarly the one real house on the scene in Hickling was complemented by a Vermont church and similar New England buildings.

People and other scenic items were added to keep the viewers' interest within the scene itself even when they are not watching the trains.

Rolling stock

The rolling stock has all been weathered, and the majority of the locomotives too. Most of the locomotives featured are secondhand or hand-me-downs, typical of short lines in



this period. Some have been painted in the Ahern County Railroad livery while others are part of the tourist fleet and represent lines in the north-eastern corner of the US, with some representatives of local lines making the odd appearance.

Operation

Operation of the layout is not really prototypical but more related to keeping the operators on their toes. The fiddle yard is behind the operator and holds three trains, each consisting of three freight cars. The layout has four car spots within the mill complex, and a series of cards shows the operator which car goes into which designated spot. The cars may already be on the layout or within any of the three aforementioned trains. Trains can only be three cars long as the depot roads will not hold longer, they are for the passenger services, after all, and these are provided by Budd Rail Diesel Cars so short platforms are not a problem!

Once the cars have been correctly spotted and the re-assembled train has departed, the cards are shuffled and a new switching puzzle set.

Forthcoming exhibitions

The layout is due to appear at: the American Model Railroad Show, Charis Centre, Crawley, in September; the Worthing MRC show in October; the Brighton MRC show in November; and Bognor Regis in January 2020. So if you see us and have any questions about the layout, do ask the chap holding the controller!

Above

Looking across the tracks towards Hickling township, a fair walk from the depot.

Below

Passing the township of Hickling, Vermont, a freight bound for Ponts Mill approaches the new depot.



Richard and Hazel Woodbridge

describe their new Z gauge layout.

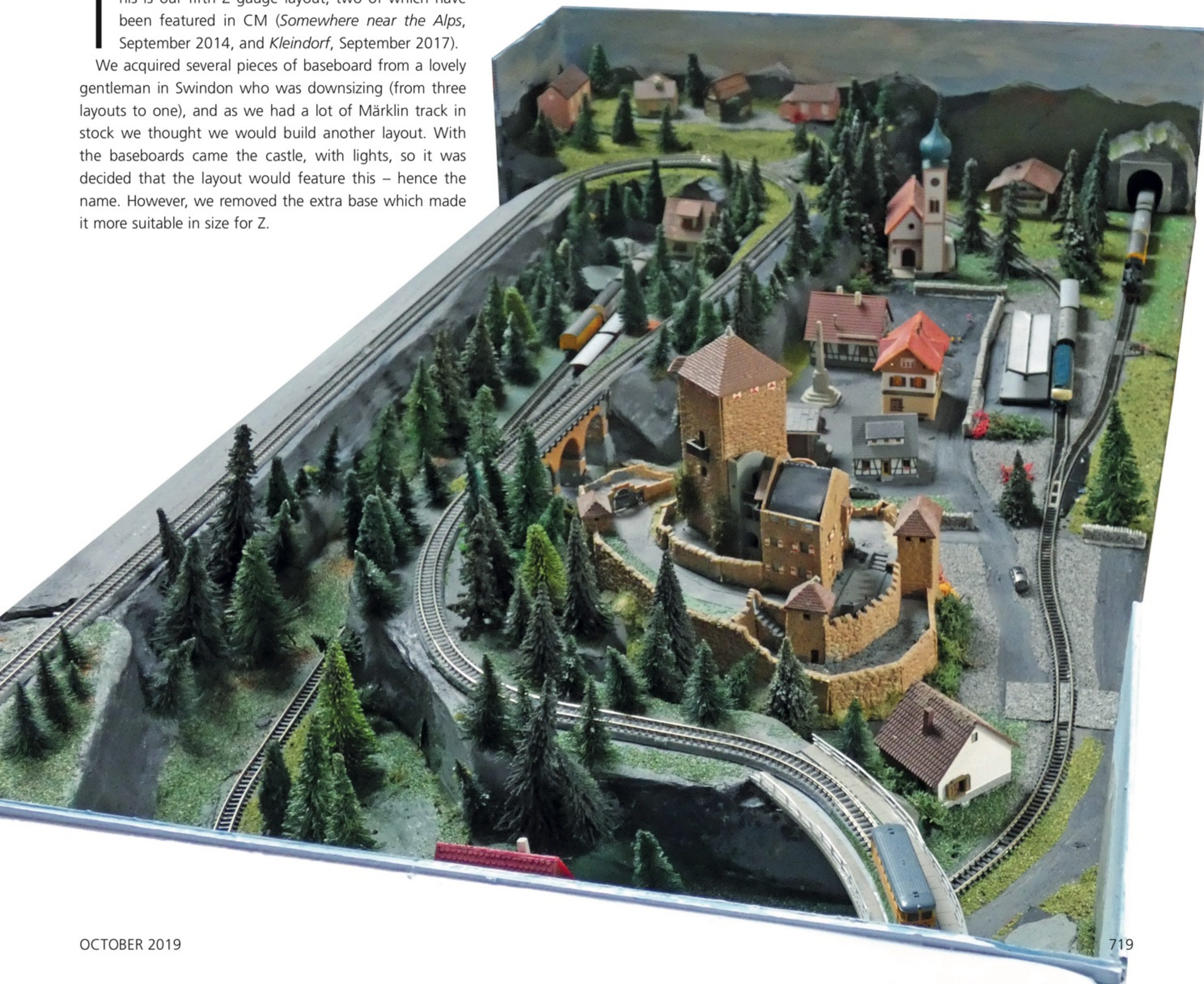
Photographs by the author.

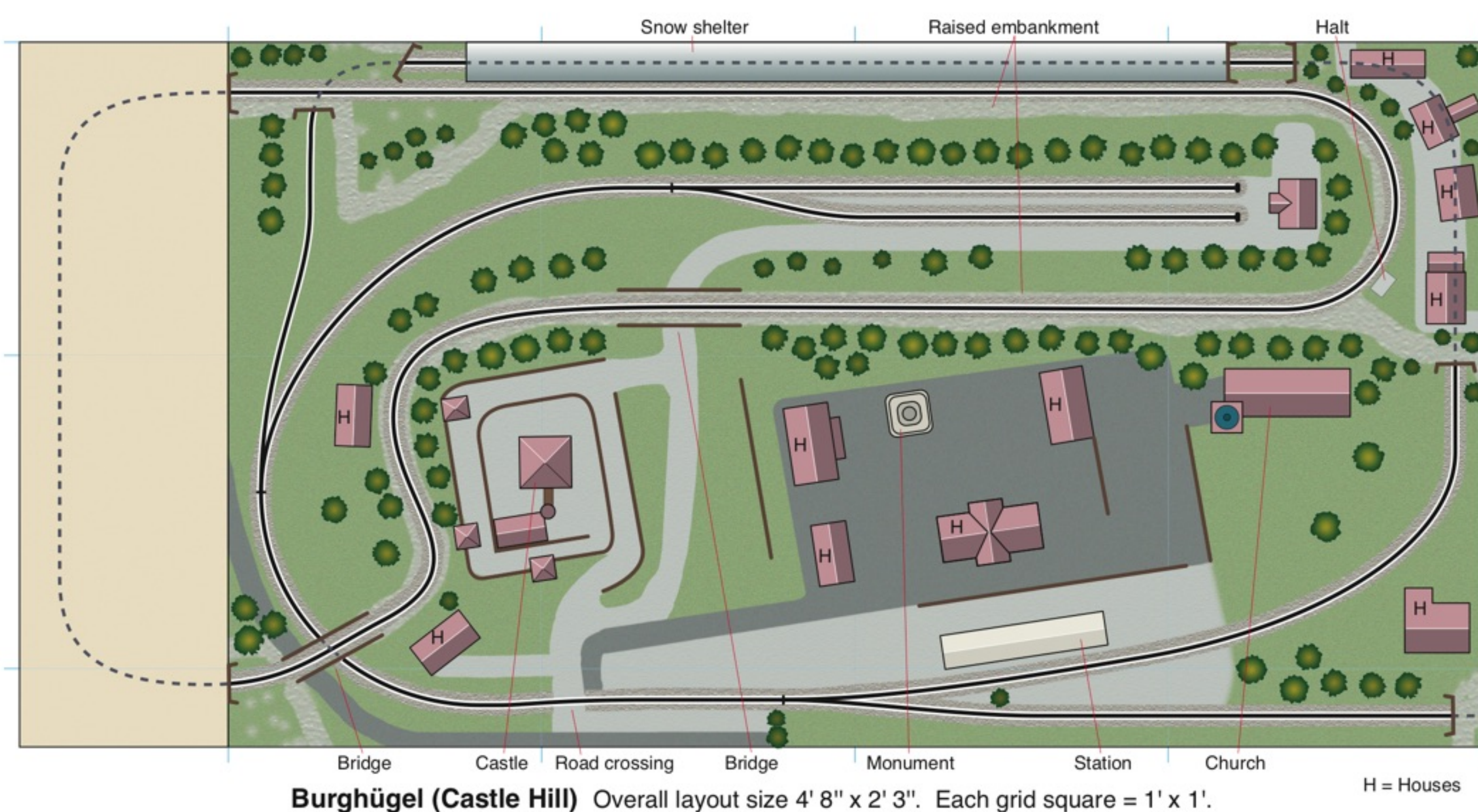
Burghügel

Castle Hill – an exercise in recycling

This is our fifth Z gauge layout, two of which have been featured in CM (*Somewhere near the Alps*, September 2014, and *Kleindorf*, September 2017).

We acquired several pieces of baseboard from a lovely gentleman in Swindon who was downsizing (from three layouts to one), and as we had a lot of Märklin track in stock we thought we would build another layout. With the baseboards came the castle, with lights, so it was decided that the layout would feature this – hence the name. However, we removed the extra base which made it more suitable in size for Z.





We decided to make two levels using the far superior Rokuhan track for the upper level and Märklin for the lower. Because Z is difficult to shunt, due to the small size, we decided that lots of scenery and a simple track plan was the order of the day. This also meant we would not have to buy further points. It is just basically two ovals with a quarry on the lower level and a shuttle on the upper level. Because you cannot 'slow run' Z gauge as it picks up dirt so easily, we have to rely on trains running continuously – this keeps the viewers happy.

This layout was made almost entirely using re-cycled materials, stock (all by Märklin) from previous layouts that have been dismantled, buildings bought secondhand at model railway shows, and plenty of fir trees suitable for Z from different sources which we already had in stock.

Several years ago, Hazel worked for an estate agent and due to a change of the business name a pack of unused but printed plastic corrugated signs became surplus. (They were printed on one side only and sandwiched together, leaving a clean white inner.) They were going to be binned so they came home, and over the years have been a valuable source of material for all kinds of projects including the layouts. A raid (with permission) on a local furniture shop's waste skip provided several pieces of 1" thick clean polystyrene foam, plus some much thicker pieces. A home building project also provided thick insulation material which is easy to cut at any angle with a sharp carving knife and is great for cliffs and mountains.

Papier mâché (made with wallpaper paste) was used extensively, but in places modelling clay and plaster bandage were more suitable, and stronger, and were really the only purchases we had to make.

There are a number of tunnels on the lower level, which uses Märklin track, and we found that it is less frustrating, especially if it is to be transported, to lay this on card ensuring that all joints are tight and level and then stick the card in place before finalising the tunnel, especially if bends are involved.



When joining the plastic corrugated board, I have found 'No more nails' to be the most reliable adhesive. Kitchen weights and clothes pegs are useful for keeping things flat and for holding two surfaces tight whilst the glue dries.

Regular testing of the track with a loco and stock makes sure tunnel entrances are set correctly and there are no conflicts with the scenery. This is Richard's contribution to the finished layout, apart from clearing up after Hazel following cutting the foam materials to the right shape!

The layout made its debut at the Monmouth exhibition in February 2019.

Forthcoming exhibition

The layout will be at the Oxford MRC exhibition (which incorporates Zedex, the specialist Z event) on Sunday 13th October.

See *Exhibition Diary* for more information.

Walter Cornelis introduces his German narrow gauge layout inspired by scenes on the Euskirchener Kreisbahnen.

Lechenich

An ingenious double-sided design

The idea for this layout occurred to me when I read a German magazine article describing the Euskirchener Kreisbahnen. This metre gauge network operated in the region between Köln and Koblenz. In the town of Lechenich the train ran along the street and through gateways in two towers in the old city wall.

I started building the layout in early 2016 and it was finished in September 2018. Its first major public exhibition was at OntraXS! in Utrecht in March 2019.

The layout consists of four sections, two of 100cm x 20cm and two 50cm x 20cm, which are connected with a click system, aligned with wooden pins, to form a rectangle. This leaves a central opening of 10cm where the double-sided background fits.





Above
The Waggonfabrik Dessau railcar
(a Tillig model) makes its way carefully
along the main street of Lechenich,
which forms one side of the layout.

Inset
The station is on the edge of town.
The passing loop is situated on the curve
around the end of the central core
which separates the two sides of the layout
and supports the valance and lighting.

Photographs by the editor.

The background consists of two sheets of plywood and is rounded off at the ends by plastic half tubes. It was painted realistically by a friend.

The canopy is a box with a valance which fits into the top of the background. It is equipped with day/night lighting.

The layout is supported on sturdy metal legs with a wooden frame on which the baseboards ultimately rest.

The track and points are from Roco. Rails and sleepers were painted before they were laid.

The landscape was formed from waste polystyrene foam, covered with coloured Sculptamold, sprinkled with scatter materials, and planted with trees made from sprayed and sprinkled sea foam – there are more than a hundred on the layout.

Each section has its own theme.

1. The town: the train runs along the street and goes through the town gate, passing the fire station. The houses are from Faller, Auhagen, and Pola. On one of them there is a chimney sweep at work. The city gate tower is an old Pola model, just big enough for the train to run underneath.

2. The station: the train leaves the street section and enters the curve. There is a passing loop here where two trains can cross, so you often find one waiting.

3. The forest: after the station the train runs through the next section, with many trees, and there is a siding for loading wood. Here you can see the forest workers in action with a working saw and working lumberjack. After the level crossing the train passes over a small bridge, and nearby is a shepherd's hut.

4. The cutting: the train runs through a curved cutting in the forest and ends up at the town section again. There is a possible connection to a three track fiddle yard.

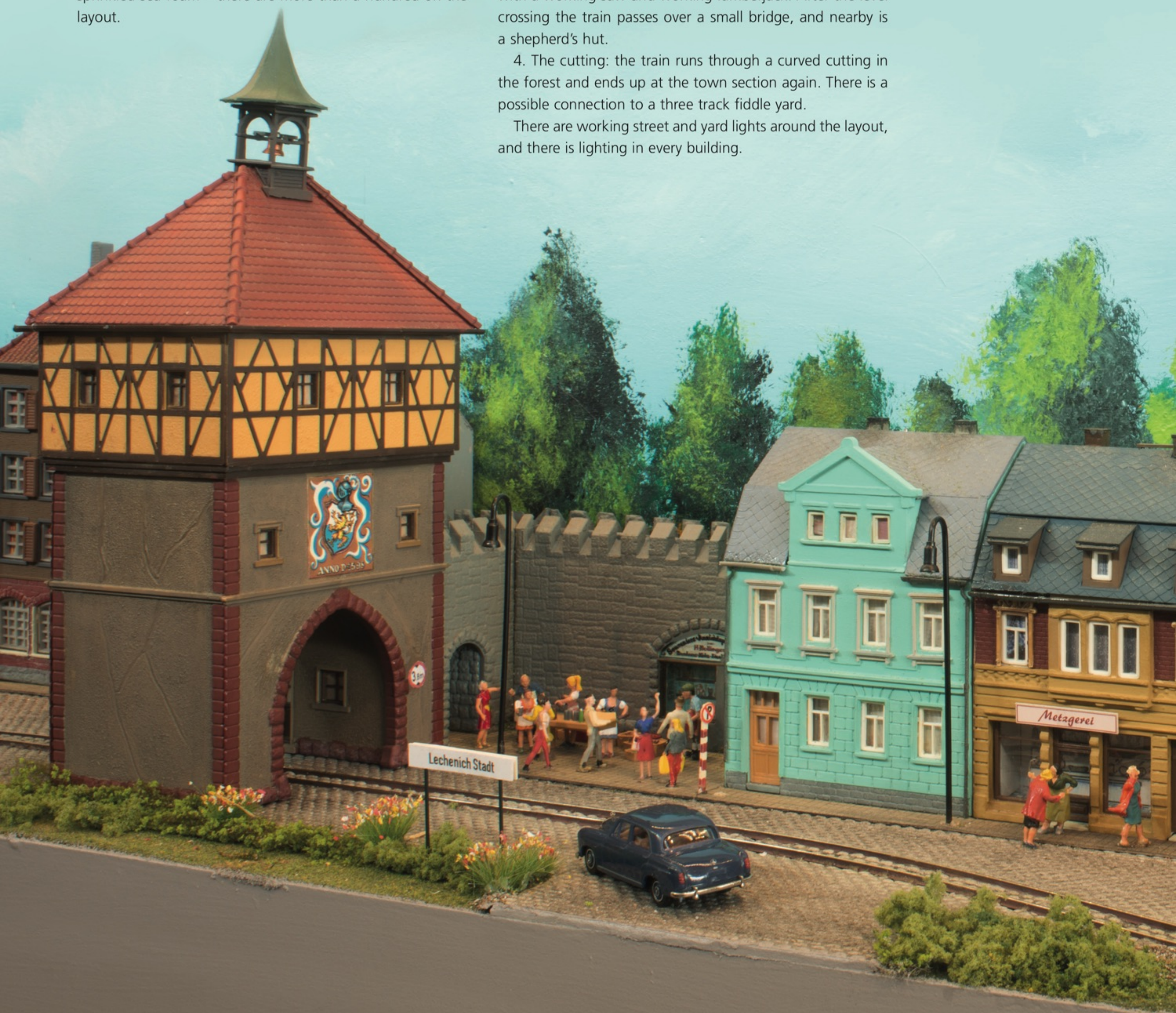
There are working street and yard lights around the layout, and there is lighting in every building.

Below

Beyond the old town gate there are more buildings; all have lights and there are some animated features such as the chimney sweep.

Right

A goods train, hauled by a former Heeresfeldbahn HF130C 0-6-0 diesel (Bemo), in the curved cutting at the other end of the layout.







Above
The goods train rumbles over
the farm track bridge.



Above
A lot of activity
in the timber yard.





The rolling stock is from Roco, Liliput, Bemo, Minitrains, and Tillig. Since there are no commercial models available of Euskirchener Kreisbahn stock, I have opted for similar stock from other local narrow gauge lines, such as the Rhein Sieg Eisenbahn and the Nassauische Kleinbahn – which also ran through a town gate.

Control is simple analogue DC. Servo motors from Conrad are used to change the points, which are self-isolating. The control panel is situated by the station.



Above left
The railcar has just left the station (off to the right) and approaches the forest.

Above
As the railcar clatters over the bridge, the shepherd has trouble controlling his flock.

Top
A Gmeinder diesel (Minitrains) has delivered a load of logs to the timber yard siding.

Luc de Martelaer describes his unusual exhibition layout which uses several scales to create an effect of distance.

Bolormaa Mashbalgir

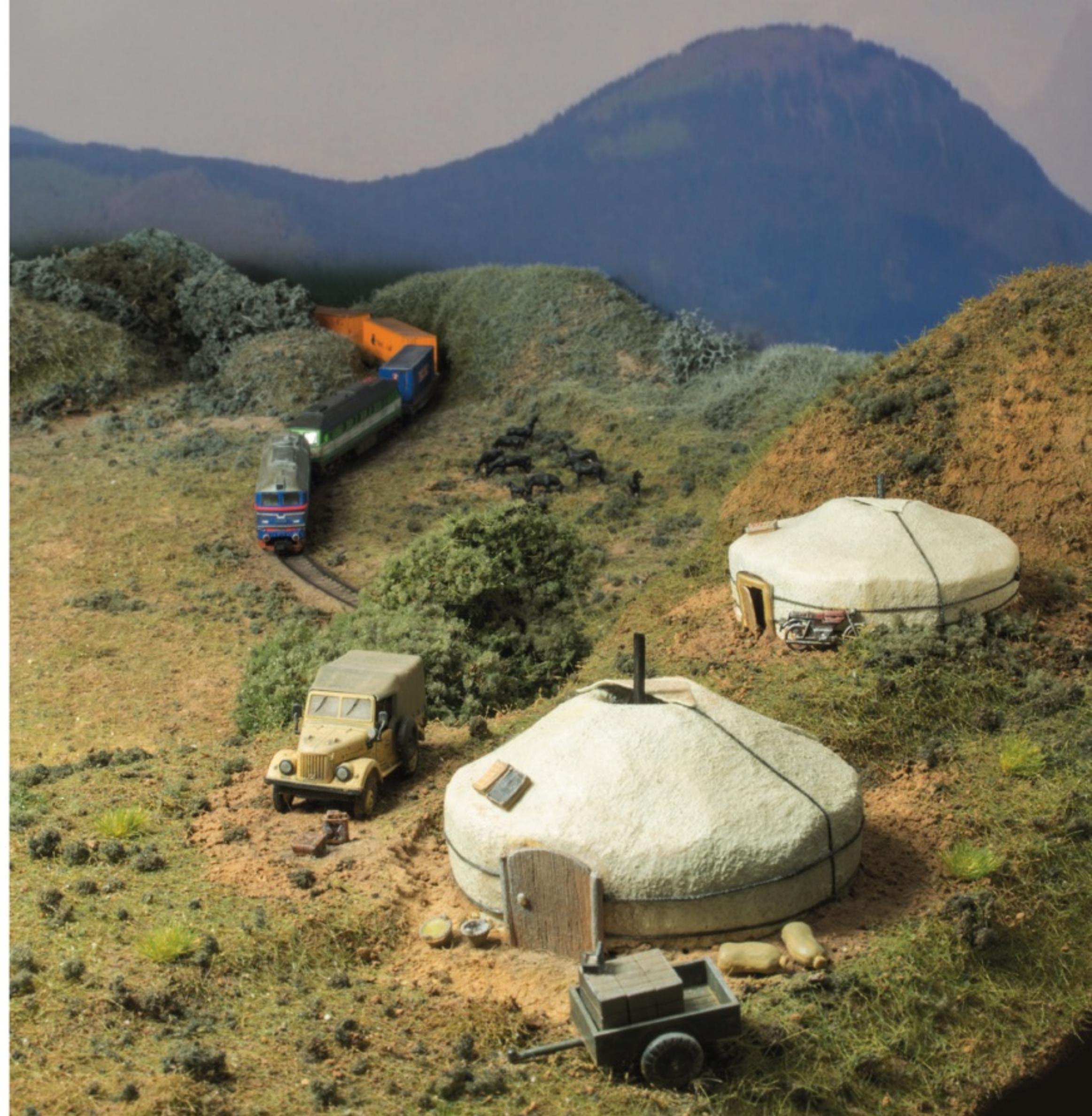
A Mongolian scene

The best ideas always come from my wife. My previous (American) layout had been to enough exhibitions, but I could not find a good plan for something else. Then my wife suggested Mongolia. We had enjoyed a nice holiday there, with a small group of friends, and we came back with our eyes full of stars. So, I decided to give it a try.

The most difficult challenge seemed to be capturing the feeling of boundlessness, of wide open spaces.

I decided on the following principles:

1) The background must be curved: no corners means that you can more easily forget that you are looking at the inside of a box.



2) A small 'window' restricts the field of view, because the foreground is HO, the middle ground (with the trains) is N, and the background is a picture. This is called 'forced perspective'. Items towards the back seem to be even further away because they are smaller. But it only works if you look at the precise height and angle, and that is why the viewing window is that small.

Above

The GAZ69 truck is by Herpa (HO). In reality the tents are made of felt, supported by a sophisticated wooden structure, with space for a proper door in a frame. For those who are still nomads, it is easy enough to take down the tent, load it onto a truck, and put it up again a little further on, where there is enough grass for the cattle.

Left

The stone pile is called an 'ovoo'. They are often built at passes, and they are a kind of collective prayer. It is the custom to stop and circle an ovoo three times, moving clockwise, in order to have a safer journey. Every traveller must put one more stone on it, and by that gift he will get the protection of the resident spirit. Usually, rocks are picked up from the ground and added to the pile. One may also leave offerings in the form of sweets, money, milk, or vodka. If one is in a hurry and does not have time to stop, sounding the horn while passing by will suffice.



3) The use of colours helps too. The foreground is brighter, the middle ground is dustier, faded – I sprayed the far middle ground with a thin layer of blue-greyish paint – and the background picture is a little misty.

The photo is one we took ourselves, and it is very pleasing to think it could be used like this. Having a picture printed on Forex is quite expensive, and I hesitated for some time. I decided to do it when I realised that I could never be happy with an imperfectly painted landscape; paying for a beautifully printed picture would hurt only once, while seeing a badly painted background would hurt every time.

The track (Peco N gauge code 80). passes under the HO foreground, which is removable. This has two benefits: I can intervene in case of accident in there, and it makes the layout easier to carry.

The layout, the trains, and compromises

The aim was to build a layout for exhibitions. I learned a long time ago that you cannot operate properly and talk to the viewers at the same time: something is bound to go wrong! It is possible if you have a large enough team, but there are only two of us, my wife and I. So, the track is a simple oval, with no points. There is only one train, consisting of two heavy diesels and a dozen wagons – flat cars loaded with containers, and tank cars.

In real life, this is part of the Trans-Mongolian Railway which connects the Trans-Siberian line to Beijing.

So, I had to find motive power and rolling stock that would be more or less appropriate for Mongolia. And it was easier than I expected! You can find a lot of containers with Chinese brand names; Fleischmann offers the M62 'Taigatrommel' diesel, Brawa produced a 'Ludmilla', I had an American Alco that could easily be repainted as a TEM3, and I made a really lucky strike: I was able to find, at Transmondia



in Paris, a genuine Chinese DF4D diesel which someone must have brought back from China in his suitcase.

All of this is a little motley, but only very few people know anything about Mongolian railways, and I do not; that is why we are almost completely free!

Some people told me that Ludmillas never left Europe, and they are probably right, but there were similar machines supplied to Czechoslovakia and Bulgaria, and in the USSR the TE109 single and 2TE109 double units.

Mongolia and Russia use the same broad gauge (1.52m), while China uses standard gauge, so a Chinese locomotive and a Russian one could not run together on Mongolian track. It is true, but it does not matter to me – I just wanted to convey an impression.

Above
The lead locomotive is a Chinese DF4D Co-Co diesel, a model in N scale from the Chinese brand 'CMR Line'. How this came to Europe, and by complete chance into my hands, remains a mystery and a miracle! It is a lovely runner.

Right
Mongolians in traditional clothes, which were made of paper in 1:87 following the techniques of Robert Gesuelli. The truck on the left is HO (by Herpa), while the other one (by Oxford) is N – even though it is only a few inches behind, it seems to be much further away, and helps to give the sense of depth.

Ovoos are meeting places: while one truck driver stops to put a stone upon the pile, there comes another, and then they start chatting, and drinking tea, and asking about the road ahead. These are lovely places, full of friendship.





Scenery and details

The landscape is made of expanded polystyrene; I paint it light brown, and then I glue dust or sand on it, and after that static grass.

The trees are basically buddleia flowers. I make my own foliage by using an old coffee grinder, latex foam, and green dye. When you become a little experienced with that technique, you can obtain fine results, with a lot of different shades and textures. The foliage is glued onto the dry flowers by using inexpensive hairspray.

Busch and Herpa sell models of Russian cars and trucks as they were used in East Germany. You can see them in all the former communist countries; we even saw them in Laos and Vietnam! So, I can put them in a Mongolian landscape. The loads are a little clump of expanded polystyrene, carved and wrapped in a paper handkerchief.

You cannot find commercial HO scale figures of Mongolian people wearing their traditional costume! However, I am lucky enough to know the ladies of the Rail Club de Meaux. They are pupils of Robert Gesuelli, and they can make any figure out of paper.

The yurts are made of expanded polystyrene, wrapped in baby wipes! You need a material that is strong, extensible, and non-woven. It is glued to the core with off-white acrylic paint.

Little yurts in a bigger yurt

When the technical part and the scenery were finished, we took a few steps back and realised that we could make a bigger yurt with the same shape. So my wife started sewing and after five days work we had something completely unexpected: small scale models within a larger scale model!



Above left

There is a flap on the top of the tent worked with a rope; not so long ago, they made a fire on the ground in the middle of the yurt, but now they use a stove in which they burn dried yak dung. It does not stink at all; it smells like burning hay. On the top of the model tent is a toothpick, standing in for the iron stovepipe. Food is often kept on top of the tents, out of reach of the dogs, goats, and other 'gourmets'. It is mostly cheese or drying meat.

Photo: author.

Above

After crossing the steppes, the trains must struggle through the Gobi Desert. An M62 working together with a TE109. It is not very difficult to find Russian locomotives: German manufacturers sell them, as they were used in former East Germany. These models are from Fleischmann and Brawa, and they have been skilfully weathered by my friend Jean-Claude Botspoel.

Left

A well-loaded East German-built IFA W50 LA PV truck (HO model by Busch), passing the ovoo.

Photo: author.



People seem to like it.

It seemed a pity to have nearly 3.77m of track ($1.2\text{m} \times \pi$) and to show only 0.4m of it. So an afterthought made me create a second viewing window on one side with a scene from the Gobi desert, using some Preiser N scale camels.

Model railway exhibitions

The layout has already been shown at several places in France, The Netherlands, and Luxemburg, and it is due to go to others in Belgium, Germany, and Switzerland. So far it has not been to the United Kingdom – but we would be very happy to be invited!



Above right

The camels are from Preiser; they are loaded with chewing gum and broom bristles.

The telegraph wires are made of fishing line, glued to the poles with cyanoacrylate (superglue). And the sand is ... sand, held down with cheap hairspray.

Photographs by the editor, unless otherwise noted.

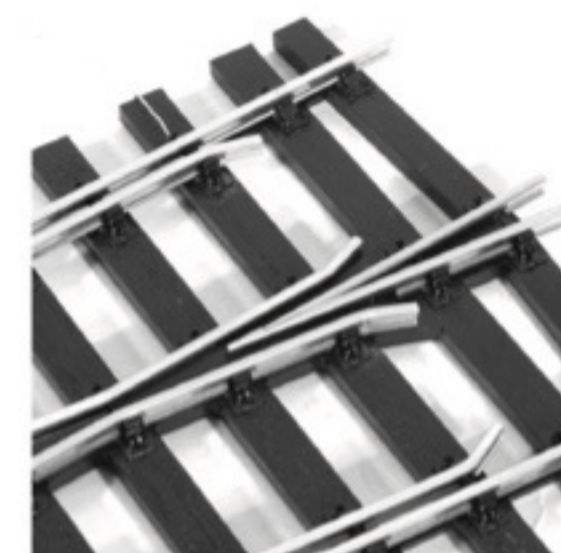
Right

I am lucky to have a spouse who not only supports me in the hobby and at shows but also is able to produce such a difficult work of sewing: a made-to-measure yurt! Thank you, my dear.



Reviews

Evaluated by our own specialist staff



HO

Italian D.145 diesel new in Expert range by Piko



Piko have recently released a new model specifically for the Italian market.

It represents the class D.145 Bo-Bo diesel electric, developed by Fiat Ferroviaria Savigliano and its sister company Elettromeccanica Parizzi at the end of the 1970s. An order was placed in July 1979 and the first deliveries began in 1982, continuing until 1985.

A version was also developed by Tecnomasio Italiano – Brown-Boveri (series 2001 – 2020).

The centre cab arrangement for better visibility when shunting was in keeping with modern design but novel for Italy. They were also the first FS locos to carry the bright orange livery.

They are powered by two Iveco four-stroke twelve-cylinder diesel engines, but can run on only one.

They are rated for 100km/h, so are capable of trip working as well as heavy shunting.

Initially used on construction and maintenance trains, they have been allocated to Turin, Bologna, Rome, Reggio Calabria, Messina, and Naples.

These locos have also been supplied by Fiat to other Italian railway companies.

At the beginning of 2017, forty-one were in service with Mercitalia, the FS freight division.

This version of the model represents FS D.145 2018.

All markings are clearly printed, and the finish is good, with the yellow angled sections clearly distinguished.

The body is accurate in major dimensions and detail.

Added details include very fine running board side and end handrails, in etched metal for strength.

The cab has an interior with control desks represented. Windscreen wipers



Plastic mouldings provide the cab door handrails, exhaust, exhaust cover, horns, aerial, short hood cover, vents, grille surround, and fan, while both top grilles are etched metal. There are glazed inspection ports in the body sides.

are moulded as part of the glazing and neatly picked out in black.

Many small plastic detail components have been added to the substantial die-cast metal underframe. The casting itself exhibits a high level of integral detail.

The bogie side frames are well detailed in deep relief. The sand pipes are in line with the treads of the blackened wheels.

The model uses the proven mechanism: the frame carries a five-pole can-type motor with precisely balanced flywheels driving gear reduction in each bogie via cardan shafts. All wheels are driven. The model runs smoothly and quietly, and is controllable from walking pace to a reasonable maximum. Two traction tyres are fitted to optimise pulling power (on one inner wheel of each bogie), though with a weight of c.375gr this should not be an issue. The minimum recommended radius is 358mm.

The directionally controlled head and tail lights are achieved with small LEDs. The printed circuit board has a NEM 658 PluX22 socket for a digital decoder; there is also provision for a loudspeaker.

The loco comes with standard model couplers in NEM pockets on close-coupling mounts at both ends. To detail the buffer beams if model couplings are not required, brake hoses and dummy scale screw couplings are supplied, along with end beams without an opening, all moulded in black plastic.

Another excellent example of Piko's ability to provide a top quality product for a fringe market.



Manufactured by

Piko Spielwaren GmbH,
Lutherstraße 30,
D-96515 Sonneberg, Germany.
www.piko.de

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE ref.52840 £215.00.



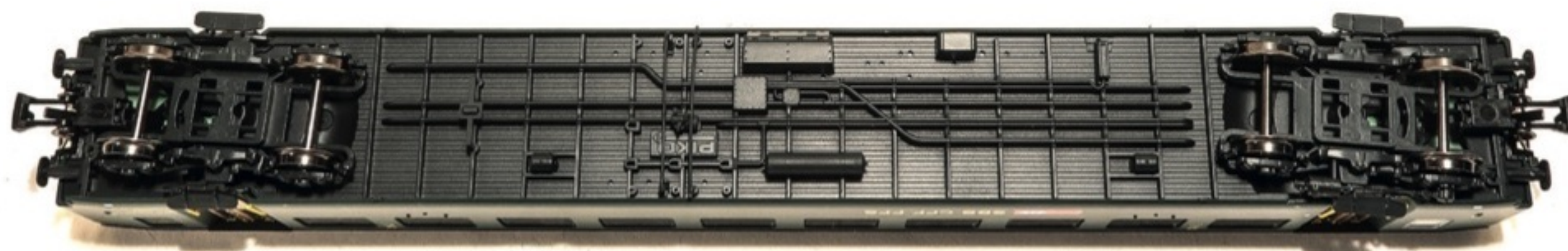
New in the Expert range from Piko are models of the first design of standard bogie coaches or *Einheitswagen* ordered by the Schweizerische Bundesbahn.

They were built between 1956 and 1967 with a length of 23.7m and a maximum speed of 140km/h.

So that the entrances could be above the bogie, the bogie frame had to be cranked in the middle. The primary suspension (between wheel and bogie frame) is by coil springs; the secondary suspension (between bogie frame and car body) came in two variants: coil springs, based on a design by SWS Schlieren or torsion rods developed by SIG Neuhausen, used on the first class coaches.

1,028 second class coaches were built in twelve years.

The seats originally had brown imitation leather upholstery. In the 1970s update they got green seats in non-smoking compartments and red in smoking sections, like the EWII. Also the original bellows corridor connections were replaced by rubber and the incandescent lighting by fluorescent tubes. In addition, the coaches were fitted with UIC door and lighting control systems. Vehicles equipped with multiple unit control cables for push-pull working were numbered over 500. From the outset, these cars had doors remotely controlled by the driver. There were also controls next to the doors for the guard to give the all-clear to the driver.



Later modifications included the original axle-driven generators and charge controllers being replaced by electronic battery chargers, supplied from the train busbar.

The recessed windows are one-piece, single glazed, and frameless; they sink up to half way into the coach side.

These coaches were used principally for regional and local services. They were also supplied to other railways in Switzerland, and some are still in use, having been modernised and updated for push-pull work.

The new model of the EWI second class coach is available in a twin pack, naturally with two different running numbers, 50 85 20 33 533-4 and 50 85 20 33 582-1, with the later SBB-CFF-FFS logo and lettering (Epoch IV).

The finish is excellent, with well-defined 'gold' doors, handles picked out in silver, and clearly printed markings.

The scale length body has some features moulded integrally – e.g. roof ribs, rain strips, destination board mounting brackets, and door handles, while the rubber corridor connections with fall plates, the short handrails in the corridor and the long ones by the doors, the roof vents, and the buffers are added parts.

The only separate detail parts supplied for the modeller to add are fine black

plastic electrical cables and silver destination boards for the coach sides.

The glazing is slightly recessed, as per prototype, with opening grips represented, and there is an interior, with the seats in different colours in each half of the vehicle.

An optional internal lighting kit is available (ref.56286); the circuit board can accept an accessory decoder.

The underframe has major components (battery boxes, tanks, air reservoirs, brake cylinder) moulded in place with brake controls and pipework added.

The bogie side frames are in deep relief, with well detailed axleboxes and suspension components; the dynamo on one bogie is an added part. The foot-steps are mounted on the bogies so as not to restrict their rotation. Brake shoes are in line with the wheel treads.

The models roll freely on blackened metal disc wheels on pinpoint metal axles, insulated one side by a plastic centre bush.

The models should cope with R2 (358mm) minimum radius curves.

Standard couplers are provided in NEM pockets on close-coupling mounts.

The coaches are packed in separate boxes held together with an outer card wrapper; there is only one instruction sheet for the pair.



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www.piko.de

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PRICE ref.96782 £130.00.





Piko has recently released a new version the successful model of the BR82 0-10-0T. This was the first of the Deutsche Bundesbahn so-called *Neubaudampflokomotiven* or newly designed steam locomotives.

They were built by Krupp and Henschel in 1950 and 1951 and also by Maschinenfabrik Esslingen in 1955.

41 were built, intended to replace older machines for heavy shunting and trip working, but were soon themselves eclipsed by diesels. They were withdrawn between 1966 and 1972.

They were suitable for lines with tight curves, such as the Hamburg Harbour railway. The last two examples were equipped with Rigenbach counter-pressure brakes, which enabled their operation on steep lines.

They were mainly used in the marshalling yards at Bremen and Hamm as well as on the harbour lines at Emden and Hamburg. They could also be seen on regular services on the steep inclines of the Westerwald and in the Black Forest.

82 008 has been preserved at Neumünster.

The model is 88mm long over buffers and has a plastic moulded boiler with cast metal tanks and cab, accurate in major dimensions. This variant represents a machine with mixing feedwater heater and the angular cab. The body is augmented with added parts such as the generator, whistle, bell, boiler fittings, handrails, ladders, and much of the pipework.

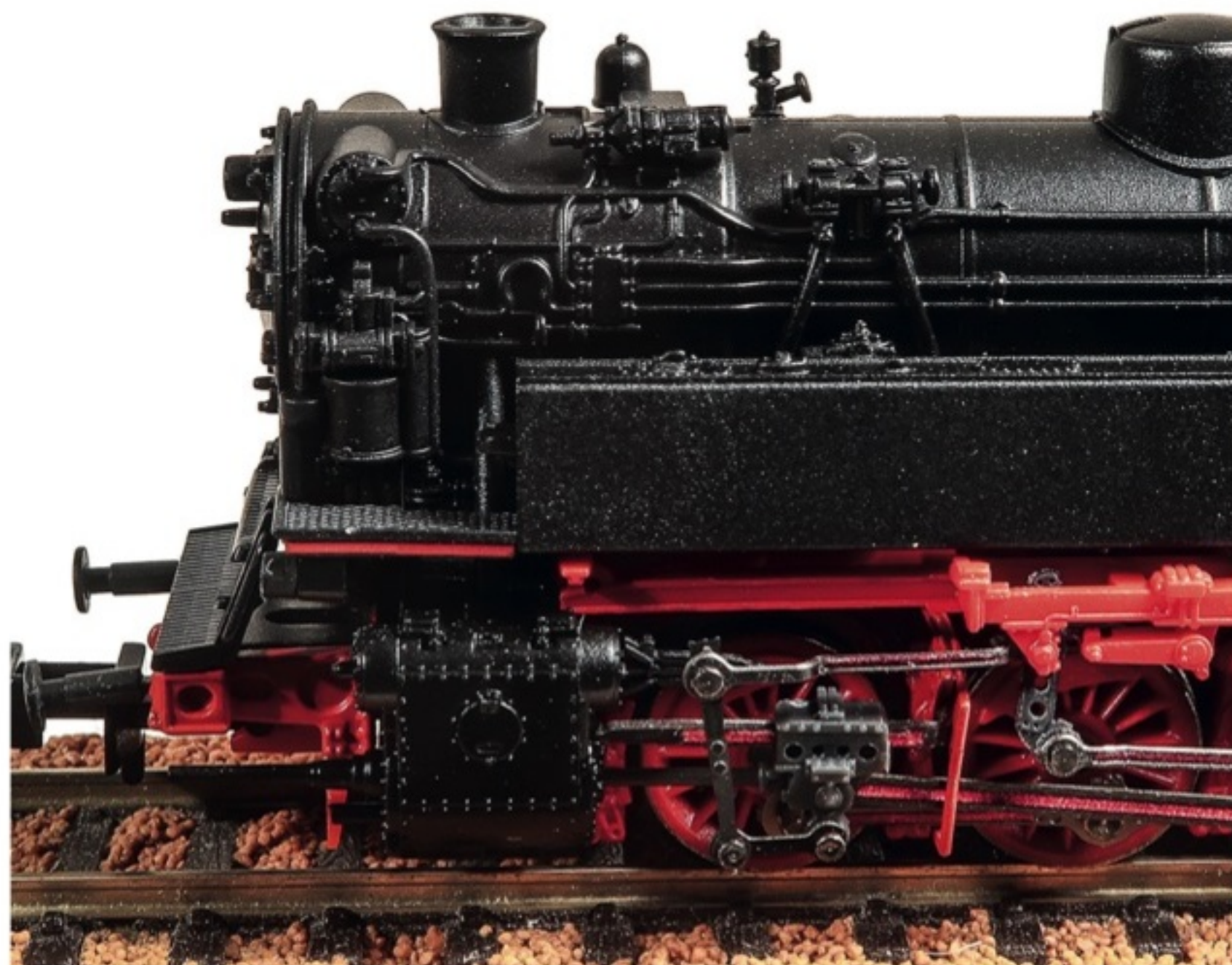


The finish is good, with early Epoch III markings clearly printed. The model represents 82 018, allocated to Wilhelmsburg shed in the Hamburg administrative area. Overhaul date is given as 21.5.51. It has the full Deutsche Bundesbahn plate on the cab side rather than the later 'keks' logo.

The mechanism uses a can-type flywheel-fitted motor in a cast metal frame, driving the fourth axle; the third and second are linked by gears, the outer wheelsets by the coupling rods.

Fine spoked wheels, detailed Heusinger valve gear, and added piston tail rod covers complete the effect.

There is current collection from all wheels via wipers.



There are two traction tyres, one side each on the second and fourth wheelsets, and with a weight of 65gr the model has reasonable pulling power (in line with the original) and runs smoothly, controllable across the range from walking pace to a reasonable maximum.

A minimum radius of 88mm is recommended.

The model has working directionally controlled LED lights.

The printed circuit board has a Next-18 socket for a digital decoder. A digital sound version is also offered, with a loudspeaker specially adapted for this model.

Standard couplers are fitted in NEM pockets, flexibly mounted on the frame.

Overall, another very nice little model.

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www.piko.de

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PRICE ref.40104 £245.00.

Deutsche Bundesbahn type Bpmz coach new from Piko



The range of large scale stock by Piko has been augmented with a new model of a Deutsche Bundesbahn 26.4m second class InterCity open saloon coach, type Bpmz^{291.0}.

Series production of this type began in 1981, using MD52 bogies. In all 540 were built, in five series. They were the first mass-produced second class coaches built for DB with air conditioning, and are still the most numerous type in locomotive hauled long distance trains.

Type Bpmz²⁹¹ defined coaches with steel springs and Bpmz²⁹² those with air suspension. Both were approved for 200km/h.

In common with other Piko large scale models of standard gauge stock, it is not to a precise scale – various dimensions suggest c.1:26; the 817mm length over buffers is close to 1:32, but the model is one window short. (The new items brochure had a modified picture of an HO model, and at the time of writing this was still used on the website.)



The vehicle carries Deutsche Bundesbahn Epoch IV markings, which are very clearly printed, right down to the smallest data. The main colours are even and cleanly separated. Brake operating handles, etc., are nearly picked in appropriate colours.

The plastic body is enhanced with added parts such as the door handrails, corridor connections with fall plates, and buffers.

The interior has seating fitted, and the glazing is flush with frame and surround in beige and black respectively.

An internal lighting kit is available as an optional extra (ref.36132).

The bogie side frames are very impressive, with axleboxes and suspension components very nicely represented.

The underframe features brake equipment moulded in medium relief.

The vehicle is made in weatherproof materials so is suitable for use outdoors as well as inside.

The model runs freely on plastic wheels on metal axles.

Regular large scale couplings are fitted, mounted on the bogies. The model

has been designed to cope with 600mm minimum radius curves.

For its sheer size, level of detail, and standard of finish, this is impressive.

A type Avmz first class coach is also planned.

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PRICE ref.37660 £280.00.

industrial 'skip' wagons new from Minitrains



Further to the note in the News section of our July issue, Minitrains have recently delivered their brand new models of a

typical industrial steel-bodied V tipper 'skip' wagon. We understand these are now being produced in Germany.



The wagons use the standard four-wheel steel channel underframe, moulded in black plastic.

The body is another one-piece moulding, in grey plastic, which simply clips into place on the supports. There is no provision for making it tip.

The braked variant has a slightly longer frame, which carries a 'wooden' deck and the end railing with brake handle.

The wagons run freely on blackened metal wheels on metal pinpoint axles, insulated both sides by plastic centre bushes.

They have the standard narrow gauge centre buffer-coupler with hook, formed as part of the frame, with wire loops at both ends.

They are supplied in packs of four, two braked and two unbraked.

Manufactured by

Standard Light GmbH,
Werftstraße 12,
D-76189 Karlsruhe, Germany.
www.minitrains.eu

SAMPLE SUPPLIED BY
Mount Tabor Models,
Scarthin, Cromford,
Matlock, Derbyshire, DE4 3QF.
www.mount-tabor-models.co.uk

PRICE £33.35 (pack of four).



The Union Pacific 'Big Boy' simple articulated 4-8-8-4 steam locomotives were manufactured by the American Locomotive Company, two groups of ten in 1941 and one group of five in 1944, and operated in revenue service until 1959. They were built to haul heavy freight trains more efficiently over the long steep gradients through the Wasatch mountains between Ogden, Utah, and Green River, Wyoming. In the late 1940s, they were re-assigned to Cheyenne, Wyoming, where they

worked over Sherman Hill to Laramie, Wyoming. The type was originally due to be called the 'Wasatch' but acquired the nickname after an unknown factory worker scrawled "Big Boy" in chalk on the front of the class leader during construction.

They were the only locomotives to use the 4-8-8-4 wheel arrangement. They were articulated, like the Mallet design, but were not compounds. The loco was carefully designed not to exceed an axle load of 67,800lbs (33.9 short tons).

They were built with a wide margin of reliability and safety, and normally operated well below 60mph in freight service even though designed to run smoothly and safely at 80mph. Peak horsepower was reached at about 41mph, and the maximum drawbar pull measured during 1943 tests was 138,000lbs while starting a train with a factor of adhesion of 4.0. They were capable machines; their rated hauling tonnage was increased several times over the years. They were held in high regard by crews, who found them

sure-footed and more 'user friendly' than other types. But post-war increases in the price of coal and labour, along with the advent of efficient, cost-effective diesel-electrics, spelled the end of their operational lives. Nonetheless, they were among the last steam locos to be withdrawn from service on the UP. The last revenue train hauled by a 'Big Boy' ended its run early in the morning of 21st July 1959. Most were stored operational until 1961 and four remained in working condition at Green River until 1962.

Remarkably, eight survive, most on static display at museums but #4014 was re-acquired by the UP in 2013 to be restored to operating condition in time for the planned celebration of the 150th anniversary of the completion of the first transcontinental railroad in 2019. The 1,912 miles of continuous line was constructed between 1863 and 1869 and connected the existing eastern network with the Pacific coast.

The restoration (which included successful conversion to oil firing) was completed in May 2019 and #4014 made its first runs since 1959 that same month, allowing it to regain the title as the largest and most powerful operating steam loco in the world after 60 years. It has been seen touring the mid-west since then.





To coincide with the restoration and mark the 150th anniversary of the completion of the Pacific railroad, Hornby International have released a special edition of the recently improved Rivarossi model of the 'Big Boy' as #4014.

With a length of 475mm (almost 19") this is indeed an impressive model.

The moulded plastic body shells have some features formed in place plus a host of separately-applied details, including steel wire handrails.

Other added components include the coupler cut bars, number plate, cooling coils, headlight, marker lamps, bell, number boards, whistle, generator, flex-

ible steam pipes, air reservoirs, tool box, and tender deck access ladders.

The only parts supplied for the modeller to fit are handrails for the back of the cab, and brake shoes for the rear drivers of the front unit (if clearance to the second pair of cylinder allows).

Most of the motion is metal, with a plastic crosshead plus piston rod and motion bracket.

The tender has a coal bunker; note that the restored machine is now oil-fired.

The model has been in the range for many years, but was substantially upgraded not long ago. The drive was

completely redesigned: the chassis is articulated, and the model runs smoothly and is controllable across the speed range, with a sensible maximum.

The motor, now five-pole and with a brass flywheel, is no longer in the cab, allowing the provision of interior backhead detail. (No crew figures are included, but the back of the cab is open and it would be easy to add them.)

The smokebox and lower firebox are now painted silver (previously black); the boiler handrails are now black, not silver; the whistle and safety valves on top of the boiler are now 'brass', not black. The printing on the cab side under the number is better.

A pair of driving wheels without traction tyres is supplied as an alternative to the third axle of the first unit.

A 21-pin socket for a digital decoder is incorporated in the boiler.

Current collection is by wipers on the backs of all driving wheels.

There is a directional headlight on each end.

The loco – tender drawbar has three coupling positions.

The rear 'fixed' axle of the tender is in fact in a swinging truck behind the frames.

The minimum recommended radius is 371mm.

There is a working knuckle coupler on the rear of the tender. A section of the front pilot can be rotated to reveal a dummy knuckle.

To minimise the risk of damage in transit, the loco and tender are in separate plastic cradles designed to hold

them tightly. Both are covered a sheet of very soft, flexible plastic which is trapped between the model and the plastic cradle to eliminate scuffs to the finish from vibration during shipping. Both cradles are packed inside a plastic sleeve that prevents either opening in transit. This sleeve in turn is held securely in a stout card box lined with dense foam to ensure no shipping damage. The only disadvantage is that the box is over 32" (802mm) long and so requires a lot of shelf space!

The special anniversary model is supplied with a pin badge and a large cast metal number plate (4" tall x 3.5" wide, 100mm x 90mm), with a black wire display stand: this seems to be intended for wall hanging as the low point of the shield projects below the base, preventing it standing on a flat surface.

Dwarfing almost all other locomotives, the 'Big Boy' is iconic and transcends American, British, and European modelling boundaries. This is without doubt a classic, though it is a little odd that such a prestige special edition is not available factory-fitted with digital sound.

Manufactured by

Rivarossi (Hornby Hobbies Ltd.),
Enterprise Road,
Westwood Industrial Estate,
Margate, Kent, CT9 4JX.
www.hornby.com

PRICE ref.HR2753 £384.99.



Das Natur-Talent

by Marcel Ackle

VerlagsGruppe Bahn GmbH,
Am Fohlenhof 9a,
D-82256 Fürstenfeldbruck, Germany.
www.vgbahn.de
www.klartext-verlag.de
295mm x 245mm 208pp
Hardback €39.95
ISBN 978-3-8375-1963-1
German text ref.581801

VGB have once again joined forces with Klartext to offer a large format hardback book which showcases the work of an extraordinary modeller, based on magazine articles but taking things much further.

Marcel Ackle practices modelling on another level. The title could be ambiguous: natural talent, or a talent for reproducing nature? Both!

In this book he shows how his models and dioramas are made. Rather than making a secret of his art, he shows his techniques step-by-step, and they are often surprisingly conventional. He mostly likes to use just a few basic materials, and his tool kit is simple and comprehensible.

This is a useful guide for modellers as he explains that perfection is not the result of magic but above all has more to do with observation, planning, preparation, and patience. Up to a thousand hours goes into each miniature masterpiece, and anyone who has seen them can well believe it. But he does not call it work – modelling should be a pleasure, and relaxing.

He began as a conventional railway modeller, but soon discovered he was more interested in the process of building than in operating a 'finished' layout.

It is no surprise to learn that he is an architect by profession, hence the emphasis on planning, and working from drawings.

He gets ideas (and indeed some raw materials) while walking in the countryside. He was drawn to industrial and suburban settings in which the railway is incidental, and indeed not featured at all in some of his most recent projects.

He first began to appear at model railway exhibitions about ten years ago, initially with the *Rymenzbuger Chnollenbahn* (see CM November 2010). His small displays – no more than a couple of square metres – were often surrounded by viewers, who could not get enough of his highly detailed photo-realistic three-dimensional works of art, very carefully presented with a surround and controlled lighting.

He works in large scales – initially 1:22.5 and now 1:35 – on relatively small scenes, with a very high level of detail. Almost everything is scratchbuilt.

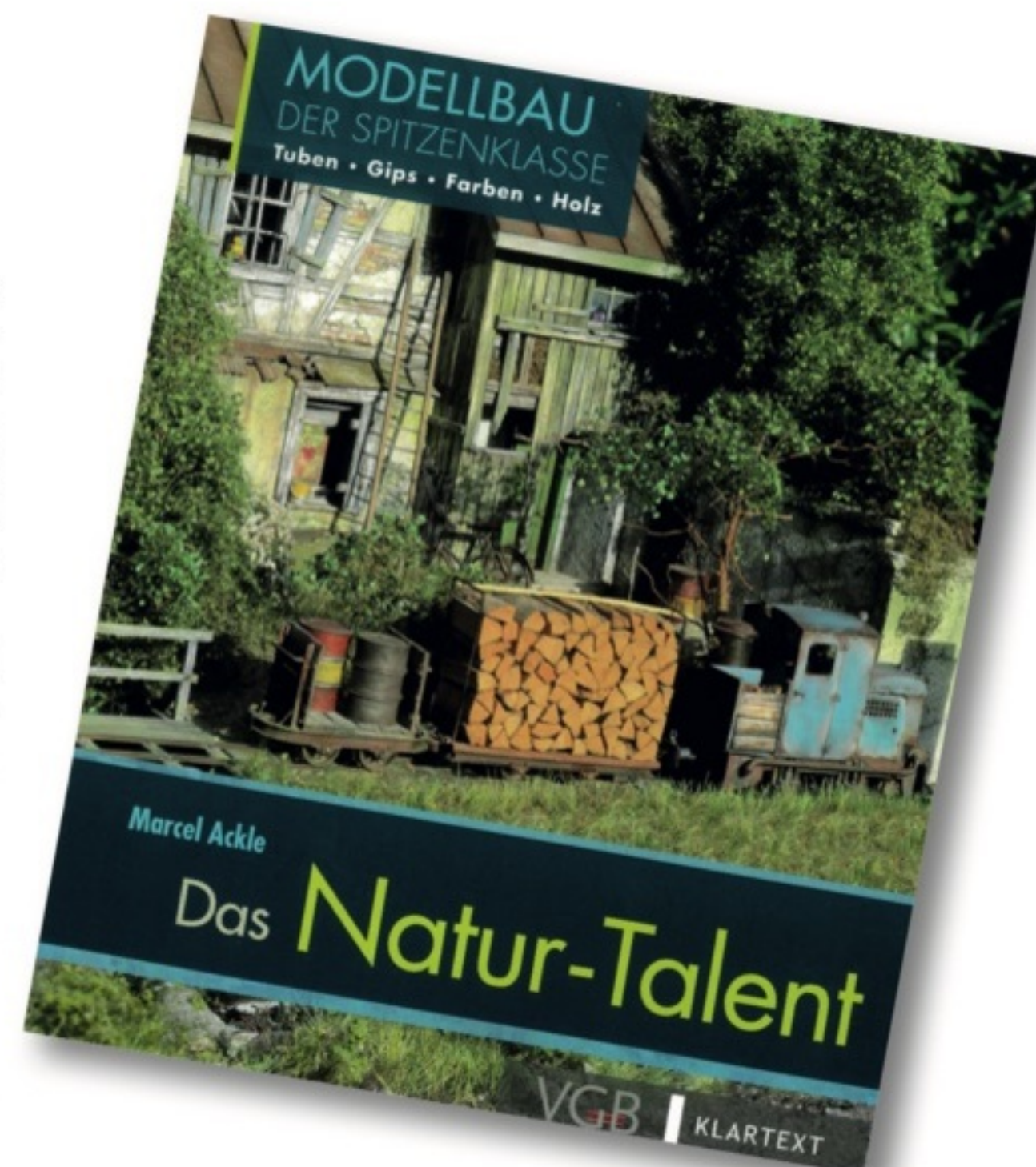
After summarising his approach and philosophy of modelling, and underlining the importance of working from a drawing, the book is in four main sections, reflecting his main projects to date: the *Chnollenbahn*, *Die Feldbahn im Bild*, *Das Hühnerparadies* (literally chicken heaven, a ramshackle structure past which a *Feldbahn* trundles, examined to the last brick and board), and *Blau ist der Regen*, based around a group of small shops, Galerie Leonardo, a cobbler, and a bric-a-brac, displayed as an adjunct to the chicken shed scene but complete in its own right.

Aspects of the *Chnollenbahn* include the shed, the outhouse, the store, the sandstone wall, the mushroom tunnel, the mushroom store shed, the oil drums, the diesel fuel point, and the rolling stock (loco and wagons).

The *Feldbahn* section includes the half relief building, the garden door, Melanie's house, the red house, the rock cellar with brick arched door, and a special feature on weathering wood.

The work is illustrated with over seven hundred and fifty photos, of superb quality, and all by the modeller himself (bar the portraits in the introduction, we assume!). They reveal every stage of the process, from planning and preparation to the finished scene.

The presentation is appropriately well-ordered: the pages have clearly been professionally designed, with a certain amount of fashionable white space. But the style seems to suit the man and his works. The main text is in fact grey, not black, but the choice of a clear font, and the size and spacing, make it perfectly legible.



Most of the topics are presented with the text and large photos on the top of the page, separated by a coloured band from a procession of smaller step-by-step pictures across the bottom. This works well, and there is enough variety to avoid it becoming formulaic.

Some photos fill a full page, and more – both modelling and image fully justify such treatment. Some views of small details have been enlarged to several times more than the actual model: the finish and detail is hyper-realistic, and paradoxically to such an extent that we know it must be a model.

Ackle makes it look easy, claiming it is a matter of patience and practice, but that is to deny his own skill and art, which we know is exceptional; nevertheless this should inspire you to try and improve your own work. Do not let the German text deter you: the illustrations alone speak volumes.

Can modelling be art? On this evidence, most certainly. And the book ends with a page advertising similar books about other equally elevated and legendary contemporary modellers – Josef Brandl, Willy Kosak, Jörg Chocolaty, and Emmanuel Nouaillier.

Loco-Revue Hors Serie 68 Peinture et Patine en modélisme

by Philippe Cousyn

Éditions Loco Revue,
BP 30 104, F-56401 Auray Cedex, France.
www.locorevue.com
297mm x 210mm 84pp
Softback €15.00
ISSN 0024 5739 French text

This new special from the leading French model railway magazine *Loco-Revue*, dated 8/2019, is concerned with painting and weathering models. Some years have passed since they last produced a special devoted to painting, in which time several things have changed. Some brands have gone, others have appeared. Some types of paint have been withdrawn, for environmental reasons. Spray cans have become more widely used. The increase in the availability of ready-to-run models finished to a high standard has reduced the need for repainting, and fewer kits are being built which require hours of careful work in the paint shop. On the other hand, more modellers are now interested in weathering structures and stock to achieve even greater realism.

This useful book contains thirty-seven topics, of between one and six pages. Some are relatively simple, but many are rather more advanced. All encourage activity.

They are presented in four main sections – paints, tools, practical techniques, and weathering.

After some basic general remarks, the first section looks at types of paints, different brands, thinners and removers, primers and fillers, varnish, paints for scenery, and aerosols.

The tools section deals with brushes and rollers; airbrush, compressor, and sand blaster; safety precautions (mask, gloves, and air filter); various supports and means of holding small parts to be painted; a spray booth; and various other small aids.

The practical features demonstrate chemical stripping and priming; painting a four-wheel open wagon; masking; using aerosol spray cans; painting the track; painting the backscene; painting stone buildings; painting metal structures; colouring grass; painting roads; painting a river bed; and painting 3D-printed models.

The weathering section first considers some products borrowed from other disciplines, goes on to feature pigments and oils, and finishes with some personal 'recipes' for locomotives, rolling stock, and structures.

The book is rounded off with a glossary of the main specialist terms used.

The presentation is in the usual clear style, generally straightforward (bar the affectation of titles in two fonts), with large illustrations. There are occasional tinted panels listing the tools and materials required for a topic, tips on orange circles, and pull quotes (more or less significant sentences repeated in large coloured type for emphasis).

Some of the points are illustrated with an appropriate photo from the *Loco-Revue* archive, while many others are step-by-step sequences created for the purpose. The photos are all in colour, well reproduced on good quality glossy paper.

A reasonable knowledge of French will be needed to get the full benefit but the high visual content – and the glossary – will help. While the examples are mostly (but not all) French models, the techniques are general.

The book includes three pages of advertisements for appropriate products and other LR Presse publications.



Le Train – spécial 98 2/2019 Strasbourg – Lyon

by Pascal Bejui

Editions Publitrain eurl,
CS80010, F-67660 Betschdorf, France.

www.letrain.com

295mm x 210mm 100pp

Softback €16.50

ISSN 1267-5008 French text

This new special from *Le Train* explores the route from Strasbourg to Lyon via Mulhouse, Besançon, and Bourg-en-Bresse, as well as the Rhine – Rhône high speed line (LGV), effectively following the eastern border of France, neighbouring Germany and Switzerland.

It took almost a quarter of a century to complete a link of around 500km. Like most of the major cross-country routes in France, it was built up from small separate sections, promoted and constructed by different independent companies with varying aims. The section from Strasbourg to Mulhouse formed part of the international link to Basel, while the Lyon – Bourg-en-Bresse stretch was integral to the route on to Geneva. Parts of the region covered have been under German influence and control for large periods of the railway age, and in some places this can be seen in the infrastructure.

The coverage proceeds more or less north to south, and moves forward through time. There is a

section devoted to the Dannemarie viaduct (between Mulhouse and Belfort). Other distinct sections feature international trains, the RTG turbo trains, the return to loco-hauled trains, and the high speed line. Motive power develops from steam through diesel to mostly electrified. The great majority of the services shown are passenger.

The publication is built around a fine collection of 174 photographs, from archive black & white images to a high proportion in colour, all very well reproduced. The earliest colour is from around 1955, while the latest images are dated May 2019.

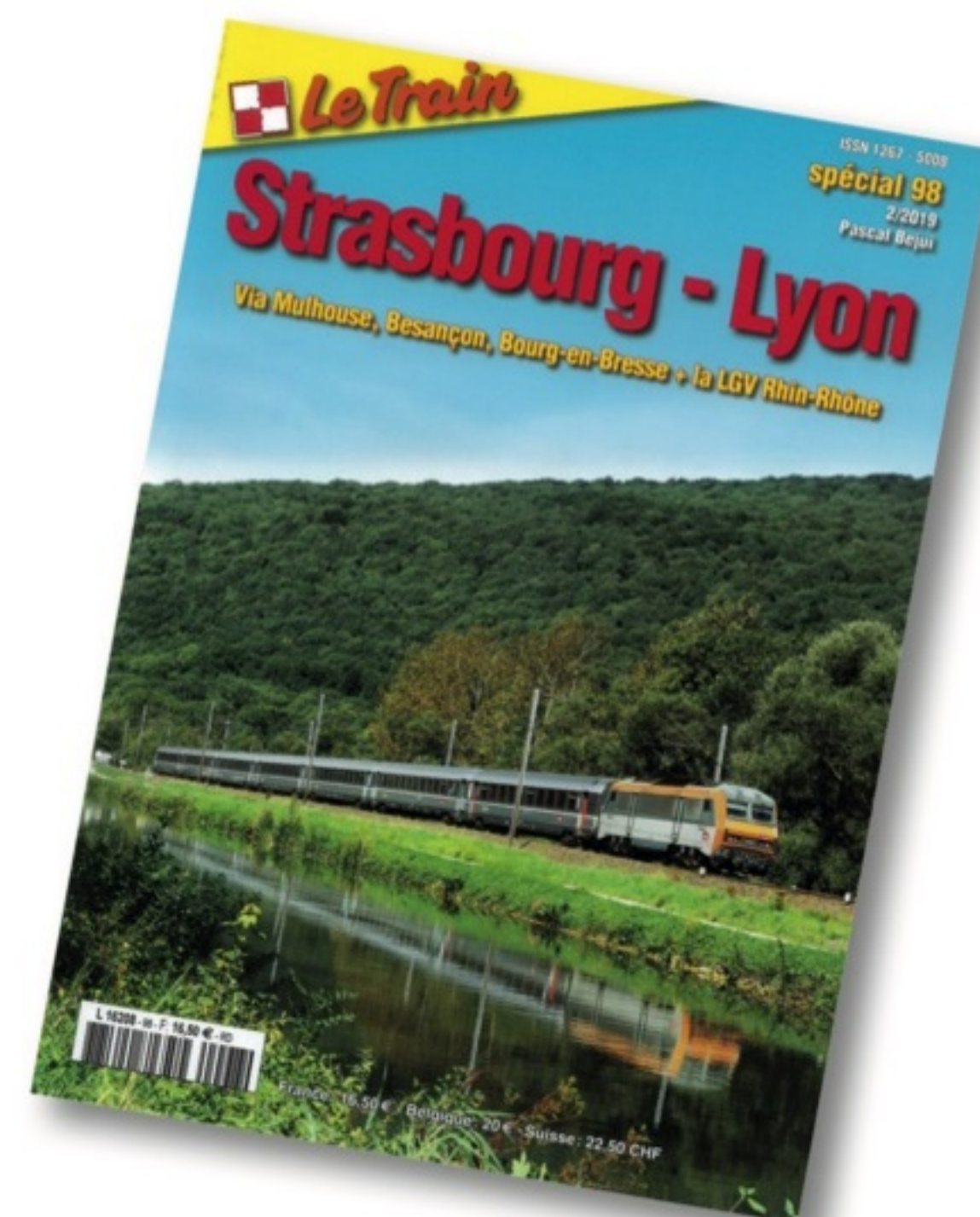
The captions are informative, and mostly placed adjacent to the relevant image, or sometimes over a blank area, of sky, for example. They are numbered in sequence as well as being prefaced with a direction (above, below, etc.).

The page design is straightforward, in the usual style, with just a subtle drop shadow in the few cases where images overlap, and graphic devices restricted to coloured chapter and section headings, along with frames and tinted background for the information panels to aid clarity.

The work is rounded off with two maps, each filling a page, in simple style but clearly showing the route. The coverage overlaps slightly.

There are seven pages of advertisements for other *Le Train* publications.

The publication is perfect bound, using high quality glossy paper between heavier varnished covers.



Some knowledge of French will be required to get the full benefit of the text, but the captions and tabular information should be clear enough, and the publication can be appreciated for the illustrations.

Journal No.43 Summer 2019

British Overseas Railways Historical Trust
West Greenwich Community & Arts Centre,
141, Greenwich High Road, London, SE10 8JA.
www.borht.org.uk

295mm x 210mm 16pp

Softback £2.50

ISSN 0952-5483

The latest issue of the BORHT *Journal* contains two major articles, two book reviews, and three obituaries.

The first article deals with researching the railways of Mauritius, a very British equipped standard gauge enterprise transported to the tropics, and recounts the different experiences of Dick Walker and Stephen Spark who met through their common interest in the island. We look forward to the eventual results if their combined efforts.

The second, shorter, article records a visit to Argentina in October and November 2017 by Professor Paddy Farrell, with numerous observations of the current railway scene as experienced on his travels.

One of the purposes of the trip was to attend the launch of a new book, *Historia de Ferrocarril Central Argentino (1854 – 1901)* by Carlos A.F.Priotti, and the

article is followed by a detailed review of this 586 page Spanish language work.

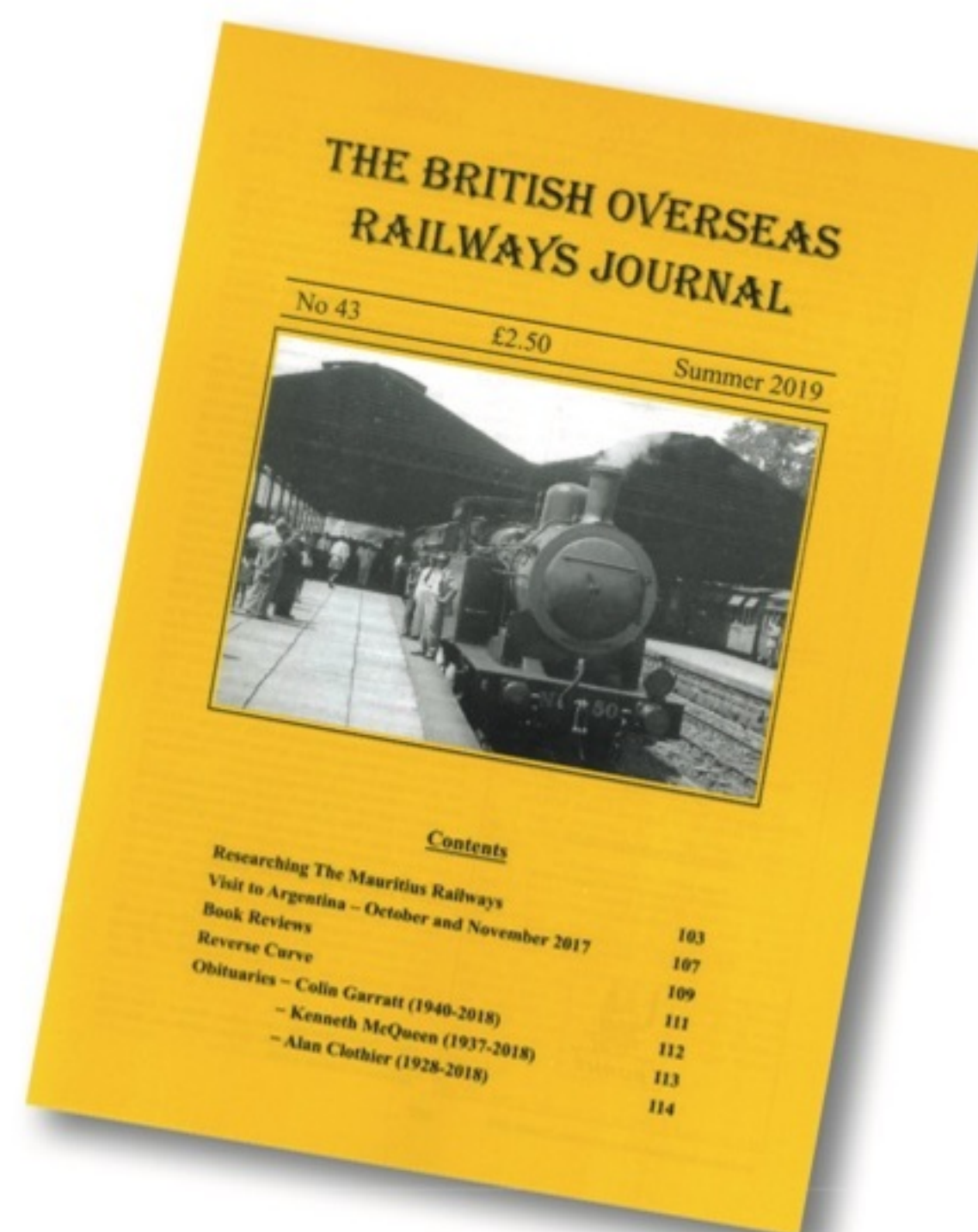
The other book reviewed is *Industrial Railways and Locomotives of Greece*, by Chris Down, recently published by the Industrial Railway Society. Despite running to 320 pages, the book does not claim to be definitive!

The obituaries are for Colin Garratt (1940 – 2018), the well-known railway photographer; Kenneth McQueen (1937 – 2018), an electrical engineer by trade with considerable experience of railways all over the world, who after retirement served as Treasurer and Trustee of BORHT; and Alan Clothier (1928 – 2018), trained as a draughtsman with the GWR who developed a particular interest in the railways of Egypt (among other things) and the overseas work of Robert Stephenson and whose collection of historical material has been donated to the Trust.

The text is augmented with occasional footnotes, appropriately in the manner of an academic journal, literally at the bottom of each page; this edition has fewer than usual.

The work is illustrated with one map and eleven black & white photographs, all reproduced well if rather small.

The price quoted above includes postage & packing in the UK, and surface post worldwide.



East German Narrow Gauge

by Philip Horton

The Nostalgia Collection
from Silver Link Publishing Ltd.,
The Trundle, Ringstead Road, Great Addington,
Kettering, Northamptonshire, NN14 4BW.

www.nostalgiacollection.com

168mm x 236mm 64pp

Softback £8.00

ISBN 978-1-85794-530-0

This landscape format book, fully titled *Railway Recollections and Guide to the East German Narrow Gauge*, does pretty well what it says on the tin (to coin a phrase) within the limitations of one enthusiast's experience – and that is not so limited, as he travelled extensively in the DDR since 1986, and with the benefit of a German-speaking wife.

It is essentially a photo album with detailed and informative captions. There is some introductory text to each section providing historic context, while the main body of the work presents between one large and four small photos per page.

The coverage is in three main sections: the Harz mountains (metre gauge), the Baltic coast (750mm and 900mm), and Saxony (750mm). Accompanying the introduction to each section is a clear map (by Stephen Edge). Specifically, the lines featured are:

1. The Selketalbahn – Gernrode to Stiege and Hasselfelde, and Harzgerode.
2. The Harzquerbahn – Nordhausen to Wernigerode, and the Brocken.
3. The 'Rasender Roland' (Rügenschke Bäderbahn) – Putbus to Göhren.
4. The 'Molli' (Mecklenburgische Bäderbahn) – Bad Doberan to Kühlunsborn West.
5. The Weisseritztalbahn – Freital-Hainsberg to Kurort Kipsdorf.
6. The Lössnitzgrundbahn – Radebeul Ost to Radeburg.
7. The Döllnitzbahn – Oschatz to Mügeln and Kemmlitz.
8. The Zittauer Schmalspurbahnen – Zittau to Kurort Jonsdorf and Kurort Oybin.
9. The Fichtelbergbahn – Cranzahl to Oberwiesenthal.

The collection concludes with an appendix listing selected heritage railways and railway museums in Saxony, correct at summer 2017.

While the presentation is generally straightforward, the section titles are on the background of a station canopy valance, while main chapter titles also have a small cartoon character as part of the heading, which is hardly necessary. Tinted page backgrounds distinguish the sections; sometimes these tints are graded across the spread, for no apparent reason.

The photos are all in colour, and the majority are by the author, taken between 1986 and 2010, with the exception of three slides from 1985 by Adrian Cornish (all on page 12) and five images from Neil Carter (pages 14, 20, 37, 46, and 56) which bring the story up to April/May 2017. Most were taken in reasonable weather, and all are well reproduced on good quality paper. The images would easily have merited a larger format.

The inside covers front and back advertise other volumes in this series: so far this is the only one dealing with overseas railways.

Design quirks aside (and they are not too irritating), this is interesting, a good introduction to a large subject, generally well produced, and must be considered good value.

Die Baureihen E11 und E42

by Jan Kotzanek, Dietmar Schlegel,
und Andreas Stange

Eisenbahn Kurier Verlag,
Lörracher Straße 16, D-79115 Freiburg, Germany.
www.eisenbahn-kurier.de
297mm x 210mm 320pp
Hardback €49.90
ISBN 978-3-8446-6034-0
German text order no.6034

This substantial publication, the latest in the *Eisenbahn Kurier* series of class portraits, is concerned with the first post-war generation of electric locomotives built for the Deutsche Reichsbahn in East Germany.

In the mid-1950s the DR entrusted the Lokomotivbau Elektrotechnische Werke (LEW) of Hennigsdorf with the development of new powerful machines to replace the many and varied older classes on the existing and planned electrified network, then being restored as equipment seized after the war was gradually being returned from the Soviet Union, though generally not in usable condition. Serious consideration was given to a fresh start adopting 25kV/50Hz but although the infrastructure would have been cheaper, the technology of the time made the locomotives more expensive, so the decision was taken to remain with 15kV/16⅔Hz, apart from the isolated Rübeland line.

The principle of bogie locomotives (like the successful E44, E93, and E94) was well established, though with the drawgear on the main frame, not the bogies.

Projekt E200 was the precursor, with a rather more rounded body shell that would not have been out of place in Poland, the Czech Republic, or even Switzerland. The eventual superstructure was much more boxy. The spoked wheels gave them a rather dated appearance which in fact belied the level of their technology.

As a universal mixed traffic loco was not technically feasible at that time, very similar machines were built with different gearing for passenger or freight duties. The first two were delivered for testing in December 1960 and April 1961.

Still thinking of Germany as a whole, they were given the next available class numbers (there being E10

and E41 in the west). After 1970 they became BR211 and BR242.

Between 1962 and 1976, ninety-six E11s were built in three batches and two hundred and ninety-two E42s in four lots. For many years these two classes were the backbone of motive power on electrified lines.

After re-unification of the railways in 1992 they were reclassified 109 (as 111 was already taken) and 142, though they were not destined to last long: all were off the DBAG books by 1997.

This book is the result of new research into previously unavailable records and resources. It presents the background to the design of these locos, clearly and precisely sets out the technical aspects, both mechanical and electrical, and describes the allocations and service careers at individual depots, ordered by administrative area – Berlin, Cottbus, Dresden, Erfurt, Griefswald, Halle, Magdeburg, and Schwerin.

Allocations to depots are recorded, with the service careers of selected locos presented in more detail, as far as known, and there are some timetables and working diagrams.

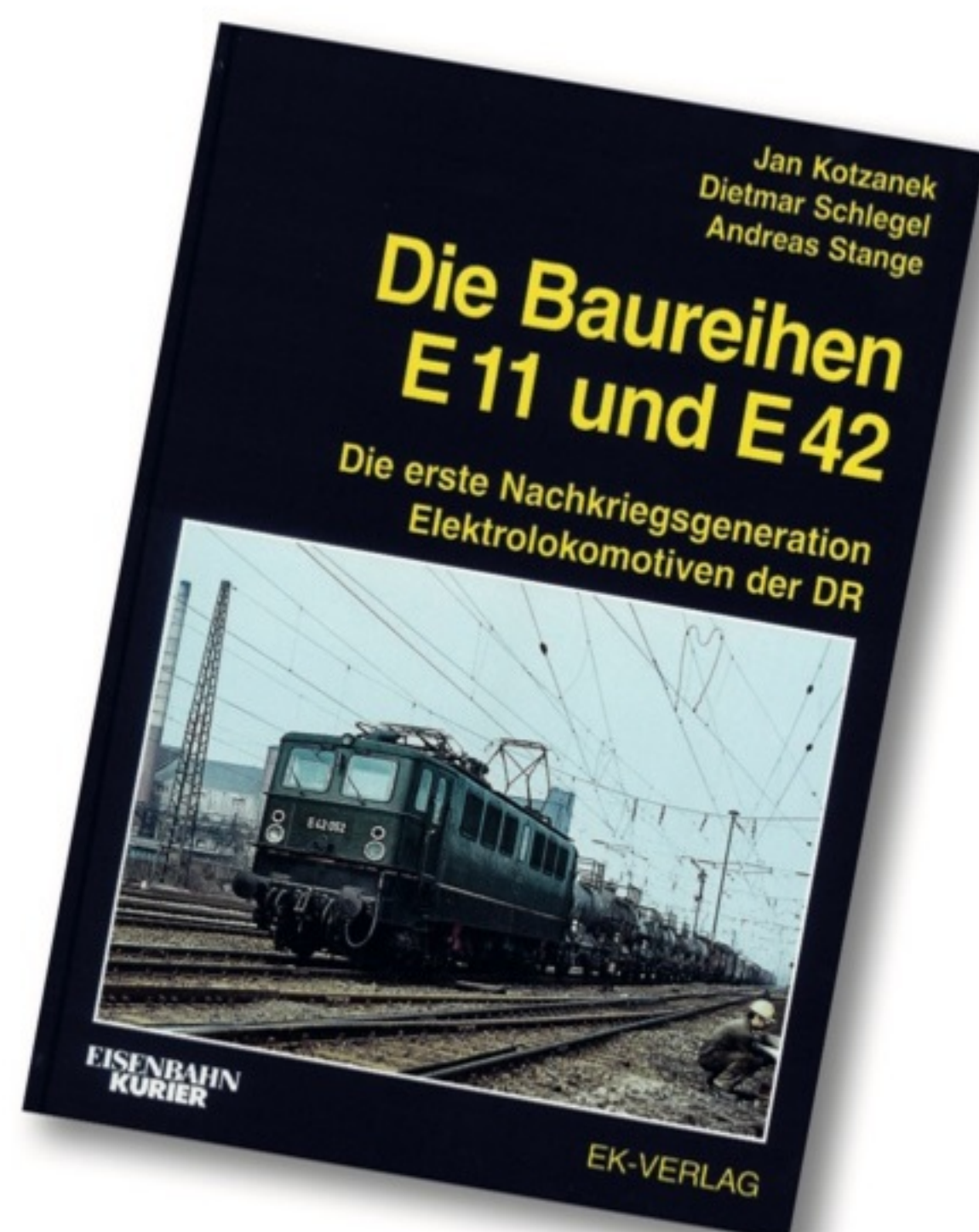
Modifications to the livery, structure, and equipment are carefully noted, and there is a section devoted to Projekt E211, a speculative one-off built with a view to the 25kV market, either in East Germany or abroad.

The last five years of each class get particular attention, and there are special sections on particular duties – on the Tharandt ramp, in commuter service around Halle, and on the Blankenheim ramp.

The units subsequently sold to private operators, some of which went to Switzerland and were modified accordingly, are also recorded. Some of these machines are still in service.

Coverage is rounded off with a list of accidents involving the class.

The work is supported with five hundred illustrations. The photographs are mostly black & white but there is a significant colour selection, on the covers and concentrated in thirty-two pages at the end of the book. Most of the photos are presented large, two or at most three to the page. The earliest colour images are from September 1965, the most recent are from August 2016. The majority are previously unpublished. Photos range from scenes during construction through



classic roster records and close-ups of specific details to views of the locos in service. Some have been drawn from manufacturers' records and official railway sources, others are from enthusiasts – revealing, incidentally, that enthusiasts were active with cameras even in the communist era. The standard of reproduction is excellent, and every image is fully captioned and credited.

The technical sections are enhanced with many drawings and diagrams, which go into considerable detail. One of the authors was involved with the operation and maintenance of these types for many years, and the facts and figures are complemented by practical experience and the occasional anecdote.

The book concludes with a comprehensive list of sources.

Another standard work of reference from EK, of interest and value to both historians and modellers.

Steam and Rail in China

Edited by Iain McCall

Mainline & Maritime,
3, Broadleaze, Upper Seagry,
near Chippenham, SN15 5EY.
www.mainlineandmaritime.co.uk
297mm x 210mm 52pp
Softback £9.95
ISBN 978-1-900340-50-2

This is effectively a themed special issue of *Locomotives International* magazine, consisting of thirteen separate articles by different authors:

The last fires – steam in China in 2018, by Michael Reilly;

Changchun 1983-4, by Rhys Owen;

Locomotives of the Peking – Kalgan Railway, by Alon Sifton, via Rhys Owen, offering maker's photos;

Train Travel in Taiwan (geographically if not presently politically China) by Victor Isaacs, relating a visit in 2017;

Zhongwei, by Keith Chester, with impressive black & white images of QJ 2-10-2s in a rugged mountain setting in 1987;

China's latest rocket – on the rails, by Peter Crush, describing a replica (now itself historic) of the first steam loco built in China (in 1881) now at Tangshan museum;

To China for steam – twenty-five years ago, by J.A.Berry, with images of active and derelict main line and industrial locos;

Guangzhou Iron and Steel Works, visited in December 1985 by John Athersuch and augmented with a couple of 2002 views by Robin Gibbons;

Chinese steam in central Iowa, by Jonathan Smith, noting the new JS class 2-8-2 acquired by the Boone & Scenic Valley Railroad in 1989 along with the two QJ class 2-10-2s imported in 2005 for special duties on the Iowa Interstate, though none of them are currently active;

History reverses itself, by Mike Bent, describing how Skoda exported locos to China between 1927 and 1939, with the aid of archive photos;

Chinese mysteries, four photos of Chinese steam locos from Keith Chester's collection about which further information is sought from readers;

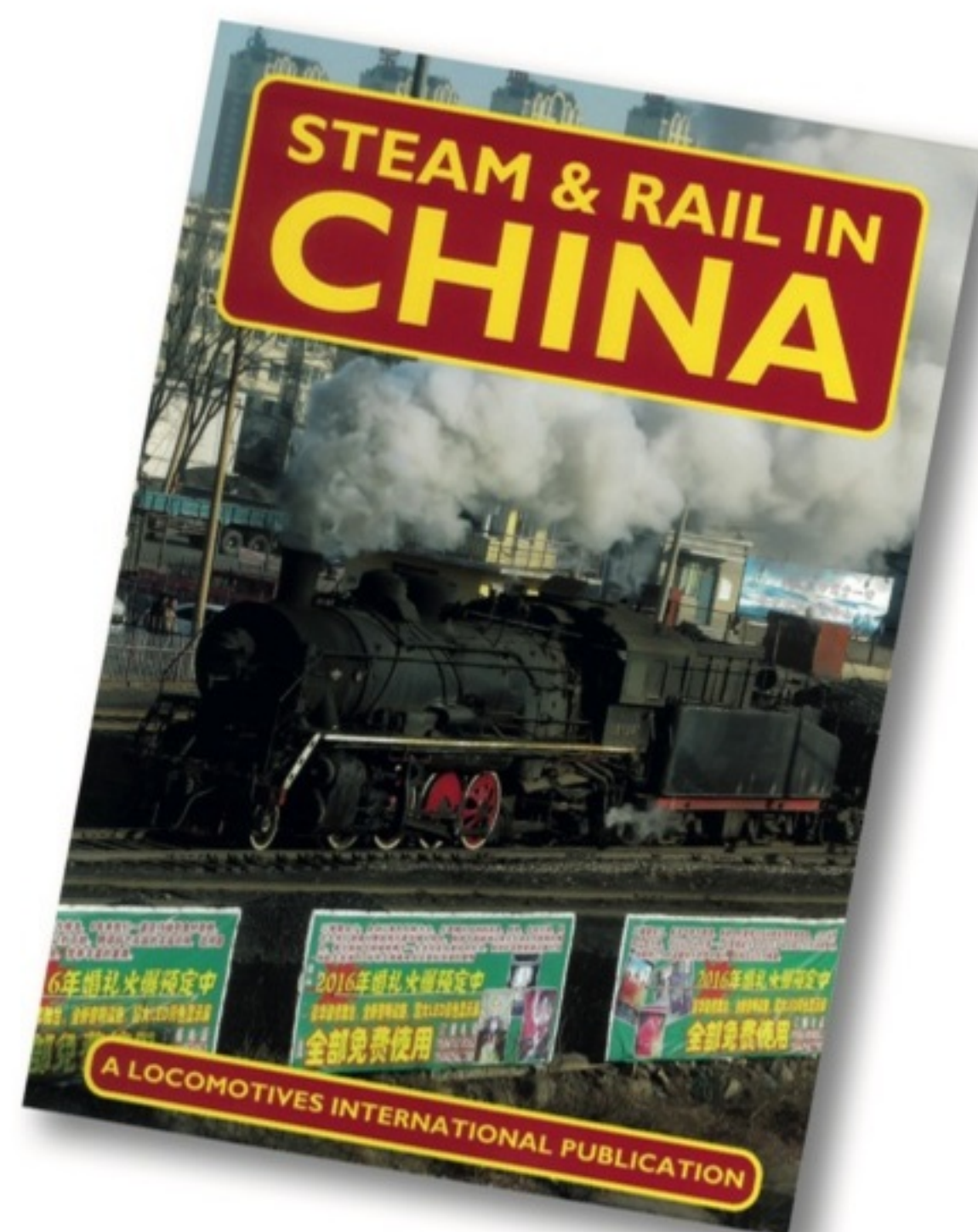
Sandaoling 2017 – the last stand for industrial steam, by Bryan Acford;

Hong Kong tramways, photos by J.A.Berry from 1976, 1991, and 2018.

So, a varied and interesting mixture, with short features of between two and six pages, in the main based around large photos. There are large coloured chapter heading panels, but otherwise the presentation is straightforward, with captions distinguished by a lightly tinted background within a panel.

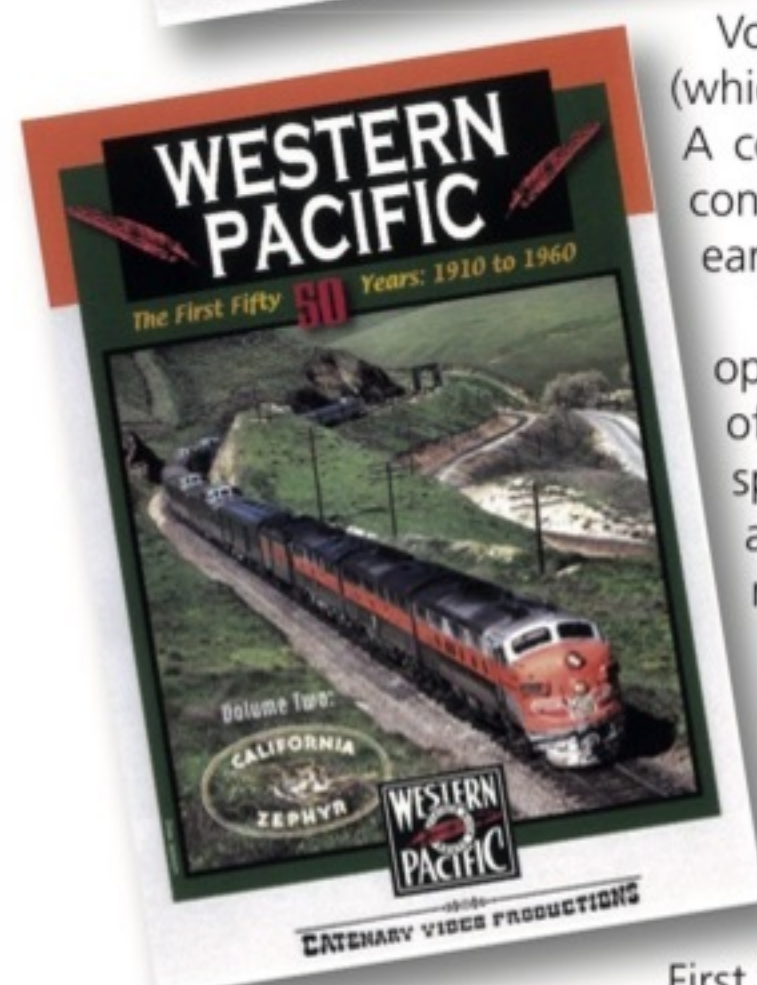
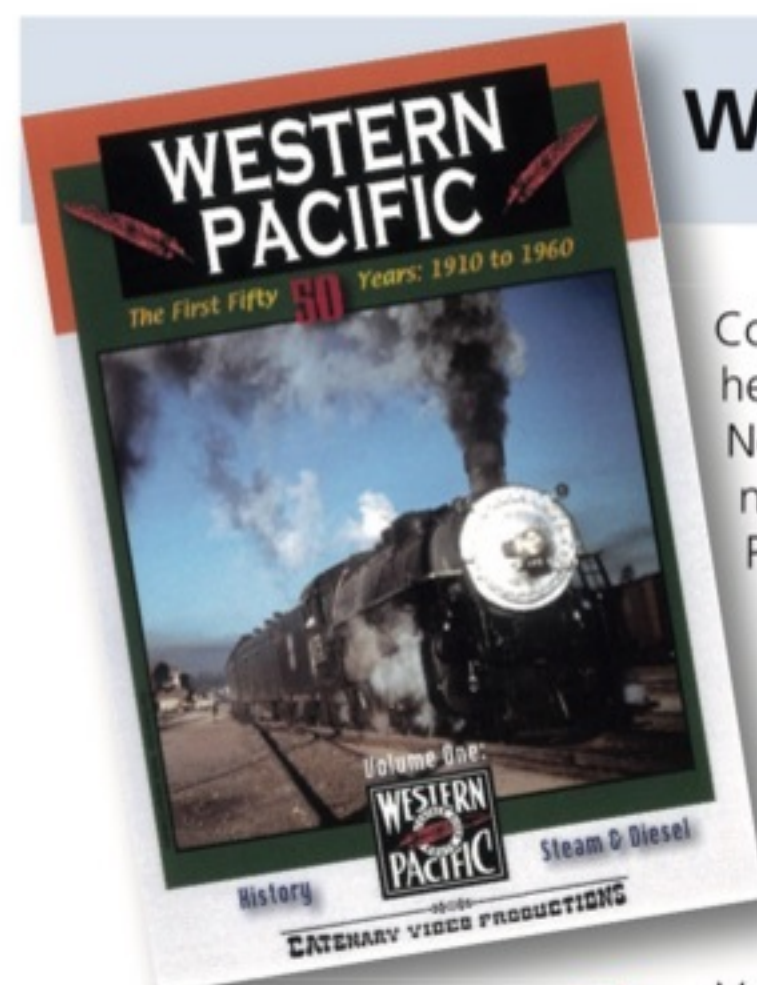
The text is supported with a good number of photos, from archive sources and enthusiasts, ranging from official works portraits to impressive arty shots with steam, smoke, and silhouettes. The majority are in colour, and have been reproduced very well, and generally large, though several of the builders' photos might usefully have been cropped to remove the large blank areas of sky or irrelevant foreground.

The covers use heavier glossy paper while the pages within have a pleasing satin finish which seems to



enhance the images whether black & white or colour.

There are just two pages of advertisements, for *LI* magazine itself and the publisher's recent book on narrow gauge in Taiwan.



Western Pacific – the first fifty years

Catenary Video Productions each c.72mins £18.25

Constructed in the early 1900s, the Western Pacific headed west from Salt Lake City in Utah across Nevada and into California on a route further north than the existing competitors, Southern Pacific and Union Pacific. Although the line was longer, it was more consistently engineered with a ruling gradient of 1% which gave considerable operating advantages.

These two programmes document the history and operation of this undertaking with a remarkable collection of film clips, plus some archive stills.

Volume 1 covers the history, in twelve chapters (which can be selected directly from an initial screen). A comparatively long introduction summarises the contents of both volumes before turning to the early history, with maps and a gradient profile.

There are scenes of the line, equipment, and operations, including the subsequent construction of the northern California extension and its 'last spike' ceremonies, the lean years of the 1930s, and post-war operations (in colour). The street running in Sacramento is a particular feature.

We see all types of WP steam locos, from early small 4-6-0s and 2-8-0s through to the huge 251 class 2-8-8-2 oil-fired articulateds. Steam survived as the railroad recovered from the efforts of wartime, but the future of motive power was clear.

First generation FT diesels arrived in the transition period, and the WP was one of the last major railroads to use them. Experimental diesel colour schemes are noted as they are observed, and we get the unusual sight of an F3 cab unit with a steam loco tender (to supply the steam heating boiler for the train, thus avoiding the need for stops). The arrival of Rail Diesel Cars is also recorded.

Other chapters cover maintenance-of-way equipment, snow clearance, the use of breakdown cranes to clear landslides and wrecks (with some dramatic footage of derailed trains having plunged down steep embankments), a chartered excursion, and even the railroad's maritime fleet, including train ferries, car barges, and tugs. It ends with a section on preserved equipment, some operating, some static in museums.

Naturally most of the footage is from the lineside, though there are some views from trains, including a sequence from the cupola of a caboose on a freight.

Volume 2 concentrates on the *California Zephyr*, the world famous 'Silver Lady', inaugurated in 1949, with an emphasis on the interior amenities and service aboard

the dome cars, dining cars, and Pullmans which represented the epitome of luxury rail travel in America.

In ten chapters we follow the background to the introduction of the train, from the *Exposition Flyer* through the *Train of Tomorrow* and the *Burlington Zephyr*. Much of the high quality film of the interior is presumably official publicity material, and we also see contemporary advertising posters and flyers.

The train featured four dome cars at the head end (after the baggage) plus a dome observation at the tail; the consist was remarkably consistent, with odd cars only seen occasionally.

The route is explored in three sections: San Francisco – Stockton, Stockton – Portola, and Portola – Salt Lake City, punctuated by interviews with employees. There is a fair amount of footage from the cab and the domes (some of the latter clearly amateur, to judge by the shakiness!), and even some aerial views.

But there is more to the programme than the CZ: a short section shows the *Royal Gorge* train (made up of F3 cab unit, water tender, combine, and coach – surely ideal for modelling!) and the *Zephyrette* service which replaced it, using Budd RDCs.

We also witness the fiftieth anniversary celebrations in 1960 to mark the 1910 entry into Oakland, with ten-wheeler #94 working an excursion out to Niles and returning at the head of the CZ, powered as usual by F3A+B+B. On one street running section the train is preceded by a VW Beetle with a photographer filming from the sun roof!

There is a miscellaneous album chapter with short clips of early diesel freights, a number of detours ("foreign" equipment on WP metals, and vice versa, due to incidents). Also provided is coverage of short line subsidiaries Sacramento Northern, Tidewater Southern, and California Central Traction, with old maps to show their routes, plus 1939 colour footage of Stockton Electric's Birney trams.

A few clips are repeated from Volume 1, in appropriate contexts.

In both volumes, the film clips are generally of a good length, and the picture quality ranges from acceptable considering its age to remarkably good. The old film is presented with black borders each side to match the 16:9 widescreen format of the DVD.

Each section is prefaced with brief introductory text on screen, and sub-titles identify the locations and other items of interest, while maps (animated in the case of the CZ) appear at intervals to show the route.

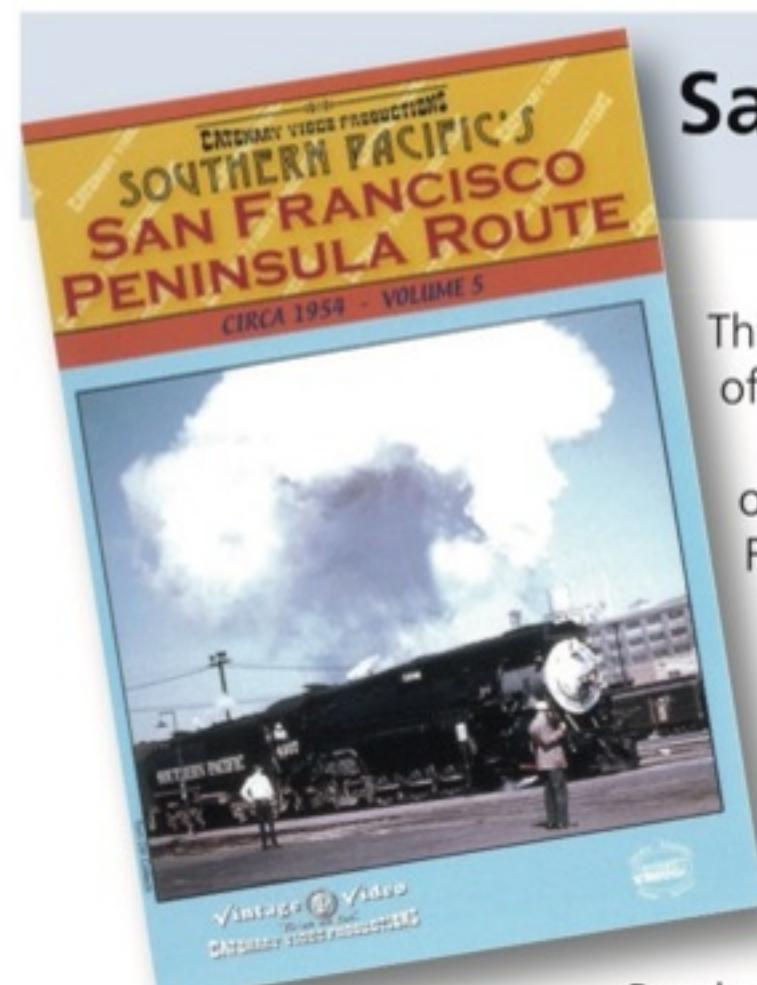
The soundtrack is in stereo, with dubbed effects that are quite convincing – indeed, so much so that the contrast between the visual and the audible can be disconcerting.

The commentary is sparing but informative when present, and clearly delivered.

There is almost constant use of backing music, sometimes at almost subliminal level.

This vintage material was originally brought together by Don Olsen in 1992, with the help of Guy Dunscomb and Ted Wurm, and the revised production has been directed by Warren Haack, with valuable contributions from historians Ted Benson, Wayne Monger, and Evan Werkema, plus anecdotes from retired WP employee Jack Starr adding an authentic perspective to Volume 1.

These DVDs are available from Camden Miniature Steam Services, Barrow Farm, Rode, Frome, Somerset, BA11 6PS. www.camdenmin.co.uk



San Francisco Peninsular Route

Catenary Video Productions c.58mins £18.25

This is the fifth volume of the producer's coverage of the Southern Pacific in the mid-1950s.

As steam locomotives were replaced by diesels on the SP, they were relegated to the San Francisco peninsula to work out their final years in commuter service and local trip freights.

Although there is no direct selection from an initial screen, the material is in fact presented in marked chapters.

They describe the history of the San Francisco & San Jose Railroad, largely with stills; construction of the Bayshore cut-off; the

Bayshore yard and shops, built on land reclaimed using rubble from the 1906 earthquake; the Mission Bay yard and shops, with views of the smart *Coast Daylight* passenger service.

Attention then turns to the terminal at Third and Townsend, and the intensive parade of rush hour commuter trains, at the peak one every three minutes, long heavy consists powered by large locos such as 2400 class Pacifics and 4400 class Mountains, as well as GS4 4-8-4s displaced from the *Daylight* and deprived of the bright livery.

After exploring the route to San Jose, we examine the ghosts of the South Pacific Coast, originally built to 3' gauge and progressively converted to standard after acquisition by the SP.

Next comes the Santa Cruz branch (also built as 3' gauge) and the so-called 'sun tan' specials which conveyed thousands directly to a terminus by the beach.

The Los Altos and Los Gatos branches are also shown, and there is a special to mark the retirement of a long-serving senior executive, hauled by a dressed-up historic 4-6-0. The next section shows other specials for sporting events, notably football matches, and we then look at the branches south of San Jose, including to Hollister and Monterey.

The next major location is Watsonville Junction, a major division point, and then we are shown some of the yards and shops in the South Bay area, including Lenzen Avenue roundhouse, Cahill Street depot, College Park Yard, and Newhall Street Yard in Santa Clara.

An excursion on the San Bruno branch (a remnant of the original main line) on 4th October 1953 completes the main coverage.

As a special bonus there is film of the Market Street (later San Francisco Municipal) Railway electrified '40 Line' which in part parallels the SP main line and Bruno branch.

This remarkable collection of rare action scenes eloquently conveys the intensity of railroading in the area at the time, forming a fitting epitaph for a bygone era. It represents the splendid finale of a hundred years of steam traction, working as though there were no end in sight, yet all steam was gone within two years.

The film clips are generally of a good length, and the picture quality is remarkably good, almost all in colour, presented in modern 16:9 widescreen format.

Each section is prefaced with brief introductory text on screen, and sub-titles identify the locations and other items of interest, while maps are shown at intervals to explain the routes.

The soundtrack is in stereo, with convincing dubbed effects.

The commentary is sparing but informative when present, and clearly delivered. The initial screen offers the option of no narration.

There is some use of backing music.

This vintage material was originally brought together by Don Olsen, and the revised production has been directed by Warren Haack.

The programme is followed by about five minutes of trailers for the previous four volumes of Southern Pacific coverage from this era.

This DVD is available from Camden Miniature Steam Services, Barrow Farm, Rode, Frome, Somerset, BA11 6PS.

www.camdenmin.co.uk



Hornby International issue new Autumn/Winter 2019 catalogue

Hornby Hobbies has published a new catalogue dedicated to its international brands, which supplements the listing released in January 2019. It includes details of new announcements under the Arnold, Jouef, Electrotren, Rivarossi, Lima, and Lima Expert labels, as well as items already available. In addition there are accessories, track packs, and buildings, plus the Skaledale, Skale Autos, and Skale Scenics collections, plus Humbrol products.

The Jouef section includes details of a new *Jouef Loisirs* line, which is intended as 'hobby' level products using new tooling but at affordable prices.

Amongst these will be the SNCF CC72000 diesel locomotive (pictured), available initially in a choice of three liveries and priced at £129.99 each.

The company's Chief Operating Officer, Tim Mulhall, explained: "We have been working behind the scenes for eighteen months to create a range of affordable models with good motors, fine livery application, and good body-shell detail. They are not intended to be cheap toys but good value models for the keen modeller. We think these models will gain a wide following. You can expect to see this strategy rolling out new locomotives, coaches, and wagons

at great prices across all our European markets from now onwards."

Copies of the new catalogue will be available from Hornby retailers and via the website: www.hornby.com



Victorian Railways J class 2-8-0 coming from Ixion in HO

The directors of Ixion Model Railways Australia have announced that their next HO scale locomotive model, due in the first quarter of 2020, will be the Victorian Railways J class 2-8-0, covering both coal and oil burning versions.

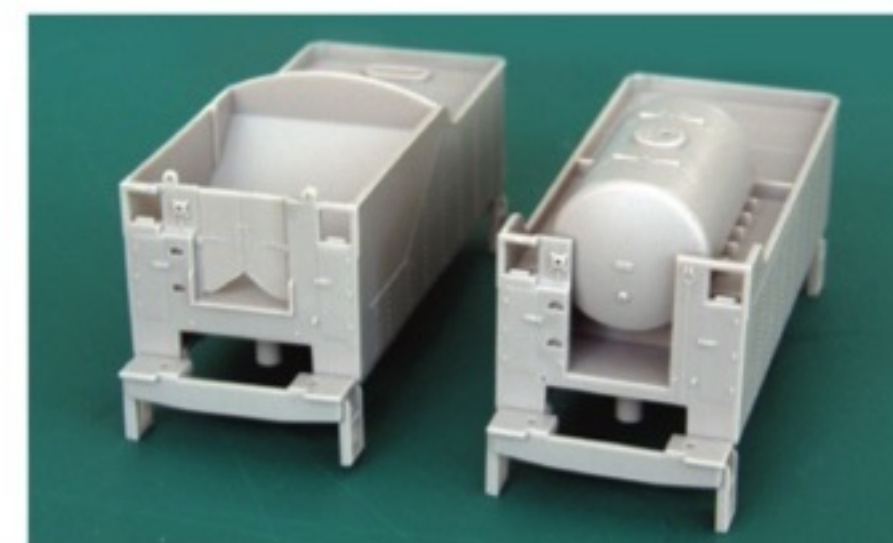
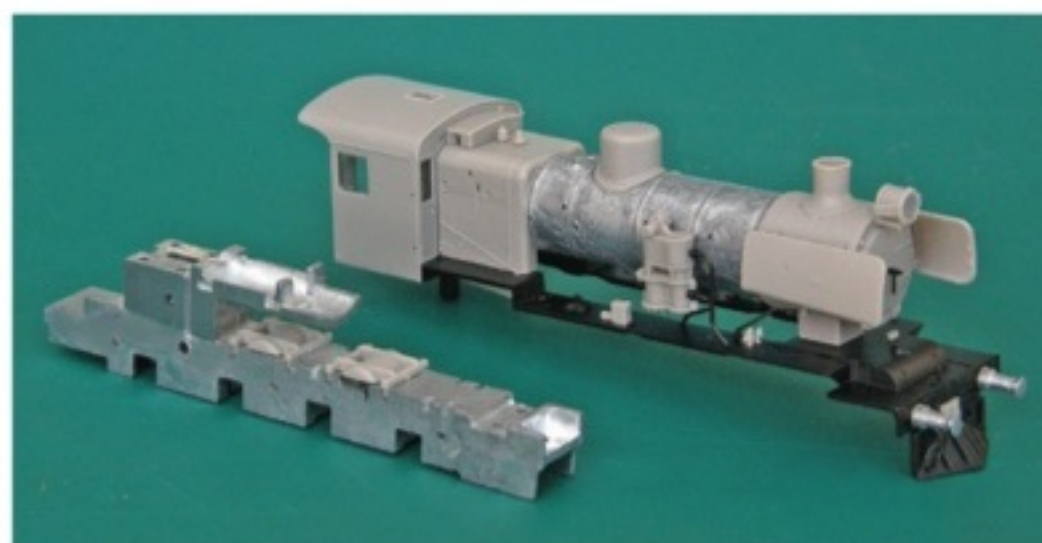
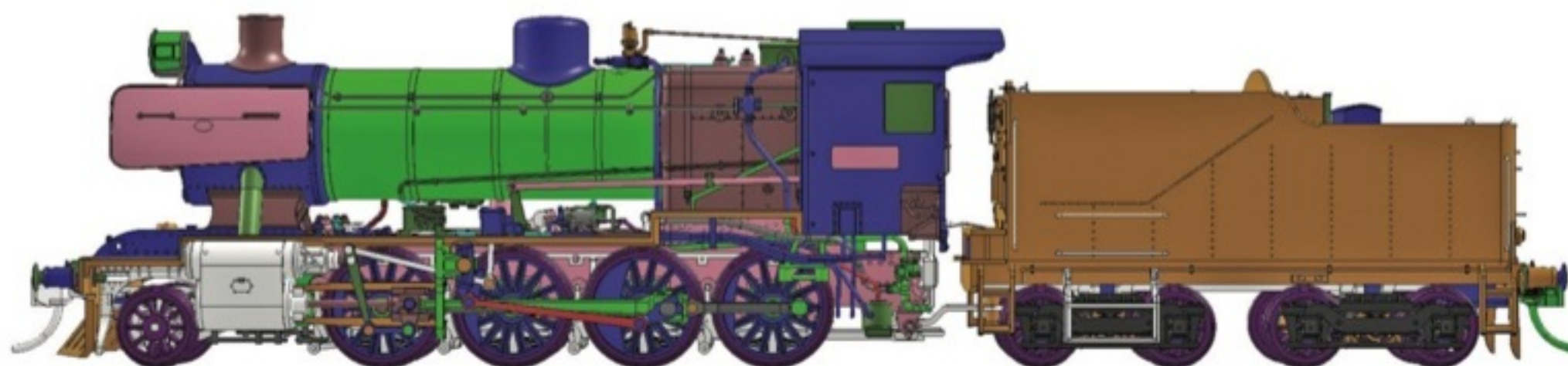
The accompanying illustrations show some of the CAD artwork and samples of the first test shots from the tooling.

The models will feature a cast metal boiler, footplate, and chassis; 40:1 gearing; pick-up from all driving and tender wheels; a 21-pin decoder socket for DCC and sound installation; sprung buffers; and Kadee couplers. The model is designed for operation on a minimum radius of 610mm on 16.5mm gauge.

Livery will be satin black with red footplate lining and smoke deflectors. The coal-fired versions will include etched fire irons for the modeller to fit as desired. Twelve different running numbers are planned.

The expected price, barring major exchange rate changes, will be A\$595.00.

For more information see the Ixion Model Railways Ltd. Facebook page, or contact: info@ixionmodels.com



New French structure kits from Cités Miniatures



As an option, a tiled roof can replace the standard zinc roof (at extra cost, due the extra machining time required).

The kit includes two buildings and the central courtyard, which is also already available separately (ref.BV-005-4-HO).

The N scale range has recently been increased with a two-track vaulted roof concrete goods shed (ref.BF-005-2-N).

This is a faithful transposition of the HO version, released two years ago, which continues to be available.

The first batch were due in the second half of September and they should also be available at the RAMMA show in Sedan.



Cités Miniatures
145, Avenue Henri Ginoux,
F-92120 Montrouge, France.
www.cites-miniatures.com

New from French structure specialists Cités Miniatures in HO is a laser-cut card kit for a pair of three-storey buildings with a shared central courtyard (ref.BV-017-HO).

This kit has a history: it was originally created for a well-known modeller in N more than two years ago, and then added to the range. Another loyal customer then asked for it in O, which was done about a year ago. Now it is finally also offered in HO.



Foreign layouts featured at Expo Narrow Gauge

The Greenwich & District Narrow Gauge Railway Society will be holding its annual Expo Narrow Gauge event (ExpoNG) on Saturday 26th October at the usual venue, the White Oak Leisure Centre, Hilda May Avenue, Swanley, Kent, BR8 7BT.

Billed as 'Europe's premier narrow gauge exhibition', the show will feature some 65 stands, among them fifteen layouts in a range of scales and gauges, together with demonstrations of modelling techniques, displays by prototype and modelling societies, and a host of specialist manufacturers and traders.

Amongst the layouts on show will be *Ulvaryd Strand* (Swedish HOe) by Charles Insley (to be featured in our November issue), *St. Amis sur Mer* (French HOm, right), presented by MSC Het Spoor from Belgium (featured in our November 2016 edition), and *Clearwater Harbor* (US On30) by Steve Waterfield (described in CM last month).

The venue is to the north of Swanley town centre, a ten minute walk from the railway station (use rear exit into Everest Place), which is served by trains from Victoria. There are good connections from the M25 (Junction 3) and the A20, and its close proximity to the Channel Tunnel connections at Folkestone, Ashford, and Ebbsfleet make it an easy day trip from the Continent.

For further details see *Exhibition Diary* or: www.expong.org



New HOe railcar kit by Tramfabriek



Tramfabriek have a new kit for a four-wheel diesel railcar, suitable for either HOe or OO9. It is based on a model made by Bob Telford for a OO9 Society competition. The kit consists of etched brass, cast brass, and resin parts, and is supplied with a motorised chassis – a Kato ref.11-104 mechanism which has been upgraded with a coreless motor for even finer running characteristics. It has working front and rear lights.

The kit contains all the parts to make a fully detailed model, including windows ready cut to size which slide neatly between the side panels, an interior, and cab interior detail.

Two similar driver figures are included: as the model can be used in two different scales, both 1:76 and 1:87 3D-printed figures are included (unpainted). The figure has been designed in-house for this railcar, which means it fits without having to chop off the legs.

Prices: DC €175.00, DCC €193.00, DCC + 'stay-alive' €213.00.

For more information and availability:

www.tramfabriek.nl

2, Locken Cottages, The Walk, Eton Wick, SL4 6LN.



Foam servicing trays new from Noch

New to the German supplier's range of accessories are Profi Foam Train Service Trays, intended for supporting rolling stock models in HO/TT (ref.99355) and N/Z (ref.99352).

In each case the foam tray has two hollows; one to support models upside down whilst undertaking inspection and lubrication, the other to enable models to be placed on one side to assist with other servicing tasks.

The trays also have other recesses to hold various tools such as a brush and oil pen. Small parts and screws can also be stored in the integrated compartments, one of which incorporates a magnetic disc to prevent small parts from becoming lost.

Noch products are distributed in the UK by Gaugemaster.

www.gaugemaster.com

www.noch.com



Warley show – advance tickets

Preparations are well under way for this year's Warley National Model Railway Exhibition, which will be taking place at the NEC in Birmingham over the weekend of Saturday 23rd and Sunday 24th November. The event will bring together more than ninety layouts, including fifteen of overseas prototypes, in scales from Z to O, some coming over from the Continent. More details in our next issue.

Alongside the layouts will be around 150 specialist trade stands, together with practical modelling demonstrations and a children's plastic kit building workshop.

Peco and CONTINENTAL MODELLER are supporting Warley MRC as event sponsors alongside Bachmann Europe Plc, Hornby Hobbies, and Crécy Publications.

One of Pecorama's fleet of 7¼" gauge steam locomotives will be on display as a centre-piece attraction: this is planned to be No.12 *Jools*, which was officially named by the musician and television presenter Jools Holland OBE in June last year.

Purchasers of advance tickets will once again be allowed in to the show from 9.15am, half an hour before visitors who purchase their tickets on the day.

Prices for one-day adult tickets are £16.00 in advance (£17.50 on the day); two-day tickets are priced £29.00 in advance (£32.00 on the day).

The 'kids for a quid' initiative launched last year is to be repeated, with two-day tickets priced at £2.00 for accompanied children 16 years and under; under fives are admitted free.

Advance tickets can be purchased by enclosing your remittance (payable to "Warley MRC Exhibitions Limited") with a stamped addressed DL-sized envelope to arrive no later than 9th November to: Warley National Model Railway Exhibition Advance Ticket Sales, 86, Gospel End Road, Sedgley, Dudley, West Midlands, DY3 3YU.

Purchases may also be made by credit/debit card from the Ticket Factory at the NEC by telephone (0844 581 0737) or online (www.theticketfactory.com), where an administration fee applies. For more details: www.thewarleyshow.co.uk

Peco to take on production of Helmsman controllers

Peco is pleased to announce that it will be taking on the production, distribution, and servicing of Helmsman Model Rail controllers and accessories, having reached an agreement with the founders of the brand, Geof and Lesley Helm, who are retiring: we wish them well for the future.

Whilst stocks are transferred there may be a period of unavailability, but as soon as production resumes Peco will notify customers through the pages of RM and CM, the retailer network, and the website. This is likely to be early in the New Year.

As with all Peco Group products, in future Helmsman controllers will only be available via Peco stockists.

In the meantime, queries about servicing and repair of existing equipment can be made to the Peco Technical Advice Bureau.

Peco Technical Advice Bureau, Underleys, Beer, Seaton, Devon, EX12 3NA.

01297 21542

info@peco.co



Exhibition Diary

Dates, events, and information

Please send details for possible inclusion to:

Peco Publications & Publicity Ltd., Beer, Devon, EX12 3NA.

Telephone: 01297 20580 Fax: 01297 20229

e-mail: rm-clubs@btconnect.com

Saturday 21 September CRAWLEY, West Sussex

Organiser: Seaboard Southern
North American Train Show.

Venue: The Charis Centre, West Green Drive, Crawley, West Sussex, RH11 7EL. (Venue car park opposite 38, Town Barn Road, RH11 7ED.)

Open: 1000 – 1600

Admission: adults £6.00, NMRA members (with valid membership card) £5.00, accompanied under 16s free.

Amenities: refreshments.

Features: layouts include *Ponts Mill* (N, in this issue), *Blue Heron* (HO), *Drew, IA.* (HO), *Mere Vista* (HO), *Springfield Storage* (HO), *Terminal Belt Transit Lines* (HO), *Fort Myers* (HO, CM September 2005), and *Black Sheep* (On30). Demonstration, NMRA(BR) display, specialist trade.

Contact:

www.seaboard-southern.org.uk

Saturday 21 September EARLEY, Berkshire

Organiser: Loddon Vale MRC.

Venue: St.Peter's Church Hall, Church Road, Earley, Reading, RG6 1EY.

Open: 1000 – 1630

Admission: adults £4.00, children £2.00, family £9.00.

Amenities: on two floors, no disabled access to first floor; refreshments.

Features: layouts include *Nippon Tetsudo* (Japanese Z). Trade.

Contact: www.lvmrc.co.uk

Saturday 21 September SHENFIELD, Essex

Organiser: Mid Essex MRC.

Venue: Shenfield High School, Oliver Road, Shenfield, Essex, CM15 8PX.

Open: 1000 – 1700

Admission: adults £6.00, children £4.00.

Amenities: parking nearby; free shuttle bus from/to Shenfield station (not suitable for those with mobility scooters, walking aids, or wheelchairs); refreshments.

Features: layouts include *Allenton* (US HO), *Mariposa Yard* (US HO), and *Oh Guano* (NG). Demonstrations, societies, displays, trade.

Contact: David Zelly 07973 382916
www.midessexmrc.org

Saturday 21 September WILLAND, Devon

Organiser: Culm Valley MRC.

Venue: Willand Village Hall, Gables Road, Willand, near Cullompton, EX15 2PL.

Open: 1000 – 1630

Admission: adults £4.00, children £1.50.

Amenities: free parking; wheelchair access; hot & cold food and drinks.

Features: over 15 layouts including *Hassenberg* (N) and *Bw.Dügelskirchen* (German HO, in this issue).

Contact: Chris Shields 01884 253945
www.cvmrc.co.uk

Saturday 21 & Sunday 22 September FLEETWOOD, Lancashire

Organiser:

Blackpool & North Fylde MRC.

Venue: Cardinal Allen High School, Melbourne Avenue, Fleetwood, FY7 8AY.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £6.00, seniors £5.00, juniors £2.50, family (2+2) £14.00.

Amenities: free parking; disabled access; refreshments.

Features: 17 layouts including *Republic Steel* (US Z, CM October 2016), *Aventaclu* (Swiss HOm), *Salsdorf* (German TT, CM May 2019), *Bear Creek Junction* (US HO/HOn3), and *Jacksonville Yard* (US HO). Demonstrations, club stand, trade.

Contact:

www.blackpoolandnorthfyldemrc.co.uk

Saturday 21 & Sunday 22 September REDDITCH, Worcestershire

Organiser: Redditch MRC.

Venue: Trinity High School, Grove Street, Redditch, Worcestershire, B98 8HB.

Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Admission: £6.00, accompanied under 16s free.

Amenities: 10 minutes from rail and bus stations; free parking; refreshments.

Features: layouts include *29th Street Wharf* (US HO, CM October 2010), Trade.

Contact: www.redditch-mrc.com

Saturday 28 September EDGBASTON, Birmingham

Organiser:

European Railways Association.

Venue: The H Suite, 100, Icknield Port Road, Edgbaston, Birmingham, B16 0AA.

Open: 1030 – 1600

Admission: adults £7.50, children under 16 £2.00, family (2+3) £16.00.

Amenities: large free car park; disabled access; refreshments. Bus 80 to venue from Birmingham New Street station; Five Ways station 1 mile.

Features: layouts include *Kenstadt* (German Z), *Klosters Dorf* (Swiss Nm), *Tamaro* (Swiss N + Nm), *Depot de Camions* (Swiss N), *90p Railway* (Swiss N), *Vartanakert* (Armenian N), *Salsdorf* (German TT, CM May 2019), *Grazellar Bahn* (Austrian HOe), *Lesen Signal Werke* (German HO), *Bahnbetriebswerk Münster Süd* (German HO), *Gouttières* (French HO, CM February 2014), *Marmagne* (French HO, CM March 2005), *Moyenmoutier* (French HO), *Zlata Vychod* (Czech HO, CM April 2017), *Obbekaer* (Danish P87, CM June 2016), *A taste of Norway* (HO, CM September 2019), *Whiteleaf Light Railway* (Swiss/Austrian G), *Gare de Brindille* (WW1 SM32, coming soon in CM), and *Fintonagh* (Irish O-21, coming soon in RM). CM will be represented. Demonstrations, including DCC; societies; specialist trade.

Contact: Piers Milne 0121 628 1551
www.eurorail.org.uk

Saturday 28 & Sunday 29 September HALIFAX, West Yorkshire

Organiser: Halifax MRC.

Venue: North Bridge Leisure Centre, North Bridge Street, Halifax, HX3 6TE.

Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Admission: adults £7.00, accompanied children under 16 free, unaccompanied children £3.00.

Amenities: 10 minutes walk to railway station; excellent access for wheelchairs, level venue.

Features: layouts include *Gresten* (Austrian HOe, CM September 2012), *Neuburg 1913* (Swiss HOm, CM Swiss Special November 2018), *Vals* (Swiss HOm), *Hansestadt Brünshafen* (German HO, CM January 2015), *Bastogne Rue de Clervaux* (Belgian HO, CM May 2015), and *Millers Crossing* (US HO). Demonstrations, trade.

Contact: www.halifaxmrc.co.uk

Saturday 28 & Sunday 29 September LYDNEY, Gloucestershire

Organiser: Forest of Dean MRC.

Venue: The Dean Academy (formerly Whitecross School), Church Road, Lydney, Gloucestershire, GL15 5DZ.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £5.00, children under 16 £2.00, under 5s free, family (2+2) £10.00.

Amenities: wheelchair access to all parts of the exhibition; refreshments.

Features: over 20 layouts, including *Snake Bend* (US N). Trade.

Contact: www.fodmrc.org.uk
01594 835935 (daytime only)

Saturday 28 & Sunday 29 September WORTHING, West Sussex

Organiser: Worthing MRC.

Venue: Durrington High School, The Boulevard, Worthing, BN13 1LA.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £5.00, accompanied children admitted free.

Amenities: free parking; disabled -friendly venue; light refreshments.

Features: layouts include *Titling* (German HO) and *Ponts Mill* (US N, in this issue). Societies, club sales, trade.

Contact: 07922 132198
www.worthingmrc.co.uk

Saturday 5 October ABBOTSKERSWELL, Devon

Organiser: South Devon MRS.

Venue: Church House, Church Path, Abbotskerswell, Newton Abbot, Devon, TQ12 5NZ.

Open: 1030 – 1630

Admission: adults £1.50, children 50p, family £3.00.

Amenities: parking in the Court Farm Inn; refreshments.

Features: club and visiting layouts in various scales including *Bw.Dügelskirchen* (German HO, in this issue) and *Crescentville*.

Contact: www.sdmrs.org.uk

Saturday 5 October AYLSHAM, Norfolk

Organiser: Broadland MRC.

Venue: The Jubilee Centre, Norwich Road, Aylsham, Norfolk, NR11 6JG.

Open: 1000 – 1600

Admission: adults £5.00, children & concessions £4.00. Free entry for Bure Valley Railway passengers: event sponsored by the BVR.

Amenities: free parking at BVR with free bus to/from venue; refreshments.

Features: layouts include *Hartenberg* (Bavarian N) and *All Cars Stop Here* (Hong Kong trams OO). Demonstrations, trade.

Contact:

www.broadlandmodelrailwayclub.co.uk

Saturday 5 October SHEFFIELD, South Yorkshire

Organiser: Sheffield MRS.

Venue: Davy's Sporting Club, Prince of Wales Road, Sheffield, S9 4ER.

Open: 1000 – 1600

Admission: adults £4.00, children 5 – 16 £2.00, under 5s free.

Amenities: free parking; disabled access via lift; refreshments, bar.

Features: layouts, including continental, in various scales.

Contact: 07940 033822

Saturday 5 October SUDBURY, Suffolk

Organiser: Sudbury MRC.

Venue: St.Peter's, Market Hill, Sudbury, Suffolk, CO10 2EA.

Open: 1000 – 1630

Admission: adults £5.00, concessions £4.00, children £2.00, wheelchair users free (carer £4.00).

Features: layouts include *Clintons Bends* (US N) and *Phoenix Junction* (US HO). Societies, trade.

Contact: www.sudburymrc.org.uk

Saturday 5 October WELLINGTON, Telford & Wrekin

Organiser:

Telford Railway Modellers Group.

Venue: Sports Hall, Charlton Academy, Apley Avenue, Wellington, TF1 3FA.

Open: 1000 – 1600

Admission: adults £4.00, children £2.00, family (2+2) £10.00.

Amenities: free parking; easy wheelchair access; refreshments.

Features: 20+ layouts, including *Kenstadt* (German Z), *Oberdorf* (German Nm, CM July 2009), *Kato Unitram* (Japanese N), and *Bourton Yard* (US HO). Demonstrations, trade.

Contact: 01952 820376
www.trmg.webs.com

Saturday 5 & Sunday 6 October DORKING, Surrey

Organiser: Dorking & District MRC.

Venue: The Oddfellows Hall, 270, High Street, Dorking, Surrey, RH4 1QT. Dorking West stations 10 minutes walk. Bus service details on website.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £5.00, children £3.00, family (2+2) £10.00.
Amenities: nearest parking is Dorking Halls car park, Deepdene, Dorking; regret no wheelchair access; light refreshments
Features: layouts include *San Maria Gandia* (Spanish HOe/HO, CM September 2018). Trade.
Contact: www.dorkingmodelrailway-club.com

Saturday 5 & Sunday 6 October FAREHAM, Hampshire

Organiser: Fareham & District MRC.
Venue: Fareham Leisure Centre, Park Lane, Fareham, Hampshire, PO17 7JU.
Open: 1000 – 1730 Saturday
1000 – 1630 Sunday
Admission: £7.00, accompanied children under 16 free (max. 2 per adult).
Amenities: free on-site parking for three hours; park & ride and free heritage bus service from Fareham railway station; disabled access; refreshments.

Features: layouts include *Villefranche-la-Chappelle* (French HO, CM September 2016), and *Beijiao* (Chinese HO). Demonstrations, societies, trade.

Contact: www.fareham-mrc.org.uk

Saturday 5 & Sunday 6 October FOLKESTONE, Kent

Organiser: Folkestone, Hythe & District Model Railway Club.
Venue: Leas Cliff Hall, The Leas, Folkestone, Kent, CT20 2DZ.
Open: 1000 – 1700 both days.
Admission: adults £7.00, children £3.00, family £17.00.
Amenities: parking; disabled access; refreshments, bar.

Features: layouts include *Shasta* (US Z, CM March 2014), *Knuddelstein* (Bavarian N), *Bitterfeld* (German HO), *Distillerie Jacdegue* (French HO), and *Mers-les-Bains* (French 1:32, CM April 2015 and August 2017). Demonstrations, societies, trade.

Contact: www.folkestone-mrc.com

Saturday 5 & Sunday 6 October WIGAN

Organiser: Wigan Finescale Railway Modellers.
Venue: Robin Park Leisure Centre, Loire Drive, Wigan, WN5 0UL. (Opposite the DW Stadium.)

Open: 1000 – 1730 Saturday
1000 – 1630 Sunday

Admission: £10.00 with free show guide. Accompanied children free. Discounted and two-day tickets available via website.

Amenities: free parking within 300yds; free vintage bus from Wigan North Western station; disabled parking; disabled access; refreshments.

Features: 30+ layouts including *Rensville* (US HO), *Bear Creek Junction* (US HO), and *Broadford* (Australian HO, now extended, CM January and March 2010, January 2013, June 2018, and June 2019). Demonstrations, societies, trade.

Contact: www.wiganfrm.co.uk

Sunday 6 October PRATT'S BOTTOM, Kent

Organiser: G Scale Society, Kent Group.
Venue: Pratt's Bottom Village Hall, Norsted Lane, Rushmore Hill, Pratt's Bottom, Kent, BR6 7PQ.

Open: 1100 – 1600
Admission: adults £2.50, children £1.00.
Amenities: parking; disabled access; light refreshments.
Features: 3+ layouts plus sales stand.
Contact: www.gscaekent.co.uk

Saturday 12 October CRADLEY HEATH, West Midlands

Organiser: Cradley Heath MRC.
Venue: Cradley Heath Community Centre, Reddal Hill Road, Cradley Heath, West Midlands, B64 5JY.

Open: 1000 – 1630
Admission: adults £5.00, children 4 – 14 £2.50, under 4s free, family (2+2) £11.00. No unaccompanied children.

Amenities: free parking at above postcode; disabled parking only at venue (B64 5JG); refreshments.

Features: layouts include *St.Frazal d'Ardèche* (French Nm), and *Boston Creek* (US On30). Trade.

Contact: www.cradleyheathmrc.co.uk

Saturday 12 & Sunday 13 October ALDERSHOT, Hampshire

Organiser: Farnham & District MRC.
Venue: Alderwood Leisure Centre, Tongham Road, Aldershot, GU12 4AS.
Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Admission: adults £6.00, children £2.00, family £15.00.

Amenities: free parking; wheelchair access; catering.

Features: layouts include *Bear Creek Junction* (US HOn3), *29th Street Wharf* (US HO, CM October 2010), and *De Tenderloc* (Dutch HO). Trade.

Contact: www.farnhammrc.org.uk

Saturday 12 & Sunday 13 October BARROW-IN-FURNESS, Cumbria

Organiser: Furness MRC.
Venue: Barrow Sixth Form College, Rating Lane, Barrow-In-Furness, Cumbria, LA13 9LE.

Open: 1000 – 1700 both days.
Admission: adults £6.00, seniors £5.00, children £3.00.

Amenities: free bus from Town Hall and railway station.

Features: layouts include *Swiss Pass* (HOM), and *Laramie Engine Terminal* (US O, CM November 2017). Societies, trade.

Contact: Ian Edwards 01229 583595
https://furnessmrc.webs.com/

Saturday 12 & Sunday 13 October BOURNEMOUTH, Dorset

Organiser: Christchurch MRC.
Venue: Bournemouth School for Girls, Castle Gate Close, off Castle Lane West, Bournemouth, Dorset, BH8 9UJ.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £5.00, accompanied children free.

Amenities: free parking; refreshments.
Features: c.15 layouts including *Pine Bluffs & Red Dot Mine* (US HO). Trade.

Contact: www.christchurchmrc.org.uk
Pete Whichter 07979 870557

Saturday 12 & Sunday 13 October DUNDEE

Organiser: Dundee MRC.
Venue: Boomerang Community Centre, Kemback Street, Dundee, DD4 6ET.

Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Admission: adults £5.00, children £3.50, family (2+2) £12.00.
Amenities: parking; refreshments.
Features: layouts include *Helmstadt, Dry Falls*, and *Williamson River*. Trade, swapmeet.
Contact: www.dundeeemrc.co.uk

Saturday 12 & Sunday 13 October SEDAN, France

Organiser: RAMMA.
Venue: Complexe Sportif Jean Rogissart, rue de la Rochefoucauld, F-08200 Sedan, France.

Open: 1300 - 1900 Saturday
1000 - 1800 Sunday

Admission: adults €8.00, children 6-16 €7.00, under 6 free, groups of ten or more €7.00/person.

Amenities: free shuttle bus from free parking areas.

Features: many quality layouts from all over Europe, in a wide range of scales and gauges, plus other hobbies, in several halls.

Contact: www.ramma.org

Sunday 13 October ABINGDON, Oxfordshire

Organiser: Oxford & District MRC.
Venue: Abingdon & Witney College, Wootton Road, Abingdon, OX14 1GG.
Open: 1030 – 1600

Admission: adults £6.00, accompanied children free.

Amenities: refreshments.

Features: c.40 layouts including *Snake Bend* (US N). Also incorporating ZEDEX, a convention of Z gauge layouts which will include *Burghügel* and *Söller* (both in this issue). Trade.

Contact: www.oxfordmrc.org.uk

Sunday 13 October KEIGHLEY, West Yorkshire

Organiser: Keighley MRC.
Venue: Clubrooms, Knowle Mills, South Street, Keighley, BD21 1SY.

Open: 1000 – 1630.
Admission: adults £4.00, children £3.00.

Amenities: free parking; lift access; refreshments.

Features: 7mm Festival – 10+ club and guest layouts, including *Echoes of the Black Canyon* (US On3). Demonstrations, clinics, specialist traders.

Contact: www.keighley-mrc.org.uk

Saturday 19 October CORSE, Gloucestershire

Organiser: M5-M50 Narrow Gauge Modellers.
Venue: Corse & Staunton Village Hall, Corse, Gloucestershire, GL19 3RQ.
Open: 1000 – 1630.

Admission: adults £5.00, accompanied children free.

Amenities: parking; disabled access; refreshments.

Features: 10 layouts, including one HOe. Demonstrations, trade.

Contact: www.m5m50ngm.co.uk
Tim Crockford 01454 327611

Saturday 19 October WINCHESTER, Hampshire

Organiser: Winchester O Gauge Continental & American Meet.
Venue: Kings School, Romsey Road, Winchester, Hampshire, SO22 5PN.
Open: 1000 – 1600.

Admission: adults £6.00, accompanied children £1.00.

Amenities: free parking; refreshments including all-day breakfast.

Features: European and American layouts in O plus large test track. Bring & buy, trade, French Railways Society.
Contact: Alan Marlow 07914 978105

Saturday 19 & Sunday 20 October NEWARK-ON-TRENT, Nottinghamshire

Organiser: Lincoln & District MRC.
Venue: Lady Eastwood Pavilion, Newark Showground, Coddington, Newark-on-Trent, Nottinghamshire, NG24 2NY.

Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Entry 0930 both days for disabled visitors/wheelchair users.

Admission: adults £8.00, children £4.00, family (2+up to 4) £20.00.

Amenities: free vintage bus between venue and railway stations; free parking; wheelchair friendly; café.

Features: layouts include *Eight Dollar Canyon* (HO), *K Street Yard* (HO), and *Zlata Vychod* (Czech HO, CM April 2017). Demonstrations, societies, trade.

Contact: www.ladmrc.co.uk

Saturday 19 & Sunday 20 October UCKFIELD, East Sussex

Organiser: Uckfield MRC.
Venue: Uckfield Civic Centre, Civic Approach, Uckfield, TN22 1AE.
A short walk from railway and bus stations.

Open: 1000 – 1700 both days.

Admission: adults £7.00, accompanied children free.

Amenities: free parking at adjacent High Street car park; wheelchair-friendly; licensed restaurant.

Features: 15+ layouts including *South Walton* (New South Wales N, CM November 2017), *De Graafstroom* (Dutch P87n3.5, in this issue), and *Chica, Illinois* (US HO, CM August 2018). Demonstrations, societies, trade.

Contact: www.uckfieldmrc.co.uk
Adrian Colenutt 01903 745759

Saturday 19 & Sunday 20 October GENK, Belgium

Euromodelbouw
Organiser: Dipro Events
Venue: Limburghal, Jaarbeurslaan 6, B-3600 Genk, Belgium.

Open: 0930 - 1800 both days.

Features: various kinds of modelling (aircraft, marine, vehicles, military) as well as railway layouts from all over Europe. Trade.

Contact: www.euromodelbouw.be

Saturday 26 October EXETER, Devon

Exeter Garden Railway Show
Organiser: South Devon Garden Railway Group Ltd.

Venue: The Matford Centre, Matford Park Road, Marsh Barton Industrial Estate, Exeter, Devon, EX2 8FD.

Open: 1030 – 1630

Admission: adults £7.00, seniors £6.00, accompanied children free.

Amenities: free parking; disabled-friendly; no dogs except assistance dogs; restaurant.

Features: all major garden railway scales, standard and narrow gauge, live steam and electric. Bring & buy, trade.

Contact: Peter Javes 01548 559283
www.exetergardenrailwayshow.com



Saturday 26 October

SWANLEY, Kent

Expo Narrow Gauge

Organiser: Greenwich & District Narrow Gauge Railway Society.

Venue: White Oak Leisure Centre, Hilda May Avenue, Swanley, Kent, BR8 7BT.

Open: 1000 – 1700

Admission: adults £8.00, accompanied children 6 – 16 £1.00.

Amenities: good disabled access but limited on-site parking; refreshments.

Features: layouts include *Ulvaryd Strand* (Swedish HOe, above, coming next month in CM), *St. Amis sur Mer* (French HOm, CM November 2016), and *Clearwater Harbor* (US On30, CM September 2019). Demonstrations, societies, specialist trade.

Contact: www.expong.org

Saturday 26 October

NEWBURY, Berkshire

Organiser: Newbury MRC.

Venue: St. Bartholomew's School, Buckingham Road (off Andover Road), Newbury, Berkshire, RG14 6JP.

Open: 1000 – 1630

Admission: adults £5.00, seniors £4.50, children £2.50, family £10.00.

Amenities: free parking; wheelchair-friendly; refreshments.

Features: 16 layouts including *Evelyn Harbor* (US N), and *San Maria Gaudia* (Spanish HOe/HO, CM September 2018). Trade.

Contact: Paul Leonard 07590 429643
www.newburymrc.co.uk

Saturday 26 & Sunday 27 October

LEEDS, West Yorkshire

Organiser: Leeds MRS.

Venue: The Grammar School at Leeds, Alwoodley Gates, Harrogate Road, Leeds, West Yorkshire, LS17 8GS.

Open: 1000 – 1700 Saturday
1000 – 1630 Sunday

Admission: adults £7.00, accompanied children free.

Amenities: ample free parking; refreshments.

Features: layouts include *Vals* (Swiss HOm), *Munrow* (US HO), and *Redwing, Iowa* (US HO). Demonstrations, trade.

Contact: John Aldrick 07900 338215
www.leedsrms.org

Saturday 26 & Sunday 27 October

POLLOKSHAW, Glasgow

Organiser: Clydeside MRC.

Venue: Pollokshaws Burgh Hall, 2025, Pollokshaws Road, Glasgow, G43 1NE.

Open: 1000 – 1700 both days.

Admission: adults £5.00, concessions £4.50, children £2.50, family (2+2) £13.00.

Amenities: plenty of free parking; short walk from Pollokshaws West or Shawlands stations; frequent bus services First 3, 57, 57A, McGill's 3.

Features: layouts include *Tuerland* (N), *Kyleandorf* (HOe), *Trenches* (OO9), *Smierweida* (TT), and *Blausee Mitholz* (HO). Societies, trade.

Contact: www.clydesidemrc.com

Saturday 26 & Sunday 27 October

TAUNTON, Somerset

Organiser:

Somerset Railway Modellers Club.

Venue: Taunton School, Staplegrove Road, Taunton, Somerset, TA2 6AD.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £8.00, seniors £7.00, accompanied children free.

Amenities: free parking; blue badge spaces; fully accessible venue; catering in main school restaurant, including Sunday roast.

Features: layouts include *Zlata Vychod* (Czech HO, CM April 2017), and *Angell Creek* (US On30). Societies, trade.

Contact: www.railextaunton.uk

Saturday 26 – Monday 28 October

BLACKROCK, Co. Dublin

Organiser: South Dublin MRC.

Venue: Blackrock College, Blackrock, Co. Dublin, Eire. (Bus route 45, Booterstown DART station.)

Open: 1100 – 1700 each day.

Admission: adults €8.00, seniors/children €6.00, family (2+2) €20.00.

Amenities: free parking. All on one level; suitable for disabled visitors by prior arrangement (contact exhibition manager).

Features: Irish, British, and continental layouts. Trade.

Contact: 00353 86 812 6112
www.sdmrc.ie

Sunday 27 October

MILLAND, West Sussex

Organiser: Liphook & District MRC.

Venue: Milland Valley Memorial Hall, Iping Road, Milland, GU30 7NA.

Open: 1000 – 1600

Admission: £2.50, accompanied children free.

Amenities: free parking; full disabled access; refreshments.

Features: layouts include the club's own *Inspired by Friedrichstrasse* (German HO). Second-hand stall.

Contact: www.liphookmrc.com

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out 17th October

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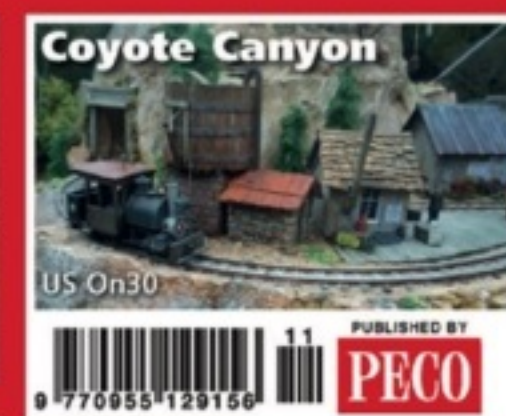
Ulvaryd Strand

Swedish HOe



Belgomine Travaux

Belgian HO



Coyote Canyon

US On30

Latest Reviews ... and much more



• Süd Harz Eisenbahn

Brutish 2-10-2 tank engines storming up steep gradients amongst pine forests first drew Paul Steedman to ideas of modelling the Harzer Schmalspurbahnen in HOm, and he has produced a large and flexible modular layout.

• Ulvaryd Strand

Research into French 60cm gauge Decauville equipment brought Swedish lines to the attention of Charles Insley and a compact HOe layout was the eventual result.

• Belgomine Travaux

An industrial siding off the SNCB/NMBS line in Temse serving the Belgomine company and other operations inspired an HO layout from Hans Baeck (MSC Het Spoor).

• Gare de Brindille

The chance discovery of a range of 16mm scale kits for War Department Light Railways locos and stock prompted Chris Hopper to start a new WW1 project.

• Coyote Canyon

Bob Wright offers another scenic creation in American On30.

... and much more!

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PLUS DAUGHTERS OF BASILDON ESSEX. N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. Plus Daughters, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon Essex SS13 1EU. Tel: 01268 726 211 www.plusdaughters.co.uk

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CONTINENTAL MODELLER – BACK ISSUES – for more information on which issues are available, please contact: Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580. Price for UK Residents £3 per issue, Europe £3 per issue plus £2 postage sent Airsaver Post and rest of the World £3.00 per issue plus £3 postage sent Airsaver Post. Airmail – please enquire.

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Fairs, Sales and Swapmeets

September 2019

28 September (Sat) 10am-3pm
GATESHEAD INTERNATIONAL STADIUM, Neilson Road, Gateshead NE10 0EF (just off Felling By pass A184) Admission £3, Concessions £2. Future date: 14th December JIM CORR. Tel: 0750 403 5955 email: jim_corr73@hotmail.co.uk

October 2019

13 October (Sun) 10.30am-3.30pm
KEMPTON PARK TOY & TRAIN COLLECTORS FAIR, Kempton Park Racecourse, Sunbury on Thames, Middlesex, (M3 junction 1 onto A308). Over 200 stalls of model railways, toys, scale models and collectables. Adults £4, Concessions £3.50, Children £1, early entry buyers £10 (from 8.30am). London's No1 Sunday Toy and Train Collectors Fair. RM TOYS LTD. Tel: 023 923 815 29 or 07957 823 507.

20 October (Sun) 10am-2pm
HALTWHISTLE off A69 between Carlisle and Newcastle. Haltwhistle Leisure Centre, Greencroft NE49 9DW. Near Haltwhistle train station and bus route 685 between Newcastle and Haltwhistle. Adult £3 Children and Seniors £2. Early entry £5. JIM CORR. Tel: 0750 403 5955, email: jim_corr73@hotmail.co.uk

29 October (Tues) 6pm-9pm

GARSTANG TOYFAIR, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON. Tel: 01282 439 009.

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Wanted Clubs and Societies

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Scale or gauge should be mentioned when referring to models. Swapmeets and other events must include a telephone number at which the organiser can be contacted.

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Sales stand and model making display

October 19th – Winchester O Gauge Meet
NT&C's 'Quiberon', David Cuthbert's French Loco Shed, Society Sales Stand

October 26th – EXPO Narrow Gauge, Swanley, Kent

Our winter reunion will be held at Lenham, Kent on Sunday 9th February 2020

Membership of the Society includes a quarterly Journal, Society library and online photographic archive – apply online or write to The French Railways Society, 93 Deakin Leas, Tonbridge TN9 2JT

www.frenchrailwaysociety.org

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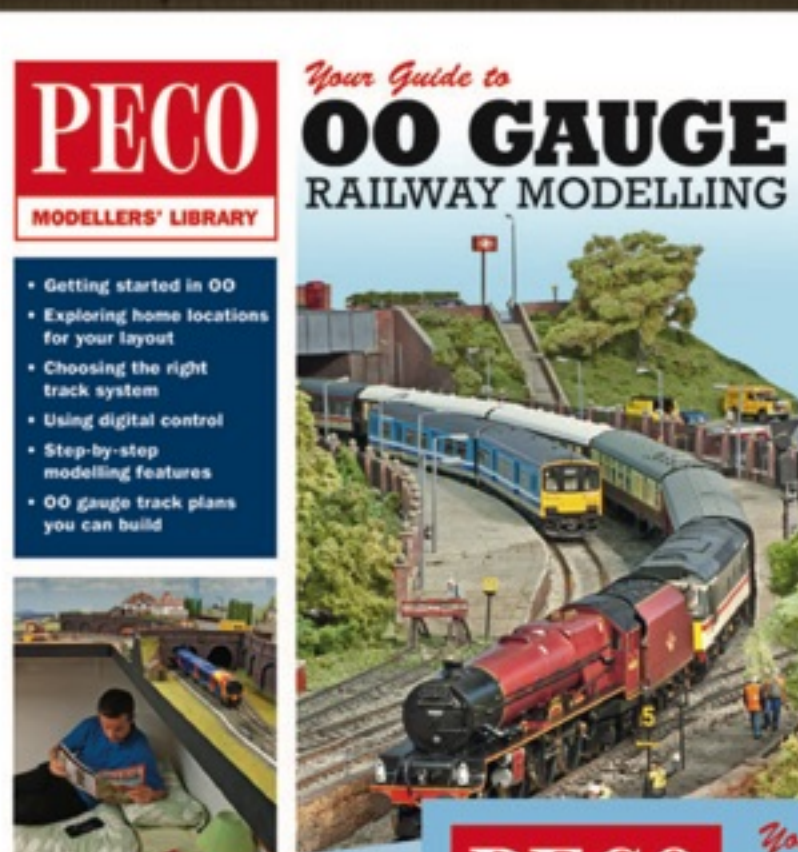
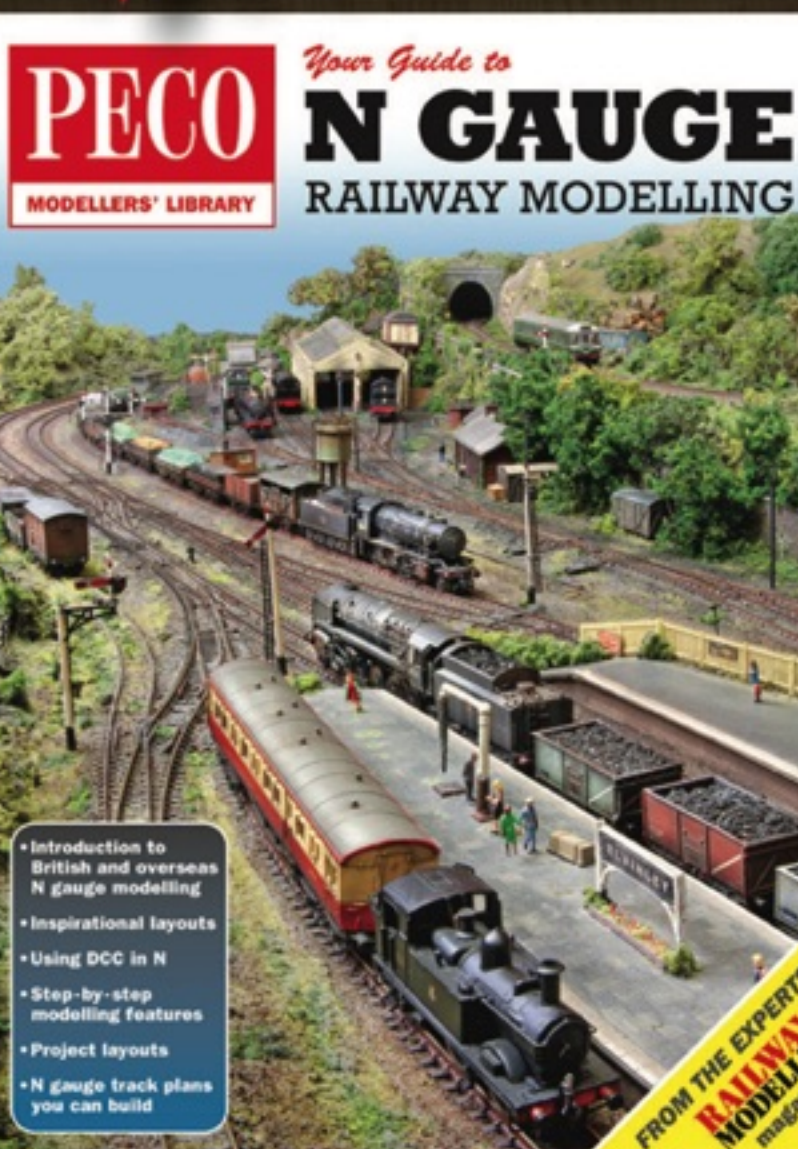
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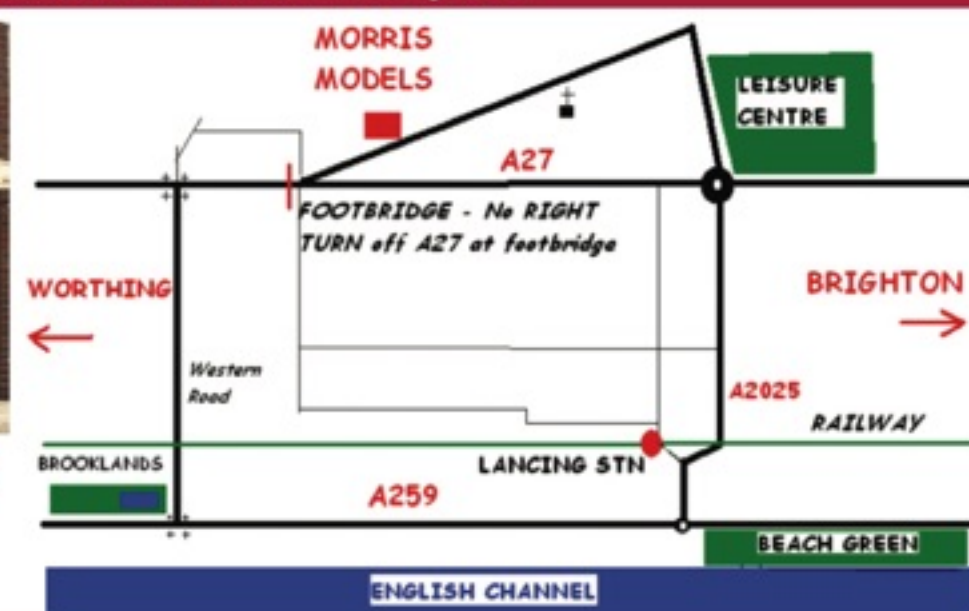


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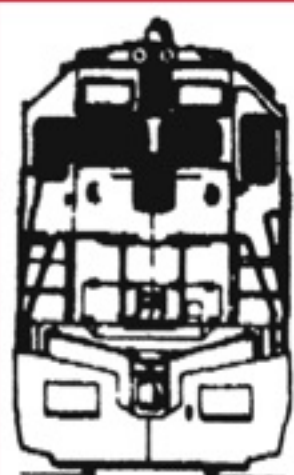
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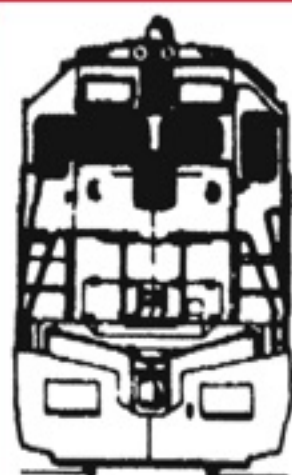


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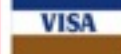
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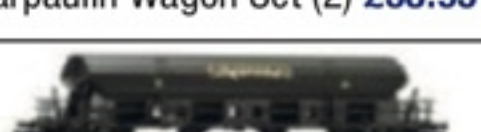
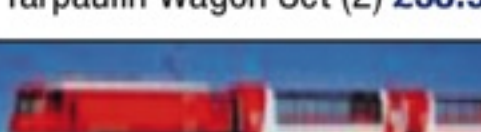

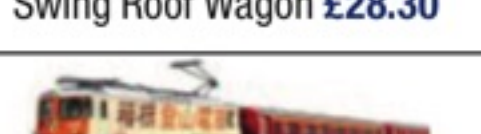

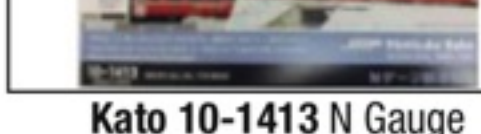



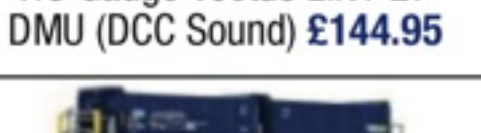
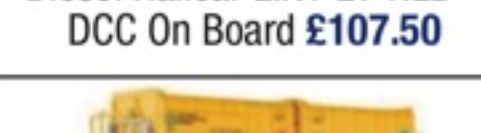
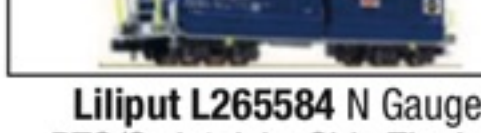
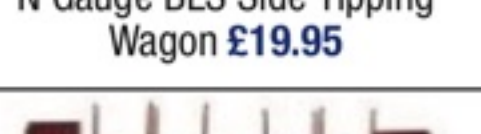
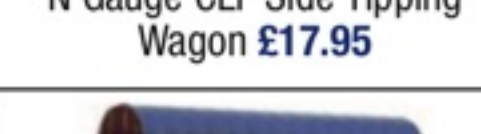

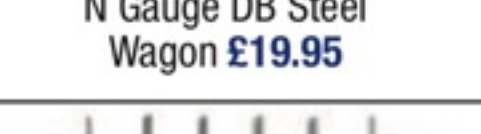

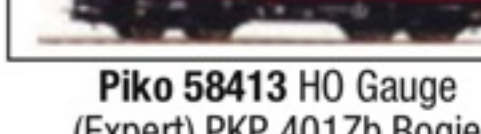
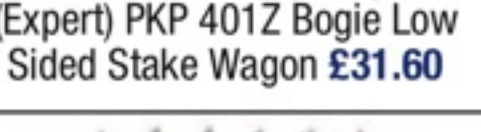
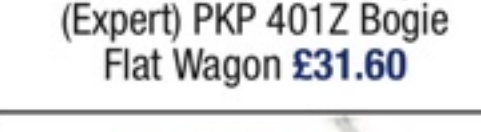
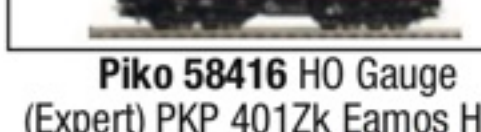
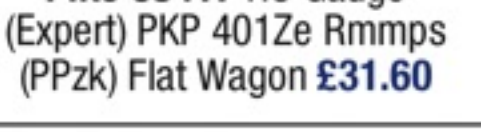

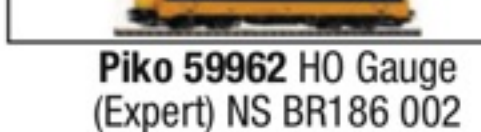
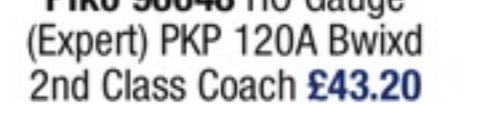

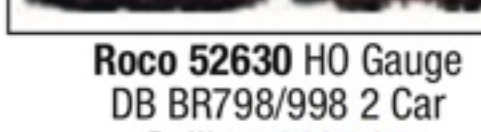
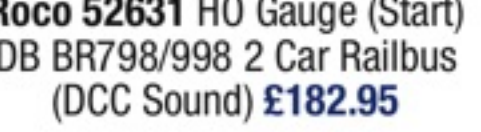
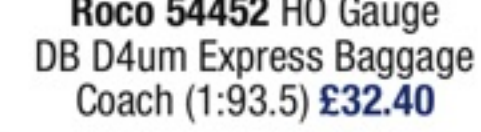
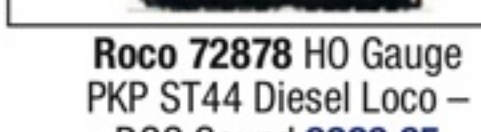
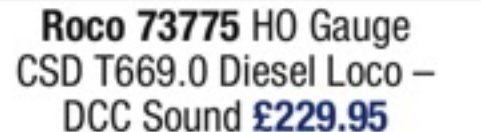

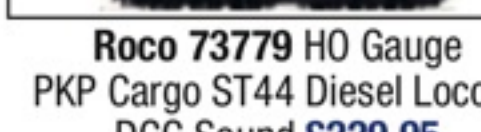
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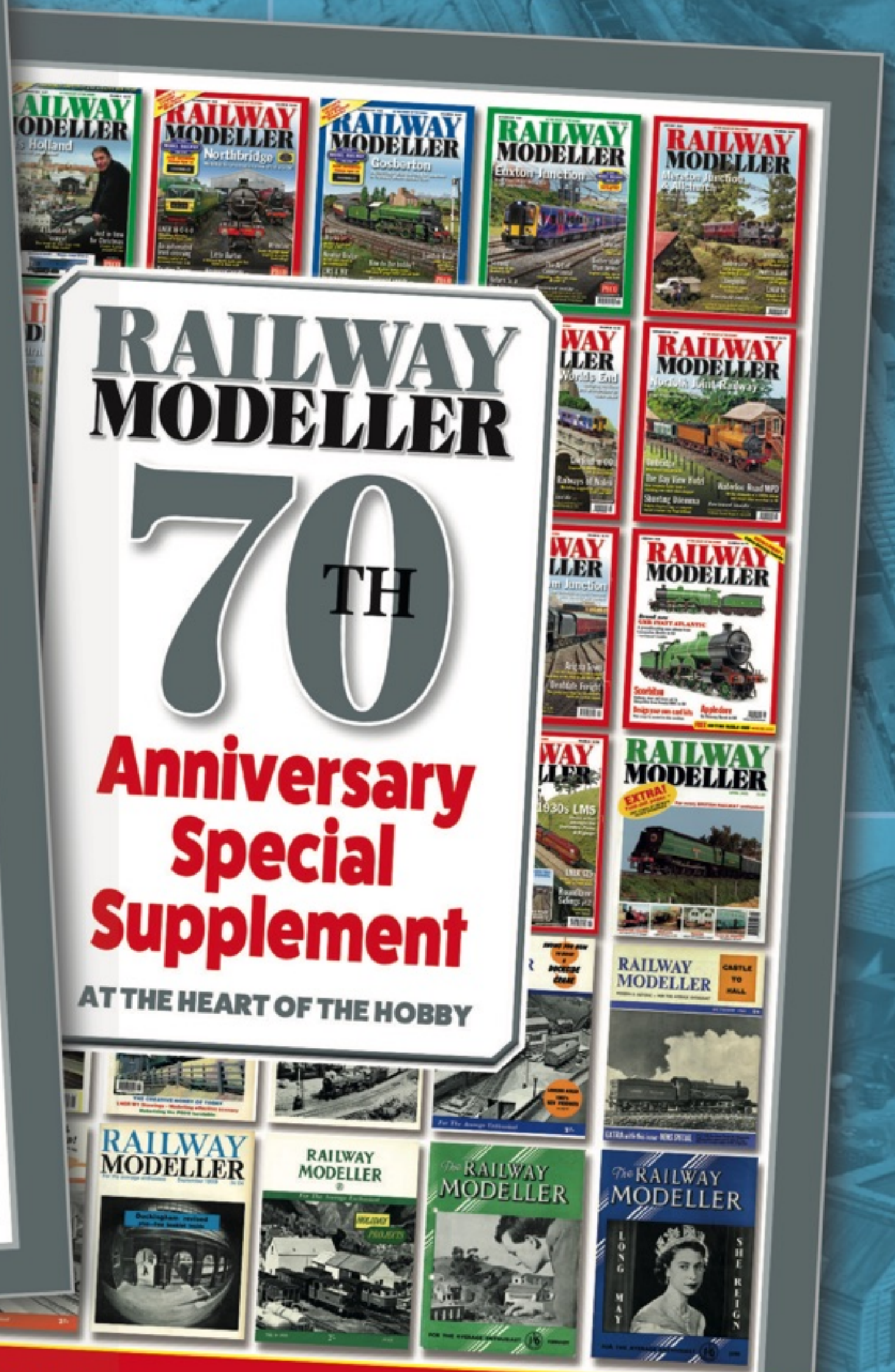
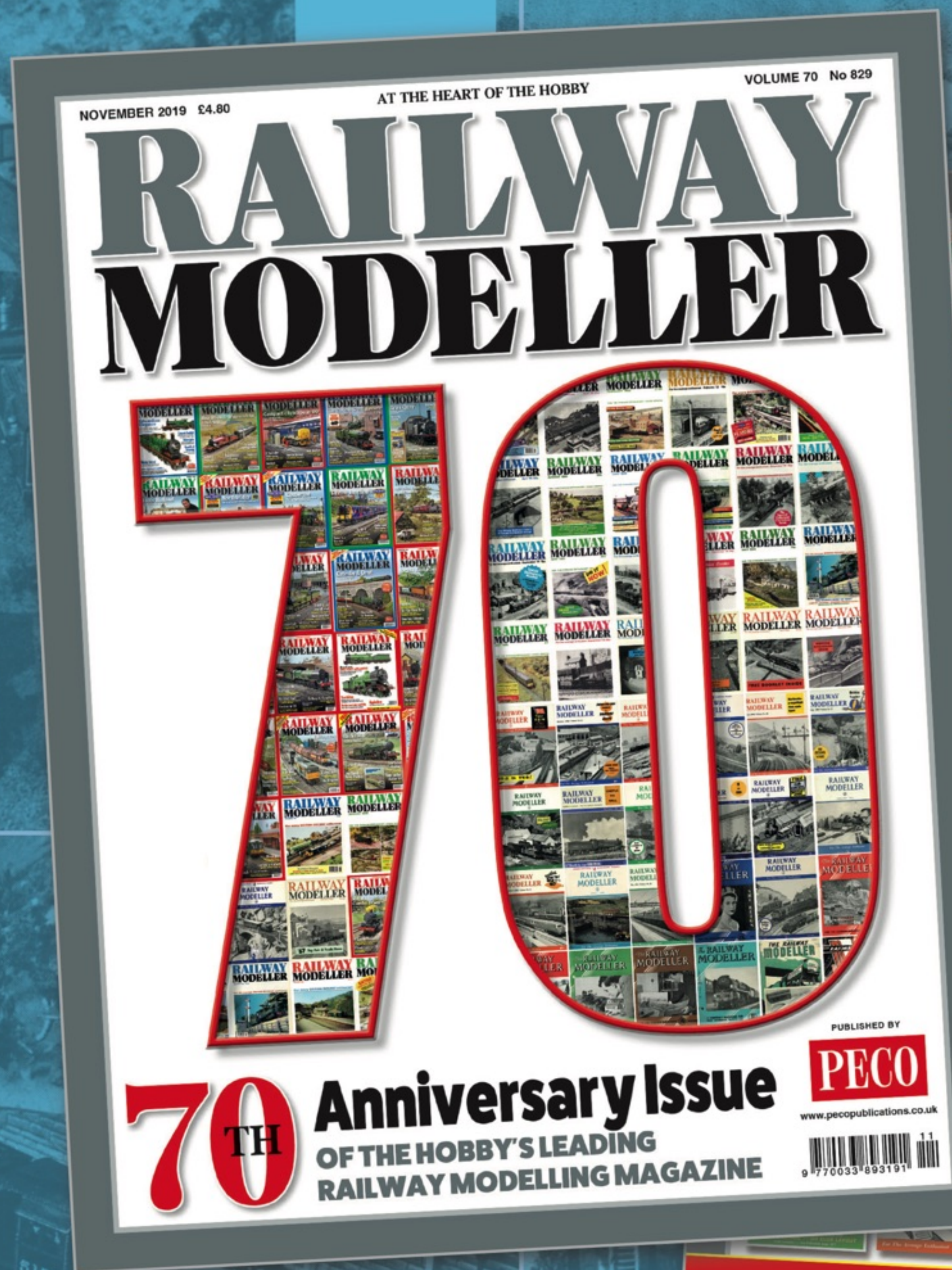
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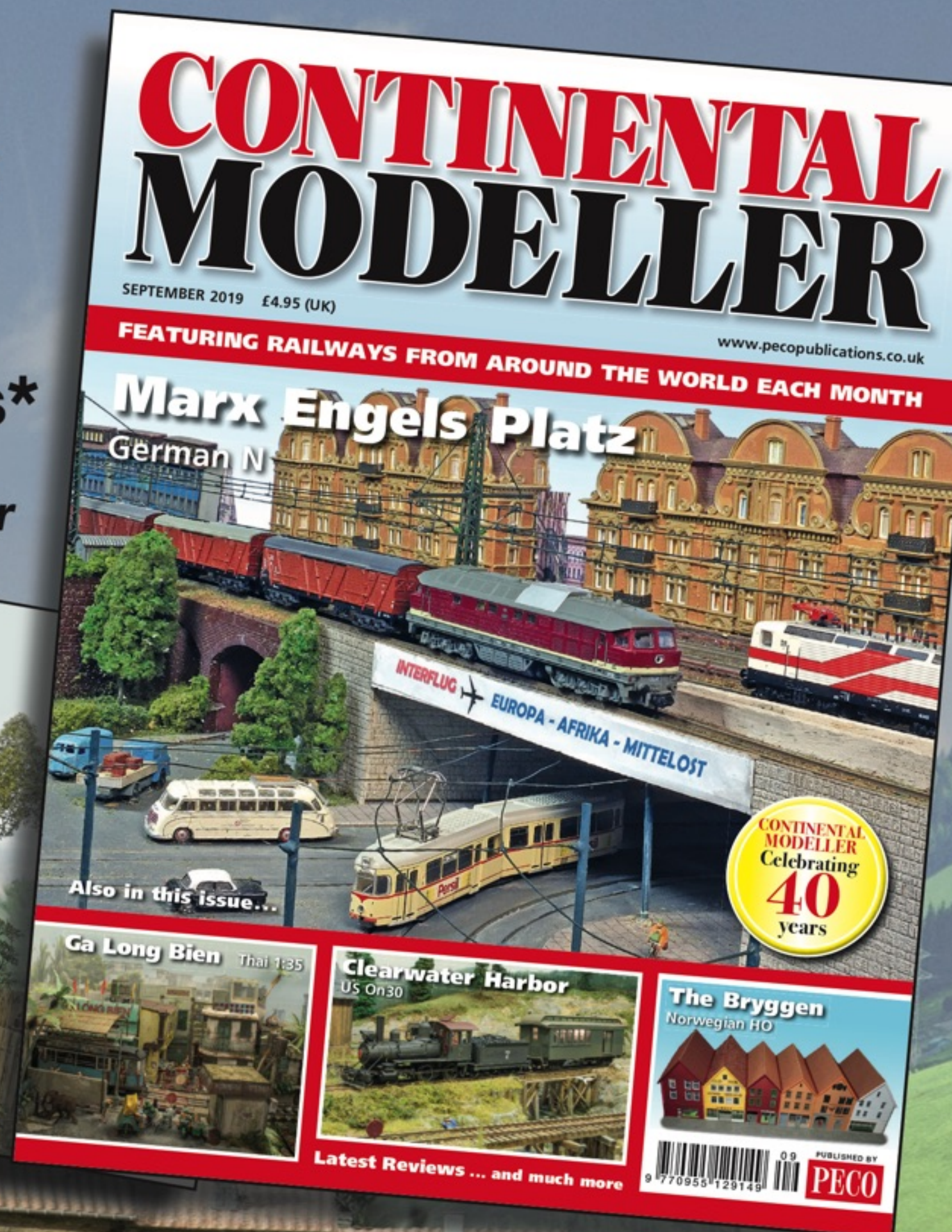


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The **Field System** makes it easy to add texture and dimension while modelling meadows, fields and pastures with incredible realism. A wide range of colours and textures allows you to model any season or grassy landscape. All products are designed to work together, and each one includes simple-to-follow instructions to make modelling easy.

Static King™ WFS639

Static King makes it easy to model large grassy areas and ensures Static Grass is applied vertically. The Static King has an ergonomic design and can be placed upside down for easy filling. It comes with different Sieve sizes for creating a varied effect using 2-12mm lengths of Static Grass. A divider allows the use of multiple colours and sizes at the same time for modelling realistic fields and other tall grasses.

- Clear hopper
- 360° Power Indicator Light
- Power via Power Supply or Batteries
- Non-slip rubber grip
- Divider allows use of multiple colours & sizes
- Easily interchange twist-lock Sieve
- Automatic discharge after each use



Also Available

- **Static Grass** in a variety of lengths and colours
- **Field Grass** to model tall grass, field grass and weeds
- **Flowers** includes four shades of colour
- **Plant Hues** includes four shades of colour
- **Briar Patch** to add brambles, thickets and shrubbery
- **Static-Tac, Tuft-Tac & Spray-Tac**
- **Model-Vac** for picking up loose landscape material
- **Accent Shakers** help easily apply **Plant Hues & Flowers**



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Rhuddlan Models - Denbighshire
Malc's Models - Derbyshire
A2B Model Railways - Derbyshire
South Devon Railway - Devon
Youings Toymaster Model Shop - Devon
Shady Lane - Devon
Buffers Model Railways - Devon
Colletts Model Trains - Devon
Bekra Models - Devon (Newton Abbot)
Setchfield's - Dorset
Model Railway Solutions - Dorset
Bluebell Railway - East Sussex
Train Times - East Sussex
John Dutfeld - Essex
Flair Rail - Essex
Cheltenham Models - Gloucestershire
Bourton Model Railways - Gloucestershire
Sawyer Models - Greater Manchester
Arcadia International Model - Greater Manchester
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Test Valley Models - Hampshire
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Upstairs Downs - Isle Of Wight
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Macs Model Railroading - Scotland (Forfar)
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Signals - Midsummer Norton (Somerset)
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Topp Trains - Staffordshire
Model Junction - Suffolk
Flair Rail - Suffolk
Orwell Model Railways - Suffolk
Roxley Models - Surrey
Kernow Model Centre - Surrey
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KDC Hobbies - Wales (Ammanford)
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Rhuddlan Models - Wales (Rhuddlan)
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Tony's Trains at Barby Model Rail - Warwickshire
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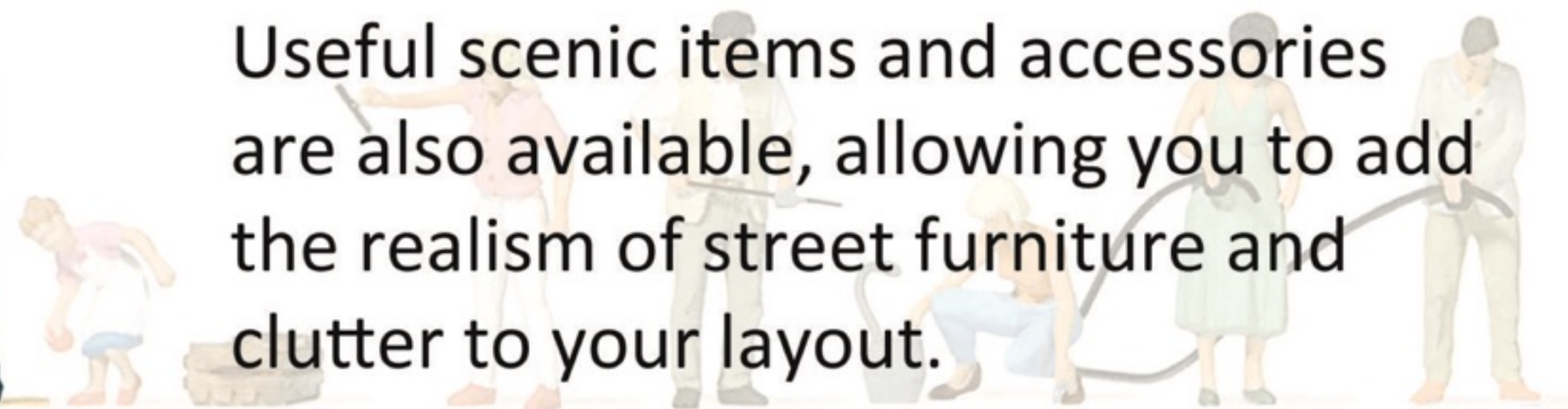
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