

# CONTINENTAL MODELLER

JULY 2019 £4.95 (UK)

[www.pecopublications.co.uk](http://www.pecopublications.co.uk)

FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

VOLUME 41

## Nordhafen

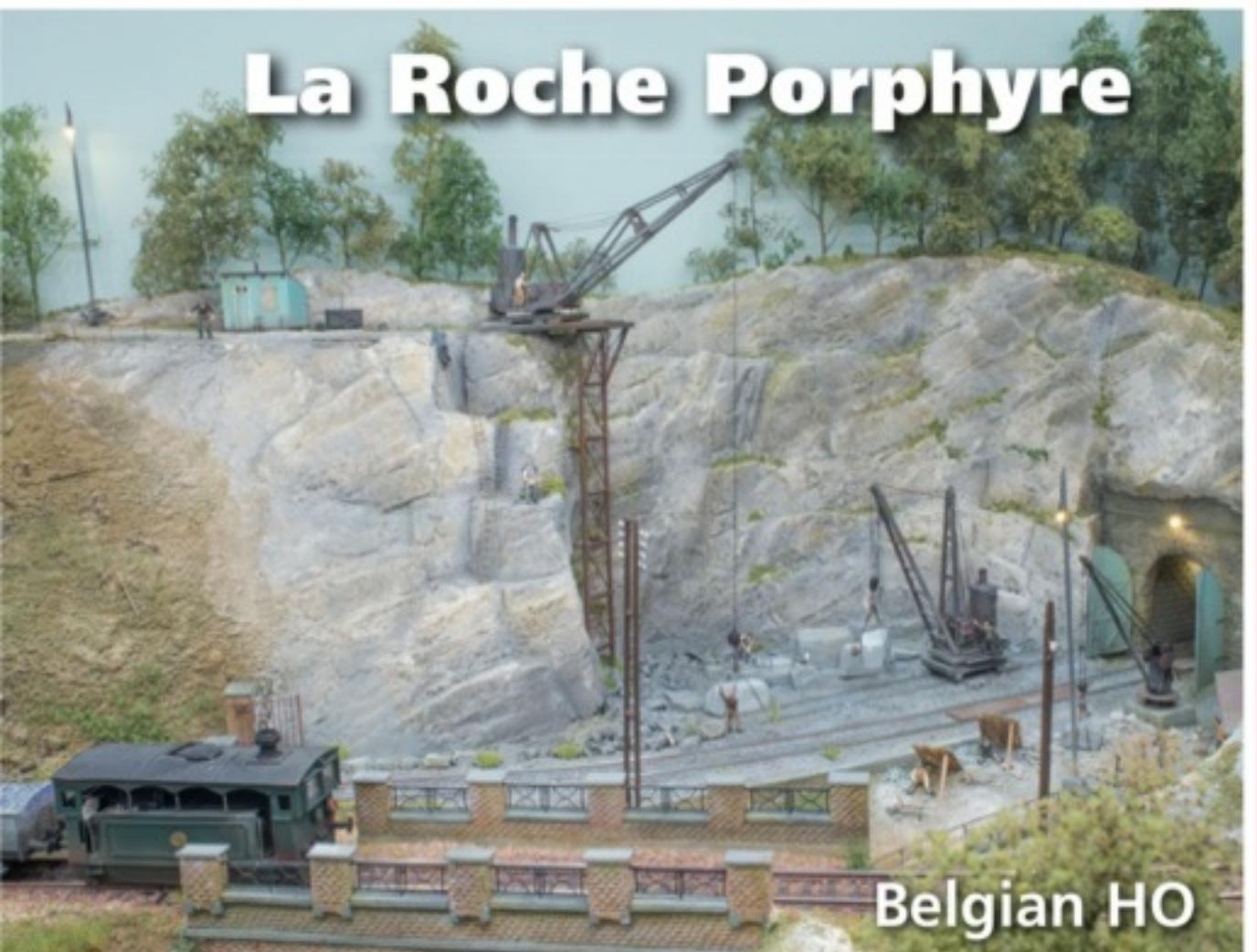
German HO



Also in this issue...

CONTINENTAL  
MODELLER  
Celebrating  
**40**  
years

**La Roche Porphyre**



Belgian HO

**Four Feather Falls**



US On30

**Porta Alpina**

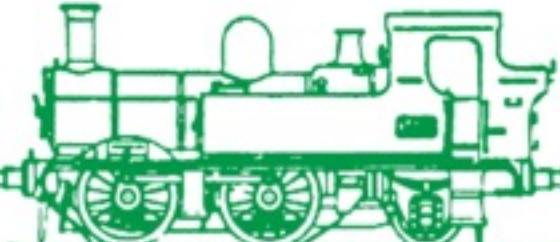


Swiss HO

Latest Reviews ... and much more



PUBLISHED BY  
**PECO**

**KITTLE HOBBY**

★ ACCESS : VISA : SWITCH CUSTOMERS ORDER BY 'PHONE.

**Walthers HO USA**

post and packing £3.99

(all items in stock at time of going to press)



451 GP9 Boston & Maine	£68.00
102 GP9 Union Pacific	£68.00
134 CNW	£68.00
113 GP9 Santa Fe (red)	£68.00
101 GP9 BN (green)	£68.00
105 GP9 CSX	£68.00
120 GP9 BNSF (pumpkin)	£68.00
10012 Plymouth Switcher (black)	£98.00



9913 F7 a unit Southern	£125.00
9909 F7 a unit Pennsylvania	£125.00
10067 Pennsylvania PA/PB	£250.00
9457 F40 Caltrain (DCC ready)	£125.00
9459 F40 Metra (DCC ready)	£99.00



9232 Western Pacific SW1	£90.00
48612 SD9 Conrail (black & white)	£135.00
10174 GE ES44AH Union Pacific	£128.00

**Track Cleaners**

1482 Denver, Rio Grand	£26.00
1756 Union Pacific	£26.00
1066 Replacement pads	£5.20
1753 Burlington Northern	£27.50

**Caboose**

8670 Union Pacific Bay Window	£34.00
1502 Union Pacific	£24.00
1503 AT & SF	£20.25
1529 Denver & Rio Grande	£17.80
1501 Burlington Northern	£17.80

**Gondola**

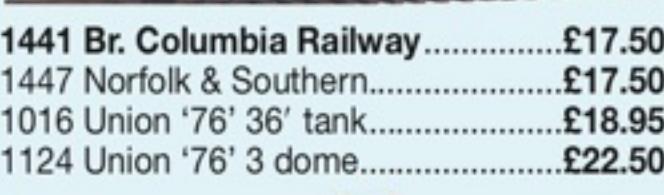
6074 Union Pacific 53'	£19.50
5671 Pennsylvania 40'	£18.95

**Flats**

5148 53' TTX	£24.80
5131 53' Conrail	£19.95
5127 53' Burlington Northern	£19.95
104511 ACL 50' Pulpwood	£21.50
5113 53' Flat C & NW	£19.95
104505 MP 50' Pulpwood Flat	£21.50

**Tanks**

1441 Br. Columbia Railway	£17.50
1447 Norfolk & Southern	£17.50
1016 Union '76' 36' tank	£18.95
1124 Union '76' 3 dome	£22.50



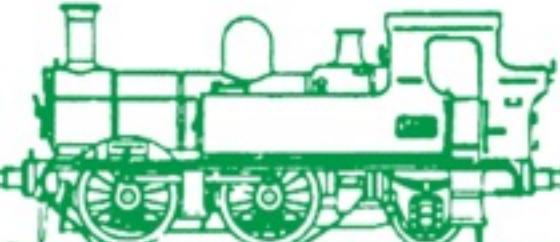
1442 Cooks Paints	£17.00
1613 Amoco	£16.50
1614 Conoco	£16.50
1612 Gulf	£17.00
100524 Pan-Am Oils	£29.95
100501 American Gas	£28.50

**Military Modelling**

MP99180 Flat with rocket launcher	£19.75
MP98665 40' Ammo Box Car	£13.45
MP98663 Army Oil Tank	£14.50



W14036 M36 Tank Destroyer	£8.85
MP99160 Flat + Patton Tank	£18.25
E4023 Land Rover WW2	£7.40

**RHONGYR UCHAF HOUSE, off Penwylt Rd, Penycae, Swansea SA9 1GD**

(Callers welcome by appointment please)

All correspondence to Kittle Hobby/Banks of Kittle

P.O. Box 05, Ystalyfera, Swansea SA9 1YE

★ NON E.E.C. CUSTOMERS OVER £50 VAT FREE.

Tel. 01639 731005

E-mail: [kittlehobby@gmail.com](mailto:kittlehobby@gmail.com)Website:  
[www.kittlehobby.com](http://www.kittlehobby.com)**Walthers HO USA**

post and packing £3.99

(all items in stock at time of going to press)



451 GP9 Boston & Maine	£68.00
102 GP9 Union Pacific	£68.00
134 CNW	£68.00
113 GP9 Santa Fe (red)	£68.00
101 GP9 BN (green)	£68.00
105 GP9 CSX	£68.00
120 GP9 BNSF (pumpkin)	£68.00
10012 Plymouth Switcher (black)	£98.00



9913 F7 a unit Southern	£125.00
9909 F7 a unit Pennsylvania	£125.00
10067 Pennsylvania PA/PB	£250.00
9457 F40 Caltrain (DCC ready)	£125.00
9459 F40 Metra (DCC ready)	£99.00



9232 Western Pacific SW1	£90.00
48612 SD9 Conrail (black & white)	£135.00
10174 GE ES44AH Union Pacific	£128.00

**Track Cleaners**

1482 Denver, Rio Grand	£26.00
1756 Union Pacific	£26.00
1066 Replacement pads	£5.20
1753 Burlington Northern	£27.50

**Caboose**

8670 Union Pacific Bay Window	£34.00
1502 Union Pacific	£24.00
1503 AT & SF	£20.25
1529 Denver & Rio Grande	£17.80
1501 Burlington Northern	£17.80

**Gondola**

6074 Union Pacific 53'	£19.50
5671 Pennsylvania 40'	£18.95

**Flats**

5148 53' TTX	£24.80
5131 53' Conrail	£19.95
5127 53' Burlington Northern	£19.95
104511 ACL 50' Pulpwood	£21.50
5113 53' Flat C & NW	£19.95
104505 MP 50' Pulpwood Flat	£21.50

# CONTINENTAL MODELLER

July 2019  
Volume 41  
Number 7

## From the Editor

**Editor** ANDREW BURNHAM  
**Assistant Editor** TIM RAYNER  
**Associate Editor & Photographer** STEVE FLINT  
**Editorial Assistant**  
**Art Director** ADRIAN STICKLAND  
**Graphic Illustration**  
DAVE CLEMENTS, GARY BICKLEY, STEVE CROUCHER  
**Review Photographer** JOLYON SARGENT  
**General & Advertisement Manager** JOHN KING  
**Advertisement Assistant** SUE DAVIS  
**Classified Advertisements** NICOLE CHARLTON  
**Direct Subscriptions** ALICIA KNIGHT  
**Chairman** C.M.PRITCHARD  
**Published on the third Thursday of the preceding month by**  
Peco Publications & Publicity Ltd.,  
Beer, Seaton, Devon, EX12 3NA, England.  
Telephone: 01297 21542. Fax: 01297 20229.  
E-mail: [cm-editor@btconnect.com](mailto:cm-editor@btconnect.com)  
Website: [www.pecopublications.co.uk](http://www.pecopublications.co.uk)

*Distribution to the model trade, & direct subscriptions (Home & Overseas): Pritchard Patent Product Co.Ltd. (address and telephone as above)*

*Distribution to the newsagency trade (Home & Overseas): Marketforce (UK), Second Floor, 5, Churchill Place, Canary Wharf, London, E14 5HU.*

*Printed by*  
William Gibbons & Sons Ltd., P.O.Box 103,  
26, Planetary Road, Willenhall, West Midlands, WV13 3XT.

© Peco Publications & Publicity Ltd. 2019

All rights reserved. No part of this magazine may be reproduced or transmitted in any form or by any means, electronic or mechanical, including photocopying, recording, or by any information storage and retrieval system, without prior permission in writing from the copyright owners. Multiple copying of the contents of this magazine without prior written approval is not permitted.

The magazine shall not without the written consent of the publishers be lent, resold, hired out, or otherwise disposed of by way of trade at a price in excess of the recommended maximum price, or in mutilated condition, or in any unauthorised cover, or affixed to or as part of any publication or advertising matter whatsoever.

Views expressed by contributors are not necessarily those of the publishers.

While every care is taken to ensure that the contents of the magazine are correct, the publishers cannot be held responsible for any errors that may occur in the editorial or advertisements nor for the actions of the advertisers.

ISSN 0955 – 1298

*Annual subscription (12 issues):*  
United Kingdom £48.06 (paid by Direct Debit), post free.  
Overseas (including Eire) £79.00.  
*Back numbers – for information, see the classified advertisement under 'Trade Sales Books'.*

## RAILWAY MODELLER

For all modelling British railway practice.  
Published on the second Thursday  
of the preceding month.



**Cover**  
**Deutsche Bundesbahn**  
**B-B diesel hydraulic 290 189-0**  
**draws a rake of wagons along**  
**the quayside at Nordhafen**  
**on the new German HO layout**  
**by Roger Nicholls.**  
Photo: Derek Shore.

## Worldwide appeal

Attending major shows on the continent, either as visitors or exhibitors, frequently finds interesting material for the magazine. Some of the results of our excursions this year so far, to OntraXS!, Intermodellbau, and Trainsmania, will be found in this issue. Among them is *Tatar Sunda*, which we encountered in Utrecht being looked after by our good friend Ted Polet. We were surprised to discover that it had been built in Indonesia, by local modellers, and then shipped to The Netherlands – though not just for the show!

At such large exhibitions we have occasionally seen layouts from eastern or southern Europe, but bringing them from much further afield becomes less practical, though there have been examples from as far as Turkey, and there was an O gauge layout brought to the UK from Australia.

Railway modelling is almost inevitably the preserve of those who have spare time, space, and resources. We may think of it as primarily occurring in developed countries (Europe, north America, Australia/New Zealand, and Japan) but there is evidence of growing interest in local prototypes among modellers in China and Taiwan, and we were also interested to hear about HO semaphore signals being produced by Model Trains India (see *News*, p.526), presumably designed initially to meet the requirements of local enthusiasts.

## The Market Deeping show incident

At the risk of dwelling on bad news, we could hardly let the incident at Market Deeping go unmentioned. During the early hours of Saturday 18th May (while this issue was being prepared) four youngsters broke in and smashed up one hall of an exhibition due to open later that day, which had been partially set up during the previous evening.

There can be absolutely no justification for such mindless vandalism and of course our sympathy goes to all those affected. As fellow modellers we understand.

We are clearly not alone, as the amazing response to the appeal for funds demonstrated, from near and far, from some celebrities and many ordinary modellers, and in a surprisingly short time. A substantial amount has been raised, far in excess of expectations, and this will bring its own burden of responsibility.

Money alone cannot compensate for much of the loss; proprietary items could be replaced but much of what was lost was, we understand, kit- or scratchbuilt, representing countless hours of effort and skill, and indeed the creation of any layout goes well beyond the sum of its component parts.

As has been noted, most of the damage was suffered by visiting layouts and trade stands, not the organising club.

The incident made national and indeed international news, which no doubt assisted the appeal, but we think it is sad that it takes such an occurrence to attract the attention of the mainstream media (print, on line, and broadcast). We were interested to observe how the 'facts' become garbled in the repeated telling, peaked with celebrity involvement, and then within a couple of days it was all over – good news is no news.

But there is some good news. One of those affected, our contributor Dave O'Rourke, present with his current British OO layout, vowed to have it rebuilt in time for its next booked appearance – in just three weeks. Significantly this nearby local event was being organised by another of the layout owners who was even more badly affected. We applaud Dave's determination not to be discouraged even after such a setback.

Our hobby should be a relaxation, an escape, if you will, from the pressures of the real world, in short – fun! We sincerely hope that for all those affected by the Market Deeping incident it can continue to be so.

# CONTINENTAL MODELLER

July 2019  
Volume 41  
Number 7

## Contents



### 458 FOUR FEATHER FALLS – RAILWAY OF THE MONTH

Ernie Melville relates how he persuaded other members of the Cupar & District MRC to build an American narrow gauge layout prompted by the Bachmann Spectrum On30 range.

### 468 TWO SMALL CAFÉS

Emmanuel Nouaillier continues his campaign devoted to modelling urban architecture in miniature, here setting the second completed café into a scene.

### 472 NORDHAFEN – PLAN OF THE MONTH

Roger Nicholls returns to European HO modelling with a compact quayside scene set in northern Germany.

### 478 LA ROCHE PORPHYRE

As always planned, Rudi Nelissen has expanded his award-winning Belgian Vicinal HO micro layout extending the station area and serving a stone quarry.



468



478

**490****508****482 PORTA ALPINA**

An ingenious way of extending a Swiss HO layout – Boyd Misstear shows how he built a tunnel station.

**486 NISHIKAWA**

Simon Holtby reveals how he managed to find space for a third layout in the railway room, as a result of random purchases of tempting Japanese N models.

**490 ERZBERGBAHN**

Ore, rack, steam, and snow – an unforgettable experience: Guy Tyteca recalls a January 1976 visit to the famous mountain line in Austria while it was still steam worked.

**494 TWIN FORKS**

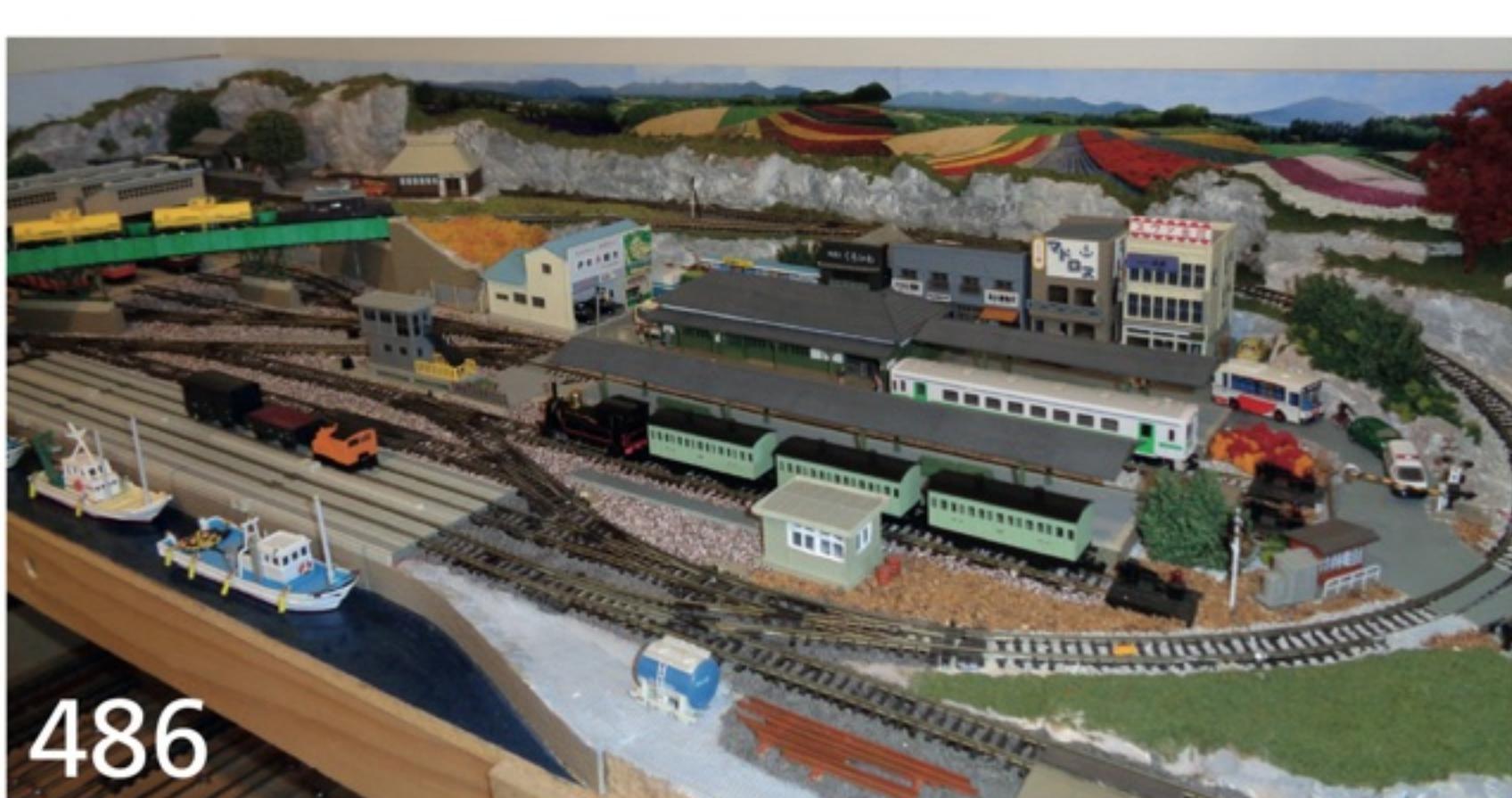
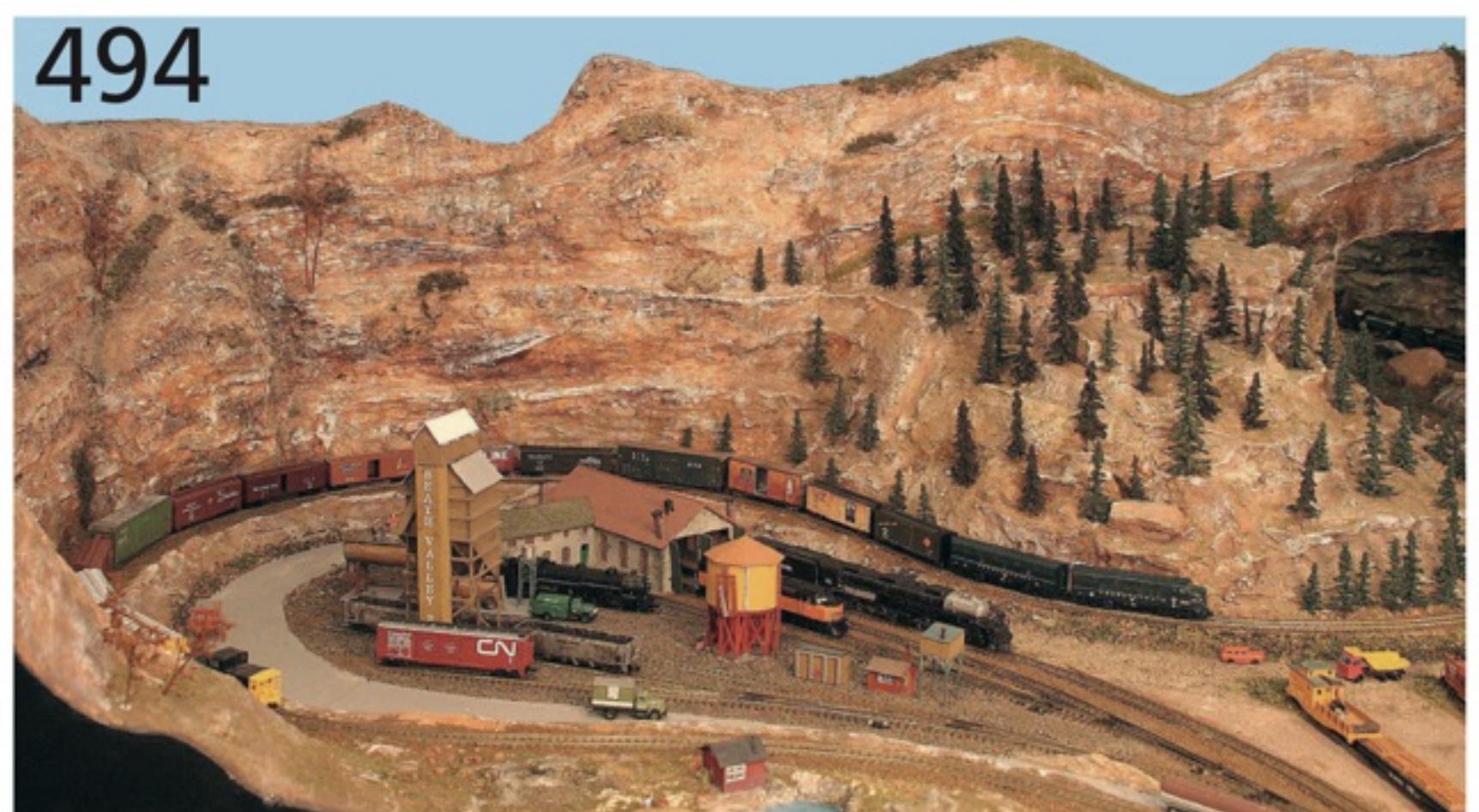
The latest instalment in the Seath Valley Railroad saga: Mike Garforth brings his extensive American N layout down from the loft to replace the G scale in the garage.

**502 TATAR SUNDA**

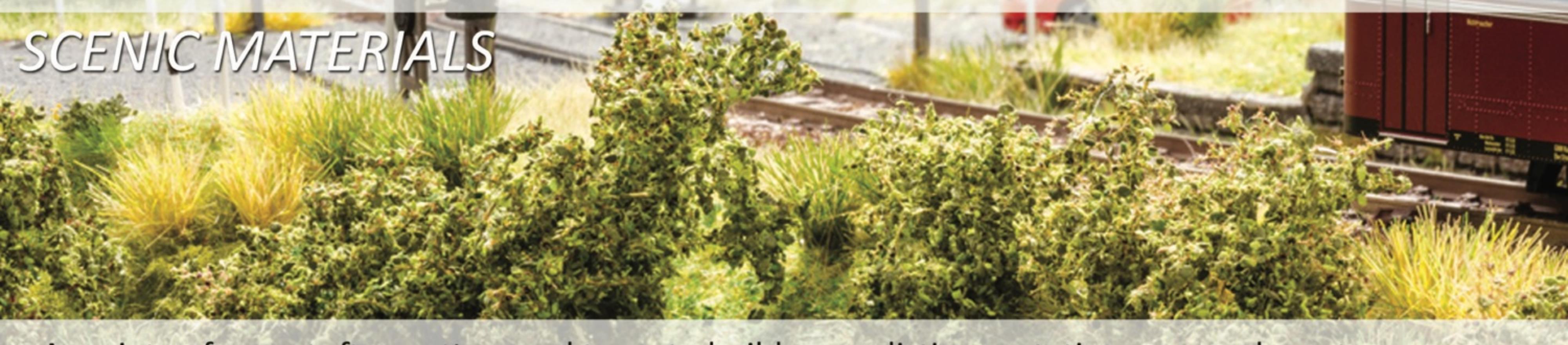
Ted Polet describes an Indonesian N gauge layout which was built by Indra Setia and Irin Kusnadi.

**508 WOLFENFELS**

The German N loft layout Jeff Ware has been developing for over two years is still considered 'work in progress'.

**516 PRODUCT REVIEWS****522 BOOK and VIDEO REVIEWS****526 NEWS****528 EXHIBITION DIARY****486****482****494****516**

## SCENIC MATERIALS



A variety of grass tufts, scatters and trees to build up realistic vegetation on your layout. Just blend them together to get the desired effect.

## MODEL FIGURES



Noch produce a comprehensive range of pre-painted and unpainted figures to populate your layout, from passengers to the emergency services.

## LASER-CUT PLANTS



Made from Laser-Cut paper, these kits simply fold together to create realistic plants and vegetables to plant onto your layout.

## DECORATIVE SCENES



A complete themed scene in a pack, ready to add life to your layout - including the new Sound Scenes range which adds an extra dimension.

# märklin



Marklin are one of the oldest model railway companies in the world.

They have over a century of expertise, and currently produce models in Z, HO, and Gauge 1 Scales.



### Z 1:220 Scale -

One of the smallest commercially available model railway scales and ideal if you only have a small space, but don't want to compromise on detail.



### HO 3-Rail 1:87 Scale -

Marklin produce a unique 3-Rail system in HO Scale, with locomotives and stock spanning the eras from the dawn of railways to the present day.



### Gauge 1 1:32 Scale -

The Gauge 1 stock produced by Marklin is breathtakingly detailed. Extra realism is provided by MFX-Sound decoders and smoke generators in the steam engines.

## Marklin Central Station 3

The CS3 is an innovative digital controller for both DC and AC layouts. It has a high resolution touchscreen display, and many possibilities for networking. It has two locomotive controllers and a central track diagram allows simple, easy control of locomotives and the layout.



MARKLIN PRODUCTS ARE DISTRIBUTED IN THE UK BY GAUGEMASTER CONTROLS LTD. AND ARE AVAILABLE FROM YOUR LOCAL MODEL SHOP, OR, IN CASE OF DIFFICULTY, DIRECT FROM OURSELVES.

GAUGEMASTER Controls Ltd., Ford Road, Arundel, West Sussex, BN18 0BN, UNITED KINGDOM

tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com [www.gaugemaster.com/marklin](http://www.gaugemaster.com/marklin)

# A Comprehensive Model Railway System

**PIKO**

Offering great value for money, PIKO is definitely the range to look at for beginners to German railways and existing collectors alike, with a variety of starter sets on offer.



The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert.

The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology.

The HO Scale A-Track has clear, simple geometry and a realistic appearance.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits.

The locomotives run smoothly, with accurately printed liveries and DCC compatibility.

This range includes many passenger multiple units that run around Europe, not just in Germany. this allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.



The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors, and is great value for money.

The locomotives feature directional lighting, and some even have sound and smoke.

Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train.

A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale.

New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.





# Preiser

## Figures for Every Scale



Preiser produce the most comprehensive range of model figures on the market. There is a vast array of different subjects available, allowing you to populate every area of your layout.

Railway Scales covered:  
Z, N, TT, HO, OO, O, G, and  
Gauge 1.



A selection of other scales useful to architects and modellers are also included in the range.



Useful scenic items and accessories are also available, allowing you to add the realism of street furniture and clutter to your layout.

Most of the range are pre-painted and ready to add to your layout, with unpainted packs available for those who want a bit more customisation. A range of single figures are available in HO Scale.

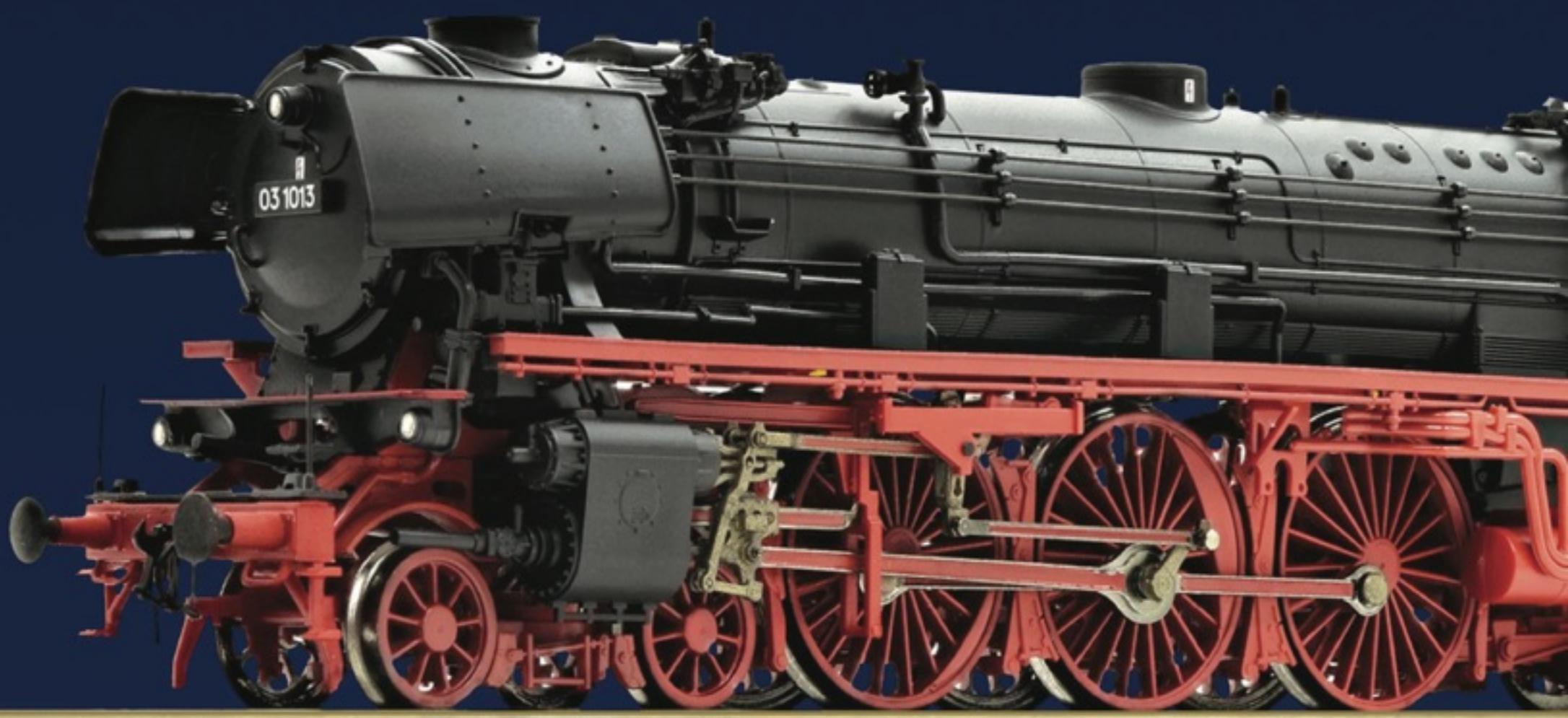


Preiser also produce value packs, allowing you to cover your layout with people for a keen price. You're sure to find what you need in the Preiser range.

## HO Scale

Roco produce a large range of HO Scale German Railway locomotives nad rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras!

A HO track system is also available, including the Rocoline pre-ballasted track.



## TT Scale

TT Scale is 1:120 Scale and is a good compromise if you want the detail of HO but only have the space for a smaller layout and don't want to go down to N Scale.

Roco produce a small but growing range of items in this size, so they are will worth exploring in more detail.



## Z21 Digital System

It can be programmed to change locomotive speed and points at the touch of a button, and as it is a free app, anyone can join in as long as they have a supported device!

The Z21 digital system is an innovative way of controlling your layout. Utilising any Apple or Android phone/tablet, you can control trains and accessories on your layout using the Z21 app.

# TRIX



## N and HO Scale Continental Specialists

### MINITRIX N 1:160 Scale

Minitrix produce an extensive range of German locomotives and rolling stock, from the steam age to the modern era.

N Scale gives you the flexibility to build a complex layout in a relatively small area.



### TRIX HO 1:87 Scale

The Trix range covers the majority of Europe in HO Scale, but especially Germany, spanning multiple eras and embracing DCC sound technology. A full range of locomotives and rolling stock is available.



### HO 1:87 Scale C-Track

The Trix C-Track range is a robust, pre-ballasted track system that simply clips together giving a firm, reliable electrical contact between track sections - simply clip together and get your locomotives running.

TRIX PRODUCTS ARE DISTRIBUTED IN THE UK BY GAUGEMASTER CONTROLS LTD. AND ARE AVAILABLE FROM YOUR LOCAL MODEL SHOP, OR, IN CASE OF DIFFICULTY, DIRECT FROM OURSELVES.

**GAUGEMASTER** Controls Ltd., Ford Road, Arundel, West Sussex, BN18 0BN, UNITED KINGDOM

tel - 01903 884488 fax - 01903 884377 email - sales@gaugemaster.com [www.gaugemaster.com/trix](http://www.gaugemaster.com/trix)

# Begin a new adventure in railway modelling!

**NEW!**

These superb new publications, brought to you by the experts at Continental Modeller and Railway Modeller magazines, contain a wealth of information about modelling the fascinating railways of Switzerland and explore the exciting possibilities of modelling the UK heritage railway scene.

**ON SALE  
NOW!**

**Only  
£6.99  
each**



Photo: Derek Shore

**PECO**

**MODELLERS' LIBRARY**

Available from: selected W.H. Smith outlets plus your local Peco Stockist. Also available by post direct\* (from the address below), or via our website [www.pecopublications.co.uk](http://www.pecopublications.co.uk) \*Please add £2.00 towards P&P.

PECO PUBLICATIONS & PUBLICITY LTD, BEER, DEVON EX12 3NA, ENGLAND Tel: 01297 626203 Fax: 01297 20229

**DB AG Electric locomotive BR 147****ARNOLD**<sup>®</sup>**HN2438 / HN2438D** DCC DIGITAL

DB AG, electric locomotive class 147, traffic red livery, DB Regio, period VI

**£ 147,99 / £ 191,99****SNCF Autorail X 2450****Jouef**<sup>®</sup>**HJ2359 / HJ2359S** DCC SOUND

SNCF, diesel railcar class X 2400 without smoke deflectors in beige/red livery, X 2450, Depot Marseille, period III

**£ 161,99 / £ 270,49****FS Steam Locomotive S 100****Rivarossi**<sup>®</sup>**HR2641 / HR2642** DCC SOUND

FS, steam locomotive S 100 (ex USATC) in black livery, period III

**£ 156,99 / £ 239,99****RENFE/SNCF Talgo "Elipsos"****Electrotren**<sup>®</sup>**E3272**

RENFE/SNCF, Talgo hotel train "Elipsos", consists of 6 coaches. The real train is in service between Madrid, Paris, Barcelona, Zurich and Milan, period V-VI

**£ 174,99**

WE PAY  
STRAIGHT AWAY!



# SELL YOUR SECONDHAND LOCOS & ROLLING STOCK FOR CASH

• Trade in with Hatton's for immediate payment on the day that we receive your goods!\* .

Get cash for your items in just four easy steps...



“ An excellent service everytime without fault. I honestly cannot think of anything which could be improved.”  
Mr A Manning, Ipswich

“ Excellent service, fair offer. I will not hesitate to use their service again and recommend it. Thank you.”  
Mr P Shaddock, Portsmouth

“ Very simple to trade-in. Beforehand I had thoughts of problems with couriers and the like, but all very easy.”  
Mr D Spottiswoode, Kilmarnock

ALL brands purchased including:



**Sell yours today at [www.hattons.co.uk/preowned](http://www.hattons.co.uk/preowned)**  
**Call us on 0151 305 1755 or email [preowned@hattons.co.uk](mailto:preowned@hattons.co.uk)**

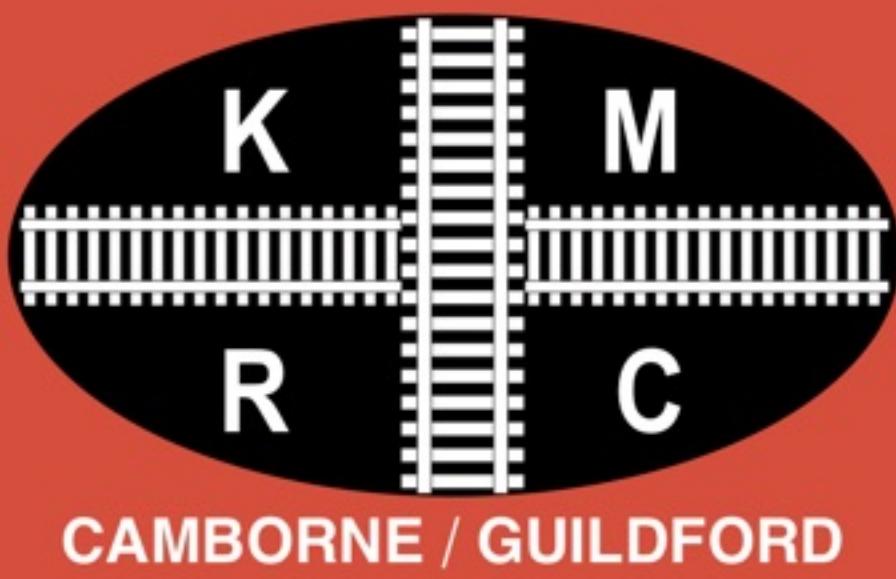
**17 Montague Road, Widnes WA8 8FZ**

Phone opening times: Mon to Sun 9am-5pm.

\*Same day payments apply for collections received Monday to Friday before 2pm. Any collections received after 2pm will be paid on the next working day.

Please note: In the unlikely event that we revise our offer price for your collection, you will be paid after responding to our offer.

For full Terms and Conditions, visit [hattons.co.uk/preownedterms](http://hattons.co.uk/preownedterms)



CAMBORNE / GUILDFORD

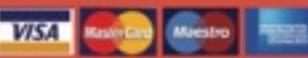
# Kernow Model Rail Centre

98A Treloowarren Street, Camborne, TR14 8AN / 75b Stoke Road, Guildford, GU1 4HT

Call our team on +44 (0) 1209 714099 [www.kernowmodelrailcentre.com](http://www.kernowmodelrailcentre.com)

UK POSTAGE & PACKING = £4 PER ORDER  
ORDERS OVER £200 = POST FREE

Shop times - Monday to Saturday: 09:30 - 17:00  
Sunday: 10:00 - 16:00



## BARGAINS - HO GAUGE

### BACHMANN HO

16701 Flat Car 35ft Trailer Santa Fe Navajo Freight.....	£28.99
16801 Northeast Steel Caboose Reading.....	£12.99
17702 36ft Wide Vision Caboose Santa Fe.....	£11.99



50606 USRA 0-6-0 Smoke & Slope Tender Canadian National No. 7356.....	£57.99
51314 Baldwin 2-8-0 Consolidation Steam Locomotive No. 722 Lehigh New England.....	£131.99
51606 USRA 0-6-0 Smoke & Slope Tender New Haven No. 233.....	£88.99
51807 ALCO 2-6-0 Steam Pennsylvania Railroad 3233. DCC Sound.....	£146.99
54301 USRA Light 2-8-2 Steam Loco Union Pacific No. 2487. DCC Sound.....	£220.99
60310 GP40 Detroit, Ironton Railroad 405. DCC On Board.....	£74.99
62811 EMD GP9 Burlington 272.....	£76.99
62812 EMD GP9 Bangor & Aroostock 80.....	£76.99



62937 PCC Streetcar Philadelphia Septa.....	£42.99
63209 ALCO S4 Diesel ATSF 1529 Zebra Stripe.....	£105.99
63308 ALCO S2 Diesel Switcher 38 Nickel Plate.....	£60.99
63310 ALCO S2 Diesel Switcher Chicago Burlington & Quincy 9302.....	£60.99
63810 F7-B Diesel Locomotive Alaska.....	£55.99
64302 F7-A Diesel Locomotive New York Central System.....	£109.99
64303 F7-A Diesel Locomotive Norfolk Southern 4271. DCC Sound.....	£109.99
64601 Alco FA2 Diesel Locomotive Canadian National.....	£75.99
64604 Alco FA2 Diesel Locomotive Louisville & Nashville.....	£75.99
64701 Alco FA2 Diesel Locomotive Canadian National.....	£89.99
65203 GG1 Electric Locomotive No. 4842 PRR Green.....	£129.99
65301 GG1 Electric Locomotive No. 4807 Pennsylvania Railroad. DCC Sound.....	£181.99
65303 GG1 Electric Locomotive No. 4935 Pennsylvania Railroad. DCC Sound.....	£185.99
80106 Baldwin Modern 4-4-0 Steam Locomotive No. 1400 Baltimore & Ohio.....	£159.99
80126 Baldwin Modern 4-4-0 Inlet Black Graphite Smoke Box Fire Box.....	£209.99
80131 Baldwin Modern 4-4-0 Locomotive No. 1401 Baltimore & Ohio.....	£109.99
80203 Single-Truck Birney Safety Streetcar Sacramento.....	£88.99
80204 Single-Truck Birney Safety Streetcar New York & Queens.....	£88.99
80205 Birney Safety Streetcar Third Avenue Railway New York Central.....	£88.99
83403 440 Modern Alco Richmond Maryland Pennsylvania 4 Wood Cab.....	£141.99
83404 440 Modern Alco Richmond Maryland Pennsylvania 5 Wood Cab.....	£141.99
83405 440 Modern Alco Richmond Maryland Pennsylvania 6 Steel Cab.....	£141.99
84607 Peter Witt Street Car with Full Interior Lights Toronto.....	£75.99

### JOUEF HO

HJ2021 Class BB17000 Electric Locomotive No. 817086.....	£123.72
HJ2089 Junior Electric Locomotive BB 22000 No. 22298 SNCF Fret.....	£62.52
HJ4091 SNCF TGV Sud Est Bar Coach.....	£26.40
HJ6063 Silo Hopper Ucs C.E.T.....	£16.86

HR6105 Silo Wagon Type Ucs with lettering Dyckerhoff DB.....

## IN STOCK - HOe GAUGE

### LILIPUT HOe



L141487 Class U Steam Tank Locomotive No. 99 7821 DRG.....	£180.87
L143000 Track Inspection Trolley STLB.....	£139.13



L245180 Flat Wagon No. X 601.9310 STLB.....	£36.43
L245400 Open Goods Wagon No. 6221 with Brakemans Cabin DR.....	£33.99
L245405 Open Goods Wagon No. 6300 with Brakemans Cabin DR.....	£33.99
L344355 Passenger Coach Bi/s 3871-6 ÖBB.....	£40.00
L344356 Passenger Coach Bi ÖBB 'Freistrialbahn'.....	£40.00
L344400 Baggage Coach DYh/s No. 831 DR.....	£54.99
L344401 Baggage Coach D3h/s No. 6301 Mariazellerbahn.....	£54.99



L344407 Postal Van F3hw/s No. 8502-2 Mariazellerbahn.....	£54.99
---	--------



L344410 Dome Wagon Museum Coach NÖLB.....	£54.99
L344412 Dome Wagon No. 2 DR.....	£48.99

### ROCO HOe

33209 Start Diesel Locomotive Feldbahn Leo.....	£44.95
33241 Class 99 0-6-0 Steam Locomotive No. 99 4306 DB.....	£105.00
34498 Side Tipping Wagons. Pack of 2.....	£14.50
34499 Side Tipping Wagons. Pack of 2.....	£13.50
34551 4 Axle Open Wagon No. 97-40-99 DR.....	£14.95
34603 Crate Wagons. Pack of 2.....	£14.50



## BARGAIN - ON-30 GAUGE

### BARGAIN - ON-30 GAUGE

#### BACHMANN ON-30



25249 2-6-0 Steam Locomotive Painted Unlettered Bumble Bee.....	£109.99
27044 Box Car in Christmas Fruit Cake livery.....	£22.99
27638 Spectrum Ventilated Box Car Atlantic & Danville.....	£27.99
28301 4-4-0 American Steam Locomotive Painted Unlettered Black livery.....	£149.99
28305 4-4-0 American Steam Locomotive Painted Unlettered Black livery.....	£149.99
28306 4-4-0 American Steam Locomotive Painted Unlettered Black livery.....	£149.99
28322 4-4-0 Modern Outside Framed American Steam Locomotive Painted Unlettered Black.....	£177.99
28323 4-4-0 Modern Outside Framed American Steam Locomotive Painted Unlettered Black.....	£177.99
28324 4-4-0 Modern Outside Framed American Steam Locomotive Painted Unlettered Black.....	£177.99
28325 4-4-0 Modern Outside Framed American Steam Unlettered Black.....	£177.99
28326 4-4-0 Modern Outside Framed American Steam Unlettered Black.....	£177.99
28657 Baldwin 4-6-0 Steam Locomotive No. 31 Greenbrier & Big Run Lumber.....	£238.99
28670 Baldwin 4-6-0 Steam Locomotive 12 Steel Cab Green w/Gold Lining Red.....	£238.99
28671 Baldwin 4-6-0 Steam Locomotive 11 Steel Cab Black w/Red Windows.....	£238.99
28696 Baldwin 4-6-0 Steam Locomotive Painted Unlettered w/Steel Cab.....	£238.99
28697 Baldwin 4-6-0 Steam Locomotive Painted Unlettered w/Wood Cab & Fluted Domes.....	£238.99
28703 2-6-2 Articulated Steam Locomotive No. 42 Greenbrier & Big Run Lumber Co.....	£248.99
28704 2-6-2 Articulated Steam Locomotive No. 40 Pocahontas Lumber Co.....	£248.99
28757 2-6-2 Articulated Steam Locomotive No. 41 Greenbrier & Big Run Lumber Co.....	£248.99
28761 2-6-2 Articulated Steam Locomotive No. 43 Midwest Quarry & Mining Co.....	£254.99
28798 2-6-2 Articulated Steam Locomotive Painted Unlettered Black w/Red & White trim.....	£255.99
28801 14 Ton Two-Truck Stearns-Heisler Loco Colorado Mining Company.....	£310.99
28805 14 Ton Two-Truck Stearns-Heisler Loco Greenbrier & Big Run 5.....	£310.99
28806 14 Ton Two-Truck Stearns-Heisler Loco Midwest.....	£310.99
28902 4-6-0 Steam Locomotive Painted Unlettered w/Wood Cab.....	£239.99
28903 4-6-0 Steam Locomotive Painted Unlettered w/Steel Cab.....	£239.99
28904 4-6-0 Steam Locomotive No. 12 ET&WNC w/Steel Cab.....	£239.99
28906 Baldwin 2-4-4-2 with Tender Painted Unlettered Steel Cab Black.....	£391.99
29001 Baldwin 2-4-4-2 with Tender Painted Unlettered Steel Cab Black/Red Windows.....	£391.99
29002 Baldwin 2-4-4-2 Painted Unlettered Steel Cab Black/Red Windows.....	£391.99
29004 Baldwin 2-4-4-2 Painted Unlettered Steel Cab Black/Red Windows.....	£391.99
29011 Baldwin 2-4-4-2 Painted Unlettered Steel Cab Black/Red Windows.....	£391.99
29301 2-6-0 Steam Locomotive No. 138 Denver & Rio. DCC Fitted.....	£240.99

## IN STOCK - ON-30 GAUGE

### BACHMANN ON-30



25242 2-6-0 Steam Locomotive No. 136 Denver & Rio Grande
--



# RAILS OF SHEFFIELD

[railsofsheffield.com](http://railsofsheffield.com)



### Corporate Partners of

# RAILWAY MUSEUM

# WANTED

## CASH OR EXCHANGE

WE WANT **ANYTHING** MODEL RAILWAY RELATED,  
ANY AGE, ANY GAUGE, FROM A SINGLE ITEM TO A  
LIFETIME COLLECTION. DISTANCE NO OBJECT.  
PLEASE TELEPHONE, OR EMAIL YOUR LISTS TO  
**SECONDHAND@RAILSOFSHEFFIELD.COM**

# BUY • SELL • EXCHANGE • ANY GAUGE • ANY AGE

**RAILS OF SHEFFIELD VALUE YOUR SECOND HAND COLLECTION VERY HIGHLY**

**LOCOS - COACHES - WAGONS - TRACK-WORK - CONTROLLERS - ACCESSORIES  
DIE-CAST - LORRIES - BUSES - PLANES - SOLDIERS - RAILWAYANA - AND MORE**

## **DECEASED ESTATES - EX SHOP STOCK A SPECIALITY FAST SERVICE WITH COMPLETE DISCRETION ASSURED**

— NO COLLECTION IS TOO LARGE —  
FOR LARGER COLLECTIONS WHERE IT IS IMPOSSIBLE TO LIST ALL ITEMS - WE CAN ARRANGE A PERSONAL VISIT

**IMMEDIATE PAYMENT BY BANK TRANSFER OR CASH  
OR WHY NOT TRADE YOUR ITEMS IN FOR EXCHANGE FOR AN EVEN BIGGER ALLOWANCE**

**We URGENTLY require collections of any size, featuring products from the following manufacturers:**



Telephone our Second Hand team on: (0114) 255 1436 or email: [secondhand@railsofsheffield.com](mailto:secondhand@railsofsheffield.com) - Ref: CM2019

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 0RL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsofsheffield.com](mailto:info@railsofsheffield.com)

Web: [railsofsheffield.com](http://railsofsheffield.com)





RAILS OF SHEFFIELD

[railsofsheffield.com](http://railsofsheffield.com)



Corporate Partners of

RAILWAY  
MUSEUM

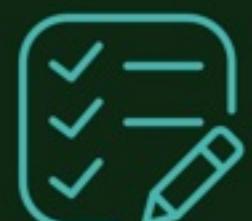
WE VALUE YOUR  
**PRE-OWNED &  
SECOND HAND**  
MODEL COLLECTION  
**VERY HIGHLY!!**

ALL BRANDS

FOR CASH

**CONTACT US**

get in touch by email,  
phone or post with  
your list of items



**STEP  
01**



**STEP  
02**

**WE MAKE AN OFFER**

our friendly, experienced staff  
review your list and make an  
honest valuation and offer

**BRING, SEND OR COLLECT**

either visit the shop, send us your  
items, or for larger collections we will  
arrange collection for you



**STEP  
03**



**STEP  
04**

**PAYMENT MADE!**

we confirm the offer and make  
a fast, secure payment via your  
choice of payment method

**TOP  
GUARANTEED  
PRICES  
PAID**

**FAMILY  
BUSINESS  
FRIENDLY  
SERVICE**

**FULL COLLECTIONS PURCHASED  
UK & WORLDWIDE COLLECTION AVAILABLE**

**BUY · SELL · EXCHANGE · ANY GAUGE · ANY AGE**

**TELEPHONE OUR SECOND HAND TEAM ON: (0114) 255 1436  
OR EMAIL: [SECONDHAND@RAILSOFSHEFFIELD.COM](mailto:SECONDHAND@RAILSOFSHEFFIELD.COM)**

FOR MORE DETAILS see our website: [railsof sheffield.com/wanted](http://railsof sheffield.com/wanted)

21-29 Chesterfield Road - Sheffield - South Yorkshire - S8 0RL - UK - Tel: (0114) 255 1436 - Fax: (0114) 255 5982

Email: [info@railsof sheffield.com](mailto:info@railsof sheffield.com)

Web: [railsof sheffield.com](http://railsof sheffield.com)

[railsof sheffield](#)

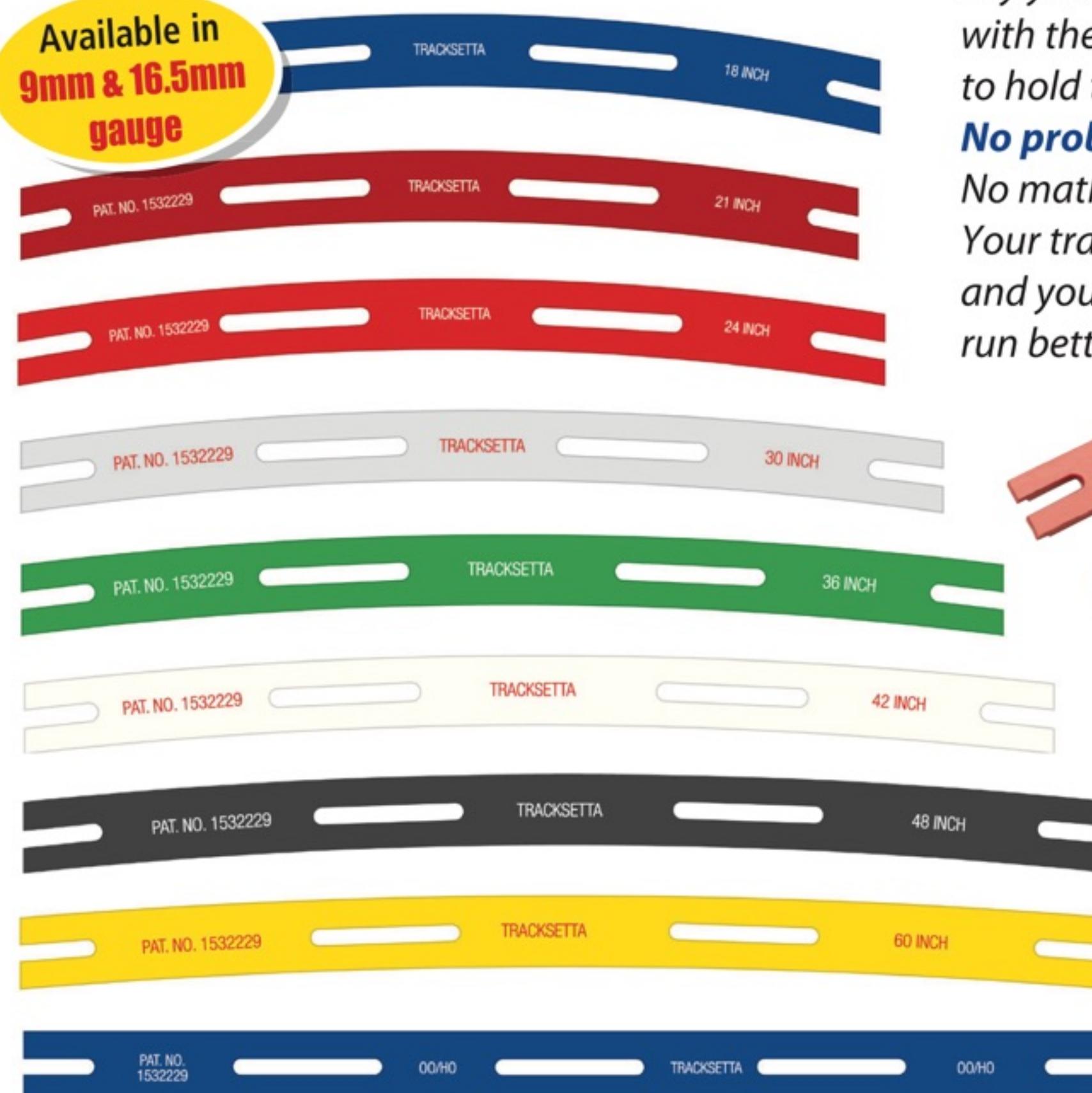
[railsof sheffield](#)

[railssheffield](#)

[railsof sheffield](#)

**The smoothest of curves  
are easy-to-lay with...**

Available in  
**9mm & 16.5mm  
gauge**



# TRACKSETTA™

Lay your Streamline track with beautiful smooth curves **the easy way** with these coated steel track guides. Simply place them between the rails to hold the track in position and pin down. Transition and reverse curves? **No problem!** – just use several different radii in succession.

No maths and no pieces of string.

Your trackwork will look great and your trains will run better too.



#### N and OO-9 Templates

NT9	228mm (9in) Radius
NT10	254mm (10in) long, Straight
NT12	305mm (12in) Radius
NT15	381mm (15in) Radius
NT18	457mm (18in) Radius
NT21	533mm (21in) Radius
NT24	610mm (24in) Radius
NT30	762mm (30in) Radius
NT36	915mm (36in) Radius

#### OO/HO and O-16.5 Templates

OOT10	254mm (10in) long, Straight
OOT18	457mm (18in) Radius
OOT21	533mm (21in) Radius
OOT24	610mm (24in) Radius
OOT30	762mm (30in) Radius
OOT36	915mm (36in) Radius
OOT42	1067mm (42in) Radius
OOT48	1219mm (48in) Radius
OOT60	1524mm (60in) Radius



For further information: Pritchard Patent Product Co. Ltd. Beer, Devon, EX12 3NA Tel: 01297 626204 [www.peco-uk.com](http://www.peco-uk.com)

**MARNO LTD.** 72 Great Titchfield Street, London W1W 7QW • Tel: 020 7580 7699  
Fax: 020 7636 1415 • Email: [marnoltd@aol.com](mailto:marnoltd@aol.com) • Web: [www.marnoltd.co.uk](http://www.marnoltd.co.uk)

For second-hand stock list and stocks only of "DISQUE ROUGE" signals and "JV" catenary from France, see our website.

#### ROCO (SBB)

Locos:	62430 (Siemens Dispolok) ES64F4 SBB Cargo	£148.00
	63538 De 4/4 Seetal No. 1667	£120.00
	63666 Re 474 Cargo (014-8)	£142.00
Coaches:		
	44889 EWIV 2nd class Kids Coach	£36.50
	45188.1 Baggage coach MC76 (338.0)	£36.00
	67837 (BLS) EW 1V 2nd class (1:93)	£35.70
	67838 (BLS) Baggage coach	£33.45
Wagons:		
	46988 Silo wagon, wine livery	£17.00
	66435 Slide-wall wagon "Otto's"	£17.70
	67800 Box car, brown	£15.85
	67802 Tank wagon "Cica"	£21.65
	67904 Set of 8 Hbilis wgn, 4 Brewery designs	£121.00

#### ROCO (ÖBB)

Locos:	73210 (OBB) Steam railcar class 3071.05	£152.00
Coaches:		
	64483 2nd class ribbed (263) Spantenwagen	£27.00
	64656 2nd class express coach (Jaffa/cream)	£40.50
	64775 (OBB) 2nd Jaffa livery	£64.00
	64776 (OBB) 2nd Jaffa (alt. number)	£64.00
Wagons:		
	47365 Tank wagon "OMV"	£21.60

#### ROCO (DB)

Locos:	63554 Class 143 (001) EKO Steel	£111.00
	63593 Class 185 "HOYER"	£122.00
	63992 Diesel class 253 'Press'	£116.00
Wagons:		
	44131 Set of 3 multi-section hopper wagons	£46.00
	46905 (RCB) Hopper wagon	£17.00
	47255 Swivel-stake wagon, Siemens	£21.50

#### ROCO (FS)

Locos:	63891.1 E444R (042) Red/Grey	£147.00
	63891.2 E444R (048) Red/Grey	£147.00
	63893 E444R (045) Red/Grey	£149.00
Coaches:		
	45628 Gran Comfort restaurant, XMPR livery	£39.90
	45632 Gran Comfort, grey/cream, XMPR logo (107-5)	£31.50

#### ROCO (NS)

Locos:	45632.1 Gran Confort, alternate number	£34.00
	45634 Gran Confort, XMPR livery, 1st class (088-7)	£31.50
Wagons:		
	66408 Yellow Service type E	£11.50
	44192 Set of 2 tank wagons "CONVOJ"	£39.95

#### ACME (TRAXX)

Locos:	60460 class 187.003 Bombardier livery	£206.00
	60462 class 187.005 Railpool livery	£206.00
Wagons:		
	386515 – Packet of 50 close couplings	£42.00
	524101 – 3 part container wagon set (Crossrail)	£118.00

#### FLEISCHMANN (HO)

Locos:	22211 (DB) Lint 41 (BR.648.2) 2 car set	£240.00
Wagons:		

#### TRIX (HO)

Locos:	22211 (DB) Lint 41 (BR.648.2) 2 car set	£240.00
Wagons:		

#### ESU (HO)

31077 Class 77 Crossrail red DE6312 + Sound	£300.00
31155 Class 847 004 Gravita Stahl + Sound	£325.00

#### SOMMERFELDT (CATENARY) (Some examples)

##### (SBB) HO

322 Switching Post	£25.95
--------------------	--------

##### (FO/MOB) HOm

280 Wood type mast	£5.85
282 Brackets (pack of 2)	£4.50

#### LENZ DIGITAL PLUS

10231.02 "Standard + V2" decoder 1.0A with NEM 652 plug	£19.50
10312.01 "Silver + plux-12" with NEM 653 plug	£26.00
10321.01 "Silver 21+" 1.0/1.8 A with 21 pin plug	£30.50
10322.01 "Silver + Plux 22"	£29.00
10330 "Silver direct" 1.0/1.8 A with plug	£31.00
LA152 Adapter, including front panel	£22.00
LH 200 Hand held rotary controller	£65.00
LS 150 Stationary decoder (6 outputs)	£50.50
Set 101 (60101) Command Station/Amplifier	
LZV200 + Manual Controller LH101	£320.00

The prices in this advert may vary from time to time due to currency fluctuations. We do not charge VAT for export orders outside the EEC.



Collections by appointment only.  
Postage charges for all orders at cost.

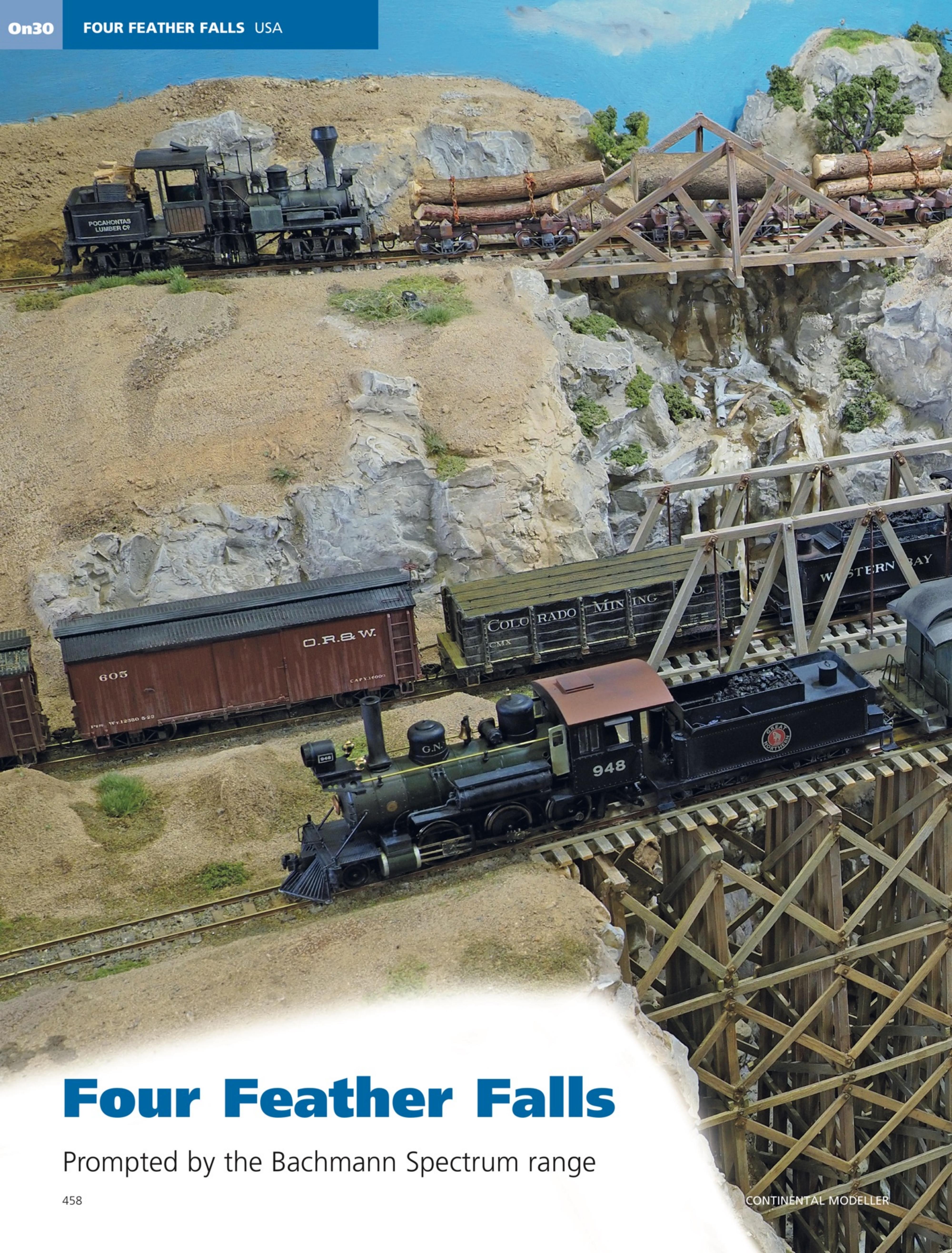
# SPOT ON!

The Peco Scene PSG-3 Precision Applicator is perfect for detailing those smaller and more intricate areas, such as this overgrown siding, utilising our extensive selection of realistic static grasses.



## PECO SCENE STATIC GRASS SYSTEM

Order your copy of the Peco Catalogue for more details of the Peco Scene Static Grass System, or visit our website at [www.peco.uk.com](http://www.peco.uk.com)



# Four Feather Falls

Prompted by the Bachmann Spectrum range



**Ernie Melville** relates how he prompted other members of the Cupar & District MRC to build an American narrow gauge layout. *Photographs by Ken Barclay.*

This layout came about when two members of the club, who were already heavily into American HO, came across the Bachmann On30 narrow gauge range while visiting an exhibition over ten years ago. The purchase of locomotives and rolling stock began, and they were soon joined by two other club members, until between us we had a stack of models still in their boxes but with no railroad to run them on.

These club members were myself, James (Hastie) Donaldson, Iain Smith, and Ryan Gray. Hastie and I devised and planned a layout which would ultimately be used as one of the club's exhibition layouts, so the size and complexity of operation had to be kept as small and simple as possible.

Above

Trains on each bridge: top, a Shay pulling a load of logs to the sawmill; in the centre, a freight hauled by a Western Bay RR 0-6-0; and at the front a Silverton Northern RR 2-6-0 with a bullion car and two passenger cars.

It was duly built and the scenics done by studying pictures of Colorado narrow gauge lines in books and magazines to get the right sort of terrain and 'feel'.

After it was built and running, we still had no idea of the location, fact or fiction, so we called it *Four Feather Falls*, a name conjured from our distant youthful television viewing (referring to a children's programme using puppets by Gerry Anderson). The name seemed to fit the layout rather well.

I called the mine on the layout 'Soloman's Mine No.4', as I have already built three previous models of the same name in HO. It was some time later, while I was studying a map of the Silverton area of Colorado, that I noticed that King Solomon Mountain was just to the east of the town, so that was it, the location of our layout.

### It happened like this ...

Otto Mears was a pioneer road and railroad builder in Colorado at the end of the nineteenth century. Between 1890 and 1891 he built the Rio Grande Southern railroad from Ridgway to Telluride, Dolores, and Durango. This was one of his finest achievements and is still regarded as one of the great feats of railroad engineering.



Right

**A view looking from above the logging camp towards the gorge with its bridges and the town beyond.**

Opposite

**A freight arriving from Howardville.**

Below

**Looking towards the tunnel mouth leading to the Highland Mary Mine.**



In 1895 he laid the tracks of the Silverton Northern line with a branch from the town of Silverton north east to Howardville, Eureka, and Animas Forks.

The miners and sundry prospectors working the dozen or more mines to the south of Howardville had to struggle along a bad road for many miles to the town, with mules and wagons carrying the ore outwards and supplies back. This road followed a watercourse called Cunningham Creek which flowed northward, joining the Animas River near Howardville.

When the Silverton Northern railroad reached Howardville, the miners of the region persuaded Mears to build a branch along the bank of the creek, down between the King Solomon and Green mountains, to serve the mines and their communities. The high quality of the ore being shipped out and the traffic in timber being cut from the slopes of the mountains helped persuade him to agree to build. Blasting tunnels through the rock and bridging the gaps which were too big to be rock filled, the line eventually reached a terminus at the Highland Mary mine, 25 miles south of Howardville.

Jumping forward to 1922, we see on the layout the mining settlement of Four Feather Falls situated on the lower slopes of King Solomon Mountain, with its depot, engine shed and yard, the mine in the distance, and a logging line running just above it.

### A guide to the layout

Trains from Howardville approach Four Feather Falls through a rock cut tunnel on the right of the layout. They pass the mine yard and on to the timber trestle bridge spanning Feather Creek, which flows into Cunningham Creek. The depot lies on the other side of the bridge where passengers rarely board if the train is heading south towards the

terminus. Trains heading north are busier, with passengers making for Howardville and beyond. The depot area has a water tower to replenish thirsty locomotives next to the maintenance-of-way hut (a grounded Bachmann car) which houses the crew who look after the track and bridges in this section of line.

A dirt road crosses the line and takes visitors to the small mining town of Four Feather Falls. Across the road is Charlie's Mining Supply store, providing everything the prospector needs before heading for the hills searching for the fabled gold and silver seams that legend has it still exist around the next boulder, though the ore has mostly been extracted by big mining companies who are taking control of the area.

A set of points in the single track main line sends you back behind the depot and out towards another bridge, which crosses the creek just in front of the Four Feathers waterfall. Crossing a truss frame bridge, it heads towards a point with one track going back towards the main line, creating a passing loop, and the other heading via a three-way point to the ore loading trestle and storage tracks (with a loop) at the mine.

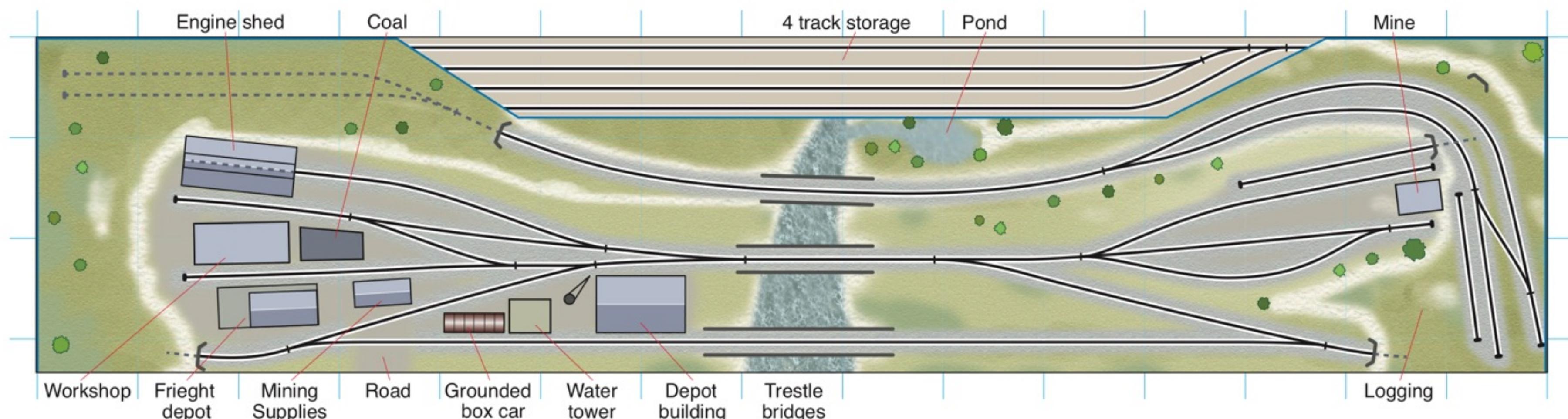
Behind the depot, the line splits into three spurs. The right-hand line leads to the mine loco shed. The middle track goes to the side of the shed used for repairs and servicing. There is a small coaling dock and an oil store next to the shed. The left spur runs behind Charlie's store to the freight depot, where larger items can be unloaded.

Back through the points leads us to the main line again. Soon we enter the tunnel and head south to the terminus at the Highland Mary Mine, where the locomotives will be turned, ready to head north again.

Running above and behind the town is the logging line which is unconnected to the main line here. The track leaves a rock cut tunnel above the engine shed and travels over the creek on an A-frame truss bridge, then heads up hill to the logging camp. The tracks there consist of spurs and a passing loop, to assist in the log loading operation. A length of unconnected track has been put down and is used to hold the kitchen car and water tank car.

Trains from the logging camp run past the town to connect with the main line lower down, at the sawmill near Green Mountain.

The viewer is asked to imagine the distance from the town end of the layout to the mine as being greater than it seems, using the central bridges to create the illusion.



**Four Feather Falls** Overall layout size 4500mm x 1000mm. Each grid square = 300mm x 300mm.



Left  
The horse waits patiently for loading to be finished at the freight depot.



Right  
The proximity of the cliff prevented the coal stage being sited in the logical place on the shed road.

Below  
Solomon Mining Company Shay #5 at the engine shed.

Below right  
A solitary passenger waits for the southbound train.



### Baseboards, track, and scenery

We found in our clubroom two spare baseboards that had been made for another layout which never came to life. Adding a third shorter board for the gorge in the centre gave us an overall size of 4.5m x 1m. The three boards, with all the stock boxes and other equipment required, fit into a normal Transit van when we attend exhibitions.

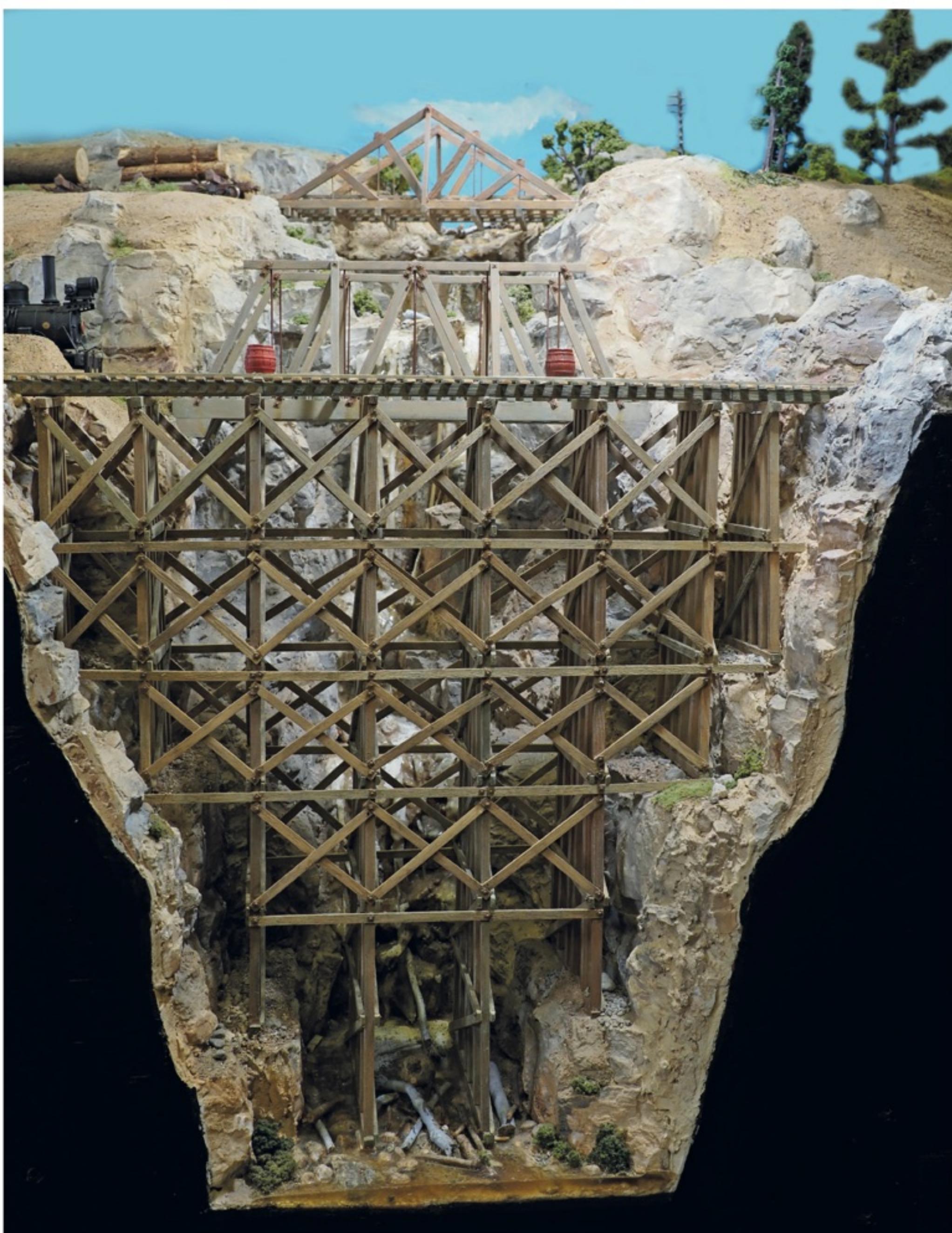
Baseboards are 40mm x 20mm timber frame with a plywood surface.

The track and points are Peco O-16.5 (code 100), apart from the three-way point leading to the mine and the hidden track in the tunnels and fiddle yard which are Peco HO/OO.

Woodland Scenics grey ballast was used mixed with a little sand. It was built up over the ties in some areas, such as the yards and mine area. When first laid, real narrow gauge track was often not ballasted between the ties or just had light earth ballast which had to be upgraded later.

The rock faces were made using four Woodland Scenics plaster moulds, which gives eight different shapes when inverted. Other casts were cut up to change their shape, and when it was all put together the effect looked right for the area we were trying to recreate.





Above  
The three bridges – trestle, Howe truss, and A-frame (on the logging line above).

Below  
The A-frame bridge over the waterfall with the truss bridge beneath.

Above right  
At the pond by the logging line, a bear is hunting while a beaver keeps its distance.



The other hillside was made with plaster bandage, covered with thin plaster, and painted with a light earth and sand wash of acrylic paints, then sprinkled with fine sand and earth to give texture to the ground surface.

Coloured washes were used on the rocks, following coloured photographs of the area around Silverton in Colorado.

The ground vegetation is various Woodland Scenics grasses, as are the small trees (left over from logging operations). Other grass and flower clumps are Peco Scenics, while the long grass tufts are various sizes of brush bristles, cut and tied together in clumps.

The centre section containing the waterfall and bridges was made as a plywood box with front and top left open. This protects the scenery and bridges in transit to shows. The rock sides and rear of the waterfall were made using the Woodland Scenics moulds. The waterfall was made from fibreglass resin as used in car repairs, built up in layers, with white paint applied between the layers to give depth to the water. Pieces of timber and rocks were placed on the sides of the gorge to depict the debris washed down the creek.

### The bridges

The bridges were scratchbuilt over a period of months using templates drawn out on paper. Timber stripwood cut down from D.I.Y. store wood shapes was cut to length and glued together to form the bents (uprights) and bridge sides. Short pieces of rail were soldered across parts of the track crossing the bridge. The plastic ties were cut away leaving the rails only, held in gauge by soldered pieces. Wooden bearers the length of the bridge were topped by wooden ties, glued across, and then placed under the rails and glued together using contact adhesive. The bents were placed under the bearers and glued in. The retaining soldered rails were removed and the track cleaned up.

The bridges behind the large trestle were put together in a similar way, the sides being attached after the track was fixed to the base.

Grandt Line bolt head castings were used to detail the bridges. Black and brown Indian ink (non-waterproof) dropped in small measures into a 250ml bottle of isopropanol was used to weather the timber.





Above  
**Soloman's Mine No.4**  
with the logging line above.



Right  
**Ore buggies at the mine exit**  
wait for empty cars to be  
placed under the chutes.



### Structures and details

The depot building is a laser-cut wooden kit from American Model Builders and the freight depot was made from a Banta Modelworks wooden kit. The mining supplies and backwoods engine shed are cast plaster kits, also from Banta. The repair shed was the lean-to part of the engine shed but was built as a stand-alone structure with extra walls and interior. The water tower is an Evergreen Hill timber kit, while the coal store and mine loading gantry were scratchbuilt from spare timber parts from the bits box. The Surry Parker style log loader was scratchbuilt from pictures in narrow gauge magazine articles. Its means of operation, the vertical steam boiler, is part of the Bachmann log skidder. The mine office was built from scratch using spare timber sheeting and plastic mouldings for the windows. The roof is Paper Creek printed corrugated sheet, which looks almost real.

Four Feather Falls and surroundings are populated in the main by cast whitemetal figures from various sources, painted, and in most cases given new headgear; some have also had arms, legs, and heads altered to fit their situation. A few Woodland Scenics figures can be found beside the logging camp, about to be disturbed by a grumpy bear. Other animal life consists of more bears at the top of the waterfall, deer, cats and dogs, and what I believe are beavers in the pool beside the logging line. (That's what my co-builder Hastie says they are!) A horse with a cart down by the freight depot is a British whitemetal kit.

### Control

The layout is analogue DC with the control panel behind the backscene. There are three Gaugemaster handheld controllers, one each for the main line, yard and mine, and the logging line, which is independent of the rest of the layout.

There is one switched section in front of the engine shed and another at the end of the mine gantry track to allow locos to be parked.

Points are controlled by switches on the panel, operating Peco point motors.

### The trains

Bachmann On30 locomotives make up most of the roster. As they are individually owned they vary in type and road name. The range from small 0-4-2T Porters through 4-4-0 Americans and 2-6-0 Moguls to 2-8-0 Consolidations, while Shays and Climaxes appear on the logging line, happily trundling a set of log trucks slowly back and forth from the logging camp to the sawmill (off stage).

Having different road names is not as unlikely as you might think as the Silverton Northern rented locomotives and stock from the Denver & Rio Grande and other railroads.

Two sets of coaching stock are used, both from Bachmann. The yellow passenger cars usually run behind the Rio Grande 'Bumble Bee' liveried locomotive, while the green set is lettered for the Silverton Northern and Rio Grande Southern. This includes a bullion car, as operated by the Wells Fargo

Above

**The loggers at their meal are disturbed by a bear.**



Company, used when a valuable load of silver is being transported to the bank in Silverton. This is a Bachmann combine with the sides removed and new ones made from scribed plasticard.

Sets of ore cars serve the mine, collecting the ore for transfer to larger gondolas bound for the smelter in Durango. Box cars, reefers, tank cars, gondolas, flats, and others all pass along the line.

Two of my gondolas have been shortened and my tank car has different tank mountings, in keeping with the period. The water carrying tool car which is usually found beside the log loader and kitchen car is a shortened flat with a rectangular tank and tool boxes made from plasticard.

### Summing up

In the ten years since construction started club members have exhibited *Four Feather Falls* at shows around central Scotland. It has been received favourably by the public and fellow modellers, and won a trophy at Perth in 2012. In 2018 we were invited to show it at the Warley NEC event, the pinnacle of exhibitions in the UK. Unfortunately I could not attend and it was left to Hastie and other club members to exhibit the layout in Birmingham, all doing sterling work and gaining much enjoyment from the experience.

On the Sunday morning of the show I received a text from our club secretary saying the layout had been presented with the CONTINENTAL MODELLER trophy. I was amazed that it had been singled out from all the marvellous layouts on show. It was the high point of our hobby year.

Above  
**The logging camp, with a kitchen car and a tool car with a water tank to supply the Surry Parker log loader. Loaded skeleton frame log cars await collection.**

Below  
**The Surry Parker log loader at the end of the line, with the kitchen car to the right.**

We have all had hours of enjoyment building, making scenery, wiring, building structures, running trains, and showing this to the wider public. It makes all the effort of transporting the layout to shows worthwhile.

My thanks to Hastie, my building and plaster casting wizard, Iain our club chairman and encyclopaedia of all things railways, Ryan, Larry, Ken, Will, Carlo, John, Andrew, Chris, and all Cupar & District Model Railway Club members past and present for their help, support, advice, and good humour over the years. It has been a joy to share our love for this fascinating hobby with you.

The club's annual show takes place on Saturday 20th and Sunday 21st July, but the layout is not due to appear.



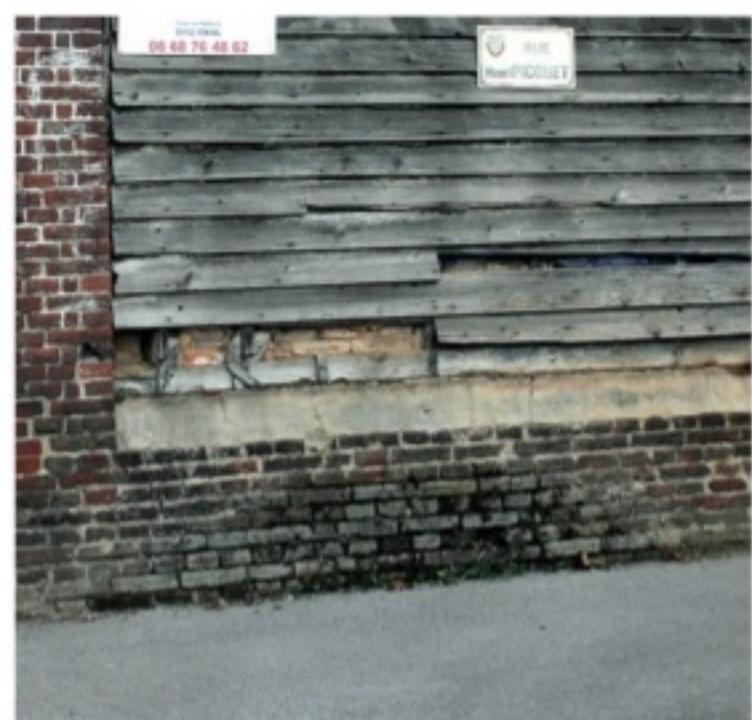
**Emmanuel Nouaillier** completes his series devoted to modelling urban architecture in miniature. *Photographs by the author.*

# Two small cafés

## 4b – setting the completed structures in small scenes (concluded)

This very distinctive structure was formerly the warehouse of a removals company, which I felt could make a perfect partner to the second café model, with some original but plausible modifications to the construction.

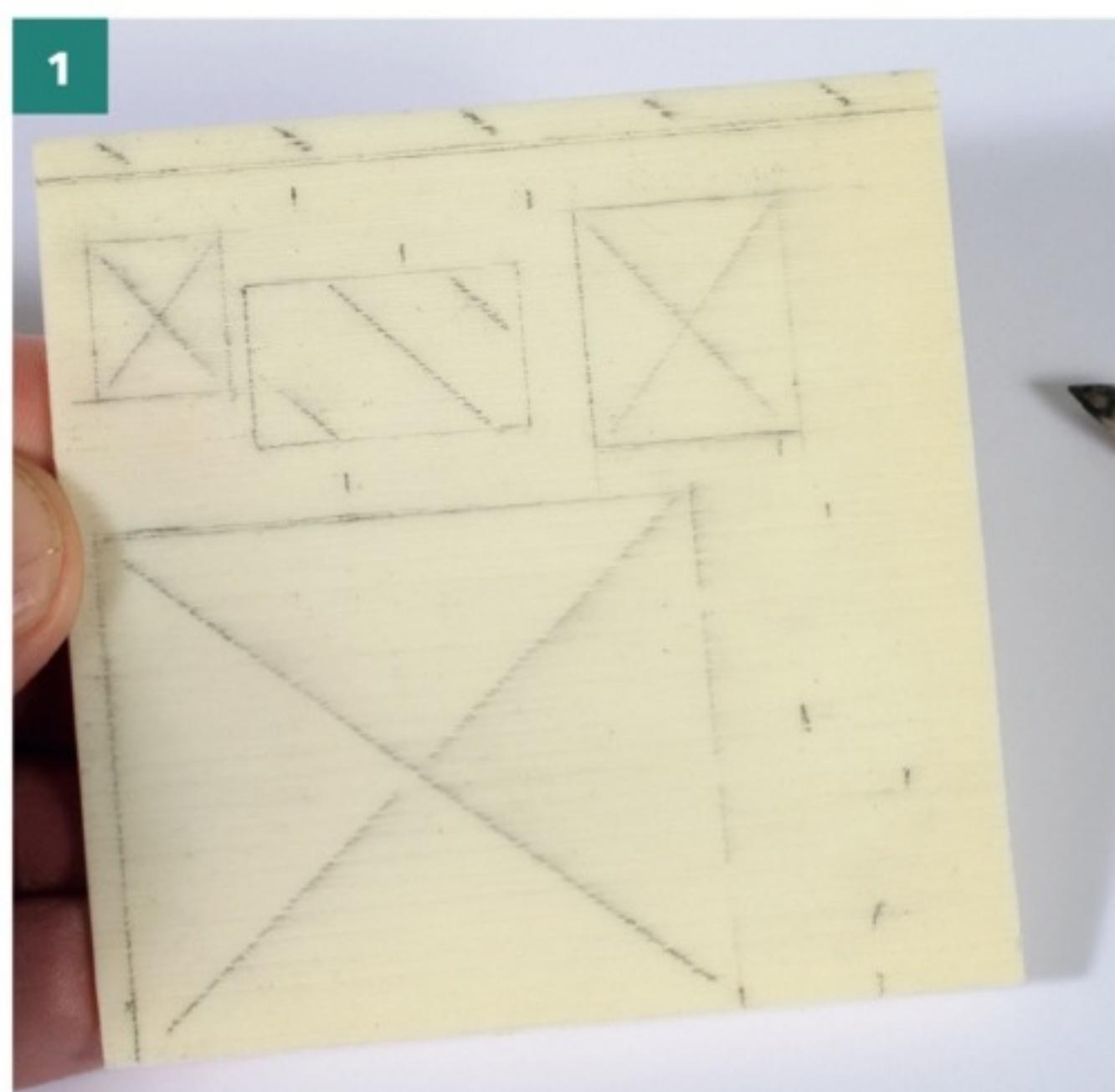
On structures like this some parts of the cladding can be displaced, revealing the wall behind, in these examples brick. This is really the best effect to imitate, even in small scales, if you want to take the weathering to the ultimate, as I do.



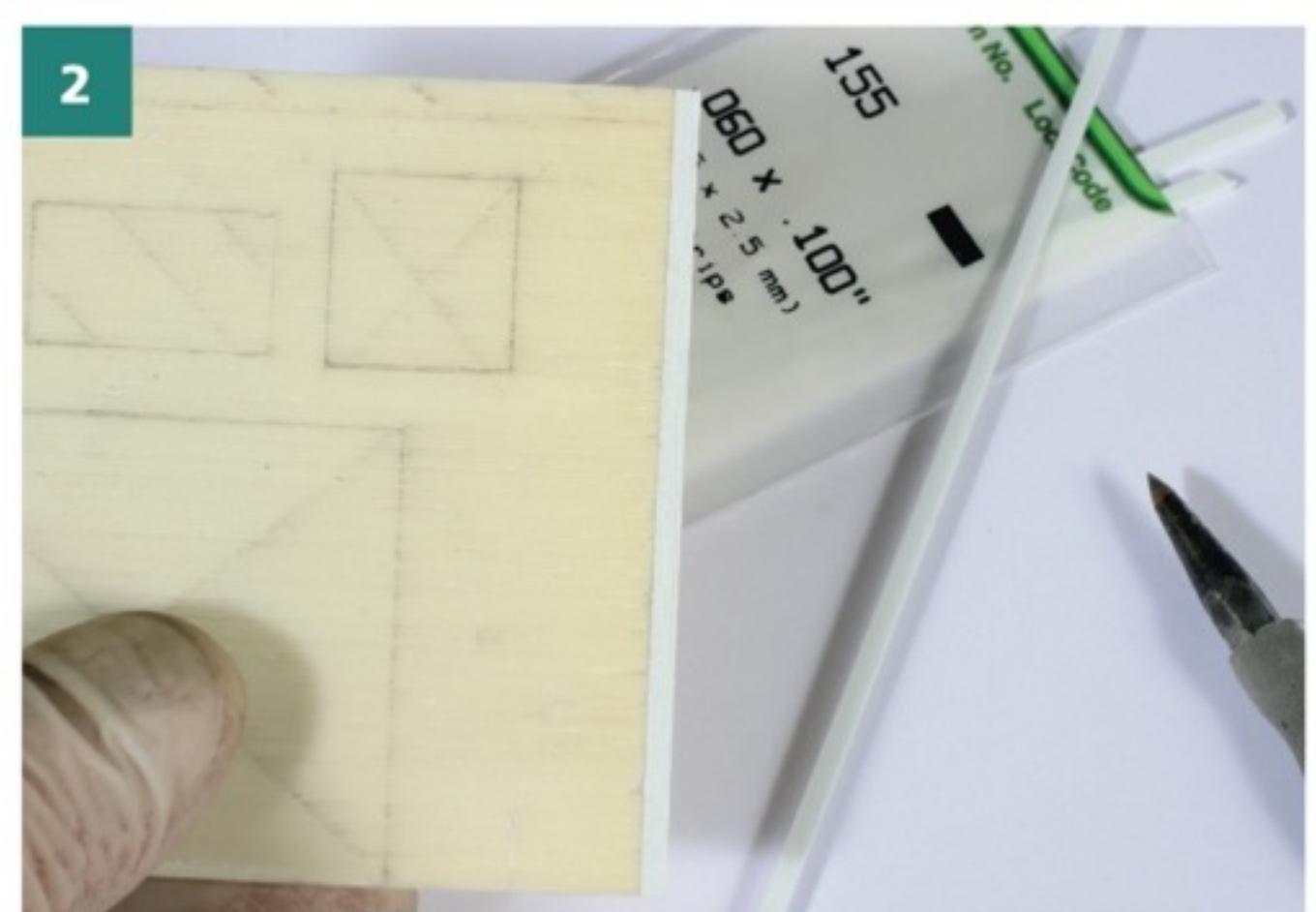
### Step 1

To save valuable time in the creation of this structure, I turned to the old Zebdec resin sheets, of which I had a good stock. These were ideally suited to making all or part of the front, facing onto the street.

From the outset I determined the locations of the different openings and any other details that will hide parts of the material in the end and so do not need texture or colour.



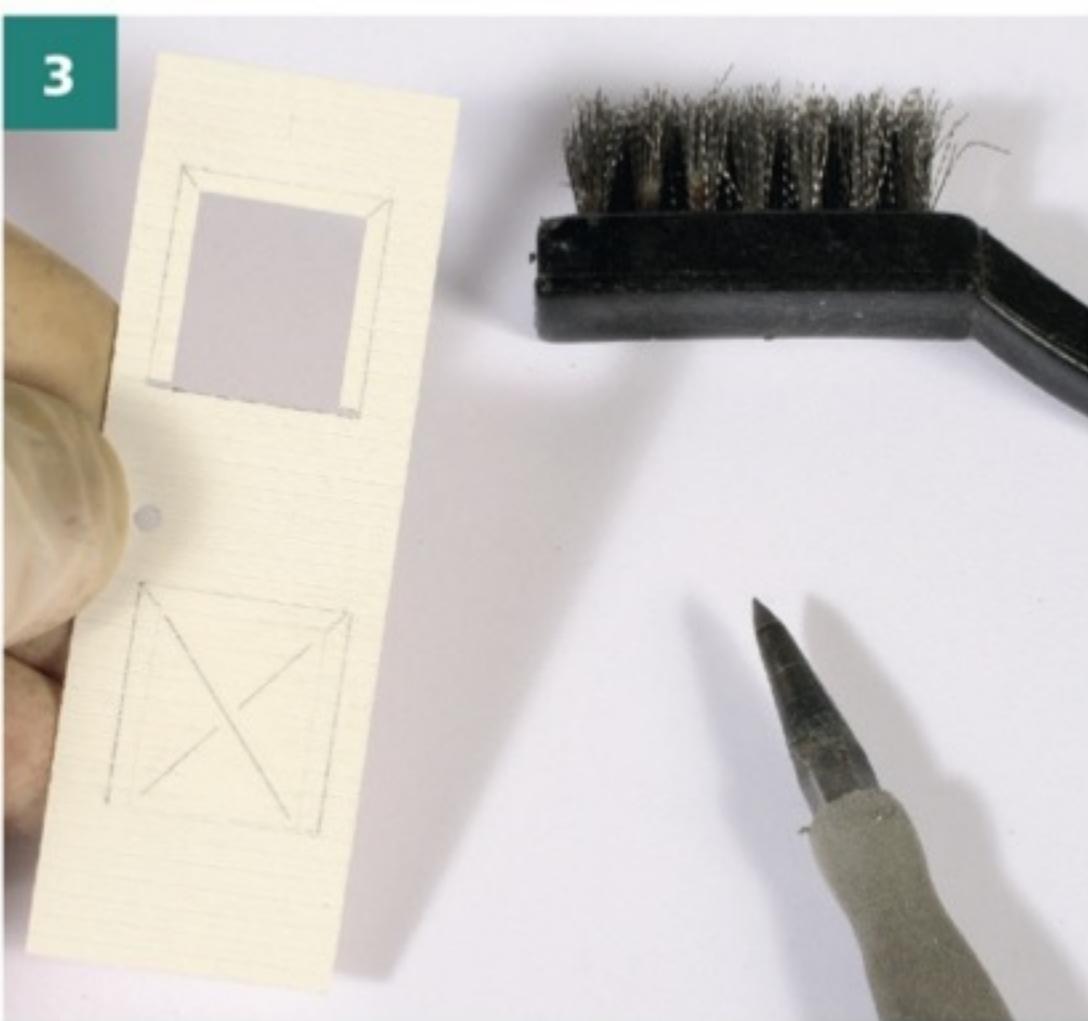
### Step 2



Following the real building, key wooden components were made using Evergreen square strip scribed and treated with a wire brush in advance to simulate the wood grain. The pieces were stuck in place with cyanoacrylate.

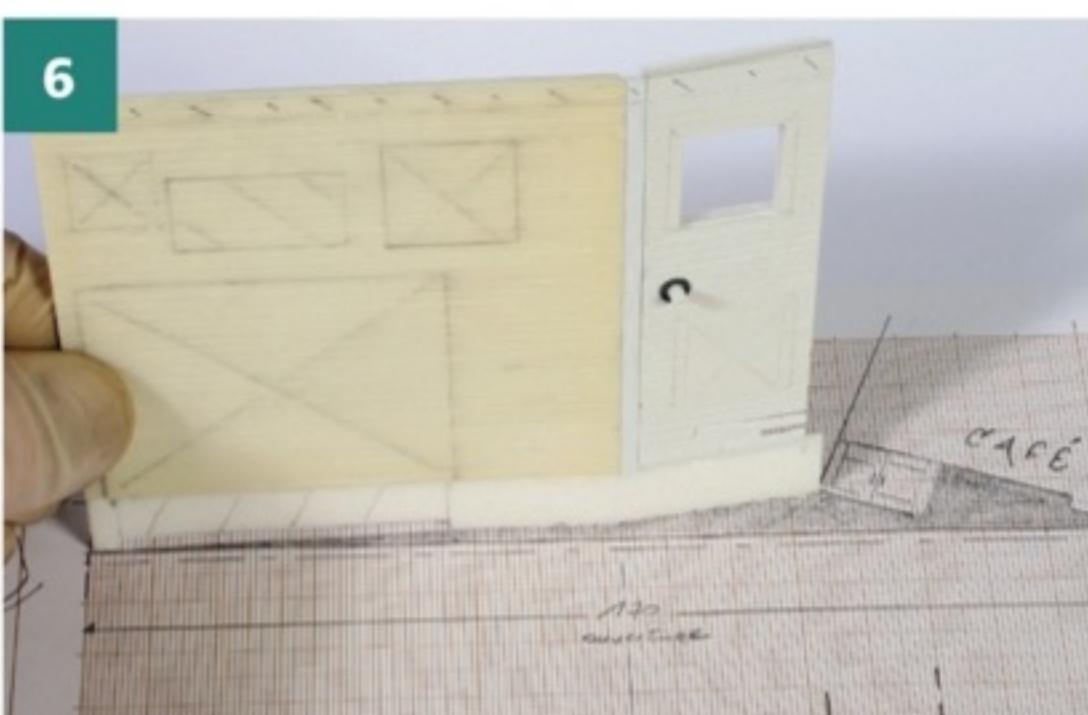
### Step 3

For the additional section at an angle, I preferred to scratchbuild the wall in 0.5mm plasticard, which not only made it to very easy to create openings (e.g. the window) but also to reproduce some effects of decay in the foundations.



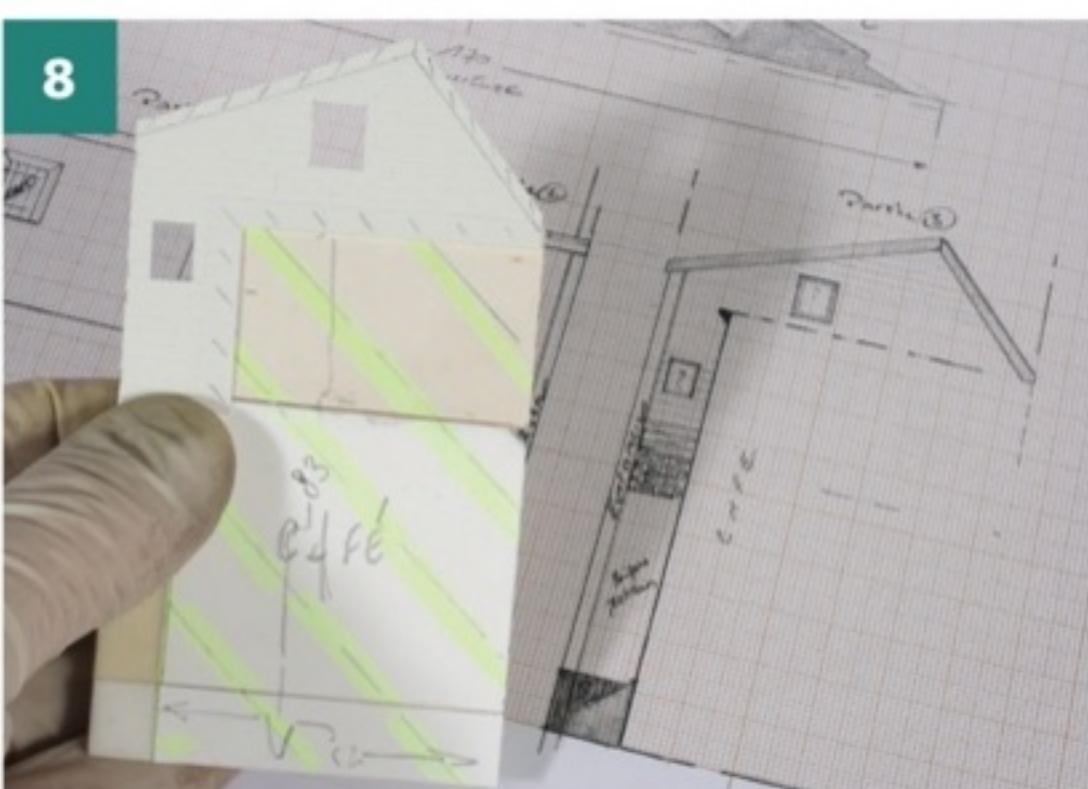
### Step 6

The two parts were then assembled, following the ground plan on graph paper. Nothing is left to chance – even a millimetre more or less could cause problems combining the structures in the scene. Note the foundations following the shape of the building made with foamboard, simulating old cement.



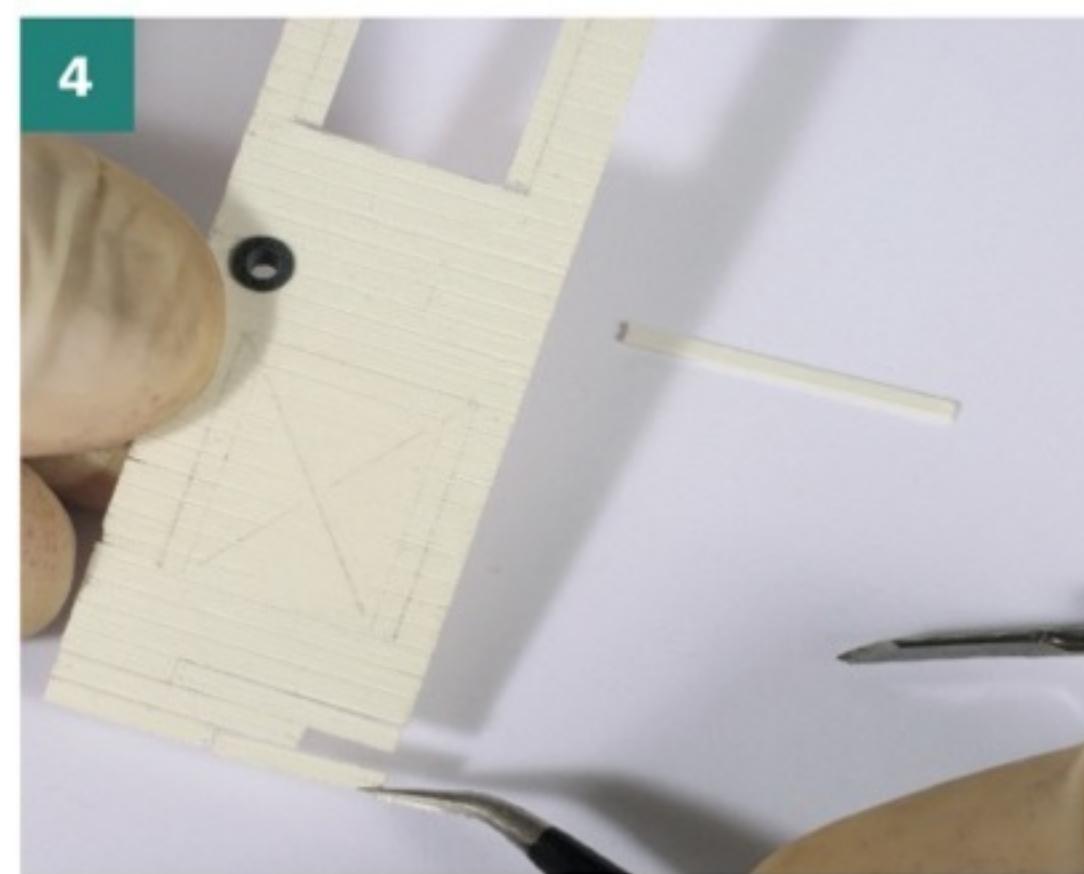
### Step 8

I continued with the end wall which will be against the café. I adapted the real situation by making the upper part with cladding and the lower storey render over bricks. I used pieces of foamboard, some Zebdec ref.87805 cast resin sheet, and grooved and ribbed 0.5mm plasticard.



### Step 4

After cutting away some boards, I re-assembled them leaving a bit of play, and removed one or two completely. For this I work with very fine tweezers and a minimum of cyano glue, spread in a very thin film on the surfaces in contact with each other ... it is a bit of precision jewellery work but will have a significant effect in the end.



### Step 7

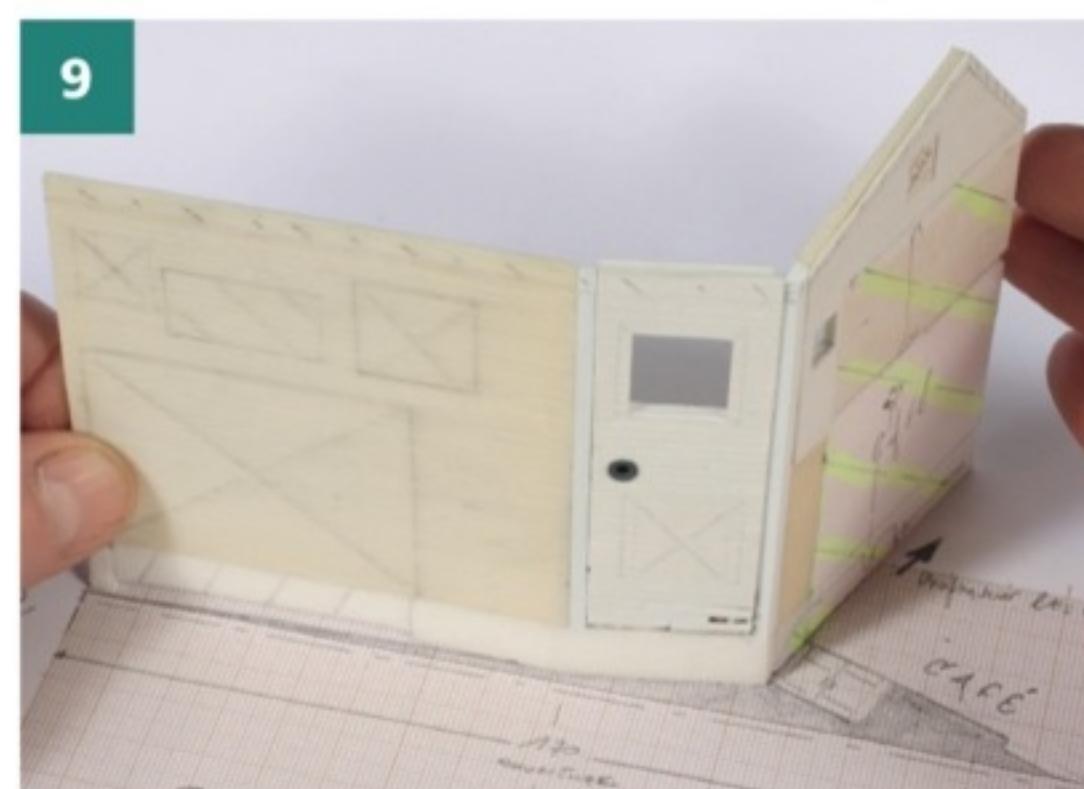
To return to the deteriorated area, I reproduced the part of the brickwork visible behind with pieces spare from the transformations made to the original Dynimo façades – nothing is wasted! I also worked on the mortar between the joints and some of the wooden battens on which the cladding was fixed.

Once this little exercise was finished, I checked that everything fitted perfectly.



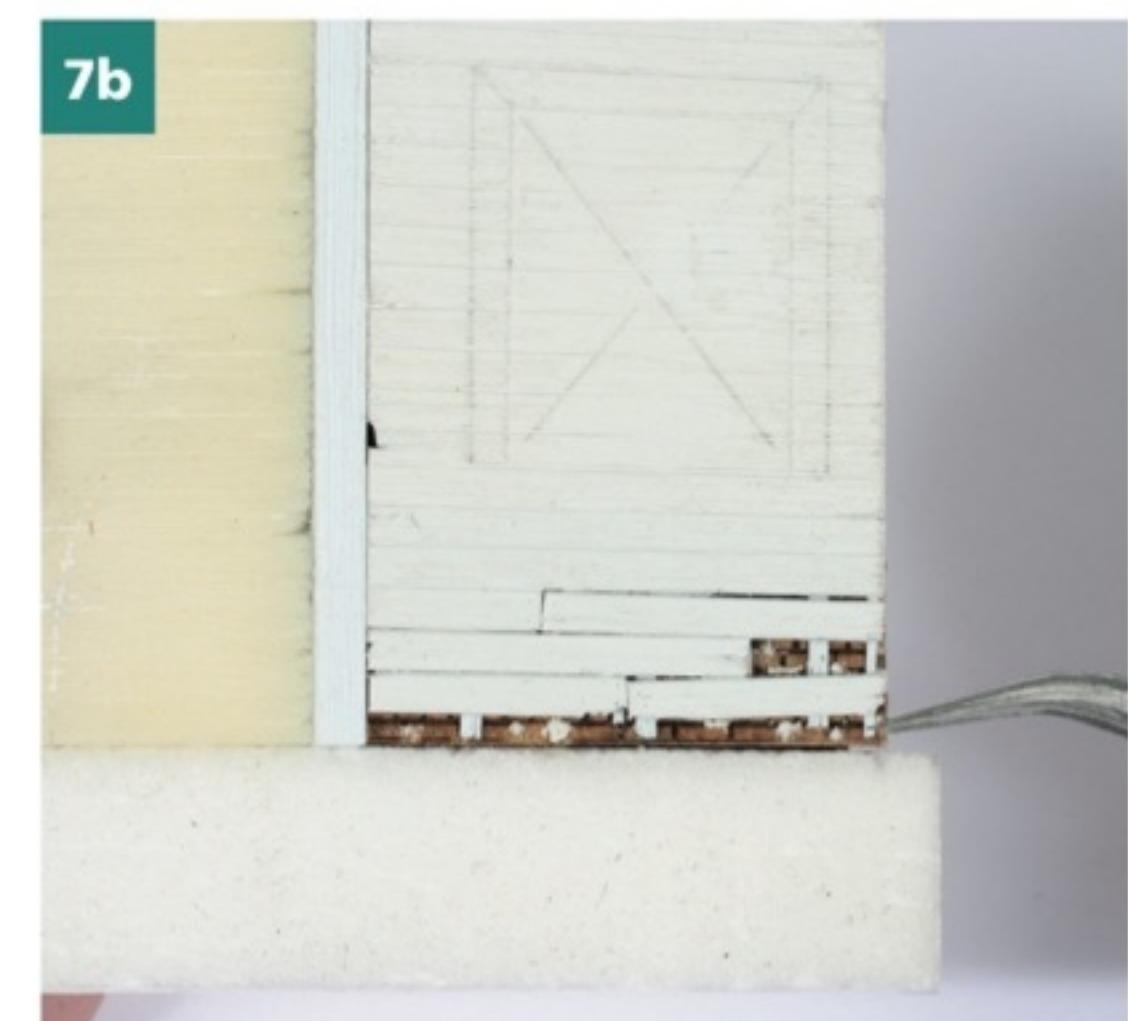
### Step 9

Once this part was ready, I checked that the overall assembly was absolutely correct on the ground plan of this mini-scene, made on graph paper, on a flat surface ... a rule that I apply for all my projects.



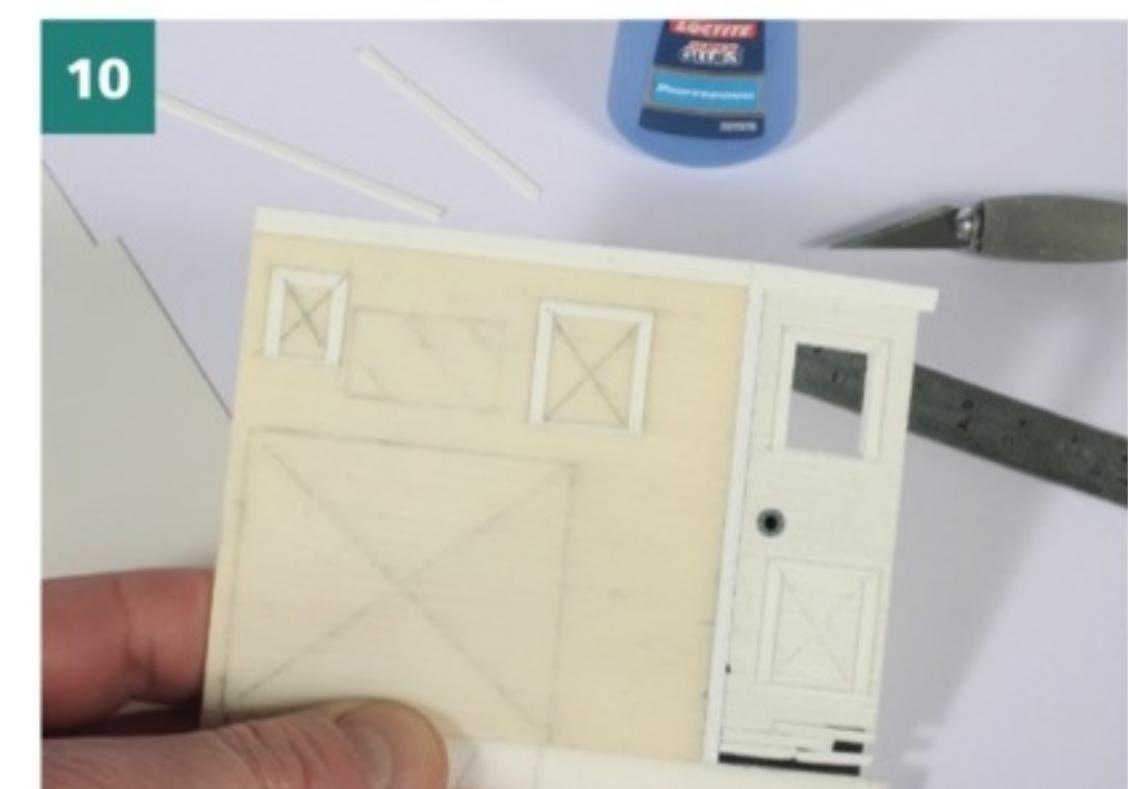
### Step 5

To match the much thicker resin part (about 2mm), I cut a duplicate of this component from 3mm foamboard to which I stuck the plasticard. This offers more rigidity and strength to the assembly, welcome during subsequent operations. You could also use 2mm plasticard or even Forex.



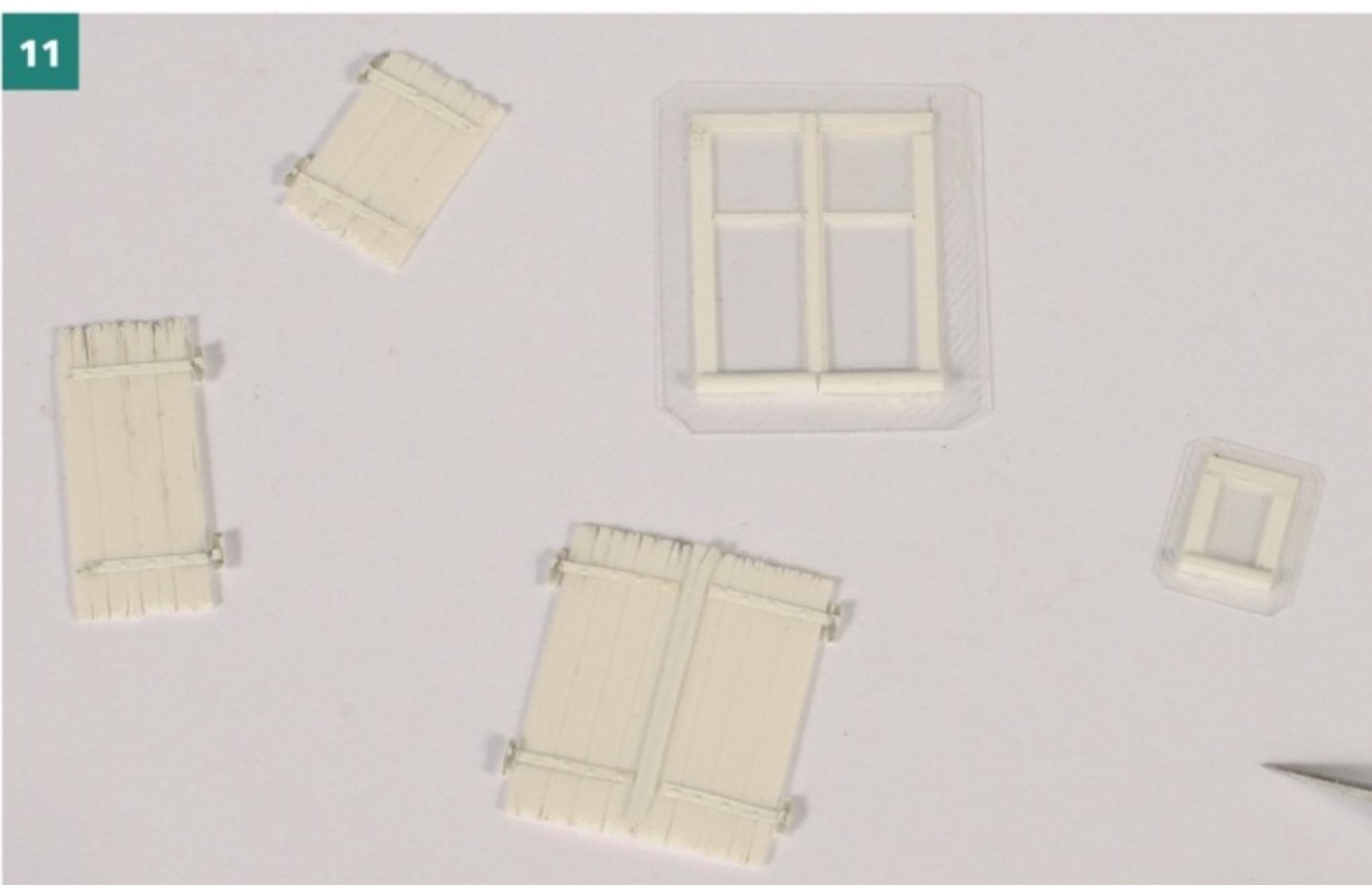
### Step 10

With most of the structure finished, I added the necessary details, in this case the moulded wooden cornice, and window surrounds made with 0.5mm Evergreen flat strips, scribed to simulate wood grain after being placed.



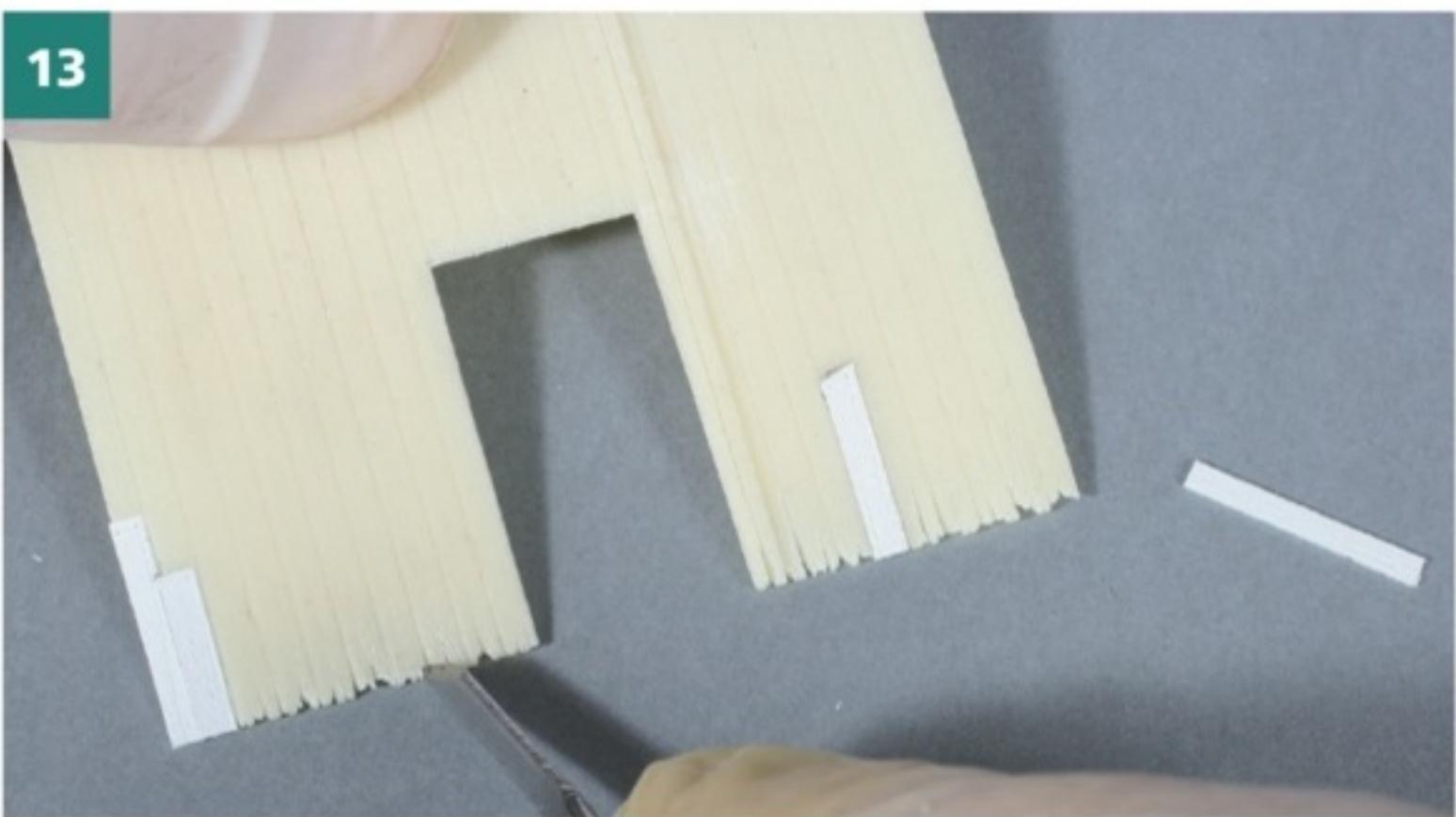
## Step 11

Next, various shutters and frames were made from scratch using Evergreen strips and plasticard, again trying to go as far as possible with the detail. This may take time, but it is always worth it in the end!



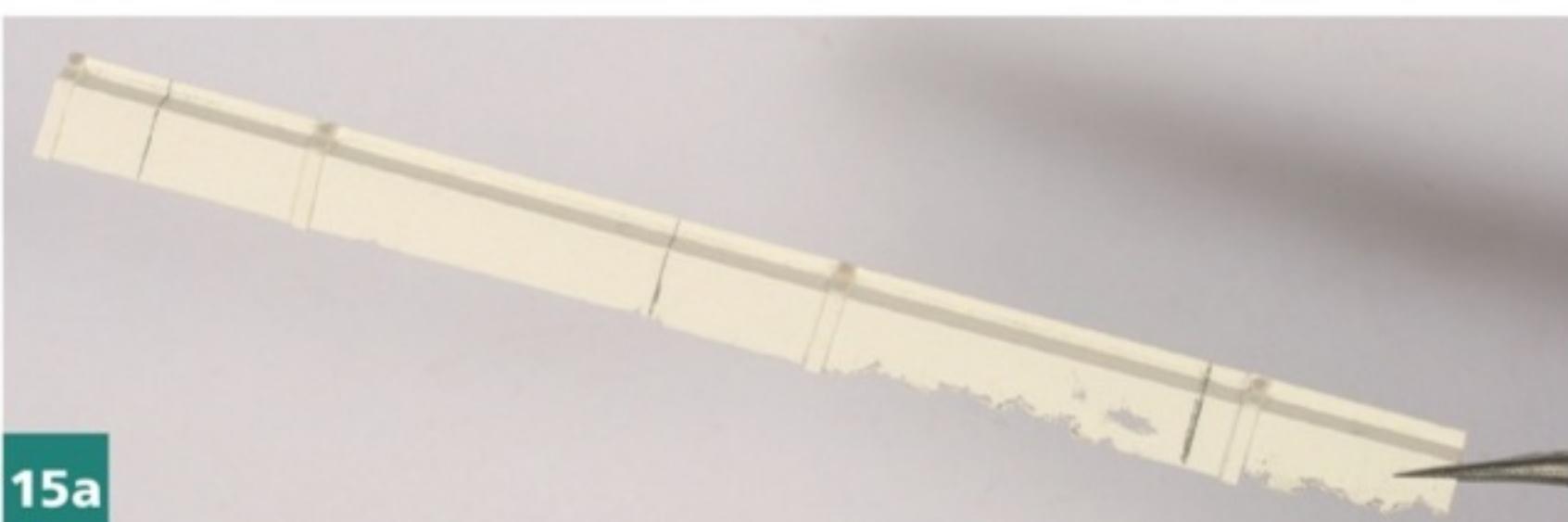
## Step 13

This was done using a small sharp blade to trim the bottom of the door. I patched some places with new boards made of plasticard – in fact the offcuts of the strips used for the window frames ... nothing is wasted!



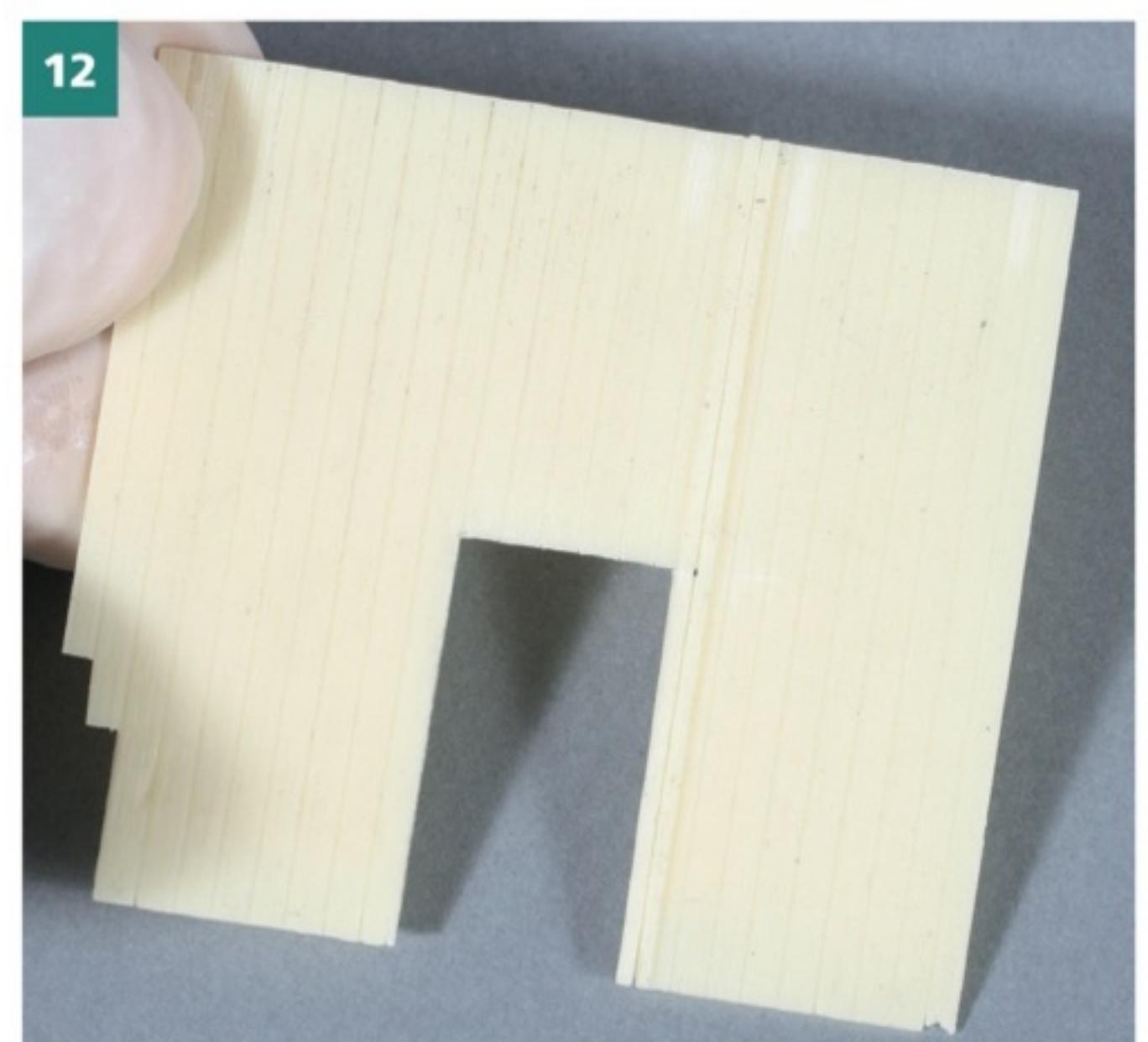
## Step 15

To hide all or part of the sliding rail (and to avoid the tedious task of making it!), I made a protective cover for it, as can sometimes be seen. I used 0.13mm plasticard shaped after lightly scribing grooves on the back. Some more strips of the same thickness, three or four bolts, and there it is. I also trimmed it in places to mimic the time-worn flashing, and I checked that it still looked right by placing the unpainted part over the door.



## Step 12

To complete the frontage in a suitable style, it seemed sensible to install a sliding wooden gate. For this I used a piece of cast resin sheet imitating boards into which I made some incisions to represent the effects of damage and decay.

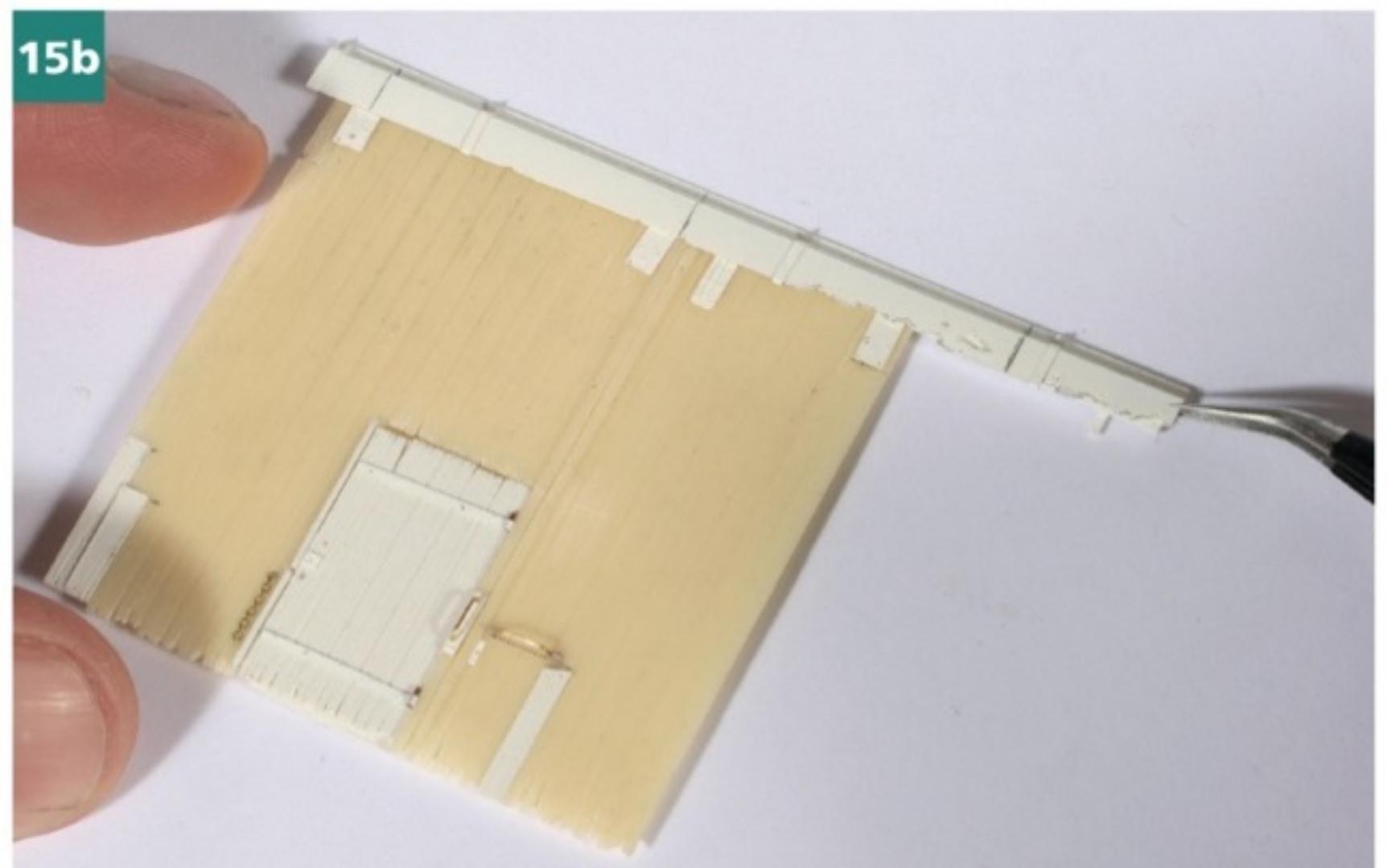


## Step 14

The entrance door was then detailed with great care, using Aber etched brass and Grandt Line moulded plastic parts. I do not set limits just because it is a smaller scale – details count, as close-up photos will show, so it must be as perfect as possible.

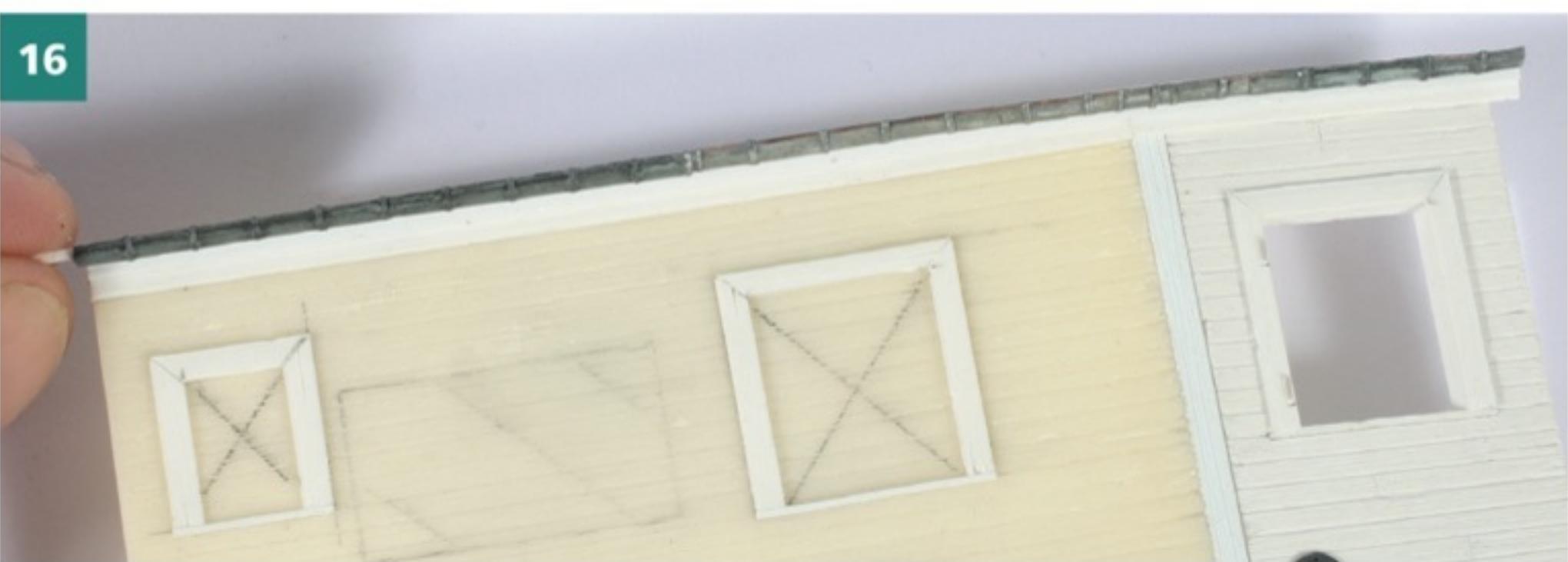


## 15b



## Step 16

Of course, the façade needs a gutter. For this I used some MKD guttering and downpipes which I refined as a test some time ago. Such components are always stored awaiting the right opportunity to use them, which saves time.



## Step 17



The components complete but unpainted, ready for a test assembly. You will notice that there are no roofs – these are not necessary given the angle the buildings will be seen when presented in the display box. If they were stand-alone structures on a layout or diorama, I would have made the roof of Flemish tiles – this could eventually be the subject of a separate article.

## The result

The new elements, as yet unpainted, in place alongside the second café, test assembled in the display box.

I hope this all goes to show that, with a little imagination and/or inspiration from the prototype, some basic materials and detail parts, and a certain amount of care and time, commercial offerings can be transformed and create unique scenes.



## Roger Nicholls

returns to European modelling with a compact quayside scene.

*Photographs by Derek Shore.*

# Nordhafen

A small harbour in northern Germany

The reason behind this layout was an idea to model something which included sea water. I decided to use German HO stock and make the setting a small coastal harbour somewhere in west Germany some time in the 1970s. This period would allow diesels in the older wine red livery as well as the recently introduced blue-beige. The line would be freight only, with as great a variety of wagons as possible.

The size of the scenic section is 63" x 12".

The frame was made from 2" x 1/2" timber with various pieces of MDF and ply for the backscene and end boards. The trackbed is 6mm ply. All the wood was painted both sides to protect from moisture.

The track plan is basically a four track 'inglenook' shunting puzzle. A three track fiddle yard allows for trains to shuffle on and off with different wagons.

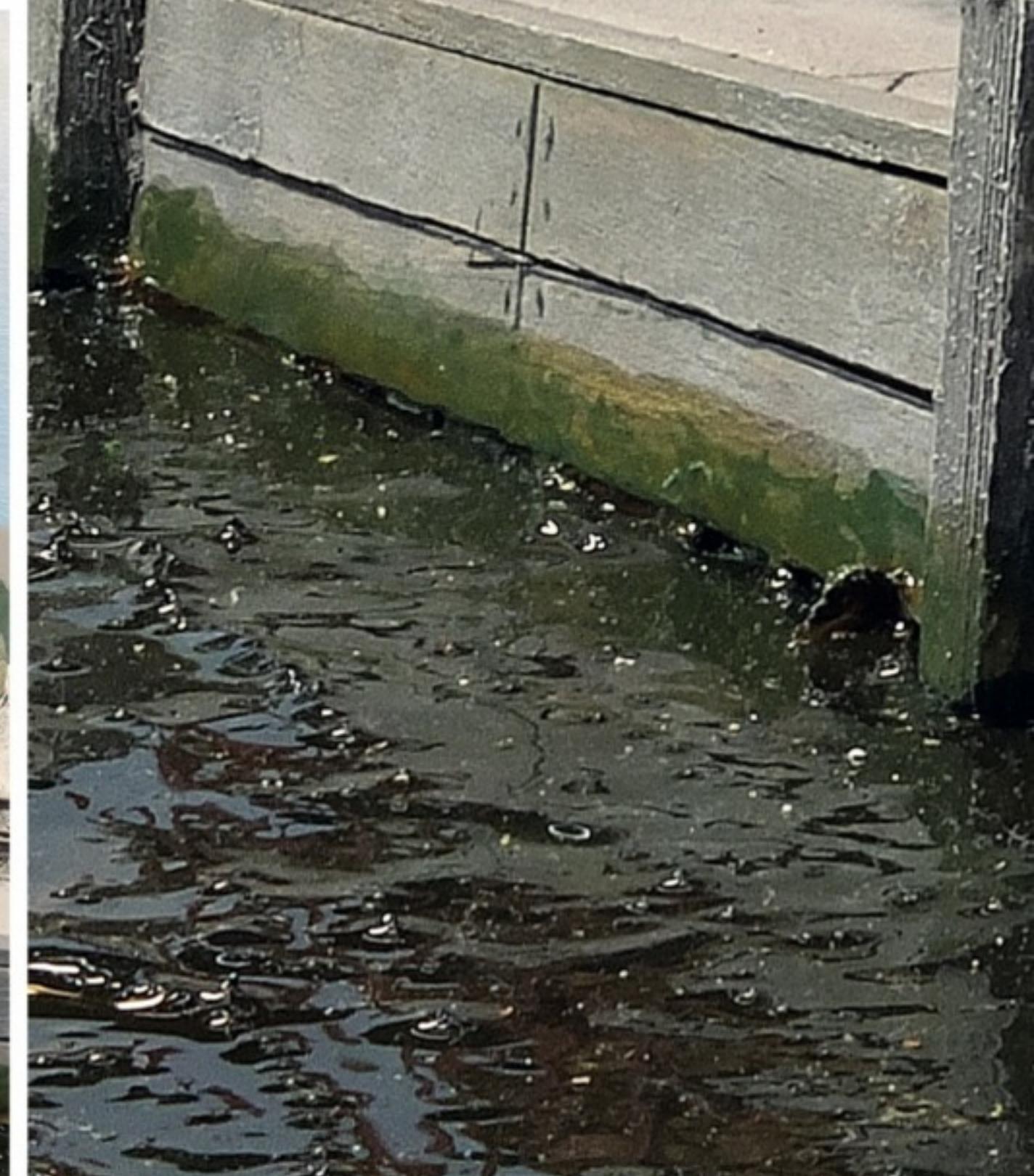


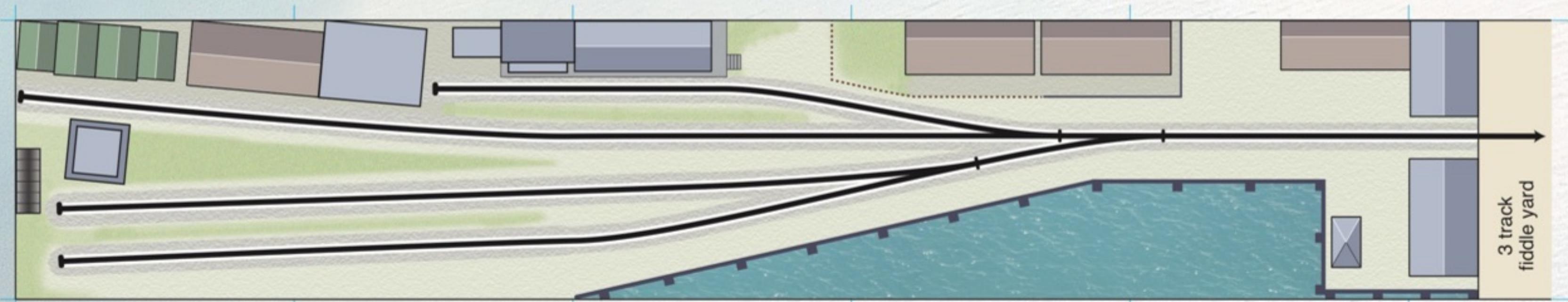
Below left

B-B diesel 211 070-8 has just positioned a lowside wagon carrying general goods by the Raiffeisen store to be unloaded.

Below

B-B diesel 290 189-0 propels a rake of assorted wagons onto the quayside siding. The harbour handles a wide range of goods.





**Nordhafen** Overall size of scenic section 5' 3" x 1'. Each grid square = 1' x 1'.





Left

A type Ucs twin silo wagon under the loading pipes at the grain store. The inlaid track was achieved with 2mm card, painted and weathered.

Below

A BR211 delivers two wagons of timber to the dockside. Note the uncoupling tuft by the goods shed, carefully positioned two standard long wheelbase wagon lengths from the buffers.

Right

On the two middle sidings the uncoupling tufts are situated in the road.



The track is Peco code 100; the points are small radius, with one wye. The points are worked simply with piano wire. The side of the rails was painted with dark brown and the sleepers an earthy grey.

The ballast and what greenery there is comes from 'Country Scenes', who offer a range of reasonably priced scenic materials on their website.

The inlaid concrete area was made with 2mm card painted with Humbrol matt 28 then suitably weathered with powders and a soft pencil.

The harbour walls were made with lolly sticks, stained with thinned Humbrol paints.

The backscene was painted using 'Moody Blue' matt emulsion paint matchpot samples from Wilko.



The buildings were all made from kits from various manufacturers, including Vollmer, Faller, Heljan, and Model Power. Particular mention must be made of the grain silo, which was made from a Piko kit, cut in half to give twice the frontage in low relief. The same applies to the block of flats, which was made from an Auhagen kit, with the walls repainted using magnolia emulsion.

The structures were carefully grouped around the harbour office end of the scene to disguise the access to the fiddle yard as much as possible.

All the buildings and stock have been weathered to some degree using matt varnish, black washes, and dry brushing.

The sea water was started with filler applied with a palette knife, then stippled with a stencil brush. When this was completely dry, it got a coat a black with some blue and green blended in. It was finished with three or four coats of gloss pine varnish which gave a nice deep, wet look.

Uncoupling is achieved with my usual and now much-used method of grass tufts between the rails. The loop is taken off the coupler at one end of all the wagons, which then of course have to be placed on the layout facing the same way. This method works equally well with European or British style couplings.





Above  
**290 139-0** departs hauling  
 just three vans – train lengths  
 are necessarily limited.  
 The sea water is three or four  
 coats of gloss pine varnish  
 over black paint with some  
 blue and green blended in  
 applied over stippled filler.

Right  
**The short open wagon**  
 carrying bales of cotton  
 will need to be unloaded  
 while the weather stays fair.  
 The harbour walls were  
 made with lolly sticks,  
 stained with thinned paint.



Control is standard analogue DC from either KPC or Gaugemaster feedback controllers, which give excellent slow speeds for shunting.

As ever, thanks to various people for their assistance – my regular operators and transport crew, Doug Houston, Robin Nixon, and Neil Burrows; Geoff and Alison of Contikits for their usual helpful service, particularly procuring those hard-to-find items; and Millennium Models of Morley.





Above left  
290 139-0 slowly enters  
the grain siding to collect  
the loaded silo wagon.

Above  
From most viewing angles,  
the link to the fiddle yard  
is hidden by the buildings.

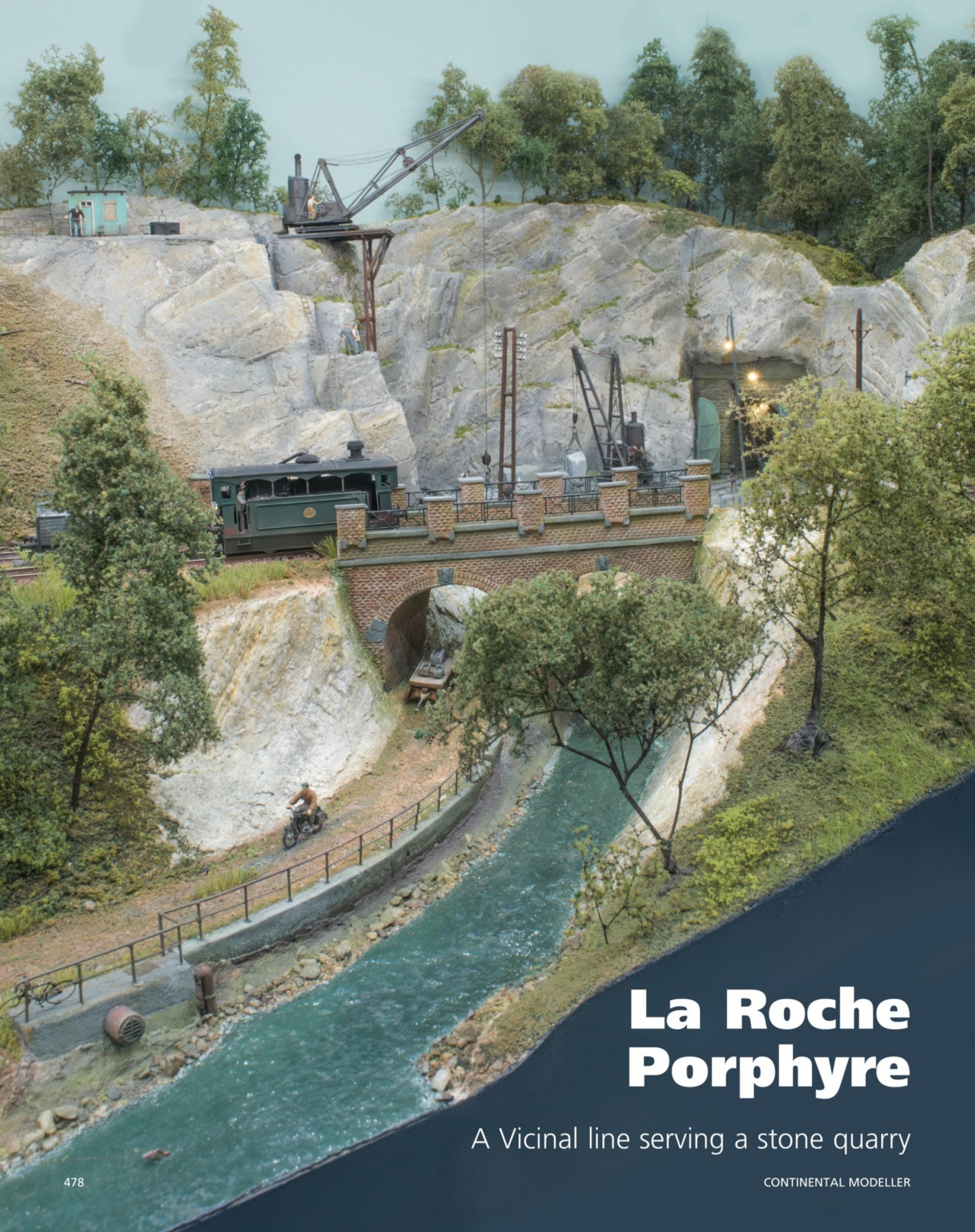
Right  
The window cleaner  
is hard at work as wagons  
are shuffled between sidings  
– the van door must be shut  
before the train departs.



#### Forthcoming show

The layout is due to  
appear at the Thirsk  
show in the Town Hall  
on Sunday 28th July.

See *Exhibition Diary*  
for more information.



# La Roche Porphyre

A Vicinal line serving a stone quarry

**Rudi Nelissen** has expanded his award-winning micro layout, as always planned.

The basis of this project is the original micro layout *La Roche-en-Ardenne* with which I won the small layout competition at the Modelspoor exhibition in Leuven in 2016. It represents a regional line in the province of Luxembourg, part of the Vicinal tramway network (SNCV), between La Roche and Melreux, with a depot and workshop at La Roche where the steam tram locos and later the diesel trams were maintained.

The mini layout was inspired by an old postcard from the early 1900s, a photo of the station and the workshop. After a look at Google Earth and a visit to the actual location, measuring and photographing, the foundation was made for the micro layout. (See CM July 2017.)

To be within the required dimensions for the competition, the first module could only feature part of the site. From the start it was clear that this would be just the beginning of a larger project with the rest of the site to be built on two additional segments. This article is only concerned with these two new modules.

## Module 2

The second module is an extension of the depot of La Roche with some more of buildings that belonged to it.

The shed next to the main building served as storage for coal, sand, and equipment. There was also a steam-powered machine to prepare the sand for use by steam locomotives.

Visible in that old postcard there was a small turntable, just in front of the entrance to the repair shed. This was used to turn a locomotive or wagon if this was necessary for the work to be performed. Due to lack of space I decided to leave this out when building the original micro layout. The turntable has subsequently been given a better interpretation on module 2, namely near a building some distance away which was also used as an engine shed. This shed still exists. After a fire, it was rebuilt and is now a clubhouse for the local scouts. During its restoration, attention was paid to the past and its authentic elements were preserved.



Top

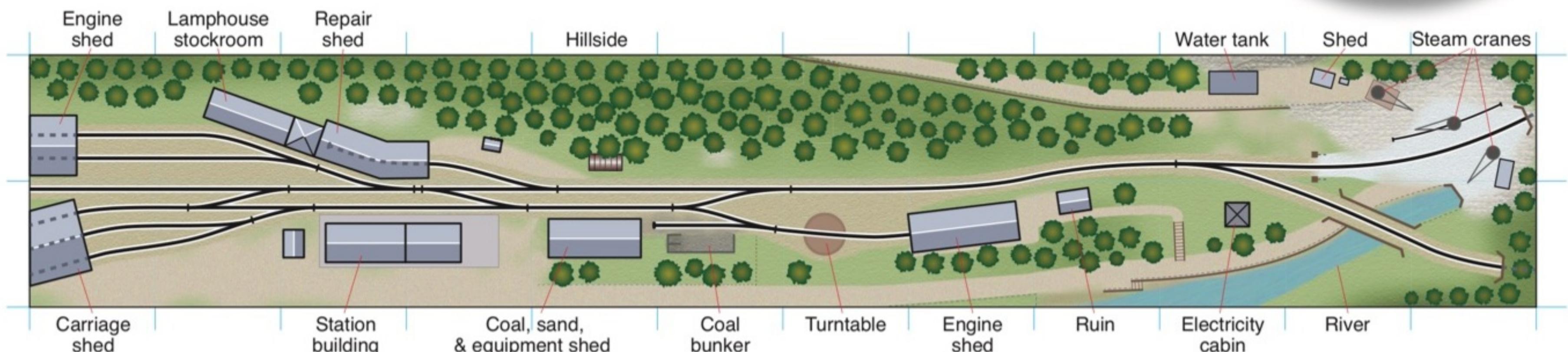
A worker opens the gates of the shed to pick up sand that has been prepared for use by the steam locos.

Above and right

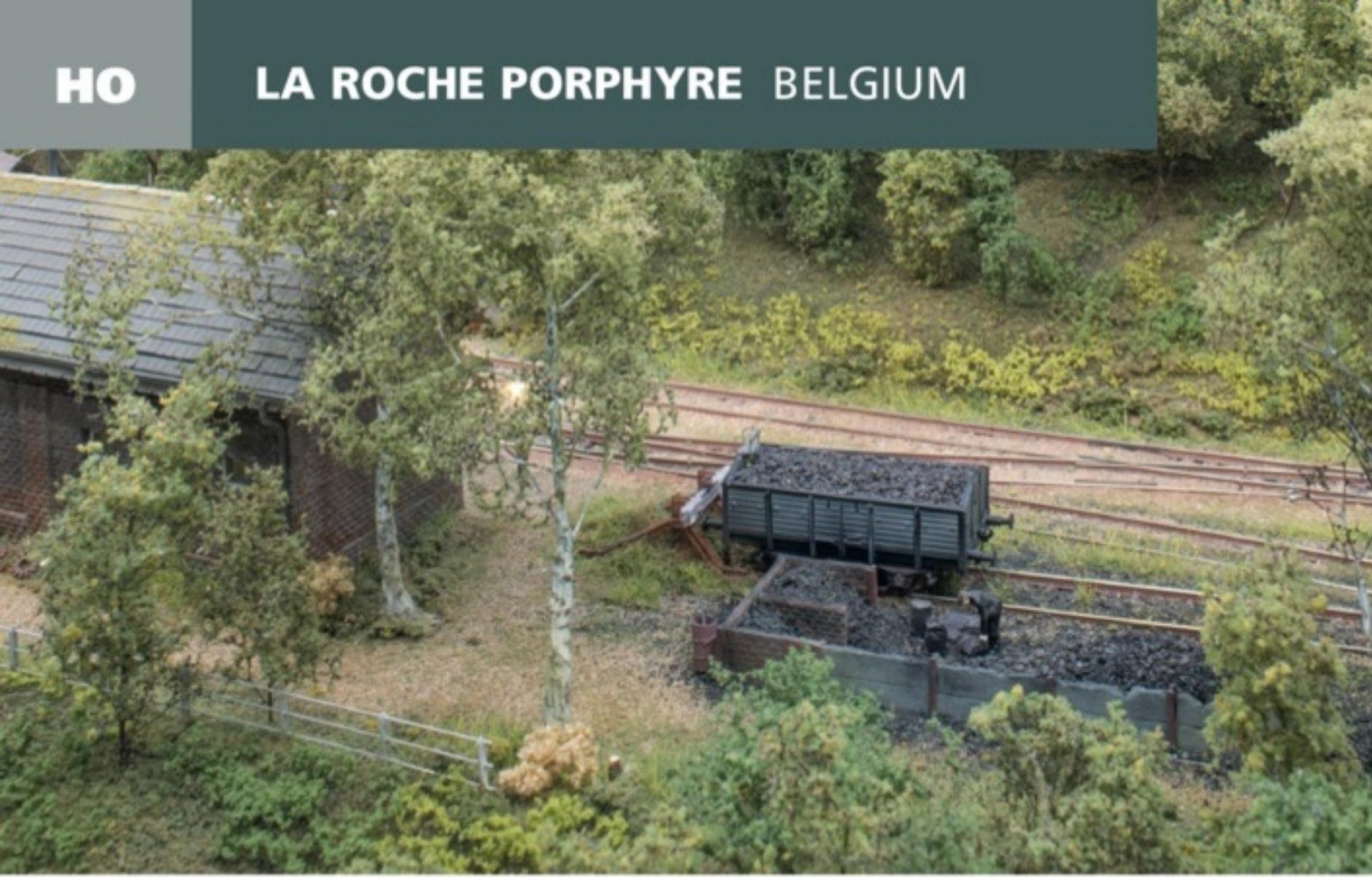
A small type 15 Vicinal loco waiting on the turntable.

Left

A Vicinal tram loco crosses the bridge with a wagon loaded with cobblestones just collected from the quarry.



**La Roche - Porphyre** Overall layout size 3600mm x 600mm. Each grid square = 300mm x 300mm.



By locating the turntable on this part of the site, operating has also become more fun. In addition, because of this and by placing an extra turnout, I also had room for a small coal bunker. This is not authentic but built from my imagination.

### Module 3

The third module does not reflect reality in La Roche. I wanted something which added to the operation, and had some working features beyond the railway. I had seen several pictures of steam trams used to transport cobbles and bluestone. Since the layout is set in La Roche, in the middle of the Belgian Ardennes, I felt it would be appropriate to choose a small (fictitious) porphyre stone quarry.

Since steam cranes were used in such quarries in the steam era, they had to be included. On the basis of old photographs, I have constructed three of them in brass. They all work and loads can be moved within the quarry. The cranes can also be used for loading and possibly unloading the wagons.

On top of the excavated hill, next to a small wooden shed and the large steam crane, there is a water tank on a brick base. This was built following the example of an existing



SNCB/NMBS water tank at Muizen near Mechelen.

The quarry is mainly occupied with mining bluestone. This was processed by stonemasons making troughs and tombstones. However, the main activity was the production of the typical Belgian cobblestones. Down in the quarry, where the underground river rises above ground, we see the cobblestone masons at work. At that time, the cobblestones were cut by hand. In large quarries they sometimes had more than a hundred men in a row to cut cobbles. To protect against the sun and the other elements, cane roofs were used and these have been modelled.

The railway siding leads to the underground part of the quarry. Here we see mostly the small but powerful Cockerill vertical boiler 0-4-0T locomotives at work.

The main line continues over a bridge. This bridge is based on the old tramway bridge at Katteberg in Bilzen. Further on, the track disappears into the mountain.

This completes the steam tramway layout in the Ardennes, for the time being. Though the option to model what is on the other side of the mountain remains open ...

Above left

**A railway worker is filling sacks with coal, which are easier to hoist on board the locomotives.**

Above

**Behind the small loco shed an abandoned house has fallen into ruin.**

Below

**Overall view of the quarry. The water tank on the hill supplies the steam cranes.**





Top  
**Much activity in the quarry.**

Above  
**The operator of the largest crane ensures that loads are lowered safely from the working face.**

Left  
**Stonemasons are cutting cobblestones by hand while fellow workers operate the cranes or assist by guiding the heavy loads.**

Photographs by the editor.

**Boyd Misstear** shows how he built a tunnel station inspired by the unsuccessful proposal for a station beneath Sedrun during the Gotthard Base Tunnel construction. *Photographs by the author, unless otherwise noted.*



# Porta Alpina

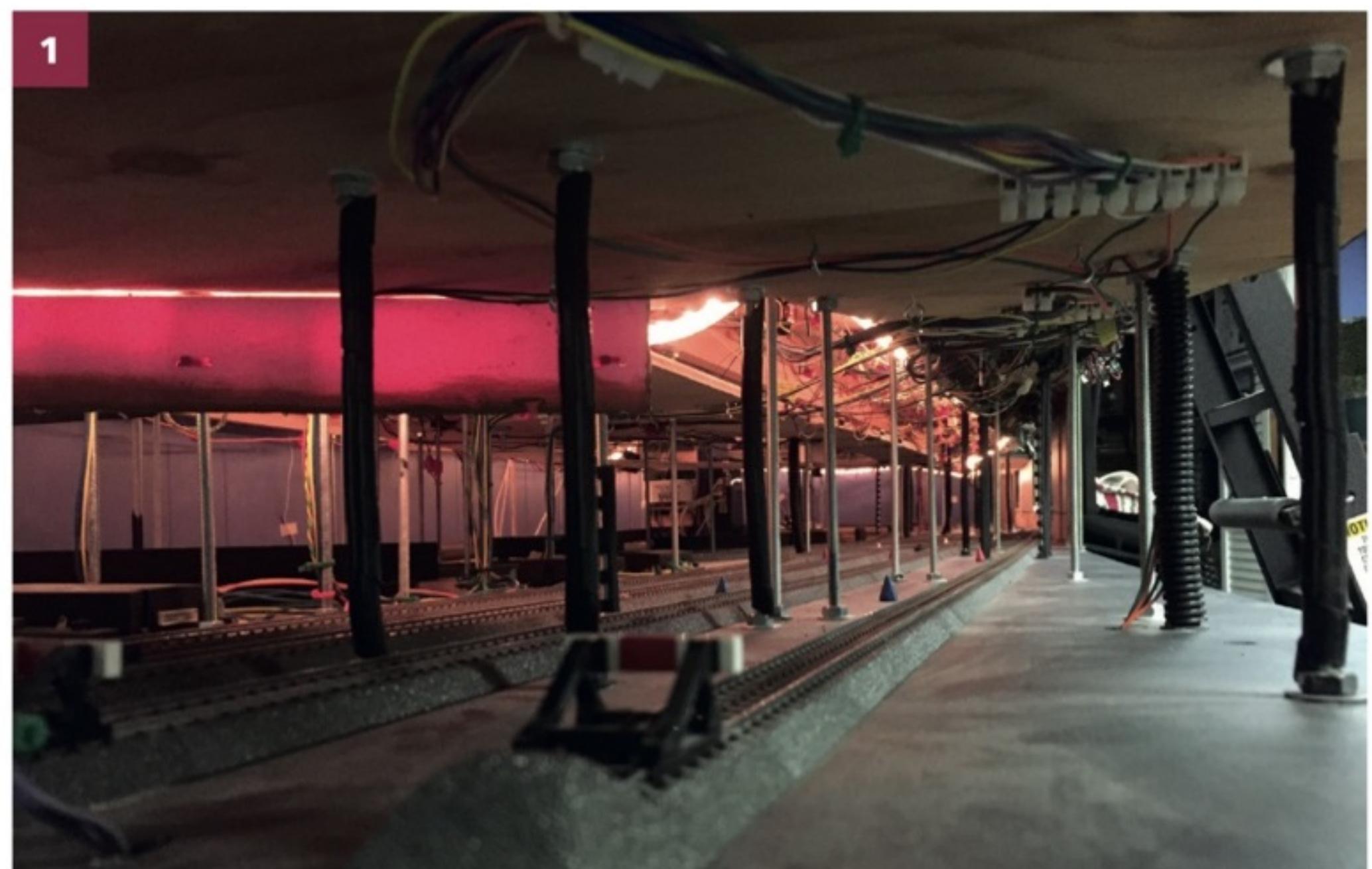
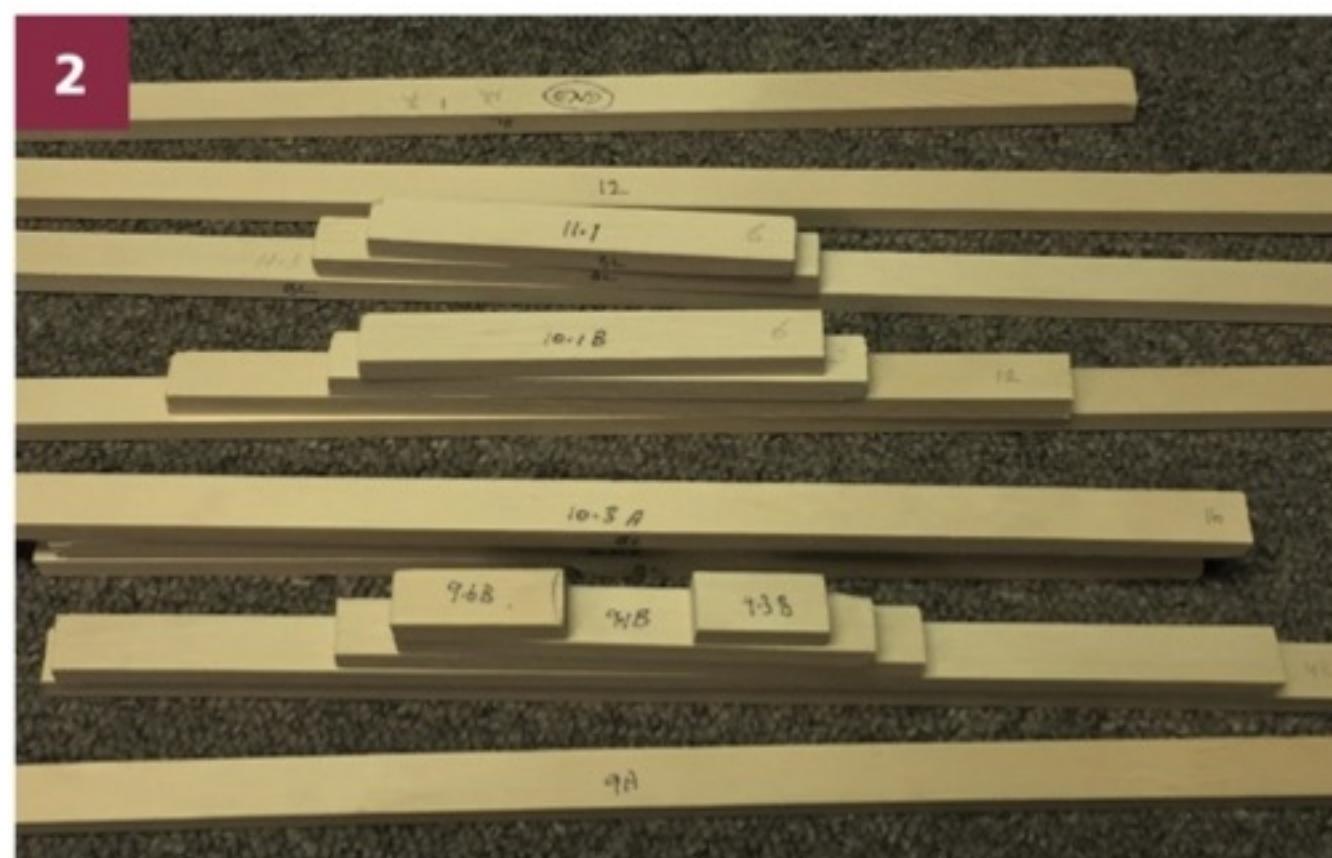
An ingenious way of extending the layout

This is essentially a pictorial essay showing the steps taken to achieve the desired result – a very modern four track terminus large enough to store three short multiple unit consists of up to four coaches and one longer main shuttle platform capable of handling six coaches plus motive power. Access for passengers is provided by escalators to a terminal hall above platform level, and from there by imaginary lifts to the metre gauge Matterhorn Gotthard Bahn layout which is located above!

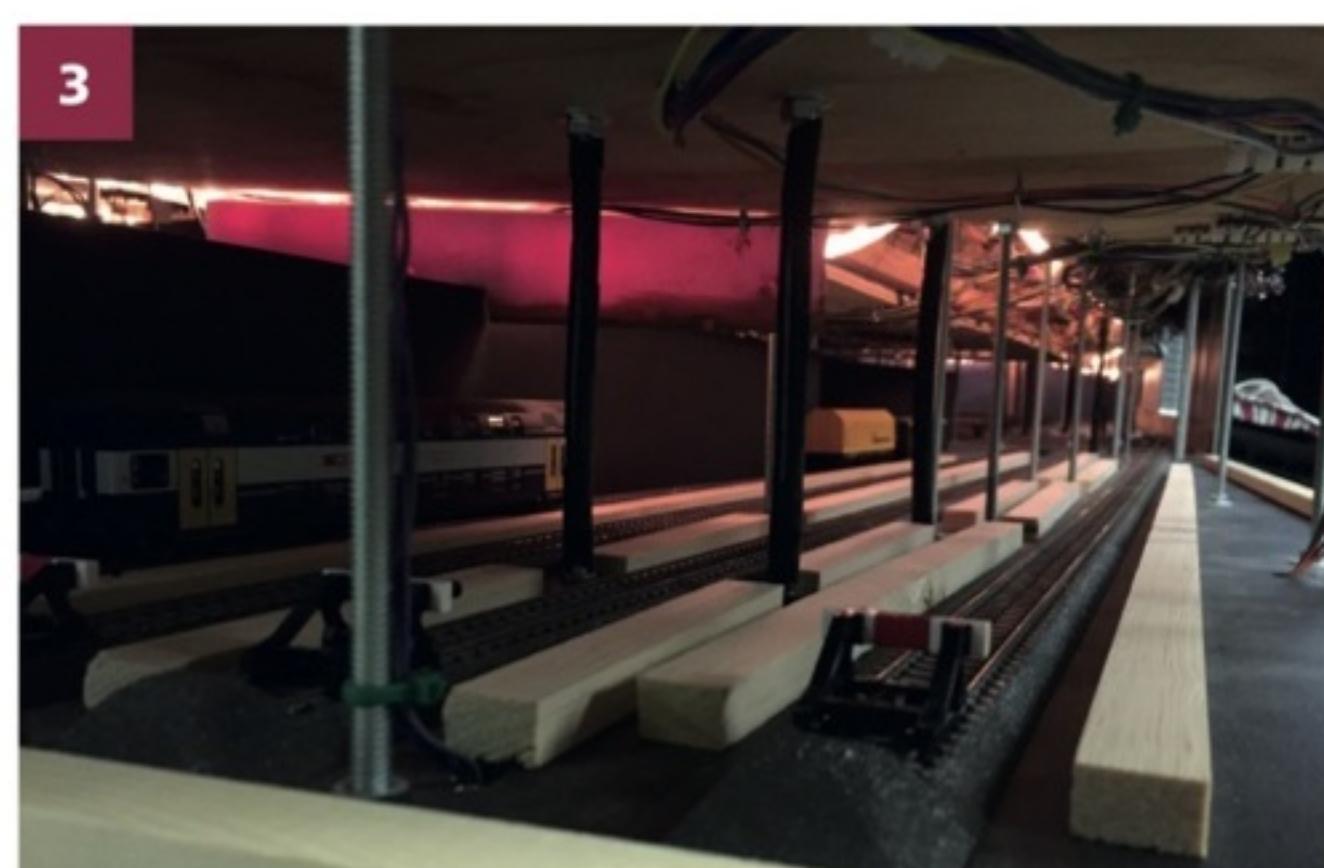
As you will immediately gather, this is in not meant to be an exact replica of the proposed prototype conversion of the single emergency station through platforms in each tunnel beneath Sedrun on the Gotthard Base Tunnel with vertical elevator connections to the outside world.

## Background

Having previously built a replica (using modelling license, of course!) of the Bern Lötschberg Simplon base tunnel beneath the HOm layout to serve primarily as a staging area to hide trains during planned standard gauge operating sessions, there was the opportunity to add four relatively short parallel sidings in unused space. What a waste not to make use of this! And so the sidings were installed and effectively forgotten about until discussion in the Swiss media about a proposal to provide Sedrun with a rival gateway station to that of Andermatt, which was referred to as 'Porta Alpina'.



The idea was probably more of a marketing thought than a true engineering and practical consideration to transform the emergency access to the GBT into a regularly served location, since this would undoubtedly have had a serious impact on the prime mission of maximizing and expediting north-south freight and passenger through services. And of course there are all the other considerations of safety of use on a regular basis and so forth that were unlikely to have been considered in depth. But in the model world we do not have to consider health and safety aspects for our Preiser figures!



Top  
Photograph by Hein Hopmans.

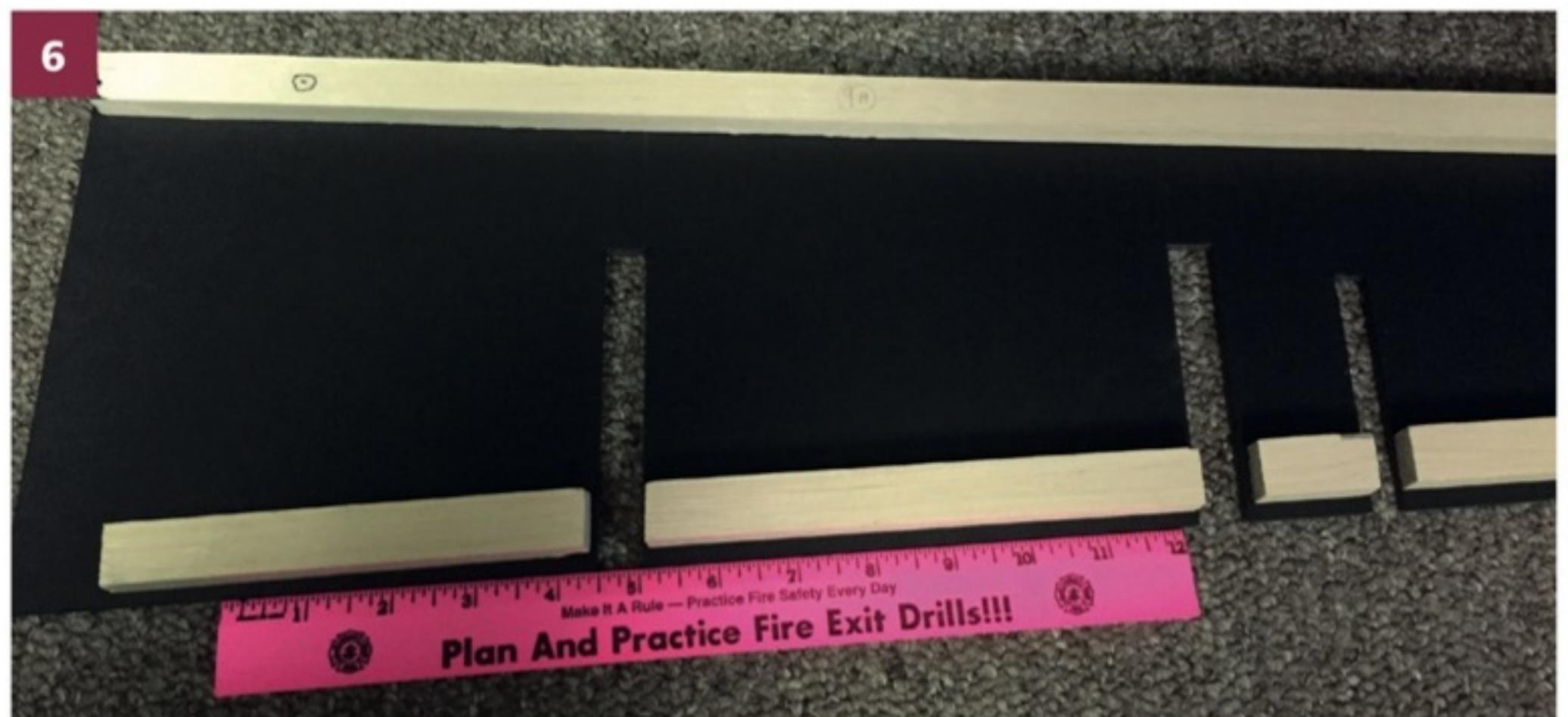
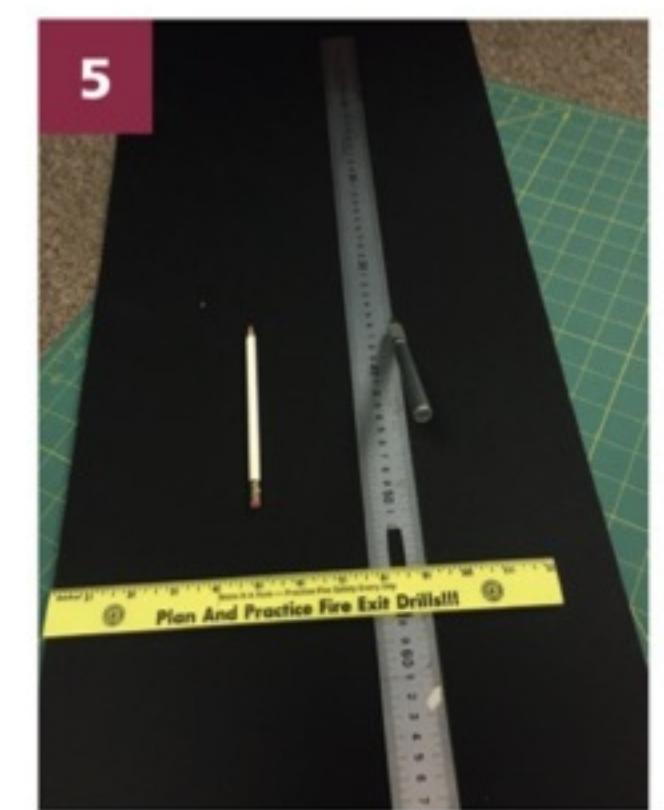
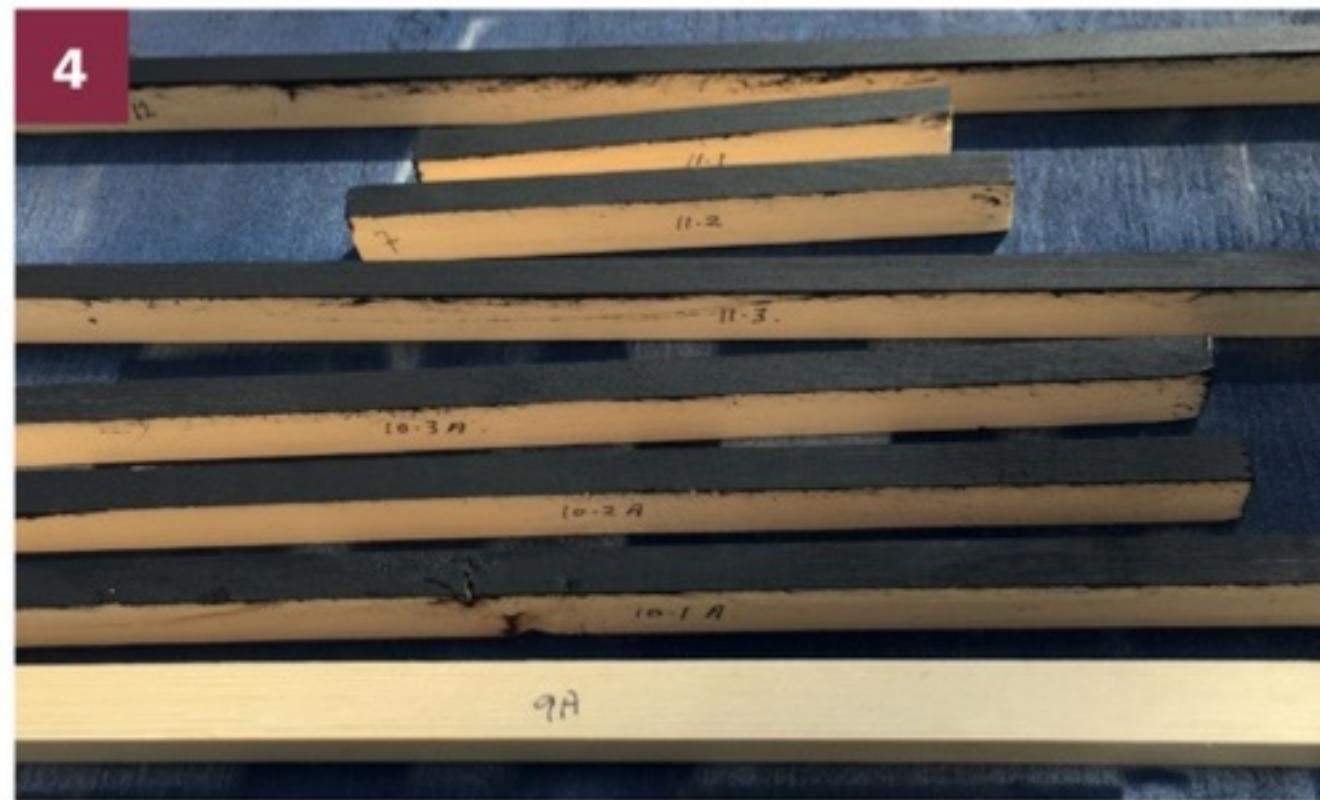
1.  
The intended station site.

2.  
Preparing the pine platform supports to size.

3.  
Test positioning the platform supports.

## Tools

mitre cut-off saw  
hot glue gun with glue sticks  
wire and sprue cutters  
selection of screwdrivers  
fine tipped soldering iron  
paint brushes (various) and stirrer  
fine anti-static tweezers  
measuring tape/ruler  
sharp knife and blades  
spring clips/clamps  
scissors  
cutting mat  
straight edge (and flat working surface!)  
suitable colour-coded wire – 22 or 26awg or equivalent  
pencils and eraser  
Microsoft Paint or similar graphic application



### 4. Paint platform supports.

### 5. Measure foam board for platform and backdrop.

## Materials

1/8" black foamboard (quantity and size to suit)  
Scotch 3M ATG Adhesive Transfer Tape #924  
(for use with paper and foam board)  
Pacer PT-56 Canopy Glue for styrene items  
(not paper/foam board)  
Tacky glue  
Solder and flux  
Basswood strips (for strength, and lighting mounts)  
Styrene L and H profiles, sizes to suit  
Cable ties  
Velcro 'hook' and 'loop'  
'Asphalt' colour paint for platform sides  
(e.g. Woodland Scenics ST 1453)  
Tamiya X-15 Light Green acrylic (to emulate BLS green)  
Spray paints, e.g. Tamiya range:  
AS-15 Tan (weathering bottom platform sides)  
TS-1 Red Brown (rail rust weathering)  
TS-15 Blue (emulate BLS blue)  
TS-17 Gloss Aluminium (station signs)  
TS-22 Light Green (emulate BLS green  
for the vertical column covers)  
LE 4100057-WW (Warm White) LED strip – 5m x 2  
LE RF Wireless Remote LED Controller  
12v power supply to suit number of LEDs  
(LEDs etc. from [www.lightingever.de](http://www.lightingever.de))  
Red LED for stop light on buffers  
Track of choice  
Drink stirrers or similar  
FFE ref.99621 escalator set (packet of two)  
Figures and other details

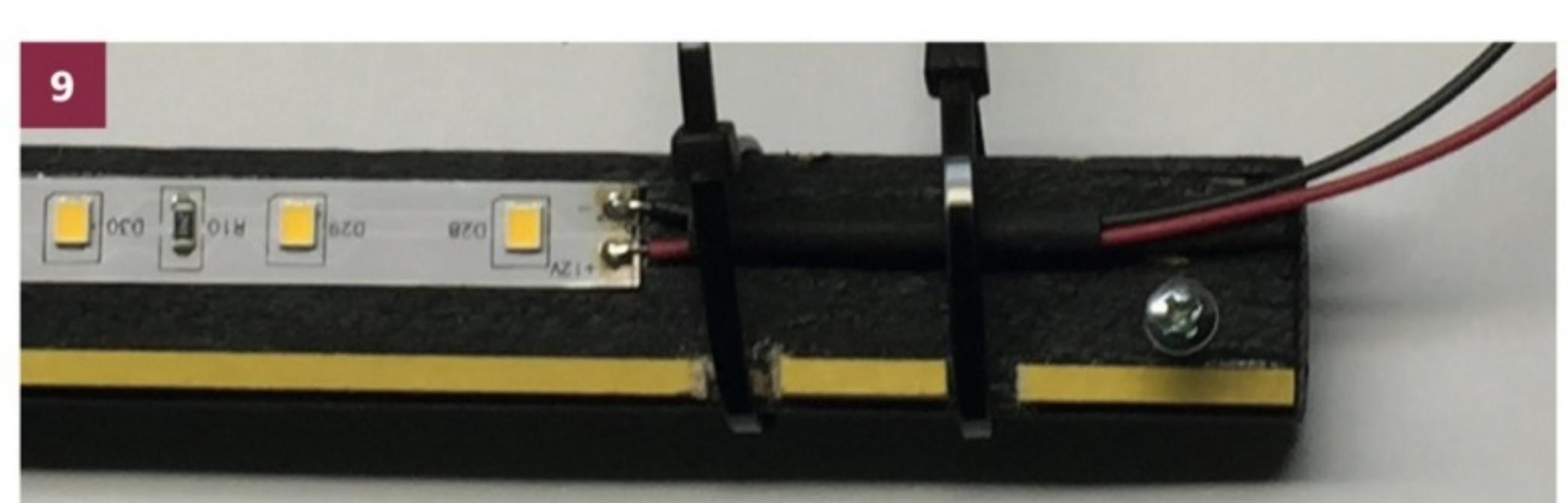
### 6. Cut platform and backdrop roof from foam board to fit.

### 7. Attach platform to supports.



### 8. Prepare L-shaped basswood for lighting strips.

### 9. Paint basswood supports and attach LED strips.

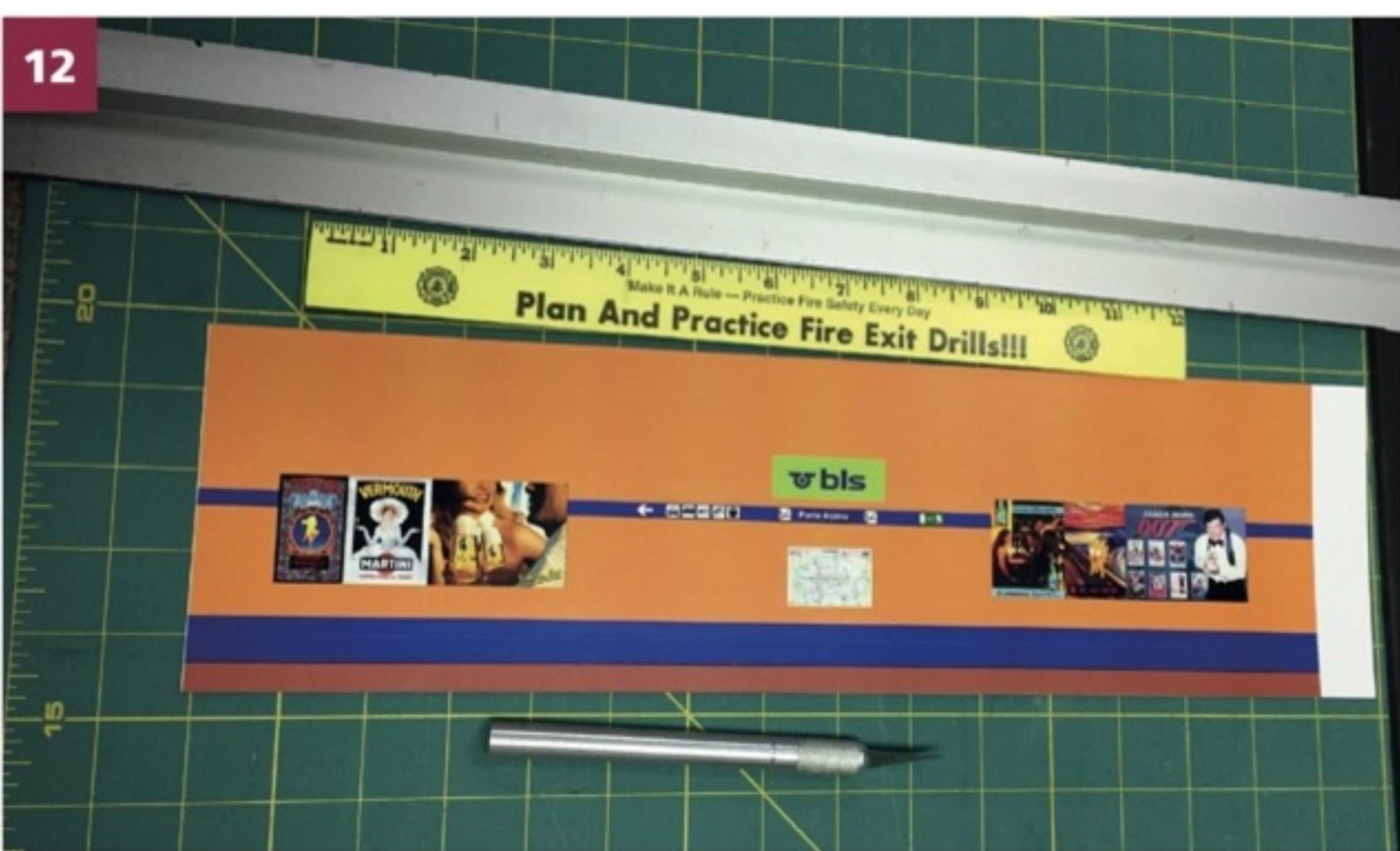
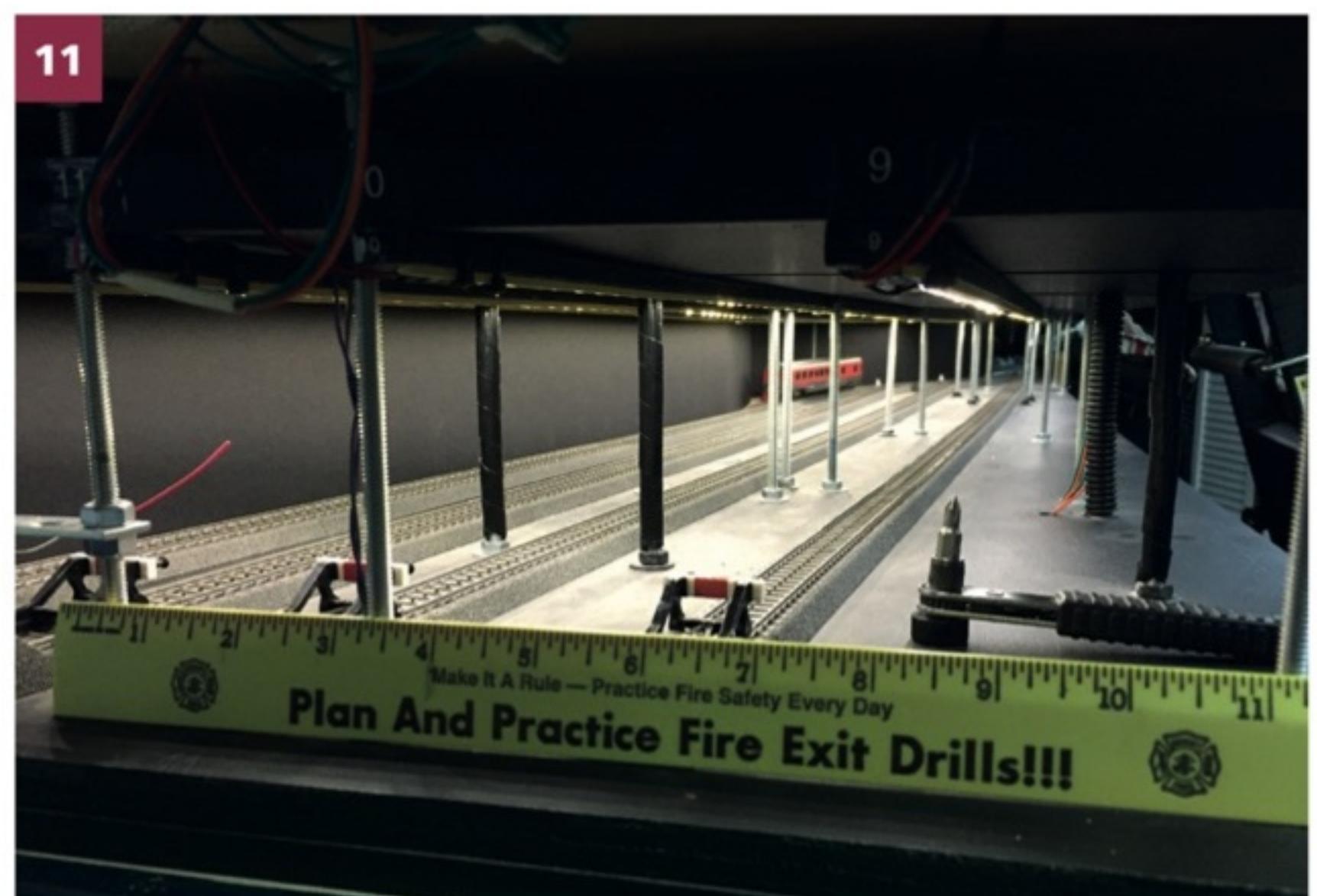


## Design

To give a light and airy feel, bright colours similar to those of the modern BLS lit by warm white LED strips applied to basswood formed into L girder strengthening frames seemed a feasible way to proceed. Approaching five hundred LEDs were installed in strips at a fraction of the cost or complexity of using individual items, and an inexpensive in-line RF dimmer provides additional realism as it allows the light intensity to be altered to match conditions on the layout. I have used this controller before. It is possible to choose whether one controller controls all receivers, or whether it is used on an individual or selective group basis.

The tools and materials can be modified to suit your own circumstances.





Prepare suitable backdrop and platform surface graphics and print in matt or gloss – matt was chosen for the backdrops while gloss paper was used for the platform surfaces to simulate new tiling. I had help with this from Dale Schutz, a fellow member of the Colonial Chapter of the European Train Enthusiasts of North America, who has assisted me enormously over the years on a number of projects.

Cable ties are used for mounting the cross supports to the vertical threaded rods. The lighting strips are mounted beneath the cross supports. The numbered roof sections slide in and rest on the lighting strips.

10.  
**Prepare and install pine cross supports for LEDs.**

11.  
**Mount LED strips to supports and test.**

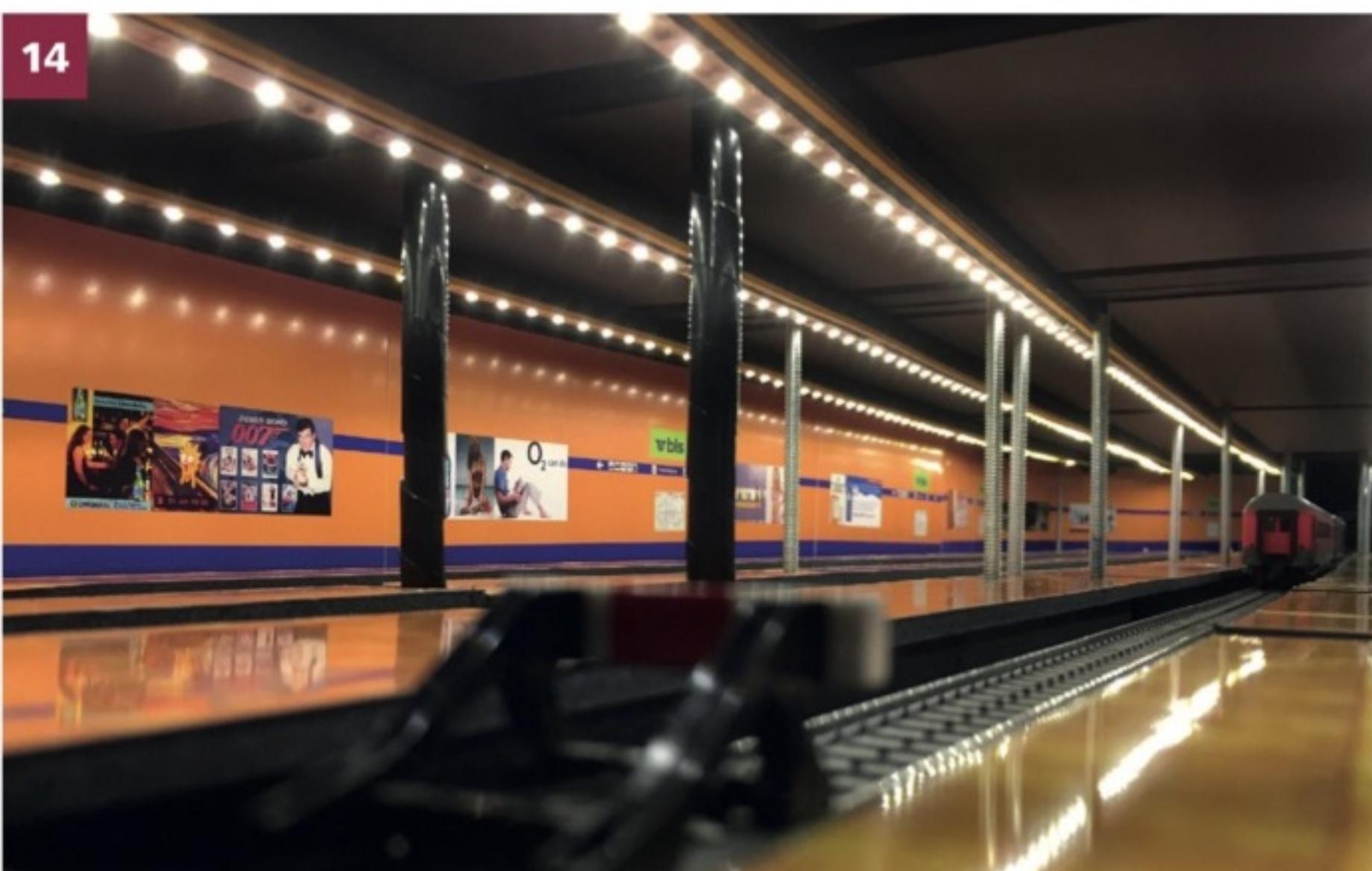
12.  
**Prepare backdrop graphics, print out, and cut to size.**

The LED strips are cut to the desired length.

The fascia is  $1/8"$  hardboard, with window aperture surrounds from inexpensive photograph frames with the backing removed, stuck in place using a hot glue gun, and painted the colour of choice – here a dark green.

The legs are covered by conference table curtain material held in place by Velcro for easy on and off.

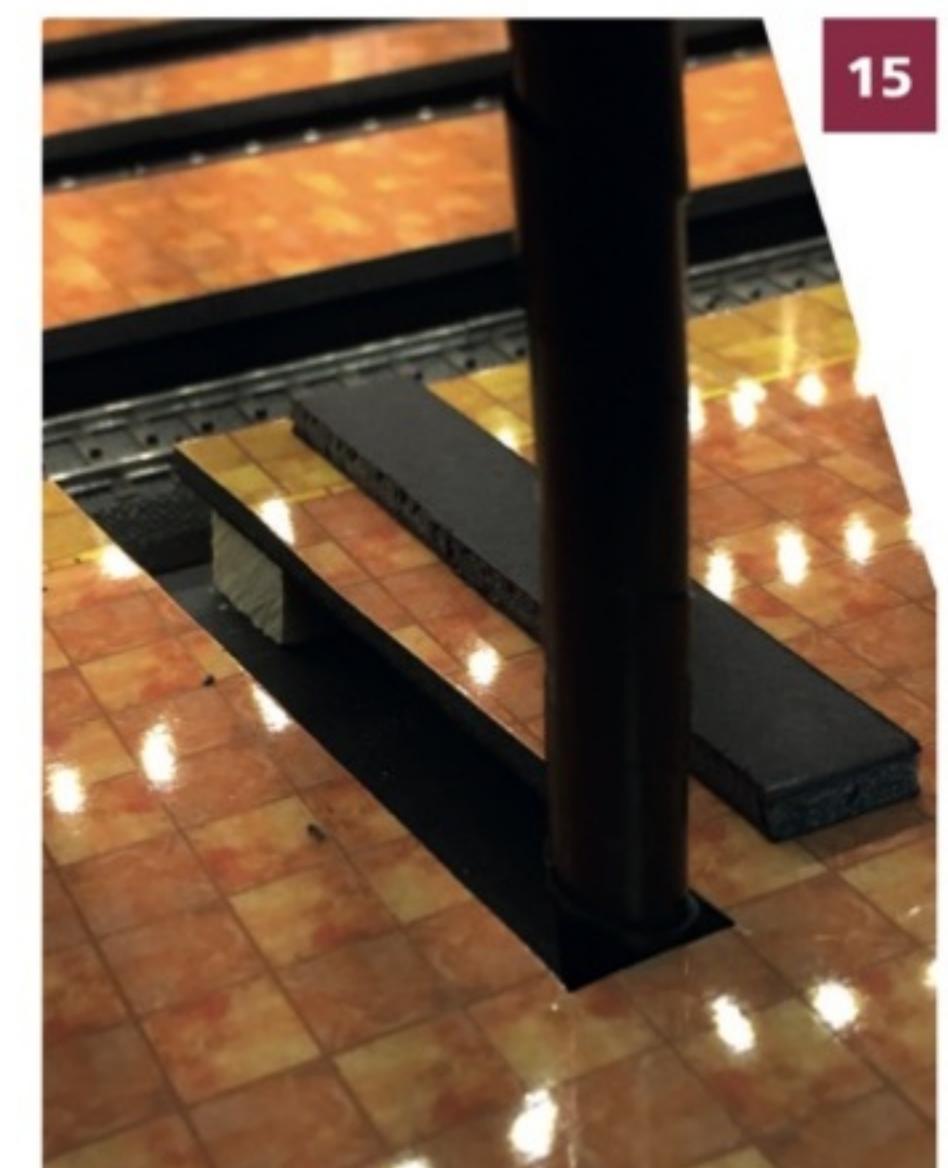
Normal safety precautions should be applied: for example, spray painting is undertaken outside or in a suitably well ventilated facility. Take care when using tools, glues, and paints, and – of course – have fun!



13.  
**Attach platform and backdrop prints to foamboard**

14.  
**Insert backdrops and platforms.**

15.  
**Install platform infills.**



16.

Cover vertical supports  
and add signage.  
Finally, detail with figures  
and fixtures.  
Photograph by Hein Hopmans.



Right

The new Porta Alpina station  
under the existing layout.



Below and below right  
The underground station  
of Porta Alpina is visible  
through two windows  
in the fascia, neatly framed.



**Simon Holtby** reveals how he managed to find space for a third layout in the railway room.  
Photographs by the author.



# The Nishikawa line

The result of random purchases

In my younger years I had always modelled the British railway scene and gave absolutely no thought to modelling anything else. It was in 1981, during my first trip to Japan that, purely by chance, I came across a model shop in Kyoto. Venturing inside I was amazed and impressed by both the quality and price of the wide range available. I just bought a couple of N gauge items (the most common scale in Japan) as souvenirs, without any intention of ever going beyond that. Subsequent trips saw more small purchases, again with no view of doing anything other than displaying them on a shelf.

Over the years I had concentrated my modelling on my Scottish *North West Highland Railway* (RAILWAY MODELLER August 2013 and August 2016). Readers who may have read these articles would note the relocation of this railway to a much bigger room, enabling me to also construct a TT layout (*Hook Norton* – RM February 2019) in the same room. While these two layouts occupied the majority of the space, a small corner remained unused corner – this provided the opportunity and motivation to build a small Japanese layout. I use the term ‘unused’ loosely as the layout actually sits partly over the fiddle yard of the TT layout.

Below  
**Nishikawa station and town.**  
A preserved 2-4-0T and vintage stock (all by Micro Ace) ready to depart – such tourist trips are a feature of a number of lines I have stumbled across during my travels in Japan. The single unit diesel railcar (Green Max) in the further platform is a local service.



## Inspiration

While still being excited and impressed with the ever growing Shinkansen network, I find that on all my visits to Japan I am drawn to the enormous variety of smaller lines, usually operated by just one- or two-coach trains. They seem to exist everywhere, using both electric and diesel traction. The trains themselves are often brightly coloured and have supporting infrastructure that seems to reflect their diminutive scale.

As an example, at Idakiso on the Kiishi line, I observed a scene which epitomised what I was trying to achieve: small train, short platform, quaint offset station building, and a small depot in close proximity.

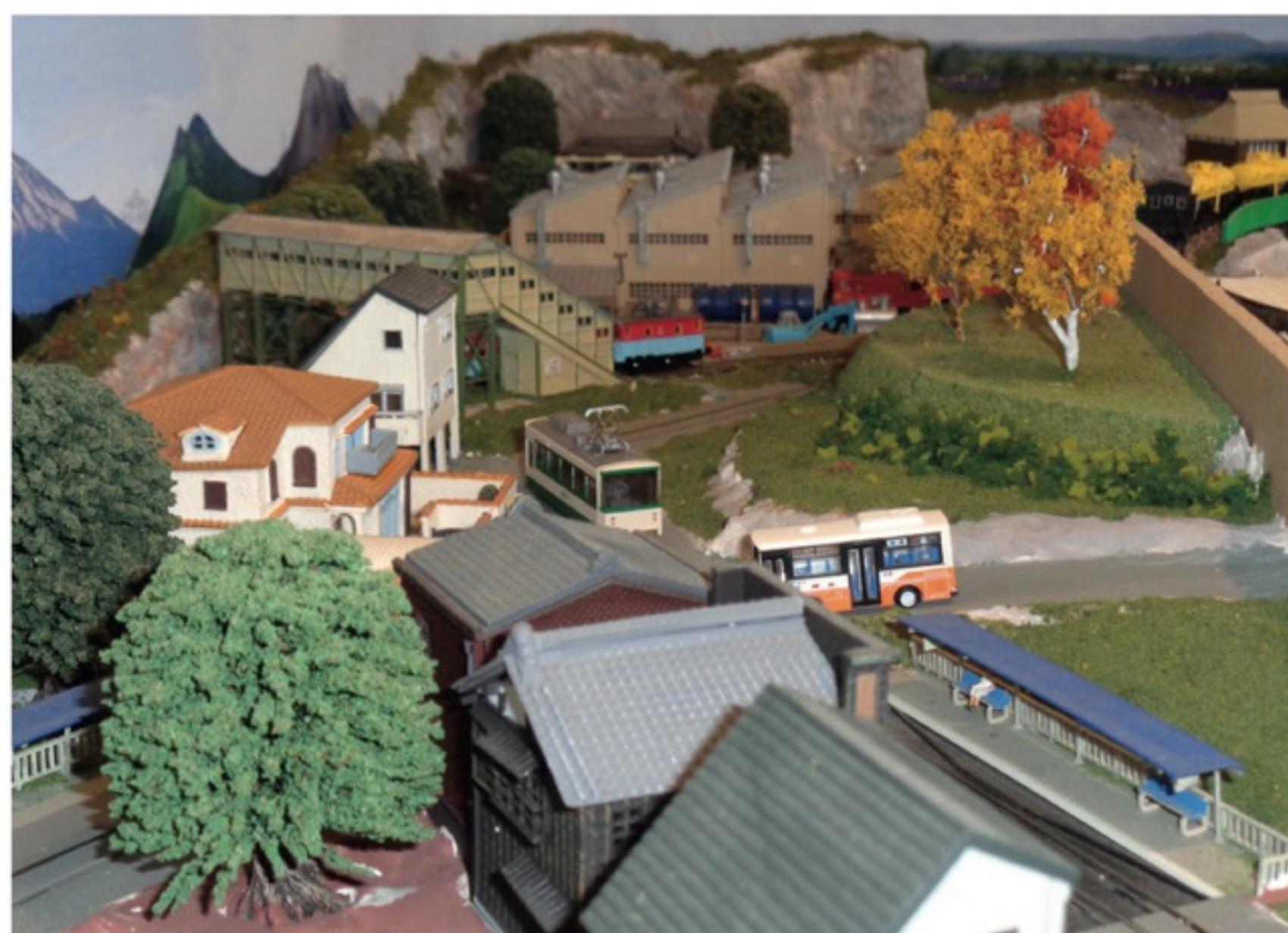
## Why Nishikawa, and where is it?

So why call the layout *The Nishikawa Line*? As it is not based on any particular geographical location, there was no obvious name forthcoming. However, the building for the small single-platform station came with a pre-printed station nameboard – ‘Nishikawadai’ – so I shortened it slightly, hence the name. Just out of interest I checked on Google Maps and discovered that there are actually two so-named stations in Japan – one in Hyogo prefecture and one in Fukushima prefecture.

My Nishikawa line bears no relation, other than the common name, to either of these. However, the backscene, made from a montage of printed screen shots of Japanese scenery, does include an image of the iconic Mount Fuji, thereby theoretically locating the layout somewhere in central Honshu. I do not want to go beyond that in defining its location or else imagination would have to be compromised!

## Approach

There was no overarching approach to planning and building the layout. The space constraint was obviously going to limit what could be achieved – so no Shinkansens whooshing around! I suppose the underlying objective was to have fun. Part of having fun is to sometimes just watch the trains go round, so a continuous run, in some form, was desired. This was accommodated using a looped-eight as the basis for the layout design.



Above  
The rolling stock depot, with the small branch terminus, Nishikawadai, beyond.

Right  
A diesel loco (Micro Ace) ready to depart the quayside with a short freight.  
The single unit diesel railcar is by Tomix.



Below left  
The left side of the layout, with a small tram (Kato) making its way along the winding track.  
The side of the depot can be seen in the distance.

Below  
A passenger train consisting of two single car units (Modemo) rounds the curve into the station area; it will reverse into the platform. These units are based on the Hakone-Tozan Railway.





### Layout

While necessarily being a very limited space layout, I was keen to get some interesting operating potential. There is a main station area which manages to incorporate a small railcar shed, only achievable by using just single or two-car units, as well a small quayside for freight operations, similarly short in nature. The space behind this station was used for a small town, the nature of the buildings reinforcing the Japanese identity.

The far corner of the layout was used for a small terminus, with a siding to provide a destination for freight traffic.

### NISHIKAWA

Overall size: 1320mm x 1270mm. Each grid square 300mm x 300mm.



The fiddle yard is, necessarily, small in size but, again utilising the benefits of short trains, is able to accommodate five trains, in addition to having a passing loop.

A small tram line, connecting into the main station area, has been provided in the area behind the fiddle yard. This is rather sinuous in nature, thereby extending its apparent length, and utilises proprietary inlaid trackwork. There are a few more buildings along the route to add interest.

## Technical

The baseboard construction is a grid of 2" x 1" timber topped by 6mm MDF board. The MDF just provides the foundation as insulation board above this forms the trackbed, as this is easy to cut, eliminates most noise, and takes track pins easily. The only place the MDF is visible is as the water surface by the quay.

Originally two separate boards were constructed, held together by bolts. As the boards are so small, they are now permanently fixed together and can be moved, if necessary, as a single unit.

Track is substantially all Peco, using the small radius Setrack points. The only exceptions to this are the two inlaid tracks on the quayside and the inlaid track of the tram route, these both using Japanese products.

As with all my layouts, control is plain 12v DC. The wiring is so simple that there are no isolating switches, the points providing all isolation required.

## Buildings

In all my British layouts I have very much enjoyed making all the buildings from scratch. However, for the Nishikawa line I have used off-the-shelf buildings or kits throughout. Part of this decision was based on a desire to ensure a proper Japanese appearance and the other was being spoilt for choice by the great range of very attractive and good value items available.



## Rolling stock

Stock has been accumulated on an *ad hoc* basis from purchases in Japanese model shops over a number of years, with a focus on interesting items representing more obscure lines. I have purchased models from a large variety of manufacturers, including Tomix, Kato, Tomytec, Green Max, Kawai, Micro Ace, TGW, OneMile, BanDai, and B Train Shorty.

## Conclusion

The layout has achieved its primary objective of being fun to operate as well as providing a dedicated place to run my collection of Japanese models. It is very revealing that all of my family find this layout far more interesting and attractive than my other larger and more 'accurate' British layouts. Perhaps they see the fun and imagination in it!

I would like to thank my wife for letting me completely monopolise the games room!

Above

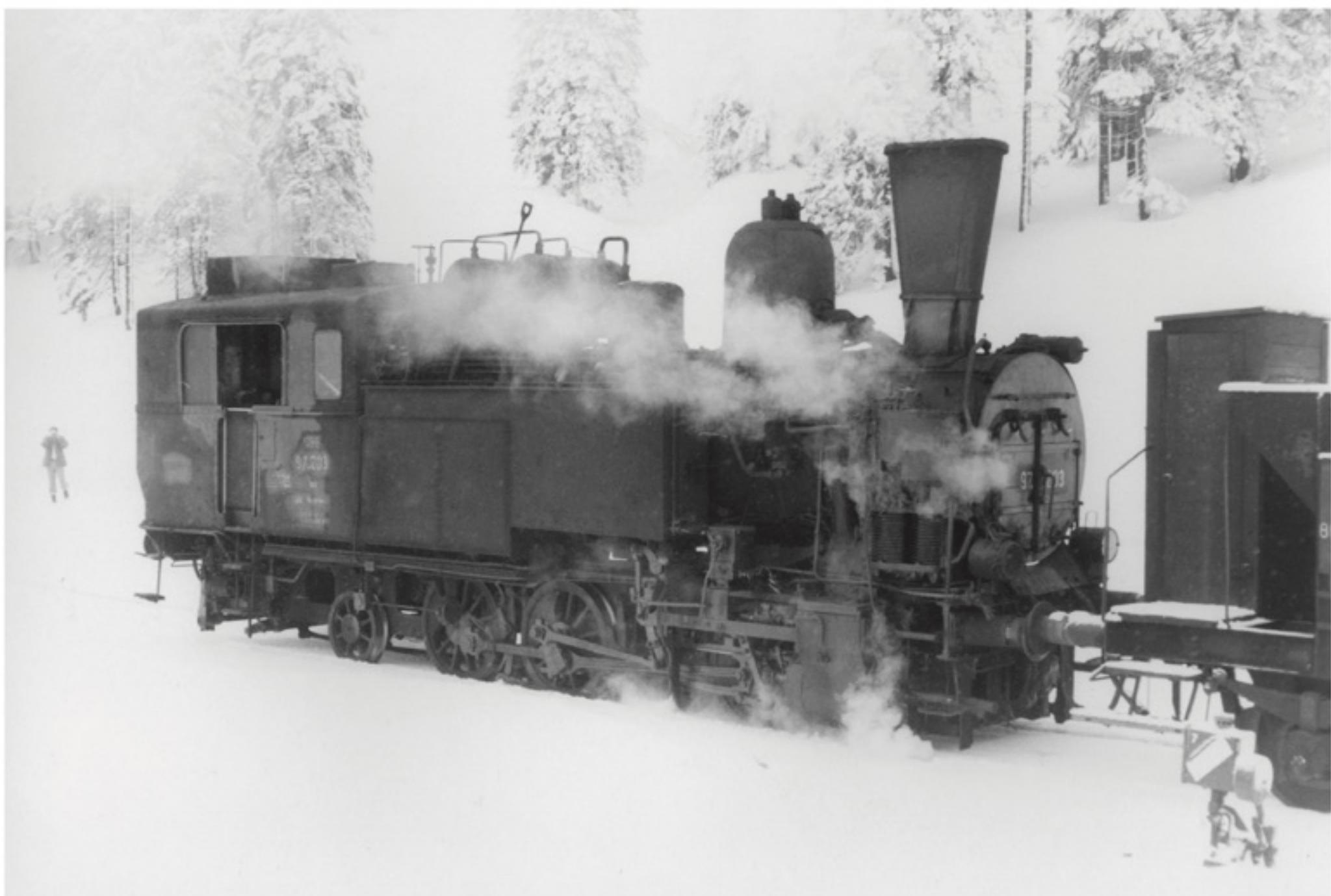
**The single unit diesel railcar. (Tomix) lays over in the loop as a small diesel shunter (Micro Ace) couples to the tank wagons (Kawai).**

Below

**The layout in its cramped position. The fiddle yard of my TT layout is below the Japanese layout and my British N is on the right.**



**Guy Tyteca** recalls a January 1976 visit to the famous mountain line in Styria while it was still worked by steam. *Photographs by the author.*



# The Erzbergbahn

## Ore, rack, snow, and steam – an unforgettable experience

Back in 1976, after some skiing in the Tyrol, I decided to travel east to discover the central part of Austria, particularly Styria, where I had heard about the famous Erzbergbahn rack railway. It was January, deep in the winter, and there was a lot of snow!

The Erzberg is a standard gauge line some 20km long of which around 14.6km between Vordernberg and Eisenerz is equipped with Abt rack system because of the steep gradients. This difficult line, opened in 1890, had five tunnels and eight stone viaducts, with an average gradient of 6% (1 in 16.6) and a maximum of 7.1% (1 in 14.1). It was built to carry good quality iron ore from a rich open cast mine.

Above  
**The class 97<sup>2</sup> 0-6-2 tanks were the main motive power on the line. 97.203 has modified water tanks and a Giesl ejector chimney.**

Below  
**Working hard banking a train out of Vordernberg.**

The summit at Präbichl, about halfway, is at 1,238m above sea level while the pit at Eisenerz is at 692m and Vordernberg is at 768m, with the main workshop and facilities. Typically, one loco pulled the consist of twenty hopper wagons while another pushed. A train of empties left Vordernberg with a loco at each end, then both locos were put together at Präbichl, in order to go down through the longest tunnels and allow the crews to breathe! At Eisenerz, all wagons are loaded and, after turning the locos on the local turntable, the first half of the train was pulled and pushed up to Präbichl, then both locos go back to Eisenerz to get the rest of the consist.

There was also a passenger service, originally coaches propelled by a steam loco and later using a diesel railcar.

Three series of rack-equipped steam locomotives were built by Wiener Locomotivfabrik AG of Floridsdorf specially for this railway (final ÖBB numbering):

Rh97<sup>2</sup> – 0-6-2T of 62 tons, built from 1890 to 1908 in three batches, a total of 18;

Rh197 – 0-12-0T of 87 tons, three built in 1912;

Rh297 – 2-12-2T of 125 tons, two built in 1941: a very complicated design, made to help the German war effort as the Reich was desperately in need of iron ore. They were not very efficient, and were withdrawn after less than twenty years, suffering lots of mechanical failures. 297.402 was cannibalised to keep the other loco working.

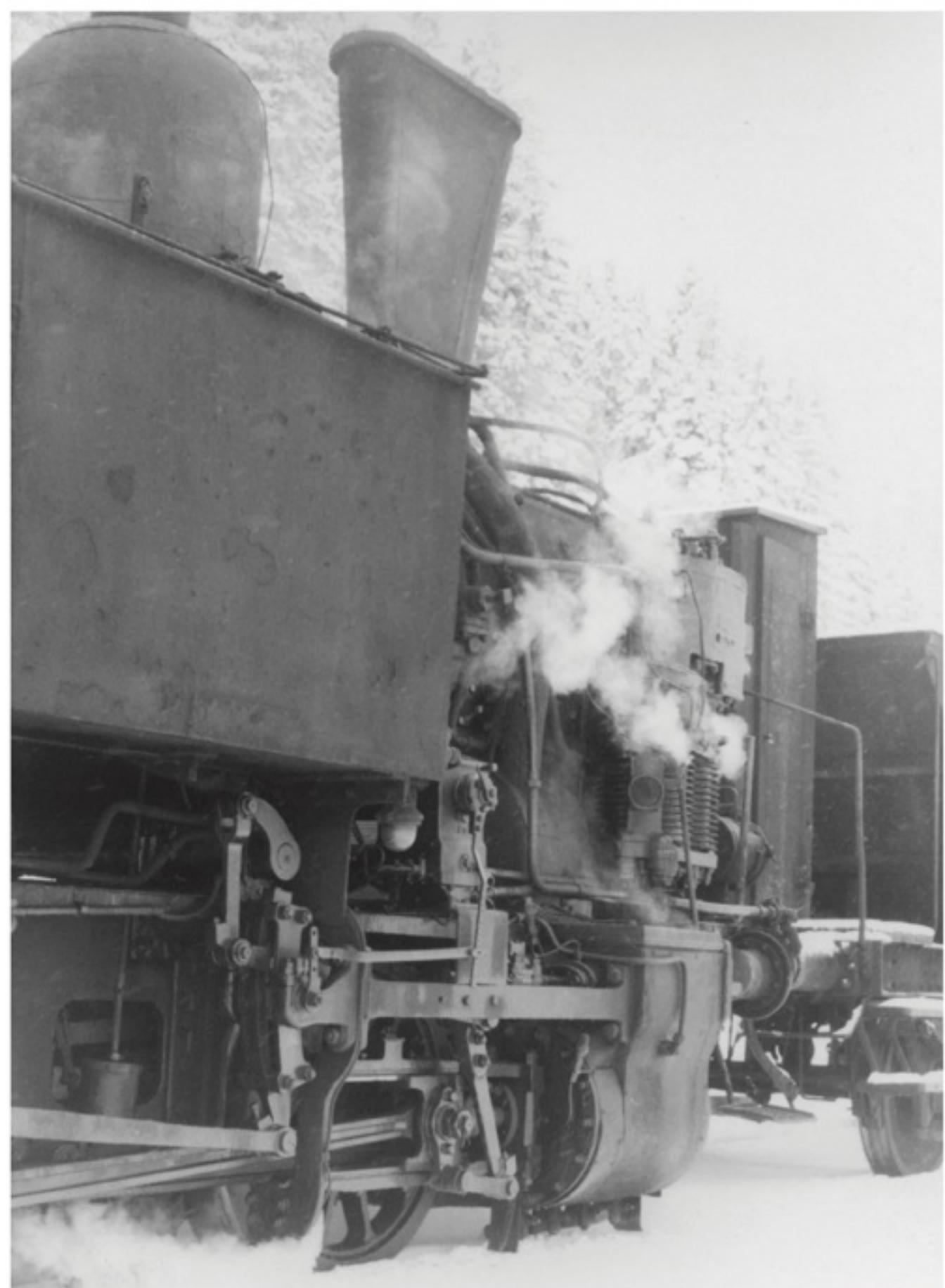
Some of the 97s were later equipped with a Giesl ejector chimney; the 197s and 297s were built with it.

The 97s were fitted with an extension to the water tank in early 1941, which can be seen in some photos.

In 1971, there were still thirteen 97s and all three 197s in working order. The final year of steam operations was 1978.

Several of the steam locos were preserved and subsequently plinthed or gone to museums: three 97s, one 197, and one 297. On top of these, two 97s are in working order on preserved lines.





Above  
0-6-2T 97.203 pauses at Präbichl having banked a train of empties up from Vordernberg.

Right  
Shunting at Präbichl, preparing to proceed to Eisenerz.

Below right  
In front of the Präbichl tunnel entrance, ready to depart.

Below  
The banking loco hard at work pushing a train of empties.





Above left  
Giesl-fitted 97.217 approaches the coal stage at Vordernberg.

Above  
Snowplough 985.223 alongside a Rh97 at Vordernberg depot.

Left  
97.201 and 97.205 (both still with original chimneys) in the loco servicing area at Vordernberg.

Below left  
97.205 at the coal hoist.

Below  
A separate crane was used to coal 97.217.



## Sources

*Die Zahnradbahn Eisenerz – Vordernberg,*  
Lokrundschau 1981.

*Eisenbahn Journal 1/85.*

*Von Vordernberg nach Eisenerz*  
– *Die Erzberg in Stereo.*

A very interesting record made in 1973. As well as the vinyl disk, there was a booklet with lots of photos, diagrams, and descriptions.

*Railway Routes of Austria – the Erzbergbahn*  
by Norman Lamb, Austrian Railway Group 2013.

After 1978 the rack was removed and Rh2043 B-B diesels were used, equipped with a speed-dependent magnetic track brake and a speed monitor with additional speedometer for the descent. Four machines were modified and reclassified 2043.5.

All traffic ended in 1988, and there is now a museum operation over part of the route. ([www.erzbergbahn.at](http://www.erzbergbahn.at))

### Postscript

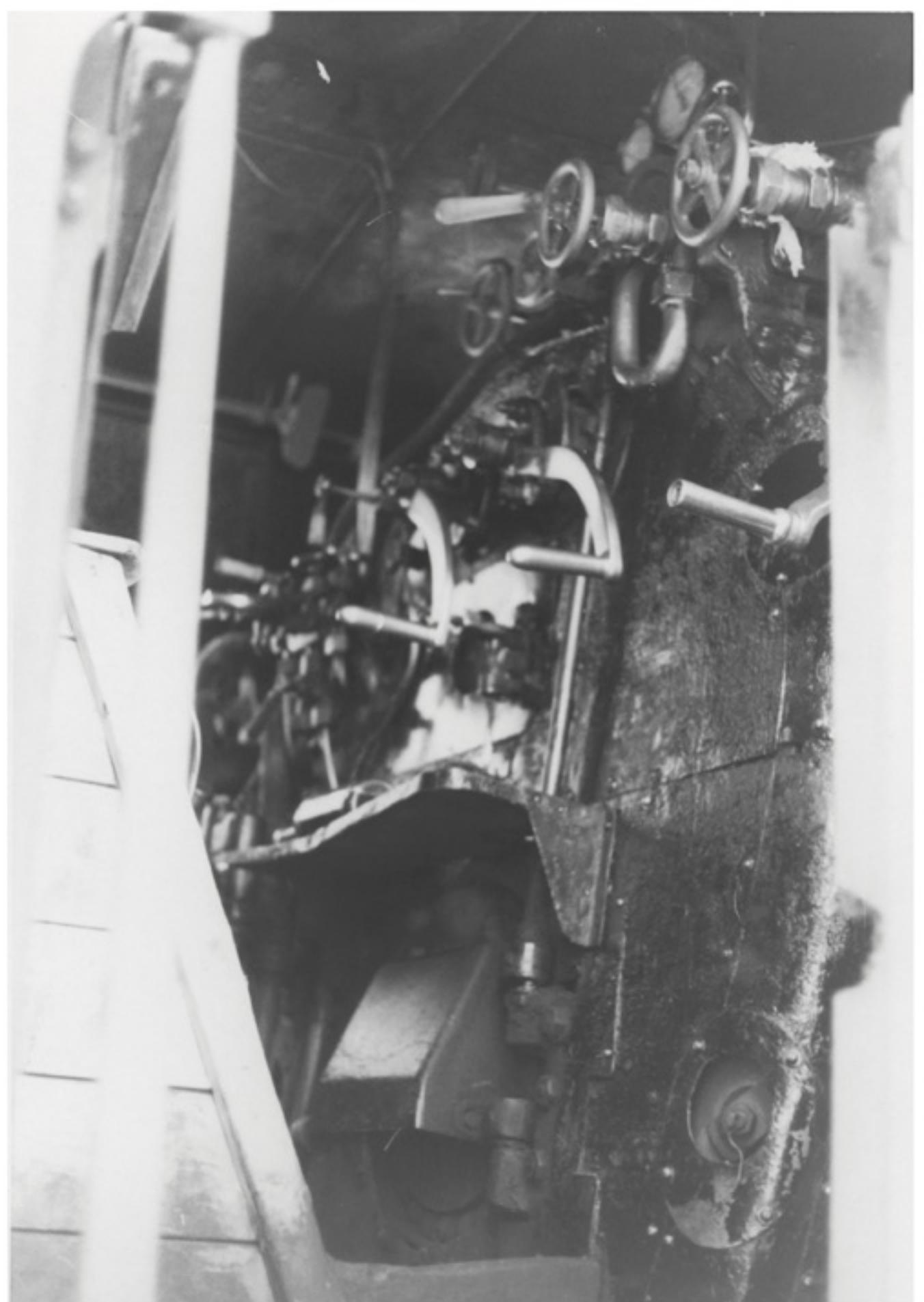
In 1978, I wanted to go back, in January again, after I read about the possibility of having a cab ride on the leading engine (after signing a waiver of liability, because of the total of 2.53km in tunnel). Most unfortunately, the regular use of steam finished the very week I was due to be there! The end of a dream ...

Instead I went not far away, to Garsten, and discovered the narrow gauge steam-worked Steyrtalbahn. But that is another story!



Above

**97.201 by the coal hoist.**



Right

**The cab interior of 97.205.**  
Note the two separate  
regulator handles for the  
rack and adhesion cylinders.

Left

**The coal hoist.**  
**The odd snow shower made**  
**photography a challenge!**

Below

**The massive 2-12-2T 297.401**  
**plinthed Vordernberg.**



**Mike Garforth** downsizes:

N descends from the loft and replaces G in the garage.

*Photographs by the author, unless otherwise noted.*



## Twin Forks

The latest instalment in the Seath Valley Railroad saga

**I**t is not every layout that begins with a knock on the head but this one certainly did. It occurred late in 2014 when I was working under my loft layout and popped up, forgetting that the layout was only 12" above my head!

At 73, I decided I was getting past the stage of grovelling beneath layouts and it was time for a change. Whilst I was still in my revolutionary frame of mind I made the almost instantaneous decision to take down the G scale layout in the garage and replace it with a new N gauge layout. That way I would be working at a track height of 58", and with sufficient forethought there would be no more knee bending, back aching, or head banging work!

To ensure my decision was final I spent the next couple of weeks in the loft packing the rolling stock and structures away, removing the control panels, and ripping up the track. There was no going back now!

My grandson had the privilege of running the last train on the G layout in February and the next day I started to take that apart – I still had G in the garden to keep me happy outdoors so it was not a total abandonment of the larger scale. I did manage to sell the points and some track, and the structures went to a fellow G scale modeller from Essex – a long way from Cumbria.

Thus by the end of March I had a clear area on which to start planning the new layout. I cut sheets of card (acquired from between the rows of tins on pallets at Tesco) as I needed to replicate the 31' x 3' space the layout would occupy and the card produced the ideal solution. I used masking tape to represent the track and Peco templates for the turn-outs, thus allowing easy adjustments at minimum cost.

**Above**  
**The author looking through the 'peephole' hole for viewing the tracks at Exton. To the left of the handheld controller hung on the fascia is a similar hole, elongated, for viewing Dunns.**  
 Photo: Dave Garforth.

**Below**  
**A train of box cars hauled by an F3 A+B set departs from Twin Forks bound for Kane. I find naming the offstage destinations adds to the operational interest.**





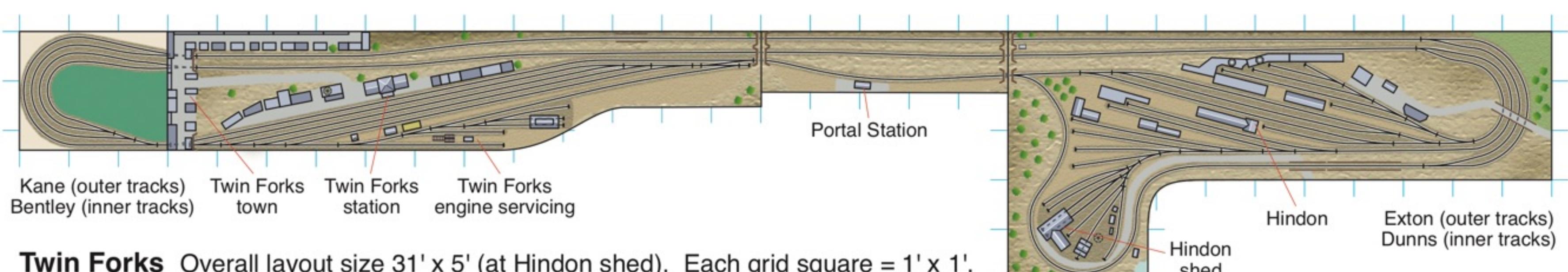
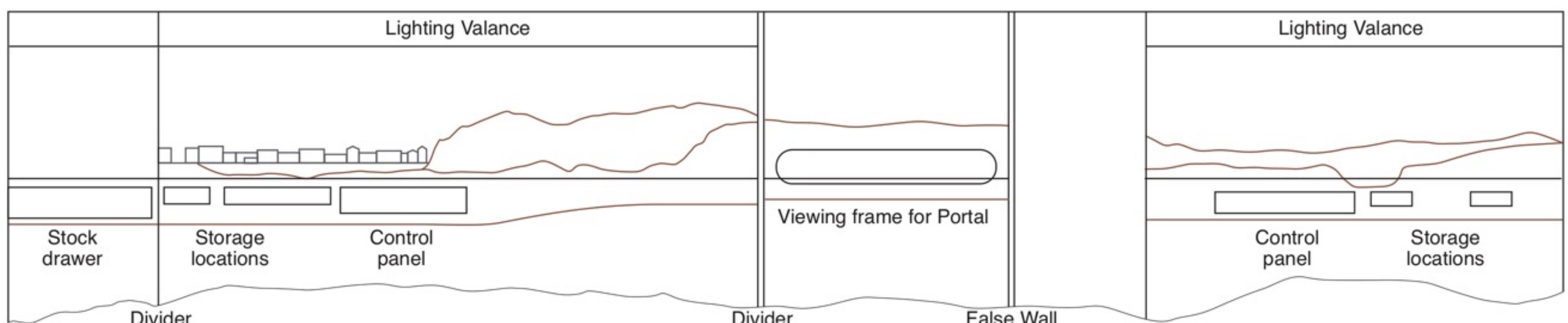
Above  
**The station at Twin Forks, from over the diesel shed, which can accommodate up to seven triple units. The tunnel is on the higher level track leading to Kane.**

My criteria for the new layout were:

- active storage for a minimum of eight trains;
- potential for meaningful switching;
- continue the theme, adopted for all my previous layouts, of a heritage railroad that permits use of locos and stock from a variety of North American railroads;
- the ability to run trains consisting of triple diesel units and a minimum of fifteen freight cars;
- incorporate the rock faces that had formed the backdrop to the old G layout;
- tracks running at different levels for additional visual and operational variety;
- suitable for a waybill system of operation.

After much head-scratching and adjustment to the initial plan, I arrived at the final track arrangement. There were to be four tracks at either end of the layout that could be used to store trains as well as provide continuous running. These tracks provide 'invisible' destinations (Kane, Bentley, Dunns, and Exton) and could easily accommodate trains of fifteen cars, as could the yard tracks at both Twin Forks and Hindon.

The engine servicing facilities at both Twin Forks and Hindon and the ability to switch cars at both destinations along with several trackside locations at Hindon ensured an ample source of activity. That is not to mention the simple fun of watching trains traverse the length of the layout whilst passing through a variety of scenic elements.



**Twin Forks** Overall layout size 31' x 5' (at Hindon shed). Each grid square = 1' x 1'.



The 31' length and internal layout of the garage meant this would not be a layout that could be operated from a sitting position, the 58" track height reinforcing that. There were many advantages to working within those limits: the trains could be viewed from track level, they disappear from view as they move from one area to another thus giving the added impression of 'going somewhere', with the need to follow the train as it traverses the layout – not only does the train move, so does the operator. The only real disadvantage is that anyone under 5'9" tall needs to stand on a stool to fully appreciate the layout, but I now find I often use the same stool to obtain yet another perspective. Weigh this against the normal back-bending position one has to adopt on the standard table-top layout height, and it is hardly a negative point.

All track is Peco code 80 with electrofrog points. All the points are operated by the proven Peco point motors mounted below track level, supplied through capacitor discharge units and activated by the tried and trusted stud and probe technique. The track is fully ballasted – time consuming, but well worth the effort for the added realism.

With over forty locos on the roster there was no way I was going to opt for conversion to DCC and so the operating system had to be analogue cab control. Each yard track and siding is isolated via simple toggle switches whilst the main running tracks are live at all times.

Control is via either Gaugemaster handheld throttles (both WS and W models) – referred to on the control panel as 'Local Control' or via the Total Train Technology (TTC) memory throttle, referred to as 'Memory Control'. This had been

Above  
**Looking south through the Twin Forks station area, with the upper tracks going through to Dunns and Exton behind and above the depot. The yard tracks each hold fourteen cars plus caboose.**

Below left  
**The north end of the layout, Kane and Bentley loops, with stock shelves above.**

Below  
**The stock drawer under the storage loops. Sliding trays allow two layers.**





Above

The edge of Hindon, with an operating system set-up sheet, Train Order card pockets, a system map, and a fast clock (one of two on the layout). There is also a small viewing hole through to the track climbing through the mountains to Exton. Photo: Dave Garforth.

used for the former G layout and operates perfectly with the smaller scale locos. The system permits you to start a train in one location, unplug the controller and walk to the next plug-in point whilst the train continues on its way until control is regained, an ideal solution, particularly for a layout that is effectively running through three areas.

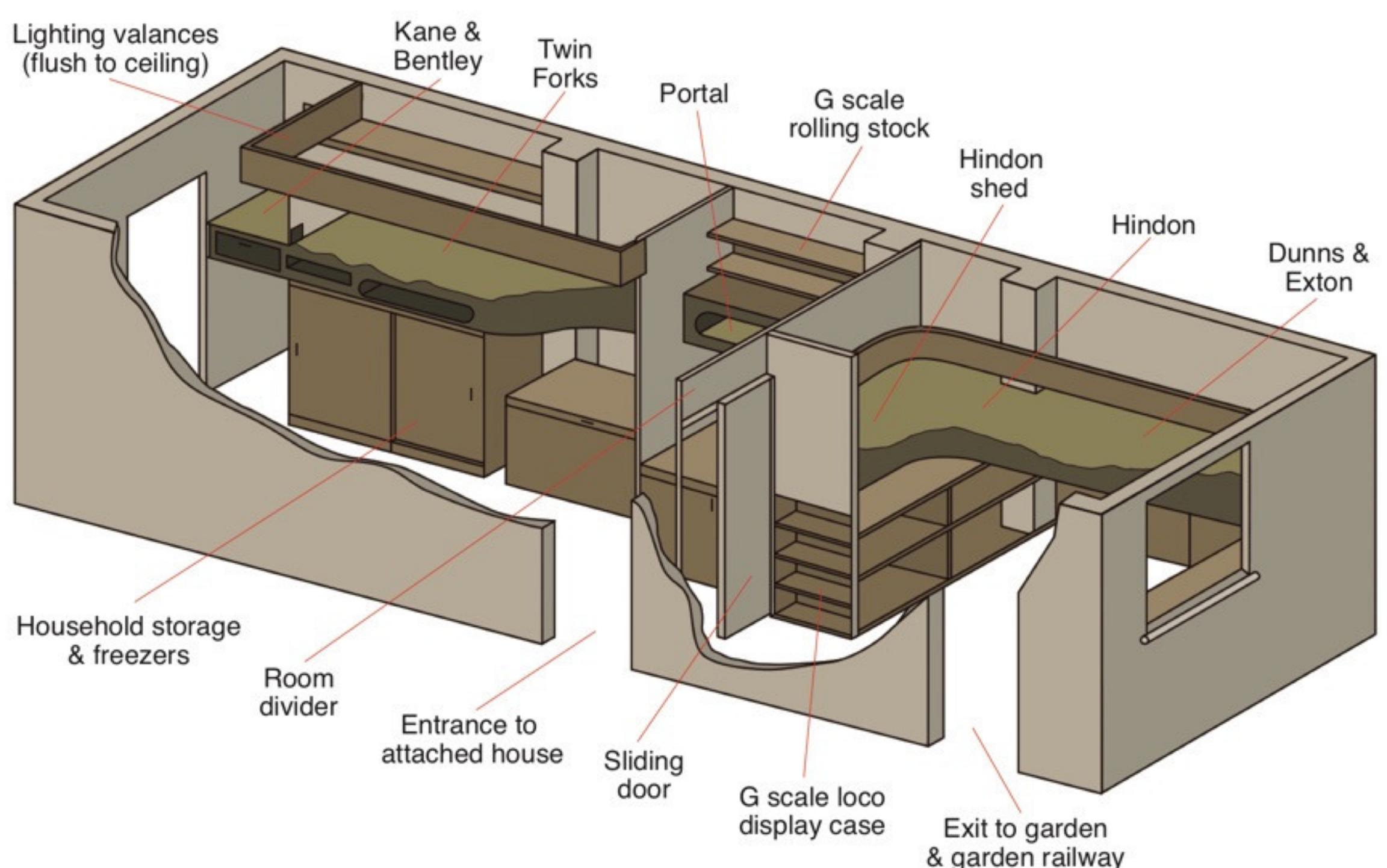
There are control panels at Twin Forks and Hindon, set into the fascia but capable of being hinged downward for maintenance, etc.

An additional feature is the provision of recessed shelves within the fascia for the temporary storage of stock, track cleaning items, or small tools, thus ensuring the layout itself does not become a workbench.

I did start producing holders for both the Gaugemaster and TTC controllers from styrene sheet to position on the fascia adjacent to the control panels but had a eureka moment when I thought of Velcro. It works a treat. With the hook section of the tape on the back of each controller and a double strip of the looped stuff at the desired locations it is a simple matter to hang the controller at the nearest point – I was initially dubious, but it works well.

The Hindon area can be operated via local control whilst trains on the outer upper level, travelling through Exton, are under the memory walk-around controller. Thus local switching can be undertaken in the lower Hindon area (shed, yards, and station) whilst trains traverse the higher level track.

The only other deviation from straightforward track isolation and control is within the shed area at Twin Forks where the two servicing tracks are split into two sections each allowing up to two triple F units to occupy each track – which makes for a busy shed area.



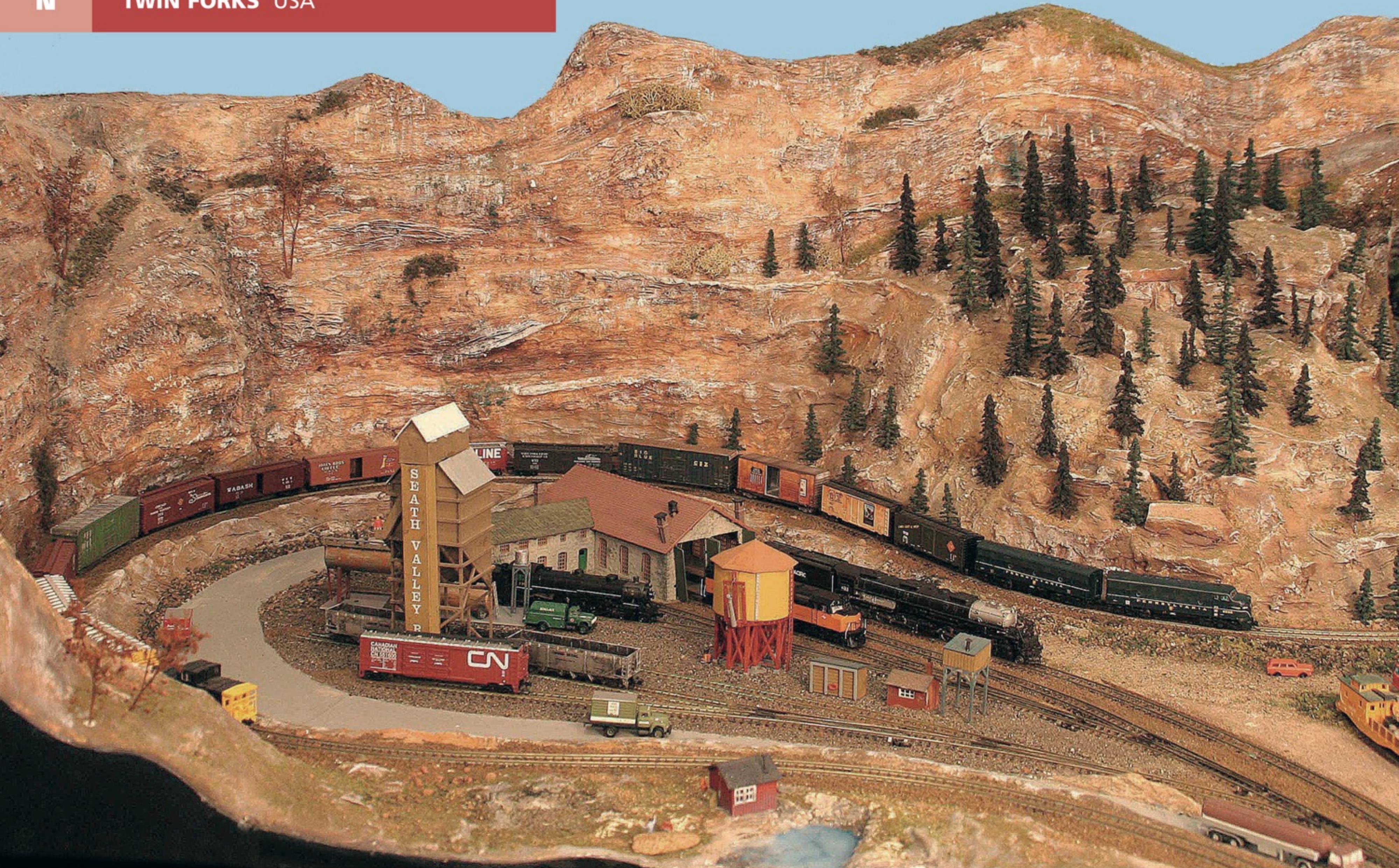
Above

The main diesel facility on the layout, at Twin Forks.

Below

2-6-0 #272 (a 30-year-old Arnold model, still running superbly!) arrives at Portal, between the two main areas. The scratchbuilt water tank and depot have served on three previous layouts.





Above

A maximum length freight (fourteen cars plus caboose) snakes around the back of the locomotive shed at Hindon, the principal steam facility on the layout.

Above right

The north end of Hindon, with the shed on the left and the three levels of track apparent on the right.

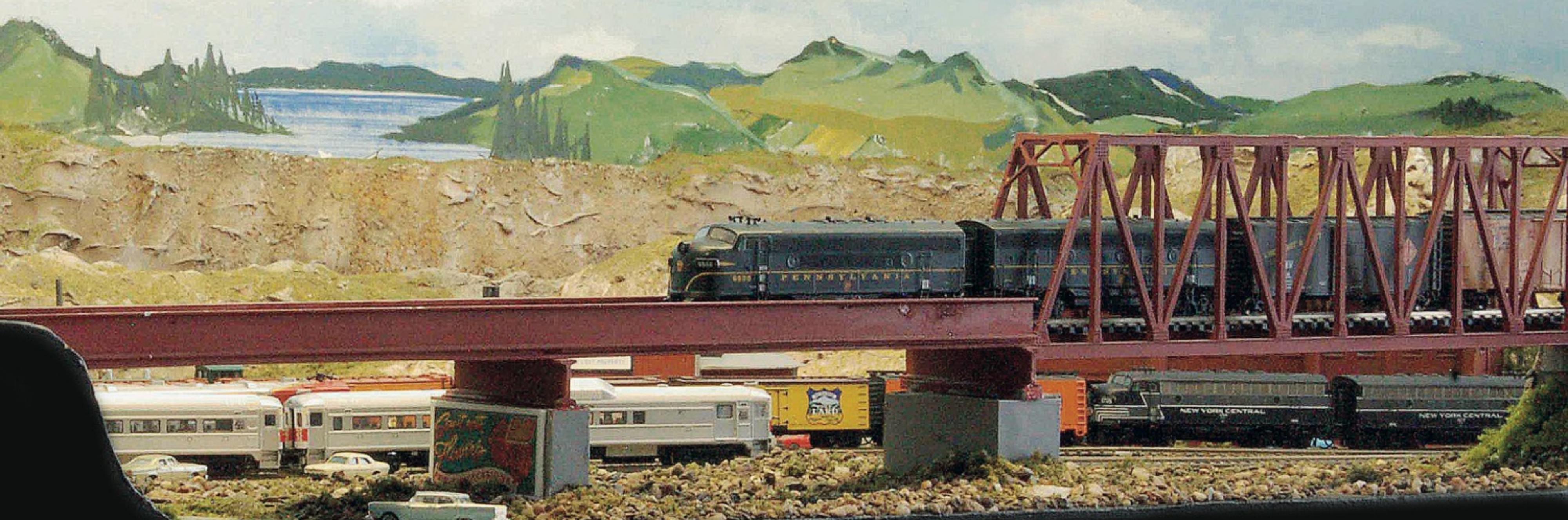
The double-headed steamers are bound for Exton while the diesel-hauled train is en route for Portal.

Below

The view of the depot at Hindon is carefully framed by the girder bridges which carry the track down from the high level loops at Exton.

Right

A higher viewpoint reveals the pair of Budd diesel railcars standing on the station spur as a set of F3s in New York Central livery passes on a train of reefers.







Scenery is based on styrene foam sheet, carved to shape and covered with Polyfilla with detail added using Sculptamold with the usual covering of flock material secured with diluted wood glue.

### Viewing

I do enjoy incorporating viewing options on any layout and *Twin Forks* is no exception. At Hindon the raised track along the front of the layout forces the viewer to look over and into the layout, whilst the bridges carrying the track over the road and dry river bed create a restricted view into the station area. There are four 'spy hole' viewing points along the layout. These consist of lengths of 1½" plastic tube inserted through the fascia at, or just above, track level so you can look down the track and gain an almost life-like track side view. Careful placing of the tube ensures the track end is blended into the scenery and is not visible from the front.

### Conclusion

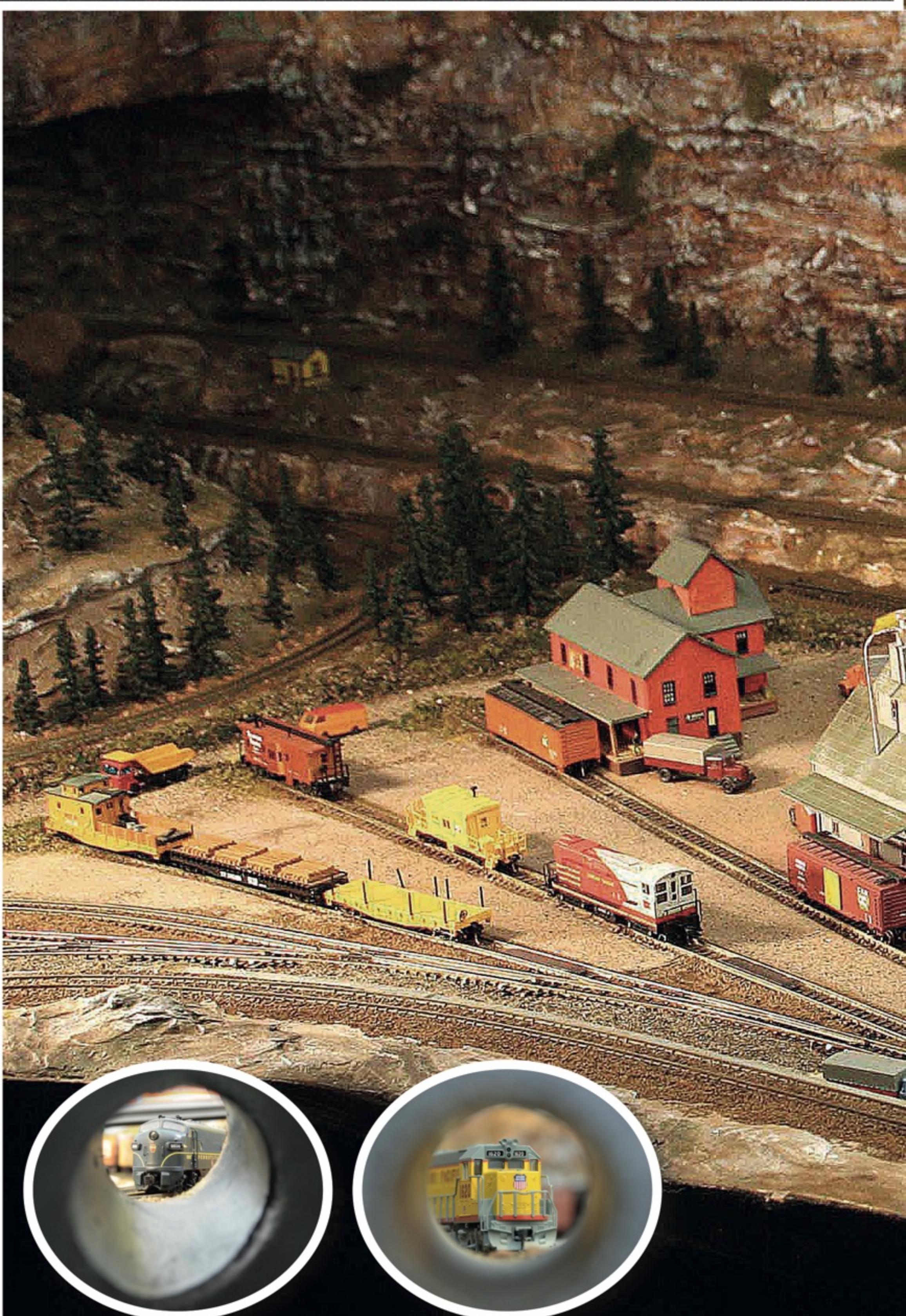
In a little under two years my indoor G scale switching layout was replaced by this N scale layout, using the original base-boards, offering long runs, switching opportunities – and no chance of banging my head on the underside of the layout!

Below right

**Viewing holes at Dunns (Pennsylvania #9506, E7A) and en route to Exton (Union Pacific ex-MP #1620, GP15-1).**  
Photos: Dave Garforth.

Below

**An assortment of steam locomotives on shed at Hindon.**



Left  
Bridges carrying the track dropping from Exton frame the Hindon depot area.

Right  
Hindon and the south end of the layout, with tracks in cuttings at different levels – these are the visible storage loops Dunn's and Exton.



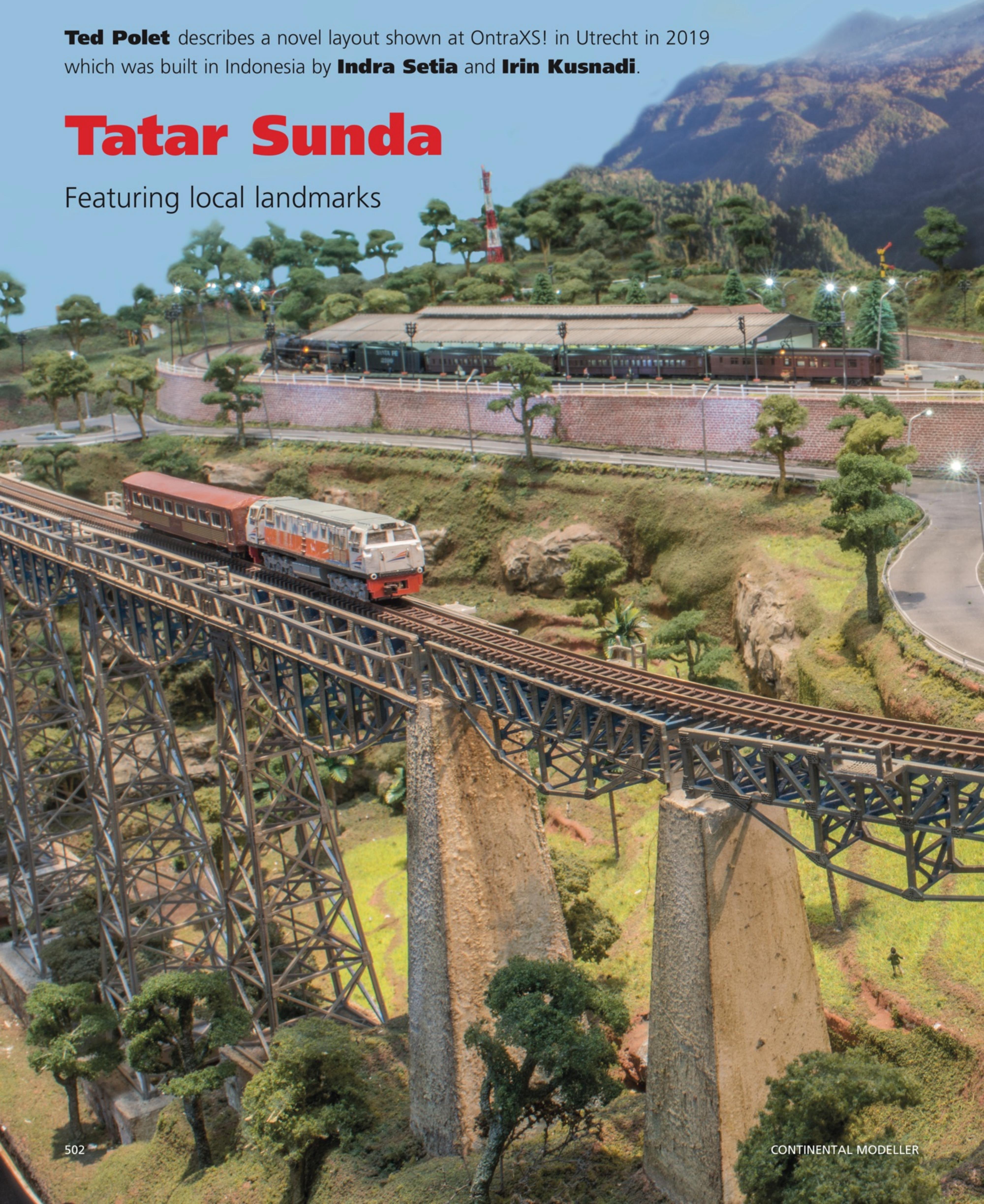
Below  
The north end of Hindon, with the freight house, industries, and depot.



**Ted Polet** describes a novel layout shown at OntraXS! in Utrecht in 2019 which was built in Indonesia by **Indra Setia** and **Irin Kusnadi**.

# Tatar Sunda

Featuring local landmarks





Below

A CC206 diesel with just one coach crosses the Cibisoro viaduct while a CC201 skirts the town. They will meet at Cimahi station where a large Mallet and some vintage coaches are waiting – so some slick shunting will be required to let the trains pass!



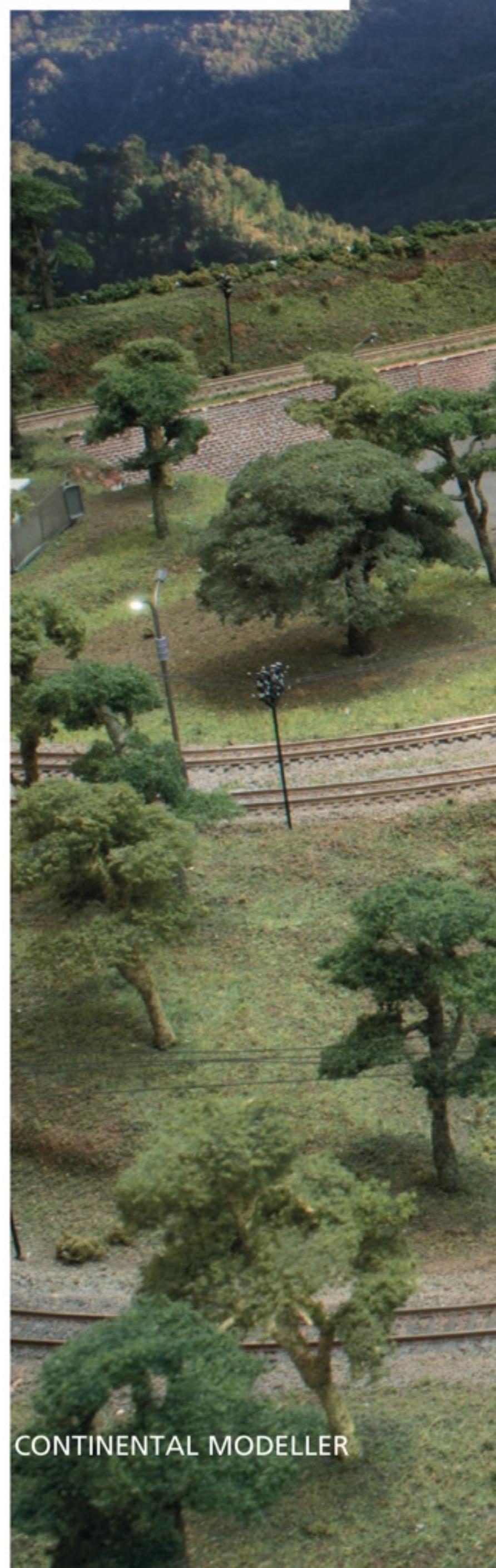
**T**atar Sunda literally means 'the land of Sunda'. Sunda is a region encompassing most of the western half of the Indonesian island of Java. The native language there is not the official language, Bahasa Indonesia, but a quite different regional language, as is the case with dozens of peoples within Indonesia.

*A note on place names: where the Indonesian language uses 'C', this is pronounced as 'CH' in English, and 'J' is pronounced as 'DJ'. Present-day place names are given with their old Dutch colonial equivalent in brackets. The Dutch used 'OE' instead of present-day 'U'.*

The city of Bandung is the capital of western Java. It lies in a temperate climate in the mountains and in Dutch colonial times was known as 'The Paris of Java'.

The theme of the layout was chosen because of the beautiful mountain railway crossing the central volcanic ridge from the north coast to Bandung. In the heyday of steam, this was the realm of Mallet locomotives.

Tatar Sunda is believed to be the first ever layout with a Sundanese theme, and took about eighteen months to build. 9mm gauge is used to represent 3'6" or 1,067mm gauge and the rolling stock is closer to 1:150 than 1:160.



Left

**This view reveals how the track is folded back on itself to give a continuous run. The train is hidden in the tunnels at either end and in the low section behind the station.**

Below left

**The layout as displayed. The controls are built in – one simple DC controller, with two pairs of buttons to work the only two points (each end of the station loop).**

Below

**The 'vintage' train waits in Cimahi station as a CC201 crosses the spindly steel trestle Cibisoro viaduct.**

The layout measures 210cm x 105cm and features an ingeniously folded continuous circuit with just one passing loop in Cimahi station.

In preparing the theme, Indra and Irin took great care in selecting focal elements of the prototype – bridges, viaducts, tunnels, a station building, and characteristic scenery, including the traditional dwellings still to be found in the mountains.

### **Cibisoro viaduct**

In the 19th century, the railway journey from Jakarta (Batavia) to Bandung (Bandoeng) was a long-drawn-out affair through Bogor (Buitenzorg), Sukabumi, Cianjur, Padalarang, Cimahi, and eventually Bandung. It took almost an entire day from end to end.

In order to shorten the travel time, the colonial State Railway (Staatsspoorwegen, SS) sought an alternative route through the central mountain ridge to connect Bandung and the coastal town of Cikampek, which already had a railway connection to Batavia. The line from Cikampek up to Purwakarta is almost flat, but from there to Padalarang it is a heavily-graded mountain railway with tall and often very long lattice girder viaducts, and many tunnels.

The prototype Cibisoro viaduct is 310m long and was built in 1903. The model is almost two metres in length, and the lattice parts were laser-cut from plastic.

Motive power on this line, up to the 1970s, was almost exclusively the domain of Mallet locomotives – huge 2-6-6-0s and 2-8-8-0s reminiscent of American practice.

### **Sasaksaat tunnel**

North of the Cibisoro viaduct is the 950m long Sasaksaat tunnel. This was cut through the mountains in 1902-1903, and it is still the longest railway tunnel in Indonesia. The ornate tunnel entrances were modelled just like the prototype.





Left

**The ornate masonry tunnel portals were modelled to be just like the prototype at Sasaksaat.**

Right

**Arches from Cikuda viaduct form the approach to the station, with traditional houses on the nearby hillside.**

### Cimahi station

The small garrison town of Cimahi (Tjimahi) used to be home to the Dutch colonial army, and at present is the base of the Special Forces of the Indonesian army.

Cimahi station is a central feature of the town and was a First Class station of the former State Railway, built in 1884. This fine station has changed little since colonial days and is still a stop for the main Jakarta–Bandung express service, a named train, the *Argo Parahyangan*. This is the successor of the *Vlugge Vier* express in colonial days, the name of which referred to the quick four-hour trip between Batavia and Bandoeng, as they were called then. Another long-distance express, the service to Malang (Eastern Java) also calls here, providing a through service from Jakarta to the east. In addition there is a stopping service between Cimahi and other stations in West Java.

### Cikuda viaduct

The prototype Cikuda viaduct is a concrete arch viaduct on the former branch line from Bandung through Rancaekek to Tanjungsari. This line was built to carry agricultural produce to Bandung, but regrettably was lifted by the Japanese occupiers during the Second World War. The Cikuda bridge is still a local landmark and is now a footpath for the locals to cross the valley near the village of Jatinangor, which is now the site of the campus of Pajajaran University, a well-known institution of West Java.

On the layout a few of the ornate concrete arches of Cikuda viaduct have been represented as an approach bridge to the model of Cimahi station.

### Traditional Sundanese dwellings

Citalang is a village about 3km from the centre of Purwakarta town. In Citalang, one of the characteristic traditional dwellings (known as 'rumah adat' in Indonesian) of this area has been preserved as cultural heritage. It was built in 1905 and once owned by the Chief of Citalang, Raden Mas Soemadiredja. This house has been preserved in its original state. The layout features three such dwellings on the hillside to the right of Cimahi station.

### Rolling stock

The mainstay of power on the line now is of General Electric origin. The most recent version is the CC206 class, a double-cab locomotive introduced in 2013. This evolved from the early CC200 class delivered as long ago as 1953. A second series, CC201, was delivered between 1977 and 1992 and has recently been upgraded. The third and fourth versions were the CC203 and CC204, both assembled in Indonesia by the PT INKA company of Madiun. The first CC203 was introduced in 1995 during the fiftieth anniversary of the republic. The CC206 was assembled from parts delivered by General Electric, with frames and bogies built by PT BARATA in Surabaya. Assembly took place in the Pengok workshops in Jogyakarta.



All of these classes use the same tried and tested eight-cylinder GE-7FDL-8 diesel engine, with only minor modifications.

A model CC206 runs on the layout, together with a train of luxury coaches used in tourist service on the West Java system. In addition, for the OnTraxs! 2019 exhibition, I converted an American diesel locomotive into a quick interpretation of the CC201 which ran with a three-coach suburban set made by Indra Setia. I also used an American Norfolk & Western Y6B Mallet and heavyweight passenger cars to commemorate the Mallet locomotives that used to work over this railway.

### Acknowledgements and postscript

Indra Setia is an Indonesian modeller running a small company in Bandung called 'Garagetoys'. He builds dioramas commercially, and *Tatar Sunda* must rank as his masterpiece. For specific modelling work he teamed up with Irin Kusnadi, another Indonesian modeller in the Bandung area, who built the original rolling stock and the exquisite model of Cimahi station. Further support in Indonesia was by several modelers in the Indonesian 87 scale group, FKM, and the Bandung Rail Model Community.

*Tatar Sunda* was flown to The Netherlands in 2018 thanks to funding arranged by Mr Agus of PT Kereta Api Indonesia (Indonesian Railways). It has been exhibited a few times – at the National Narrow Gauge Museum in Katwijk (known to many as Valkenburg), during an Indonesian festival in Rijswijk, and finally at the OnTraxs! 2019 exhibition. Local

support in The Netherlands was provided by Indonesian railway enthusiasts messrs Widoyoko, Gerard de Graaf, and Ted Polet on behalf the National Narrow Gauge Museum.

In May 2019 the layout was formally presented to the venerable Nederlandse Vereniging van Belangstellenden in het Spoor- en Tramwegwezen (NVBS) in the presence of the Cultural Attaché of the Embassy of the Republic of Indonesia in The Netherlands.

Below  
**Cimahi station is still a stopping place for the Jakarta–Bandung express, the *Argo Parahyangan*.**

Photographs by the editor.



**Jeff Ware** describes the layout he has been developing for over two years, which is still work in progress.  
*Photographs by the author.*

# Wolfenfels



## Background

Wolfenfels is an imaginary town somewhere in south-western Germany. It is not too far from the border with France and lies close to and immediately south of the Wolfenbach, a lesser-known tributary of one of the rivers that ultimately flow into the Rhine on its western side.

The town is dominated by the ruins of the Wolfenberg castle to its south-west, which formerly controlled passage across the river and through the pass immediately adjacent. A small walled town of *Fachwerk* houses developed to the east and north below the castle. The town inside the walls is mostly impassable to motor vehicles owing to the narrow lanes between the houses. The Baroness, the last of her line, now keeps and breeds horses in a large comfortable farm close to the Neustadt branch line north-east of the town, the family having abandoned the castle many years ago.

Above

The old town of Wolfenfels, with the ruined castle on the hill beyond. A streamlined 0110 brings a train out of the tunnel while a BR03 Pacific waits in the station with a rake of skirted coaches and a BR52 2-10-0 departs with a short goods.



In the early 19th century coal was discovered and was initially quarried in the cliff below the castle, but latterly a deep mine, the Königsgrube, and associated industry was developed and is still in operation. This resource attracted the railway in the late 1870s, and, as was common in those days, took little heed of the historic nature of the town and parts of the medieval fortifications were flattened to make way for the station and the junction of three railway lines that ultim-

ately connect into the network leading to the Ruhr. However, in a rare concession the station buildings were designed to match the medieval town.

The additional prosperity brought by the railway and coal mining resulted in the building of the Neustadt outside the walls with wide avenues and imposing buildings. Quarrying for stone was carried out beyond the Wolfenbach where an abandoned gravel plant lies. Also beyond the Wolfenbach there is a camping area.



There was destruction of a swathe of the new town during the Second World War with the gaps being filled later by typical 1960s blocks. Despite this the town retained its charm and at the period of the layout is rapidly developing as a tourist attraction.

Over time and particularly in the 1930s the layout of the station area was changed, with further changes taking place after 1945. This is evident in the odd arrangement of platforms and shadows where tracks were formerly laid, and a reduction from double to single track for the line passing through the area close to the coal mine. Proposals have been made to re-model and simplify the track layout in the station

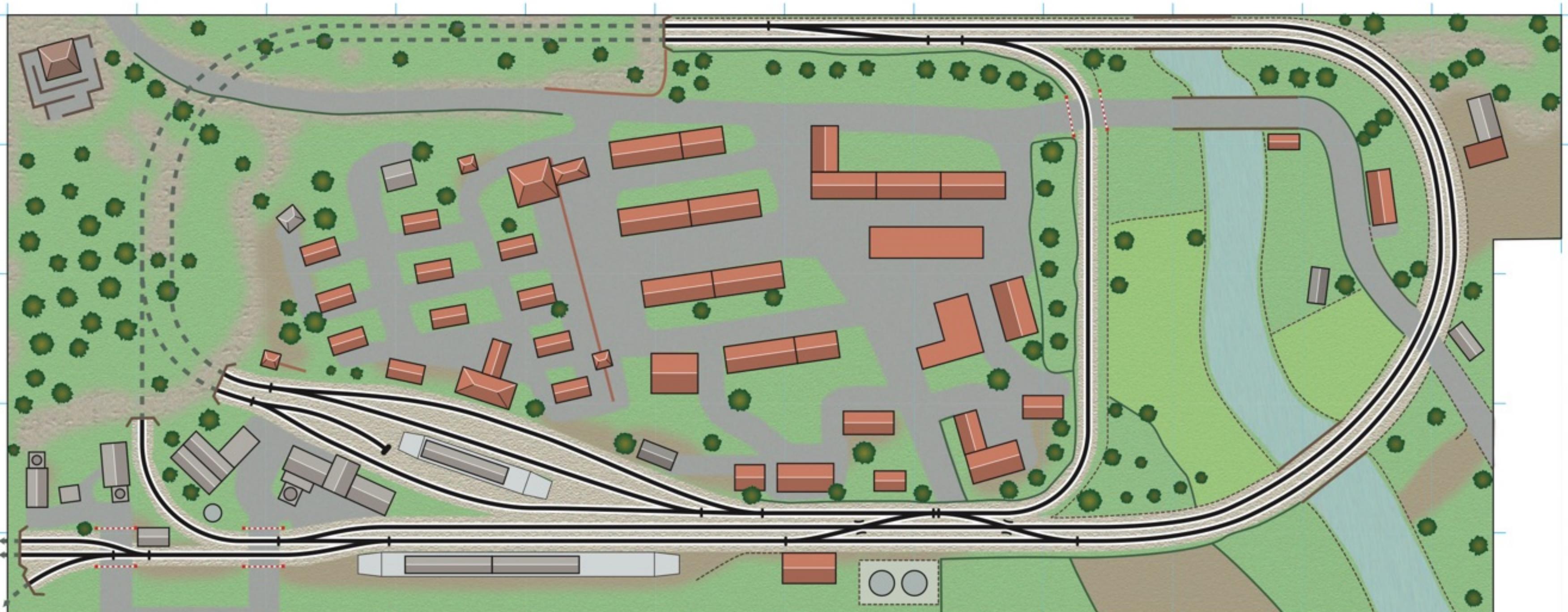
in order to reduce costs, but this is facing stiff opposition from the residents who argue that the increasing tourist traffic should be encouraged to use rail rather than road. For this reason the station area is becoming more dilapidated.

### Construction

The 10' x 4' layout is in the loft of my house. It is constructed on a scaffold framework and the base was made up of five 4' x 2' boards bolted together. This is to facilitate any possible future relocation as the loft hatch dimensions are a limiting factor. Rubber between the boards and the metal stops the transmission of vibration or noise into the house.

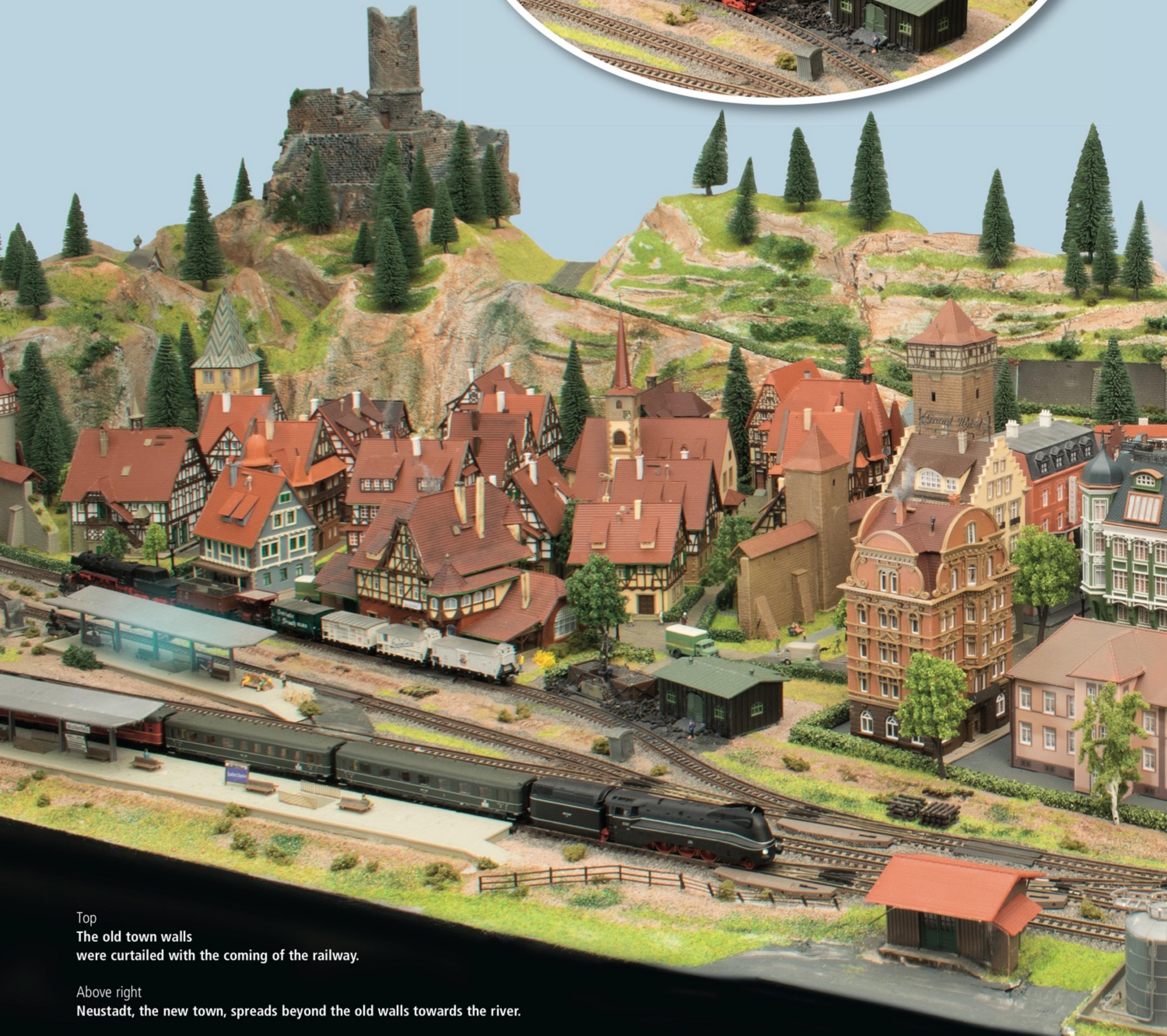
Above  
**A BR290 B-B diesel shunts the maintenance train into the industrial area.**

Right  
**The ruined castle still dominates the old town. A BR52 with tub tender is at the coaling stage.**



**Wolfenfels** Overall layout size approx. 3600mm x 1400mm. Each grid square = 300mm x 300mm.





Top  
The old town walls  
were curtailed with the coming of the railway.

Above right  
Neustadt, the new town, spreads beyond the old walls towards the river.

Above  
The old and new towns, with a BR52 waiting with a goods, a BR03 leaving the station, and an E10 electric arriving with an express.  
The period setting is not strict! And catenary is on the 'to do' list.



Most of the track is Fleischmann Piccolo, with some Kato and Peco elements. All points and point motors are by Fleischmann, equipped with TrainTech accessory decoders.

Control is digital via a Fleischmann/Roco Z21 DCC system, which controls the points as well as the locomotives. An iPad or iPhone is used to access the free app used to run the layout wirelessly.

Buildings are a mixture of kits by Vollmer, Kibri, and Faller. The castle is a Noch HO item that has been re-scaled by altering stair ways and other out of scale elements to make them visually acceptable.

Scenic elements are made from Modroc over a card or foam framework.

Most of the scenic scatters and trees are by Gaugemaster and Noch.

Locomotives and rolling stock are a mixture of Fleischmann and Minitrix, mostly Epoch III, but some are earlier and still operating at this period.



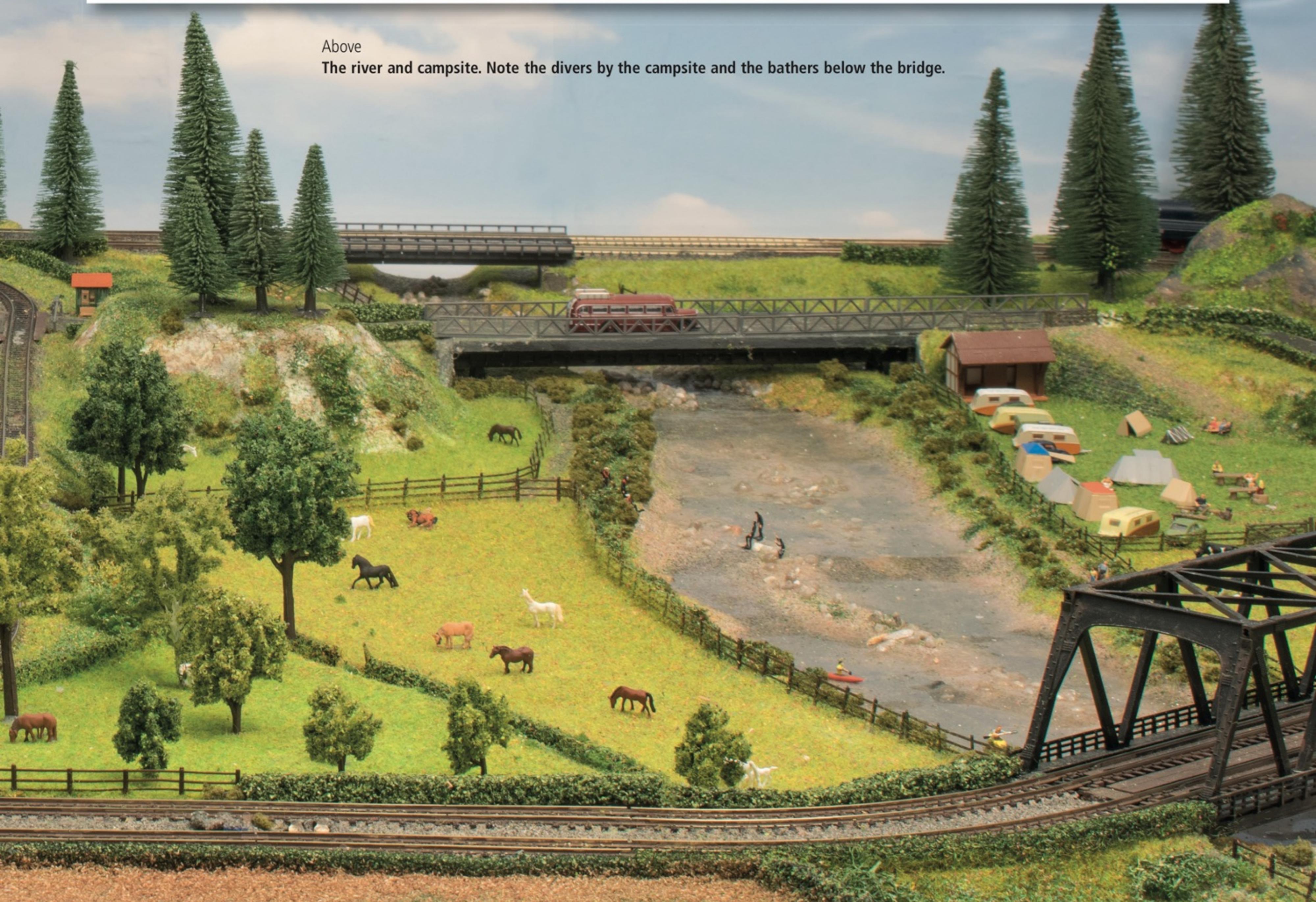
Above  
BR03 and train passing the disused stone quarry.

Below  
Neustadt, with the baroness' farm in the foreground.





Above  
The river and campsite. Note the divers by the campsite and the bathers below the bridge.



# Reviews

Evaluated by our own specialist staff



N

## DBAG ICE 4 new from Kato



ICE 4 is the name for the new long-distance InterCity Express high-speed trains being supplied to Deutsche Bahn.

Around 2008 the process began to find replacements for locomotive-hauled InterCity and EuroCity services, and later this was expanded to include replacements for the ICE 1 and ICE 2 high-speed trains. In 2011 Siemens was awarded a contract for 130 seven-car inter-city train replacements and 90 ten-car ICE train replacements, plus further options.

The contract for the ten-car sets was modified in 2013 to expand the length to twelve vehicles.

Developed under the title ICx, the trains were branded ICE 4 at the unveiling of the first set in 2015 and given the class designation 412 by Deutsche Bahn.

The ICE 4 is based on the power car concept: a 1.65MW power car combines an entire traction system in one vehicle, including the transformer, traction con-

verter, traction cooling unit, and four traction motors. Configurations from five to fourteen cars are possible, with certain conditions, using combinations of powered and trailer cars. This means that the train can be adapted to specific transport tasks in terms of acceleration, speed, and passenger capacity. Even double traction is feasible, with two seven-car sets forming a 400m long train.

The two main variants initially envisaged were a seven-car 499-seat train with three powered cars and a top speed of 230km/h to replace loco-hauled trains, while the second was a ten-car 724-seat variant with five powered cars and a top speed of 250km/h to replace ICE 1 and ICE 2 trains – the replacements were expected to be 20 tonnes lighter, resulting in claimed energy savings of 30%. Each set was to feature a restaurant/bistro, cycle storage, provision for wheelchairs, and a family area.

Two pre-production sets had been built by 2016 and were used for testing prior to the planned introduction of the main series in late 2017.

In 2018, DB ordered eighteen extra seven-car trains, bringing the total to thirty-seven, along with fifty more intermediate power cars to extend the twelve-car trains to a new thirteen-car composition, 374m long and accommodating up to 918 passengers. DB wants to have these certified for 265km/h instead of 250km/h.

By late 2018, around twenty ICE 4s were being operated on the Hamburg – München and Hamburg – Stuttgart routes. From December 2018, the ICE 4 was to be introduced on the Berlin – Erfurt – München and Köln – Frankfurt/Main routes. The first services to Switzerland were scheduled to commence in December 2019. Registration is planned for Austria, and approval for

France, Luxembourg, Italy, Poland, and the Czech Republic is an option.

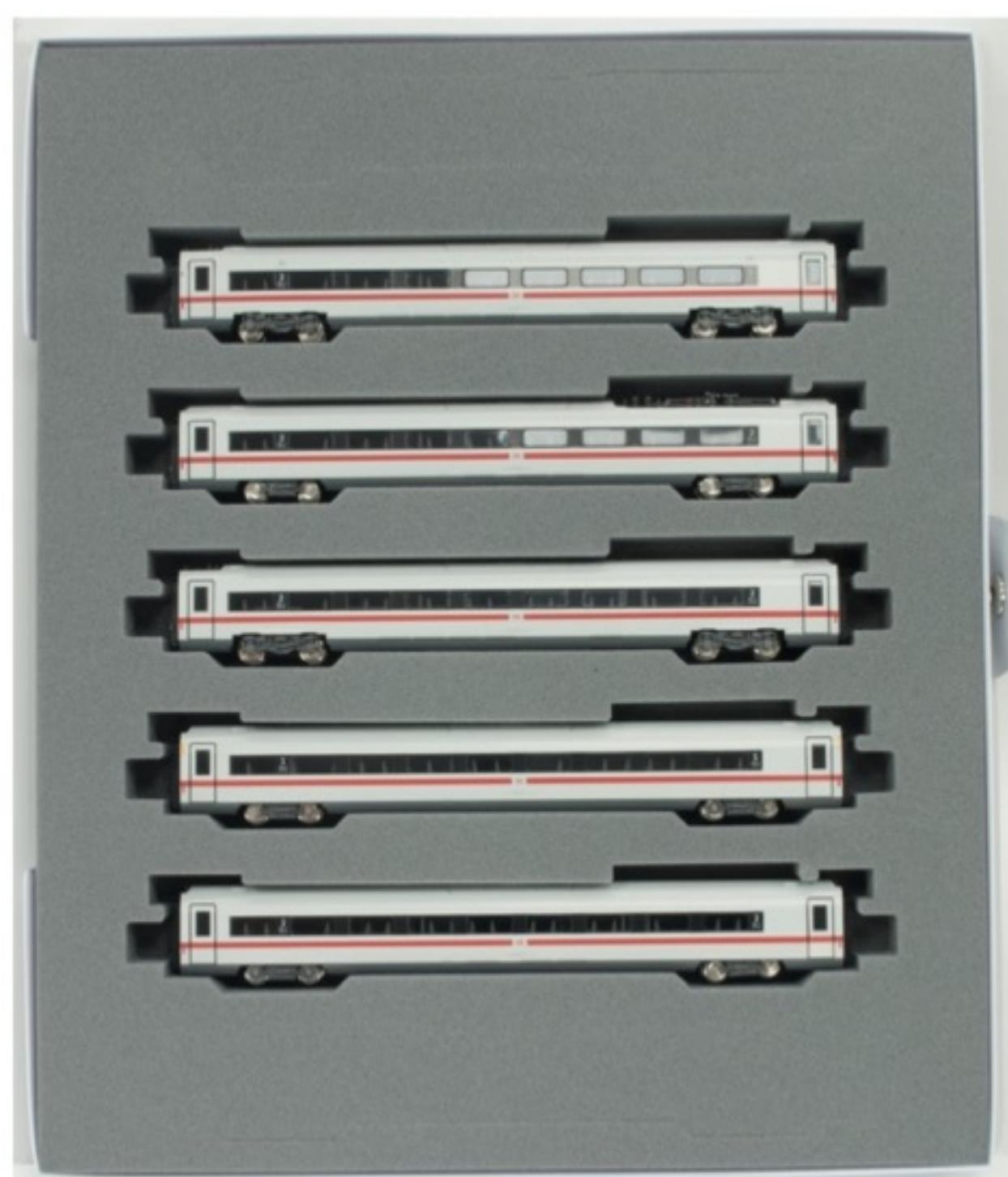
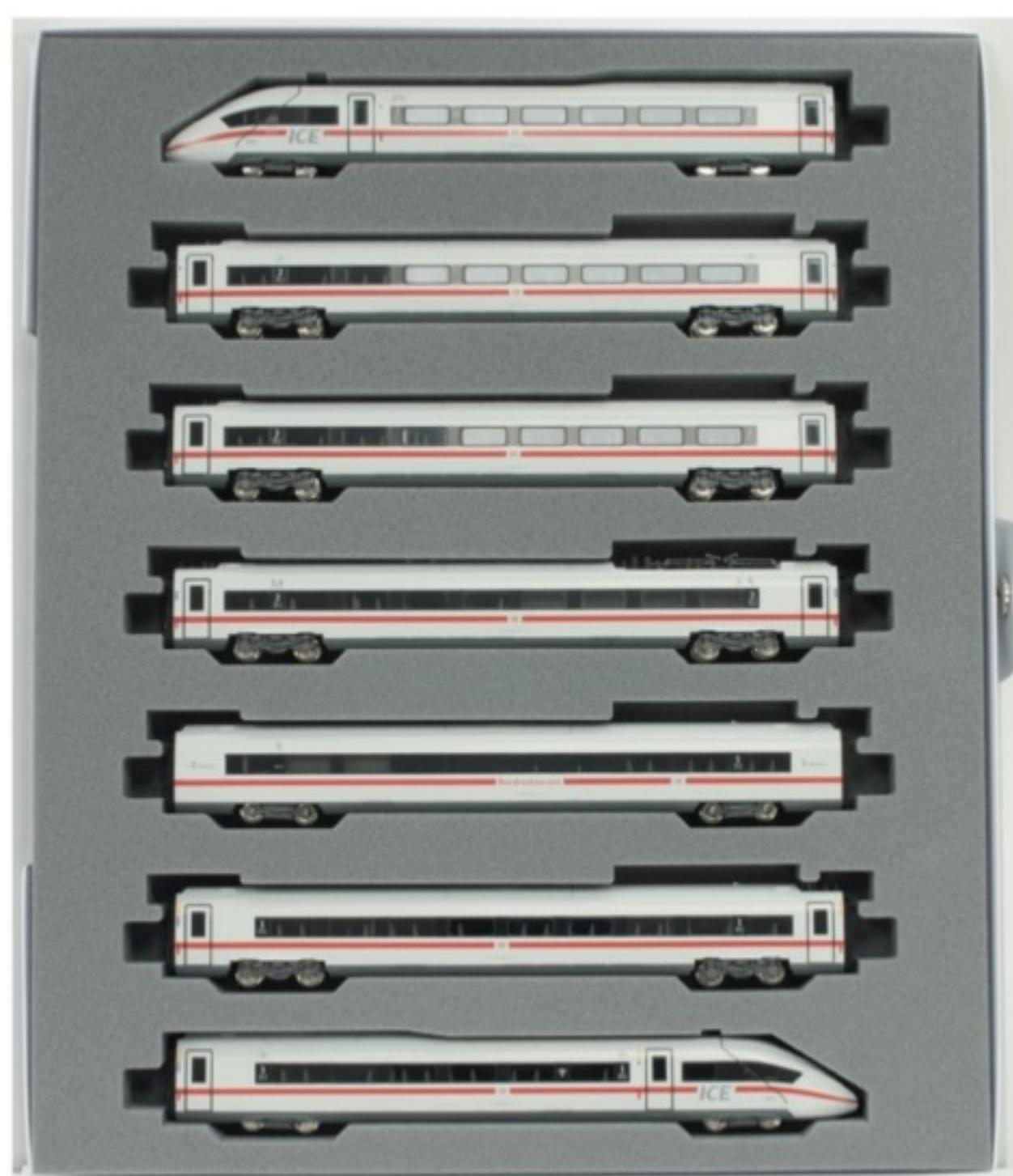
The ICE 4 orders have been changed several times. At September 2018 the situation was:

- 37 x 7-car sets, numbered from 9201, class 412.2;
- 50 x 12-car sets, numbered from 9001, class 412.0;
- 50 x 13-car sets, numbered from 9451.

The short set will be around 200m long and have three power cars. Maximum speed is 'only' 230km/h. It will have 456 second and 77 first class seats. The first units are due to enter service at the end of 2020 – their introduction has been pushed back considerably as a result of the modifications of the contract. Nevertheless, a first short set was being tested in February 2019 by DB Systemtechnik, though curiously only six cars.

The brand new model from Kato is an exact 1:160 scale replica, and comes as a seven-car base set (numbered 9010) with the option of a five-car extension pack; the instructions show how these can be combined in the overall context of a fourteen-car set – envisaged in the original specifications but (so far) not ordered. Both sets are supplied in a 'book' case with a slip cover, as often used for models of Japanese trains.

The seven-car set consists of what are noted as cars 1, 2, 5 (with motor), 9, 10, 11, and 14; the five-car extension provides cars 3, 4, 6 (with motor), 7, and 12. Note cars 8 and 13 are not modelled,





so the packs allow the modeller to represent the current seven and twelve car trains. Note also that the two powered models are together in the centre of the train.

#### Seven car pack (1,278mm long):

- end car, second class, with cycle stowage and wi-fi, 5812.0
- second class, quiet coach, 2412.8
- second class, 2412.3 (motorised model)
- second class, with pantographs, with family area and disabled facilities, 6412.0
- Bordrestaurant, second and first class, with central crew doors only and no end vestibules 8812.0
- first class, 1412.0
- end car, first class, quiet coach, 0812.0

#### Five car pack (910mm long):

- second class, quiet coach, 2412.5
- second class, with pantographs, 2812.0
- second class, 2412.0 (motorised model)
- second class, 2812.0
- first class, 1812.0

The '4' indicates a powered car, '8' a trailer. It will be noted that the model seven car pack has four powered cars whereas the prototype is only due to use three. The combined twelve cars have the correct balance (six powered).

No two vehicles are identical in function and hence markings even if the body shells are common. There are four body types: end; intermediate; intermediate with pantograph; restaurant.

The firsts and seconds use the same interior moulding.

The motorised unit differs only in the frame.

The cars ride on two different bogie designs, representing the powered and carrier types.

The finish is very good, with very neatly printed markings. Some of the body panels, doors, and grilles are printed, others are moulded in relief as separate parts, such as the air-conditioning units in the roof.

The body shell has flush glazing, and internal fittings despite the dark 'glass'.

The pantographs are made of fine plastic parts – they are not intended for current collection. They can be raised but are not sprung.

One end of each vehicle has a recess in the roof to accommodate the electrical equipment (added parts) – the actual links are not represented, for obvious practical reasons.

The corridor connections function as couplings; they are a bit fiddly to engage until you get the knack, but then are secure yet quite easy to separate (by lifting one vehicle).

Each pack includes one motorised car, powered by a five-pole motor with flywheel, driving both axles of both bogies. Current is collected from all wheels of the powered units. In the base set, there is a traction tyre on one inner wheel of each bogie, on opposite sides. The extension pack powered car has no traction tyres.

Performance is all you would expect of a Kato mechanism – quiet, smooth,



and controllable across the speed range from walking pace to something suitably fast!

The maximum current draw is quoted at 0.42A for the seven-car set and 0.3A for the five-car set.

The end cars have white and red directionally controlled head and tail lights, and all cars are prepared for LED interior lighting – the kit is available individually or as a pack of six. Axle-end pick-ups are already in place, as are the internal diffusers.

The model can also be converted to digital operation, with a choice of separate decoders for the motor(s), end lights, and interior lighting.

The sets are not through wired for either motor control or lighting.

As per prototype, sets can be joined: the nose caps are separate parts and can be removed, revealing a coupling pin for which a link bar (not supplied) would be required; the accessory part number is not specified, unlike most others.

The minimum radius is 282mm, and the maximum gradient 4%.



There is an excellent illustrated instruction leaflet, in German and English.

An impressive train which can only be fully appreciated in action on a layout.

#### Manufactured by

Kato (Sekisui Kinzoku Co.Ltd.),  
Tokyo, Japan.  
[www.katomodels.com](http://www.katomodels.com)

DISTRIBUTED IN THE UK BY  
Gaugemaster Controls,  
Gaugemaster House, Ford Road,  
Arundel, West Sussex, BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

PRICES  
7-car base set  
ref.K10950/10-1512A £310.00.  
5-car extension set  
ref.K10951/10-1513A £215.00.



**HO**

## new version of SNCF oil-fired 141R from Jouef



At the end of the Second World War, there was a shortage of motive power in France and to obtain the large number of locomotives needed quickly they were ordered from the main American and Canadian locomotive builders.

The first order – for seven hundred – was split between Baldwin, the American Locomotive Company, and Lima. The second batch (six hundred and forty) was ordered from those three plus the Montreal Locomotive Works and the Canadian Locomotive Company. Most of this batch were built as oil burners, using pre-heated heavy fuel oil (known in French as *mazout*), unlike the first batch which were coal fired.

141Rs were used all over the French railway network from 1945 to 1974.

Jouef have released several versions of their revised model of the 141R and the latest depicts oil burner 141R 1155 with a type 30R 13m<sup>3</sup> high capacity tender, built (as witnessed by the legible maker's plate) by Alco as works number 74950 in 1946 and based at Nice. All markings are neatly applied, and the green livery looks very smart.

This first impression is soon reinforced by closer inspection: this is a well-made and finely detailed model, with many separate added parts on the injection moulded plastic boiler and under the running plate.

These include footsteps, buffers, brake pump with shroud and other front deck fittings, headlights, feedwater heater, chimney, top feed, safety valves, whistle, steam manifold, pipework and handrails, cab doors, fall plate, injectors and pipework, power reverser, generator, air reservoir, and mechanical lubricators.

The motion is made up of blackened metal components and plastic parts, and looks consistent.

Added parts on the tender include the handbrake wheel, oil pipes, handrails, tank fillers, TIA water treatment unit, and the access ladder.



Accessories provided for the modeller to fit include a brake hose for the rear buffer beam if a working model coupler is not required, delightful metal three-link screw couplings, fire extinguisher in housing, and an etched metal *Mistral* headboard. (1155 is recorded as having hauled this prestigious train.)

Jouef have made provision for representing different prototype variants, and this machine correctly has a hinged smokebox door, Kylchap exhaust, smoke deflectors with ribs, and a Delta rear truck. It also has the generator on the running plate, not on the top of the boiler.

It is fitted with Bopok drivers throughout, and is coupled to a tender with a welded body.

If one were to be very critical, the lamps are slightly large, the handrail on the back of the tender is a bit long, and the incorrect front of the tender has not been modified – though, to be fair, this is not normally visible.

Loco and tender are permanently linked with a close-coupling bar.

Both loco and tender have metal frames, and the model is powered by a can motor with two flywheels in the boiler driving via a worm and gears hidden in the firebox onto the rear drivers, which are fitted with traction tyres. The other axles are driven by the coupling rods.



The wheels are to NEM profile with 0.8mm flanges.

The model runs quietly and smoothly from walking pace at c.2.5v to c.130km/h on full power (slightly above the 105km/h of the prototype).

Current is collected from the eight driving wheels and all tender wheels.

There are working directionally controlled headlights.

A 21-pin socket for a digital decoder is located in the tender, which also incorporates an enclosure for loudspeaker – the base is ready perforated.

The model is also offered with a digital sound decoder factory-fitted.

With a weight 380gr, pulling power should be ample for ten bogie coaches on the level.

The minimum recommended radius is 360mm.

The rear of the tender is equipped with a NEM pocket on a close-coupling mount, fitted with a standard coupler.

There is no similar provision for a working front coupler.

The illustrated instruction sheet is labelled in French and English.

In short, a very nice model.

### Manufactured for

Jouef (Hornby Hobbies Ltd.),  
Enterprise Road,  
Westwood Industrial Estate,  
Margate, Kent, CT9 4JX.  
[www.hornby.com](http://www.hornby.com)

PRICE ref.HJ2276 £261.99.  
ref.HJ2277 DCC sound £368.99.

**HO**

## Roco release revised models of SBB EWII stock

New from Roco are optically revised scale length models of Swiss standard type II coaches, with particular attention paid to the doors and vestibules.

They have been produced with early Epoch IV markings. The range includes a first, a first/second composite, and two seconds (naturally with different running numbers). Epoch V versions have also been announced.

The finish on our sample is superb, with sharply printed lettering. Note such fine details as the silver handles on the gold doors.

The model has interior fittings, and is flush glazed, with moulded representations of the droplight handles.

The toilet compartment window frames are neatly silvered.

Interior lighting is an optional extra; we presume the kit will have instructions how to open the body shell as this is not shown on the included leaflet.

The body and underframe are augmented with a number of separately applied plastic parts – vents standing clear of the ribbed roof, corridor connections and fall plates, brake gear and battery boxes. Other underframe features are formed in place, in medium relief.

The bogies have finely detailed side frames moulded in deep relief, with brake shoes in line with the wheel treads. The electrical generator and drive shaft is added at one end.

Parts provided for the purchaser to fit include the door handrails (yellow plastic), electrical connections (black), and delightful perforated footstuds (which attach to the bogie side frames rather than the body so as not to restrict bogie swing).



The models run freely on blackened metal wheels on pinpoint metal axles, insulated one side by a plastic centre bush.

As supplied they have standard couplers in NEM 362 pockets on close-coupling mounts 'steered' by the bogies, with Roco close couplers provided as alternatives.

Minimum recommended radius is not quoted.

### Manufactured by

Roco,  
A-5101 Bergheim, Austria.  
[www.roco.cc](http://www.roco.cc)

DISTRIBUTED BY  
Gaugemaster Controls,  
Gaugemaster House, Ford Road,  
Arundel, West Sussex, BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE ref.74562 £53.00.

**HOm**

## Harz bogie flat wagon new from Tillig

Brand new from Tillig is a type SSm bogie flat with side stakes from the Harz metre gauge network.

The DR Epoch III markings are clearly printed. The model represents number 99-04-73, with an overhaul date of 9/2/61. The wagon is 10.67m long over buffers, with bogie centres at 6.60m. Bogie wheelbase is 1.2m. It weighs 8.85t and is rated to carry 21t on a load space 9.1m long and 2.3m wide.

It is one of a series of bogie wagons built between 1900 and 1905 for the Nordhausen – Wernigerode Eisenbahn; specifically No.254, one of the final batch which may have been built as an open with brake platform but was soon converted to a flat with side stakes and end bulkheads. Under the DRG it became 10.12S4p, and got the current number in 1951.

As in the DR era freight was mostly carried over the metre gauge in standard gauge wagons on transporters, rendering the original wagons redundant, most were scrapped and only a handful survived to be preserved as museum stock; since 1998 this is one of them.

The moulded plastic body has good plank and strapping detail, with bolt



heads, on the ends, and a planked floor. The end balcony railing is formed in resilient material with good detail and handbrake pillar in place. The side stakes are fixed and cannot be removed.

Detail parts supplied for the modeller to fit include brake hoses, centre buffers, dummy scale couplings, and coupling hooks. These fit easily but firmly into holes in the buffer beam; a small amount of superglue is recommended to secure them.

Note that when fitted these parts may impede the working of the model coupler.

The underframe is nicely detailed, with main components and brake gear moulded in medium relief; the queen posts and truss rods are added resilient plastic parts.

The bogie sides have good axlebox and spring detail. The brake shoes are added, in line with the wheel treads, correctly only on the outer ends of the bogies.

The model rolls freely on wheels with plastic spoked centres and blackened metal tyres.

Regular narrow gauge couplers are fitted in sockets on the bogies; this

allows for alternatives, including a NEM pocket.

The model comes in very secure clear plastic 'bubble' packaging within a card tray box.

The model is also offered for HOe (9mm gauge); it is comparable in length to Sachsen stock though a little wider, as might be expected, but does not look completely out of place next to 750mm gauge vehicles.

### Manufactured by

Tillig Modelleisenbahnen GmbH,  
Promenade 1,  
D-01855 Sebnitz, Germany.  
[www.tillig.com](http://www.tillig.com)

DISTRIBUTED BY  
Golden Valley Hobbies,  
Unit 1, Pontrilas Business Park,  
Pontrilas, Herefordshire, HR2 0AZ.  
[www.goldenvalleyhobbies.com](http://www.goldenvalleyhobbies.com)

PRICE ref.15981 £39.91.



Eisenbahn Canada offer a steadily growing range of high quality hand-crafted model locomotives (and some rolling stock); the selection includes prototypes from all over the world – Europe, America, and Asia. The projects have been created by investing thousands of hours in research into historical sources all around the world.

We illustrate three European models which may be taken as typical of the variety and the quality of their products, and the range of the subjects selected.

The models are of all metal construction, and as can be seen are exquisitely finished. Performance is as good as the appearance thanks to the use of coreless motors and precision drive trains, cleverly concealed within the model so as not to compromise the accurate appearance. Current is collected from all wheels, and there are no traction tyres. There are working lights as long as the wiring does not spoil the appearance. They do not have decoder sockets.

Note that these are scale models and will require realistic curves – 1,600mm is recommended as a minimum.

A word about the packaging and presentation. The quality experience begins even before you see the model.

Within a heavy cloth drawstring bag there is a stout card box with a folding lid secured by concealed magnetic catches. Inside this is a firm foam liner with appropriate cut-outs, topped by an elegant velvet surface. This holds a thick card box with lift-off lid which has an inner foam insert. The model itself is wrapped in acid-free tissue paper and held by clear plastic film onto a wood or metal base strip with grooves for the 'rails'.

A pair of white cotton gloves is provided for handling the model.

Oddly for products of this high calibre there is no instruction sheet, and no technical information on the website.

#### Württemberg Class B 4-4-0

Built from 1865 by the Maschinenfabrik Esslingen, these locos had a Crampton boiler, inside Allan valve gear and, for the first time in Württemberg, a Prümann chimney. They were equipped with 2T6.5 tenders.

The model is powered by a Swiss Maxon motor, and uses gold plated parts to simulate the brasswork. Note the laser lettering on the wheel rims, and smokebox doors which can be opened to reveal authentic interior features.

The selection includes:

No.140 *Wien* in 'Russian Iron' livery with red-brown main frames and wheels (illustrated). 70 pieces worldwide. ref.HO-009/1. US\$1,650.00.

No.141 *Paris* in green livery with red-brown main frames and wheels. 25 pieces worldwide. ref.HO-009/3. US\$1,650.00.

No.142 *Strassbourg* in black livery with red-brown main frames and wheels. 30 pieces worldwide. ref.HO-009/2. US\$1,650.00.

No.143 *Coeln* in overall gold-plated finish. 10 pieces worldwide. ref.HO-009/5. Sold out.

No.143 *Coeln* 4-4-0 in black livery with red-brown main frames and wheels. ref.HO-009/6, 10 pieces, exclusive for the Lemke collection.

[www.lemkecollection.de](http://www.lemkecollection.de)

#### Paris-Orléans – Midi streamlined Pacific

The tremendous success of the P-O Pacifics transformed by André Chapelon between 1929 and 1932 encouraged the company to go even further and in 1934 modify another ten locos, from among numbers 3523 to 3542, which became 231-722 to 231-731 under the new numbering when the P-O and Midi came together in that same year. The valve gear was modified (with reinforced recoil springs) to allow the machines to reach 200km/h. Following the experi-

ence gained with 240-701, the capacity of the intake valves was increased by 22% and that of the exhaust from the low pressure cylinders by 30%, and the volume of the low pressure cylinders was increased by 10%.

In 1937 231-726 was streamlined and presented at an exhibition in Paris. The streamlining had little effect on improving performance and was ultimately more significant for the image in publicity material.

The model has a fully detailed and painted cab interior, and functional lamps and marker lights. It is driven by a Swiss motor through an 'invisible' gearbox. Loco and tender are packed separately.

Several versions were offered:

– in 1937 condition, in express passenger red livery with silver bands (illustrated). 30 pieces worldwide. ref.HO-011/2. US\$2,695.00.

– in 1937 blue livery. Only 10 pieces worldwide. ref.HO-011/3. US\$2,750.00.

– in 1934 condition in grey with silver bands (the standard livery for express locos at the time). 50 pieces worldwide. ref.HO-011/1. US\$2,695.00.

– SNCF 1942 condition as 231H in black livery. 15 pieces worldwide. ref.HO-011/4 US\$2,750.00.

#### 2D2 5500

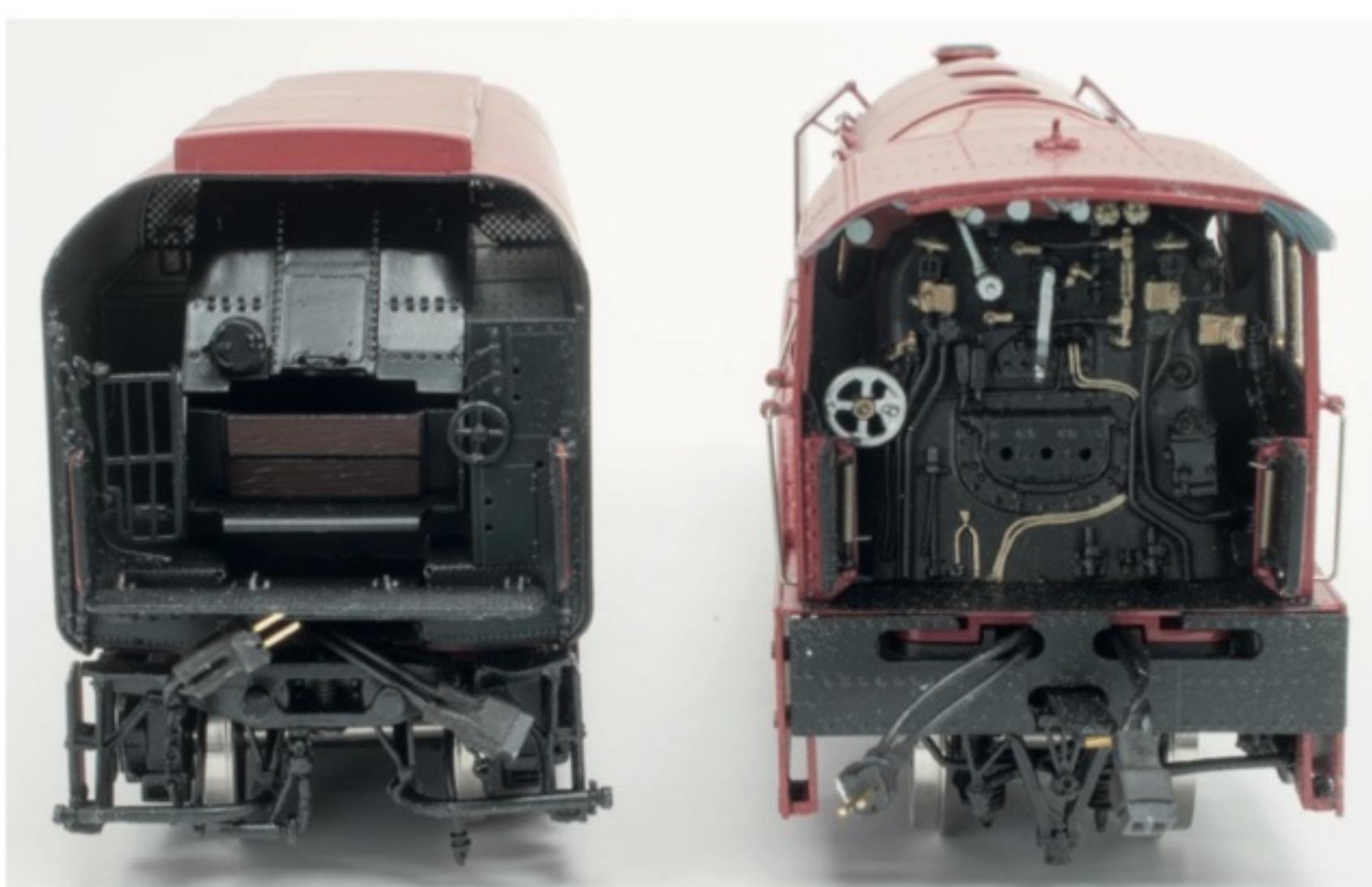
This series of electric locomotives with four driven axles and a carrying bogie at each end was originally built for the Paris-Orléans to a Brown Boveri design with Büchli transmission (one six-pole motor on each axle). They were in service from 1933 to 1980.



There were four sub-series:

Prototypes – 2D2 5501 and 5502 nicknamed "pig's nose" (later "grandmother") (P-O E 501 and 502, became SNCF 2D2 501 and 502).

First series – 2D2 5503 to 5537 "pig's nose" (P-O then P-O-Midi E 503 to 537, became SNCF 2D2 503 to 537) delivered between May 1933 and June 1934.



Second series – 2D2 5538 to 5545 "pregnant woman" (P-O-Midi E 538 to 545 became SNCF 2D2 538 to 545) delivered between December 1937 and July 1938.

Third series – 2D2 5546 to 5550 "Waterman" delivered to the SNCF between April 1942 and October 1943, with roller-bearing axleboxes from new.

The nicknames were coined due to the appearance of the fronts of the locomotives. "Waterman" refers to the shape of the bottle of ink for fountain pens.

Eisenbahn Canada opted to produce the *femme enceinte* (pregnant woman) sub-series in the following versions:

P-O – Midi 2D2 E 541 in original condition, in green livery (illustrated). 40 pieces worldwide. ref.HO-007/2.

P-O – Midi 2D2 E 543 in original condition in grey livery, a special edition, now sold out. ref.HO-007/4.

SNCF 2D2 5544 in green livery. 20 pieces worldwide. ref.HO-007/1.

SNCF 2D2 5545 in green livery. 20 pieces worldwide. ref.HO-007/3.

All versions are priced at US\$1,950.00.

The model features doors which can be opened, cabs and equipment room with interior details, and directionally controlled lighting. It is powered by a Maxon motor.

In short – miniature masterpieces.



#### Manufactured for

Eisenbahn Canada  
P.O.Box 88088,  
Lansdowne PO, Richmond,  
British Columbia V6X 2X0,  
Canada.  
[www.eisenbahncanada.com](http://www.eisenbahncanada.com)

PRICES in text.

## Loco-Revue Hors Serie 66 24 plans de réseaux

Edited by Denis Fournier Le Ray

Éditions Loco Revue,  
BP 30 104, F-56401 Auray Cedex, France.  
www.locorevue.com

297mm x 210mm 84pp  
Softback €15.00

ISSN 0024 5739 French text

This new special from the leading French model railway magazine *Loco-Revue*, dated 4/2019, offers twenty-four layout plans intended to help the beginner start small and simple, then develop the track plan and wiring, learning from experience.

It begins with outline reminders of some basic principles – types of layout, choice of scale and gauge, theme, baseboard construction, control system, and wiring. It then presents a series of projects making some use of commercial products, organised according to the level of difficulty in the carpentry, the track plan, the wiring, or the scenery, always recognising that the space available for our hobby is often limited. The smallest is 1m diameter, the largest is a 3.8m x 2.8m L shape.

The plans are grouped into three chapters: eight layouts to start, eleven to progress, and five to go further. Each occupying two or three pages, they cover a wide range of subjects. Each is built around a photo or two for inspiration or reference (prototype or model), an artist's impression, a full design, and a plan of the track components. Most are by Alexis Avril, with some from

François Fontana. Most, but by no means all, feature a French theme; most could be 'relocated' relatively easily. They are self-contained and not designed to be extended, though the first two of the final section are linked. Most are for HO, with two N and one O, but each could probably be adapted by changing the dimensions.

The presentation is in the usual clear style, straightforward (bar the affectation of titles in two fonts) and uncluttered, with large illustrations and relatively little text – so those with little French need not be discouraged!

There are tinted panels with summary information – scale, period, type, dimensions, situation, off-stage area, track used (brand and specific components), and often even suggested stock – plus pros and cons (on coloured circles). The accompanying text sets the scene and offers notes on the plan, the construction, and the scenery, plus suggestions for operation, in many cases the genesis of each idea. Reference is often made to other LR publications for further or more detailed information on specific topics or techniques.

Some idea of the time typically required is given in some cases, but of course this is very dependent on personal circumstances.

The majority of the photos are in colour, and all are well reproduced on good quality glossy paper. Each section is prefaced with a double-page spread.

Given that most of these plans are relatively small, the operational potential of some may seem rather limited but they are intended to be an encouraging start, achievable in terms of space, time, and resources.



They could be followed exactly by the novice to ensure success or simply used as the basis of something more elaborate, the source of an idea, by the more experienced modeller.

The book includes just two pages of advertisements.



## Model Railroading – the ultimate guide *Model Railroader* Special Issue 2019

Edited by David Popp

Kalmbach Media  
20127, Crossroads Circle, P.O.Box 1612,  
Waukesha, Wisconsin 53187-1612, U.S.A.  
www.ModelRailroader.com  
273mm x 210mm 100pp  
Softback US\$9.99  
ISSN 1086-5586

Making a bold and essentially unsustainable claim in its subtitle (unless they literally do not plan any more, which we doubt!) this is in effect 'the book of the film' – or rather, several films. Every year *Model Railroader* creates over a hundred and fifty features for ModelRailroaderVideoPlus.com, their website accessible by subscription. It carries how-to videos that cover all areas of modelling, at all skill levels, offering expert advice, great tips, and practical techniques to help build a layout. As well construction material generated by the in-house staff with project layouts and articles contributed by independent modellers, visits to layouts and prototype locations are also shown.

This publication provides printed versions derived from a selection of those features. As a taster, the related videos can be watched for free using access codes within the article.

There are fifteen sections, with subjects including repainting and detailing the diesel loco and passenger cars for a commuter train, scratchbuilding a sawmill and all its equipment, modelling a lake, making a wetland with tall grass, creating a forest by enhancing commercial tree kits, making road surfaces with printed paper, enhancing a plain area on a layout, pre-shading with an airbrush as a weathering technique, weathering a cement hopper car, upgrading a diesel sound decoder, the Copper Basin Railway as a prototype worth modelling (with suggested layout plan), and railfanning at certain 'hotspots' in the mid-west.

Most of the modelling described is HO, but there is also something on a 7½" gauge garden line – not something most modellers will be able to emulate!

The features range in length from two to ten pages.

Most are in the form of step-by-step photo sequences with explanatory text appended.

Presentation is in the familiar MR format, with information/summary panels (tools and materials needed, helpful tips, etc.) which for clarity have a border or frame, and sometimes tinted background. Sometimes the sub-headings are on coloured backgrounds, and occasionally the wording is unnecessarily "humorous".

In some of the features, the first few words of the text of each step are in bold type: the extent is inconsistent and we found it irritating. Likewise the titles are in two colours, for no good reason. Use of such effects can be helpful if it aids clarity, but here they rarely seem to achieve this.

The cover looks rather 'busy', with lots of type (in different sizes and colours) over an interesting composite image, with a model 'dropped in' to an old black & white photo.

Diagrams and plans are computer-generated artwork, to a high standard, and usually with the top view of some figures included to give some sense of the scale. Most of the photos are in colour, and all are clear and well reproduced.

The introduction has a number of hand-drawn caricatures or cartoons of the authors with short 'job', 'fave', and 'fact' "bullet points". Readers may judge for themselves whether this is an improvement over the personality photos which have been a feature of American magazines for some time.

Notwithstanding our reservations about the style of presentation, there is some excellent modelling here and some very useful information.

Although aimed at the American market, much of the general modelling will have wider appeal.

There are four-and-a-half pages of advertisements, some for other Kalmbach publications, some from relevant suppliers (Bachmann and Peco).

## **MIBA-Spezial 120** **Warten, pflegen,** **verbessern**

**Edited by Martin Knaden**

VGB – Verlagsgruppe Bahn GmbH, MIBA-Verlag,  
Am Fohlenhof 9a,  
D-82256 Fürstenfeldbruck, Germany.

www.miba.de  
295mm x 210mm 108pp  
Softback €12.00

ISSN 0938-1775

German text ref.12012019

The latest special from MIBA is concerned with the care and maintenance of the layout and its motive power and rolling stock, with repairs when necessary. In some cases the work amounts to an upgrade or improvement.

A layout, including stock and accessories represents a significant value, not just in the ideal sense for the owner, but also in absolute terms. This must be maintained, so long term care is important.

The layout becomes increasingly unsightly when it gets old and a layer of dust has covered the landscape. Here fundamental cleaning is needed. There is a feature on dust removal and consideration of the replacement of broken and missing parts as well as restoring faded colours.

It is also important to deal with the motive power and rolling stock, which suffers from wear and tear.

Topics include consideration of suitable tools and equipment (most with applications beyond care and maintenance, naturally); reviewing a silicon worktop mat; analysing a new LED tester; testing a digital decoder tester; methods of removing dust from the layout without causing damage; refreshing an old layout with new scenery; track cleaners; regular care of locos; more involved overhauls; installing a new printed circuit board with decoder in an old Fleischmann N gauge diesel; digitally operated couplings for N; fitting the handrails and other often separately supplied accessories; rolling stock maintenance; testing and comparing the spare parts services of several European suppliers – an appendix provides addresses, telephone numbers, e-mails, and websites; and stock storage boxes with foam interiors.

The features range in length from one to ten pages. They are by several different MIBA staff members and regular contributors with expert credentials, including Karl Gebele, Sebastian Koch, Horst Meier, and Bernd Schneider.

The work is illustrated by over 300 photographs, all in colour and very well reproduced on good quality paper. Many are in step-by-step sequences showing the processes in detail.

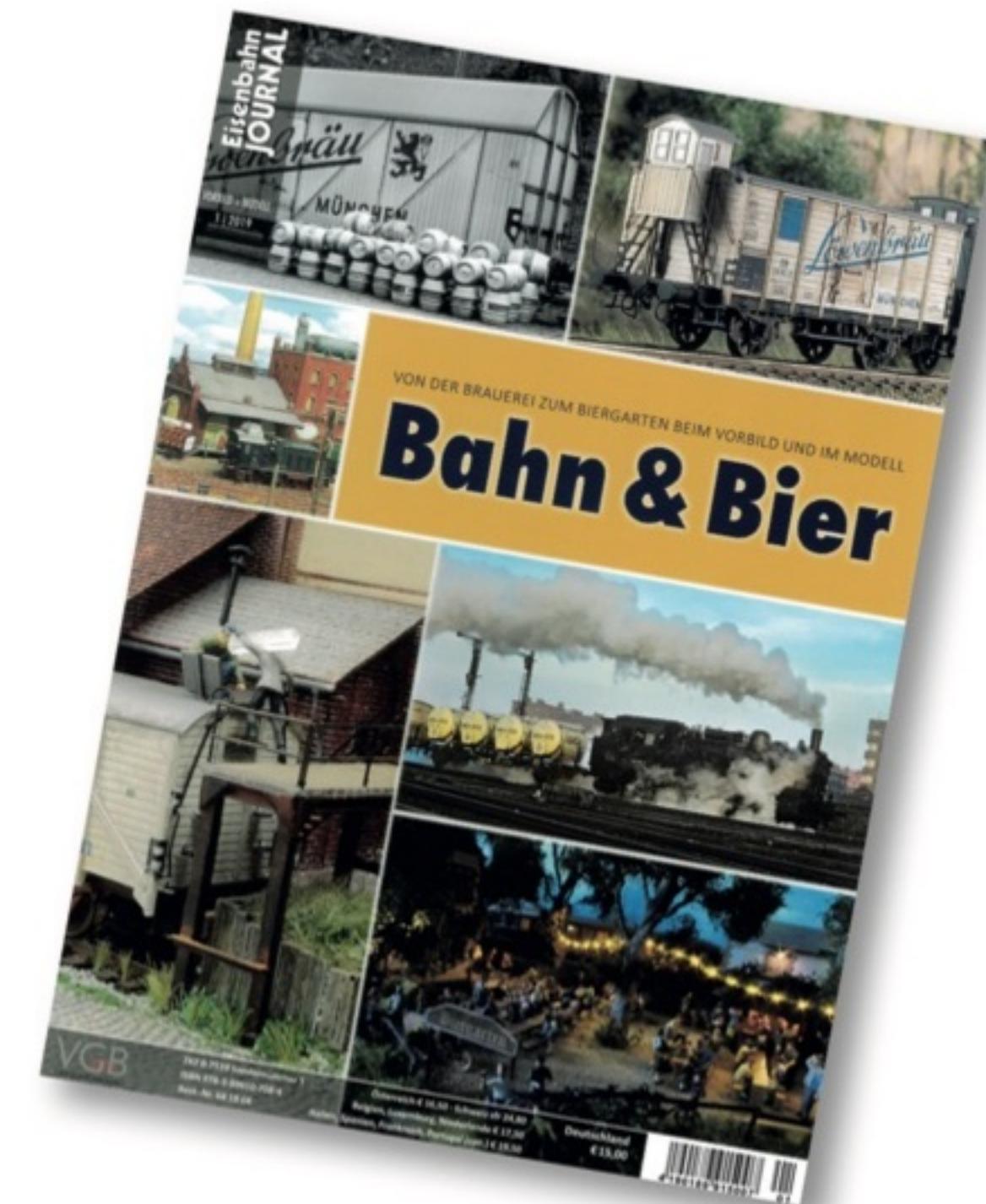
Presentation is in the usual clear, straightforward MIBA style with no extravagant use of fonts, different type sizes, or colours.

A reasonable ability in German will be required to get the full benefit from the text, but the high pictorial content makes the book worthwhile.



This is a useful reference for old hands, returnees, and newcomers alike.

The issue includes around ten pages of advertisements, mostly for other MIBA and VGB publications, but with some from relevant suppliers and retailers.



## **Eisenbahn Journal**

### **Vorbild & Modell 1/2019**

#### **Bahn & Bier**

#### **Von der Brauerei zum Biergarten** **beim Vorbild und im Modell**

VGB Verlagsgruppe Bahn GmbH,  
Am Fohlenhof 9a,

D-82556 Fürstenfeldbruck, Germany.

www.eisenbahn-journal.de

295mm x 210mm 98pp

Softback €15.00

ISBN 978-3-89610-708-4

German text ref.681804

Beer as rail freight? Why not! This new publication takes a trip back to the time when it was still commonplace to transport large and small containers full of beer by train. But not only the finished product reached consumers by rail – raw materials such as barley grain, hops, and yeast, plus barrels, empty bottles, and crates began their journey by train.

The first three chapters ('Bahn & Bier', 'Lass Dir raten: Fahre Sparen!', and 'Von der Brauerei zum Museum') look at the history of beer transport by train. There are particularly good illustrations of the many privately owned beer vans, a list of examples of beer wagons available in HO from manufacturers includ-

ing Brawa, Fleischmann, Liliput, Märklin, Piko, Roco, Sachsenmodelle, and Trix, and a suggested plan for a brewery on a layout.

The next six chapters ('Bierwagen-Umbau', 'Bunte Böhmen-Bierwagen', 'Veteranenpflege', 'Vier fette Fässer', 'Ladegut: Stangeneis', and 'Vorbild und Fantasie') deal with various aspects of modelling beer vans, showing how to detail and weather these vehicles and even recreating in model form the necessary addition of block ice for cooling!

The final five chapters ('Bierseligkeit', 'B(r)auerei nach Gusto', 'Die Brückenwalder Brauerei', 'Brauereianlage', and 'Idyll im Freien') cover the building of model breweries, the track layouts, associated buildings and structures, and how to model a beer garden.

A few pages are devoted to advertising MIBA and other VGB publications and DVDs. There is also a list of selected model railway shops in Germany and Switzerland. An appendix gives the names and addresses of specialist suppliers.

This is a handy, well-produced, and informative reference work which should appeal to anyone interested in recreating the transport of beer on a layout.

Whilst a good knowledge of the German language will be required to get the best from it, over 250 black & white and colour illustrations, many of which show step-by-step modelling processes, clearly show what can be achieved.

## **Le Train – spécial 97 1/2019**

### **Les Grand Trains**

#### **Tome 3**

**by Jean-Marc Dupuy**

Editions Publitrain eurl,  
CS80010, F-67660 Betschdorf, France.

www.letrain.com

297mm x 210mm 100pp

Softback €16.50

ISSN 1267-5008 French text

This new special from *Le Train* is the third to be concerned with prestige trains, and deals with the Orient Express, the Simplon Orient Express, the Arlberg Orient Express, and the current Venice Simplon Orient Express (VSOE), explaining the distinctions between them, often a source of confusion.

The story begins with trains of six-wheel stock in the 1880s, and relates how the Orient Express became symbolic of the Belle Époque. Subsequent sections deal

with the inter-war years, the period after the Second World War, and the slow decline of such services in the face of more cars, better roads, and the growth of air travel, to the current situation with a luxury hotel on wheels as an experience in itself as much as a stylish, elegant, and comfortable means of travel for those who can afford it.

Motive power depicted is mainly steam and then (in some places from quite early) electric, with only the occasional diesel. An appendix gives some typical train compositions by period, and some coach types are separately illustrated but there are no technical drawings or interior diagrams, and (perhaps understandably) no comprehensive lists of the stock used with principal dimensions.

The publication is built around an excellent collection of 172 photographs, the majority historic black & white images, reproduced as well as the originals will allow. Most are presented large, and there are several very impressive full pages and double page spreads featuring trains in the landscape, in a wide range of scenic

settings across France, Germany, Switzerland, Austria, Italy, Hungary, Greece, and Turkey, with appropriate motive power.

The older images have been drawn from archives and official sources, with the contemporary views supplied by many different photographers. The earliest colour is from July 1958, the most recent picture from August 2018.

As usual, there are informative captions, numbered, and mostly adjacent to the relevant image, or sometimes over a blank area, of sky, for example.

The photos are augmented with facsimiles of old documents, promotional posters, and artwork. There are schematic maps showing the routes, and the variations, at different periods.

There are seven pages of advertisements for other *Le Train* publications.

Some knowledge of French will help to get the full benefit of the text, but the captions should be clear enough and the publication can be appreciated for the illustrations.

## Corpet, Louvet et Cie

by Stefan Hooß

*Édition Shéhérazade Nayla Hooß*

Available from

Camden Miniature Steam Services, Barrow Farm,  
Rode, Frome, Somerset, BA11 6PS.

[www.camdenmin.co.uk](http://www.camdenmin.co.uk)

297mm x 210mm 512pp

Hardback £82.95

No ISBN. French text

This substantial new book aims to fill a lack of information about one of the most familiar yet not well documented names in French railway manufacturing.

Founded in Paris in 1853, Corpet, Louvet et Cie produced steam locomotives until 1951. The bulk of these were 0-6-0 tanks of various gauges for industry, and in metre gauge for many French minor railways. Locomotives for major railways were relatively rare, and usually designed elsewhere; one of their last products was arguably the apogee of French steam – de Caso's wonderful 232 U1 'La Divine'.

The author is at pains to point out this is not a complete account but more a convenient collection of available information and images; he accepts it is not perfect but hopes it will form a good basis for future study and discussion. He is too modest: it is a remarkable work of research and compilation, and should be much appreciated by historians and enthusiasts.



It begins with a summary of the Corpet family history, by a descendant. To give the context, there is a chronology of known facts; a section on the evidence of production in the contemporary press, mostly facsimiles of exhibition reports and technical documents; an illustrated list of selected steam locomotives in chronological order, if not complete then suitably representative, and the core of the work; a summary of production; illustrations of an experimental railcar and two different diesel locos; an account of the construction of SNCF 232 U1, a huge 4-6-4 quite different from the light railway machines; locos in use in various départements, 29 in all, mostly in old postcards, with map extracts showing the lines on which they worked; the Vaujours gypsum works; a reproduction of the 1900 catalogue, with diagrams; thirty promotional cards, each with a works photo and principal dimensions; contemporary accounts of the Brown valve gear, reproduced in facsimile (thus some are in English); and a final section featuring a few odd but interesting photos which simply did not fit anywhere else!

From this it can be seen that the main part of the work is essentially a photo album; a high proportion are from the company records, but they do not just show locos posed at the factory – often they are in use, and not just in France.

There are some views of the factory at various periods, with one section showing activity in various departments in c.1945.

Naturally most are in black & white, but there are some coloured scale drawings (eight) and some colour photos (twelve), principally but not exclusively of preserved examples – one dates from 1956, and is also used on the front cover. All are reproduced large, and as well as the originals will allow. The scale drawings show end as well as side elevations.

Left

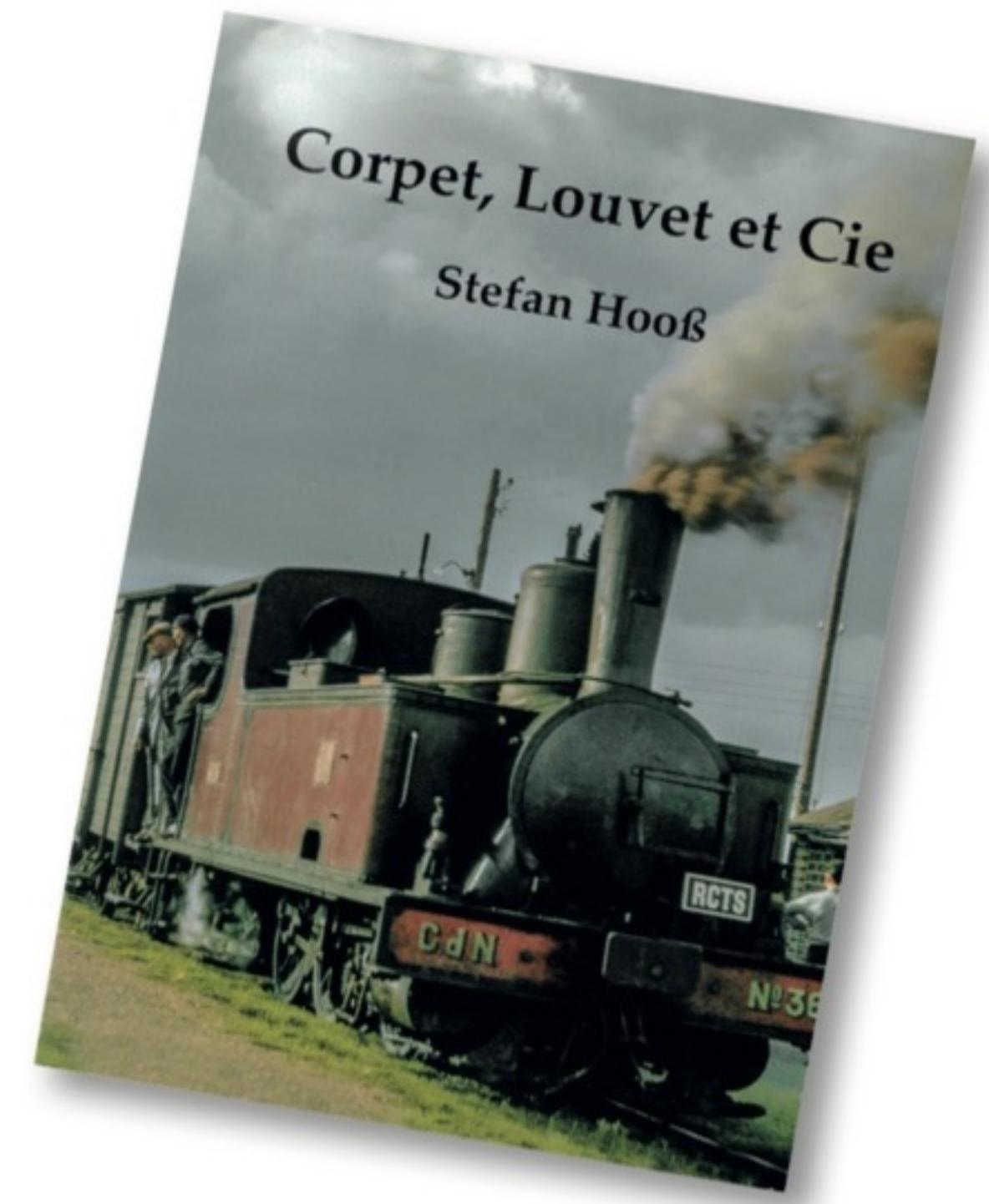
A Corpet 0-6-0T by LGB on Hans Louvet's *La Trinité* layout.

The model was weathered by Len de Vries.

The coal bunker contains real coal. The screw couplings either side of the centre buffer are Märklin Gauge 1.

See CM October 2003.

Photo: Steve Flint.



Around fifteen photos appear twice, deliberately, in both the loco and location sections, where no alternatives were available.

There is no absolute works list, but nevertheless this book is a remarkably thorough record of the company's production. It took much more work than the author expected, full of discoveries and surprises, and shows that Corpet was capable of much more than the typical light railway 0-6-0T which may spring to mind at the mention of the name.

An absolutely superb book, especially for anyone with any interest in French minor railways and their motive power. Heartily recommended.

The book has also been produced in a German language version.

## Swiss Postbuses

Written and compiled by Martin Fisher

Swiss Railways Society,  
Sales, Glyn Jones, Alpigen,  
13, Welland Close, Dalebrook,  
Burton-upon-Trent, Staffordshire, DE15 0AG.  
[www.swissralsoc.org.uk](http://www.swissralsoc.org.uk)

297mm x 210mm 206pp

Hardback £28.00 + £3.50 UK postage.

ISBN 978-0-9926812-9-6

This new book from the Swiss Railways Society is in fact a considerably expanded second edition, with additional information, updates, corrections, and more photographs. It is unusual in that it is a substantial hardback – and it is not about railways! Well, not directly, but in Switzerland the public transport network is well integrated and where there are no railways, connecting buses provide a service. Where those bus routes are not operated by urban authorities or commercial concerns, the Post bus will be found. (In fact, as the book reveals, many Post buses are worked by commercial operators under contract.)

Modellers accustomed to placing a Post bus in the station forecourt as an authentic accessory may be surprised to discover the variety of vehicles used. All are described here, which should help choosing the right bus for the period and the location.

Almost all the vehicles used are in the familiar yellow & white livery.

The introduction provides some historical background, then there is a survey by region – Bern, central,

eastern, Graubünden, northern, Ticino, Valais, western, and Zürich, plus the activities spreading into France and Liechtenstein. There are sections on night buses and the Publicar experimental dial-a-ride service, a detailed review of the vehicles used in the past, and a full listing of the modern fleet.

Extensive appendices provide: an account of the trials of early buses; a list of current bus operators; a list of buses supplied to the PTT up to 1960; a list of vehicles supplied from 1960 to the end of the P system in 2003; re-numbering of the fleet in 2003; a sample list of current numbering; and sample tickets.

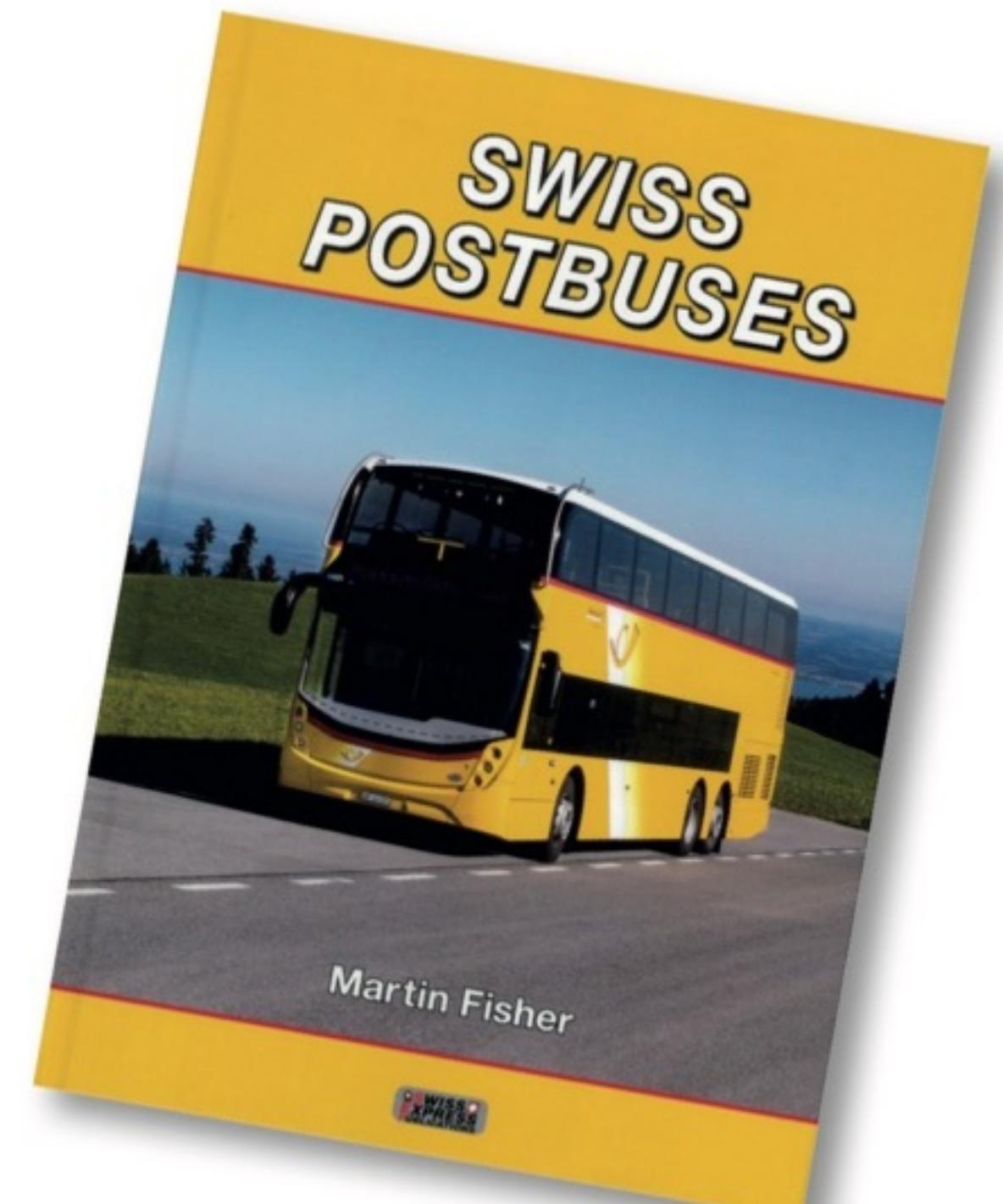
The work is rounded off with a bibliography and index.

This may sound like heavy going with a lot of data, but the presentation is clear and simple, with tinted backgrounds to the occasional information panels and the extensive tables.

The account is in fact built around a comprehensive collection of photographs, from historic black & white through to contemporary colour, all well reproduced. Some are from the author along with David Stevenson and other Society members, some (naturally) from archives and official sources, and some from the internet. Every photo is fully captioned and (where relevant) credited.

There are schematic maps clearly showing the routes by region.

Some of the roads used are quite challenging – as certain illustrations demonstrate. (We will be cautious about criticising 'unlikely' gradients and corners on layouts in future ... )



The book concludes with a page advertising other Society publications, while the back cover promotes the Swiss Travel Centre – with relevant information and illustrations.

For overseas postage rates, and details of credit card payment, contact – [sales@SwissRailSoc.org.uk](mailto:sales@SwissRailSoc.org.uk)



## TRAXX-Lokomotiven

The Bombardier TRAXX locomotive family can now be seen in Germany and in many other European countries. Dating back to the prototype electric 12X, the first classes to enter service were the 145, 146, and 185, followed later by the 146.5, 186, 147, 147.5, 187, and now also the 188. Also belonging to this family are the diesel-electrics of classes 246, 285, and the four-engined 245.

This programme takes a detailed look at this locomotive family with scenes filmed at Bombardier's factory in Kassel, on the test circuit at Velim in the Czech Republic, and in the cabs of many locos, plus a wide range of scenes filmed from the lineside on both passenger and freight workings with Deutsche Bahn AG, with private operators in Germany, and in other European countries.

Coverage begins with scenes of a variety of different TRAXX locos, electric and diesel, on both freight and passenger services filmed from the lineside. We see the prototype 12X (128 001) and film from 1997 shows the very first 145. Further film of this class in service follows with scenes showing them at the head of freight trains, observed from the lineside and at depots. A 145 is also seen on a train of DB Regio double-deck coaching stock, with another on a similar working at Königstein in the Elbe Valley.

Private operator examples include a Rail4Chem green liveried 145 at the head of a train of bogie tank wagons, and 185 001 is seen on display at the Inno Trans exhibition. Further examples of this class are then seen from the lineside on a variety of freight services. At Romanshorn a member of this class passes through the station on a train of containers and SBB Cargo 482 001 is also observed at Großkorbetha, south of Halle (Saale) on a train of bogie tank wagons. BLS Cargo examples are also shown.

At Essen Hbf. 146 001 is seen on a train of DB Regio double-deck coaching stock and further scenes show this train from the lineside. 146 020 is also seen on a similar working in the Elbe Valley. Metronom locomotives are seen working services formed of double-deck coaching stock in their distinctive yellow and blue livery. 146 122 is seen at Recklinghausen on a DB Regio service to Münster Hbf. formed of double-deck stock, and further film shows similar services from the lineside and at Geislingen (Steige) station. At Villingen (Schwarzwald) station a train to Konstanz formed of DB Regio double-deck stock leaves behind 146 231.

At the Bombardier factory in Kassel we see TRAXX locomotives under construction, including classes 146 and 185.

Class 185s are seen on freight services filmed from the lineside and an OHE-owned example is also shown. Other private operator units are shown on freight, including those in service with Captrain, MRCE, HSL, and Railpool. At the Inno Trans event we see E186 111 on display.

Diesel-electric 246 001 is seen at Cuxhaven on a Metronom service formed with double-deck coaching stock.

EK Videothek DVD c.58mins ref.8456 €19.80

Class 285 diesel-electrics are also seen at the Inno Trans event, followed by film of this class in action, including those in service with hvl and Captrain. Angel Trains E186 locomotives are then shown working freight for BLS Cargo, and Polish examples are seen on freights.

At the Bombardier factory in Kassel E186 324 is seen running light on test with subsequent scenes from the lineside and inside the cab. Side views clearly show the four pantographs. Further examples of this class are shown, including E186 333 at Würzburg Hbf. with a train of logs.

Next we see 245 003, a diesel-electric, and we are able to take a look into the engine room. 245 008 is then seen at Mühldorf (Oberbayern) depot in Bavaria, along with sister locomotive 245 001 at the head of a train of DB Regio double-deck stock departing the station for München Hbf. Further scenes of these locomotives at work on similar services are filmed from the lineside, along with some on Sylt Shuttle workings between Niebüll and Westerland (Sylt). Nord-Ostsee-Bahn examples are also shown, while at Lindau Hbf. DB Regio 245 037 is seen departing at the head of a train of double-deck stock bound for Friedrichshafen. Further scenes show these locomotives on similar services filmed from the lineside along the Allgäu main line.

DB Fernverkehr InterCity-liveried 146 553 is seen leaving Halle (Saale) with a train of matching double-deck stock bound for Leipzig Hbf. on the opening day of this service. The 146 is shown pushing the train out of the station with further views from the lineside and from inside the cab.

At the test circuit in Velim, in the Czech Republic, 187 003 is seen being tested both from the lineside and inside the cab. The loco is seen first running under diesel and then electric power and at the head of a heavy train of bogie hopper wagons. A representative of Bombardier Transportation is interviewed and describes the locomotive as we watch it in action and we take a look inside the equipment rooms. A WLE example, No.82, is also seen on test, then Captrain and Raildox class 187s are seen on mixed freight services.

Back at the Bombardier factory in Kassel, Deutsche Bahn AG 147 002 is shown and described. It is shown running light and then at the head of a DB Regio train of double-deck stock. 147 014 is also observed on a similar working.

187 102 is then shown and described, while 187 001 is seen negotiating a depot's traverser and we get a tour around the inside of the loco from its driver. 187 106 is shown moving off shed and we later see this loco leaving Mannheim with a freight. There is further film of this working from the lineside, and another member of this class is seen with a train of bogie tank wagons.

Further DB Fernverkehr Inter City-liveried 147s are then shown at speed with double-deck stock; at Mühlacker station we see one of these arrive and depart.

The programme ends with a look at the first new class 188.

Very little background music is used in this production, mainly towards the end, and it is not too intrusive. As usual, the commentary is only available in German.

The main feature is followed by approximately 45 minutes of trailers for other Eisenbahn Kurier programmes.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. [www.eisenbahn-kurier.de](http://www.eisenbahn-kurier.de)

## Edition XL

### Kult-Dieselloks der DR Die Baureihen V180, V200, 130-132 und 142

by Konrad Koschinski, Dr Franz Rittig,  
and Manfred Weisbrod

VGB Verlagsgruppe Bahn GmbH,  
Am Fohlenhof 9a,  
D-82256 Fürstenfeldbruck, Germany.  
[www.vgbahn.de](http://www.vgbahn.de)

295mm x 210mm 240pp  
Softback €19.95  
ISBN 978-3-8375-1966-2  
German text ref.601801

This is a combination of three *Eisenbahn Journal* 'Sonderausgaben' (special issues) on the East German V180, V200 'Taigatrommel', and V300 'Ludmilla' diesels.

The history and development of each class is covered in great depth, with sections dealing with the background, prototypes, technical details, re-engining, deployment, those used by the East German Government (V180s), those in use with private operators, liveries carried, and those saved for posterity as 'Museumsloks'.

Many of the over 400 black & white and colour photographs and the illustrations included are extracts from Deutsche Reichsbahn publicity material. There are scale drawings and cut-away diagrams showing the arrangement of power units, transmissions, and cooler group within the locomotive body, and many of the archive photos show locos under construction.

At the end of each chapter there is a list of locos built showing for each the running number, factory number, date of delivery, date rebuilt\*, date of withdrawal\*, and date scrapped\* (\* as applicable).

In the section on the class V200 'Taigatrommel', the Russian type M62 locos are also featured, as well as those used in Hungary, Poland, Czechoslovakia, and Cuba.

In the chapter about the 132 family, drawings of the proposed driving trailer BR234 of 1994 are featured.

This is an excellent and well researched history of these iconic Deutsche Reichsbahn diesel locomotives. Whilst a good knowledge of the German language will be required to get the best from it, the wealth of photographs alone should whet the appetite of all those with an interest in DR motive power, making it a worthy addition to the bookshelf.

Just two pages are given over to advertisements for other books from VGB, including other titles in the Edition XL series.



# News

Information from CONTINENTAL MODELLER



## Recent HO kits from Faller



Recently released is one of the new items announced at Nürnberg, a straight steel fishbelly girder bridge for double track, with stone abutments (ref.120503). It is suitable for use with all model track brands. The kit contains fifty-nine components in four colours, and of course has full instructions.

Also now available is the first in a new modular range, representing a production, forwarding, or storage hall and is based on originals by the 'Goldbeck' building construction company (ref.130890). All parts that make up the basic hall, such as the wall and roof sections, roof lights and hinged windows,



the roll-up doors (which can be installed open or closed), and the trolley inside the hall can be arranged as desired and supplemented with other compatible components in the series. Separately available there are elements such as walls (optionally with gates, doors, or windows), steel truss girders, concrete

columns, and roof sections.

The kit contains 124 components in seven colours, clear foil for windows, and instructions.

Faller products are distributed in the UK by Gaugemaster.  
[www.gaugemaster.com](http://www.gaugemaster.com)  
[www.faller.de](http://www.faller.de)

## Recent Z releases from AZL



New for Z from AZL are Baltimore & Ohio lightweight passenger cars, representing vehicles used on the *Columbian* named train which ran from New York to Chicago with stops in Washington D.C., Pittsburgh, and Akron.

The streamlined set consisted of dome cars, four coaches, diner, and observation car as well as baggage and mail cars. These dome cars were the only ones on the Chicago – Washington – New York routes. They were termed 'strata-domes' by the B&O and carried names such as *Sky* and *High*.

The models are equipped with AZL AutoLatch™ couplers.

The range includes the dome car (choice of two names), a 6-6-4 sleeper, diner (choice of two names), coach, tail observation car (choice of two names), baggage, and a United States Mail Railway Post Office.

The matching F3 A-B set to haul this train is also in stock; three running numbers are available.



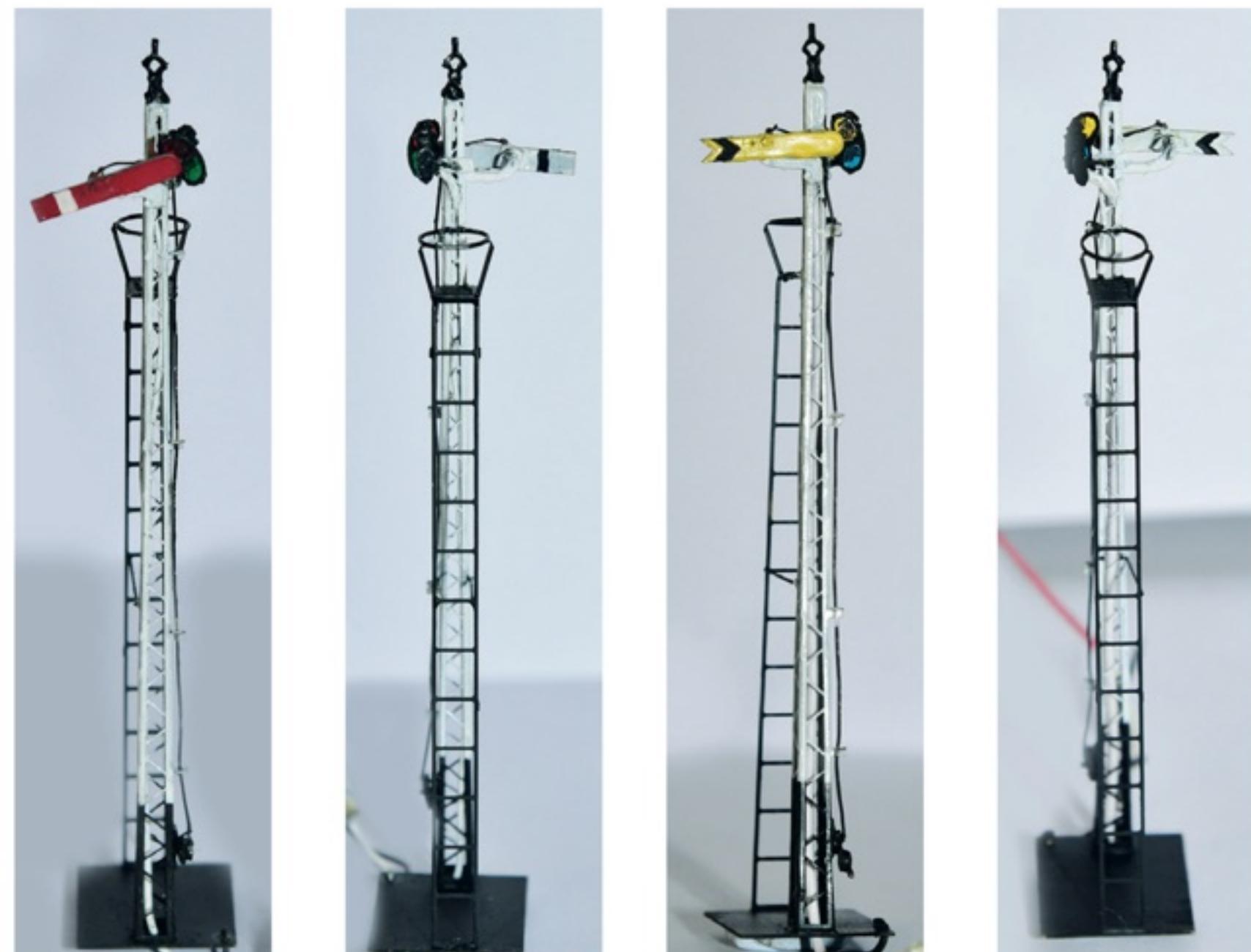
Motive power of a later generation is represented by the Genesis P42, which is now offered in the classic New Haven colour scheme with a choice of two running numbers.

The 1937 40' AAR boxcars are now available lettered for the Erie, singly, in two packs, and in sets of four, all with different running numbers.

Orders can be placed at:  
<http://ztrackdistribution.com>



## Indian semaphore signals for HO



We were interested to see some lattice post semaphore signals (home and distant) being produced for HO in the **Model Trains India** range by Nitish Industries.

They are made of brass, with a cast whitemetal lamp body, and have a working light and movable arm mechanism. They are supplied assembled and ready to use.

The height is 114mm including finial, and the base is 25mm x 20mm.

The price is £40.00 each.

Postage via EMS Speed Post with tracking costs £14.00; this service usually delivers within ten working days.

(Editor's note: these items may be liable to tax and duty when delivered to the UK, and perhaps also to other countries.)

They can be purchased via the website, through eBay UK, or directly by e-mail ([nitishindustries111@gmail.com](mailto:nitishindustries111@gmail.com)) with secure payment via PayPal.

Model Trains India welcome trade enquiries for these items. They are also interested in manufacturing custom bulk orders.

See the website for further details: [www.modeltrainsindia.com](http://www.modeltrainsindia.com)



## Latest HOe models from Minitrains

After a long production delay caused by manufacturing problems last autumn, when no new models were released, it is with great delight that Minitrains announce the release of a host of new goods wagons, all based on a standard underframe, with imitation grain on the 'wooden' body parts.

The range includes bolsters, flats, flats with ends (for pulpwood), boxes (for waste), and tipping V-skips.

The wagons are sold in packs of four – with brake, without brake, or two of each.

The Plymouth four-wheel diesel loco is now available in nine different liveries, including a simple self-assembly version which is supplied with the motor installed and the chassis ready-to-run; the body is unpainted grey and comes with extra detailing accessories. Liveries available are all black, black/black/red

(cab/body/frames, *illustrated*), black/yellow/black, black/red/black, yellow/yellow/red, all yellow, black/red/red, and green/green/black.

There is no price increase, but €:£ exchange rate variations are possible.

Minitrains,  
Standard Light GmbH.,  
D-76189 Karlsruhe, Germany.  
[www.minitrains.eu](http://www.minitrains.eu)



## Cycle van for HOe from Ferro Train



To mark the hundredth anniversary of the narrow gauge Mariazellerbahn in Austria, the railway put into service the prestige Ötscherbär train set in a plain brown livery which reflected traditional colour scheme. During the summer season in addition to the baggage van, bicycle vans were added to the train allowing the carriage of an additional forty cycles. Only two were painted in the matching brown livery, Nos.16 824 and 16 841. Ferro-Train have created a

limited edition model, finely printed, to enable modellers to complete an authentic train consist. Separate printed panels are included which allow the vans to be used in different periods. Only twenty have been made, so if you are interested we suggest acting quickly!

Note new address since 12th June.  
Ferro-Train  
A-1230 Wien, Leopoldigasse 15-17,  
Austria.  
[www.ferro-train.com](http://www.ferro-train.com)

## New for 1:35 by Matho Models



The Belgian accessory specialist has released three new items intended for 1:35 scale.  
ref.35102 Movie Posters A – 1940s.

ref.35104 Movie Posters C – 1970s and 1980s.

ref.35108 Warning and Caution Tapes (double sided) A – English.

For more details, including prices, how to order, and UK suppliers:  
[www.mathomodels.com](http://www.mathomodels.com)



## Arnold offers Swedish vans in N

Arnold (Hornby) have announced two new Swedish Epoch III versions of the familiar four-wheel refrigerated vans in N: singly in "nykarnat" blue (ref.HN6403) and a twin pack in "THERMOTRANSPORT" blue & grey (ref. HN6402). Both are registered with the SJ (Statens Järnvägar).

For more details: [www.hornby.com](http://www.hornby.com)



## Innovative wheel cleaner

New from KPF Zeller is an ingenious and very efficient motorised wheel cleaner for HO which is designed to be built in to a convenient track on the layout. This avoids having to handle the locomotives.

It uses two powered moving platforms – the locomotives run over these under power. These cleaning units oscillate slowly, at a frequency which has been carefully calculated (and is deliberately not changeable) for a consistently good cleaning result.

It is very quiet – much quieter than a domestic refrigerator.

It works on two- or three-rail, analog or digital, with all commercial HO track systems (except Trix Express).

No cleaning fluid is used, and the cleaning cloth material (like that used so successfully in their rail cleaning wagon) is inexpensive and readily available.

The overall length is c.250mm and the width 52mm.

If required, the firmware can be updated free in future via a USB link.

The device has been developed and is manufactured in Germany by reliable in-house employees.

The unit comes ready to use, with all electronic components including a plug-in 5V power supply, with a cable approximately 1.5m long, extendable to a maximum of 25m, and there are detailed operating instructions.

The cobblestone side pieces shown are not included – these were attached to the demonstration unit simply to show how easily the whole thing unit can be built in to the layout.

KPF Zeller, Friedhofstrasse 6,  
D-73108 Gammelshausen, Germany.  
[https://kpf-zeller.de](http://https://kpf-zeller.de)



# Exhibition Diary

Dates, events, and information

## Saturday 22 June

### KING'S LYNN, Norfolk

**Organiser:** King's Lynn MRC.  
**Venue:** Lynnsport, Green Park Way, King's Lynn, Norfolk, PE30 2NB.  
**Open:** 1030 – 1600  
**Admission:** adults £5.00, children (5 – 15) £2.00, under 5s free.  
**Amenities:** disabled-friendly; refreshments.  
**Features:** layouts include *Gila Canyon* (US N, CM November 2018), and vintage French O gauge three-rail. Trade.  
**Contact:** <https://klmrc.co.uk>

## Sunday 23 June

### BARTON LE CLAY, Bedfordshire

**Organiser:** Beds and Bucks Narrow Gauge Modellers.  
**Venue:** Barton Le Clay Village Hall, Hexton Road, Barton Le Clay, MK45 4JY.  
**Open:** 1000 – 1600  
**Admission:** adults £5.00, accompanied children free.  
**Amenities:** free parking; refreshments.  
**Features:** open day – 11 layouts, plus displays, trade, and OO9 Society sales.  
**Contact:** Brian Key 01908 615287

## Saturday 29 & Sunday 30 June

### PERTH

**Organiser:** Perth MRC.  
**Venue:** Dewars Centre, Glover Street, Perth, PH2 0TH.  
**Open:** 1000 – 1730 Saturday  
1000 – 1700 Sunday  
**Admission:** adults £9.00, children £4.00, family (2+2) £20.00.  
**Amenities:** next to railway station; park & ride from Broxden on Saturday; on-site parking; buffet and bar.  
**Features:** layouts include *Ronshafen* (N), *Treffburg* (Austrian/German HO, CM December 2013) *Campo de Leste* (Portuguese HO, CM February 2018), *Cascade Yard* (US HO), *Broadford* (Australian HO, CM January and March 2010, January 2013, and June 2018), *Mariana Mesa* (US On30), and *Eu (Le Treport) depot* (French O, CM November 2018). Societies, trade.  
**Contact:** 07365 115955  
[www.perthmrc.com](http://www.perthmrc.com)

## Saturday 29 & Sunday 30 June

### EXETER, Devon

**Organiser:** Exe MRS.  
**Venue:** Matford Livestock Centre, Matford Park Road, Marsh Barton, Exeter, Devon, EX2 8FD.  
**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday  
**Admission:** £7.50, accompanied children free.  
**Amenities:** free parking (limited on Sunday); Park & Ride service (Green route) to Matford Park Road (Saturday only); disabled access; restaurant.  
**Features:** 35+ layouts including *Bw.Dügelskirchen* (German HO, first appearance), *Canadian National Jillsburgh* (HO), and various OO9/HOe layouts from the local area group of the OO9 Society. Trade.  
**Contact:** 01392 421906  
[www.exemrs.co.uk](http://www.exemrs.co.uk)

## Saturday 6 July

### BEACONSFIELD, Buckinghamshire

**Organiser:** Beaconsfield & District MRC.  
**Venue:** The Beaconsfield School, Wattleton Road, Beaconsfield (old town), Buckinghamshire, HP9 1SJ.  
5 minutes from J2 M40 (signposted).  
**Open:** 1030 – 1700  
**Admission:** adults £5.00, children £2.50.  
**Amenities:** free parking on site; refreshments.  
**Features:** layouts include *San Maria Gandia* (Spanish HOe/HO, CM September 2018) and *Elmwood Yard* (HO). Trade.  
**Contact:** Roy Jackson 01753 643724  
[www.beaconsfieldmrc.org](http://www.beaconsfieldmrc.org)

## Saturday 13 July

### HODDESDON, Hertfordshire

**Organiser:** The Friends of Robert Barclay Academy.  
**Venue:** The Robert Barclay Academy, Cock Lane, Hoddesdon, EN11 8JY.  
**Open:** 1030 – 1630  
**Admission:** adults £4.50, concessions £3.00, family (2+2) £12.00.  
All profits to academy projects.  
**Amenities:** free parking on site; refreshments.  
**Features:** layouts include *Schweizer Kleinstadt* (Z), *Brownsville* (HO), and *Transcontinentally Tri-ang* (OO & HO). Demonstrations, societies, trade.  
**Contact:** 07866 641215

## Saturday 13 & Sunday 14 July

### BEER, Devon

**Organiser:** Pecorama.  
**Venue:** The Station Gallery, Pecorama, Beer, Seaton, Devon, EX12 3NA.  
**Open:** 1000 – 1700 both days  
**Admission:** free (normal Pecorama admission prices apply).  
**Amenities:** parking; disabled access (hilly site renders three-wheel mobility scooters unsuitable); refreshments.  
**Features:** *The Andeer Line* (Swiss HOm modular layout). Visitors are invited to help operate the layout, with trains driven individually using smart 'phones and tablets. There will also be demonstrations of the foam baseboard construction and the use of electrostatic grass. Drop in for as long as you wish. See website for details.  
**Contact:** [www.pecorama.co.uk](http://www.pecorama.co.uk)

## Saturday 13 & Sunday 14 July

### HUNSLET, Leeds, West Yorkshire

**Organiser:** Middleton Railway Trust.  
**Venue:** Middleton Railway, The Station, Moor Road, Hunslet, Leeds, LS10 2JQ.  
**Open:** 1000 – 1700 both days (steam trains operating between 1100 – 1600).  
**Admission:** adults £5.50, children £2.50, family £14.00. Exhibition/Engine House plus unlimited train rides – adults £8.50, children £3.50, family £20.00.

**Amenities:** disabled access to exhibition and trains but not footplate experiences; light refreshments.

**Features:** layouts in many scales and gauges including *Dornochbahn* (HOe) and *Roanoake Roundhouse* (HO). Footplate experience on a full-size loco £10.00; minimum age 18, maximum 76, no need to book in advance. Trade, and other attractions on site including nearby retail outlets.

**Contact:** Ian Dobson 01274 613687 (eves only)  
[www.middletonrailway.org.uk](http://www.middletonrailway.org.uk)

## Saturday 20 July

### BRIGHTON

**Organiser:** Sussex Vintage Model Railway Collectors.  
**Venue:** The Knoyle Hall, Knoyle Road, Preston Park, Brighton, BN1 6RB.  
Off A23, close to main bus routes and a short walk from Preston Park station.  
**Open:** 1000 – 1630  
**Admission:** adults £4.00, children free (must be accompanied).  
**Amenities:** unrestricted and two-hour kerbside parking nearby; wheelchair access to main hall but not to first floor displays; refreshments.  
**Features:** layouts including classic Hornby O, Hornby Dublo, Bassett-Lowke, Lionel, Trix Twin, working Meccano, and more. Trade.  
**Contact:** 07785 567251  
[www.sussex-transport.co.uk/svrmrc](http://www.sussex-transport.co.uk/svrmrc)

## Saturday 20 July

### FELIXSTOWE, Suffolk

**Organiser:** WorldWide Group (WWG) of the N Gauge Society.  
**Venue:** Trinity Methodist Church, 26, Hamilton Road, Felixstowe, IP11 7AN.  
**Open:** 1000 – 1600; AGM 1610.  
**Admission:** adults £3.00, WWG members and accompanied children free. Admission refunded to visitors who join WWG on the day.  
**Amenities:** parking nearby; railway station 10-15 minutes walk; disabled access (but no parking on site); refreshments.  
**Features:** layouts include *Hindenbach* (German), *Hartenberg* (Austrian), *Depot de Camions*, *L'Isle de St. George*, and *Schiebourg* (French), and *East Tyrone* and *Southerland* (US). Members' sales, trade.  
**Contact:** [secretary@ngauge-wwg.org.uk](mailto:secretary@ngauge-wwg.org.uk)

## Saturday 20 & Sunday 21 July

### CUPAR, Fife

**Organiser:** Cupar & District MRC.  
**Venue:** The Corn Exchange, St.Catherines Street, Cupar, Fife, KY15 4BT.  
**Open:** 1000 – 1630  
**Admission:** adults £4.50, children £2.50, family (2+2) £12.00.  
**Amenities:** free parking nearby; good public transport; refreshments.  
**Features:** layouts include *Altdorf Ost* (HO), *Burlington Central* (N), and *Ripon WI* (HO). Club and society stands, trade.  
**Contact:** [www.cuparmrc.co.uk](http://www.cuparmrc.co.uk)

Please send details for possible inclusion to:

Peco Publications & Publicity Ltd., Beer, Devon, EX12 3NA.  
Telephone: 01297 20580 Fax: 01297 20229  
e-mail: [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com)

## Saturday 27 & Sunday 28 July

### NORTH SHIELDS, Tyne & Wear

**Organiser:** Blyth & Tyne MRS.  
**Venue:** John Spence Community High School, Preston Grange, North Shields, Tyne & Wear, NE29 9PU.  
**Open:** 1000 – 1700 Saturday  
1000 – 1600 Sunday  
**Admission:** adults £7.00, children £3.00, family (2+2) £16.00.  
**Features:** up to 30 layouts, including *The Bridge at Remagen* (German N), *Derby Freemo American HO modular*, and *Broadford* (Australian HO, recently extended, see CM June 2019). Demonstrations, trade.  
**Contact:** [www.railexne.com](http://www.railexne.com)

## Sunday 28 July

### THIRSK, North Yorkshire

**Organiser:** Thirsk MRG.  
**Venue:** Thirsk Town Hall, Westgate, Thirsk, North Yorkshire, YO7 1QR.  
**Open:** 1030 – 1630  
**Admission:** adults £4.00, children £2.00, family £10.00.  
**Features:** 12+ layouts, including *Nordhafen* (German HO, in this issue). Demonstrations, trade.  
**Contact:** 07766 697708  
[www.expo-thirsk.co.uk](http://www.expo-thirsk.co.uk)

## Meetings

### Tuesday 2 July – 20.00

#### The Enfield Transport Circle

*British Steam in South Africa 1987 to the present* by Ken Livermore.  
**Venue:** First Floor Meeting Room, The Drill Hall Sports Club, 1, Old Park Avenue, Enfield, EN2 6PJ.  
**Admission:** £4.00.  
**Contact:** [k.h.crane@hotmail.co.uk](mailto:k.h.crane@hotmail.co.uk)  
Kenneth Crane 07596 142117

### Monday 8 July – 19.30

#### Lincoln Railway Society

*Gold In Them Thar Hills And Railways* by Graham Lightfoot.  
**Venue:** St.Hugh's Church, Harewood Crescent, North Hykeham, Lincoln, LN6 8JG.  
**Admission:** visitors £3.00.  
**Contact:** 01522 698085  
[grahamlightfoot1948@gmail.com](mailto:grahamlightfoot1948@gmail.com)

### Thursday 18 July – 19.15

#### Marlow & District Railway Society

*Travels with the LCGB in North Korea in 2014/5* by Colin Miell.  
**Venue:** Bourne End Community Centre, Bourne End, Buckinghamshire, SL8 5SX.  
**Contact:** 01494 638090  
[www.mdrs.org.uk](http://www.mdrs.org.uk)

### Saturday 27 July – 14.00

#### T&LRS – East Midlands Area

*Australian Tramways 1981, 1996, and 2016* by Bob Bracegirdle.  
**Venue:** Sixth Beeston Scout Hut, Middle Street, Beeston, Nottingham, NG9 1GA.  
**Contact:** David Hanger 01455 203307

# News

Information from CONTINENTAL MODELLER

## New version of Tillig railcar



Further to our review of the Harz four-wheel railcar by Tillig last month, Tillig have recently released a further version which is not restricted to the Harz system, in Mittelbadische Eisenbahn-Gesellschaft (MEG) blue & white livery (ref.02941).

MEG railcars T1 to T8 were diesel-mechanicals driven on one axle (A1). The Gotha wagon factory, owned by Orenstein & Koppel since 1931, supplied these eight railcars between 1934 and 1941. They are thus the largest standard class of railcar on any German narrow gauge railway. They were able to haul a trailer when necessary. They took over the majority of passenger traffic on the 115km MEG network between Rastatt and Lahr.

The vehicles followed the lightweight construction principles of the time, with a body welded from sections and sheet. The design allowed for variants to meet customer requests.

The drive used a Daimler-Benz OM 65 engine and Mylius transmission and was arranged below the floor.

The equipment also included a whistle, bell, electric lighting, heating stove, indirect brake, and the standard MEG central buffer with coupling hook beneath. Side chains were added later. The vehicles performed well, especially on the street running sections.

T7 has survived and is now owned by the Interessengemeinschaft Historischer Schienenverkehr e.V. (IHS).

Tillig offer the model in both H0m (12mm gauge) and H0e (9mm gauge). [www.tillig.com](http://www.tillig.com)

**SAMPLE SUPPLIED BY**  
Mount Tabor Models,  
Scarthin, Cromford, Matlock,  
Derbyshire, DE4 3QF.  
[www.mount-tabor-models.co.uk](http://www.mount-tabor-models.co.uk)

Photo: Graham Lightfoot.

## New mini motor from Tramfabriek

In 2017, Tramfabriek made those who model in small scales very happy with the introduction of an affordable 8x16mm 12V coreless motor (ref.0816S) which is still available. Customers asked for a similar motor with two shafts, as well as a smaller version. Tramfabriek was able to produce 8x16mm (ref.0816D) and 7x16mm (ref.0716D) coreless motors with two shafts. It is notable that the 0816D was a world first.

Tramfabriek is now proud to announce an even smaller motor: a 6x15mm (ref.0615S) and, yes, it is 12V. At 12V it rotates at 21,800rpm, 21% slower than its closest rival (Maxon). The 8mm 0.8mm diameter shaft is three times longer than the Maxon 10V motor, which has a short 2.5mm shaft on each end: this makes it much easier to fit a worm, probably the most common method amongst railway modellers.

It can also run very slowly. It makes a perfect 12V replacement for the Busch H0f Feldbahn locos, and runs much quieter than the geared motor fitted.

This motor makes it possible to make Tramfabriek belt drive chassis even smaller. We illustrate one of these motors installed on a brass chassis with 5.6mm diameter wheels on a 13.8mm wheelbase, which makes it the smallest drive Tramfabriek has ever made.

No load current: 15mA max

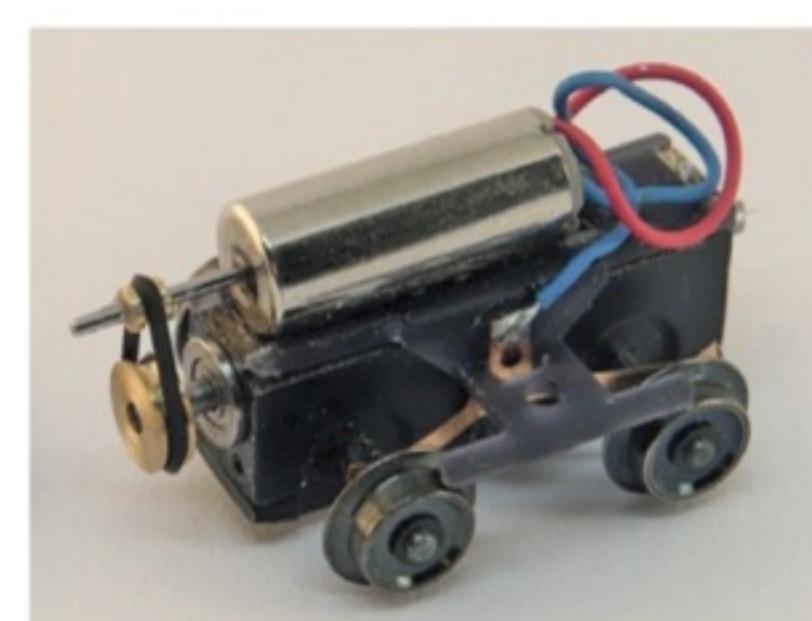
Stall current: 110mA max

Stall torque: 4.9g.cm

Terminal resistance: 113Ω ±5%

Tramfabriek

2, Locken Cottages, The Walk,  
Eton Wick, Berkshire, SL4 6LN.  
<https://tramfabriek.nl>



## Coming next month

out 18th July

# CONTINENTAL MODELLER

AUGUST 2019 £4.95 (UK)

[www.pecpublications.co.uk](http://www.pecpublications.co.uk)

FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

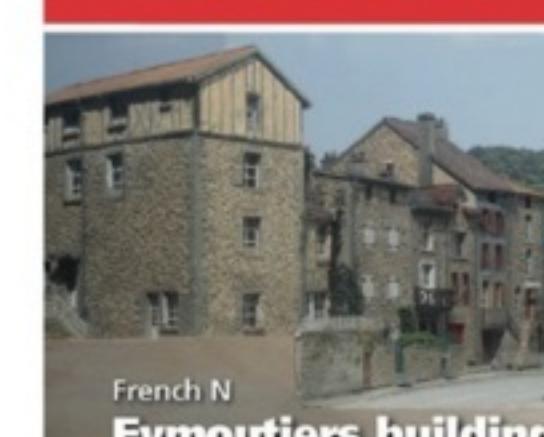
## Rusinowo Waskotorowe

Polish H0e

CONTINENTAL MODELLER  
Celebrating  
40 years



Also in this issue...

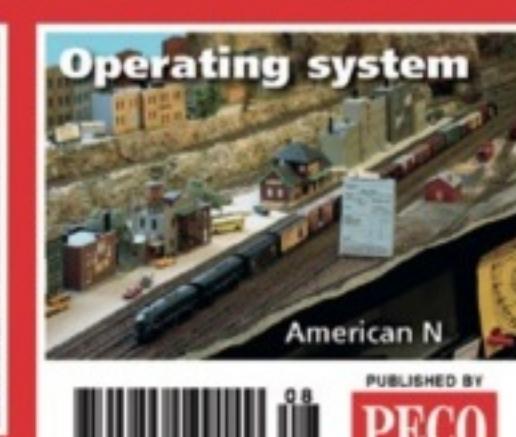


French N  
Eymoutiers buildings



German G  
Brohltal diesel D5

Latest Reviews ... and much more



Operating system

American N

PECO

PUBLISHED BY

PECO

### • Rusinowo Waskotorowe

Stefan Marszał has carefully recreated in H0e a small branch terminus in rural Poland.

### • Buildings from Eymoutiers

Ian Stock has been busy making buildings borrowed from a genuine location adapted to suit his current French N gauge project.

### • Brohltal diesel D5

Günter Völler adapts another proprietary product into a more accurate large scale model of a specific metre gauge locomotive with which he is very familiar.

### • Operating system

Following the description of his US N layout *Twin Forks* (in this issue), Mike Garforth explains how it is worked.

### • Through the garden

Peter Norman explains how he utilised an existing O gauge line between two sheds to provide somewhere to run a growing collection of American locomotives and rolling stock.

... and much more!

# Opportunity Yours

## CONTINENTAL MODELLER Classified Advertisements

To buy, sell or exchange through the columns of Great Britain's best selling magazine for enthusiasts modelling overseas railways

If, when responding to advertisements, you have a query which needs a reply, please enclose a stamped self-addressed envelope.

Replies to box numbers should be sent to:  
Peco Publications & Publicity Ltd, Beer, Devon, EX12 3NA, England.

### Retailers Announcements

**LOWESTOFT**, Parrs, 252 London Road South, Kirkley for PECO PRODUCTS and SCENIC MATERIALS and many other lines. Tel: 01502 565695.

**HULL – EAST YORKSHIRE'S ONLY SPECIALIST** model railway shop. Model railways bought, sold and exchanged. 53A MODELS, 430 Hessle Road, Hull. 01482-227777 (closed Mondays). [www.53amodels.co.uk](http://www.53amodels.co.uk)

**PLUS DAUGHTERS OF BASILDON ESSEX.** N gauge specialist for the UK, Continental and American Modeller. New and second hand, bought, sold or exchanged. *Plus Daughters, 8 Britannia Court, Burnt Mills Industrial Estate, Basildon Essex SS13 1EU.* Tel: 01268 726 211 [www.plusdaughters.co.uk](http://www.plusdaughters.co.uk)

### Trade Sales

**NEW WEBSITE:** [www.zetlandmodelrailways.co.uk](http://www.zetlandmodelrailways.co.uk)  
Good quality second-hand North American, Continental and British, "O", "HO", "OO", "N" & "Z" scales. [email Bob@zetlandmodelrailways.co.uk](mailto:Bob@zetlandmodelrailways.co.uk)

### Trade Sales Books

**CONTINENTAL MODELLER – BACK ISSUES** – for more information on which issues are available, please contact: *Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580.* Price for UK Residents £3 per issue, Europe £3 per issue plus £2 postage sent Airsaver Post and rest of the World £3.00 per issue plus £3 postage sent Airsaver Post. Airmail – please enquire.

### Wanted (Trade)

This section is reserved for the use of traders only.

**MODEL RAILWAYS WANTED** – cash waiting for British and American N and OO/HO scales plus Continental HO model railway equipment. British and American O-scale also considered along with interesting items of railwayana. 53A MODELS, 430 Hessle Road, Hull. 01482-227777 (closed Mondays). [www.53amodels.co.uk](http://www.53amodels.co.uk)

### THINKING OF SELLING YOUR MODEL RAILWAY

**COLLECTION?** If so contact me for an immediate evaluation, distance no object. Collections large or small. PAUL CULL, telephone: 01302 481 274 or mobile: 07767 356 890 or send your list to: *The Bungalow, Denton Green Lane, Kirk Sandal, Doncaster DN3 1JP* email: [simonecull06@tiscali.co.uk](mailto:simonecull06@tiscali.co.uk)

### MODEL RAILWAY COLLECTIONS URGENTLY

**REQUIRED.** Small and large – any scale and any condition. British, European and American collections all required. Nationwide collection, distance no object. Contact us today for a quick and friendly service. Telephone: 01302 371 623, mobile: 07526 768 178, email: [anoraksanonymous@googlemail.com](mailto:anoraksanonymous@googlemail.com)

### Fairs, Sales and Swapmeets

June 2019

**30 June (Sun) 10am-2pm**  
**CATTERICK FAIR**, Catterick Leisure Centre, Gough Road, Catterick Garrison DL9 3EL 10 minutes off A1, Adults £3, Children & Seniors £2. Early entry £5. Free parking. JIM CORR.  
Tel: 0750 403 5955, email: [jim\\_corr73@hotmail.co.uk](mailto:jim_corr73@hotmail.co.uk) for further information.

July 2019

**30 July (Tues) 6pm-9pm**  
**GARSTANG TOYFAIR**, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON.  
Tel: 01282 439 009.

### Private

### Property for Sale

Cut the expense of moving house by advertising your property in the RAILWAY MODELLER or CONTINENTAL MODELLER for just 15p a word its great value for money!!

### Private Wanted

**G SCALE AND OTHER GARDEN RAILWAYS**  
**WANTED.** Gauge 1, SM32, Bachmann, LGB, Piko, Accucraft, Roundhouse etc. Locomotives, rolling stock, track, points and accessories all required. Tel: 01302 371623 email: [sophiecull99@gmail.com](mailto:sophiecull99@gmail.com)

**WANTED ETS O GAUGE TRAM TRACK.** Telephone: 0113 2296 141 or email: [helen.mander@hotmail.co.uk](mailto:helen.mander@hotmail.co.uk)

### Wanted Clubs and Societies

**NARROW GAUGE STOCK WANTED BY THE 009 SOCIETY**, collections, locos, rolling stock, ready-made BEMO, LILLIPUT, ROCO etc or kit and scratchbuilt. Contact: 009 Society, 5 Quarry Road, Colehill, Wimborne. BH21 2NP. Email: [members-sales@009society.com](mailto:members-sales@009society.com) for society details see [www.009society.com](http://www.009society.com)

# OPPORTUNITY YOURS

CM Classified Advertisement Order Form — last date for receiving copy for September 2019 issue is 4th July 2019

Please insert advertisement under the section headed:

**PLEASE NOTE:** If you do wish to renew an existing advert, phone, write, fax or email within 3 months of the original advert being placed.  
**MINIMUM CHARGE £4.20 per advert (£3.50 + 70p VAT)**

Prices below do not include VAT. Please add 20% VAT to total.

**Rates per word:** Private 15p, Trade 25p, Swapmeets 40p.

Box number £5.50 extra. Tick if a box number is required

Advertisements without a box number must include your name, address or telephone number. We would recommend stating gauge. Standard abbreviations (LMS, METR, 0-6-0), normal hyphenations (O-gauge, 2-rail, pre-war) and prices count as one word. Telephone numbers, whether exchange or all figure, count as two words.

**CONDITIONS OF ACCEPTANCE:** All copy is subject to our approval. We reserve the right to request amendments.

Scale or gauge should be mentioned when referring to models. Swapmeets and other events must include a telephone number at which the organiser can be contacted.

Advertisers are expected to acknowledge all enquiries that enclose an SAE (even if the item has already been sold).

Advertisers are reminded of the Trade Descriptions Act and must avoid misrepresentation of goods offered. The Business Advertisements (Disclosure) Order 1977 requires all who sell goods in the course of a business to indicate this clearly in the advertisement.

I enclose cheque/PO for £ .....

(made payable to Peco Publications).

*It is unwise to send coins or banknotes either in payment for the advertisement or goods.*

Name.....

Address.....

..... Postcode.....

Please send remittance to: **Classifieds Department, Peco Publications, Beer, Seaton, Devon, EX12 3NA**

Alternatively you can telephone: 01297 21542, fax: 01297 20229 or email: [classifieds@peco.co](mailto:classifieds@peco.co)

This form may be copied to save cutting the page

UK:  
MODRATEC UK  
17a Top Barn Business Centre  
Holt Heath, Worcester WR6 6NH  
01905 590069  
0845 224 1604  
modratec.co.uk

**MODRATEC**



## MODEL RAILWAY LAYOUTS

### EXCELLENT QUALITY AND VERY AFFORDABLE

any scale or size, standard designs or built to your specification. (Complete or part built sections).

Fold-up designs to allow ease of storing if required.

From train set boards to finescale exhibition quality layouts. Friendly service guaranteed.

Please phone **01924 496053** or write to:

### CHARMING LITTLE LAYOUTS

THE ENCHANTED GARDEN, WHITLEY, DEWSBURY, WEST YORKSHIRE WF12 0LZ  
(sae for information)

Earn rewards as a Master Model Railroader®

32 page glossy colour magazine 6x a year

Discounts at model retailers in UK

Get all this and more just £30 per year

Join online at <https://www.nmrabr.org.uk/amember/signup>



## THE FRENCH RAILWAYS SOCIETY



For all enthusiasts and modellers of the railways of France. Members' benefits include a quarterly Journal, Society library & photo archive. For full details of the Society's events and membership details visit our website [www.frenchrailwaysociety.org](http://www.frenchrailwaysociety.org) or write to The French Railways Society, 93 Deakin Leas, Tonbridge TN9 2JT

## Lever Frames by MODRATEC

MODRATEC Lever frames are rugged solid brass kits. Interlocked and non-interlocked frame available. Electrical releases available. FREE interlocking design software. Mech or elec connection to points and signals. Unique WIT (wire-in-tube) mechanisms. Popular 6- & 12-lever Starter Packs. The clever and affordable option for real hands-on control.

Australia:  
MODRATEC  
PO Box 98  
Glass House Mtns 4518  
(07) 3077 7018  
+61 7 3077 7018  
modratec.com

## ONLINE SHOP

## LENDONS

OVER 70 YEARS IN BUSINESS EST. 1944

**ARNOLD**

**Electra**

**Jouef**

**Rivarossi**

**HORNBY**

**Hornby Collectors Centre and Hornby Warranty Repair Centre**

Large selection of spares

192 Fidlas Road, Llanishen, Cardiff CF14 5LZ

029 2075 2563 9am-5pm

[www.hornbyinternationaluk.co.uk](http://www.hornbyinternationaluk.co.uk)



## SWISS RAILWAYS SOCIETY

Our UK subscription is only

**£20.00 PER YEAR**

for a quarterly full colour A4 magazine and a nationwide network of Branches

Look out for our new hugely expanded A4 2nd Edition book "SWISS POSTBUSES"



For joining details please e-mail: [membership@SwissRailSoc.org.uk](mailto:membership@SwissRailSoc.org.uk)

Or write to: SRS MEMBERSHIP SECRETARY (CM)

55 The Boulevard, Sutton Coldfield, B73 5JB. Or check our website:

**www.swissralsoc.org.uk**

**S**cale & HiRail by American Models

22 Steam and Diesel Engines.  
25 Freight, Passenger Cars and Cabooses.  
Realistic AM and scale Tomyaco Track.  
Over 30 year reputation.  
Email or send SAE for stock list.  
Catalogue - £3.00 incl p&p UK  
Des Browne, 20 Priory Manor,  
HOLYWOOD, BT18 0LD  
browne151@btinternet.com

## INDEXES

PLEASE SEND SAE  
FOR DETAILS OF AVAILABILITY  
AND CHARGES:

PECO TECHNICAL ADVICE  
BUREAU, BEER, NR SEATON,  
DEVON EX12 3NA

  
**THE GERMAN RAILWAY SOCIETY**  
QUARTERLY JOURNAL ↔ LOCAL GROUPS ↔ EXPERT HELP

For details e-mail [memsec@grs-uk.org](mailto:memsec@grs-uk.org)  
or send an A5 SAE to  
Membership Secretary: B Tozer  
49 Queens Road, St George, Bristol BS5 8HT  
Telephone: 0117 954 2424  
**WWW.GRS-UK.ORG**

## FLEISCHMANN N MINT BOXED PRE-OWNED

7078 DB Class 78  
Baltic Tank 4-8-4  
Tank Loco.....£149.00

7094 DB Class  
94 0-10-0  
Tank Loco.....£145.00

7170 DB Class  
011 Pacific  
Loco (MB).....£139.00

7171 DB Class  
012 Pacific  
Loco (MB).....£139.00

7179 DB Class  
50 2-10-0  
Loco (MB).....£139.00

7380 DB Class  
151 Electric  
Loco (MB).....£129.00

 7430 DB Class  
614 Car DMU  
Pack (MB).....£139.00

8120 DB Silverfish  
Driving End  
Coach.....£47.50

8128 DB 1st/2nd  
Bogie Local  
Coach.....£25.99

 9350 Lufthansa  
Airport Express  
Pack (MB).....£245.00

Many more coaches  
and wagons at [marcway.net](http://marcway.net)

Visit our spacious shop ★ 1200 square feet of sales area.  
WORLDWIDE MAIL ORDER SERVICE & ALL MAJOR CREDIT CARDS TAKEN

598-600 Attercliffe Road,  
Sheffield S9 3QS

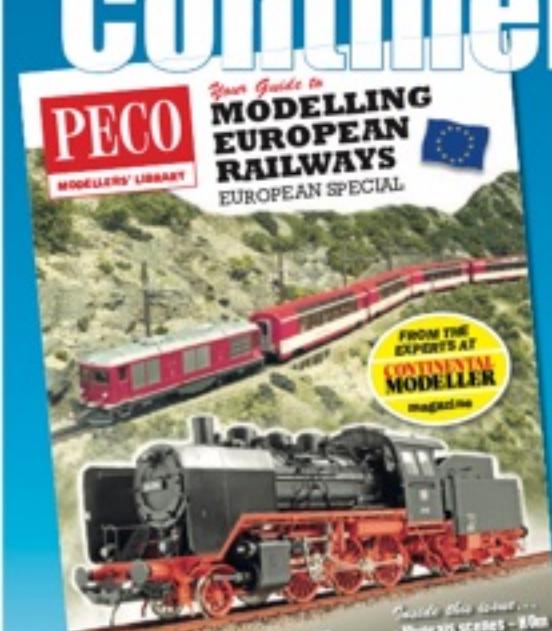
T: 0114 2449170 Open 10.00-4.00  
Closed Wednesday and Sunday

**www.marcway.net**

Situated 2 miles from M1 Junc 34 (Meadowhall Exit)  
200 yards from Attercliffe Tram Stop

0114 244 9170  
FAX: 0114 261 9729

**Strictly Continental**



For every European railway enthusiast

ONLY £6.99

From and all good model shops, by post direct\* (address below), or via our website [www.pecopublications.co.uk](http://www.pecopublications.co.uk)  
\*Please add £1.50 towards P&P.

PECO PUBLICATIONS & PUBLICITY LTD, BEER, DEVON EX12 3NA, ENGLAND

**The European Railways Association**

For a FREE copy of our magazine "Eurorail" ask [editor@eurorail.org.uk](mailto:editor@eurorail.org.uk)

Two e-Magazines  
Many information sources

Local Meetings  
Annual Show



In July you can say Hello to our members with layouts at Basildon (5/6th), Hoddesdon (11th), Pevensey (20th) and Bridport (27th). We will also have a presence at the NGS WorldWide Group Exhibition in Felixstowe on 20th.

Worldwide subscription is still only £11.50 p.a.  
Contact [membership@eurorail.org.uk](mailto:membership@eurorail.org.uk) for information.

**WWW.EURORAIL.ORG.UK**

**GOLDEN VALLEY HOBBIES**





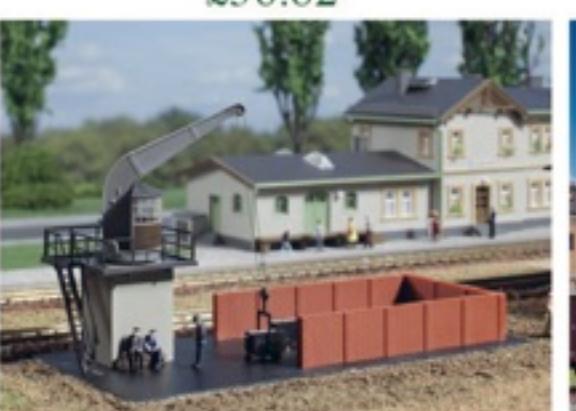
Kibri 39420  
Coaling Store  
£36.82



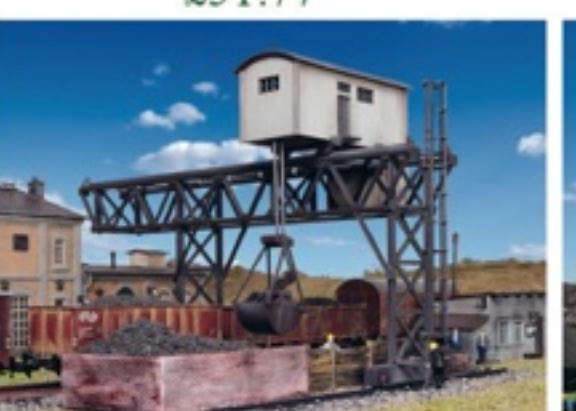
Vollmer 45720  
Large Coaling Stage  
£51.77



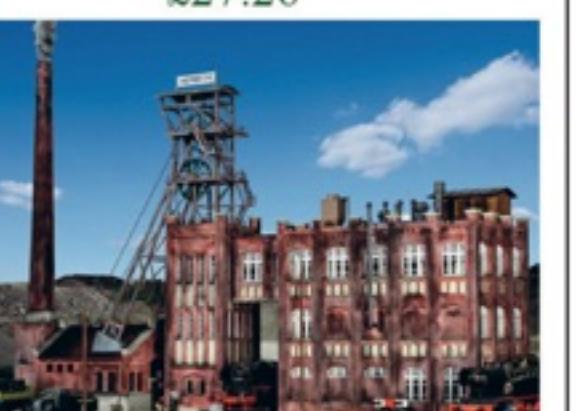
Busch 1045  
Coal Stockpile & Loader  
£27.26



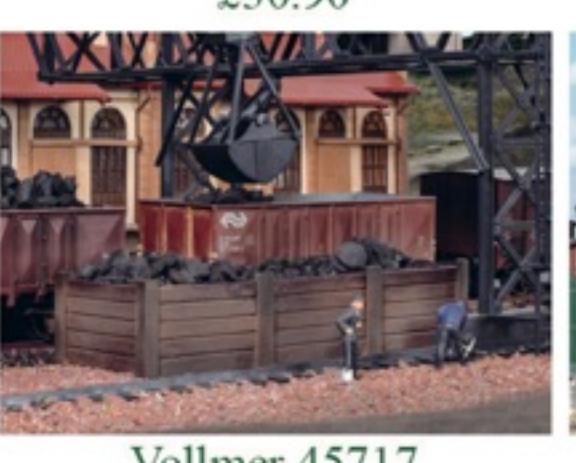
Auhagen 11445  
Coal Loading Crane  
£30.90



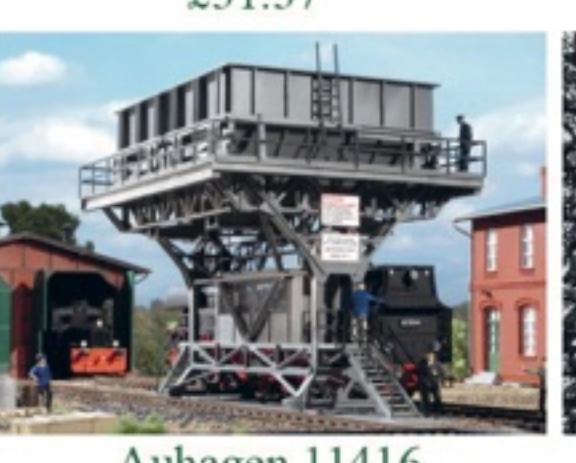
Vollmer 45714  
Coal Loader With Crane  
£31.37



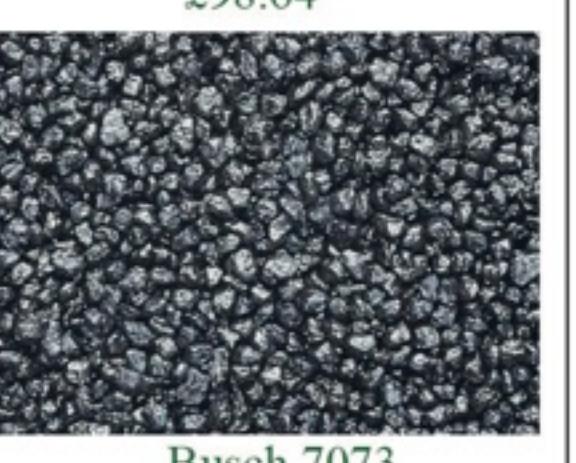
Kibri 39846  
Coal Mine  
£98.64



Vollmer 45717  
Coal Bunker  
£8.18



Auhagen 11416  
Large Coaling Station  
£56.30



Busch 7073  
Large Grade Coal  
£1.99

**www.GoldenValleyHobbies.com**  
Unit 1 Pontrilas Business Park, Pontrilas, Herefordshire, HR2 0AZ  
See our website, write or call to order. Trade enquiries welcome. 01981 241 237  
Prices are correct at time of print and are subject to change.

**FRIZINGHALL**

WWW.MODELRAILSHOP.CO.UK  
01274 747447  
shop@fmrdirect.co.uk  
8A Sapper Jordan Rossi Park  
Baldon, Otley Road  
West Yorkshire, BD17 7AX

Ref	MINITRAINS	MINITRAINS	MINITRAINS	MINITRAINS	Scale
MT1021	Brigade Tank Loco 0-8-0 Black.....	£135.99			H0e
MT1023	Brigade Tank Loco 0-8-0 Green.....	£135.99			H0e
MT1031	Forney 0-4-4 Loco Black/Brown.....	£127.50			H0e
MT1032	Forney 0-4-4 Loco Black No. 5.....	£125.99			H0e
MT1053	Schneider Locotracteur Red.....	£135.99			H0e
MT1072	Baldwin 2-6-2T Closed Cab Grey.....	£132.75			H0e
MT1081	Decauville Progres 0-6-0 Green.....	£135.99			H0e
MT2001	Contractor Loco Black.....	£127.50			H0e
MT2021	Diesellok Ns2f Green.....	£109.99			H0e
MT2022	Diesellok Ns2f Red.....	£109.99			H0e
MT2023	Diesellok Ns2f Blue.....	£109.99			H0e

**LILIPUT HO OFFERS LILIPUT**

L131963	Steam Loco 677 GKB .....	£310.00 ..(was £403.48)
L132003	Diesel Loco BR 225 032-2 Raili.....	£100.00 ..(was £188.60)
L132024	Diesel Loco BR 219 DB Red Ep.IV.....	£199.00 ..(was £290.60)
L133102	Diesel Railcar LINT 27 HEX Ep.V/VI.....	£105.00 ..(was £230.43)
L133981	FLIRT Multiple Unit EUROBAHN Ep.....	£150.00 ..(was £292.20)
L131968	Tender Loco Class 680 GKB Ep.III.....	£330.00 ..(was £403.38)
L133031	Diesel Luggage Railcar VT 10 002 DR.....	£125.00 ..(was £223.20)
L133122	Diesel Railcar LINT 27 HEX Ep DCC.....	£199.00 ..(was £248.30)
L132522	Electric Loco Prototype Ep.IV.....	£115.00 ..(was £239.12)
L133000	Track Inspection Trolley OBB Ep.III.....	£105.00 ..(was £135.60)

**FREE POSTAGE FOR ORDERS OVER £150.00 (UK Mainland Only)**

**DCC Sound & DCC Decoder Fitting Weathering & Rename / Renumber**

**OPENING HOURS - MONDAY to SATURDAY 09.30 - 17.30**

**mkn**  
**digital**

**Märklin Repairs**



ABN : 13693291720

**Märklin OO and H0 1947 to 2019**  
**Servicing, Digital Conversions and Advice.**  
**Quality Workmanship.**

From \*800, 30\*/31\* to 36\*,37\*/39\* series locomotives undertaken.  
AC to MM2 or MFX formats with/without sound. Reverse Units.  
Large range of Märklin spares and decoders kept in stock.

Proprietor: David Neale  
P.O. BOX 40 BOWRAL NSW 2576 AUSTRALIA  
Mobile : 0414 309 120 Telephone : (02) 4861 1761  
Email : [mkn\\_digital@bigpond.com](mailto:mkn_digital@bigpond.com) Website : [mkn-digital-train-repairs.com](http://mkn-digital-train-repairs.com)

**Australia Wide Märklin Model Train Repairs**



**modelrail**   
track, trains & digital

Track, Trains and Complete DCC Layouts at low prices

**10% DISCOUNT ON ALL N DCC LOCOMOTIVES**

CODE LOCO101 VALID UNTIL JULY 31 2019

Specialists in German n scale and digital DCC  
[www.modelrail.co](http://www.modelrail.co) [info@modelrail.co](mailto:info@modelrail.co) 07918-111111

*American Model Railroading in HO*

Locomotives	Worldwide Shipping		<i>Everything You Need To Go Model Railroading</i>
Paints & Adhesives	Freight Cars		DCC Systems
Styrene Plastics	Books		Thousands Of Items In Stock
Scenics	Passenger Cars		High Quality Pre-Owned Models
Vehicles	Tools		Buildings

**Website Only Shows In Stock Items and Clearly Marked Advance Reservations**

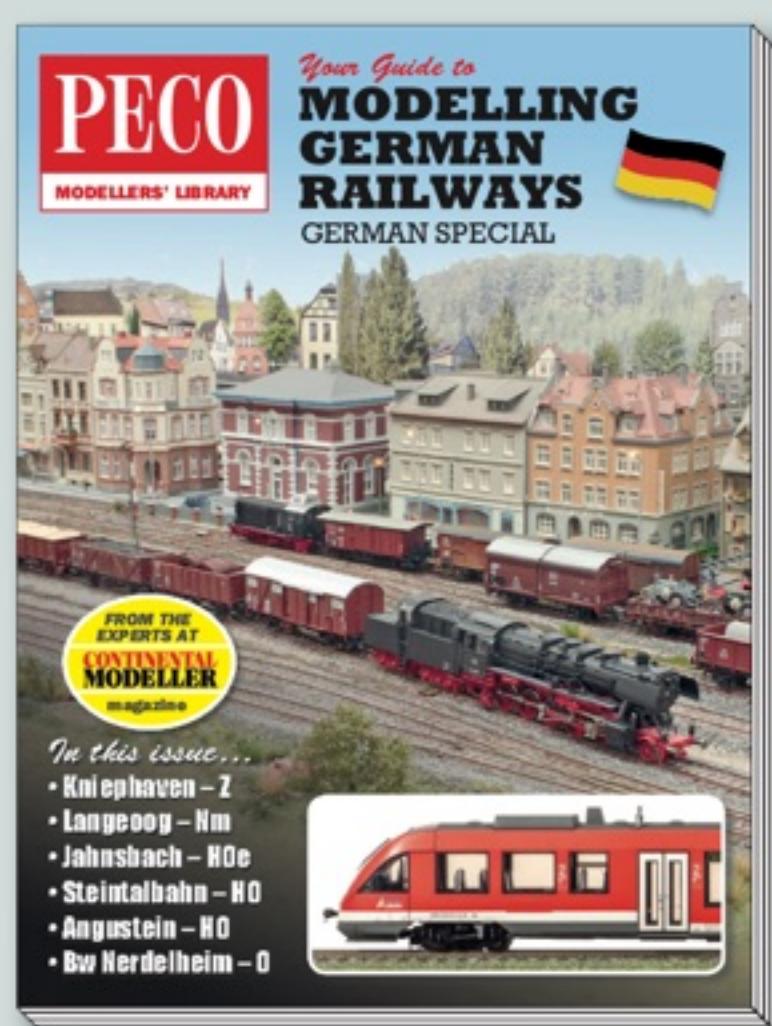
Our Yeovil Road shop is open Tuesday to Friday 10.00am to 4.00pm, Saturday 10.00am to 1.00pm (Closed Bank Holidays)  
Shop open 5 days a week - Website 24/7 - 916 Yeovil Road, Slough Trading Estate, Berkshire, SL1 4JG

**www.modeljunction.info**

# AROUND THE WORLD IN 4 PUBLICATIONS

## Your Guide to Modelling European Railways PM-205

The scope for modelling European railways is huge and may seem daunting to enthusiasts used to modelling only British scenes. Once you've chosen a region this superb introduction serves as an invaluable aid in getting a good start on a continental layout.

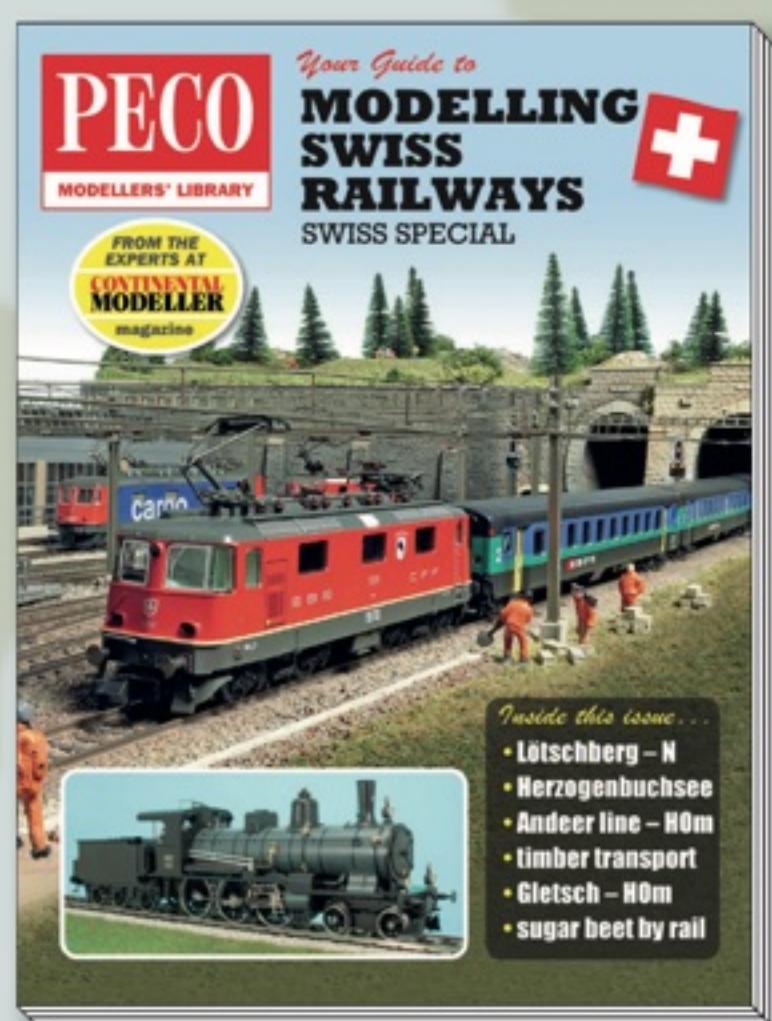
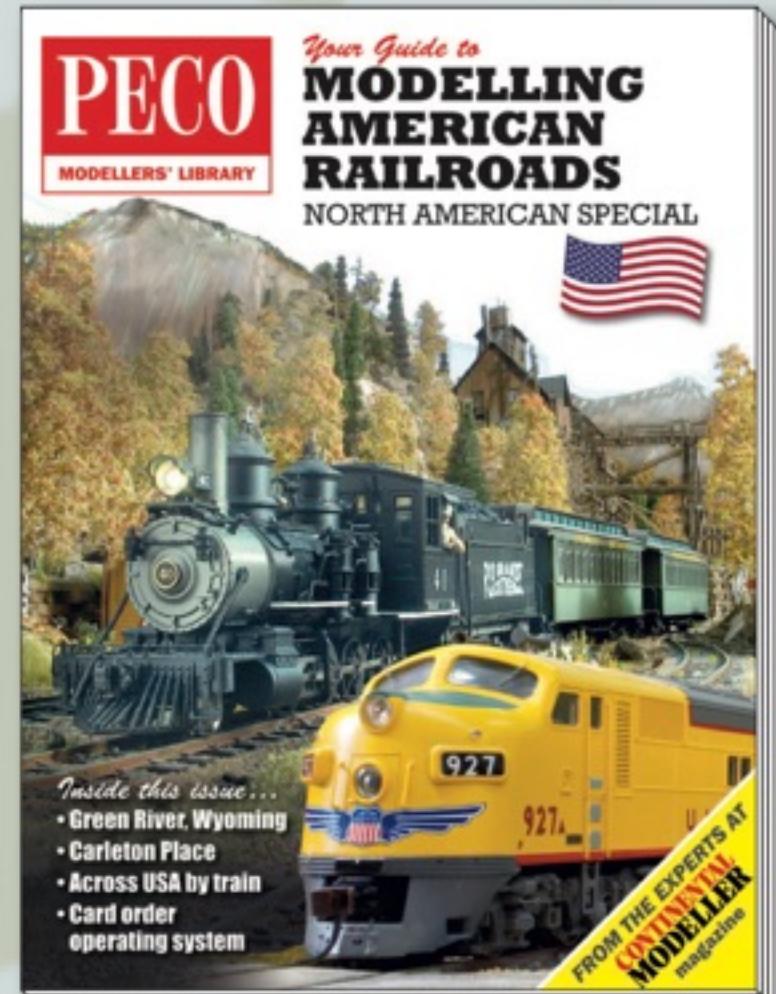


## Your Guide to Modelling German Railways PM-207

Consider the huge range of equipment offered by German and other manufacturers (easily available in Britain) coupled with the history and geography of the region, is it any wonder German railway subjects are as popular as ever. This guide will help you get started.

## Your Guide to Modelling American Railroads PM-201

The sheer variety of railroads in North America makes them a subject with enormous potential for enthusiasts everywhere. This invaluable guide is aimed at anyone who wishes to start modelling this perennially fascinating subject.



## Your Guide to Modelling Swiss Railways PM-209

Swiss railways have a special attraction for many enthusiasts. The intense network of lines, volume of traffic, and seemingly endless variety of companies and liveries complements the incredibly attractive Alpine scenery so often associated with the railways of this land-locked country.

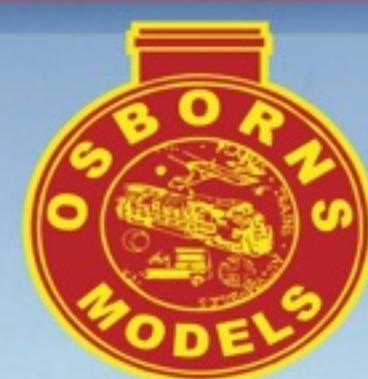
Get them at your local model shop or direct from PECO.  
Visit [www.pecopublications.co.uk](http://www.pecopublications.co.uk) for details or call 01297 626204

Technical Advice Bureau • Peco Publications • Beer • Devon • EX12 3NA

# OSBORN'S MODELS

DOING 'N' SCALE  
on a  
BIG SCALE

**KATO**  
PRECISION RAILROAD MODELS



7 HONESTONE ST, BIDEFORD, N.DEVON, EX39 2DL 01237 423453  
[www.osbornsmodels.com](http://www.osbornsmodels.com)

**Conwy Valley Railway Museum**

**BETWS-Y-COED LL24 0AL**  
STEAM & DIESEL LOCOS OPERATING OUR  
7 1/4" SCENIC RAILWAY. 10 MINUTE RIDE  
MUSEUM WITH MODEL RAILWAY LAYOUTS

- 'OO' Hornby - Bachmann Branchline - 'N' Gauge - 'Z' Gauge
- IN STOCK 'N', 'HO' & 'G' GAUGE CABLE CARS
- 'O' Gauge Bassett-Lowke Locos and Rolling Stock
- Large stocks 'G' Gauge - Roundhouse Live Steam
- NOW IMPORTING FULL RANGE LIVE STEAM WILESCO
- Excellent stocks of Peco - 009 - 'HO' - Scenery
- Brawa, Marklin, Trix and TrixTwin

Mail Order Tel: 01690 710568 Fax: 01690 710132  
OPEN DAILY ALL YEAR 10.00am to 5.00pm Large Car Park  
e-mail: [info@conwyrailwaymuseum.co.uk](mailto:info@conwyrailwaymuseum.co.uk)  
web site: [www.conwyrailwaymuseum.co.uk](http://www.conwyrailwaymuseum.co.uk)



The best way to ensure that you receive your copy of **Continental Modeller** every month is to support your local newsagent by placing a standing order. He can get it hot off the press each month and should be able to either deliver to your door or reserve it for you in the shop.

**HAND THIS REQUEST IN TO YOUR LOCAL NEWSAGENT TODAY**

Dear Retailer,  
Please deliver/reserve for me a copy of **CONTINENTAL MODELLER** every month starting with the next available issue.

Name .....

Address .....

..... Telephone .....

If you don't want to cut your magazine, you can use a photocopy



EXE MODEL RAILWAY SOCIETY

presents

# EXETER 2019 Model Railway Exhibition

SATURDAY 29th JUNE

10am – 5pm

SUNDAY 30th JUNE

10am – 4pm

Matford Livestock Centre  
Matford Park Road  
Marsh Barton, Exeter EX2 8FD

Admission £7.50

Accompanied Children FREE

- Over 35 Model Railway Exhibits
- Up to 20 Trade Stands
  - Restaurant
  - Disabled Access
  - Free Parking

(Limited parking Sunday morning)

For latest information and directions see:

[www.exemrs.co.uk](http://www.exemrs.co.uk) or call  
01392 421906

## MAC'S MODEL



## RAILROADING

4-8 Reform Street Kirriemuir  
Scotland DD8 4BS

Authorized Exactrail Dealer, Railflyer Prototype, Kato, Gaugemaster, Digitrax, Broadway Limited, Woodland Scenics

## BRITISH, AMERICAN AND CONTINENTAL OUTLINE

To contact us: email is best or mail to the above address.

[ian@ecscenics.co.uk](mailto:ian@ecscenics.co.uk) [www.macsmodels.co.uk](http://www.macsmodels.co.uk)

Telephone 01575 572397

Payment:- £4.95 PER ORDER. Overseas at cost.  
Postal orders, Cheque, IMO (stirling),  
All Major Credit Cards & Debit Cards

Opening Hours Monday - Saturday 10.30 -5.00pm

Closed Thursday

Our full terms & conditions are available on our website or by sending us a SSAE

MORRIS MODELS  
NORTH LANCING

80 Manor Road, North Lancing,

West Sussex, BN15 0HD

Tel 01903 754850

[www.morris-models.co.uk](http://www.morris-models.co.uk)

[info@morrismodels.co.uk](mailto:info@morrismodels.co.uk)

Your nearest Continental Model Railway Shop to Brighton & Worthing

## FLEISCHMANN

SPECIAL OFFER whilst stock lasts



Fleischmann N Gauge: 735501:  
DBAG BR101 Electric Locomotive  
Baden-Wurttemberg: Era 5

Was £166 NOW £149.95

## BICYC-LED

OO/HO & N  
Cycles &  
Motorcycles  
with working  
lights



Cycles £15.50 each; M/cycles £16.95 each  
Police Motorcycles - incl 3 flashing blue lights  
and control unit - £45.00 (N & OO/HO)  
(+ postage & packing)

Selection of designs available including  
Scooters, Mopeds and Motor-cycles  
For full details of range - visit our website

In shop digital demonstration layout - N & HO/OO gauge - Decoder fitting service

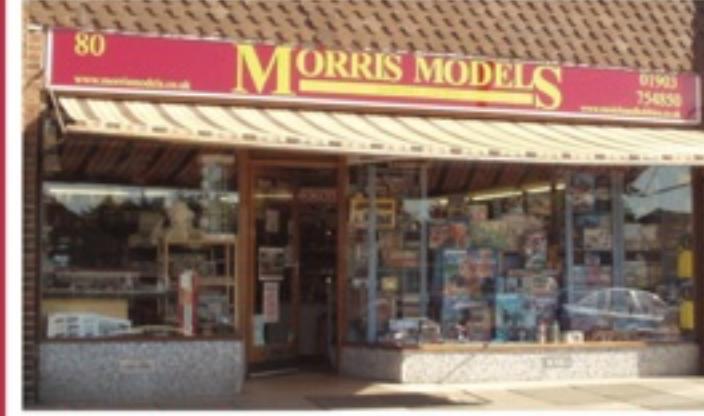
We stock the following ranges:

FLEISCHMANN Roco TRIX BRAWA KLEINER FALLER kibri VOLLMER  
NOCH BUSCH herpa WIKING Preiser LENZ ELEKTRONIK WOODLAND SCENICS PECO  
HORNBY PARISH by BACHMANN HELJAN dapol METCALF SUPERQUICK

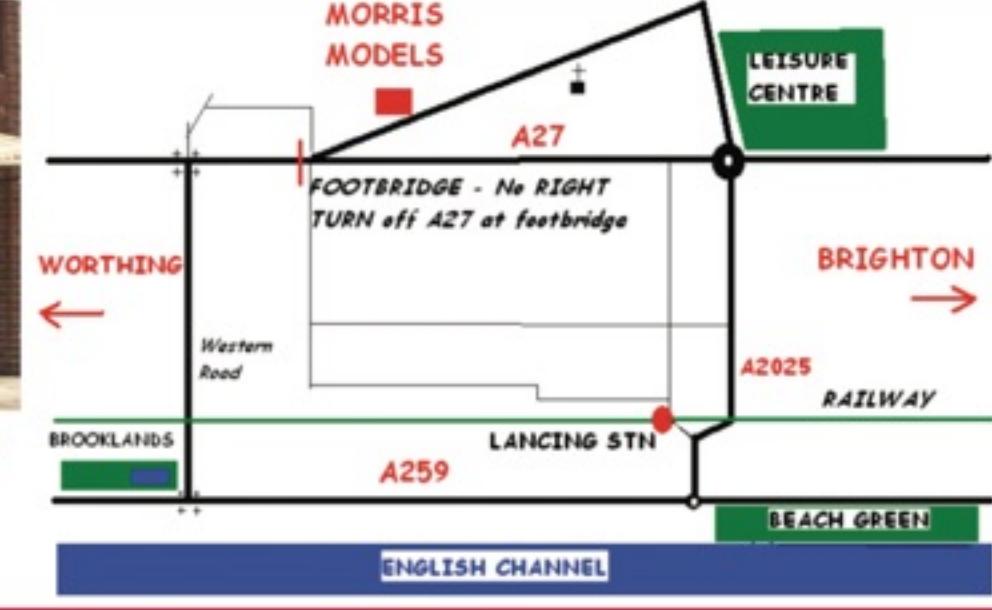
We also stock other scenics and N & HO/OO diecast road vehicles.

NEW ONLINE SECURE WEB SHOP - [www.morris-models.co.uk](http://www.morris-models.co.uk)

All major credit/debit cards accepted



We are just off A27 between Brighton & Worthing just west of Shoreham Airport - easy free parking.  
Open Mon - Sat 9.30 to 5.00



## DCC Supplies

Excellence in model control

Email: [info@dccsupplies.com](mailto:info@dccsupplies.com) Website: [www.dccsupplies.com](http://www.dccsupplies.com)  
Telephone: 01905 621999

## Green Scene

Scenic Modelling Accessories

## CREATING Realistic Landscapes FOR MODEL RAILWAYS



Tony Hill

## 2-day Scenery Workshop

By John Lloyd & Tony Hill

This course is aimed at modellers of all abilities, and will interest beginners and veterans alike! During the two days attendees will be led through the stages of landscape modelling and super detailing.

Date: 15th & 16th July 2019

Price: £145.00 per person (includes refreshment & Lunch both days)

### Shop Opening Times:

- Mon–Thur 10am–5pm, Fri 12pm–5pm
- Sat 10am–4pm

### Phone Opening Times:

- Mon-Fri 10.30am–12pm & 2pm-4.30pm

Unit 17A Top Barn Business Centre, Holt Heath, WR6 6NH UK

# THE HOBBY SHOP

Est. 1988

www.hobb-e-mail.com 01795 531666 info@hobb-e-mail.com

85 Preston Street, Faversham, Kent, ME13 8NU



Opening hours: Tuesday to Saturday 9.30 am to 5.00 pm  
TRY OUR SAME-DAY DESPATCH MAIL ORDER SERVICE.  
ALL MAJOR CREDIT/DEBIT CARDS ACCEPTED.



## New Arrivals at C&M Models

ACME MAV (Hungarian) EuroCity coaches, Light Blue/White Livery, Era V

52042	Restaurant Car, MAV Start Logo	£57.30
52053	Luggage Car, MAV Start Logo	£55.90
52062	1st Class Open Coach, MAV Start Logo	£55.90
52072	2nd Class Open Coach, MAV Start Logo	£55.90



Roco SBB EW-II Schlieren Coaches, Green Livery, Era IV

74560	1st Class Open Coach	
74561	1st/2nd Class Composite Open Coach	
74562/74563	2nd Class Open Coach, 2 different numbers	
74564	Baggage Coach	All versions: £42.35



1 Crosby Street, Carlisle CA1 1DQ

Tel: 01228 514689, Tues > Sat 10a.m. > 5p.m.

WEBSITE ADDRESS <https://candmmodels.co.uk>

E-mail [sales@candmmodels.co.uk](mailto:sales@candmmodels.co.uk)



# Kent Garden Railways

## 16mm and G Scale Specialists

Whether its live steam, battery or electric for your garden we have the stock to get you going



LGB \* Accucraft \* Peco \* Piko \* Bachmann \* Trainline \* Roundhouse \* Pola \*

## Great Stocks of On30, OO, HO, OO9/HOe & N

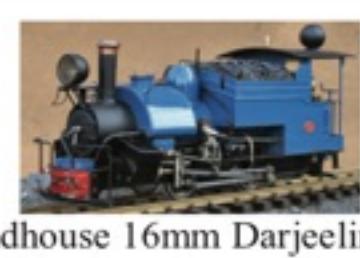
We have large stocks from the leading manufacturers



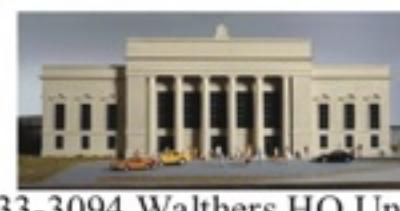
Hornby \* Bachmann \* Peco \* Minitrains  
Gaugemaster \* Metcalfe \* Heljan  
Graham Farish \* Dapol \* Woodland  
Scenics \* Kato \* Walthers \* Kadee  
Expo \* Noch \* Oxford Diecast \* Kestrel  
Atlas \* Parkside \* Oxford Rail \* Ratio  
Wills \* Modelscene and much more



Piko G Scale DR BR95 Steam Locomotive



Roundhouse 16mm Darjeeling B - live steam locomotive



933-3094 Walthers HO Union Station Kit



LGB G Scale 23530 RhB Class G 4/5 Steam Loco



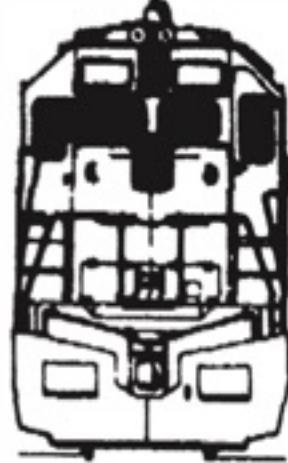
Walthers HO Mainline GE ES44AH Evolution Series GEVO Locomotive

## Kent Garden Railways

66 High Street, St Mary Cray, Orpington, Kent, BR5 3NH  
Tel 01689 891668 email [sales@kgrmodels.com](mailto:sales@kgrmodels.com) [www.kgrmodels.com](http://www.kgrmodels.com)

Shop open 0900-1700 Tuesday to Saturday

Diesels Steam Engines Books Buildings Freight Cars Passenger Cars



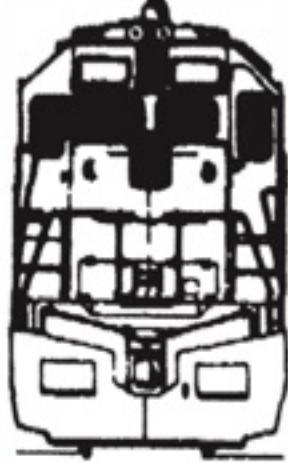
## THE AMERICAN RAILROAD CENTRE

Specialist Importers and Retailers of American Model Railroad Equipment

15 Lower Bore Street, Bodmin, Cornwall PL31 2JR

Telephone: (01208) 72025 Ext 1

E Mail: [americantrains@aol.com](mailto:americantrains@aol.com)



Tuesday-Friday 10am-4pm  
Saturday 10am-3.30pm ★ Closed Monday



Trucks & Cars Signals Backscenes

DCC

DVD

Trackwork

Scenics Detailing Parts

Magazines

Visit us Online at:  
[www.americanrailroadcentre.com](http://www.americanrailroadcentre.com)

# THE GREAT RAILWAY ADVENTURE

at Beer,  
Devon

Sunday  
16th June

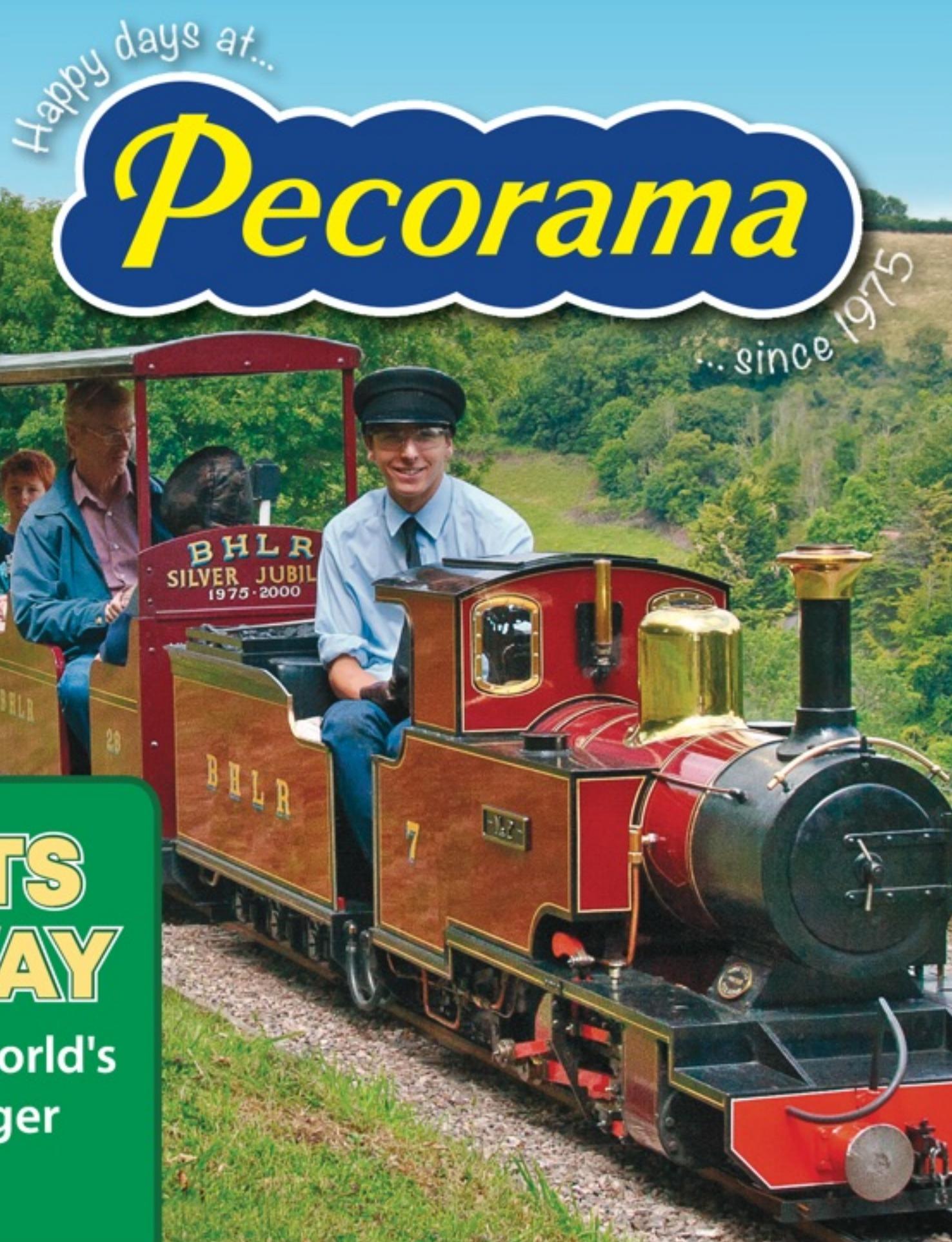
*Fathers Day*

FREE ENTRY when  
accompanied by  
their children

*Ride the*

## BEER HEIGHTS LIGHT RAILWAY

Enjoy a trip on one of the world's  
finest 7 1/4" gauge passenger  
carrying railways.

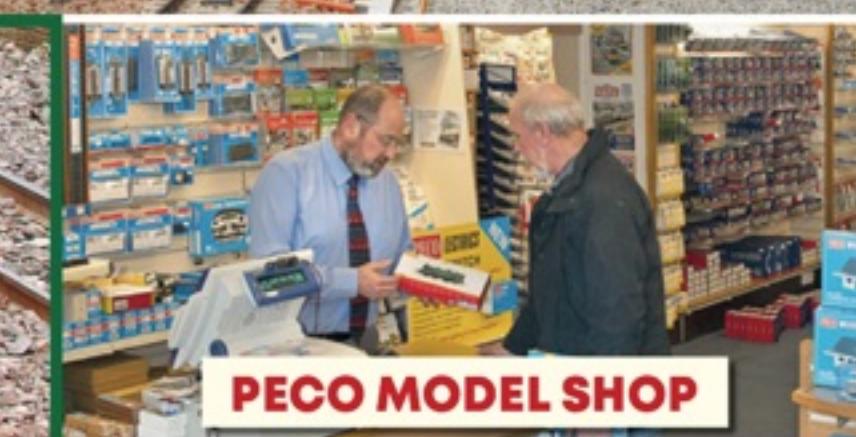
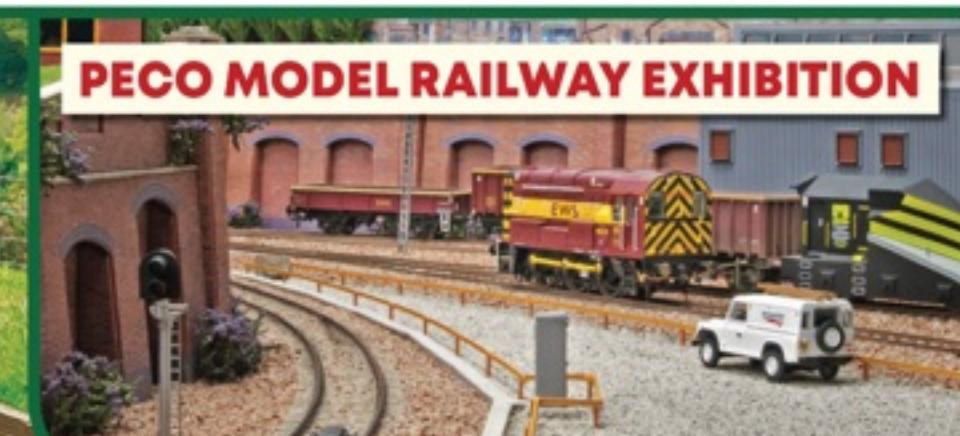


### SPECIAL EVENT!

**'Thomas II' 40th  
Birthday Gala**  
**Weekend of 29th  
& 30th June**

With extra trains steaming up  
for this celebration weekend!

And Special Guest **Paul Atterbury**  
from BBC1's Antiques Roadshow



PECORAMA • BEER • DEVON • EX12 3NA • t: 01297 21542 • [www.pecorama.co.uk](http://www.pecorama.co.uk)



**The U.K.'s Premier North American Model Railroad Store**

Extensive stock of **HO, N & Z scales**, including hard to find product  
We are official dealers for most American brands

**STOP PRESS;**

**Now official dealers for Broadway Limited, Scaletrains, Tichy**

'Bricks & Mortar' store with **FREE** parking, tea & coffee

Selected Sale Items (more on our website): -

Bachmann USA HO scale freight cars

17502 Santa Fe (ATSF) Centre Flow Hopper	£14	19611 Maine Central 50' Boxcar	£25
17531 Cargill (ACFX) Hopper	£14	19612 Norfolk Southern 50' Boxcar	£25
17928 Erie Lackawanna Steel Reefer	£14		

Atlas HO scale freight cars

20003500 Canadian Pacific (CP) Boxcar	£18	20004560 Erie Lackawanna 70 ton Ballast car	£28
---------------------------------------	-----	---	-----

Fox Valley Models HO scale freight cars

FVM 30248 Great Northern (Green) 7 Post Box Car	£28	FVM 30249 Great Northern (Green) 7 Post Box Car	£28
---	-----	---	-----

Trainworx N scale freight cars

24031-3-040 MP 100 Ton 12 Panel Quad Hopper (3 car set)	£40	24033-3-036 MP 100 Ton 12 Panel Quad Hopper (3 car set)	£40
24032-3-041 MP 100 Ton 12 Panel Quad Hopper (3 car set)	£40	24034-3-031 MP 100 Ton 12 Panel Quad Hopper (3 car set)	£40
24051-3-023 C&NW 100 Ton 12 Panel Quad Hopper (3 car set)	£40	24053-3-020 C&NW 100 Ton 12 Panel Quad Hopper (3 car set)	£40
24052-3-022 C&NW 100 Ton 12 Panel Quad Hopper (3 car set)	£40	24054-3-019 C&NW 100 Ton 12 Panel Quad Hopper (3 car set)	£40

### MECH MODELS

Unit 9, Lancaster Park, Newborough Road, Burton on Trent, DE13 9PD  
[www.mech-models.com](http://www.mech-models.com) [admin@mech.co.uk](mailto:admin@mech.co.uk) [01283-575444](tel:01283-575444)

## Unit 2, 48-52 Tomline Road, Ipswich IP3 8DB

Opening Hours: 1000-1600 Mon-Fri, 1000-1300 Sat. Closed Sun & Bank Hols  
Phone: 01473 252009. Email: scograil@btinternet.com Web: www.scograil.co.uk  
UK Delivery: Orders under £100 = £3, £100 - £195 = £4, Free if over £195

Arnold HN6400 N Gauge NS Kbs Container Wagon Set (2) £44.95	Arnold HN6401 N Gauge NS Railpro Flat Wagon with Load Set (2) £44.95	Fleischmann 739001 N Gauge DBAG BR147 Electric Loco £119.95
Fleischmann 781902 N Gauge SBB Re450 Zurich S-Bahn Train Pack £329.95	Fleischmann 831603 N Gauge NS Box Wagon Set (3) £56.00	Hobbytrain (by Lemke) H23483 N Gauge ÖBB Nightjet Rh1116 Taurus Electric Loco £129.95
Hobbytrain (by Lemke) H23484 N Gauge Holcim Uacs Silo Wagon £29.95	Hobbytrain (by Lemke) H2783 N Gauge ÖBB Nightjet Rh1116 Taurus Electric Loco £129.95	Hobbytrain H23466 N Gauge SBB Cargo Tagnppss Bogie Silo Hopper Wagon Set (2) £59.95
Hobbytrain H23468 N Gauge SBB Cargo Tagnppss Bogie Silo Hopper Wagon Set (2) £59.95	Hobbytrain H23482 N Gauge Holcim Silo Wagon Set (2) £59.95	Kato (by Lemke) K137118 N Gauge SBB Re460 Mondaine Electric Loco £151.95
Kibri 10922 HO Gauge 6 x 40ft Containers £15.40	Kibri 10924 HO Gauge 8 x 20ft Containers £13.90	Piko 50108 HO Gauge Classic DR BR01.15 Steam Loco £169.95
Piko 52089 HO Gauge Classic DBAG BR642 Desiro 2 Car DMU £179.95	Piko 57919 HO Gauge Hobby ÖBB Rh1216 Electric Loco £53.95	Piko 57922 HO Gauge Hobby ÖBB Rh1116 Electric Loco £53.95
Piko 58806 HO Gauge Hobby Koleje Mazowieckie Bi-Level Coach VI (1:100) £37.95	Piko 58807 HO Gauge Koleje Mazowieckie Bi-Level Driving Car VI (1:100) £55.95	Piko 58961 HO Gauge Expert SBB Pannono-Ethanol Funnel Flow Tank Wagon £42.95
Piko 59153 HO Gauge Expert Koleje Mazowieckie BR146.2 Electric Loco £99.95	Piko 59687 HO Gauge Expert DB B4ym 2nd Class Coach £41.95	Piko 59688 HO Gauge Expert DB AB4ym 1st/2nd Class Coach £41.95
Piko 59689 HO Gauge Expert DB B4ymf 2nd Class Control Coach £58.95	Piko 96377 HO Gauge Expert PKP EU06-08 Electric Loco £147.95	Roco 66069 HO Gauge ÖBB Stake Car Wagon Set (2) £49.95
Roco 66195 HO Gauge ÖBB Telescopic Hood Wagon Set (3) £38.95	Roco 67080 HO Gauge RENFE Side Discharge Hopper Wagon Set (3) £59.95	Roco 67082 HO Gauge DB Self Discharge Hopper Wagon Set (3) £62.95
Roco 67131 HO Gauge 2 piece set: Stake wagons DBAG £49.95	Roco 67151 HO Gauge 3 piece set: ÖBB Self-unloading hopper £59.95	Roco 67197 HO Gauge 3 piece set: DB Schenker dump wagons £69.95
Roco 67199 HO Gauge 3 piece set: DB Cargo dump wagons £69.95	Roco 72146 HO Gauge SNCB Serie 25 Steam Loco £219.95	Roco 72966 HO Gauge ZSSK 750 131-5 Diesel Loco £139.95
Roco 73679 HO Gauge Lokomotion EU43-007 Electric Loco £148.50	Roco 73876 HO Gauge SNCF BB7200 Electric Loco IV - DCC Sound £247.50	Roco 74393 HO Gauge BLS EW IV Baggage Coach £36.95
Roco 74560 HO Gauge SBB EW II 1st Class coach £47.70	Roco 74561 HO Gauge SBB EW II 1st/2nd Class coach £47.70	Roco 74562 HO Gauge SBB 2nd Class fast train coach EW II £46.95
Roco 74563 HO Gauge SBB 2nd Class fast train coach EW II £46.95	Roco 74564 HO Gauge SBB Baggage coach EW II £46.95	Roco 75959 HO Gauge 12 piece realistically weathered Railpro £274.95

# Mount Tabor Models

Come and visit Mount Tabor Models, the friendly model shop, based in the heart of the Peak District, where there is plenty for the rest of the family to do and see while you look round our shop.  
If you can't visit simply phone, Fax or E-mail your requirements.

## NEW IN STOCK

### TRIX HO BARGAINS AND NEW IN STOCK

T22433	Br24 + 2'2 'T 26 tender DB Ep3 MFX sound + smoke	£205.00
T22912	Br08 express loco DR Ep3 MFX smoke and sound	£495.00
T22905	Br01.5 reko express boxpok wheels MFX sound Ep3	£390.00
T22505	Br23 steam loco MFX smoke and sound DB Ep3	£310.00
T22292	Br94 heavy tank MFX Sound DRG Ep2	£245.00
T22119	Br100 Kof ii MFX Sound + telex couplers DR Ep4	£195.00
T22602	Vt 8.5 Paris-Ruhr 4 car Diesel railcar MFX sound Ep3	£495.00
T22981	Br44 heavy freight loco MFX Sound DB Ep3	£420.00

### JUST ANNOUNCED

### TRIX/MARKLIN DUE LATER

T22913	EST Class 13 French express loco MFX/DCC Sound Ep2	€650.00
T23219	5 car "Simplon Orient Express" coach set blue Sound Ep2	€560.00
T23220	3 car "Simplon Orient Express" coach set blue Sound Ep2	€310.00
M39243	EST Class 13 French express loco MFX Sound Ep2	€650.00
M42790	5 car "Simplon Orient Express" coach set blue Sound Ep2	€560.00
M42971	3 car "Simplon Orient Express" coach set blue Sound Ep2	€310.00

### BRAWA HO NEW RELEASES

B43236	BrE 75 DRG rod drive electric DC basic grey livery Ep2	£249.00
B43238	BrE 75 DRG rod drive electric Digital + grey livery Ep2	£380.00
B43240	BrE 75 DBAG rod drive electric DC basic grey livery Ep5	£249.00
B43242	BrE 75 DBAG rod drive electric Digital + grey livery Ep5	£380.00
B40860	Prussian G10 Freight loco DC Basic + green livery Ep1	£335.00
B40862	Prussian G10 Freight loco Digital extra green livery Ep1	£450.00
B40864	Br57.10 DB Freight loco DC basic + black livery Ep3	£335.00
B40866	Br57.10 DB Freight loco Digital Extra black livery Ep3	£450.00
B40868	Br57.10 DRG Freight loco DC basic + black livery Ep2	£335.00
B40870	Br57.10 DRG Freight loco Digital Extra black livery Ep2	£450.00

### BRAWA N GAUGE NEW RELEASES

B65009	Swiss open balcony bogie coach BC4 SBB, Ep2	£44.00
B65010	Swiss open balcony bogie coach C4 SBB/CFF, Ep2	£44.00
B65011	Swiss open balcony bogie coach C4 SBB/CFF, Ep2	£44.00
B65012	Swiss open balcony bogie baggage coach Ep2	£44.00

### SEE OUR TRADE STAND AT:

7th July German Railway Society, Devon Group Show, St Michael's Church Hall, Chudleigh Road, Kingsteignton, Newton Abbot, Devon TQ12 3NY

Shop open Tuesday and Thursday afternoons 1 pm till 5 pm other times by appointment.

Mail order continental model railway specialists.

Mount Tabor Models, Scarthin, Cromford, Matlock, Derbyshire DE4 3QF

Phone & Fax 01629 822294

E-mail: mount.tabor@btopenworld.com

# A LEGEND REBORN

CSD Reihe 324.1 (ex. K.K.St.B. Reihe 56)

100 pieces worldwide limited. || Four different liveries.

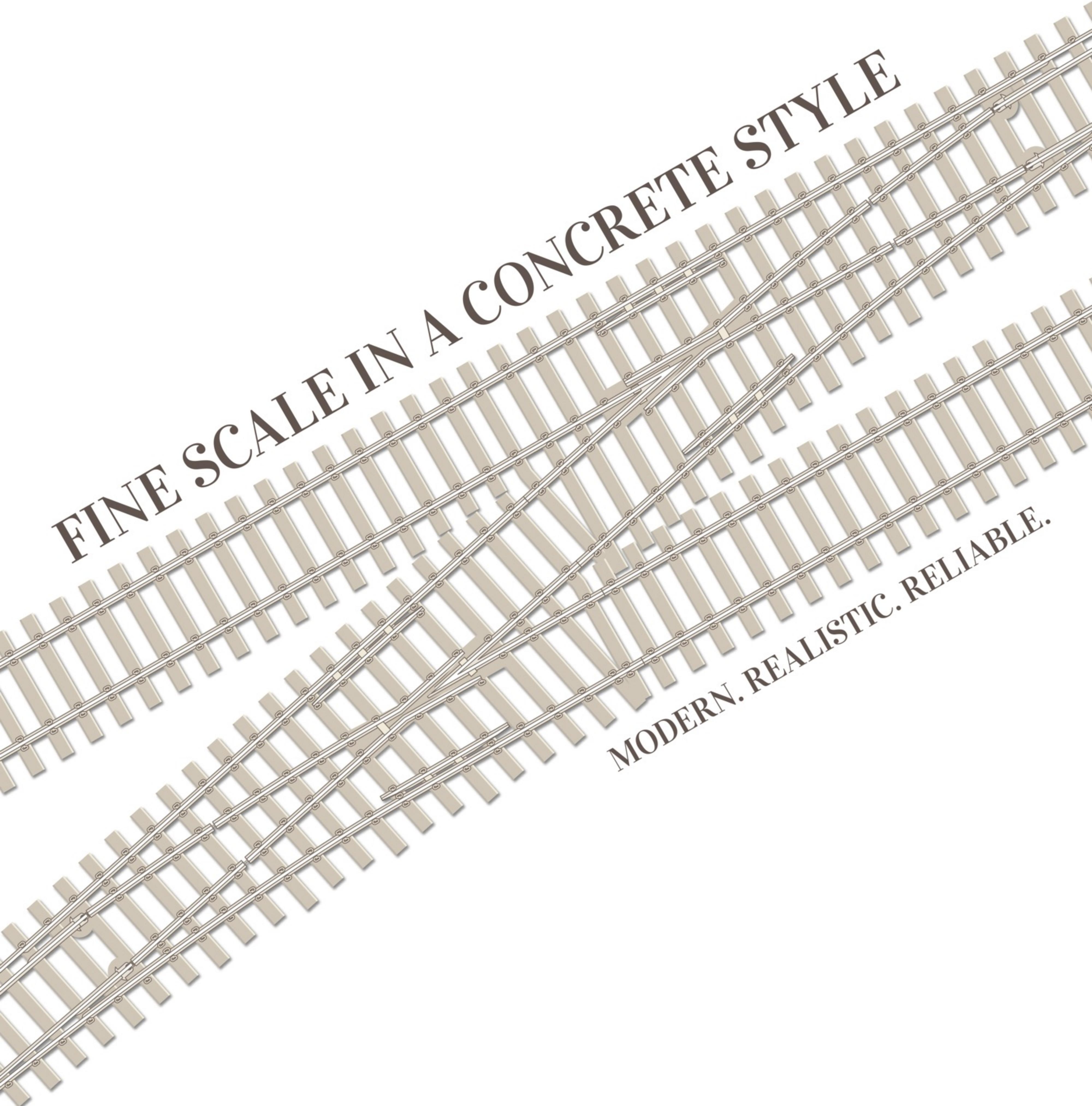
Delivery date: Q2/2019.



 **Eisenbahn**  
Canada

Eisenbahn Canada PO Box 88088 Richmond, BC V6X 2X0 Canada  
[www.eisenbahncanada.com](http://www.eisenbahncanada.com) | [sales@eisenbahncanada.com](mailto:sales@eisenbahncanada.com)

**FINE SCALE IN A CONCRETE STYLE**



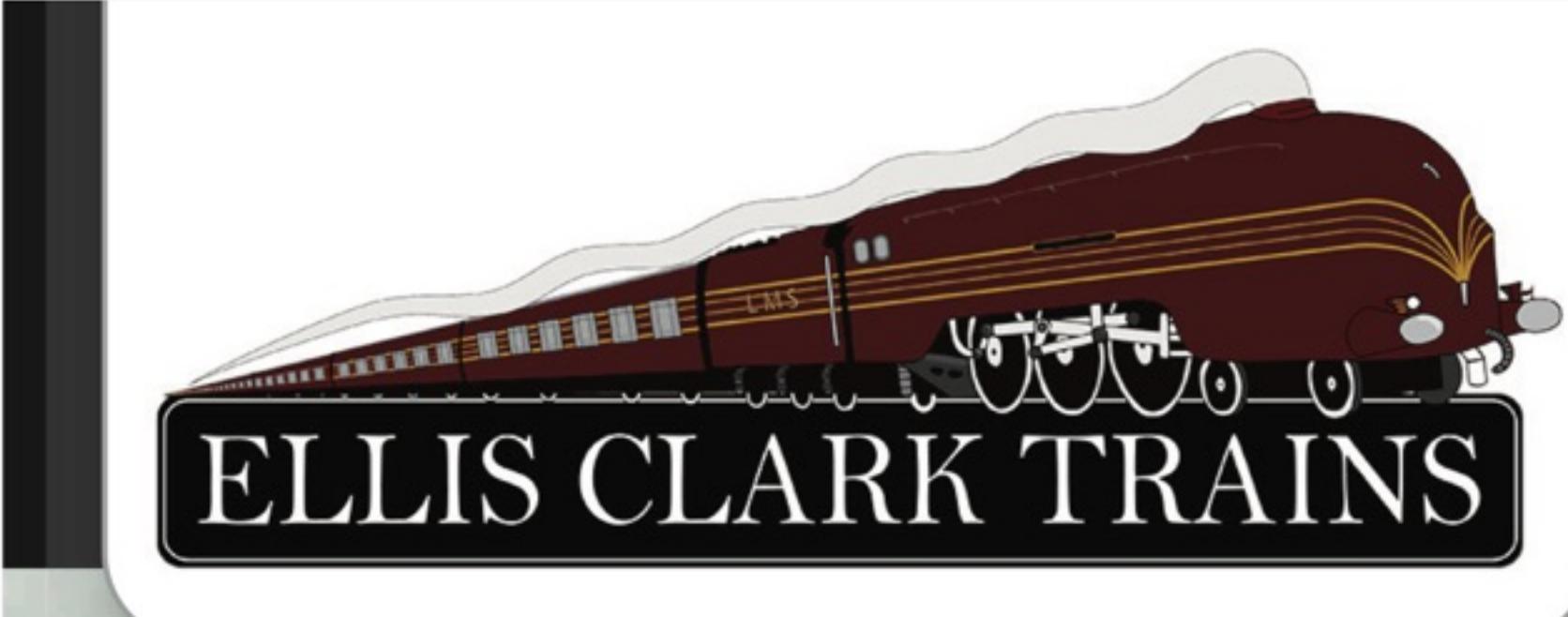
**MODERN. REALISTIC. RELIABLE.**

**PECO STREAMLINE HO CODE 75**

**MADE IN GREAT BRITAIN. ENJOYED THE WORLD OVER.**



Pritchard Patent Product Co. Ltd. Beer, Devon, EX12 3NA England Tel: 01297 626204 Fax: 01297 20229 [www.peco-uk.com](http://www.peco-uk.com)



Ellis Clark Trains  
Unit 1 Toller Court  
Shortbank Road  
Skipton, North Yorkshire BD23 2HG



Top prices paid for your collections  
of model railways, diecast & tinplate

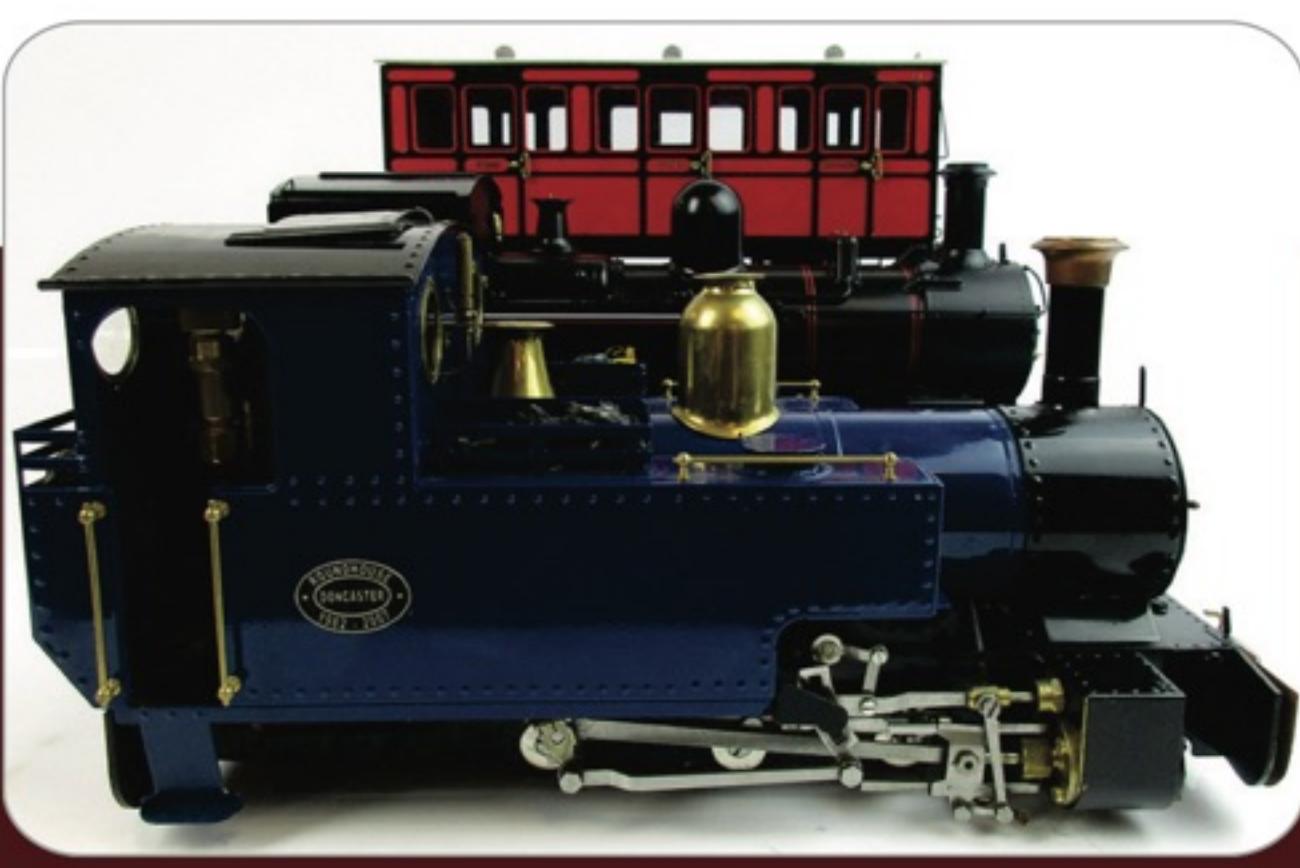
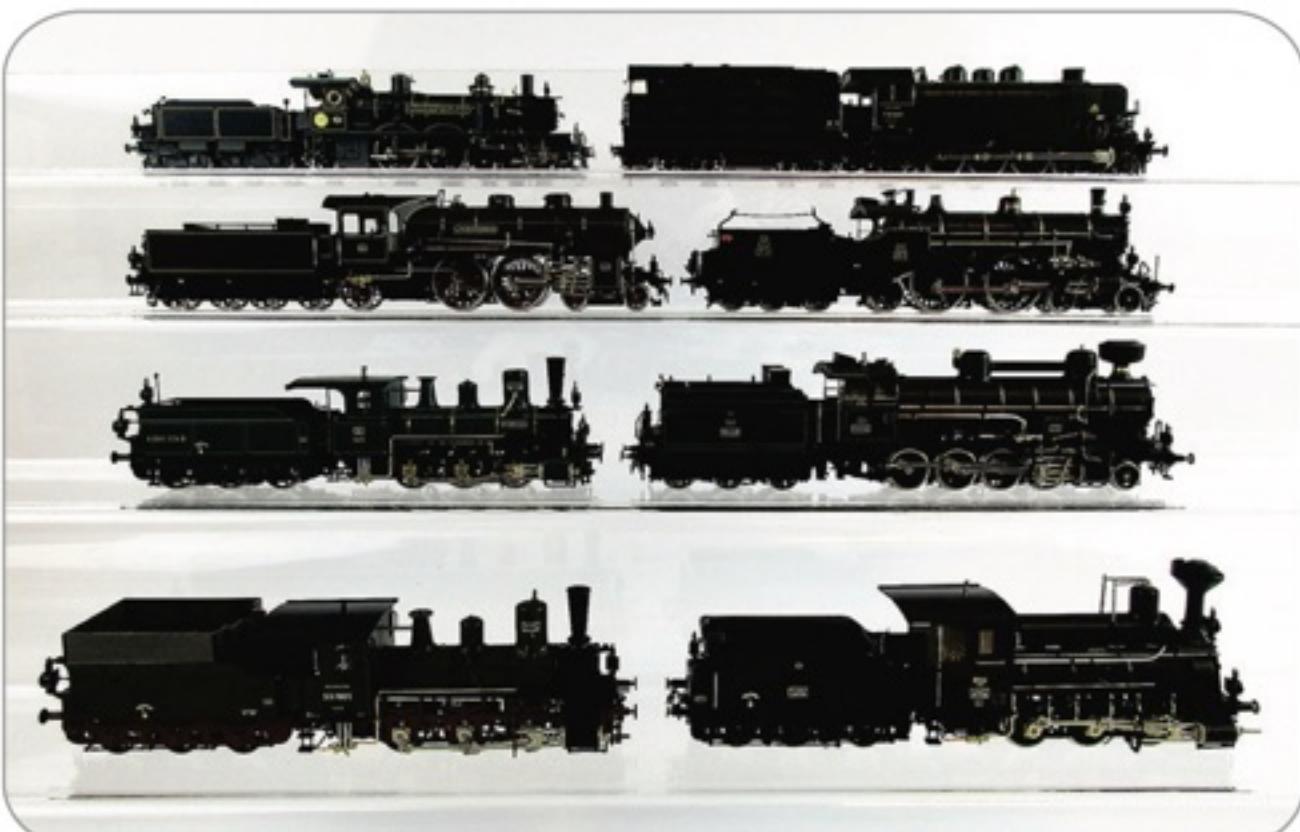
For more info call Ellis on 07799 554491

# WANTED

- ✓ Call, email or post a list
- ✓ All makes, gauges and ages considered
- ✓ British, Continental and American
- ✓ All gauges of kit/scratch built and live steam
- ✓ Diecast & tinplate cars, buses, planes, soldiers
- ✓ Payment in your preferred method
- ✓ Happy to travel nationwide to view & collect
- ✓ Friendly and professional service
- ✓ Large collections a speciality

T 01756 701451  
M 07799 554491  
E [ellis@ellisclarktrains.com](mailto:ellis@ellisclarktrains.com)  
W [ellisclarktrains.com](http://ellisclarktrains.com)

Buy at our  
**EBAY SHOP**  
Ellis Clark  
Trains



# CHOOSE YOUR SUBSCRIPTION OFFER

## OPTION 1

**14 ISSUES FOR THE PRICE OF £59.40**

*This offer not available with Direct Debit.*

*Both offers include:*

- ✓ **Delivery direct to your door**
- ✓ **Free Digital Edition Access\***

**UK only offers**

## OPTION 2

**12 ISSUES BY DIRECT DEBIT £48.06**

# CONTINENTAL MODELLER

JUNE 2019 £4.95 (UK)

FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

**Dietrichsdorf**  
German HO

CONTINENTAL  
MODELLER  
Celebrating  
40 years



\*Worth £39.99. Available for your PC, Mac, tablet or mobile device. Fully searchable back issues starting from Jan 2011. Access to digital content available only while subscription is active.

visit [www.pecopublications.co.uk](http://www.pecopublications.co.uk) or call 01297 626203

**STRICTLY MAIL ORDER.** Specialists in Top Quality European equipment.  
Postage £4 per order. Overseas at cost (regret no VAT relief). Sorry no Credit Cards, but check our prices! 2 IRC's with all overseas enquiries please.

**VISIT OUR WEBSITE:**  
[www.contikits.com](http://www.contikits.com)

**PLEASE NOTE:** Items shown are simply a selection of our stock which is constantly changing. It is impossible to list everything and it is always worth writing or giving us a ring even if you do not see what you want on this page.

A SELECTION OF NEW AND SECONDHAND STOCK.  
ALL EXCELLENT CONDITION. BRACKETED NUMBERS INDICATE CURRENT STOCK LEVEL.

NOTE: ub — unboxed, or in the case of locos in an incorrect box.

t/b — turquoise/beige.

Catalogue Numbers quoted are those which appear on the Model's box where applicable.

#### HO SCALE EUROPEAN

##### ADE

DB2nd/baggage BDUm 51 80 720 44-5, green

£10.00

41317 SBB Hups, brown

£22.00 (6)

41321 OBB Sdgmnss hupac

£22.00 (4)

41311 OBB Sdgmnss hupac

£20.00

AKU

10891 SBB K3 van, grey

£15.00

9007 SBB HBL5 van, VMB, blue

£15.00

9017 SBB Hups van, Jelmo, white/purple/green

£15.00

9022 BLS 4wh cereal hopper (Roco upgrade)

£18.00

9023 BLS 4wh cereal hopper (Roco upgrade)

£18.00

BRAWA

40796 BBO Br178.802 0-8-0T

£165.00

40604 BBO 175.15 0-8-0T, epII

£150.00

45912 DB B212-3 x boxe convert (Jubilee set) Ltd Ed

£285.00

44208 DB VT45 2-8-2 unit DMU, maroon, epII

£175.00

45540 DB 4wh Cvn-33 coach for Railcar, maroon, epII

£45.00

45541 DB 4wh CVN-33 coach for use with Railcar, maroon, epII

£45.00

41500 DB V90 052, maroon, epII

£125.00

42500 DB 733 767-4, Koffl, epIV, maroon/weathered

£75.00

0384 DB Car 216 102-4, red

£65.00

44302 DB 773 184-2 Twin railbus, two tone green/grey, epV

£115.00

41544 DB 90039, maroon, epII

£125.00

44300 DB LVT 772 342-2, red/white, epV

£110.00

0534 DB LVT 772 342 twin railbus, two tone green/grey

£110.00

(TC5 digital)

41090 DB Br179.159-1, red/weathered, Digital sound

£155.00

42718 DBAG Gravita diesel 9280 epII, red (Digital sound)

£235.00

43920 DBAG 146 216-7, electric, red (Digital sound)

£155.00

43916 DBAG 146 216-7, electric, red

£135.00

43926 DBAG Railion 185.2, epII, red, digital sound (#1874)

£145.00

41440 DBAG 185.1, red, epII

£65.00

42720 DBAG Gravita diesel 150, red, epII

£135.00

41016 DR 114 250-1, maroon/white stripe (Digital sound)

£125.00

44188 DR 137 329 Railcar

£175.00

0402 DR 229 100-3, red/cream stripe

£80.00

0410 DR 32 371-5, maroon/cream stripe

£80.00

0210 DR 15 02 4T, artic, green

£145.00

0201 DR 24 088-1 electric, maroon/white stripe

£75.00

42605 DR 115 223-0, 4T diesel, epII

£75.00

0530 DR VT17 twin railbus, maroon (TC5 digital)

£78.00

44212 DR 137 370 2 unit Railcar, green

£235.00

0210 DR E9 02, Artic Electric, green

£175.00

40430 DR Br19 1001 2-8-2 steamline, epII

£225.00

40200 DRG 06 002 Streamline 4-8-4

£245.00

43908 MRCE TRAXX 85.2, black, epII, DCC fitted (#5717)

£100.00

40780 OBB 92 2250, 0-8-0T

£165.00

41632 OBB 2065.01, diesel, green

£115.00

0590 RWE Energy Kof, yellow

£55.00

42876 Bahnhof 213 332-2 diesel yellow, epV

£125.00

42854 Wube 211 digit. Digital extra sound

£185.00

45633 BBO 3 x 4wh coaches, brown

£95.00

45534 BBO 3rd 4wh, blue/cream

£35.00

45649 BBO 3rd 4wh, green

£35.00

45650 BBO 3rd 4wh, green

£35.00

45600 DB Gepäckwagen, PwI, epII

£25.00

46170 DB Wg4u-36-50 Gesellschaftswagen, maroon, epII

£45.00

46172 DB Belpassagenwagen B4, green, epII

£45.00

46173 DB Belpassagenwagen B4, green, epII

£45.00

46302 DR Pw 6wh Cogé/CW4, green, epII

£78.00

46301 DBAG 1st/2nd epII/407.1, red, epII

£90.00

46007 DR 00212, centre entrance, green/cream, epII

£90.00

45630 DR 00212, 4wh, green, epII

£125.00

42545 DR 00212, 4wh, green, epII

£125.00

42546 DR 00212, 4wh, green, epII

£125.00

42547 DR 00212, 4wh, green, epII

£125.00

42548 DR 00212, 4wh, green, epII

£125.00

42549 DR 00212, 4wh, green, epII

£125.00

42550 DR 00212, 4wh, green, epII

£125.00

42551 DR 00212, 4wh, green, epII

£125.00

42552 DR 00212, 4wh, green, epII

£125.00

42553 DR 00212, 4wh, green, epII

£125.00

42554 DR 00212, 4wh, green, epII

£125.00

42555 DR 00212, 4wh, green, epII

£125.00

42556 DR 00212, 4wh, green, epII

£125.00

42557 DR 00212, 4wh, green, epII

£125.00

42558 DR 00212, 4wh, green, epII

£125.00

42559 DR 00212, 4wh, green, epII

£125.00

42560 DR 00212, 4wh, green, epII

£125.00

42561 DR 00212, 4wh, green, epII

# •CONTIKITS•

## STRICTLY MAIL ORDER

Continued from the previous page

50000728 L+N 90T Hopper #182720, brown	£5.00 (4)
50000729 L+N 90T Hopper #182778, brown	£5.00
39846 L+N C+O Cupola Caboose #912, red	£8.00
30861 Lanza Inc (ACFX) 23500 Gal Tank #75446, black	£10.00
5000956 Maine Cent 40' PS-1 box car, brown	£6.00
38958 Maine Cent 50' Single door box car, yellow	£7.00
PPR-1 Manitou Pikes Peak railway Caboose #5, red	£9.00
32645 N+W 90T Hopper #12575, black	£5.00 (3)
50001845 N+W 90T Hopper #12100, black	£8.00
50000978 N+W St Cupola Caboose #518553, red	£12.00
50000979 N+W St Cupola Caboose #518595, red	£12.00
001-881603 Wisc Cent 50' Auto Box #20001, red	£7.00
43031 N+W Standard Cupola caboose, blue	£7.00
43032 N+W Standard Cupola caboose, blue	£7.00
43072 N+W Standard Cupola caboose, red	£7.00
43071 N+W Standard Cupola caboose, red	£7.00
31091 Northwest hardwoods Evans DPD Box #13150, yellow	£8.00
43048 NP St Cupola Caboose #1101, brown	£12.00
43099 NS (Horse Head) St Cupola Caboose #555518, red	£12.00
50002163 NS 2 Bay Offset Side Hopper #9114, brown	£7.00
33792 NS 50' FGE Box #650008, brown	£5.00
50000980 NS St Cupola Caboose #555048, red	£12.00
50000981 NS St Cupola Caboose #555589, red	£12.00
34574A NYC 40' PS-1 box, green	£7.00
50001811 Penn Central 40' Plug Door box, green	£6.00
50001812 Penn Central 40' Plug Door box, green	£6.00
31142 Plywood Marketing Evans DPD box, orange/yellow #10557	£8.00
50002497 Roberval Saguenay 1932 ARA Box #1001, grey	£10.00
32993 Seaboard Line 90T hopper, brown	£7.00
35981 SF Caboose #1783, brown	£7.00
50002630 SOO Line 70T Ore Car #81903, brown	£7.00
50002631 SOO Line 70T Ore Car #81919, brown	£7.00
50002218 Soo Line Thrall 2743 Gondola #69101, silver/red	£12.00
50000738 Southern 90T Hopper #360301, brown	£5.00
50000739 Southern 90T Hopper #360384, brown	£5.00
32722 Southern 90T Hopper #360560, brown	£5.00
32723 Southern 90T Hopper #360563, brown	£5.00
50002364 SP 50' SD Box #651400, brown	£7.00
32442 Standard Chemical Beer Can Tank #509, red	£7.00
50000085 Thenderson ACF 5250 Hopper Car #9804, grey	£8.00
32165 Thunder Bay Mining 70T Ore Car #721, brown	£6.00
50002594 UP 40 Cupola Caboose #903002, green	£8.00
35625 UP Cupola Caboose #3529, yellow	£8.00
39995 UP Line 70T Ore Car #26469, silver	£6.00
43804-40 Utility Fuels Coalveyer, black/orange	£8.00
3963 WC ACF 4 Bay Centreflow #83058, grey	£8.00
3735 West Pacific 3 Bay CVD Hopper, grey	£7.00
50001424 Weyerhaeuser 53' Double Plug door box car, green	£9.00
43807-5 Wisconsin Electric Power Coalveyer, black/yellow	£8.00
31422 Wyandotte Chemicals Beer tanker, black	£7.00
<b>BLUFORD SHOPS</b>	
17041 CP 3 Bay Offset Side Hopper + woodchip Extensions	£14.00
14303 Clinchfield (Post 1967) 14 Panel Rib Side Hopper, black	£10.00 (3)
14301 Clinchfield (Post 1967) 14 Panel Rib Side Hopper, black	£10.00
93303 Southern NE-6 Caboose #X252, red	£7.00
<b>CONCOR (KD STYLE COUPLINGS)</b>	
0001-004020 B+O Smoothside x 5 car set, blue/grey	£40.00
003-540005 BN Smoothside 5 car set, grey/blue	£40.00
0001-041008-2 D+H 7 x coach + box car set, blue/grey/yellow	£70.00
4081K EL Baggage, grey/maroon	£12.00
4001K EL coach, grey/maroon	£12.00 (2)
4071K EL Diner, grey/maroon	£12.00
4061K EL Dome, grey/maroon	£12.00

See us at the following events:  
**29th & 30th June @ Perth MRS, 7th July @ Hampsthwaite Summer Show and 27th & 28th July @ Railex NE.**  
 Let us know what you would like to see and we will happily bring it along, no obligation to buy! PLEASE give a few days notice. We are happy to buy or part exchange, just let us know what you intend to bring.

4021K EL RPO, grey/maroon	£12.00	176-294 UP E8/9B #935B, yellow	£45.00	46470 Pennsy gondola, brown	£9.00
4011K EL sleeper, grey/maroon	£12.00	126-0401 UP FEF-3 #844 4-8-4	£150.00	48610 Pennsy 50' St side gondola, brown	£9.00
4072E ERIE Diner, blue/grey	£12.00	176-5506 UP SD70 #4841, yellow	£50.00	55030 C+O 33' Twin hopper, black	£9.00
4022E ERIE RPO, blue/grey	£12.00	176-5609 UP SD90/43MAC #8039, yellow	£55.00	65050 Transcontinental Tank car 39' single door, silver	£9.00
4012E ERIE sleeper, blue/grey	£12.00	176-5606 UP SD90/43MAC #8105, yellow	£55.00	35077 ATSF (Herald) 40' stock car, brown	£9.00
0001-20204 GN Alco PA1, orange/green	£35.00 (2)	106-014 UP Smooth Side Coach, 6 car set, yellow	£80.00	34472 Pennsy 50' St Box, d door, brown	£9.00
0001-004040 GN Empire Builder x 5 car set, orange/green	£40.00	106-085 UP Water Tender 2 car set, yellow/yellow with flag	£50.00	29170 NP 40' Outside Braced Box car, brown	£9.00
0001-04024 IC Smoothside coach set X5, orange/brown	£40.00	176-6008 VIA Rail P42 #906 green/silver/yellow (DCC fitted)	£55.00	43510 LV 40' D.Sheathed Box, brown	£9.00
0001-004030 MP Smoothside 5 car set, grey/blue	£40.00	106-097 VIA Rail Smooth Side 6 car set, blue/yellow	£90.00	33451 WM 50' St. Box, brown	£9.00
0003-54006 RI coach/Comb/Dome/Coach/Obs set, silver	£40.00	106-015 VIA Rail Smooth Side 6 car set, blue/yellow	£90.00	47070 Swift refrig 40' box, yellow	£9.00
0001-004002 SF Valley Flyer x 5 car set, silver	£40.00	106-1801 Wabash 4 x Corrugated coach set, silver/blue	£60.00	28288 SP+S 40' Outside Braced box, brown	£9.00
0003-570007 Southern Pacific Heavyweight x 6 coach set, orange/red	£50.00	106-4620 CSX 8 x Bethgon Coalporter	£75.00 (2)	65062 UTIX 39' Single Dome tank, black	£9.00
001-160102-4 Tru-Sweet 40ft tank #17525, black	£8.00	106-4604 UP 8 x Bethgon Coalporter	£75.00	79010 Pennsy 50' wagon top box, brown	£9.00
001-556103-4 UTIX 60ft Tank #64728, blue	£9.00	106-4607 CHTT 8 x Bethgon Coalporter	£75.00	50620 Pennsy 34' wood sheathed caboose, brown	£10.00
0001-040045 VIA Rail 85ft coach #2239, blue/yellow	£15.00	106-4609 UP 8 x Bethgon Coalporter	£75.00	100020 MP 36' rivet steel side caboose, red	£9.00
0001-040028 VIA Rail 85ft coach #2246, blue/yellow	£15.00 (2)			02000987 CP 40' St Box, Single door #29110	£14.00
0001-040728 VIA Rail 85ft Diner, Diamond Lake, blue/yellow	£15.00			04500280 CP 50' Flat, fishbelly with log loads	£14.00
0001-040128 VIA Rail 85ft Pullman, Riverview, blue/yellow	£15.00			105110 CP 50' Steel side Fixed end Gondola #340222	£12.00
001-881603 Wisc Cent 50' Auto Box #20001, red	£7.00			74020 CP Mandarin Orange Exp 40' St Box, Plug Dr w/o Roof walk	£12.00
<b>DELUXE INNOVATION</b>				10500120 CSX Transportation 50' Steelside 14 Panel Gondola	£12.00
15040K-A Sealand/NYSW 5 unit Artic (For twin stack conts), red	£35.00			77080 D+H 'I Love New York' 50' Standard Box, white/blue	£9.00 (2)
15040M-B Sealand/NYSW 5 unit Artic (For twin stack conts), red	£35.00			11100121 Ferromex 89' Tri-Level Closed Auto rack, yellow/silver	£18.00
15020MA SF 5 unit Artic (For twin stack conts), yellow	£35.00			11100122 Ferromex 89' Tri-Level Closed Auto rack, yellow/silver	£18.00
<b>FOX VALLEY MODELS</b>				110050 GN 56' Tankcar, black	£12.00
70463 NS GE-ES-H #8093, black	£55.00			100120 Hershey 36' Rivet Steel Caboose Offset Cupola, brown	£12.00
<b>HALLMARK BRASS (BY SAMHONGSA)</b>				#4011	£12.00
GP30 Standard Low Nose (undecorated)	£115.00			07300090 IC 40' St Box, single door, brown	£9.00
Pennsy Baldwin Centipede, dark green/yellow stripe	£185.00			69170 Klondikes Whitemint Rivet side reefer, silver	£8.00 (2)
<b>INTERMOUNTAIN</b>				56380 KO+G 33' Twin Bay Hopper, black	£8.00
67209-01 ADM Trinity 5161 Cuff Hopper #52108, grey	£12.00			77130 L+N 50' box Single door, wo roofwalk, blue	£10.00
60104-05 ALPX Cylindrical Cov Hopper #628330, blue	£9.00			03200430 Libby's Famous Foods 50' St Box, turquoise	£11.00
AMTRAK MOW diesel fuel oil tank, orange	£14.00			110030 Macintyre Fuels 56' Tankcar, black	£12.00
RN-17515 BR Columbia Thrall All Door Box, green, 3 pack	£30.00			10100020 Milw Rd 40' Hy-Cube box, brown	£12.00 (2)
65202-42 CN Environmental Cylindrical Cov Hopper #370896, grey	£12.00			24200 MP 40' Standard Box car, brown	£7.00
69301-03 CN SD40-2W, red/black/white	£70.00			92050 N+W 2 Bay ACF Cov Hopper, grey	£10.00
65327-03 GATX 3 Bay hopper, white	£14.00			05900140 Needham Packing Co 40' box, blue	£9.00
65324-28 GTW PS 3 Bay Ribbed Hopper #138245, blue	£12.00			03200190 NH 50' box, Plug door, red	£10.00
65315-09 Illinois Central Gulf 4750 Cu Ft 3 Bay hopper, orange	£15.00			NMRA 60th Anniversary MP Box #266929, brown	£15.00
65207-16 Potacan cov hopper, orange (all diff numbers)	£15.00			10100010 NP 40' Hy-Cube Box, green	£11.00
65207-16 Potacan Cylindrical Cov Hopper #850, orange	£12.00			02100170 NP 40' St Box, Plug door, green	£10.00
67074-01 Prairie Malt 3-Bay Hopper, ACF #1003, brown	£12.00			09200240 NS 2 Bay ACF Centre flow cov hopper, grey	£10.00
91426 Southern (Green Letters) ACF Centre Flow 2 Bay Hopper, grey	£10.00			75110 NS 50' Box, double plug doors, wo roofwalk, brown	£8.00
65232-03 UNPX Alcan'Chemical Cylindrical Cov Hopper #121405, yellow	£12.00			11100071 NS 89' Tri-Level Closed Auto rack, brown/silver	£18.00
RN-17406-3 US Plywood Box, green	£10.00			7517 NP SW9/1200 #138	£32.00
<b>KATO</b>				7514 NRP SW9/1200 #240	£32.00
106-6282 AMFleet, Viewliner IC Express phIV, silver/blue	£45.00			7079 NYC PA #4200, two tone grey	£30.00
AMTRAK F40PH #334, silver/red/blue (DCC fitted)	£45.00			7041 PRR E7A #5856B, Tuscan/yellow stripes (Dummy)	£15.00
176-6102 AMTRAK F40PH phill #376, silver/red/blue (DCC fitted)	£45.00			7040 PRR E7A #5863A, Tuscan/yellow stripes (Dummy)	£15.00
106-011 AMTRAK Smooth side 6 car set, silver/red/blue	£90.00			7039 PRR E7A #5894, Tuscan/yellow stripes	£30.00
106-7122 AMTRAK Superliner 2 car set, phill (Set B) silver/red/blue	£30.00 (2)			7059 PRR PA #5752A, Tuscan/yellow stripes	£30.00
106-3511 AMTRAK Superliner 4 car set, phase					



# Downunder's Best European Model Shop



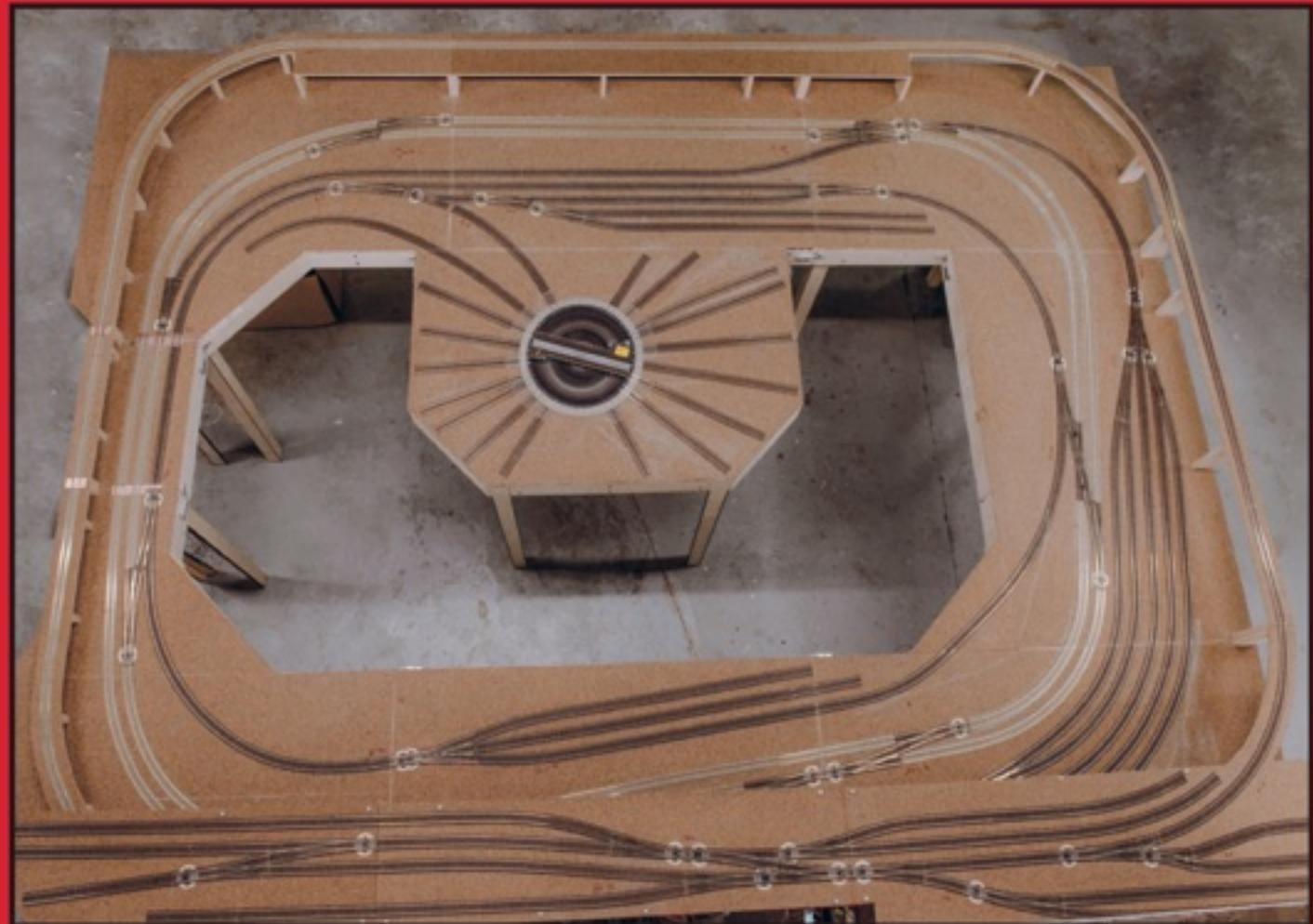
**ONLINE • SHOWROOM • PHONE**  
**[www.allaboardexclusive.com.au](http://www.allaboardexclusive.com.au)**  
**42 years of service in the model railways business in Australia**

68-72 Old Hume Hwy Nth Mittagong/Braemar NSW 2575. Email: [vic@allaboard.com.au](mailto:vic@allaboard.com.au)  
PO Box 388 Mittagong 2575. Ph: (02)4871 2966. Open 9.30-4.30. Closed Sundays, Public Holidays



**White Rose Modelworks**  
**Unit 10, Bedale Station,**  
**The Bridge, BEDAILE,**  
**North Yorkshire DL8 1BZ**  
**Tel: +44 (0)1677 422444**

**Check our new website:**  
**[www.whiterosemodelworks.co.uk](http://www.whiterosemodelworks.co.uk)**  
**Email your enquiries:**  
**[info@whiterosemodelworks.co.uk](mailto:info@whiterosemodelworks.co.uk)**



**Modular Laser Cut Baseboards**  
**40 Standard Sizes to Suit Your**  
**Space & Needs**  
**Control Shelves and Backscenes**  
**Unique Plug and Latch System for**  
**Easy Joining**

**Laser Cut Helixes in 4 Standard Sizes**  
**Also Stretchered & Bespoke Helixes**  
**All Baseboards & Helixes Made from**  
**Baltic Birch Plywood**

**Tracklaying, Electrics and Scenics**  
**Control Systems for DCC & Analogue**



**Available in Kit Form, Built-up Boards**  
**or Fully Assembled Railway**

## INDEX TO ADVERTISERS

### A

All Aboard, Australia.....33a  
[www.allaboardexclusive.com.au](http://www.allaboardexclusive.com.au)  
American Railroad Centre, Cornwall.....25a  
[www.americanrailroadcentre.com](http://www.americanrailroadcentre.com)

### B

Bachmann Europe Plc .....35a  
[www.bachmann.co.uk](http://www.bachmann.co.uk)  
Des Browne.....21a  
email: [browne151@btinternet.com](mailto:browne151@btinternet.com)

### C

C&M Models, Carlisle .....25a  
[www.candmmodels.co.uk](http://www.candmmodels.co.uk)  
Contikits (m).....32a & 33a  
[www.contikits.com](http://www.contikits.com)  
Continental Modeller Subscriptions.....31a  
Conwy Valley Railway Museum .....23a  
[www.conwyrailwaymuseum.co.uk](http://www.conwyrailwaymuseum.co.uk)

### D

DCC Supplies.....24a  
[www.dccsupplies.com](http://www.dccsupplies.com)

### E

Editorial .....3a  
Eisenbahn, Canada.....28a  
[www.eisenbahncanada.com](http://www.eisenbahncanada.com)  
Ellis Clark Trains .....30a  
[www.ellisclarktrains.com](http://www.ellisclarktrains.com)  
Enchanted Garden .....21a  
European Railways Association.....21a  
[www.eurorail.org.uk](http://www.eurorail.org.uk)  
Exeter 2019 Exhibition .....24a  
[www.exemrs.co.uk](http://www.exemrs.co.uk)

### F

Features .....4a & 5a  
French Railways Society.....21a  
[www.frenchrailwaysociety.org](http://www.frenchrailwaysociety.org)  
Frizinghall Models & Railways.....22a  
[www.modelrailshop.co.uk](http://www.modelrailshop.co.uk)

### G

Gaugemaster/Faller.....OBC  
Gaugemaster/Marklin.....7a  
Gaugemaster/Noch.....6a  
Gaugemaster/Piko .....8a  
Gaugemaster/Preiser .....9a  
Gaugemaster/Roco.....10a  
Gaugemaster/Trix.....11a  
[www.gaugemaster.com](http://www.gaugemaster.com)

German Railway Society.....21a  
[www.grs-uk.org](http://www.grs-uk.org)  
Golden Valley Hobbies.....22a  
[www.goldenvalleyhobbies.com](http://www.goldenvalleyhobbies.com)

### H

Hattons Model Railways .....14a  
[www.hattons.co.uk](http://www.hattons.co.uk)  
Hobby Shop Faversham .....25a  
[www.hobb-e-mail.com](http://www.hobb-e-mail.com)  
Hornby Hobbies International .....13a  
[www.hornby.com](http://www.hornby.com)

### K

Kent Garden Railways.....25a  
[www.kgrmodels.com](http://www.kgrmodels.com)  
Kernow Model Rail Centre, Cornwall .....15a  
[www.kernowmodelrailcentre.com](http://www.kernowmodelrailcentre.com)  
Kittle Hobby, Swansea.....2a  
[www.kittlehobby.com](http://www.kittlehobby.com)

### L

Lendons of Cardiff.....21a  
[www.lendons.co.uk](http://www.lendons.co.uk)

### M

Mac's Model Railroading, Kirriemuir .....24a  
[www.macsmodels.co.uk](http://www.macsmodels.co.uk)  
Marcway Models & Hobbies, Sheffield .....21a  
[www.marcway.co.uk](http://www.marcway.co.uk)  
Marno Ltd .....18a  
[www.marnoltd.co.uk](http://www.marnoltd.co.uk)  
MECH Models, Burton on Trent .....26a  
[www.mech-models.com](http://www.mech-models.com)  
mkn Digital/Marklin Repairs .....22a  
email: [mkn\\_digital@bigpond.com](mailto:mkn_digital@bigpond.com)  
Model Junction .....22a  
[www.modeljunction.info](http://www.modeljunction.info)  
Modelrail.co .....22a  
[www.modelrail.co](http://www.modelrail.co)  
Modratec, Australia.....21a  
[www.modratec.com](http://www.modratec.com)  
Morris Models, West Sussex .....24a  
[www.morris-models.co.uk](http://www.morris-models.co.uk)  
Mount Tabor Models, Matlock .....27a  
email: [mount.tabor@btopenworld.com](mailto:mount.tabor@btopenworld.com)

### N

Newsagents Form .....23a  
The NMRA British Region .....21a  
[www.nmrabr.org.uk](http://www.nmrabr.org.uk)

### O

Osborn's Models .....23a  
[www.osbornsmodels.com](http://www.osbornsmodels.com)

### P

PECO Code 75 .....29a  
PECO Heritage Railways .....12a  
PECO Modellers Library .....23a  
PECO Static Grass .....page 457  
PECO Strictly Continental .....21a  
PECO Subscriptions .....31a  
PECO Tracksetta .....18a  
PECORAMA .....26a

### R

Rails of Sheffield .....16a & 17a  
[www.railsofsheffield.com](http://www.railsofsheffield.com)

### S

Scograil, Ipswich .....27a  
[www.scograil.co.uk](http://www.scograil.co.uk)  
Swiss Railways Society .....21a  
[www.swissralsoc.org.uk](http://www.swissralsoc.org.uk)

### W

White Rose Modelworks .....34a  
[www.whiterosemodelworks.co.uk](http://www.whiterosemodelworks.co.uk)



**WOODLAND  
SCENICS®**

# **FIELD SYSTEM**

**A complete system for modelling  
natural fields and grassy locations**

The **Field System** makes it easy to add texture and dimension while modelling meadows, fields and pastures with incredible realism. A wide range of colours and textures allows you to model any season or grassy landscape. All products are designed to work together, and each one includes simple-to-follow instructions to make modelling easy.

## **Static King™** WFS639

Static King makes it easy to model large grassy areas and ensures Static Grass is applied vertically. The Static King has an ergonomic design and can be placed upside down for easy filling. It comes with different Sieve sizes for creating a varied effect using 2-12mm lengths of Static Grass. A divider allows the use of multiple colours and sizes at the same time for modelling realistic fields and other tall grasses.

- Clear hopper
- 360° Power Indicator Light
- Power via Power Supply or Batteries
- Non-slip rubber grip

- Divider allows use of multiple colours & sizes
- Easily interchange twist-lock Sieve
- Automatic discharge after each use



### Also Available

- **Static Grass** in a variety of lengths and colours
- **Field Grass** to model tall grass, field grass and weeds
- **Flowers** includes four shades of colour
- **Plant Hues** includes four shades of colour
- **Briar Patch** to add brambles, thickets and shrubbery
- **Static-Tac, Tuft-Tac & Spray-Tac**
- **Model-Vac** for picking up loose landscape material
- **Accent Shakers** help easily apply **Plant Hues & Flowers**



## **AVAILABLE IN THESE STORES:**

AGR Model Railway Store - Bedfordshire  
Model Railways Direct - Bristol  
Berkshire Dolls House Company - Berkshire  
Trident Trains Model Shop - Cheshire  
Peters Spares Model Railways - Cleveland  
Kernow Model Rail Centre - Cornwall  
Broughton Newsagents - Cumbria  
Jackson's Models & Railways - Cumbria  
Rhuddlan Models - Denbighshire  
Malc's Models - Derbyshire  
A2B Model Railways - Derbyshire  
South Devon Railway - Devon  
Youings Toymaster Model Shop - Devon  
Shady Lane - Devon  
Buffers Model Railways - Devon  
Colletts Model Trains - Devon  
Setchfield's - Dorset  
Model Railway Solutions - Dorset  
Bluebell Railway - East Sussex  
Train Times - East Sussex  
John Dutfield - Essex  
Flair Rail - Essex  
Cheltenham Models - Gloucestershire  
Burton Model Railways - Gloucestershire  
Sawyer Models - Greater Manchester  
Arcadia International Model - Greater Manchester  
Ron Lines - Hampshire  
AC Slotcars - Hampshire  
Test Valley Models - Hampshire  
Alton Model Centre - Hampshire

The Model Shop - Hampshire  
Wicor Models - Hampshire  
Upstairs Downs - Isle Of Wight  
Mark's Models - Ireland (Dublin)  
Times Past Antiques - Ireland (Louth)  
Hobby Shop - Kent  
Rochester Games & Model - Kent  
Kent Garden Railways - Kent  
Gravesend Games & Models - Kent  
Maidstone Games & Models - Kent  
The Signal Box - Leicestershire  
Ashtree Enterprises - Lincolnshire  
DB Models - Lincolnshire  
Mad About Trains - Lincolnshire  
Digitrains - Lincolnshire  
MRSG - Lincolnshire  
DKG Hobbies - Merseyside  
Bure Valley Railway - Norfolk  
Great Eastern Models - Norfolk  
A & H Models - Northamptonshire  
The Railway Conductor - Northamptonshire  
J & J Models - Northumberland  
Arcane Scenery and Models - Nottinghamshire  
DJ & F Benham - Nottinghamshire  
The Goods Yard Ltd - Nottinghamshire  
Trains4U - Peterborough  
Macs Model Railroading - Scotland (Forfar)  
KMS Railtech - Scotland (Aberdeenshire)  
Scoonia Hobbies - Scotland (Fife)  
Hollylovestoys - Somerset

Frome Model Centre - Somerset  
Topp Trains - Staffordshire  
Model Junction - Suffolk  
Flair Rail - Suffolk  
Orwell Model Railways - Suffolk  
Kernow Model Centre - Surrey  
KDC Hobbies - Wales (Ammanford)  
Pontypool & Blaenavon Railway - Wales (Blaenavon)  
Rhuddlan Models - Wales  
Lord & Butler Model Railways - Wales (Cardiff)  
Firestorm Games - Wales (Cardiff)  
Tony's Trains at Barby Model Rail - Warwickshire  
Our Place - Warwickshire  
Ian Allan Ltd Birmingham - West Midlands  
Hobbyrail - West Midlands  
Going Loco - West Midlands  
Salisbury Model Centre - Wiltshire  
Spot On Models - Wiltshire  
Green Scene - Worcestershire  
Worcester War Games - Worcestershire  
DMB/Footplates - Worcestershire  
Redcar Models & Hobbies - Yorkshire (Redcar)  
Peters Spares Model Railways - Yorkshire (Middlesbrough)  
The Model Centre (TMC) - Yorkshire (Whitby)  
Panda Models - Yorkshire (Doncaster)  
Rails of Sheffield - Yorkshire (Sheffield)  
Monk Bar Model Shop - Yorkshire (York)  
Millennium Models - Yorkshire (Leeds)

Please visit [www.bachmann.co.uk](http://www.bachmann.co.uk) to find a retailer and view tutorial videos

Model • Collect • Create





Faller are world famous for their plastic building kits. With a huge range in different scales of Continental-style buildings, any layout will benefit from the inclusion of Faller kits.

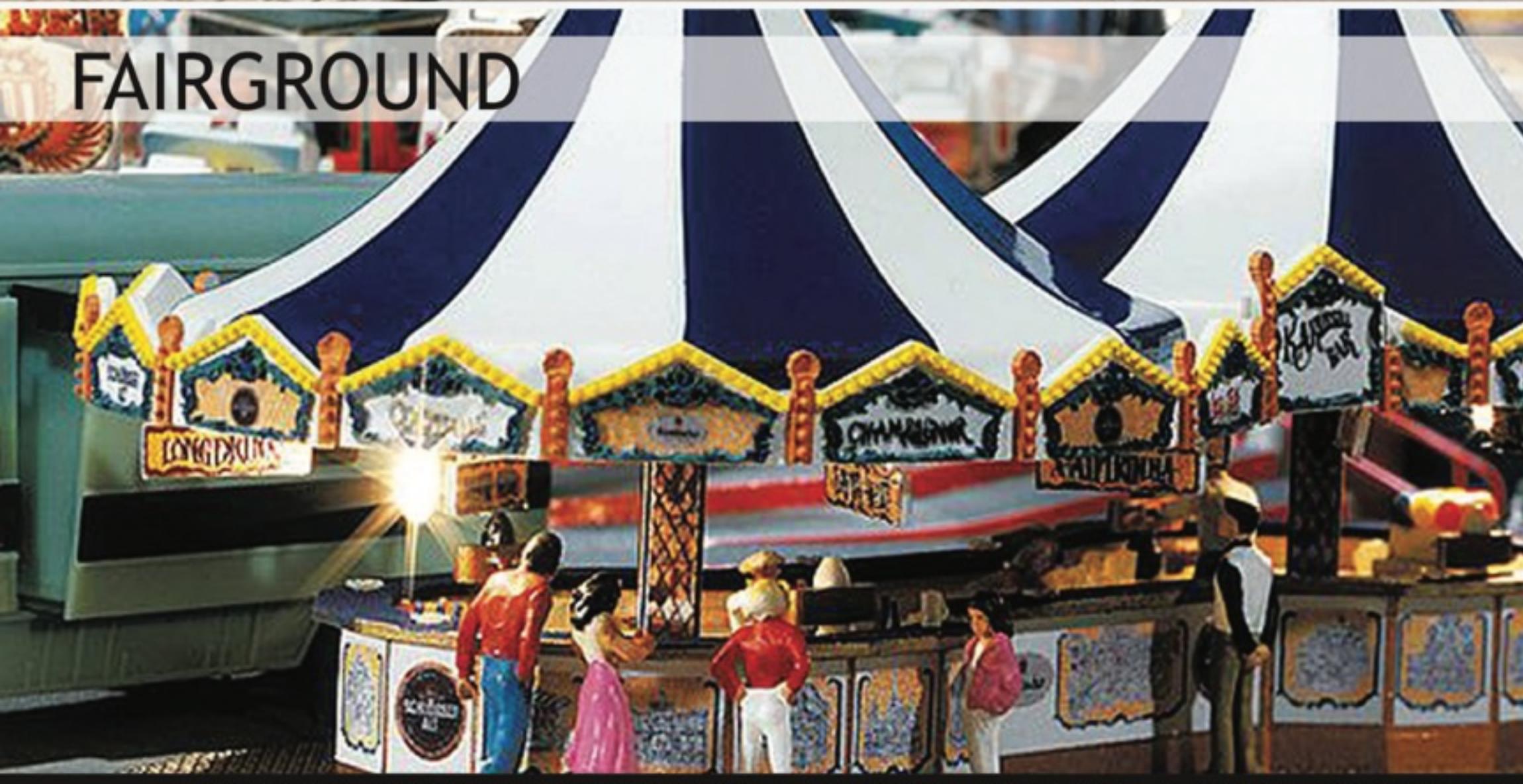
To compliment the buildings, Faller also produce a range of figures and accessories to enhance the atmosphere of any street scene, as well as trees and other scenic items to add to town and countryside.

## CAR SYSTEM

The CAR SYSTEM range allows you to have moving vehicles on your layout with no visible tracks or wires. Automate junctions, traffic lights, and even speed cameras!



## FAIRGROUND



The Faller Fairground range is produced in N and HO Scale, and is almost a separate hobby in itself! You can add sound and movement to your layout with interesting rides and attractions.

