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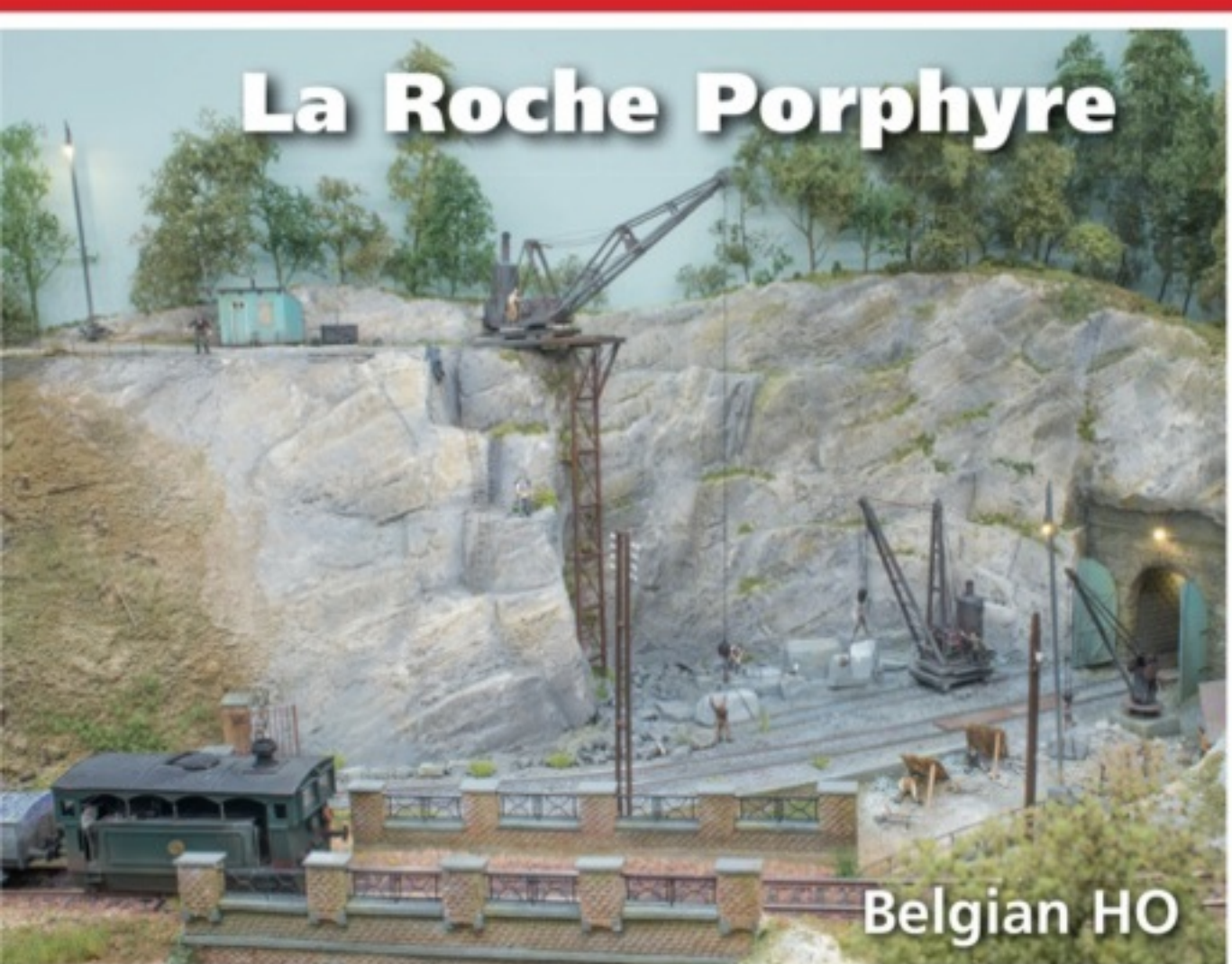
VOLUME 41

Nordhafen

German HO



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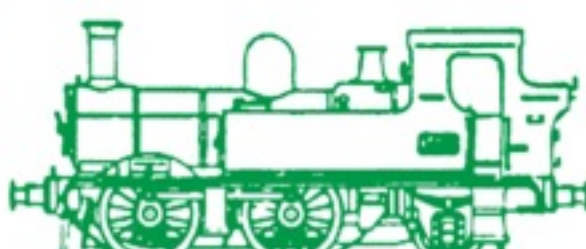


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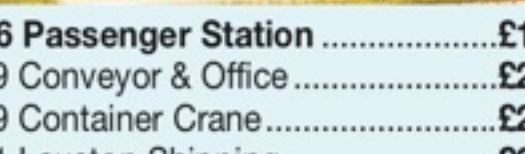


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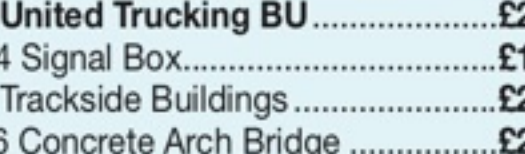
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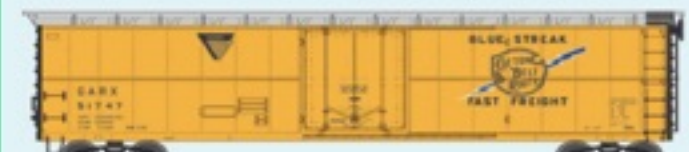
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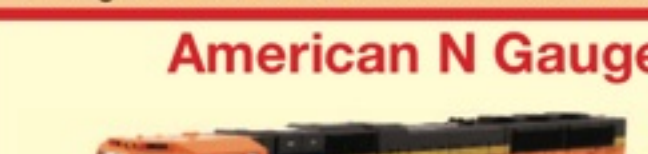
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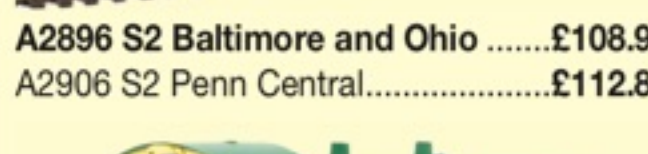
B58154 GN 4-8-4.....	£180.00
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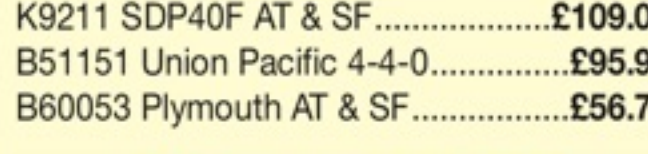
A2896 S2 Baltimore and Ohio.....	£108.95
A2906 S2 Penn Central.....	£112.80



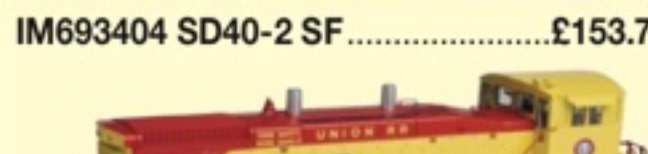
W50114 SW1200 Reading.....	£98.00
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B51151 Union Pacific 4-4-0.....	£95.95
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IM693404 SD40-2 SF.....	£153.72
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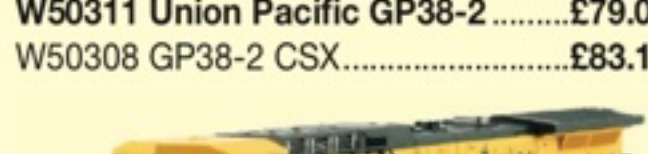
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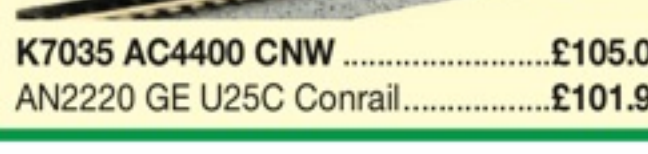
W50311 Union Pacific GP38-2.....	£79.00
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K7035 AC4400 CNW.....	£105.00
AN2220 GE U25C Conrail.....	£101.95



W50311 Union Pacific GP38-2.....	£79.00
W50308 GP38-2 CSX.....	£83.15



K7035 AC4400 CNW.....	£105.00
AN2220 GE U25C Conrail.....	£101.95



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B45608 Cape Cod House x 2.....	£22.50
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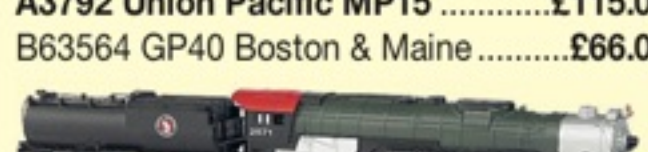
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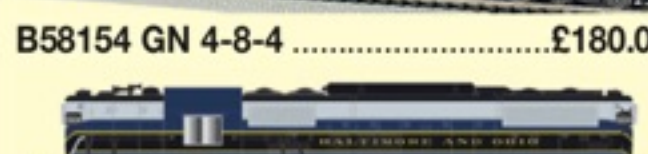
K6321 SD70 BNSF.....	£105.00
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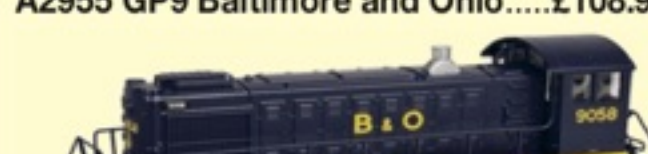
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B63564 GP40 Boston & Maine.....	£66.00



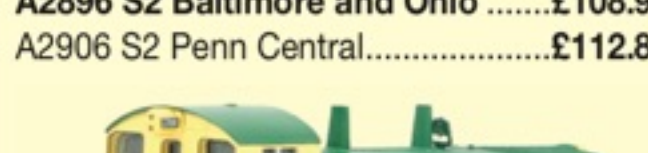
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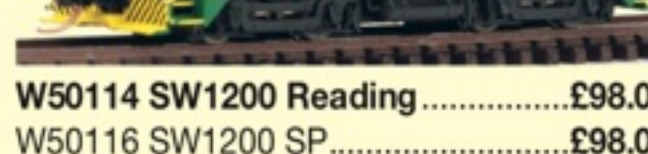
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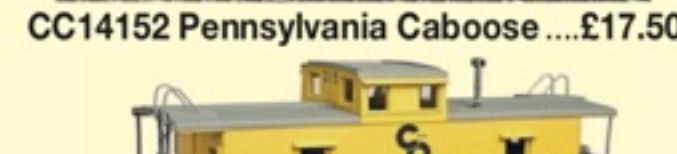
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CC14152 Pennsylvania Caboose.....	£17.50
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A3322 C & O Caboose.....	£18.50
CC14126 CSX Caboose.....	

CONTINENTAL MODELLER

July 2019
Volume 41
Number 7

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**Published on the third Thursday
of the preceding month by**

Peco Publications & Publicity Ltd.,
Beer, Seaton, Devon, EX12 3NA, England.

Telephone: 01297 21542. Fax: 01297 20229.

E-mail: cm-editor@btconnect.com

Website: www.pecopublications.co.uk

*Distribution to the model trade, & direct subscriptions
(Home & Overseas): Pritchard Patent Product Co.Ltd.
(address and telephone as above)*

*Distribution to the newsagency trade (Home & Overseas):
Marketforce (UK),
Second Floor, 5, Churchill Place, Canary Wharf, London, E14 5HU.*

*Printed by
William Gibbons & Sons Ltd., P.O.Box 103,
26, Planetary Road, Willenhall, West Midlands, WV13 3XT.*

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ISSN 0955 – 1298

*Annual subscription (12 issues):
United Kingdom £48.06 (paid by Direct Debit), post free.
Overseas (including Eire) £79.00.
Back numbers – for information, see
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RAILWAY MODELLER

For all modelling British railway practice.
Published on the second Thursday
of the preceding month.



Cover

Deutsche Bundesbahn
B-B diesel hydraulic 290 189-0
draws a rake of wagons along
the quayside at Nordhafen
on the new German HO layout
by Roger Nicholls.

Photo: Derek Shore.

Worldwide appeal

Attending major shows on the continent, either as visitors or exhibitors, frequently finds interesting material for the magazine. Some of the results of our excursions this year so far, to OntraXS!, Intermodellbau, and Trainsmania, will be found in this issue. Among them is *Tatar Sunda*, which we encountered in Utrecht being looked after by our good friend Ted Polet. We were surprised to discover that it had been built in Indonesia, by local modellers, and then shipped to The Netherlands – though not just for the show!

At such large exhibitions we have occasionally seen layouts from eastern or southern Europe, but bringing them from much further afield becomes less practical, though there have been examples from as far as Turkey, and there was an O gauge layout brought to the UK from Australia.

Railway modelling is almost inevitably the preserve of those who have spare time, space, and resources. We may think of it as primarily occurring in developed countries (Europe, north America, Australia/New Zealand, and Japan) but there is evidence of growing interest in local prototypes among modellers in China and Taiwan, and we were also interested to hear about HO semaphore signals being produced by Model Trains India (see *News*, p.526), presumably designed initially to meet the requirements of local enthusiasts.

The Market Deeping show incident

At the risk of dwelling on bad news, we could hardly let the incident at Market Deeping go unmentioned. During the early hours of Saturday 18th May (while this issue was being prepared) four youngsters broke in and smashed up one hall of an exhibition due to open later that day, which had been partially set up during the previous evening.

There can be absolutely no justification for such mindless vandalism and of course our sympathy goes to all those affected. As fellow modellers we understand.

We are clearly not alone, as the amazing response to the appeal for funds demonstrated, from near and far, from some celebrities and many ordinary modellers, and in a surprisingly short time. A substantial amount has been raised, far in excess of expectations, and this will bring its own burden of responsibility.

Money alone cannot compensate for much of the loss; proprietary items could be replaced but much of what was lost was, we understand, kit- or scratchbuilt, representing countless hours of effort and skill, and indeed the creation of any layout goes well beyond the sum of its component parts.

As has been noted, most of the damage was suffered by visiting layouts and trade stands, not the organising club.

The incident made national and indeed international news, which no doubt assisted the appeal, but we think it is sad that it takes such an occurrence to attract the attention of the mainstream media (print, on line, and broadcast). We were interested to observe how the 'facts' become garbled in the repeated telling, peaked with celebrity involvement, and then within a couple of days it was all over – good news is no news.

But there is some good news. One of those affected, our contributor Dave O'Rourke, present with his current British OO layout, vowed to have it rebuilt in time for its next booked appearance – in just three weeks. Significantly this nearby local event was being organised by another of the layout owners who was even more badly affected. We applaud Dave's determination not be discouraged even after such a setback.

Our hobby should be a relaxation, an escape, if you will, from the pressures of the real world, in short – fun! We sincerely hope that for all those affected by the Market Deeping incident it can continue to be so.

CONTINENTAL MODELLER

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Number 7

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468 TWO SMALL CAFÉS

Emmanuel Nouaillier continues his campaign devoted to modelling urban architecture in miniature, here setting the second completed café into a scene.

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Roger Nicholls returns to European HO modelling with a compact quayside scene set in northern Germany.

478 LA ROCHE PORPHYRE

As always planned, Rudi Nelissen has expanded his award-winning Belgian Vicinal HO micro layout extending the station area and serving a stone quarry.



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482 PORTA ALPINA

An ingenious way of extending a Swiss HO layout – Boyd Misstear shows how he built a tunnel station.

486 NISHIKAWA

Simon Holtby reveals how he managed to find space for a third layout in the railway room, as a result of random purchases of tempting Japanese N models.

490 ERZBERGBAHN

Ore, rack, steam, and snow – an unforgettable experience: Guy Tyteca recalls a January 1976 visit to the famous mountain line in Austria while it was still steam worked.

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Ted Polet describes an Indonesian N gauge layout which was built by Indra Setia and Irin Kusnadi.

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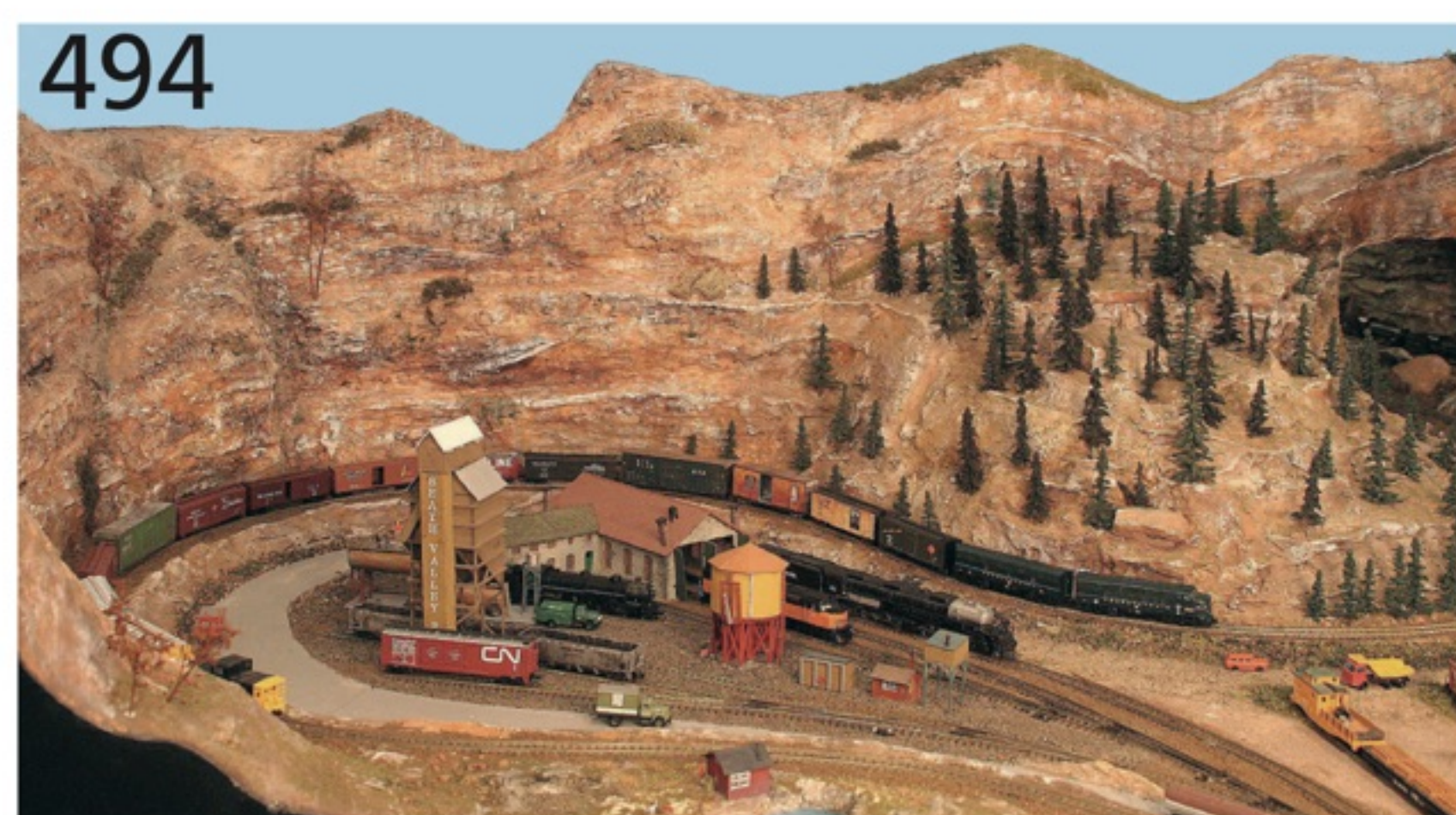
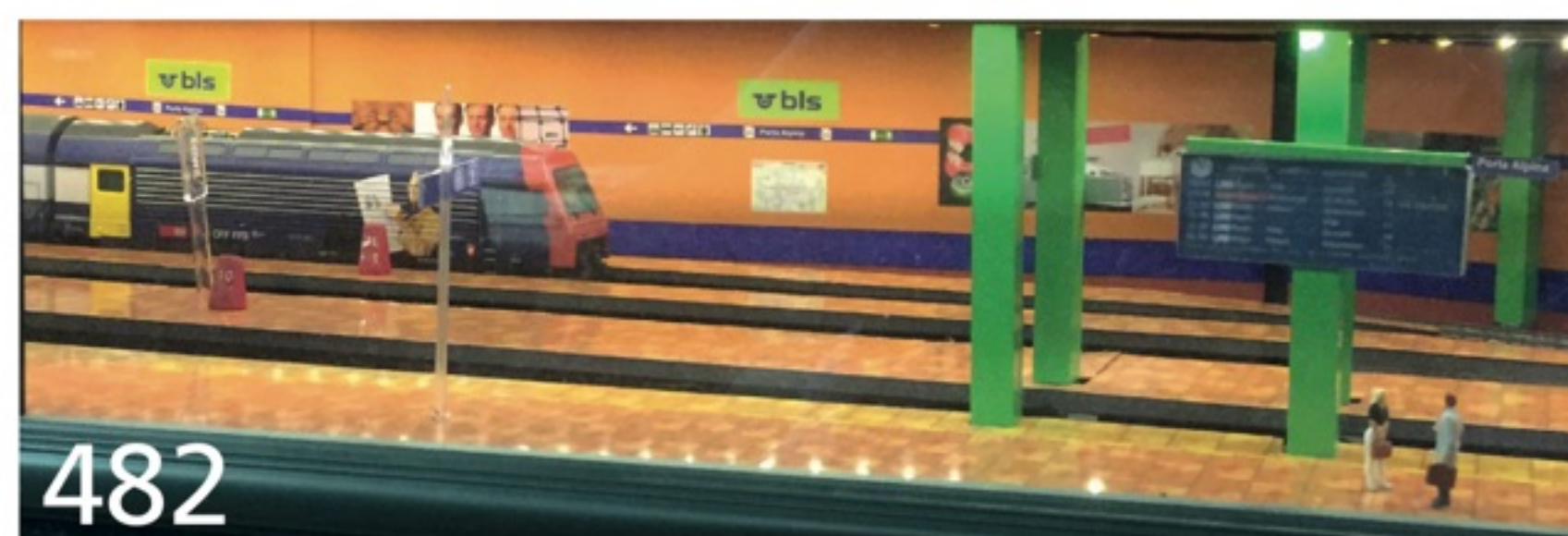
The German N loft layout Jeff Ware has been developing for over two years is still considered 'work in progress'.

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The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert. The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology. The HO Scale A-Track has clear, simple geometry and a realistic appearance.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits. The locomotives run smoothly, with accurately printed liveries and DCC compatibility. This range includes many passenger multiple units that run around Europe, not just in Germany. This allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.



The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors, and is great value for money. The locomotives feature directional lighting, and some even have sound and smoke. Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train. A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale. New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.



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HO Scale

Roco produce a large range of HO Scale German Railway locomotives and rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras!

A HO track system is also available, including the Rocoline pre-ballasted track.



H0e Scale

The H0e Scale range is growing in both size and popularity, with Narrow Gauge layouts being built on their own as well as part of existing HO layouts, which are the same scale.

A starter set is the ideal place to begin, with extra track also available to extend your Narrow Gauge line!



TT Scale

TT Scale is 1:120 Scale and is a good compromise if you want the detail of HO but only have the space for a smaller layout and don't want to go down to N Scale.

Roco produce a small but growing range of items in this size, so they are well worth exploring in more detail.



Z21 Digital System

The Z21 digital system is an innovative way of controlling your layout. Utilising any Apple or Android phone/tablet, you can control trains and accessories on your layout using the Z21 app.

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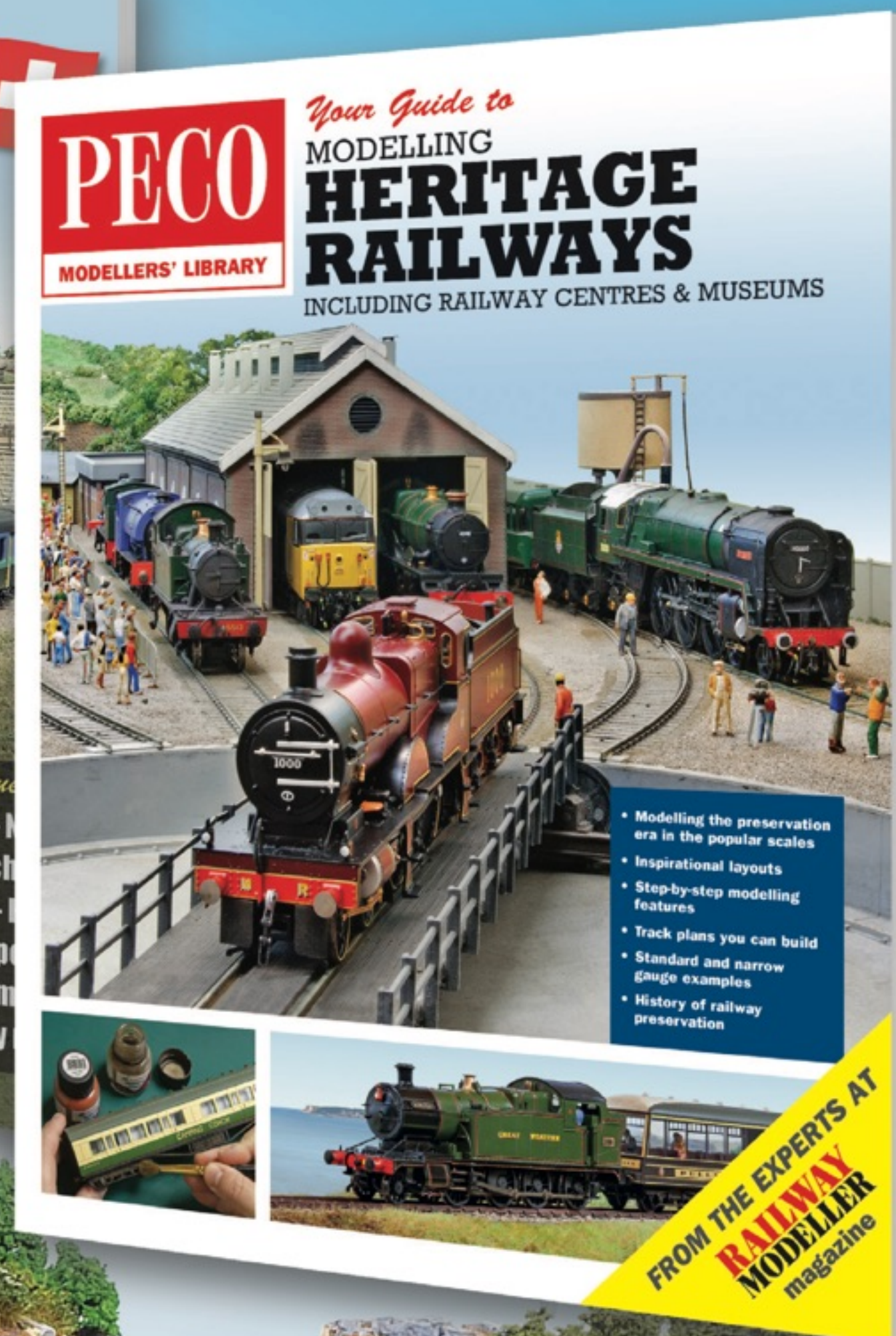
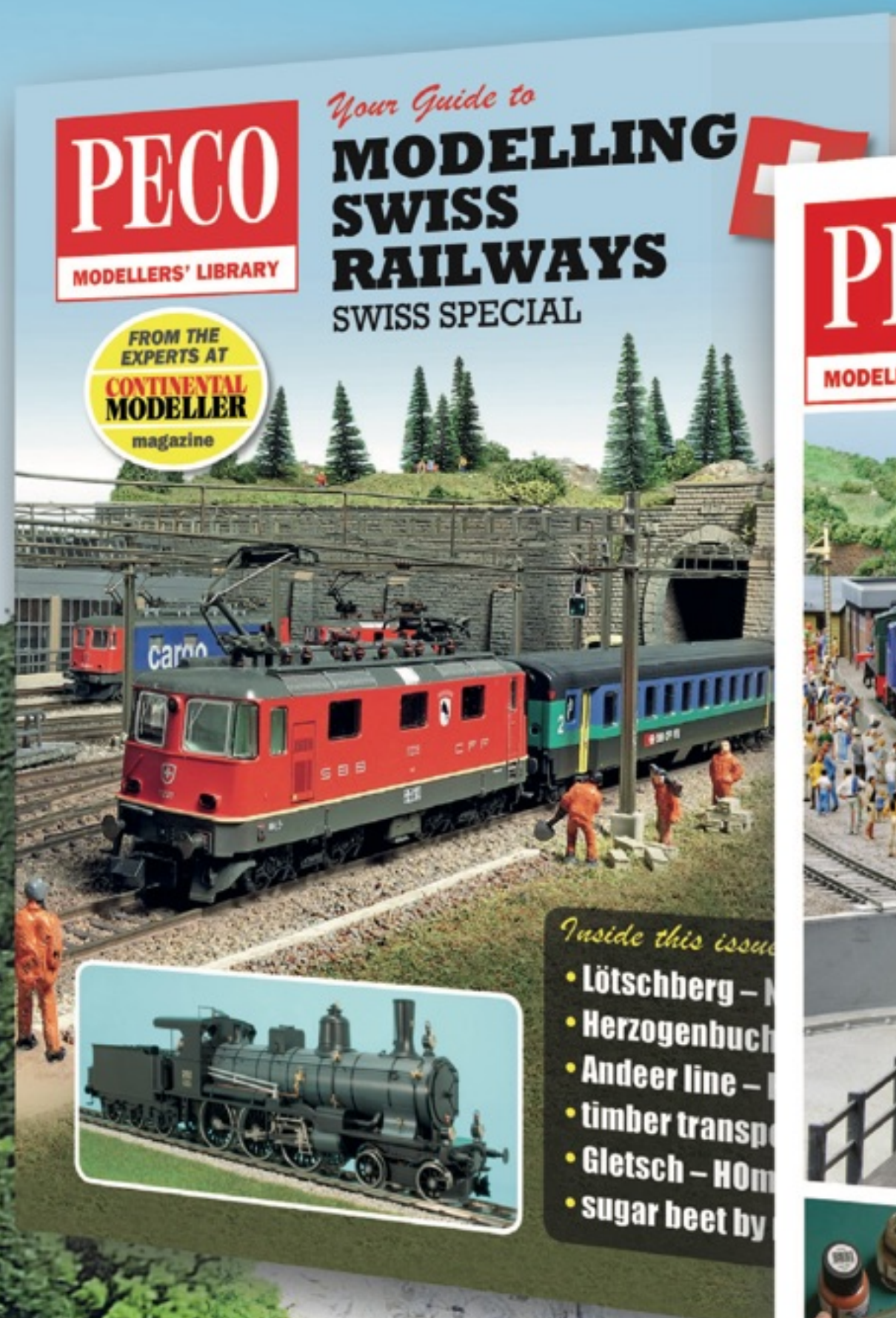


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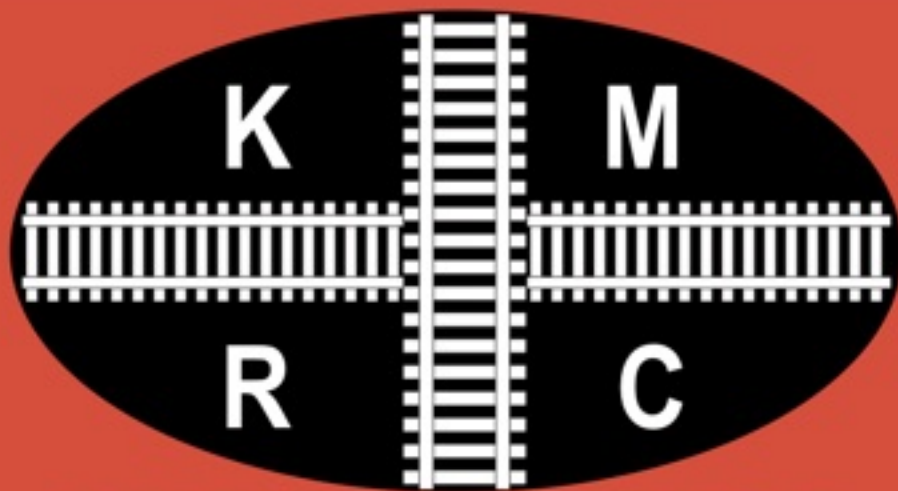
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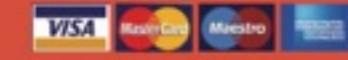
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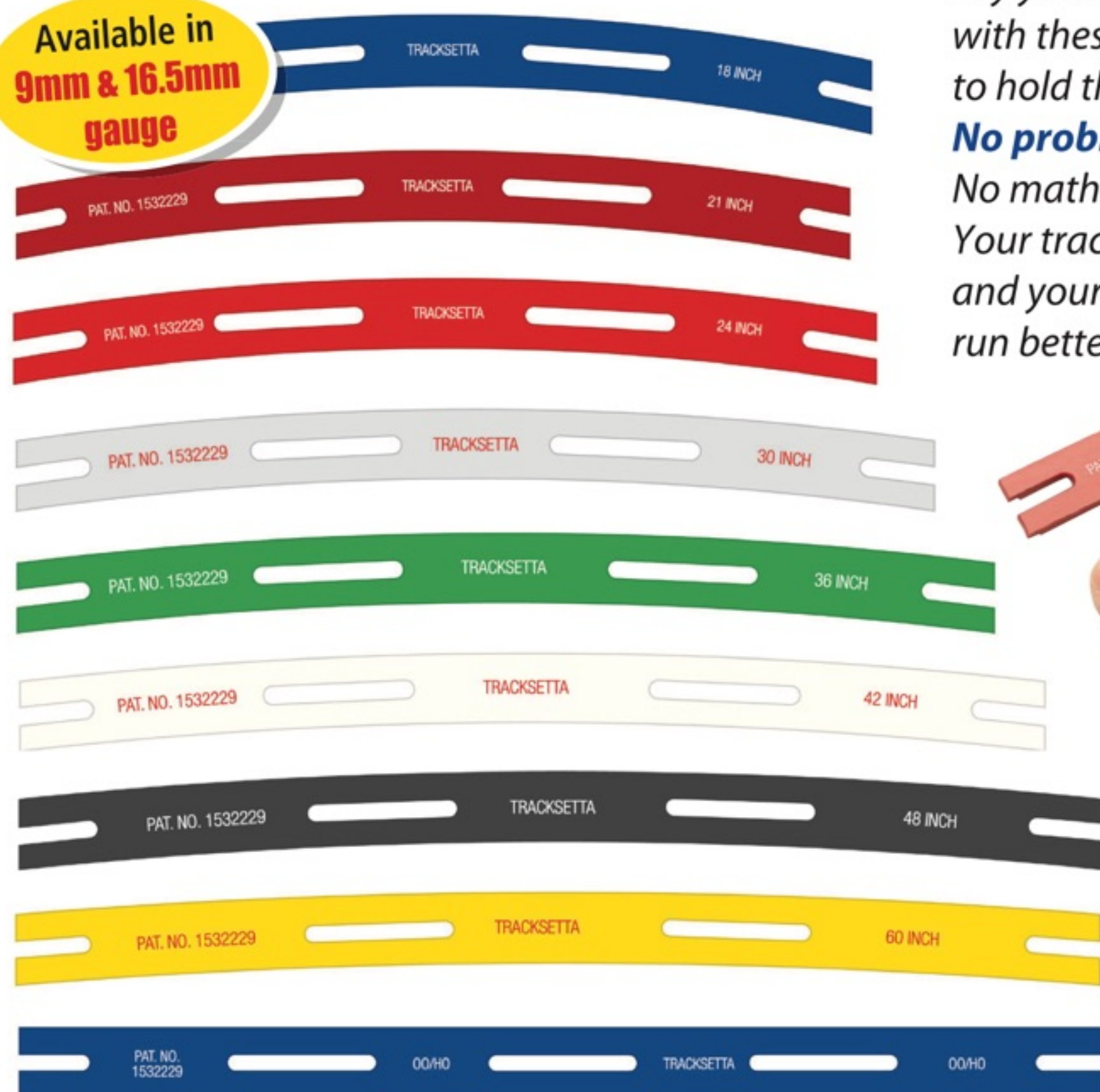
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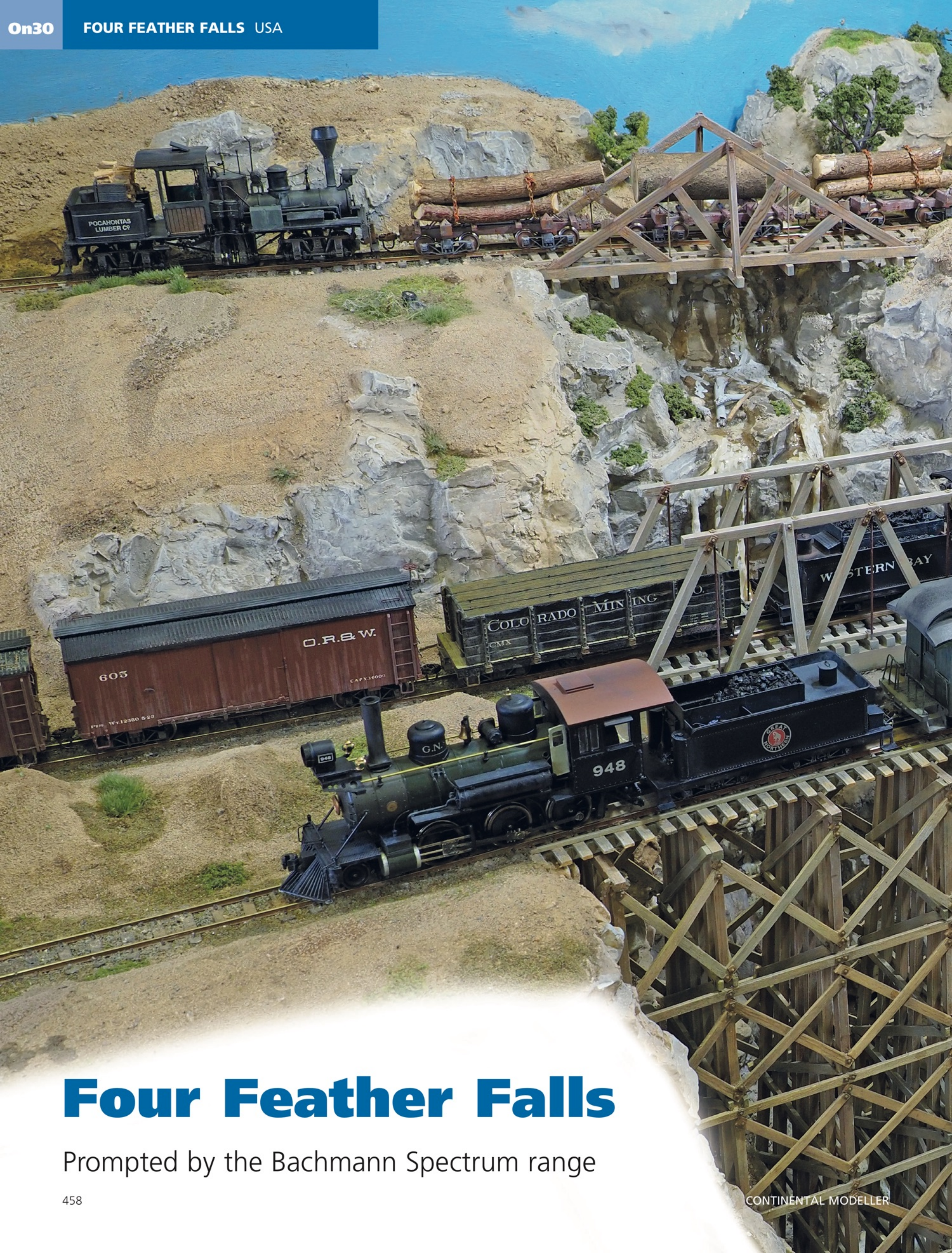
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Four Feather Falls

Prompted by the Bachmann Spectrum range



Ernie Melville relates how he prompted other members of the Cupar & District MRC to build an American narrow gauge layout. *Photographs by Ken Barclay.*

This layout came about when two members of the club, who were already heavily into American HO, came across the Bachmann On30 narrow gauge range while visiting an exhibition over ten years ago. The purchase of locomotives and rolling stock began, and they were soon joined by two other club members, until between us we had a stack of models still in their boxes but with no railroad to run them on.

These club members were myself, James (Hastie) Donaldson, Iain Smith, and Ryan Gray. Hastie and I devised and planned a layout which would ultimately be used as one of the club's exhibition layouts, so the size and complexity of operation had to be kept as small and simple as possible.

Above

Trains on each bridge: top, a Shay pulling a load of logs to the sawmill; in the centre, a freight hauled by a Western Bay RR 0-6-0; and at the front a Silverton Northern RR 2-6-0 with a bullion car and two passenger cars.

It was duly built and the scenics done by studying pictures of Colorado narrow gauge lines in books and magazines to get the right sort of terrain and 'feel'.

After it was built and running, we still had no idea of the location, fact or fiction, so we called it *Four Feather Falls*, a name conjured from our distant youthful television viewing (referring to a children's programme using puppets by Gerry Anderson). The name seemed to fit the layout rather well.

I called the mine on the layout 'Soloman's Mine No.4', as I have already built three previous models of the same name in HO. It was some time later, while I was studying a map of the Silverton area of Colorado, that I noticed that King Solomon Mountain was just to the east of the town, so that was it, the location of our layout.

It happened like this ...

Otto Mears was a pioneer road and railroad builder in Colorado at the end of the nineteenth century. Between 1890 and 1891 he built the Rio Grande Southern railroad from Ridgway to Telluride, Dolores, and Durango. This was one of his finest achievements and is still regarded as one of the great feats of railroad engineering.

Right

A view looking from above the logging camp towards the gorge with its bridges and the town beyond.

Opposite

A freight arriving from Howardsville.

Below

Looking towards the tunnel mouth leading to the Highland Mary Mine.



In 1895 he laid the tracks of the Silverton Northern line with a branch from the town of Silverton north east to Howardsville, Eureka, and Animas Forks.

The miners and sundry prospectors working the dozen or more mines to the south of Howardsville had to struggle along a bad road for many miles to the town, with mules and wagons carrying the ore outwards and supplies back. This road followed a watercourse called Cunningham Creek which flowed northward, joining the Animas River near Howardsville.

When the Silverton Northern railroad reached Howardsville, the miners of the region persuaded Mears to build a branch along the bank of the creek, down between the King Solomon and Green mountains, to serve the mines and their communities. The high quality of the ore being shipped out and the traffic in timber being cut from the slopes of the mountains helped persuade him to agree to build. Blasting tunnels through the rock and bridging the gaps which were too big to be rock filled, the line eventually reached a terminus at the Highland Mary mine, 25 miles south of Howardsville.

Jumping forward to 1922, we see on the layout the mining settlement of Four Feather Falls situated on the lower slopes of King Solomon Mountain, with its depot, engine shed and yard, the mine in the distance, and a logging line running just above it.

A guide to the layout

Trains from Howardsville approach Four Feather Falls through a rock cut tunnel on the right of the layout. They pass the mine yard and on to the timber trestle bridge spanning Feather Creek, which flows into Cunningham Creek. The depot lies on the other side of the bridge where passengers rarely board if the train is heading south towards the



terminus. Trains heading north are busier, with passengers making for Howardsville and beyond. The depot area has a water tower to replenish thirsty locomotives next to the maintenance-of-way hut (a grounded Bachmann car) which houses the crew who look after the track and bridges in this section of line.

A dirt road crosses the line and takes visitors to the small mining town of Four Feather Falls. Across the road is Charlie's Mining Supply store, providing everything the prospector needs before heading for the hills searching for the fabled gold and silver seams that legend has it still exist around the next boulder, though the ore has mostly been extracted by big mining companies who are taking control of the area.

A set of points in the single track main line sends you back behind the depot and out towards another bridge, which crosses the creek just in front of the Four Feathers waterfall. Crossing a truss frame bridge, it heads towards a point with one track going back towards the main line, creating a passing loop, and the other heading via a three-way point to the ore loading trestle and storage tracks (with a loop) at the mine.

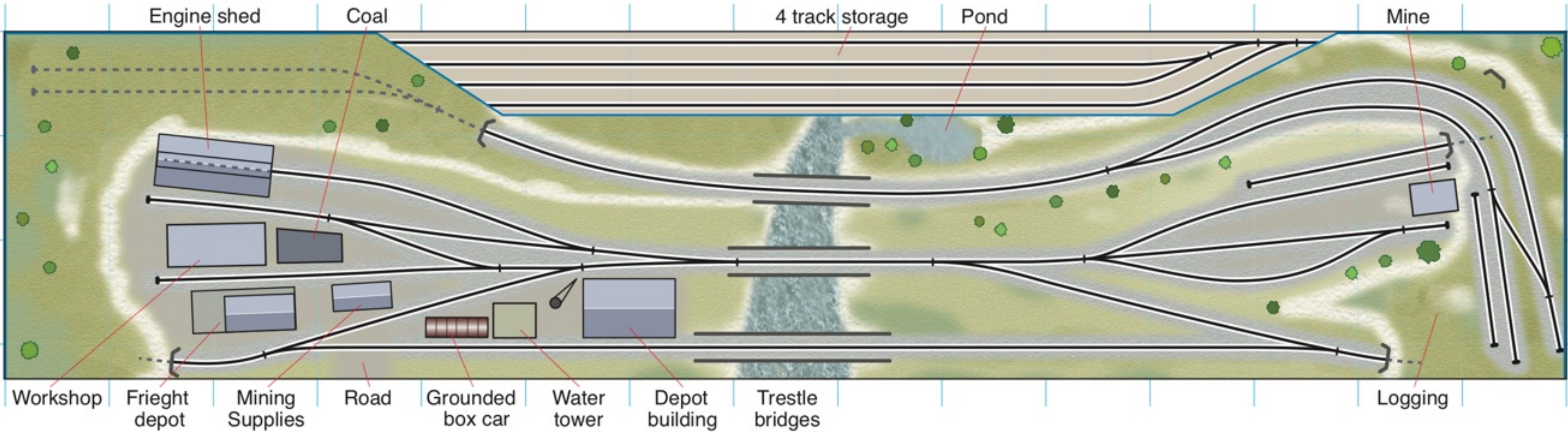
Behind the depot, the line splits into three spurs. The right-hand line leads to the mine loco shed. The middle track goes to the side of the shed used for repairs and servicing. There is a small coaling dock and an oil store next to the shed. The left spur runs behind Charlie's store to the freight depot, where larger items can be unloaded.

Back through the points leads us to the main line again. Soon we enter the tunnel and head south to the terminus at the Highland Mary Mine, where the locomotives will be turned, ready to head north again.

Running above and behind the town is the logging line which is unconnected to the main line here. The track leaves a rock cut tunnel above the engine shed and travels over the creek on an A-frame truss bridge, then heads up hill to the logging camp. The tracks there consist of spurs and a passing loop, to assist in the log loading operation. A length of unconnected track has been put down and is used to hold the kitchen car and water tank car.

Trains from the logging camp run past the town to connect with the main line lower down, at the sawmill near Green Mountain.

The viewer is asked to imagine the distance from the town end of the layout to the mine as being greater than it seems, using the central bridges to create the illusion.



Four Feather Falls Overall layout size 4500mm x 1000mm. Each grid square = 300mm x 300mm.



Left
The horse waits patiently for loading to be finished at the freight depot.

Right
The proximity of the cliff prevented the coal stage being sited in the logical place on the shed road.

Below
Solomon Mining Company Shay #5 at the engine shed.

Below right
A solitary passenger waits for the southbound train.





Baseboards, track, and scenery

We found in our clubroom two spare baseboards that had been made for another layout which never came to life. Adding a third shorter board for the gorge in the centre gave us an overall size of 4.5m x 1m. The three boards, with all the stock boxes and other equipment required, fit into a normal Transit van when we attend exhibitions.

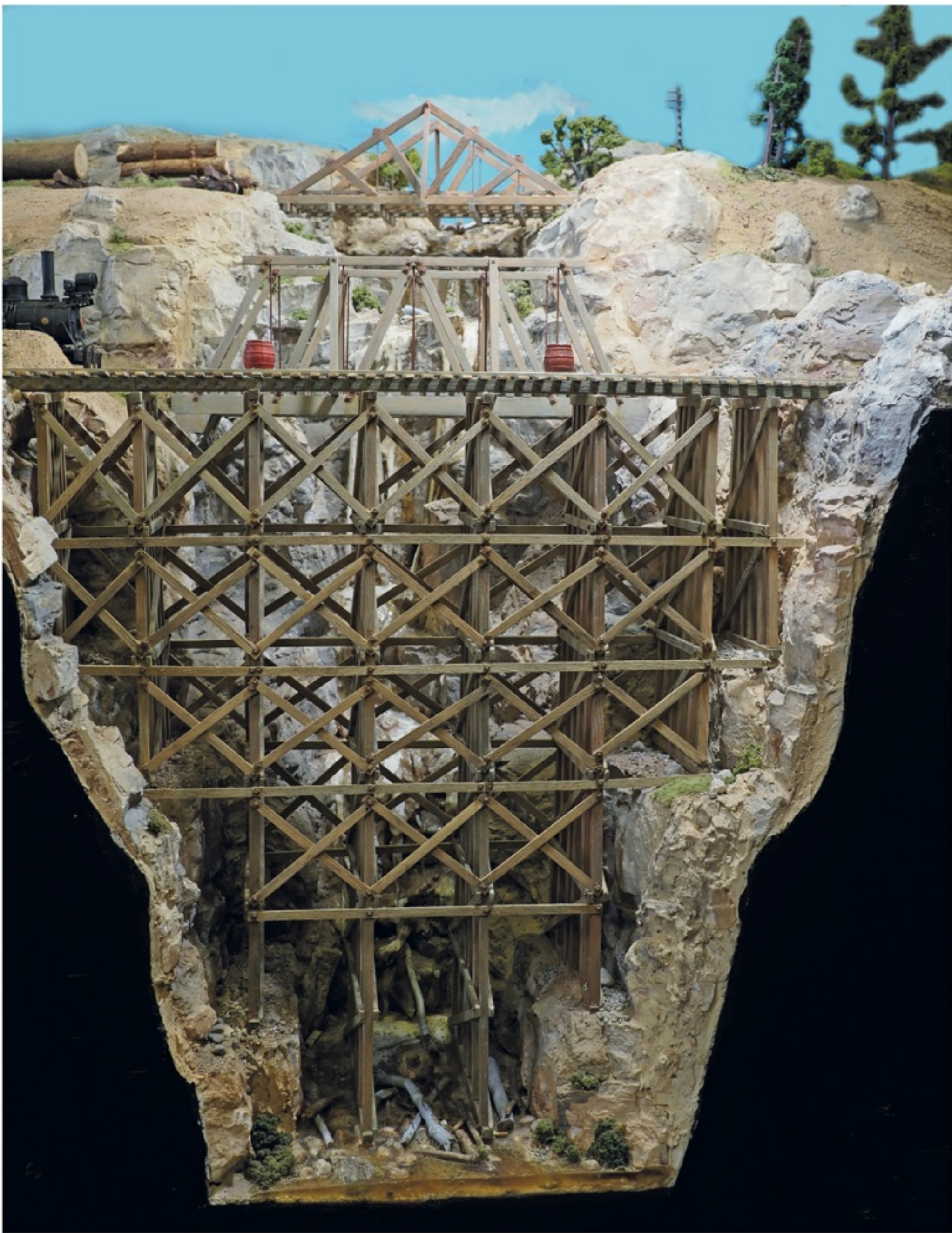
Baseboards are 40mm x 20mm timber frame with a plywood surface.

The track and points are Peco O-16.5 (code 100), apart from the three-way point leading to the mine and the hidden track in the tunnels and fiddle yard which are Peco HO/OO.

Woodland Scenics grey ballast was used mixed with a little sand. It was built up over the ties in some areas, such as the yards and mine area. When first laid, real narrow gauge track was often not ballasted between the ties or just had light earth ballast which had to be upgraded later.

The rock faces were made using four Woodland Scenics plaster moulds, which gives eight different shapes when inverted. Other casts were cut up to change their shape, and when it was all put together the effect looked right for the area we were trying to recreate.





Above
The three bridges – trestle, Howe truss, and A-frame (on the logging line above).

Below
The A-frame bridge over the waterfall with the truss bridge beneath.

Above right
At the pond by the logging line, a bear is hunting while a beaver keeps its distance.



The other hillside was made with plaster bandage, covered with thin plaster, and painted with a light earth and sand wash of acrylic paints, then sprinkled with fine sand and earth to give texture to the ground surface.

Coloured washes were used on the rocks, following coloured photographs of the area around Silverton in Colorado.

The ground vegetation is various Woodland Scenics grasses, as are the small trees (left over from logging operations). Other grass and flower clumps are Peco Scenics, while the long grass tufts are various sizes of brush bristles, cut and tied together in clumps.

The centre section containing the waterfall and bridges was made as a plywood box with front and top left open. This protects the scenery and bridges in transit to shows. The rock sides and rear of the waterfall were made using the Woodland Scenics moulds. The waterfall was made from fibreglass resin as used in car repairs, built up in layers, with white paint applied between the layers to give depth to the water. Pieces of timber and rocks were placed on the sides of the gorge to depict the debris washed down the creek.

The bridges

The bridges were scratchbuilt over a period of months using templates drawn out on paper. Timber stripwood cut down from D.I.Y. store wood shapes was cut to length and glued together to form the bents (uprights) and bridge sides. Short pieces of rail were soldered across parts of the track crossing the bridge. The plastic ties were cut away leaving the rails only, held in gauge by soldered pieces. Wooden bearers the length of the bridge were topped by wooden ties, glued across, and then placed under the rails and glued together using contact adhesive. The bents were placed under the bearers and glued in. The retaining soldered rails were removed and the track cleaned up.

The bridges behind the large trestle were put together in a similar way, the sides being attached after the track was fixed to the base.

Grandt Line bolt head castings were used to detail the bridges. Black and brown Indian ink (non-waterproof) dropped in small measures into a 250ml bottle of isopropanol was used to weather the timber.



Above
Soloman's Mine No.4 yard
with the logging line above.



Right
Ore buggies at the mine exit
wait for empty cars to be
placed under the chutes.



Structures and details

The depot building is a laser-cut wooden kit from American Model Builders and the freight depot was made from a Banta Modelworks wooden kit. The mining supplies and backwoods engine shed are cast plaster kits, also from Banta. The repair shed was the lean-to part of the engine shed but was built as a stand-alone structure with extra walls and interior. The water tower is an Evergreen Hill timber kit, while the coal store and mine loading gantry were scratchbuilt from spare timber parts from the bits box. The Surry Parker style log loader was scratchbuilt from pictures in narrow gauge magazine articles. Its means of operation, the vertical steam boiler, is part of the Bachmann log skidder. The mine office was built from scratch using spare timber sheeting and plastic mouldings for the windows. The roof is Paper Creek printed corrugated sheet, which looks almost real.

Four Feather Falls and surroundings are populated in the main by cast whitmetal figures from various sources, painted, and in most cases given new headgear; some have also had arms, legs, and heads altered to fit their situation. A few Woodland Scenics figures can be found beside the logging camp, about to be disturbed by a grumpy bear. Other animal life consists of more bears at the top of the waterfall, deer, cats and dogs, and what I believe are beavers in the pool beside the logging line. (That's what my co-builder Hastie says they are!) A horse with a cart down by the freight depot is a British whitmetal kit.

Control

The layout is analogue DC with the control panel behind the backscene. There are three Gaugemaster handheld controllers, one each for the main line, yard and mine, and the logging line, which is independent of the rest of the layout.

There is one switched section in front of the engine shed and another at the end of the mine gantry track to allow locos to be parked.

Points are controlled by switches on the panel, operating Peco point motors.

The trains

Bachmann On30 locomotives make up most of the roster. As they are individually owned they vary in type and road name. The range from small 0-4-2T Porters through 4-4-0 Americans and 2-6-0 Moguls to 2-8-0 Consolidations, while Shays and Climaxes appear on the logging line, happily trundling a set of log trucks slowly back and forth from the logging camp to the sawmill (off stage).

Having different road names is not as unlikely as you might think as the Silverton Northern rented locomotives and stock from the Denver & Rio Grande and other railroads.

Two sets of coaching stock are used, both from Bachmann. The yellow passenger cars usually run behind the Rio Grande 'Bumble Bee' liveried locomotive, while the green set is lettered for the Silverton Northern and Rio Grande Southern. This includes a bullion car, as operated by the Wells Fargo

Above

The loggers at their meal are disturbed by a bear.



Company, used when a valuable load of silver is being transported to the bank in Silverton. This is a Bachmann combine with the sides removed and new ones made from scribed plasticard.

Sets of ore cars serve the mine, collecting the ore for transfer to larger gondolas bound for the smelter in Durango. Box cars, reefers, tank cars, gondolas, flats, and others all pass along the line.

Two of my gondolas have been shortened and my tank car has different tank mountings, in keeping with the period. The water carrying tool car which is usually found beside the log loader and kitchen car is a shortened flat with a rectangular tank and tool boxes made from plasticard.

Summing up

In the ten years since construction started club members have exhibited *Four Feather Falls* at shows around central Scotland. It has been received favourably by the public and fellow modellers, and won a trophy at Perth in 2012. In 2018 we were invited to show it at the Warley NEC event, the pinnacle of exhibitions in the UK. Unfortunately I could not attend and it was left to Hastie and other club members to exhibit the layout in Birmingham, all doing sterling work and gaining much enjoyment from the experience.

On the Sunday morning of the show I received a text from our club secretary saying the layout had been presented with the CONTINENTAL MODELLER trophy. I was amazed that it had been singled out from all the marvellous layouts on show. It was the high point of our hobby year.

Above

The logging camp, with a kitchen car and a tool car with a water tank to supply the Surry Parker log loader. Loaded skeleton frame log cars await collection.

Below

The Surry Parker log loader at the end of the line, with the kitchen car to the right.

We have all had hours of enjoyment building, making scenery, wiring, building structures, running trains, and showing this to the wider public. It makes all the effort of transporting the layout to shows worthwhile.

My thanks to Hastie, my building and plaster casting wizard, Iain our club chairman and encyclopaedia of all things railways, Ryan, Larry, Ken, Will, Carlo, John, Andrew, Chris, and all Cupar & District Model Railway Club members past and present for their help, support, advice, and good humour over the years. It has been a joy to share our love for this fascinating hobby with you.

The club's annual show takes place on Saturday 20th and Sunday 21st July, but the layout is not due to appear.



Emmanuel Nouaillier completes his series devoted to modelling urban architecture in miniature.

Photographs by the author.

Two small cafés

4b – setting the completed structures in small scenes (concluded)

This very distinctive structure was formerly the warehouse of a removals company, which I felt could make a perfect partner to the second café model, with some original but plausible modifications to the construction.

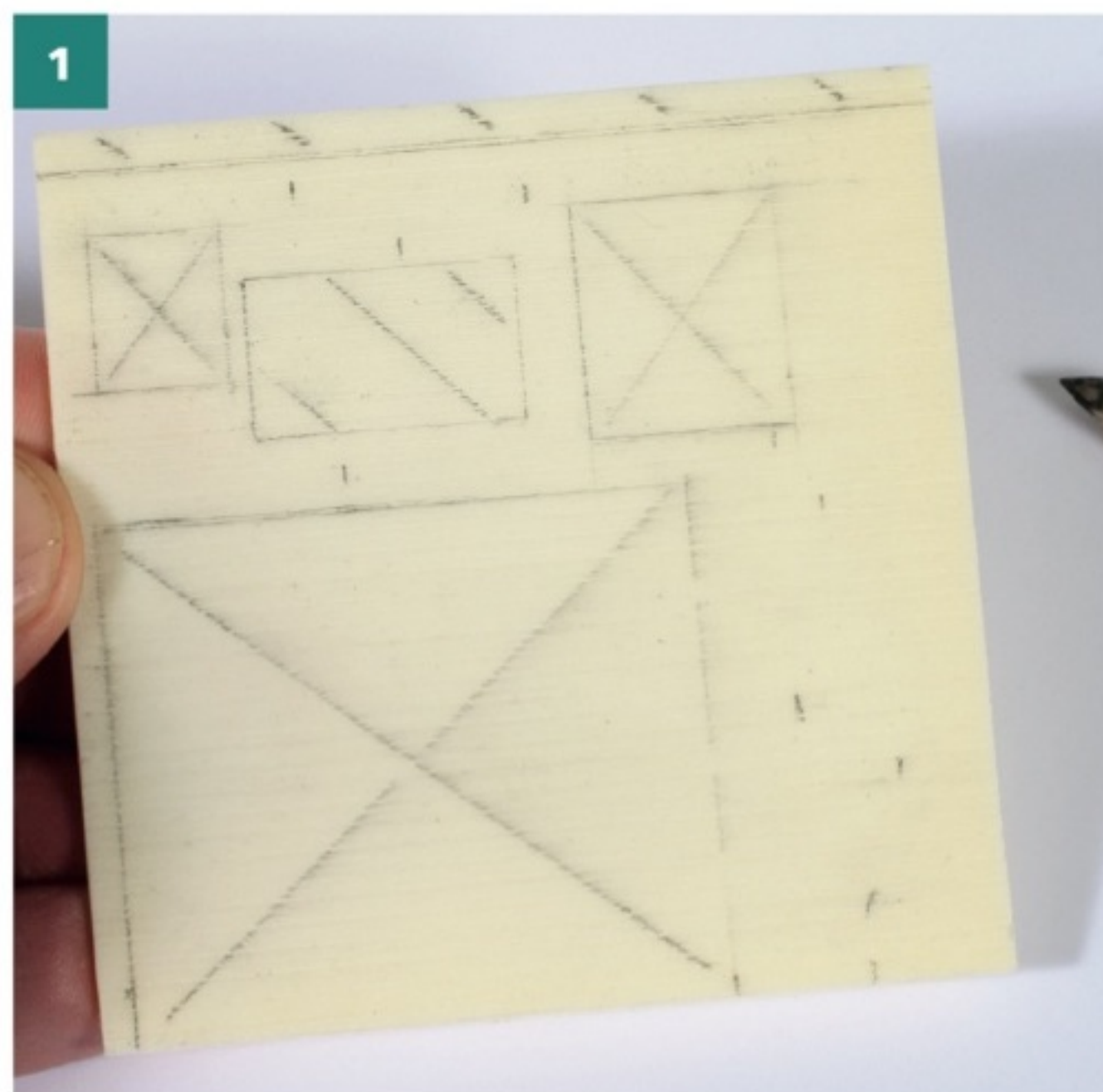
On structures like this some parts of the cladding can be displaced, revealing the wall behind, in these examples brick. This is really the best effect to imitate, even in small scales, if you want to take the weathering to the ultimate, as I do.



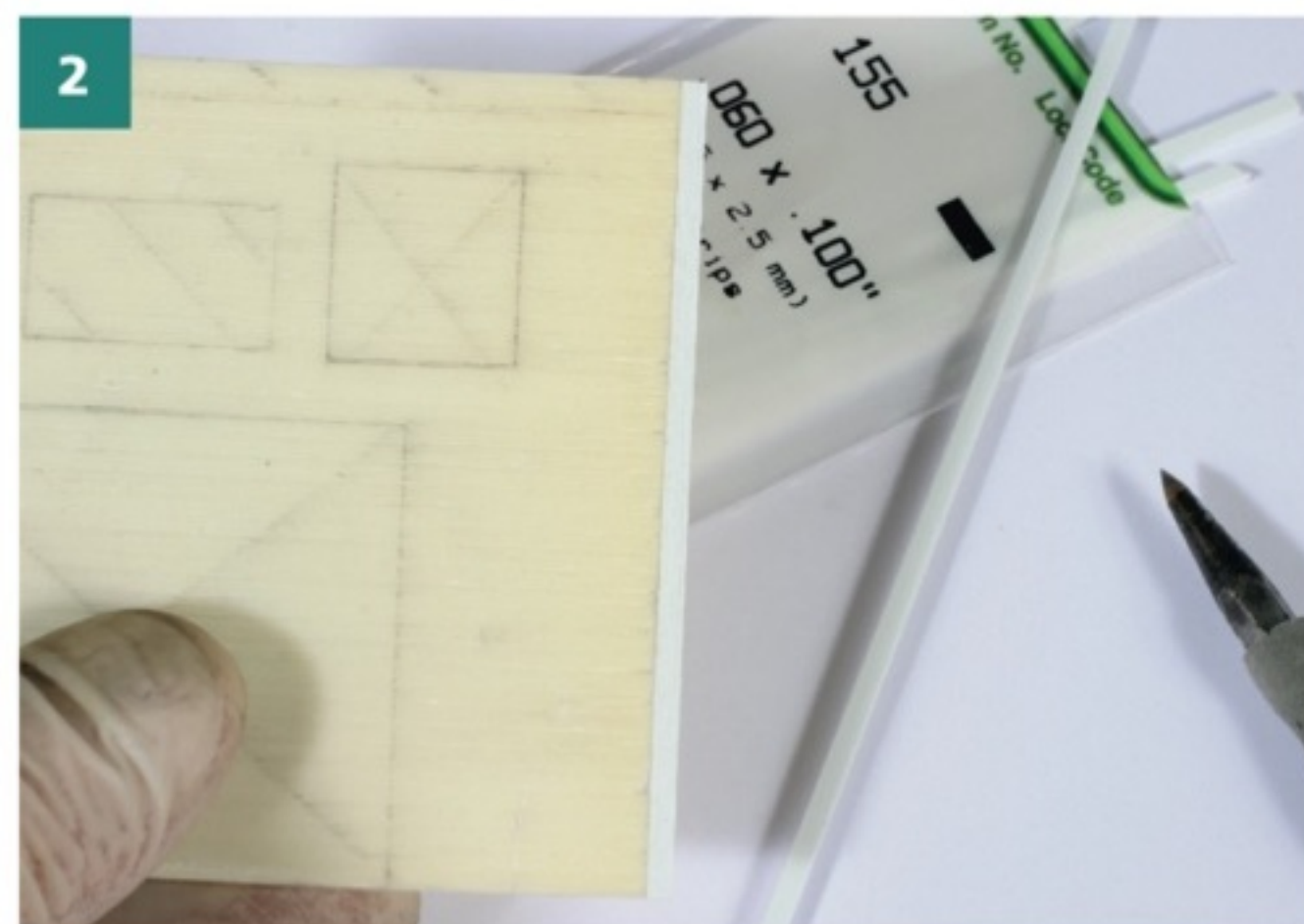
Step 1

To save valuable time in the creation of this structure, I turned to the old Zebdec resin sheets, of which I had a good stock. These were ideally suited to making all or part of the front, facing onto the street.

From the outset I determined the locations of the different openings and any other details that will hide parts of the material in the end and so do not need texture or colour.



Step 2



Following the real building, key wooden components were made using Evergreen square strip scribed and treated with a wire brush in advance to simulate the wood grain. The pieces were stuck in place with cyanoacrylate.

Step 3

For the additional section at an angle, I preferred to scratchbuild the wall in 0.5mm plasticard, which not only made it very easy to create openings (e.g. the window) but also to reproduce some effects of decay in the foundations.



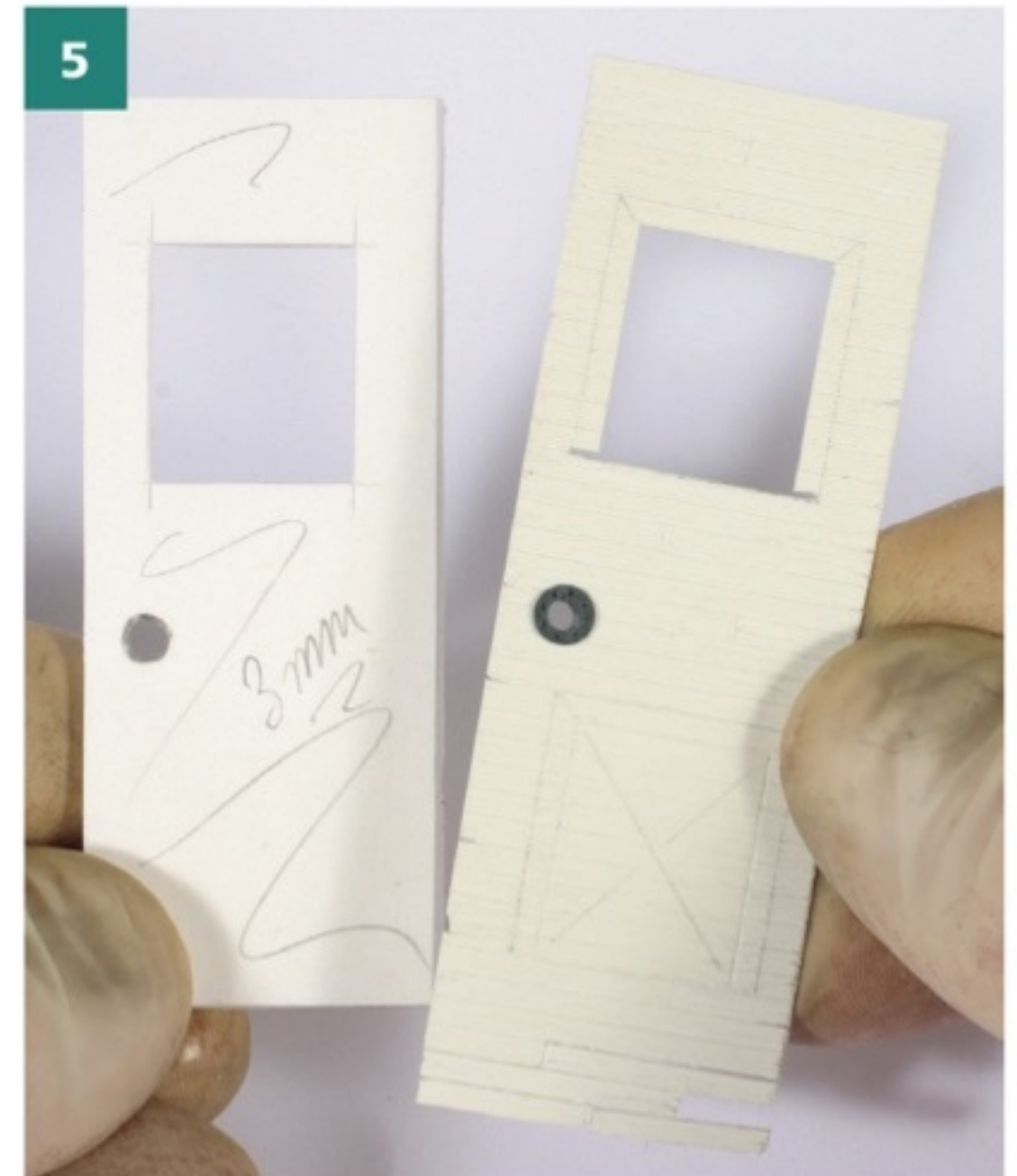
Step 4

After cutting away some boards, I re-assembled them leaving a bit of play, and removed one or two completely. For this I work with very fine tweezers and a minimum of cyano glue, spread in a very thin film on the surfaces in contact with each other ... it is a bit of precision jewellery work but will have a significant effect in the end.



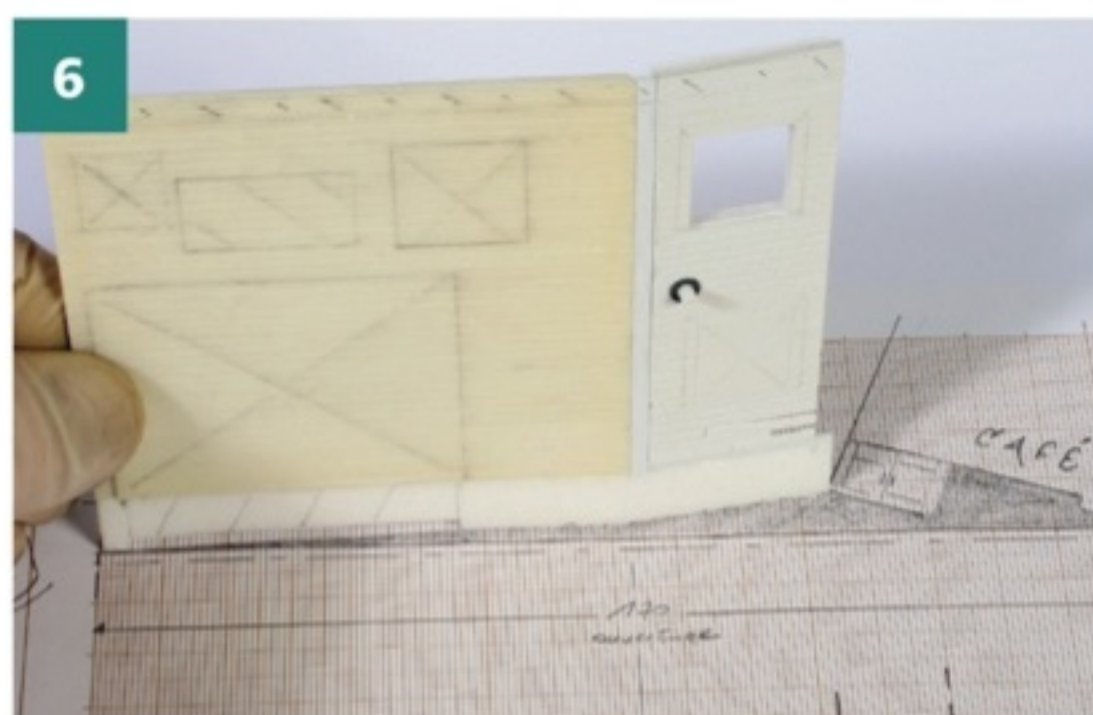
Step 5

To match the much thicker resin part (about 2mm), I cut a duplicate of this component from 3mm foamboard to which I stuck the plasticard. This offers more rigidity and strength to the assembly, welcome during subsequent operations. You could also use 2mm plasticard or even Forex.



Step 6

The two parts were then assembled, following the ground plan on graph paper. Nothing is left to chance – even a millimetre more or less could cause problems combining the structures in the scene. Note the foundations following the shape of the building made with foamboard, simulating old cement.



Step 7

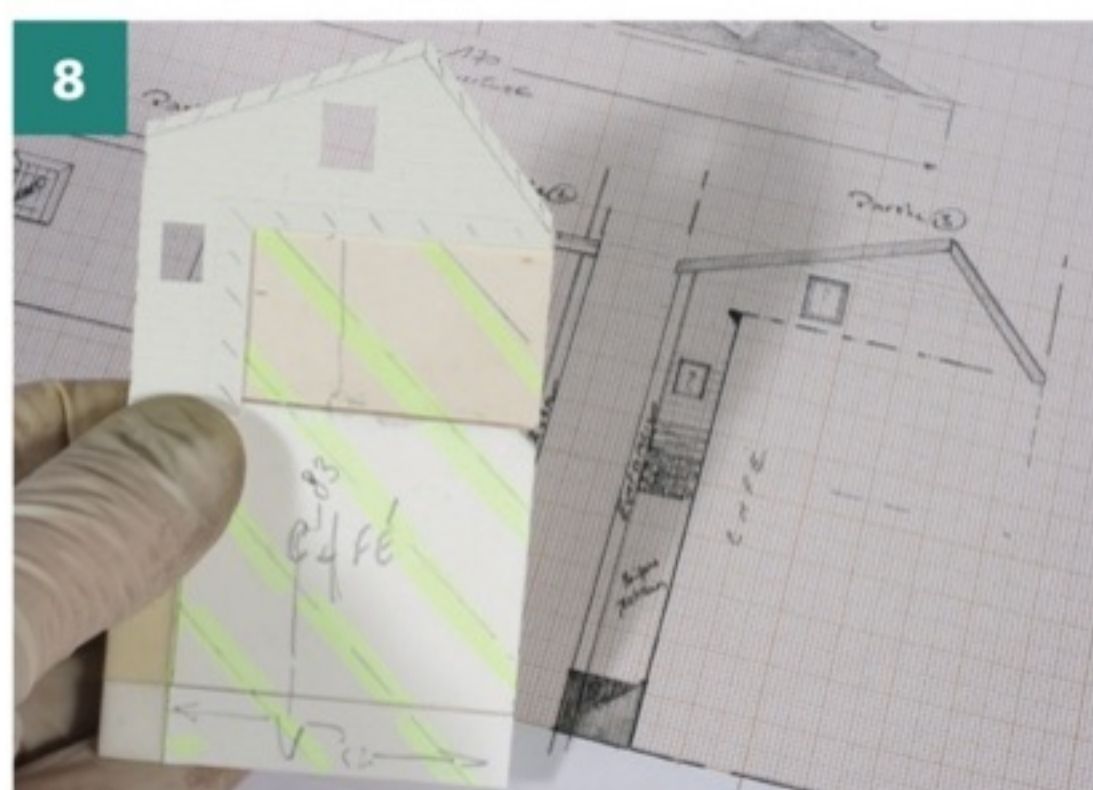
To return to the deteriorated area, I reproduced the part of the brickwork visible behind with pieces spare from the transformations made to the original Dynimo façades – nothing is wasted! I also worked on the mortar between the joints and some of the wooden battens on which the cladding was fixed.

Once this little exercise was finished, I checked that everything fitted perfectly.



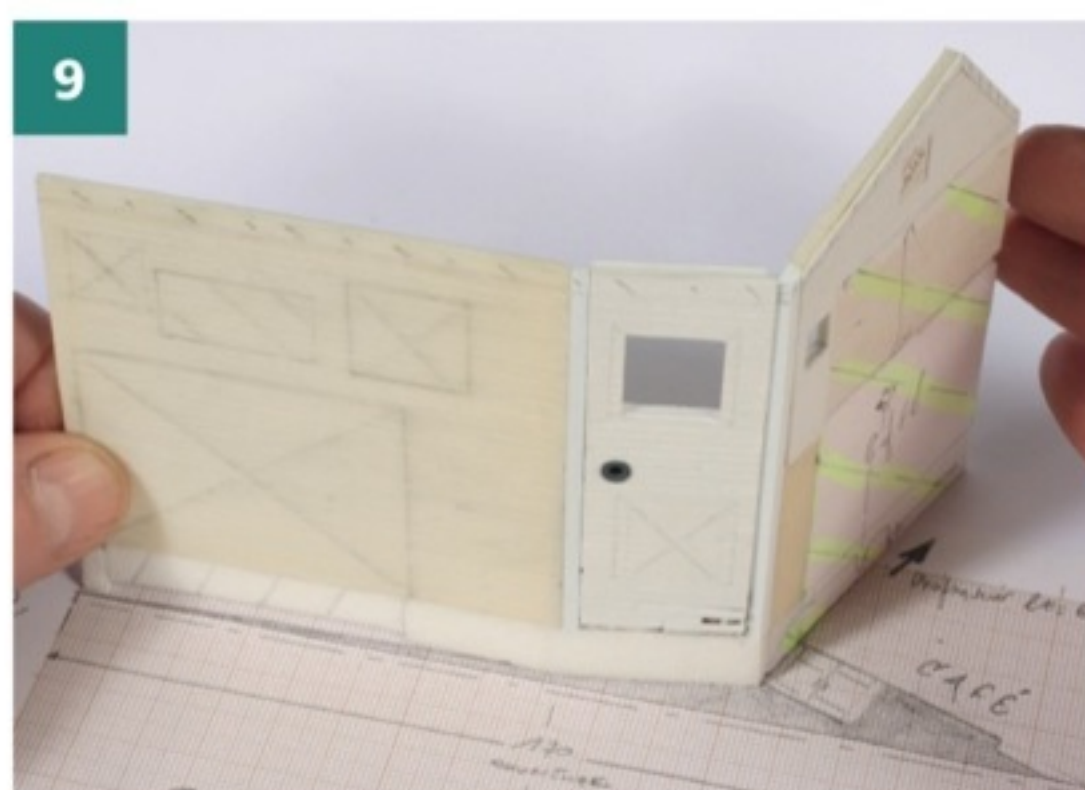
Step 8

I continued with the end wall which will be against the café. I adapted the real situation by making the upper part with cladding and the lower storey render over bricks. I used pieces of foamboard, some Zebdec ref.87805 cast resin sheet, and grooved and ribbed 0.5mm plasticard.



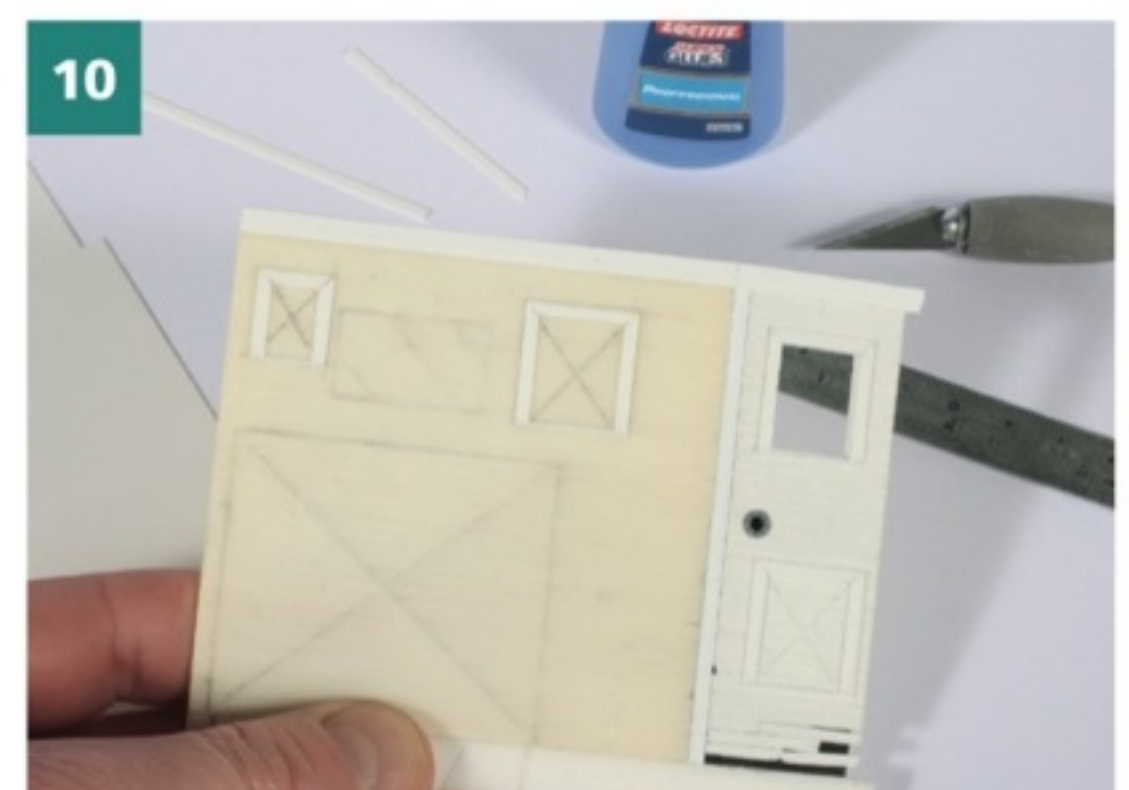
Step 9

Once this part was ready, I checked that the overall assembly was absolutely correct on the ground plan of this mini-scene, made on graph paper ... a rule that I apply for all my projects.



Step 10

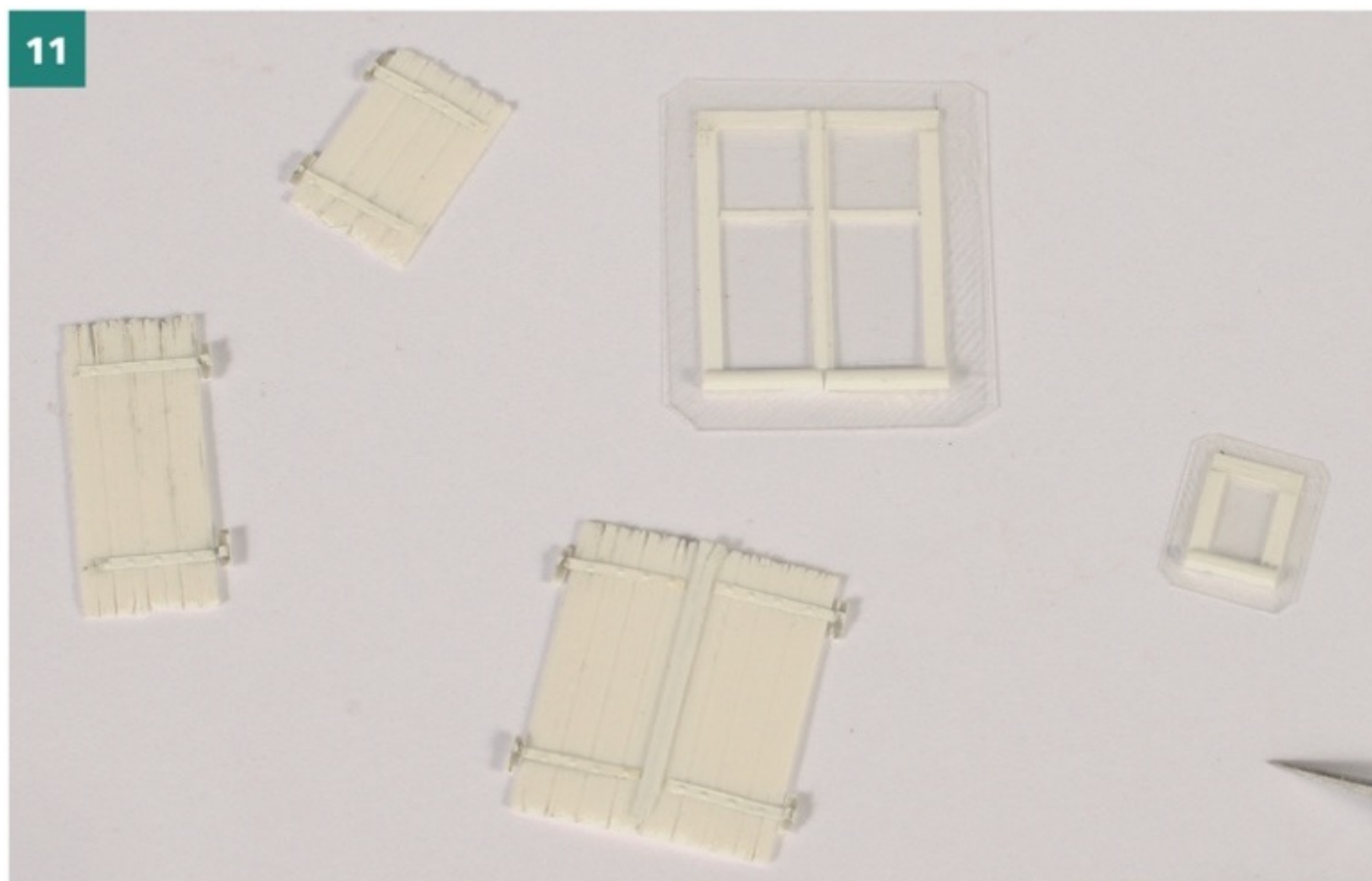
With most of the structure finished, I added the necessary details, in this case the moulded wooden cornice, and window surrounds made with 0.5mm Evergreen flat strips, scribed to simulate wood grain after being placed.



Step 11

Next, various shutters and frames were made from scratch using Evergreen strips and plasticard, again trying to go as far as possible with the detail. This may take time, but it is always worth it in the end!

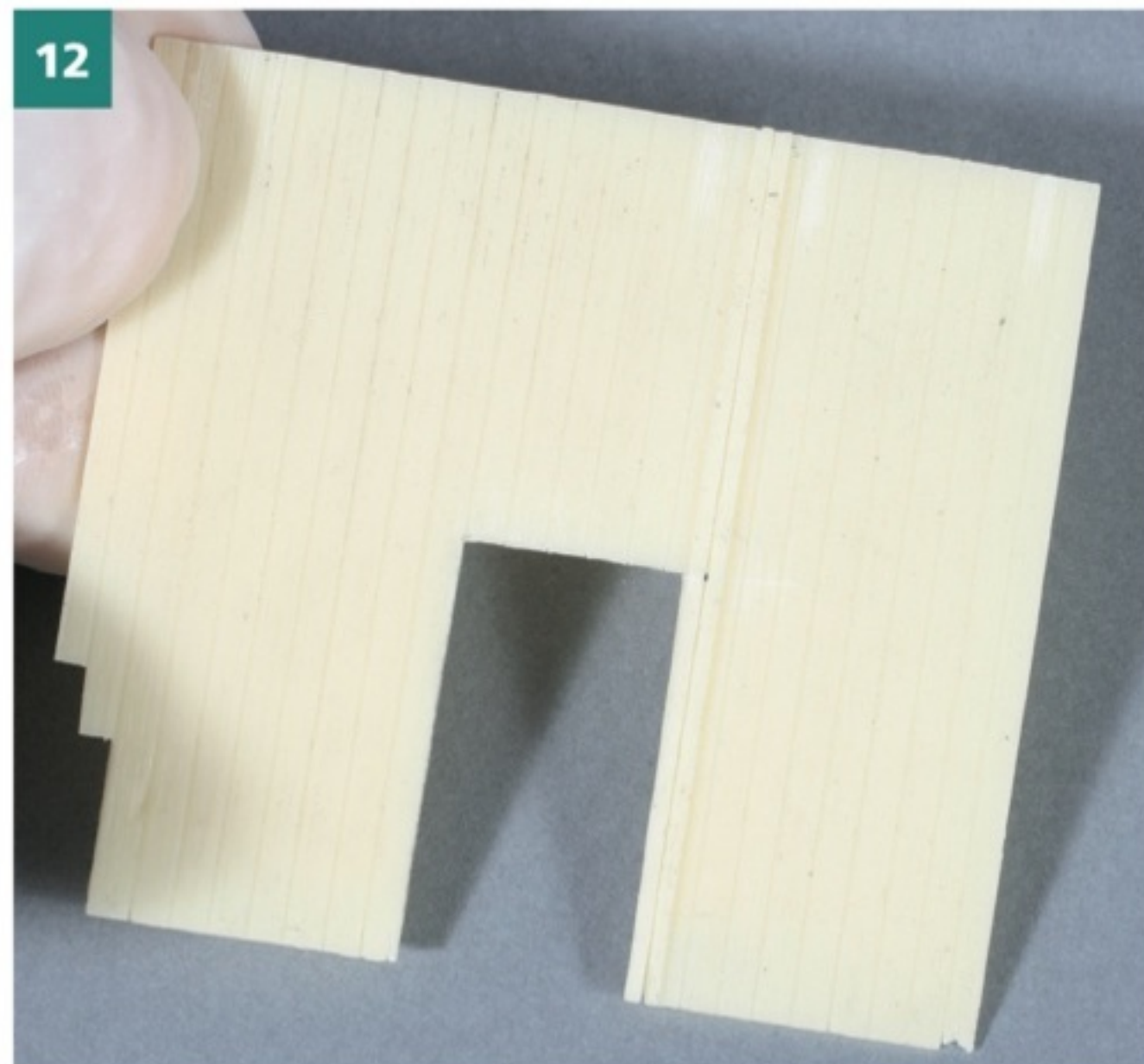
11



Step 12

To complete the frontage in a suitable style, it seemed sensible to install a sliding wooden gate. For this I used a piece of cast resin sheet imitating boards into which I made some incisions to represent the effects of damage and decay.

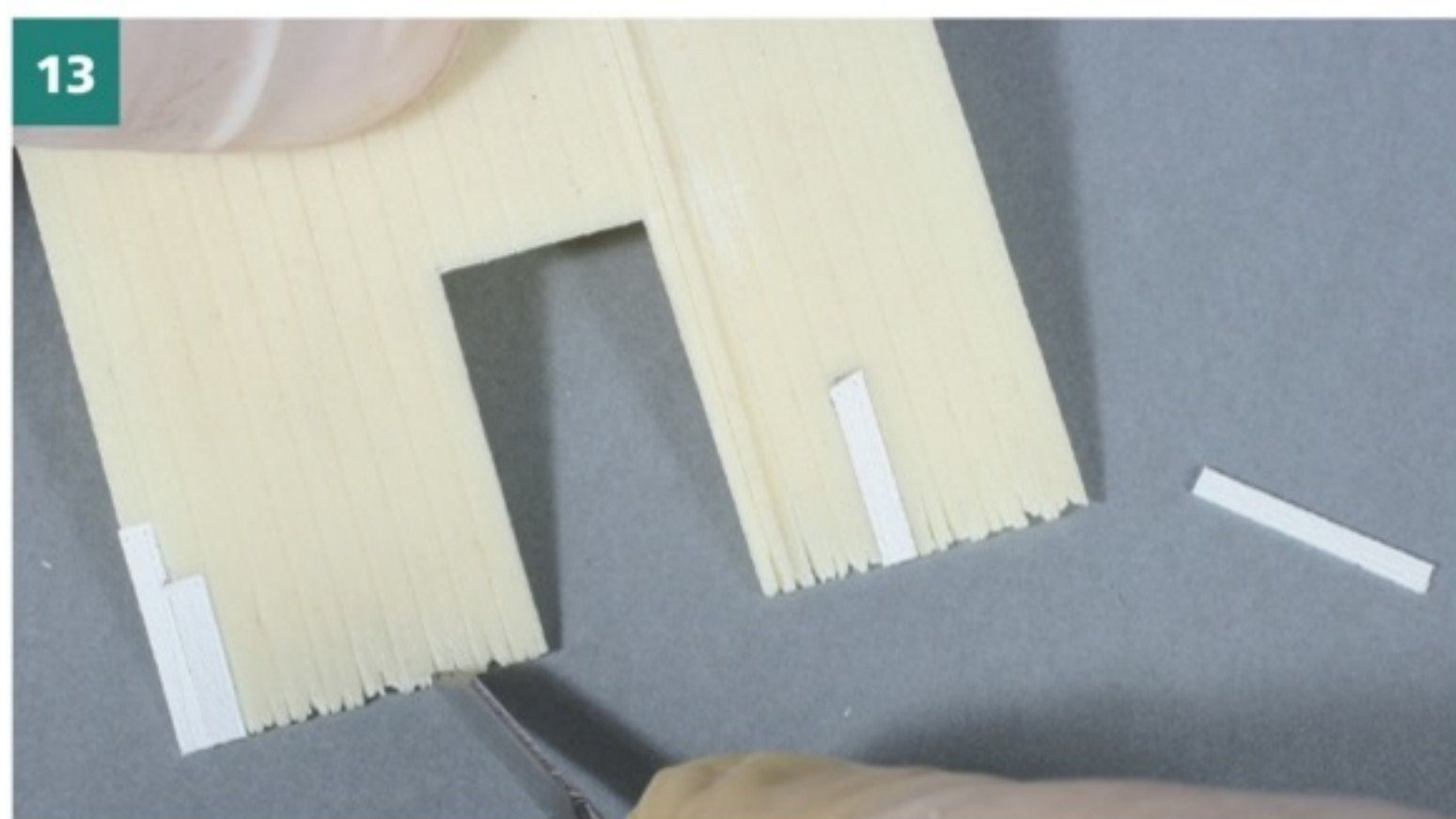
12



Step 13

This was done using a small sharp blade to trim the bottom of the door. I patched some places with new boards made of plasticard – in fact the offcuts of the strips used for the window frames ... nothing is wasted!

13



Step 14

The entrance door was then detailed with great care, using Aber etched brass and Grandt Line moulded plastic parts. I do not set limits just because it is a smaller scale – details count, as close-up photos will show, so it must be as perfect as possible.

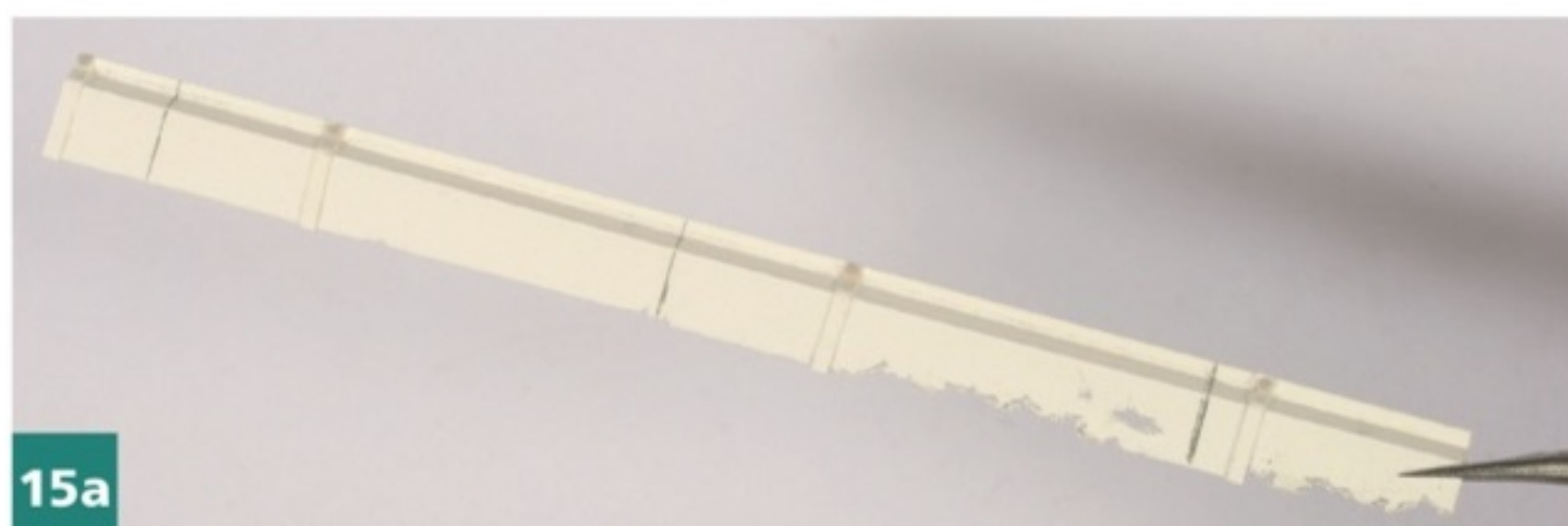
14



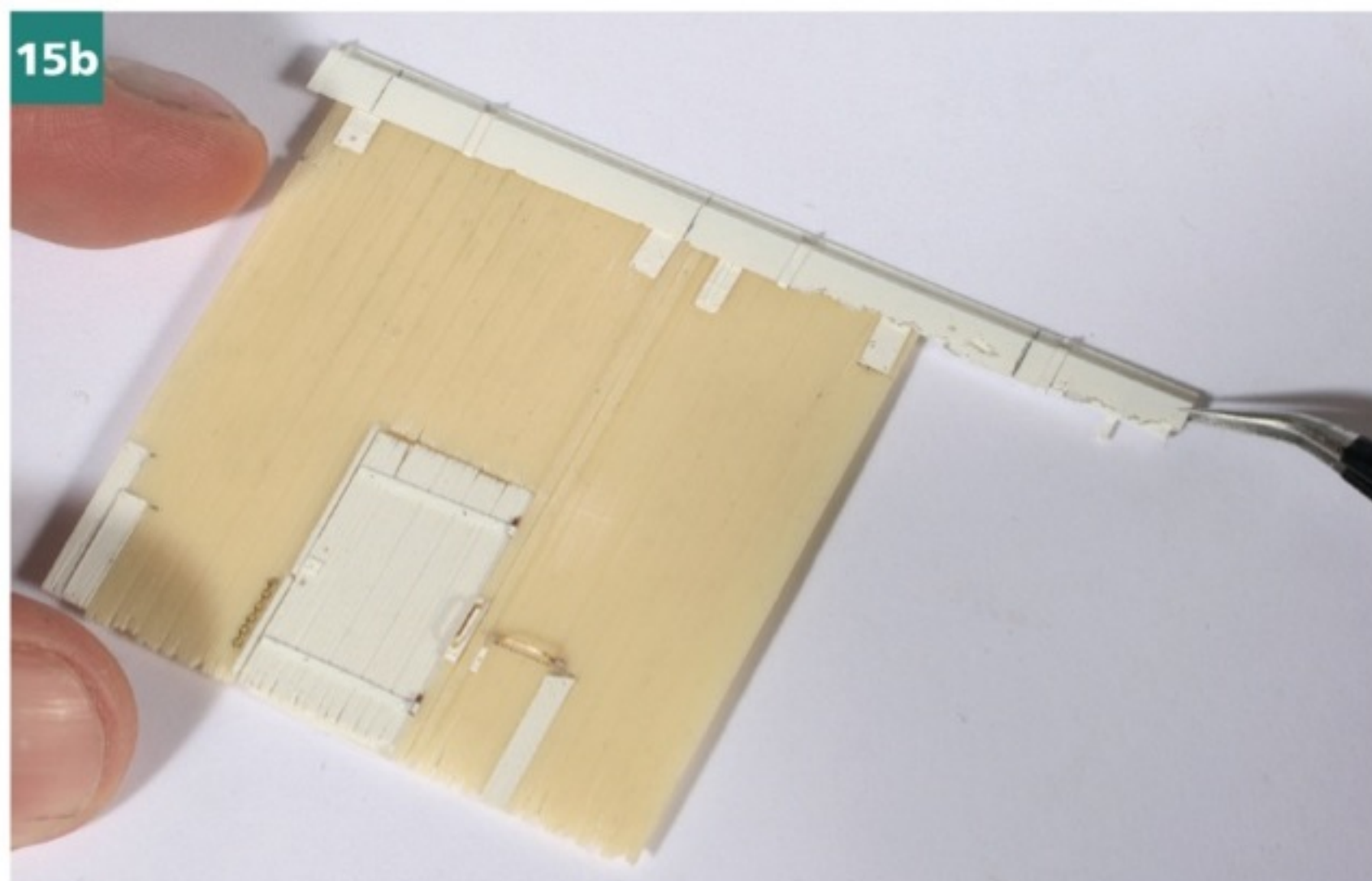
Step 15

To hide all or part of the sliding rail (and to avoid the tedious task of making it!), I made a protective cover for it, as can sometimes be seen. I used 0.13mm plasticard shaped after lightly scribing grooves on the back. Some more strips of the same thickness, three or four bolts, and there it is. I also trimmed it in places to mimic the time-worn flashing, and I checked that it still looked right by placing the unpainted part over the door.

15a



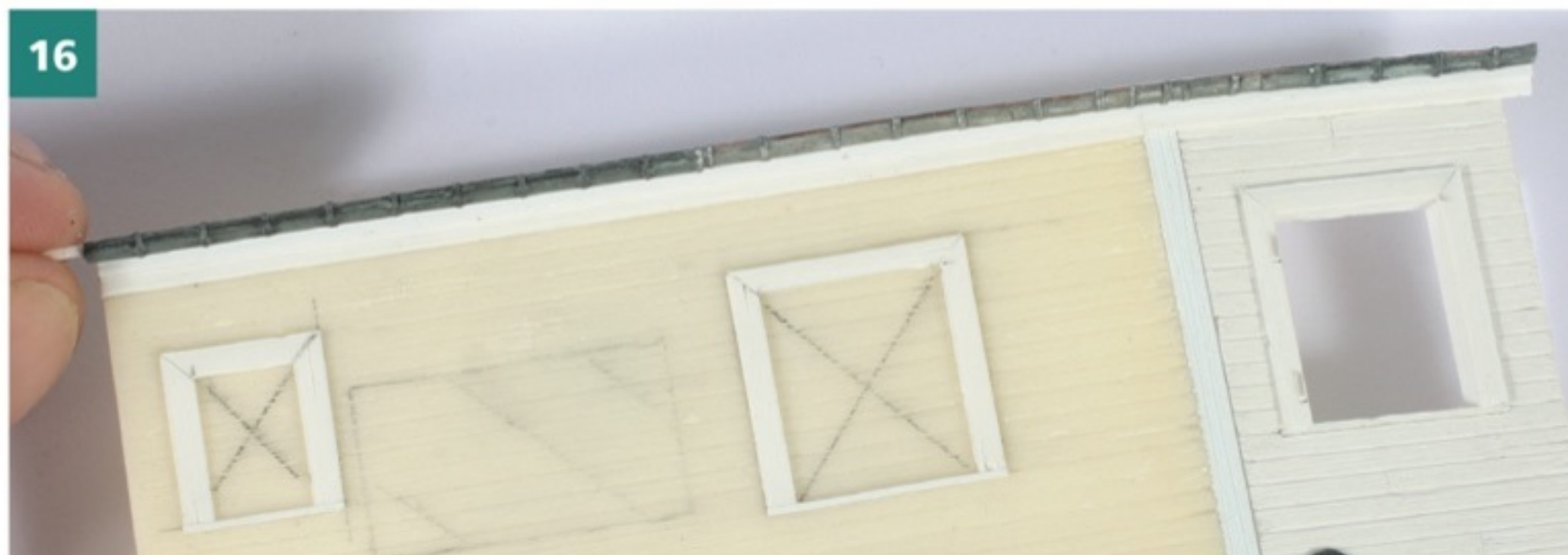
15b



Step 16

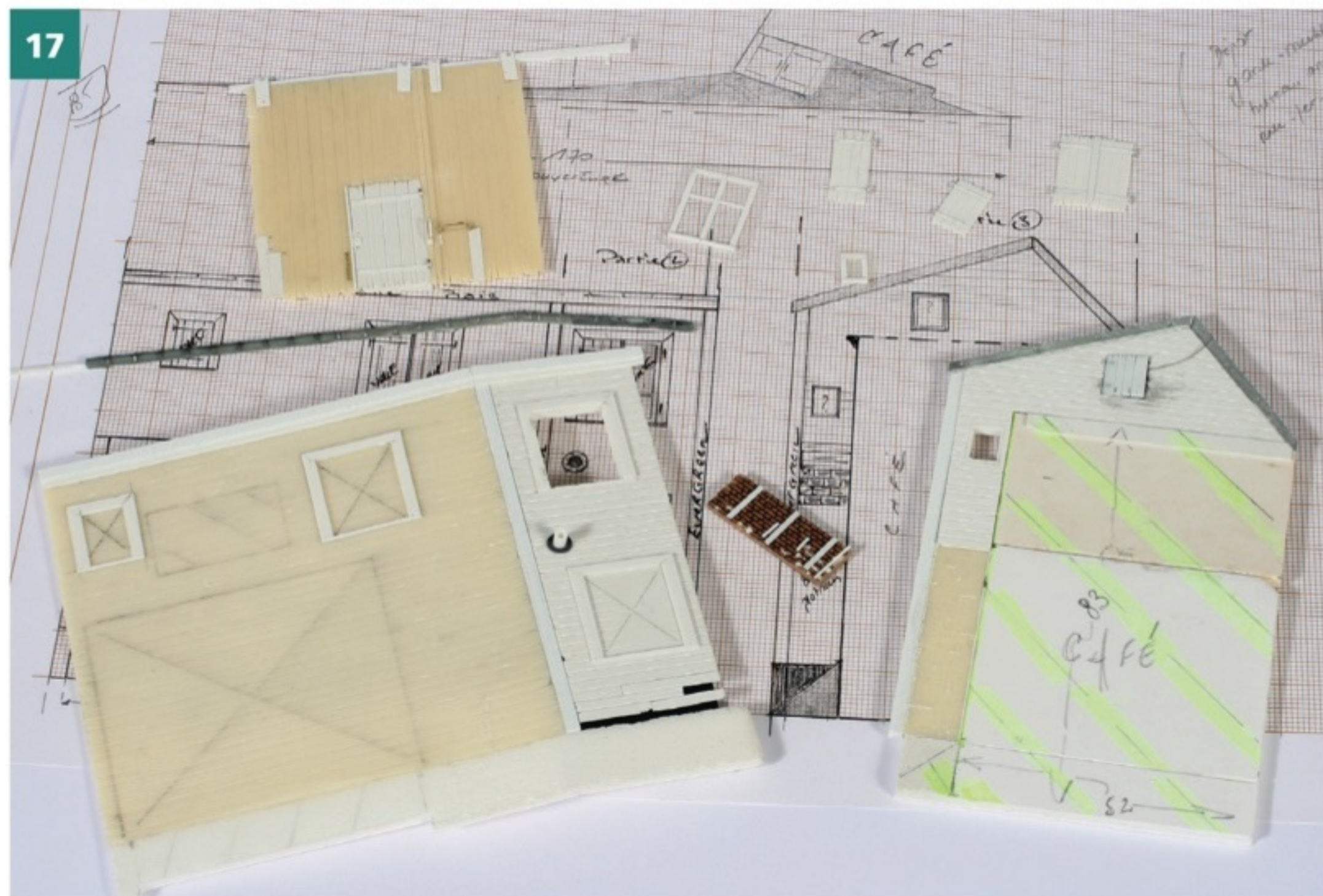
Of course, the façade needs a gutter. For this I used some MKD guttering and downpipes which I refined as a test some time ago. Such components are always stored awaiting the right opportunity to use them, which saves time.

16



Step 17

17



The components complete but unpainted, ready for a test assembly. You will notice that there are no roofs – these are not necessary given the angle the buildings will be seen when presented in the display box. If they were stand-alone structures on a layout or diorama, I would have made the roof of Flemish tiles – this could eventually be the subject of a separate article.

The result

The new elements, as yet unpainted, in place alongside the second café, test assembled in the display box.

I hope this all goes to show that, with a little imagination and/or inspiration from the prototype, some basic materials and detail parts, and a certain amount of care and time, commercial offerings can be transformed and create unique scenes.



Roger Nicholls

returns to European modelling with a compact quayside scene.

Photographs by Derek Shore.

Nordhafen

A small harbour in northern Germany

The reason behind this layout was an idea to model something which included sea water. I decided to use German HO stock and make the setting a small coastal harbour somewhere in west Germany some time in the 1970s. This period would allow diesels in the older wine red livery as well as the recently introduced blue-beige. The line would be freight only, with as great a variety of wagons as possible.

The size of the scenic section is 63" x 12".

The frame was made from 2" x 1/2" timber with various pieces of MDF and ply for the backscene and end boards. The trackbed is 6mm ply. All the wood was painted both sides to protect from moisture.

The track plan is basically a four track 'inglenook' shunting puzzle. A three track fiddle yard allows for trains to shuffle on and off with different wagons.

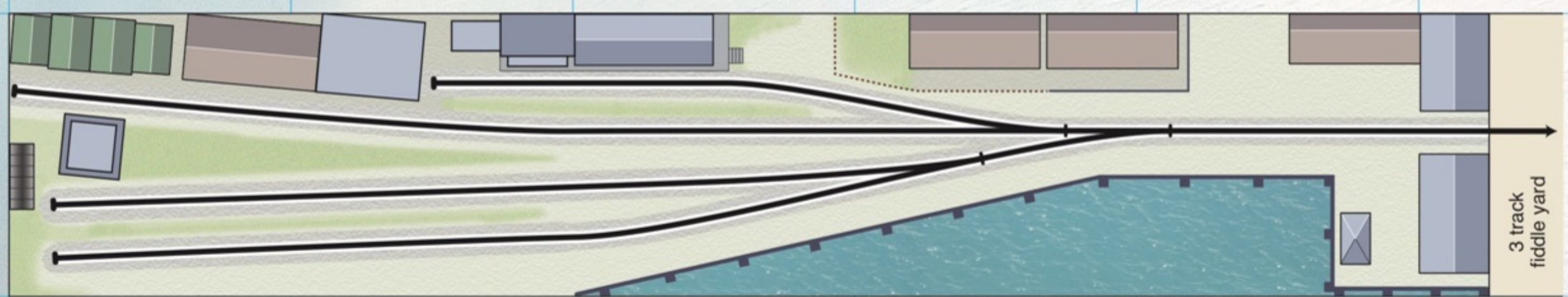
Below left

B-B diesel 211 070-8 has just positioned a lowside wagon carrying general goods by the Raiffeisen store to be unloaded.

Below

B-B diesel 290 189-0 propels a rake of assorted wagons onto the quayside siding. The harbour handles a wide range of goods.





Nordhafen Overall size of scenic section 5' 3" x 1'. Each grid square = 1' x 1'.





Left

A type Ucs twin silo wagon under the loading pipes at the grain store. The inlaid track was achieved with 2mm card, painted and weathered.

Below

A BR211 delivers two wagons of timber to the dockside. Note the uncoupling tuft by the goods shed, carefully positioned two standard long wheelbase wagon lengths from the buffers.

Right

On the two middle sidings the uncoupling tufts are situated in the road.



The track is Peco code 100; the points are small radius, with one wye. The points are worked simply with piano wire. The side of the rails was painted with dark brown and the sleepers an earthy grey.

The ballast and what greenery there is comes from 'Country Scenes', who offer a range of reasonably priced scenic materials on their website.

The inlaid concrete area was made with 2mm card painted with Humbrol matt 28 then suitably weathered with powders and a soft pencil.

The harbour walls were made with lolly sticks, stained with thinned Humbrol paints.

The backscene was painted using 'Moody Blue' matt emulsion paint matchpot samples from Wilko.



The buildings were all made from kits from various manufacturers, including Vollmer, Faller, Heljan, and Model Power. Particular mention must be made of the grain silo, which was made from a Piko kit, cut in half to give twice the frontage in low relief. The same applies to the block of flats, which was made from an Auhagen kit, with the walls repainted using magnolia emulsion.

The structures were carefully grouped around the harbour office end of the scene to disguise the access to the fiddle yard as much as possible.

All the buildings and stock have been weathered to some degree using matt varnish, black washes, and dry brushing.

The sea water was started with filler applied with a palette knife, then stippled with a stencil brush. When this was completely dry, it got a coat a black with some blue and green blended in. It was finished with three or four coats of gloss pine varnish which gave a nice deep, wet look.

Uncoupling is achieved with my usual and now much-used method of grass tufts between the rails. The loop is taken off the coupler at one end of all the wagons, which then of course have to be placed on the layout facing the same way. This method works equally well with European or British style couplings.





Above
290 139-0 departs hauling just three vans – train lengths are necessarily limited. The sea water is three or four coats of gloss pine varnish over black paint with some blue and green blended in applied over stippled filler.

Right
The short open wagon carrying bales of cotton will need to be unloaded while the weather stays fair. The harbour walls were made with lolly sticks, stained with thinned paint.



Control is standard analogue DC from either KPC or Gaugemaster feedback controllers, which give excellent slow speeds for shunting.

As ever, thanks to various people for their assistance – my regular operators and transport crew, Doug Houston, Robin Nixon, and Neil Burrows; Geoff and Alison of Contikits for their usual helpful service, particularly procuring those hard-to-find items; and Millennium Models of Morley.



Above left
290 139-0 slowly enters
the grain siding to collect
the loaded silo wagon.

Above
From most viewing angles,
the link to the fiddle yard
is hidden by the buildings.

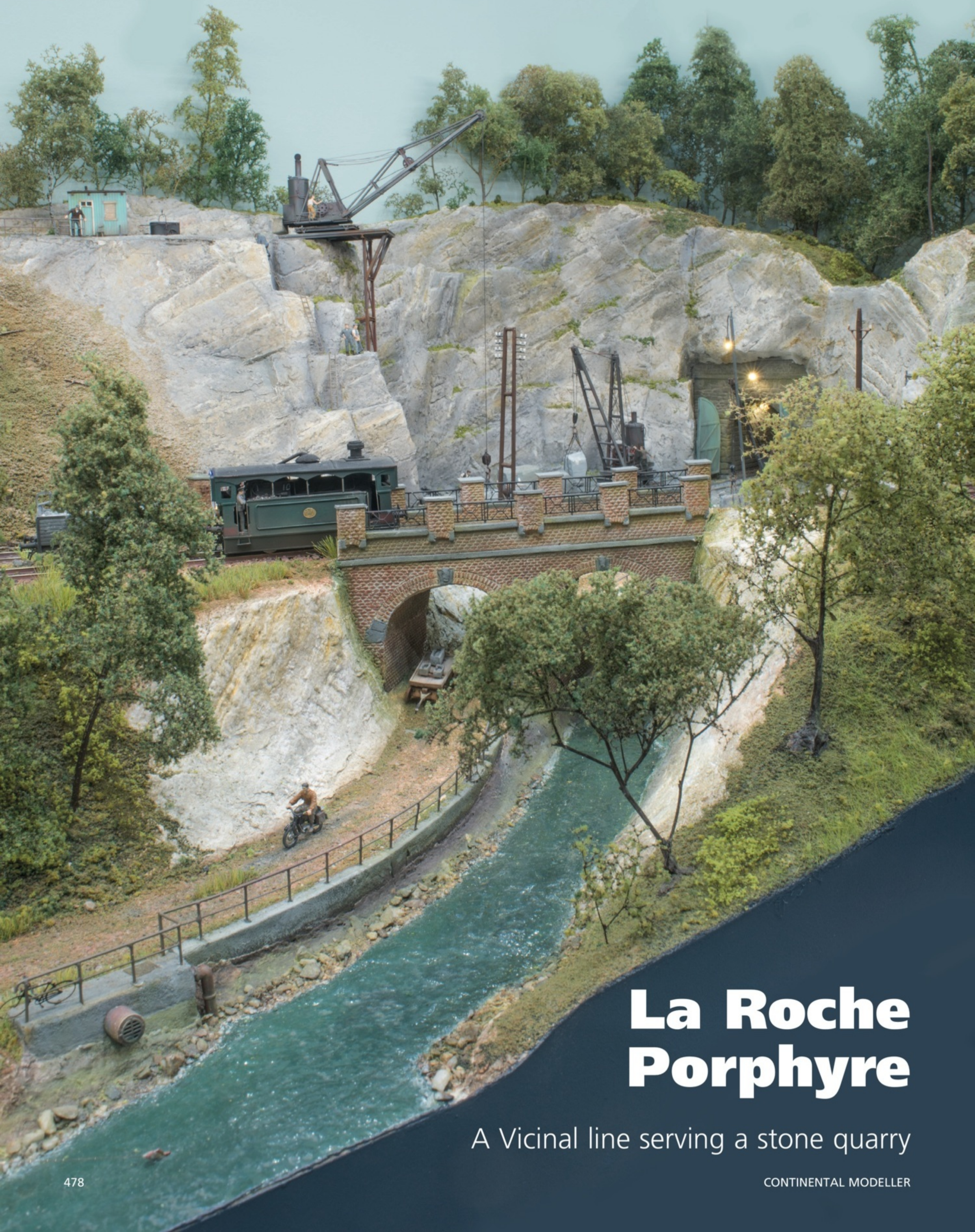
Right
The window cleaner
is hard at work as wagons
are shuffled between sidings
– the van door must be shut
before the train departs.

Forthcoming show

The layout is due to
appear at the Thirsk
show in the Town Hall
on Sunday 28th July.

See *Exhibition Diary*
for more information.





La Roche Porphyre

A Vicinal line serving a stone quarry

Rudi Nelissen has expanded his award-winning micro layout, as always planned.

The basis of this project is the original micro layout *La Roche-en-Ardenne* with which I won the small layout competition at the Modelspoor exhibition in Leuven in 2016. It represents a regional line in the province of Luxembourg, part of the Vicinal tramway network (SNCV), between La Roche and Melreux, with a depot and workshop at La Roche where the steam tram locos and later the diesel trams were maintained.

The mini layout was inspired by an old postcard from the early 1900s, a photo of the station and the workshop. After a look at Google Earth and a visit to the actual location, measuring and photographing, the foundation was made for the micro layout. (See CM July 2017.)

To be within the required dimensions for the competition, the first module could only feature part of the site. From the start it was clear that this would be just the beginning of a larger project with the rest of the site to be built on two additional segments. This article is only concerned with these two new modules.

Module 2

The second module is an extension of the depot of La Roche with some more of buildings that belonged to it.

The shed next to the main building served as storage for coal, sand, and equipment. There was also a steam-powered machine to prepare the sand for use by steam locomotives.

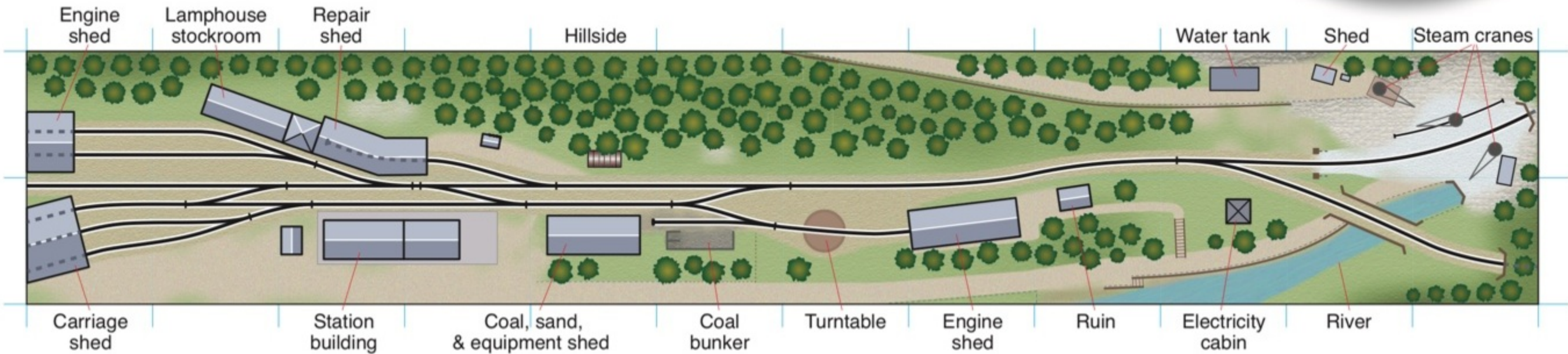
Visible in that old postcard there was a small turntable, just in front of the entrance to the repair shed. This was used to turn a locomotive or wagon if this was necessary for the work to be performed. Due to lack of space I decided to leave this out when building the original micro layout. The turntable has subsequently been given a better interpretation on module 2, namely near a building some distance away which was also used as an engine shed. This shed still exists. After a fire, it was rebuilt and is now a clubhouse for the local scouts. During its restoration, attention was paid to the past and its authentic elements were preserved.



Top
A worker opens the gates of the shed to pick up sand that has been prepared for use by the steam locos.

Above and right
A small type 15 Vicinal loco waiting on the turntable.

Left
A Vicinal tram loco crosses the bridge with a wagon loaded with cobblestones just collected from the quarry.



La Roche - Porphyre Overall layout size 3600mm x 600mm. Each grid square = 300mm x 300mm.



By locating the turntable on this part of the site, operating has also become more fun. In addition, because of this and by placing an extra turnout, I also had room for a small coal bunker. This is not authentic but built from my imagination.

Module 3

The third module does not reflect reality in La Roche. I wanted something which added to the operation, and had some working features beyond the railway. I had seen several pictures of steam trams used to transport cobbles and bluestone. Since the layout is set in La Roche, in the middle of the Belgian Ardennes, I felt it would be appropriate to choose a small (fictitious) porphyre stone quarry.

Since steam cranes were used in such quarries in the steam era, they had to be included. On the basis of old photographs, I have constructed three of them in brass. They all work and loads can be moved within the quarry. The cranes can also be used for loading and possibly unloading the wagons.

On top of the excavated hill, next to a small wooden shed and the large steam crane, there is a water tank on a brick base. This was built following the example of an existing

SNCB/NMBS water tank at Muizen near Mechelen.

The quarry is mainly occupied with mining bluestone. This was processed by stonemasons making troughs and tombstones. However, the main activity was the production of the typical Belgian cobblestones. Down in the quarry, where the underground river rises above ground, we see the cobblestone masons at work. At that time, the cobblestones were cut by hand. In large quarries they sometimes had more than a hundred men in a row to cut cobbles. To protect against the sun and the other elements, cane roofs were used and these have been modelled.

The railway siding leads to the underground part of the quarry. Here we see mostly the small but powerful Cockerill vertical boiler 0-4-0T locomotives at work.

The main line continues over a bridge. This bridge is based on the old tramway bridge at Katteberg in Bilzen. Further on, the track disappears into the mountain.

This completes the steam tramway layout in the Ardennes, for the time being. Though the option to model what is on the other side of the mountain remains open ...

Above left

A railway worker is filling sacks with coal, which are easier to hoist on board the locomotives.

Above

Behind the small loco shed an abandoned house has fallen into ruin.

Below

Overall view of the quarry. The water tank on the hill supplies the steam cranes.





Top
Much activity in the quarry.

Above
The operator of the
largest crane ensures that
loads are lowered safely
from the working face.

Left
Stonemasons are cutting
cobblestones by hand
while fellow workers
operate the cranes or assist
by guiding the heavy loads.

Photographs by the editor.

Boyd Misstear shows how he built a tunnel station inspired by the unsuccessful proposal for a station beneath Sedrun during the Gotthard Base Tunnel construction. *Photographs by the author, unless otherwise noted.*

Porta Alpina

An ingenious way of extending the layout



This is essentially a pictorial essay showing the steps taken to achieve the desired result – a very modern four track terminus large enough to store three short multiple unit consists of up to four coaches and one longer main shuttle platform capable of handling six coaches plus motive power. Access for passengers is provided by escalators to a terminal hall above platform level, and from there by imaginary lifts to the metre gauge Matterhorn Gotthard Bahn layout which is located above!

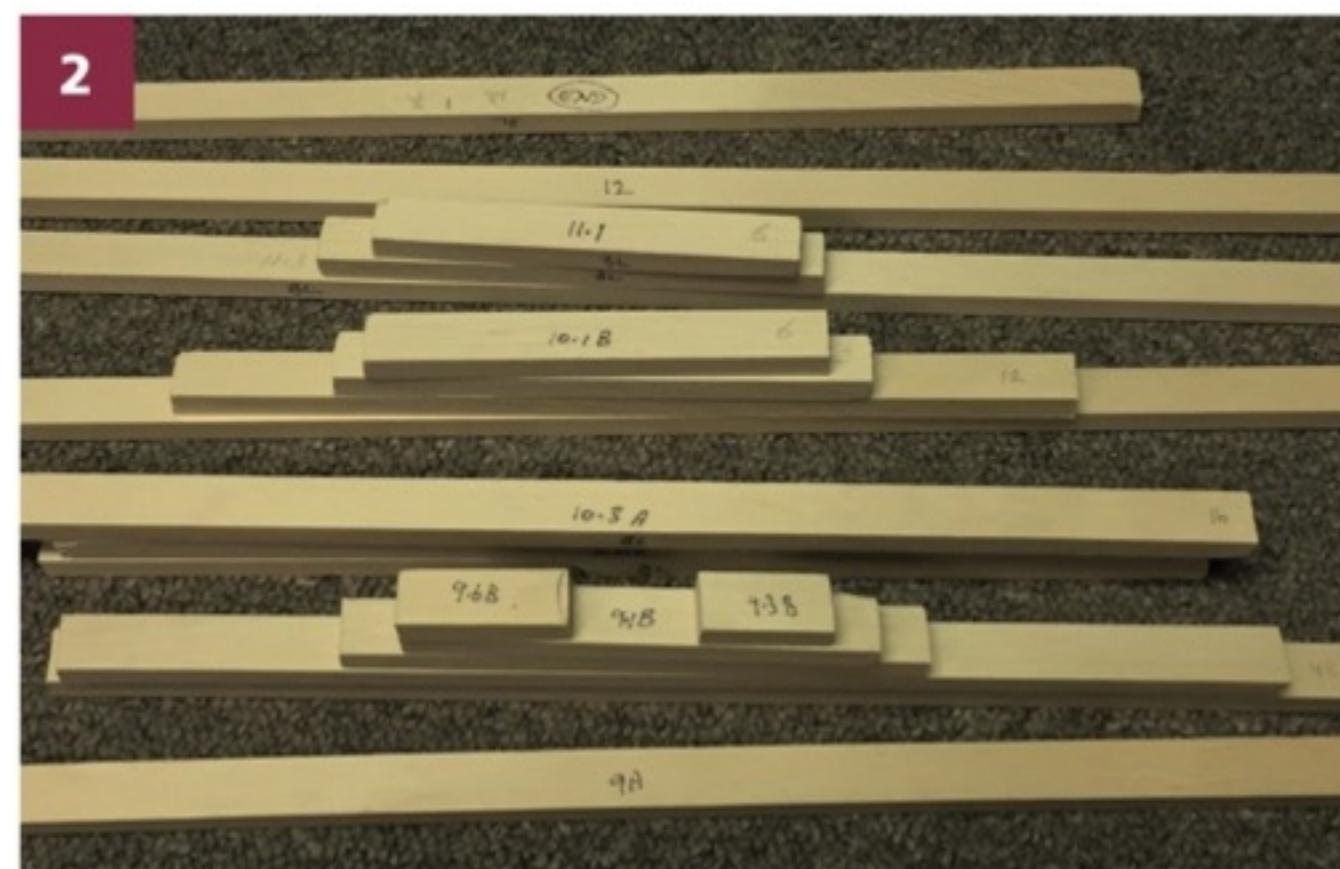
As you will immediately gather, this is in not meant to be an exact replica of the proposed prototype conversion of the single emergency station through platforms in each tunnel beneath Sedrun on the Gotthard Base Tunnel with vertical elevator connections to the outside world.

Background

Having previously built a replica (using modelling license, of course!) of the Bern Lötschberg Simplon base tunnel beneath the HOm layout to serve primarily as a staging area to hide trains during planned standard gauge operating sessions, there was the opportunity to add four relatively short parallel sidings in unused space. What a waste not to make use of this! And so the sidings were installed and effectively forgotten about until discussion in the Swiss media about a proposal to provide Sedrun with a rival gateway station to that of Andermatt, which was referred to as 'Porta Alpina'.



The idea was probably more of a marketing thought than a true engineering and practical consideration to transform the emergency access to the GBT into a regularly served location, since this would undoubtedly have had a serious impact on the prime mission of maximizing and expediting north-south freight and passenger through services. And of course there are all the other considerations of safety of use on a regular basis and so forth that were unlikely to have been considered in depth. But in the model world we do not have to consider health and safety aspects for our Preiser figures!



Top
Photograph by Hein Hopmans.

1.
The intended station site.
2.
Preparing the pine platform supports to size.
3.
Test positioning the platform supports.

Tools

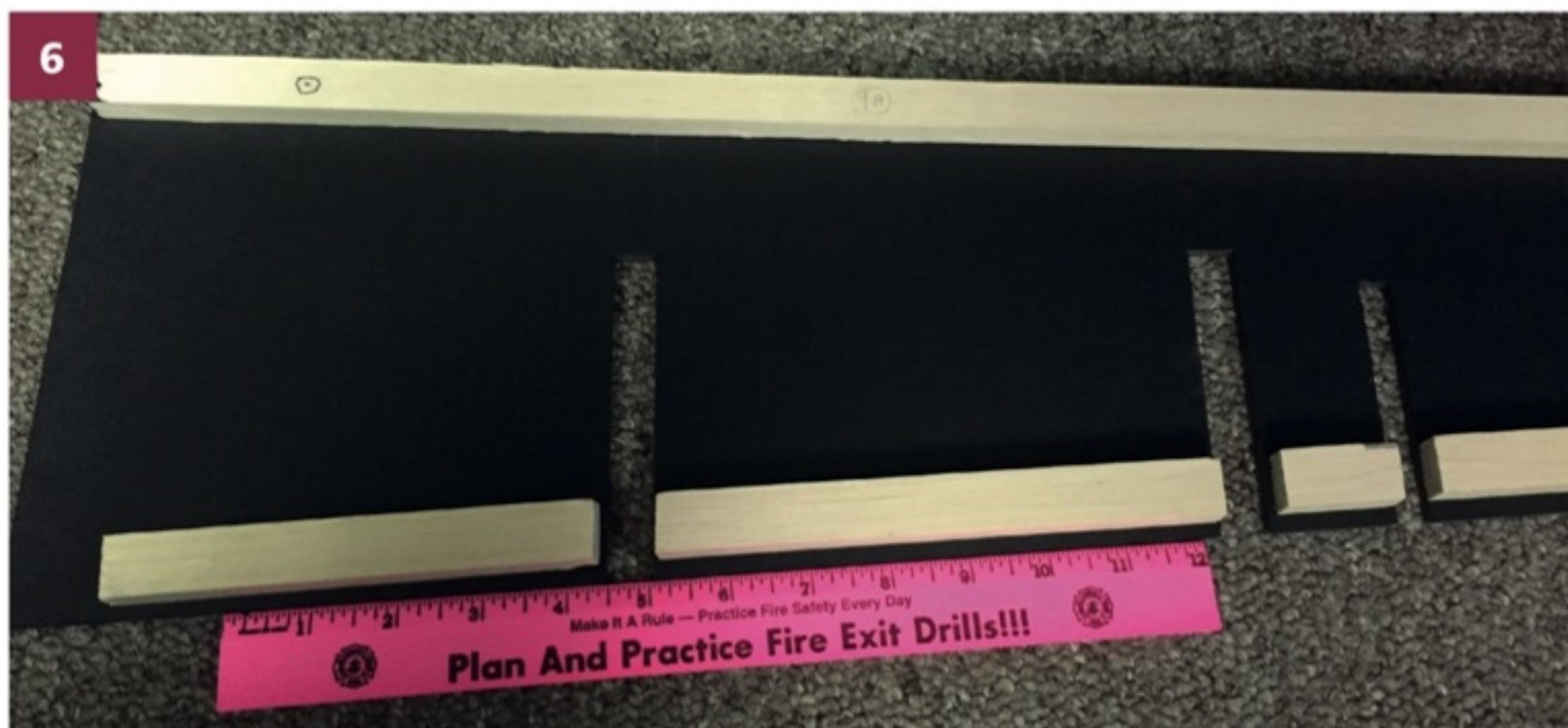
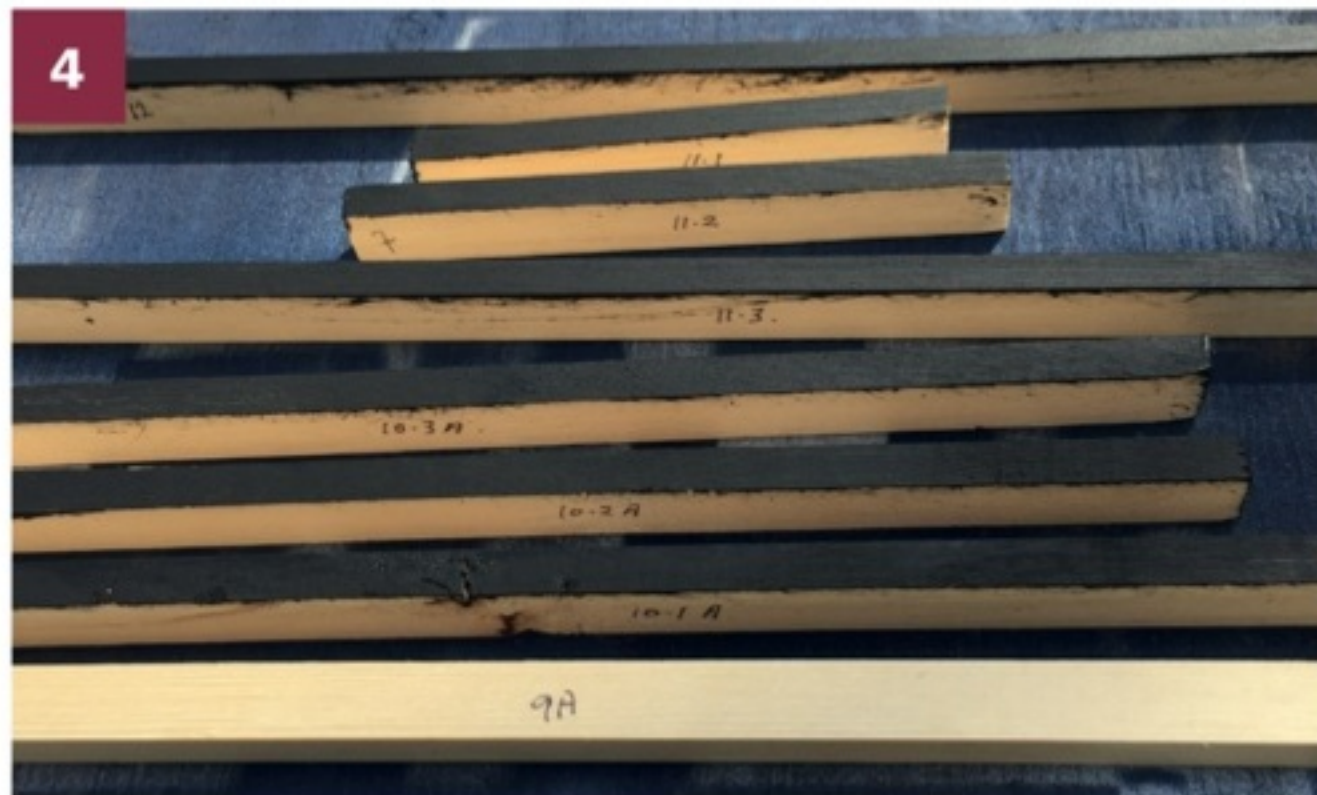
mitre cut-off saw
hot glue gun with glue sticks
wire and sprue cutters
selection of screwdrivers
fine tipped soldering iron
paint brushes (various) and stirrer
fine anti-static tweezers
measuring tape/ruler
sharp knife and blades
spring clips/clamps
scissors
cutting mat
straight edge (and flat working surface!)
suitable colour-coded wire – 22 or 26awg or equivalent
pencils and eraser
Microsoft Paint or similar graphic application

4. Paint platform supports.

5. Measure foam board for platform and backdrop.

Materials

1/8" black foamboard (quantity and size to suit)
Scotch 3M ATG Adhesive Transfer Tape #924
(for use with paper and foam board)
Pacer PT-56 Canopy Glue for styrene items
(not paper/foam board)
Tacky glue
Solder and flux
Basswood strips (for strength, and lighting mounts)
Styrene L and H profiles, sizes to suit
Cable ties
Velcro 'hook' and 'loop'
'Asphalt' colour paint for platform sides
(e.g. Woodland Scenics ST 1453)
Tamiya X-15 Light Green acrylic (to emulate BLS green)
Spray paints, e.g. Tamiya range:
AS-15 Tan (weathering bottom platform sides)
TS-1 Red Brown (rail rust weathering)
TS-15 Blue (emulate BLS blue)
TS-17 Gloss Aluminium (station signs)
TS-22 Light Green (emulate BLS green
for the vertical column covers)
LE 4100057-WW (Warm White) LED strip – 5m x 2
LE RF Wireless Remote LED Controller
12v power supply to suit number of LEDs
(LEDs etc. from www.lightingever.de)
Red LED for stop light on buffers
Track of choice
Drink stirrers or similar
EFE ref.99621 escalator set (packet of two)
Figures and other details



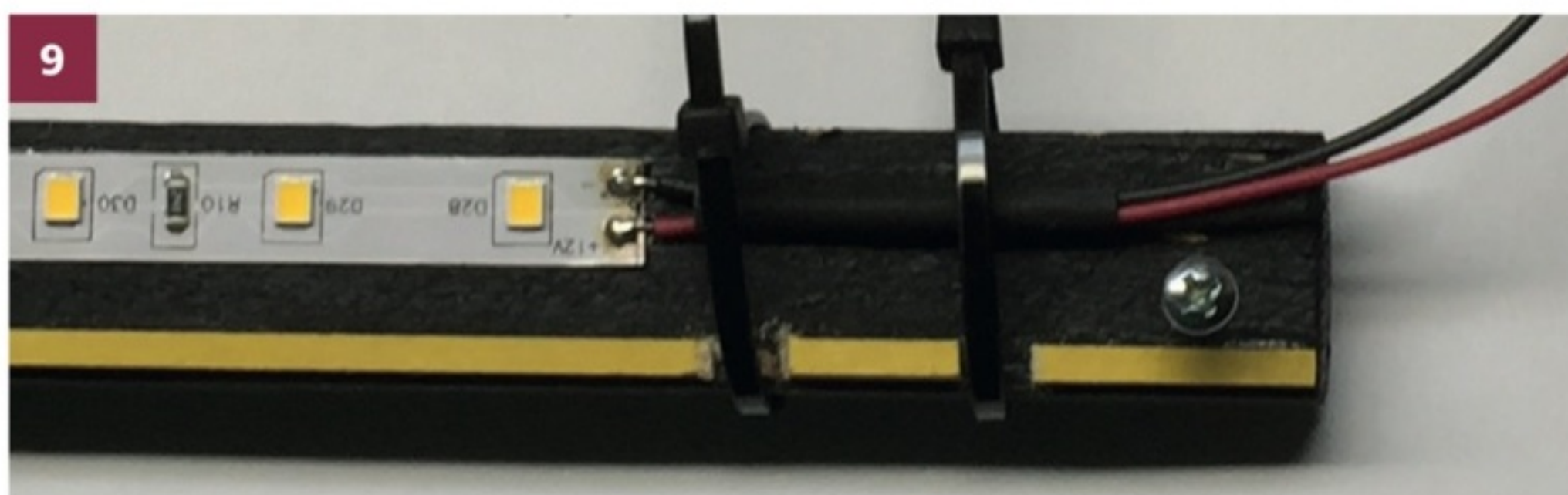
6. Cut platform and backdrop roof from foam board to fit.

7. Attach platform to supports.



8. Prepare L-shaped basswood for lighting strips.

9. Paint basswood supports and attach LED strips.



Design

To give a light and airy feel, bright colours similar to those of the modern BLS lit by warm white LED strips applied to basswood formed into L girder strengthening frames seemed a feasible way to proceed. Approaching five hundred LEDs were installed in strips at a fraction of the cost or complexity of using individual items, and an inexpensive in-line RF dimmer provides additional realism as it allows the light intensity to be altered to match conditions on the layout. I have used this controller before. It is possible to choose whether one controller controls all receivers, or whether it is used on an individual or selective group basis.

The tools and materials can be modified to suit your own circumstances.





Prepare suitable backdrop and platform surface graphics and print in matt or gloss – matt was chosen for the backdrops while gloss paper was used for the platform surfaces to simulate new tiling. I had help with this from Dale Schutz, a fellow member of the Colonial Chapter of the European Train Enthusiasts of North America, who has assisted me enormously over the years on a number of projects.

Cable ties are used for mounting the cross supports to the vertical threaded rods. The lighting strips are mounted beneath the cross supports. The numbered roof sections slide in and rest on the lighting strips.

10.
Prepare and install pine cross supports for LEDs.

11.
Mount LED strips to supports and test.

12.
Prepare backdrop graphics, print out, and cut to size.

The LED strips are cut to the desired length.

The fascia is $\frac{1}{8}$ " hardboard, with window aperture surrounds from inexpensive photograph frames with the backing removed, stuck in place using a hot glue gun, and painted the colour of choice – here a dark green.

The legs are covered by conference table curtain material held in place by Velcro for easy on and off.

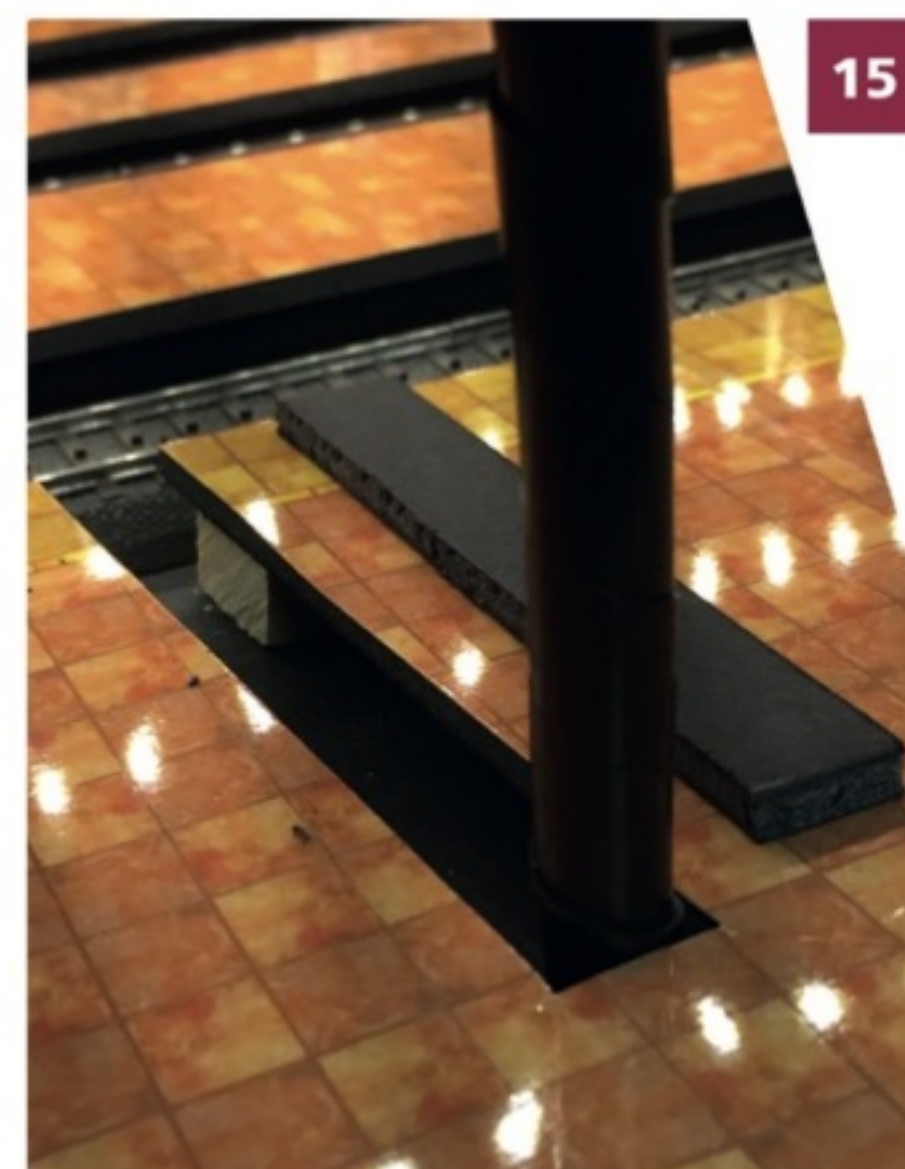
Normal safety precautions should be applied: for example, spray painting is undertaken outside or in a suitably well ventilated facility. Take care when using tools, glues, and paints, and – of course – have fun!



13.
Attach platform and backdrop prints to foamboard

14.
Insert backdrops and platforms.

15.
Install platform infills.



16.
Cover vertical supports
and add signage.
Finally, detail with figures
and fixtures.
Photograph by Hein Hopmans.



Right
The new Porta Alpina station
under the existing layout.



Below and below right
The underground station
of Porta Alpina is visible
through two windows
in the fascia, neatly framed.





Simon Holtby reveals how he managed to find space for a third layout in the railway room.

Photographs by the author.

The Nishikawa line

The result of random purchases

In my younger years I had always modelled the British railway scene and gave absolutely no thought to modelling anything else. It was in 1981, during my first trip to Japan that, purely by chance, I came across a model shop in Kyoto. Venturing inside I was amazed and impressed by both the quality and price of the wide range available. I just bought a couple of N gauge items (the most common scale in Japan) as souvenirs, without any intention of ever going beyond that. Subsequent trips saw more small purchases, again with no view of doing anything other than displaying them on a shelf.

Over the years I had concentrated my modelling on my Scottish *North West Highland Railway* (RAILWAY MODELLER August 2013 and August 2016). Readers who may have read these articles would note the relocation of this railway to a much bigger room, enabling me to also construct a TT layout (Hook Norton – RM February 2019) in the same room. While these two layouts occupied the majority of the space, a small corner remained unused corner – this provided the opportunity and motivation to build a small Japanese layout. I use the term ‘unused’ loosely as the layout actually sits partly over the fiddle yard of the TT layout.

Below

Nishikawa station and town. A preserved 2-4-0T and vintage stock (all by Micro Ace) ready to depart – such tourist trips are a feature of a number of lines I have stumbled across during my travels in Japan. The single unit diesel railcar (Green Max) in the further platform is a local service.



Inspiration

While still being excited and impressed with the ever growing Shinkansen network, I find that on all my visits to Japan I am drawn to the enormous variety of smaller lines, usually operated by just one- or two-coach trains. They seem to exist everywhere, using both electric and diesel traction. The trains themselves are often brightly coloured and have supporting infrastructure that seems to reflect their diminutive scale.

As an example, at Idakiso on the Kiishi line, I observed a scene which epitomised what I was trying to achieve: small train, short platform, quaint offset station building, and a small depot in close proximity.

Why Nishikawa, and where is it?

So why call the layout *The Nishikawa Line*? As it is not based on any particular geographical location, there was no obvious name forthcoming. However, the building for the small single-platform station came with a pre-printed station nameboard – ‘Nishikawadai’ – so I shortened it slightly, hence the name. Just out of interest I checked on Google Maps and discovered that there are actually two so-named stations in Japan – one in Hyogo prefecture and one in Fukushima prefecture.

My Nishikawa line bears no relation, other than the common name, to either of these. However, the backscene, made from a montage of printed screen shots of Japanese scenery, does include an image of the iconic Mount Fuji, thereby theoretically locating the layout somewhere in central Honshu. I do not want to go beyond that in defining its location or else imagination would have to be compromised!

Approach

There was no overarching approach to planning and building the layout. The space constraint was obviously going to limit what could be achieved – so no Shinkansens whooshing around! I suppose the underlying objective was to have fun. Part of having fun is to sometimes just watch the trains go round, so a continuous run, in some form, was desired. This was accommodated using a looped-eight as the basis for the layout design.



Above

The rolling stock depot, with the small branch terminus, Nishikawadai, beyond.

Right

A diesel loco (Micro Ace) ready to depart the quayside with a short freight. The single unit diesel railcar is by Tomix.

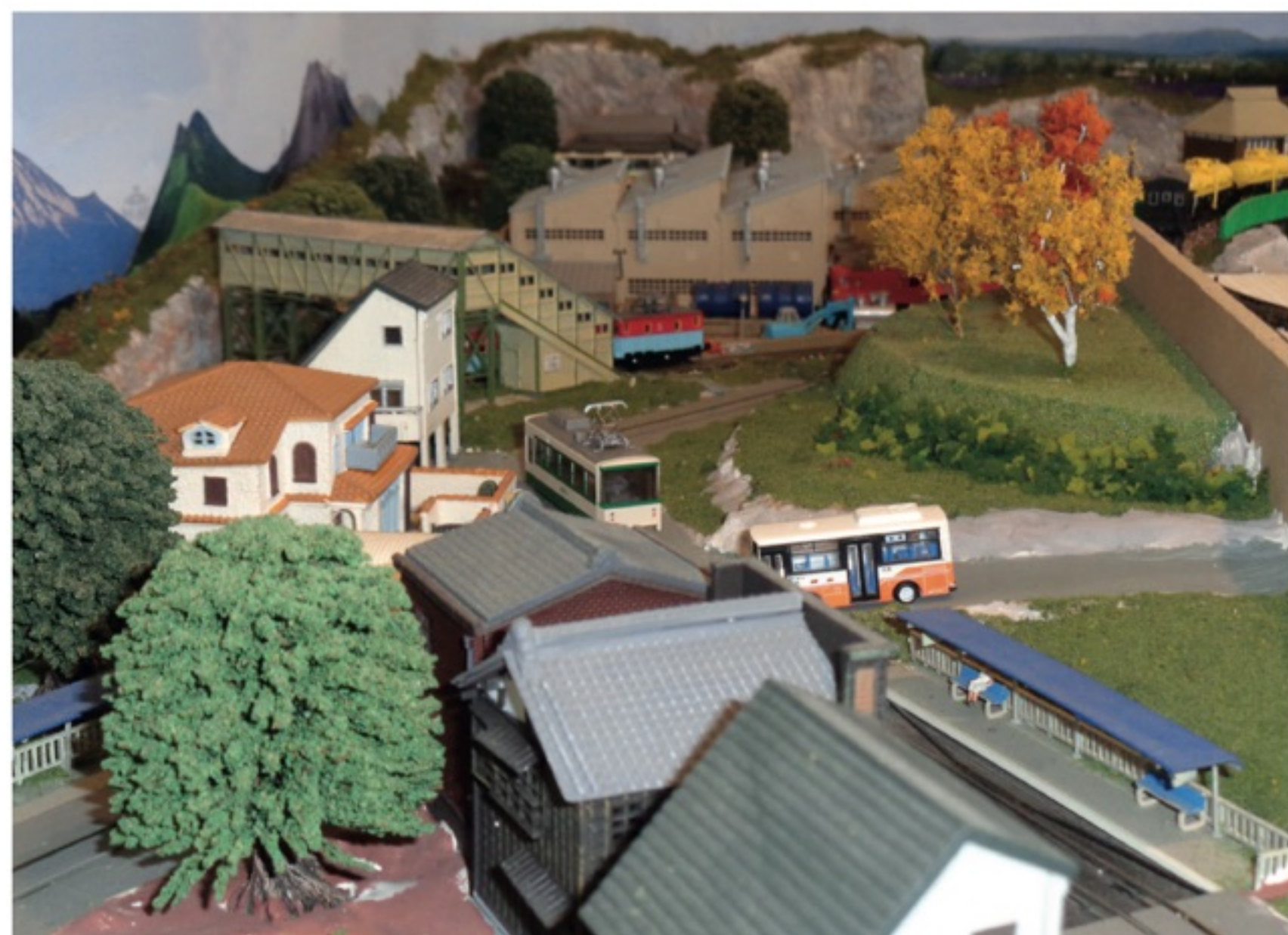


Below left

The left side of the layout, with a small tram (Kato) making its way along the winding track. The side of the depot can be seen in the distance.

Below

A passenger train consisting of two single car units (Modemo) rounds the curve into the station area; it will reverse into the platform. These units are based on the Hakone-Tozan Railway.





NISHIKAWA

Overall size: 1320mm x 1270mm. Each grid square 300mm x 300mm.

Below

Nishikawa quayside, station, and town, with the depot and the branch terminus beyond.



Layout

While necessarily being a very limited space layout, I was keen to get some interesting operating potential. There is a main station area which manages to incorporate a small railcar shed, only achievable by using just single or two-car units, as well a small quayside for freight operations, similarly short in nature. The space behind this station was used for a small town, the nature of the buildings reinforcing the Japanese identity.

The far corner of the layout was used for a small terminus, with a siding to provide a destination for freight traffic.

The fiddle yard is, necessarily, small in size but, again utilising the benefits of short trains, is able to accommodate five trains, in addition to having a passing loop.

A small tram line, connecting into the main station area, has been provided in the area behind the fiddle yard. This is rather sinuous in nature, thereby extending its apparent length, and utilises proprietary inlaid trackwork. There are a few more buildings along the route to add interest.

Technical

The baseboard construction is a grid of 2" x 1" timber topped by 6mm MDF board. The MDF just provides the foundation as insulation board above this forms the trackbed, as this is easy to cut, eliminates most noise, and takes track pins easily. The only place the MDF is visible is as the water surface by the quay.

Originally two separate boards were constructed, held together by bolts. As the boards are so small, they are now permanently fixed together and can be moved, if necessary, as a single unit.

Track is substantially all Peco, using the small radius Setrack points. The only exceptions to this are the two inlaid tracks on the quayside and the inlaid track of the tram route, these both using Japanese products.

As with all my layouts, control is plain 12v DC. The wiring is so simple that there are no isolating switches, the points providing all isolation required.

Buildings

In all my British layouts I have very much enjoyed making all the buildings from scratch. However, for the Nishikawa line I have used off-the-shelf buildings or kits throughout. Part of this decision was based on a desire to ensure a proper Japanese appearance and the other was being spoilt for choice by the great range of very attractive and good value items available.



Rolling stock

Stock has been accumulated on an *ad hoc* basis from purchases in Japanese model shops over a number of years, with a focus on interesting items representing more obscure lines. I have purchased models from a large variety of manufacturers, including Tomix, Kato, Tomytec, Green Max, Kawai, Micro Ace, TGW, OneMile, BanDai, and B Train Shorty.

Conclusion

The layout has achieved its primary objective of being fun to operate as well as providing a dedicated place to run my collection of Japanese models. It is very revealing that all of my family find this layout far more interesting and attractive than my other larger and more 'accurate' British layouts. Perhaps they see the fun and imagination in it!

I would like to thank my wife for letting me completely monopolise the games room!

Above
The single unit diesel railcar (Tomix) lays over in the loop as a small diesel shunter (Micro Ace) couples to the tank wagons (Kawai).

Below
The layout in its cramped position. The fiddle yard of my TT layout is below the Japanese layout and my British N is on the right.



Guy Tyteca recalls a January 1976 visit to the famous mountain line in Styria while it was still worked by steam.

Photographs by the author.



The Erzbergbahn

Ore, rack, snow, and steam – an unforgettable experience

Back in 1976, after some skiing in the Tyrol, I decided to travel east to discover the central part of Austria, particularly Styria, where I had heard about the famous Erzbergbahn rack railway. It was January, deep in the winter, and there was a lot of snow!

The Erzberg is a standard gauge line some 20km long of which around 14.6km between Vordernberg and Eisenerz is equipped with Abt rack system because of the steep gradients. This difficult line, opened in 1890, had five tunnels and eight stone viaducts, with an average gradient of 6% (1 in 16.6) and a maximum of 7.1% (1 in 14.1). It was built to carry good quality iron ore from a rich open cast mine.

Above

The class 97² 0-6-2 tanks were the main motive power on the line. 97.203 has modified water tanks and a Giesl ejector chimney.

Below

Working hard banking a train out of Vordernberg.

The summit at Präbichl, about halfway, is at 1,238m above sea level while the pit at Eisenerz is at 692m and Vordernberg is at 768m, with the main workshop and facilities. Typically, one loco pulled the consist of twenty hopper wagons while another pushed. A train of empties left Vordernberg with a loco at each end, then both locos were put together at Präbichl, in order to go down through the longest tunnels and allow the crews to breathe! At Eisenerz, all wagons are loaded and, after turning the locos on the local turntable, the first half of the train was pulled and pushed up to Präbichl, then both locos go back to Eisenerz to get the rest of the consist.

There was also a passenger service, originally coaches propelled by a steam loco and later using a diesel railcar.

Three series of rack-equipped steam locomotives were built by Wiener Locomotivfabrik AG of Floridsdorf specially for this railway (final ÖBB numbering):

Rh97² – 0-6-2T of 62 tons, built from 1890 to 1908 in three batches, a total of 18;

Rh197 – 0-12-0T of 87 tons, three built in 1912;

Rh297 – 2-12-2T of 125 tons, two built in 1941: a very complicated design, made to help the German war effort as the Reich was desperately in need of iron ore. They were not very efficient, and were withdrawn after less than twenty years, suffering lots of mechanical failures. 297.402 was cannibalised to keep the other loco working.

Some of the 97s were later equipped with a Giesl ejector chimney; the 197s and 297s were built with it.

The 97s were fitted with an extension to the water tank in early 1941, which can be seen in some photos.

In 1971, there were still thirteen 97s and all three 197s in working order. The final year of steam operations was 1978.

Several of the steam locos were preserved and subsequently plinthed or gone to museums: three 97s, one 197, and one 297. On top of these, two 97s are in working order on preserved lines.





Above
O-6-2T 97.203 pauses at Präbichl having banked a train of empties up from Vordernberg.

Right
Shunting at Präbichl, preparing to proceed to Eisenerz.

Below right
In front of the Präbichl tunnel entrance, ready to depart.

Below
The banking loco hard at work pushing a train of empties.





Above left
Giesl-fitted 97.217 approaches the coal stage at Vordernberg.

Above
Snowplough 985.223 alongside a Rh97 at Vordernberg depot.



Left
97.201 and 97.205 (both still with original chimneys)
in the loco servicing area at Vordernberg.

Below left
97.205 at the coal hoist.



Below
A separate crane was used to coal 97.217.



Sources

Die Zahnradbahn Eisenerz – Vordernberg,
Lokrundschau 1981.

Eisenbahn Journal 1/85.

Von Vordernberg nach Eisenerz
– *Die Erzberg in Stereo.*

A very interesting record made in 1973. As well as the vinyl disk, there was a booklet with lots of photos, diagrams, and descriptions.

Railway Routes of Austria – the Erzbergbahn
by Norman Lamb, Austrian Railway Group 2013.

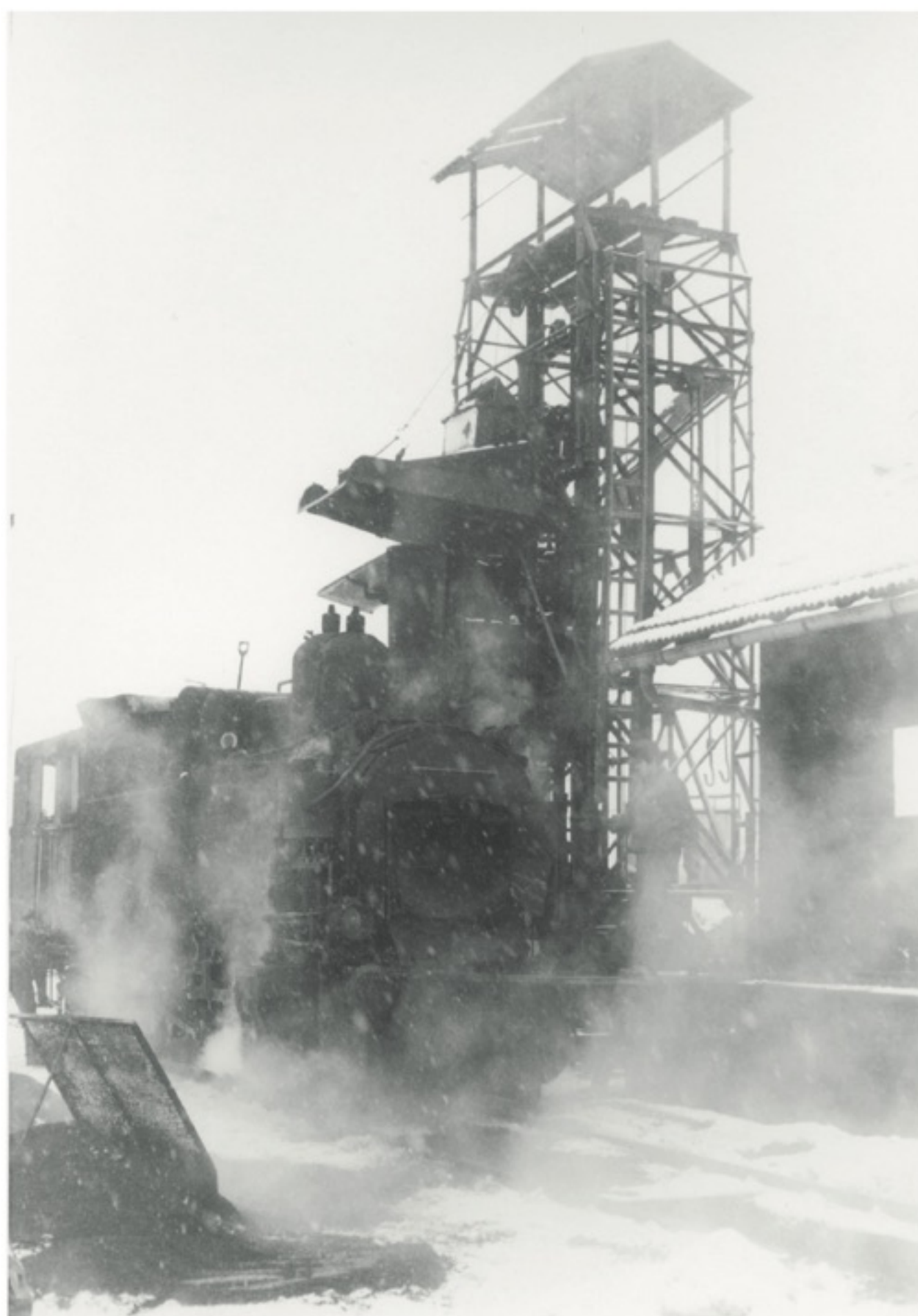
After 1978 the rack was removed and Rh2043 B-B diesels were used, equipped with a speed-dependent magnetic track brake and a speed monitor with additional speedometer for the descent. Four machines were modified and reclassified 2043.5.

All traffic ended in 1988, and there is now a museum operation over part of the route. (www.erzbergbahn.at)

Postscript

In 1978, I wanted to go back, in January again, after I read about the possibility of having a cab ride on the leading engine (after signing a waiver of liability, because of the total of 2.53km in tunnel). Most unfortunately, the regular use of steam finished the very week I was due to be there! The end of a dream ...

Instead I went not far away, to Garsten, and discovered the narrow gauge steam-worked Steyrtalbahn. But that is another story!



Above
97.201 by the coal hoist.

Right
The cab interior of 97.205. Note the two separate regulator handles for the rack and adhesion cylinders.

Left
The coal hoist. The odd snow shower made photography a challenge!

Below
The massive 2-12-2T 297.401 plinthead Vordernberg.



Mike Garforth downsizes:

N descends from the loft
and replaces G in the garage.

*Photographs by the author,
unless otherwise noted.*



Twin Forks

The latest instalment in the Seath Valley Railroad saga

It is not every layout that begins with a knock on the head but this one certainly did. It occurred late in 2014 when I was working under my loft layout and popped up, forgetting that the layout was only 12" above my head!

At 73, I decided I was getting past the stage of grovelling beneath layouts and it was time for a change. Whilst I was still in my revolutionary frame of mind I made the almost instantaneous decision to take down the G scale layout in the garage and replace it with a new N gauge layout. That way I would be working at a track height of 58", and with sufficient forethought there would be no more knee bending, back aching, or head banging work!

To ensure my decision was final I spent the next couple of weeks in the loft packing the rolling stock and structures away, removing the control panels, and ripping up the track. There was no going back now!

My grandson had the privilege of running the last train on the G layout in February and the next day I started to take that apart – I still had G in the garden to keep me happy outdoors so it was not a total abandonment of the larger scale. I did manage to sell the points and some track, and the structures went to a fellow G scale modeller from Essex – a long way from Cumbria.

Thus by the end of March I had a clear area on which to start planning the new layout. I cut sheets of card (acquired from between the rows of tins on pallets at Tesco) as I needed to replicate the 31' x 3' space the layout would occupy and the card produced the ideal solution. I used masking tape to represent the track and Peco templates for the turnouts, thus allowing easy adjustments at minimum cost.

Above

The author looking through the 'peephole' hole for viewing the tracks at Exton. To the left of the handheld controller hung on the fascia is a similar hole, elongated, for viewing Dunns.

Photo: Dave Garforth.

Below

A train of box cars hauled by an F3 A+B set departs from Twin Forks bound for Kane. I find naming the offstage destinations adds to the operational interest.

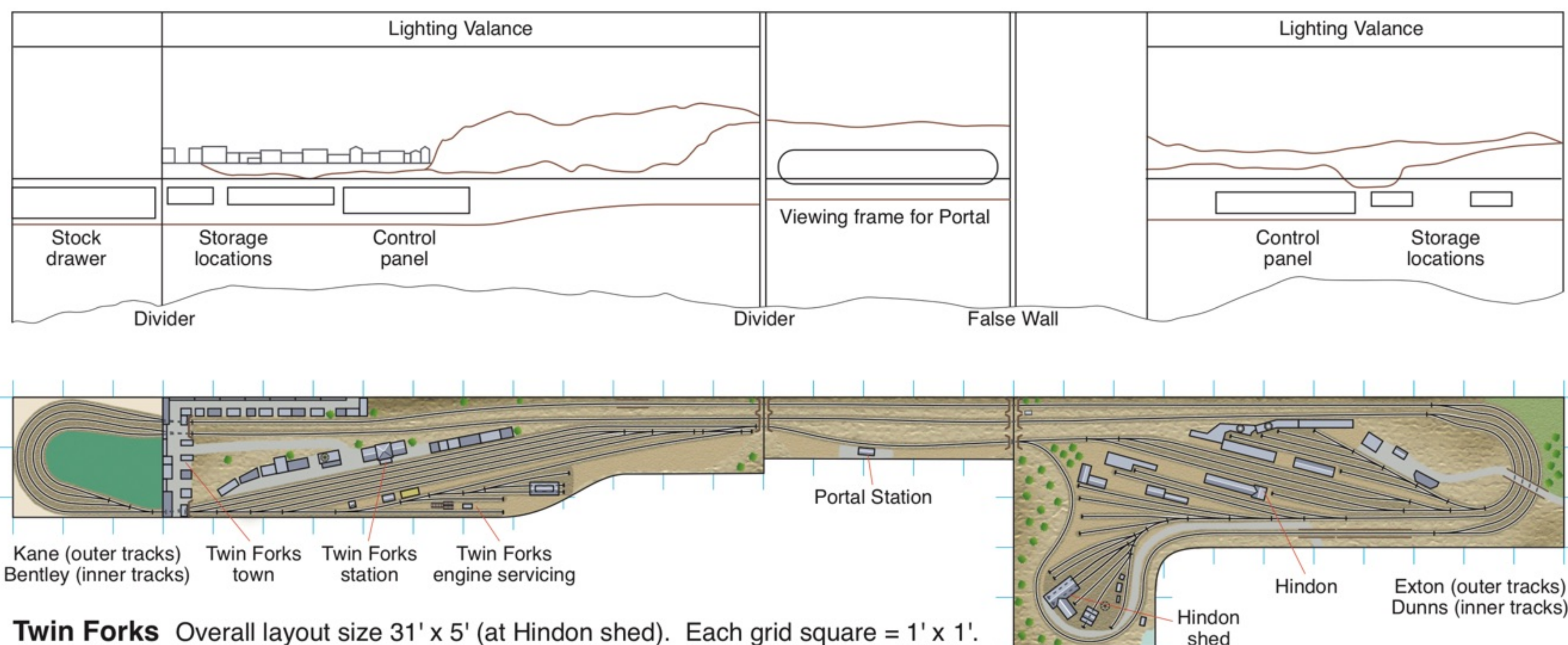




Above
The station at Twin Forks, from over the diesel shed, which can accommodate up to seven triple units. The tunnel is on the higher level track leading to Kane.

- My criteria for the new layout were:
- active storage for a minimum of eight trains;
 - potential for meaningful switching;
 - continue the theme, adopted for all my previous layouts, of a heritage railroad that permits use of locos and stock from a variety of North American railroads;
 - the ability to run trains consisting of triple diesel units and a minimum of fifteen freight cars;
 - incorporate the rock faces that had formed the backdrop to the old G layout;
 - tracks running at different levels for additional visual and operational variety;
 - suitable for a waybill system of operation.

After much head-scratching and adjustment to the initial plan, I arrived at the final track arrangement. There were to be four tracks at either end of the layout that could be used to store trains as well as provide continuous running. These tracks provide 'invisible' destinations (Kane, Bentley, Dunns, and Exton) and could easily accommodate trains of fifteen cars, as could the yard tracks at both Twin Forks and Hindon. The engine servicing facilities at both Twin Forks and Hindon and the ability to switch cars at both destinations along with several trackside locations at Hindon ensured an ample source of activity. That is not to mention the simple fun of watching trains traverse the length of the layout whilst passing through a variety of scenic elements.





The 31' length and internal layout of the garage meant this would not be a layout that could be operated from a sitting position, the 58" track height reinforcing that. There were many advantages to working within those limits: the trains could be viewed from track level, they disappear from view as they move from one area to another thus giving the added impression of 'going somewhere', with the need to follow the train as it traverses the layout – not only does the train move, so does the operator. The only real disadvantage is that anyone under 5'9" tall needs to stand on a stool to fully appreciate the layout, but I now find I often use the same stool to obtain yet another perspective. Weigh this against the normal back-bending position one has to adopt on the standard table-top layout height, and it is hardly a negative point.

All track is Peco code 80 with electrofrog points. All the points are operated by the proven Peco point motors mounted below track level, supplied through capacitor discharge units and activated by the tried and trusted stud and probe technique. The track is fully ballasted – time consuming, but well worth the effort for the added realism.

With over forty locos on the roster there was no way I was going to opt for conversion to DCC and so the operating system had to be analogue cab control. Each yard track and siding is isolated via simple toggle switches whilst the main running tracks are live at all times.

Control is via either Gaugemaster handheld throttles (both WS and W models) – referred to on the control panel as 'Local Control' or via the Total Train Technology (TTC) memory throttle, referred to as 'Memory Control'. This had been

Above
Looking south through the Twin Forks station area, with the upper tracks going through to Dunns and Exton behind and above the depot. The yard tracks each hold fourteen cars plus caboose.

Below left
The north end of the layout, Kane and Bentley loops, with stock shelves above.

Below
The stock drawer under the storage loops. Sliding trays allow two layers.





Above
The edge of Hindon, with an operating system set-up sheet, Train Order card pockets, a system map, and a fast clock (one of two on the layout). There is also a small viewing hole through to the track climbing through the mountains to Exton. Photo: Dave Garforth.

used for the former G layout and operates perfectly with the smaller scale locos. The system permits you to start a train in one location, unplug the controller and walk to the next plug-in point whilst the train continues on its way until control is regained, an ideal solution, particularly for a layout that is effectively running through three areas.

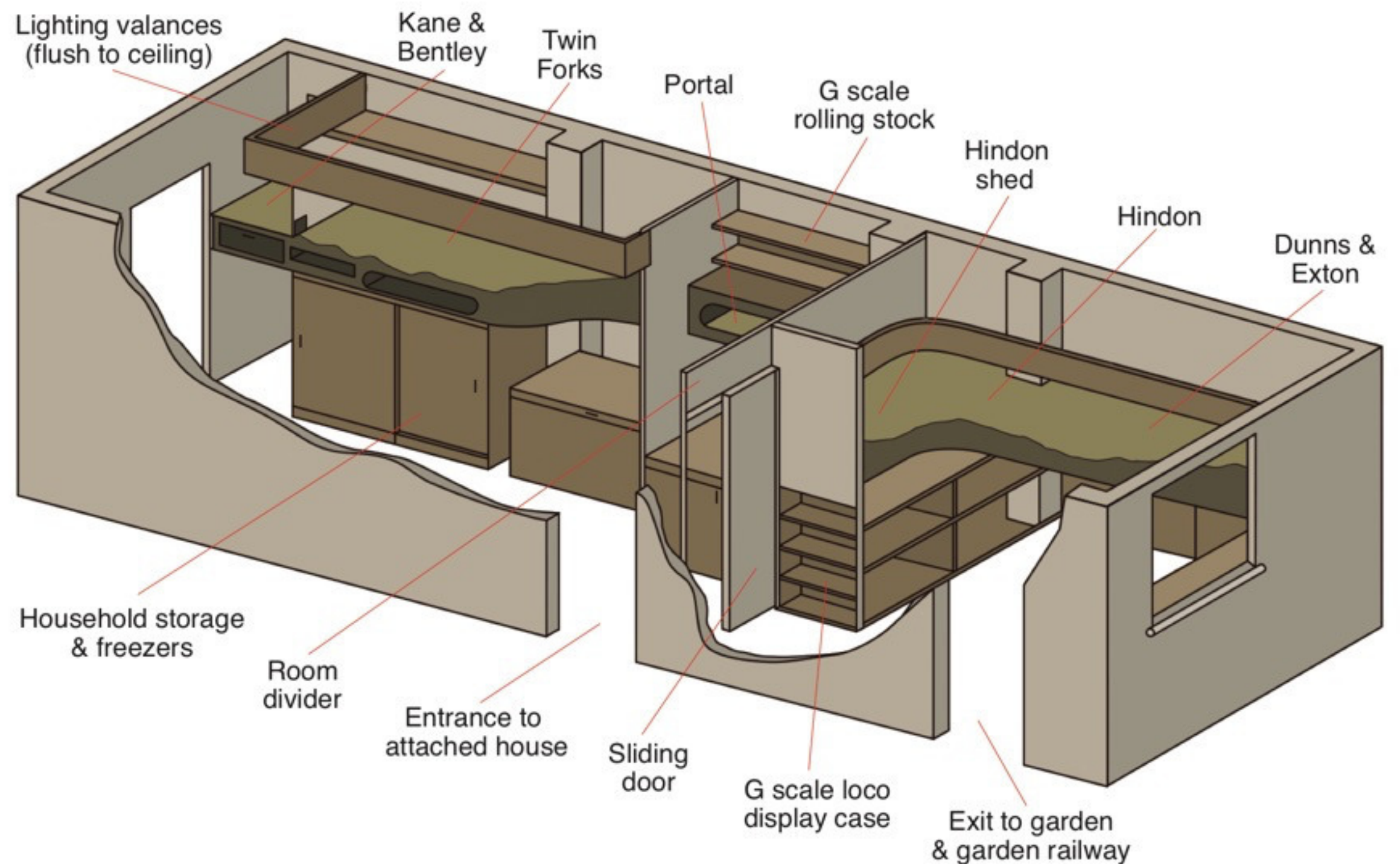
There are control panels at Twin Forks and Hindon, set into the fascia but capable of being hinged downward for maintenance, etc.

An additional feature is the provision of recessed shelves within the fascia for the temporary storage of stock, track cleaning items, or small tools, thus ensuring the layout itself does not become a workbench.

I did start producing holders for both the Gaugemaster and TTC controllers from styrene sheet to position on the fascia adjacent to the control panels but had a eureka moment when I thought of Velcro. It works a treat. With the hook section of the tape on the back of each controller and a double strip of the looped stuff at the desired locations it is a simple matter to hang the controller at the nearest point – I was initially dubious, but it works well.

The Hindon area can be operated via local control whilst trains on the outer upper level, travelling through Exton, are under the memory walk-around controller. Thus local switching can be undertaken in the lower Hindon area (shed, yards, and station) whilst trains traverse the higher level track.

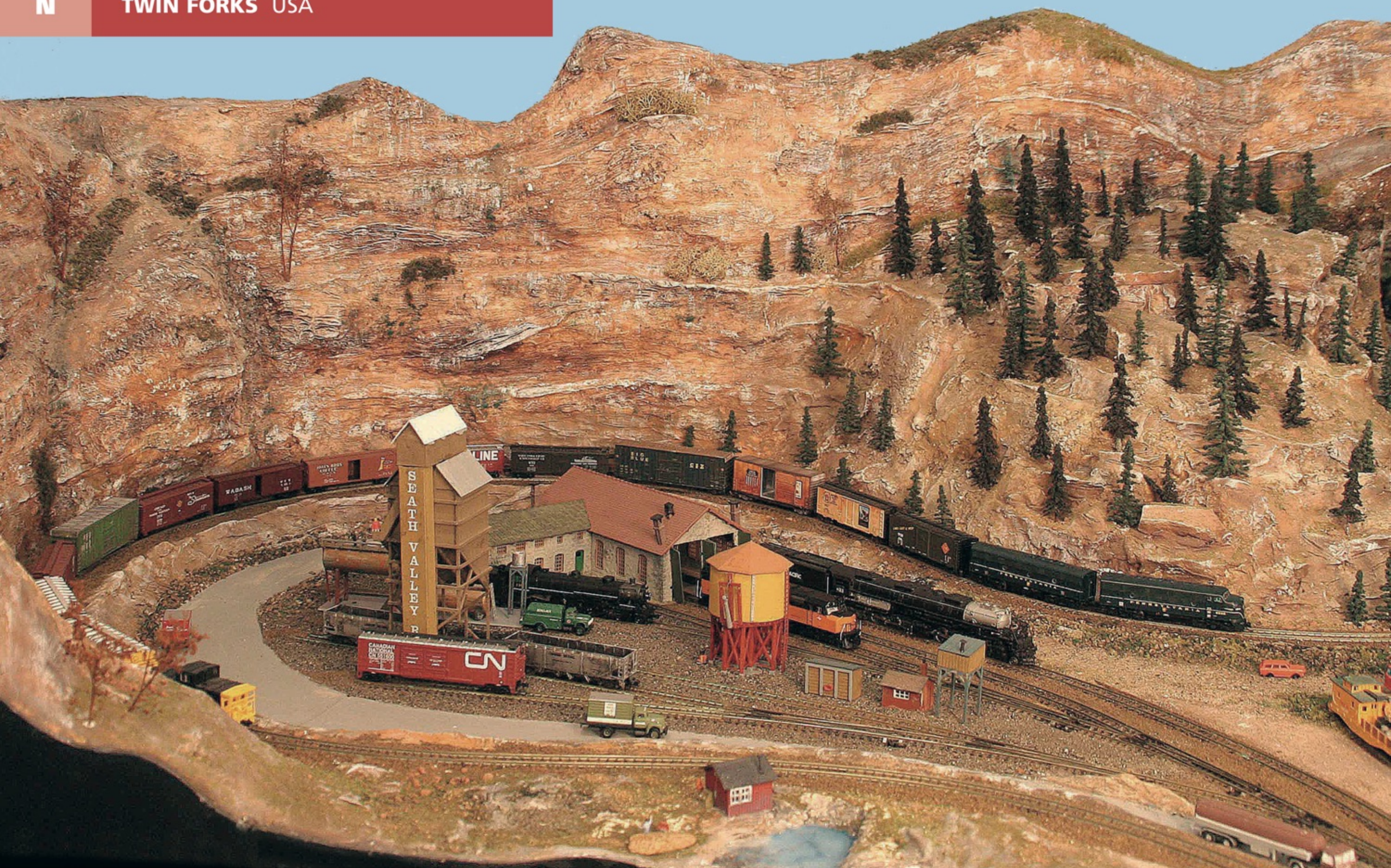
The only other deviation from straightforward track isolation and control is within the shed area at Twin Forks where the two servicing tracks are split into two sections each allowing up to two triple F units to occupy each track – which makes for a busy shed area.



Above
The main diesel facility on the layout, at Twin Forks.

Below
2-6-0 #272 (a 30-year-old Arnold model, still running superbly!) arrives at Portal, between the two main areas. The scratchbuilt water tank and depot have served on three previous layouts.





Above

A maximum length freight (fourteen cars plus caboose) snakes around the back of the locomotive shed at Hindon, the principal steam facility on the layout.

Above right

The north end of Hindon, with the shed on the left and the three levels of track apparent on the right. The double-headed steamers are bound for Exton while the diesel-hauled train is en route for Portal.

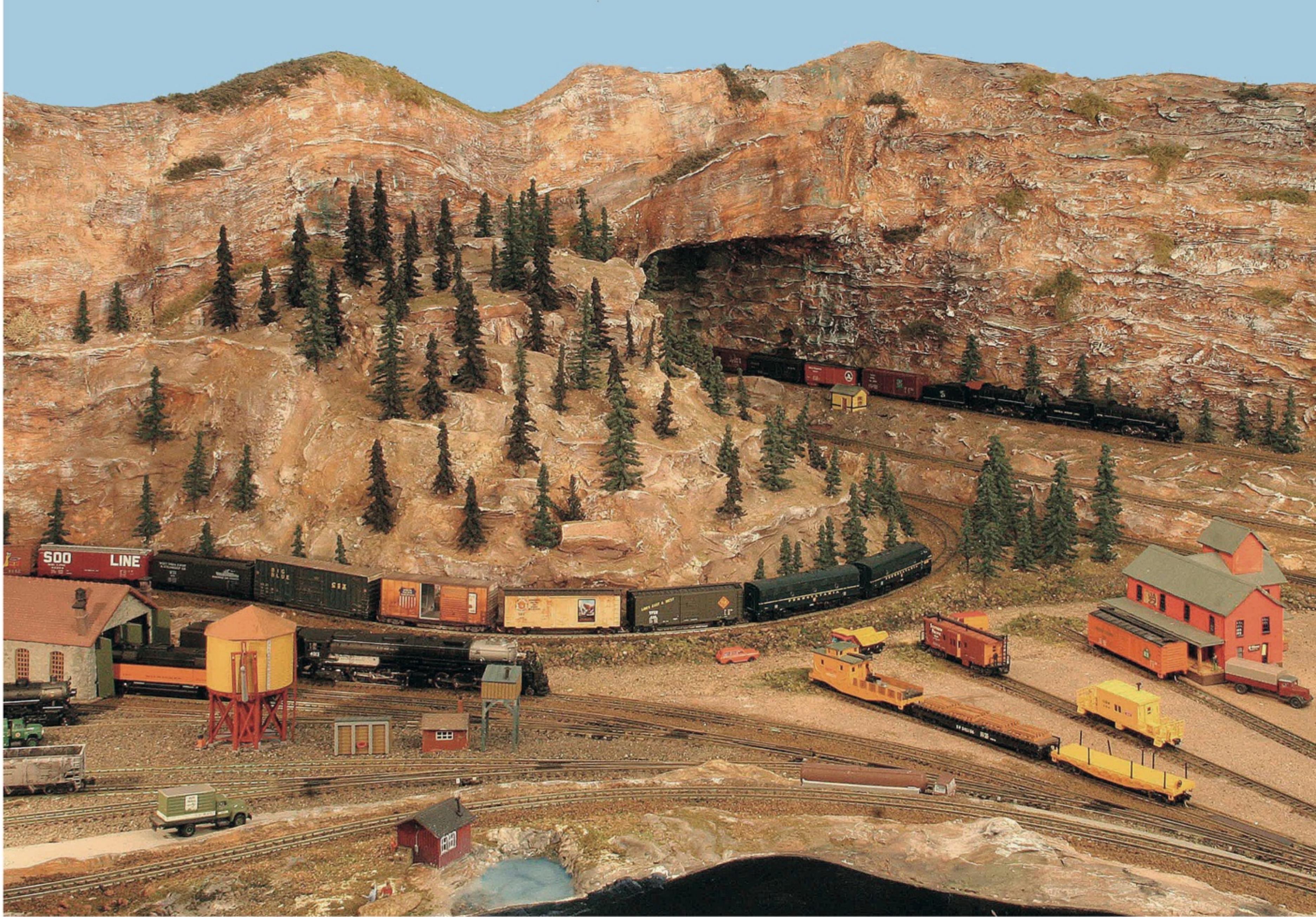
Below

The view of the depot at Hindon is carefully framed by the girder bridges which carry the track down from the high level loops at Exton.

Right

A higher viewpoint reveals the pair of Budd diesel railcars standing on the station spur as a set of F3s in New York Central livery passes on a train of reefers.







Scenery is based on styrene foam sheet, carved to shape and covered with Polyfilla with detail added using Sculptamold with the usual covering of flock material secured with diluted wood glue.

Viewing

I do enjoy incorporating viewing options on any layout and *Twin Forks* is no exception. At Hindon the raised track along the front of the layout forces the viewer to look over and into the layout, whilst the bridges carrying the track over the road and dry river bed create a restricted view into the station area. There are four 'spy hole' viewing points along the layout. These consist of lengths of 1½" plastic tube inserted through the fascia at, or just above, track level so you can look down the track and gain an almost life-like track side view. Careful placing of the tube ensures the track end is blended into the scenery and is not visible from the front.

Conclusion

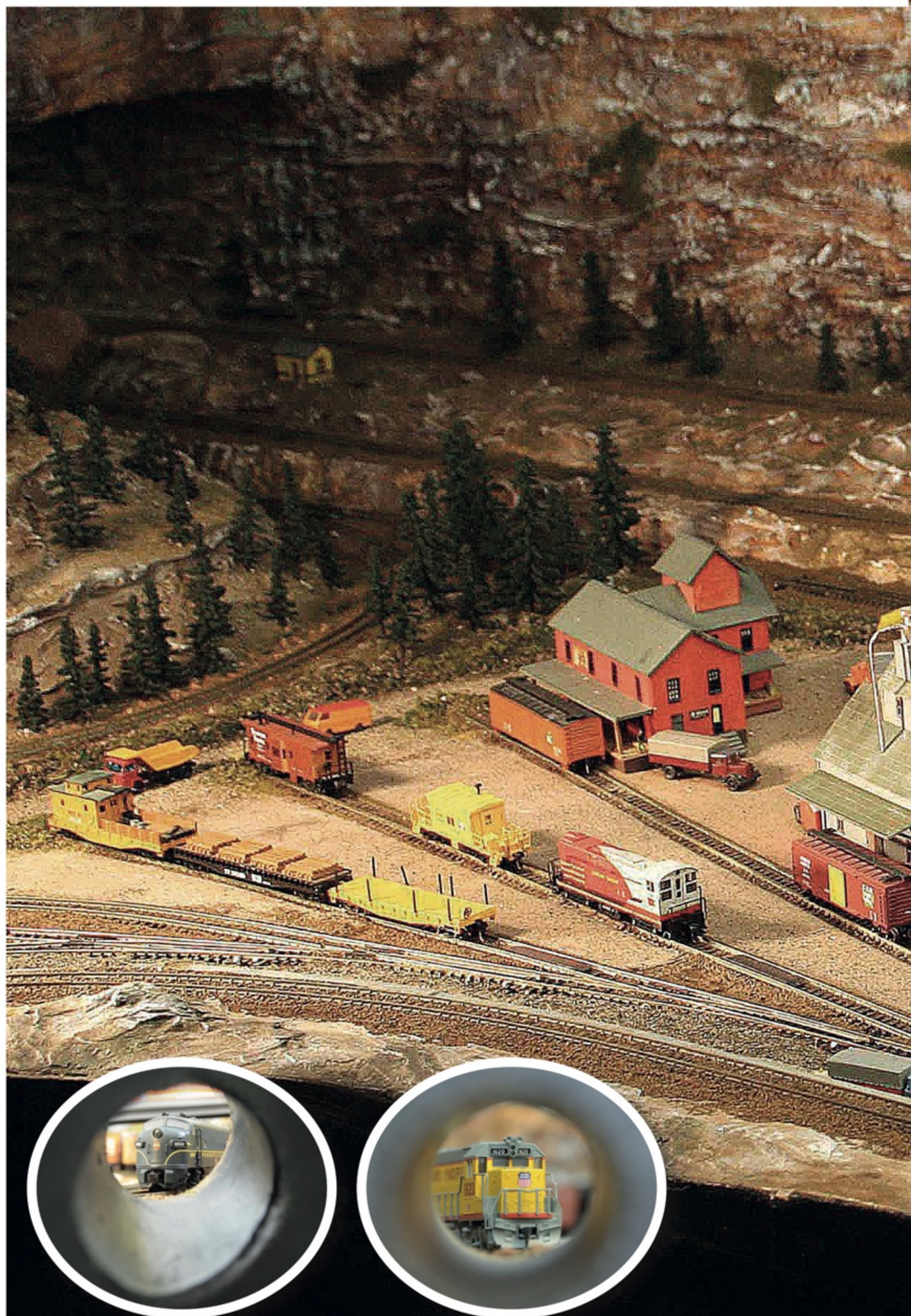
In a little under two years my indoor G scale switching layout was replaced by this N scale layout, using the original baseboards, offering long runs, switching opportunities – and no chance of banging my head on the underside of the layout!

Below right

Viewing holes at Dunns (Pennsylvania #9506, E7A) and en route to Exton (Union Pacific ex-MP #1620, GP15-1).
Photos: Dave Garforth.

Below

An assortment of steam locomotives on shed at Hindon.

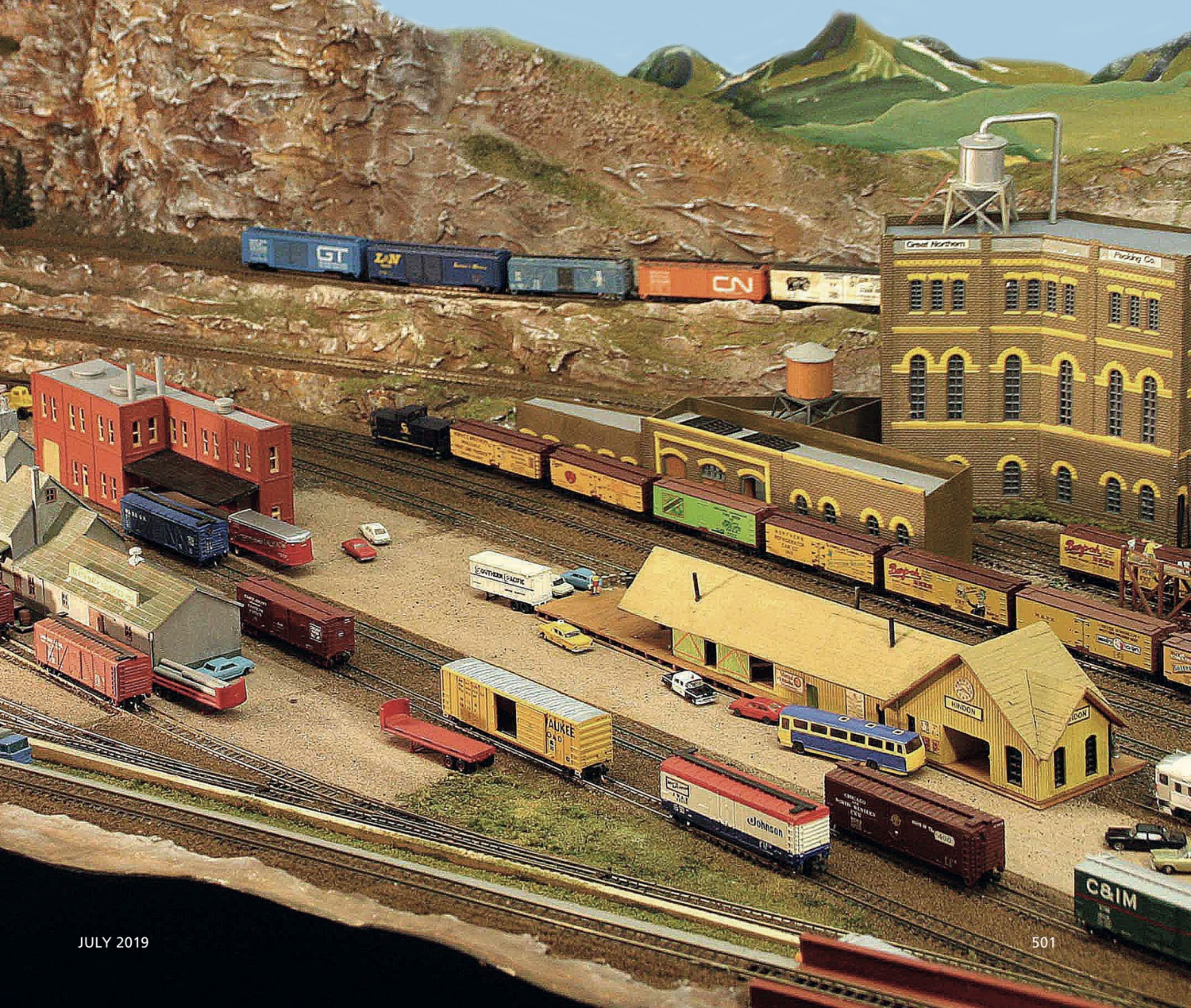


Left
Bridges carrying the track
dropping from Exton frame
the Hindon depot area.

Right
Hindon and the south end
of the layout, with tracks in
cuttings at different levels –
these are the visible storage
loops Dunn's and Exton.



Below
The north end of Hindon, with the freight house, industries, and depot.



Ted Polet describes a novel layout shown at OntraXS! in Utrecht in 2019 which was built in Indonesia by **Indra Setia** and **Irin Kusnadi**.

Tatar Sunda

Featuring local landmarks





Below

A CC206 diesel with just one coach crosses the Cibisoro viaduct while a CC201 skirts the town. They will meet at Cimahi station where a large Mallet and some vintage coaches are waiting – so some slick shunting will be required to let the trains pass!



Tatar Sunda literally means 'the land of Sunda'. Sunda is a region encompassing most of the western half of the Indonesian island of Java. The native language there is not the official language, Bahasa Indonesia, but a quite different regional language, as is the case with dozens of peoples within Indonesia.

A note on place names: where the Indonesian language uses 'C', this is pronounced as 'CH' in English, and 'J' is pronounced as 'DJ'. Present-day place names are given with their old Dutch colonial equivalent in brackets. The Dutch used 'OE' instead of present-day 'U'.

The city of Bandung is the capital of western Java. It lies in a temperate climate in the mountains and in Dutch colonial times was known as 'The Paris of Java'.

The theme of the layout was chosen because of the beautiful mountain railway crossing the central volcanic ridge from the north coast to Bandung. In the heyday of steam, this was the realm of Mallet locomotives.

Tatar Sunda is believed to be the first ever layout with a Sundanese theme, and took about eighteen months to build. 9mm gauge is used to represent 3'6" or 1,067mm gauge and the rolling stock is closer to 1:150 than 1:160.



Left
This view reveals how the track is folded back on itself to give a continuous run. The train is hidden in the tunnels at either end and in the low section behind the station.

Below left
The layout as displayed. The controls are built in – one simple DC controller, with two pairs of buttons to work the only two points (each end of the station loop).

Below
The 'vintage' train waits in Cimahi station as a CC201 crosses the spindly steel trestle Cibisoro viaduct.

The layout measures 210cm x 105cm and features an ingeniously folded continuous circuit with just one passing loop in Cimahi station.

In preparing the theme, Indra and Irin took great care in selecting focal elements of the prototype – bridges, viaducts, tunnels, a station building, and characteristic scenery, including the traditional dwellings still to be found in the mountains.

Cibisoro viaduct

In the 19th century, the railway journey from Jakarta (Batavia) to Bandung (Bandoeng) was a long-drawn-out affair through Bogor (Buitenzorg), Sukabumi, Cianjur, Padalarang, Cimahi, and eventually Bandung. It took almost an entire day from end to end.

In order to shorten the travel time, the colonial State Railway (Staatsspoorwegen, SS) sought an alternative route through the central mountain ridge to connect Bandung and the coastal town of Cikampek, which already had a railway connection to Batavia. The line from Cikampek up to Purwakarta is almost flat, but from there to Padalarang it is a heavily-graded mountain railway with tall and often very long lattice girder viaducts, and many tunnels.

The prototype Cibisoro viaduct is 310m long and was built in 1903. The model is almost two metres in length, and the lattice parts were laser-cut from plastic.

Motive power on this line, up to the 1970s, was almost exclusively the domain of Mallet locomotives – huge 2-6-6-0s and 2-8-8-0s reminiscent of American practice.

Sasaksaat tunnel

North of the Cibisoro viaduct is the 950m long Sasaksaat tunnel. This was cut through the mountains in 1902-1903, and it is still the longest railway tunnel in Indonesia. The ornate tunnel entrances were modelled just like the prototype.





Left

The ornate masonry tunnel portals were modelled to be just like the prototype at Sasaksaat.

Right

Arches from Cikuda viaduct form the approach to the station, with traditional houses on the nearby hillside.

Cimahi station

The small garrison town of Cimahi (Tjimahi) used to be home to the Dutch colonial army, and at present is the base of the Special Forces of the Indonesian army.

Cimahi station is a central feature of the town and was a First Class station of the former State Railway, built in 1884. This fine station has changed little since colonial days and is still a stop for the main Jakarta–Bandung express service, a named train, the *Argo Parahyangan*. This is the successor of the *Vlugge Vier* express in colonial days, the name of which referred to the quick four-hour trip between Batavia and Bandoeng, as they were called then. Another long-distance express, the service to Malang (Eastern Java) also calls here, providing a through service from Jakarta to the east. In addition there is a stopping service between Cimahi and other stations in West Java.

Cikuda viaduct

The prototype Cikuda viaduct is a concrete arch viaduct on the former branch line from Bandung through Rancaekek to Tanjungsari. This line was built to carry agricultural produce to Bandung, but regrettably was lifted by the Japanese occupiers during the Second World War. The Cikuda bridge is still a local landmark and is now a footpath for the locals to cross the valley near the village of Jatinangor, which is now the site of the campus of Pajajaran University, a well-known institution of West Java.

On the layout a few of the ornate concrete arches of Cikuda viaduct have been represented as an approach bridge to the model of Cimahi station.

Traditional Sundanese dwellings

Citalang is a village about 3km from the centre of Purwakarta town. In Citalang, one of the characteristic traditional dwellings (known as 'rumah adat' in Indonesian) of this area has been preserved as cultural heritage. It was built in 1905 and once owned by the Chief of Citalang, Raden Mas Soemadiredja. This house has been preserved in its original state. The layout features three such dwellings on the hillside to the right of Cimahi station.

Rolling stock

The mainstay of power on the line now is of General Electric origin. The most recent version is the CC206 class, a double-cab locomotive introduced in 2013. This evolved from the early CC200 class delivered as long ago as 1953. A second series, CC201, was delivered between 1977 and 1992 and has recently been upgraded. The third and fourth versions were the CC203 and CC204, both assembled in Indonesia by the PT INKA company of Madiun. The first CC203 was introduced in 1995 during the fiftieth anniversary of the republic. The CC206 was assembled from parts delivered by General Electric, with frames and bogies built by PT BARATA in Surabaya. Assembly took place in the Pengok workshops in Yogyakarta.



All of these classes use the same tried and tested eight-cylinder GE-7FDL-8 diesel engine, with only minor modifications.

A model CC206 runs on the layout, together with a train of luxury coaches used in tourist service on the West Java system. In addition, for the OnTraxs! 2019 exhibition, I converted an American diesel locomotive into a quick interpretation of the CC201 which ran with a three-coach suburban set made by Indra Setia. I also used an American Norfolk & Western Y6B Mallet and heavyweight passenger cars to commemorate the Mallet locomotives that used to work over this railway.

Acknowledgements and postscript

Indra Setia is an Indonesian modeller running a small company in Bandung called 'Garagetoy'. He builds dioramas commercially, and *Tatar Sunda* must rank as his masterpiece. For specific modelling work he teamed up with Irin Kusnadi, another Indonesian modeller in the Bandung area, who built the original rolling stock and the exquisite model of Cimahi station. Further support in Indonesia was by several modelers in the Indonesian 87 scale group, FKM, and the Bandung Rail Model Community.

Tatar Sunda was flown to The Netherlands in 2018 thanks to funding arranged by Mr Agus of PT Kereta Api Indonesia (Indonesian Railways). It has been exhibited a few times – at the National Narrow Gauge Museum in Katwijk (known to many as Valkenburg), during an Indonesian festival in Rijswijk, and finally at the OnTraxs! 2019 exhibition. Local

support in The Netherlands was provided by Indonesian railway enthusiasts messrs Widoyoko, Gerard de Graaf, and Ted Polet on behalf the National Narrow Gauge Museum.

In May 2019 the layout was formally presented to the venerable Nederlandse Vereniging van Belangstellenden in het Spoor- en Tramwegwezen (NVBS) in the presence of the Cultural Attaché of the Embassy of the Republic of Indonesia in The Netherlands.

Below
Cimahi station is still
a stopping place for the
Jakarta–Bandung express,
the *Argo Parahyangan*.

Photographs by the editor.



Jeff Ware describes the layout he has been developing for over two years, which is still work in progress.

Photographs by the author.

Wolfenfels



Background

Wolfenfels is an imaginary town somewhere in south-western Germany. It is not too far from the border with France and lies close to and immediately south of the Wolfenbach, a lesser-known tributary of one of the rivers that ultimately flow into the Rhine on its western side.

The town is dominated by the ruins of the Wolfenberg castle to its south-west, which formerly controlled passage across the river and through the pass immediately adjacent. A small walled town of *Fachwerk* houses developed to the east and north below the castle. The town inside the walls is mostly impassable to motor vehicles owing to the narrow lanes between the houses. The Baroness, the last of her line, now keeps and breeds horses in a large comfortable farm close to the Neustadt branch line north-east of the town, the family having abandoned the castle many years ago.

Above

The old town of Wolfenfels, with the ruined castle on the hill beyond. A streamlined 01¹⁰ brings a train out of the tunnel while a BR03 Pacific waits in the station with a rake of skirted coaches and a BR52 2-10-0 departs with a short goods.

In the early 19th century coal was discovered and was initially quarried in the cliff below the castle, but latterly a deep mine, the Königsgrube, and associated industry was developed and is still in operation. This resource attracted the railway in the late 1870s, and, as was common in those days, took little heed of the historic nature of the town and parts of the medieval fortifications were flattened to make way for the station and the junction of three railway lines that ultim-

ately connect into the network leading to the Ruhr. However, in a rare concession the station buildings were designed to match the medieval town.

The additional prosperity brought by the railway and coal mining resulted in the building of the Neustadt outside the walls with wide avenues and imposing buildings. Quarrying for stone was carried out beyond the Wolfenbach where an abandoned gravel plant lies. Also beyond the Wolfenbach there is a camping area.





There was destruction of a swathe of the new town during the Second World War with the gaps being filled later by typical 1960s blocks. Despite this the town retained its charm and at the period of the layout is rapidly developing as a tourist attraction.

Over time and particularly in the 1930s the layout of the station area was changed, with further changes taking place after 1945. This is evident in the odd arrangement of platforms and shadows where tracks were formerly laid, and a reduction from double to single track for the line passing through the area close to the coal mine. Proposals have been made to re-model and simplify the track layout in the station

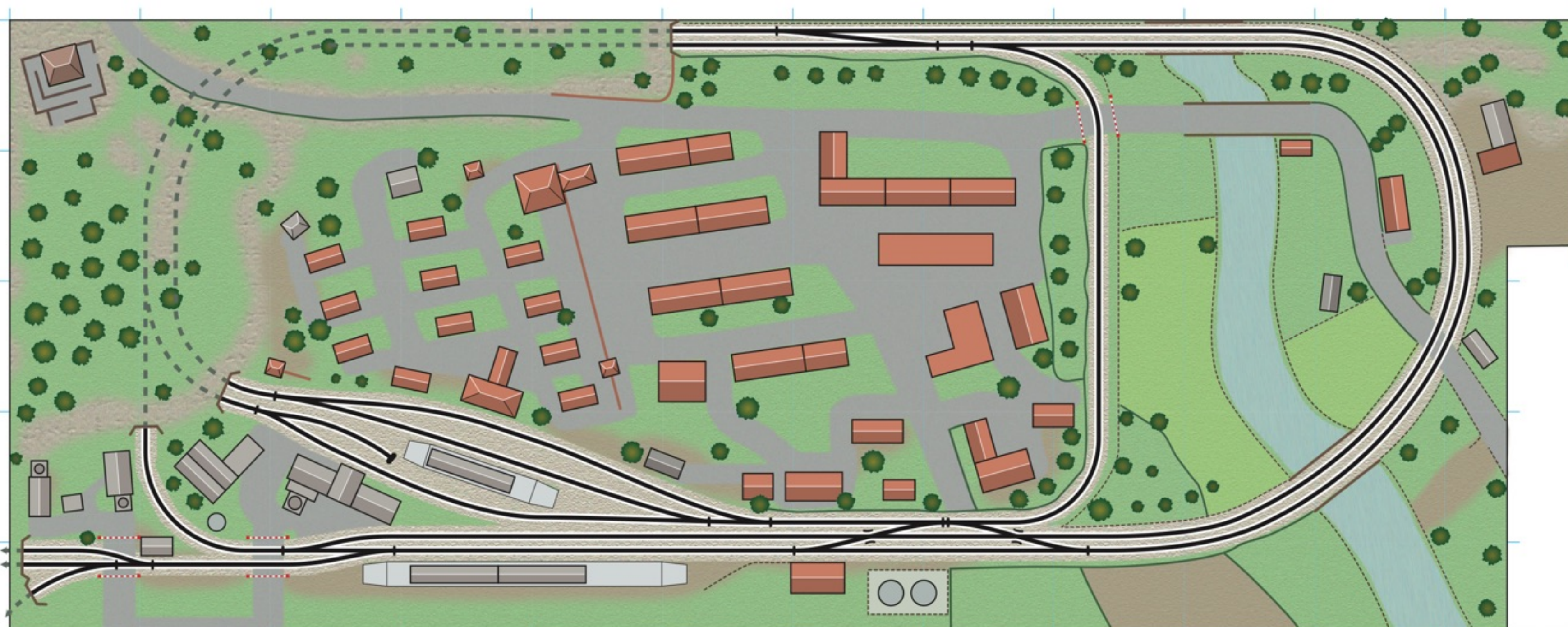
in order to reduce costs, but this is facing stiff opposition from the residents who argue that the increasing tourist traffic should be encouraged to use rail rather than road. For this reason the station area is becoming more dilapidated.

Construction

The 10' x 4' layout is in the loft of my house. It is constructed on a scaffold framework and the base was made up of five 4' x 2' boards bolted together. This is to facilitate any possible future relocation as the loft hatch dimensions are a limiting factor. Rubber between the boards and the metal stops the transmission of vibration or noise into the house.

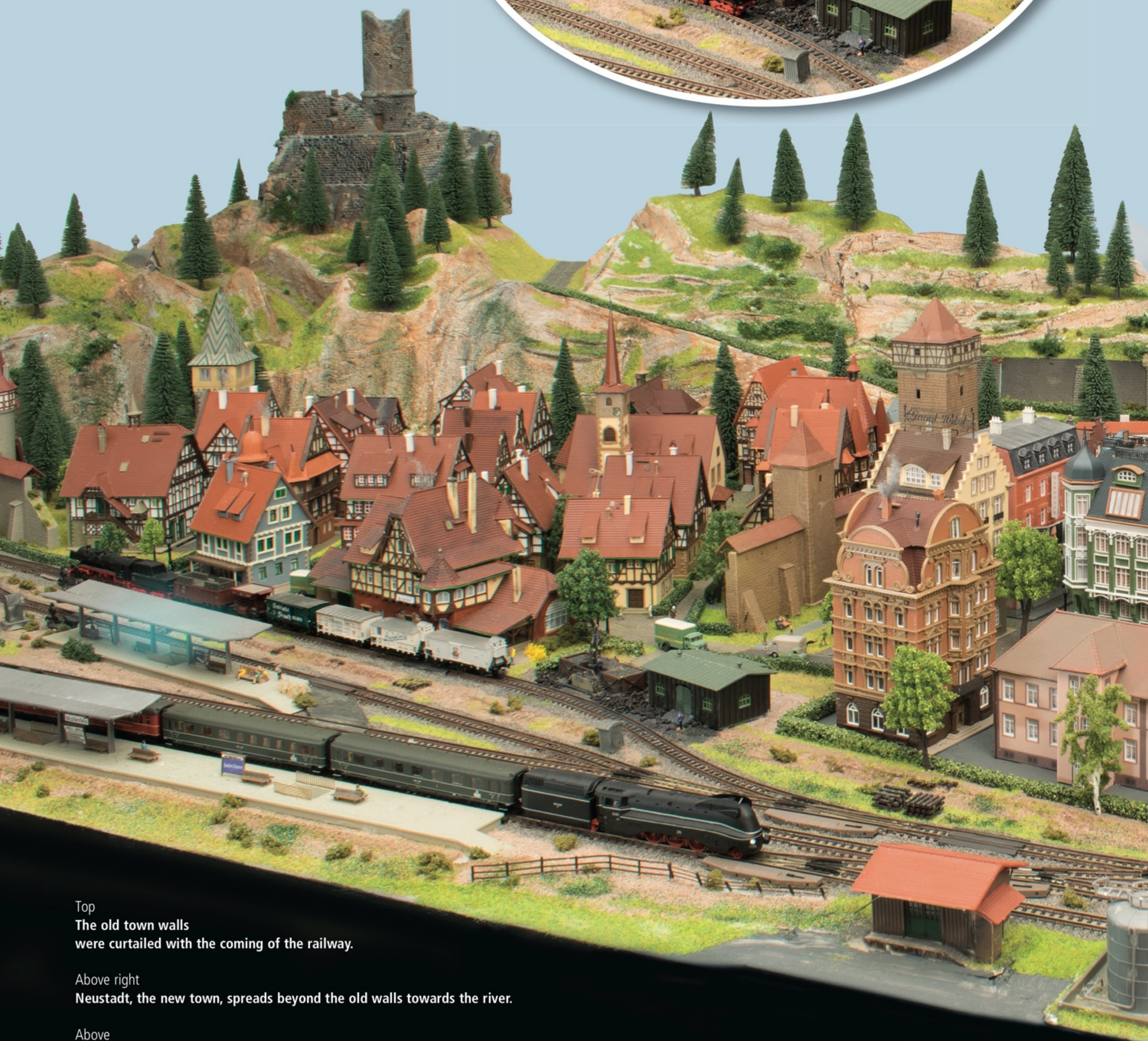
Above
A BR290 B-B diesel shunts the maintenance train into the industrial area.

Right
The ruined castle still dominates the old town. A BR52 with tub tender is at the coaling stage.



Wolfenfels Overall layout size approx. 3600mm x 1400mm. Each grid square = 300mm x 300mm.





Top
The old town walls
were curtailed with the coming of the railway.

Above right
Neustadt, the new town, spreads beyond the old walls towards the river.

Above
The old and new towns, with a BR52 waiting with a goods, a BR03 leaving the station, and an E10 electric arriving with an express.
The period setting is not strict! And catenary is on the 'to do' list.



Most of the track is Fleischmann Piccolo, with some Kato and Peco elements. All points and point motors are by Fleischmann, equipped with TrainTech accessory decoders.

Control is digital via a Fleischmann/Roco Z21 DCC system, which controls the points as well as the locomotives. An iPad or iPhone is used to access the free app used to run the layout wirelessly.

Buildings are a mixture of kits by Vollmer, Kibri, and Fallar. The castle is a Noch HO item that has been re-scaled by altering stair ways and other out of scale elements to make them visually acceptable.

Scenic elements are made from Modroc over a card or foam framework.

Most of the scenic scatters and trees are by Gaugemaster and Noch.

Locomotives and rolling stock are a mixture of Fleischmann and Minitrix, mostly Epoch III, but some are earlier and still operating at this period.



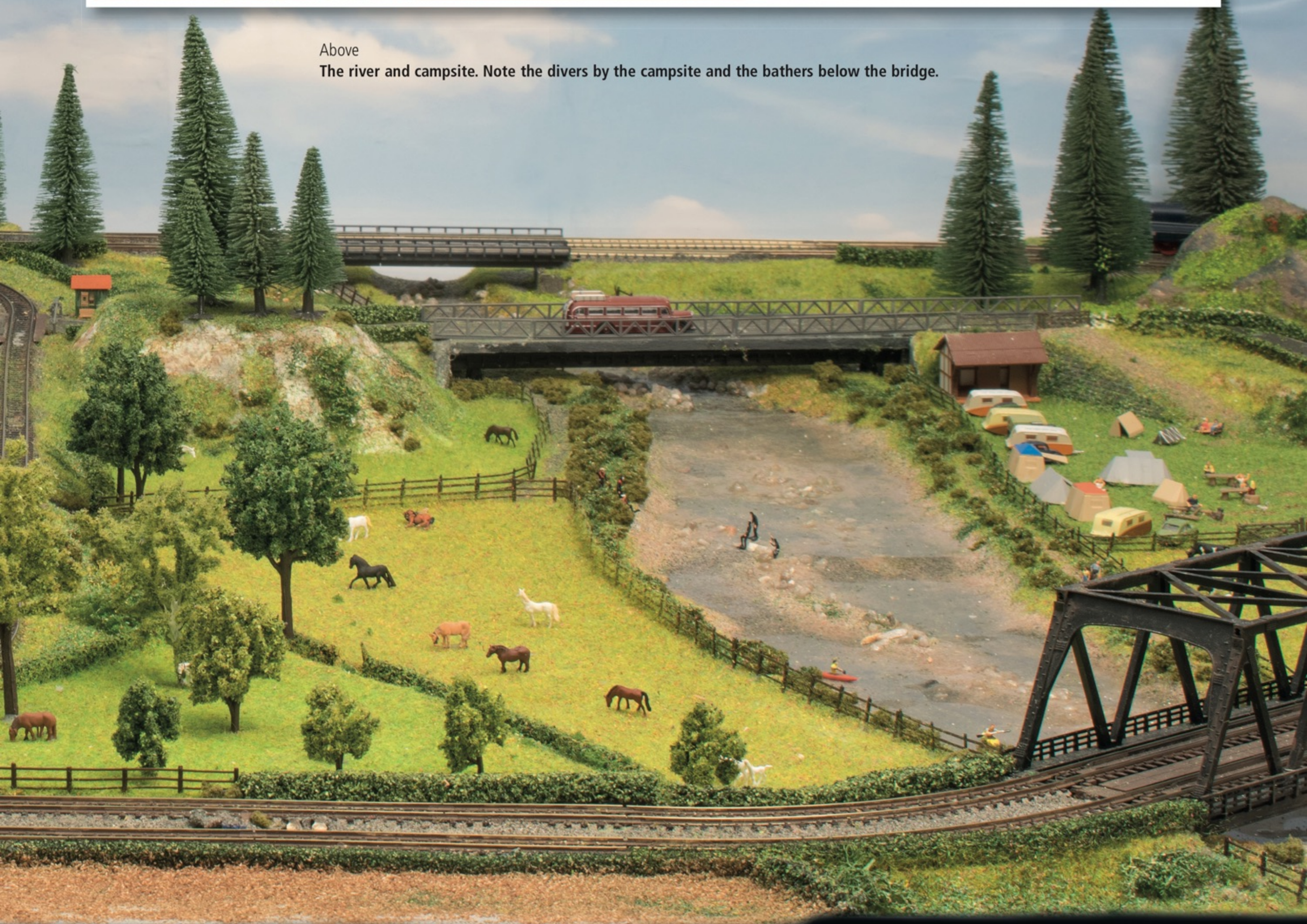
Above
BR03 and train passing the disused stone quarry.

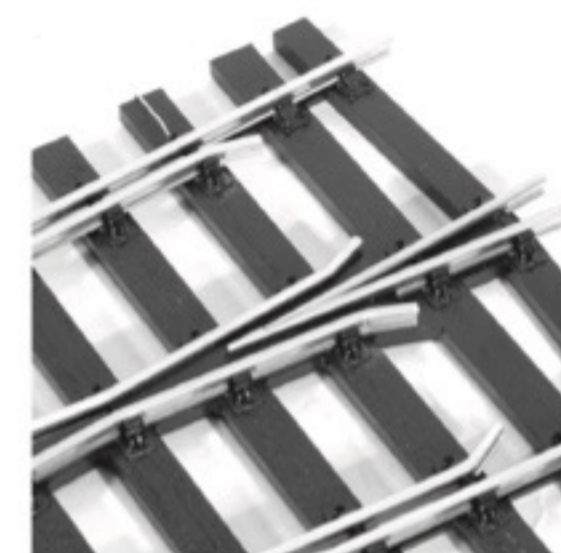
Below
Neustadt, with the baroness' farm in the foreground.





Above
The river and campsite. Note the divers by the campsite and the bathers below the bridge.





N

DBAG ICE 4 new from Kato



ICE 4 is the name for the new long-distance InterCity Express high-speed trains being supplied to Deutsche Bahn.

Around 2008 the process began to find replacements for locomotive-hauled InterCity and EuroCity services, and later this was expanded to include replacements for the ICE 1 and ICE 2 high-speed trains. In 2011 Siemens was awarded a contract for 130 seven-car inter-city train replacements and 90 ten-car ICE train replacements, plus further options.

The contract for the ten-car sets was modified in 2013 to expand the length to twelve vehicles.

Developed under the title ICx, the trains were branded ICE 4 at the unveiling of the first set in 2015 and given the class designation 412 by Deutsche Bahn.

The ICE 4 is based on the power car concept: a 1.65MW power car combines an entire traction system in one vehicle, including the transformer, traction con-

verter, traction cooling unit, and four traction motors. Configurations from five to fourteen cars are possible, with certain conditions, using combinations of powered and trailer cars. This means that the train can be adapted to specific transport tasks in terms of acceleration, speed, and passenger capacity. Even double traction is feasible, with two seven-car sets forming a 400m long train.

The two main variants initially envisaged were a seven-car 499-seat train with three powered cars and a top speed of 230km/h to replace loco-hauled trains, while the second was a ten-car 724-seat variant with five powered cars and a top speed of 250km/h to replace ICE 1 and ICE 2 trains – the replacements were expected to be 20 tonnes lighter, resulting in claimed energy savings of 30%. Each set was to feature a restaurant/bistro, cycle storage, provision for wheelchairs, and a family area.

Two pre-production sets had been built by 2016 and were used for testing prior to the planned introduction of the main series in late 2017.

In 2018, DB ordered eighteen extra seven-car trains, bringing the total to thirty-seven, along with fifty more intermediate power cars to extend the twelve-car trains to a new thirteen-car composition, 374m long and accommodating up to 918 passengers. DB wants to have these certified for 265km/h instead of 250km/h.

By late 2018, around twenty ICE 4s were being operated on the Hamburg – München and Hamburg – Stuttgart routes. From December 2018, the ICE 4 was to be introduced on the Berlin – Erfurt – München and Köln – Frankfurt/Main routes. The first services to Switzerland were scheduled to commence in December 2019. Registration is planned for Austria, and approval for

France, Luxembourg, Italy, Poland, and the Czech Republic is an option.

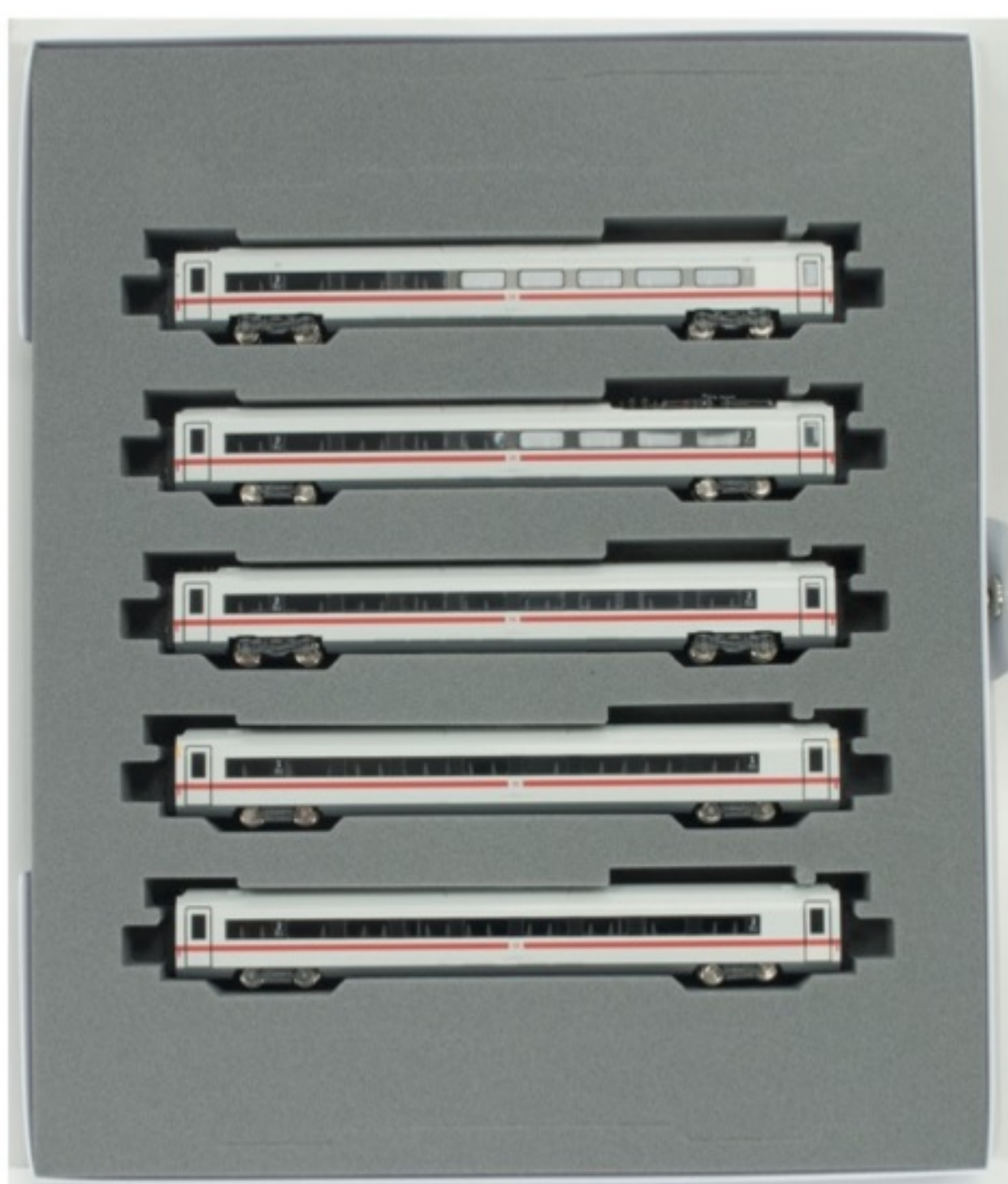
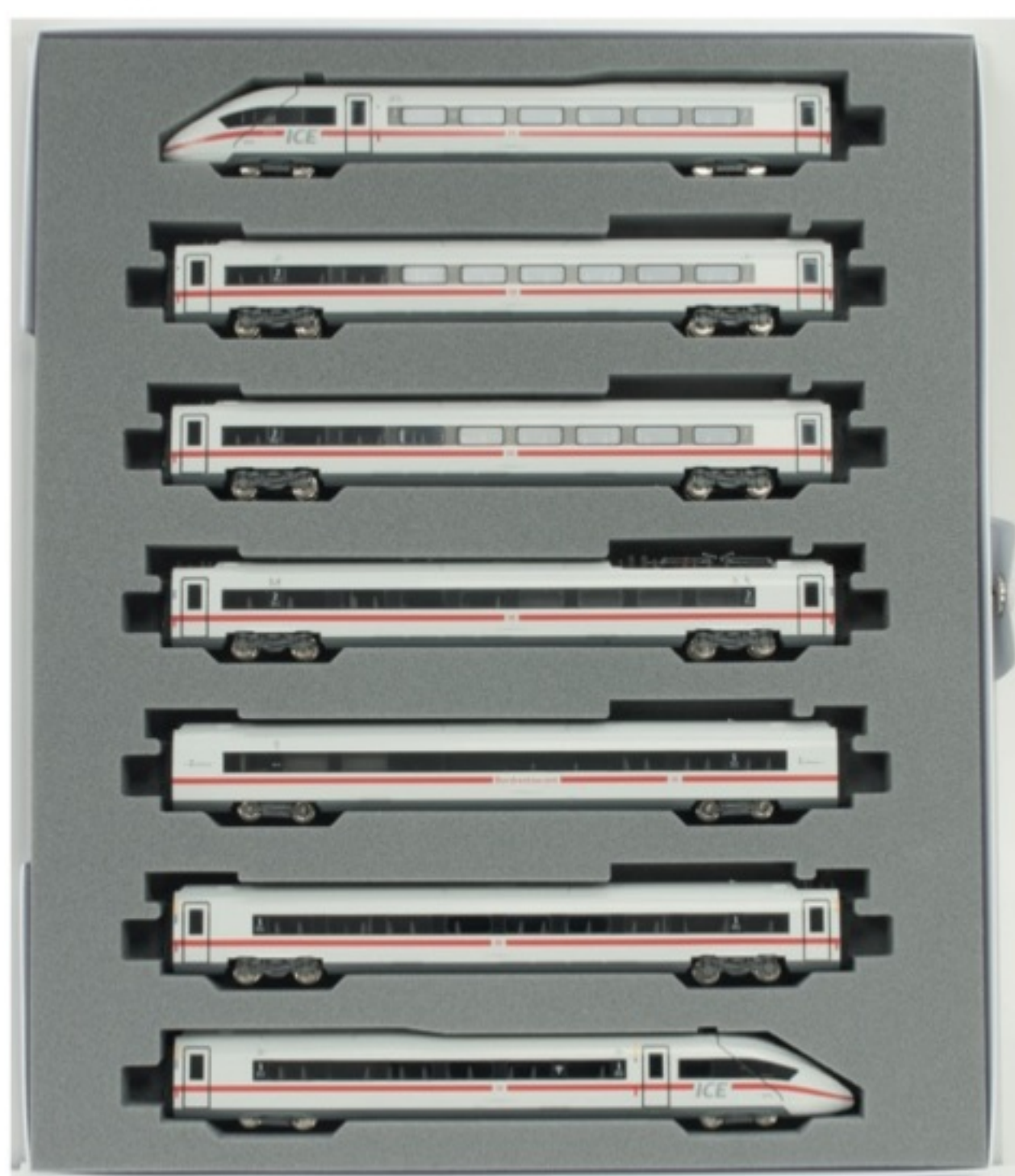
The ICE 4 orders have been changed several times. At September 2018 the situation was:

- 37 x 7-car sets, numbered from 9201, class 412.2;
- 50 x 12-car sets, numbered from 9001, class 412.0;
- 50 x 13-car sets, numbered from 9451.

The short set will be around 200m long and have three power cars. Maximum speed is 'only' 230km/h. It will have 456 second and 77 first class seats. The first units are due to enter service at the end of 2020 – their introduction has been pushed back considerably as a result of the modifications of the contract. Nevertheless, a first short set was being tested in February 2019 by DB Systemtechnik, though curiously only six cars.

The brand new model from Kato is an exact 1:160 scale replica, and comes as a seven-car base set (numbered 9010) with the option of a five-car extension pack; the instructions show how these can be combined in the overall context of a fourteen-car set – envisaged in the original specifications but (so far) not ordered. Both sets are supplied in a 'book' case with a slip cover, as often used for models of Japanese trains.

The seven-car set consists of what are noted as cars 1, 2, 5 (with motor), 9, 10, 11, and 14; the five-car extension provides cars 3, 4, 6 (with motor), 7, and 12. Note cars 8 and 13 are not modelled,





so the packs allow the modeller to represent the current seven and twelve car trains. Note also that the two powered models are together in the centre of the train.

Seven car pack (1,278mm long):

- end car, second class, with cycle stowage and wi-fi, 5812.0
- second class, quiet coach, 2412.8
- second class, 2412.3 (motorised model)
- second class, with pantographs, with family area and disabled facilities, 6412.0
- Bordrestaurant, second and first class, with central crew doors only and no end vestibules 8812.0
- first class, 1412.0
- end car, first class, quiet coach, 0812.0

Five car pack (910mm long):

- second class, quiet coach, 2412.5
- second class, with pantographs, 2812.0
- second class, 2412.0 (motorised model)
- second class, 2812.0
- first class, 1812.0

The '4' indicates a powered car, '8' a trailer. It will be noted that the model seven car pack has four powered cars whereas the prototype is only due to use three. The combined twelve cars have the correct balance (six powered).

No two vehicles are identical in function and hence markings even if the body shells are common. There are four body types: end; intermediate; intermediate with pantograph; restaurant.

The firsts and seconds use the same interior moulding.

The motorised unit differs only in the frame.

The cars ride on two different bogie designs, representing the powered and carrier types.

The finish is very good, with very neatly printed markings. Some of the body panels, doors, and grilles are printed, others are moulded in relief as separate parts, such as the air-conditioning units in the roof.

The body shell has flush glazing, and internal fittings despite the dark 'glass'.

The pantographs are made of fine plastic parts – they are not intended for current collection. They can be raised but are not sprung.

One end of each vehicle has a recess in the roof to accommodate the electrical equipment (added parts) – the actual links are not represented, for obvious practical reasons.

The corridor connections function as couplings; they are a bit fiddly to engage until you get the knack, but then are secure yet quite easy to separate (by lifting one vehicle).

Each pack includes one motorised car, powered by a five-pole motor with flywheel, driving both axles of both bogies. Current is collected from all wheels of the powered units. In the base set, there is a traction tyre on one inner wheel of each bogie, on opposite sides. The extension pack powered car has no traction tyres.

Performance is all you would expect of a Kato mechanism – quiet, smooth,



and controllable across the speed range from walking pace to something suitably fast!

The maximum current draw is quoted at 0.42A for the seven-car set and 0.3A for the five-car set.

The end cars have white and red directionally controlled head and tail lights, and all cars are prepared for LED interior lighting – the kit is available individually or as a pack of six. Axle-end pick-ups are already in place, as are the internal diffusers.

The model can also be converted to digital operation, with a choice of separate decoders for the motor(s), end lights, and interior lighting.

The sets are not through wired for either motor control or lighting.

As per prototype, sets can be joined: the nose caps are separate parts and can be removed, revealing a coupling pin for which a link bar (not supplied) would be required; the accessory part number is not specified, unlike most others.

The minimum radius is 282mm, and the maximum gradient 4%.



There is an excellent illustrated instruction leaflet, in German and English.

An impressive train which can only be fully appreciated in action on a layout.

Manufactured by

Kato (Sekisui Kinzoku Co.Ltd.),
Tokyo, Japan.
www.katomodels.com

DISTRIBUTED IN THE UK BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICES

7-car base set
ref.K10950/10-1512A £310.00.
5-car extension set
ref.K10951/10-1513A £215.00.





At the end of the Second World War, there was a shortage of motive power in France and to obtain the large number of locomotives needed quickly they were ordered from the main American and Canadian locomotive builders.

The first order – for seven hundred – was split between Baldwin, the American Locomotive Company, and Lima. The second batch (six hundred and forty) was ordered from those three plus the Montreal Locomotive Works and the Canadian Locomotive Company. Most of this batch were built as oil burners, using pre-heated heavy fuel oil (known in French as *mazout*), unlike the first batch which were coal fired.

141Rs were used all over the French railway network from 1945 to 1974.

Jouef have released several versions of their revised model of the 141R and the latest depicts oil burner 141R 1155 with a type 30R 13m³ high capacity tender, built (as witnessed by the legible maker's plate) by Alco as works number 74950 in 1946 and based at Nice. All markings are neatly applied, and the green livery looks very smart.

This first impression is soon reinforced by closer inspection: this is a well-made and finely detailed model, with many separate added parts on the injection moulded plastic boiler and under the running plate.

These include footsteps, buffers, brake pump with shroud and other front deck fittings, headlights, feedwater heater, chimney, top feed, safety valves, whistle, steam manifold, pipework and handrails, cab doors, fall plate, injectors and pipework, power reverser, generator, air reservoir, and mechanical lubricators.

The motion is made up of blackened metal components and plastic parts, and looks consistent.

Added parts on the tender include the handbrake wheel, oil pipes, handrails, tank fillers, TIA water treatment unit, and the access ladder.



Accessories provided for the modeller to fit include a brake hose for the rear buffer beam if a working model coupler is not required, delightful metal three-link screw couplings, fire extinguisher in housing, and an etched metal *Mistral* headboard. (1155 is recorded as having hauled this prestigious train.)

Jouef have made provision for representing different prototype variants, and this machine correctly has a hinged smokebox door, Kylchap exhaust, smoke deflectors with ribs, and a Delta rear truck. It also has the generator on the running plate, not on the top of the boiler.

It is fitted with Boxpok drivers throughout, and is coupled to a tender with a welded body.

If one were to be very critical, the lamps are slightly large, the handrail on the back of the tender is a bit long, and the incorrect front of the tender has not been modified – though, to be fair, this is not normally visible.

Loco and tender are permanently linked with a close-coupling bar.

Both loco and tender have metal frames, and the model is powered by a can motor with two flywheels in the boiler driving via a worm and gears hidden in the firebox onto the rear drivers, which are fitted with traction tyres. The other axles are driven by the coupling rods.

The wheels are to NEM profile with 0.8mm flanges.

The model runs quietly and smoothly from walking pace at c.2.5v to c.130km/h on full power (slightly above the 105km/h of the prototype).

Current is collected from the eight driving wheels and all tender wheels.

There are working directionally controlled headlights.

A 21-pin socket for a digital decoder is located in the tender, which also incorporates an enclosure for loudspeaker – the base is ready perforated.

The model is also offered with a digital sound decoder factory-fitted.

With a weight 380gr, pulling power should be ample for ten bogie coaches on the level.

The minimum recommended radius is 360mm.

The rear of the tender is equipped with a NEM pocket on a close-coupling mount, fitted with a standard coupler.

There is no similar provision for a working front coupler.

The illustrated instruction sheet is labelled in French and English.

In short, a very nice model.

Manufactured for

Jouef (Hornby Hobbies Ltd.),
Enterprise Road,
Westwood Industrial Estate,
Margate, Kent, CT9 4JX.
www.hornby.com

PRICE ref.HJ2276 £261.99.
ref.HJ2277 DCC sound £368.99.



New from Roco are optically revised scale length models of Swiss standard type II coaches, with particular attention paid to the doors and vestibules.

They have been produced with early Epoch IV markings. The range includes a first, a first/second composite, and two seconds (naturally with different running numbers). Epoch V versions have also been announced.

The finish on our sample is superb, with sharply printed lettering. Note such fine details as the silver handles on the gold doors.

The model has interior fittings, and is flush glazed, with moulded representations of the droplight handles.

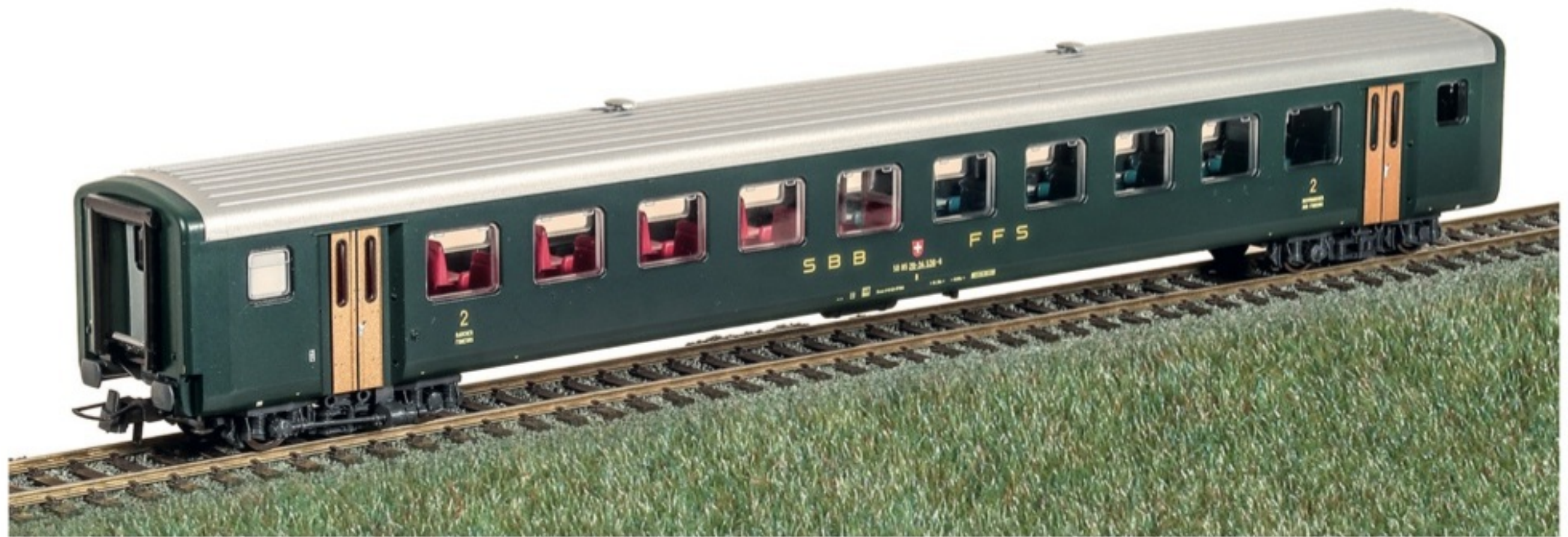
The toilet compartment window frames are neatly silvered.

Interior lighting is an optional extra; we presume the kit will have instructions how to open the body shell as this is not shown on the included leaflet.

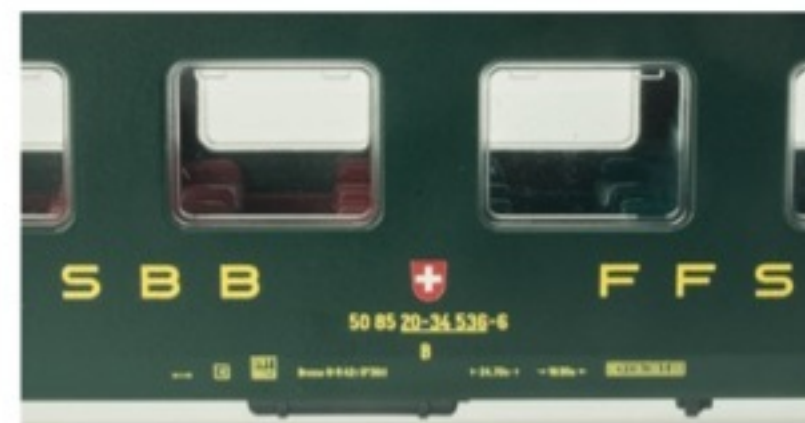
The body and underframe are augmented with a number of separately applied plastic parts – vents standing clear of the ribbed roof, corridor connections and fall plates, brake gear and battery boxes. Other underframe features are formed in place, in medium relief.

The bogies have finely detailed side frames moulded in deep relief, with brake shoes in line with the wheel treads. The electrical generator and drive shaft is added at one end.

Parts provided for the purchaser to fit include the door handrails (yellow plastic), electrical connections (black), and delightful perforated footsteps (which attach to the bogie side frames rather than the body so as not to restrict bogie swing).

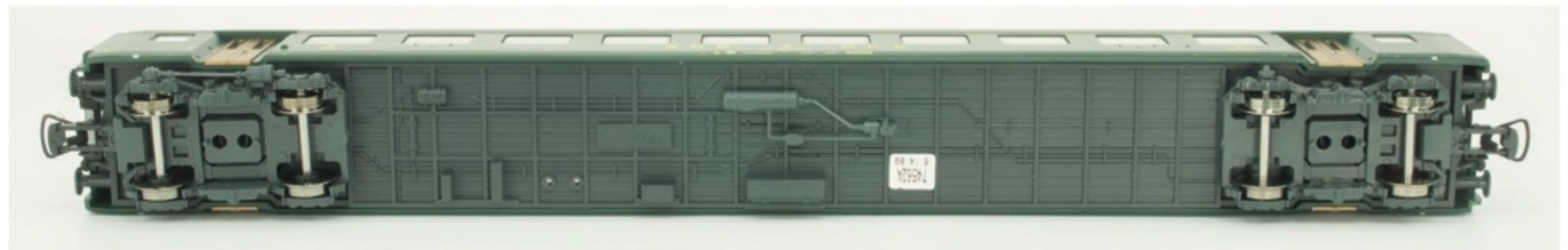


The models run freely on blackened metal wheels on pinpoint metal axles, insulated one side by a plastic centre bush.



As supplied they have standard couplers in NEM 362 pockets on close-coupling mounts 'steered' by the bogies, with Roco close couplers provided as alternatives.

Minimum recommended radius is not quoted.



Manufactured by

Roco,
A-5101 Bergheim, Austria.
www.roco.cc

DISTRIBUTED BY
Gaugemaster Controls,
Gaugemaster House, Ford Road,
Arundel, West Sussex, BN18 0BN.
www.gaugemaster.com

PRICE ref.74562 £53.00.

Harz bogie flat wagon new from Tillig

Brand new from Tillig is a type SSm bogie flat with side stakes from the Harz metre gauge network.

The DR Epoch III markings are clearly printed. The model represents number 99-04-73, with an overhaul date of 9/2/61. The wagon is 10.67m long over buffers, with bogie centres at 6.60m. Bogie wheelbase is 1.2m. It weighs 8.85t and is rated to carry 21t on a load space 9.1m long and 2.3m wide.

It is one of a series of bogie wagons built between 1900 and 1905 for the Nordhausen – Wernigerode Eisenbahn; specifically No.254, one of the final batch which may have been built as an open with brake platform but was soon converted to a flat with side stakes and end bulkheads. Under the DRG it became 10.12S4p, and got the current number in 1951.

As in the DR era freight was mostly carried over the metre gauge in standard gauge wagons on transporters, rendering the original wagons redundant, most were scrapped and only a handful survived to be preserved as museum stock; since 1998 this is one of them.

The moulded plastic body has good plank and strapping detail, with bolt



heads, on the ends, and a planked floor. The end balcony railing is formed in resilient material with good detail and handbrake pillar in place. The side stakes are fixed and cannot be removed.

Detail parts supplied for the modeller to fit include brake hoses, centre buffers, dummy scale couplings, and coupling hooks. These fit easily but firmly into holes in the buffer beam; a small amount of superglue is recommended to secure them.

Note that when fitted these parts may impede the working of the model coupler.

The underframe is nicely detailed, with main components and brake gear moulded in medium relief; the queen posts and truss rods are added resilient plastic parts.

The bogie sides have good axlebox and spring detail. The brake shoes are added, in line with the wheel treads, correctly only on the outer ends of the bogies.

The model rolls freely on wheels with plastic spoked centres and blackened metal tyres.

Regular narrow gauge couplers are fitted in sockets on the bogies; this

allows for alternatives, including a NEM pocket.

The model comes in very secure clear plastic 'bubble' packaging within a card tray box.

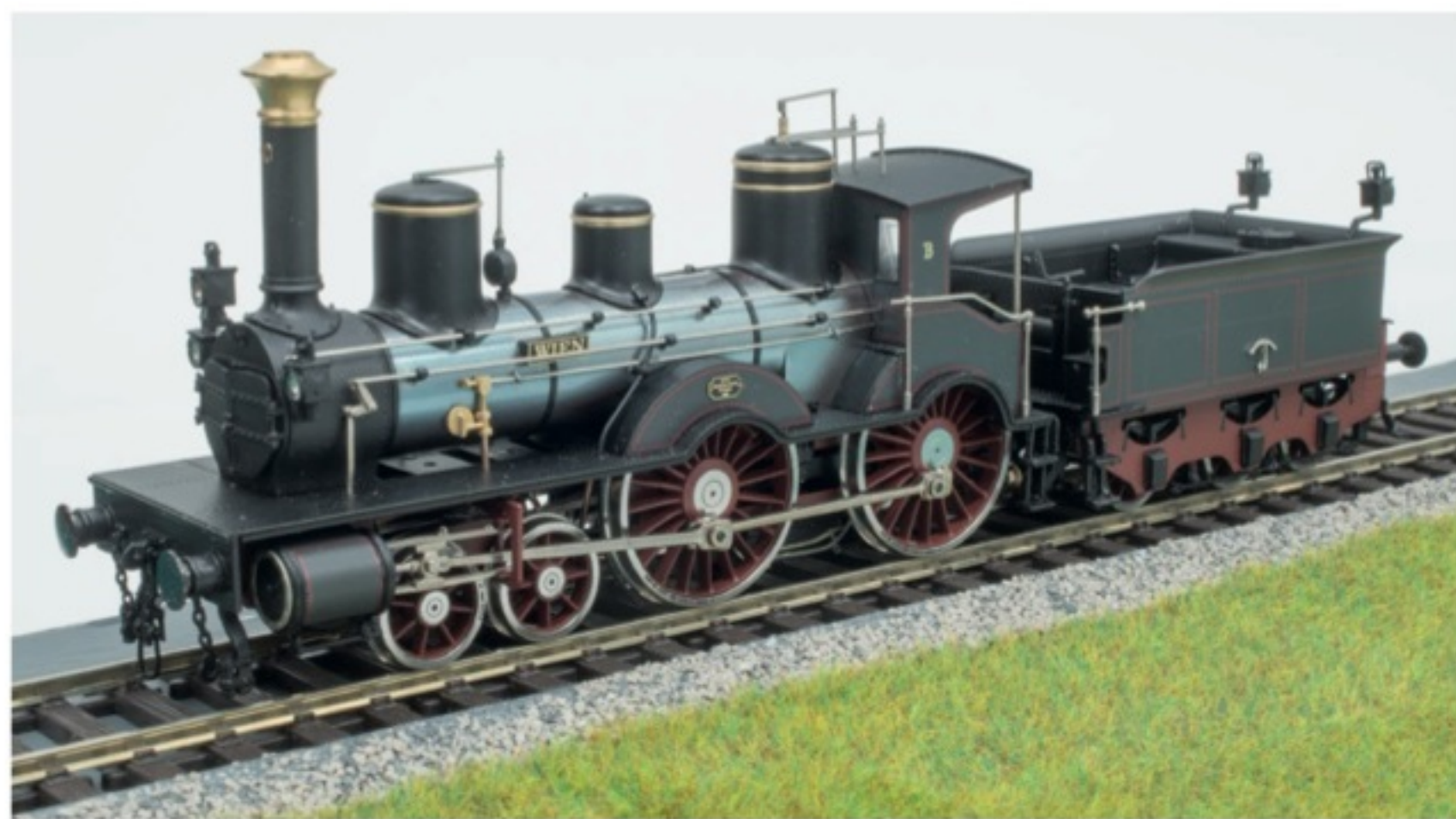
The model is also offered for HOe (9mm gauge); it is comparable in length to Sachsen stock though a little wider, as might be expected, but does not look completely out of place next to 750mm gauge vehicles.

Manufactured by

Tillig Modelleisenbahnen GmbH,
Promenade 1,
D-01855 Sebnitz, Germany.
www.tillig.com

DISTRUBUTED BY
Golden Valley Hobbies,
Unit 1, Pontrilas Business Park,
Pontrilas, Herefordshire, HR2 0AZ.
www.goldenvalleyhobbies.com

PRICE ref.15981 £39.91.



Eisenbahn Canada offer a steadily growing range of high quality hand-crafted model locomotives (and some rolling stock); the selection includes prototypes from all over the world – Europe, America, and Asia. The projects have been created by investing thousands of hours in research into historical sources all around the world.

We illustrate three European models which may be taken as typical of the variety and the quality of their products, and the range of the subjects selected.

The models are of all metal construction, and as can be seen are exquisitely finished. Performance is as good as the appearance thanks to the use of coreless motors and precision drive trains, cleverly concealed within the model so as not to compromise the accurate appearance. Current is collected from all wheels, and there are no traction tyres. There are working lights as long as the wiring does not spoil the appearance. They do not have decoder sockets.

Note that these are scale models and will require realistic curves – 1,600mm is recommended as a minimum.

A word about the packaging and presentation. The quality experience begins even before you see the model.

Within a heavy cloth drawstring bag there is a stout card box with a folding lid secured by concealed magnetic catches. Inside this is a firm foam liner with appropriate cut-outs, topped by an elegant velvet surface. This holds a thick card box with lift-off lid which has an inner foam insert. The model itself is wrapped in acid-free tissue paper and held by clear plastic film onto a wood or metal base strip with grooves for the 'rails'.

A pair of white cotton gloves is provided for handling the model.

Oddly for products of this high calibre there is no instruction sheet, and no technical information on the website.

Württemberg Class B 4-4-0

Built from 1865 by the Maschinenfabrik Esslingen, these locos had a Crampton boiler, inside Allan valve gear and, for the first time in Württemberg, a Prüssmann chimney. They were equipped with 2T6.5 tenders.



The model is powered by a Swiss Maxon motor, and uses gold plated parts to simulate the brasswork. Note the laser lettering on the wheel rims, and smokebox doors which can be opened to reveal authentic interior features.

The selection includes:

No.140 *Wien* in 'Russian Iron' livery with red-brown main frames and wheels (*illustrated*). 70 pieces worldwide. ref.HO-009/1. US\$1,650.00.

No.141 *Paris* in green livery with red-brown main frames and wheels. 25 pieces worldwide. ref.HO-009/3. US\$1,650.00.

No.142 *Strasbourg* in black livery with red-brown main frames and wheels. 30 pieces worldwide. ref.HO-009/2. US\$1,650.00.

No.143 *Coeln* in overall gold-plated finish. 10 pieces worldwide. ref.HO-009/5. Sold out.

No.143 *Coeln* 4-4-0 in black livery with red-brown main frames and wheels. ref.HO-009/6, 10 pieces, exclusive for the Lemke collection.

www.lemkecollection.de

Paris-Orléans – Midi streamlined Pacific
The tremendous success of the P-O Pacifics transformed by André Chapelon between 1929 and 1932 encouraged the company to go even further and in 1934 modify another ten locos, from among numbers 3523 to 3542, which became 231-722 to 231-731 under the new numbering when the P-O and Midi came together in that same year. The valve gear was modified (with reinforced recoil springs) to allow the machines to reach 200km/h. Following the experi-

ence gained with 240-701, the capacity of the intake valves was increased by 22% and that of the exhaust from the low pressure cylinders by 30%, and the volume of the low pressure cylinders was increased by 10%.

In 1937 231-726 was streamlined and presented at an exhibition in Paris. The streamlining had little effect on improving performance and was ultimately more significant for the image in publicity material.

The model has a fully detailed and painted cab interior, and functional lamps and marker lights. It is driven by a Swiss motor through an 'invisible' gearbox. Loco and tender are packed separately.

Several versions were offered:

– in 1937 condition, in express passenger red livery with silver bands (*illustrated*). 30 pieces worldwide. ref.HO-011/2. US\$2,695.00.

– in 1937 blue livery. Only 10 pieces worldwide. ref.HO-011/3. US\$2,750.00.

– in 1934 condition in grey with silver bands (the standard livery for express locos at the time). 50 pieces worldwide. ref.HO-011/1. US\$2,695.00.

– SNCF 1942 condition as 231H in black livery. 15 pieces worldwide. ref.HO-011/4 US\$2,750.00.

2D2 5500

This series of electric locomotives with four driven axles and a carrying bogie at each end was originally built for the Paris-Orléans to a Brown Boveri design with Büchli transmission (one six-pole motor on each axle). They were in service from 1933 to 1980.



There were four sub-series:
 Prototypes – 2D2 5501 and 5502 nicknamed “pig’s nose” (later “grand-mother”) (P-O E 501 and 502, became SNCF 2D2 501 and 502).

First series – 2D2 5503 to 5537 “pig’s nose” (P-O then P-O-Midi E 503 to 537, became SNCF 2D2 503 to 537) delivered between May 1933 and June 1934.



Second series – 2D2 5538 to 5545 “pregnant woman” (P-O-Midi E 538 to 545 became SNCF 2D2 538 to 545) delivered between December 1937 and July 1938.

Third series – 2D2 5546 to 5550 “Waterman” delivered to the SNCF between April 1942 and October 1943, with roller-bearing axleboxes from new.

The nicknames were coined due to the appearance of the fronts of the locomotives. “Waterman” refers to the shape of the bottle of ink for fountain pens.

Eisenbahn Canada opted to produce the *femme enceinte* (pregnant woman) sub-series in the following versions:

P-O – Midi 2D2 E 541 in original condition, in green livery (*illustrated*). 40 pieces worldwide. ref.HO-007/2.

P-O – Midi 2D2 E 543 in original condition in grey livery, a special edition, now sold out. ref.HO-007/4.

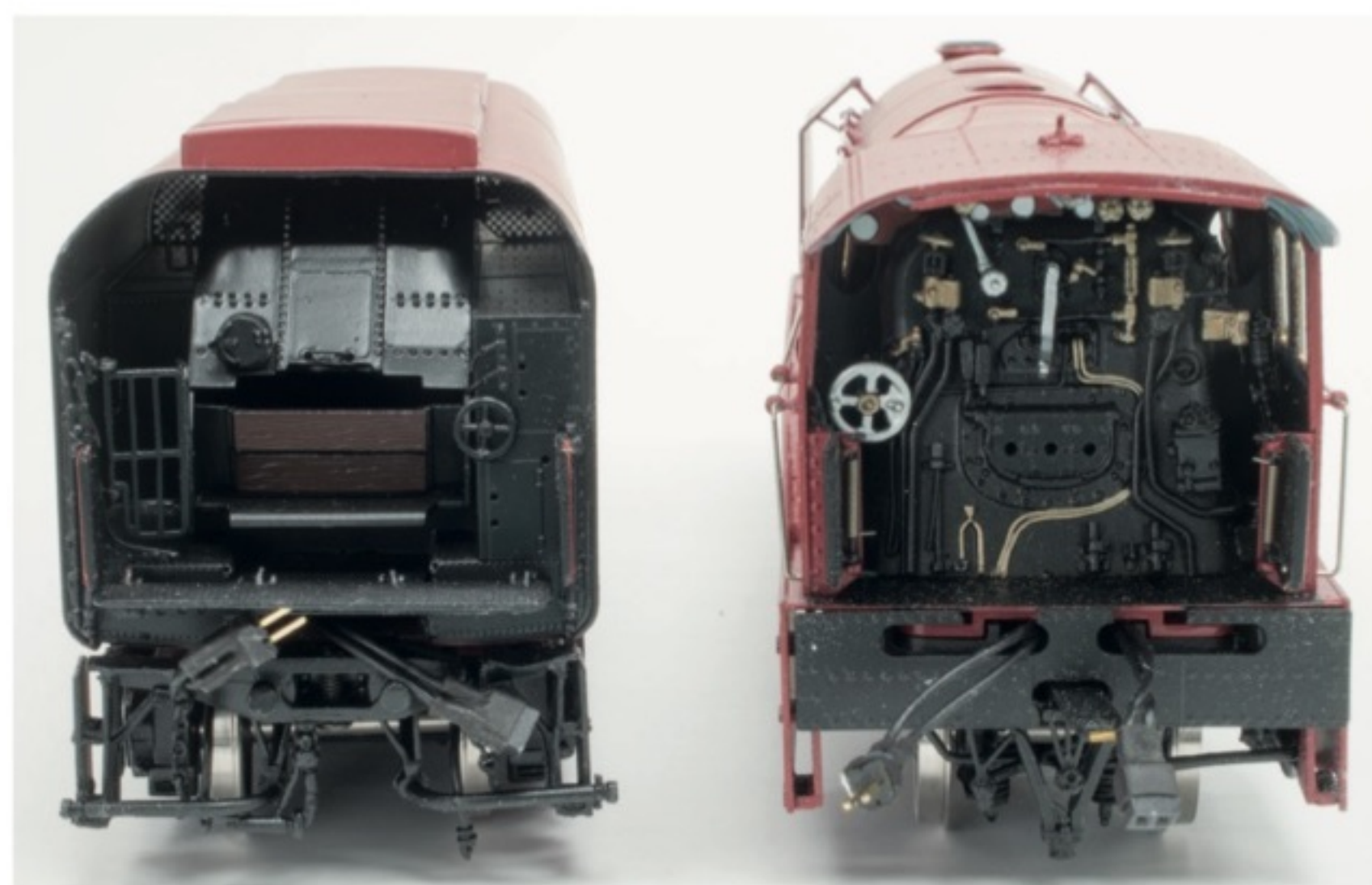
SNCF 2D2 5544 in green livery. 20 pieces worldwide. ref.HO-007/1.

SNCF 2D2 5545 in green livery. 20 pieces worldwide. ref.HO-007/3.

All versions are priced at US\$1,950.00.

The model features doors which can be opened, cabs and equipment room with interior details, and directionally controlled lighting. It is powered by a Maxon motor.

In short – miniature masterpieces.



Manufactured for

Eisenbahn Canada
 P.O.Box 88088,
 Lansdowne PO, Richmond,
 British Columbia V6X 2X0,
 Canada.
www.eisenbahncanada.com

PRICES in text.

Loco-Revue Hors Serie 66 24 plans de réseaux

Edited by Denis Fournier Le Ray

Éditions Loco Revue,
BP 30 104, F-56401 Auray Cedex, France.
www.locorevue.com

297mm x 210mm 84pp

Softback €15.00

ISSN 0024 5739 French text

This new special from the leading French model railway magazine *Loco-Revue*, dated 4/2019, offers twenty-four layout plans intended to help the beginner start small and simple, then develop the track plan and wiring, learning from experience.

It begins with outline reminders of some basic principles – types of layout, choice of scale and gauge, theme, baseboard construction, control system, and wiring. It then presents a series of projects making some use of commercial products, organised according to the level of difficulty in the carpentry, the track plan, the wiring, or the scenery, always recognising that the space available for our hobby is often limited. The smallest is 1m diameter, the largest is a 3.8m x 2.8m L shape.

The plans are grouped into three chapters: eight layouts to start, eleven to progress, and five to go further. Each occupying two or three pages, they cover a wide range of subjects. Each is built around a photo or two for inspiration or reference (prototype or model), an artist's impression, a full design, and a plan of the track components. Most are by Alexis Avril, with some from

François Fontana. Most, but by no means all, feature a French theme; most could be 'relocated' relatively easily. They are self-contained and not designed to be extended, though the first two of the final section are linked. Most are for HO, with two N and one O, but each could probably be adapted by changing the dimensions.

The presentation is in the usual clear style, straightforward (bar the affectation of titles in two fonts) and uncluttered, with large illustrations and relatively little text – so those with little French need not be discouraged!

There are tinted panels with summary information – scale, period, type, dimensions, situation, off-stage area, track used (brand and specific components), and often even suggested stock – plus pros and cons (on coloured circles). The accompanying text sets the scene and offers notes on the plan, the construction, and the scenery, plus suggestions for operation, in many cases the genesis of each idea. Reference is often made to other LR publications for further or more detailed information on specific topics or techniques.

Some idea of the time typically required is given in some cases, but of course this is very dependent on personal circumstances.

The majority of the photos are in colour, and all are well reproduced on good quality glossy paper. Each section is prefaced with a double-page spread.

Given that most of these plans are relatively small, the operational potential of some may seem rather limited but they are intended to be an encouraging start, achievable in terms of space, time, and resources.



They could be followed exactly by the novice to ensure success or simply used as the basis of something more elaborate, the source of an idea, by the more experienced modeller.

The book includes just two pages of advertisements.

Model Railroading – the ultimate guide Model Railroader Special Issue 2019

Edited by David Popp

Kalmbach Media

20127, Crossroads Circle, P.O.Box 1612,
Waukesha, Wisconsin 53187-1612, U.S.A.
www.ModelRailroader.com

273mm x 210mm 100pp

Softback US\$9.99

ISSN 1086-5586

Making a bold and essentially unsustainable claim in its subtitle (unless they literally do not plan any more, which we doubt!) this is in effect 'the book of the film' – or rather, several films. Every year *Model Railroader* creates over a hundred and fifty features for ModelRailroaderVideoPlus.com, their website accessible by subscription. It carries how-to videos that cover all areas of modelling, at all skill levels, offering expert advice, great tips, and practical techniques to help build a layout. As well construction material generated by the in-house staff with project layouts and articles contributed by independent modellers, visits to layouts and prototype locations are also shown.

This publication provides printed versions derived from a selection of those features. As a taster, the related videos can be watched for free using access codes within the article.

There are fifteen sections, with subjects including repainting and detailing the diesel loco and passenger cars for a commuter train, scratchbuilding a sawmill and all its equipment, modelling a lake, making a wetland with tall grass, creating a forest by enhancing commercial tree kits, making road surfaces with printed paper, enhancing a plain area on a layout, pre-shading with an airbrush as a weathering technique, weathering a cement hopper car, upgrading a diesel sound decoder, the Copper Basin Railway as a prototype worth modelling (with suggested layout plan), and railfanning at certain 'hotspots' in the mid-west.

Most of the modelling described is HO, but there is also something on a 7½" gauge garden line – not something most modellers will be able to emulate!

The features range in length from two to ten pages.

Most are in the form of step-by-step photo sequences with explanatory text appended.

Presentation is in the familiar MR format, with information/summary panels (tools and materials needed, helpful tips, etc.) which for clarity have a border or frame, and sometimes tinted background. Sometimes the sub-headings are on coloured backgrounds, and occasionally the wording is unnecessarily "humorous".

In some of the features, the first few words of the text of each step are in bold type: the extent is inconsistent and we found it irritating. Likewise the titles are in two colours, for no good reason. Use of such effects can be helpful if it aids clarity, but here they rarely seem to achieve this.

The cover looks rather 'busy', with lots of type (in different sizes and colours) over an interesting composite image, with a model 'dropped in' to an old black & white photo.

Diagrams and plans are computer-generated artwork, to a high standard, and usually with the top view of some figures included to give some sense of the scale. Most of the photos are in colour, and all are clear and well reproduced.

The introduction has a number of hand-drawn caricatures or cartoons of the authors with short 'job', 'fave', and 'fact' "bullet points". Readers may judge for themselves whether this is an improvement over the personality photos which have been a feature of American magazines for some time.

Notwithstanding our reservations about the style of presentation, there is some excellent modelling here and some very useful information.

Although aimed at the American market, much of the general modelling will have wider appeal.

There are four-and-a-half pages of advertisements, some for other Kalmbach publications, some from relevant suppliers (Bachmann and Peco).



MIBA-Spezial 120 Warten, pflegen, verbessern

Edited by Martin Knaden

VGB – Verlagsgruppe Bahn GmbH, MIBA-Verlag,
Am Fohlenhof 9a,

D-82256 Fürstenfeldbruck, Germany.

www.miba.de

295mm x 210mm 108pp

Softback €12.00

ISSN 0938-1775

German text ref.12012019

The latest special from MIBA is concerned with the care and maintenance of the layout and its motive power and rolling stock, with repairs when necessary. In some cases the work amounts to an upgrade or improvement.

A layout, including stock and accessories represents a significant value, not just in the ideal sense for the owner, but also in absolute terms. This must be maintained, so long term care is important.

The layout becomes increasingly unsightly when it gets old and a layer of dust has covered the landscape. Here fundamental cleaning is needed. There is a feature on dust removal and consideration of the replacement of broken and missing parts as well as restoring faded colours.

It is also important to deal with the motive power and rolling stock, which suffers from wear and tear.

Topics include consideration of suitable tools and equipment (most with applications beyond care and maintenance, naturally); reviewing a silicon worktop mat; analysing a new LED tester; testing a digital decoder tester; methods of removing dust from the layout without causing damage; refreshing an old layout with new scenery; track cleaners; regular care of locos; more involved overhauls; installing a new printed circuit board with decoder in an old Fleischmann N gauge diesel; digitally operated couplings for N; fitting the handrails and other often separately supplied accessories; rolling stock maintenance; testing and comparing the spare parts services of several European suppliers – an appendix provides addresses, telephone numbers, e-mails, and websites; and stock storage boxes with foam interiors.

The features range in length from one to ten pages. They are by several different MIBA staff members and regular contributors with expert credentials, including Karl Gebele, Sebastian Koch, Horst Meier, and Bernd Schneider.

The work is illustrated by over 300 photographs, all in colour and very well reproduced on good quality paper. Many are in step-by-step sequences showing the processes in detail.

Presentation is in the usual clear, straightforward MIBA style with no extravagant use of fonts, different type sizes, or colours.

A reasonable ability in German will be required to get the full benefit from the text, but the high pictorial content makes the book worthwhile.



This is a useful reference for old hands, returnees, and newcomers alike.

The issue includes around ten pages of advertisements, mostly for other MIBA and VGB publications, but with some from relevant suppliers and retailers.

Eisenbahn Journal Vorbild & Modell 1/2019 Bahn & Bier

Von der Brauerei zum Biergarten
beim Vorbild und im Modell

VGB Verlagsgruppe Bahn GmbH,

Am Fohlenhof 9a,

D-82256 Fürstenfeldbruck, Germany.

www.eisenbahn-journal.de

295mm x 210mm 98pp

Softback €15.00

ISBN 978-3-89610-708-4

German text ref.681804

Beer as rail freight? Why not! This new publication takes a trip back to the time when it was still commonplace to transport large and small containers full of beer by train. But not only the finished product reached consumers by rail – raw materials such as barley grain, hops, and yeast, plus barrels, empty bottles, and crates began their journey by train.

The first three chapters ('Bahn & Bier', 'Lass Dir raten: Fahre Sparen!', and 'Von der Brauerei zum Museum') look at the history of beer transport by train. There are particularly good illustrations of the many privately owned beer vans, a list of examples of beer wagons available in HO from manufacturers includ-

ing Brawa, Fleischmann, Liliput, Märklin, Piko, Roco, Sachsenmodelle, and Trix, and a suggested plan for a brewery on a layout.

The next six chapters ('Bierwagen-Umbau', 'Bunte Böhmen-Bierwagen', 'Veteranenpflege', 'Vier fette Fässer', 'Ladegut: Stangeneis', and 'Vorbild und Fantasie') deal with various aspects of modelling beer vans, showing how to detail and weather these vehicles and even recreating in model form the necessary addition of block ice for cooling!

The final five chapters ('Bierseligkeit', 'B(r)auerei nach Gusto', 'Die Brückenwalder Brauerei', 'Brauereianlage', and 'Idyll im Freien') cover the building of model breweries, the track layouts, associated buildings and structures, and how to model a beer garden.

A few pages are devoted to advertising MIBA and other VGB publications and DVDs. There is also a list of selected model railway shops in Germany and Switzerland. An appendix gives the names and addresses of specialist suppliers.

This is a handy, well-produced, and informative reference work which should appeal to anyone interested in recreating the transport of beer on a layout.

Whilst a good knowledge of the German language will be required to get the best from it, over 250 black & white and colour illustrations, many of which show step-by-step modelling processes, clearly show what can be achieved.



Le Train – spécial 97 1/2019 Les Grand Trains Tome 3

by Jean-Marc Dupuy

Editions Publitrain eurl,

CS80010, F-67660 Betschdorf, France.

www.letrain.com

297mm x 210mm 100pp

Softback €16.50

ISSN 1267-5008 French text

This new special from *Le Train* is the third to be concerned with prestige trains, and deals with the Orient Express, the Simplon Orient Express, the Arlberg Orient Express, and the current Venice Simplon Orient Express (VSOE), explaining the distinctions between them, often a source of confusion.

The story begins with trains of six-wheel stock in the 1880s, and relates how the Orient Express became symbolic of the Belle Époque. Subsequent sections deal

with the inter-war years, the period after the Second World War, and the slow decline of such services in the face of more cars, better roads, and the growth of air travel, to the current situation with a luxury hotel on wheels as an experience in itself as much as a stylish, elegant, and comfortable means of travel for those who can afford it.

Motive power depicted is mainly steam and then (in some places from quite early) electric, with only the occasional diesel. An appendix gives some typical train compositions by period, and some coach types are separately illustrated but there are no technical drawings or interior diagrams, and (perhaps understandably) no comprehensive lists of the stock used with principal dimensions.

The publication is built around an excellent collection of 172 photographs, the majority historic black & white images, reproduced as well as the originals will allow. Most are presented large, and there are several very impressive full pages and double page spreads featuring trains in the landscape, in a wide range of scenic

settings across France, Germany, Switzerland, Austria, Italy, Hungary, Greece, and Turkey, with appropriate motive power.

The older images have been drawn from archives and official sources, with the contemporary views supplied by many different photographers. The earliest colour is from July 1958, the most recent picture from August 2018.

As usual, there are informative captions, numbered, and mostly adjacent to the relevant image, or sometimes over a blank area, of sky, for example.

The photos are augmented with facsimiles of old documents, promotional posters, and artwork. There are schematic maps showing the routes, and the variations, at different periods.

There are seven pages of advertisements for other *Le Train* publications.

Some knowledge of French will help to get the full benefit of the text, but the captions should be clear enough and the publication can be appreciated for the illustrations.

Corpet, Louvet et Cie

by **Stefan Hooß**

Édition Shéhérazade Nayla Hooß

Available from

Camden Miniature Steam Services, Barrow Farm,
Rode, Frome, Somerset, BA11 6PS.

www.camdenmin.co.uk

297mm x 210mm 512pp

Hardback £82.95

No ISBN. French text

This substantial new book aims to fill a lack of information about one of the most familiar yet not well documented names in French railway manufacturing.

Founded in Paris in 1853, Corpet, Louvet et Cie produced steam locomotives until 1951. The bulk of these were 0-6-0 tanks of various gauges for industry, and in metre gauge for many French minor railways. Locomotives for major railways were relatively rare, and usually designed elsewhere; one of their last products was arguably the apogee of French steam – de Caso's wonderful 232 U1 'La Divine'.

The author is at pains to point out this is not a complete account but more a convenient collection of available information and images; he accepts it is not perfect but hopes it will form a good basis for future study and discussion. He is too modest: it is a remarkable work of research and compilation, and should be much appreciated by historians and enthusiasts.



It begins with a summary of the Corpet family history, by a descendant. To give the context, there is a chronology of known facts; a section on the evidence of production in the contemporary press, mostly facsimiles of exhibition reports and technical documents; an illustrated list of selected steam locomotives in chronological order, if not complete then suitably representative, and the core of the work; a summary of production; illustrations of an experimental railcar and two different diesel locos; an account of the construction of SNCF 232 U1, a huge 4-6-4 quite different from the light railway machines; locos in use in various départements, 29 in all, mostly in old postcards, with map extracts showing the lines on which they worked; the Vaujours gypsum works; a reproduction of the 1900 catalogue, with diagrams; thirty promotional cards, each with a works photo and principal dimensions; contemporary accounts of the Brown valve gear, reproduced in facsimile (thus some are in English); and a final section featuring a few odd but interesting photos which simply did not fit anywhere else!

From this it can be seen that the main part of the work is essentially a photo album; a high proportion are from the company records, but they do not just show locos posed at the factory – often they are in use, and not just in France.

There are some views of the factory at various periods, with one section showing activity in various departments in c.1945.

Naturally most are in black & white, but there are some coloured scale drawings (eight) and some colour photos (twelve), principally but not exclusively of preserved examples – one dates from 1956, and is also used on the front cover. All are reproduced large, and as well as the originals will allow. The scale drawings show end as well as side elevations.

Left

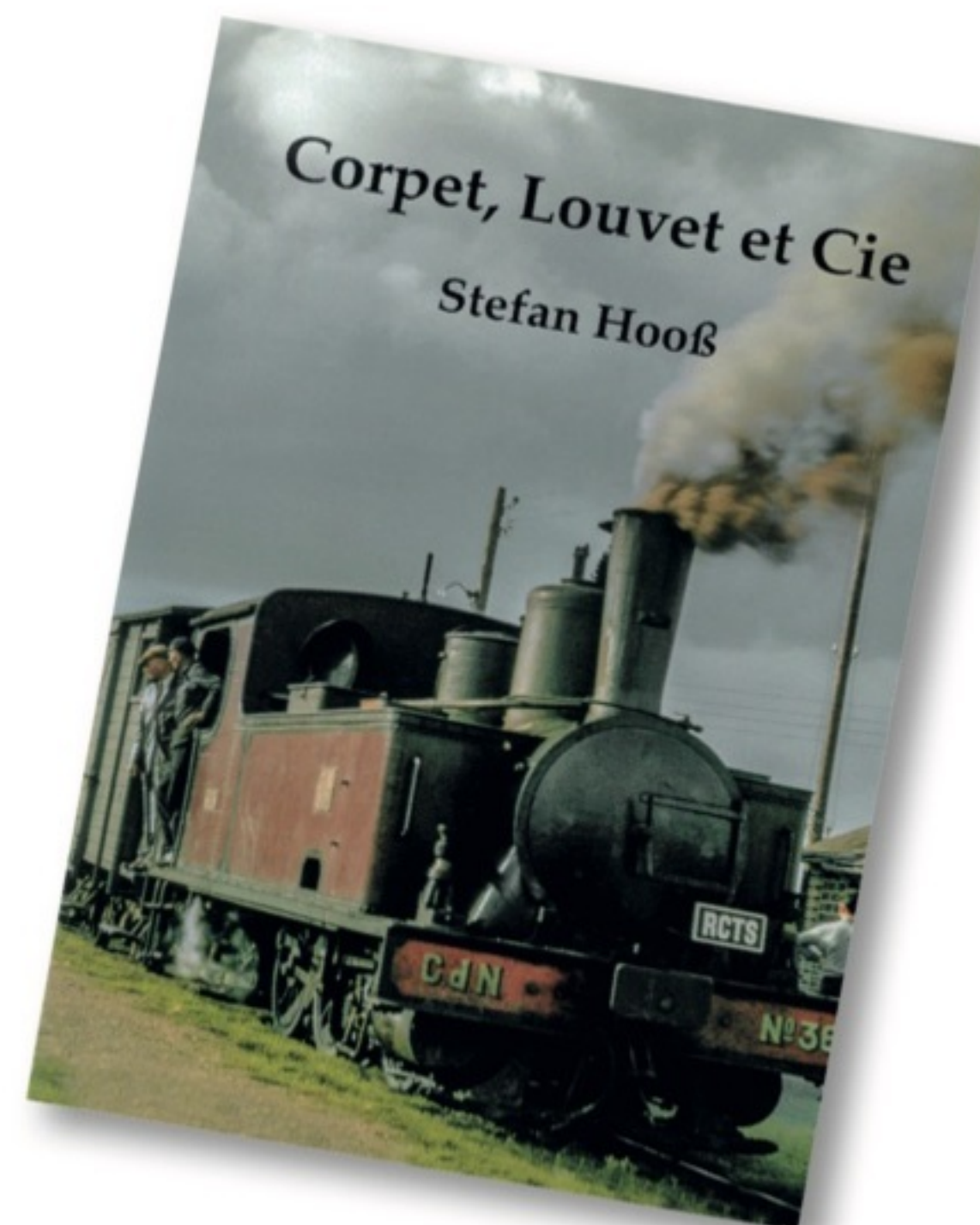
A Corpet 0-6-0T by LGB on Hans Louvet's *La Trinité* layout.

The model was weathered by Len de Vries.

The coal bunker contains real coal. The screw couplings either side of the centre buffer are Märklin Gauge 1.

See CM October 2003.

Photo: Steve Flint.



Around fifteen photos appear twice, deliberately, in both the loco and location sections, where no alternatives were available.

There is no absolute works list, but nevertheless this book is a remarkably thorough record of the company's production. It took much more work than the author expected, full of discoveries and surprises, and shows that Corpet was capable of much more than the typical light railway 0-6-0T which may spring to mind at the mention of the name.

An absolutely superb book, especially for anyone with any interest in French minor railways and their motive power. Heartily recommended.

The book has also been produced in a German language version.

Swiss Postbuses

Written and compiled by **Martin Fisher**

Swiss Railways Society,

Sales, Glyn Jones, Alpigen,

13, Welland Close, Dalebrook,

Burton-upon-Trent, Staffordshire, DE15 0AG.

www.swissrailsoc.org.uk

297mm x 210mm 206pp

Hardback £28.00 + £3.50 UK postage.

ISBN 978-0-9926812-9-6

This new book from the Swiss Railways Society is in fact a considerably expanded second edition, with additional information, updates, corrections, and more photographs. It is unusual in that it is a substantial hardback – and it is not about railways! Well, not directly, but in Switzerland the public transport network is well integrated and where there are no railways, connecting buses provide a service. Where those bus routes are not operated by urban authorities or commercial concerns, the Post bus will be found. (In fact, as the book reveals, many Post buses are worked by commercial operators under contract.)

Modellers accustomed to placing a Post bus in the station forecourt as an authentic accessory may be surprised to discover the variety of vehicles used. All are described here, which should help choosing the right bus for the period and the location.

Almost all the vehicles used are in the familiar yellow & white livery.

The introduction provides some historical background, then there is a survey by region – Bern, central,

eastern, Graubünden, northern, Ticino, Valais, western, and Zürich, plus the activities spreading into France and Liechtenstein. There are sections on night buses and the Publicar experimental dial-a-ride service, a detailed review of the vehicles used in the past, and a full listing of the modern fleet.

Extensive appendices provide: an account of the trials of early buses; a list of current bus operators; a list of buses supplied to the PTT up to 1960; a list of vehicles supplied from 1960 to the end of the P system in 2003; re-numbering of the fleet in 2003; a sample list of current numbering; and sample tickets.

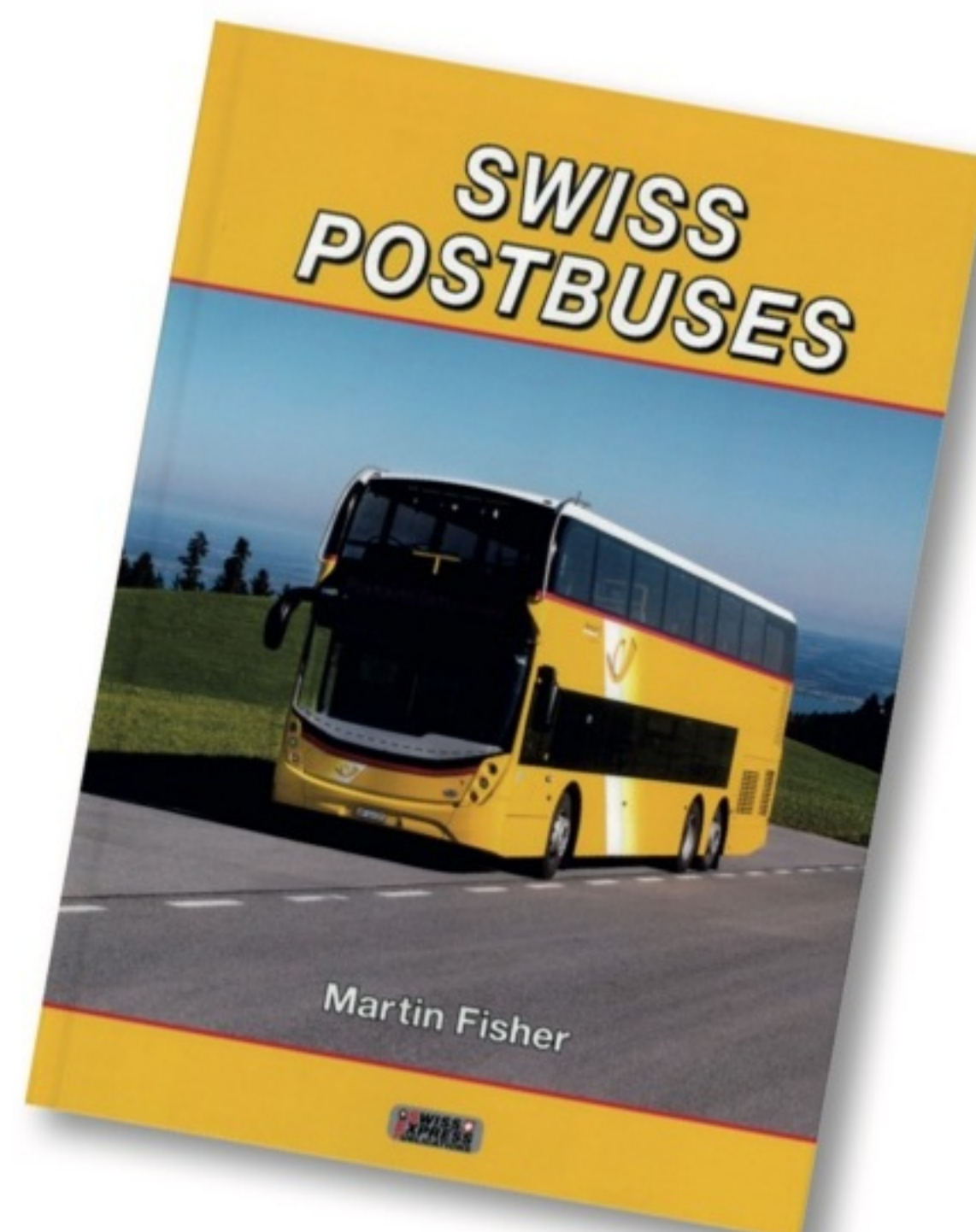
The work is rounded off with a bibliography and index.

This may sound like heavy going with a lot of data, but the presentation is clear and simple, with tinted backgrounds to the occasional information panels and the extensive tables.

The account is in fact built around a comprehensive collection of photographs, from historic black & white through to contemporary colour, all well reproduced. Some are from the author along with David Stevenson and other Society members, some (naturally) from archives and official sources, and some from the internet. Every photo is fully captioned and (where relevant) credited.

There are schematic maps clearly showing the routes region by region.

Some of the roads used are quite challenging – as certain illustrations demonstrate. (We will be cautious about criticising 'unlikely' gradients and corners on layouts in future ...)



The book concludes with a page advertising other Society publications, while the back cover promotes the Swiss Travel Centre – with relevant information and illustrations.

For overseas postage rates, and details of credit card payment, contact – sales@SwissRailSoc.org.uk



TRAXX-Lokomotiven

EK Videothek DVD c.58mins ref.8456 €19.80

The Bombardier TRAXX locomotive family can now be seen in Germany and in many other European countries. Dating back to the prototype electric 12X, the first classes to enter service were the 145, 146, and 185, followed later by the 146.5, 186, 147, 147.5, 187, and now also the 188. Also belonging to this family are the diesel-electrics of classes 246, 285, and the four-engined 245.

This programme takes a detailed look at this locomotive family with scenes filmed at Bombardier's factory in Kassel, on the test circuit at Velim in the Czech Republic, and in the cabs of many locos, plus a wide range of scenes filmed from the lineside on both passenger and freight workings with Deutsche Bahn AG, with private operators in Germany, and in other European countries.

Coverage begins with scenes of a variety of different TRAXX locos, electric and diesel, on both freight and passenger services filmed from the lineside. We see the prototype 12X (128 001) and film from 1997 shows the very first 145. Further film of this class in service follows with scenes showing them at the head of freight trains, observed from the lineside and at depots. A 145 is also seen on a train of DB Regio double-deck coaching stock, with another on a similar working at Königstein in the Elbe Valley.

Private operator examples include a Rail4Chem green liveried 145 at the head of a train of bogie tank wagons, and 185 001 is seen on display at the Inno Trans exhibition. Further examples of this class are then seen from the lineside on a variety of freight services. At Romanshorn a member of this class passes through the station on a train of containers and SBB Cargo 482 001 is also observed at Großkorbetha, south of Halle (Saale) on a train of bogie tank wagons. BLS Cargo examples are also shown.

At Essen Hbf. 146 001 is seen on a train of DB Regio double-deck coaching stock and further scenes show this train from the lineside. 146 020 is also seen on a similar working in the Elbe Valley. Metronom locomotives are seen working services formed of double-deck coaching stock in their distinctive yellow and blue livery. 146 122 is seen at Recklinghausen on a DB Regio service to Münster Hbf. formed of double-deck stock, and further film shows similar services from the lineside and at Geislingen (Steige) station. At Villingen (Schwarzwald) station a train to Konstanz formed of DB Regio double-deck stock leaves behind 146 231.

At the Bombardier factory in Kassel we see TRAXX locomotives under construction, including classes 146 and 185.

Class 185s are seen on freight services filmed from the lineside and an OHE-owned example is also shown. Other private operator units are shown on freight, including those in service with Captrain, MRCE, HSL, and Railpool. At the Inno Trans event we see E186 111 on display.

Diesel-electric 246 001 is seen at Cuxhaven on a Metronom service formed with double-deck coaching stock.

Class 285 diesel-electrics are also seen at the Inno Trans event, followed by film of this class in action, including those in service with hyle and Captrain. Angel Trains E186 locomotives are then shown working freight for BLS Cargo, and Polish examples are seen on freights.

At the Bombardier factory in Kassel E186 324 is seen running light on test with subsequent scenes from the lineside and inside the cab. Side views clearly show the four pantographs. Further examples of this class are shown, including E186 333 at Würzburg Hbf. with a train of logs.

Next we see 245 003, a diesel-electric, and we are able to take a look into the engine room. 245 008 is then seen at Mühldorf (Oberbayern) depot in Bavaria, along with sister locomotive 245 001 at the head of a train of DB Regio double-deck stock departing the station for München Hbf. Further scenes of these locomotives at work on similar services are filmed from the lineside, along with some on Sylt Shuttle workings between Niebüll and Westerland (Sylt). Nord-Ostsee-Bahn examples are also shown, while at Lindau Hbf. DB Regio 245 037 is seen departing at the head of a train of double-deck stock bound for Friedrichshafen. Further scenes show these locomotives on similar services filmed from the lineside along the Allgäu main line.

DB Fernverkehr InterCity-liveried 146 553 is seen leaving Halle (Saale) with a train of matching double-deck stock bound for Leipzig Hbf. on the opening day of this service. The 146 is shown pushing the train out of the station with further views from the lineside and from inside the cab.

At the test circuit in Velim, in the Czech Republic, 187 003 is seen being tested both from the lineside and inside the cab. The loco is seen first running under diesel and then electric power and at the head of a heavy train of bogie hopper wagons. A representative of Bombardier Transportation is interviewed and describes the locomotive as we watch it in action and we take a look inside the equipment rooms. A WLE example, No.82, is also seen on test, then Captrain and Raildox class 187s are seen on mixed freight services.

Back at the Bombardier factory in Kassel, Deutsche Bahn AG 147 002 is shown and described. It is shown running light and then at the head of a DB Regio train of double-deck stock. 147 014 is also observed on a similar working.

187 102 is then shown and described, while 187 001 is seen negotiating a depot's traverser and we get a tour around the inside of the loco from its driver. 187 106 is shown moving off shed and we later see this loco leaving Mannheim with a freight. There is further film of this working from the lineside, and another member of this class is seen with a train of bogie tank wagons.

Further DB Fernverkehr Inter City-liveried 147s are then shown at speed with double-deck stock; at Mühlacker station we see one of these arrive and depart.

The programme ends with a look at the first new class 188.

Very little background music is used in this production, mainly towards the end, and it is not too intrusive. As usual, the commentary is only available in German.

The main feature is followed by approximately 45 minutes of trailers for other Eisenbahn Kurier programmes.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. www.eisenbahn-kurier.de

Edition XL

Kult-Dieselloks der DR Die Baureihen V180, V200, 130-132 und 142

by Konrad Koschinski, Dr Franz Rittig,
and Manfred Weisbrod

VGB Verlagsgruppe Bahn GmbH,
Am Fohlenhof 9a,
D-82256 Fürstenfeldbruck, Germany.
www.vgbahn.de

295mm x 210mm 240pp

Softback €19.95

ISBN 978-3-8375-1966-2

German text ref.601801

This is a combination of three *Eisenbahn Journal* 'Sonderausgaben' (special issues) on the East German V180, V200 'Taigatrommel', and V300 'Ludmilla' diesels.

The history and development of each class is covered in great depth, with sections dealing with the background, prototypes, technical details, re-engining, deployment, those used by the East German Government (V180s), those in use with private operators, liveries carried, and those saved for posterity as 'Museumsloks'.

Many of the over 400 black & white and colour photographs and the illustrations included are extracts from Deutsche Reichsbahn publicity material. There are scale drawings and cut-away diagrams showing the arrangement of power units, transmissions, and cooler group within the locomotive body, and many of the archive photos show locos under construction.

At the end of each chapter there is a list of locos built showing for each the running number, factory number, date of delivery, date rebuilt*, date of withdrawal*, and date scrapped* (* as applicable).

In the section on the class V200 'Taigatrommel', the Russian type M62 locos are also featured, as well as those used in Hungary, Poland, Czechoslovakia, and Cuba.

In the chapter about the 132 family, drawings of the proposed driving trailer BR234 of 1994 are featured.

This is an excellent and well researched history of these iconic Deutsche Reichsbahn diesel locomotives. Whilst a good knowledge of the German language will be required to get the best from it, the wealth of photographs alone should whet the appetite of all those with an interest in DR motive power, making it a worthy addition to the bookshelf.

Just two pages are given over to advertisements for other books from VGB, including other titles in the Edition XL series.





Recent H0 kits from Fallar



Recently released is one of the new items announced at Nürnberg, a straight steel fishbelly girder bridge for double track, with stone abutments (ref.120503). It is suitable for use with all model track brands. The kit contains fifty-nine components in four colours, and of course has full instructions.

Also now available is the first in a new modular range, representing a production, forwarding, or storage hall and is based on originals by the 'Goldbeck' building construction company (ref.130890). All parts that make up the basic hall, such as the wall and roof sections, roof lights and hinged windows,



the roll-up doors (which can be installed open or closed), and the trolley inside the hall can be arranged as desired and supplemented with other compatible components in the series. Separately available there are elements such as walls (optionally with gates, doors, or windows), steel truss girders, concrete

columns, and roof sections.

The kit contains 124 components in seven colours, clear foil for windows, and instructions.

Fallar products are distributed in the UK by Gaugemaster.

www.gaugemaster.com
www.fallar.de

Recent Z releases from AZL



New for Z from AZL are Baltimore & Ohio lightweight passenger cars, representing vehicles used on the *Columbian* named train which ran from New York to Chicago with stops in Washington D.C., Pittsburgh, and Akron.

The streamlined set consisted of dome cars, four coaches, diner, and observation car as well as baggage and mail cars. These dome cars were the only ones on the Chicago – Washington – New York routes. They were termed 'strata-domes' by the B&O and carried names such as *Sky* and *High*.

The models are equipped with AZL AutoLatch™ couplers.

The range includes the dome car (choice of two names), a 6-6-4 sleeper, diner (choice of two names), coach, tail observation car (choice of two names), baggage, and a United States Mail Railway Post Office.



The matching F3 A-B set to haul this train is also in stock; three running numbers are available.



Motive power of a later generation is represented by the Genesis P42, which is now offered in the classic New Haven colour scheme with a choice of two running numbers.

The 1937 40' AAR boxcars are now available lettered for the Erie, singly, in two packs, and in sets of four, all with different running numbers.

Orders can be placed at:
<http://ztrackdistribution.com>



Indian semaphore signals for H0



We were interested to see some lattice post semaphore signals (home and distant) being produced for H0 in the **Model Trains India** range by Nitish Industries.

They are made of brass, with a cast whitmetal lamp body, and have a working light and movable arm mechanism. They are supplied assembled and ready to use.

The height is 114mm including finial, and the base is 25mm x 20mm.

The price is £40.00 each.

Postage via EMS Speed Post with tracking costs £14.00; this service usually delivers within ten working days.

(Editor's note: these items may be liable to tax and duty when delivered to the UK, and perhaps also to other countries.)

They can be purchased via the website, through eBay UK, or directly by e-mail (nitishindustries111@gmail.com) with secure payment via PayPal.

Model Trains India welcome trade enquiries for these items. They are also interested in manufacturing custom bulk orders.

See the website for further details: www.modeltrainsindia.com



Latest H0e models from Minitrains

After a long production delay caused by manufacturing problems last autumn, when no new models were released, it is with great delight that Minitrains announce the release of a host of new goods wagons, all based on a standard underframe, with imitation grain on the 'wooden' body parts.

The range includes bolsters, flats, flats with ends (for pulpwood), boxes (for waste), and tipping V-skips.

The wagons are sold in packs of four – with brake, without brake, or two of each.

The Plymouth four-wheel diesel loco is now available in nine different liveries, including a simple self-assembly version which is supplied with the motor installed and the chassis ready-to-run; the body is unpainted grey and comes with extra detailing accessories. Liveries available are all black, black/black/red

(cab/body/frames, *illustrated*), black/yellow/black, black/red/black, yellow/yellow/red, all yellow, black/red/red, and green/green/black.

There is no price increase, but €:£ exchange rate variations are possible.

Minitrains,
Standard Light GmbH.,
D-76189 Karlsruhe, Germany.
www.minitrains.eu



Cycle van for H0e from Ferro Train



To mark the hundredth anniversary of the narrow gauge Mariazellerbahn in Austria, the railway put into service the prestige Ötscherbär train set in a plain brown livery which reflected traditional colour scheme. During the summer season in addition to the baggage van, bicycle vans were added to the train allowing the carriage of an additional forty cycles. Only two were painted in the matching brown livery, Nos.16 824 and 16 841. Ferro-Train have created a

limited edition model, finely printed, to enable modellers to complete an authentic train consist. Separate printed panels are included which allow the vans to be used in different periods. Only twenty have been made, so if you are interested we suggest acting quickly!

Note new address since 12th June.

Ferro-Train
A-1230 Wien, Leopoldigasse 15-17,
Austria.
www.ferro-train.com

New for 1:35 by Matho Models



The Belgian accessory specialist has released three new items intended for 1:35 scale.

ref.35102 Movie Posters A – 1940s.

ref.35104 Movie Posters C – 1970s and 1980s.

ref.35108 Warning and Caution Tapes (double sided) A – English.

For more details, including prices, how to order, and UK suppliers:
www.mathomodels.com



Arnold offers Swedish vans in N

Arnold (Hornby) have announced two new Swedish Epoch III versions of the familiar four-wheel refrigerated vans in N: singly in "nykarnat" blue (ref.HN6403) and a twin pack in "THERMOTRANSPORT" blue & grey (ref. HN6402). Both are registered with the SJ (Statens Järnvägar).

For more details: www.hornby.com



Innovative wheel cleaner

New from KPF Zeller is an ingenious and very efficient motorised wheel cleaner for HO which is designed to be built in to a convenient track on the layout. This avoids having to handle the locomotives.

It uses two powered moving platforms – the locomotives run over these under power. These cleaning units oscillate slowly, at a frequency which has been carefully calculated (and is deliberately not changeable) for a consistently good cleaning result.

It is very quiet – much quieter than a domestic refrigerator.

It works on two- or three-rail, analog or digital, with all commercial HO track systems (except Trix Express).

No cleaning fluid is used, and the cleaning cloth material (like that used so successfully in their rail cleaning wagon) is inexpensive and readily available.

The overall length is c.250mm and the width 52mm.

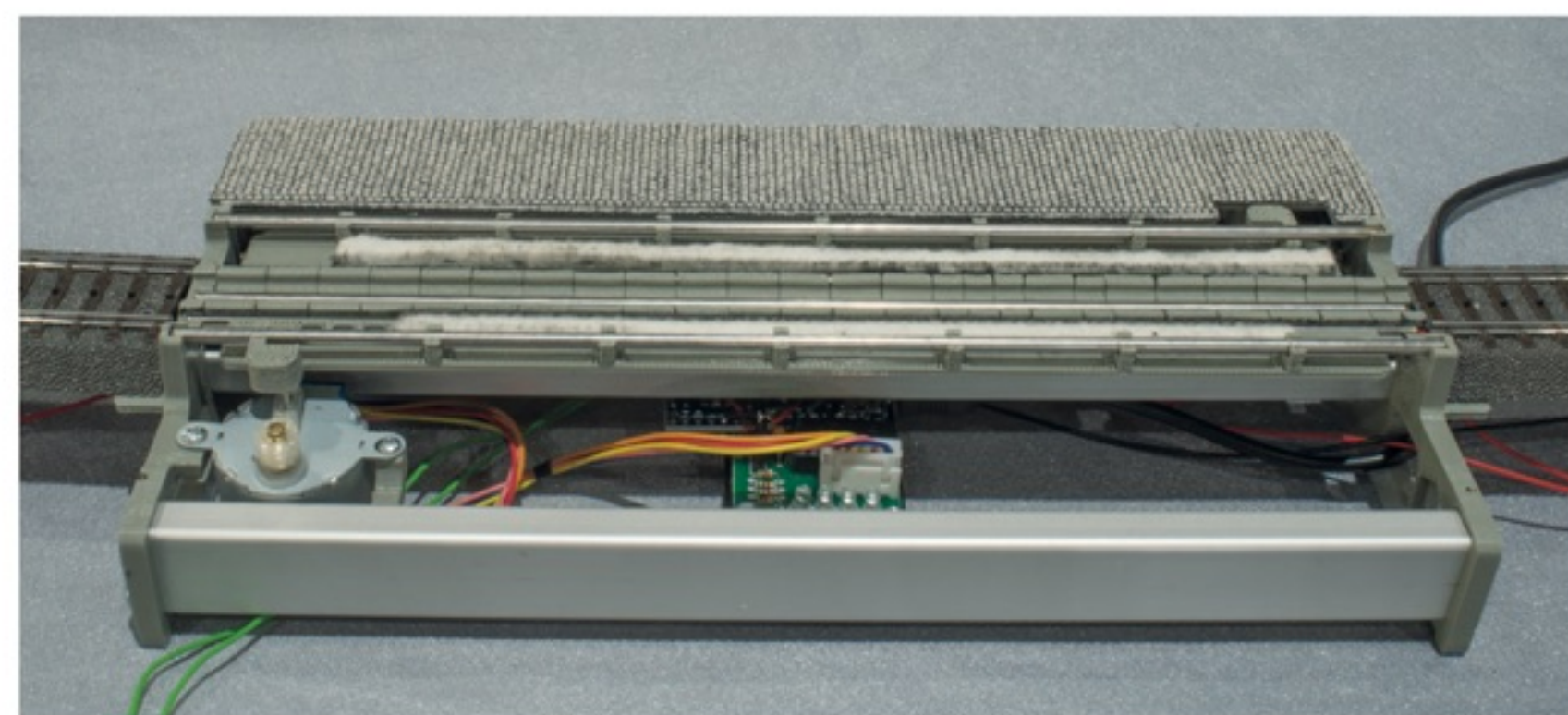
If required, the firmware can be updated free in future via a USB link.

The device has been developed and is manufactured in Germany by reliable in-house employees.

The unit comes ready to use, with all electronic components including a plug-in 5V power supply, with a cable approximately 1.5m long, extendable to a maximum of 25m, and there are detailed operating instructions.

The cobblestone side pieces shown are not included – these were attached to the demonstration unit simply to show how easily the whole thing unit can be built in to the layout.

KPF Zeller, Friedhofstrasse 6,
D-73108 Gammelsheim, Germany.
<https://kpf-zeller.de>



Exhibition Diary

Dates, events, and information

Please send details for possible inclusion to:

Peco Publications & Publicity Ltd., Beer, Devon, EX12 3NA.

Telephone: 01297 20580 Fax: 01297 20229

e-mail: rm-clubs@btconnect.com

Saturday 22 June

KING'S LYNN, Norfolk

Organiser: King's Lynn MRC.

Venue: Lynnsport, Green Park Way, King's Lynn, Norfolk, PE30 2NB.

Open: 1030 – 1600

Admission: adults £5.00, children (5 – 15) £2.00, under 5s free.

Amenities: disabled-friendly; refreshments.

Features: layouts include *Gila Canyon* (US N, CM November 2018), and vintage French O gauge three-rail. Trade.

Contact: <https://klmrc.co.uk>

Sunday 23 June

BARTON LE CLAY, Bedfordshire

Organiser: Beds and Bucks

Narrow Gauge Modellers.

Venue: Barton Le Clay Village Hall, Hexton Road, Barton Le Clay, MK45 4JY.

Open: 1000 – 1600

Admission: adults £5.00, accompanied children free.

Amenities: free parking; refreshments.

Features: open day – 11 layouts, plus displays, trade, and OO9 Society sales.

Contact: Brian Key 01908 615287

Saturday 29 & Sunday 30 June

PERTH

Organiser: Perth MRC.

Venue: Dewars Centre, Glover Street, Perth, PH2 0TH.

Open: 1000 – 1730 Saturday
1000 – 1700 Sunday

Admission: adults £9.00, children £4.00, family (2+2) £20.00.

Amenities: next to railway station; park & ride from Broxden on Saturday; on-site parking; buffet and bar.

Features: layouts include *Ronshafen* (N), *Treffburg* (Austrian/German HO, CM December 2013) *Campo de Leste* (Portuguese HO, CM February 2018), *Cascade Yard* (US HO), *Broadford* (Australian HO, CM January and March 2010, January 2013, and June 2018), *Mariana Mesa* (US On30), and *Eu (Le Treport) depot* (French O, CM November 2018). Societies, trade.

Contact: 07365 115955
www.perthmrc.com

Saturday 29 & Sunday 30 June

EXETER, Devon

Organiser: Exe MRS.

Venue: Matford Livestock Centre, Matford Park Road, Marsh Barton, Exeter, Devon, EX2 8FD.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: £7.50, accompanied children free.

Amenities: free parking (limited on Sunday); Park & Ride service (Green route) to Matford Park Road (Saturday only); disabled access; restaurant.

Features: 35+ layouts including *Bw.Dügelkirchen* (German HO, first appearance), *Canadian National Jillsburgh* (HO), and various OO9/HOe layouts from the local area group of the OO9 Society. Trade.

Contact: 01392 421906
www.exemrs.co.uk

Saturday 6 July

BEACONSFIELD, Buckinghamshire

Organiser:

Beaconsfield & District MRC.

Venue: The Beaconsfield School, Wattleton Road, Beaconsfield (old town), Buckinghamshire, HP9 1SJ. 5 minutes from J2 M40 (signposted).

Open: 1030 – 1700

Admission: adults £5.00, children £2.50.

Amenities: free parking on site; refreshments.

Features: layouts include *San Maria Gandia* (Spanish HOe/HO, CM September 2018) and *Elmwood Yard* (HO). Trade.

Contact: Roy Jackson 01753 643724
www.beaconsfieldmrc.org

Saturday 13 July

HODDESDON, Hertfordshire

Organiser: The Friends of

Robert Barclay Academy.

Venue: The Robert Barclay Academy, Cock Lane, Hoddesdon, EN11 8JY.

Open: 1030 – 1630

Admission: adults £4.50, concessions £3.00, family (2+2) £12.00.

All profits to academy projects.

Amenities: free parking on site; refreshments.

Features: layouts include *Schweizer Kleinstadt* (Z), *Brownsville* (HO), and *Transcontinentally Tri-ang* (OO & HO). Demonstrations, societies, trade.

Contact: 07866 641215

Saturday 13 & Sunday 14 July

BEER, Devon

Organiser: Pecorama.

Venue: The Station Gallery, Pecorama, Beer, Seaton, Devon, EX12 3NA.

Open: 1000 – 1700 both days

Admission: free (normal Pecorama admission prices apply).

Amenities: parking; disabled access (hilly site renders three-wheel mobility scooters unsuitable); refreshments.

Features: *The Andeer Line* (Swiss HOm modular layout). Visitors are invited to help operate the layout, with trains driven individually using smart 'phones and tablets. There will also be demonstrations of the foam baseboard construction and the use of electrostatic grass. Drop in for as long as you wish. See website for details.

Contact: www.pecorama.co.uk

Saturday 13 & Sunday 14 July

HUNSLET, Leeds, West Yorkshire

Organiser: Middleton Railway Trust.

Venue: Middleton Railway,

The Station, Moor Road, Hunslet, Leeds, LS10 2JQ.

Open: 1000 – 1700 both days (steam trains operating between 1100 – 1600).

Admission: adults £5.50, children £2.50, family £14.00. Exhibition/Engine House plus unlimited train rides – adults £8.50, children £3.50, family £20.00.

Amenities: disabled access to exhibition and trains but not footplate experiences; light refreshments.

Features: layouts in many scales and gauges including *Dornochbahn* (HOe) and *Roanoake Roundhouse* (HO). Footplate experience on a full-size loco £10.00; minimum age 18, maximum 76, no need to book in advance. Trade, and other attractions on site including nearby retail outlets.

Contact:

Ian Dobson 01274 613687 (eves only)
www.middletonrailway.org.uk

Saturday 20 July

BRIGHTON

Organiser: Sussex Vintage Model Railway Collectors.

Venue: The Knoyle Hall, Knoyle Road, Preston Park, Brighton, BN1 6RB.

Off A23, close to main bus routes and a short walk from Preston Park station.

Open: 1000 – 1630

Admission: adults £4.00, children free (must be accompanied).

Amenities: unrestricted and two-hour kerbside parking nearby; wheelchair access to main hall but not to first floor displays; refreshments.

Features: layouts including classic Hornby O, Hornby Dublo, Bassett-Lowke, Lionel, Trix Twin, working Meccano, and more. Trade.

Contact: 07785 567251

www.sussex-transport.co.uk/svmrc

Saturday 20 July

FELIXSTOWE, Suffolk

Organiser: WorldWide Group (WWG) of the N Gauge Society.

Venue: Trinity Methodist Church, 26, Hamilton Road, Felixstowe, IP11 7AN.

Open: 1000 – 1600; AGM 1610.

Admission: adults £3.00, WWG members and accompanied children free. Admission refunded to visitors who join WWG on the day.

Amenities: parking nearby; railway station 10-15 minutes walk; disabled access (but no parking on site); refreshments.

Features: layouts include *Hindenberg* (German), *Hartenberg* (Austrian), *Depot de Camions*, *L'Isle de St. George*, and *Schiebourg* (French), and *East Tyrone* and *Southerland* (US). Members' sales, trade.

Contact: secretary@ngauge-wwg.org.uk

Saturday 20 & Sunday 21 July

CUPAR, Fife

Organiser: Cupar & District MRC.

Venue: The Corn Exchange, St.Catherines Street, Cupar, Fife, KY15 4BT.

Open: 1000 – 1630

Admission: adults £4.50, children £2.50, family (2+2) £12.00.

Amenities: free parking nearby; good public transport; refreshments.

Features: layouts include *Altdorf Ost* (HO), *Burlington Central* (N), and *Ripon WI* (HO). Club and society stands, trade.

Contact: www.cuparmrc.co.uk

Saturday 27 & Sunday 28 July

NORTH SHIELDS, Tyne & Wear

Organiser: Blyth & Tyne MRS.

Venue: John Spence Community High School, Preston Grange, North Shields, Tyne & Wear, NE29 9PU.

Open: 1000 – 1700 Saturday
1000 – 1600 Sunday

Admission: adults £7.00, children £3.00, family (2+2) £16.00.

Features: up to 30 layouts, including *The Bridge at Remagen* (German N), Derby Freemoo American HO modular, and *Broadford* (Australian HO, recently extended, see CM June 2019). Demonstrations, trade.

Contact: www.railexne.com

Sunday 28 July

THIRSK, North Yorkshire

Organiser: Thirsk MRG.

Venue: Thirsk Town Hall, Westgate, Thirsk, North Yorkshire, YO7 1QR.

Open: 1030 – 1630

Admission: adults £4.00, children £2.00, family £10.00.

Features: 12+ layouts, including *Nordhafen* (German HO, in this issue). Demonstrations, trade.

Contact: 07766 697708
www.expo-thirsk.co.uk

Meetings

Tuesday 2 July – 20.00

The Enfield Transport Circle

British Steam in South Africa

1987 to the present by Ken Livermore.

Venue: First Floor Meeting Room, The Drill Hall Sports Club, 1, Old Park Avenue, Enfield, EN2 6PJ.

Admission: £4.00.

Contact: k.h.crane@hotmail.co.uk
Kenneth Crane 07596 142117

Monday 8 July – 19.30

Lincoln Railway Society

Gold In Them Thar Hills And Railways by Graham Lightfoot.

Venue: St.Hugh's Church, Harewood Crescent, North Hykeham, Lincoln, LN6 8JG.

Admission: visitors £3.00.

Contact: 01522 698085
grahamlightfoot1948@gmail.com

Thursday 18 July – 19.15

Marlow & District

Railway Society

Travels with the LCGB in North Korea in 2014/5 by Colin Miell.

Venue: Bourne End Community Centre, Bourne End, Buckinghamshire, SL8 5SX.

Contact: 01494 638090
www.mdrs.org.uk

Saturday 27 July – 14.00

T&LRS – East Midlands Area

Australian Tramways 1981, 1996, and 2016 by Bob Bracegirdle.

Venue: Sixth Beeston Scout Hut, Middle Street, Beeston, Nottingham, NG9 1GA.

Contact: David Hanger 01455 203307

News

Information from CONTINENTAL MODELLER

New version of Tillig railcar



Further to our review of the Harz four-wheel railcar by Tillig last month, Tillig have recently released a further version which is not restricted to the Harz system, in Mittelbadische Eisenbahngesellschaft (MEG) blue & white livery (ref.02941).

MEG railcars T1 to T8 were diesel-mechanicals driven on one axle (A1). The Gotha wagon factory, owned by Orenstein & Koppel since 1931, supplied these eight railcars between 1934 and 1941. They are thus the largest standard class of railcar on any German narrow gauge railway. They were able to haul a trailer when necessary. They took over the majority of passenger traffic on the 115km MEG network between Rastatt and Lahr.

The vehicles followed the lightweight construction principles of the time, with a body welded from sections and sheet. The design allowed for variants to meet customer requests.

The drive used a Daimler-Benz OM 65 engine and Mylius transmission and was arranged below the floor.

The equipment also included a whistle, bell, electric lighting, heating stove, indirect brake, and the standard MEG central buffer with coupling hook beneath. Side chains were added later. The vehicles performed well, especially on the street running sections.

T7 has survived and is now owned by the Interessengemeinschaft Historischer Schienenverkehr e.V. (IHS).

Tillig offer the model in both HOm (12mm gauge) and HOe (9mm gauge).
www.tillig.com

SAMPLE SUPPLIED BY
Mount Tabor Models,
Scarthin, Cromford, Matlock,
Derbyshire, DE4 3QF.
www.mount-tabor-models.co.uk

Photo: Graham Lightfoot.

New mini motor from Tramfabriek

In 2017, Tramfabriek made those who model in small scales very happy with the introduction of an affordable 8x16mm 12V coreless motor (ref.0816S) which is still available. Customers asked for a similar motor with two shafts, as well as a smaller version. Tramfabriek was able to produce 8x16mm (ref.0816D) and 7x16mm (ref.0716D) coreless motors with two shafts. It is notable that the 0816D was a world first.

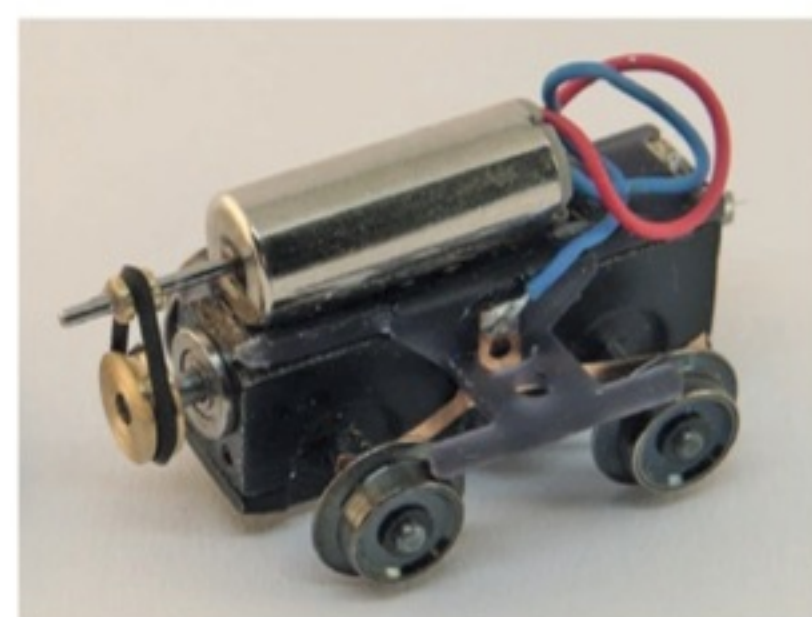
Tramfabriek is now proud to announce an even smaller motor: a 6x15mm (ref.0615S) and, yes, it is 12V. At 12V it rotates at 21,800rpm, 21% slower than its closest rival (Maxon). The 8mm 0.8mm diameter shaft is three times longer than the Maxon 10V motor, which has a short 2.5mm shaft on each end: this makes it much easier to fit a worm, probably the most common method amongst railway modellers.

It can also run very slowly. It makes a perfect 12V replacement for the Busch HO of Feldbahn locos, and runs much quieter than the geared motor fitted.

This motor makes it possible to make Tramfabriek belt drive chassis even smaller. We illustrate one of these motors installed on a brass chassis with 5.6mm diameter wheels on a 13.8mm wheelbase, which makes it the smallest drive Tramfabriek has ever made.

No load current: 15mA max
Stall current: 110mA max
Stall torque: 4.9g.cm
Terminal resistance: 113Ω ±5%
Tramfabriek

2, Locken Cottages, The Walk,
Eton Wick, Berkshire, SL4 6LN.
<https://tramfabriek.nl>



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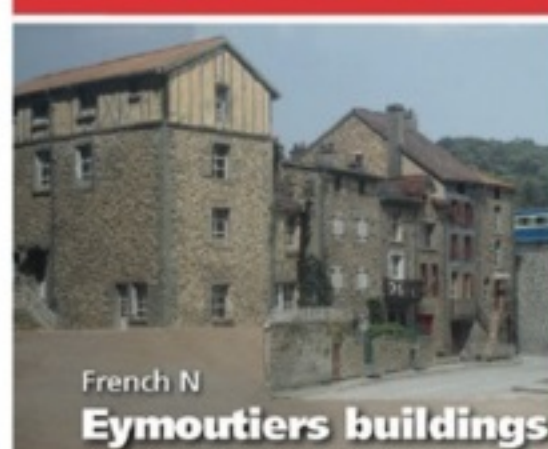
FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

Rusinowo Waskotorowe Polish HOe

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Also in this issue...



French N
Eymoutiers buildings



German G
Brohltal diesel D5



American N
Operating system

Latest Reviews ... and much more

PECO

• Rusinowo Waskotorowe

Stefan Marszal has carefully recreated in HOe a small branch terminus in rural Poland.

• Buildings from Eymoutiers

Ian Stock has been busy making buildings borrowed from a genuine location adapted to suit his current French N gauge project.

• Brohltal diesel D5

Günter Völler adapts another proprietary product into a more accurate large scale model of a specific metre gauge locomotive with which he is very familiar.

• Operating system

Following the description of his US N layout *Twin Forks* (in this issue), Mike Garforth explains how it is worked.

• Through the garden

Peter Norman explains how he utilised an existing O gauge line between two sheds to provide somewhere to run a growing collection of American locomotives and rolling stock.

... and much more!

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CONTINENTAL MODELLER – BACK ISSUES – for more information on which issues are available, please contact: *Technical Advice Bureau*, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580. Price for UK Residents £3 per issue, Europe £3 per issue plus £2 postage sent Airsaver Post and rest of the World £3.00 per issue plus £3 postage sent Airsaver Post. Airmail – please enquire.

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Auctions

UK TOY AND MODEL AUCTIONS – The next auction will be on Monday 22nd July 2019 at Craxton Wood Hotel, Parkgate Road, Ledsham, South Wirral, near Chester CH66 9PB. Viewing 9.30am to 11.30 am. Auction starts 11.30am. Up to 550 lots to include: Live steam, Märkin and Bing Gauge 1, G scale, Hornby and Kit-built O gauge, Hornby Dublo, Hornby, Bachmann, Mainline, Lima, Dapol, Continental and American Outline Locos and Rolling Stock including MTH, Paragon, Atlas, Bachmann Spectrum, Fleischmann, Roco, Liliput etc. Dinky Toys, Corgi, Matchbox, EFE and OOC Buses, Drake and WSI Trucks, Minichamps, Vitesse and Quartz Porsche Cars, Mamod, Plastic Kits and Quality Books plus much more. Toy collections large or small always required for our forthcoming auctions, assistance given if selling as part of an estate. Full catalogue and further details on our website www.uktoyauctions.com or send £3.50 to 46 Wirral Gardens, Bebington, Wirral, Merseyside CH63 3BH or telephone BARRY STOCKTON on 0151 334 3362 or TONY OAKES on 01270 652 773 for an informal, no obligation chat.

Fairs, Sales and Swapmeets

June 2019

30 June (Sun) 10am-2pm

CATTERICK FAIR, Catterick Leisure Centre, Gough Road, Catterick Garrison DL9 3EL 10 minutes off A1, Adults £3, Children & Seniors £2. Early entry £5. Free parking. JIM CORR.

Tel: 0750 403 5955, email: jim_corr73@hotmail.co.uk for further information.

July 2019

30 July (Tues) 6pm-9pm

GARSTANG TOYFAIR, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON.

Tel: 01282 439 009.

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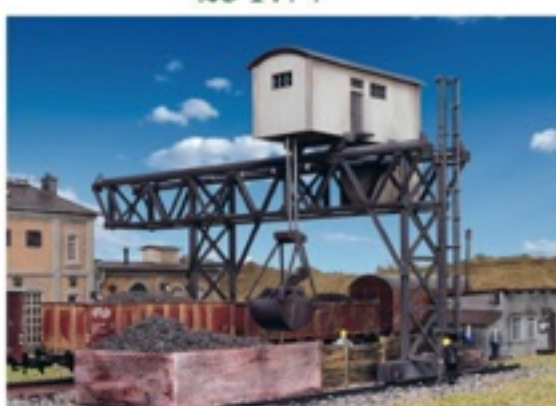
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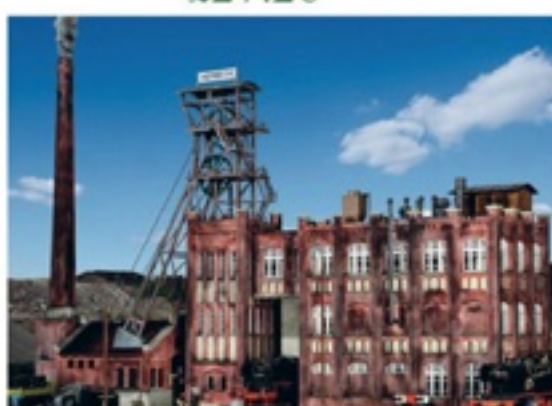
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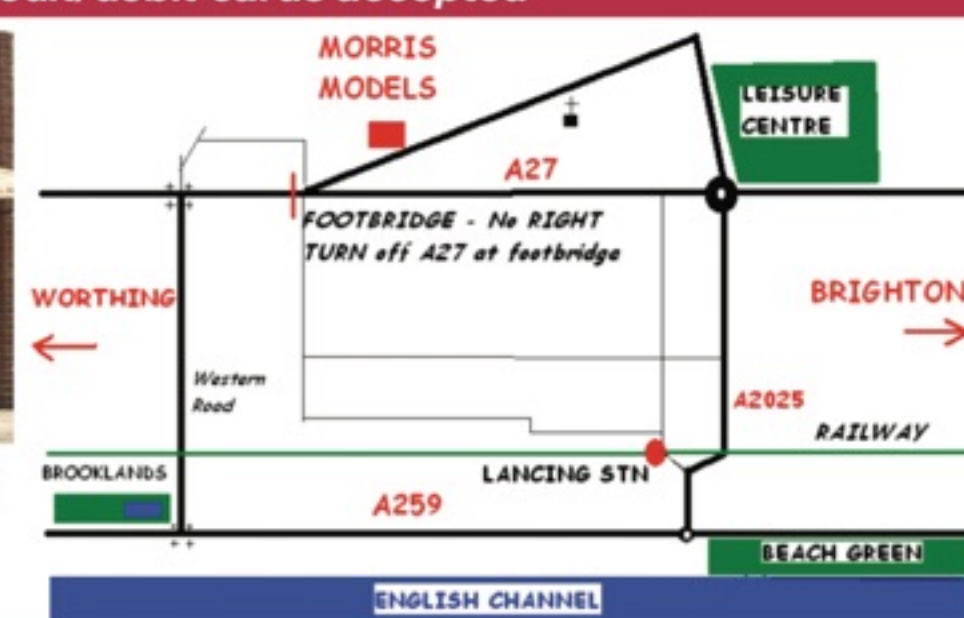


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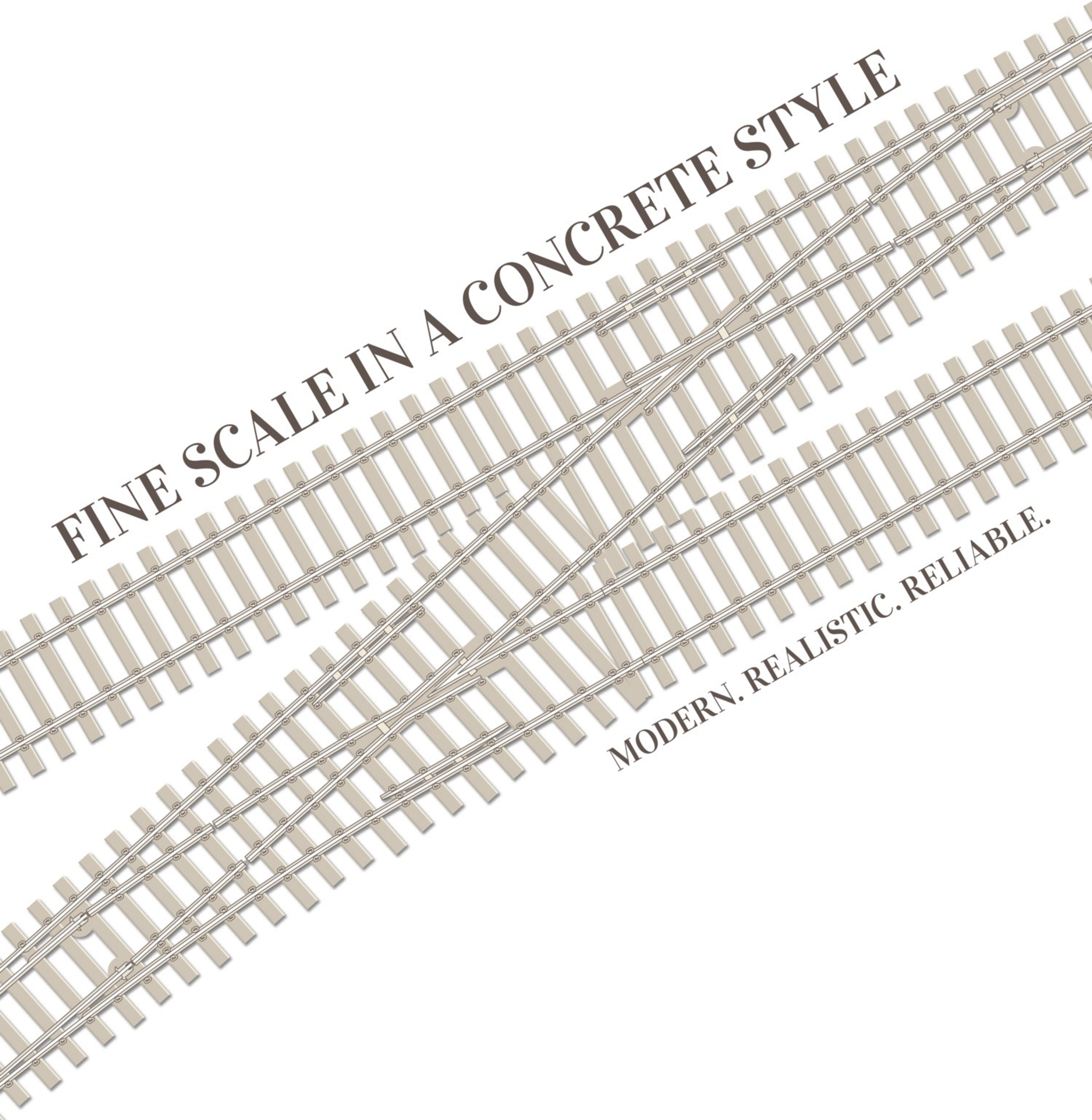
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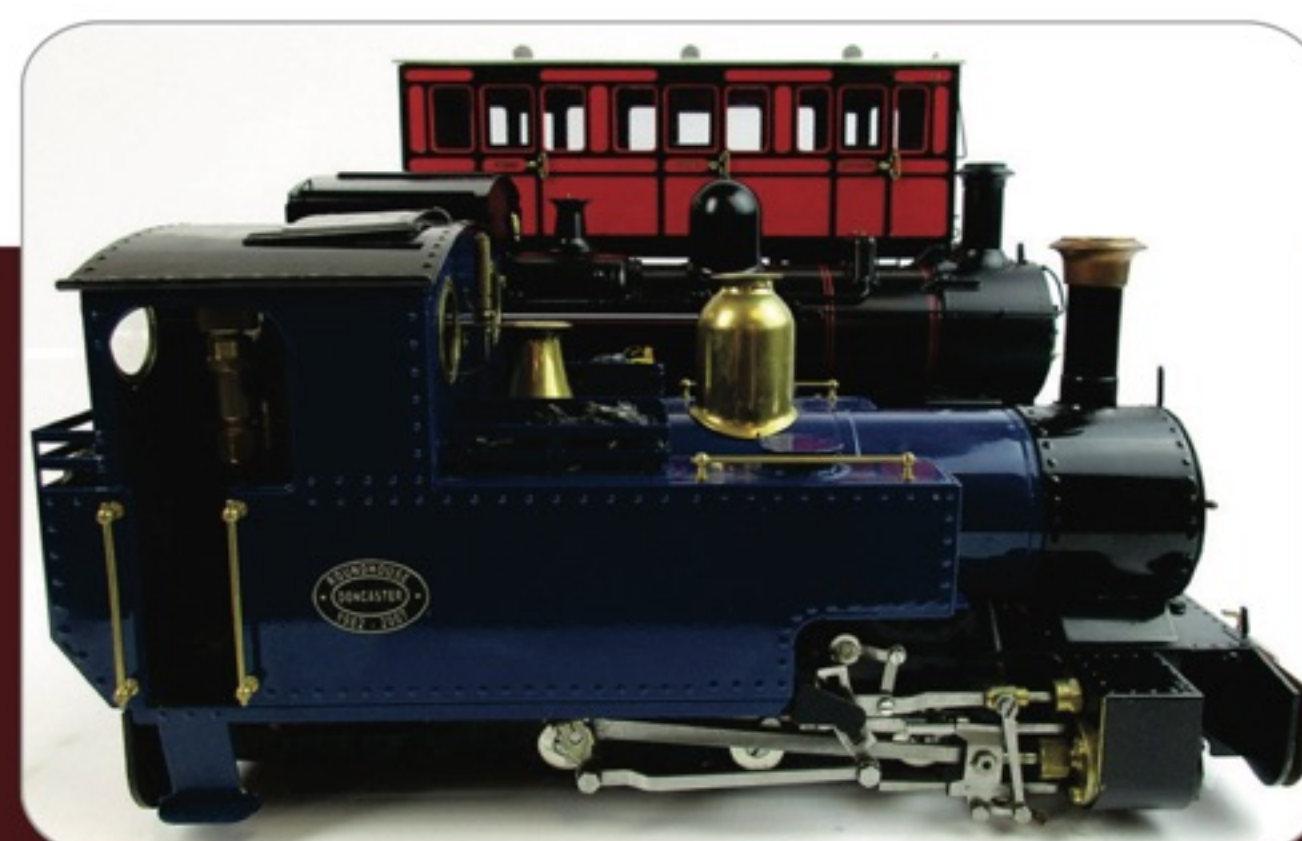
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CONTIKITS

STRICTLY MAIL ORDER

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5000728 L+N 90T Hopper #182720, brown.....	\$5.00 (4)
5000729 L+N 90T Hopper #182778, brown.....	\$5.00
39846 L+N C+O Cupola Caboose #912, red.....	\$8.00
30861 Lanza Inc (ACF) 23500 Gal Tank #75446, black.....	\$10.00
5000956 Maine Cent 40' PS-1 box car, brown.....	\$8.00
39558 Maine Cent 50' Single door box car, yellow.....	\$7.00
PPR-1 Mantou Pikes Peak railway Caboose #5, red.....	\$9.00
32645 N+W 90T Hopper #12575, black.....	\$5.00 (3)
50001845 N+W 90T Hopper #12100, black.....	\$8.00
50000978 N+W St Cupola Caboose #518553, red.....	\$12.00
50000979 N+W St Cupola Caboose #518595, red.....	\$12.00
43031 N+W Standard Cupola caboose, blue.....	\$7.00
43032 N+W Standard Cupola caboose, blue.....	\$7.00
43072 N+W Standard Cupola caboose, red.....	\$7.00
43071 N+W Standard Cupola caboose, red.....	\$7.00
31091 Northwest hardwoods Evans DPD Box #13150, yellow.....	\$8.00
43048 NP St Cupola Caboose #1101, brown.....	\$12.00
43099 NS Horse Head St Cupola Caboose #55518, red.....	\$12.00
50002163 NS 2 Bay Offset Side Hopper #9114, brown.....	\$7.00
33732 NS 50' FGE Box #650008, brown.....	\$12.00
50000980 NS St Cupola Caboose #555568, red.....	\$12.00
50000981 NS St Cupola Caboose #555568, red.....	\$12.00
34574 NYC 40' PS-1 box, green.....	\$7.00
50001811 Penn Central 40' Plug Door box, green.....	\$6.00
50001812 Penn Central 40' Plug Door box, green.....	\$6.00
31142 Plymouth Marketing Evans DPD box, orange/yellow #10557.....	\$8.00
50002947 Robinsden Saguenay 1932 AFA Box #1001, grey.....	\$10.00
32993 Seaboard Line 90T hopper, brown.....	\$7.00
39581 SF Caboose #1783, brown.....	\$7.00
50002630 SDO Line 70T One Car #61903, brown.....	\$7.00
50002631 SDO Line 70T One Car #61919, brown.....	\$7.00
50002218 SDO Line Thrall 2743 Gondola #9101, silver/red.....	\$12.00
50002738 Southern 90T Hopper #360301, brown.....	\$5.00
50002739 Southern 90T Hopper #360304, brown.....	\$5.00
32722 Southern 90T Hopper #360360, brown.....	\$5.00
32723 Southern 90T Hopper #360363, brown.....	\$5.00
50002364 SP 50' SD Box #651400, brown.....	\$7.00
32442 Standard Chemical Beer Can Tank #509, red.....	\$8.00
50000085 Thunderson ACF 5250 Hopper Car #9804, grey.....	\$8.00
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50002594 UP 40 Cupola Caboose #903002, green.....	\$8.00
35625 UP Cupola Caboose #9329, yellow.....	\$8.00
39995 UP Line 70T One Car #26469, silver.....	\$8.00
43804-40 Utility Fuels Coalwayer, black/orange.....	\$8.00
3963 WAC ACF 4 Bay Centreflow #83058, grey.....	\$8.00
3735 West Pacific 3 Bay CVD Hopper, grey.....	\$7.00
50001424 Weyerhaeuser 53' Double Plug door box car, green.....	\$8.00
43807-5 Wisconsin Electric Power Coalwayer, black/yellow.....	\$8.00
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4081K EL Baggage, grey/maroon.....	\$12.00 (2)
4011K EL coach, grey/maroon.....	\$12.00 (2)
4071K EL Diner, grey/maroon.....	\$12.00
4061K EL Dome, grey/maroon.....	\$12.00

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4072E ERIE Diner, blue/grey.....	\$12.00
4022E ERIE RPO, blue/grey.....	\$12.00
4012E ERIE sleeper, blue/grey.....	\$12.00
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0001-04042 IC Smoothside coach set X5, orange/brown.....	\$40.00
0001-040430 MP Smoothside 5 car set, grey/blue.....	\$40.00
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0001-040402 SF Valley Flyer x 5 car set, silver.....	\$40.00
0003-570007 Southern Pacific Heavyweight x 6 coach set, orange/red.....	\$50.00
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001-656104-1 UTX 60ft Tank #64728, blue.....	\$9.00
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0001-040028 VIA Rail 85ft coach #2246, blue/yellow.....	\$15.00 (2)
0001-040728 VIA Rail 85ft Diner, Diamond Lake, blue/yellow.....	\$15.00
0001-040128 VIA Rail 85ft Pullman, Riverview, blue/yellow.....	\$15.00
001-881603 Wisc Cent 50' Auto Box #20001, red.....	\$7.00

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65207-16 Potacum Cylindrical Cov Hopper #850, orange.....	\$12.00
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176-8415 BNSF SD70 Ace #9370 (Swallow), orange.....	\$50.00
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46470 Pennsy gondola, brown.....	\$9.00
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FIELD SYSTEM

A complete system for modelling natural fields and grassy locations

The **Field System** makes it easy to add texture and dimension while modelling meadows, fields and pastures with incredible realism. A wide range of colours and textures allows you to model any season or grassy landscape. All products are designed to work together, and each one includes simple-to-follow instructions to make modelling easy.

Static King™ WFS639

Static King makes it easy to model large grassy areas and ensures Static Grass is applied vertically. The Static King has an ergonomic design and can be placed upside down for easy filling. It comes with different Sieve sizes for creating a varied effect using 2-12mm lengths of Static Grass. A divider allows the use of multiple colours and sizes at the same time for modelling realistic fields and other tall grasses.

- Clear hopper
- 360° Power Indicator Light
- Power via Power Supply or Batteries
- Non-slip rubber grip
- Divider allows use of multiple colours & sizes
- Easily interchange twist-lock Sieve
- Automatic discharge after each use



Also Available

- **Static Grass** in a variety of lengths and colours
- **Field Grass** to model tall grass, field grass and weeds
- **Flowers** includes four shades of colour
- **Plant Hues** includes four shades of colour
- **Briar Patch** to add brambles, thickets and shrubbery
- **Static-Tac, Tuft-Tac & Spray-Tac**
- **Model-Vac** for picking up loose landscape material
- **Accent Shakers** help easily apply **Plant Hues & Flowers**



AVAILABLE IN THESE STORES:

AGR Model Railway Store - Bedfordshire
Model Railways Direct - Bristol
Berkshire Dolls House Company - Berkshire
Trident Trains Model Shop - Cheshire
Peters Spares Model Railways - Cleveland
Kernow Model Rail Centre - Cornwall
Broughton Newsagents - Cumbria
Jackson's Models & Railways - Cumbria
Rhuddlan Models - Denbighshire
Malc's Models - Derbyshire
A2B Model Railways - Derbyshire
South Devon Railway - Devon
Youings Toymaster Model Shop - Devon
Shady Lane - Devon
Buffers Model Railways - Devon
Colletts Model Trains - Devon
Setchfield's - Dorset
Model Railway Solutions - Dorset
Bluebell Railway - East Sussex
Train Times - East Sussex
John Dutfield - Essex
Flair Rail - Essex
Cheltenham Models - Gloucestershire
Bourton Model Railways - Gloucestershire
Sawyer Models - Greater Manchester
Arcadia International Model - Greater Manchester
Ron Lines - Hampshire
AC Slotcars - Hampshire
Test Valley Models - Hampshire
Alton Model Centre - Hampshire

The Model Shop - Hampshire
Wicor Models - Hampshire
Upstairs Downs - Isle Of Wight
Mark's Models - Ireland (Dublin)
Times Past Antiques - Ireland (Louth)
Hobby Shop - Kent
Rochester Games & Model - Kent
Kent Garden Railways - Kent
Gravesend Games & Models - Kent
Maidstone Games & Models - Kent
The Signal Box - Leicestershire
Ashtree Enterprises - Lincolnshire
DB Models - Lincolnshire
Mad About Trains - Lincolnshire
Digitrains - Lincolnshire
MRSG - Lincolnshire
DKG Hobbies - Merseyside
Bure Valley Railway - Norfolk
Great Eastern Models - Norfolk
A & H Models - Northamptonshire
The Railway Conductor - Northamptonshire
J & J Models - Northumberland
Arcane Scenery and Models - Nottinghamshire
DJ & F Benham - Nottinghamshire
The Goods Yard Ltd - Nottinghamshire
Trains4U - Peterborough
Macs Model Railroading - Scotland (Forfar)
KMS Railtech - Scotland (Aberdeenshire)
Scoonie Hobbies - Scotland (Fife)
Hollylovestoys - Somerset

Frome Model Centre - Somerset
Topp Trains - Staffordshire
Model Junction - Suffolk
Flair Rail - Suffolk
Orwell Model Railways - Suffolk
Kernow Model Centre - Surrey
KDC Hobbies - Wales (Ammanford)
Pontypool & Blaenavon Railway - Wales (Blaenavon)
Rhuddlan Models - Wales
Lord & Butler Model Railways - Wales (Cardiff)
Firestorm Games - Wales (Cardiff)
Tony's Trains at Barby Model Rail - Warwickshire
Our Place - Warwickshire
Ian Allan Ltd Birmingham - West Midlands
Hobbyrail - West Midlands
Going Loco - West Midlands
Salisbury Model Centre - Wiltshire
Spot On Models - Wiltshire
Green Scene - Worcestershire
Worcester War Games - Worcestershire
DMB/Footplates - Worcestershire
Redcar Models & Hobbies - Yorkshire (Redcar)
Peters Spares Model Railways - Yorkshire (Middlesbrough)
The Model Centre (TMC) - Yorkshire (Whitby)
Panda Models - Yorkshire (Doncaster)
Rails of Sheffield - Yorkshire (Sheffield)
Monk Bar Model Shop - Yorkshire (York)
Millennium Models - Yorkshire (Leeds)



Please visit www.bachmann.co.uk to find a retailer and view tutorial videos

Model • Collect • Create



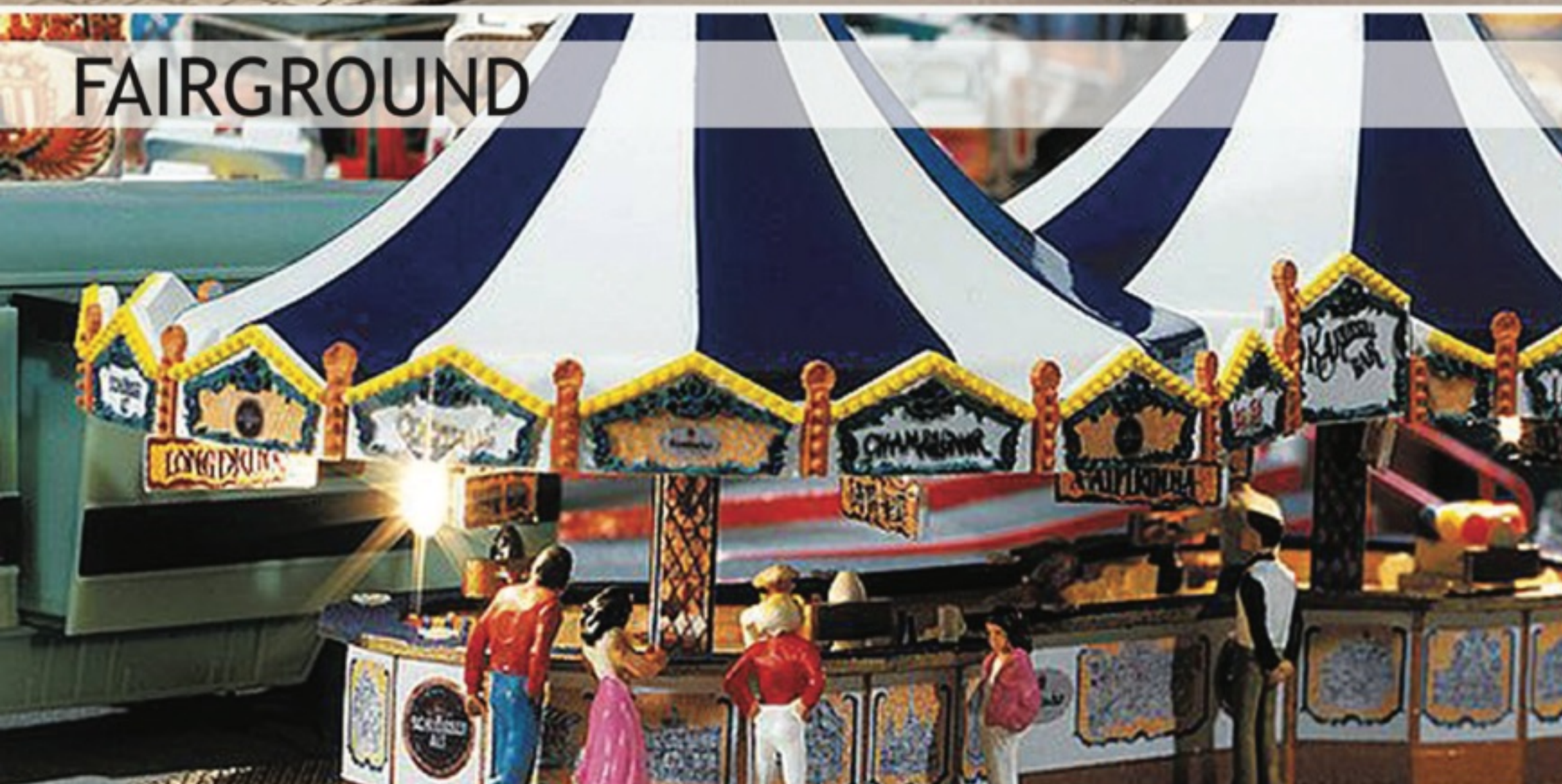
Faller are world famous for their plastic building kits. With a huge range in different scales of Continental-style buildings, any layout will benefit from the inclusion of Faller kits. To compliment the buildings, Faller also produce a range of figures and accessories to enhance the atmosphere of any street scene, as well as trees and other scenic items to add to town and countryside.

CAR SYSTEM

The CAR SYSTEM range allows you to have moving vehicles on your layout with no visible tracks or wires. Automate junctions, traffic lights, and even speed cameras!



FAIRGROUND



The Faller Fairground range is produced in N and HO Scale, and is almost a separate hobby it itself! You can add sound and movement to your layout with interesting rides and attractions.