

# CONTINENTAL MODELLER

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FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH

VOLUME 40



## Austrian HO Zeltweg

Also in this issue...



Bavaria HO  
**Pottendorf**



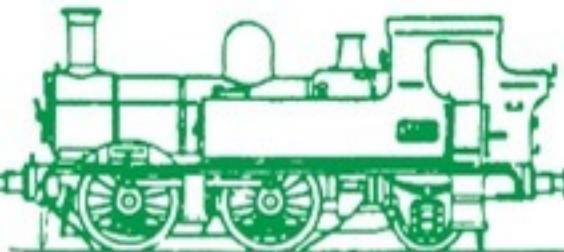
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**La Noé sur Erdre**  
French HO

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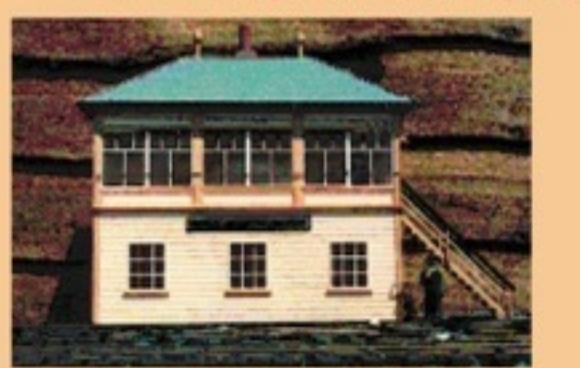
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# CONTINENTAL MODELLER

July 2018  
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## RAILWAY MODELLER

For all modelling British railway practice.  
Published on the second Thursday  
of the preceding month.



Cover

ÖBB Co-Co electric 1020.20 leaving Zeltweg at the head of a mixed goods train. The HO layout was created to evoke memories of the town that was home to the builder's maternal grandparents.

Photo: editor.

## Small and not so small layouts

We have occasionally heard the comment that many of the articles deal with relatively small layouts: there may be several good reasons for this – and indeed it may depend on your definition of 'small'.

Small layouts are very practical in terms of the space, resources, stock, and time required. As we largely reflect what is being modelled – the articles are contributed by modellers as and when their layout or project is at a suitable stage, not created by us to suit a publishing schedule – it is inevitable that most features will be concerned with the activities of what our sister magazine RAILWAY MODELLER used to describe as 'the average modeller' (before the word 'average' acquired derogatory connotations), although paradoxically the smallest layouts can sometimes generate the longest descriptions ...

The example of a modest project may encourage others to try something new, and such schemes enable prolific modellers to complete a theme and move on. On the other hand, we cannot but admire those who adopt one theme and make it their life's work.

Of course, when the opportunity arises, in the pursuit of the variety we noted last month, in treatment and presentation as well as the subject, size, and style of the layouts, we are very pleased to feature larger layouts – our Railway of the Month in this issue, *Zeltweg*, is a good size, more than most homes could easily accommodate, and certainly the result of a larger vision and long term approach. The same could be said of *Vif* despite it being constrained in final size, and the *Columbia & Western* is large as a private layout by any standard; it also exploits the third dimension, height, in a way not usually feasible in smaller spaces or with portable layouts. It also illustrates that large scales tend to need exponentially more room, while going into the garden as one way of solving the space problem usually means adopting appropriate larger scale stock, as in the case of *Thurnsfeld*.

In the interest of balance and variety, this issue also presents some small layouts – *Pottendorf*, *La Noé sur Erdre*, *Oresome Aggregates*, and *Naples Street*.

While such self-contained features can be very satisfying, layouts like *Zeltweg*, *Vif*, and *Columbia & Western* are – or will be – in two (or more) parts, partly because of the number of illustrations, partly because of the amount of interesting information to be conveyed on the background, construction, description, and operation.

But even small layouts can provide the opportunity for a surprising number of photos, if there are many details and cameo scenes and a good range of stock, perhaps, like *Vif*, representing different periods, with a change of road vehicles and even structures and accessories. So the size of the layout does not necessarily determine the length of the resulting article.

On the subject of large layouts, we were sorry to hear that Andrew Cook's magnificent Swiss HO layout *Brig* (CM December 2009) had to be dismantled – but large sections have been salvaged and it is hoped they can be re-used in a modified format in a new location.

Also, as this issue was being prepared we heard that Peter Herzl-Smith was going to move house and it may not be possible to accommodate *Zeltweg* in its present form in the new location. At least it was built in movable sections even though a house move was not envisaged when the project was started. Moral – plan to be flexible!

## Additional layouts on show at Pecorama

As an additional attraction for visitors to Pecorama on the weekend of 21st and 22nd July, there will be an exhibition of portable layouts in the Lecture Theatre/Station Gallery. All have been selected to show different aspects of the hobby from those in the permanent exhibition. See *Exhibition Diary* and the *News* section for more details.

# CONTINENTAL MODELLER

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### 454 ZELTWEG – RAILWAY OF THE MONTH

As a first foray into railway modelling, Peter Herzl-Smith has modelled in HO the station serving the Austrian town which was home to his maternal grandparents.

### 462 VIF – 2

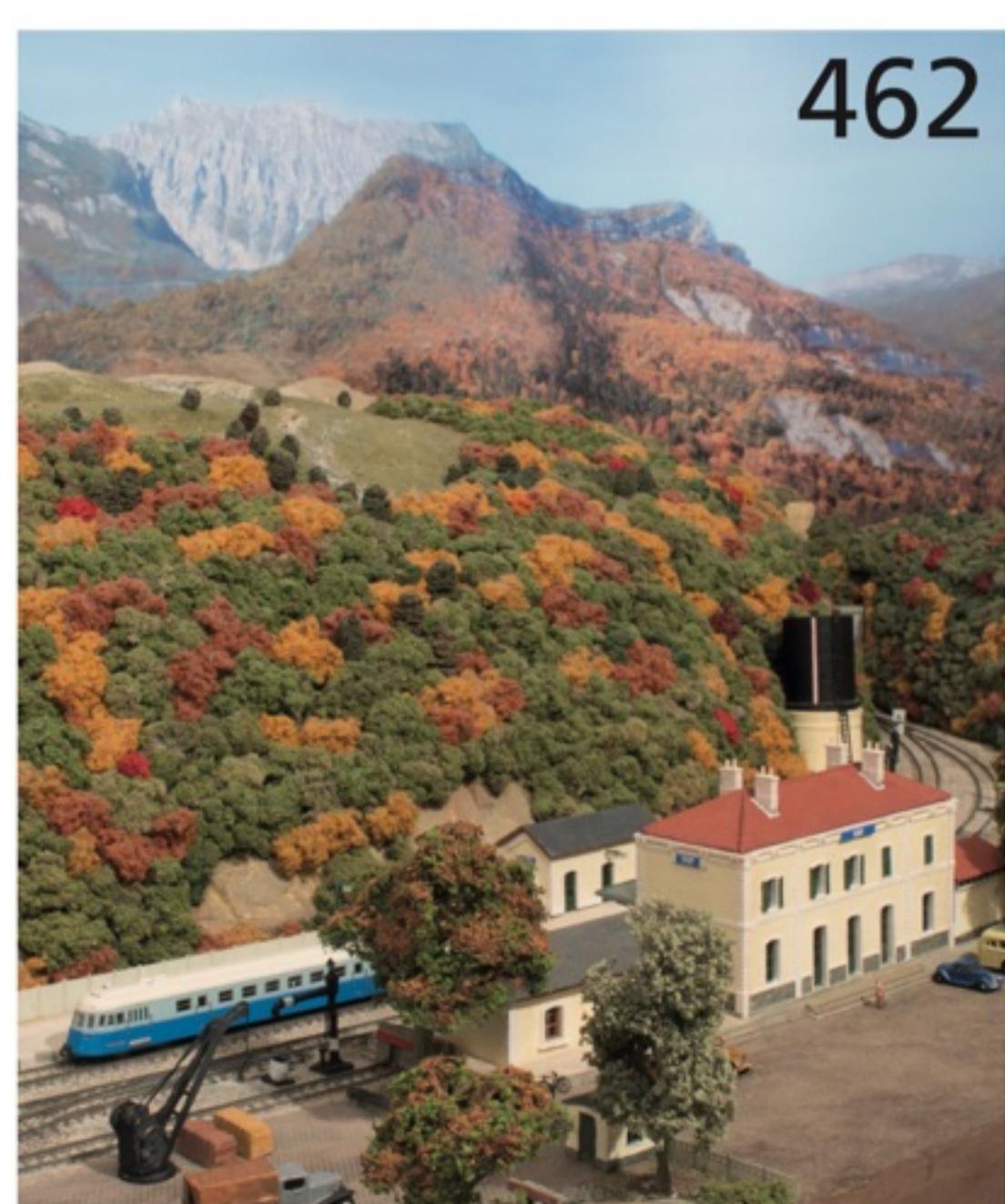
Mark Booker concludes the detailed description of his scenic N gauge layout, based closely on a real French location, commenced in our June issue.

### 468 1918 BEHIND THE LINES – A LAYOUT REVISED

A memorial layout expanded and enhanced: Roy Hirst developed a First World War OO9 layout into a more representative record of the stock involved.

### 476 POTTENDORF, 1912

High summer before the storm: Tim Hale has created an easily transportable and attractive Bavarian HO layout by the creative use of readily-available components.





## 484 ORESOME AGGREGATES

Moving from a house to a flat prompted Peter Smith to build a new compact *Feldbahn* layout in HO (1:87 scale, 6.5mm gauge) which was later extended.

## 490 LA NOÉ SUR ERDRE

Olivier Taniou travels back in time to spend a day at a halt in the French countryside, modelled in HO.

## 498 COLUMBIA & WESTERN – PLAN OF THE MONTH

Mike Dobson continues the account of the extensive O scale layout set on the Canadian Pacific in the 1950s, which was introduced in the May 2018 issue.



## 506 THURNSFELD HAUPTBAHNHOF

Large scale fun in the garden: Doug and Jenny Thompson took time to plan before eventually starting work on their garden layout.

## 514 NAPLES STREET

Rod Shaw has built an American switching layout but in an unusual scale for such subjects – TT.

## 520 LATEST REVIEWS

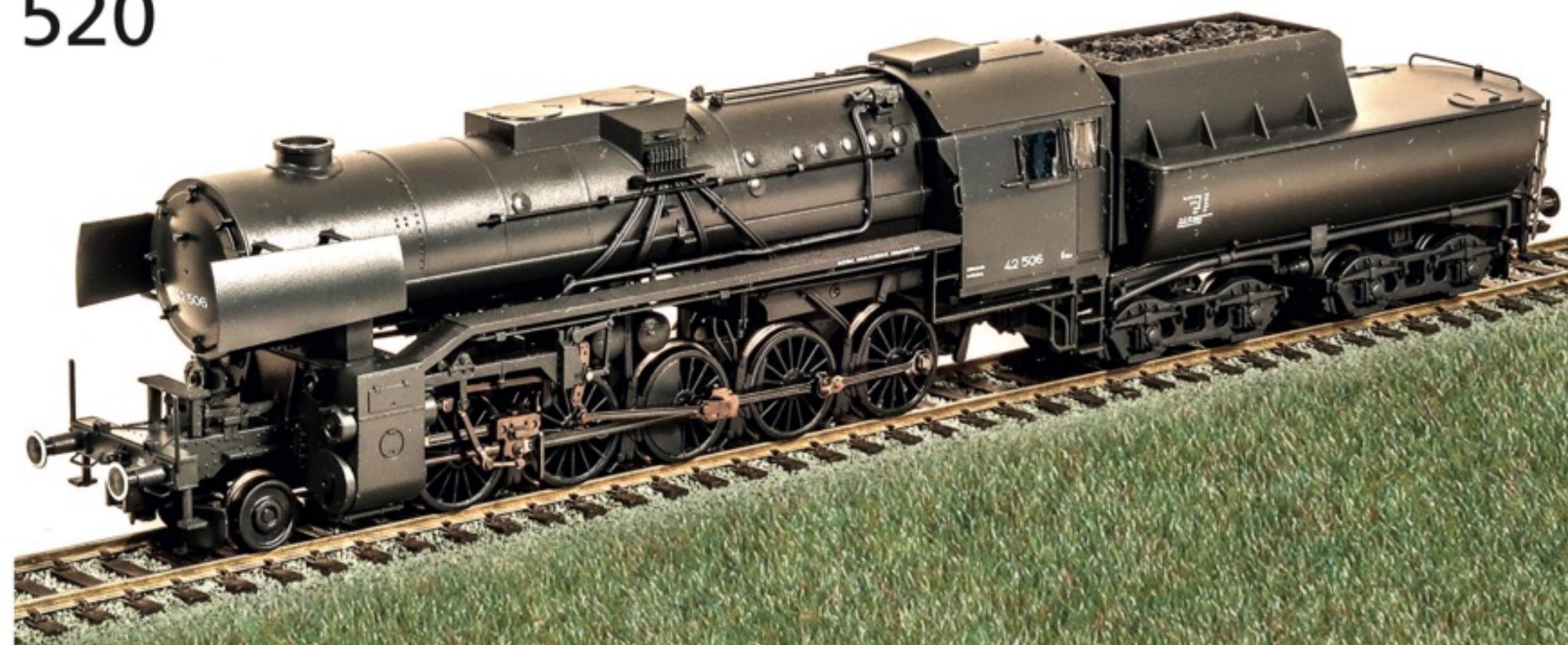
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# FLEISCHMANN

# Tradition and Passion

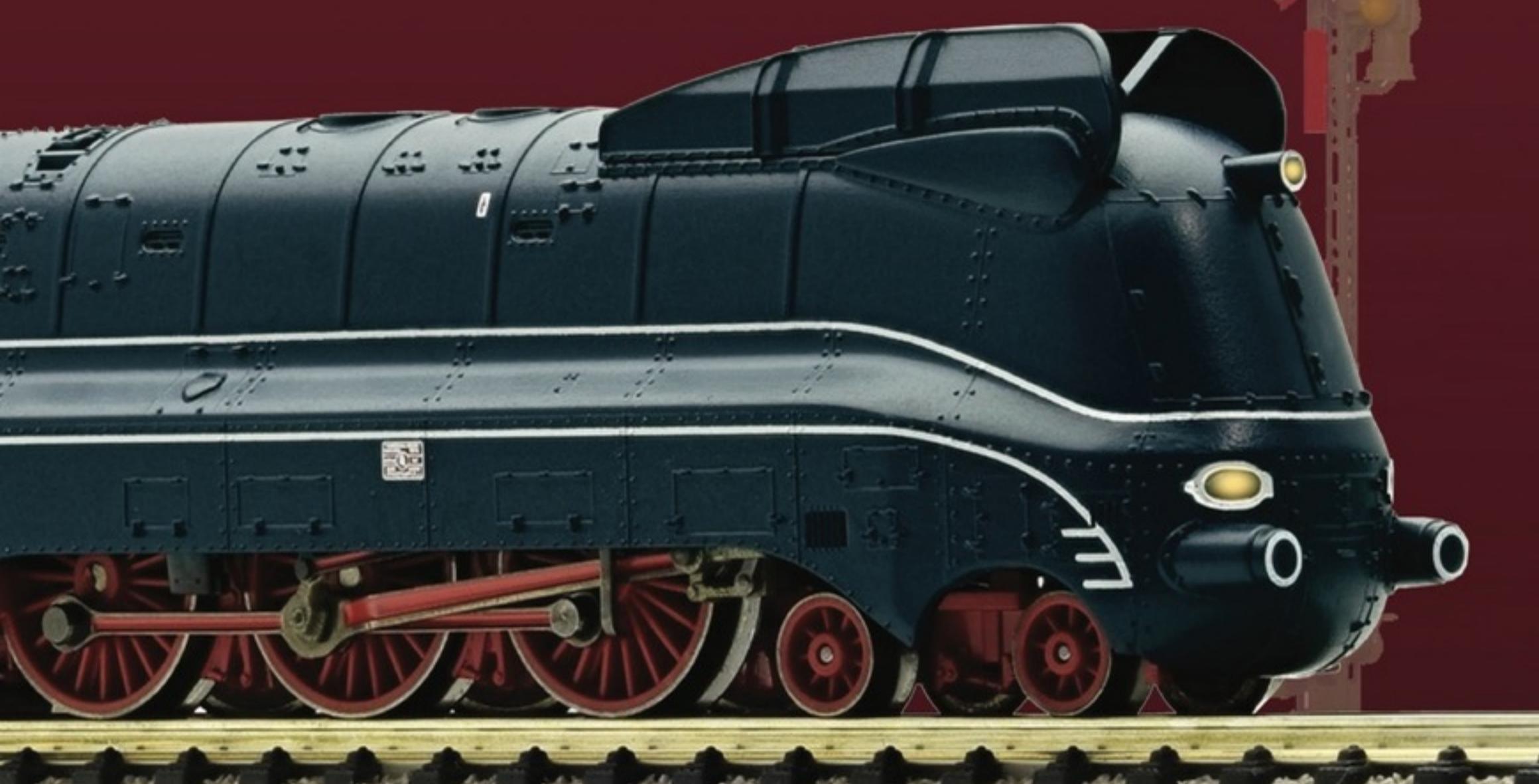
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**“Tradition and Passion” is the long-standing motto of Fleischmann, whose model train products are well known by their high performance, as well as their visual and mechanical excellence. All of their models are durable yet detailed.**

Fleischmann is a full product range, with locomotives, rolling stock, control equipment, and track available in one system, with optional innovative DCC technology and accessories such as point control and signalling.

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Marklin are one of the oldest model railway companies in the world.

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### Gauge 1 1:32 Scale -

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## Marklin Central Station 3

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# A Comprehensive Model Railway System

**PIKO**

Offering great value for money, PIKO is definitely the range to look at for beginners to German railways and existing collectors alike, with a variety of starter sets on offer.



The PIKO HO Scale (1:87) range is split into three parts; Hobby, Classic, and Expert.

The Hobby range is an inexpensive entrance into the world of model railways with high quality, robust models. The Expert range has detailed, robust DCC Ready locomotives. The Classic range mixes a high level of detail with sophisticated technology.

The HO Scale A-Track has clear, simple geometry and a realistic appearance.



The PIKO N Scale (1:160) range features a modest selection of locomotives and stock, as well as a range of building kits.

The locomotives run smoothly, with accurately printed liveries and DCC compatibility.

This range includes many passenger multiple units that run around Europe, not just in Germany. this allows interesting commuter and inter-urban layouts to be modelled if they are based around a specific locomotive.



The PIKO G Scale (1:22.5) range covers locomotives, rolling stock, a track system, and a large range of building kits. It can be used both indoors and outdoors, and is great value for money.

The locomotives feature directional lighting, and some even have sound and smoke.

Stock from both Europe and the USA are featured in the range and the track is high quality and robust, and easy to lay.



TT Scale (1:120) is gaining in popularity, and PIKO produce a growing range of locomotives and rolling stock, including the famous ICE High Speed Train.

A good 'compromise' scale, TT offers the detail of HO Scale but the space-saving convenience of N Scale.

New modellers should definitely consider this scale, as many generic scenic materials can also be used to create layouts in TT Scale.



## HO Scale

Roco produce a large range of HO Scale German Railway locomotives nad rolling stock, harnessing technologies such as DCC Sound and even locomotives with cameras!

A HO track system is also available, including the Rocoline pre-ballasted track.



## TT Scale

TT Scale is 1:120 Scale and is a good compromise if you want the detail of HO but only have the space for a smaller layout and don't want to go down to N Scale.

Roco produce a small but growing range of items in this size, so they are will worth exploring in more detail.



## Z21 Digital System

It can be programmed to change locomotive speed and points at the touch of a button, and as it is a free app, anyone can join in as long as they have a supported device!

## HOe Scale

The HOe Scale range is growing in both size and popularity, with Narrow Gauge layouts being built on their own as well as part of existing HO layouts, which are the same scale.

A starter set is the ideal place to begin, with extra track also available to extend your Narrow Gauge line!



The Z21 digital system is an innovative way of controlling your layout. Utilising any Apple or Android phone/tablet, you can control trains and accessories on your layout using the Z21 app.

# TRIX



## N and HO Scale Continental Specialists

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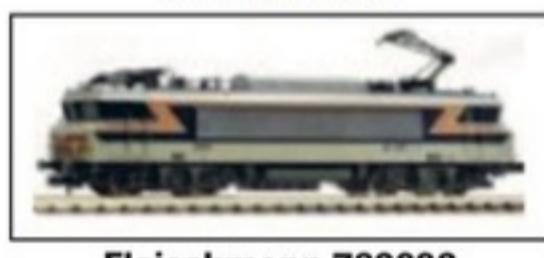
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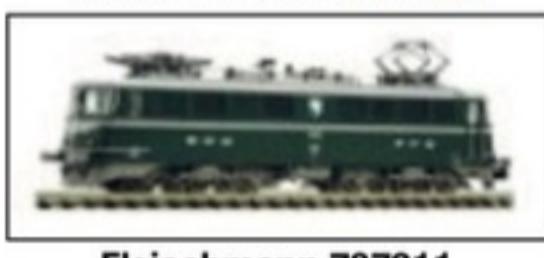
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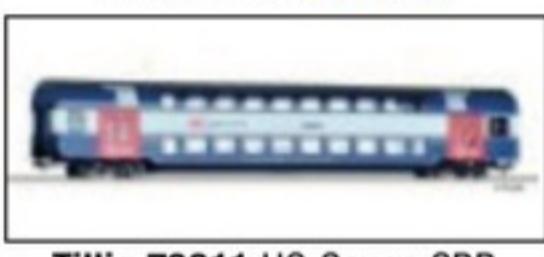
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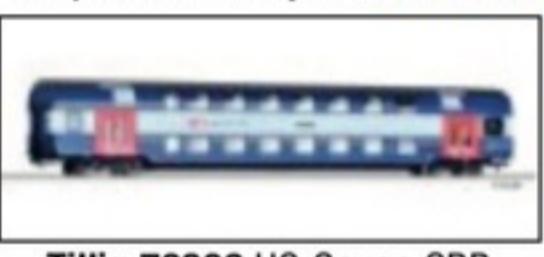
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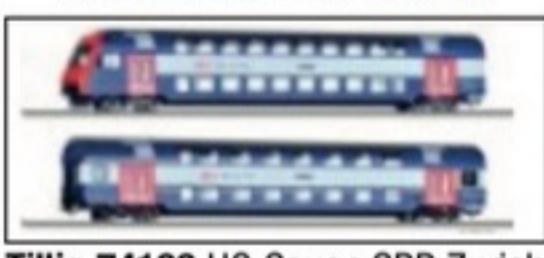
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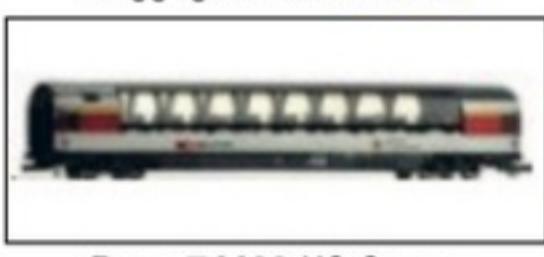
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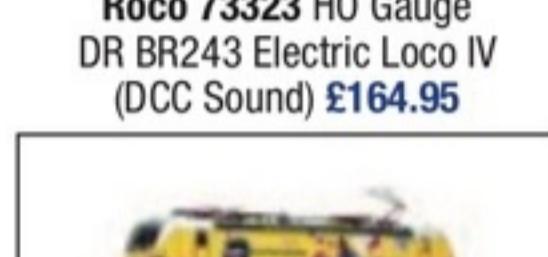
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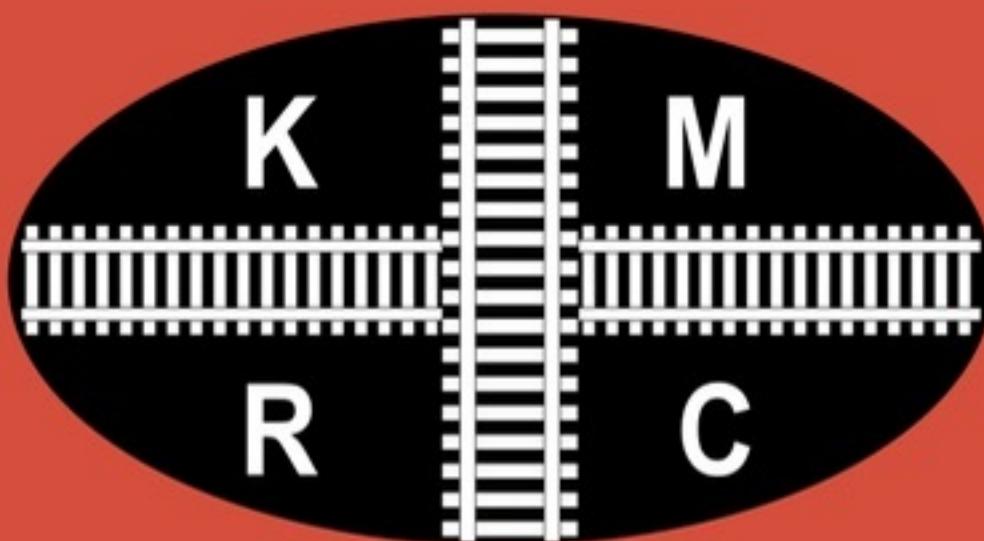
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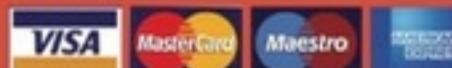
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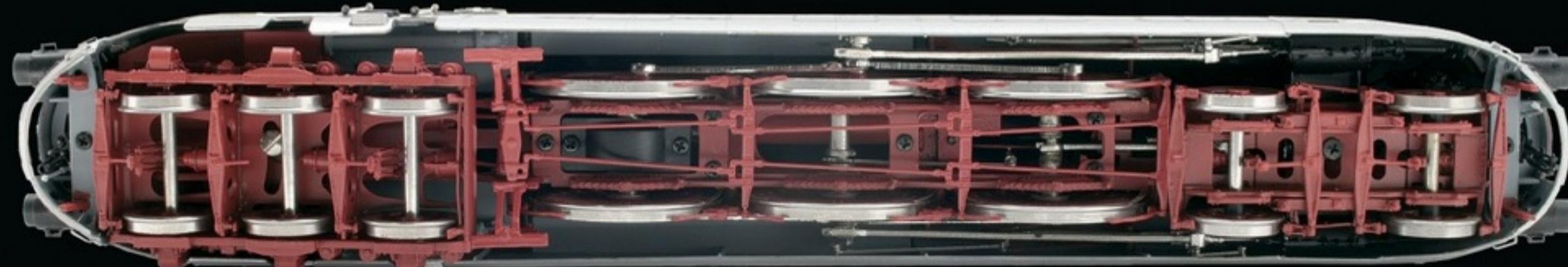
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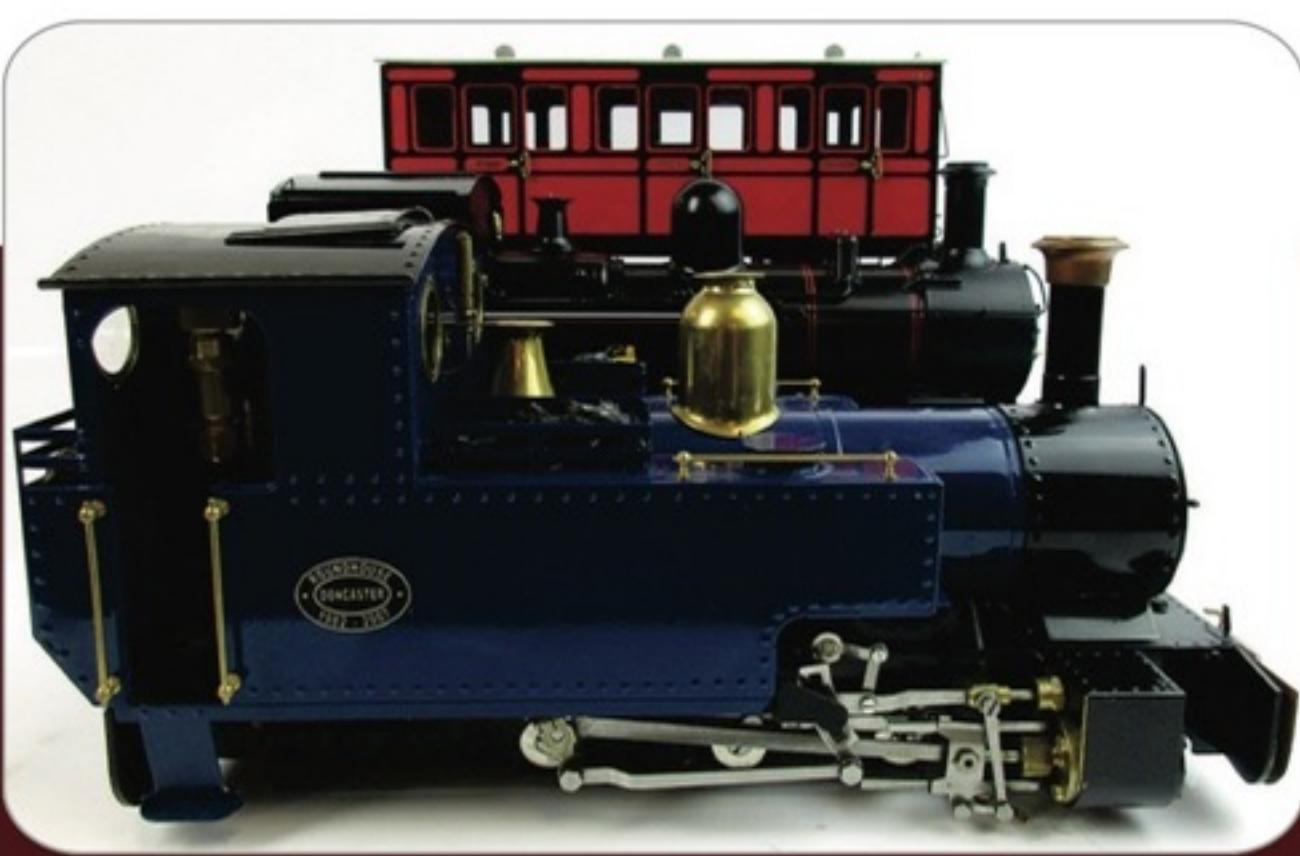
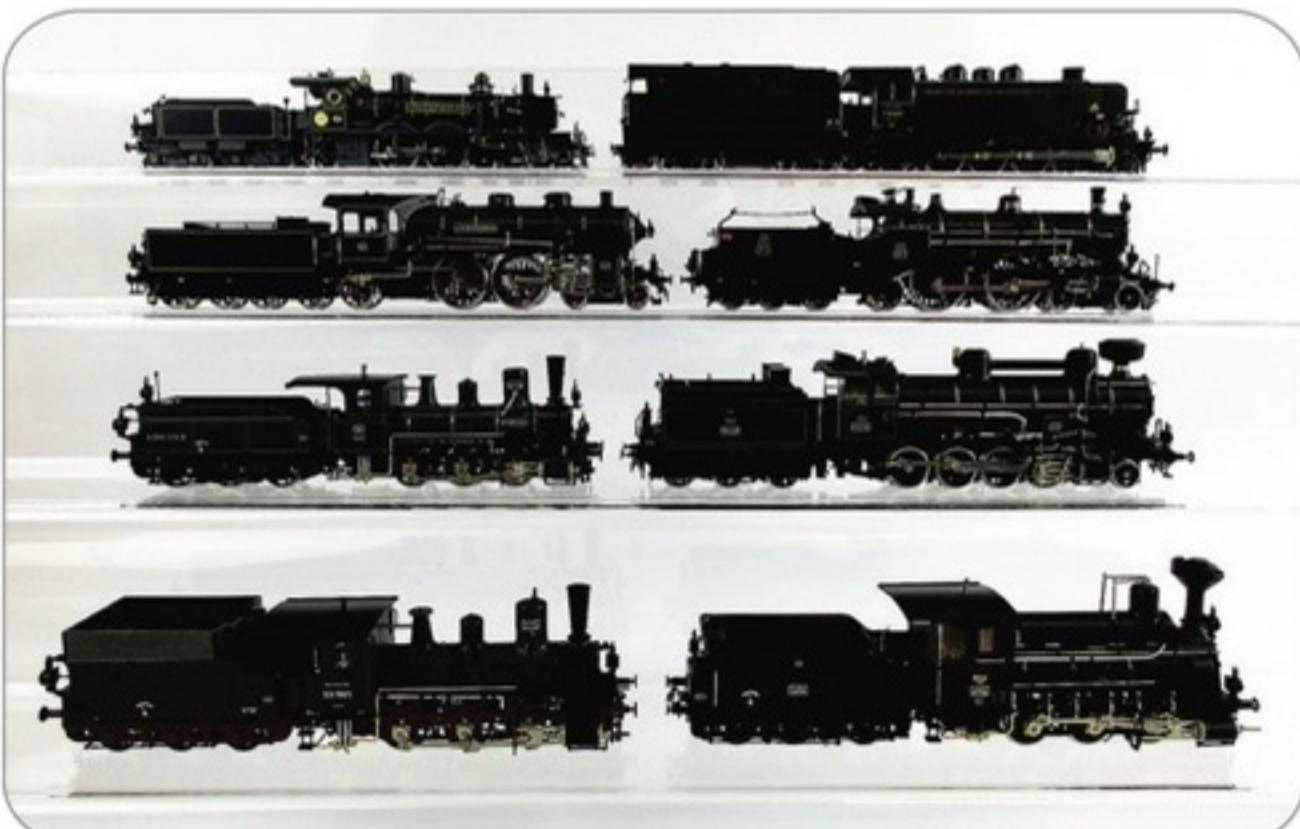
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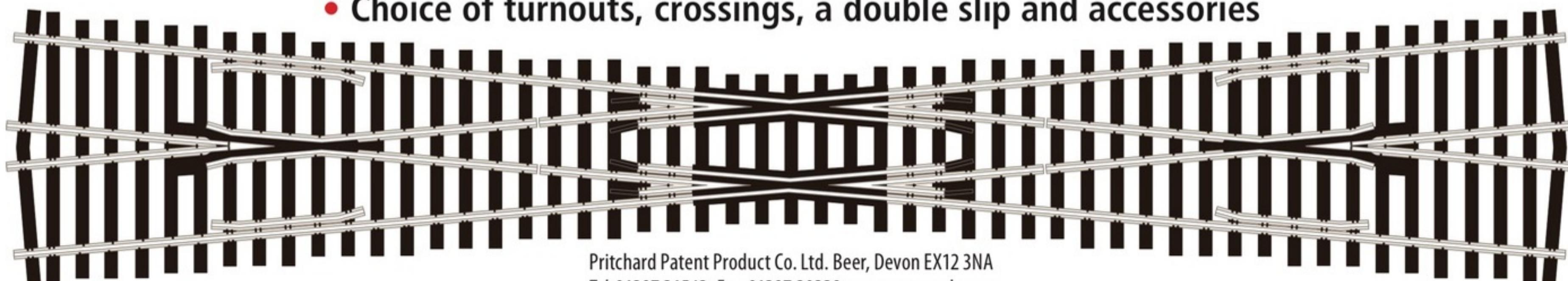
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# Zeltweg

A first foray into railway modelling – 1

**Peter Herzl-Smith** has modelled the station in Austria serving the town which was home to his grandparents.



When attending exhibitions or reading CM I am often surprised at the relative dearth of Austrian layouts. German and Swiss settings feature prominently, and for understandable reasons, but I often wonder if modellers are perhaps missing an opportunity.

For steam enthusiasts Switzerland offers relatively few prospects. For lovers of extreme alpine terrain, Germany, with a few notable exceptions, cannot compete with Switzerland, whereas Austria offers both, and more. The early electrification of many main lines, diesels on non-electrified single lines, plus the late running (into 1978) of steam on numerous lines, combine to offer a wide variety of motive power. Add to that the narrow gauge *Lokalbahn* services and preserved lines that often merge with standard gauge main routes and it seems that a modeller has everything on offer to suit a diverse range of tastes. Stunning alpine scenery is not only attractive in itself but offers many possibilities for 'hiding' the exit to the fiddle yard – as exemplified by Malcolm Rochford's superb *Wiesenbach* (CM December 2010) – so much better than a 'hole in the sky'.



Above  
Co-Co electric 1110 01 (a Roco model)  
heads an express across the river gorge.

Right  
The train on the approach to Zeltweg station.  
In the foreground is my grandparents' house.

Photographs by the editor.

My own ambition to create a proper layout, following receipt of my first Triang OO train set at the age of 9, has been simmering for forty years but a combination of a busy career and lack of space sentenced me to an adult life of visiting exhibitions, and dreaming. Then with early retirement and a new extension to our house came the opportunity for me to consider the options. Starting for the first time this late in life has forced me to move swiftly but plan carefully and I cannot speak too highly of Ian Rice's *Layout Design* (Haynes). It covers everything you need to know from a to z; and, due to his lovely writing style, is a joy to read.

Having spent my early years in a small Somerset village close to Yeovil Junction where the Southern and Western regions crossed, it would have been natural and very interesting to model that location but for many reasons I decided upon an Austrian scene. If I am typical of many modellers then I suspect the desire to create layouts is primarily driven by childhood nostalgia and so for me the setting had to be Zeltweg in Steiermark.

From a very early age I was taken regularly by my Austrian mother to her home town of Zeltweg to spend long summer holidays with my Opa and Oma (Grandpa and Grandma). The journey from the Belgian port of Oostende to St.Michael ob Leoben aboard the *Tauern Express* was easily the most exciting experience I ever had as a child or even young teenager and left an indelible mark upon me.

I would be unable to sleep for many days before the trip and certainly not once aboard when climbing onto the couchette with strict instructions to sleep, which naturally I completely ignored. I can still feel the thrill as night fell of rattling across those foreign lands (a true adventure at that time in the 1950s when I was the only pupil in my class who had been abroad) and the bubbling excitement as we pulled in to major stations such as Köln or Frankfurt in the early hours of the morning. My experience of English railway stations at night had been of solitary, ghostly places, but here I was in bustling stations with frequent announcements in foreign tongues; people in unfamiliar clothes (in that period, fashion had not yet become universally homogeneous) saying goodbye to friends or relatives, some in military uniform;





Above  
**A veteran Rh1180 0-10-0 (E) rod-coupled electric (Klein Modellbahn) with a regional train on the main line as an ore train hauled by a Rh93 2-8-2T (Liliput) tackles the Erzberg line banked by a Rh86 2-8-2T (Fleischmann).**

Below  
**A Rh2060 four-wheel diesel (Liliput) shunting hoppers at Preg quarry.**

advertisement hoardings for products unknown to me; and all the while seeing the coming and going of fascinating steam and electric locos hauling rakes of coaches that were similarly unfamiliar to my eyes, and from different countries. München was always a delight with plenty of locomotives in different liveries standing around in the numerous sidings ahead of the main terminus building. The only exception I remember was my first sighting, at München, of a V200 diesel hydraulic and thinking what a strange coincidence that it looked so similar to the Warship class on our Western Region. (Little did I know then that the British Railways Warships, built in Britain between 1958 and 1962, were

closely based on Krauss-Maffei's V200.0 technology, including the distinctive shape.)

I also remember seeing swathes of devastation around some German cities in the early 1950s, a legacy of allied bombing raids, but these were mere passing curiosities to a boy who was more interested in observing as much as possible of the intriguing continental railway system.

I learned in time to bag the middle couchette as this gave me the best position from which to view the mysterious foreign landscapes without disturbing my mother or other occupants – the top couchette being above the window and the bottom couchette well below the window; both required much body movement in order to peer out.

Our arrival in Salzburg was always a time of celebration as we began to see the glorious mountains of Austria and tumbling, turquoise alpine rivers. This was then followed, upon arrival in Bischofshofen, with a ritual purchase of delicious Frankfurters accompanied by Senf and a fresh Semmel from a cheery lady, in a pretty dirndl dress, who wheeled her heated trolley along the platform crying "Würstel mit Senf, ... Würstel mit Senf". Time also for inspection of passports from stern-faced border guards in impressive uniforms who passed through the train strictly checking each and every passenger. I seem to remember my father, on one of his infrequent journeys with us, falling foul of the authorities as he had stepped out of the carriage (without his passport) to stretch his legs and ventured onto that part of the Salzburg Bahnhof platform that was regarded as West German territory for border control purposes!



Selzthal was also a memorable stop with yet another opportunity to observe more varied locomotives amid the spectacular scenery of mountains, rivers, and forests.

When we would finally arrive at our destination around 4pm, I was always ready for bed having spent the previous twenty hours mostly without sleep and in a high level of excitement. The entire journey from Oostende in Belgium, across West Germany and down to the small station of St.Michael ob Leoben in Austria had been accomplished without having to change trains. We had sat in our compartment for the entire journey over two days while coaches were added and taken away at various stations along the way – something that is impossible in our new and 'improved' modern rail system.

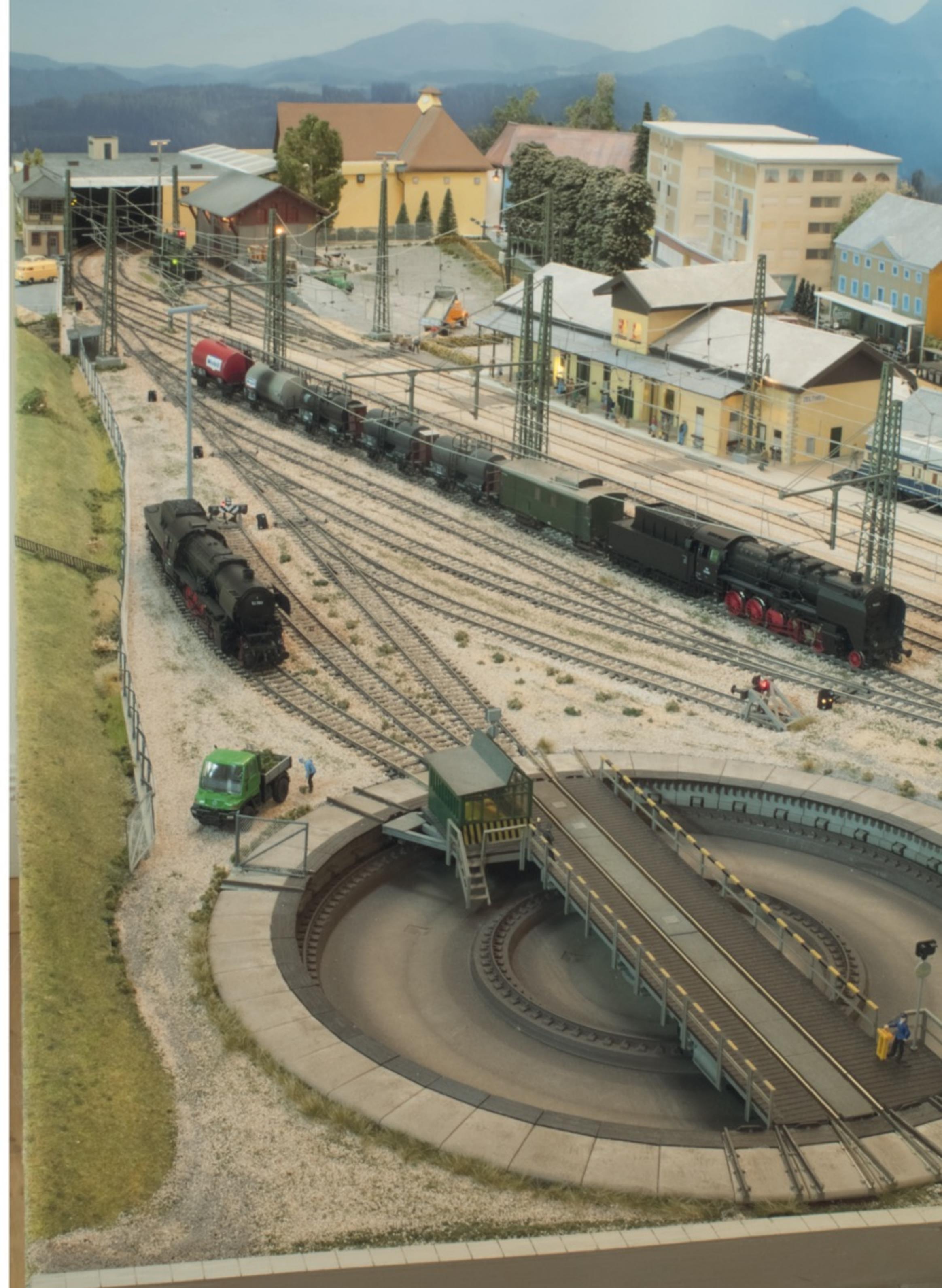
It is perhaps difficult for most people nowadays to appreciate how anyone could be so excited and nostalgic about a mere railway journey across Europe, but when I reflect back I conclude that the joy and excitement was all down to 'difference' – every aspect of these foreign railways and daily life was so different from England; features that have been largely lost in our globalised economy. Each country had its own unique character, culture, dress, music, traditions, etc., in complete contrast to anything I had known in England. In Austria that lasted well into the 1960s, for I can remember being an object of some amusement in 1967 when walking through Zeltweg in my new Carnaby Street outfit that signalled a 'foreigner' in the town. Today I go there in jeans and tee shirt and I blend in with the locals, except on Sundays when, thankfully, large numbers of Austrian men and women still put on their national dress, and traditional music can be heard at village festivals.

I spent many happy hours on Zeltweg Bahnhof just watching trains come and go or goods wagons being marshalled in the many sidings. Sadly, I did not possess a camera at that time and so pictorial records are confined to three black & white snaps taken on a Kodak Baby Brownie borrowed from my Opa.

This Murtal location offers much variety for Epoch III modelling: local and long distance trains, goods shed and marshalling yard, steam, electric, and diesel motive power, a junction of twin main line tracks merging with four separate single tracks coming from different directions, industrial movement activity from the local ÖAM (now Vöest Alpine) steel works and the two Natron (now Mondi) paper factories; and all of this with a backdrop of picturesque green mountains and two rivers, the Mur and the Pöls.

1:87 (HO) is for me the ideal size to provide the best of all worlds: to appreciate finer detail than is possible with N but without having to go to the space-limiting O. There is also an abundance of ready-to-run equipment in HO.

Some time before my layout plan was decided I started collecting Fleischmann and Roco locomotives plus rolling stock, of both DB and ÖBB outline, and so I am having to employ 'modeller's licence' to run DB trains through the station by moving Zeltweg somewhat closer to the German border. I also bring iron-ore hoppers from the Erzbergbahn and ballast wagons from the Preg quarry through Zeltweg when, in fact, their natural routes are some 60km and 20km distant.



### Layout plans

Our house extension provided an area for the layout 8.7m long by 0.7m at one end growing to 1.1m at the other. Continental modellers seem to favour continuous operation whereas British modellers often opt for end-to-end layouts. I can see the merits of both but rather than wrestling with these choices I have produced a hybrid that combines, I hope, the best features of each.

However large the available space, a modeller will always encounter restrictions to intended plans!

Taking up modelling for the first time at such a late stage in life with little experience forced me to undertake a large amount of research via books, magazines, exhibitions, and the internet, all of which was very interesting and provided me with numerous 'expert' opinions (many contrary to each other!) and enabled me to pick the best and most appropriate advice for my purposes.

One piece of received wisdom is to commence for the first time with a small, simple, uncomplicated layout in order to build experience and progress incrementally, but as time was not on my side I had to take the plunge with something more ambitious.

Above

**Although the turntable had been removed by 1971, it has been reinstated on the model. On the left, former Kriegslok 2-10-0 52 7064 waits its next assignment as 2-10-0 50 1805 is held in the yard with a train of oil tanks.**



Left

**Board A** accommodates the station, goods yard, and loco facilities. I often arrived at Zeltweg on just such an express.

Right

The main feature of board B is the river gorge. A Rh5045/6545 'Blauer Blitz' railcar and trailer set (Piko) gathers speed as it pulls away from Zeltweg.

Below

Grössing mountain looks down on Zeltweg Bahnhof and the buildings along Bahnhofstraße.

Planning commenced with 1:100 sketches which were then transferred to full-scale plans on rolls of wall lining paper with the following aids:

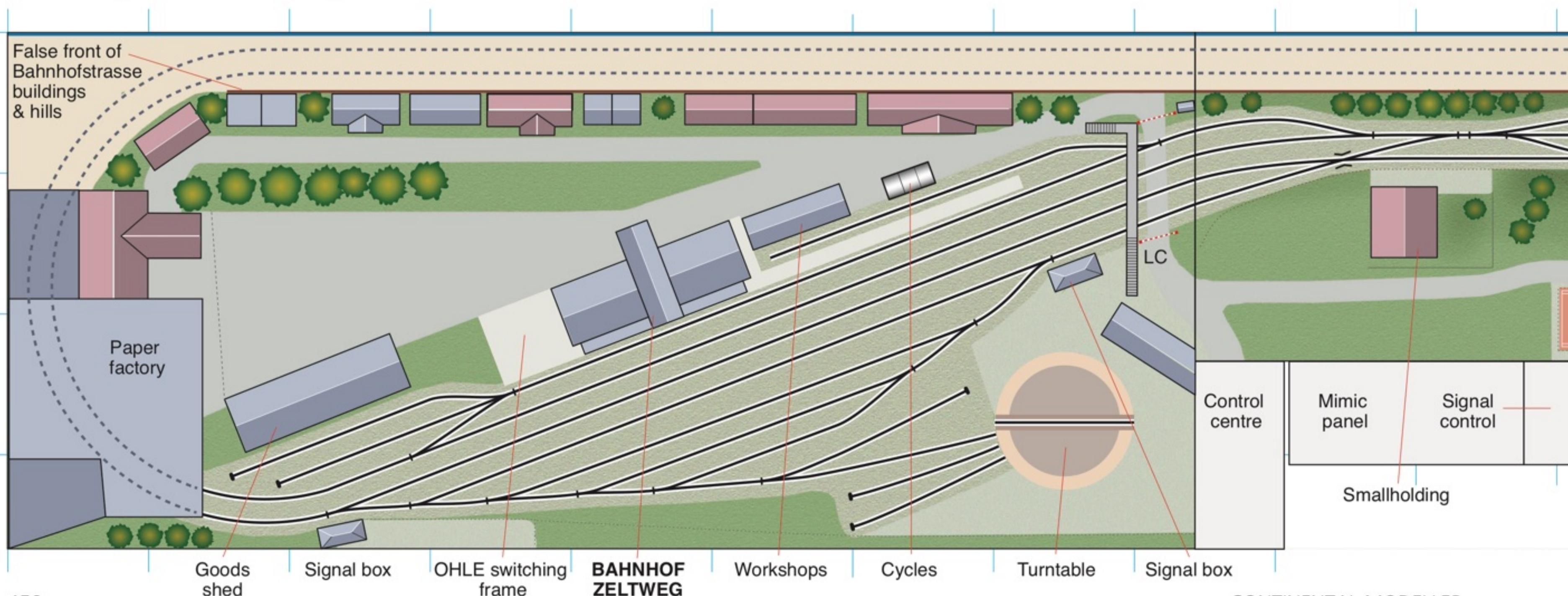
- Peco paper point templates (stuck to cardboard for durability);
- a selection of templates made from softwood strips to easily and quickly transcribe track outlines and track spacing;
- a track radius 'compass' with a sharp nail at one end and holes at intervals to take a pencil lead for marking R1, R2, and R3 curves.

I finally settled on a continuous twin track layout of an irregular dumbbell configuration occupying three boards as follows:

Board A – 2,520mm x 1,100mm: main station, goods and marshalling yard.

Board B – 2,440mm x 690mm: river gorge and open country beyond the station.

Board C – 1,700mm x 1,100mm (reducing to 690mm at the join with 'B'): open country and quarry, with a 1 in 30 gradient single-track line to 'Erzberg'.



Room width restrictions ruled out a fiddle yard or storage sidings and so I opted for a 'cassette traverser' with trains made up in individual cassettes stored on a wall and introduced to, and removed from, the tracks via the traverser board at the end.

I have tried to accurately model Zeltweg Bahnhof as far as space will allow but as I am working with space restrictions and from both memory and old photographs I will not pretend that it is 100% correct. However, as far as possible I have adhered to the prototype and as an example, the backscene to Board A is the actual landscape behind the station with the distinctive flat-topped Grössing mountain correctly positioned. With the other two boards I have taken great liberties in order to incorporate all that I wish from the surrounding areas so they are most definitely inspired scenes rather than exact replicas.

Board B represents countryside to the west of Zeltweg Bahnhof – the down line to Klagenfurt – except that room orientation and spatial restrictions have forced me to reverse it to that of Leoben and Erzberg in the east.

Near St. Michael ob Leoben the line runs along an escarpment close to the river Mur and whilst I have tried to represent this as best I can, I must admit there are elements of the Gesäuse gorge between Hieflau and Admont which I love and therefore have influenced certain sections.

### Baseboard design and construction

There are many approaches to baseboard construction but I chose an open frame and designed my own hybrid frame inspired by Barry Norman's plywood beam from his book *Landscape Modelling*.

To create the beams, I cut a 2,440mm x 1,220mm sheet of 9mm plywood into 100mm wide strips. I cut some 100mm x 24mm softwood planks into numerous 100mm square blocks.

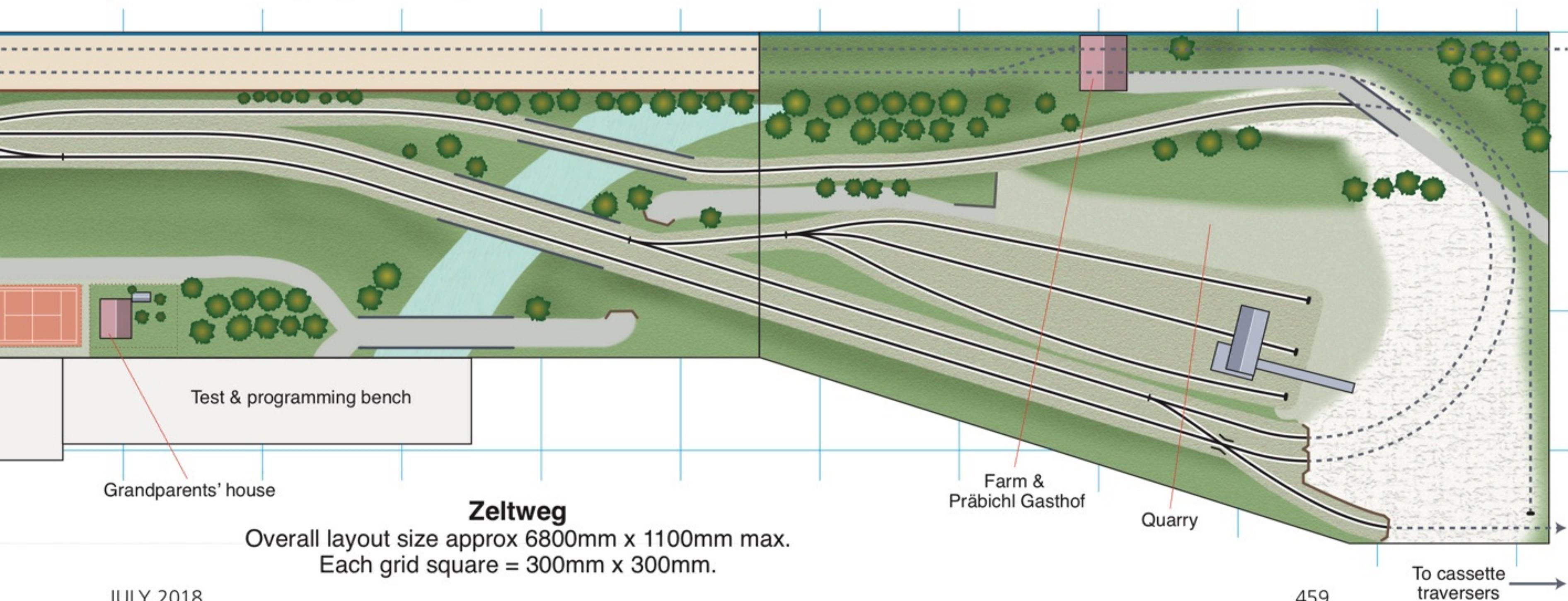
The blocks were then sandwiched between the ply strips at 100 – 150mm intervals by pinning and gluing to form a girder beam.

These beams were then glued and screwed to form rectangular frames with heavy duty internal corner brackets for extra strength. (Lack of previous experience means that, for peace of mind, everything is over-engineered!) These then



formed the open frames onto which cross-spans were fixed at intervals to carry the raised track beds, also made from 9mm ply with 43mm x 14mm timber edging to prevent warping.

Below  
Rh5081 railbus/6581 trailer (Trix) heads up the branch as Bo-Bo 1245 520 (Roco) passes the quarry.





Left

2-10-0 52 7070, fitted with a Giesl ejector and tub tender with cabin (Roco), trundles through the station yard with a mixed freight.

Right

*Kriegslok* 2-10-0 52 7070 passing 4-6-0 38 4114 (a former Südbahn 109) (Roco) on a semi-fast service. The hills and rocky outcrops in front of the backscene cover the return loops.

As the station area is flat without the contours of the open country sections, I simply screwed a sheet of ply to the frame of Board A with some added cross bracing where appropriate. I think the beam approach has many merits, amongst which is the immense strength it provides for a light weight and of course the blank canvas it provides for modelling alpine topography.

The three boards are mounted on individual bespoke trolley bases complete with rubber castors for ease of movement

since I intend this to be a permanent but movable layout. They are screwed from the underside to the trolley bases and can be removed very quickly for wiring, etc., as I do not fancy working upside down, especially when soldering! A continuous shelf was installed in each trolley base to carry plastic storage boxes of rolling stock, etc., hidden from view by pleated curtains secured on Velcro tracks. Space was left beneath the trolleys to allow heat from the floor heating system to escape.

Below

A Rh86 2-8-2T (Fleischmann) assists a Rh93 2-8-2T (Liliput) with an ore train. The Erzberg open cast mine can be seen in the distance on the backscene (just above the green Schenker lorry and trailer on the Präbichl pass nearing the farm/Gasthof).





Unlike many modellers, I actually enjoy woodwork (much more than soldering and electrical work) and so this part of the project gave me great pleasure and satisfaction.

All woodwork, especially the ply, was treated with two coats of matt varnish to inhibit moisture absorption that could lead to distortion.

Other than the plywood sheets, all timber was either salvaged from the recent building extension or had been saved from previous household projects; hoarding like this shows how much otherwise ends up in a skip! Certainly, when modelling it is surprising how many uses can be found for 'waste' materials that would normally be discarded.

Each board is lined up to the next by brass cabinet-makers' dowels, and secured using engineering bolts and 'wing nuts' made from small blocks of wood with an inset nut fixed with Araldite.

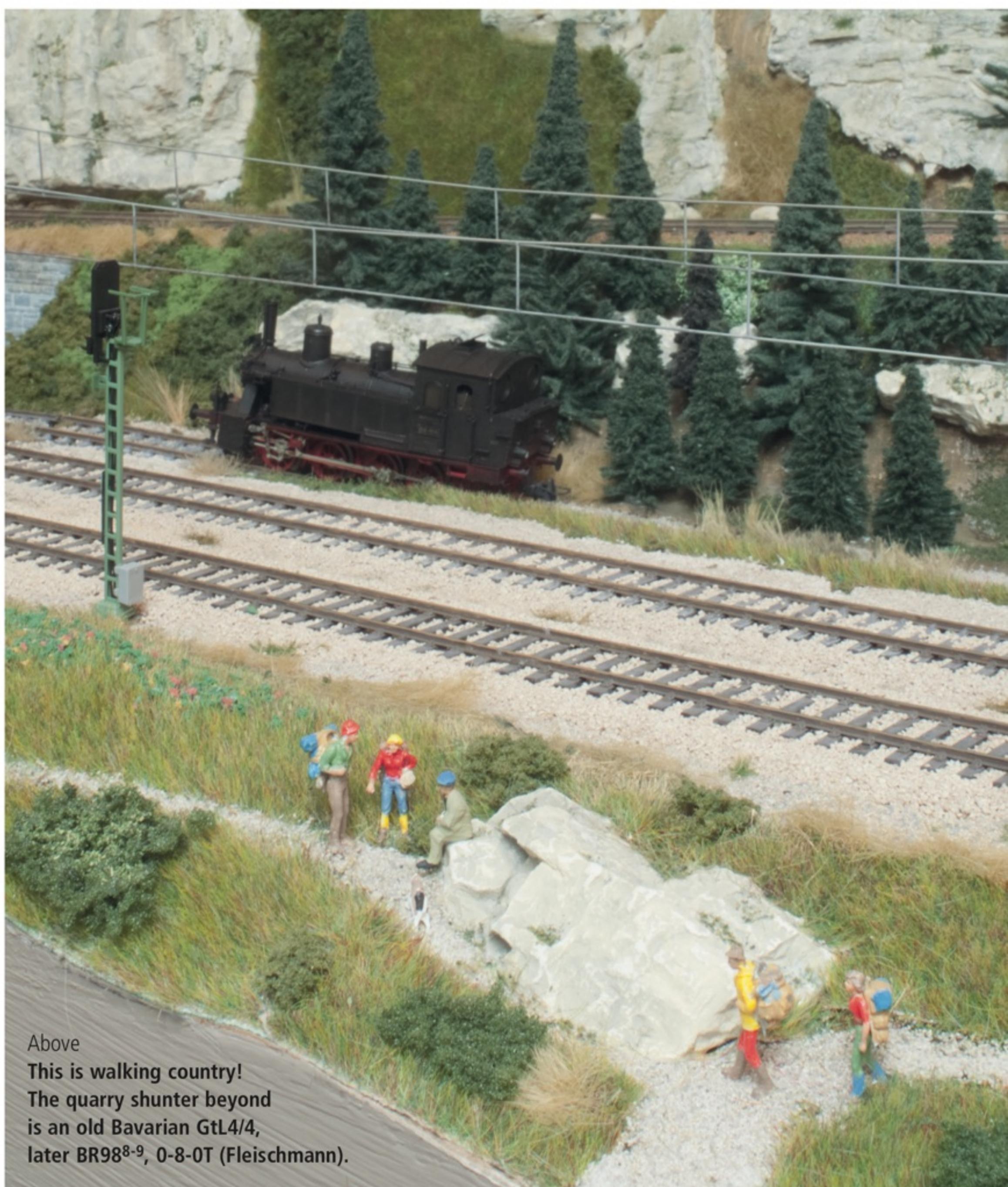
The wooden floor of the extension was laid on a German product called Elastilon, a 3mm thick sound-absorbing foam sheet with a high-tack adhesive on one side. It occurred to me that this could serve as a substitute for the usual cork or foam track underlay. It was placed adhesive side down on the ply trackbeds and EvoStik was used to secure the Peco code 75 track to the upper (non-adhesive) surface.

### Backscene

A backscene 7.7 metres long was created on strong vinyl sheet from holiday photographs of the mountains behind Zeltweg. Fortunately my nephew works for a graphic design firm and was able to produce this for me as I do not possess the necessary computer skills or product knowledge to make such high quality material. The vinyl is extremely strong and tear-proof. I fixed it to the wall using 3M double-sided tape.

In front of the backscene, towards the rear of each board, there is a background of low relief buildings, hills, rocky outcrops and trees which hide the return tracks to the traverser.

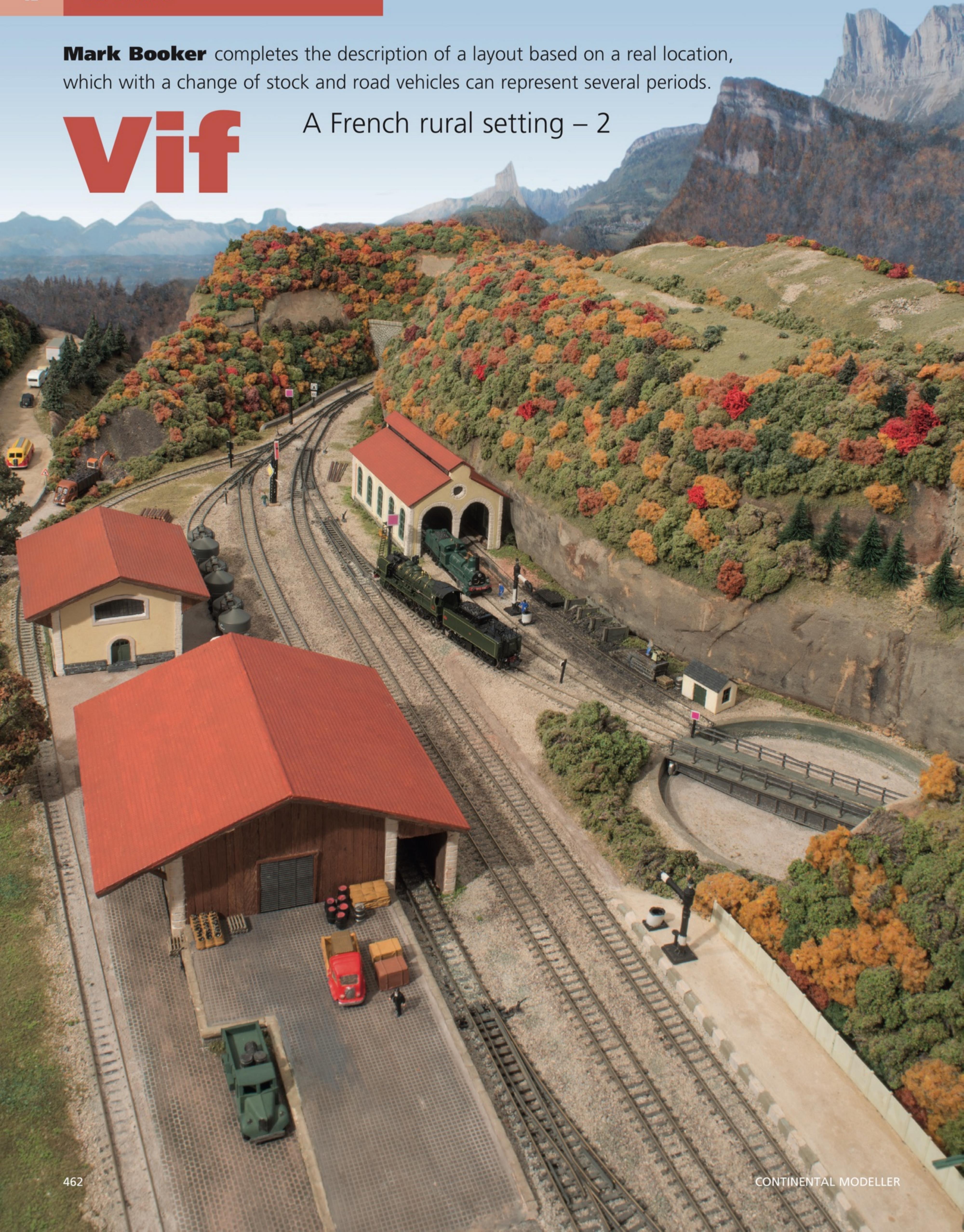
*... to be continued.*



**Mark Booker** completes the description of a layout based on a real location, which with a change of stock and road vehicles can represent several periods.

# vif

## A French rural setting – 2





Above  
**A 141P makes its way through Vif with an express freight, around 1960. Some twin silo cement wagons are stabled in the yard.**

Left  
**Vif station in the late 1930s, with a PLM Pacific backing on to the turntable and a veteran unrebuilt 'Bourbonnais' 030 on shed. In the left distance, the Chartreuse mountains of Neron and Dent de Crolle; in the right centre, Les Deux Soeurs in the Vercors mountains.**

### Buildings and lineside equipment

Before starting on buildings and structures, I read widely. I found two articles particularly helpful: *Scratchbuilding buildings* by Michael Watts in CM February 2005 and *Scale drawings: Gîtes de France* by Ian Jowett in CM November 2003. Michael is an advocate of plasticard construction and produces impressive results. Ian favours card and Artex on the grounds that plastic is "an awful lot more work than using traditional card construction." I tended to go with Ian's preference, for the additional reason that inaccuracies can be easily corrected or concealed with card and Artex construction, therefore better accommodating my level of skill! For the render-covered and colour-whitewashed prototypes at Vif, Artex was also the more appropriate covering material and base for scribing stonework. Some plastic was also used, such as Vollmer roofing sheets and very thin plastic brick sheeting, and also some platform bases from Ratio cut and painted to represent dressed stone elements at the base of some buildings.

All buildings are set into the landscape, a few smaller ones being permanent and the large ones removable for storage.

The prototype had an unusual number of buildings and structures for a rural station. I thought the layout was approaching completion once I had completed the track laying, the landscape, and foliage. I did not realise how long it would take to make up these structures, of which only one major one is a straightforward purchased kit. This is the Sud Modelisme five door *Bâtiment Voyageur* main station building (unfortunately no longer available). This is moulded in resin except for a plaster roof and a large number of brass detail parts, such as doors and windows. Although the detailing is excellent, it fitted together atrociously because of

distortion and lack of alignment of different parts and it was not possible completely to overcome this, but still it looks the part and I was very lucky to buy it years before it was needed. Scratchbuilding it would have been beyond me.

Also from Sud Modelisme is the PLM water tower (also no longer available). It had an exposed regular stone construction lower half as found on some parts of the PLM network, but on the *Ligne des Alpes* the water towers were rendered, almost certainly over rubble-stone, so I had to render the stonework, but otherwise it is a standard kit. The water within is by Woodland Scenics. This was fine for flat areas, but would not run downhill successfully before congealing, so I abandoned putting a sliver of running water into the canyon.

The main goods shed is made up of two modified components from another Sud Modelisme plaster kit, the rest scratchbuilt from balsa (for the interior base) and card. The main façade with its large oriel window was merely extended at the bottom in card and scribed Artex to be able to fit into the landscape base. The loading bay façade for road vehicles was extended in length, as the original piece was far too short, and this was also extended at the base. Real wooden laths were then cut to cover a third façade and new stone quoins had to be made up out of Artex as the kit was designed to be made up the wrong way round for the layout. A fourth façade, on the rail loading bay, was entirely scratchbuilt, including the stone quoins at each end. Finally a roof was cut out of Vollmer sheet to fit the longer dimensions and the framing under the roof made up of real wood laths. It might have taken less time to copy all the façades to card and make up from scratch using only the brass parts for the doors and windows, etc., as I have with some other structures.



The smaller goods shed uses the left-over original dimension side from the Sud Modelisme *halle* kit, the remainder being all scratchbuilt in card and Artex and real wood laths with a Vollmer roof. I had photocopied the brass components from the Sud Modelisme kits onto acetate and this provided new components such as windows.

The open goods loading platform adjoining the large goods shed was made from balsa, covered with plasticard sheet for the top surface and the cut stone lower structure, the coping stones being individually made from thick paper.

I could find no photograph of the front or side of the engine shed, but an early photograph of the engine shed at Digne and also a more recent one in a 1985 *RMF* magazine gave an indication of how it probably looked. The shed has end walls made from card while the sides were taken from a secondhand German outline loco shed kit, the sides being reversed, re-glazed, and rendered. A Kibri kit provided the profile for the rendered card entrance doorways and oriel window above. The office section at the rear came from an unused part of the Sud Modelisme goods shed kit. The clerestory vertical panels of the secondhand kit were cannibalised to form part of the roof structures, and Vollmer sheet again provided the roof tiling.

I had read that scratchbuilt curved platforms are to be avoided at all costs as they are very difficult to achieve and this is certainly good advice if you have that possibility. Unfortunately, I had to compress slightly the distance between the tunnel mouth outside the station, which is on quite a curve on the prototype, and the platform end, and

this necessitated extending that curve into the platforms themselves. I decided to fit the platforms to the trackwork as the platform profiles could then be adjusted until they looked right, rather than to risk disturbing the alignment of trackwork. The process also involved checking that my longest vehicles, the 27m long Corail coaches and X2400 and Panoramique *autorails*, as well as the longest steam locos, did not foul the platform edges at any point. The platforms were made (for lightness) of a layer of architect board and a top of balsa. The platform edges were then covered with individually painted 'stones' made from thick paper. Final ballasting of the track edges took place after the stones were in place. The platforms were then surfaced with sieved extra fine sand found at Eridge Rocks near Tunbridge Wells.

The tunnel mouths on the two prototype tunnels have a very specific egg-like profile which are quite unlike anything found in Britain or Germany, so nothing was available off the shelf in N scale and I found no drawings either, but I had a number of my own and other photographs of tunnels to work from. So I started with a Peco single track tunnel mouth for gauging dimensions, drew the outline, and started modifying this through a number of variants until I was satisfied by eye that I had achieved the correct profile. This was then transferred to card and a tunnel mouth cut out and covered with Artex and then scribed to produce the main arch, the coping stones, and all the interlocking rough cut 'honeycomb' stonework used to give strength to these structures on French railways. Having completed both tunnel mouths, I later found a very old French article that detailed



tunnel types and illustrated one in N scale. It would have saved me a lot of work, but I was rather pleased to find I had actually arrived at pretty much the right profile and dimensions by eye alone.

The main roadway was built up over a proprietary polystyrene gradient profile which was then covered with cork sheet and then a first layer of ground-up grey ballast material followed by fine sand from Eridge Rocks. The grey shows through in places to give the appearance of the heavily patched country roads of the 1950s and 1960s in the area.

I spent ages trying to find suitable flashing for chimneys on the station building, most materials being too thick to adopt the profile of individual roof tiles or too shiny, or both. Boiling kitchen foil was quite effective in giving a matt, worn appearance but a certain Lindt chocolate bar foil wrapping had exactly the right flexible thickness and a satin finish one side. Cutting 2mm strips was rather fiddly but it was worth persevering with a sharp blade. Some Tesco dark chocolate wrapping foil currently has similar satin finish.

The water cranes were adapted from a Faller kit (ref.N2139). The cranes had a very similar profile to PLM ones and the oil lamps could be cut off the arm and placed on the top of the main stem as in PLM practice.

The distinctive PLM white painted water troughs next to the water cranes were cut from minute rubber stoppers kept for years 'in case they might come in handy', transformed from round to oval profile by insertion of a 4mm length of balsa. Apparently useless bits and pieces often found an unexpected use years later on this project!

P&D Marsh provided the station lamps and lattice work posts adapted for the two principal signals. Scratchbuilding PLM-era signals in N scale was immensely fiddly as I wanted the semaphores, the large yellow distance lozenge, and square purple siding signals to be operational by hand at least. The signal faces were obtained from photocopies reduced to N scale taken from magnificently illustrated reference books on signalling published by Presses et Éditions Ferroviaires. Small purple signals were added to each end of the turntable, an interesting detail I had found in a photograph, but they are fixed. Point levers came from French

Above  
**Vif in the early 1960s, with a 141P on an express freight.**

Below  
**A busy time in the goods yard, with CNC twin silo cement wagons, STEF refrigerated stock, a Cinzano tanker, and an articulated lorry (UNIC) bringing timber.**



artisan Haxo. They are almost invisible, but do show up in photographs! Other lineside signs were made up using lengths of rail with signs photocopied from a variety of sources and then mounted onto plasticard.

### Rolling stock and road vehicles

The first thing to be said about N gauge French outline motive power and rolling stock is that its availability is mostly ephemeral or it can be very expensive (e.g. brass models from Fulgurex or Lemaco/Lematec), or both! I strongly endorse Gordon Wiseman's advice: "If you see it, buy it" because it will most likely have been discontinued when you next look. The same applies to buildings kits. Gordon's articles *Modelling French railways in N* in CM (September and October 2002, March and May 2003, and January and September 2004) gave a comprehensive guide to what had been produced in the past. A number of new models have come out in the last few years from Arnold, Minitrix, Mabar, and REE/Mikadotrain. Although the situation has markedly improved, there is never a great choice of new stock at any one time and the secondhand market seems to have dried up. I bought a large range of French passenger and goods vehicles secondhand at exhibitions in England and in model shops in Paris, prices in the latter usually being at or above the prices when new, but then economists tell us that scarcity dictates the market price! It is easy to check that these are complete and in running order.

With locomotives and other motive power, I have found that it is worth buying new when you can. It is very rare to find secondhand French locomotives in Britain but those I acquired have operated reasonably well. However, I would not recommend buying a secondhand locomotive from a French commercial supplier.

There are some very skilled French artisan enthusiasts such as Gil Fafarnoux and Philip Fabre producing small-run body kits in brass or resin to fit onto proprietary or scratchbuilt chassis.

To access these hidden gems it is

worth joining AFAN, the French N gauge association or 'Le Forum du N' on the internet, but a high level of skill is required to assemble and paint such small models.

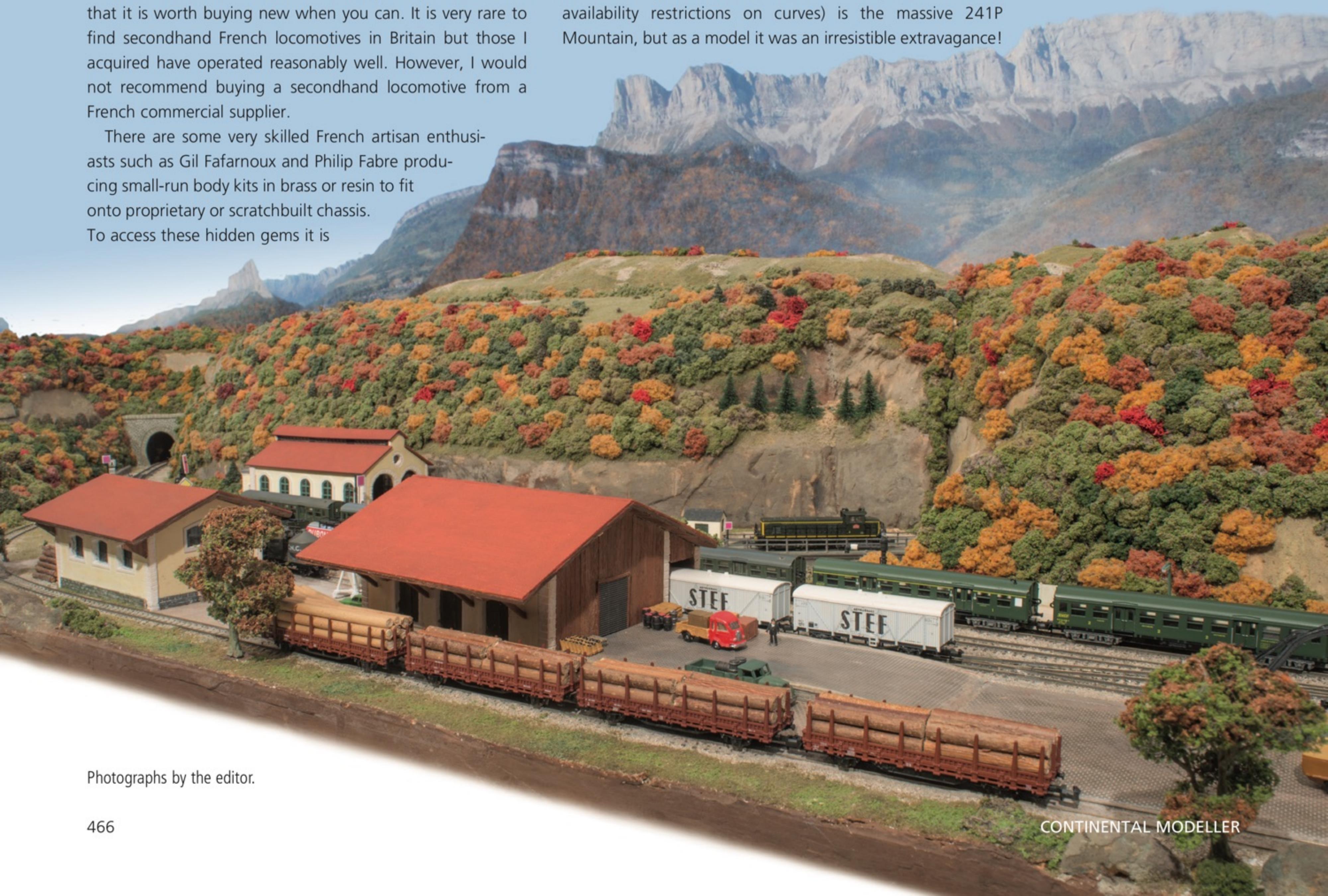
For pre-1970s road vehicles I am mainly indebted to Peter and Joy Brumwell for their resin-moulded cars and commercial vehicles, other vehicles being repainted examples from SAI and Wiking.

From 1935 onwards most passenger traffic on the line was handled by PLM (and after 1938 SNCF) diesel *autorails*, the pre-war Decauville 52000 class being built specially to handle the line's gruelling gradients and lasting until 1973. There were also X2400s for a while, succeeded by X2800s (both unmodernised in red and cream and later modernised in blue and grey), followed by X2700 RGP (unmodernised and later modernised units) which saw out the millennium. The beautiful X4200 Panoramiques were principally designed for tourist traffic on the *Ligne des Alpes* and *Ligne des Cévennes* and I will not forget my disappointment on boarding one of these with a second class ticket as an impoverished student in 1975 only to find that the panoramic dome was reserved for first class passengers only!

Until the latest generation of 'globalised' motive power, virtually every SNCF diesel locomotive and *autorail* was stress-tested as a prototype on the *Ligne des Alpes*, giving further licence to operate almost anything you like on the layout. Ironically in view of the difficulties it gave me with my track laying, the only one of my locomotives that would never have appeared on or worked the line (because of route availability restrictions on curves) is the massive 241P Mountain, but as a model it was an irresistible extravagance!

Below

**Timber traffic awaiting collection and STEF insulated vans waiting unloading in the goods yard. A stopping service to Briançon is made up of Romilly stock.**



Photographs by the editor.

There were two daily expresses from Grenoble to Marseille and these remained steam-hauled until the 1960s when the 67000 diesels took over, occasionally joined in the 1970s by 72000s. 67001 came to the line as a prototype and these attractive locos were the mainstay of the line for over fifty years and the last 67400s still see occasional duty on it today at peak times.

The *Ligne des Alpes* is a rural single track railway but built largely to main line standards. It frequently used to operate as a diversionary route for the Lyon-Marseille main line as well as handling twelve-coach winter sports sleeper trains from Paris to Briançon until quite recently. So the *Train Bleu* and its CIWL sleepers or the day time *Côte d'Azur Pullman Express* will have passed through Vif on occasion and are thus able to do so on my layout!

Gradients limited freight trains to about 250 tonnes in steam days and not massively more than this without double heading with diesels, so freight compositions would not generally consist of more than ten to fifteen four-wheel wagons. In fostering the illusion of a scale installation, I have tended to stick to four-wheel wagons and mainly 23m or shorter coaches as you get more of these in a given length than bogie wagons and UIC coaches.

The modelled installations favour operations set approximately between 1958 to 1978, so the hinged-door Corail coaches introduced in 1975 can be used to good effect as well as any rolling stock dating back to the 1920s. But a scene set in the 1930s with PLM *autorails* in blue and light grey, an ageing 19th century PLM 0-6-0 tender engine, and a small-boot Citroën *Traction Avant* in the station yard requires almost no changes.

### What next?

There used to be an extensive cement transfer installation for the Cimenterie Vicat on the second siding in from the foreground on the layout. The *cimenterie* also occupied the smaller of the two goods sheds. I have been assembling material from which to attempt to build a scaled down version of these installations capable of discharging two wagons at a time. Until the model is built (at least in mock-up), I will not know whether it will fit convincingly into the layout without overpowering the rest of the (scaled down) goods installations. I do not think it can, though one option would be to alternate the smaller goods shed and the *cimenterie* as alternative modules, the goods shed already being detachable. This would complete the exceptionally wide range of structures which once graced this rural railway station.

There would also be the possibility of fairly easily transforming the fiddle yard on the reverse side of the mountains into Clelles-Mens station further up the line from Vif, which conveniently also has a tunnel at one end. This would need an additional backscene back to back with the existing one.

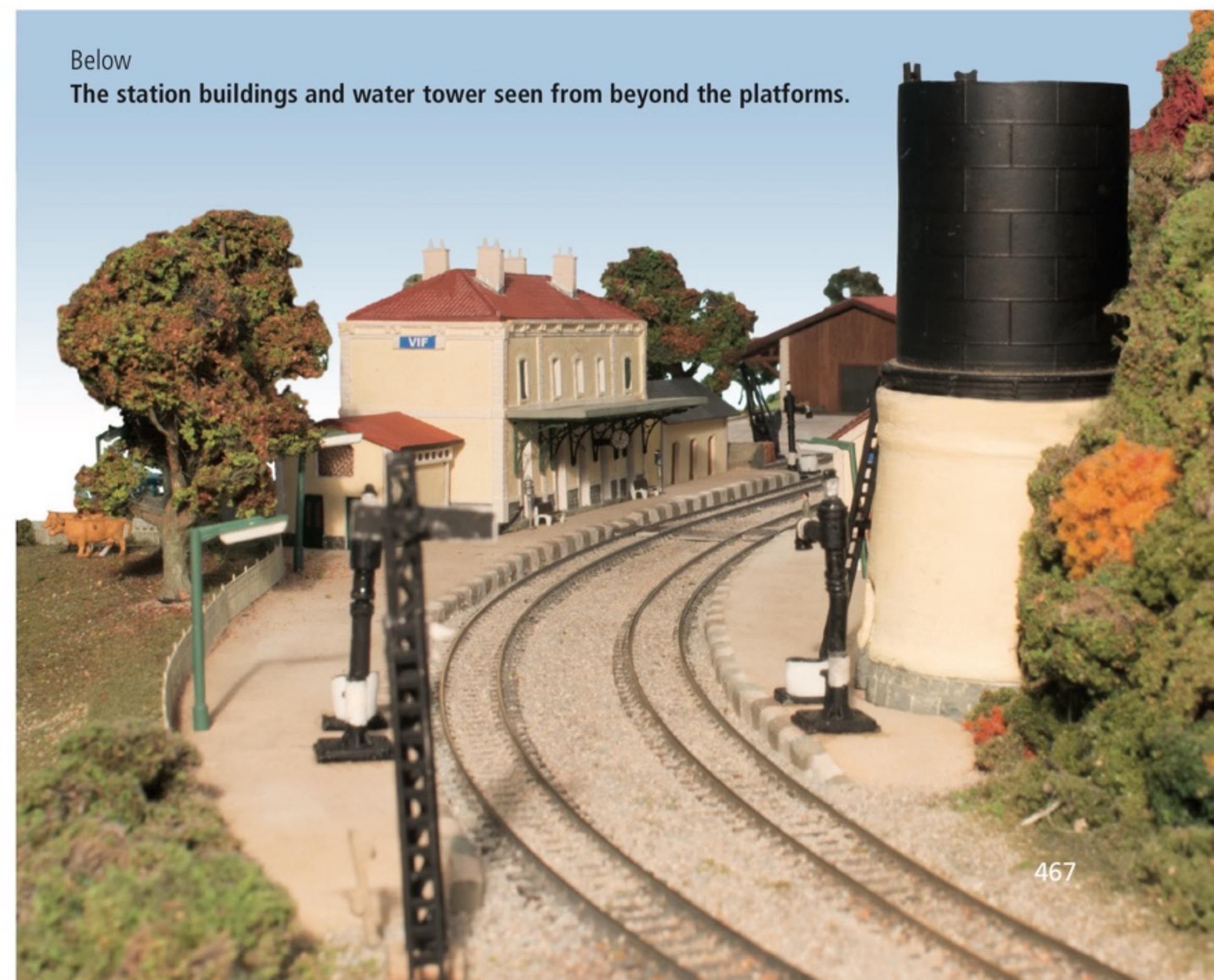
The *Ligne des Alpes* is currently under threat of closure, as are many surviving rural railway lines in France. Even with modern traction it is still a pretty exciting experience to travel from Grenoble to Veynes, the morning train at around 8.30 being best for clear views and photography during the summer months. February is best for chances of decent snow on the mountains and autumn is another good time for photographs. Do try to ride the line in 2018, and if you enjoy



Above  
It is 1958: a diverted  
Marseilles express including  
Pullman and restaurant cars  
hauled by a 241P is waiting  
to cross a local train headed  
by a 141R.

it, tell your friends. There are moves afoot to have it added to the UNESCO list of World Heritage Sites but whether this can happen in time to save it is an open question.

Below  
The station buildings and water tower seen from beyond the platforms.



**Roy Hirst** has developed an original layout into a more comprehensive record of the stock and personnel involved in the First World War.  
Photographs by Derek Shore.

# 1918 – behind the lines

## A memorial layout expanded and enhanced

The 1918 – *behind the lines* layout by Darryl Foxwell appeared in CM (December 2010), followed by an article by myself on the First World War rolling stock which populated it (October 2011). The original layout was a remnant of an earlier project which we re-modelled and re-sized for display. However, it increasingly revealed hidden age problems which periodically had to be corrected and would soon require drastic action.

Unfortunately, Darryl's health has slowly declined which has had an impact on the time he could spend on the hobby and he needed to scale down his activities. To help, I asked if I could take over 1918 rather than it be dismantled.

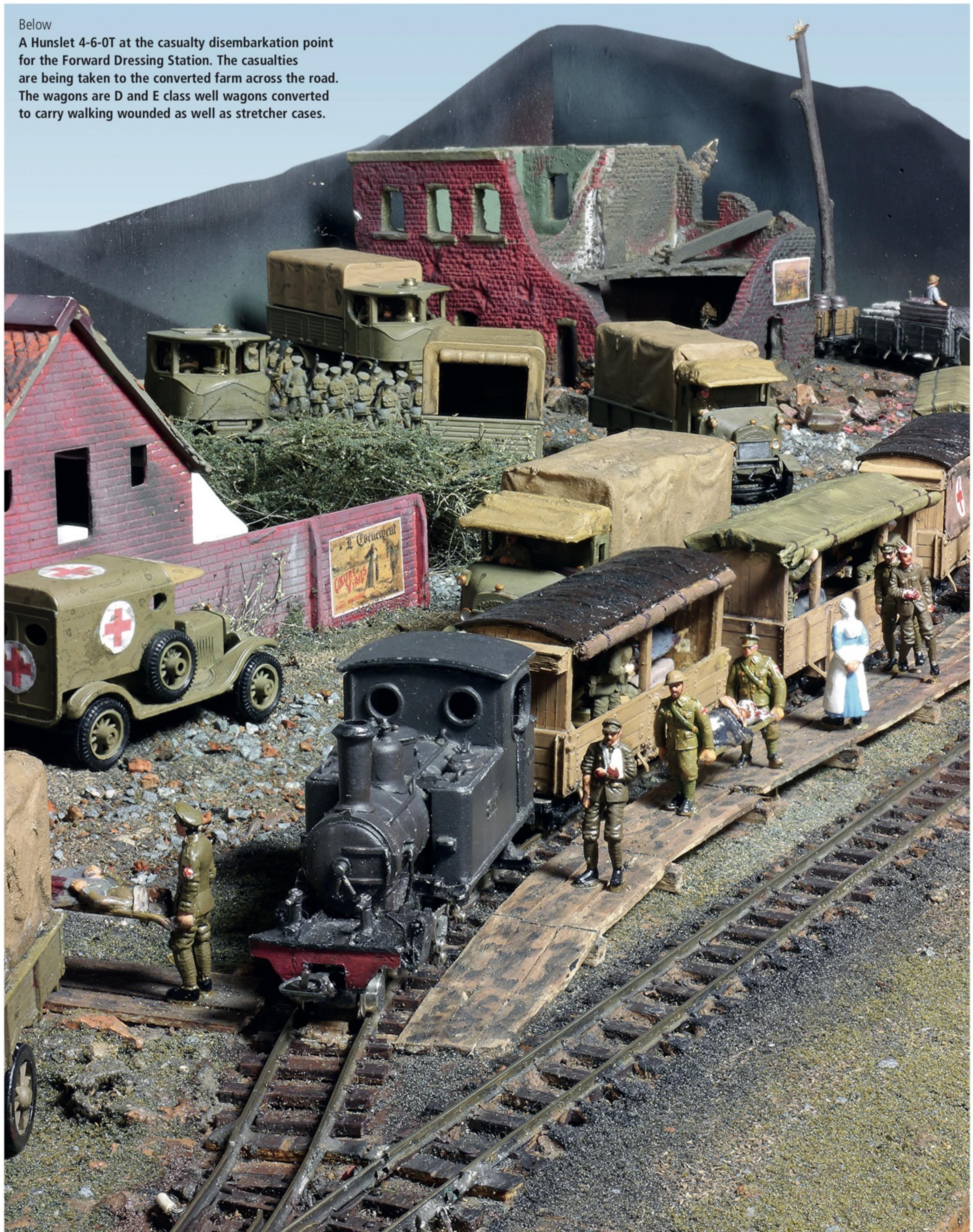
My collection of hand-made and kit-bashed locos and stock was getting larger and had outgrown the layout as it was. So, what to do? Well, this was an ideal chance to re-vamp the layout.

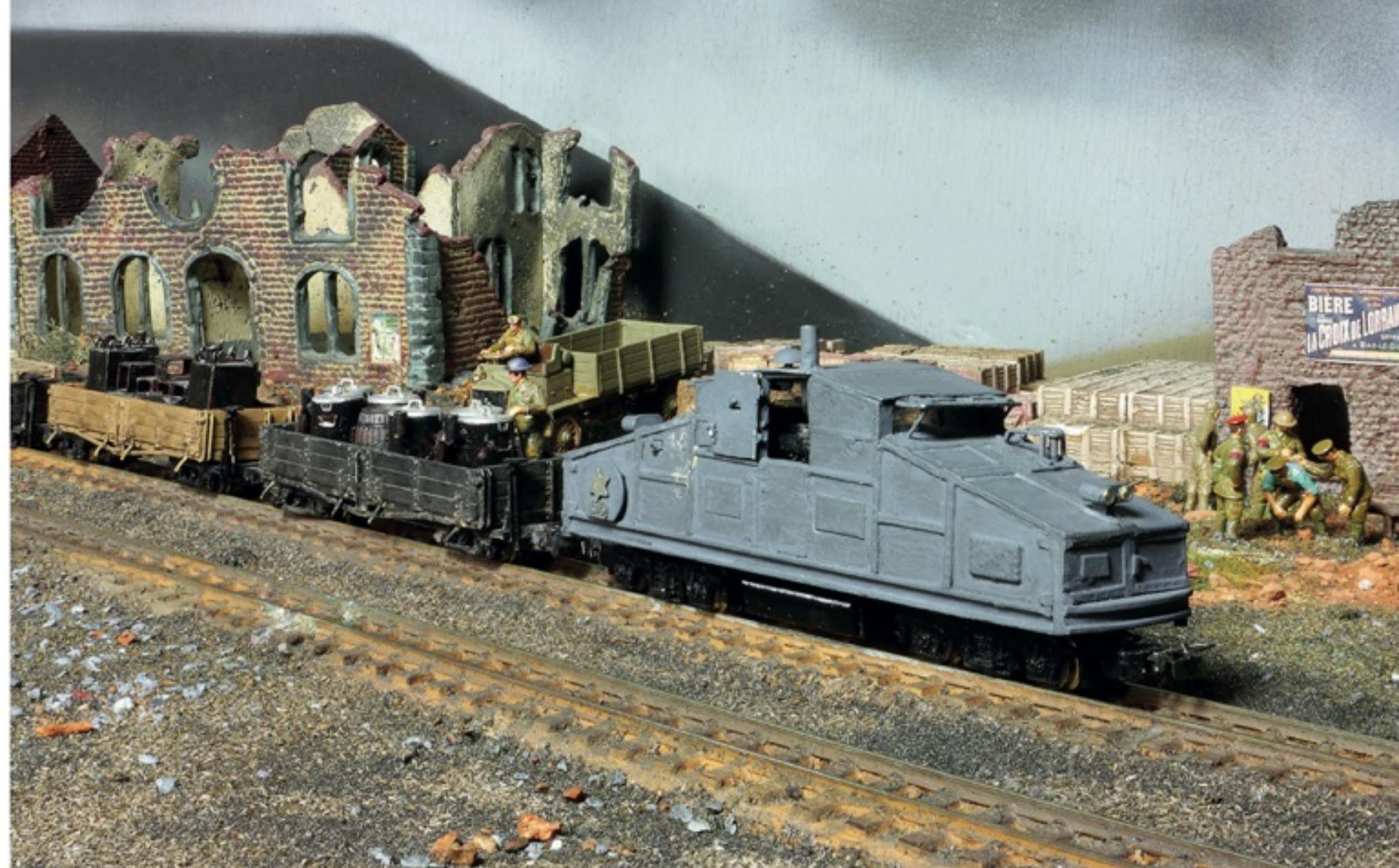
Below  
An Alco 2-6-2T with a rake of D class wagons taking troops to the front. Behind is the billet of a Royal Engineers tunnelling unit.



Below

A Hunslet 4-6-0T at the casualty disembarkation point for the Forward Dressing Station. The casualties are being taken to the converted farm across the road. The wagons are D and E class well wagons converted to carry walking wounded as well as stretcher cases.





Above

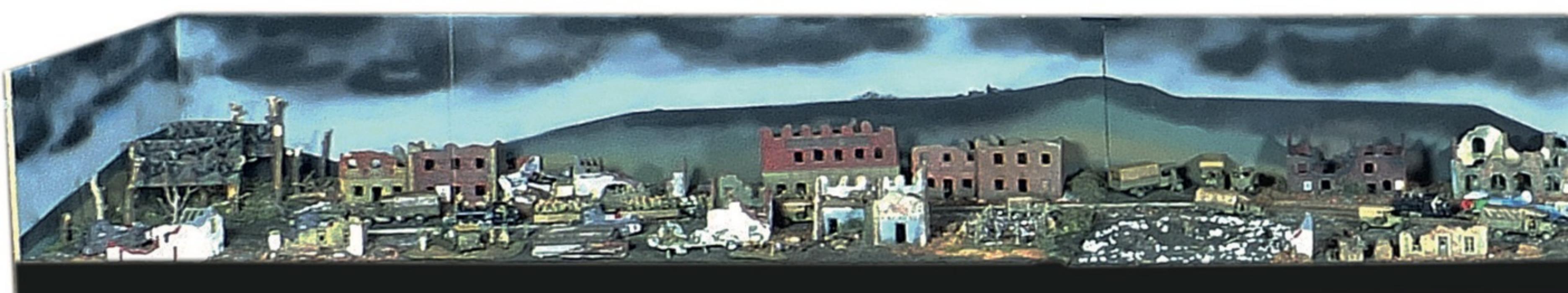
One of the French Crochat petrol-electrics (scratchbuilt) acquired by the Americans pulling a rake of six wagons containing a complete field kitchen – Soyer boilers and No.4 field cookers, dixies, food containers, hay boxes, hotplates, and chimneys for brick ovens as well as collapsible tables and preparation tents – all hand made.

Left

A mobile winch truck with a Royal Flying Corps observation balloon. The truck is a modified kit with scratchbuilt motor, winch, and field telephone with crew and even a dispatch rider to carry information back to HQ. All figures are from WD Models. The observation balloon itself was compiled from technical drawings and photographs from museum collections and is correct down to the chest 'phone, map board, harness, and basket.

Below

The billet of the Royal Engineers tunnelling section (known as 'clay-kickers'). Around the building can be seen a dump of their equipment, including tunnel building materials such as lifting winches, air and water hand-pumps, piping, and tunnel trolleys. In front is an open 40hp Simplex, detailed to show all its interior fittings.





Above  
**An Alco 2-6-2T transporting a section of troops along the main street of the town. Troops, motor transport, and a Whippet tank are moving in the opposite direction. On the left, an anti-aircraft pom-pom mounted on an armoured vehicle protects the observation balloon.**

### Stage 1

The initial work involved exchanging the bases for a stronger construction, replacing the track, points, and wiring. First to go was 4' of the 16' length and the old fiddle yard which was reduced in width to 13". This reduced the sidings to six plus a through line.

To make up for some of that loss, a new cassette system was incorporated within the fiddle yard with two extra sliding sections for interchanging rakes of wagons and locos. This forms a fiddle yard of three boards; one 4' and one 4'8" for the sidings and a third of 3'4" for the cassette section. 8" was borrowed from the cassette board to maximise the

sidings, hence the different lengths. This left three boards each 4' x 18" for the front display. All bases were given new fittings from White Rose, i.e. centre pins to position the boards and clips to hold the bases together firmly giving quick and reliable assembly – easily better and faster than the old bolts and wing nuts.

Due to the increasing size of the collection, I wanted to maximise the number of trains being seen by the public but not sacrifice historical accuracy. The over-large fiddle yard plus the single track was limiting, so reducing the fiddle yard and adding a passing loop, and later a further siding for ambulance trains next to the Forward Dressing Station for the transportation of casualties, all made operation faster and more manageable. The other advantage was that more trains could be circulated and seen by the public in a given period and provided historical accuracy in the workings of the military systems. The cassettes added to this by simplifying the exchange of rakes of wagon types and locomotives.

Below  
**A panoramic view of the layout. It is loosely based on a single line that ran through Liévin, a small town to the north of Arras. The track ran into the town from the north-west, passed down the main street, and ran out north-east towards the front line. At this point a single line carried on east towards a main dump. One line branched off south in the centre of the town but has not been modelled.**





## Stage 2

The next stage was to maximise the presentation of the full collection. To do this I converted an old display stand which consisted of two sides and four open steps. Vertical back panels were added onto which were pasted sections of original battlefield panoramas. Track guide strips were added on which the models are placed. A sliding tray was constructed at the base, fitted with two ovals of track. This gave the display a moving aspect, somewhere smaller locomotives could run with short rakes. Hollow metal tubes were fixed to the sides which carried the wiring for the lighting system for both the tracks and the four display shelves. A second controller behind the display powers the tracks and the lighting.

Whilst developing the display stand, I realised that I could also show examples from other nations involved on the Western Front.

In the early years of the war the British lacked military narrow gauge railways and initially just used Belgian and French equipment left behind as they took over more of the

defences. In doing this, it was realised what a great asset for supplying the army the light railways were and a system was hastily developed based on ideas that had previously been shelved.

Examples of captured German equipment appear on the layout, as well as American stock under their newly formed railway companies. This has added variety to the collection.

Jim Hodgkiss, who has worked with the team during the anniversary, is building up a collection of his own very fine models, and these are often displayed on the bottom step of the stand. Together we are planning future First World War projects.

The second stage fulfilled my aim to not only display the running layout but also be able to inform the public of the development and operation of the lines during the war, and discuss the equipment used and loads carried.

The latest addition is a laptop placed between the displays showing nearly six hours of archive footage of aspects of military railways from all the nations involved.

Below

### Display Detail

Front – American 'Pershing' water tank, flat car with stanchions, and gondola.  
Middle – captured prisoners and damaged field gun, Kerr Stuart 'Joffre' 0-6-0T, and D class wagon loaded with bales of hay (horse fodder).  
Back – Canadian-built stretcher wagons, Dick,Kerr petrol-electric, and modified D class stretcher wagon and hospital vans.



Below

### Display Detail

Front – part of the American 'Pershing' rake: box car and flat with stanchions.  
Middle – E class wagons with German prisoners and trophies from a trench raid.  
The E on the left has been converted to a D by removing the well but leaving the well frames in place. This was done to avoid back injuries during loading.  
Back – Canadian casualty stretcher wagons.



Above left

**A 6" naval gun mounted on a field chassis being made ready for transport. This is one of four types of gun carried on narrow gauge wagons. On the left, a Baldwin 4-6-0T pulls trench materials. Behind, a cameo scene of a motor canteen operated by the YMCA.**

Above

**One of the many estaminets serving off-duty soldiers. Note the 'sick' Tommy on the left showing the effects of imbibing a little too much having his number taken by the 'Redcaps'.**

### Stage 3

When I first started modelling railways, I saw many excellent layouts but I felt that many seemed to be empty – there were very few people and few representations of everyday life, though this aspect has been improving in leaps and bounds. I did not want my layout to fall short in depicting rail usage during the war. I am used to building scenes and cameos in military modelling to represent what happened in a particular period, so I used those skills on the layout.

I began by converting the farmyard into a Casualty Collecting Post and Forward Dressing Station, and adding the siding for casualty trains.

The next major addition was the observation balloon and its winch truck, dispatch rider, and Royal Flying Corps staff. The balloon was a challenge with its rigging plus map boards, telephone equipment, and the basket (complete with parachutes) for the crew.

Then anti-aircraft defence was added in the form of an armoured vehicle mounting a pom-pom and a 13lb gun mounted on a Thorneycroft truck, both with authentic crew and equipment.

There are also scenes showing *estaminets*, a tank maintenance area, mobile canteens, a prisoner cage, and even the 'clay-kickers' (Royal Engineers tunnelling troops).

This development continues and I am in the process of adding specialist scratchbuilt vehicles to these scenes, such as ambulances and a mobile workshop for the tanks.

All these scenes are a means of populating the layout and provide points for conversation and education during shows as well as linking the railways to these activities.

### Conclusion

For me, the most important part of the task was, as much as possible, to depict correctly the subject and the period. It was this that drove me to assemble the collection. It is emphatically not just a case of running nice model trains around an attractive track.

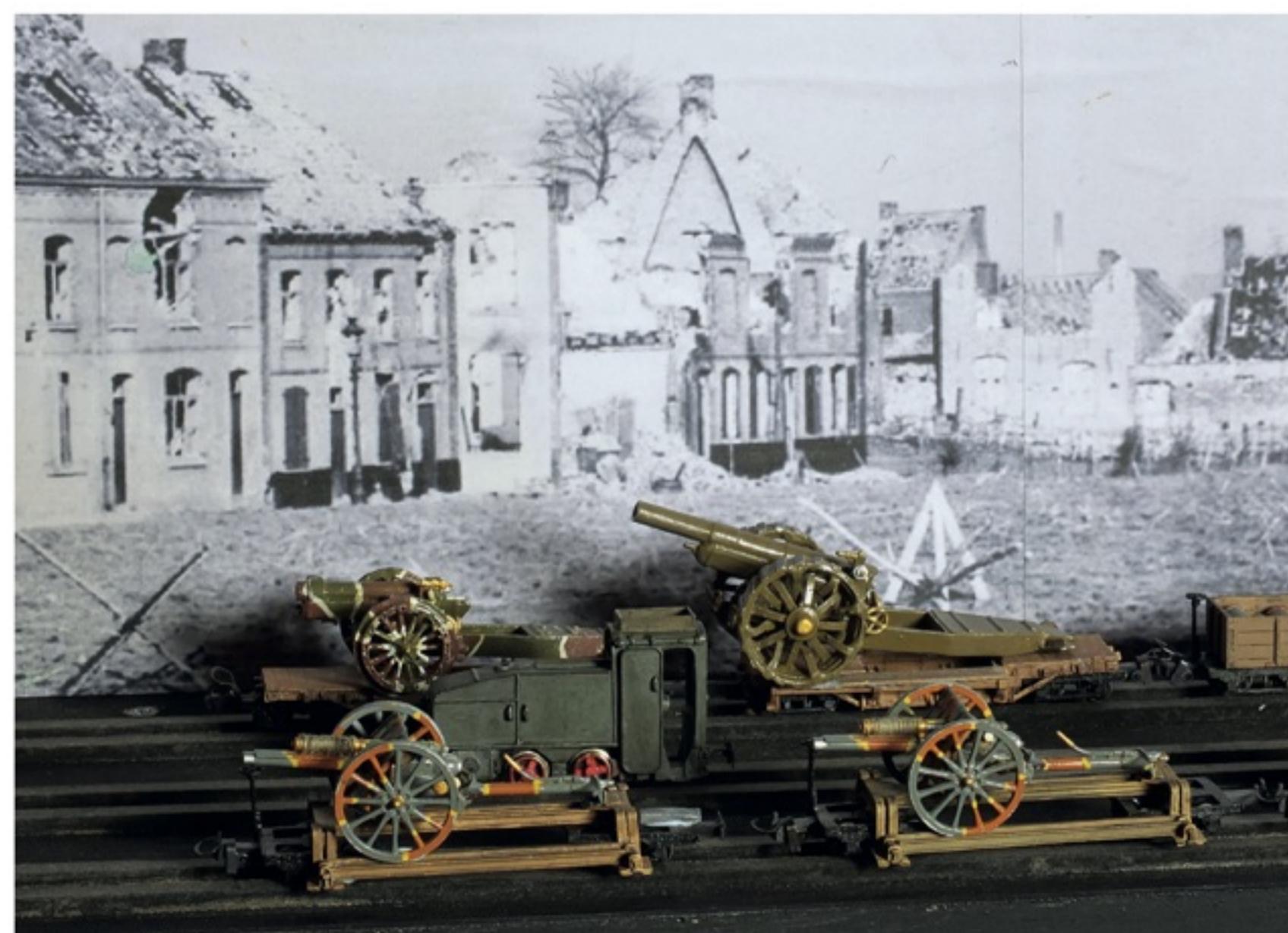
Below

#### Display Detail

Front – 18lb field guns on wagons well designed by Canadian workshops.

Middle – a captured Deutz *Benzollok*.

Back – E class well wagons converted to carry guns – on the left a 6" howitzer, on the right an 8" howitzer. There is another (unclassified) wagon in the collection to carry even larger field guns.

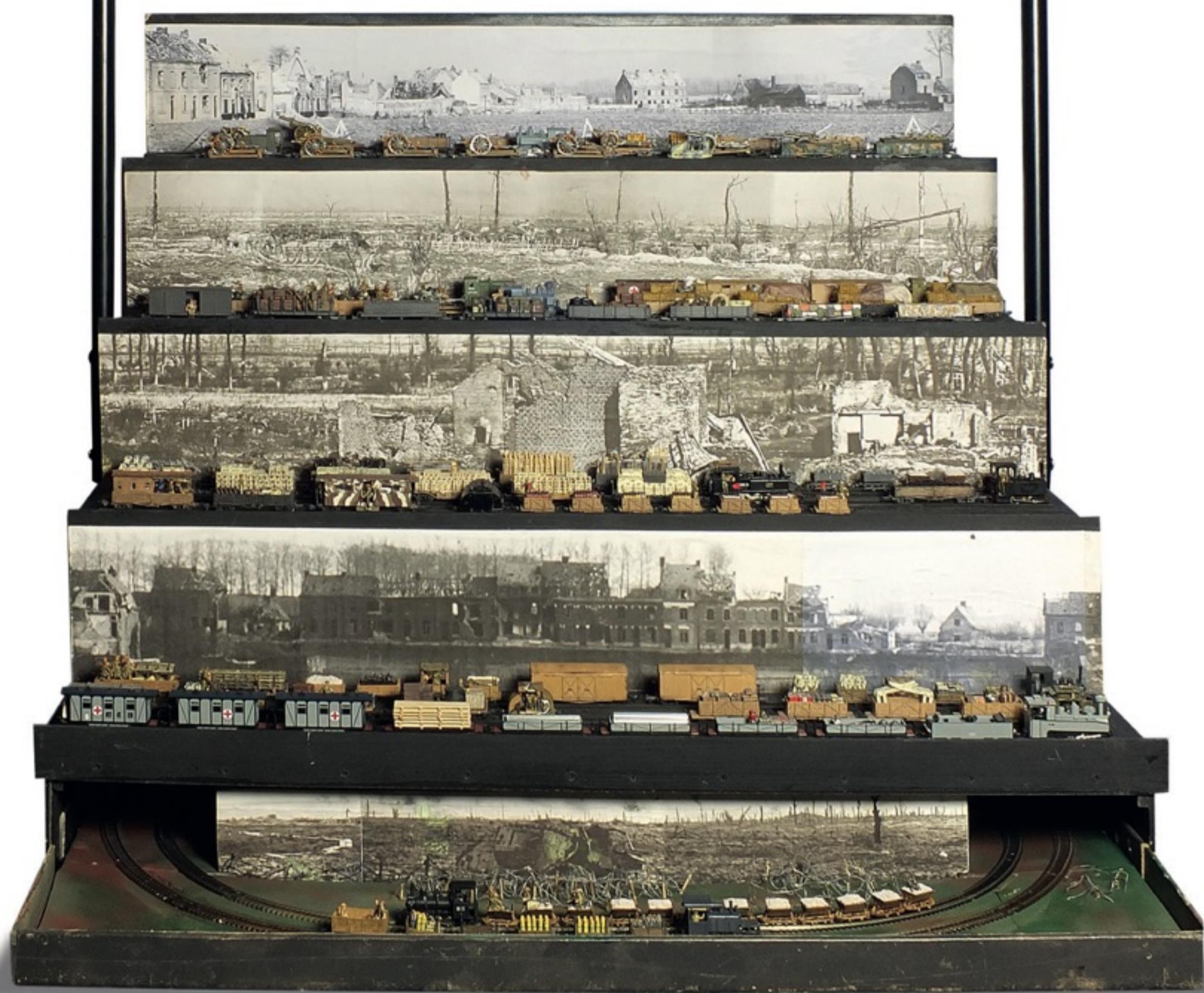


### W.W.1 Narrow Gauge Locos and Rolling Stock

Circa 1917 - 1918

WESTERN FRONT

Darry



Above

The stepped display stand used to show the rest of the collection. It has authentic panoramas as backdrops. The models represent the wide range of locomotive types and a complete range of War Department wagons plus many Royal Engineers variants. I also display (and run) a range of 'borrowed' French and captured German stock, which reflects what happened during the war. Two ovals of track were added to run smaller locomotives and their rakes of wagons.

Below

#### Display Detail

Front – A class wagons carrying gas cylinders to the front.

Middle – an Alco 2-6-2T with F class wagons containing ammunition boxes (various loads of shells, grenades, and rifle ammunition).

Back – trench items (shoring materials, duckboards, and trench toilets), a protected 40hp Simplex, and American design L class tipper wagons.





#### Forthcoming exhibitions

July 13th – 15th	Tracks to the Trenches, Apedale
September 1st	Selby
October 27th	ExpoNG, Swanley (t.b.c.)
November 11th & 12th	Barnsley
November 16th – 18th	Wakefield
December 1st & 2nd	Manchester



Above

**The casualty train, showing details of the wagon interiors – both D and E class conversions intended to carry stretchers and walking wounded.**

Left

**The prisoner holding cage, where Germans captured in trench raids were kept during initial interrogation before being moved to the rear, is also based on archive photographs.**

Behind, a Baldwin 4-6-0T is taking a crashed Fokker VII back for inspection and assessment; the kit was modified to show the correct damage and construction details. The blue and white markings show the squadron, the red and green areas and the fuselage emblem were the choice of the individual pilot to aid recognition in the air.

There is also ancillary equipment and the recovery shear legs for lifting.

The Baldwin 4-6-0T is a heavily modified GEM kit with added piping, lamps, etc., and scratchbuilt valve gear.

The J class stanchion wagons were converted from kits for E class well wagons.

Though the First World War was a horrible period, we have to accept that it is part of our history. To model it we must do it justice and respect the people that risked – and lost – so much to carry it through. With the centenary, now coming towards a close, people have been re-learning what has been somewhat forgotten. After one show, a retired RE Major wrote to thank us for keeping the memory alive; his recent recruits did not know how much RE troops had been involved in the railways. His father had commanded a railway company in the First World War and he commented on how the models accurately depicted the contents of his father's photographs. Praise indeed.

There are now no veterans surviving to pass on their experiences to future generations. Relatives have now realised what their (and my own) grandfathers' generation did; archives are being dusted off and re-examined; archaeologists and historians continue to uncover new informa-



Above

**The Casualty Collecting Post established in a farm courtyard, with various casualties and nursing/hospital staff. Amongst many other types, Model T Ford ambulances were a common sight. Despite their size they were found to be one of the best vehicles in poor or muddy conditions. Another variant is featured as part of the static display.**

tions; and authors produce new literature. This has resulted in a great increase in the number of people who have visited the battlefields and memorials, learning about and searching for their forebears. I would like to think that what I do in some small way adds to this process.

I never envisaged that I would develop the project as far as I have, or that it would open up such an extensive subject. I have met such an array of people, from the professionals mentioned above to relatives talking about their family stories. I have met fellow modellers, budding historians, and battlefield visitors, and made many new and interesting friends.

For me bringing the narrow gauge military railways of the First World War to life in a small way has been educational, challenging, frustrating, and tiring, but ultimately rewarding and, perhaps not surprisingly, at times a very moving and emotional experience.

**Tim Hale** has created an easily transportable layout for exhibiting largely using readily-available components carefully chosen and used.

# Pottendorf, 1912

## High summer before the storm

It seems that I have spent almost a lifetime living in and travelling to the region known as Oberfranken, just north of Nürnberg, nominally part of Bavaria but emphatically Franconia – local inhabitants consider themselves as belonging to Franken first and Bayern a poor second..

Early days were spent cycling on dusty gravel roads or hopping on to local trains to discover new villages and towns. The rural life was peppered with mem-

ories of the horse-drawn carts of the greengrocer and milkman (exhaust-belching road vehicles were yet not filling the streets) and blue-clad ladies still worked the fields, their white pinafores stained with the pale dust of hard toil.

Pottendorf is likely to be my last layout and is a personal tribute to this beloved area. Various visible clues identify its location, and it is intended to portray an almost forgotten life of blue remembered hills and sun bleached roadsides.

Below  
The classic image of a Bavarian branch line – PtL2/3 2-4-0T No.6058 arriving at Pottendorf with a train from Nürnberg.

Photographs by the editor.





Above

**The stationmaster emerges from his office, advised of the imminent arrival of the train by the signal bell.**

After over thirty years building and exhibiting layouts, I had to call a halt when my knees finally collapsed, but after their replacement with titanium a year ago, I decided to spend my forthcoming retirement exhibiting.

### Concept and theme

The design is based on the proposed terminus of the Wiesenttalbahn near Pottenstein; it would have been a typical small terminal station in Upper Franconia in the early years of the twentieth century.

The layout was intended to be an easy project that could be attempted by any competent hobbyist with the minimum of skills. Rather than using rare and exotic items, everything on *Pottendorf* is readily available, either from a good model shop or by mail order. Anyone could build a similar layout.

### Baseboards

These are laser-cut plywood units sourced from a professional carpenter, with four pairs of well-made legs that are stable and light – no wobbles! The boards are both light and robust, and the overall size is just 3.4m x 0.6m (c.11' x 2').

The off-scene storage uses cassettes.





### Track plan

The track plan was inspired by Ebern, in northern Bavaria, in its original form as a terminus with a locomotive shed, and uses just four turnouts, one of which is a three-way.

Operating the layout is never boring despite its small size. This is vitally important when exhibiting.

Track is Peco code 75, and, rather than expensive imports, all buffer stops are modified Peco.

All point motors are Peco low current types and have been fitted with additional switch contacts.

### The structures

Rather than choose 'somewhere in Germany', all structures were carefully selected to suit the supposed location in Upper Franconia.

Most were adapted from very old Pola kits, hoarded for a rainy day, but the half-timbered barn is a resin kit from Martin of Mueller's Bruchbuden in Nürnberg.

The station building is very similar to that at Redwitz an der Rodach, while the originals of all the other buildings are located no more than 25km from Forchheim.

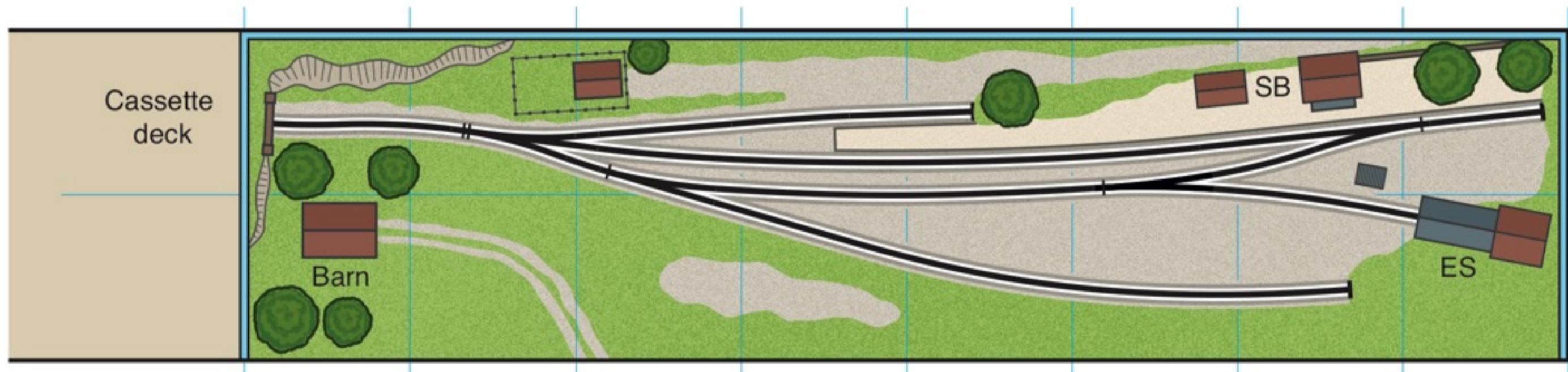
Above

**BBII Mallet No.2510 approaches the station with a freight.**

Above right

**The half-timbered cottage with its vegetable garden, wooden outhouse, beehives beyond the fence, and a few fruit trees, is very typical of the region.**

Scenic area: 8' 0" x 2' 0". Overall area: 11' 0" x 2' 0". Grid squares represent 1ft x 1ft.



Below

**Although the layout is not very large it does manage to convey a feeling of space.**





## Scenery

The region of Franconian Switzerland is notable for its distinctive limestone cliffs; a small outcrop has been used to conceal the exit to the fiddle yard, made from modified moulded foam pieces. The distinctive limestone colour has been used throughout the layout to create a harmonious feel to the scene.

The overall dusty appearance of the road surfaces and sun-bleached vegetation would be familiar to anyone who knows the area. The textured road surface is Stangel acrylic matte medium intended for the walls of 1:45 scale buildings.

The vegetation is a mix of hanging basket liner which has been multi-layered with various static grasses from Heki and WSS Scenics. Other scenery materials were sourced from MiniNatur in Germany and MBR in Poland.

Careful inspection will reveal many features typical of the region and details correct for the period.

## Stock

A tight grip on the wallet has restricted the rolling stock to items which actually ran on the Wiesentalbahn, apart from



the PtL2/3 (Fleischmann) which is used for excursion trains from Nürnberg. There are just a couple of other appropriate locos – a DXI (Trix) and a BBII Mallet (Roco) along with a suitable clutch of *Lokalbahn* stock.

The rolling stock is a mix of Brawa, Fleischmann, and Trix, some fitted with RP25 wheels.

Above  
**Outside the wooden goods shed are items to be loaded into the baggage van. The small station building is enough for the local traffic. Signal bells are by Brawa.**





Above

The latest technology! A motor lorry threatens the dominance of horse-drawn vehicles.  
The rail-built buffer stop was adapted from a Peco accessory.

Below

DXI 0-6-2T No.2739 pulls away from the platform  
with an excursion passenger train.  
Most local services would be mixed.



## Couplings

I started to use the Kadee system but they have always been problematic, mostly due to the inconsistencies of the NEM362 close-coupling system. Even suitably weighted to ensure that they do not roll away as they are coupled, European models are not really designed for Kadees which require a certain amount of 'heft' in order for the knuckles to 'shake hands'. They are also rather temperamental because they are susceptible to variation in the coupling height. Also, I do not think that the 'shuffle dance' when uncoupling is particularly realistic.

Consequently I went back to basics having rediscovered the humble European standard coupling which comes with most models. Simply remove the loops from the coupling on one end of each wagon, except the ones on each end of the train so that it can be drawn out of the station or simply shunted. All locos only have the hooks.

I have settled on using a cut-down Märklin short coupling head with NEM 362 boxes. The loop and side extensions are removed before fitting the coupling to the loco, whilst

the lower trip dropper is cut off the loop as well as the side extensions for fitting to wagons/coaches. The result is very discrete and very reliable.

The Kadee magnets are now gone, a huge improvement.

To uncouple, I use a neodymium micro-magnet on a thin wand to lift the loop from the hook from above. See – <http://timhalesblog.blogspot.co.uk/search/label/Coupling>





### Weathering

The stock has been subtly weathered with careful washes of heavily diluted Lifecolor sleeper colour, whilst the buildings have received a fine layer of very pale yellowish dust, typical of a very dry August.

### Control and operation

The layout was designed for one person operation with all control by DCC using Roco MultiMaus handsets – this provides excellent control for the locos and the turnouts.

### Exhibiting

Based on past experience, the layout and supporting equipment fits nicely in the load space of the 'codgermobile' and there is still room for luggage. By providing the transport in this way and requiring just two operators, the cost of exhibiting is significantly reduced; as a result, we already have several bookings, at the moment more in mainland Europe than in the UK! Our next outing is to Taunton in October, and we welcome further invitations. [pottendorfho@gmail.com](mailto:pottendorfho@gmail.com)

### Lighting

When exhibiting it is impossible to rely upon the ambient lighting even in the better venues, and it is essential to control the presentation. I use kitchen cabinet LED units which provide 'daylight' rather than the yellowish hue from normal domestic fluorescent units. A proscenium arch was added to the original framework to carry the lighting.

### Conclusion

Despite some initial concerns, the layout has fulfilled its remit. It is easy to transport, and everything can be packed away for transit and storage within thirty minutes.

To conclude, thanks to my 'partner in crime', Neil Kinison, without whom none of this would have been possible.



Above  
The PtL2/3 draws forward.  
Note the water crane,  
coal stage, and shed interior.



Above and below  
The coaches stand in the station  
as the loco has its supplies replenished.  
The small tank cart (from Artitec) carries lubricants.



Moving from a house to a flat prompted **Peter Smith** to build a new compact layout, which was subsequently extended.

# Oresome Aggregates

A Feldbahn layout in HOf (1:87 scale, 6.5mm gauge)



Photographs by the editor.



Above  
A train of side tippers crosses the bridge (a Wills kit).  
The locomotive is a modified 3D-print by Chris333 from Shapeways;  
similar locos were used on a brickworks railway in France (briqueterie Chimot).

## Why HOf?

I am sure our decision to downsize from a large house to a retirement flat had something to do with it, but the immediate prompt came from my favourite internet forum, *Narrow Gauge Railway Modelling – Online*, and a 'too good to refuse' show offer on a Busch starter set at the Warley NEC event in 2016 decided it.

Prior to that, over many years, I had modelled American and British outline in all the popular gauges and scales. Some of these efforts have graced the pages of CM, including *Cuba Today* (November 2002 and January 2005) and *Cariboo Lumber and Stone* (October 2015).

## Development of the layout

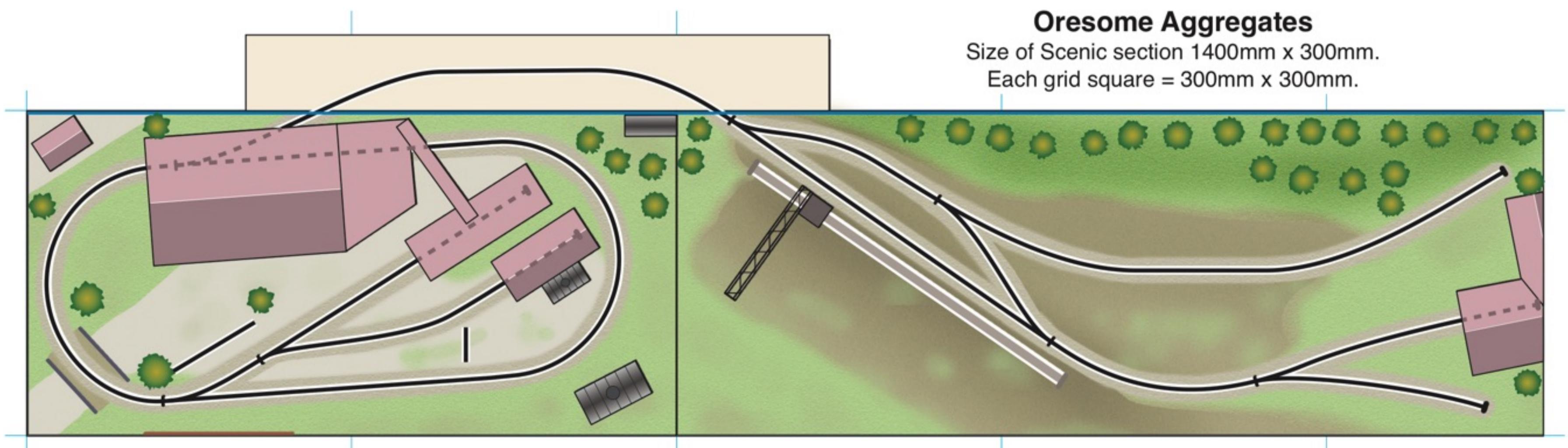
During 2016 I had enjoyed building a small diorama as a competition entry for Expo Narrow Gauge, and I wanted a small industrial layout with more operating potential. So I added a couple of turnouts and straight track lengths to the Busch starter set and on a board 54cm wide and 30cm deep built *Oresome Aggregates* phase 1. The turnouts inside the oval serve an ore unloading shed and a small loco shed.

The buildings, including the larger one linked to the unloading shed, form a view block.

The backscene was painted by Ron North, a fellow member of High Wycombe MRS.

## Top

At the unloader is a scratchbuilt Diema with inside chain drive. The steam locos (by Busch) have been repainted and modified. The stored diesel is a Jung (a static model by Auhagen).



Left  
**A number of similar small powered railcars are used on narrow gauge coastal railways in north Germany for personal local transport. This model is powered by a Rokuhan 'Shorty' Z gauge mechanism.**

**Oresome Aggregates**  
 Size of Scenic section 1400mm x 300mm.  
 Each grid square = 300mm x 300mm.

To help further disguise the simple oval, the track base was raised by 4.5cm to allow an underbridge and for the ground to fall away at the sides. In addition, the oval was skewed so that the straight track was not parallel with the front edge of the layout.

**Control system**

At this stage I was using Busch locos and stock 'out of the box' and the 3v DC fixed speed battery controller supplied with the starter set. I described the layout on a thread on NGRM-Online and a suggestion was made there that I try a Gaugemaster Model W handheld controller with a reduced AC input. A 6v AC transformer worked just fine, giving full speed control within the 0-3v DC range of the standard Busch chassis. My enjoyment was transformed!

**Exploiting the Busch system**

With speed control came a desire to make the best of the well engineered and ingenious Busch system but lose the 'toy train set' appearance it often has. The code 60 track is quite pleasing and can be easily ballasted without losing the magnetic effect that holds the locos to the track to improve operating reliability. Ballast and scenic materials must not be magnetic, and every speck of surplus ballast must be removed as well as any loose scenic material, and the track kept clean; I use surgical spirit on a home-made track cleaner.





Above  
**The first part of the layout with the second, scratch-built, locomotive shed. This shows how the basic oval of track is skewed and raised to allow the road to go under the bridge and the land to fall away at the sides.**  
**In the centre is a row of stored or abandoned locos and stock, as seen at many industrial sites; the Deutz diesel (Busch) proved to be too heavy for the line!**

The rolling stock can easily be repainted and weathered, as can the locos, but the range is still quite limited and for me they shout 'Busch'. I discovered a wonderful series of books, *Die Feldbahn*, published by Verlag Zeunert in Germany (and often reviewed in CM). These well illustrated (mostly in colour) books feature German Feldbahnen (field railways) in all their different forms; Band (volume) 12 describes the wide variety of motive power. This was a wholly new area of interest for me and very enjoyable.

I also learned that some sellers on Shapeways offer 3D-printed bodies that either fit the Busch chassis or can be adapted for it. This solved the loco problem, and I have since also scratchbuilt my own bodies in styrene and metal. More recently Narrow Planet have released a complete HO scale body kit for an Orenstein & Koppel RL2 diesel to fit the Busch chassis (see review in CM June 2018).

### Operation

Fortunately I did lots of trials before I built the layout, and as well as very clean track I learned that track joins must be smooth and level, and the steel strip to attract the magnet under locos must not be interrupted. The strip is stepped at the track joins for this reason. I tried soldering the rail joiners at the joint but that is not necessary.

I also learned that the turnouts must have switched frogs. Even so, the turnouts are the weak link as I find the gap at the frog is too wide and wheels can drop into it, disrupting power pick-up.

It is worth mentioning that the Busch track system is designed for common return wiring with only one insulated rail joiner on turnouts. Extra insulated rail joiners are available and my layout is wired conventionally with double breaks and feeds. A spare metal rail joiner comes with each turnout





and I keep these carefully as I do not know of a source for them.

The Busch chassis are all tested prior to delivery and are reliable, if somewhat noisy, but performance, especially through turnouts, is surprisingly variable mainly because the back-to-back wheel measurement varies. I check everything carefully and oil the gears very sparingly with Labelle 108. (Busch do their own lubricant.)

As a result of all this I have achieved reliable operation which satisfies me and makes it a pleasure.

The system uses coupler magnets on the locos and some stock together with simple hook and eye couplers. The hooks are attracted by the loco coupler magnets and the system works well, but I have fixed rakes of tippers as coupling in the event of a derailment is a problem as my years advance! A blob of two part epoxy on the hook at just the right time ensures the loop stays coupled but still moves as required. Generally locos have magnets at the rear end only but I was able to get more coupling magnets via the internet.

### Extension

Having got this far, I extended on to a second board to make the total length 140cm. I decided that as far as possible this would be true Feldbahn, and I wanted to use the Busch excavator/loader (*Eimerkettenbagger*). The raised track base featured on the original board allowed the land to fall away from the bucket excavator quite naturally and the join between the two boards is hardly noticeable.

A small brickworks, based on a photo in *Die Feldbahn Band 15*, forms the end of the extension. Small local brickworks with their own railways abounded in some parts of Germany. The sods and hillsides were simply turned over wherever clay was to be found and abandoned, often with equipment left behind, when supplies were exhausted.

A simple loop and two sidings laid at an angle so it could be viewed along its length as well as across its width greatly improved the operating potential.

The link between the boards is hidden behind the backscene just long enough for watchers to wonder if the train will reappear!

The second loading point adds interest; the crane loader is a 3D-print by T3 Design from Shapeways.

I was pleased with the result and went back and replaced the ubiquitous Wills garage engine shed with a scratchbuilt shed in the German Fachwerk (timber framed) style.

The German setting does not stop me operating locos suitable for field railways from various parts of the world.

### Conclusion

This little layout is perfect for our flat, and was popular at its first outing, at Narrow Gauge South in April 2018. It had a second outing in High Wycombe (June) and is on the reserve list for Wyclair (3rd November).

Viewers say how small everything is, but I constantly tell myself it is only HO scale.

Above left and left

**The bucket excavator/loader (*Eimerkettenbagger*)** is a recent laser-cut kit by Busch. The loco is an Orenstein and Koppel RL2 (body kit by Narrow Planet). The loop is angled so it can be viewed along its length as well as across its width.

Right

**The brickworks** was scratchbuilt, based on a photograph in *Die Feldbahn, Band 15*. The stored diesel locomotives are the Narrow Planet O&K and two modified 3D-prints by T3 Design.

## Sources

**Busch:** there are a number of outlets stocking Busch in the UK. I have had good service from Golden Valley Hobbies (via my local model shop, Transport Treasures of Aston Clinton), the Ffestiniog Railway shop, and The Hobby Shop, Faversham, Kent.

**3D-prints** of Feldbahn material for the Busch chassis have been designed by T3 Design, Toma Model Works, and Chris333, all available through Shapeways.

**O&K RL2 kit:** Narrow Planet,

[www.narrowplanet.co.uk](http://www.narrowplanet.co.uk)

**Coupler magnets:** Spider Magnetics Ltd.,

[www.web-magnets.co.uk](http://www.web-magnets.co.uk)

**Controller:** Gaugemaster Model W.

**Transformer:** AC adaptor MHB 0600 2000,  
230-240v AC to 6v AC, from Amazon.

**Frankfurter Feldbahn Museum Guide**

– illustrated guide to the exhibits,  
with dimensioned drawings.

**Die Feldbahn, Bands 1-15**, Verlag Zeunert

I believe this has been discontinued due to the death of the publisher but copies may still be available, along with the excellent Frankfurt museum guide, from Karl Paskarb –

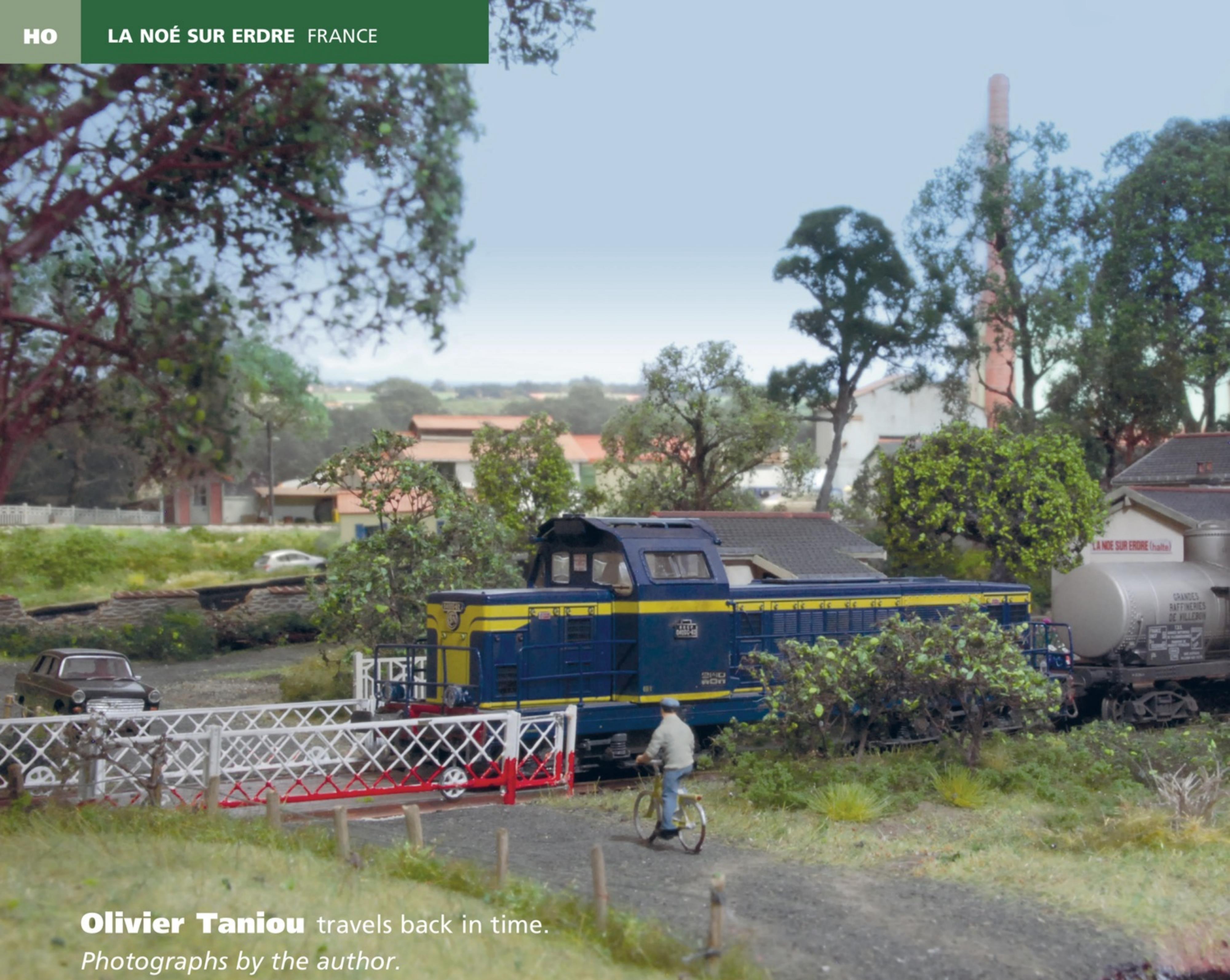
[www.schmalspurverlag.de](http://www.schmalspurverlag.de)

I am grateful to my fellow club members, especially Ron North for the extended backscene, and NGRM-Online members including Blair Hobson who suggested the downrated Gaugemaster controller.

Above right and right

**The second loader** uses a German crane with a grab bucket (a 3D-print by T3 Design). The loco is a scratchbuilt Diema with a dropped driving position.





**Olivier Taniou** travels back in time.

*Photographs by the author.*

A day at the halt of

# La Noé sur Erdre

The best way to present this diorama, which I built to photograph my rolling stock, is to imagine spending a beautiful summer day by the side of the single track line built by the Paris – Orléans company linking Nantes to Rennes via Châteaubriant in the north-west of France, at various times between 1960 and 1980.

After assembling a kit for the halt building, produced by Architecture & Passion, I wanted to go further and include it in a diorama which could be used to photograph stock in a detailed scenic environment. It was also an opportunity to try out some new scenic techniques.

The scene was built on a base of 10cm thick extruded polystyrene foam just 110cm x 50cm. This material has the advantage of not disintegrating into little balls when worked.

The upper part was made using conventional expanded polystyrene foam, which was first cut with a saw and then rounded with coarse sandpaper.

During this part of the work, I used a vacuum cleaner frequently to reduce the mess.

I sculpted the landscape so as to have the back of the diorama higher than the front, to enclose the view. The left side is at the level of the track, so the stock can be photographed in the scenery. On the right, a sloping meadow allows some views from a low angle.

The roadbed is a piece of 10mm thick plywood glued between layers of polystyrene. The assembly was then covered with plaster, then painted brown or green, depending on how each area was to be treated later.

Above

**A rumbling sound followed by the metallic clang of the level crossing rolling barriers closing alerts me to the approach of a train.**

**Facing me is the halt of La Noé sur Erdre. On the other side of the barriers, a new black Peugeot 404 waits patiently, identifying the period as the 1960s. I just have time to capture the 040DG and its train of bogie tank wagons coming from Rennes.**



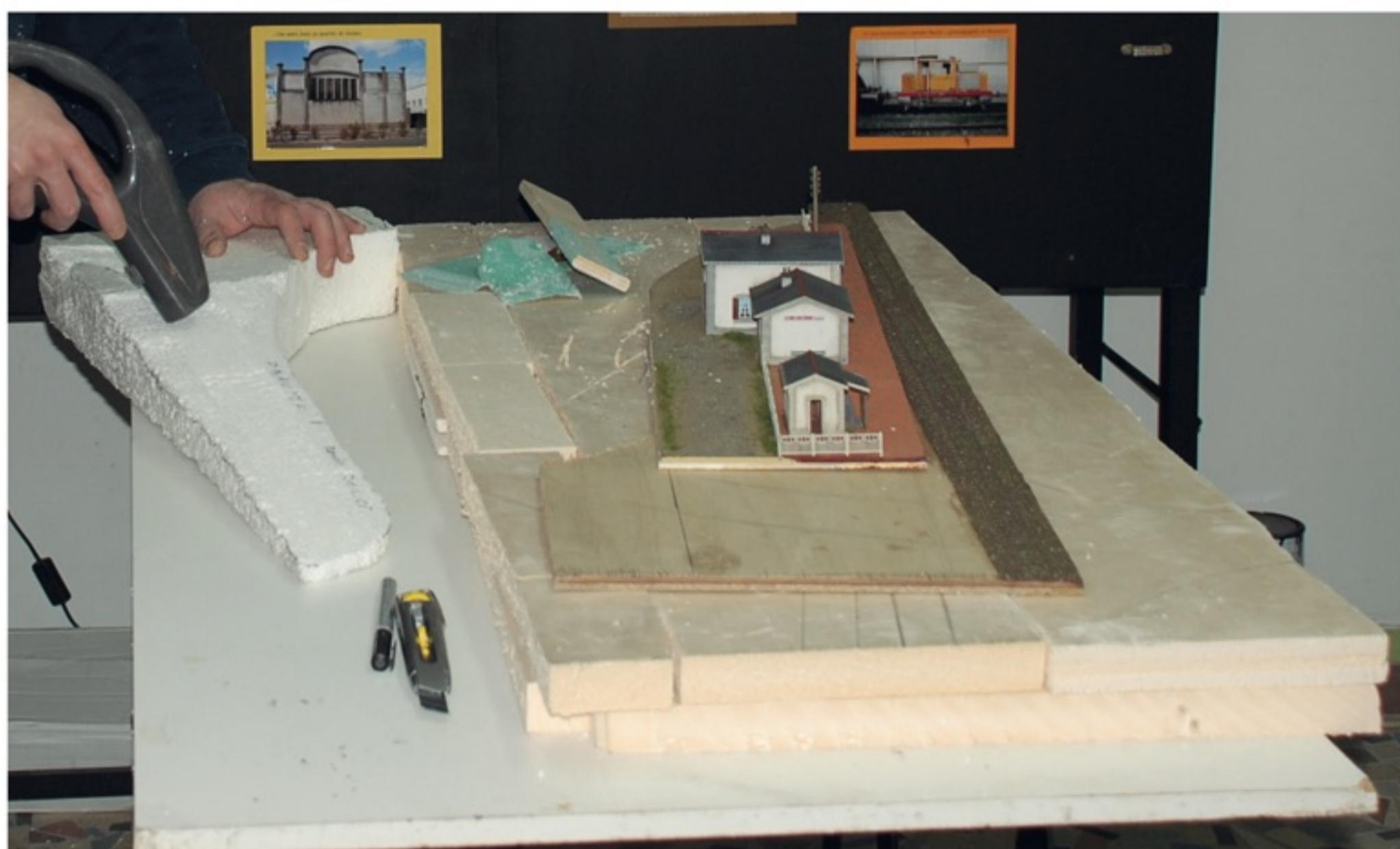
Above

A horn in the distance tells me that another train is approaching. An X3800 *autorail* from Rennes depot stops in front of the building, but no-one gets on or off. The purring of the engine drowns the strident cries of swallows. The farmer on his Renault 3041 tractor descends a dirt road leading to the Erdre.

Below

Time passes quickly ... the day is now well advanced. From the bottom of the slope, I get a photo of a faded C61000 from Nantes. It slowly waddles past the halt. The train is short, only five wagons. The two-tone horn briefly animates the scene.





Left and above  
Cutting and shaping the polystyrene foam landscape base.



Left and below  
Several methods were used for tree construction.





Above  
**The modest halt, typical of those located in rural areas, is still well maintained but some way from the village it is supposed to serve. On the platform, invaded by the scent of roses and daisies, is a bench, the signal bell, and a bicycle. The floor of the waiting room is covered with tiles. In one corner is the ticket office, overlooking the platform.**

The various sands and soils are natural materials from our garden.

The construction of a diorama allowed me to try new methods that will be applied to a larger layout later. Among them was the use of the electrostatic GrasMaster, which was a successful experiment.

Mixtures of foliage nets represent brambles. Flowers and kitchen garden accessories are by Busch, repainted and weathered to cover the plastic look.

My wife took care of the making of the trees, among other things. Several methods were tried. Pieces of thyme, pear, and sea foam coated with filler made the trunks. The branches are also made of sea foam.

The pollarded lime on the platform was made of stranded electrical wire patiently soldered together and coated with filler.

The track is SMP, ballasted with Zéb-dec products, from the Zébulon VPC scenery range.



Right  
**A well tended flower bed surrounds the lime tree on the platform. Nearby is the separate toilet block.**



Above

**The characteristic sound of the six-cylinder Poyaud engine resonates as a Y7100 in original livery hauls a goods train. At that time mixed traffic still reigned supreme and the train is varied: four-wheel tanks, an insulated van, open wagons, a flat, and a guard's van – still an essential element of freight trains on the western region at that time.**

Below and below right

**A short goods train hauled by a Y6400 on the level crossing. A worker is returning from work on his Alcyon bicycle.**



#### Selected details

On a small diorama, each accessory has more importance, because it will be looked at so closely. It is therefore necessary to be careful with its location and decoration.

Some items, such as the electrical substation and the way-side shrine, come from Zéb-Dec. The well and the toilets at the bottom of the garden were constructed from plasticard covered with Redutex sheets. A bicycle from SMD graces the platform, along with concrete fence and old sleepers from Architecture & Passion.

For road vehicles, I chose the beautiful Peugeot 404 by Brekina and a 4hp Renault. The Renault 3041 tractor (left) was made from a whitemetal and resin kit produced some time ago by L'Obsidienne.

A Preiser cyclist waits patiently in front of the rolling crossing barriers (which are from ABE).





Above

The signal bell sounds, and the barriers are closed once again: a Renault ABJ 3 *autorail*, coming from Nantes, stops at La Noé sur Erdre; a few travellers descend. The 'bigoudène', so called because the radiators on the roof are reminiscent of the head-dresses of South Brittany, seems to have been overhauled recently: it is very clean, and the roof is red, as it was in the late 1960s. As it departs, I enjoy the 'melody' emitted by the 300hp Renault V12 engine.

Right

Behind the white-painted concrete fence on one side of the halt is the kitchen garden, which is rather well maintained.

Below

As usual, the toilet block is a separate structure.

Below right (2)

Details were built from various plastic sections, plasticard, etched parts, cast components, and Redutex textured sheets.





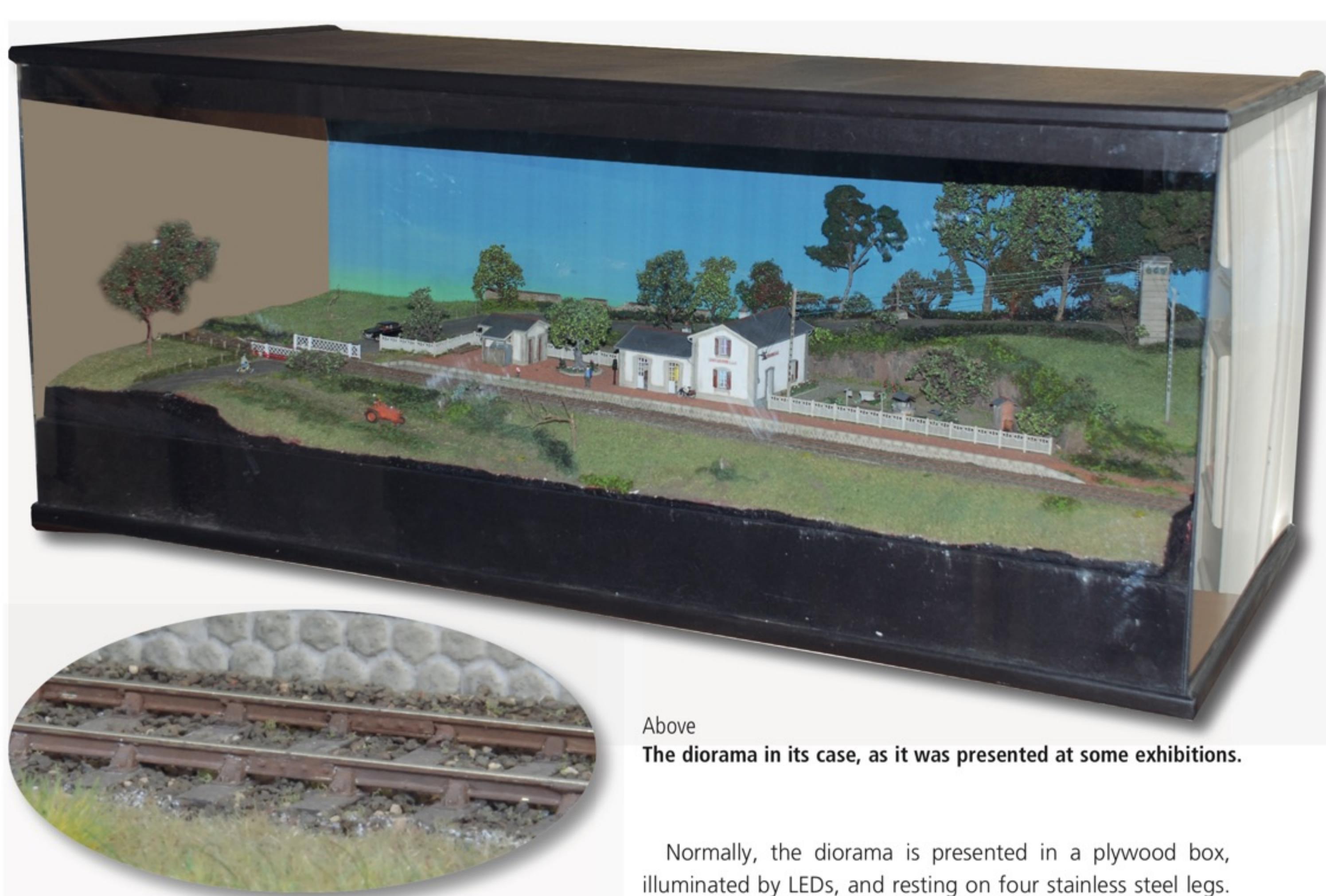
Above

Returning to the crossing, I catch another passenger train, this time loco-hauled – DEV or UIC coaches (I never know which) behind an A1A A1A 68000. The train does not stop here.



Left

I had just decided to leave when a goods rolled gently over the crossing – a rake of a dozen bogie hoppers pulled by a BB 63000 in the 1980s Arzens livery passed too quickly for me to photograph it properly; I only had time to catch the rear of the train.

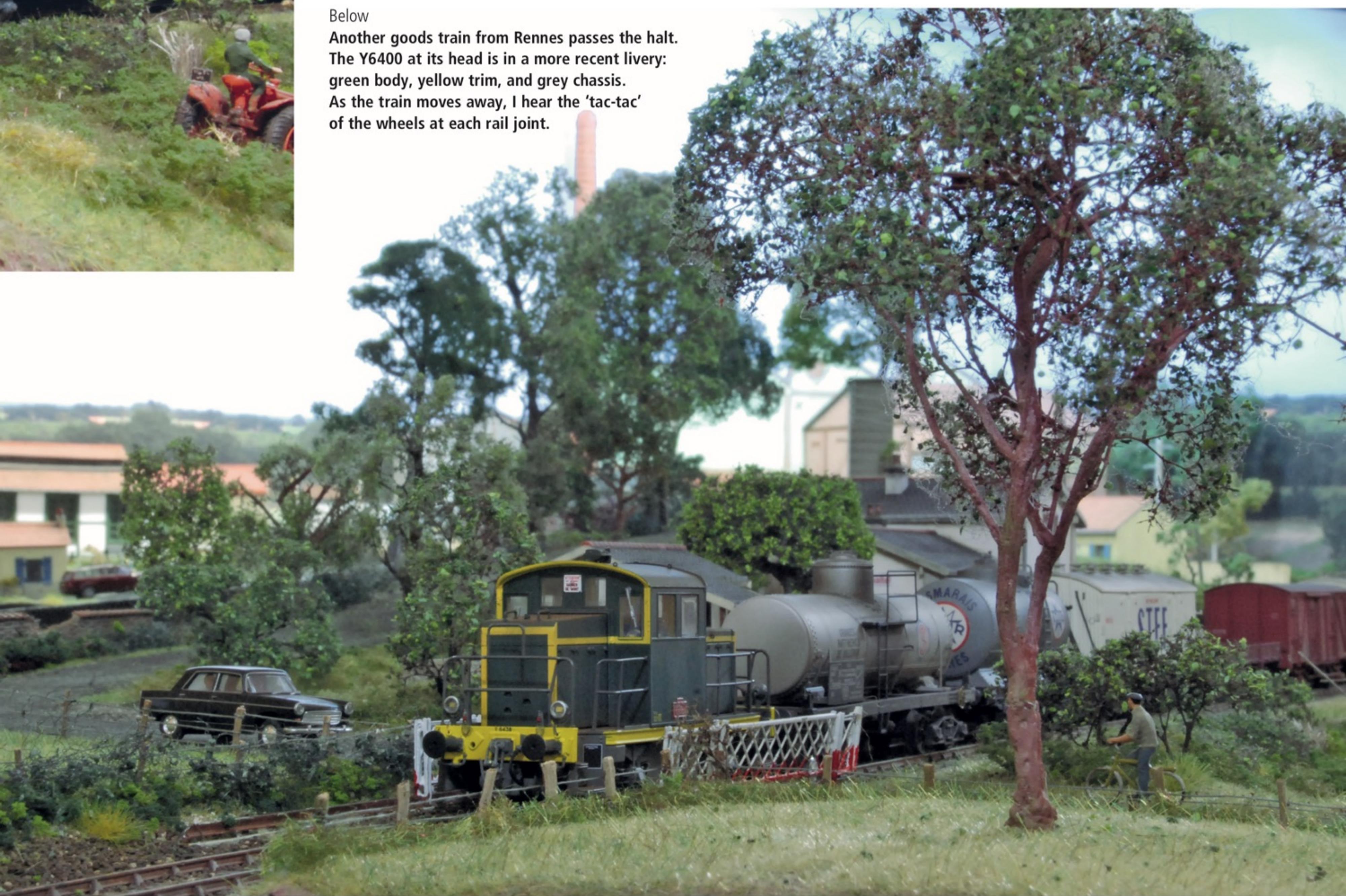


Above  
**The bullhead rail track, typical of lines inherited from the Paris – Orléans, even has the bolted joiners represented.**

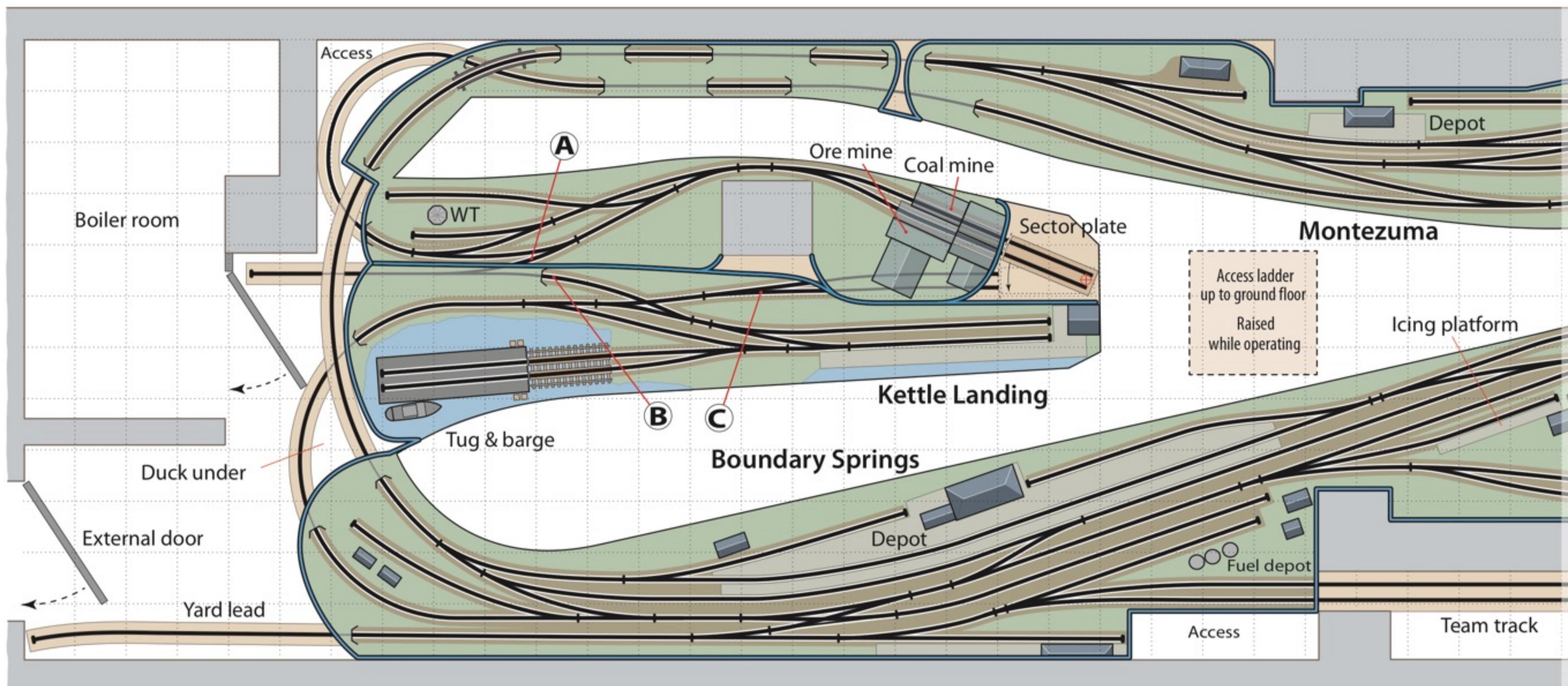
Below  
**Another goods train from Rennes passes the halt. The Y6400 at its head is in a more recent livery: green body, yellow trim, and grey chassis. As the train moves away, I hear the 'tac-tac' of the wheels at each rail joint.**

Above  
**The diorama in its case, as it was presented at some exhibitions.**

Normally, the diorama is presented in a plywood box, illuminated by LEDs, and resting on four stainless steel legs. At the back a painted panel represents blue sky. Clear plastic sides and front protect it from dust.



# Columbia & Western



Overall size 60' x 16'

each grid square = 1 sq.ft.

Set on the Canadian Pacific in British Columbia in the 1950s

**Mike Dobson** has created an extensive layout in his cellar.

Continued from the May 2018 issue.

## Layout planning

The space available for the layout was a basement approximately 42' x 12' (maximum) and a connected garage about 15' x 15'.

With a lower than normal ceiling height, any multi-deck plan was not possible, and a width of only 12' restricted the intended main line to a simple loop round the basement, with a minimum radius of 5'6" – acceptable for a secondary single track main line. The two junctions would fit down each long wall, while the return loops would fit in the garage, above each other, including the connecting ramps between them. With the garage ceiling being much higher than the basement, there would even be room for a third level above in the garage should I ever run out of projects (no chance of that in the foreseeable future!).

Subsequent radius checks confirmed suitable locos could cope with a 3'6" curve to the proposed *Kettle Landing* branch down the middle of the basement and still allow an adjacent branch with logging/mining industries to be reached (surprisingly) with only 2' radius curves from the other side of the basement, although, as with the prototype, a check rail proved very helpful to improve the reliability, and most of the sharper curves were hidden for better realism.

With each station having to be almost adjacent to its own return loop, and therefore at different levels, there was a real need for a significant grade between the two, providing the sought-after helper operation. There were also grades on both branches from their respective junctions, because they needed to terminate at an identical height to enable the paired industries to exchange freight cars.

I initially wanted the layout as near to my eye level as possible, but it was obvious this would lead to operating difficulties with more distant tracks, and also lack of realistic clearance under a couple of low support beams, so the maximum height of the layout was eventually set at 57.5" above the basement floor, with the lowest part being 52" above the floor.

The perennial problem of aisle width reared its head at this point. It was apparent that if the ideal width to allow operators to pass each other (say c.3') while following their trains was employed, there would be such a simple track plan with so little operation and so few operators that it would not be necessary to have many operators pass each other at all!

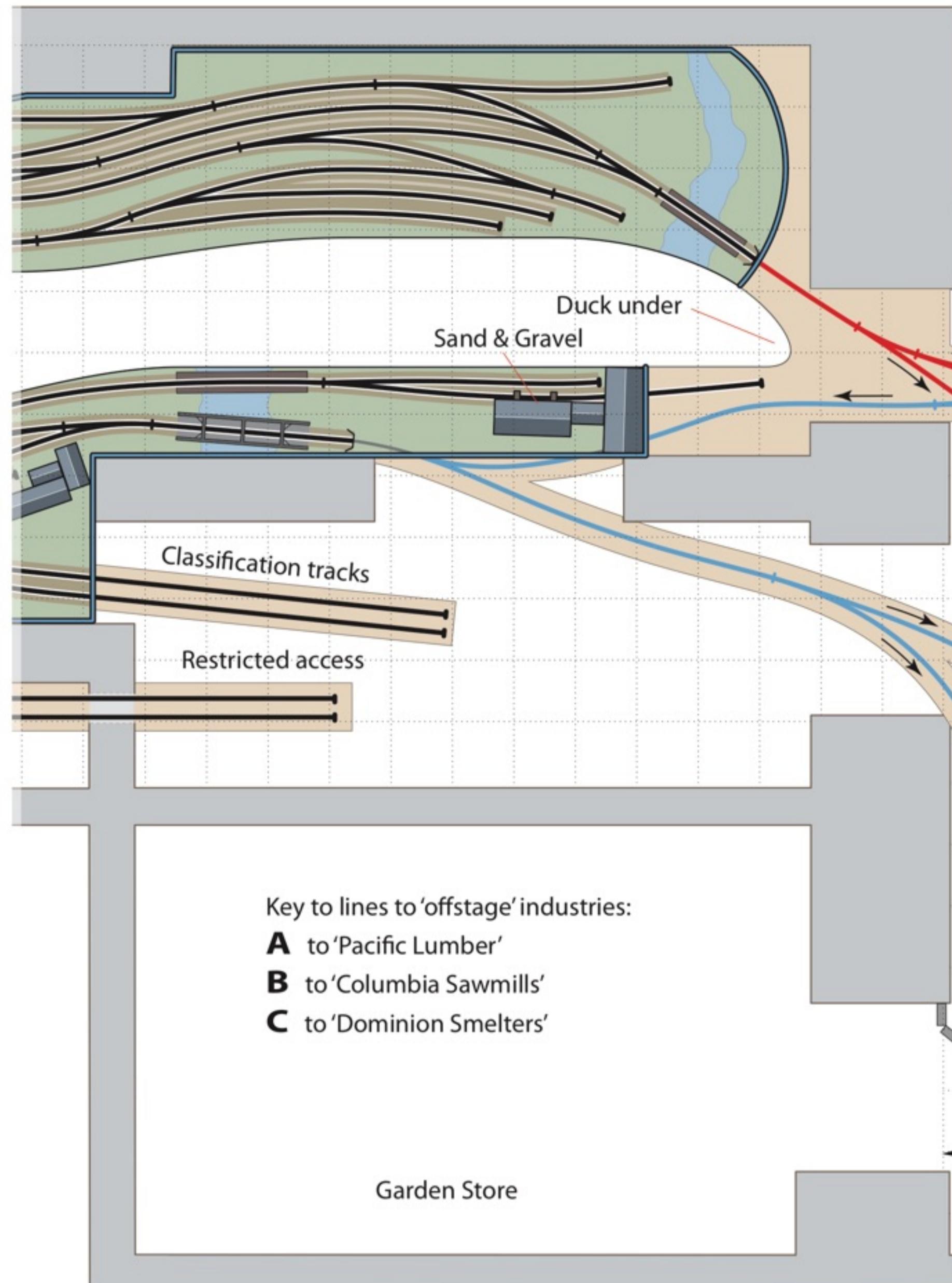
Also given that there were no other potential operators living nearby, I did expect that as well as building the layout



*Land of the Osprey*

Above right  
Great Northern H4 Pacific  
#1477 and 08 2-8-2 #3388  
at Boundary Springs.

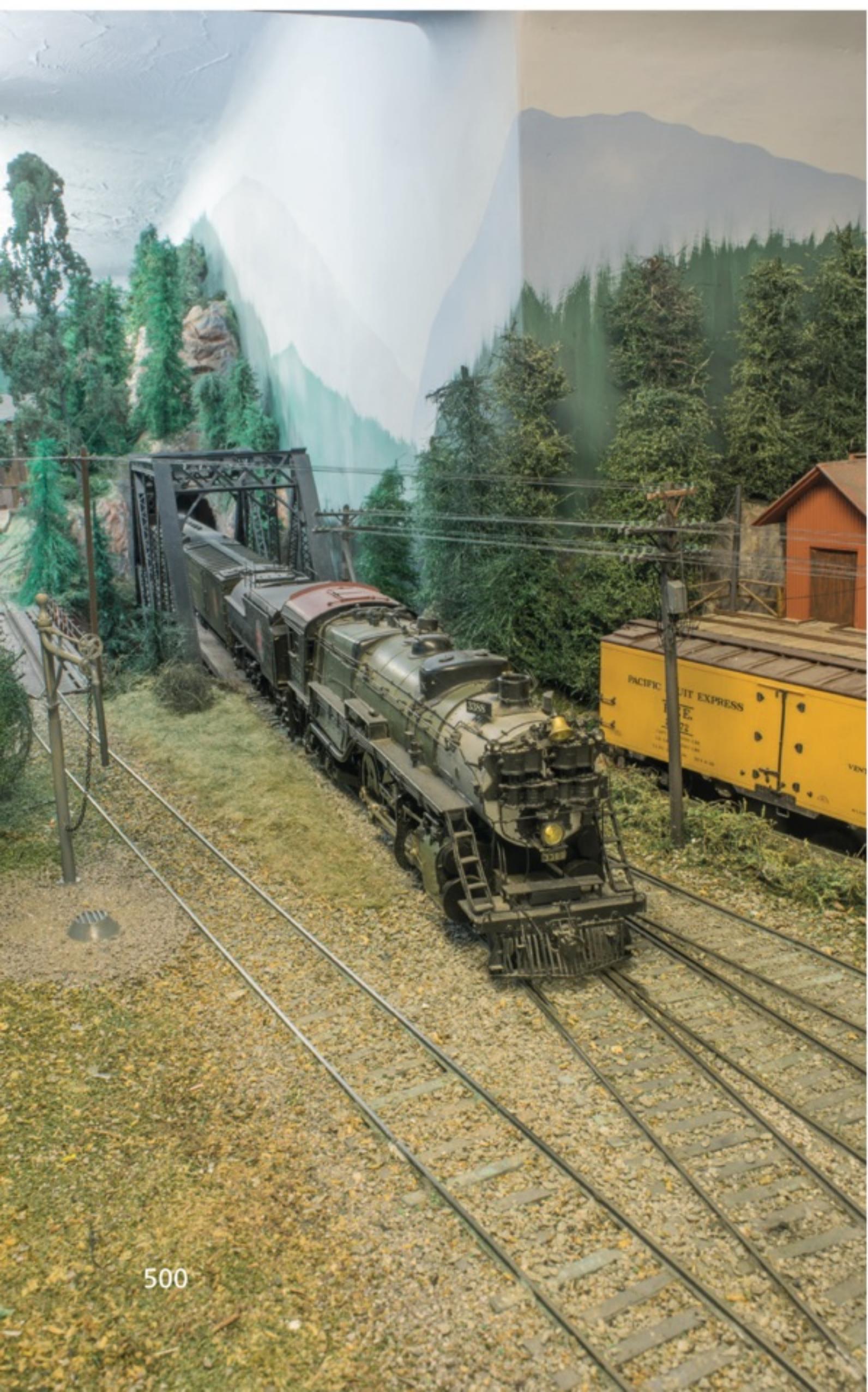
Right  
The main aisle between  
the sand and gravel loader  
(on the left) and Montezuma  
(on the right).



alone I might have to operate it alone for much of the time, although that particular fear later proved unfounded.

In any case, at the start we would be operating on DC with tethered controllers, so the plan was to enable each operator to control a succession of trains within his reach, with the handover rule being that trains between locations would normally be controlled by the operator the train was moving towards. Being designed for single-handed operation, the whole layout could be operated from any of the controller locations.

Some wishes that could not be included because of lack of space included a loco depot, and also any significant scenic items beyond the railway boundary, such as garages, stores, houses, etc., all in the interests of maximising the operational fun in the space available. It was also recognised that the lack of space for appropriate buildings would necessitate most industries being treated as 'off stage', i.e. behind a hill or line of trees, etc. So not only does this apply to the paired industries but also even the yard itself, where most of the tracks, including the yard lead, disappear 'off-stage' to provide more storage (and hence operational interest) than the space and scenery would typically allow.



Above

**The main part of Boundary Springs.** Note the valance with lighting and the fascia with track diagrams, section toggle switches, wood knobs for the train order signal, black rotary switches with LEDs for the turnouts, and pockets for the car cards.

Left

**Great Northern 08 2-8-2 #3388** leaving the lower storage loops en route to Boundary Springs. The spur on the left serves the sand and gravel loader.

### Construction

I started in late 2000 with the return loops in the garage. It was quite tricky to fit in all ten loops (three upper return loops, three lower return loops, two up ramps, and two down ramps) since there were multiple composite curves, all at around head height or above (the garage floor is 12" lower than the basement floor). Having spent a career in IT, you would have thought I would use a PC-based layout planner, but I was impatient to proceed, relying instead on a plank with one end swivelling on the top of a step-ladder with the aid of a screw at that end and at the other end multiple holes every couple of inches for radii from about 5' to 7'. It meant moving the ladder every time a curve radius changed, and climbing back up onto the baseboard in question to continue to draw the new planned curve. There were also the extra complications of not only planning the two levels, but also ensuring the ramps between them connected satisfactorily. The exercise over several weeks did much for my fitness if not for my sanity.

With each loop holding two full-length trains, the result was off-stage storage for up to twenty trains, with each return loop leading to a single track into the basement, the lower track leading along the left wall, and the upper track along the right (as seen from the garage).

As I wanted the orientation to be such that when facing the scenery, west was always to the left, and east to the right, as on a map, this meant that the downhill direction would be to the west. Looking at maps of the Boundary area, there was a westbound grade descending from Eholt down past Boundary Falls, but since the modelled scenery



and stations could bear no resemblance to their full-size counterparts, it was decided to rename them respectively as Montezuma (a name picked off a map of the general area) and Boundary Springs.

Wood for the supports, track bed, etc., mostly came from whatever was left over from the recent extensive renovation, with only the occasional sheet of plywood and support timbers being purchased. In the basement, the baseboards are generally cantilevered out from the walls (except for the central peninsular, of course) to improve under-baseboard access and allow storage by minimising the number of support legs alongside the aisles.

I was initially concerned that this might sag in due course, so I climbed up onto the structure frequently in the early days, particularly when laying track to the rear of the boards, painting the backscene, etc., with no apparent problems.

Given the odd shape of the layout to maximise the floor space, the likelihood of ever being able to dismantle it and resurrect it in a new home was very remote, so no time was wasted building it in sections.

The basics were completed within a couple of years or so, although only consisting of bare boards and track. But this allowed some rudimentary operations to begin, to test the design and build, before detailed scenery would make further changes far more complicated.

A fascia, painted matt black, was attached to the benchwork all round the layout, and the track layout was marked in schematic fashion on this. Curtains were installed beneath, not only to hide all the stored boxes but also to help focus attention on the layout itself.

Above the fascia a valance, also matt black, was attached to the ceiling, which was also painted matt black above all the operating aisles between the valances. Between each of the main scenic locations, the fascia was increased in height to meet the valance, so providing a visual break between the scenic locations. This helps to increase the apparent distance between locations, which are otherwise unrealistically far too close, but still far enough apart to allow switching to take place at both ends of the single track sections without any interference.

**Above**  
**Boundary Springs seen from the other side, showing how the layout had to fit around the shape of the cellar.**

**Below**  
**The depot building at Boundary Springs.**





A series of fluorescent tubes was installed behind the valance to light the layout, with shorter tubes being used where necessary to follow any curvature in the valance more closely. I prefer 'cold white' tubes which seem to my eyes to better capture that clear white light usually seen in the mountains, rather than the more popular warmer whites. Were I installing a lighting system now, I would probably choose LEDs instead.

More recently, strings of blue LED lights have also been installed behind the valances for a reasonably effective night time scene. Many buildings, lampposts, and several vehicles have had lighting installed, but many more have to be completed before I feel we can satisfactorily operate the night mail, etc., realistically.

Above

**#3388 brings the passenger train into Boundary Springs where the local passenger hauled by Pacific #1477 is held for a meet before following the through train up the hill to Montezuma.**

Below

**Montezuma depot at night.**

Below right

**Boundary Springs depot is also illuminated after dark.**

Night photos by the author.

### Trackwork

For *Kettle Landing* I had used Marcway soldered track components and code 124 flat bottom rail, and this was continued. There is some custom pointwork (three-way and curved points are excellent space savers) and some hand-built pointwork, while flowing curves between them can be constructed with ease, including easements where possible. Copper-clad sleepers were cut to length and soldered to the rails; every fourth sleeper is copper, the rest being wood. This system may lack detail compared to the latest products, but this can largely be disguised. The appearance of the tie bar was improved by turning the 'sleeper' on edge (to look more like a tie rod) before re-soldering it back to the blades using an L-shaped brass bracket for improved strength.





Above  
**GM-EMD GP9 #8639 pulls forward onto the yard lead. The working switch stands are very useful indicators of the turnout settings, which can be difficult to see given the height of the layout; they can be seen from much further away along the aisle than the LEDs on the fascia adjacent to the switches.**

The closer spacing of the sleepers either side of the tie bar made the later installation of working switch stands that much easier and more realistic.

All blades are isolated from the frog, and bonded to their adjacent rails, so avoiding any shorts from the backs of metal wheels touching the open blade. This was particularly important as in the early days I had to find equipment from many different sources with different wheel standards.

It was originally planned to repeat my previous method of changing points – the widely-used wire and tube method controlled by a two-way slider switch which also changed the frog polarity. Over half the points were set up in this way, but with the layout now far higher than before, and with the three-way points in particular, it was not entirely obvious which direction was set, or which of the pairs of overlapping blades had to be moved in which sequence, depending on the road selected – another good reason for starting operations before proceeding too far with the scenery. So I decided to change over to Tortoise switch machines controlled by rotary switches mounted on the adjacent fascia. The two sets of switching contacts are only rated at 1 amp each, which initially caused me some concern, with many of the locos drawing up to 3 amps under full load, but with both sets of contacts dedicated to this purpose the shared current is mostly within the guidelines. There have only been a couple of failures (from over sixty units) in approximately sixteen years, so they have proved their reliability.

The rotary switch circuits use diodes enabling route selection where required, with LEDs on the fascia indicating the route selected – greatly advantageous when the actual switches were now becoming increasingly hidden from view as the hills and foliage started to take shape.

Another change from the previous layout was to replace the electro-magnetic 'under the track' uncouplers. I think the extra height of the O scale rail and sleepers meant the magnetic field was too weak to reliably generate sufficient sideways pull on the coupler tails, so permanent magnets were used instead at strategic locations, glued together in threes to increase the pull. These are raised as required on a hinge, attached by a thread through the fascia, and return to the lower, non-operational position by gravity when not required, to avoid any accidental uncoupling. After many years of operations, some operators now prefer to use uncoupling 'pencils', which are actually spade shaped at one end to force the Kadee couplers apart anywhere on the track, even working satisfactorily between passenger car diaphragms.

Once a ceiling-high backscene was installed down the central peninsular between the adjacent branch terminals, the three paired industries each side of the backscene also required additional protective circuitry, to ensure that only one of the paired industries could be accessed at any one time. To request access to an industry, the operator has to ensure the relevant green LED is lit – if it is red then the 'paired' industry is already in use. On eventually getting clearance, the operator must then check to ensure the relevant track does not already contain any outbound cars before dropping off his inbound cars.

The lumber camp/sawmill pair share a single track, with access via a point controlled by the relevant industry in control, while the coal mine and ore mine are also single tracks, sharing a two-track sector plate, with the shared industry being a two-track smelter. The sector plate itself is controlled manually by the operator pulling on a cord through the fascia, on gaining a green LED.



Above  
**Canadian Pacific D10 4-6-0 #1031 coasts down into Boundary Springs yard with a westbound freight.**

Below  
**GM-EMD GP7 #8420 seen switching at Kettle Landing. The track diagram on the fascia has switches and LED indicators for the points.**

### Controls

With DCC still being in its infancy at the time, the layout was originally wired for traditional DC operation. Every section was connected to a two-way centre-off switch orientated to move either left or right. A twelve wire heavy gauge bus was installed round the layout, and six 12 volt power supplies were installed, each connected to a pair of wires on the bus. Twelve-way plug-in points were scattered at various locations along the fascia into which any of six hand-held controllers with a unique pair of leads, identified as C1 to C6, could be plugged. Then at each of the six major locations (lower return loop, Boundary Springs, Kettle Landing, Montezuma, the mining/logging branch, and the upper return loop), a left and a right rotary switch (two-pole, six-way) was installed to select one of C1 to C6. This combination of selections determined which controller each track section was connected to when switched to the left or the right. This was simpler in practice than it sounds!

With suitably located uncouplers at section breaks, it was possible to control both the coupling together of locos for double-heading up the grade and then uncoupling of the helper at the top of the grade. Alternatively a rear-end helper and train loco could be still individually controlled by switching sections from the train loco's controller to the helper's controller at the relevant moment.

So a few years of satisfactory operation ensued, with my relatively meagre selection of locos and rolling stock from my previous branch layout being kindly augmented by the loan of many suitable items from friends.

But with the steady improvements in DCC, particularly sound, it was looking increasingly attractive. Choice of a system was delayed until I was sure that there would be a UK-licensed radio handset, and that it would be able to handle relatively heavy O scale currents.

I must record here my sincere thanks over many years to Ted Smale of Sunningwell Command Controls who has patiently answered all my questions both before and after taking the plunge into DCC.

Eventually a Digitrax system was chosen, initially using tethered throttles, and we simply replaced the C1 DC controller with DCC. Their Loconet system was installed, connected to various plug-in sockets around the fascias, where several locos could be independently controlled subject to the maximum 5 amp capacity. We had very few DCC locos to begin with between us, so there were very few mistakes in connecting locos to the wrong controller, and no permanent problems were experienced following such errors. I guess we were lucky, after since hearing about other motors, decoders, etc., often being damaged in such circumstances.

One unexpected benefit of DCC sound was it encouraged more realistic operating speeds, as it exposed those of us prone to be a little heavy-handed with the controller!





The pendulum fairly quickly swung towards DCC as more sound-equipped locos started to appear on the layout, so further power districts were gradually introduced. Loconet was expanded to introduce the long-awaited radio receivers (one each end of the layout for more reliable transmissions) with more radio throttles to match.

We then started to get all sorts of problems, having simply added more power to the original wiring, so it was decided to leave just one DC controller (C6), mainly for testing purposes, and use the rest of the redundant DC bus wiring to beef up the DCC wiring, with the aid of a special DCC meter (a RRampmeter) which also highlighted several track sections needing further wiring improvements.

All the original section switches and rotaries are still in place, but the latter are now restricted to just two positions, either ON or OFF. Power to the left rotary is DC, and power to the right rotary is DCC. During normal operations, the DC controller is disconnected for safety, and all the original section switches installed for DC are set to the right so all sections are all live to DCC but can be immediately isolated to quickly establish the whereabouts of any DCC problem.

Given the number of sound locos in use at any one time, the system has now expanded to five power districts, with a spare controller and booster in the event of a catastrophic failure during an operating session. (A career in IT has taught me the importance of avoiding any single critical point liable to failure if at all possible!) The controller itself is not used to provide power to any of the five power districts, so only needs to concentrate on being purely a controller, hopefully for better responses.

Another unforeseen bonus from DCC has been the ability to store more than two trains in each loop (subject to their combined overall length), facilitating the introduction of further trains to the operating schedule.

The latest development has been to install a dedicated PC in the basement running Decoder Pro to help keep track of all the locomotives and their decoder settings, so that we can gradually move towards standardised settings, further improving operations regarding matching of speeds for helpers, consists, etc.

After my son (also in IT) had completed the above installation, he also had a brief look at the included Wi-Throttle software and seemingly in no time at all was controlling a loco from his mobile 'phone. Scary for us old-timers!

We have no plans to convert the points to DCC, as it seems much quicker to continue to use a rotary on the fascia rather than look for then dial up the identity of a point.

Recently, I have started to install unlit switch-stands operated by the tie bar, not only for improved scenic realism but also for an indication of the route setting, particularly useful when the relevant LEDs on the fascia cannot easily be seen if some distance away.

**Above**  
**Approaching Montezuma:**  
**three-truck Shay #6 slowly**  
**climbing the branch from the**  
**logging camp is overtaken**  
**by Fairbanks-Morse**  
**Trainmaster #8912.**

Photographs by the editor unless otherwise noted.

**Below**  
**The aisle between the mine**  
**and logging peninsular (left)**  
**and Montezuma (right).**

... to be continued.





Above:  
A loco-hauled main line train  
and the branch railbus  
connect at the pond station.

Right:  
The Wismar railbus  
pulls up to the platform.



**Doug and Jenny Thompson** took time to plan before eventually starting work on their garden layout. *Photographs by Paul Bason.*

# Thurnsfeld

Large scale fun in the garden

## Introduction

This railway has been built in our garden over the last five years using LGB set track, and has gradually evolved, as most layouts do. It is not based on any particular place, or country, or period. As most of the rolling stock is LGB and mainly continental, the railway follows some form of continental working practices. The layout could be likened to a preserved railway, running whatever rolling stock is available, like the Nene Valley Railway.

When we moved to our current house about six years ago, one of the things we considered was whether the garden could have a railway. Within a few weeks of moving in, some track was simply placed on some of the paving slabs and some trains were run, just so that we could get our rail 'fix'.

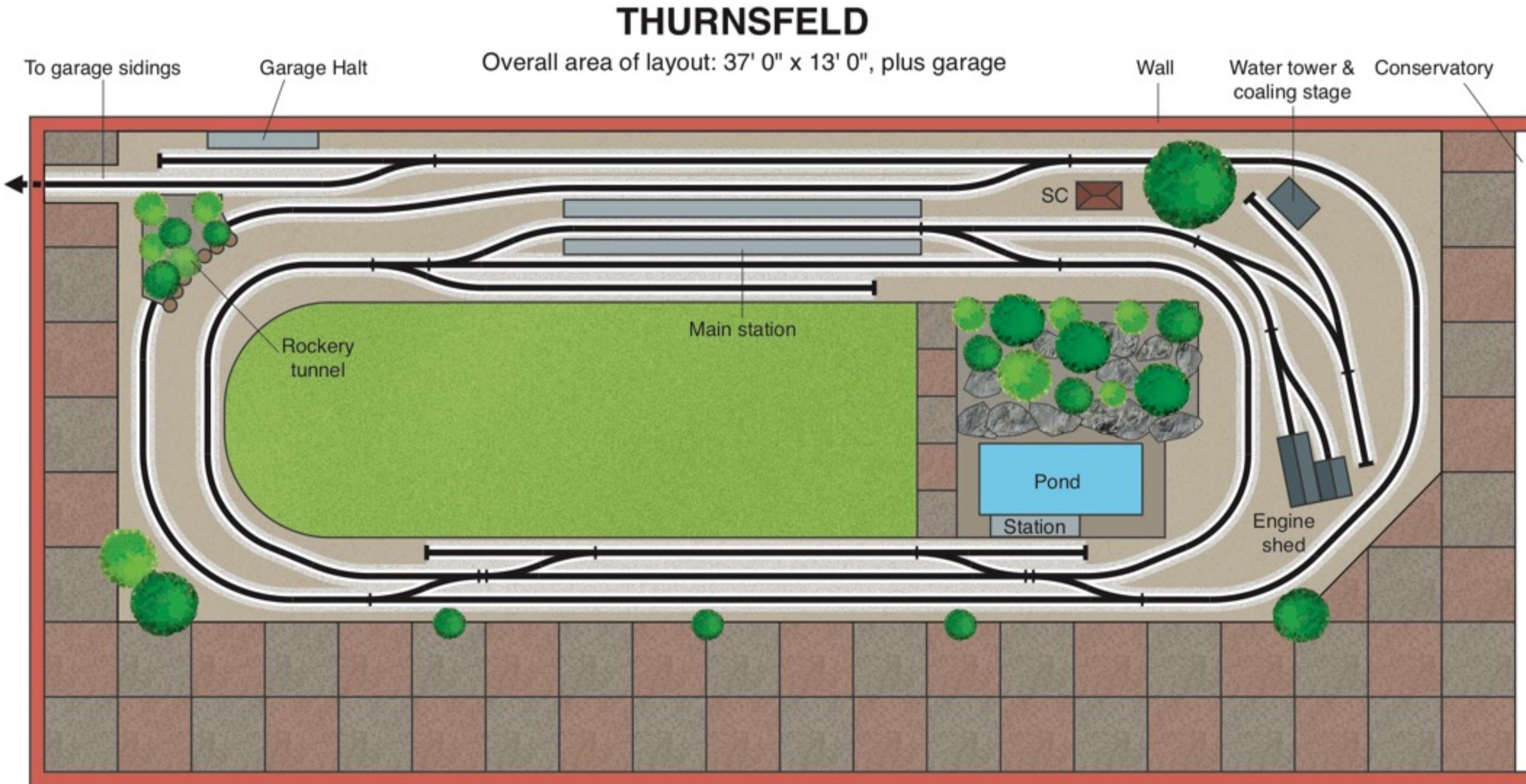
When thinking about the layout there were two things that we wanted. One was to set two trains running round separate loops and watch them whilst enjoying the British summer. The other requirement was that there had to be plenty of scope for shunting and different train movements. A station was a definite, with a passing loop on each track so that four trains could be on the layout at the same time.

Over the next few weeks the basic layout was sketched on scraps of paper until a potential design was decided. Then the bits of track we had were placed in the garden to see if the layout translated from the paper to the real world. Many different ideas were tried; some worked, and some did not.





Above  
The main line train at Thurnsfeld Hauptbahnhof. The colour light signals have been mounted on tramway catenary masts.



Below  
The garden layout seen from the house.



Below  
The layout looking back towards the house.



Having decided on a station at the pond, there needed to be another station somewhere else, probably on a branch line, so that a shuttle train could run between them. Where this other station was going to be was not clear in the early stages.

## Description

The layout today is much as originally planned, although on the outer loop it has not been possible to include a passing place at the station for a second train to be held whilst the other runs round the loop. This is because that the space was needed to allow the trains to climb the incline to the branch station by the garage and enter the storage sidings.

There are unlikely to be any major developments to the track in the next few years.

The layout consists of two main running circuits, with a main station with three platform tracks, and one siding off the inside loop. On the pond side is a third track that can be a passing loop as well as providing access to the pond station. The other branch station is by the garage, accessed from the outer loop up a steady incline. A junction just before this station provides access through the 'train flap' to the four storage sidings.

## Building the layout

When we started the layout, we put the track straight on the grass – and then realised how uneven this was. So another method was required to provide a stable trackbed, but our building skills were limited. At one end of the garden was a fairly flat patio area, and it was decided to keep half of this. The other half of the slabs were used for the path, which was moved closer to the wall. The small slabs from the original path were used to form the base for half of the main station area. We needed to build the rockery by the garage, which included the tunnel. From our local garden railway shop (Glendale Junction in Market Deeping) we purchased secondhand some pre-formed strong plastic bases for the third radius curves, which gave an idea of the size of the rockery. Then, using bricks that were spare in the garden, the two retaining back walls were built, one layer at a time.

The next stage was to build the side walls for the tunnel. It was decided to use some of the spare slabs as walls and these were cemented into the ground. This does give a very large tunnel opening, some 12", but this allows for rolling stock of any height to pass through the tunnel.

As the tunnel is on a curve, the next challenge was how to put a roof on it. It was decided to use marine ply, and to make sure that it stayed in place we put some small 25mm x 25mm wood battens each side of the slabs to hold the roof in position. Then some marine ply was cut to form the walls at the end of the tunnel – these may eventually be properly painted and made to look more realistic. The tunnel roof was covered with plastic, and a log was placed to form the front of the rockery. After a day or so, the earth was slowly put in and pressed down. Then it was off to the local garden centre to find some alpine plants which we hoped would thrive in the rockery. To date there has not been any problem with the tunnel, and it has stayed dry inside.

We purchased some pre-cut reinforced plastic curves and straights for the track to rest on; these were put in place for



the main station area once the slabs had been finished, and then they were joined to the curved sections at the rockery. Small stones were used to try and level the track bed as much as possible.

On the other side of the loop to the station area was a long straight run which it was decided would be three tracks wide, one for each circuit plus a passing loop for access to the pond station. We purchased more reinforced plastic boards to put the track on. We dug the ground on which these were to be placed, and they were laid and levelled using more small stones.

Above

**Pride of the line at present is a massive Harz 2-10-2T, which has been designed to run even on the LGB small radius set track curves.**

Below

**The yellow diesel is from the LGB digital start set, the red one is the LGB track cleaner, and the steam loco is from an American Christmas set.**





The track is not fixed down to the bed, but it is all clipped together.

Structures and details, such as loco shed, stations, signals, etc., would be added later as time and money allowed.

After a couple of years, the three track section by the path was causing problems as the boards were not staying level and, despite our best efforts, were not proving easy to level. The 'real' railway lays track on ballast, so the decision was taken to put all our track on ballast. This involved purchasing a large quantity of small stones and some metal holding bars. When this all arrived, all track was lifted, all weed mat was checked, and then the stones were slowly poured into place, with the metal holding bars being held in position either by tent pegs in the earth or screwed into the slabs. All this had the desired effect of levelling the problem section as well as having all the layout on one type of ballast. It must be said that when the stones were first laid they looked very white, but after a year or so they faded to a more realistic colour.

### Operation

Operation is fairly basic, even though we work with a digital system. Initially we used LGB equipment, but as the number of locomotives grew, this could not cope. At this stage we decided to upgrade to the Massoth system.

With digital control, all that is needed is two wires to the track. All the points are live frog.

With the LGB system the controllers were initially linked to the control box by wires, which had the limitation of not being able to walk around the layout to see what was happening at the other end of the garden. So LGB wireless adaptors were purchased, giving the freedom to move around as required. This was important for the first couple of years as the points were worked by hand. Before upgrading to the Massoth system, all the points on the main running lines were changed to electric so that they could be controlled by the handset.





Above left

**Main line trains passing at Thurnsfeld Hauptbahnhof.**

Above

**The Harz 2-10-2T passes an Austrian 2095 B-B diesel. Low growing alpine plants were carefully selected for the 'scenery'.**

Left

**As the diesel-hauled train emerges from the tunnel under the rockery by the garage, a French Corpet,Louvet 0-6-0T climbs the gradient towards the storage sidings in the garage.**

It was also a great desire of mine that the layout should have working signals. Three LGB colour light signals were purchased which could be linked with the points, providing some indication of which way the points were set. After a year or so, some small O gauge two colour LED ground signals were noted at Glendale Junction and after discussion with the owners we worked out that these could be used on the layout and linked with the points. We had to work out how to raise up some of these signals as we did not want all of them as ground signals. There was an attempt to build a gantry to hold the lights at one end of the station, but this failed. In the past we had purchased some catenary masts for overhead line when a tram layout was being considered – we tried fixing the lights to them, and this was successful, although careful positioning of the masts was required to make sure no rolling stock would foul them. So far, half the layout (the station side) has working colour lights, which are only used to show what route is set. The colour lights are either red and green or yellow and green: the yellow and green ones are used to show if the main or secondary route is set, while the red and green lights show if the points are set for or against the direction of travel. This is a compromise as to have the colour lights working properly was too difficult for us.

### **Rolling stock**

All our motive power and rolling stock is ready-to-run, mainly from LGB, but with some Bachmann items.

The locomotives are steam and diesel; we do not have any electrics as we do not have any overhead – I do not like to see electric locos on a layout without the overhead masts and wires in place.

When running trains, we try to match locomotives with the appropriate stock, but this is not always possible as our collection is not that large.

Many of the coaches have people inside and interior lights. Some of the coach lights pick up from the track and some are battery operated. Most of the locomotives have working headlights, and many of the brake coaches have a tail light (or two). This means that if we have a running session at night, with the coach lights, the signal lights, and two yard lights, the layout takes on a whole new feel.

We also have Thomas with Annie and Clarabel as well as a couple of 'troublesome trucks' for the grandchildren to use when they come round. We have also adapted two special wagons: one for the princess to travel in has a glitter floor and bunting, and we have Nemo in a fish tank on another wagon.



Above  
**The station by the pond on the path side of the garden.**

Below  
**The platform and canopy at the pond station. All of the structures can be removed for storage over the winter.**

### Buildings

We had to make sure that the buildings could cope with the British weather. We purchased a couple of kits, a signal box and a two track loco shed. These spend the summer outside, but during the winter they are stored inside. Also recently purchased was a concrete waiting shelter for the country station, which we have painted. A couple of small platform sections with a canopy are used for the pond station at the moment.

### Scenery

When choosing plants for the garden, care had to be taken to make sure that these did not grow too big. After reading

a couple of books, the general practice was to use alpine plants, as these do not grow too tall. A number were purchased, smaller ones to go between the path and railway and a couple of larger bushes to protect the layout from my clumsy feet as I enter the back gate. In the rockery, we put in plants we liked. By the garage we have a couple of shrubs that spread and are growing over the edge of the rockery to take away the sharpness of the straight edges. All our planting has been trial and error, but fortunately there have been few errors.

### Future development

The next two projects are the installation of the second half of the signal lights, so that the main running circuits are fully signalled. The platforms in the station will be replaced with permanent concrete ones. Allowances will be made with the new platforms so that lighting can be added later. At the moment the layout only has two lights, in the loco yard, but eventually the idea is to light all the buildings to add to the overall effect when running at night.

### Conclusion

This has been a great learning curve so far and we have been lucky in having very few failures and most of the ideas that we had have worked. This is a great layout to work as we can both have fun, either shunting or just watching the trains go round whilst sat in a garden chair.

The knowledge we have gained from our layout has helped us to develop, with others, a garden railway at Wansford on the Nene Valley Railway. This is a more basic system, being two separate loops and an independent branch line running on an automatic shuttle system. This layout is open when the volunteers can get there to run it.





Above  
**The garden railway layout at Wansford station on the Nene Valley Railway.**

### From Jenny's point of view

I have been interested in trains for as long as I can remember. My younger sister and I had a OO layout built for us by my dad when I was about six, with lots of rolling stock, and I have childhood memories of 'playing trains'.

We lived not far from London, so once I was old enough on many a school holiday day I would cycle to our local station and take the train into London to have a day riding around on the Underground.

As children we also visited lots of model villages, and I loved the idea of a train running through the garden, but as mum was a very keen gardener and wanted all the room for her plants, this was not going to happen in her garden! Little did I know that one day I would have my own garden railway.

When Doug and I moved into our new house together, it was a foregone conclusion that trains would figure heavily. He had already had a small G scale garden layout at a previous house so had some idea of what was involved, but it was all new to me.

We did not rush it but gave ourselves a year of planning how we wanted our layout to look, what we wanted to include, and how we wanted to run it. This involved trying lots of different configurations in the garden to see what could work and what definitely did not. We visited different layouts to get ideas – it was quite useful to see what we did not like as much as what we did.

Once the layout was decided upon, we had to work out what we had to do to achieve it. This involved teaching ourselves a few new skills. We needed to build a brick wall to retain the earth that was going to cover the tunnel. I had not mixed cement before, but it is not unlike making cake mix!

Removing quite a lot of the paving slabs built up some muscles that I did not know I had, as I am not very tall and

did not have the same strength as Doug, but they needed to be moved. Digging a pond was another job that I had not done before; at the time it seemed very deep, but our fish seem to appreciate it now.

Gardening and planting was something that was familiar to me, but making sure that we chose plants that would not grow too big was essential. A lot of it was guesswork, but we do not seem to have had too many failures or mistakes.

Something else I did know about was wiring and soldering, as in a previous job I worked for a company that manufactured wiring looms and assembled printed circuit boards. Once the rust was scraped off my soldering iron and soldering skills, I have been able to wire the points and associated lights.

I do like all things crafty, so making up building kits or painting figures is something I find very enjoyable and relaxing. I have not had the confidence to make anything from scratch yet, but I am sure that once all the signals and building lights are done I will try my hand at creating an original piece for our layout.

Building the layout together has been, and is, great fun. It helps that we are on the same wavelength and that for us it is a fun hobby.

It is great watching the grandchildren filling the trucks with Playmobil people and dropping them at the next station, or sitting on a warm summer evening just watching the train lights go round the garden.

Within our garden railway group there are quite a few couples that are involved, so as a female I do not feel like the odd one out. But I know from visiting model railway exhibitions that females are outnumbered. That is not a problem for me as we all have an interest in trains, and besides – why should the blokes have all the fun?!

**Rod Shaw** has built a conventional switching layout but in a comparatively unusual scale. *Photographs by the author.*

# Naples Street

An American switching layout – in TT

**H**aving spent a good few years building, operating, and exhibiting an American HO scale switching layout, in mid-2017 I decided it was time for a change. I still liked American outline, and being a fan of TT from my British 3mm scale modelling years, I started looking at what was available in 1:120 scale American TT.



Above  
Naples Street grade crossing bisects the industrial area.

The answer was not much. Certainly not in the UK, or the US either for that matter. The handful of suppliers of American TT are based mainly in Europe and have a limited choice of ready-to-run or kit offerings.

Below  
The GM-EMD SW1200 is from the Czech firm MTB. The tank and the high cube box car are from Zeuke. The flat was made from a kit by Lok-n-Roll, the gondola is a 3D-print from TT Nut.





But a short switching layout with a handful of industries does not need that much stock – nine or ten freight cars would more or less fill it up. And based on past experience, if I bought much more I would not use a good half of it anyway.

So, having sold my HO layout and a good amount of stock, I ordered a few American TT items and started designing a new layout.

**Below**

40' box cars by Zeuke are ideal for a compact layout.

### The setting

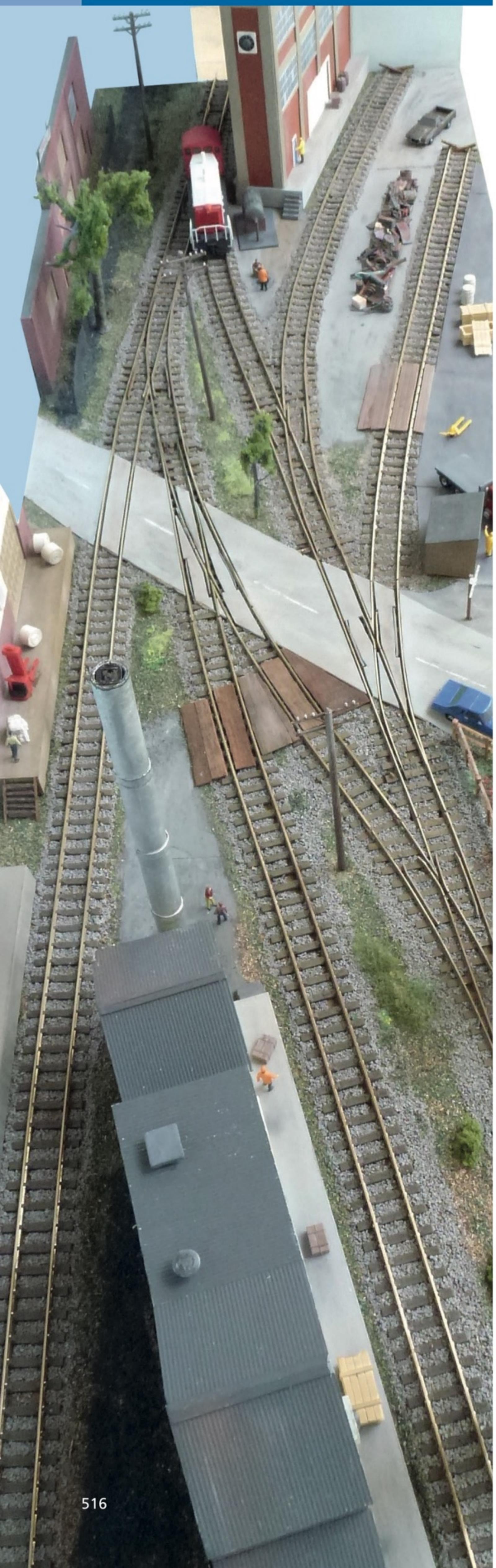
The small industrial park straddling Naples Street is served by a branch of Conrail in the imaginary town of Sorrento, Illinois, in the late 1970s to 1990s. A mixed bag of rolling stock makes an appearance, moved around by a couple of small switchers. Naples Street itself runs diagonally across the tracks.

There is a mixture of cars from different eras and to justify this I have imagined that short boxcars with roofwalks were not phased out in the 1970s but lasted over a decade longer.

**Above**

**The SW8 is also from MTB. Both locos have been fitted with Zimo MX622N decoders and run very smoothly.**





### Track plan

The track plan is very simple – four sidings off a central run-round loop, each with room for two or three freight cars. The layout measures 40" by 11", and the fiddle yard adds another 20".

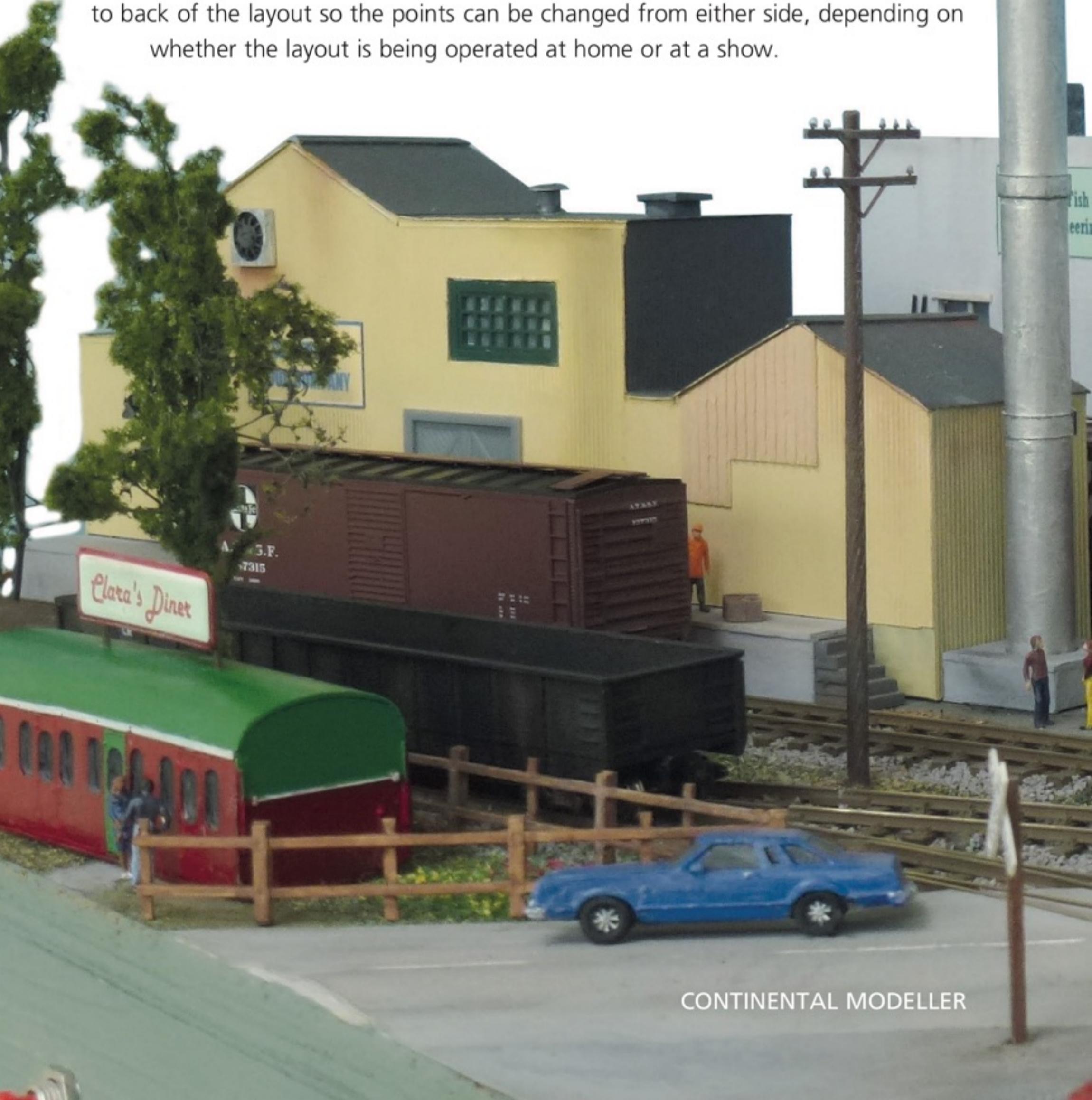
I originally designed the layout without the run-round and with an extra siding but decided to incorporate the run-round for more switching flexibility. As it doubles as a siding for switching anyway, I have not really lost anything.

### Track and wiring

Track is Peco HOM, left over from my 3mm modelling days. The ties (sleepers) are a bit on the chunky side for TT but look acceptable when painted and ballasted. The gaps between the ties would be too wide for a main line but are fine for a set of industrial sidings.

The turnouts are a combination of Peco and hand-built with radii between 15" and 20". They all have live frogs, and, like the track, are mostly left over from previous layouts. The exceptions are two turnouts I built myself, and which, much to my surprise, work without problems.

All turnouts have their switch blades electrically bonded to the adjacent stock rail, the hand-built ones by design, the Peco ones through modification. This means that all frogs need to be electrically isolated from the blades and that their polarity needs to be switched according to which way the points are set. This is done by using under-baseboard push rods, each of which works a microswitch as the points are changed. The push rods run through from front to back of the layout so the points can be changed from either side, depending on whether the layout is being operated at home or at a show.





## Buildings

American TT buildings are also in short supply, whether ready-built or as kits. A couple of card kits are available from Zeuke (a station building and a small warehouse), and you may be able to get away with HO scale kits from the likes of Walthers or Pikestuff, as these are sometimes under size for 1:87 and close enough to 1:120 scale to be acceptable. The most crucial factor is probably door height – if it is around 16.5 or 17mm, or you can make it so, it will do.

Short of finding what you want, one alternative is to buy TT plastic kits from European companies such as Auhagen and convert them to something a bit more American. Another way is to download printable kits in PDF format from one of the many suppliers and print them out at the right scale.

Alternatively, you could scratchbuild. This is not that much harder than cutting out and assembling a downloaded printable kit. Embossed plastic sheet and corrugated paper give a more three dimensional look than flat prints, and you also have the advantage that you can make something to suit the space you have, based on any prototype for which you can find pictures.

The buildings on Naples Street were made from a combination of embossed plastic brick sheet, plastic strip, card, and corrugated paper, painted with various brands of acrylic and enamel modelling paints, and weathered with pastels. Most of them are loosely based on photos of actual buildings in Illinois. Some are detailed with left-over HO windows and doors from Walthers kits. The personnel doors and some of the factory windows were designed using Microsoft Excel.





The only buildings I did not make myself are Clara's Diner and the small workmen's hut, which are 3mm scale resin castings. They are not prototypically American but I like to think they more or less blend in.

The oil tank outside Napoli Distribution is one of two from a Ratio N scale kit.

### Road vehicles

The road vehicles are 3D-prints from Shapeways: a Ford Thunderbird and a Ford Ranchero from Madaboutcars, and a Mack truck from Stony Smith Designs.

All the American automobiles which Madaboutcars featured on the Shapeways website came in pairs but when I explained I only wanted two, both different, they changed the design to accommodate me. The world of 3D printing has been a revelation.

### Figures

The figures are from Preiser, both painted and unpainted. As the setting is industrial, there are plenty of blokes around wearing serious clothing. There is no call for passengers here but Clara's Diner attracts a few non-railway customers from the neighbourhood because the food is so good.

Above

**Road vehicles are 3D-prints from Shapeways – this Mack truck is by Stony Smith Designs. The hut is a resin kit.**

Right

**The 40' box cars are from Zeuke, while the gondola was made from a 3D-print courtesy of TT Nut.**

Below

**Figures are from Preiser. The oil tank was adapted from a Ratio N kit.**



### Animated details

The chimney at Can-Freeze Food Company was made from rolled-up paper and contains a Seuthe 117 smoke unit wired to a timer, enabling spectators to make it smoke for a pre-set time by pressing a button on the front of the layout.

On cold days, the workers at Napoli Distribution like to keep warm by gathering round a glowing brazier in the yard.

### Backscene and lighting

The backscene is made from 5mm ply, screwed into the back and sides of the baseboard, and covered with poster paint to give the effect of streaky cloud against a blue sky.

The lighting is a self-adhesive LED strip fixed to a removable wooden frame attached to the layout's side panels.

### Locos and stock

The two locomotives are near-identical General Motors – Electro Motive Division switchers, an SW8 and an SW1200, one lettered for SOO and the other for Conrail. The models are by the Czech company MTB and were supplied by Zeuke in The Netherlands. They come with an NEM651 DCC socket which enabled me to fit a Zimo MX622N decoder very easily. They run very smoothly.

Sound for the locos comes from two MRC under-baseboard diesel sounders, which produce the right sound for EMD equipment. A bit of a compromise, but they will do until I steel myself to fit sound decoders and speakers into these small locos. I know it has been done!

Stock consists of two tank cars, four 40' boxcars, and a 60' high-cube boxcar, all from Zeuke; a flat car and 50' boxcar made from kits acquired from Lok-n-Roll in Germany; and a 3D-printed gondola from Shapeways courtesy of TT Nut.

The Lok-n-Roll kits come complete with trucks and wheelsets and just need couplers. Lok-n-Roll supplied these, and also wheelsets and couplers for the Shapeways gondola. I also bought some Bone Valley Scale Models brake wheels from Shapeways.

Couplers are either Kadee 713s or 714s, the difference being in the colour – rust or black. They are intended for HO<sub>n</sub>3 but are also perfect for TT. They work well but are fiddly to assemble and need much patience – the springs used are even smaller than those for HO, and do not come ready fitted. Luckily a few spares are included!

I have decorated all stock that was not ready-to-run with home-made decals.

### Operation

There are plenty of industries to switch. Along the back, over the road from a defunct and forlorn-looking glass factory, we have a printer and an engineer, while to the front are a food processor, a general goods distributor, a scrap dealer, and a team track.

Operating the layout is a matter of assembling a short train of two or three freight cars in the fiddle yard, pulling or pushing them onto their various spots, and picking up cars already spotted, making use of the run-round where needed. Each location can accommodate up to two spots depending on car length.

As there is only one loco in operation at any time, there is no need to give them different DCC addresses and to keep

things simple I have left the addresses at the factory default of 3 rather than assign loco numbers to them.

I do not use car cards or waybills, but for the operationally minded I have written a switchlist generator program, which produces switching moves in list format or in a basic car card/waybill style. If anyone is interested in using it, I can be contacted at: showerydan@live.com

## Conclusion

The June 2016 issue of *Model Railroader* carried an article entitled 'What became of TT scale?'. It started "TT scale has disappeared from our consciousness" and, disappointingly, it made no mention of any of today's small suppliers. While it will most likely always be a niche scale, I hope that a visit to the [ttnut.com](http://ttnut.com) forum will reassure interested readers that American TT is alive. I hope that my small layout will help to bring it back into the general consciousness!

The layout is booked to appear at the Northern Counties American Modellers (NCAM) show in the Memorial Hall, Hollins Lane, Hampsthwaite, near Harrogate, North Yorkshire, HG3 2EY, on Sunday 1st July, between 10.00am and c.2.00pm. Admission – £3.00 for day visitors. Contact – David Nuttall 01943 862425

## Resources for American TT

American TT is rare – even in America – and products can be hard to obtain.

Here are some links I found useful to get me going.

[www.ttnut.com](http://www.ttnut.com) – an invaluable forum with a truly international character, where you will find answers to most of your TT questions.

[www.zeuke-tt.com](http://www.zeuke-tt.com) – Zeuke are Dutch-based suppliers and manufacturers of a small range of locos, freight cars, and buildings.

[www.lok-n-roll.de](http://www.lok-n-roll.de) - Lok-n-Roll are German-based suppliers and manufacturers of a small range of locos, freight cars, and accessories.

[www.shapeways.com](http://www.shapeways.com) – Shapeways produce a variety of loco and freight car body shells and road vehicles from designers such as **Madaboutcars**, **Stony Smith Designs**, and **TT Nut**.

[www.gaugemaster.com](http://www.gaugemaster.com) – suppliers of Preiser TT figures.

[www.auhagen.de](http://www.auhagen.de) – buildings, components, and scenic items for TT, some suitable for or adaptable to US outline.

Other stockists of TT items which I have not yet used:

**Art & Detail** – German supplier of a limited range of US loco and freight car kits, and components.

**Tillig and Kühn** – German suppliers of 12mm gauge track.

**TT-West** – a US supplier of a limited range of decals and kits.

**Siggis** – German supplier of US freight car kits.

**Muellerradsatz** – German supplier of 12mm gauge wheelsets suitable for US freight cars.

Various printable kit suppliers, e.g. **3DK**, **Clever Models**, and **modelbuildings.org**, whose kits can be downloaded and printed at 1:120.



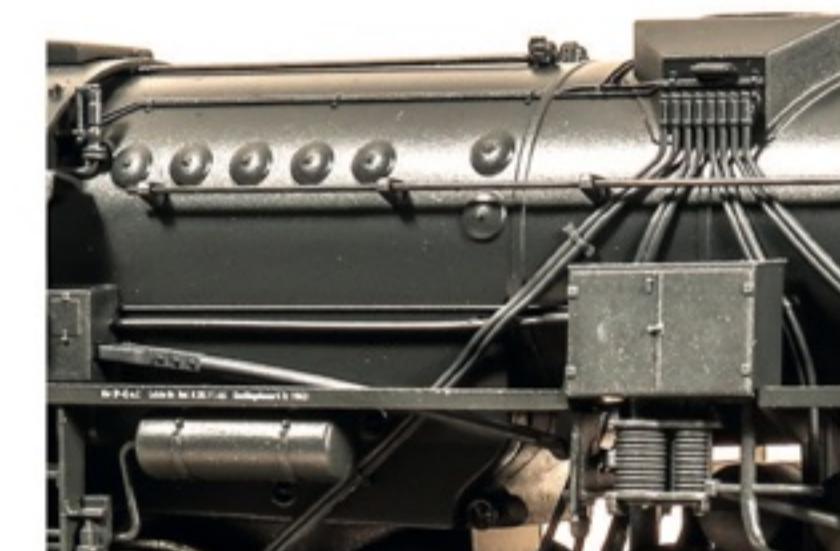
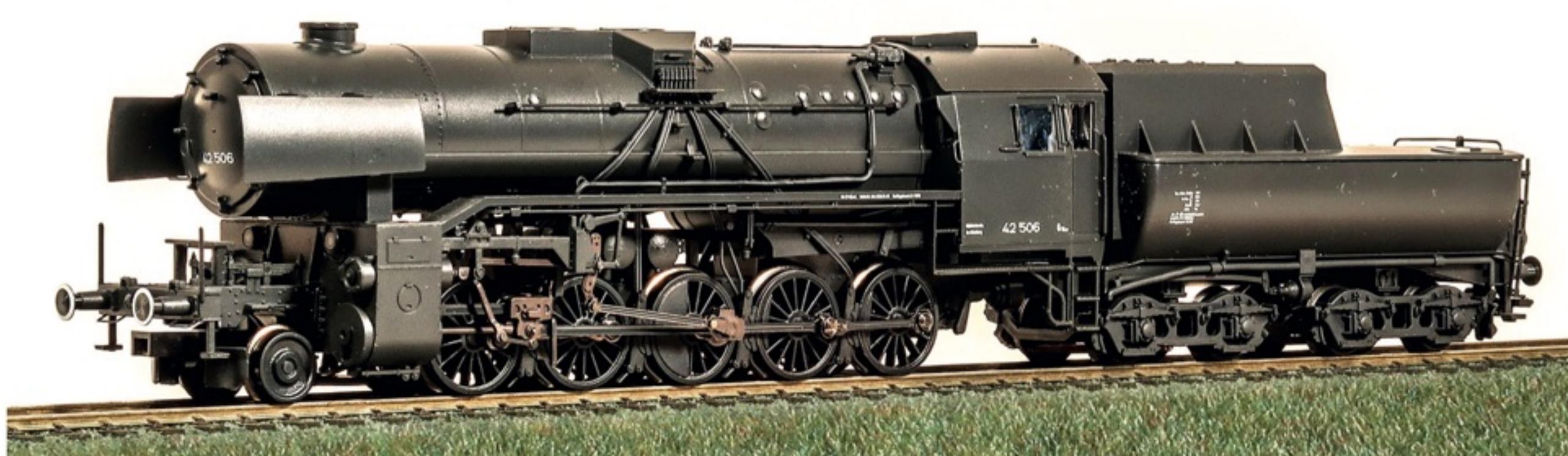
# Reviews

Evaluated by our own specialist staff



HO

## wartime BR42 2-10-0 variant new from Trix



By 1941, a third design of war production 2-10-0 for heavy goods trains was being considered for use on routes in Austria and the occupied territories in Russia, with an axle load of 18t, making use of the class 44 boiler on the chassis of the class 50. From twenty proposals, eventually two were preferred. In contrast to the class 52 *Kriegslok*, which was effectively a stripped-down class 50, the 42 was considerably redesigned. Outwardly, it had the same simple design with a closed cab and only one side window, basic smoke deflectors, and disc wheels in the front truck. But with the lamps integrated in the cylinder block and the short front deck ahead of the cylinders it looked quite different. The arrangement of the domes with cladding was also unusual. The class was rated at c.1,800hp with a top speed of 80km/h.

The originally intended quantities did not eventuate because of the war: in total, 865 were built. At the end of the war there were still half-finished engines in the workshops. So after 1945, 16 were built by Maschinenfabrik Esslingen, 126 were built by Polish factories for the PKP as Class Ty43 (three ex-DRG engines also went to the PKP as Class Ty3), and another 76 were produced by the Lokomotivfabrik Floridsdorf in Vienna – these were not built for the ÖBB but were sold in 1949 to Luxembourg (20 as CFL class 55), in 1952 to Bulgarian State Railways (33 as BDZ class 16), and the rest to the DR and various industrial enterprises in East Germany.

The DB set them aside quite quickly, the last being withdrawn at the end of March 1956, but with the incorporation of the Saarland in 1957 more 42s joined the DB roster. They remained until October 1962 in the Saarbrücken area, mostly used on heavy ore trains and for banking.

The DR continued to use them until 1968, the BDZ until 1988.

Trix and Märklin have backdated their model of the BR42 with significant alterations to represent an Epoch II machine, running number 42 506, in 1944 condition, with small smoke deflectors and a 2'2'T30 tub tender. The front truck has disc wheels, and the front headlights are part of the cylinder block, as delivered, and there is no washout protection sheet under the smokebox door. It has frost protection around the air pump and lubricator pipework, and rail clearers with large blades.

The dark grey wartime austerity livery is well done, with markings clearly printed, revealing the loco to be allocated to Bw Oldenburg.

The body is mostly metal, cleanly cast with some details formed in place but many more added parts – boiler handrails and pipework, air pump housing, generator, cab side handrails, and cab roof vent. The metal tender body is similarly enhanced with pipework, handrails, and access ladder.

As usual with Märklin products, the buffer beams are slightly too high.

Piston tail rod covers, full brake hoses, and dummy scale couplings are supplied for modellers to fit.

The cab interior is clear and detailed, though hardly visible due the commendable close-coupling of loco and tender (which can be adjusted). The interior has a light, switchable under digital control. Windows are glazed.

The chassis is also metal, with partially open bar frames as per prototype, enhanced with added air reservoirs, brake cylinders, and brake shoes close to the treads of all wheels.

The running gear is fine, a mixture of plastic and metal parts; the fluted rods have the infill neatly painted.

The first and fourth axles are fixed in the frame, while the second, third, and fifth are allowed some side play. As a result, the minimum recommended radius is 360mm.

The model is powered by a high efficiency five-pole can-type motor with flywheel in the boiler, driving via a worm onto the fourth wheelset, which has traction tyres. The other wheels are coupled by the rods.

The slow running is very smooth; the top speed is slightly more than the 80km/h of the prototype, but is controllable, and the decoder could be reprogrammed.

A shunting speed range and direct drive (inertia off) are available as digital functions.

The model weighs c.460gr and is 264mm long over buffers.

The DCC decoder with extensive sound functions is in the tender, with the loudspeaker under the circuit board. There are running sounds, plus whistle, shunting whistle, letting off steam, coal being shovelled, grate shaking, air pump, water pump, injectors, sanding, generator, coupling clank, water filling, rail joints, and squealing brakes. Access depends on the controller used.

The lighting is by warm-white maintenance-free LEDs; the headlights are directionally controlled, and under digital control can be left on while shunting.

The model is ready to accept an optional smoke unit, which can be switched under digital control.

There are NEM pockets on close-coupling mounts at both ends, and a Märklin style coupler is fitted on the back of the tender; another is supplied for the front if required.

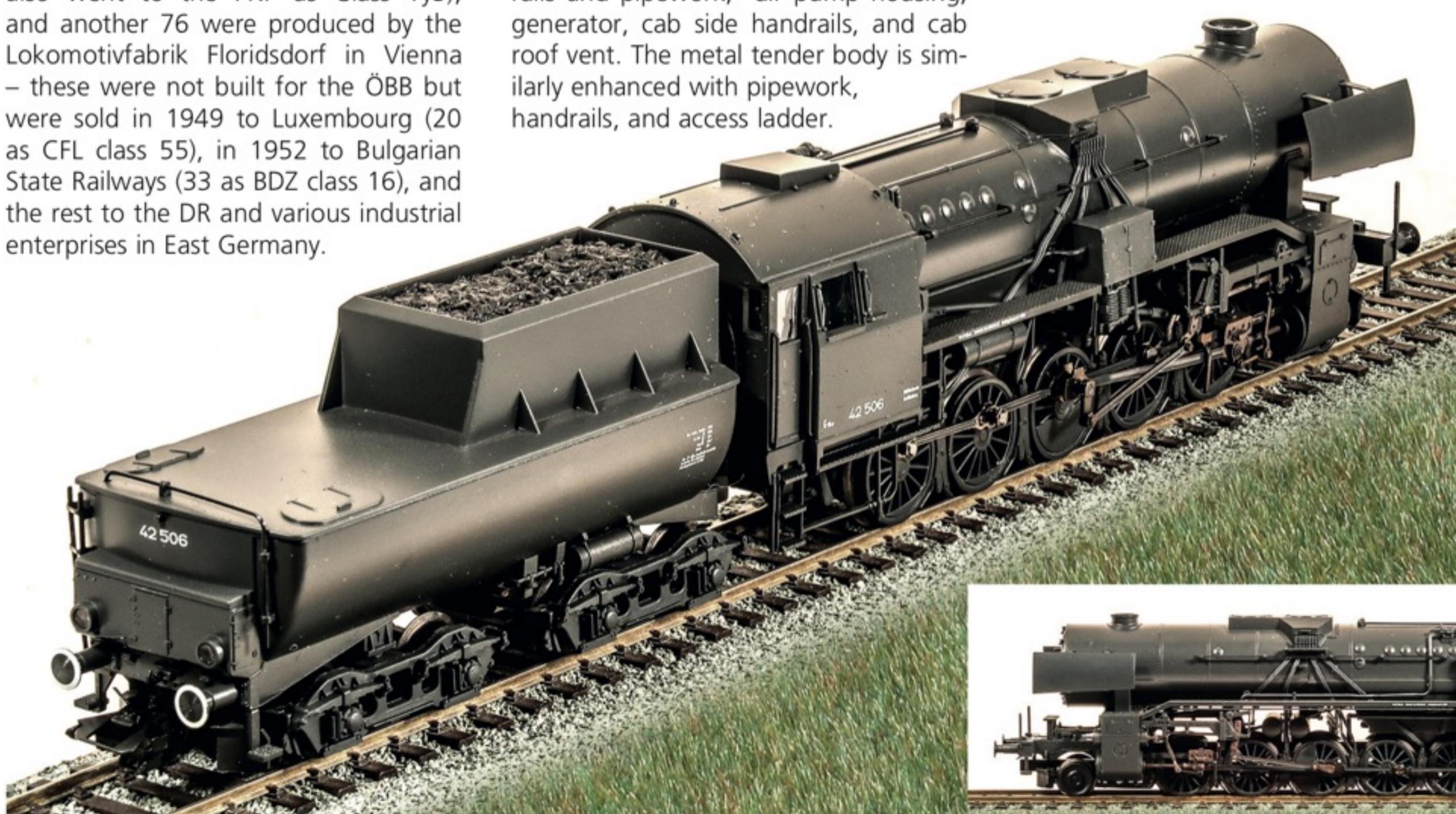
The Märklin version of this loco, with mfx decoder, is ref.39044.

### Manufactured by

Trix,  
Stuttgarter Straße 55-57,  
D-73033 Göppingen, Germany.  
[www.trix.de](http://www.trix.de)

**SAMPLE SUPPLIED BY**  
Gaugemaster Controls,  
Gaugemaster House, Ford Road,  
Arundel, West Sussex, BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

ref.22225 £445.00



**HOe**

## French military Schneider *locotracteur* new from Minitrains



Minitrains have recently delivered a brand new model, representing the Schneider type LG outside framed jack-shaft drive six-coupled 60cm gauge *locotracteur* of which 120 were built for the French military in 1916/7. Rated at 55hp, the 5m long (over buffers) 10t machines had a top speed of 16km/h and could be run on various fuels – benzol, ethanol, or petrol. The centre wheels were flangeless and minimum radius was just 20m. They must have proved their worth as they do not seem to have been sold off after the conflict.

The model measures 52mm long (over buffer beams), 22mm wide (over cab, handrails extra), and 34mm high (rail to top of exhaust). The wheelbase is 19mm, unequally divided as per the original. These dimensions indicate the model is accurate for HO (1:87).

The model is initially offered in three colours – grey (ref.1051), green with black frames and red front buffer beam (ref.1052), and dark red with black frames and buffer beams (ref.1053). Grey is the original military livery, while the only surviving example (a static memorial at the base of the engineer regiment in Toul) is presently green.

The body is moulded plastic, with good panel, louvre, and grille detail, rows of subtle rivets, and some prominent bolt heads – like the real thing. It corresponds closely with the works photos and drawings. Added parts are the exhaust on the cab roof, the front lamp, and the many (wire) handrails on the bonnet top and cab side. Bonnet door handles are moulded in relief.

The grey version has the number and maker's plates neatly printed on the sides of the cab.

The outside frames are detailed with underhung springs. The coupling rods and flycranks are metal. The wheels have blackened tyres; the centre set are flangeless, as per prototype.

The mechanism follows the proven formula: the can-type motor is mounted vertically in the cab, topped with a balanced flywheel, driving all axles through a worm and spur gears. Current collection is by wipers on the back of all wheels. The model runs smoothly and quietly, and is responsive to the controller, with a sensible top speed. It is quite heavy for its size (c.45gr), which assists with electrical contact and pulling power. There are no traction tyres.



The LED headlight is of virtually constant brightness and directionally controlled, though not completely extinguished in reverse.

There is no provision for connecting a digital decoder, and little space for one.

There are no instructions for dismantling the model.

The motor is not too apparent when the loco is running on the layout, and it could be disguised to some extent with a driver.

The model is fitted with the common narrow gauge buffer-couplings, without loops.

A charming machine with definite character – almost an honorary steam loco with the outside frames and rods! – which will no doubt be welcomed by narrow gauge modellers as it could be justified in many situations.



### Manufactured for

Standard Light GmbH,  
Werftstraße 12,  
D-76189 Karlsruhe, Germany.  
[www.minitrains.eu](http://www.minitrains.eu)

PRICE €149.90.



New from Arnold for TT (1:120) is a further version of the East German type Dbvqe four-car articulated double-deck set with driving compartment at one end in 'Coca-Cola' overall advertising livery.

The livery has been very well applied, with clear definition between the colours and sharply printed markings.

(COCA-COLA, COKE, the Contour Bottle Design, and the Dynamic Ribbon Device are trademarks of The Coca-Cola Company, used with permission.)

The vehicles are noted as Epoch IV-V on the box but surely should be later – the website gives Epoch V-VI.

The running numbers are 50 50 26-12 153-1 + 50 50 26-12 154-9 + 50 50 26-12 155-6 + 50 50 26-12 156-4, and the set is allocated to Dresden.

These sets were designed for push-pull operation with electric or diesel locomotives.

The model has a detailed driving end, with fine brake hoses, scale coupling, hoses, electrical connectors, horn, and separate end grabs.

The non-driving end has buffers, shunters' grabs, and electrical connections.

There are separate side door handrails on the outer doors.

The coaches have flush glazing, and the hopper sections on the lower deck windows have metallic frames.

Each body shell has interior fittings.

The corridor diaphragms are part of articulated bogie, and the vehicles plug together with close coupling links.

At each end of the set there are coupler pockets on flexible mounts; a close coupler is fitted at the non-driving end, and a spare is supplied for the driving end if needed.

The door steps move sideways to allow bogie rotation.



The models run freely on blackened metal disc wheels, insulated one side by a plastic centre bush.

The driving end has working directional head and tail lights; the end bogie is fitted with wiper pick-ups on all wheels.

Within the coach is a NEM651 six-pole socket (as noted in the instructions, not the NEM662 Next18 given on the website) for a digital decoder.

The models are delivered well packed in individual plastic boxes within an overall card wrapper.

The total length of the set is 612mm.



#### Manufactured by

Arnold,  
Hornby Deutschland GmbH,  
Oeslauer Straße 36,  
D-96472 Rödental, Germany.

Hornby Hobbies Ltd.,  
3rd Floor, The Gateway,  
Innovation Way, Discovery Park,  
Sandwich, Kent, CT13 9FF.

[www.hornbyinternational.com](http://www.hornbyinternational.com)

PRICE ref.HN9508 £189.99





The range of large scale stock by Piko has been augmented with a new variant of a four-wheel flat wagon with brake platform, now equipped with three cylindrical tanks mounted in a 'wooden' frame. It is described as a 'Behelfskesselwagen' or makeshift substitute, common in the early post-war years when stock was in short supply.

The vehicle carries Deutsche Bundesbahn Epoch III markings, which are very clearly printed, even in the channel solebars. The lettering on the tanks indicates that the wagon is based at Hamburg Süd and used for butane gas.

The overall length is quoted as 9.2m; the model measures 386mm, suggesting a scale of approximately 1:24.

The wagon is made in weatherproof materials so is suitable for use outdoors as well as inside.

The plastic body has moulded wood grain effect on the deck and brake platform surfaces, and is enhanced with added parts such as the tank top and end caps, the strapping, and the vertical tie rods.

The axleboxes and suspension components are very nicely represented, and the underframe features brake equipment moulded in medium relief. Brake shoes are aligned with the wheel treads.



#### Manufactured by

Piko Spielwaren GmbH,  
Lutherstraße 30,  
D-96515 Sonneberg, Germany.  
[www.piko.de](http://www.piko.de)

DISTRIBUTED BY  
Gaugemaster Controls,  
Gaugemaster House, Ford Road,  
Arundel, West Sussex, BN18 0BN.  
[www.gaugemaster.com](http://www.gaugemaster.com)

PRICE ref.37831 £135.00.

## laser-cut kit for modern office block new from Holzmann

New from Holzmann Modelismo in Brasil is a kit for a modern six storey office block named 'Sapphire 78'. (The street number is engraved in the centre of the 'glass' frontage, over the main door.)

The 56 parts are all laser-cut, the majority from a dense wood composite with an excellent smooth surface.

Very fine tabs hold the parts in the 'fret' and are easy to cut with a sharp knife; most are on non-critical edges which will not be seen when assembled.

The wood components are intended to be painted as required; the sample shown is unpainted.

Painting and detailing the interior should be undertaken before assembly.

There is blue acrylic for the tinted glass walls and the sides of the front desk, grey acrylic for the counter top, clear plastic for the windows and canopy, and translucent plastic for the light diffusers.

Both wood and plastic parts have detail 'etched' into the surface where appropriate.

All the parts fit accurately. The fit is generally loose, but everything locks together cleverly as the structure is built up. The only exception is the counter top, which is made up of two layers, and the tabs are a tight fit into the slots in the back wall.



The light diffusers are slightly too small for the opening in the foyer ceiling, and supports will need to be fabricated.

Construction is not difficult, although some dexterity will be needed to align all the tabs and slots when multiple parts are involved, notably the foyer ceiling (A2).



The equipment access door at the rear of the ground floor (step 2, part 6) should be installed with the detail facing outwards; no doubt it is shown reversed to aid identification.

Four types of glue are recommended for the various materials – white PVA, superglue gel (unlike the liquid version, no fumes to stain the acrylic), epoxy, and wood glue.

Also supplied is a pre-wired strip of three small LEDs to illuminate the foyer; cutouts have been prepared in each of the other floors to accept further lighting units (ref.05-300). It is important that only LEDs should be used as



incandescent bulbs will generate too much heat.

No apertures have been left for the wiring – these will need to be planned and cut before assembly.

The instructions occupy two sides of A3, and consist of a sequence of clear diagrams, with parallel text in Portuguese and English. Eight steps make up three main sub-assemblies.

All the wood parts are identified with etched numbers, either on the fret adjacent or an area of the part itself which will not be visible when assembled.

The building occupies an area of 90mm x 125mm (the canopy projects a further 20mm) and stands 230mm high.

Most modellers should have no difficulty successfully assembling this kit.

#### Manufactured for

Holzmann Modelismo  
Rua Dá Coufal, 460  
- Cep 91760-020  
Porto Alegre/RS, Brasil.  
[www.holzmann.net.br](http://www.holzmann.net.br)

PRICE ref.02-202 R\$159.95.  
c.£35.00, €40.00.

## Kursbuch der deutschen Museums-Eisenbahnen 2018

Compiled by Sabine Ressel & Sebastian Werner  
*Eisenbahn Kurier Verlag*,  
 Lörracher Straße 16, D-79115 Freiburg, Germany.  
[www.eisenbahn-kurier.de](http://www.eisenbahn-kurier.de)  
 210mm x 140mm 194pp  
 Softback €7.90  
 German text ref.6838  
 ISBN 978-3-8446-6838-4

Appearing annually for almost forty years, this handy guide to museum railways in Germany has become an institution: it is now published by Eisenbahn Kurier in collaboration with the Verband Deutscher Museums- und Touristikbahnen (VDMT).

Over the years it has grown in size and scope, and the level of detail, to reflect the number and variety of the railway museums and preserved lines in Germany: the VDMT now has over a hundred members.

The basis of any railway operation – state, private, or preserved – is the timetable, so at the core of this work are timetables of all active museum railways, along with information about the route, location, access, operating days, train times, prices, a summary of the motive power in use, and contact details.

Most entries also now include a QR code which will connect appropriate devices to the relevant internet site.

In a convenient compact form, this useful guide provides the railway enthusiast with all they might need to know about societies, museums, preserved railways, and railway companies with any preserved and/or historic stock.

The information is arranged Land by Land, roughly from north-west to south-east. An overall map at the start explains the regions, then specific locations are noted on a map at the opening of each regional section. A comprehensive contents list also aids finding any specific line.

The work is illustrated with occasional photographs, placed in relevant places to balance the content; some are black & white, some colour, but all have been well reproduced on good quality paper. The presentation is straightforward.

Very little German will be needed to understand the basically tabular information.

There is the equivalent of around twenty-eight pages of advertisements, most for other EK publications, but many from the lines and institutions featured, distributed through the book, placed as far as possible where relevant.

In short, a mine of useful information, whether for planning railway visits in a particular area or check-



ing to see what may be nearby and when it will be operating, and amazing value for the amount of data presented.

It would be quite a challenge to try and visit every entry, especially within one year!

Within the book is a postcard to pre-order next year's edition – and a request for all preserved lines to send in information as soon as possible.

## Lexikon der österreichischen Schmalspur-Triebfahrzeuge

by Dr Stefan Lueglinger  
*Verlag Ingrid Zeunert*,  
 Hindenburg Straße 15,  
 D-38518 Gifhorn, Germany.  
[www.zeunert.de](http://www.zeunert.de)  
 240mm x 170mm 144pp  
 Hardback €28.00  
 ISBN 978-3-945336-10-6 German text

It was a very pleasant surprise to receive a new railway publication from Zeunert, given that the founder and publisher died two years ago, after over fifty years devoted to producing books and magazines on minor railway subjects. However, the author of this guide to Austrian narrow gauge motive power decided to complete the book, which was already well advanced.

If you ignore the horse-worked Budweis – Linz – Gmunden, the story of the narrow gauge in what is now the republic of Austria began with the opening of the Steyrtalbahn from Garsten to Grünburg in 1889. In the following years a number of other lines were opened, mostly 760mm gauge, but very varied

– there were rack railways, electric railways, and several through especially attractive scenery. Only a small amount of the total built is still in regular operation today, although some sections have been preserved as museum lines, often with imported foreign stock, and some of the surviving commercial operations have been much modernised, so there is a wide range of contrasting stock.

This survey commences with a tour of Austria from west to east, through the states of Vorarlberg, Tirol, Land Salzburg, Kärnten, Steiermark, Oberösterreich, and Niederösterreich, briefly introducing all still working narrow gauge lines.

Then each type of motive power (steam, electric, and diesel) to be found in Austria in 2017 is presented in turn, class by class, with representative photos – in all there are four black & white and 185 colour pictures, all well reproduced, with brief but informative captions.

Maintenance equipment is not included.

The material concludes with four pages of principal dimensions and data in tabular form, and a two page bibliography.

The strong pictorial content means that even those with little German should be able to appreciate this book as an introduction to the varied world of the narrow gauge in Austria today.



The price quoted above does not include postage & packing: if ordering direct, please add €1.50 within Germany, €6.00 elsewhere in Europe.

## Journal No.41 Winter 2017

Edited by Dr Paul Waters and Julian Rainbow  
*British Overseas Railways Historical Trust*  
 West Greenwich Community & Arts Centre,  
 141, Greenwich High Road, London, SE10 8JA.  
[www.borht.org.uk](http://www.borht.org.uk)  
 295mm x 210mm 20pp  
 Softback £2.50  
 ISSN 0952-5483

The latest issue of the BORHT Journal presents just one article, by Tim Edmonds, about the British section of the Kowloon – Canton Railway as it was in 1948.

It was prompted by the discovery of photos sent to the author's late father by John Harrison, latterly involved with the Fairbourne and Festiniog railways, who had done his national service in Hong Kong. Subsequent research discovered more (27) of his photos in another well-organised collection, and a good number (18) of them are included here, as well as two photos of Harrison himself, three modern views of Hong Kong by the author, and two period maps (reproduced as photos). The images are reproduced as

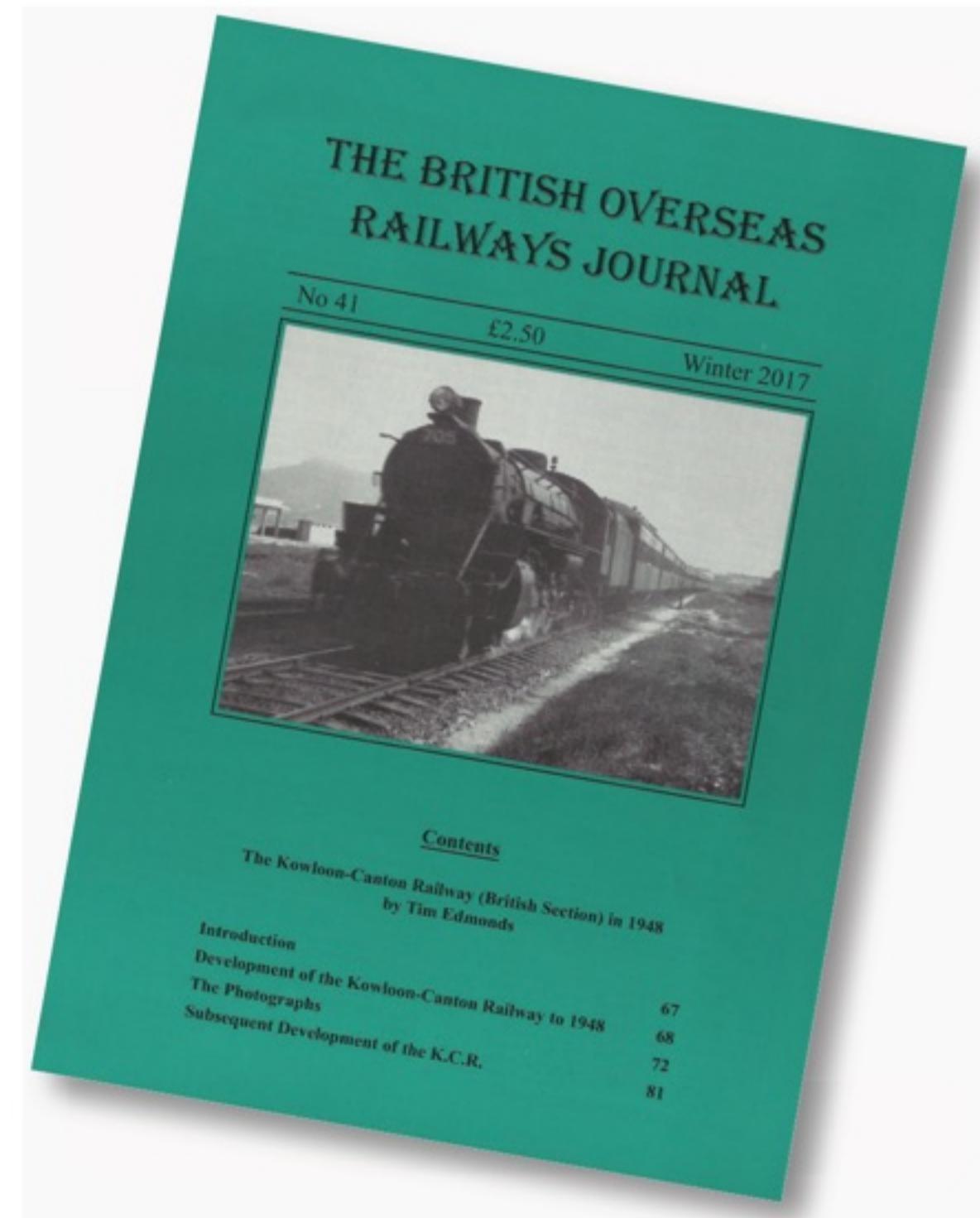
well as the originals will allow on good quality glossy paper. Most are presented large, usually two to a page, with lengthy captions.

The photos are prefaced with an introduction which explains the source of the material, and set in context by an informative summary history of the railway and its motive power. Tables summarise the five U.N.R.R.A. S200 2-8-2 and twelve W.D. 2-8-0 locomotives brought in after the Second World War.

Text and captions are augmented throughout with copious footnotes, literally at the bottom of each page. As the space required for this varies, it might have been useful to use a typeface that was less like the body copy, or place the notes within a framed box or on a lightly tinted background. Also, the use of a two column format for the captions in the main part of the collection results in the layout of text sometimes being a little wayward – for example, the second column of the caption to photo P21/12 on page 76 by chance consists of just one word, and this is too easily read as part of the first line of the first column.

However, such minor technicalities do not detract from the inherent interest of the material.

The price quoted above includes postage & packing in the UK, and surface post worldwide; air mail extra.



## Le Train – sup-modélisme

### Le réseau modèle 7

#### Le réseau des Carpates

by Gilbert Gribi

*Editions Publitrain eurl,  
CS80010, F-67660 Betschdorf, France.  
www.lettrain.com*

295mm x 210mm €16.50  
ISSN 1296-5537 French text

This new modelling special from *Le Train* is devoted to the work of one talented modeller, Gilbert Gribi, and one layout, which represents a logging line in the Carpathian mountains, modelled in 1:43 using 16.5mm gauge track (Oe). The layout occupies an area of 14.4m<sup>2</sup>, in a dedicated room, equipped for the purpose with fitted furniture, heating, ventilation, and a sink with hot and cold running water – not to mention an array of specialist equipment, including a casting centrifuge and a laser cutter. This is no average modeller ...

The layout meanders around the four walls of the available space and projects into two peninsulas. It is described as the realisation of an old dream, and has taken fifteen years to get to its present state.

Chapters deal with preparing the room, the tools and equipment used, the baseboard structure, the track and related accessories, wiring and digital con-

trol, scenery and vegetation, the buildings, motive power and rolling stock, the train ferry, vehicles and machinery, figures, details and accessories, lighting, and an explanation of how the photos were taken and processed using computer software. The collection is completed with a few final tips and tricks – and advice on how to avoid some of the mistakes the author made. It is encouraging to know not even he got everything right first time!

The work is copiously illustrated with photos and diagrams, all by the author, all of a high standard, all in colour, and all excellently reproduced. Many are presented large, with some double page spreads, and many page width or more – the quality of both modelling and photography justifies such enlargements. They have informative captions, and include many step-by-step sequences showing the techniques used in detail. The diagrams were all drawn using a computer program, and are very clear.

The page design is straightforward. Graphic devices are restricted to coloured section and chapter headings, the latter in an unnecessarily elaborate font which does not really enhance the presentation.

There are ten pages of advertisements.

The publication is perfect bound, using high quality glossy paper between heavier varnished covers.

Some knowledge of French will help to get the most out of the text, but the publication can still be appreciated for the excellent illustrations.



Packed with ideas and techniques which could be applied to many subjects and almost any scale, even without access to similar facilities.

## Covered vans for the Russian and Soviet 750mm gauge lines

by Sergei Dorozhkov & David Scotney

*Mainline & Maritime, 3, Broadleaze, Upper Seagry,  
near Chippenham, SN15 5EY.  
www.mainlineandmaritime.co.uk*

234mm x 156mm 60pp  
Softback £9.95  
ISBN 978-1-900340-47-2

This might appear to be a very specialised subject, and indeed it is! It looks like a section from a larger work, and is thus oddly out of context – it would be useful to have some background on Russian and Soviet 750mm gauge lines in general, not to mention other rolling stock, and indeed motive power.

Before the First World War, narrow gauge vans were developed by various wagon works to meet the needs of the expanding public and industrial railways.

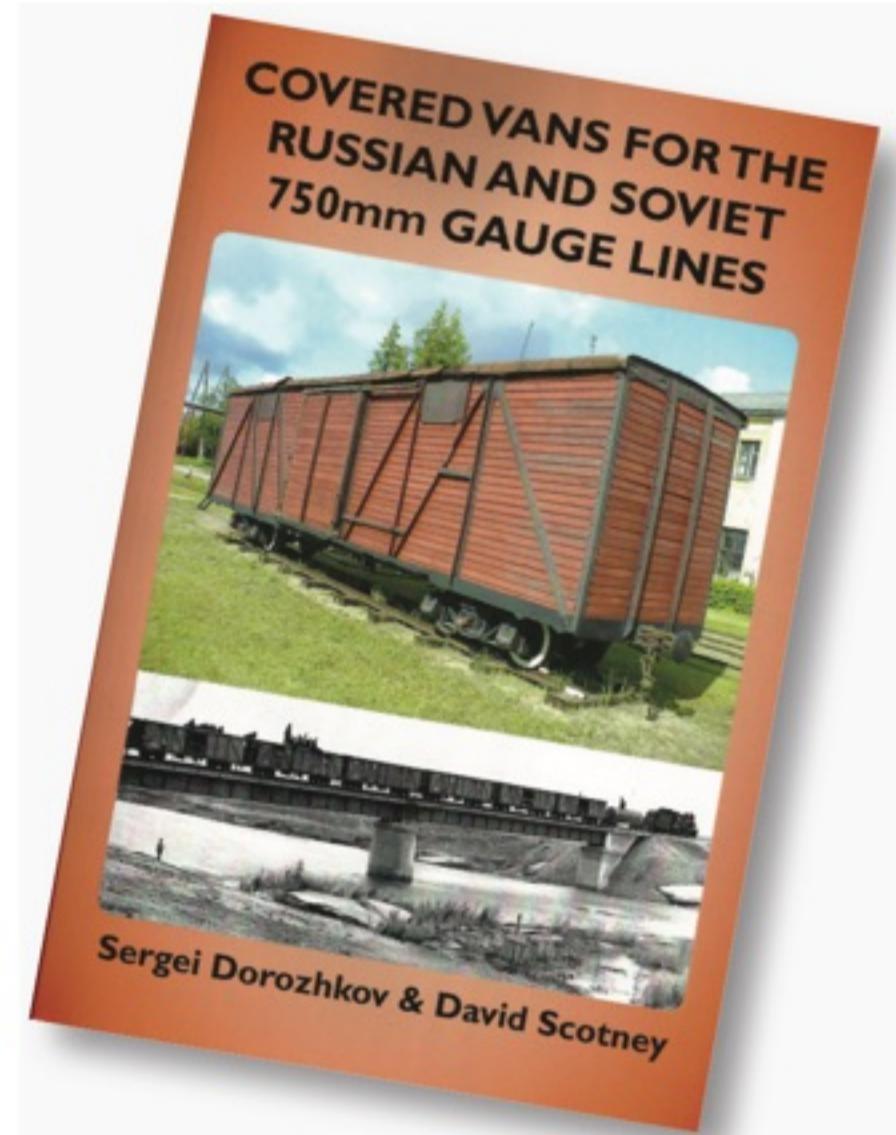
There was further construction over the decades, with a boost from the mid-1950s as part of the 'Virgin Lands' development scheme which continued into the 1970s.

After general consideration of the use of vans and basic types on the broad gauge, narrow gauge vans are examined by period – 1892 - 1917, 1917 - 1953, and 1954 - 1991. Following chapters cover subsequent modifications; surviving examples; and principal manufacturers. Throughout the text, numbered notes (111 in all) refer to sources, which are collected as a list of references at the end of the work. The book concludes with a summary and brief acknowledgments.

It is illustrated with a good selection of photographs, archive black & white and colour, reproduced as well as the originals will allow, sourced from archives and official records as well as individual enthusiasts.

There are also clear scale drawings of sixteen types of van, signed DJSS (presumably David Scotney) – these are reproduced rather small, but to a consistent scale; no dimensions are shown but a scale bar (c.8.5mm for 1m) is present; the gauge is c.6mm, suggesting 1:120 (TT) was intended. The drawings provide side and both end elevations, a plan, and in most cases also the interior layout, plus some details, supported by tables of principal dimensions. There is also a summary table of side elevations to compare the designs.

The presentation is straightforward, without graphic sophistication. It has the look of a thesis or dissertation



– the clue may be the acknowledgment of the Transport Research Institute at Edinburgh Napier University.

Hardly light reading but everything you might want to know about Russian narrow gauge vans.

## Modelling German Trains and Settings

Edited by Chris Ellis

*Kristall Productions,  
4, Surbiton Hall Close, Kingston, Surrey, KT1 2JX.  
www.ModelTrainsInternational.co.uk  
295mm x 210mm 44pp  
Softback £7.50 post free.  
ISBN 978-0-904811-33-6*

This new guide from *Model Trains International* follows on from the previous *Modelling German Railways* (2016) and includes coverage of actual locations in Germany served by rail with layout suggestions based on them, in all cases keeping them compact. There is also consideration of selected motive power and rolling stock models suitable for such layouts, and other useful information for German modelling.

The single page introduction looks at the potential of modelling the German scene, past and present, and this is followed by a short photo survey of suitable locos from various periods for small layouts.

The collection consists of thirteen articles, which vary between one and five pages.

They are: on the Nebenbahn (the appeal of local lines); branch line stations (with sample track layouts); the 'Seekuh' (from Erlangen to Grafenberg) and other

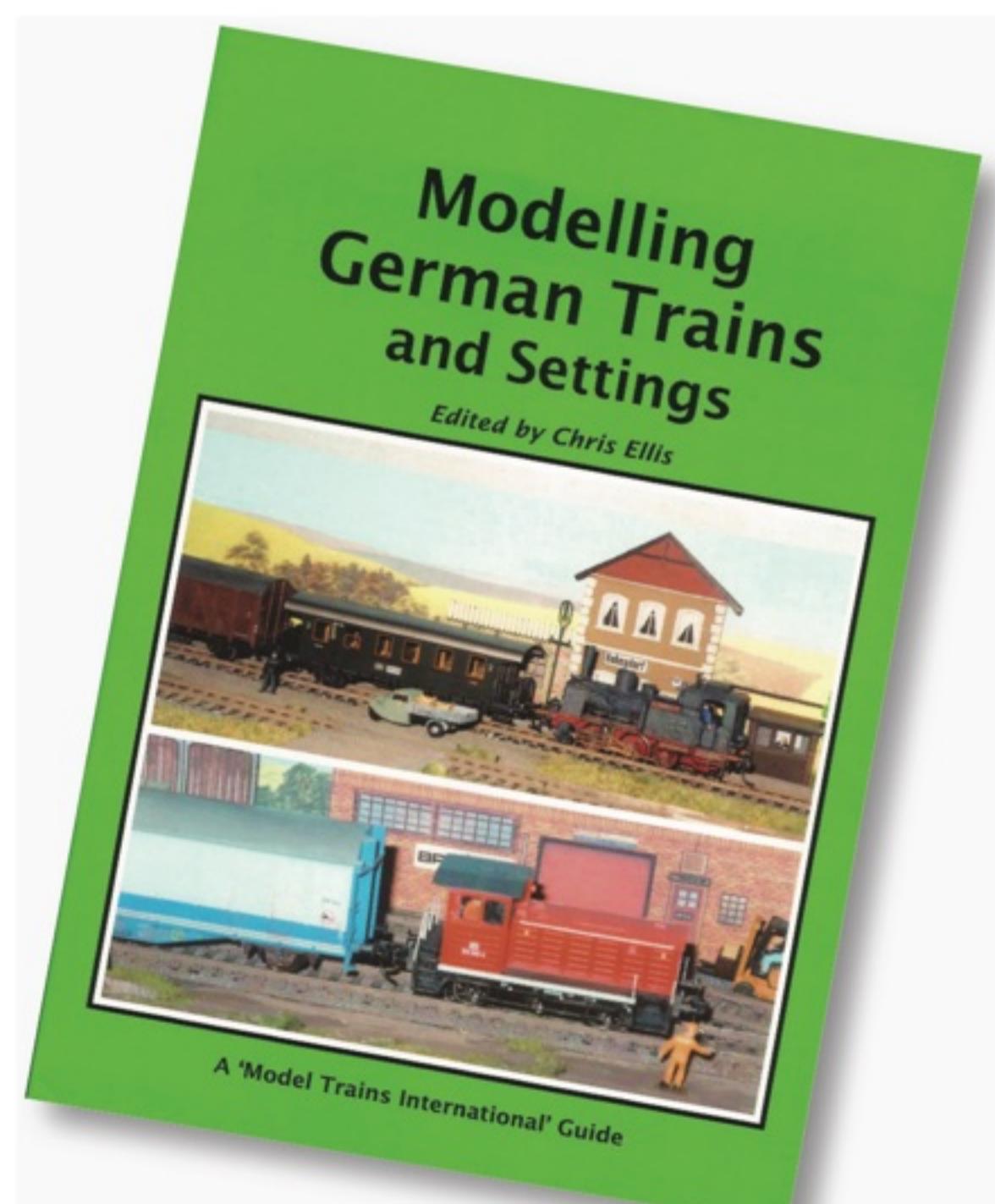
similar lines; Neustadt (a 'vintage' Märklin HO layout); street running in Germany; Blutnok (a Bavarian branch terminus in HO); Nürnberg docks; Belt Line and Ringbahn (mainly Nürnberg again); lines at Stadtgrenze (between Nürnberg and Fürth); Micro Yard (a switching layout in N built to demonstrate Micro-Trains® knuckle couplers in a German context); modelling a shunters' hut in HO; the DR V100 diesel loco (prototype and selected models); and *Bahnbetriebswerk Dügelsheim* (a diesel depot modelled in HO).

Some are suggestions based on a real place, others are layouts which have actually been built. These are generally small, intended to fit typical domestic situations – and practical budgets.

The articles are illustrated with between two and twenty-one photographs, generally not reproduced large, and most feature a track plan.

The presentation is basic, uniform with the former *MTI* magazine, and indeed some of the articles are reprints from the 1980s and 1990s. They seem to have been scanned from the printed pages, not re-made from original material, so image quality is restricted; the text is not very sharp.

Apart from colour on the covers, all the photos are black & white. The simple presentation and standard of reproduction does not make the most of some potentially interesting material, which might appeal to those with limited time, space, and resources.



The book is available from the publisher at the address above. Overseas postage £2.50.

## SBB electric locomotives past & present

Written and compiled by Martin Fisher and Paul Russenberger

Swiss Railways Society, Sales, Glyn Jones, Alpigen, 13, Welland Close, Dalebrook, Burton-upon-Trent, Staffordshire, DE15 0AG.  
www.swissralsoc.org.uk  
297mm x 210mm 124pp  
Softback £16.00  
ISBN 978-0-9926812-5-8

This interesting new book from the SRS offers exactly what it claims on the cover.

Relying on imported coal to fuel steam locomotives, Swiss railways were early to explore electrification. The Seebach – Wettingen line near Zürich was equipped as an experiment from 1904, while parts of the Simplon route used an Italian 3.3kV three-phase system from 1906. By 1913, when the BLS were electrifying their line from Spiez to Früttigen at 15kV 16 $\frac{2}{3}$ Hz AC, it made sense for the SBB to follow suit – after long consideration and a slow start. But in relatively short order between 1919 and the early 1940s most of the national network was electrified.

This book does not provide a complete fleet list – such is available elsewhere – but aims to illustrate the locomotive types from the early days to the present. The bogie arrangement now regarded as standard took some time to evolve, and early machines displayed quite a range of drive systems. Some were to be short-lived but others lasted a long time.

The focus of the book is on locomotives, and it does not include multiple units or (more surprisingly) shunters. However, some railcars, capable of hauling coaches or wagons, are covered.

The locomotive designs are presented in sections in chronological order: the 1920s; the 1930s and 1940s; the 1950s, 1960s, and 1970s; the 1980s and 1990s; and the 21st century. Each sub-section has a descriptive text, and there are several tables of comparative dimensions.

The account is very well illustrated with photographs, from historic black & white through to contem-

porary colour, simply presented and well reproduced. Some are from the authors and other Society members, some (naturally) from archives and official sources, and some from the internet. Every photo is captioned and (where relevant) credited.

The book is dedicated to Alan Pike OBE, past Chairman and current President of the SRS (and a respected contributor to CM) but oddly uses none of his wide-ranging collection of photos.

Three schematic maps show the SBB network in sections – north-west, north-east, and south.

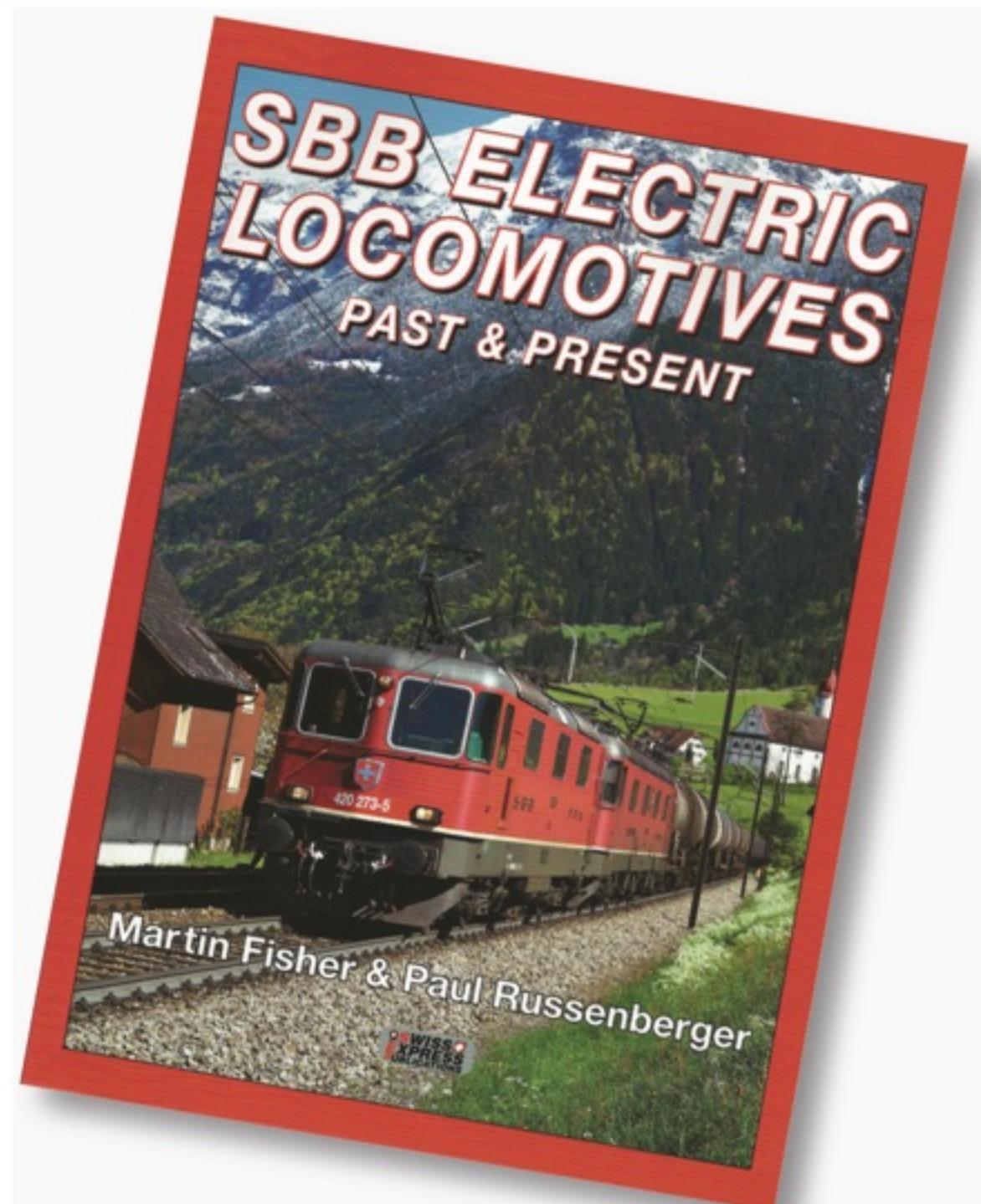
The work is rounded off with appendices on the technical evolution of Swiss electric locos (informative but necessarily condensed), publications listing all current Swiss locos, the classification of Swiss motive power, and an explanation of the manufacturer abbreviations.

It concludes with two pages advertising other Society publications.

The price quoted above includes postage within the UK; for overseas rates, and details of credit card payment, contact – sales@SwissRailSoc.org.uk

Below

SBB Re4/4<sup>i</sup> No.10043 at Montreux in April 1977 with the 'omnibus' to St.Maurice. Photo: Alan Pike.



## Rolling Thunder An Engineman's Journey

by Stewart Anderson

Published by Helen Anderson  
350mm x 250mm 304pp  
Hardback A\$115.00 + p&p  
ISBN 978-0-646-95573-5

The brief two page introduction to this substantial book informs us that Stewart Anderson was born in 1964 in a suburb of Melbourne. He began training as a locomotive driver in 1984, and qualified in 1988. Sadly he was diagnosed with cancer in 2005, and eventually died in 2016. Faced with the inevitable, he fortunately had time to sort and select from his considerable collection of railway photographs, and this impressive book is the result.

His first book was the joint venture *Liveries in the Landscape* and he also worked on *Motive Power* magazine and then managed *Australian Railways Illustrated*.

He had travelled all over Australia, and the pictures are grouped state by state: Victoria (59 pages), New South Wales (59 pages), South Australia (49 pages), Queensland (30 pages), Western Australia (32 pages), including mineral lines as well as the government network, Tasmania (40 pages), and Northern Territory (4 pages). He also visited North America, and 50 pages are devoted to trips there in 2010 and 2012, with a high concentration of 'vintage' diesels in short line service. The collection concludes with three pages (five pictures) from Scotland (all taken in September 2010) and a one page shot from Singapore (in October 2010).

There is no list of contents or index to assist finding a particular image.

But for two 1962 pictures of steam locos in Tasmania, taken by his father, the earliest photos date from 1982, the latest 2013. Presented in more-or-less chronological order within each geographical section, they thus cover the period of transition from monolithic state-run systems to a bewildering range of private operators.

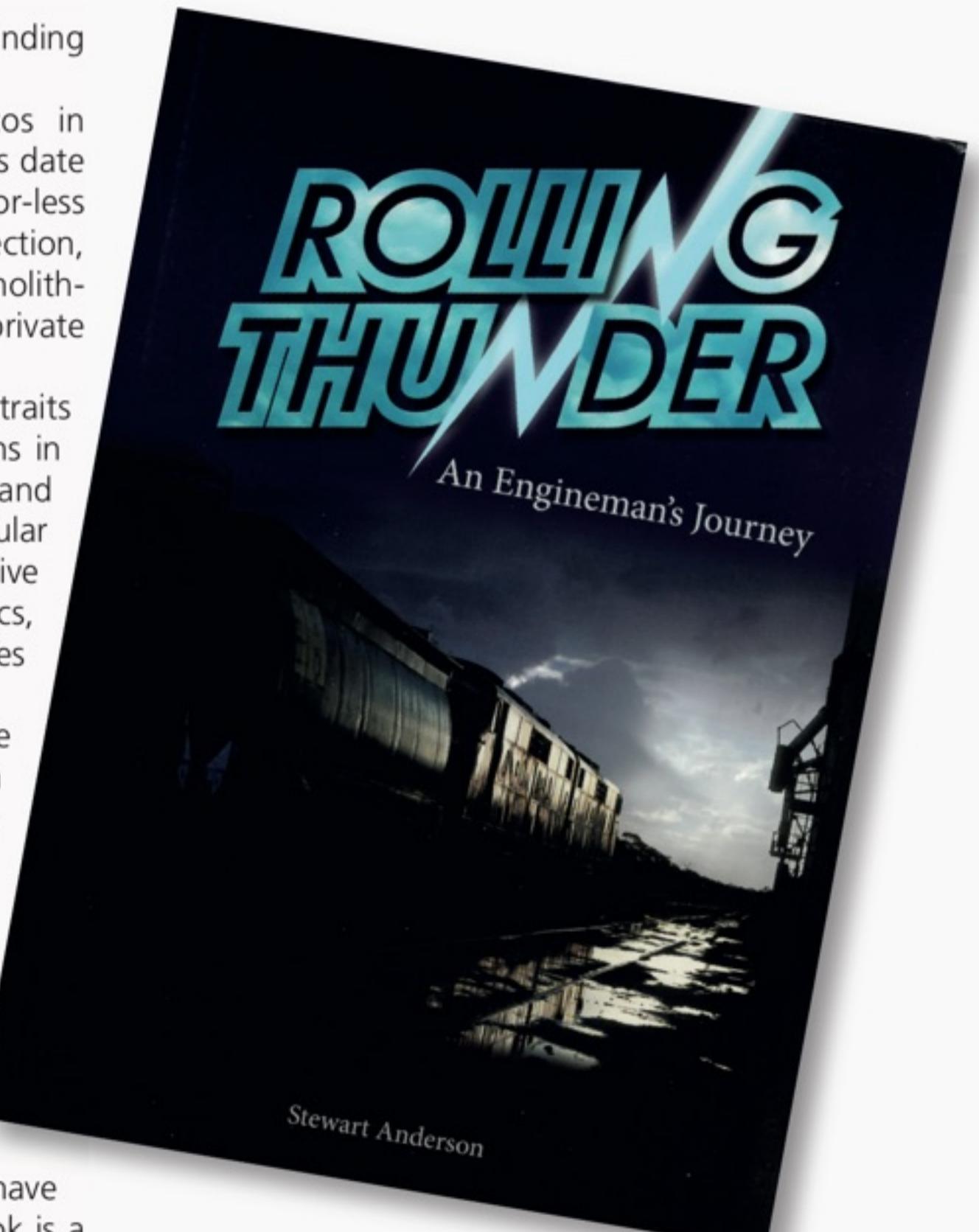
The images are as varied as the subjects: portraits and action shots; shed and station scenes; trains in the landscape; observation of detail; evening and night views. Most of the trains shown are regular services, passenger and freight, and the motive power is predominantly diesel, with some electrics, and the odd steam special, but no preserved lines or museums.

The book has been prepared and printed by the same company as produce the acclaimed Train Hobby albums, so it is to a very high standard. The large size allows detail in the images to be appreciated. The pictures are simply laid out, with a certain amount of space, usually two to a page, with some full pagers: the images chosen for this treatment are certainly good enough to stand such enlargement.

There are informative captions adjacent to each image, all specifically dated.

This particular engineman's journey may have reached its destination prematurely, but this book is a wonderful legacy.

The book can be obtained from Australian retailers such as Train World (Melbourne) and Australian



Modeller (Sydney). Note that postage is likely to be costly as the book is large and heavy (c.2.5kg).



## Die Triebwagen der Baureihe VT 628 Ablösung für den legendären Schienenbus

EK-Videothek: Baureihen

DVD c.58mins ref.8441 €19.80

The diesel railcars of class 628.2 first appeared on the then Deutsche Bundesbahn in 1986. Successors to the 627, 628.0, and 628.1, these two-car units for local passenger services appeared in a number of different variants, and nearly 400 were built.

This programme records the life of these units in all parts of Germany, with a look at the history of the classes 627, 628.0, 628.1, 628.2, 628.9, and 629 to put them in context. We look behind the scenes to see these

reliable units being maintained, and there is also footage of those working for private operators and those used on the Sylt-Shuttle Plus. 628 201 is now preserved by the DB Museum. A replacement for these units is currently being sought.

We begin with scenes of 628 428/928 428 leaving a station, followed by views at Köln Hbf of another 628/928 set leaving to Neuss Hbf. Further scenes show 628 580/928 580 at Aschau (Chiemseebahn), then 628 657/928 657 is seen with the driver climbing aboard and talking us through the checks and engine start up procedure. The driver speaks well of the class, stating that they are "old technology but very reliable".

At Mühldorf (Oberbayern) 628 646/928 646 is seen on a service to Burghausen while 628 629/928 629 runs in on the adjacent platform road bound for Rosenheim, with further scenes from the lineside

A two-car 628.0 in ocean blue & beige is observed from the lineside and on arrival and departure from Durach station, south-east of Kempten (Allgäu) on the line to Reutte im Tirol in Austria. There a mint green/turquoise 628.0 is seen departing bound for Garmisch-Partenkirchen and watched from the lineside. Blue & beige 627 102 is seen arriving at and departing from a local station, with another of the class in the later traffic red colour scheme filmed from the lineside in the snow.

One of the class 704 'Turmtriebwagen' (overhead catenary inspection vehicles), based on the 627, is briefly seen in its yellow livery, then 628.2/928.2 units are shown in action sporting the mint green/turquoise colour scheme, with some scenes filmed from the cab and at Neustadt (Schwarzwald) station on a service to Villingen. This is

followed by 628 255/928 255, also in mint green/turquoise, at Berlin Zoologischer Garten station on test. Further scenes show the driver's desk and the first and second class passenger accommodation on these trains, with sequences filmed around Berlin from the lineside and at work in former East Germany, including one set at Gera leaving on a service to Leipzig Hbf.

A 628/928 pair in mint green/turquoise is seen at AEG Werk Hennigsdorf, and at Hof in Bayern a number of these units sporting this livery are seen, followed by film of 628/928 units in action on the Oldenwaldbahn – one is seen arriving and departing Hetzbach, with further film from the cab.

Back in former East Germany at Karow a set is seen departing the station and being overtaken by a steam-hauled special. At Kremmen a traffic red 628/928 set is seen on a service to Berlin-Charlottenberg, with further scenes from the lineside. At Berlin-Lichtenberg another traffic red pair is seen leaving the depot, with views inside the passenger accommodation. Also seen in traffic red is 628 507/928 507, with further sets in this colour scheme at Würzburg Hbf., Lindau Hbf., Nonnenhorn, Friedrichshafen Stadt, and Densborn station on the Eifelstrecke.

Moving to the north of Germany, we see traffic red 628/928 units at Timmendorfer Strand in Schleswig-Holstein, crossing the Fehmarnsundbrücke en route to Puttgarden, and at Burg. We then move back to Berlin Zoologischer Garten station where we observe two 628/928 sets arriving with a cross-border service to Poland. There are further scenes of this at Stettin, where we also see an RE6 service to Lübeck Hbf. leaving, worked by a 628/928 unit.

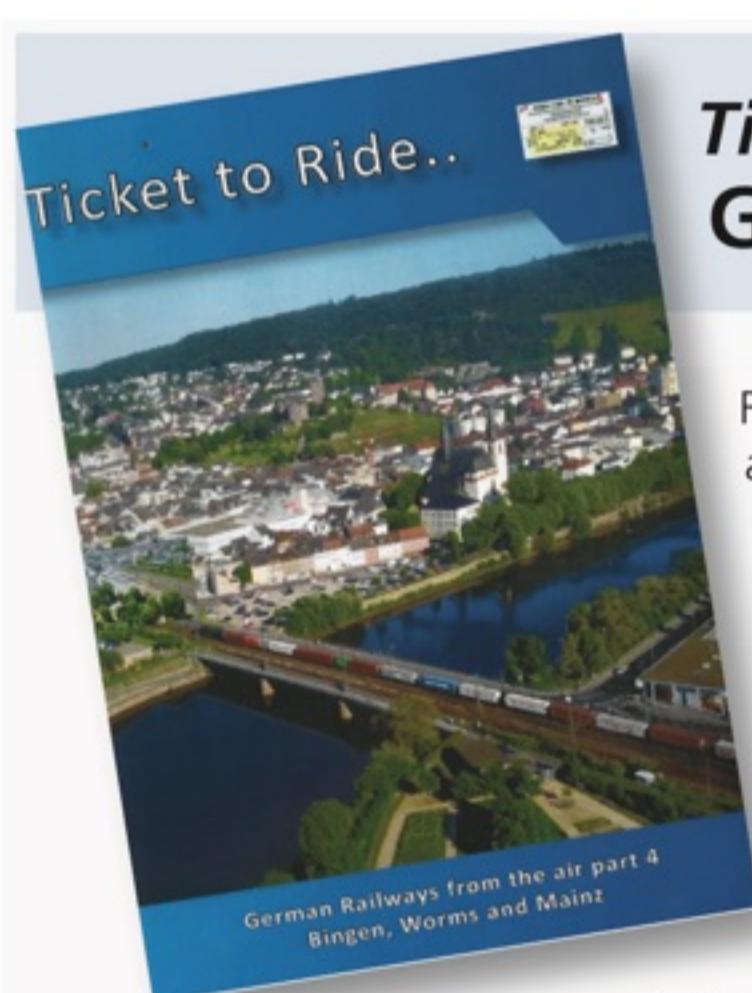
Next the HL Bahn's class 629 is shown and an EVB 628/928 is seen on a local, followed by the shutdown procedure and maintenance checks on 628 657/928 657 described and illustrated by a driver, finishing with scenes of 628/928 units in use on the Sylt-Shuttle Plus service between Niebüll and Westerland (Sylt) and at Tüssling and at Altötting stations in south-east Bayern.

Some background music is used, mainly towards the end, but it is not too obtrusive.

The commentary, only in German, is clear and informative.

Around 45 minutes of trailers for other Eisenbahn Kurier DVDs follow the programme.

The Eisenbahn Kurier railway video library is published by EK-Verlag, Lörracher Straße 16, D-79115 Freiburg, Germany. [www.eisenbahn-kurier.de](http://www.eisenbahn-kurier.de)



## Ticket to ride

### German railways from the air 4 – Bingen, Worms, and Mainz

colour maps of Mainz are shown and we then see Mainz cathedral and the statue the famous printer Johannes Gutenberg.

At Mainz-Römisches Theater station, footage from the platforms shows a DB Fernverkehr class 101 on an Inter-City service and a DB Regio local passenger service. This is followed by aerial views of the Südbrücke river crossing and an ICE 3 set is seen passing over it.

We then see the vineyards in this area from the lineside and from the air as a DB Fernverkehr class 101 passes with a Euro-City service formed of Swiss stock while in the opposite direction an ÖBB express from Klagenfurt in Austria races past. At Worms on the Rhine's left bank, aerial views show the town, its industry, the Hauptbahnhof, and the freight yard. Local passenger services are shown being worked by both diesel and electric multiple units.

At Biblis aerial views show the town and, in the distance, the Odenwald hills. Local passenger services are seen, and at the station two ICE services pass in opposite directions. We also see a French TGV set en route from Frankfurt am Main to Paris. Freight services are also observed, with some filming from the lineside.

At Weinheim we see the rebuilt station and an RTS Siemens 'Hercules' diesel-electric is seen on an engineers' train, followed by further freight action filmed from the platforms. Local passenger services are shown, worked by DB Regio LINT electric railcars. DB Regio 111 108 is also seen arriving on a train of refurbished 'Silverfish' bound for Heidelberg, followed by the passing of a Bombardier TRAXX electric with a container train and a TGV.

The programme closes with a brief trailer for the next in this series, visiting Mannheim and the Neckar Valley.

The commentary (in English) is both clearly delivered and very informative. There is a certain amount of noise which can be heard from the propellers of the drone used for aerial photography but this, along with the background music, used only at the very beginning and end of the programme, does not spoil the enjoyment.

This programme is available from FHP Audio Visual, Hayfields, Upton Scudamore, Warminster, BA12 0AE. [www.tickettoride.co.uk](http://www.tickettoride.co.uk)

The price quoted includes postage and packing within the UK and Europe; please add £2.50 for the rest of the world.

Italy intermodal freight working. River cruise vessels are seen moored and on the Rhine's right bank an SNCF Fret electric heads south with a freight train, quickly followed by a DB Cargo electric with a northbound train of bogie tank wagons seen from the riverbank, plus DB Cargo electrics double-heading a long train of bogie hoppers and a FLIRT electric multiple unit on a local passenger service.

Further aerial views look down on Bingen and the confluence of the rivers Nahe and Rhine. At Rüdesheim film taken from the riverbank shows a Bombardier TRAXX electric on a train of containers heading north. At the town's station ground level footage shows both north- and south-bound freight services behind DB Cargo electrics.

A map on screen then shows the rail routes from Mainz to Frankfurt am Main, Darmstadt, Mannheim, and Ludwigshafen. This is followed by scenes of freight trains passing over the Rhine and a local passenger service, an ICE 3 set, and a DB Regio class 143 on double-deck stock. Both aerial and ground level views are then shown of freight services at Mainz Kastel on the right bank of the Rhine. A FLIRT e.m.u. is also seen arriving and departing with a local passenger working. The bridges over the river Main are shown and we see barges loaded with construction materials.

A visit is then made to Mainz-Bischofsheim marshalling yard (*Rangierbahnhof*) and we see aerial views of Mainz Hbf. as well as some archive stills of how it looked in former times. There are views inside the Hauptbahnhof and from its platforms; a number of local passenger services are observed. Archive black & white and then



## Extra layouts on show at Pecorama



As an additional attraction for visitors to Pecorama on the weekend of 21st and 22nd July, there will be an exhibition of portable layouts in the Lecture Theatre/Station Gallery.

All have been selected to show different aspects of the hobby from those in the permanent exhibition which will of course be open during the weekend, as will the well-stocked model shop. The Beer Heights Light Railway will also be in operation. The Sunday is garden charity day.

The layouts booked to appear are:

– *The Andeer line* (Swiss HOm) by Julia & Derek Boswell, which depicts a section of the metre gauge Rhätische Bahn and uses innovative lightweight foam baseboards for ease of storage and transportation. Visitors will be invited to operate the layout, which employs digital control, with a central 'signal box' and trains driven using smart 'phones and tablets.

– *Bw.Nerdelheim* (German O, above, CM German Special October 2017) by Birgit & Peter Martin, which uses Lenz DCC equipment and features sound-equipped locos.

– *Bw.Dügelhausen* (German HO, CM March 2016) by Doug Tompkins, which demonstrates that an interesting scene and working layout can be modelled in a compact area.

– *Picnic in Gelderland* (Dutch HOe) by Sven van der Hart, an example of superb scenic modelling in just 30cm square and displays the delightful and beautifully engineered Tramfabriek products.

– *Corris 1930* (Welsh OO9) by Rod Allcock and Geoff Broadhurst, an accurate scale model and scenic masterpiece, as featured in RM December 2013.

Open from 10.00am to 5.00pm both days, admission is included within the normal Pecorama ticket price (see website for details).

[www.pecorama.co.uk](http://www.pecorama.co.uk)

## Cardboard boxes for 1:35 from Matho Models



Matho Models has expanded its 1:35 scale range of scenic accessories with a trio of packs for cardboard boxes. Each represents current packaging styles for branded coffee (ref.35071, *illustrated*), bottled water (ref.35072), and fizzy drinks (ref.35073, *inset*). Each pack contains between 30 and 36 boxes,

of six different designs, printed flat on thick paper. Each box must be cut out and folded to assemble. The insides are printed with a brown cardboard texture, enabling them to be displayed 'open' if desired.

Price – €6.95 per set.  
[www.mathomodels.com](http://www.mathomodels.com)

## 50th Railex North East

Railex North East, promoted as the north east's premier model railway show, will be taking place over the weekend of 28th and 29th July at the John Spence Community High School, Preston Road, North Shields, Tyne & Wear, NE29 9PU. It will be the fiftieth Railex North East to be staged by the Blyth & Tyne Model Railway Society.

More than twenty-five layouts have been booked to appear, in gauges from N to O, of British and overseas prototypes. Amongst them will be *Kayreuth* (German N by Ian Hogben, CM April 2011, January 2012, and

September 2014), *Tyskie-Wielkopolska* (Polish HO by Alan Wardman, below, CM September 2017), *West Pickering* (Canadian HO by Craig Watson), and *2nd (Hand) Yard* (American O) by Arry Dodd.

There will be a number of modelling demonstrations (including locomotive weathering), together with a wide range of traders.

For further details, including opening times and admission prices, see *Exhibition Diary* or: [www.railexne.com](http://www.railexne.com)

*Photo: Derek Shore.*



## Brawa announces V60 in HO

Brawa has revealed that it is developing new tooling for 1:87 scale models of the Deutsche Bundesbahn V60 0-6-0 640hp diesel hydraulic shunting locomotives. The models are due to be released during the second quarter of 2019; advance orders will be taken from 15th September 2018.

Brawa is inviting potential purchasers to vote (before 30th June 2018) via the website for their preferred liveries.

The V60 (later BR260 and 261, according to weight) was introduced in 1956, and 942 were constructed for the DB by various builders over the following eight years. Some were subsequently fitted with radio control and automatic couplings. From 1987 they were reclassified BR360/361 as Kleinloks

to allow operation by shunters rather than fully qualified drivers, to save costs. Since 1997 many have been re-motored.

As well as West Germany, the design has been used in Belgium, Norway, Greece, Croatia, Turkey, and Israel.

Many are still in service in Germany with DBAG and private operators.

The new model will feature etched radiator grilles, separate handles and handrails, an unobstructed view through the cab, working lights using maintenance-free LEDs, a finely detailed chassis, and NEM coupler pockets on close coupling mounts. DCC versions will be equipped with comprehensive sound and light functions, together with optimised motor and load control.

[www.brawa.de](http://www.brawa.de)



# Rails of Sheffield opens new showroom

On Friday 12th May Rails of Sheffield held a special event for invited guests to formally open a brand new showroom, which has been created within the existing premises on Chesterfield Road. Representatives from the trade and model press were shown the new facilities and introduced to staff.

The showroom is smartly fitted out with extensive glazed display cabinets and modern lighting; novel features include full-size buffers as seats for visitors and an elevated O gauge line that runs around just below the ceiling.

The showroom was open to the public from the following day.

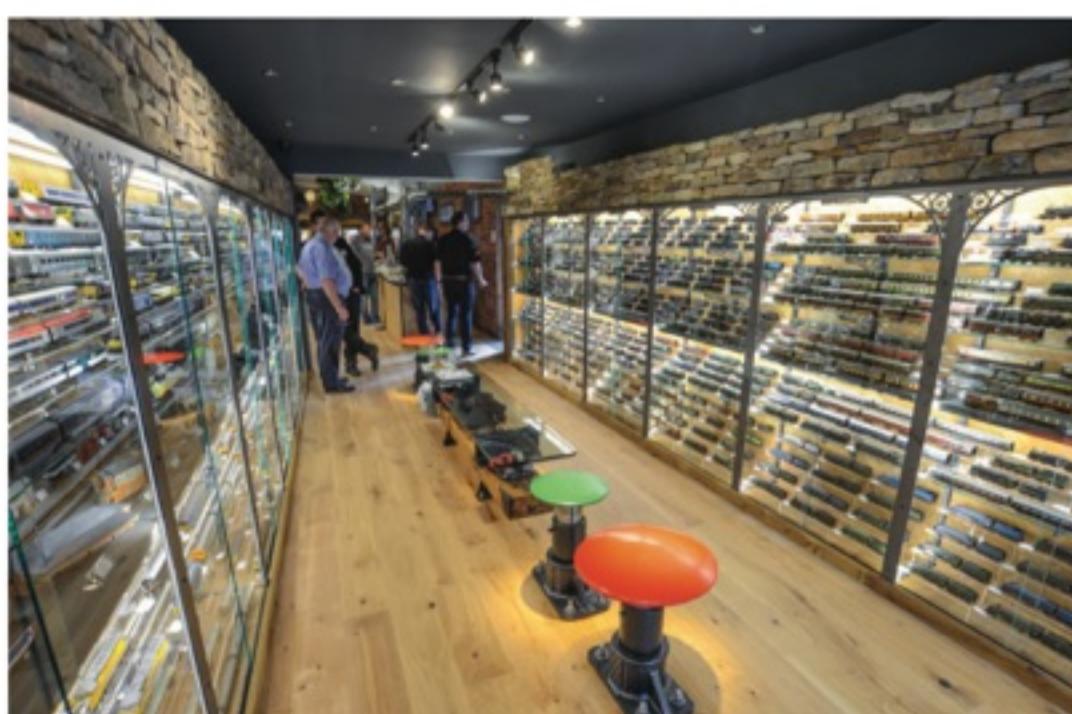
The opening of the new showroom represents a significant investment by proprietors John and Tracy Barber in the shop's future high street presence. The business was established in the 1970s by John's father.

Rails specialises in new and secondhand model railway equipment (continental and American as well as British outline), stocking items from a wide range of manufacturers and suppliers including Peco, Hornby International, Kato, Noch, and Rapido Trains.

Opening times are 9.00am to 5.00pm Monday to Saturday.

Rails of Sheffield

21 – 29, Chesterfield Road, Sheffield, S8 0RL.  
0114 255 1436 [www.railsofshfieeld.com](http://www.railsofshfieeld.com)



## American laser-cut structures in HO and O



Right On Track Models manufacture laser-cut wood structure kits in HO and O scales, with styrene windows and doors, styrene roof, cast resin and 3D-printed details, and signage. All kits come with fully illustrated instructions. Two new kits have recently been released.

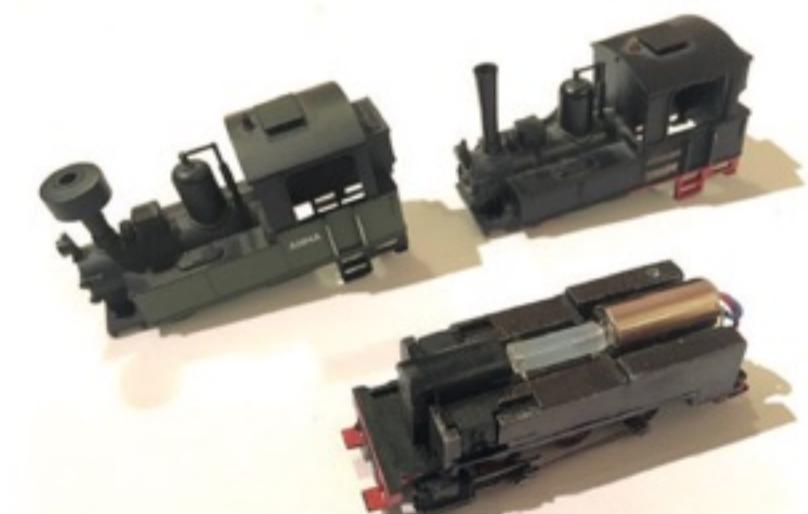
The first is for a 'Canoe & Paddle Store' in O scale, with a footprint of 12" x 8", standing 7.5" tall, priced at US\$124.95.



The second release represents an icing platform, as used until the early 1960s to load reefers with block ice so that perishable products could be transported long distances by rail. Kits are available for O (24" x 6.5", 11" high, US\$135.00) and HO (11" x 3", 5.25" high, US\$48.00).

Prices do not include overseas shipping.  
[www.rightontrackmodels.com](http://www.rightontrackmodels.com)

## Tramfabriek re-motorising kits



Tramfabriek offer a kit to re-motor Roco HOe steam locomotives (refs.33201, 4100, 33240, 33242, 33261 or similar). Within an hour you can fit a coreless motor which works as well as a similar Faulhaber or Maxon motor, but is much cheaper (€25.50). Not only will it dramatically improve the running quality, even at a crawl, but it is also very quiet. In addition, the low profile leaves the upper cab open. The small but powerful motor measures just 8mm x 16mm. This unique motor has been specially made for Tramfabriek, and is used in all their model locomotives. It works on 12V without the need for a resistor,

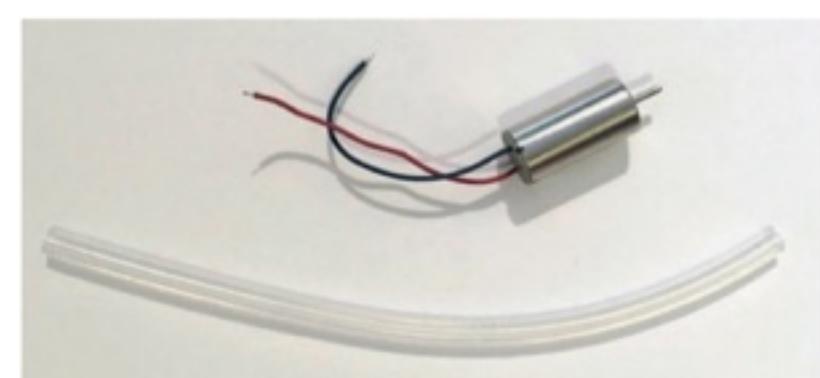
and can be connected directly to Lenz Silver+ Mini and Lenz Gold+ Mini DCC decoders.

For the installation you only need epoxy glue, a small screwdriver, and a soldering iron.

There is also a similar motor conversion kit for the Roco HOe industrial diesel (refs.31025, 31027, 33205, 33206, 33207, 33208, 33209 or similar) (€25.50). This is just as easy and quick to install as for the steam loco, and with the same excellent results.

The motor can also be obtained separately (€14.50), or in a kit with 10cm of strong flexible silicone tube to connect the motor to a shaft of 1mm to 2.5mm diameter (€17.50).

<https://tramfabriek.nl>



## Kato RhB Ge4/4" coming in N

Kato is currently in the process of shipping the new model of the Rhätische Bahn Ge4/4" No.631 Untervaz to model shops.

The Ge4/4" is still widely used on the RhB, hauling the Glacier Express from Disentis to St.Moritz and the Bernina Express from Chur to St.Moritz lines as well as on freight trains.

The model is in the current standard red livery and matches the existing Kato models of the RhB EW1 coaches.

The model features operating headlights and close couplers; minimum recommended radius is 150mm, and max-



imum gradient 5%. It can be converted to DCC with a dedicated Digitrax decoder printed circuit board (not included).  
[www.gaugemaster.com](http://www.gaugemaster.com)

## Rokuhan Z 'Shorty' flat wagons

Rokuhan has announced the first freight wagon for the 'Shorty' series, a bogie container flat which can carry a variety of Rokuhan shipping containers. The wagons will be packed individually and will be available in either blue or grey. Anticipated delivery is late August this year; the price is not yet known.



Advance orders are welcome now.  
[www.azldirect.com](http://www.azldirect.com)

## In brief

### Warwick auction

Warwick & Warwick, the auctioneer of collectables, will be staging their next sale of model railway items on Sunday 8th July at the Ballroom of the Court House, Jury Street, Warwick, CV34 4EW.

Further auctions are planned for 19th September and 21st November.

Catalogues will be available to view online three weeks prior to the auction date.

Warwick & Warwick Ltd.,  
Chalon House, Scar Bank, Millers Road,  
Warwick, CV34 5DB.  
01926 499 031  
[www.warwickandwarwick.com](http://www.warwickandwarwick.com)

### New plastic cement from Deluxe Materials

The latest addition to the Deluxe Materials range of adhesives is 'Plastic Magic 10 Second Cement', a thin liquid plastic cement which provides 10–15 seconds for adjustment following application. It is non-toxic and non-flammable with minimal odour, and can be used to bond styrene, ABS, and most kit plastics. The cement is supplied in a stable anti-spill bottle with a choice of two brushes (precision and long reach).

Deluxe Materials products are distributed to the trade in the UK by Gaugemaster:  
[www.deluxematerials.com](http://www.deluxematerials.com)  
[www.gaugemaster.com](http://www.gaugemaster.com)

### Spotted in Dortmund

Seen on test on one of the O gauge layouts at the major Intermodellbau exhibition in Dortmund in April was an advance sample of the brand new Lenz model of the German BR94 0-10-0T.  
[www.lenz-elektronik.de](http://www.lenz-elektronik.de)



# Exhibition Diary

Dates, events, and information

## Saturday 23 June

### KING'S LYNN, Norfolk

**Organiser:** King's Lynn MRC.  
**Venue:** Lynnsport, Greenpark Avenue, King's Lynn, Norfolk, PE30 2NB.  
**Open:** 1030 - 1630.  
**Admission:** adults £5.00, under 15s £2.00, under 5s free.

**Amenities:** refreshments.

**Features:** layouts include *Obervaz* (Swiss HOm), *Jernies End* (US HO), and *Chica, Illinois* (US HO). Trade.

**Contact:** [www.klmrclub.simplesite.com](http://www.klmrclub.simplesite.com)

## Saturday 23 June

### STEVENTON, Oxfordshire

**Organiser:** Oxfordshire Narrow Gauge Modellers.  
**Venue:** Steventon Village Hall, The Green, Steventon, OX13 6RR.  
**Open:** 1000 - 1600.

**Admission:** £4.00 for 009 Society members, £5.00 for non-members; accompanied children under 16 free.

**Amenities:** limited free parking; disabled access; refreshments.

**Features:** 14 narrow gauge layouts, including *Sam's Creek* (Australian On30). 009 Society sales stand, trade.

**Contact:** [www.oxngm.org.uk](http://www.oxngm.org.uk)

Paul Spray 01865 247801

## Saturday 23 & Sunday 24 June

### PERTH, Scotland

**Organiser:** Perth & District MRC.  
**Venue:** Dewar's Ice Rink, Glover Street, Perth, PH2 0TH.  
**Open:** 1030 - 1730 Saturday  
1000 - 1700 Sunday

**Admission:** adults £7.00, children £3.50, family (2+2) £16.00.

**Amenities:** full disabled access; buffet/bar.

**Features:** 40+ layouts from N to O, including *St. Adrian* (Swiss Z, CM February 2003), *Ronshafen* (N), *St. Combs* (Canadian N, CM April 2018), *Untermütten* (Swiss HOm, CM April 2010), *Handegg* (Swiss HOm), *Rippon Cement* (HO), *Zauberwaldbahn* (German Oe, CM January 2011), and *Arcadia Pennsylvania Terminal* (US O). Demonstrations, seminars, specialist societies, trade.

**Contact:** Stan Moug 07588 754959  
[www.perthmrc.com](http://www.perthmrc.com)

## Saturday 23 & Sunday 24 June

### ROMSEY, Hampshire

**Organiser:** Romsey & District Railway Modellers Society.  
**Venue:** Crosfield Hall, Romsey, Hampshire, SO51 8GL.  
Town centre location, close to car parks and bus station.

**Open:** 1030 - 1700 Saturday  
1000 - 1630 Sunday

**Admission:** adults £4.00, children £2.00, family (2+2) £10.00.

**Amenities:** free parking on Sunday; full disabled access; refreshments.

**Features:** 15+ layouts, including *Ermengerst* (German HO, CM March 2017). Demonstration, trade.

**Contact:** 01794 514861  
[www.rdrms.com](http://www.rdrms.com)

## Saturday 30 June & Sunday 1 July

### EXETER, Devon

**Organiser:** Exe MRS.  
**Venue:** The Matford Centre, Matford Park Road, Marsh Barton, Exeter, EX2 8FD.  
**Open:** 1000 - 1700 Saturday  
1000 - 1600 Sunday

**Admission:** adults £7.00, accompanied children free.

**Amenities:** free parking; disabled friendly; restaurant.

**Features:** 35+ layouts, representing British, continental, and American outline, including *The Andeer line* (Swiss HOm), *Bw. Dügelhausen* (German HO, CM March 2016), *Ostrava* (O), and *Jillsburgh Sub* (Canadian HO). Demonstrations, trade.

**Contact:** [www.exemrs.co.uk](http://www.exemrs.co.uk)

## Saturday 30 June & Sunday 1 July

### GATESHEAD, Tyne & Wear

**Organiser:** North East of England Group of the N Gauge Society.  
**Venue:** Gateshead Masonic Hall, Alexandra Road, Gateshead, NE8 1RB. Five minutes walk from Gateshead Inter-change station.

**Open:** 1000 - 1700 Saturday  
1000 - 1600 Sunday

**Admission:** adults £3.00, children £1.50, family £5.00.

**Amenities:** free dedicated car park; full disabled facilities, everything on one level; refreshments.

**Features:** club and individuals' layouts, Northumberland Group Canadian layout, bookstall, trade.

**Contact:** Ian Holmes  
[i.d.holmes@btinternet.com](mailto:i.d.holmes@btinternet.com)  
Neil Walker  
[neil.walker64@btinternet.com](mailto:neil.walker64@btinternet.com)

## Sunday 1 July

### HAMPSTHWAITE, North Yorkshire

**Organiser:** Northern Counties American Modellers (NCAM).  
**Venue:** Memorial Hall, Hollins Lane, Hampsthwaite, near Harrogate, North Yorkshire, HG3 2EY.  
**Open:** 1000 - c.1400

**Admission:** day visitors £3.00.

**Amenities:**

**Features:** American layouts, including *Naples Street* (US TT, in this issue).

**Contact:** David Nuttall 01943 862425

## Saturday 7 July

### BEACONSFIELD, Buckinghamshire

**Organiser:** Beaconsfield & District MRC.  
**Venue:** The Beaconsfield School, Wattleton Road, Beaconsfield (old town), Buckinghamshire, HP9 1SJ. Five minutes from M40 J2. Nearest station Beaconsfield.

**Open:** 1030 - 1700

**Admission:** adults £5.00, children £2.50.

**Amenities:** free parking on site; refreshments.

**Features:** 18 layouts, including *Clara-Zetkin-Stadt* (German HO) and *Nippon Tetsudo* (Japanese Z).

**Contact:** Roy Jackson 01753 643724  
[www.beaconsfieldmrc.org](http://www.beaconsfieldmrc.org)

## Please send details for possible inclusion to:

Peco Publications & Publicity Ltd., Beer, Devon, EX12 3NA.  
Telephone: 01297 20580 Fax: 01297 20229  
e-mail: [rm-clubs@btconnect.com](mailto:rm-clubs@btconnect.com)

**Features:** 21+ layouts including *Niederfell Mosel* (German N), *Dobris* (Czech HO), *On Picasso Lines* (HO), and *Bay City Terminal Railroad* (US HO). Demonstrations, society and club stands, trade.

**Contact:** Joe Lock 07866 6412215

## Saturday 14 July

### MANSFIELD, Nottinghamshire

**Organiser:** East Midlands Group of the OO9 Society.  
**Venue:** The Portland College, Nottingham Road (A60), Mansfield, NG18 4TJ.  
**Open:** 1030 - 1630

**Admission:** members £3.50, guest day membership £4.00, under 16s accompanied by an adult free.

**Amenities:** free parking; disabled access; snacks and drinks available.

**Features:** at least 25 narrow gauge layouts, mainly OO9, plus Society sales stand, trade.

**Contact:** [rogerbugg@tiscali.co.uk](mailto:rogerbugg@tiscali.co.uk)  
[bobtelford4468@gmail.com](mailto:bobtelford4468@gmail.com)

## Saturday 14 & Sunday 15 July

### BARRY, South Wales

**Organiser:** Barry & Penarth MRC.  
**Venue:** St. Cyres School, Sully Road, Penarth, CF64 2XP.

**Open:** 1000 - 1630 Saturday  
1000 - 1600 Sunday

**Admission:** adults £5.00, children £3.00.

**Amenities:** heritage bus from Cogan railway station.

**Features:** 40th anniversary show.

**Contact:** 07766 732805  
[bpmrc@outlook.com](mailto:bpmrc@outlook.com)

## Saturday 21 July

### HOUNSLOW, Greater London

**Organiser:** Twickenham & District MRC.  
**Venue:** Kerswell Hall, Wills Crescent, Hounslow, TW3 2JF.

**Open:** 1000 - 1630.

**Admission:** free (donations most welcome).

**Amenities:** refreshments.

**Features:** T&DMRC annual open day. Club layouts (three OO, plus O and N displays), demonstrations, secondhand sales.

**Contact:** 01932 783253  
[www.tdmrc.org.uk](http://www.tdmrc.org.uk)

## Saturday 21 & Sunday 22 July

### BEER, Seaton, Devon

**Organiser:** Pecorama.  
**Venue:** Lecture Theatre/Station Gallery, Pecorama, Beer, Seaton, Devon, EX12 3NA.

**Open:** 1000 - 1700 both days.

**Admission:** free (within usual Pecorama ticket price). Sunday is garden charity day – proceeds to local good causes.

**Amenities:** free parking. The venue is wheelchair accessible, but note that the site is hilly and some paths cannot be negotiated by wheelchair users unaided. Garden Room licensed restaurant, *Orion Pullman* café.

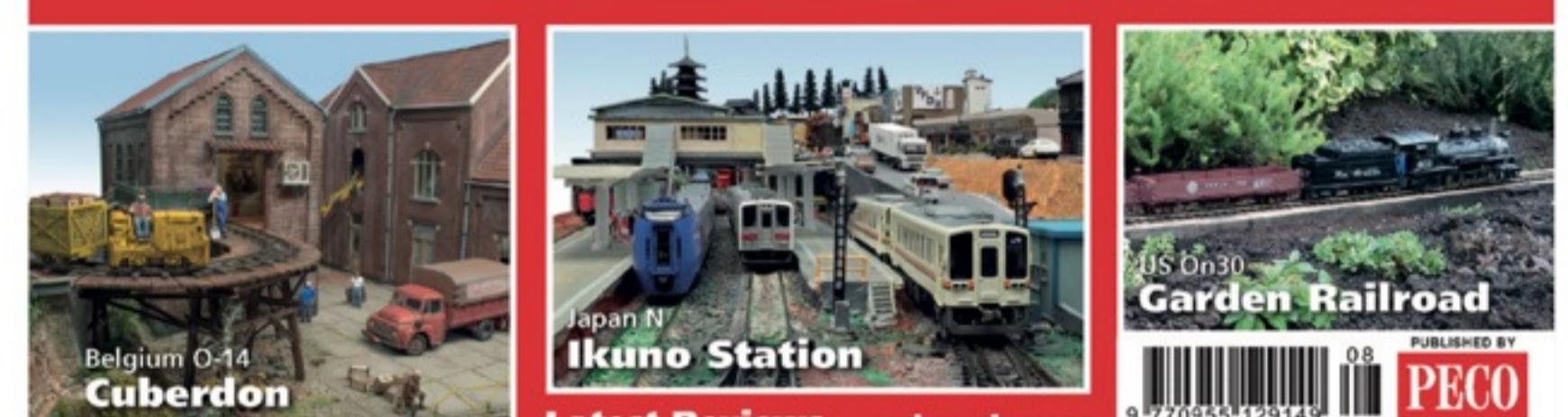
# Coming next month

out 19th July

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FEATURING RAILWAYS FROM AROUND THE WORLD EACH MONTH



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## • Köszeg Óvarós

Peter Phillips created Köszeg Óvarós (Köszeg Old Town) due to growing interest in Hungarian narrow gauge – though the standard gauge has not been forgotten.

## • Cuberdon

Rene Ceulemans imagined the factory producing a local sweet speciality is served by a narrow gauge railway.

## • Ikuno Station

Pete Sayers built a layout set on the island of Hokkaido to accommodate a collection of Japanese N stock.

## • Adventures in the garden

Lacking the space indoors to find an adequate home for his substantial American On30 locomotives, Bill Longley-Cook ventured out into the garden where arranging the scenery is civil engineering!

## • Chica, Illinois

Peter North returns to one of his favourite themes, a lonely flag stop somewhere on the Rock Island. It only needs the tumbleweed rolling down Main Street!

... and much more!

**Features:** an additional exhibition of portable layouts in the Lecture Theatre/Station Gallery, all selected to show different aspects of the hobby from the permanent displays in the main Pecorama exhibition, including *Bw.Dügelhausen* (German HO, CM March 2016), *Bw.Nerdelheim* (German O, CM German Special October 2017), *Picnic in Gelderland* (Dutch HOe), and *The Andeer line* (Swiss HOm, digitally controlled with locos driven by smart 'phone and tablet – visitors will be encouraged to participate in its operation).

**Contact:** [www.pecorama.info](http://www.pecorama.info)

### Saturday 28 July

#### HORSHAM, West Sussex

**Organiser:** West Sussex Area Group of the N Gauge Society.

**Venue:** The Forest School, Compton's Lane, Horsham, RH13 5NT.

**Open:** 1000 - 1630. Including the N Gauge Society A.G.M. at 1430.

**Admission:** adults £3.00, NGS members £2.00, children £1.00.

**Amenities:** refreshments.

**Features:** layouts, trade.

**Contact:** [www.wsng.co.uk](http://www.wsng.co.uk)

### Saturday 28 & Sunday 29 July

#### NEW MILTON, Hampshire

**Organiser:** South Coast MRC.

**Venue:** The Arnewood School, Gore Road, New Milton, BH25 6RS.

**Open:** 1030 - 1700 Saturday

1000 - 1600 Sunday

**Admission:** adults £5.00, children £3.50, family (2+2) £15.00.

**Amenities:** free parking; level disabled access; refreshments.

**Features:** layouts include *Solent Summit* (US N), and *Somewhere in France* (OO9 WWI). Trade.

**Contact:**

[www.southcoastmodelrailwayclub.co.uk](http://www.southcoastmodelrailwayclub.co.uk)

### Saturday 28 & Sunday 29 July

#### NORTH SHIELDS, Tyne & Wear

RailexNE

**Organiser:** Blyth & Tyne MRS.

**Venue:** John Spence Community High School, Preston Road, North Shields, Tyne & Wear, NE29 9PU. Blue Arrow bus 11 passes venue; nearest Metro station North Shields.

**Open:** 1030 - 1700 Saturday

1000 - 1600 Sunday

**Admission:** adults £7.00, children £3.00, family (2+2) £16.00.

**Features:** 50th anniversary show. Layouts include *Kayreuth* (German N, CM April 2011, January 2012, and September 2014), *Tyskie-Wielkopolska* (Polish HO, CM September 2017), *West Pickering* (Canadian HO), and *2nd (Hand) Yard* (US O). Demonstrations, club sales, trade.

**Contact:** [www.railexne.com](http://www.railexne.com)

### Saturday 28 & Sunday 29 July

#### STOKE ON TRENT, Staffordshire

**Organiser:**

St.John's Charity Exhibition.

**Venue:** St.John's Primary School, Wheatley Avenue, Trent Vale, Stoke on Trent, ST4 6SB. Three miles from M6 J15.

**Open:** 1030 - 1700 Saturday  
1000 - 1630 Sunday

**Admission:** adults £4.00, children £2.00, family (2+2) £10.00. Proceeds shared between Douglas McMillan Hospice and Midlands Air Ambulance.

**Amenities:** ample free parking; disabled access; light refreshments.

**Features:** layouts include *Neuburg* (German HO, CM March 2018). Demonstrations, trade.

**Contact:** 07747 087050  
[jrf.cox@hotmail.co.uk](mailto:jrf.cox@hotmail.co.uk)

### Sunday 29 July

#### THIRSK, North Yorkshire

**Organiser:** Thirsk & District MRG.

**Venue:** Thirsk Town Hall, Westgate, Thirsk, North Yorkshire, YO7 1QR.

**Open:** 1030 - 1630

**Admission:** adults £4.00, children £2.00 (under 14 accompanied by an adult), family (2+2) £10.00. Proceeds to local charities.

**Amenities:** parking nearby on Market Place; light catering on site.

**Features:** 12 layouts in various scales, including *Neuburg 1913* (Swiss HOm), *Junction City* (US HO), and *Livathia* (Greek HO, t.b.c.). Trade.

**Contact:** [www.topcliffemill.com](http://www.topcliffemill.com)  
[p.simmerson@btinternet.com](mailto:p.simmerson@btinternet.com)

## Meetings

### Monday 2 July – 19.30

#### Peak Rail Association – Sheffield Branch

*Toy Trains in the Sky*  
by David Charlesworth.

**Venue:** The Harlequin, 108, Nursery Street, Sheffield, S3 8GG.

**Contact:** Dave Sharp 0114 274 5478  
[www.peakrail.co.uk/sheffieldbranch](http://www.peakrail.co.uk/sheffieldbranch)

### Monday 9 July – 19.30

#### Lincoln Railway Society

*America 2016 – Denver to Barstow*  
by Roger Childs.

**Venue:** St.Hugh's Church, Harewood Crescent, North Hykeham, Lincoln, LN6 8JG.

**Admission:** visitors £3.00.

**Contact:** Graham Lightfoot  
01522 698085  
[grahamlightfoot1948@gmail.com](mailto:grahamlightfoot1948@gmail.com)

### Monday 30 July – 20.00

#### Nantwich & Market Drayton Railway Society

*Railways of the Peloponnese (Greece)*  
by Mike Lister.

**Venue:** King's Head Inn, Shrewsbury Road, Market Drayton, Shropshire, TF9 3EH.

**Contact:** [johnsllynn147@virginmedia.com](mailto:johnsllynn147@virginmedia.com)

### Saturday 28 & Sunday 29 July

#### NORTH SHIELDS, Tyne & Wear

RailexNE

**Organiser:** Blyth & Tyne MRS.

**Venue:** John Spence Community High School, Preston Road, North Shields, Tyne & Wear, NE29 9PU. Blue Arrow bus 11 passes venue; nearest Metro station North Shields.

**Open:** 1030 - 1700 Saturday

1000 - 1600 Sunday

**Admission:** adults £7.00, children £3.00, family (2+2) £16.00.

**Features:** 50th anniversary show. Layouts include *Kayreuth* (German N, CM April 2011, January 2012, and September 2014), *Tyskie-Wielkopolska* (Polish HO, CM September 2017), *West Pickering* (Canadian HO), and *2nd (Hand) Yard* (US O). Demonstrations, club sales, trade.

**Contact:** [www.railexne.com](http://www.railexne.com)

# Nekoya Line narrow gauge models from Tomytec

Rolf Höhmann reports on an interesting range of narrow gauge models from Japan, which he thinks should be better known – and more widely available.

Besides the 3'6" gauge main network there are – or were – numerous 'true' narrow gauge railways, mostly of 762mm (2'6") gauge in all parts of Japan. Most of them have vanished, with the exception of two electrified commuter lines near Nagoya, and several museum and tourist undertakings, including the well known Kiso forest railway.

Models of these lines and their sometimes curious rolling stock have always been very popular in Japan, starting in the 1970s with the Sango and Joe Works brass kits, which became quite well known in the west, and from several other producers, such as World Kougei and Imon Models. It seems that nearly every Japanese narrow gauge prototype has been produced in kit form in limited runs. Many pictures of small narrow gauge layouts and dioramas have appeared in Japanese model railway magazines – quite understandable when we think about the small apartments and the limited space available in that country.

However, mass produced and (nearly) ready-to-run models have only been available since late 2016, when Tomytec introduced their 'Nekoya Line'. This is a fictitious route: the map on the packaging shows a main line running from the sea to a spa, with two branches, to a mine and a temple, typical destinations and reasons for existence of such lines.

The first models were two sets of a railcar and a trailer in cream & blue and cream & maroon livery. These were followed by more modern versions with different fronts and colour schemes. Another old railcar with typical front balconies and a smaller modern railcar in two colour schemes with only one folding door per side have been announced for delivery in April and June 2018.

The same variety can also be found with the trailers, where four different body types and colours were produced, including an old passenger car with a clerestory roof and open end platforms.

In late 2017, two trains with different locomotives and either a trailer or two small bogie goods wagons in open and covered versions were presented.

All models follow prototypes from different Japanese narrow gauge railways but are not true scale reproductions and might therefore be called generic models. The cream & blue models show many resemblances to the large roster of the Senpoku railway in northern Honshu, the Shinzoku line, the Sunen railway near Nagoya, and of the Ogoya railway at Komatsu. The newest



Above  
The first release in 2016 was an older railcar with modernised trailer in cream & blue livery.

Photos: Rolf Höhmann.



Above  
The modern railcar with matching trailer follows a prototype from the Shizuoka line built in 1960.



Above  
More colourful is railcar No.13 with an old passenger coach.

developments – a modern railcar with a milk tank car – are good reproductions of stock of the lightly-laid Tomibetsu lines in Hokkaido, which were only closed at the end of the 1960s and were well known for their milk traffic using cans and tank containers which could be craned onto trucks.

Printed on the packages is the scale ratio of 1:80, a compromise introduced in Japan to run 3'6" gauge prototypes on 16.5mm (HO) track, but measurements show that the Nekoya Line tends to be 1:87. Maximum width of the models is 26mm, height 38mm. They are thus within the range of sizes of HOe and OO9 models.

Technically these models are quite simple: produced in China, the bodies are mostly moulded in one piece with clear windows, set flush for the newer prototypes; handrails are moulded in place. All vehicles have a railway logo, a Japanese character, and a number print-

ed on the sides and the ends. The railcars have only one (non functional) headlight on the top. The chassis has archbar trucks with plastic wheelsets. The couplers provided are either standard N or a small non-functional link and pin.

The sets are meant to be collectors' items for display, and plastic track for this purpose is included.

To make the railcars operational, you need the Tomytec 'Railway Collection' N gauge power unit TM-07R while TT03-R is needed for the trailers, providing eight wheelsets and ballast weights for two vehicles. The locomotives use the minuscule TM-TR02 four-wheel power bogie motorising kit. The conversions are easy, just a plug in. The bogie railcar power units are good runners, while the small loco mechanism suffers a bit from the short wheelbase which limits electrical contact.

Prices in Japan are quite reasonable: ¥3,610 to 4,200 for the sets (at present

rates about £24.15-£28.00 or €27.50-€32.00), ¥3,060 (£20.45 or €23.30) for the power bogie, and ¥900 (£6.00 or €6.85) for the trailer chassis pack.

These products do not seem to be readily available in Europe. Although Faller is the official importer for Tomytec N gauge trams and the bus range, complementing their Car System, they have not decided to import the Nekoya Line as well. So the only way to buy these models at the moment is online, either through an auction site or direct from a Japanese model shop, many of which offer a fast and reliable mail order service. Note that shipping costs and import tax or customs duty must be added to the price.

Another obstacle is the availability: the Nekoya Line seems to be extremely popular in Japan – the models sell out quickly and even advance reservations are closed long before the actual delivery dates.



The three railcars.



The three trailers.

# Opportunity Yours

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### Trade Sales Books

**CONTINENTAL MODELLER – BACK ISSUES** – for more information on which issues are available, please contact: *Technical Advice Bureau, PECO, Underleys, Beer, Seaton, Devon, EX12 3NA, England, telephone: 01297 20580.* Price for UK Residents £3 per issue, Europe £3 per issue plus £2 postage sent Airsaver Post and rest of the World £3.00 per issue plus £3 postage sent Airsaver Post. Airmail – please enquire.

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### MODEL RAILWAY COLLECTIONS URGENTLY

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### Fairs, Sales and Swapmeets

June 2018

**24 June (Sun) 11am-3pm**

**CATTERICK FAIR**, Catterick Leisure Centre, Gough Road, Catterick Garrison DL9 3EL. 10 minutes off A1, Adults £3, Children & Seniors £2. Early entry £5. Free parking. JIM CORR.

Tel: 0750 403 5955, email: [jim\\_corr73@hotmail.co.uk](mailto:jim_corr73@hotmail.co.uk) for further information.

July 2018

**29 July (Sun) 10am-2.30pm (Early Bird from 8.30am)**

**ASTON CLINTON**, Green Park, Stablebridge Road, near Aylesbury, HP22 5NE. Free parking. Supporting the Thames Valley Air Ambulance. Refreshments by BIG RED SNACK VANS. All enquiries to BRIAN. Tel: 07864 640 320.

**31 July (Tues) 6pm-9pm**

**GARSTANG TOYFAIR**, Kirkland and Catterall Memorial Hall, corner of A6 and A586, refreshments available. JANET PEARSON. Tel: 01282 439 009.

### Private

### Private Sales

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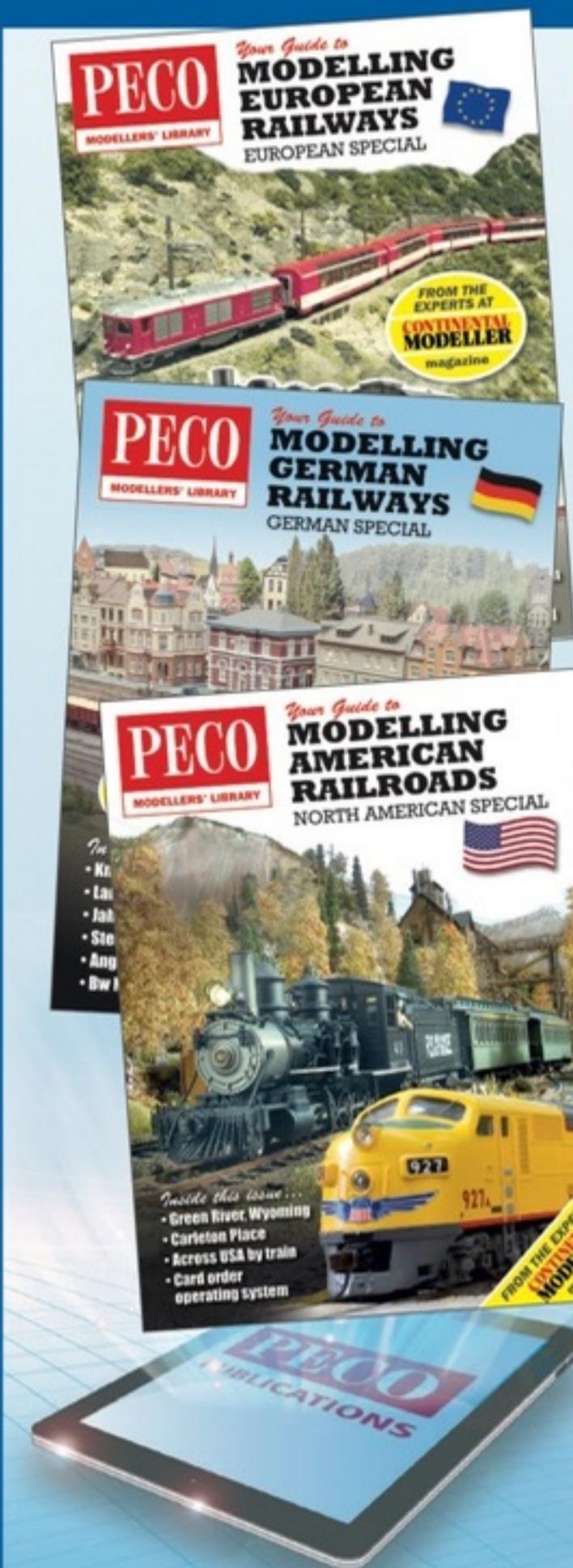
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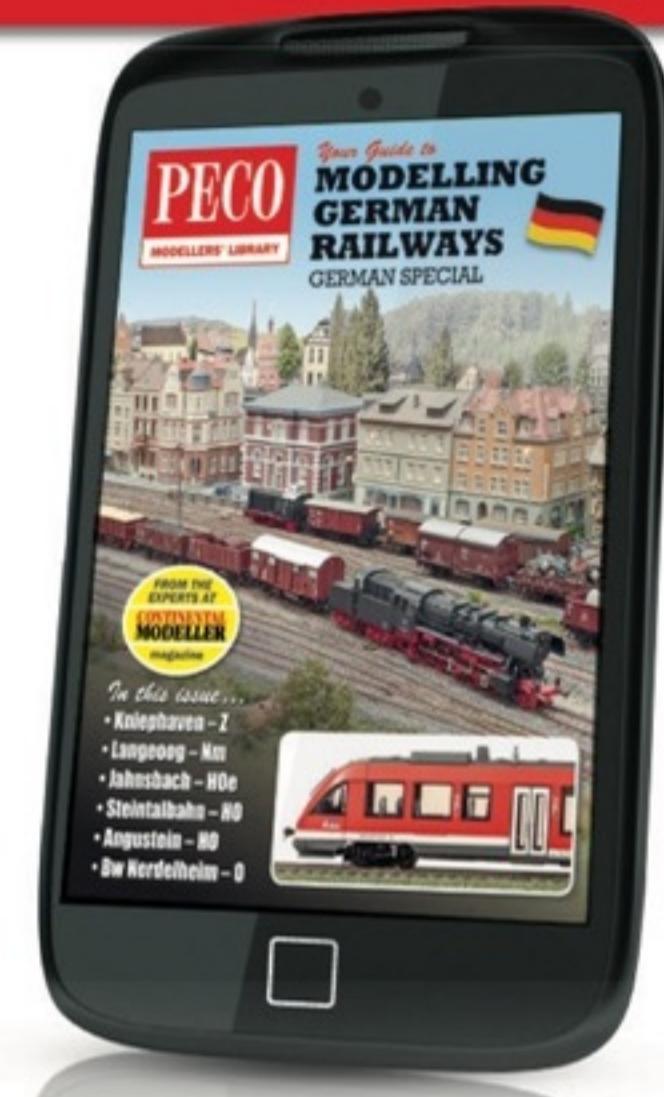
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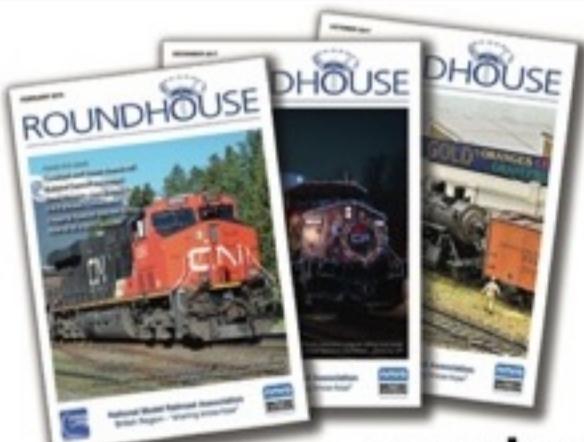
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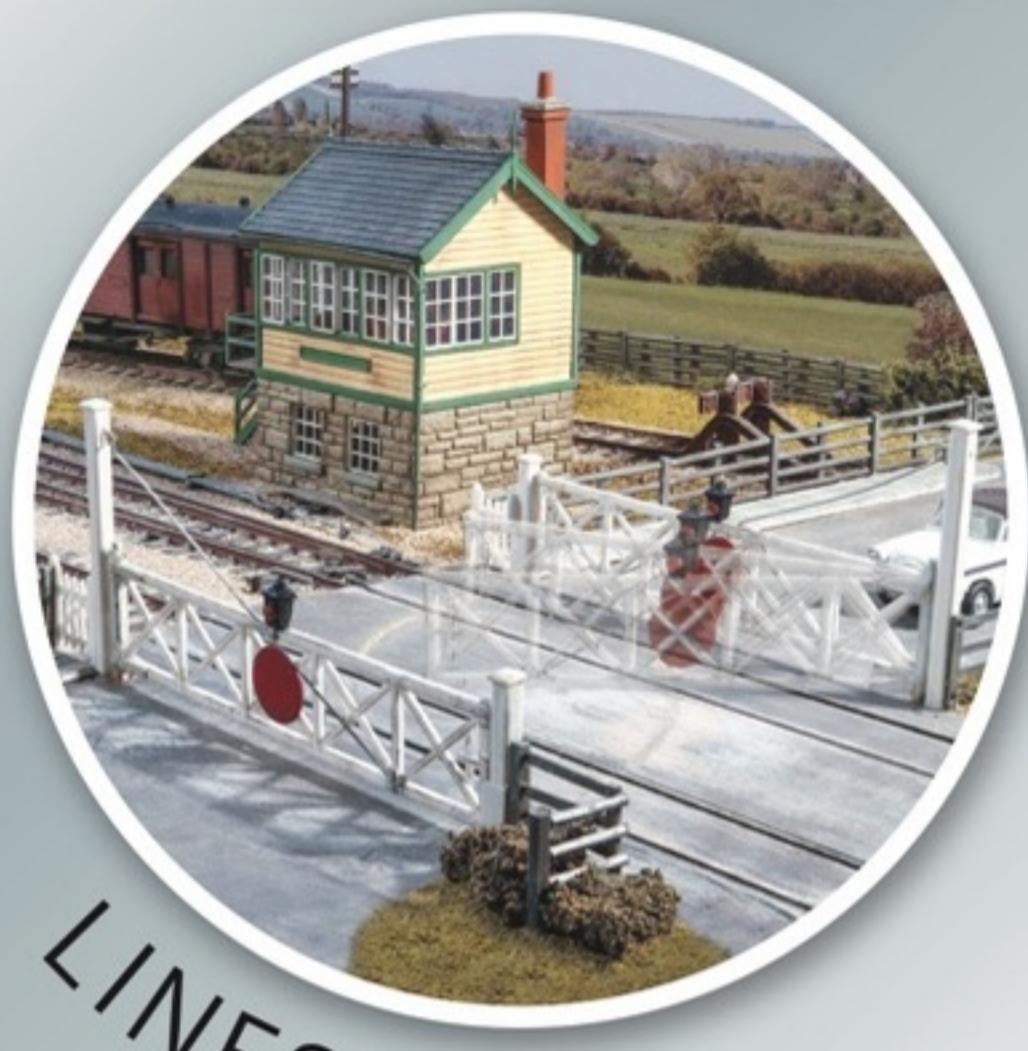
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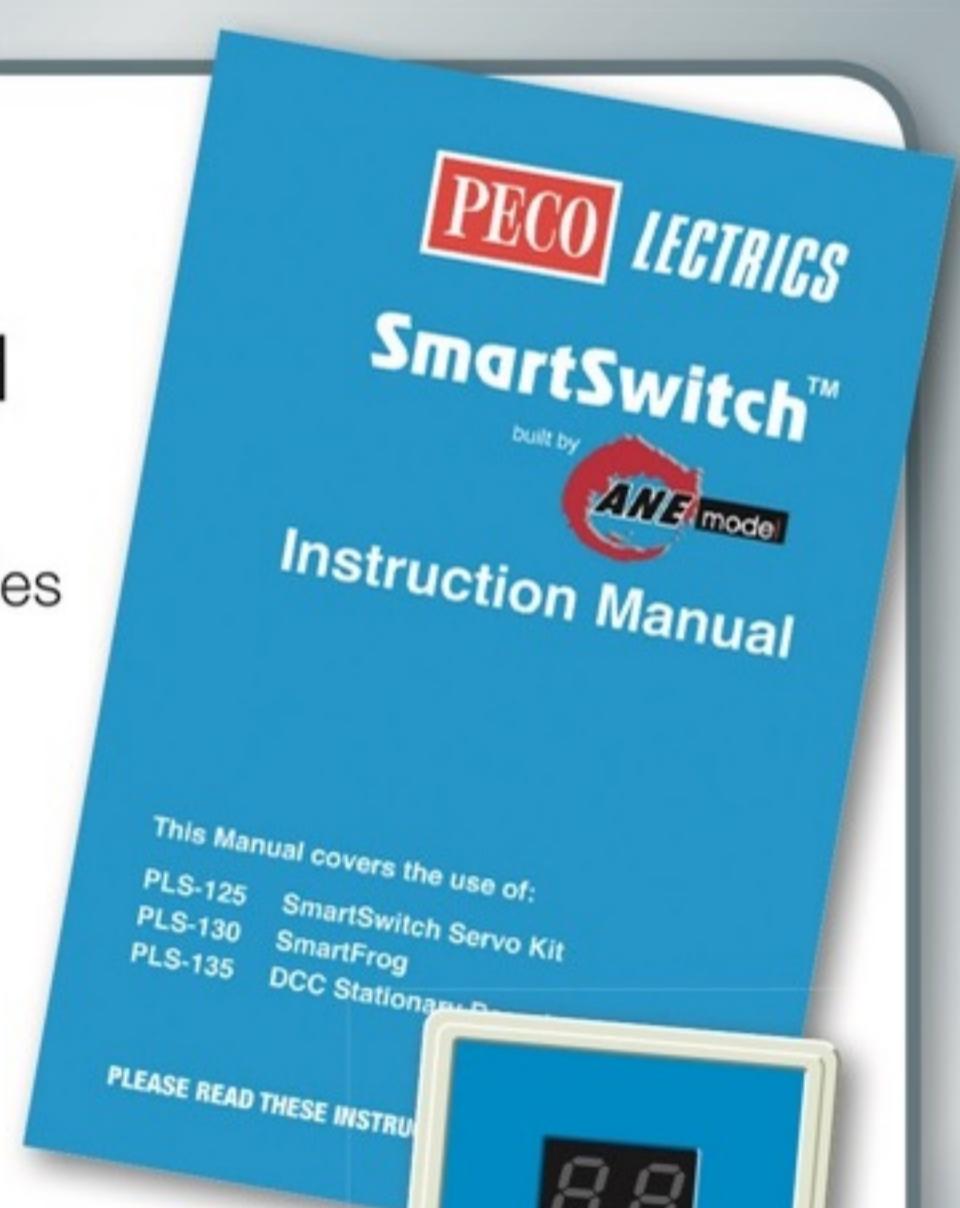
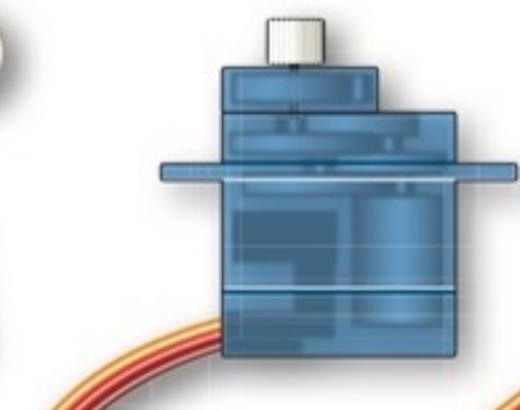
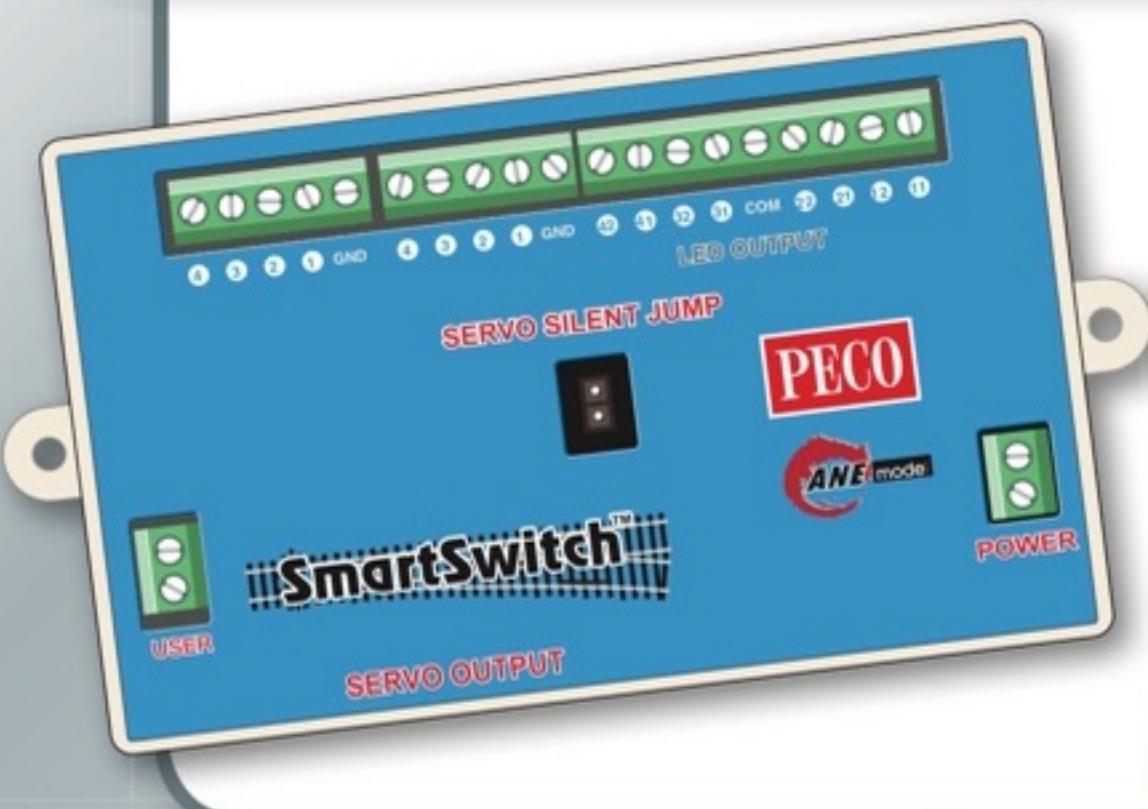
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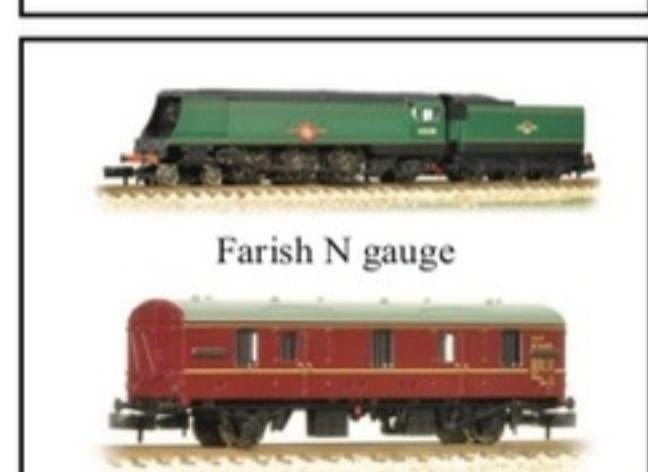
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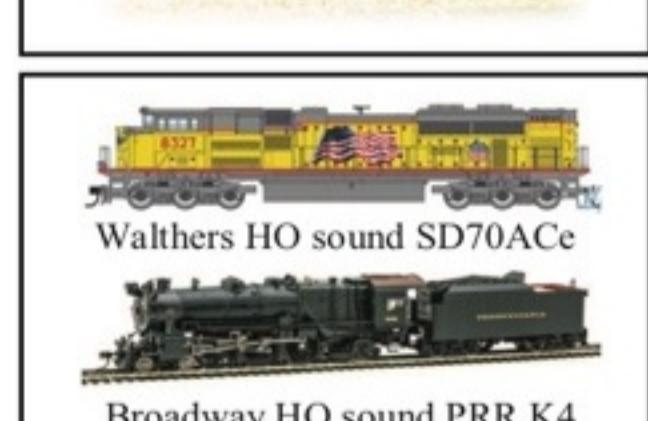
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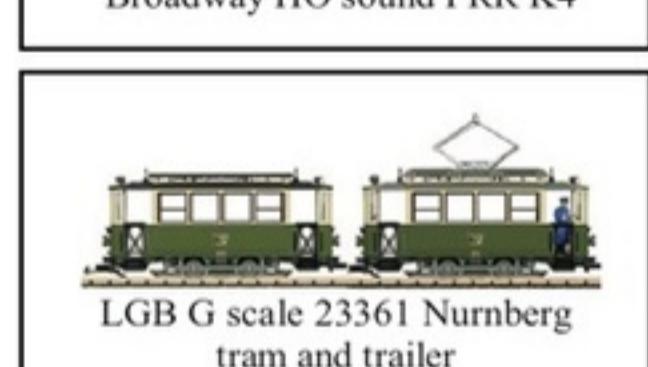
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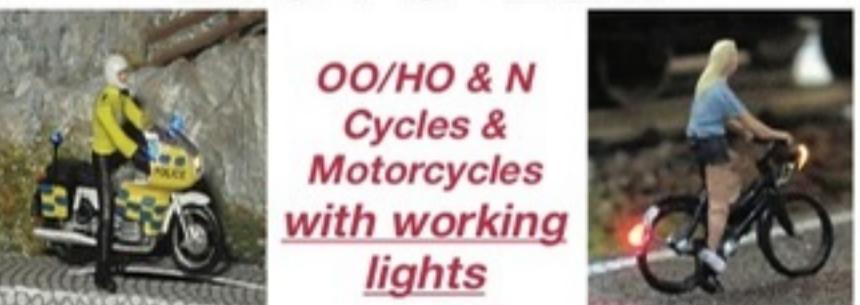
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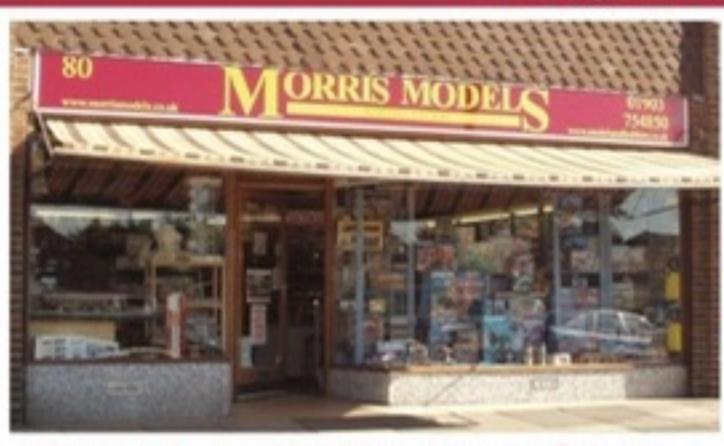
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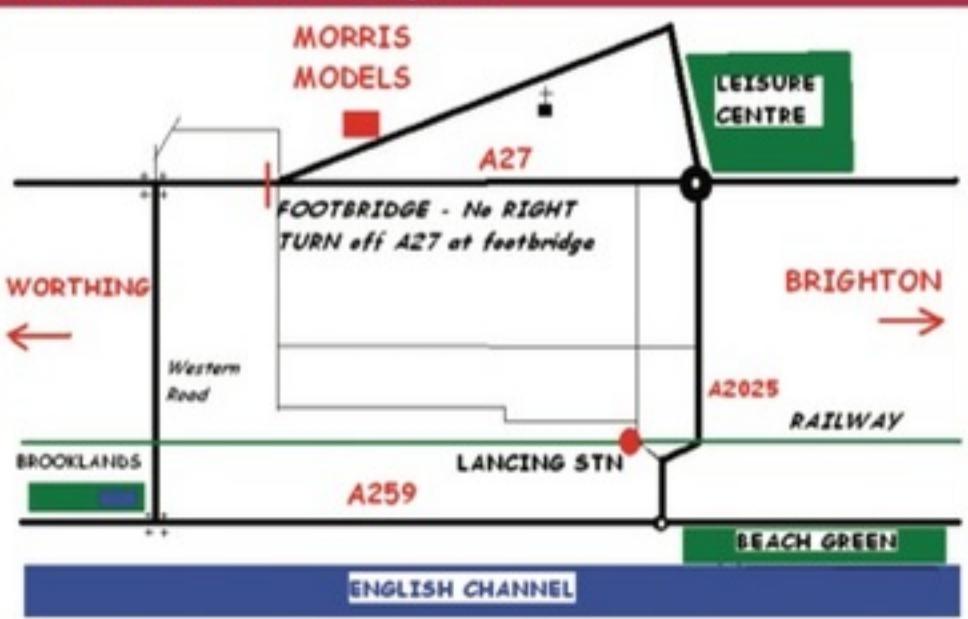


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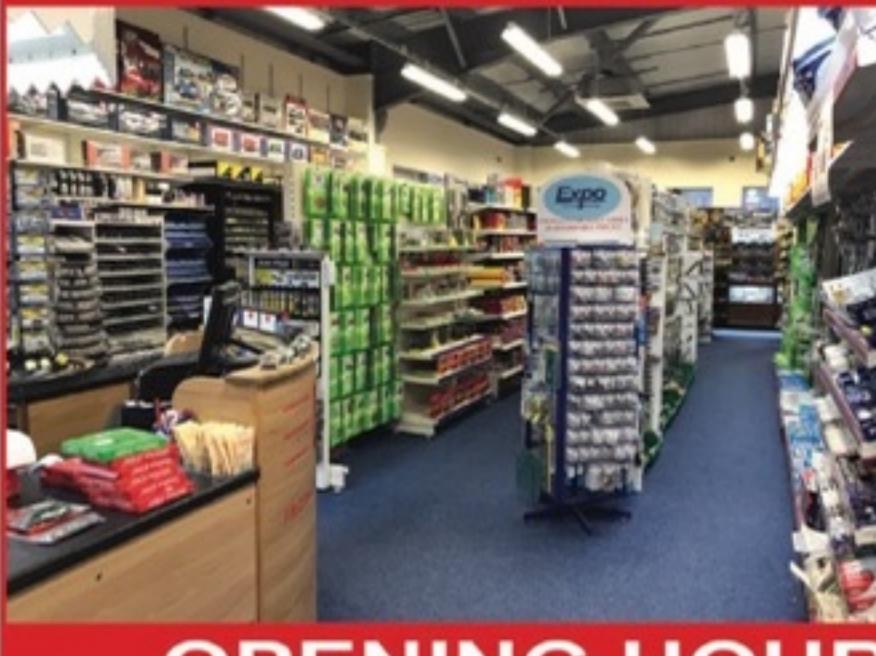


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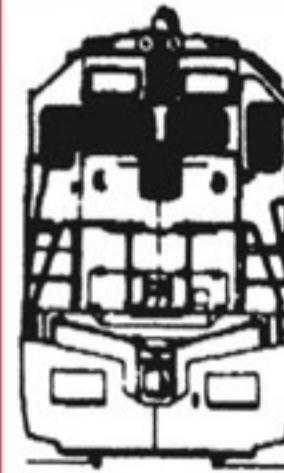
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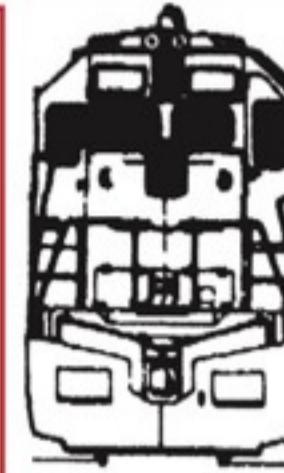


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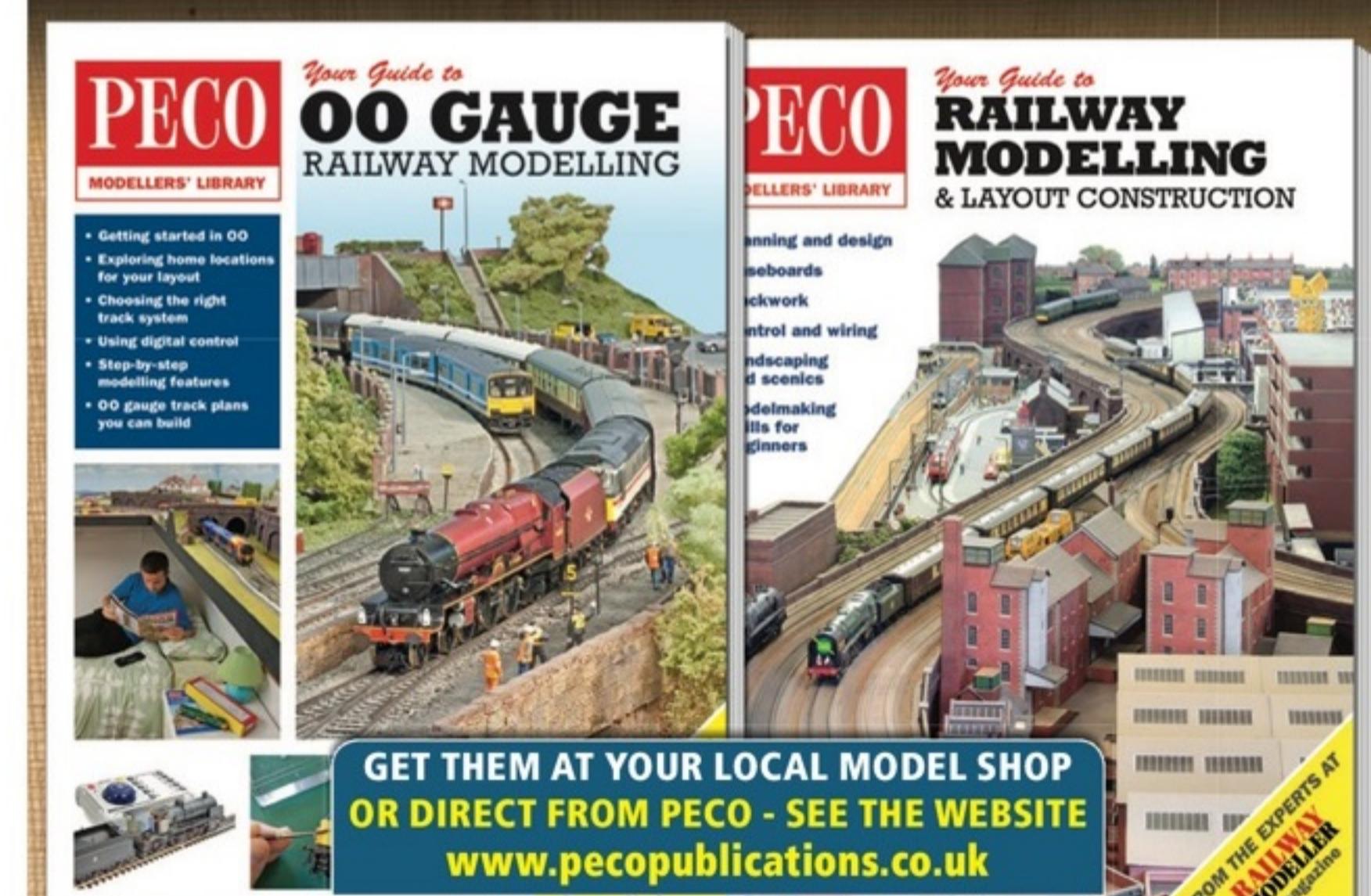
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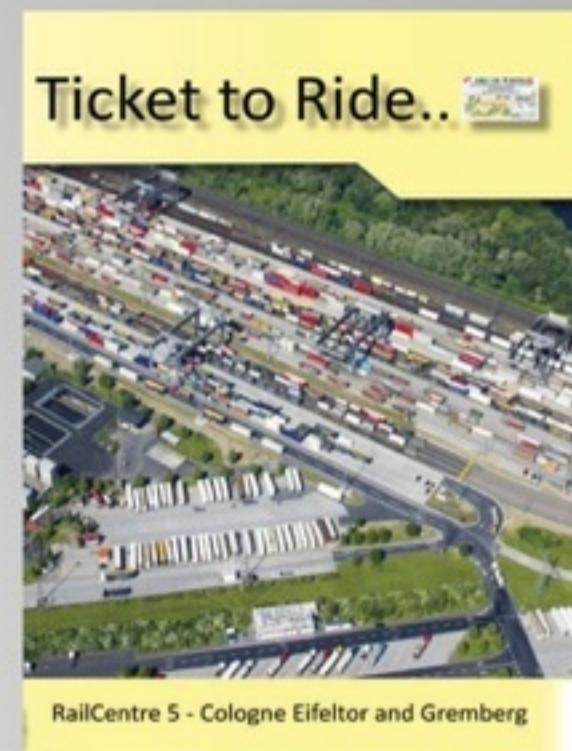
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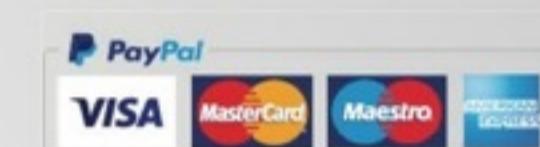
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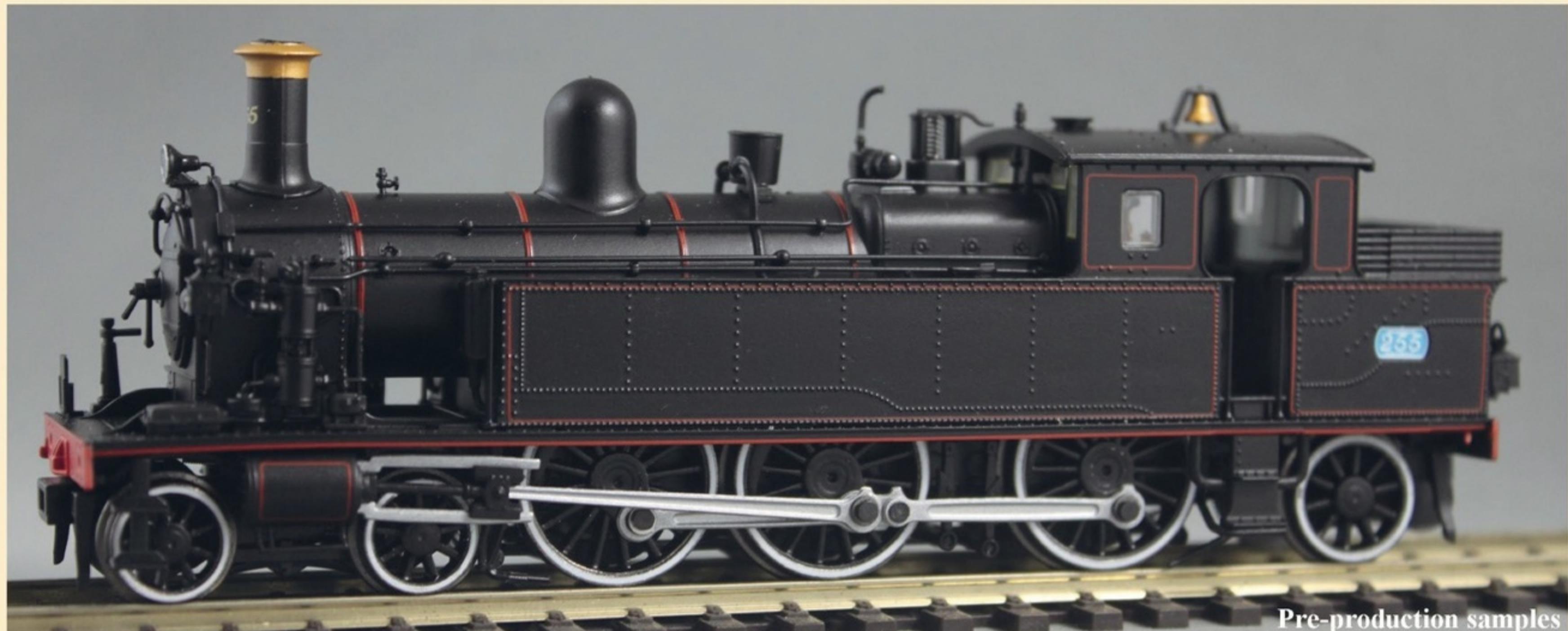
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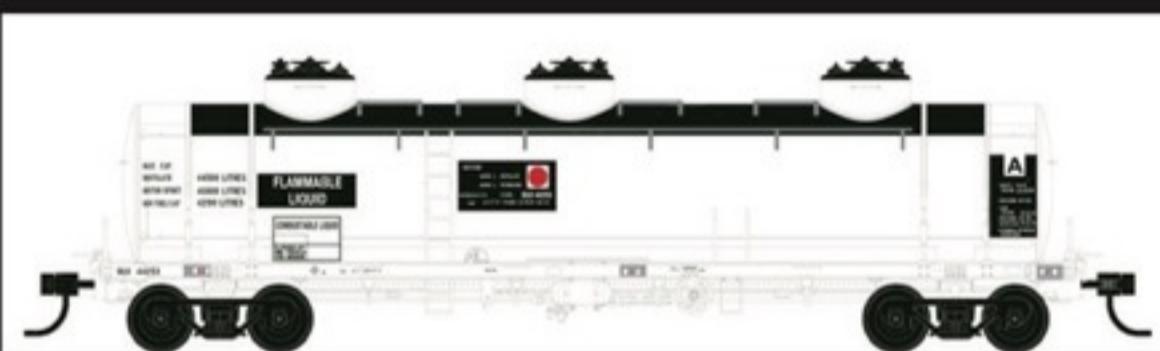
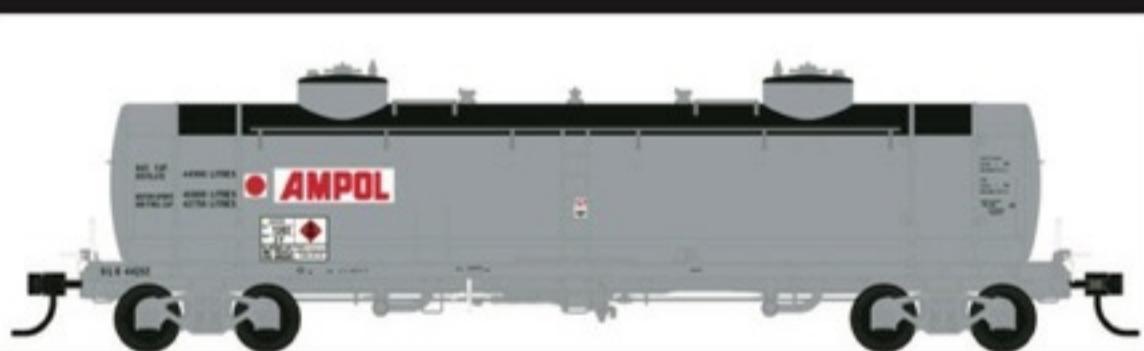
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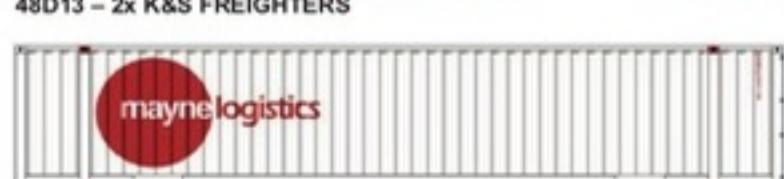
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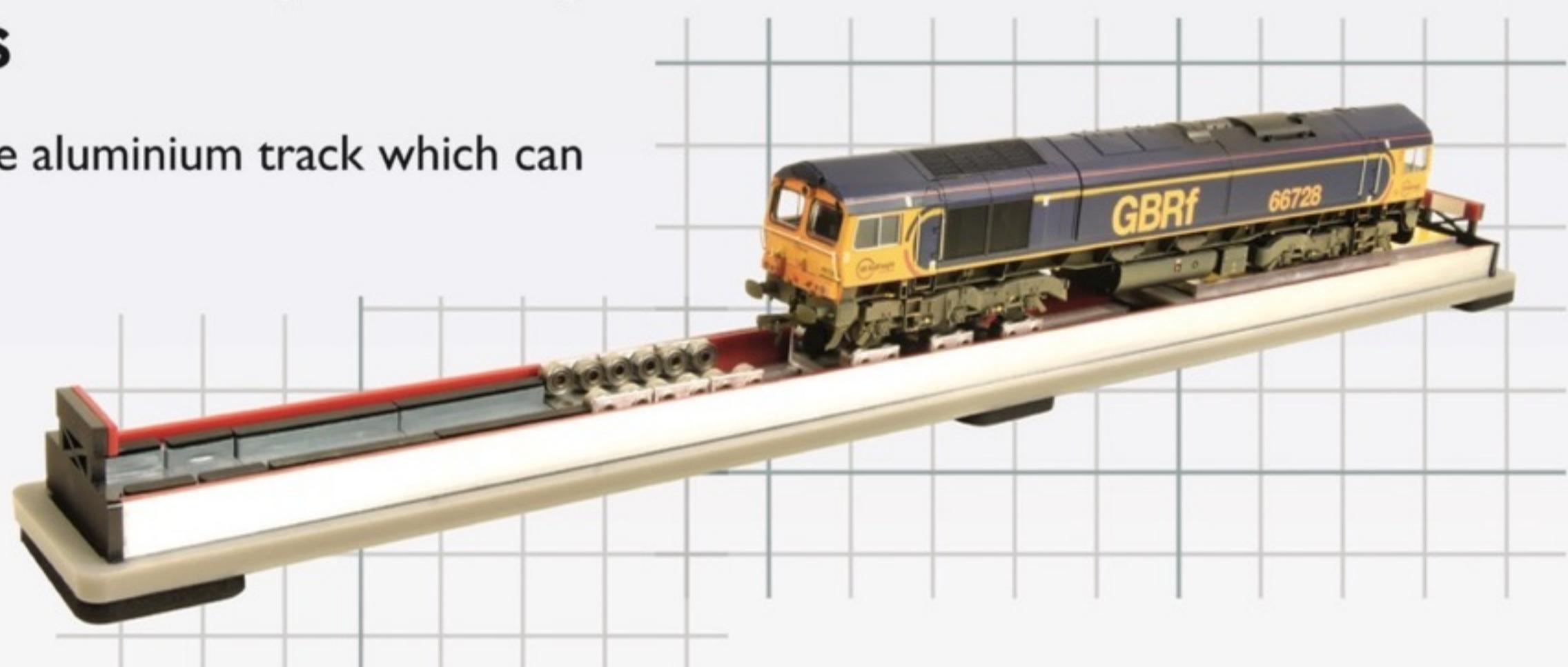
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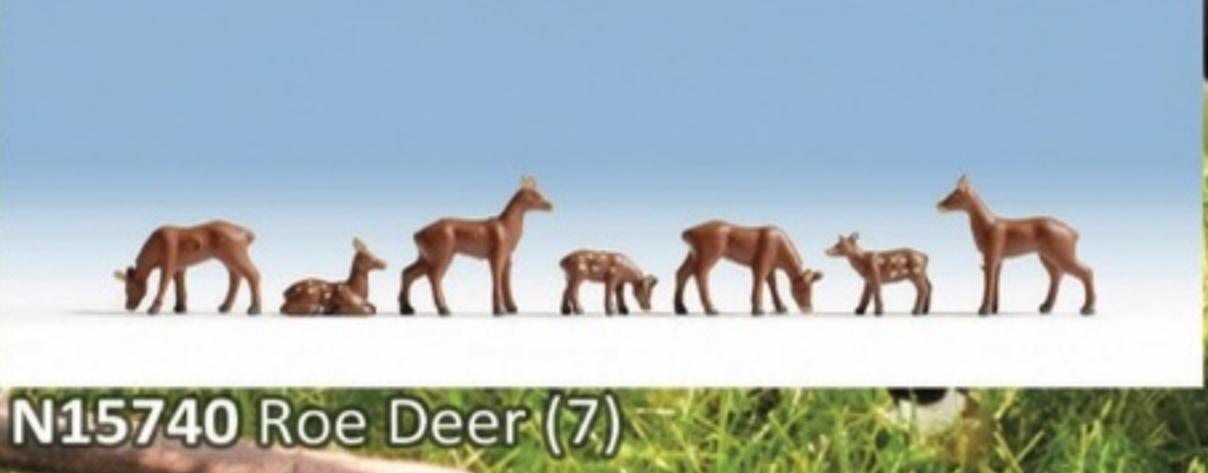
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