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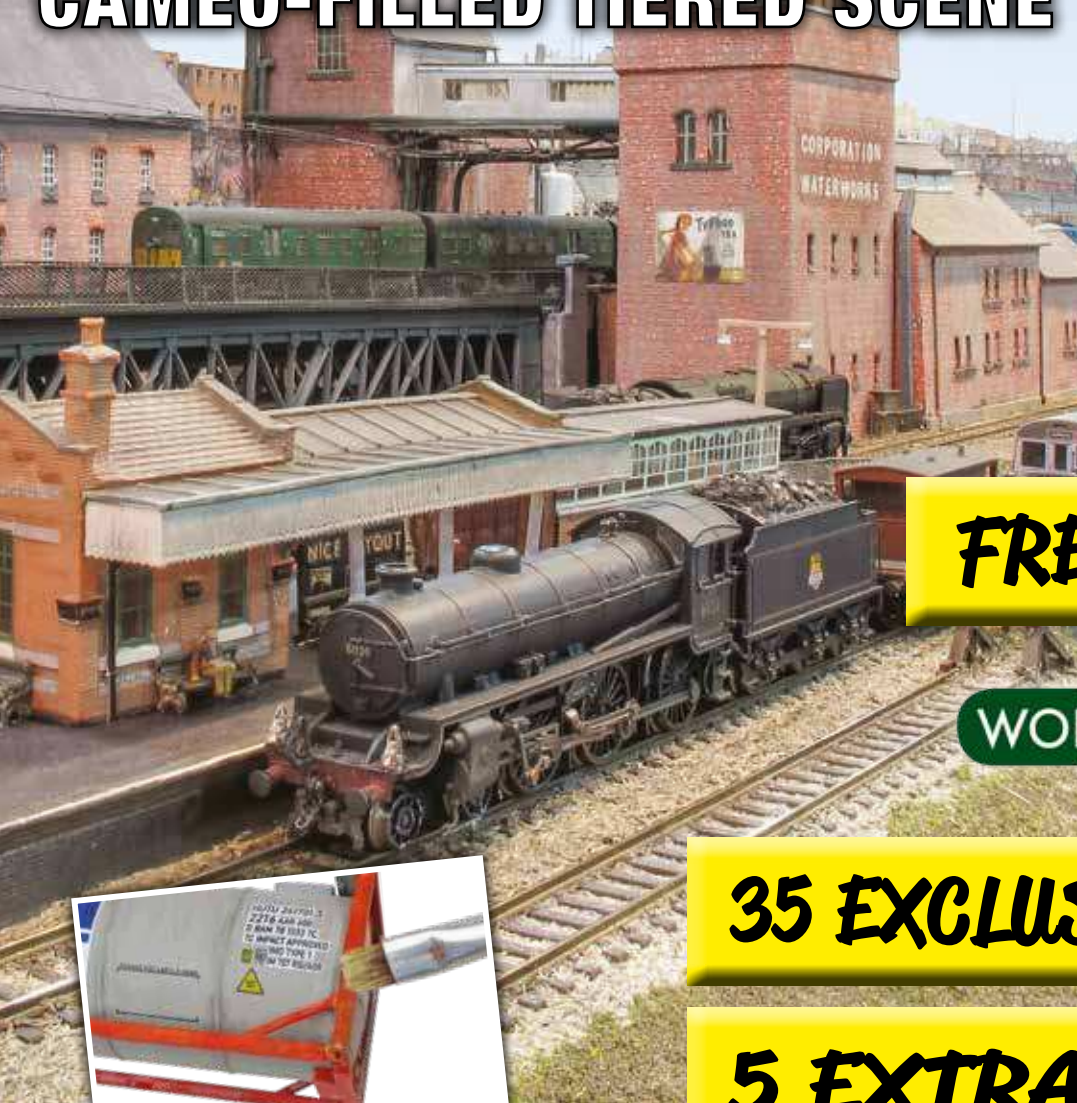
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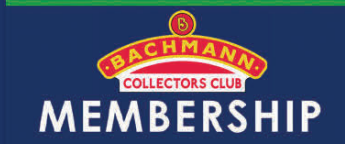
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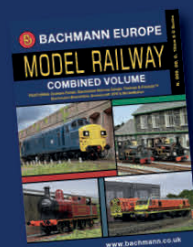
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FEATURE

World of Railways show overview

Missed our shows this year? Join Howard Smith for a look back at highlights from Doncaster, London and Bristol model railway exhibitions.

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



PLAY

PRACTICAL

New and old model railway technology

We take a look at a modern kit with an exciting control method, and then delve into the past with one of the earliest ready-to-plan buildings for OO gauge.



PLAY

LAYOUT

Explore the magnificent Ventnor West in OO

Chris Gardner explains the history and his model of the 'chocolate box-esque' Isle of Wight location.



PLAY

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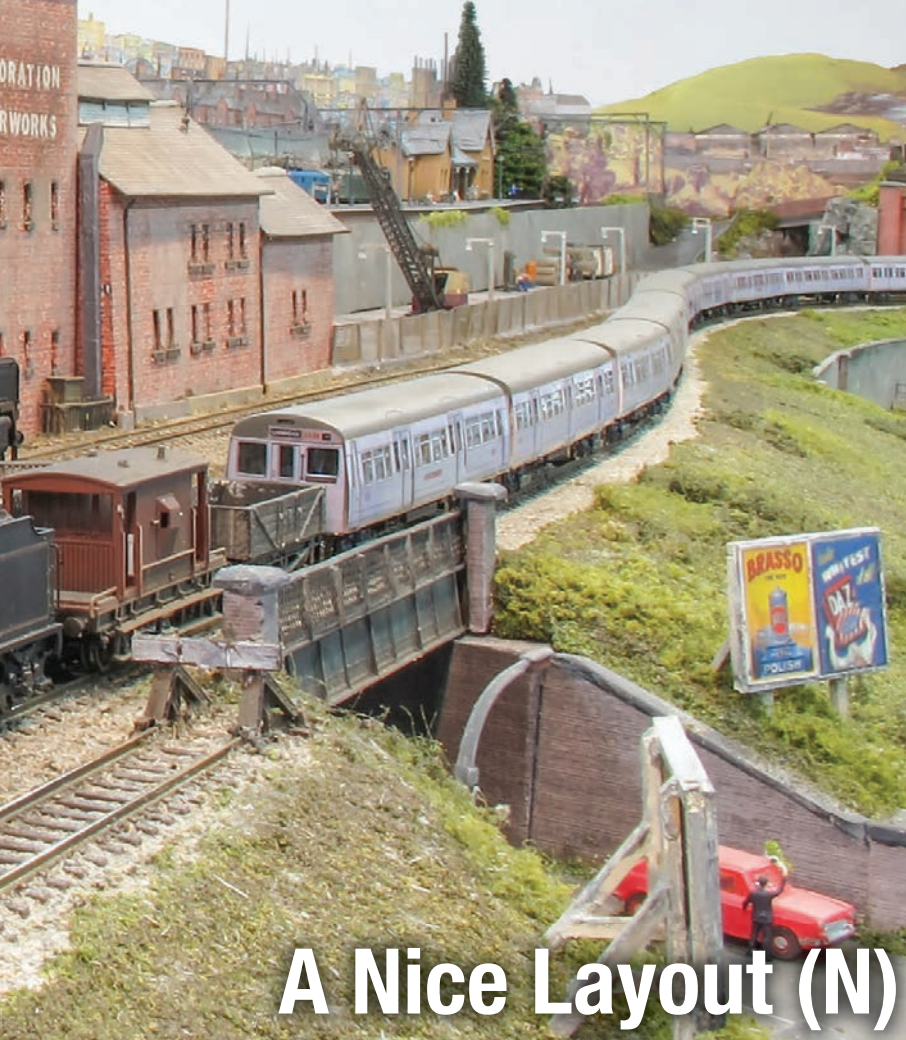
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A Nice Layout (N)

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BRITISH RAILWAY MODELLING

BRM

Welcome

to June

We're all guilty of overthinking things sometimes, and when it comes to our layouts, we can take days, sometimes months, even years, mulling over the right period, location, or even name. Quite often, when we speak to layout owners, one of the things they say they regret the most is taking too long to make a decision, instead of getting stuck into the modelling.

Faced with the challenge of thinking of a new name for his latest layout project, Gary Atkinson decided not to overthink things and simply named it, 'A Nice Layout', a comment he frequently hears at exhibitions.

In truth, 'nice' doesn't really do this fantastic layout justice, which had its exhibition debut at our Doncaster show this year. Filled with amusing cameos, Gary's urban model is home to his esoteric collection of adapted and kit-built stock and is wonderfully detailed. It's certainly one of our favourite N gauge layouts to feature in BRM this year so far.

Like last month, when we introduced a new look and feel to the layouts in the magazine, this issue introduces a refreshed news and reviews section. We hope you like the changes, and if you have any feedback on how we can improve the magazine further, please let us know at brm@warnersgroup.co.uk.

Our June issue comes jam-packed with layouts, practical features and reviews, with a variety of scales and eras covered, hopefully providing ample inspiration for our readers.

Get in touch if there is any specific you would like us to cover in the future, we always welcome ideas!

Happy Modelling!

The BRM team

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R3900	GWR Cornwall Class 08.....	£159.30
R30089	CI67 Transport for Wales.....	£174.59
R30184	CI. 67 Colas (RR plus).....	£87.30
R30178	CI. 67 DB Royal Diamond (RR plus).....	£87.30
R30042TTS	CI. 47 R.O.G. sound.....	£108.90
R30073	CI. 56 BR Blue.....	£196.19
R30177	Railroad HST pack.....	£130.95
R30099	East Coast Trains HST.....	£326.69
R30077	Midland Pullman Class 43 pack.....	£343.35
R30098	GWR Castle Class 43.....	£326.69
R30215	Platinum Jubilee HST.....	£326.70

R30166	CI 91 VTEC 91110.....	£201.60
R30031	CI87 City of Birmingham.....	£196.20
R30030	CI87 City of Glasgow.....	£196.20
R30120	BR Green Class 31.....	£196.20
R30158	BR Blue Class 31.....	£201.60
R30203	Class 121 Coca-Cola.....	£87.30
R30152	GBRf Class 66 Sector.....	£87.30
R30223	DRS Class 66.....	£87.30

HORNBY TRACK

R083	Buffer Stop.....	£2.50
R207	Track Fixing Pins.....	£3.25
R600	Straight.....	£2.15
R604	Double Straight.....	£2.55
R605	Double Curve 1st Rad.....	£3.25
R606	Curve 2nd Rad.....	£2.15
R607	Double Curve 2nd Rad.....	£3.25
R608	Curve 3rd Rad.....	£2.55
R609	Double Curve 3rd Rad.....	£3.25
R610	Short Straight.....	£1.65
R614	LH Diamond Crossing.....	£13.95
R615	RH Diamond Crossing.....	£13.95
R617	Uncoupling Ramp.....	£3.95
R618	Double Isolating Rail.....	£9.90
R620	Railer Uncoupler.....	£6.95
R628	Half Curve 3rd Rad.....	£2.80
R643	Half Curve 2nd Rad.....	£2.25
R8072	LH Point.....	£11.85
R8073	RH Point.....	£11.85
R8074	LH Curved Point.....	£18.85
R8075	RH Curved Point.....	£18.85
R8076	Y Point.....	£13.90
R8077	LH Express Point.....	£18.85
R8078	RH Express Point.....	£18.85
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R8206	Power Track.....	£8.95

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35-078	E4 BR Late.....	£93.45
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31-002	Robinson BR Early Crest.....	£94.99
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31-136DC	D11/2.....	£115.90

31-931	LMS Compound.....	£112.90
31-932DC	BR Compound.....	£127.75
31-465	C Class BR.....	£93.46
31-461A	C Class SR Black.....	£106.20
31-462A	C Class BR Early.....	£106.20
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31-635B	GWR 64xx.....	£118.95
31-976B	BR 3mt green.....	£140.20
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32-614	CI. 90 BR RES red/grey.....	£195.45
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32-612A	CI. 90 Freightliner green.....	£195.45
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31-265A	Class 419 MLV BR green.....	£135.95
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31-498	Class 158 ScotRail.....	£280.45
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31-502A	CI158 WPTE Metro.....	£280.45
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NR-P987C	Shell/BP black no. 5177.....	£13.45
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NR-P987EW	Shell/BP black no. 5172 withd.....	£14.25
NR-P987FW	Shell/BP black no. 5178 withd.....	£14.25
NR-P987G	Shell/BP black no. 5171.....	£13.45
NR-P987H	Shell/BP black no. 5176.....	£13.45
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NR-P987JW	Shell/BP black no. 5173 withd.....	£14.25
NR-P987KW	Shell/BP black no. 5179 withd.....	£14.25
NR-P987LW	Shell/BP black no. 5168 withd.....	£14.25
NR-P993A	BP Grey no. BP067266.....	£13.45
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NR-P993C	BP Grey no. BP067095.....	£13.45
NR-P993DW	BP Grey no. BP067168 withd.....	£14.25
NR-P993EW	BP Grey no. BP067177 withd.....	£14.25
NR-P993FW	BP Grey no. BP067184 withd.....	£14.25
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NR-P994EW	BP Green no. BP067783.....	£14.25
NR-P994FW	BP Green no. BP067574.....	£14.25
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NR-P995B	BP Black no. BP065764.....	£13.45
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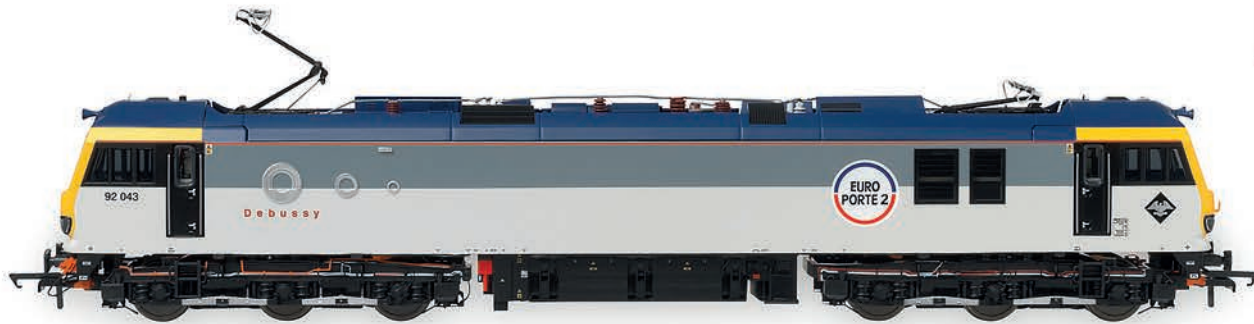
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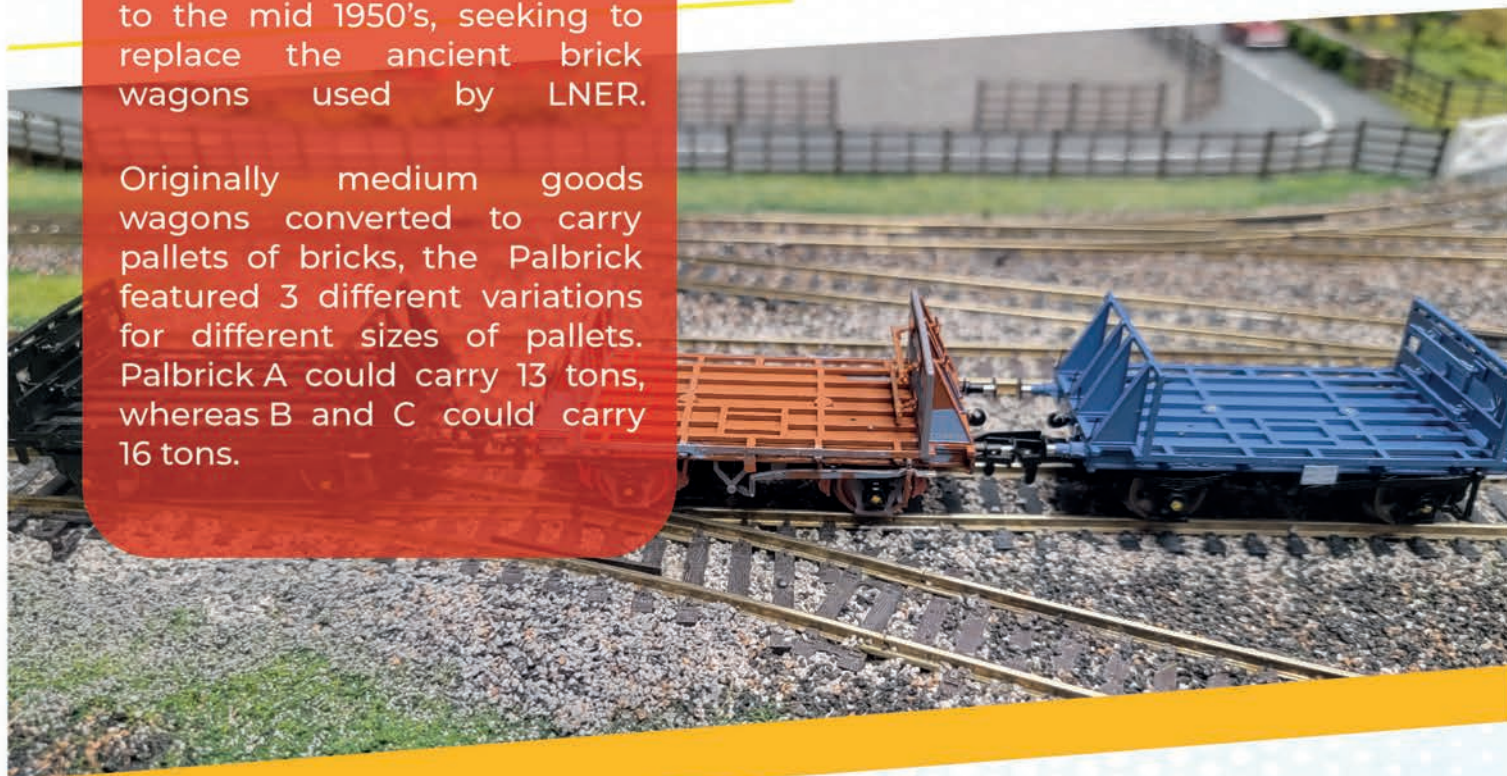
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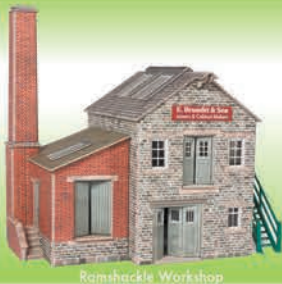
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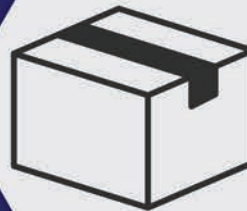
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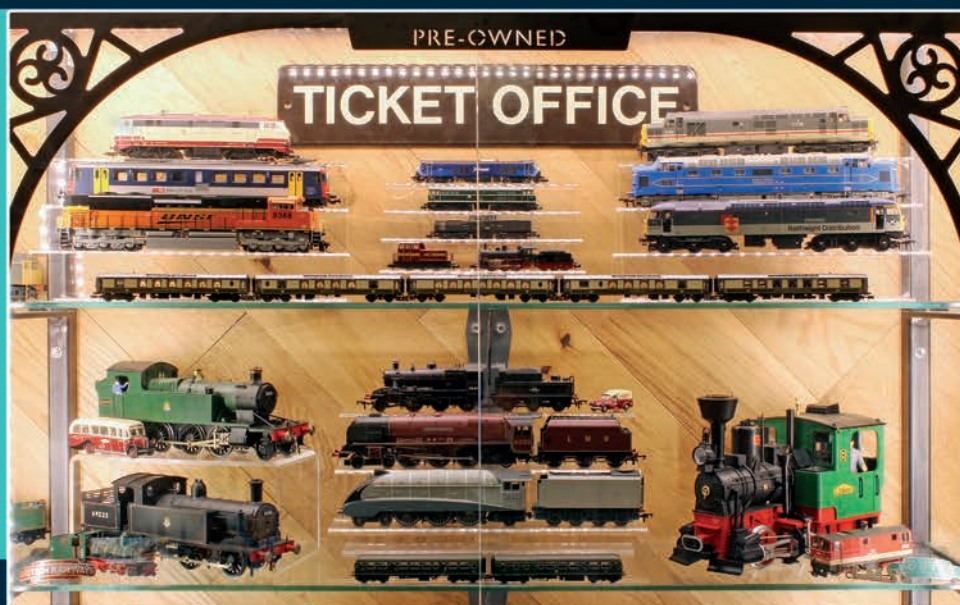
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00 Gauge Steam Locos

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GER 'Buckjumper' 0-6-0T
Due: Q2 2024
Price: from £139.99
Digital: 18-pin

dapoi



Hawthorn Leslie 0-4-0ST
Due: Q3 2023
Price: from £107.41
Digital: 18-pin



Class 43xx Mogul 2-6-0
Due: Q3 2023
Price: from £154.18
Digital: 18-pin

HELJAN



Class 02 Tango 2-8-0
Due: Q4 2023
Price: from £186.15
Digital: 18-pin

HORNBY



LNER Class B17/5 4-6-0
Due: Q4 2024/ Q1 2025
Price: £188.99
Digital: 21-pin



LMS Turbomotive 4-6-2
Due: July 2023
Price: from £239.84
Digital: 21-pin

3x3



Metropolitan E Class 0-4-4T
Due: Early 2024
Price: from £153
Digital: 18-pin



LMR 'Lion' 0-4-2
Due: Q2 2023
Price: from £152.96
Digital: 18-pin



LBSCR E1 0-6-0T
Due: TBC 2023
Price: from £140.21
Digital: 18-pin



Class 15xx 0-6-0PT
Due: Q2 2023
Price: from £127.50
Digital: 18-pin



HR 'Jones Goods' 4-6-0
Due: TBC 2023
Price: from £186.96
Digital: 21-pin

00 Gauge Diesel & Electrics

accurascale



Class 31
Due: Q2 2023
Price: from £169.99
Digital: 21-pin



Class 50
Due: Q4 2023
Price: from £169.99
Digital: 21-pin



Class 66
Due: Q1 2024
Price: from £169.99
Digital: 21-pin

HELJAN



BR NBL Prototype 10800
Due: May 2023
Price: from £216.71
Digital: 21-pin



Class 02 Shunter
Due: Q4 2023
Price: from £143.65
Digital: 18-pin



Class 26/1
Due: Q4 2023
Price: from £177.65
Digital: 21-pin



Class 47
Due: Q3 2023
Price: from £212.46
Digital: 21-pin



Class 57
Due: Oct 2023
Price: from £160.65
Digital: 21-pin



Class 86/2
Due: Q1 2024
Price: from £216.71
Digital: 21-pin

HORNBY



Ruston 88DS Shunter
Due: Jun 2023
Price: £102.59
Digital: 6-pin



Class 04 Shunter
Due: TBC
Price: TBC
Digital: TBC

00 Gauge Multiple Units

HELJAN



W&M 4-wheel Railbus
Due: Q3 2023
Price: from £169.15
Digital: 21-pin



GWR AEC Parcels Railcar
Due: Sep 2023
Price: from £198.90
Digital: 21-pin



Class 104
Due: TBC 2023
Price: from £330.65
Digital: 21-pin

HORNBY



Class 423 4-VEP EMU
Due: Q3 2023
Price: £326.69
Digital: 21-pin



Class 755 FLIRT BIMU
Due: 2023/ 2024
Price: £326.69
Digital: 21-pin

00 Gauge Coaches & Wagons

accurascale



BR Mk1 Suburbans
Due: Q3 2023
Price: £64.95
Digital: N/A



BR Mk2B & Mk2Cs
Due: Q2 2023 & Q1 2024
Price: £59.95
Digital: N/A



NER 20t Hoppers
Due: Q1 2024
Price: £84.95 (3pk)
Digital: N/A



SR 10 ton Banana Vans
Due: Q4 2023
Price: £84.95 (3pk)
Digital: N/A

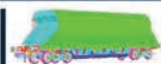


FNA-D Nuclear Flasks
Due: Q3 2023
Price: £74.95 (2pk)
Digital: N/A

dapoi



14 ton Air Ministry Tanks
Due: Q3 2023
Price: £32.13 each
Digital: N/A



JHA P&D Hoppers
Due: Q3 2023
Price: from £46.75
Digital: 6-pin (Taillamps)



GWR Tophat City Coaches
Due: Q2 2023
Price: £55.08 each
Digital: 6-pin



GWR Churchward Autocoach
Due: Q1 2024
Price: from £68
Digital: 21-pin

HELJAN



TCV Car Transporters
Due: Q2 2023
Price: £194.65 (3pk)
Digital: N/A

HORNBY



TTA Tank Wagons
Due: Q3 2023
Price: £31.49 each
Digital: N/A



YMO/YMA Salmon Flats
Due: Q2 2024
Price: £34.64 each
Digital: N/A

3x3



Wisbech & Upwell Tramcar
Due: Q2/ Q3 2023
Price: £63.71
Digital: N/A



GWR B-Set Coaches
Due: Q2/ Q3 2023
Price: £63.71
Digital: N/A



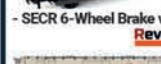
LMS 5-Plank Wagons
Due: Q4 2023/ Q1 2024
Price: £28.01 each
Digital: N/A



Other Plank wagons due 2023/ 2024 from Rapido:
- GWR 5-plank Wagons, SR 8-Plank Wagons & RCH 1907 5 & 7-plank Wagons



GWR Toad Brake Van
Due: Q2 2023
Price: £33.96
Digital: N/A



SECR 6-Wheel Brake van also due in 2023

Revolution



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Price: £48.95
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Due: Q3 2023
Price: from £93.50
Digital: PCB Swap required



GWR 63xx Mogul 2-6-0
Due: Q1 2024
Price: from £156.06
Digital: 18-pin



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Due: Q3 2023
Price: from £128.52
Digital: 6-pin



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Price: from £189.98
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Price: from £127.46
Digital: 18-pin



Class 43 HST (Updated)
Due: Q3 2023
Price: from £156.06
Digital: 18-pin



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Due: Q2/ Q3 2023
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Digital: 18-pin



Class 66 (New Tool)
Due: Q2 2024
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Digital: 18-pin



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Price: from £110
Digital: 18-pin

3x3



Class 44, 45 & 46 'Peak'
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Price: TBC
Digital: 18-pin

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Due: Q2 2023
Price: from £119.30
Digital: 18-pin



Class 156 Super Sprinter
Due: Q2 2023
Price: from £146.88
Digital: 18-pin



Class 221 Super Voyager
Due: Q3 2023
Price: £213.78
Digital: 6-pin

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Mk3 Packs & DVTs
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Price: Prices vary
Digital: 18-pin



Grampus Open Wagons
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Digital: N/A



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Price: £84.95
Digital: N/A



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FNA-D Nuclear Flasks
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LSWR Class B4 0-4-0T
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Digital: 21-pin



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Digital: 21-pin



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Digital: 21-pin



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Digital: 21-pin

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Digital: ESU XL



Class 45 Peak
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Due: Q4 2023
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OO Gauge (1:76 Scale)

Steam locos



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OR76AR005 Class 415 Adams Radial 4-4-2T No.5 in East Kent Railway green (Oxf) (RRP £109.95) **BARGAIN** £74



R3986 Class 9F 2-10-0 92167 in BR Black late crest (Hor)..... £228
R3987 Class 9F 2-10-0 92104 in BR Black Late Crest (Hor)..... £228



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Diesel locos



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R30079TTS Class 47/7 47712 'Lady Diana Spencer' in ScotRail (as part of the 'Staycation Express') - TTS sound fitted - Railroad Plus range (Hor) £109



4D-005-000 Class 59/0 59005 'Kenneth J Painter' in Foster Yeoman (Dap) £147.22



4D-005-001 Class 59/1 59103 'Village of Mells' in ARC (Dap)..... £147.22



R30152 Class 66/7 66793 in Railfreight Construction triple grey GB Railfreight markings (Hor)..... £87.29



9771 Ruston 165 0-6-0 diesel shunter 45 in NCB dark green (Hel)..... £169.15



9770 Ruston 165 0-6-0 diesel shunter in industrial yellow (Hel) £169.15

Electric locos



R3568 Class 71 E5018 in BR green no yellow ends (Hor) (RRP £206.99) **BARGAIN** £89



R30030 Class 87 87006 'City of Glasgow' in BR large logo grey (Hor) £196.19



R30031 Class 87 87009 'City of Birmingham' in Intercity Executive (Hor) £196.19

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R30077 Pair of Class 43 HST Power Cars M43046 and M43055 in Midland Pullman blue (Hor) £343

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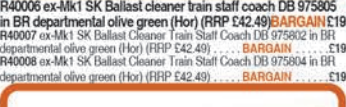
R60081 BR QTV (ex-SR Van C Passenger Brake) Van in BR departmental olive green - 975140 (Hor) £32.84



R4879A Collett 57 'Bow ended' non-corridor composite (Right-hand) W6242W in BR crimson (Hor) (RRP £59.49) **BARGAIN** £25



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R40007 ex-Mk1 SK Ballast Cleaner Train Staff Coach DB 975802 in BR departmental olive green (Hor) (RRP £42.49) **BARGAIN** £19



R40008 ex-Mk1 SK Ballast Cleaner Train Staff Coach DB 975804 in BR departmental olive green (Hor) (RRP £42.49) **BARGAIN** £19



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R4974A Mk1 RB restaurant buffet IC1653 in Intercity Executive (Hor) (RRP £42.49) **BARGAIN** £19



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009 Narrow Gauge

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N Gauge

Steam locos



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DMUs



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O Gauge (1:43 Scale)

Steam locos



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FACTFILE

SCALE/GAUGE: 2mm:1ft scale/9mm gauge/N

SIZE: 18ft x 2ft

ERA/REGION: 1960s to present/preservation

LAYOUT TYPE: Continuous loop

A NICE LAYOUT

Filled with amusing cameos, and popular at shows, Gary Atkinson's urban model is home to his esoteric collection of adapted and kit-built stock.

Words: Gary Atkinson **Photography:** Phil Parker



A repainted and renumbered Graham Farish Class 37 crosses the road bridge and enters the station. This is a Darnall-based locomotive representing the York/Bournemouth green rake that traversed the Great Central in the '60s. Lattice bridge ironwork is brass etch from Scalelink Fretcetera.

I couldn't think of a proper name for my latest creation, so I went for a comment often heard at exhibitions – “nice layout”.

Having modelled specific periods in the past, this layout was created to display the locomotives and trains I have collected over 30 years of N gauge modelling. The closest to a theme for ‘Nice Layout’ is Great Central in the ‘60s and into the preservation era, plus whatever I would like to run at the time. A purposefully vague timescale means I can run whatever takes my fancy, hopefully, for

the entertainment of the paying public and fellow enthusiasts.

Layout construction

Baseboards are made from a 2in x 1in timber frame with 5mm ply surface and 5mm cork tiles glued to this, the backscene boards being hardboard braced with timber. Track is Peco Code 55 on the scenic side and Code 80 in the 14 storage sidings. Seep point motors are used throughout. When ballasting N gauge track, it is important not to use coarse material as this



makes the track look too overscale. I have used flock/scatter mixed with wallpaper paste and wood glue, airbrushing certain areas for a tonal effect, and the all-important painting of the rail sides to cover the shiny surface. All this helps to reduce the overscale appearance of ready-made track (hats-off to 2FS modellers!).

Wiring is basic analogue, kept as simple as possible and Gaugemaster handheld controllers are used.

The buildings are largely scratch-built from card, braced with balsa wood. The factory complex is

mainly in low-relief buildings. All the window apertures were grafted into the basic shape of the building using leftover Metcalfe kits. Various brick papers were used and weathered with a wash of matt black or brown paint. Two of the waterworks buildings are detachable to hide the baseboard and backscene joints.

The main platform buildings are Farish, slightly modified with columns under the canopy and repainted awnings, all with a coat of matt varnish. At one end of the layout is Atkinson Terrace, again,

BELOW: A Clayton Class 17 hauls a short coal train. A locomotive not known for its pulling power, wagons are a mixture of kit-built and RTR hoppers. A crane rests in the waterworks yard (Langley kit). In the background, a Class 506 'Hadfield unit' waits at the upper-level station.





a Metcalfe terraced house kit modified on an incline and covered with brick paper. This backs onto the rear of Sunshine Desserts delivery area. Corrugated sheets by Noch form the roof.

The hills are formed from expanded polystyrene glued on with PVA, shaped with a craft knife and sandpaper (very messy). I then painted on a coat of matt black emulsion. I've used Woodland Scenics scatter and foliage mixed with wallpaper paste and PVA for the greenery.

The upper branch line has overhead catenary for running electric trains. This is all scratch-built from brass 'I'-section and 0.31 mm diameter brass wire. This looks fine enough, but the pantographs need to run just below the wire, or they will push the wire out of the way or get tangled. A tricky job to do, and you need to have a small amount of tension on the wires to keep them looking straight.

Comical cameos

The baseboard joint is partly hidden by the walkway between the waterworks buildings, the more observant visitor may notice Basil Fawlty thrashing his car and Del Boy's three-wheeled van, both from Shapeways 3D printers. The local shop, inspired by TV's *League of Gentlemen*, occupies the hill at the left-hand side of the layout.

I like to run unusual locomotives and trains rather than all out-of-the-box items, the oldest item is a

Cravens Class 105 DMU, from a BH Enterprises kit. It had an old Farish motor bogie to power it at first, but that became too unreliable, so it was shelved for many years. I recently put a new Farish Class 108 chassis on it and now it runs well. The underframe and bogies are not correct, but the cab ends immediately identify any DMU, so I'm happy to run it at present. I have a GT3 locomotive, which has a resin body and tender by The Clays Railway Circle/Parkwood. The Co-Bo I use is an old Ian Stoate kit. The S160 is a modified Japanese locomotive from Kato with a scratch-built Plastikard tender.

I have a train of Class 76s being towed to a scrap yard. These are Shapeways 3D-printed items with chassis and bogies from Minitrix Class 27 spares, leftover from my Class 76s on my 'Woodhead' layout. The train of London Underground A60 stock was purchased from Electra Rail Graphics. Again, 3D-printed bodies with self-adhesive sides and ends, running on Kato bogies.

Class 506 EMUs can be seen on the upper level. I've used Bill Bedford sides on coach bodies and 3D-printed bodies for these. They run on Green Max and Tomix Chassis.

The 3D-printed items I've had from Shapeways claim to be fine detailed, smooth finish items. This was not the case with the locomotive bodies I received. The Class 76s had quite prominent layer lines and underground stock needed a high build auto

ABOVE: A 'Britannia' with a long rake of Mk.1s passes through the station. All locomotives have the front coupling removed and lamps fitted for extra detail.

RIGHT: A B1 hauls a rake of ex-works 'A60' stock. Built at Cravens in Sheffield, they were dragged down the GC to London for service on the underground network.



Brand new Hymeks from Newton le Willows works being delivered down the GC to the Western Region this time. These are Dapol models with the front one powered and three unpowered dummies. The LNER Sentinel steam railcar is a Langley kit with a heavily-butchered Minitrix Warship chassis. Built by a friend back in the 1970s, it still runs well and has featured on several layouts since then. The (not so) local shop for local people overlooks the scene.



primer to make the roofs look smooth. They are a good base to work from, but require attention.

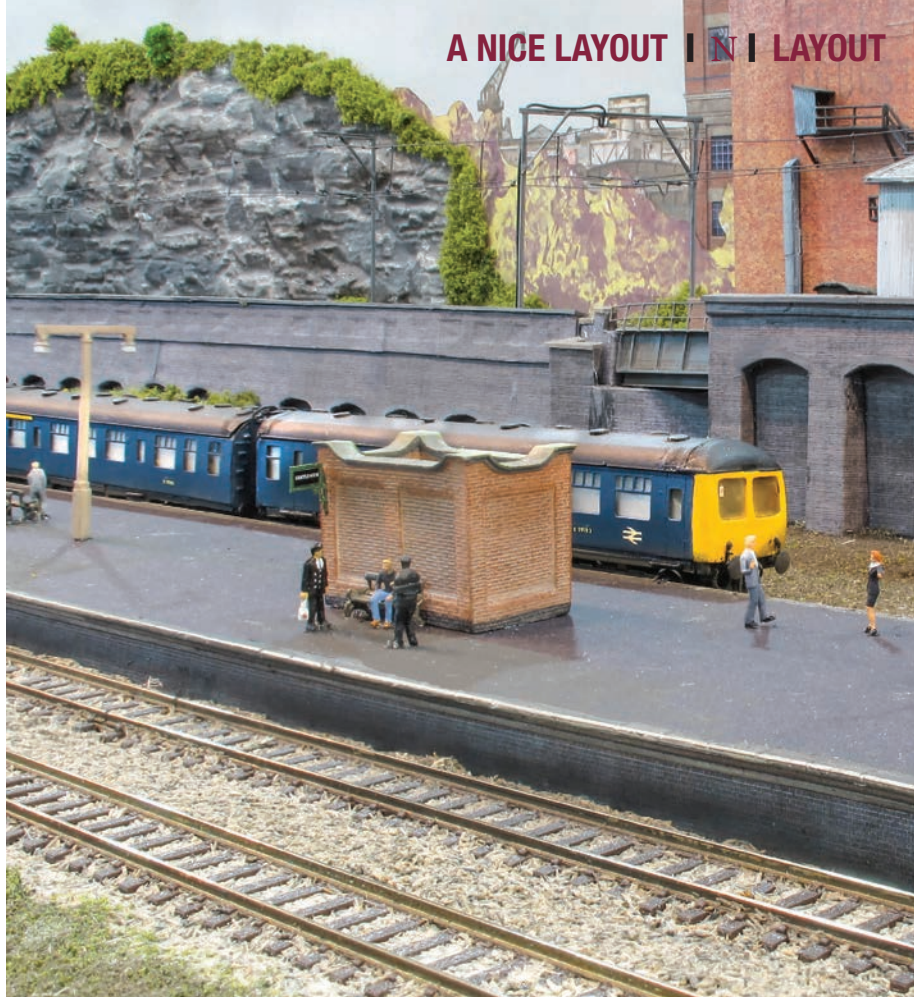
The waterworks has its own narrow-gauge railway – a short piece of Z gauge track, set in a concrete base. A Z gauge steam tank locomotive chassis is used on a scratch-built body, with a scratch-built workshop/mess van. It shuffles back and forth using a Gaugemaster auto shuttle unit – another point of interest to keep the paying public entertained.

See it for yourself

The layout was started during the 2020 Covid lockdown. When the DIY shops were open, I purchased the wood while venturing out for food (honest). I have a 22ft x 10ft garden shed to house this and my other layouts.

The N gauge locomotives and stock have come a long way since the Farish Poole days. The fine detail and much-improved motors/chassis are very welcome – if you can afford them these days, that is! I'm certainly glad I can stock an exhibition layout without buying everything I need now at modern-day prices.

The layout's first outing was at Doncaster Racecourse 2023. It was well received, with lots of positive and complimentary comments from the public and fellow modellers. The next booked outing is The N Gauge Show in Leamington this September.



ABOVE: A Cravens Class 105 DMU from BH Enterprises represents the Rugby to Arkwright Street in Nottingham truncated GC services.

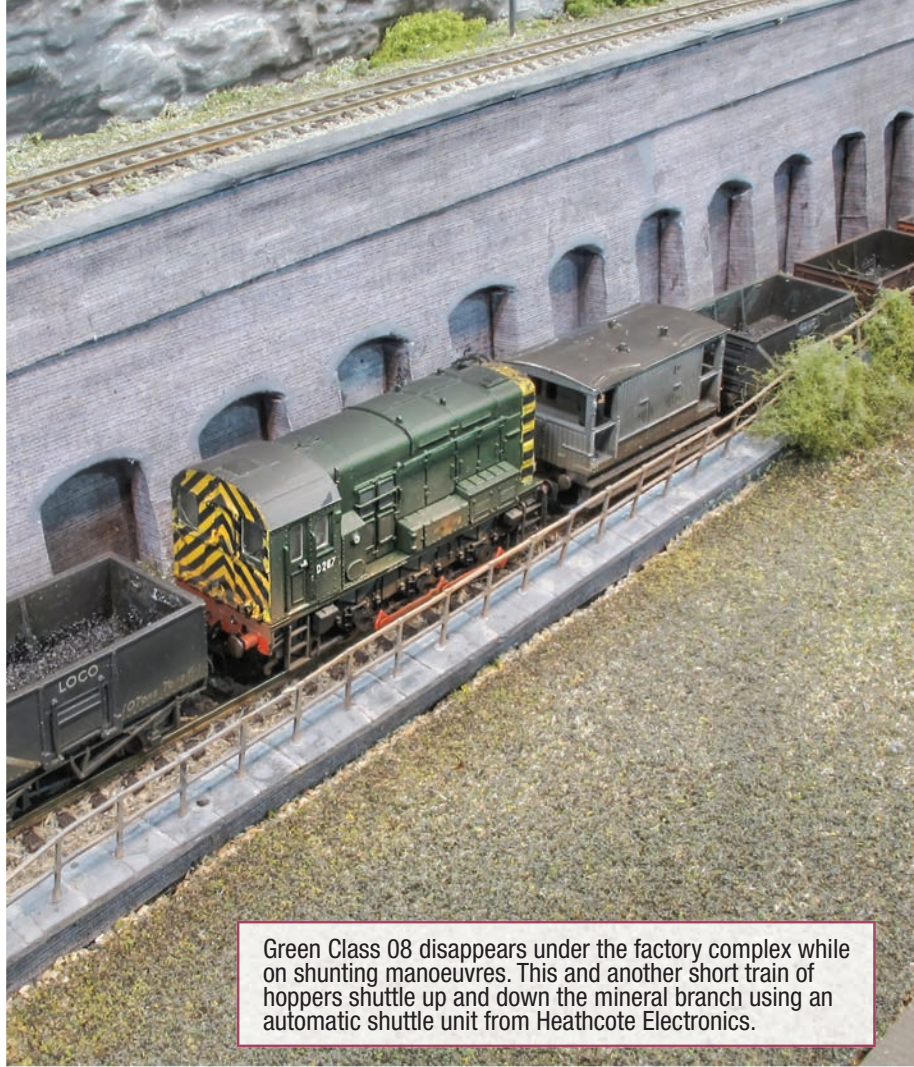
LEFT: The upper-level station sidings display some of my Class 76 and 77 DC electric locomotives from my 'Woodhead' layout. 26020 is now preserved at the NRM York, plus a model of prototype EM1 *Tommy*. The concrete retaining wall is made from balsa wood.



A blue Class 506 EMU winds its way through the factory and waterworks complex under the brass, scratch-built Woodhead-style catenary. The small signal box is made from Plastikard with a card roof. It represents a small MSL style square box found on the Woodhead route and GC lines.



The somewhat sad sight of a train of Class 76s on their way to be scrapped. The Class 47 and front Class 76 are both powered. Military vehicles by Gramodels feature in the foreground as a cameo scene. The layout features several retaining walls. The large one here is made from several Metcalfe kits, extended and lengthened and covered with brick paper from Model Railway Scenery.



Green Class 08 disappears under the factory complex while on shunting manoeuvres. This and another short train of hoppers shuttle up and down the mineral branch using an automatic shuttle unit from Heathcote Electronics.



ABOUT THE MODELLER

Name: Gary Atkinson

Age: 60

Name of first layout: 'Inkaston Moor'

Favourite era/region: Great Central in the '60s

Favourite locomotive: WD's and EM1s/Class 76s

Previous layouts:

- 'Inkaston Moor'
- 'Somewhere in England'
- 'Woodhead'
- 'Market Roundham'
- 'Gamesley Staff Holt'

Other layouts Gary is involved with:

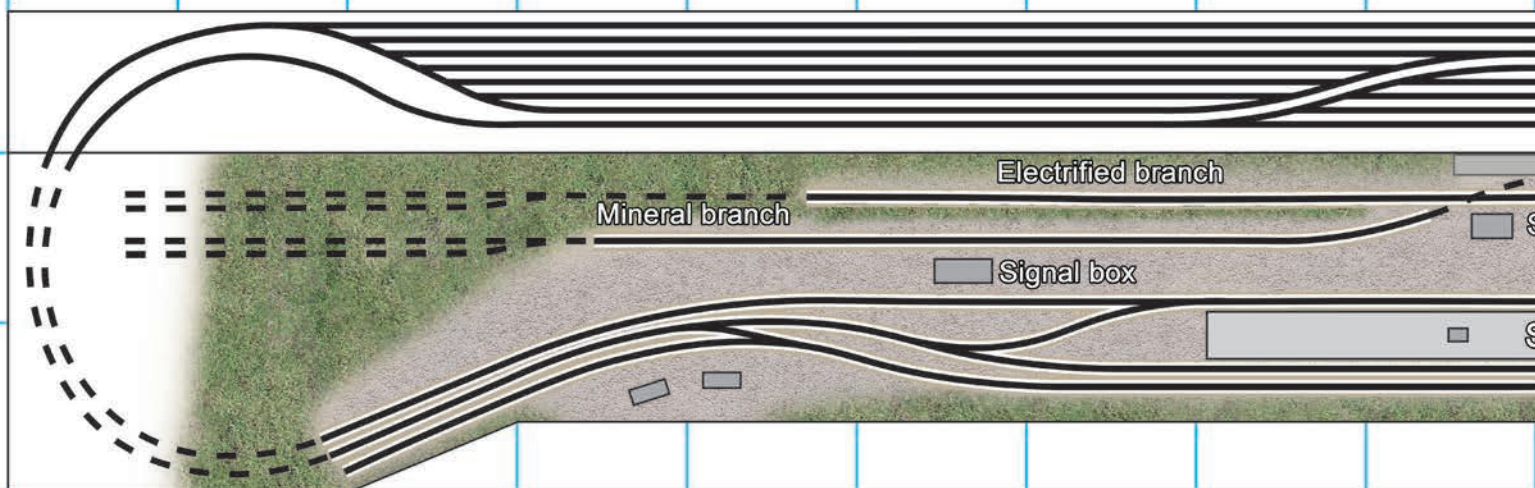
- 'Littlewood'
- 'Idleford Town'

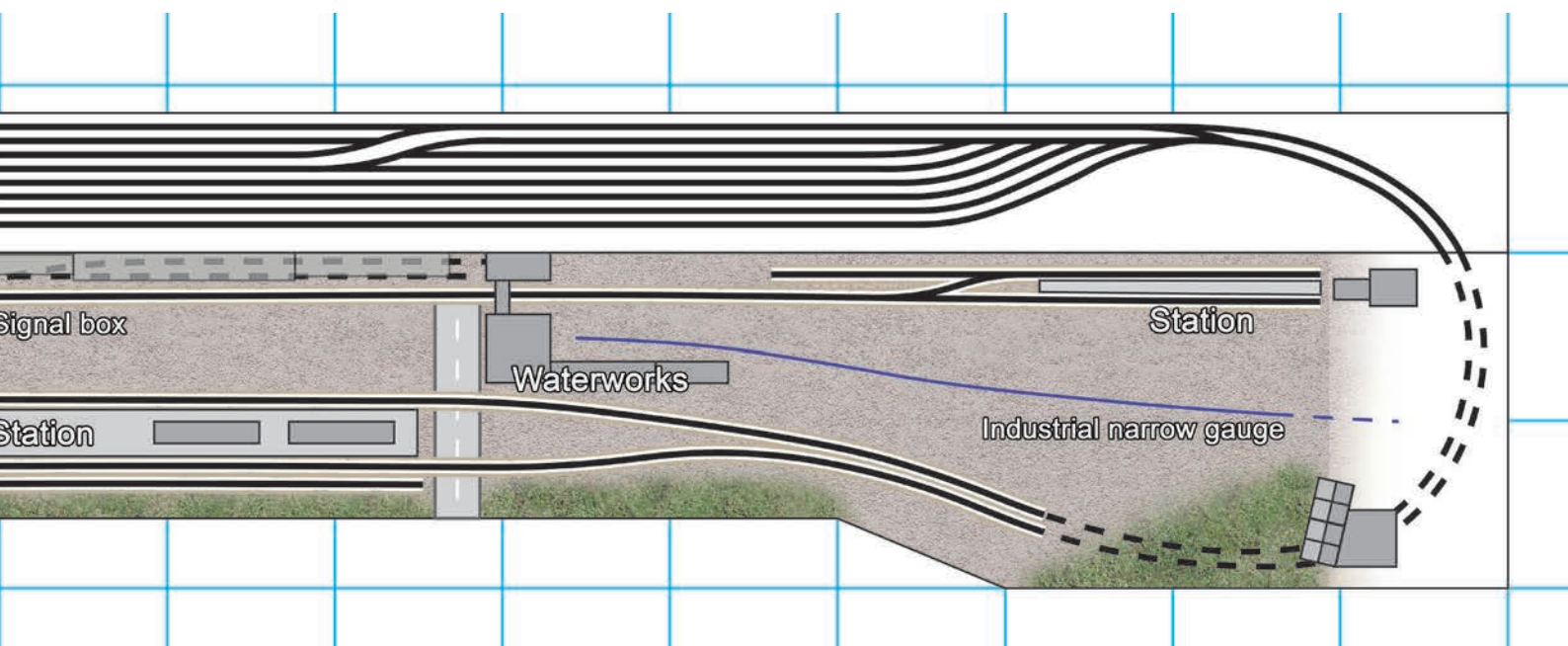


The excellent Farish Blue Pullman glides to a halt at a red signal. I have replaced the couplings on this train with magnetic ones from GLR railways. This reduces the gap between the coaches for a more authentic look. Station buildings are out-of-the-box Farish/Bachmann items. They capture the look of the real thing very well. I've repainted the awning in white and added some cast iron (white metal) Langley columns.

TRACKPLAN

1 square = 1 foot



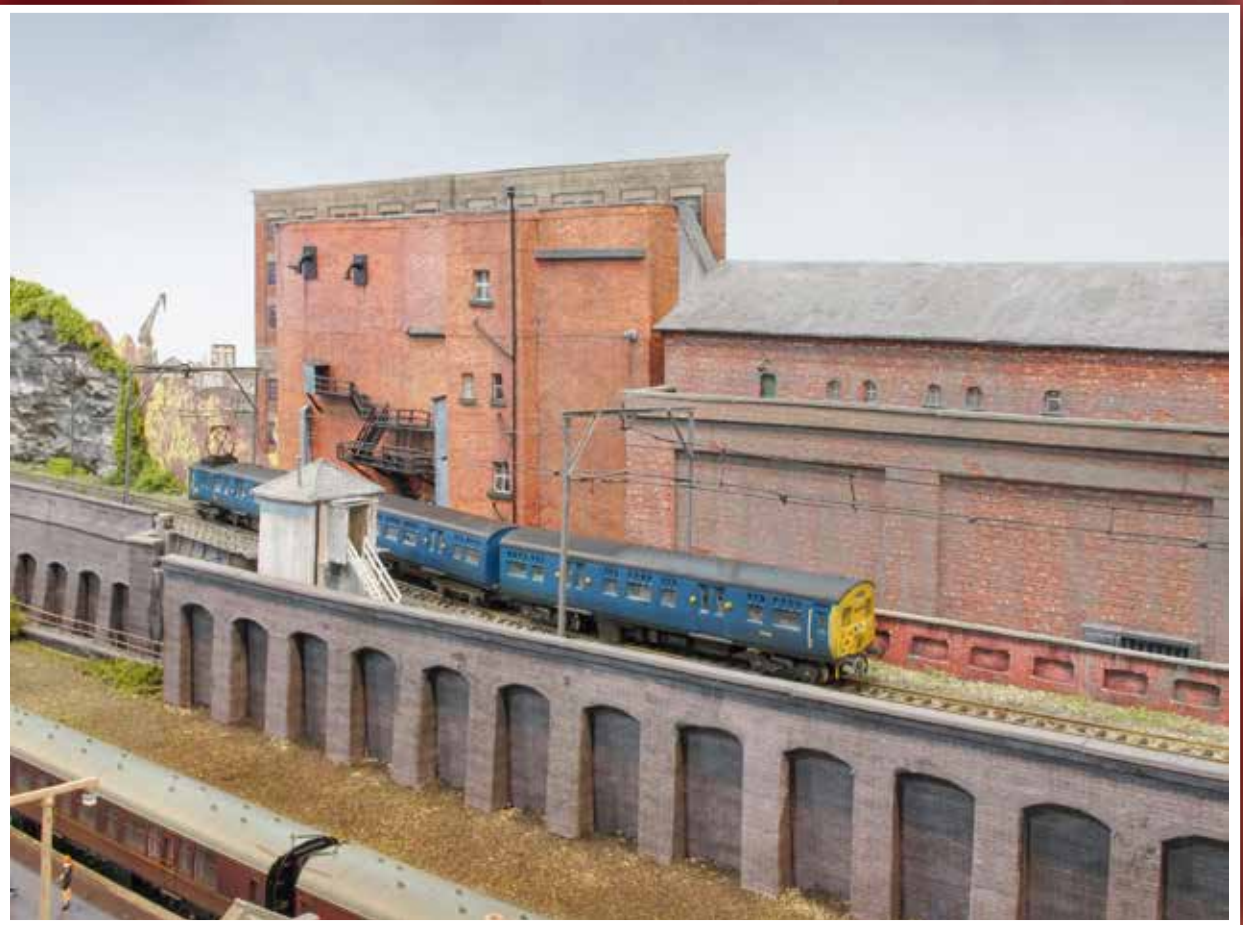


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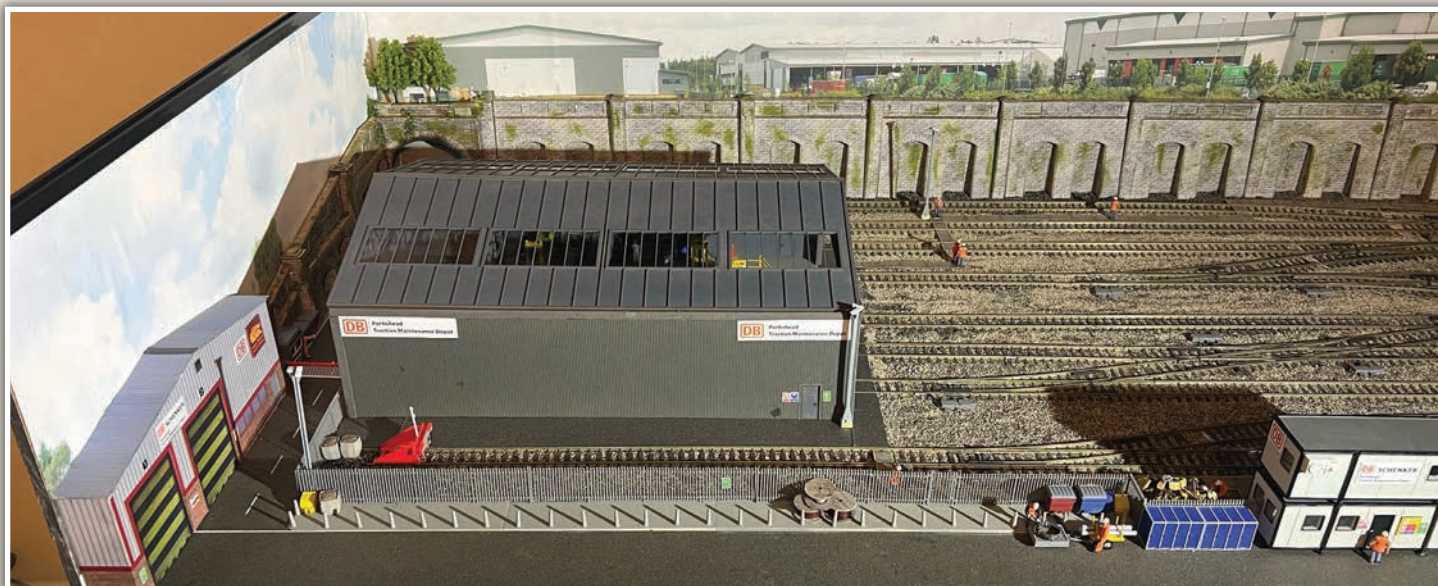
KEEP IN TOUCH

IN CONSTRUCTION

My husband and I have an exhibition layout that we would like to bring to your shows and help build up the community of modelling. Our layout is 10ft by 2ft and is based on a TMD. When we got this layout, it only had track and ballast and, as you can see from the photographs, a lot of time and effort has been put into this. The layout is still under construction, so will have more done to it, but the photographs show the progress so far. The layout is fully operational with DCC sound.

We are both turning 30 this year, and both have an interest in modelling and railways, and most weekends we visit different railways and model shops.

Charlotte and Jack Carpenter



5-STAR SERVICE



I live in Wellington New Zealand and would like to express my gratitude for the excellent service from both Dapol and DCC in going the extra mile, or in my case, 12,000 miles, in honouring the warranty when my new Western failed. Particularly, Kevin at Dapol, who went well beyond my expectations and is a credit to Dapol. I would also like to acknowledge the efforts of Cheltenham Model Centre.

Chris Jackson

BRM REFRESH

Thank you BRM! Many years ago, I emailed you regarding the captions on coloured photographs being hard at times to see. At long last the captions are printed on white backgrounds, a boon for us oldies.

As to the new-look magazine, it is still a good read and I think well laid out, but I couldn't see a reminder for the NGS weekend at the NRM York on May 13 and 14. Don't neglect the N gauge readers!

Eddie Hill

BRM says: Thanks Eddie for the positive feedback, we did promise to sort this out when speaking with readers at shows, and we're next turning our attention to news and reviews! We're always doing our best to cover every corner of the market, quite often there's just not enough space in the issue – but we'll try harder.

SONIC MODELS A5



I have just read Tony Wright's review of the Sonic A5 and see that he has picked up on the fitted head and tail lamps. As fitted, they would seem more appropriate to a Morgan tricycle than a steam locomotive. However, the real problem is that they are glued in place and any attempt to remove them also removes the bracket. I tried, and that was the result. There are common themes on the internet. While etched brackets are available, it would have been much better to have left the lamps in the 'extras' bag to save the additional work, although I know his views on the fragility of such proprietary fittings. Interestingly, one review on the web did say how nice the lamps looked! Only the BR liveried examples seem to have been afflicted.

Apart from the above, I am very satisfied with my model, and it brings back fond memories of days in various signal boxes with my dad watching them pass along with other examples of Colwick's finest.

I have first-hand experience travelling behind A5s, including one on an express from Nottingham Vic' to Grantham, the coaches of which were attached to the rear of a Kings Cross-bound train at that point. There is a prototype for most things.

Kenneth Leathley

Tony Wright says: Thank you for your comments. I thought overall it was a rather fine model, except for the absurd lamps. I have no need of one, of course, having two kit-built ones from years ago on my 'Little Bytham'.

I saw them at Manchester London Road, as it was then, on outer suburban services to Marple and the like. They worked in common with Gorton's L1s, until DMUs saw them off. The one below is Tony Geary's work.



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DB0078 Cockpit Green	DB0090 Beige Green	DB0096 RAF Blue
DB0106 Ocean Grey	DB0127 US Ghost Grey	DB0155 Olive Drab
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AA0583 Gunmetal	AA0610 Aluminium	AA0655 Scarlet
AA0669 Flesh	AA0672 Leather	AA0684 Cockpit Green
AA0936 Coal Black	AA1242 Rust	AA1463 US Ghost Grey
AA1688 Olive Drab	AA6608 Yellow Red	

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NEWTON HEATH

This dual-gauge layout had us imagining an exploration of the scene and its woodland behind, if we were miniaturised – it's very immersive!

Words: Mike Baker and Martin Finney **Photography:** Jamie Warne

Mention Newton Heath to most railway enthusiasts, and images of rows of run-down steam locomotives in a smoky depot during the last days of steam may well be imagined.

Our 'Newton Heath' is very different, lying on the remote heathland that stretches from the western shores of Poole Harbour to the Purbeck Hills in Dorset, an area rich in high-quality clay, much sought after by the ceramics industry, among others.

Our model was built primarily as an exhibition layout, and, although freelance in nature, we have taken features from these railways in an attempt to capture some of the atmosphere of the area around the middle of the 20th Century. The scene represents the clay works buildings that may have existed at the once extensive clay pit at Newton.

The original railway from Newton to the interchange,

with the Swanage branch at Norden, was 3ft 9in gauge, but we have altered history for this to become a standard gauge line worked by the clay company, with a direct connection with the Swanage branch at Norden.

On our model, standard and narrow gauge lines run across the scene, with loaded narrow gauge skips being automatically emptied into waiting standard gauge wagons, or taken into the bagging shed, where they are automatically emptied into a hopper that discharges into a container under the baseboard. The material used to represent the clay is cat litter, the colour of which bears a good resemblance to some of the clay samples from the area.

The skips are returned empty into the fiddle yard and are refilled manually, and the full standard gauge wagons have to be emptied manually.

A LITTLE HISTORY...

Clay working in the area has been carried out since Roman times, and from the early-19th Century until the late 1960s, railways have played a large part in transporting the material. Benjamin Fayle and Pike Brothers were the main proponents in the industry, both having their respective narrow gauge railway system, Pike's with a gauge of 2ft 8in, and Fayle's being 3ft 9in. The first railways in the area transported the clay to piers, where it was loaded onto barges and taken across Poole Harbour to the deep water quays and loaded onto larger vessels. With the arrival of the L&SWR, eventually both companies had exchange facilities with the Swanage branch, Pike's at Furzebrook, and Fayle's a little further down the branch at Norden. Interestingly, the narrow gauge lines in Purbeck used six different gauges over the years, the remaining lines finally being re-gauged to 2ft after WWII.

The area is still producing clay, but, nowadays, all is transported by road – the last load of clay was dispatched by rail in 1969.





Industrial track

The baseboards are constructed from 6mm ply, and, for transportation, the two 1m x 720mm scenic baseboards bolt together to form a box-like structure. The two fiddle yard boards also bolt together in a similar fashion.

When erected, the overall length of the layout is 3.4m. Lighting is by two 12V LED strips, a pelmet at the front having one strip, with another towards the rear. The rear pelmet also supports the one-piece backscene, expertly painted by Gordon Gravett on roller blind material. This rolls up and is carried in a cardboard tube when the layout is dismantled.

To portray industrial track, it was necessary to construct the standard gauge track from components using Code 100 flat bottom rail, so we thought we might as well use the same method for the narrow gauge, which uses Code 80.

Construction begins with a printed template glued to the baseboard. Copper-clad sleepers, having holes jig drilled to accept 1/8in brass rivets, are fitted between every third sleeper on the plan. The rivets are fitted in the holes and soldered to the sleepers, the underside of the rail is then soldered to the top of the rivets using a jig to set the underside of the rail at 3mm above the baseboard.

When all is wired up and tested, 3mm thick wooden

sleepers (distressed with a wire brush and stained) are slid under the rail and glued in positions marked on the plan. Micro spikes from EDM models are then added by drilling 0.5mm either side of the rail and pushing them in.

The ballast is added, which renders the copper-clad sleepers invisible, as they are much thinner than the wooden sleepers. Ballast is a mixture of colours and grades from Woodland Scenics.

While this is certainly not the quickest way of producing track, on our relatively small layout, the task was not too onerous and the end result is robust and realistic.

We have used 16.2mm gauge for the narrow gauge line, basically, a "tightened up" version of 16.5mm gauge, allowing for finer clearances at crossings while using standard 16.5mm finescale wheelsets. The standard gauge rails have been set at 31.25mm for the same reason.

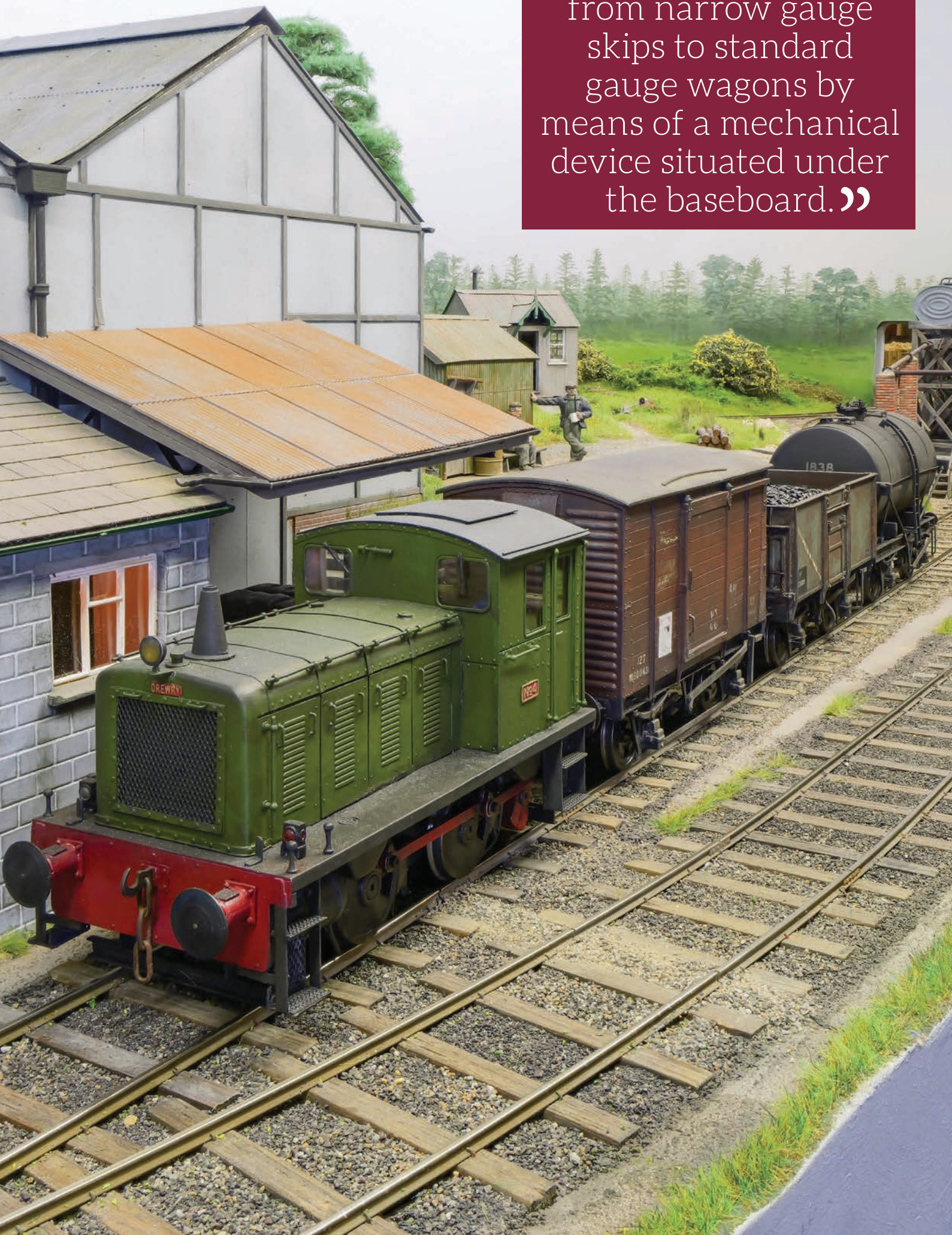
Electrically, the layout is controlled by conventional DC, with separate controllers for the standard and narrow gauge. Operation is from the front, with the left-hand operator responsible for the standard gauge and L/H fiddle yard, and the right-hand position for the narrow gauge and R/H fiddle yard. Slide switches change point polarity, with each switch having a mechanical linkage to the point blades.

RIGHT: The Clay Company's 150HP Drewry diesel is built from an etched brass Connoisseur models kit. The box van is built from a Parkside plastic kit, while the mineral wagon and fuel oil tank wagon are RTR items from Lionheart Trains. All the stock is weathered to varying degrees.

BELOW: Open wagons stand by the loading bank, ready to be loaded with clay from the narrow gauge skip wagons. Sacks of bagged clay can be seen on the loading platform, and will be loaded into a box van.



“Clay is transferred from narrow gauge skips to standard gauge wagons by means of a mechanical device situated under the baseboard.”



Adding detail

Scenery is fairly conventional, using mostly commercially available materials from Green Scene and Woodland Scenics. A Noch Grassmaster was used not only for the grassed areas, but also for the Scots pine and the gorse bushes, which use a twisted wire frame. After the static grass was added and the glue dried, foliage net and microfibre with scatter material attached with hairspray was added to give the appearance of the heathland. Rubberised horsehair was also used for the larger scrub.

The buildings are all scratch-built and based on photographs or drawings in Chris Legg's two excellent books on the subject (*The Furzebrook Railway* and *Fayles Tramway*, Twelvehead Press). It was also possible for us to measure some of the buildings that still exist.

Probably the most recognisable structure on the layout is the curious bridge carrying the narrow gauge line over the standard gauge. This still exists at Norden on the preserved Swanage Railway.

The model bridge has been reduced in dimensions due to space constraints (the prototype has wider decking and was built for double track to pass under, although the Swanage branch was never doubled).

The main mill building is also based on buildings that existed at Furzebrook Works, but again, the



ABOVE: The little cameo scene on the loading bank depicts a rather permanent tea break. In reality, the men would manually tip the skips of clay into waiting standard gauge open wagons. The corrugated iron hut is based on one that stood near the loading bank at Furzebrook, and is scratch-built from Wills 4mm:1ft scale corrugated asbestos sheets, which is just right for 7mm:1ft scale corrugated iron.

BELOW: An overall view of the loading area. The engine shed is based on one that still stands at Ridge. As the prototype was built for 3ft 9in gauge, the dimensions were modified to suit standard gauge. The Hawthorne-Leslie saddle tank on shed is built from an Agenoria kit.





LEFT: The brick-built lean-to boiler house and the bagging shed are constructed from MDF. The mortar courses in the brickwork were laser-cut to represent the brick bond used on similar buildings at the site. The bagging shed spans a baseboard joint, which is disguised by a rainwater downpipe (see photo below). The roof lifts off in one piece when the layout is dismantled for transport.



dimensions have been reduced. This building incorporates the loading bay for bagged products, the boiler house for drying the clay, and the illuminated office, where Doris, the secretary, can be seen at the desk. A siding runs into the building where clay is unloaded for shredding, drying and bagging.

Bringing the layout to life

Standard and narrow gauge locomotives and rolling stock are built from kits and are of various prototypes. Unfortunately, no kits are available in 7mm:1ft scale for steam locomotives that worked in the area. However, as the small internal combustion locomotives used after the conversion of the lines to 2ft gauge were found to be a little under-powered, so Welsh Highland Railway *Russell* was purchased at auction by the clay company after the War and put to work on the line, perhaps by this means, other locomotives could have found their way to this part of Dorset.

We do have a rake of scratch-built end tipping wagons that were used on the line, along with a small Ruston diesel locomotive and a Simplex, both from ABS kits, types that worked on the line in later years.

Three-link or screw-link couplings are used on the standard gauge, while the narrow gauge uses the link and pin type. At exhibitions, the narrow gauge stock is



ABOVE: The narrow gauge skips are propelled into the bagging shed and tipped automatically by means of a rail constructed from 1.5mm dia. copper. The material falls through a hopper into a container below the baseboard.



LEFT: A wagon load of coal is brought to the coal stage for unloading, while on the narrow gauge, a Manning Wardle descends the incline with clay for the weathering beds.

RIGHT: A rake of six skips are used to fill a standard gauge 5 plank open wagon. The skips and the Ruston loco hauling them are from the late lamented ABS/Wrightlines kits. The Andrew Barclay 0-4-0ST *The Green Man* is from a Mercian Models etched kit.



run in fixed rakes, each with its own locomotive.

Clay is transferred from narrow gauge skips to standard gauge wagons by means of a mechanical device situated under the baseboard, and consists of a small geared motor driving cams, which, in turn, drive straight wire 'pushers'. These emerge from the ground behind the skips and tip the buckets, while a second rod emerges underneath the wagon and returns them to the upright position when empty. The siting of the 'pushers' ensures the whole operation is fairly discreet.

Trucks propelled into the bagging shed are emptied by means of a rail, constructed from copper wire alongside the track.

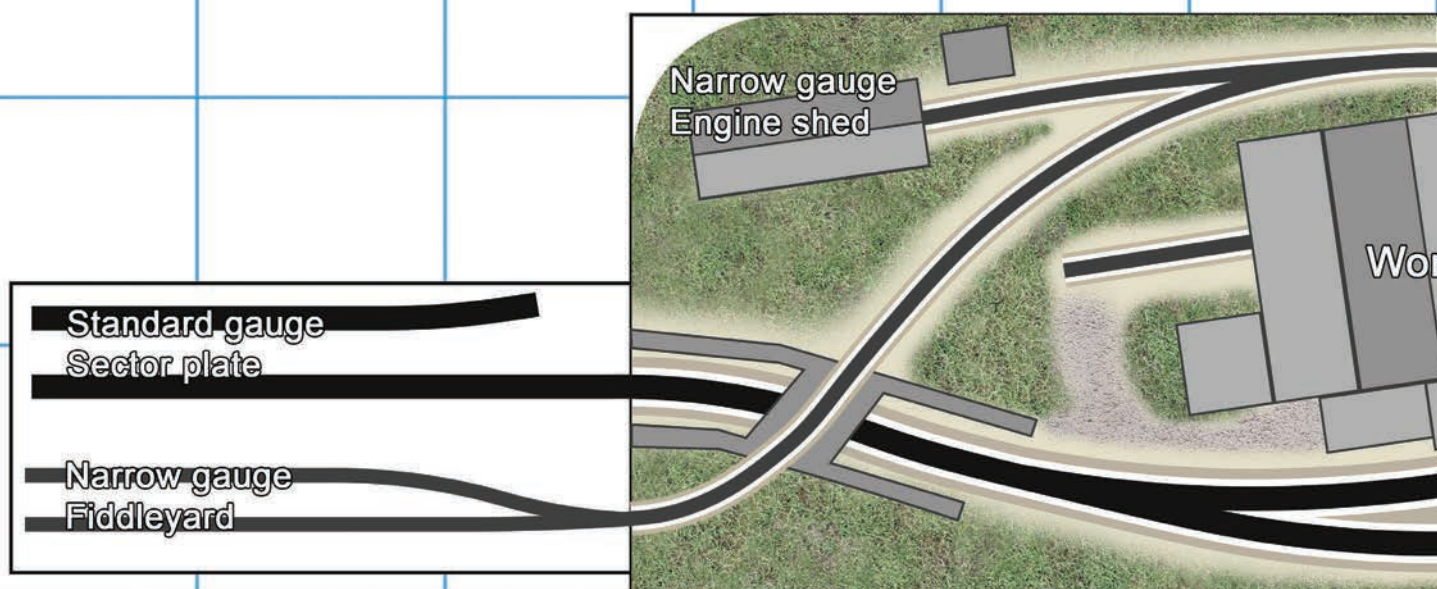
Despite its modest size, the layout has much operating potential, and can handle six narrow gauge and two short standard gauge trains. When the layout is packed away, most of the locomotives and stock are returned to my 'Melcombe Magna' layout, featured in the November 2021 issue of BRM.



RIGHT: Fletcher Jennings O-4-OT *Stanley* (EDM 3D-printed body on a Dapol B4 chassis) propels a rake of empties across the bridge. The bridge was originally built to carry a narrow gauge rope worked incline down to a clay pit. When the pit was worked out, the line was extended westwards and the decking was built up level under the track, as can be seen in the photo.

LEFT: The little Simplex (ABS whitmetal kit) pauses by the Foreman's office. We were able to measure this building, as it has been preserved at the Purbeck Mining Museum at Norden, near Corfe Castle. The model is constructed from Plastikard.

TRACKPLAN



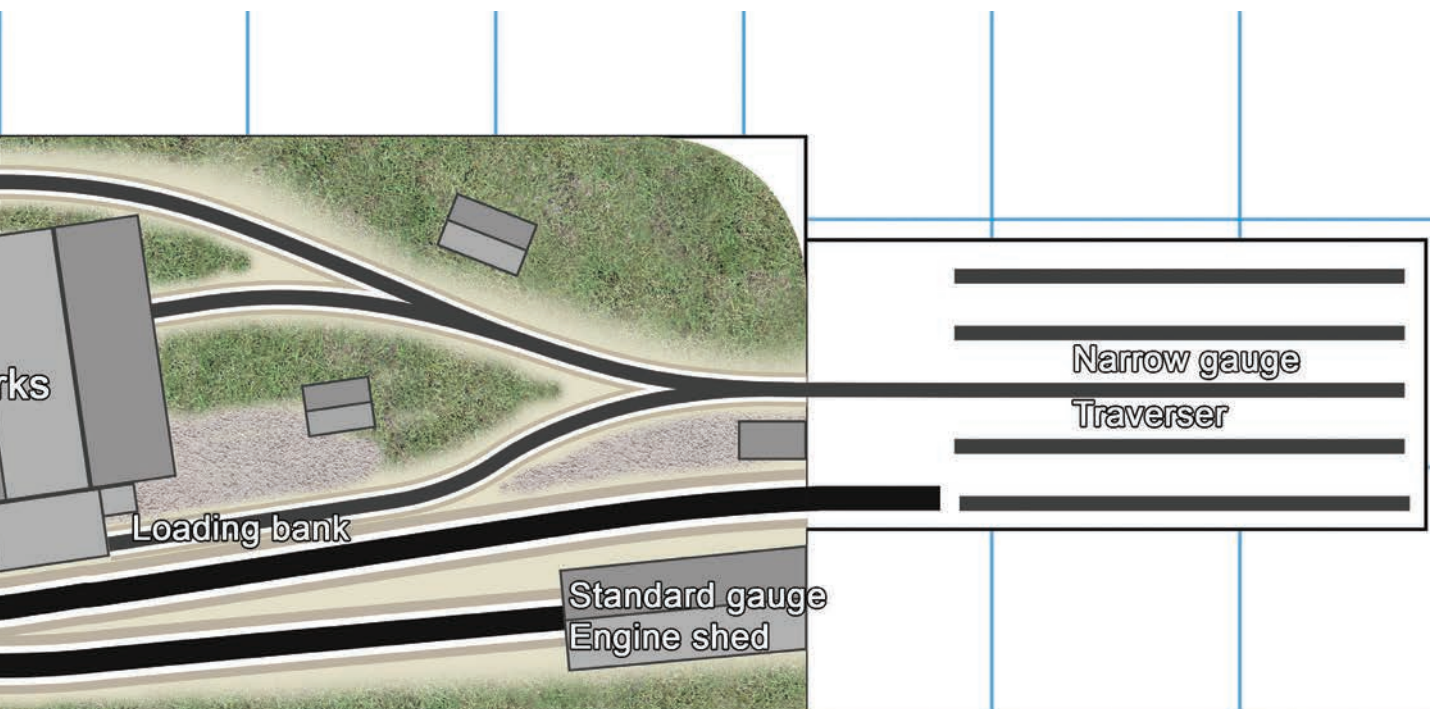
FACTFILE

SCALE/GAUGE: 7mm:1ft scale/32mm and 16.5mm gauge/O & O-16.5

SIZE: 6ft 6in x 2ft 3in

ERA/REGION: 1950s Dorset

LAYOUT TYPE: Fiddleyard to Fiddleyard

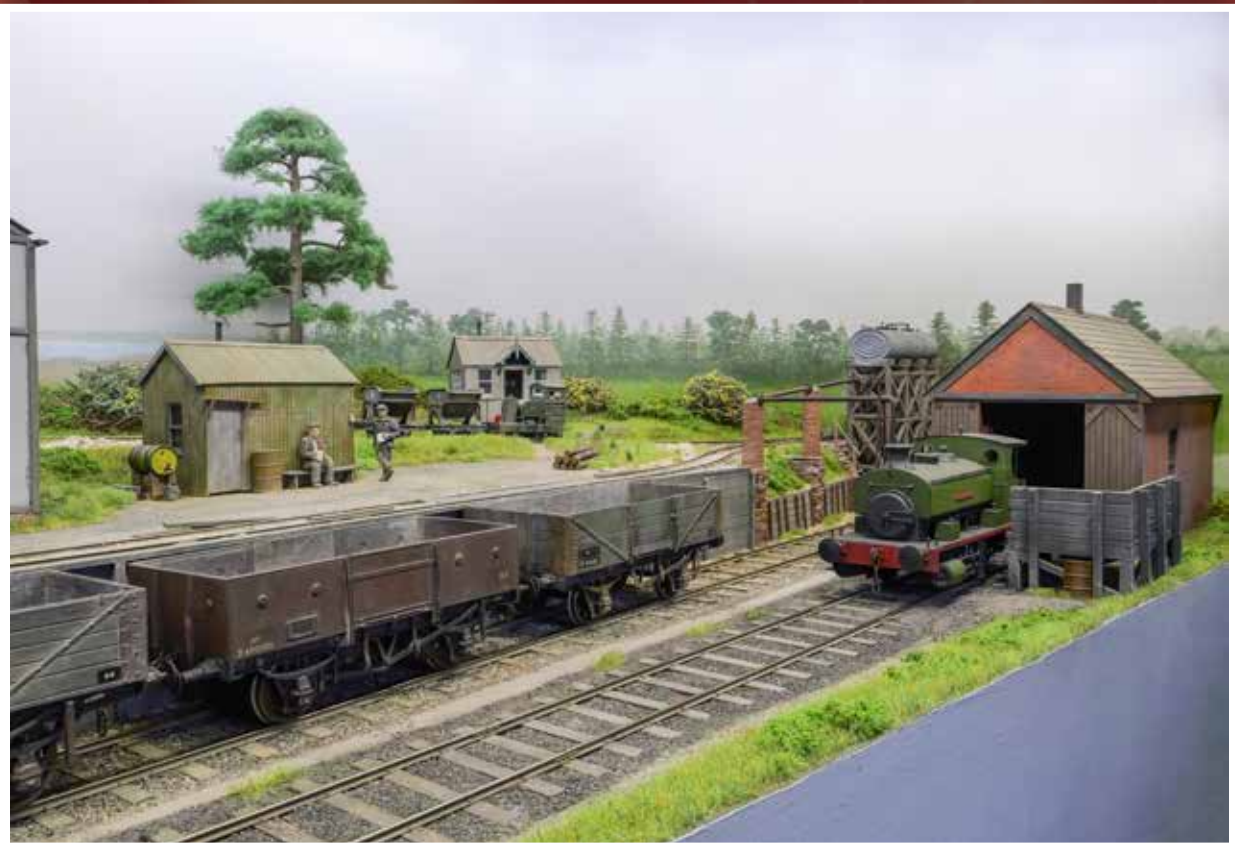


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A visit to Newton Heath Works

This dual-gauge model railway by Mike Baker and Martin Finney had us imagining an exploration of the scene and its woodland behind – it's very immersive!

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Factfile

Layout name: Melcombe Magna
Scale/Gauge: 1:43.5 scale / 32mm gauge / O
Size: 21ft 6in x 11ft plus outdoor section
Era/Region: 1955-60 (W)R(S)R/ Mid-Dorset
Layout type: Fiddleyard to terminus

ON EXCLUSIVE

MELCOMBE MAGNA

Indoor operation for gloomy winter evenings, or outdoor operation for more clement weather – this feature-filled layout permits both.

Words & photography: Mike Baker



Collett 2251 locomotives gradually replaced the S&D 'Bulldogs'. 2223 was at Templecombe in the early 1960s, and is seen ready to depart with the branch B set and an SR van – a very common combination on the Highbridge branch.

A map of Dorset will not reveal the town of Melcombe Magna, as it only exists in model form. There, in my imagination, you may find traces of the abandoned medieval village of Melcombe Horsey, close by Melcombe Bingham, deep in the hills to the north east of Dorchester. This area is the setting for my 7mm:1ft scale model railway, and being born and bred within sight and sound of the S&D, naturally, this was my first choice.

The layout is a development of the 'Leinthall Earls' plan by Iain Rice in his book *Light Railway Layout Designs* and is housed in an outbuilding measuring approximately 21ft 6in x 11ft.

Baseboards consist of 'T' girders formed from 6mm ply supporting 2in x 1in cross members, which in turn support the 6mm ply top. This arrangement has stood the test of time and no problems have arisen in the 20 odd years since I built them.

After leaving the fiddle yard, trains pass through a hatch in the wall and run on covered staging through a 90-degree curve with a radius of 4ft 6in, entering the station through another hatch on the opposite side of the building.

The outside staging consists of a framework of Dexion slotted angle, which supports a base of 19mm shuttering ply. This is covered with a 'lid' that consists of lightweight twin wall acrylic sheet for weather protection, enabling operation at all times. This cover is hinged, rather like a car bonnet, to facilitate track cleaning and maintenance.

The line up to the quarry passes over the main line and the curve here is somewhat sharper, at around 3ft radius, but with short wheelbase locomotives and stock, no problems have been encountered. The line then runs into a two-road sector plate hidden behind the bagging shed.

Track is C&L flexi-track with turnouts constructed from their components and operated with Tortoise point motors, which I have to say have been 100% reliable over many years.

Layout control

In parts of the quarry section, I have used Code 100 flat bottom rail on copper-clad sleepers to represent the lighter track section often found on industrial lines, and have incorporated a few kinks and bumps as would be appropriate in such settings. Likewise, the narrow gauge Waterloo Tramway uses Code 75 flat bottom rail and is built to 16.5mm gauge.



ON EXCLUSIVE

The back of the bagging shed. I have tried to give this area an air of a rural industry, with various parts of abandoned machinery lying around and gradually being overgrown with vegetation. The Fergie is used in the event of locomotive failure.



Originally the fiddleyard consisted purely of cassettes, but in this scale, I found they could be a little unwieldy, so I converted to a four-road sector plate with locomotive spurs. Trains arriving at the sector table have the locomotive uncoupled and run back on to a spur. The train is then pushed onto the appropriate road, ready for a locomotive from one of the spurs to couple up when required.

As I have more rolling stock than the sector plate can accommodate, some set train formations are stored on the cassettes, and when one of these trains is required, the sector plate can be moved clear of the running line and an adapter fitted between the cassette and the running line.

Control is DC, with three hand-held

controllers – one for the main line, one for the quarry line, and one for the narrow gauge 'Waterloo Tramway'. I have tried most makes of controller, but the old Pentrollers seem unbeatable for smooth operation. If required, the main line controller can be switched to control the quarry.

I must admit to being rather fussy in regards to smooth running and reliability, I use a cloth soaked in a little WD40 to clean the track. This will maybe raise a few eyebrows, but it works for me and only needs infrequent treatment.

Scenery & buildings

For some years, the backscene was just a sky colour painted with tester pots on more 6mm ply. However, I eventually plucked up

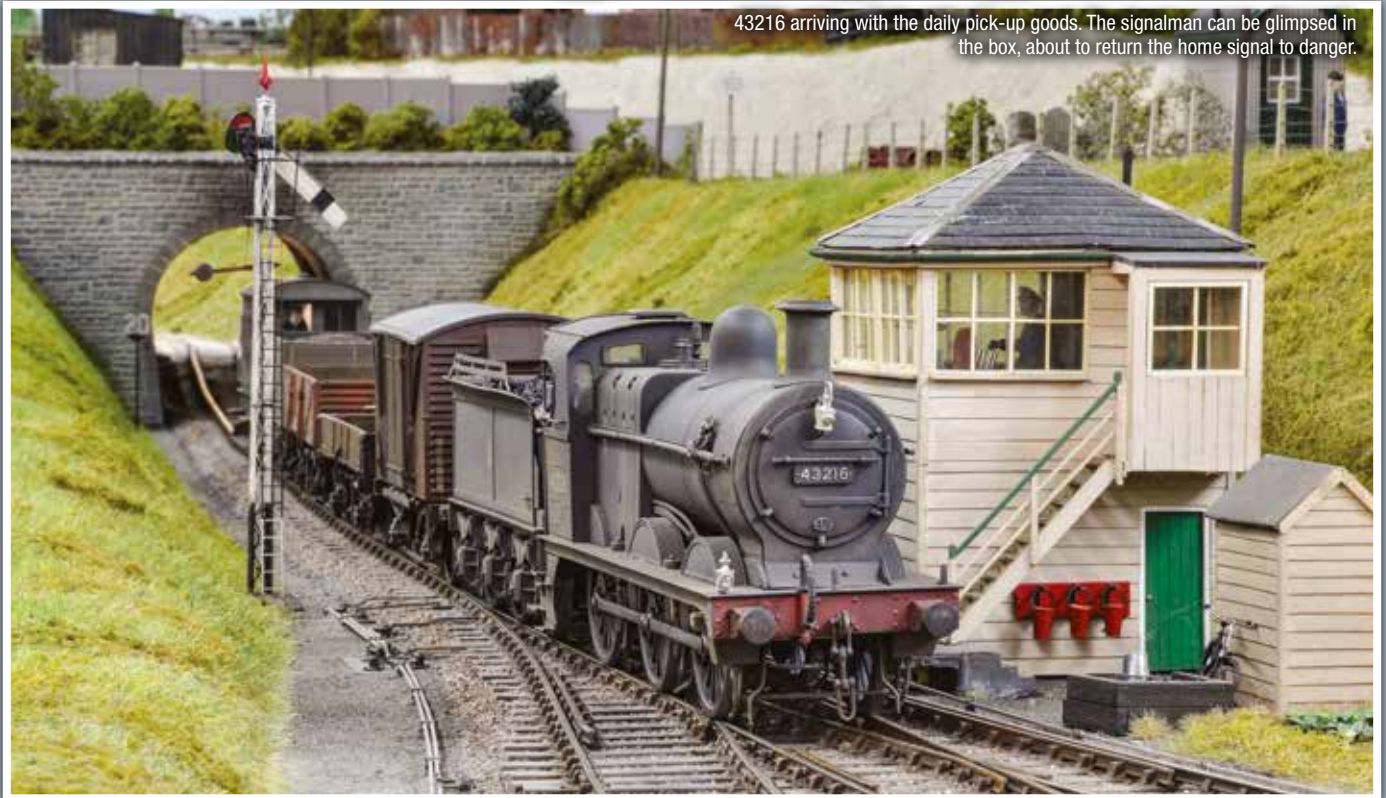
the courage to have a go at giving it a little more detail, using artist's acrylics. The result is definitely not a work of art, but I think it is preferable to plain sky, and by this method, the scene can be painted to suit the layout.

The buildings are a mixture of scratch-built and resin kits. The station building is a resin kit from Highland Castings with a modified canopy and a gents W.C. added. The signal box, goods lock up and industrial locomotive shed are scratch-built from card, with the large bagging shed constructed using Wills 4mm:1ft scale corrugated asbestos sheets, which is just about right for corrugated iron in 7mm:1ft scale. I have tried to give the area around the quarry a rural industry feel, with various pieces of abandoned equipment lying around.

A Drott bulldozer is shunted by the "Bulldog". The cans on the platform barrow contain water for the crossing keeper and will be placed on the front of the locomotive to be delivered on the return trip.



ON EXCLUSIVE



43216 arriving with the daily pick-up goods. The signalman can be glimpsed in the box, about to return the home signal to danger.



The outgoing products from Waterloo Works include cement, bagged in vans, and lime in sheeted open wagons, I have some of the private owner lime wagons from Dapol, although goodness knows what they are doing in Dorset. Incoming goods are mainly coal and culm (coal slack) for the kilns. The odd piece of machinery on a 'Lowmac' or flat wagon is also to be seen occasionally.

Outgoing wagons are brought down the incline by one of the quarry company's locomotives to the exchange siding behind the platform. Incoming wagons for the quarry are exchanged by the branch locomotive. When there are heavy loads, such as incoming coal, two locomotives are needed to push the wagons up to the quarry.

With passenger trains arriving and departing from the S&D and the GW branch, sometimes with tail traffic being attached or detached, plus the daily pick-up goods shunting the small yard, not forgetting the narrow gauge Waterloo Tramway, two operators can be kept quite busy.

A brief – yet imaginary – history



In the latter part of the nineteenth century, there was a need to improve communications in this part of Dorset, and a light railway was proposed. Logically, this should have been aimed for the county town of Dorchester, around eight miles away. The Lord of the Manor, however, had different ideas. He owned a small lime works called Waterloo Works, and if he could transport the products to a ready market to the north, considerable expansion of the works would be guaranteed.

The line was therefore constructed to join the Somerset and Dorset Joint Railway at a junction south of Stalbridge. The line was consequently absorbed by the S&D.

As expansion of the industry continued, the GWR took an interest and built a branch line from the Wilts, Somerset and Weymouth line near Dorchester, terminating a few hundred yards away from the S&D terminus, and serving another quarry, which was closed by the 1930s. The passenger traffic, however, was quite healthy.

During World War II there was urgent need for the products from the Waterloo Works to be transported to Portland Harbour, so, consequently, a chord was constructed connecting the two stations. The GW passenger station was then closed, with trains from both lines now running into the S&D terminus.



The local coal merchant has a small office in the yard, but coal is normally bagged up straight from the wagon when possible.

ON EXCLUSIVE



Ivatt class 2 tanks were regular performers on the S&D and were well liked by the crews. 41283 came quite late to Templecombe, but I have a soft spot for it as I worked there as a fireman and have a photograph of us at Evercreech Junction.



Maximum effort is exerted by the Peckett and the Hawthorn Lesley propelling a loaded coal train up the incline. The signal at the top of the incline is a model of the one that stood on the Clandown Colliery siding at Radstock S&D, and had neither spectacle glasses nor lamps.

The future

I have no plans to replace 'Melcombe Magna' but will maintain and enjoy operating it for the foreseeable future. I am fortunate, however, to have space elsewhere for further small projects. I am currently building a small locomotive depot with a turntable. Again, this is 7mm:1ft scale, but this time control will be DCC with sound-fitted locomotives.

I have also jointly built 'Newton Heath Works', a small exhibition layout inspired by the clay mining industry in the Isle of Purbeck – the subject of a future article.

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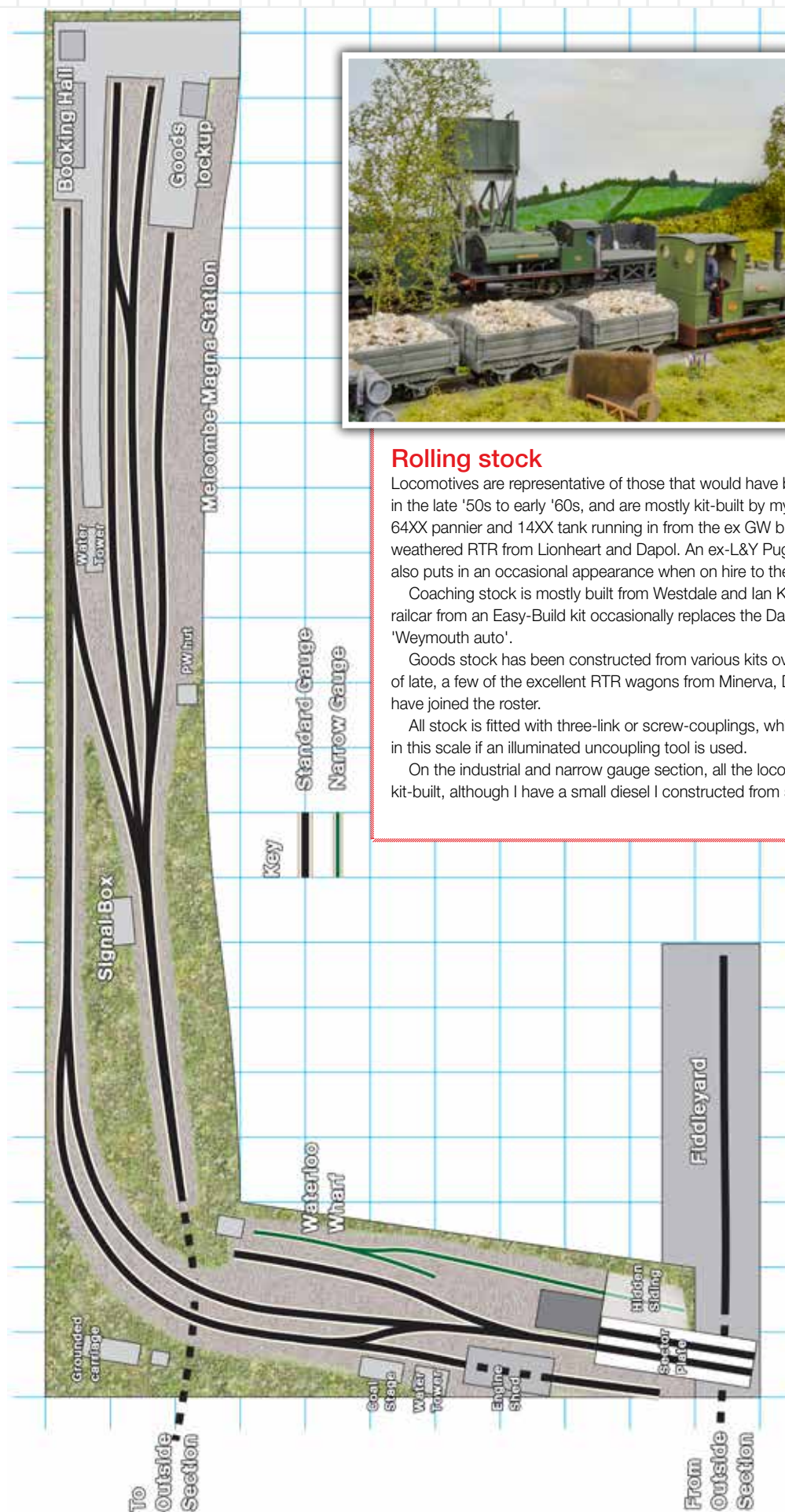


The 14XX has worked a goods up the ex-GW branch from Weymouth, and is seen shunting a sheeted open. This is a Dapol locomotive. The wagon is a resin and whitmetal kit and the wagon sheet is hand-lettered.



A Pressed Steel single unit arriving from Weymouth. I must fix that drooping upper quadrant signal arm!

MELCOMBE MAGNA



Rolling stock

Locomotives are representative of those that would have been seen on the S&D in the late '50s to early '60s, and are mostly kit-built by myself, although the 64XX pannier and 14XX tank running in from the ex GW branch are detailed and weathered RTR from Lionheart and Dapol. An ex-L&Y Pug from Tower Models also puts in an occasional appearance when on hire to the quarry company.

Coaching stock is mostly built from Westdale and Ian Kirk kits – a Class 121 railcar from an Easy-Build kit occasionally replaces the Dapol auto car for the 'Weymouth auto'.

Goods stock has been constructed from various kits over the years, although of late, a few of the excellent RTR wagons from Minerva, Dapol and Lionheart have joined the roster.

All stock is fitted with three-link or screw-couplings, which I find perfectly OK in this scale if an illuminated uncoupling tool is used.

On the industrial and narrow gauge section, all the locomotives and stock are kit-built, although I have a small diesel I constructed from scratch.

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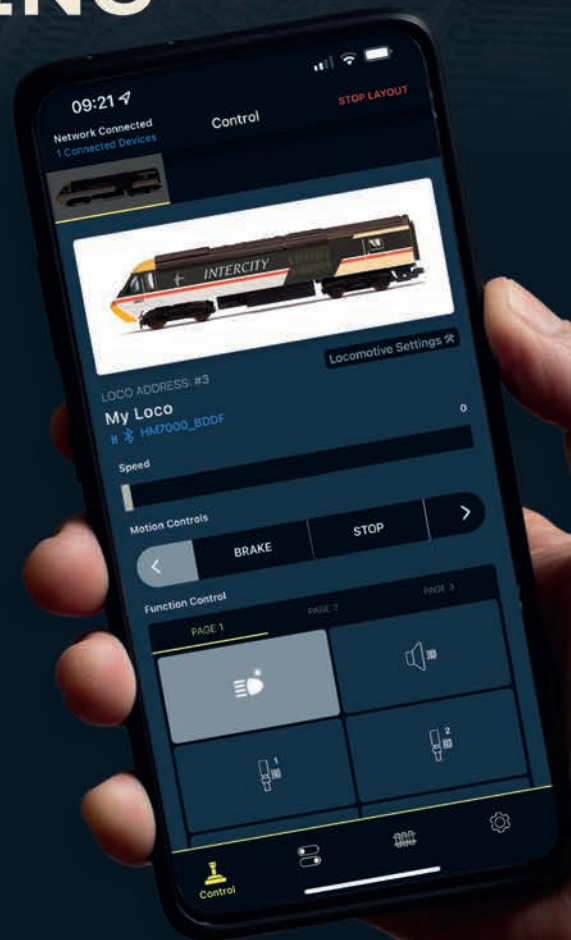
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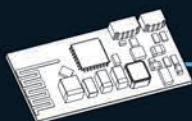
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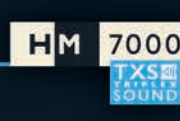
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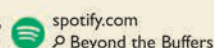
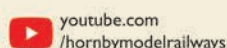
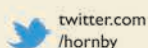
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HOW TO DETAIL A COTTAGE KIT

Words & photography: Michael Russell



Laser-cut kits, such as this one, are lovely to build – they're so accurate that it's much more difficult for things to go wrong during construction. You would have to go out of your way to build this model and not make it square. The building has a high degree of usefulness too, being typical of many country dwellings and can easily be adapted to any region.

For example, you could easily employ LX410 or LX415 roof tiles from its manufacturer, Scale Model Scenery, to change to grey roof tiles/slate. I made the

simple addition of some small brackets for hanging baskets and a water overflow pipe to personalise my model.

The kit includes four different overlay wrappers for the walls, giving you plenty of choice. You can choose between brick or stone finishes, both depicting a greater or lesser degree of damage to the rendering. Alternatively, you can do what I have done and apply your own finish.

Read all the instructions and use the Kit Map to familiarise yourself with where the parts are. I find it helpful to transfer the

part labels from the map to the parts or the runners alongside. Note that the shorter tabs on part B fit onto the thinner internal wall A, and the longer tabs into the outer wall.

Assemble the carcass using PVA glue and, after holding it in place for a few seconds, leave the unit on a flat surface to dry. Don't use rubber bands, as part F is long and is weak around the door frames and easily damaged.

Now that you've completed the basic build, here are steps to elevate the model further...

PREPARING FOR LIGHT FITTING



If you intend to fit lights, drill hole(s) through floor B. Measure your lights or wires to find the diameter required, otherwise 3-4mm should suffice.



If planning to fit the building with lights, paint the interior walls at this stage using art acrylic paint. Don't get paint on the four tab slots cut into part A – the centre wall. And, don't forget to paint the upstairs floors.



To strengthen the long outer wall, and provide flooring if the building is to be lit, cut pieces of mounting card to fit into the bottom spaces. Drill holes for fitment of lights, too, if you intend to do that now or in the future. Glue in place after you have fitted the windows and doors.

CLADDING



Add household filler to cover over the tab slots. When dry, file flat, then brush off the dust. Wear a mask. Don't worry if the finish isn't perfect as old cottages are often irregular. The main thing is that the uniform pattern of the tabs is disrupted.



You should now add the overlay wrappers as described in the kit instructions. However, if, like me, you aren't using these, fix the chimney pieces in place using PVA glue at this stage.



Paint the walls with an emulsion paint of your choice, then use a sponge with a dabbing motion to create a rough pattern. You can add more texture by adding filler powder to the paint beforehand. Skip this step if using the overlay wrappers.



Marker pen
Craft knife
Rule
3mm, 0.45mm, 0.6mm drills
Pin vice
Small paintbrushes
Fan paintbrush
Small piece of sponge
Modelling file
Small clamps
Seam roller (if using overlays)



(KX078-00) Percival Cottage kit
(LX134-00) Small Brackets
www.scalemodelsenery.co.uk

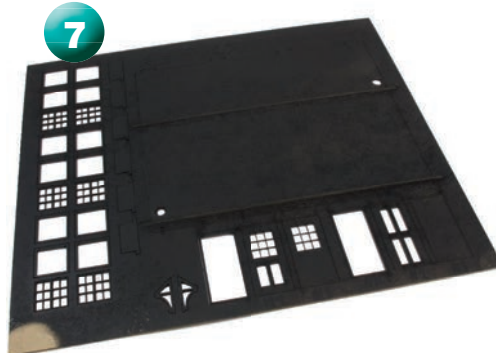
0.45mm Brass Straight Wire 250mm long
www.eileensemporium.com

250ml Expo Fast Set PVA
Anti-shine varnish
www.expotools.com

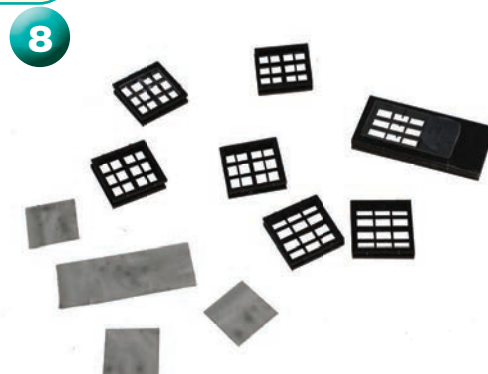
Acrylics: black, grey
Weathering powders: smoke or black, chrome oxide green & white
www.humbrol.com

White emulsion paint
Indian ink
Art acrylic paint
Household filler
Antibacterial wipes

DETAILING



Spray the windows and door frames with matt black acrylic paint while they are still on the runners. Ensure that you include the guttering, which is on the edge of the two ceiling pieces, I and J.



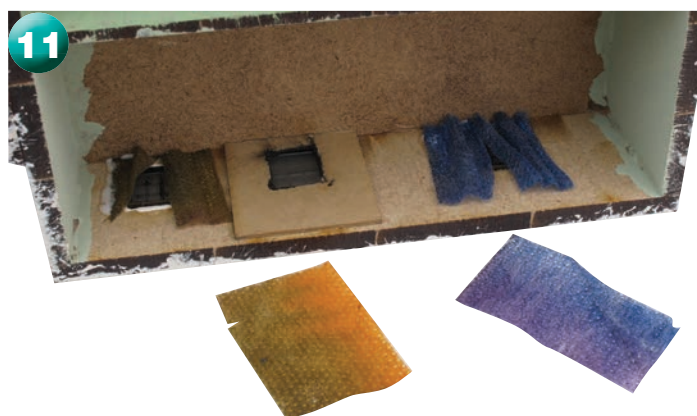
Dirty the windows by weathering the glazing before fitting into place. I used anti-shine varnish with a little grey acrylic paint added.



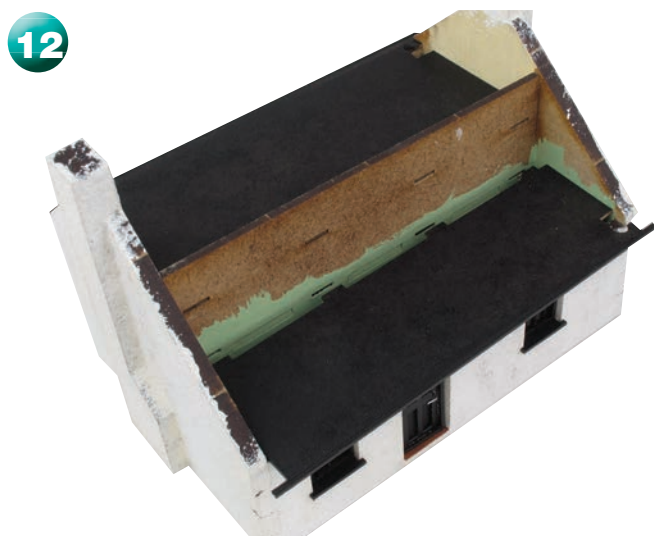
The back door is too small for the opening. To disguise this, and to also make it easier to attach, I glued a rectangle of card cut from a space on sheet 3 that was already painted black from spraying the windows. This also helps strengthen the back wall.



Add door handles made from a staple, fitted using a 0.6mm drill. Push the windows in flat to the rear of the wall using a rule as a backstop. To facilitate this, cut the window sills flush with the inside wall before fitting.



Make windows from antibacterial wipes that have dried out. Die with ink or dilute acrylic paint. Fold them up and score with a blunt tool to form creases. Cut into two curtains and drape as the real thing would.

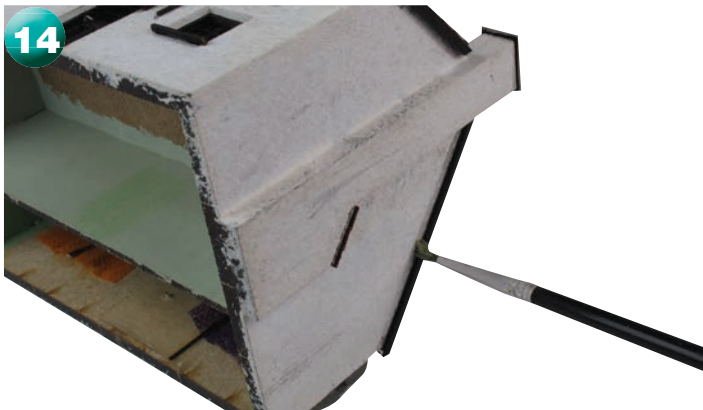


Fit the ceilings in place, ensuring that the groove representing the guttering is uppermost. The four tabs at the rear fit into slots in part A. It is much better not to get paint into these in the first place, then to try to clear them at this stage.



After fitting the roof tiles, add the bargeboards at the gable ends – these parts don't have an identifying letter in the instructions. If you were wondering what parts R are for, they are the capping pieces for the chimneys.

WEATHERING AND ADDING SHADOWS



Add shadows using a 75% water and 25% Indian ink combination. Apply using a brush under any part that stands out proud from the walls – roof, chimney capping, or window sills. Keep the brush as flat as possible to get an even stroke.



Using the same ink mixture, drag a fan brush across the roof using the dry brush technique. This ties in the rows of tiles and gives the weathering a more unified appearance. Do the same for the walls, but sparingly because it's easy to overdo this.



Use weathering powders to add dirt around the base of the building to represent spatter from rainfall. Use small quantities and cover over with white acrylic if you overdo it. This also helps to tie the building to the ground.



Drill a small hole and add a 0.45mm piece of brass rod to represent an overflow pipe from the internal water system. Overflow can be represented by green marks on the wall beneath added using weathering powders.



Fit the chimney pots last as they are exposed and delicate, making them easy to knock off while building and weathering takes place. Use Indian ink to cover up the white edges of the paper on the chimney pots. Job complete!

CONCLUSION

Despite my damaging one side of the building during construction, this is a strong model. Because of the thickness of the wall material, it is possible to add just about any exterior finish that you desire, making the model highly adaptable and capable of representing a building from any region of the UK. If I were to improve the model further, I would add a television aerial and some better, and more interesting, chimney pots.

Although I haven't fitted lights yet, I will at some stage. Thinking ahead is always a good idea. A little thought at construction time can pay massive dividends as rectification can be difficult or impossible later.



MODELLING AN EMBANKMENT IN N

Words & photography: David Lund



To model realistic plants and landscapes, fixing a time and a place, and doing a bit of research, can really make a difference, with plenty of photographs available in magazines and on the internet to help inspire you. The scale you are modelling in is also important to consider – how high would the grass on a verge be in 2mm:1ft scale compared to 4mm:1ft scale?

I tend to work in N gauge, which is large enough for individual plants to be modelled, but small enough to allow for a more impressionistic approach. The scale also allows for a much larger area of the natural landscape to be modelled in

the same space compared to OO gauge, which means more realistic scenes can be included.

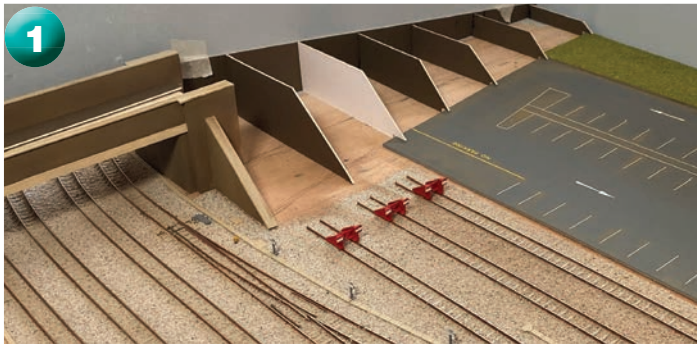
In this two-part article, I am going to outline the steps I took to build two elements of the natural landscape on my own layout: an overgrown embankment in this issue, with trackside plants and weeds, including the ever-present buddleia, next month.

Adopting a layered approach, and using some of the latest landscape products available, I will show how it's possible to achieve a level of realistic detail when modelling trees and plants in N gauge.

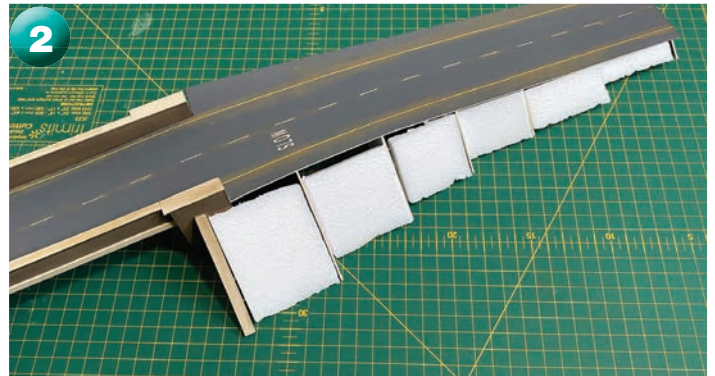
In recent years, the range of landscaping

products available for the modeller has expanded dramatically. Long gone are the days where lichen bushes and heavy plastic trees were the best options on sale. A number of European manufacturers, in particular, produce ranges of high-quality landscaping products that are simple to use, including Model Scene, Lars op't Hof Scenery and Martin Welberg Scenic Studios.

From realistic scenic mats to individual bushes and flowers, alongside manufacturers such as Tasma Products, Woodland Scenics and Jarvis ranges, it has never been easier to produce realistic model landscapes.



In making the embankment scene, the first step was to create the basic profile using card formers. It is often tempting when building a model railway to compress the rise and fall of the landscape to fit everything in. This can sometimes lead to very sharp gradients on roads and embankments approaching bridges, so where possible, it's a good idea to work from photographs to see how the land naturally falls.



This embankment is in a corner of the layout, so I assembled the basic structure as a separate piece that could be removed. This made it much easier to construct, as the next few steps will show. Once I had added the painted and weathered road surface, I filled in the gaps between the card formers with shaped blocks of polystyrene foam.



Pencil
Scissors
Tweezers
Paint brush
PVA glue
UHU glue
Superglue



Model Scenery Supplies

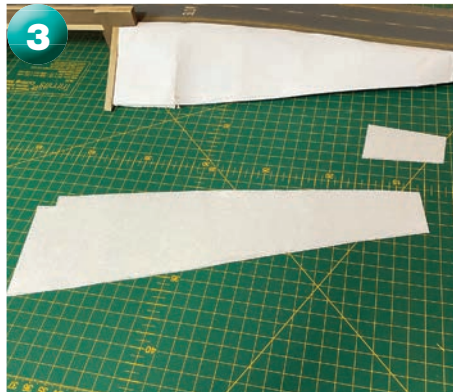
Lars op 't Hof Wild Spring Verge
Landscape Mat
Model Scene Bushes Summer
Woodland Scenics Bushes Olive
Green
Summer Green Static Grass Tufts
Extra Large

Model Tree Shop

Martin Welberg Wild Shrubbery Mat
Summer
Martin Welberg Bushes Type D
Medium Green
Martin Welberg Low Bushes Spring
Green
Martin Welberg Layered Tufts with
Flowers 2-4mm Red
Cypress Trees (TW06) - various
heights
Alder Trees (TWO9) - various heights
Etched Brass Trees (C15-EB23) -
various heights
Etched Brass Trees (C5-EB23) -
various heights
Silver Birch (TW13-105)

4D Modelshop

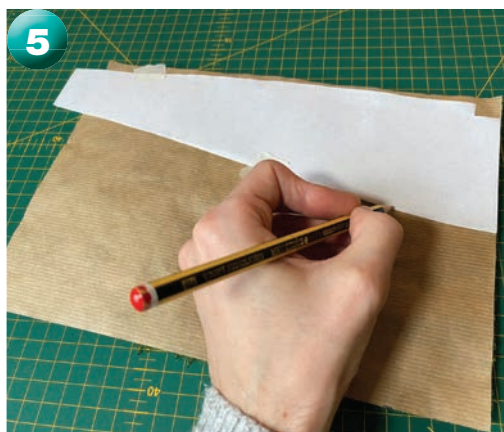
Etched Medium Green Trees -
various heights



Covering the slope with paper, the next step was to draw around the embankment to create a template to be used when cutting the grass mat that was going to serve as the base layer of the scene. When cutting the template, it is useful to make it slightly larger than you think you will need. You can always trim it back later, and this is much easier than finding you cut things too small and have awkward gaps to fill. The first time I drew the template, I also forgot to extend the top edge so that the mat will curve onto the edge of the road surface for a neater finish.

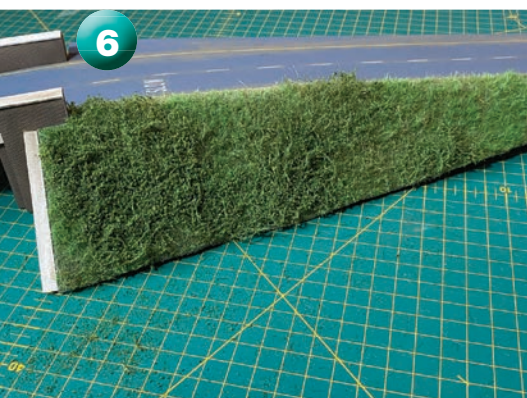


When it comes to modelling landscapes, I always work in a layered approach: grasses, the tree canopy, bushes, and then ground covering plants and weeds. Working in layers helps replicate the real natural world. The first layer that needs to be modelled, which forms the base upon which everything else gets added, is the grass. For the embankment scene, I used a Wild Spring Verge mat from Lars op't Hof Scenery. As with the Martin Welberg mats, these are paper-backed static grass sheets that come in a variety of different versions to match the four seasons and different types of grassland from meadows to overgrown forest floors.

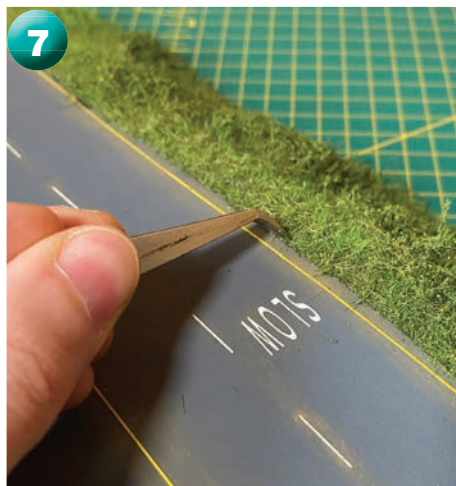


As the mats come in A4-sized sheets, I had to cut two sections. When drawing around the template, flip it over so you cut out the correct shape on the back of the grass mat. I have made this mistake many times when rushing! The two sections of the grass mat will be invisibly joined later on, but turning them over, the benefits of using these more detailed mats over a simpler roll of plain static grass is clear. Already there are different lengths of grass visible, and weeds and low-spreading plants.

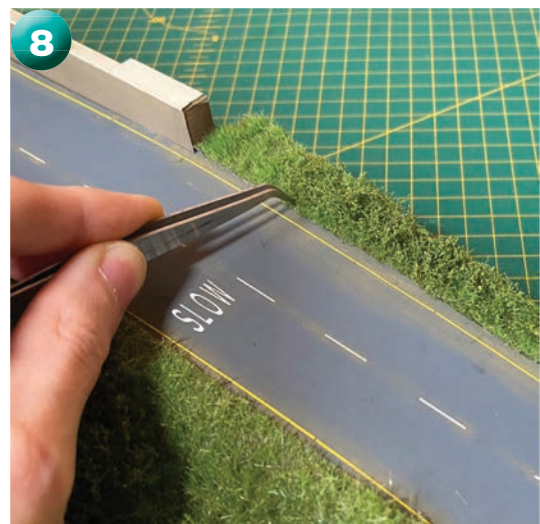
TECHNIQUES



UHU glue, or a similar adhesive, applied to the rear of the mat is sufficient to secure it to the embankment base. To get a neat edge, I have found that running a thin bead of glue just a few millimetres in from the edge of the mat works best. Leaving the glue to become tacky, turning the mat over, it fitted into place easily. Already this is starting to look like an overgrown embankment.



With the top of the mat extending onto the card road surface, pressing down hard with a pair of tweezers all the way along the edge helps create a neat and almost invisible joint. Any glue that seeps out during this process can be wiped away with a piece of kitchen roll.



For the other side of the road, where it will meet the backscene, I simply cut a long triangle of grass mat and applied it in the same way as before, again pressing down on the edge with a pair of tweezers.



With the embankment grass in place, the whole piece was ready to be stuck down to the layout. Any gaps that appear when you do this can be covered later on, but given the awkward position in a corner of the layout, it was much easier to get the grass in place before sticking the embankment down.



Choosing trees can be a bit of a minefield, especially as there are so many different ways they can be made. When buying pre-made trees, there are a number of things to consider. Firstly, where are you modelling, and when? Secondly, are you modelling young trees or established ones? For this scene, I used a mix of pre-made trees from 4D Modelshop and The Model Tree Shop.



Starting with the largest and most established trees, I applied a small amount of superglue to the base and pushed them gently through the grass mat and into the polystyrene foam underneath. Holding them upright while the glue set prevented any major wonkiness.



Placing trees on a scene can be a bit of trial and error until you find an arrangement that feels right. Moving from the back of the scene towards the front, I placed the trees where I wanted them, being careful not to have anything too large right next to the bridge.



The next layer of landscaping I tackled was the bushes. For this scene, I started by adding Early Summer Bushes from Model Scene. This comes as a single mass and can be cut with a pair of scissors to whatever size bush you need. These are ideal to create the appearance of large, woody bushes. With a generous blob of UHU glue underneath, these large bushes were placed along the embankment between the trees.



Cutting the bushes to different sizes helps give a more natural appearance to the scene, and I was careful to choose scenic products that were roughly of the same hue. Spring planting tends to be more vibrant than early summer, while later summer gets very yellow. Mixing too many different colours can really confuse the eye as to which season you are modelling, so I kept referring to photographs of early summer landscapes to keep the colours the same.



Using UHU glue again, I placed these tufts fairly randomly across the embankment and underneath the trees to create a feeling of a wild, unkempt landscape. Pressing down on the tufts helps embed them into the grass matt. You need to be careful that they don't completely fall apart as you do so.



With the individual bushes in place, the three layers added so far to the embankment really start to merge into one another, creating a fairly realistic scene. The joint between the grass mat and the rest of the baseboard is still very visible at this stage, however, and this is where the final layer – ground cover – comes into play.



Having placed the larger woody bushes onto the embankment, I then added some smaller ones using the excellent shrub tufts from the Martin Welberg range. These are sheets of static tufts that can be pulled off in clumps of any size you choose using a pair of tweezers. You can select from a range of different sheets that cover all the four seasons.



The final stage in the bush layer was to add individual 'feature' plants. For these, I turned to the Martin Welberg range again and used one of the four types of bush packs produced. These are finely detailed individual plants, which come lightly stuck to a paper base. A good tug with some tweezers releases them.



Martin Welberg Scenic Studios produces a large range of flowering grass tufts and sheets of general weeds. To add a little bit of colour to the scene, I decided to use some of its red flowering tufts. Pulling these from the backing sheet with tweezers, you can select the size of tuft you want.



Using a combination of the flowering tufts and shorter tufts from the Spring Bushes sheet, I covered the joint between the grass mat and the ballasted area of the layout, pressing down with the tweezers to hide any joints. The same approach was used on the embankment to hide any remaining joints between the two parts of the grass mat.

WEATHER TANKTAINERS

Words & photography: Michael Russell



If you want realism in your modelling, you have to weather everything. This is quite a bit of work, but it means that you can tailor your models to look exactly how you want. You don't have to use an airbrush and, on this occasion, I used only very basic equipment that any modeller is already likely to possess.

You will no doubt have invested a sizeable chunk of your spending money on your hobby and the thought of affecting the resale value of your kit through poor workmanship can be daunting. In fact, if you are unable to live with your efforts, you are faced with the replacement costs. Don't let this put you off as it is easier than you think and the results when you get it right are worth all the effort.

Since you can't expect to be a weathering expert overnight, use older, less valuable models or accessories to begin with and choose high-contrast subjects where the results can clearly be seen. Take your time, add the weathering in small amounts and, after each addition, come back to it later to evaluate your efforts before continuing.

How to best learn weathering

The most important thing when learning to weather is to have a point of reference to judge your efforts. If we have a set of vehicles, it makes sense from a practical stance to weather them all in one go, however, it helps if we keep one back in pristine condition. Weathering should be added in small increments, but it is very difficult to judge your progress this way.

You may look at a vehicle after a weathering session and think that you haven't made any progress, but, when you compare it with a pristine vehicle, you can see the difference and if it's subtle then so much the better. Of course, this slows things down and requires patience. If you have good lighting, a series of high-resolution detail photographs is a good second choice as a reference point.



Small paint brushes (flat and round)
Mixing bowl
Tissues



Add a small amount of dark grey acrylic paint to some matt acrylic varnish, mixing well. Use a small brush to apply it to the crevices of the model, paying attention to the walkways, vents and filling/uploading points and bands around the tank.



Use a damp brush to soften the edges of the line of grey, wiping away excess with a tissue. This will help blend the colour into the surrounding paintwork without leaving a stark edge.



With the model dry, add a buff colour acrylic to matt acrylic varnish, mixing well. Apply this over the model to fade the paintwork. Do this in stages so that you always have a dry part to handle.



Dry-brush a dark rust colour, then follow with a light rust colour on the metal framework. Concentrate on the outer edges and corners where the most wear would occur. Try not to overdo this stage.



It is important that your weathering marks aren't symmetrical as this doesn't look natural. Therefore, between each mark, rotate the brush slightly in your grip.



Add a small quantity of black weathering powder to a mixing bowl. Add white weathering powder and mix in until you have a dark grey colour. This will be used to form streaks along the side of the tanks.



Add the grey powder to a flat brush and drag this very, very lightly over the surface of the tanks using a downward motion. You will need to add this to the top, middle and lower sections of the tank as the frame stops you making one continuous stroke.



You now have the option of sealing the weathering powders with a matt varnish applied as a spray. This will make the model more durable for handling, but you may find the colours alter slightly depending on the varnish used.



(R60130) Concisa, TIP & Hoyer Container 20ft Tanktainer three-pack
www.hornby.com

Humbrol acrylic paints dark grey, pale stone, rust, sand, black and white weathering powders
www.humbrol.com

Anti-Shine Matt Varnish
www.expotools.com

Knowing when to stop

Any artist will tell you that a big challenge is knowing when a painting is finished and the same is true of weathering. It is difficult to over-weather many subjects, but hard to stop at the light, or medium weathering stages. In other words, it is easy to fall into the trap of making everything subject to heavy weathering. It is good to put a model aside for a while and come back to it with fresh eyes when we are more likely to make an objective decision. Of course, this is contrary to our natural inclination, because we are no doubt excited and probably impatient to see the results of our efforts. Ironically, the motivating force that makes us model is the same that can hold us back from progressing. This, of course, is where self-control comes in. Having several projects on the go simultaneously that we swap between is one method that may help with this issue.



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TOP 5 DCC PITFALLS

The common mistakes and how to avoid them.

Words: Ian Mellors



1 Budget Decoders
Just about everyone who starts out with DCC control makes this mistake – I did. The initial outlay can be quite prohibitive, and the temptation to get a cheap pack of decoders is very tempting. This is false economy because they may fail after a short time, and not provide good enough motor control. Moreover, they may not have all the features that you need as you grow with DCC. Quality decoders from the likes of

Zimo, Lenz and ESU will prove to be more reliable, less susceptible to short circuits and will have more sophisticated motor control, so will be more likely to work with your locomotive without complex adjustment. Standardising on a good, quality supplier makes things simpler when managing a fleet of locomotives.

It is also too easy to think, “It’s an old locomotive, I can’t justify an expensive decoder in it”. This is the opposite of what is needed. Older locomotives need decoders with very good motor control – as a rule of thumb, the older the locomotive, the more

you need to pay for your decoder.

Sometimes, it may be difficult to tell if you are buying a quality decoder, or a “re-badged” one made for a company by someone else. For example, the Bachmann 36-553 three function decoder is a Lokpilot Basic decoder 1.0 made by ESU and so would be considered a quality decoder, if a little limited in functionality.

Retailers and model shops who source their own branded decoders, are more focused on price. Always research where decoders are made to get what you need. If in doubt, stick to reputable brands.

2 Hornby Locomotives need Hornby decoders and controllers

You have bought your new Hornby locomotive and want to fit a decoder. It is natural to assume that you should fit a Hornby one, but this limits your choice of decoder.

The NMRA specification that all reputable suppliers work to, means that any decoder should work with any locomotive, provided you can fit it inside the bodyshell.

I've used Hornby as an example, but the same thing applies to all manufacturers who sell their own decoders, such as Bachmann, or Dapol.

Likewise, DCC controllers made to NMRA specifications will work with any suitable DCC decoder. You do not have to stick to one manufacturer's products. Mix and match different suppliers to get what works best for you.

3 Jerky running

One of the most common issues that people get when starting out with DCC, is problems with locomotives that ran fine on DC, starting to stall, or running jerkily once converted to DCC.

When using analogue DC control, we are applying a variable voltage to the motors of our locomotives directly. If there is a spot of dirty track or an insulfrog turnout is being used, chances are the locomotive will not be affected too badly by this, as the momentum of the locomotive and inertia in the motor will keep things running over this momentary loss of power.

With DCC, however, there's a microprocessor on board the decoder, between the track and motor. This needs to be powered at all times and will shut down at the slightest interruption of power, stopping the locomotive even if its momentum has taken it over the problem

area of track. DCC users also tend to run their locomotives a little slower than their DC counterparts, because it allows for better motor control. This can make the issue of dead spots more noticeable, too.

Scrupulously clean track and wheels are a must with DCC, but sometimes this is not enough. Having more wheels fitted with pick-ups is a great help – the trend for all-wheel pick-up from manufacturers is driven by this need.

In recent years, the option of fitting a power pack or 'Stay Alive' to the decoder has become more common. It provides a stored amount of charge to keep the microprocessor running for that split second when power is lost, keeping the locomotive running. Most modern decoders come ready-fitted with wires, or solder pads to allow the fitting of such devices. There are also specialists out there who can modify older decoders to suit, too. Of course, you need the space inside the locomotive to fit them – some small locomotives have barely enough room for the decoder, let alone a power pack.

4

Two or four-digit addressing

Most decoders can be set up with two-digit or four-digit addresses, it's up to you which option you use. However, a problem can occur when using two-digit addresses. It is easy to think you have given a locomotive a two-digit address of '26', for example, but have in fact set a four-digit address of '0026'. Some computer-based control systems, such as DecoderPro can do this if you are not careful.

Not a problem you would think, but some controllers like the NCE Powercab – a very popular handset, make a very clear distinction between these two addressing systems and will not respond to a locomotive set with an address of '0026' when keying in '26'.

It is best to set your locomotive addresses using the controller that you will use to operate your layout. This will avoid confusion.

5

Locomotive runs the wrong way

Sometimes you may find that your locomotive, once fitted with a decoder, runs the wrong way. What you expect to be forward ends up being backwards. This is especially common for those with 8-pin sockets – it is all too easy to plug the decoder in the wrong way round. Due to how the socket is designed, the locomotive will run, but in reverse. You would think that manufacturers would make it clear which is pin 1 on their locomotives, but this is not always the case. It is a simple matter to unplug the decoder and put it in the other way round.

There is another way to reverse the direction of movement without taking the body off – you can adjust the decoder's configuration value (CV) CV29. This method can throw up another issue, though. CV29 is a 'multifunction CV' – its setting controls several aspects of the decoder, depending on its value. Not only direction of travel, but amongst others, DC running, number of speed steps and information relating to four-digit addressing are also stored in it.

You may hear someone say, "Just set CV29 to 7 to reverse the direction". This is fraught with danger. Have you disabled DC running? Or more importantly, are you using a four-digit address? If so, setting CV 29 to 7 will mess up this address and the locomotive will not respond as you expect.

Always read the value of CV29 before making changes to it. The 2mm association has a great online calculator on its website that allows you to feed in the current value and display what this value means. Make your changes and the calculator displays the value that you need to write into CV29.

Finally, continuing my theme of using the correct terminology – can we stop using the term 'chip' when talking about decoders? Chips either come with your fish on a Friday, or in bags if you are American. It's a common term for the microprocessor that forms part of the decoder circuit board, not the whole assembly. 'Decoder' is a more appropriate term for what we fit into our locomotives...

BACKDATE A HATTONS

ANDREW BARCLAY

Words & photography: Dave Hall



In the past few years, modellers of UK industrial railways have been treated to several ready-to-run industrial locomotives in 4mm:1ft scale/OO gauge, starting with Hornby's rendition of the Sentinel four-wheel diesel-hydraulic. Since then, we have had diesel and steam engines from other manufacturers, with more in the pipeline, from the likes of Dapol and Rapido. Of all that have appeared so far, the most versatile for modification is the Andrew Barclay 0-4-0ST, from Hattons.

For those more used to main line railways and their locomotive types, the classification of industrial locomotives must seem rather strange inasmuch as there were usually no specific classes of steam locomotives, certainly nothing in the way, for instance,

an A4 denotes an LNER streamlined Pacific, or a Q6 a heavy freight 0-8-0 – both recognisable because all their classmates looked near identical to one another. The nearest thing to a classification system in the industrial world was often no more than a reference to the size of the cylinders – the bore and stroke – and the wheel arrangement.

Hattons offered a 14in and 16in 0-4-0ST, the latter having a version with a deep saddle tank and one with a shallower tank (as used on the 14in model). The design and build represented by the Hattons models is from around 1921 until the late 1940s. It is recognisable by the high pitch of the boiler and the simple, squared-off style of the cab side openings. The frame plates were also

squared-off at their lower corners.

Both of Hattons models are flawed but the 14in less so than the 16. The problem with the 16in is that it is overall shorter than the prototype in length. The reason being, presumably, that they did not want the expense of tooling to give it the proper length tank (it is 3mm too short) and so compromised by giving both types the same length tank and the same cabs. The flaw in the 14in is that the windows in the front of the cabs are positioned slightly too high. Again, presumably to save on tooling as the deeper tank of the 16in would partly obscure them if they were placed correctly.

They are still decent models but the 14in variant lends itself better to modifications, as I'll demonstrate...

Andrew Barclay works photo of w/n 1142, built for the Fife Coal Coy. In 1908. Courtesy Alan C. Baker and the Industrial Railway Society.



Andrew Barclay, Sons & Co.

Andrew Barclay, Sons & Co., to give it its full title, was located at Caledonia Works in the Ayrshire town of Kilmarnock and was founded by the eponymous Andrew Barclay in the late 1840s. Kilmarnock was also home to at least four other industrial locomotive builders, the products of all sharing a certain and recognisable family resemblance. One such company, Grant, Ritchie & Co. was set up by two former employees of Andrew Barclay. Thomas Maxwell Grant had been Barclay's works manager and the story goes that he and William Ritchie left Caledonia works one evening with bundles of drawings under their arms! How true that is we will never know and as that happened in 1876 it doesn't itself explain how locomotives being built more than 30 years later still looked almost identical, but it is a good story nonetheless.

The very earliest Barclay locomotives were of a completely different style to the subject of the Hattons model and were usually what is known as a "box tank"

saddle tanks. This style was common in Scotland, with Neilson locomotives being probably the best known of the type. Following the box tank, Barclays, and other Scots builders, used a style of saddle tank known as an Ogee (sometimes known as a Piano Tank). These appear to have gone out of fashion, to be replaced by the slab-sided saddle tank that we recognise almost as a Barclay trademark, sometime around 1890.

With a new batch of 14in and 16in Andrew Barclay 0-4-OSTs announced by Hattons, here's how to modify its smaller, more adaptable variant.

The subject of this article is the style of 14-inch 0-4-OST that, with various detail differences, was used from around 1890 until around 1908. It is a distinctive style that is easily recognised by the unusual portholes in the cab sides. Although there are no special tools required, and any modeller of moderate skill should be capable of the conversion, it does mean butchering an otherwise perfectly good model, which not everyone is willing to do but fortune favours the brave!



STRIP DOWN

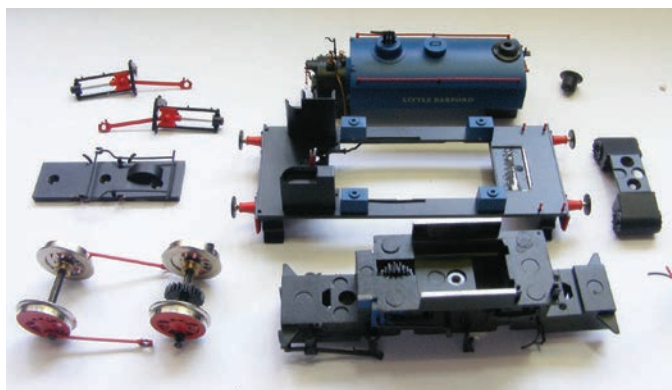
1 Firstly, we require a Philips-type screwdriver, a pair of tweezers, a dressmaker's pin and a 14BA spanner or socket. Some parts are small and easily lost – keep everything in a container.

To remove the chassis from the body, ease out the clacks from the boiler. These will probably snap off as they are extremely flimsy but don't worry if they do as replacements aren't difficult to make from brass wire and tube. Unfasten the two retaining screws on the underside and, minding that the clacks don't get trapped, ease the chassis out past the

injectors (on the underside of the running plate).

Next, remove the three coarse-threaded screws that fix the tank assembly to the running plate. The cab will also come away with the tank, so be careful to unplug the rear of the cab from the running plate, too. The cab can then be separated from the tank by easing one side open and it should then slide out.

The smaller parts, such as handrails and smokebox door handles can be gently prised out with the use of tweezers or by pushing a pin at the rear of the hole in which they sit.



REMOVING THE BOILER



2 The boiler of our chosen prototype, although of the same length and diameter as the type represented by the Hattons model, was pitched lower in relation to the frames and this is an obvious difference that can be seen when comparing photographs.

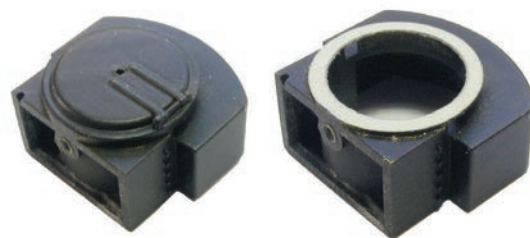
To get it to appear to sit lower, we must remove material from the lower side of both smokebox and firebox. There are

three pipes emitting from the top of the firebox, which run down the sides of it – these must be trimmed by taking 2mm off their ends before commencing the alteration of the mouldings. The oil pot at the front of the smokebox must also be removed.

Measure and note the distance from the bottom of the smokebox to the top of the tank and the distance from the bottom of the firebox mountings to the top of the curve of the firebox.

By rubbing the assembly on a sheet of wet and dry abrasive paper, and by checking the measurements regularly to ensure the assembly remains level. The overall height of the assembly needs to be lowered until a flat of approx. 1.5mm width appears on the outer ring of the smokebox. The corners of the underside of the tank, at the rear, must be trimmed to allow the modified assembly to fit on the chassis block.

MODIFY THE SMOKEBOX



3 The smokebox and door are one moulding and can be unclipped from the tank by easing and sliding one side out. Push the door handles out from the rear, using a dressmaker's pin and keep the handles safe. Removal of the smokebox door is effected by rubbing it against a sheet of 400 grit sand paper until the entire door is rubbed away and a hole in the outer ring is revealed. Continue until the ring is worn, evenly, as thin as possible without destroying it completely.

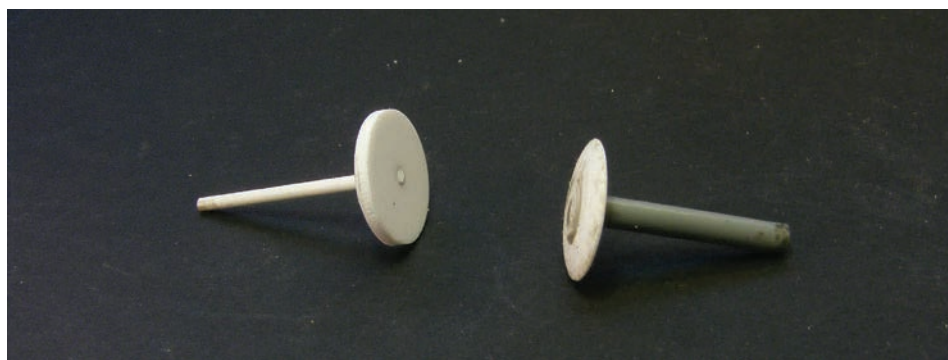
NEW SMOKEBOX DOOR

4 The Hattons model has what is a fairly standard Barclay smokebox door, suited to that prototype and also seen on earlier styles such as ours. What is not clear is just how many, if any, of our type were built with this style as Barclay works photographs show a smoother style than is on the Hattons model, and the outer ring of the smokebox was almost flush with the front of the tank. On the Hattons model, the ring protrudes approximately 1mm forward of the tank and, in general, this is not correct for our prototypes.

A new door can be made from a disc of styrene. To shape the disc, fit it to a short length of styrene rod and spin it in a Dremel-type drill. The shape is formed by holding wet and dry paper against the spinning disc. I use wooden coffee

stirrers with small pieces of wet and dry sheet glued to them. Keep wetting the disc in as even on the drill's slowest speed enough heat is generated to melt the styrene.

New hinges can be made from suitable lengths of 0.25mm Plastikard, with a hinge pin made from 0.5mm styrene rod. Finally, drill a 0.5mm hole in the centre of the new door and refit the handles. The door can be mounted on the smokebox using MEK or similar. The remains of the hole, from which the oil pot came, will need to be filled and a new one made to enable the pot to be refitted on the front of the smokebox.



NEXT MONTH: Dave shows us how to alter the cab



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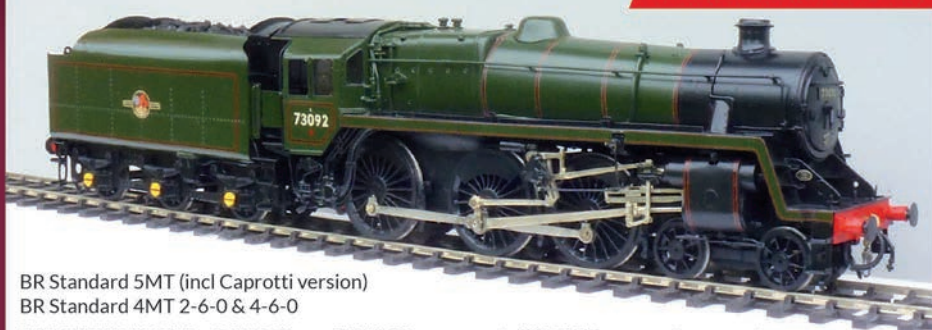
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DIGITAL EDITION



ON EXCLUSIVE

THE CALDER VALE MINERAL RAILWAY

The pre-grouping era is growing in popularity with adventurous modellers. Dave Hall's model is a playground for new RTR models and kits alike.

Words & photography: Dave Hall



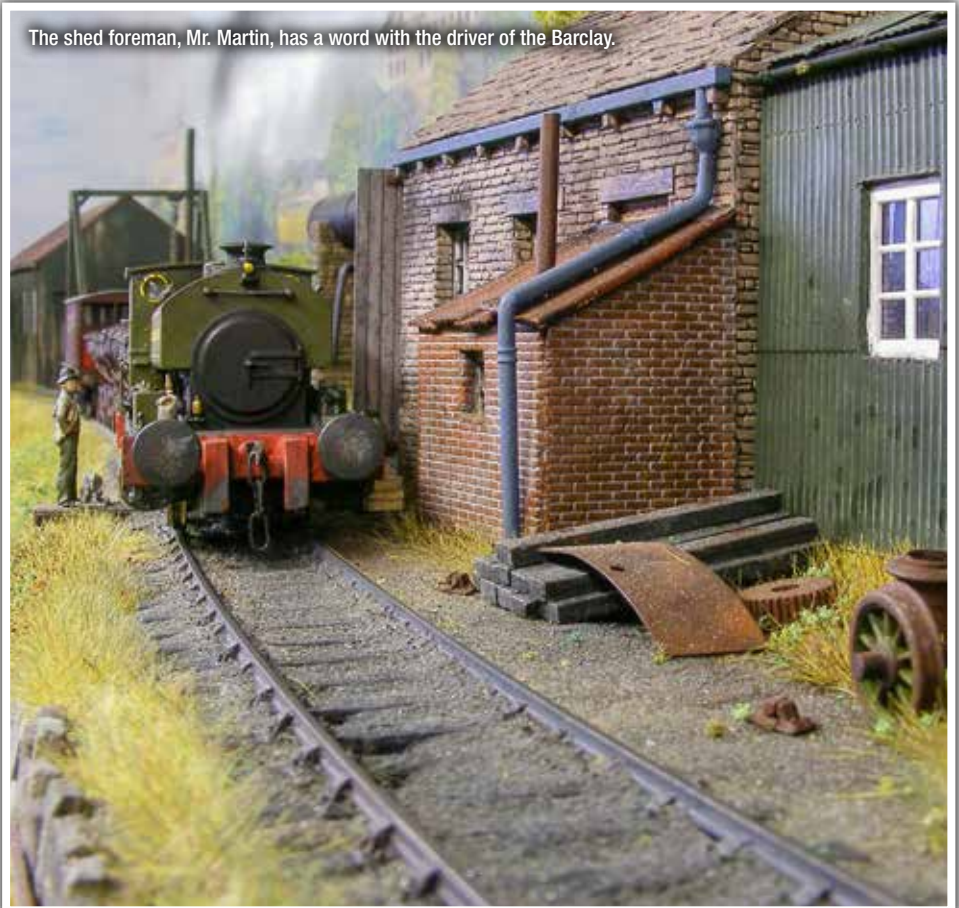
The Coal & Clay Companies' 16in Barclay passing the wagon works with a train of chaldrons from Ashgrove Collieries.

DIGITAL EDITION

As with all of my layouts, this one isn't main line, or mainstream, but, rather than pure industrial, I wanted a passenger service to run alongside the industrial aspect, so the idea of a small system incorporating a light railway came about. The period is 1900 until the Grouping of 1923, but I'm not strict on this.

When I originally built the layout, the intention was to go as far back as the 1870s. This period is something that I have never before modelled and the reason for choosing it is that I had taken an interest in the locomotives of the real-life I.W. Boulton. Isaac Watt Boulton was an engineer, locomotive builder and hirer in the last quarter of the 19th century. Details of his business and locomotives can be found in a fascinating book that is quite well-known in industrial railway circles – *The Chronicles of Boulton's Siding*, by A.R. Bennett. I had built three locomotives from Boulton's stable and this got me interested more generally in Victorian industrials and in the pre-Grouping railway era. It is so different from the commonly-modelled era of British Rail, where decrepitude is the order of the day for industrials and where weathering is taken

The shed foreman, Mr. Martin, has a word with the driver of the Barclay.



A load of fireclay, in contractors' type side-tipping wagons, being propelled from the Collieries. The wagons are a mixture of scratch-built and RT Models kits.



Knowles' pipeworks, manufacturers of sanitary pipes and chimney pots.

*A potted history of the 'CVMR' down to the year 1898, taken from the Oakwood Press book *The Calder Vale Mineral Railway*, by Albert Tatlock F.F.S. **

During the mid-nineteenth century, in the small area of the West Riding to the east of Halifax and north of the River Calder, there was a great variety of extractive industry, which included coal mines, fireclay mines and quarries. Manufacturing was also present in brick and pipe works, woollen mills and dyeworks. The companies were either opened up by, or taken over by two men. The first of whom was John (later Sir John) Taylor, who already owned mills and clay works near Sowerby Bridge (the Moorend Coal & Clay Co.). The other was Mr. Ozias Harding, a notoriously difficult and short-tempered man, who owned quarries, brickworks and coal mines in the area. Harding took a great interest in Egyptology and had his house, which he named Egypt House, built on a rocky outcrop so that he could observe the comings and goings on the railway from its octagonal tower. It was Harding who instigated the policy of naming the railway's locomotives after Egyptian gods and goddesses.

Although the distances from the various works to the local goods depots and sidings of the Lancashire & Yorkshire Railway were not great, the roads were poor and cartage was expensive, and so the two men joined to form a company that would build and operate a mineral railway to link up all of the works and take traffic in and out through one connection to the L&YR, at Elland. As the two men owned most of the land over which the railway was to run there was no need for an act of Parliament and what they didn't already own they either bought or negotiated wayleaves for and thus The Calder Vale Mineral Railway was born, with Harding as the Managing Director.

After considering tenders to build the railway from other railway contractors, the company settled on Exuperias Gittins & Co. Gittins was an eccentric character who visited the work sites on a velocipede and was also known for being miserly, or, as some would say, shrewd. As part-payment, he took a parcel of land on which he based his plant yard, which would, in turn, provide an additional source of traffic for the railway. The locomotives to build the railway were an assortment of weird and wonderful machines, hired from I.W. Boulton of Ashton-Under-Lyne.

For 40 years, the only passenger service on the line was for workmen but it was unofficially used by the local population for many years before, in 1897, the company applied for a Light Railway Order. After completing the required work, which included the addition of signalling and a staff and ticket system for single-line working, the railway was inspected by Lt. Col. Addison of the Board of Trade and was passed fit for use. A locomotive suitable for use on passenger working was acquired second hand from its builders' Beyer Peacock & Co. This locomotive was used in July 1898 to bring in the first public paying passenger service. The band played 'See the conquering hero comes', speeches were given and the directors, local dignitaries and all retired to The Colliers' Arms for a meal.

The 'CVMR' was connected to the L&YR at Elland and climbed through woodland to follow, at a higher level, the Calder & Hebble Navigation until turning northwards to run up a small valley at the foot of Hove Edge, until reaching Hipperholme. The only intermediate station was at Brookfoot, where a branch from the quarries joined the line, and where traffic to and from the Calder Vale Coal & Clay Co. came in from Ashgrove collieries. The railway company provided motive power for the public passenger service, the quarry trains and for the general goods services, while the Coal & Clay Co. handled its own trains with its own locomotives.

(* Fellow of the Flat cap Society)

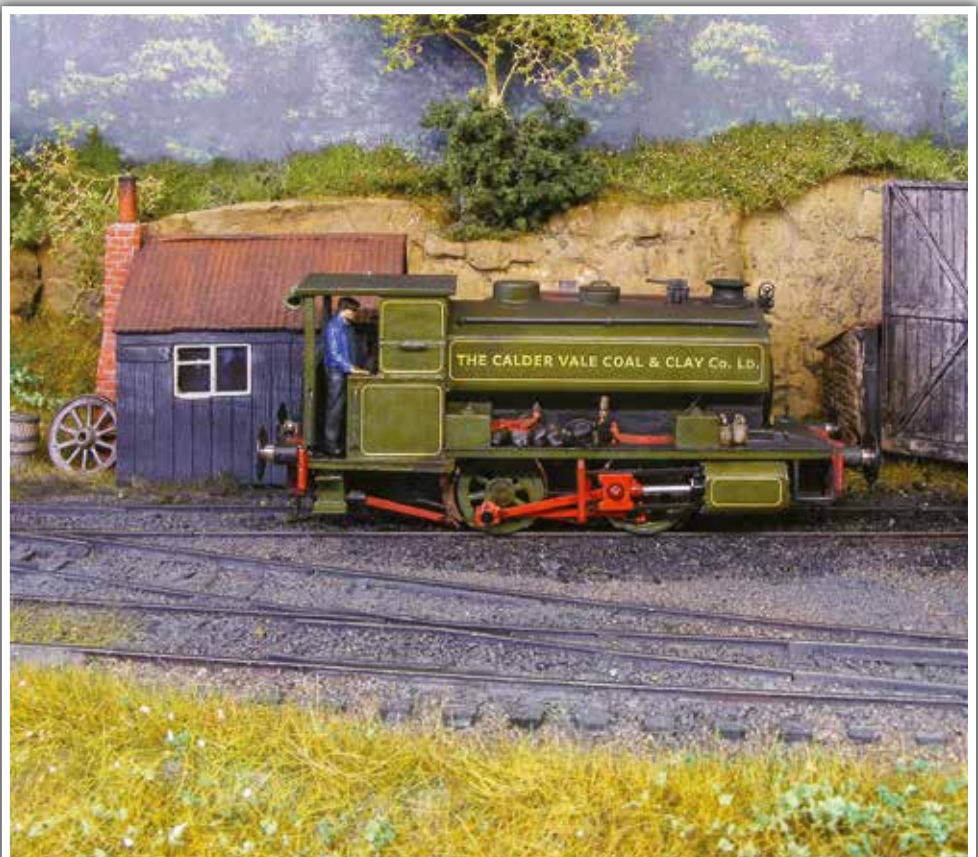
to extremes. Having built the models, which didn't fit in with the period of any layout that I owned at the time, they were set aside as display pieces, but I don't like having models that are only kept in a display case. I wanted something they could be run on.

After building the layout, I ran the varied fleet of unusual prototypes on plain DC control, but interest in the layout waned. Although not an exhibition layout, it was built to be operated by two people, but just as things were finished, and in a state to have friends visit for running sessions, the Coronavirus arrived on our shores, with Government-enforced lockdowns. That, and the fact that I had begun the construction of 'Charlie Strong's Yard', meant that I lost interest in the 'Calder Vale Mineral Railway' ('CVMR') and it lay dormant until recently when I decided to revive it. With 18 months of operating 'Charlie Strong's Yard' under my belt, I have become so taken with DCC control, sound-fitted locomotives, and the huge benefits that come with having stay-alive units on board, that I took the step of converting 'CVMR' to DCC control. Unfortunately, this has meant that many of the original fleet of locomotives have ended up back in a display case because they are too small, or it's too difficult to fit DCC apparatus into them. I intend to rebuild as many of them as possible so that they can be made to run on DCC and used on the layout once more. For now, however, the locomotive fleet is small and consists mostly of RTR items.

Planning begins

I wanted something to provide visual and operational interest and that meant having track on more than one level, with passenger trains, goods trains to the main line, and internal goods movements. An engine shed was essential – I love industrial engine sheds. I also wanted to add in the flavour of some of the industry that still exists in the area in the form of the pipe works but other than that, I really made most of it up as I went along.

The set-up had to have a canal and show the valley side to reflect the area between Brighouse and Elland, even though not one bit of it is like the real place. I managed to get in all my requirements, but it meant that the only level section on the layout is the passing loop at the passenger platform. The gradient from the lower-level to the upper-level line is about 1-in-18, which presents challenges to the small locomotives in use,





Locomotives of the 'CVMR'

The original fleet of kit and scratch-built locomotives had grown to 11 before switching to DCC. Out of those, there are as few as three that can be converted to DCC, so the fleet at the moment stands at just four, all of which are RTR, albeit modified. The motive power stud is also supplemented by visiting locomotives, belonging to my friend Scott Martin, when he comes to have a running session with the layout.

Four locomotives are required for a full running session, with a company one dedicated to the passenger service, another company one on goods, a Coal & Clay Co. locomotive, and a quarry locomotive.

At the moment, available motive power is:

A Beyer Peacock 2-4-0T, named Ra, of the type used on the Isle of Wight Railway, among others. It is built more or less as intended from a whitmetal kit, by Golden Arrow Models. I have also fitted sound and a stay-alive. It is powered by a Mashima 1020 motor, driving a High Level 90:1 gearbox.

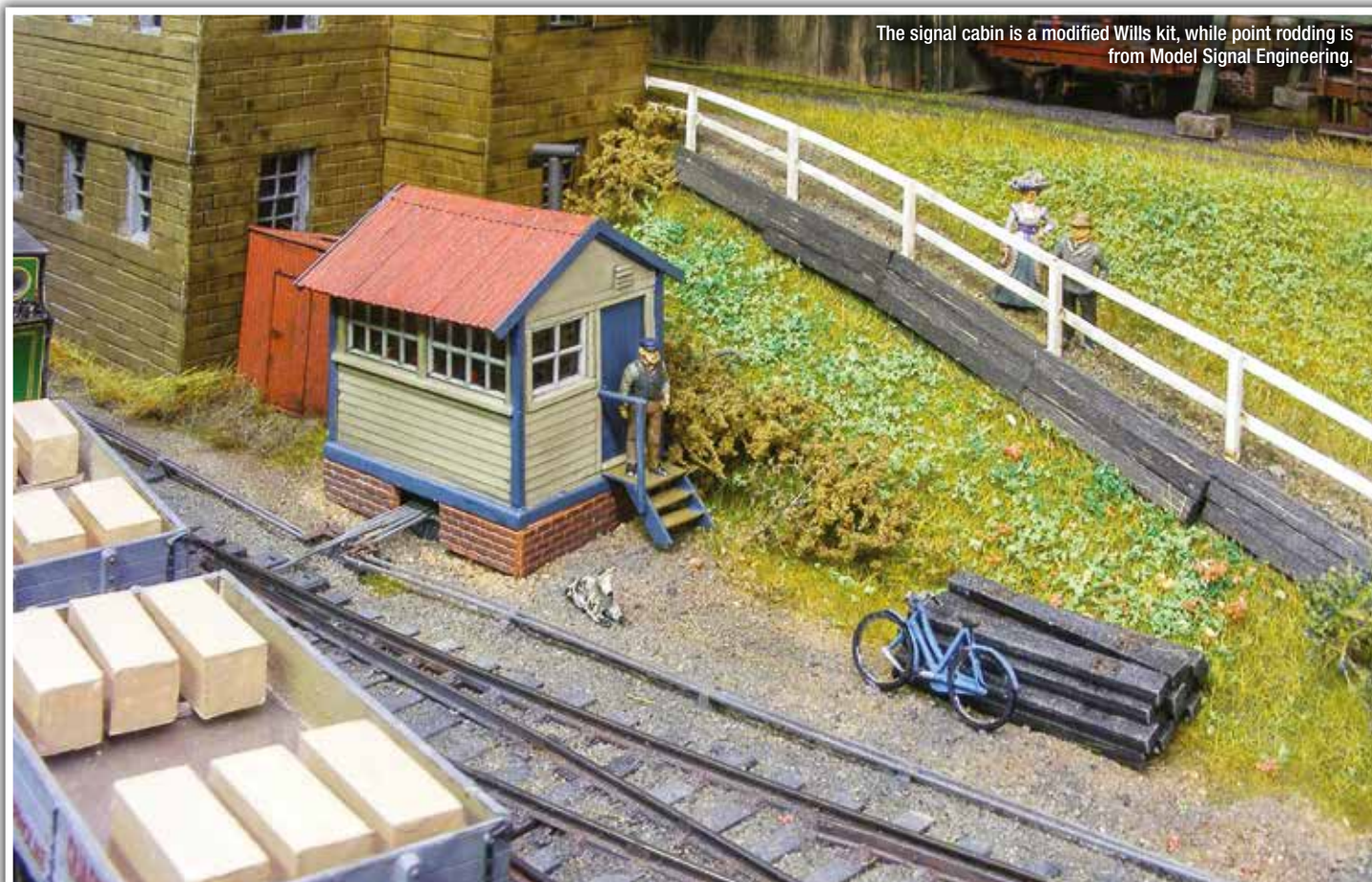
A Peckett W4, named Osiris. This is a Hornby RTR product that has been detailed, weathered and fitted with sound and a stay-alive. To allow the stay-alive to be fitted, the Hornby chassis block was milled out so a High Level 60:1 Roadrunner gearbox could be fitted, which in turn freed up space. The underside of the chassis block has been milled out under the cab to allow a 11 x 15 x 5mm speaker to be fitted.

A Peckett C Class 0-6-0ST, named Nephtyhs. This started out as a Hornby B2 Peckett but has had the chassis milled to alter the profile. A new keeper plate has been made and the brake gear repositioned to suit the replacement wheels, which are from Hornby W4 Pecketts. The motor mount was removed and the motor refitted in such a way to allow a decoder, speaker and stay-alive to be fitted without any of it being visible in the cab.

The Coal & Clay locomotive is an Andrew Barclay 0-4-0ST. This is a modified Hattons RTR product and is at the very end of my permitted time period, being as it could not have been built before 1921. This model has had quite extreme modification, with a dropped footplate/lowered cab, cut-down chimney, flattened cylinder sides and relocation of the safety valves. It has had these modifications based on a prototype example that worked at Provan gas works. Internally it has been modified to fit sound and a stay-alive. This work included re-motoring it and milling a space in the chassis block to enable the fitting of the stay-alive.

The visiting locomotives are a standard Hornby B2 Peckett, which has had DCC sound and a stay-alive fitted, and a Manning Wardle 16in special. This was built using a Railwaymania 3D-printed body kit on a Hornby B2 Peckett chassis.





but as it limits the length of trains, it means the fiddle yards didn't need to be much longer than the short trains themselves.

Layout control

When built, the layout was wired for DC control, using centre-off DPDT switches, so that either operator could take control of a section. Both operators used Gaugemaster Model W hand-held controllers. The wiring for DC wasn't ideal for DCC operation and although it all works as far as running is concerned, various issues remain to be sorted out.

The new control system is the same type used on 'Charlie Strong's Yard', namely a Piko Smartcontrol Light. It was chosen so that, when I have a second operator, I simply borrow the handset from 'Charlie Strong's Yard', or they can bring along their own, providing it is also a Loconet system.

To keep things simple, points are switched by pulling and pushing wooden knobs that are attached to piano wire rods under the baseboard. Micro switches take care of the frog polarity. Two points, however, are electrically switched as it was impractical to get the manual system in. These use Seep solenoid motors, but the toggle switches that

control them have been disguised with the same wooden knobs as on the manually-controlled points and signals.

Setting the scene

The hillside and embankment are made from cardboard and bits of plywood, covered in papiermâché, made from newspaper and PVA glue. It's old school and it's cheap. This is the greenest layout that I have ever built and it took a lot of static grass and ground coloured foam to cover it. I'm reasonably happy with how it's turned out, though perhaps it could be better.

The areas of exposed rock face beneath the manager's house were made from plaster, using Woodland Scenics moulds, and from expanding foam that was carved to shape after setting.

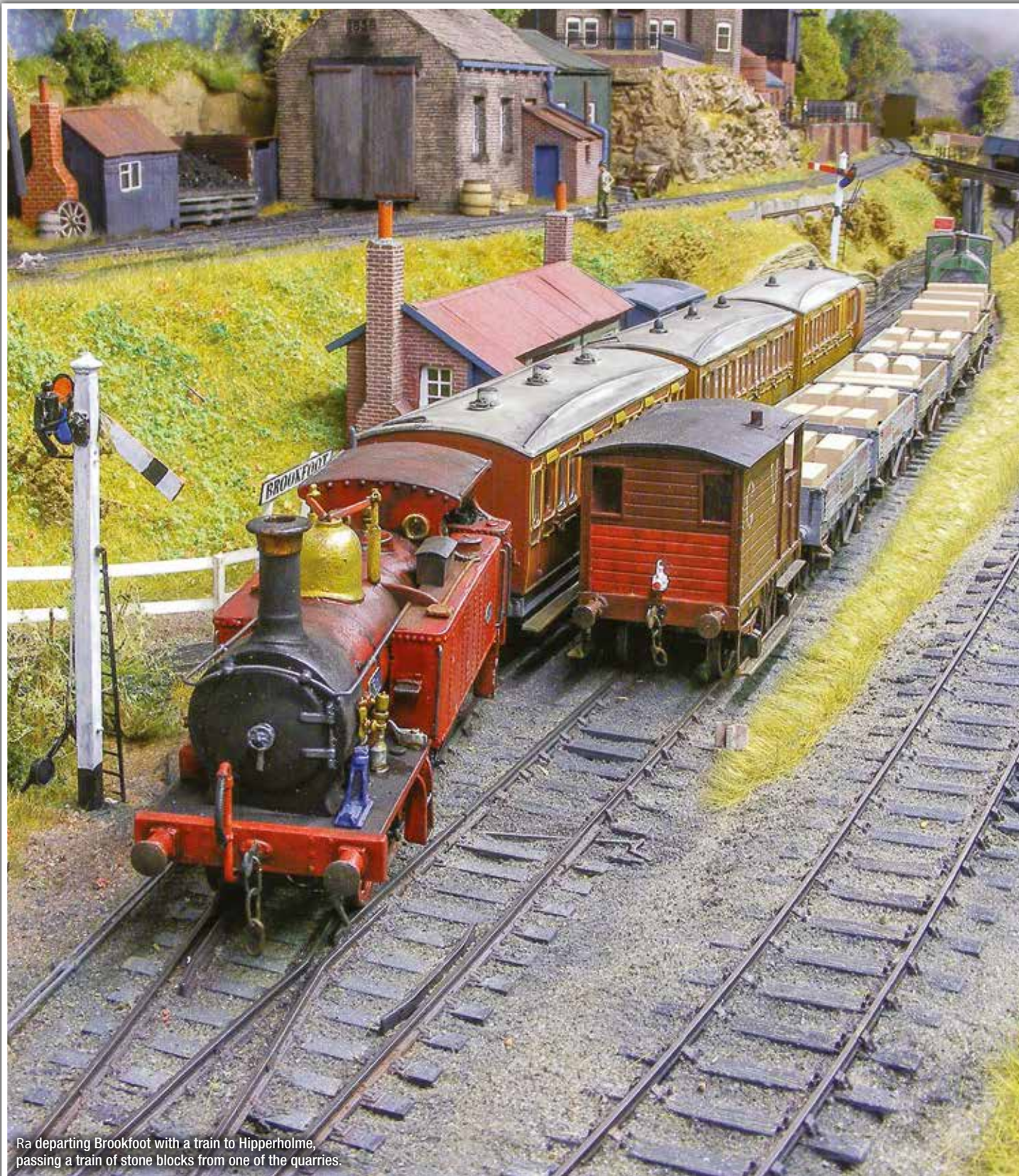
When it came to buildings, I had bought some ready-to-place resin items at a bargain price, but using them as they were would have meant altering the track layout to suit them, and I wasn't happy with the result. Two have been used, those being the main building of Knowles pipe works (a Hornby colliery building), which has been modified and includes some scratch-built additions, and Egypt House, the residence of Ozias

Harding (Bachmann Hampton House). I particularly liked this building, with its octagonal tower, and could imagine that Harding would have his office in the tower, where he could observe the comings and goings on his railway. The beehive kilns and chimneys at the pipe works aren't ready-to-place, but aren't kits either, being single-piece unpainted resin mouldings from Skytrex.

Other buildings and structures are modified kits, or are scratch-built, with the engine shed being my best effort at scratch-building on this layout. It uses a card shell, covered with embossed stone Plastikard. The roof trusses are made from cut-down coffee stirrers and the roofing, representing the local style of stone flags, was done by cutting and fitting individual pieces of thick paper, as used for watercolour paintings.

Running schedule

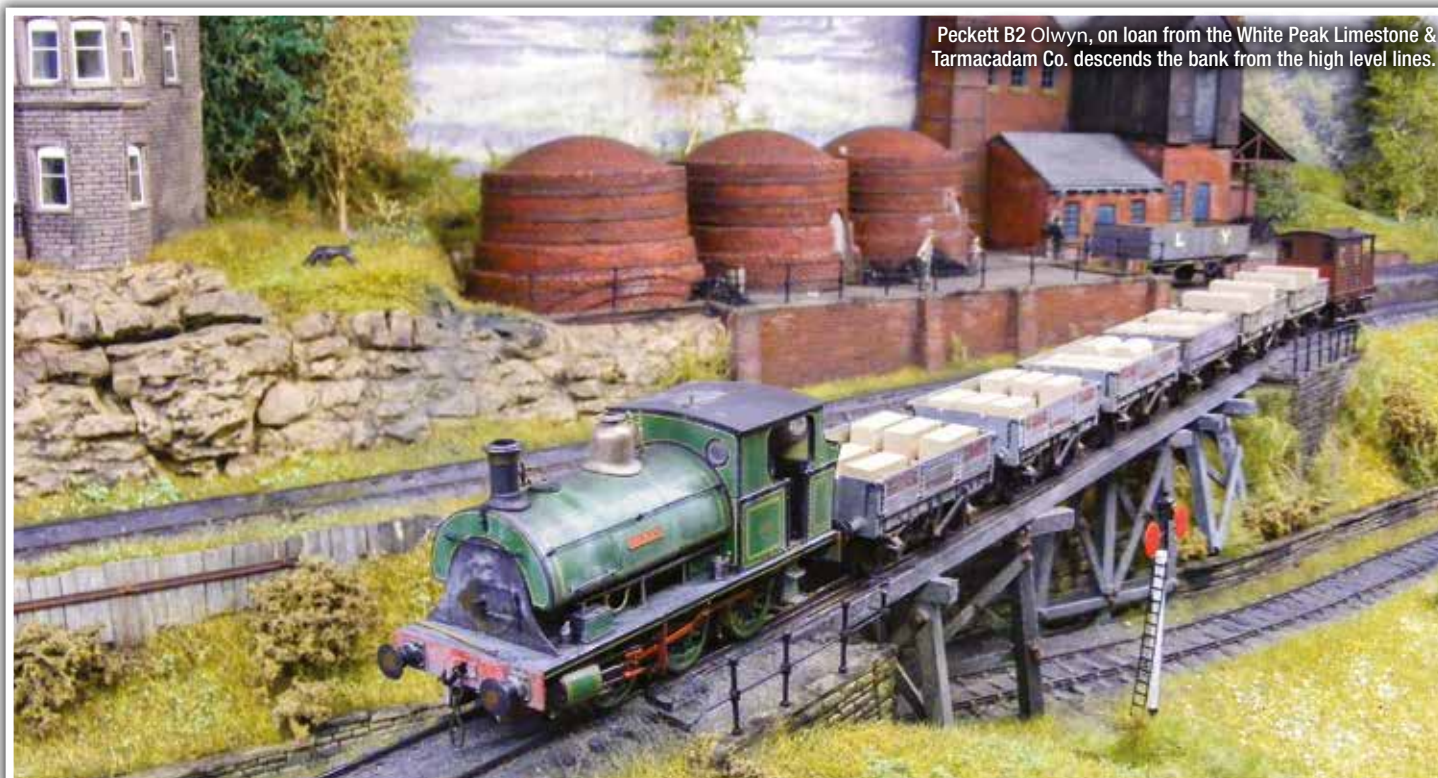
Being what is essentially a private railway, there is no strict timetable as such (except for the passenger service) but rather a sequence of movements is worked out and must be followed, more or less. The 'CVMR' is supposedly connected to the L&YR at Elland and its 'main' line runs along the



Ra departing Brookfoot with a train to Hipperholme, passing a train of stone blocks from one of the quarries.

Signals

Being a mineral/light railway, there is no need for the same amount of signalling as there would be on a main line railway and so there are only three in the modelled area. They are manually-worked in the same way as the points. In certain places, there are stop boards instead of signals and on the descent from the high-level line there is a stop board and a moveable block on the line instead of a catch point. The block doesn't work in model form but in theory, the driver of a train descending the bank would stop at the board and the fireman or shunter would cross to the signal cabin to collect the token, which would have a key for the padlock on the block attached to it to unlock and move the block. Two of the modelled signals were made from parts bought from Model Signal Engineering, while the other, an ancient L&YR bar signal, was scratch-built from brass, with an MSE whitemetal lamp.



Peckett B2 Olwyn, on loan from the White Peak Limestone & Tarmacadam Co. descends the bank from the high level lines.

bottom of Hove Edge to Hipperholme, where there is a passenger station and public goods facilities, so all of the passenger trains run to and from Hipperholme.

Public goods trains detach wagons at the modelled station of Brookfoot and these can go to any of five destinations – Taylor's mill, The quarries, Knowles Siding (the pipe works), Gittins' yard, and the colliery of the Calder Vale Coal & Clay Co. Wagons left at Brookfoot from these locations are also picked up to go to Elland and beyond. This is worked by using a pack of cards with the specific wagon and its destination on each card so that different wagons get used and the logistics of getting them to their destinations is always changing.

Other trains are what could be described as block trains in that the whole train originates at one place and travels as a train

to the L&YR for marshalling further on. These are the coal and the stone trains.

There are also internal trains that remain on the CVMR. Coal, loaded in chaldron wagons, goes to a wharf up the Calder & Hebble Navigation and clay goes from the colliery to a brick works.

Rolling stock

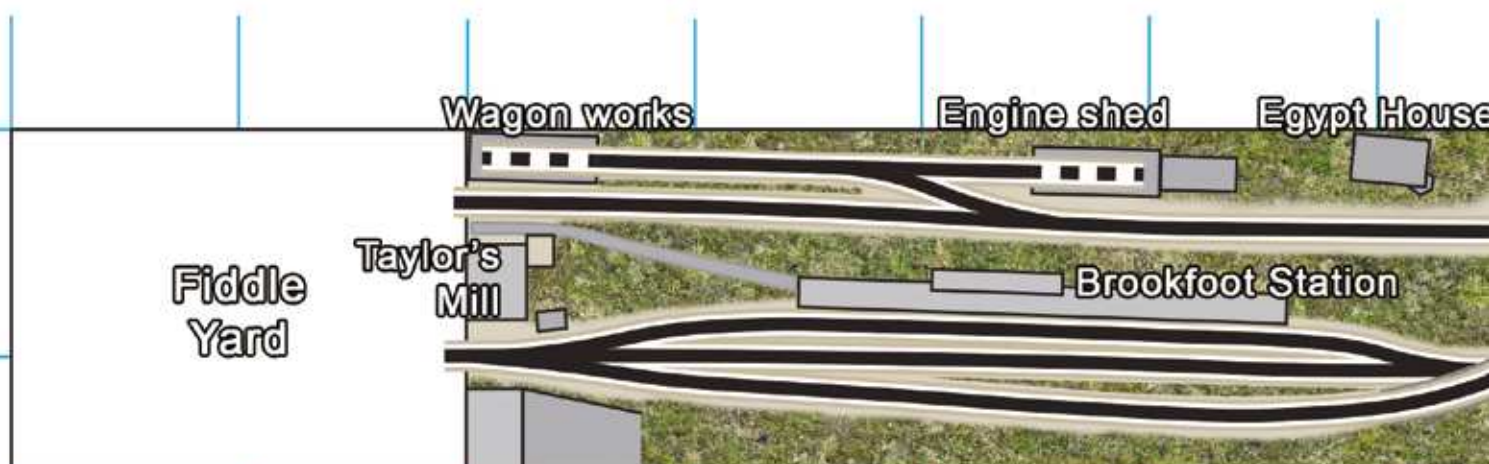
In addition to being an industrial locomotive freak, I am also a bit of a wagon nerd and delving into the pre-Grouping era has shown up just how poorly served by the RTR model industry it is. Having chosen to set my fictional mineral railway in L&YR territory, I found that there isn't a single L&Y wagon available and with David Geen's kits being no longer available, even kits are scarce, but it hasn't put me off. At the most basic, I have used generic RTR wagons with a repaint,

lettering and the addition of sheets. I have also scratch-built some to L&Y diagrams, thanks to the excellent Wild Swan books by Noel Coates.

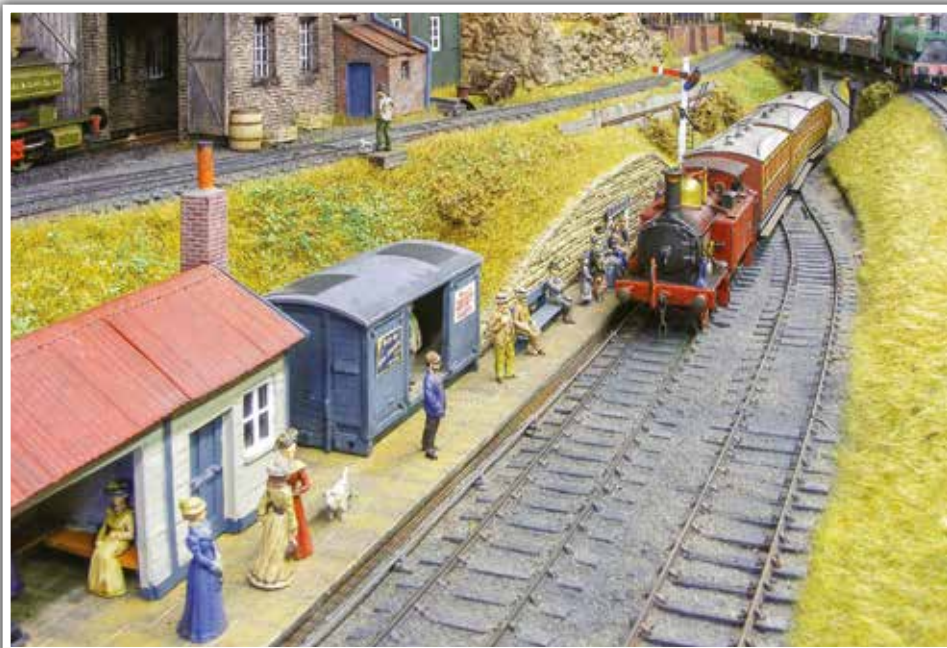
Other railway company wagons have been built from kits or by modifying RTR items. There are a few RTR Private Owner wagons thrown in, too. Other kits and scratch-builds have been lettered for the fictional companies served by the CVMR.

The CVMR brake vans are a mixture of modified kits and freelance scratch-builds that are based on prototypes from the Great Northern Railway, Midland Railway and the Rhymney Railway.

There's something about old four-wheeled coaches that made me want to model them and have a passenger service. I was never interested in them before building this layout.



ON EXCLUSIVE



Beyer Peacock Ra enters Brookfoot Station.



Peckett Osiris halted at the ancient former L&YR bar signal at the entry to Brookfoot Station.

The current coaching stock consists of two Great North of Scotland Railway coaches that date from the 1850s/'60s. They are built from Prickley Pear Products etched brass kits, which are very well designed. Other coaches are also made from etched brass kits, by London Road Models and are 1860s London & North Western Railway types. These are supposedly the original coaches of the 'CVMR' that are now relegated to use on workmen's trains and have been finished in a fictional livery. The stock for the public passenger service consists of three of Hornby's recent generic four-wheel coaches. Also running in the passenger service is a birdcage brake van, which is a modified Slater's kit for a North Eastern Railway goods brake.

Looking ahead

The immediate plan is to rewire the layout specifically for DCC. I would also like to add a few more trees along the backscene and other scenic touches. A more complex future plan is to allow the timeline to be brought forward. This would require things such as the signal box and passenger facilities to be done away with and things to look worn and overgrown in order to bring the layout up to the 1960s, where it would be quite run down, goods-only and operated by diesel power. The problem is that I don't want that change to be permanent and I haven't yet thought how things such as adding overgrown track and extra foliage encroaching on the track could be undone.

FACTFILE

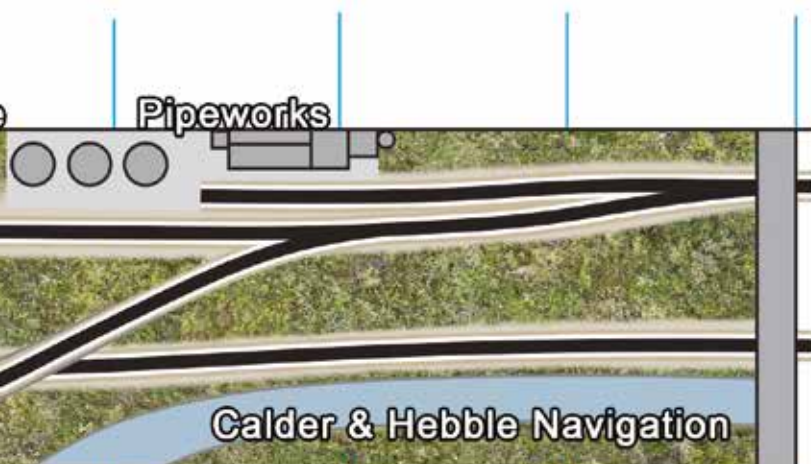
Layout name: The Calder Vale Mineral Railway

Scale/Gauge: 1:76 scale / 16.5mm gauge / OO

Size: 13ft x 18in

Era/Region: 1900-1923. The Calder Valley, West Riding

Layout type: End-to-end



Fiddle
Yard

Calder & Hebble Navigation

A DISPLAY FIT FOR A QUARRY HUNSLET

Phil Parker builds a dust-proof little layout ideal for showcasing your latest locomotive.

Words & photography: Phil Parker

Building model railways in unusual containers has a long history. It seems modellers can't look at a guitar case without thinking, "I could build a layout in there".

If you visit the National Railway Museum, the North Hall contains a display case holding the early 2mm scale "Inversnecky and Drambuie Railway", built by the late Ron Bryant.

I don't have a guitar case, nor any musical ability whatsoever, but I do spend time looking at cake, and it occurred to me that an acrylic cake cover might make an amusing layout holder. At the very least, it would keep the dust off my model, and hopefully protect it from finger-poking at exhibitions.

The Japanese specialise in very tiny layouts, and while I didn't want to build a highly-detailed two-inch square novelties, fascinating though they are, there's still material to be used. In this case, a circle of Tomix inlaid track, sent to me by a friend. It's really good, and would make an excellent basis for a layout.

As it is, I was able to prove a Bachmann 009 Quarry Hunslet would happily circle on it – and so, if I laid a slightly larger circle of Peco in the more spacious cake cover, all my small locos could take a turn. Short wheelbases are the key, however. The Hunslet is fine, but the same firm's longer Baldwin won't work.

Therefore, my layout (assuming you can call something so small and simple a layout) is, therefore, a novelty which gives a loco the chance to stretch its legs. I imagine it sitting on a sideboard or coffee table, so it has to look attractive to non-enthusiasts. Pretty scenery and a cute building are called for, and I knew what I wanted to build for the latter.

Really, there's not much space for actual scenery, but a good, thick clump of trees



hides half of the circle. Potentially, I could build a second scene behind the trees, effectively, two layouts in one! Having the loco go in and out of view makes the model look a lot more interesting, and less toy-like. I've added a red telephone box to encourage on-lookers to move around the model for

different views.

Now complete, I'm sure this tiny model will amuse visitors to the World of Railways stand at future exhibitions. Probably more so than larger layouts I've built, but then people love novelty!



Inspiration. As I stood in the queue for food at Alexandra Palace this year, I found myself wondering what sort of diorama could be built under one of these transparent covers. After all, if a delicious-looking Victoria Sponge will fit, there's got to be plenty of space in there.



Nisbets

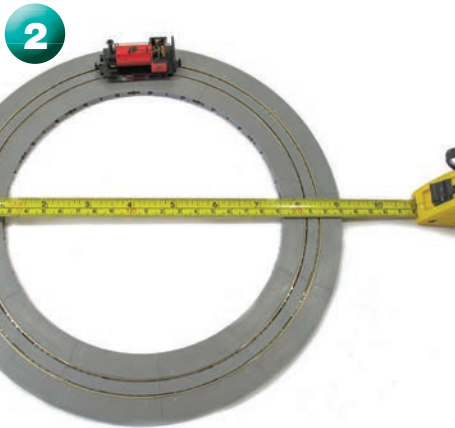
FE473 - Olympia Kristallon Polycarbonate Display Cover
FE474 - Olympia Kristallon Polycarbonate Display Plate

Dapol

C011 - Trackside Buildings
Langley Models
F31 - Telephone box kit

Scale Model Scenery

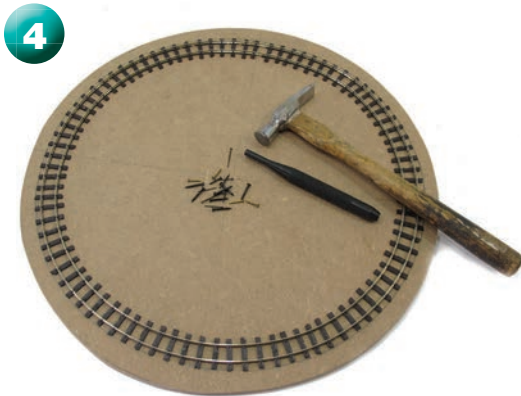
LX102 - School/Park railings and gates
LX239 - Parish/Village noticeboards
S022 - Early station signs



The cover is 29cm diameter, and this circle of Tomix track is 22 – and since a Bachmann Quarry Hunslet will trundle around it perfectly happily, a working layout appears to be on the cards.



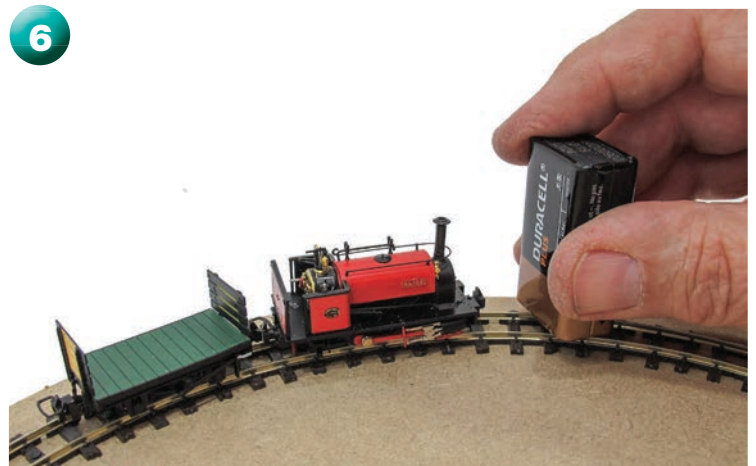
First, we need a baseboard. Normally, I'd use plywood, but without bracing, it can warp once the wet scenic materials are applied, so this time, I'm using some 6mm thick MDF carefully cut to shape with a jigsaw fitted with a laminate blade. The result was so neat, it didn't even need to be sanded.



A length of Peco 009 track is fixed down. It's not a perfect circle, there is a 2cm long straight at the front and back, so the rail joint isn't on a bend. That way leads to doglegs as the rails try to straighten themselves.



With such tight curves, it pays to give each sleeper a wiggle to ensure it's as close to right angles to the rail as possible. This reduces the stresses as the rails are properly supported.



Time for a quick test with the Hunslet. A 9V battery is perfect for this, wiring up can come later, although we are only talking about a pair of wires, nothing too taxing.

COUPLING ON TIGHT CURVES

Talking to other micro layout builders, once you bend track very tightly, there can be a problem with the standard Bemo couplings fitted to most 009 rolling stock. On a short wheelbase wagon, they can stick out a long way, and on a tight curve, which means the coupling pins will be closer to the rail rather than the centre of the track.

West Hill Wagon Works has produced a new, swivelling version, that fits into the NEM pocket. In addition, it incorporates magnets for coupling, although a pin is still fitted for BEMO compatibility.



Before ballasting, with Geoscenics N gauge limestone, the track is sprayed with Railmatch sleeper grime. It's important to leave this overnight to harden before ballasting, or the stones will strip the paint off the sleepers.



Most importantly, check the trains will pass before fixing the platform in place. The slide bar supports on the Hunslet stick out a bit more than I thought, and could easily catch the platform if it's not set well back.



I'm using a 3D-printed 009 platform from West Hill Wagon Works. However, this is straight, so needs to be chopped up with a fine saw to bend it to match the track. The resin is a little brittle, so go carefully, or chunks can break off. Superglue can affect repairs if this happens.



Scenery starts with a big blob of DIY store wall filler to build up some ground contours. This is painted with emulsion paint using sponges as this makes blending the colours much easier than if you use a brush. In the photo, I'll dab along the line between the light and dark brown, and the result will look like I sprayed it.



As plaster dries, it shrinks. Cracks open up in the surface, especially when it's 6mm thick. What I should do is carefully fill these, but as I'm in a rush, I put a bit of PVA down the crack, and rub some green scatter into it, the result looking like an unkept road.



Trees are nothing more sophisticated than pieces of seafoam ("Forest in a box") planted in holes, sprayed with hair lacquer and sprinkled with green scatter material. I'm aiming for a clump to block the view from front to back of the model, rather than some nice specimen models.



Finally, fencing, noticeboards and "Beware of the Trains" sign, are all laser-cut kits from scalemodels scenery and bring the scene to life.

FROM COAL OFFICE TO STATION



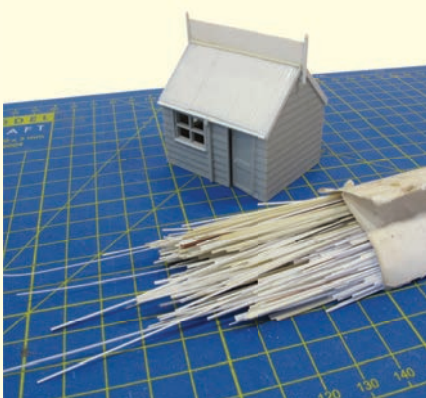
Inspiration for the station building comes from the late Roy C Link's Crowsnest Tramway. Roy built his line several times, in both 1:32 and 16mm scale, but the station remained largely the same in each version. I've always loved the look of the model, and was lucky enough to come across one version at a Canadian model railway exhibition.



Roy was undoubtedly a master modelmaker, and I am not, so my station would be influenced by his design, rather than a copy. The basic building starts as the venerable Airfix Coal Office, but I replace the slate roof, which isn't great, with some Wills Finecast corrugated iron.



A handy hint to improve Airfix buildings is to fit the windows inside out. On the left, we have the window fitted as per the instructions. On the right, fitted from the inside, with a plastic window sill attached at the bottom.



The signboard on top is scratch-built, mainly from pieces from my ever-useful assorted pack of Slater's microstrip. No modeller should be without these handy materials; I tend to top the packet up every so often so there's usually a piece the right size when I need it.



Roy wasn't only a superb modelmaker, but a talented signwriter, so his signboard looks more ornate than mine, which was quickly run off on the computer. Green window frames set the model before the 1960s when ICI introduced non-fading white paint.

VENTNOR WEST

No, not the one most think about, the other – Chris Gardner explains the history and his model of the 'chocolate box-esque' Isle of Wight location.

Words: Chris Gardner Photography: Jamie Warne

One of the attractions for modelling the railways on the Isle of Wight is that of all the prototypes one could choose, they most closely resembled the model railway “norm” of short trains, compact stations and (at least in summer) a lot of movement. The branch line to Ventnor Town, later renamed Ventnor West, was the last addition to the busy network and was completed in 1900.

It was an ambitious scheme by the Isle of Wight Central Railway (IWCR) to compete with the Isle of Wight Railway (IWR), which already had a station at Ventnor. Ventnor was served by a direct route from Ryde, where ferries arrived from Portsmouth, and the Ventnor West branch was intended to benefit from holidaymakers who sailed from Southampton to Cowes. Its main selling point was that at 168ft above Ventnor, it was more convenient than its competitor, 294ft above. One cannot help feeling that the town

might have been better served if a funicular railway had been constructed from the original Ventnor Station, following the example set in 1890 at Lynton and Lynmouth in Devon.

The ambition of the line’s promoters was reflected in the specification of Ventnor West station, which boasted two 300ft platforms, as well as ‘refreshment rooms’.

The line never fulfilled the expectation. The passenger service, originally eight through trains from Cowes daily, was reduced over time to a push-pull service from Merstone Junction. In 1952, it became the first Island line to be closed. The route is now open again, but only for cyclists, equestrians and pedestrians.

In 2016, a local modelling friend of mine, Mark Pretious, had completed a model of Merstone Junction, and I lighted on the idea of building ‘Ventnor West’ as an extension.



W8 *Freshwater* arrives with the push-pull from Merstone. The locomotive is by Hornby, suitably re-lettered and numbered.



It's 1949 and the branch push pull, with freshly-painted rolling stock, waits for departure while No. 1 *Medina* takes water in the background. The push pull set is an authentic model, a brass kit from Worsley Works.

FACTFILE

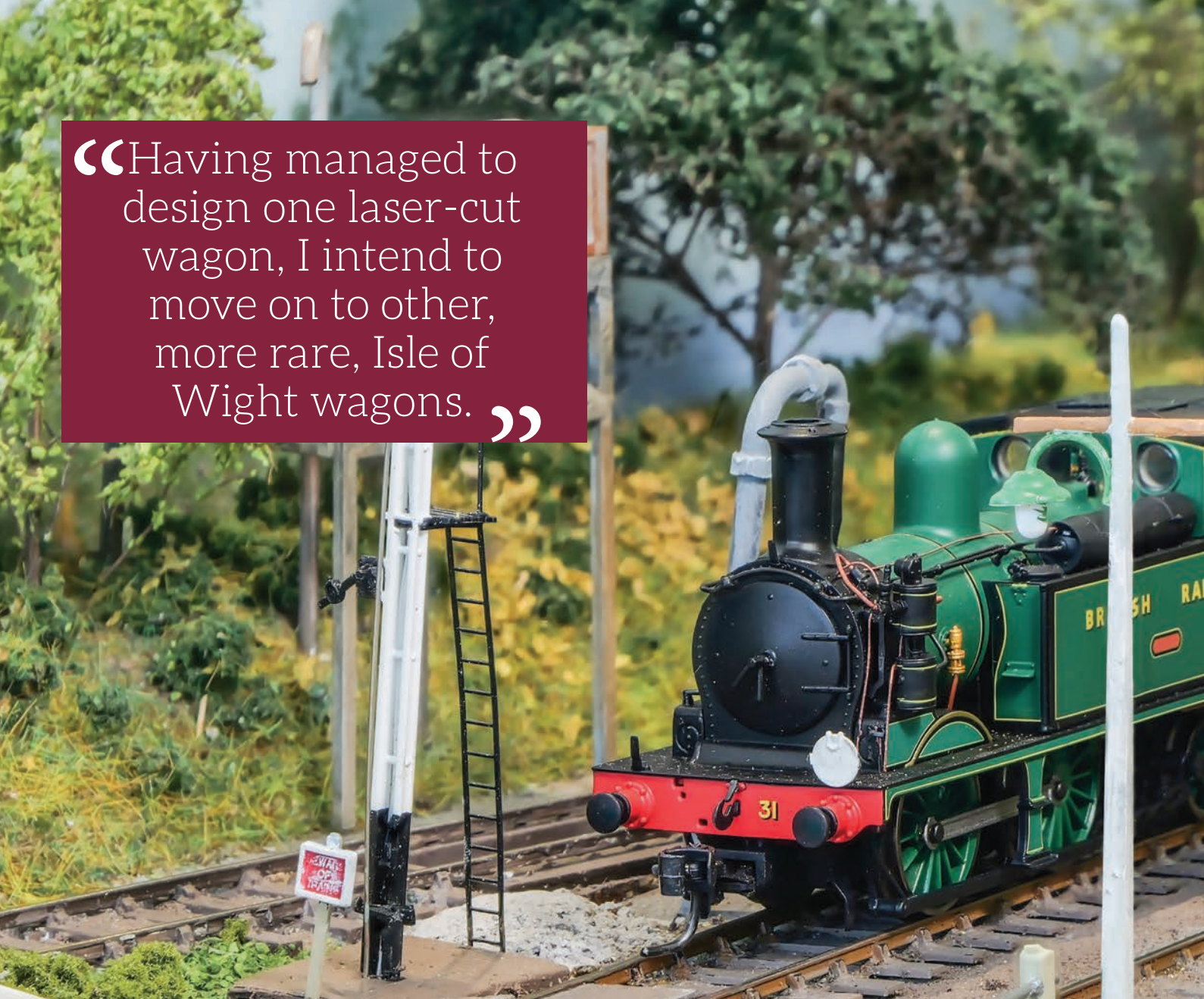
SCALE/GAUGE: 4mm:1ft scale/16.5mm gauge/OO

SIZE: 10ft x 18in scenic section

ERA/REGION: 1950s BR(S)

LAYOUT TYPE: Terminus to fiddle yard

“Having managed to design one laser-cut wagon, I intend to move on to other, more rare, Isle of Wight wagons.”



We first exhibited it at the Farnham Show in October 2017. Since then, it has grown and is now 45ft long, featuring the station at St Lawrence, built by Mark.

Compressing the prototype

I made various compromises to fit 'Ventnor West' onto the two five-foot-long baseboards that I had bought from a friend at a very reasonable price. The goods shed, for example, is in the wrong position (the baseboard being too narrow), and the overall length of the station has been reduced.

The track layout is correct for the later BR era, and the buildings (laser-cut by the amazing Julie at York Modelmaking from my CAD artwork) are pretty accurate. The layout also has my last scratch-built structure: the signal box, constructed in the time-honoured way of Plastikard using a craft knife. The house, 'Little Pax', which sits at the top of the embankment overlooking the station throat, is a 17th Century building that had been the official summer residence of the Governor of the Island. It turned out also to have been the childhood home of another local modelling friend, Godfrey Glyn.

The plain track is from C&L, and the turnouts are hand-built on Timber Tracks sleeper bases. They are driven by Cobalt slow-motion motors. Signals are by Wizard Models, Dapol and Ratio. They are driven by servos mounted in Dingo Servo Mounts and controlled by a Megapoints controller. Judging by the reaction at exhibitions, the *pièce de résistance* is the mechanically-interlocked lever frame from a Modratec kit. Modratec was a range conceived and developed in Australia by Harold Fanshawe. The kits required careful filing, fettling and drilling to produce a customised installation, using software designed by Harold. I stopped short of catch points and ground signals, and the basic interlocking that remains prevents conflicting routes from being set – a starter cannot be pulled off unless the points are set for the appropriate departure road, and so on. It is a matter of great sadness to me that this range of equipment is no longer available. A hand-built bell signal set provides communication down the line to Merstone Junction.

Hands-off coupling and uncoupling are two of my must-haves. Initially, I used Sprat & Winkle couplings, but decided to change to Kadec couplers. While these promise easy installation using NEM plug-in sockets,

ABOVE: A colourful combination of locomotive and coaches from early BR days arriving at Ventnor West. The locomotive, No. 31 *Chale* is an EFE model and the coaches are brass kits (Branchlines and Worsley Works).

RIGHT: E1 Class No. 1 *Medina* arrives with the morning freight with coal for the local merchant. The gentleman watching the train's arrival from under the canopy is the layout's time-travelling owner – one of the first subjects for Modelu's scanning service.



one needs to be wary of the fundamental differences between the US models for which they were designed and the UK models on which they are now often used. Kadees were designed primarily for bogie stock without buffers, whereas UK stock has buffers and four-wheeled freight stock. These differences affect the operating characteristics. I have found that using built-in NEM sockets featured on RTR stock has not generally resulted in exhibition-quality reliability and my best results have involved Kadee Whisker (243) couplers, from the USA range. They have to be mounted carefully and precisely, but the silky smoothness of the couplers makes all the difference. For kit-built locomotives and rolling stock, the mountings must be considered a fundamental element of the building, and I have found etched brass NEM sockets developed and supplied by MJT/Dart Castings are the 'bees' knees' for coach bogies.

Scenery incorporates a "Ha Ha" to create the illusion of a site at the top of a cliff, overlooking the sea. The large number of trees – almost 100, I think – are from sea moss. While these are cost-effective and highly realistic, they are too frail to cope with being transported to exhibitions. A programme is in hand to replace them with Woodland Scenics' trees.

Rolling stock

I have been building and accumulating IOW locomotives and stock for over 20 years. There are three Wills Finecast O2s, which are fitted with flywheels and (though I say it myself) they run beautifully, as well as an E1 0-6-0T with the same pedigree. The 'Terriers' are the recent Hornby models, and there are a number of Kernow and EFE O2s. They are light, and the running quality is variable, but the bodies are beautiful. Future plans involve re-motoring some of them with South Eastern Finecast chassis.

Passenger stock includes a number of conversions based on Ratio Midland Railway 48ft compartment stock. These are cheap, quick to build, and with some modification, are a respectable representation of LBSCR stock. I have also built a number of Roxey Mouldings brass kits, a push-pull set from Worsley Works, as well as models from Bill Bedford and Branchlines.

Freight stock reflects the predominance of open wagons on the Island, and is being added to with various kit-built items such as LBSCR vans from Linny's Lasercraft, and cast resin kits from Smallbrook Works. Having at one stage lost faith in Kernow Model Rail Centre's ambition to release a model of the SR Road Van (the most numerous type of brake van on the Island), I designed my own laser-cut kit. Of course, as soon as I had taken delivery of the kits, Kernow announced the arrival of its model. Sod's Law is alive and well in the railway modelling world! Nevertheless, having managed to design one laser-cut wagon, I intend to move on to other, more rare, Isle of Wight wagons.



A view that cannot be seen at exhibitions, from the back of the layout, showing the signal box, scratch-built from Plastikard, shortly before CAD took over.



No. 17 *Seaview* at the end of the line, about to run around its train. The locomotive is a Kernow model, lightly weathered to reflect the generally high standard of upkeep on the Island.



The house, 'Little Pax', which overlooks the station throat. This 17th-century building still stands as a private house, having been extended in recent years. Originally, it was the official summer residence of the Governor of the Island.



The branch push-pull has arrived during a pause in the shunting activity. A Southern Vectis bus is there to meet passengers. Typically, there seems to be almost no-one on the train.

Final thoughts

It would be fair to admit that we have had a few problems working out how best to operate the layout. A passenger train takes very little time to 'turn around' at Ventnor West, and so there is a constant call for trains to travel to and from Merstone Junction to maintain spectator interest. Moreover, the track layout at Merstone Junction means that only one of the roads through the station feeds the branch, whereas the through Newport – Sandown route can access both platform faces. Priority, therefore, has to be given to the 'Ventnor West' service to keep things moving, which, of course, is not authentic.

After some experimentation, 'Ventnor West' is now operated from the front. This means we can engage the audience more easily and this usually goes down well, especially with people who remember the station or the location from holidays on the Island... plus, a fair few who mistake it for the 'other' Ventnor and are fascinated to discover that there were once two stations serving the town. ■

ABOUT THE MODELLER

Name: Chris Gardener

Age: 67

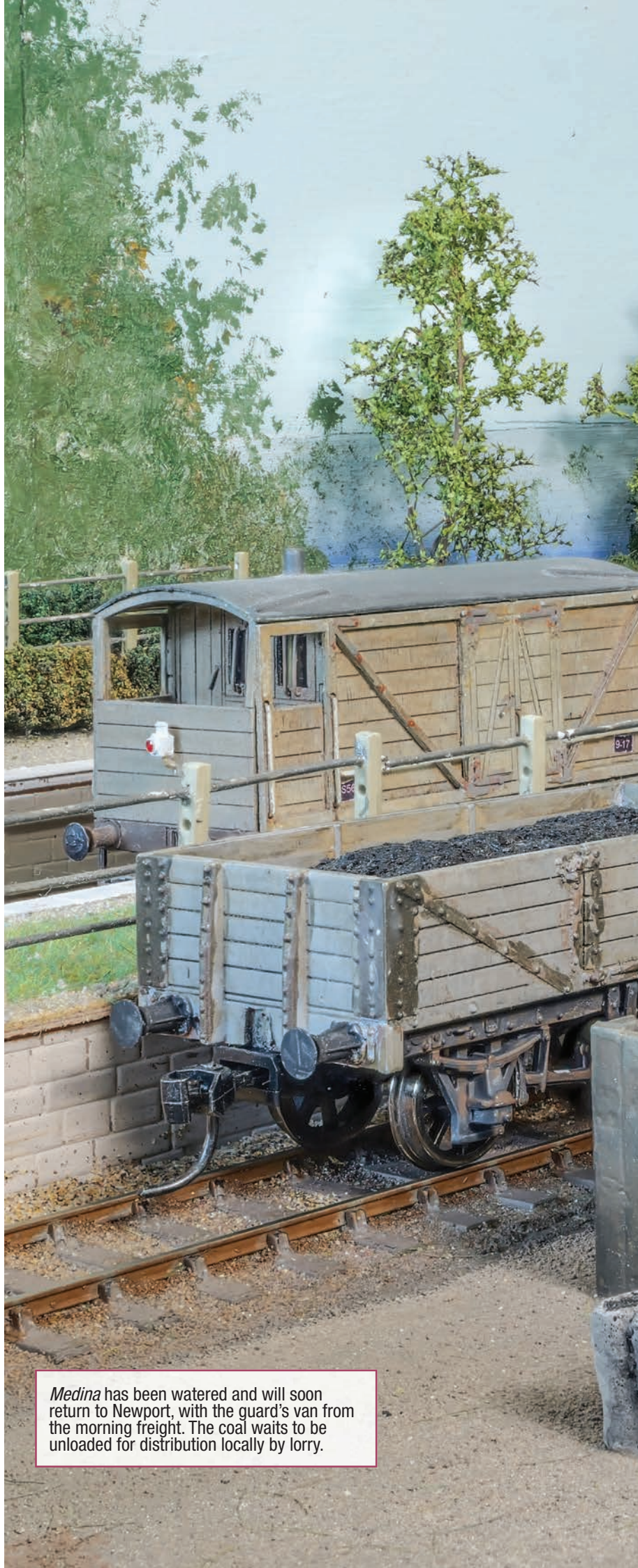
Number of years modelling: 60

Name of your first layout: *Lone Star Treble-O-Lectric* on a 4ft by 2ft board

Favourite era/region: BR and British Rail, Southern

Favourite locomotive: BR Standard Class 4 2-6-0

My modelling life goes back to the early 1960s and has involved the usual progression for the 'baby boomer' generation. I started with Hornby O gauge clockwork and progressed through Lone-Star Treble-O-Lectric to Tri-ang OO. After a diversion into N gauge, firstly US, and then UK outline, I returned to OO in the 1980s. Come the millenium, and as a member of Alton Model Railway Group, I had the courage to build an exhibition layout, a model of Cowes, on the Isle of Wight. I exhibited this a few times before a full-time job and family life took over. It was not until the children had grown up and I was able to take an early retirement that I returned to modelling with a grand plan for the loft. An ambitious project comprising Cowes, Newport and Medina Wharf evolved over many years. In time, I realised it would be more than I could handle and attention turned to building something smaller.



Medina has been watered and will soon return to Newport, with the guard's van from the morning freight. The coal waits to be unloaded for distribution locally by lorry.

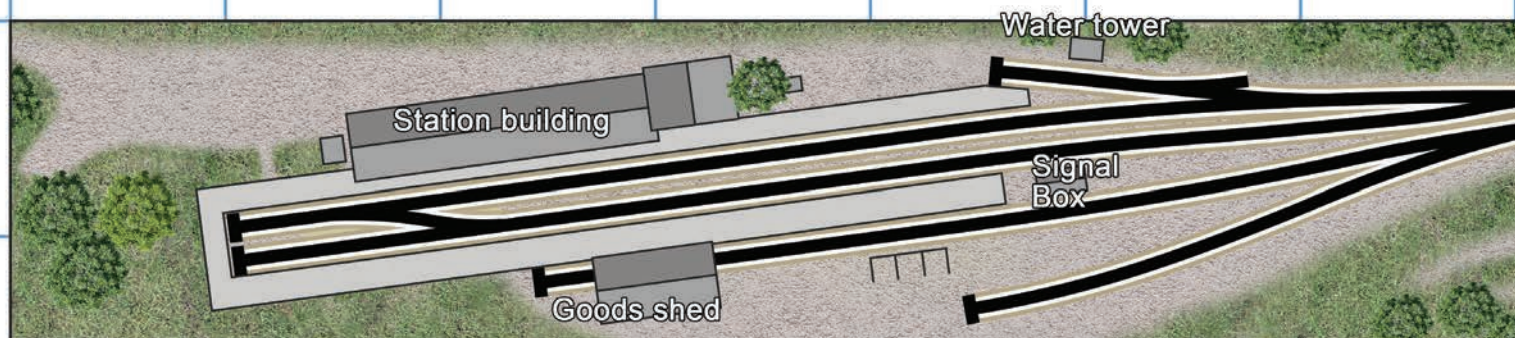


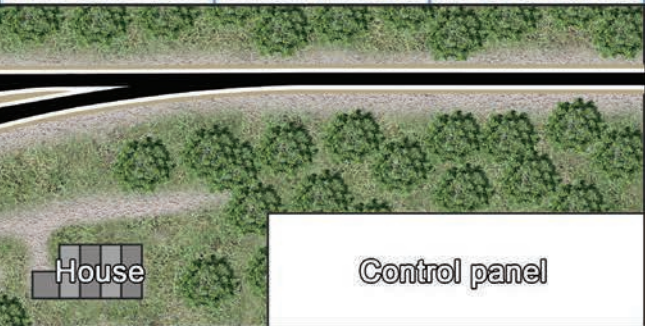
No. 31 *Chale* arrives at Ventnor West with a train of LBSCR stock. The coach is a Branchlines' kit of an LBSCR prototype. The coal merchant's lorry waits for the coal merchant to begin the delivery round.



TRACKPLAN

1 square = 1 square foot





House

Control panel

Fiddleyard

BRITISH RAILWAY MODELLING

BRM **DIGITAL**

**DIGITAL EDITION
EXCLUSIVE**

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BUILDING A CLIFF TRAMWAY

Paul King describes his second attempt at creating this cliff tramway, inspired by childhood visits to Hastings, using more modern modelling products.

Words & photography: Paul King



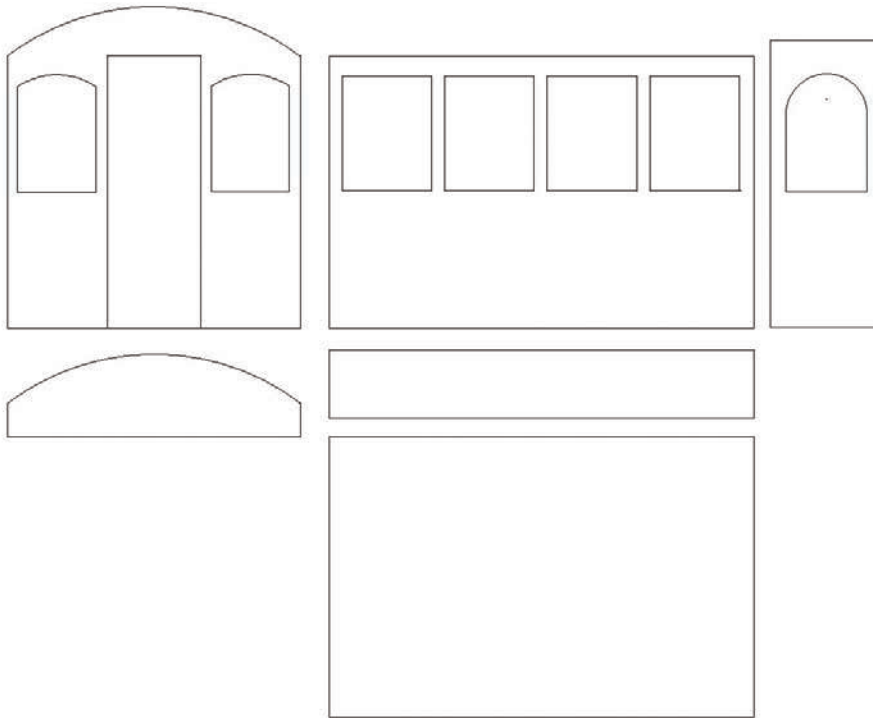
Many years ago, in the late '70s, it had always been a wish of mine to build a model railway, but when time was available, space was at a premium. I was always impressed on childhood visits to Hastings, with the two cliff tramways, particularly the East Lift. I thought this would be a way of building a model railway in the minimum of space.

Using a 15cm wide base, I made a 4mm:1ft scale cliff tramway inspired by the Lifts at Hastings. I decided to make the stations in a style in keeping with the more commercial seaside resorts resembling light-coloured stucco, rather tired-looking, even dreary, in appearance.

Unfortunately, this model had to be disposed of several years ago due to a house move and lack of storage space. During the recent lockdown, I thought I would make another Cliff Tramway at minimal cost using more up-to-date techniques. I would try to copy as near as possible the original, as I was happy with the results, but make improvements. I decided to make this one free-standing, as the previous one was mounted on a wall with lift-off brackets. The base was constructed from 6mm MDF with a fold-out support leg set to give an angle of 45 degrees. The total running length was made to a convenient length of Peco 91mm (1 yard) OO gauge flexitrack, giving a scale run of approximately 225ft.

Having access to a laser-cutter in my job, I decided to use it to produce the buildings in 2mm MDF, but these could well be constructed from scratch or from modified kits in card or embossed plastic.

The lower building was made with a through walk and two ticket offices. The top building was made with a 'motor house' in the middle with corridor walk



rounds. Many cliff tramways are worked by using a water balance system where the cars have a tank under the saloon, the top car is filled with water, which propelled the car down by gravity, lifting the lighter car to the top. The west hill lift at Hastings was originally powered by a gas engine and later converted to electric power.

The buildings were drawn in a computer design package and laser-cut. The roofs were etched in 0.9mm laser board. These were assembled with wood adhesive and sanded with fine wet and dry paper. They were then given two coats of acrylic white paint, lightly rubbing down between coats. For the windows, I experimented with a technique demonstrated by Phil Parker in his BRM 'Taxi Rest Hut' construction video (BRM DVD Spring 2021). Scribing the glazing bars into clear packaging material with an Olfa cutter and rubbing paint into the etched lines. While this gave an excellent result, I turned again to laser-cutting the frames in 0.9mm laser board, which can be cut thin and maintain strength, mounting clear plastic behind.

The buildings were weathered by adding cracks to the rendering using a sharp pencil and then a wash of thin black acrylic paint brushed on. This gives a realistic 'seaside' finish. The roof sign was made again in 0.9mm laser board, but as an alternative, a similar result could be obtained using Slater's plastic letters. Cast whitmetal seagulls were then added to the ridge tiles using Superglue.

Both buildings were to be mounted on their own removable base cut from 2mm MDF. The positions of the buildings were marked on the bases, then both painted. To facilitate the traction cables, the top building was mounted on a raised base made from 6mm square softwood, leaving gaps for the cables to pass through. The same wood was used to make location strips under the building base. A similar arrangement was used for the lower building. The bases were again laser-etched and painted. The fencing was laser-cut, again in 0.9mm laser board, and assembled using wood adhesive.

Moving on to the mechanics, the drive for cars was made from a sub-miniature geared 12V motor, which gives a realistic speed. A 40mm plastic pulley wheel was attached to the motor drive spindle. I decided to mount the entire motor assembly on a sliding carriage to enable adjustment of the cable length as, with past experience, getting the cable exactly

the correct length is almost impossible.

To give the appearance of the track being laid on a bed carved out of the rocks, I previously used cork bark. This time, I decided to use blue Styrofoam as I could tailor-make the rocks to fit. A strip of MDF was attached along the edge of the 'baseboard' and the Styrofoam was cut into strips and glued together with PVA. These were left to thoroughly set, and loosely placed in position to ensure they were straight. The strips of foam were removed from the base and carefully carved to shape with a round surform file – a job best done outside, as the small chips of foam go everywhere and stick to everything due to the static charge!

Once both sides were complete, the foam was given a coat of thinned-down PVA to seal the surface. While these were drying, the 'track bed' was painted to represent a weathered concrete appearance. A light grey matt emulsion was used and dry-brushed with vertical streaks using darker matt grey acrylic. The foam strips were then fixed in place on the base with PVA and a coat of modelling filler was smoothed into the foam creating the rock texture. I found it was easier to do this by hand, with a rubber glove, having more control of the surface. When hard, the strips were painted with matt white emulsion paint.

As the model was sited somewhere on the south coast, I dry-brushed matt mid-grey acrylic onto the surface, leaving plenty of white showing, to give the appearance of chalk cliffs common in that area. The upper surfaces of the 'rocks' were then coated with thinned PVA, and static grass applied. A few grass tufts were then added to some of the top surfaces.

A small groove was made with a small round file on the end of each rail to accommodate a cable guide 'roller' made from a piece of 1mm dia. brass wire, cut to the rail width and glued in place on the end of the rails. The track lengths were then glued into place on the base with track centres of 40mm to ensure the drive cable was central to each track.

The cars were made as two parts – saloon and running unit. The saloons for each car were scratch-built from Plastikard and populated before the roof was added, with passengers observing the view.

The running units were made by cutting an old coach bogie in half leaving two axle units. Quite a lot of work with a small file was involved, carefully removing parts of the bogie units to ensure nothing rubbed on the track. For





strength, the saloon mounting plate was cut from 2mm MDF, as were the lower wheel mounting struts; the upper wheel unit being mounted direct onto the underside of the floor. Pin-point bearings were fitted to the axle boxes and metal wheels fitted to give some weight and ensure smooth running. Fitting the lower wheel unit was quite fiddly to ensure the angle of 45 degrees was accurate – any discrepancy would be obvious between cars and on the layout. A small hook, made from thin brass wire, was made and attached to the top end of the chassis to attach the traction cable. The saloons were then mounted on the chassis units.

The length of cable run was carefully measured and a length of very thin twine was cut, a small loop being tied at one end. With the sliding motor unit placed in its mid position, the cable threaded and marked for accurate length. Another loop was tied at the other end. With the cars, one at each end of the run, the motor unit position was adjusted to give correct alignment of the cars.

The final details were added to the upper and lower buildings by adding undergrowth from rubberised horse hair covered with static grass. I decided to make the lower station the mid-point of a small section of the seafront. A tea garden was constructed using similar scenic techniques as before for the grass bank using Styrofoam. The tea hut and tables are laser-cut, but similar items are readily available from several companies.

The pole fence was laser-cut in 2mm MDF with thin wire threaded through before sticking down. Some station benches were used as 'park benches' – very typical of a British seafront. Cups and chip paper, complete with seagull, were added to the tables. Period signs were added and the triangular bins scratch-built.

To the left side of the station, I decided to have a line of beach huts, again laser-cut, but these can also be obtained in kit form. These were stuck to a 'concrete' plinth, with a rough lawn to the front.

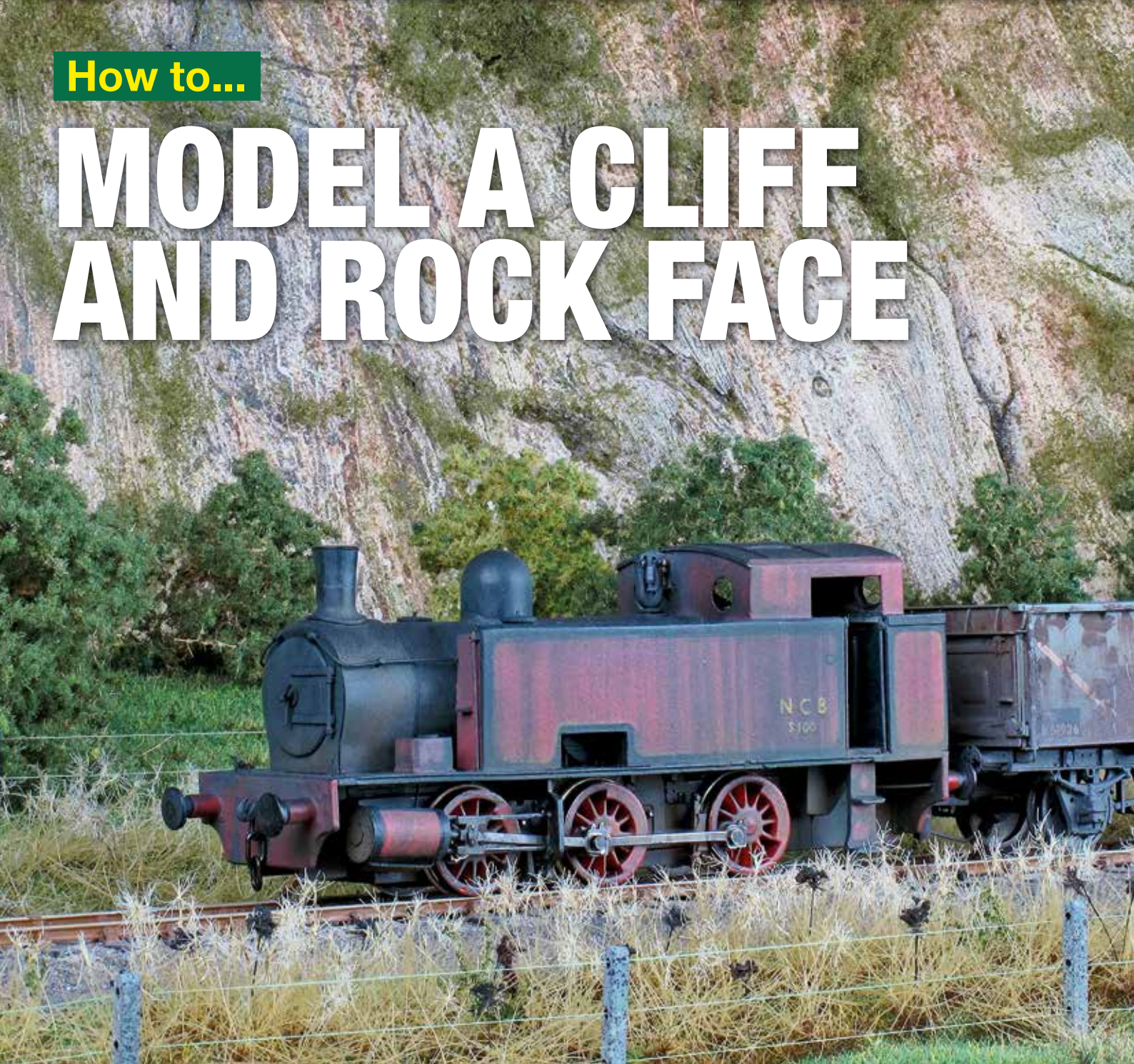
I opted for manual control of the cars using a simple homemade controller, but automatic control could be added using a timer circuit and two reed relays with associated magnets, such as the Gaugemaster Super Shuttle unit.

To finally complete, I need to source holidaymakers to populate the tea garden and seafront.

Want to share your latest modelling project with us? Get in touch by sending a few paragraphs and a couple of pictures to brm@warnersgroup.co.uk.

How to...

MODEL A CLIFF AND ROCK FACE



Observation is everything, says Michael Russell as he shares his techniques to create a low-cost detailed rock face.

Rock faces and cliffs are distinguishing features of many locations.

They're prevalent in nature, and are a common feature resulting from the construction of railway lines where rock is exposed when making cuttings and building tunnels. It should be part of your study of the topography of an area you're modelling.

The rock face I've modelled here is generic and demonstrates the techniques I use, but you should research the type of

rock you want to model and take reference photographs of the colouration and structure for later use. It's not particularly difficult to make and you can create dramatic scenery if you want.

Make it look like the railway is passing through your terrain – it's too easy to make it look like a flat railway with piles of rock either side. Many North American layouts feature dramatic mountainous scenery and are worth investigating. ■

Shopping list

- Extruded insulation foam
- Poly Fibre
- Acrylic paints
- Wall filler
- Ground foam
- Wood saw
- Rasp
- Files
- Scribe
- Wire brush
- Drill and drill bits
- 1" paint brush
- Spatula
- Mixing bowl



1» Peel and discard the aluminium foil covering the foam insulation sheets, then cut it to shape with a wood saw.



2» Study photographs of real locations you want to base your model on, then distress the foam with rasps, files, wire brushes or drills to get different shapes.



5» Just like when baking, you'll find the mixture produces peaks, so once dry, sand them down. Add final detailing here - fine strata can be produced using a wire brush and scribers.



8» Dark-coloured pastels can be worked into the crevices to emphasise these more. You need to be subtle because it's easy to overdo. Ensure the base coat is dry first.

“creating a good-looking rock face takes careful research and planning”



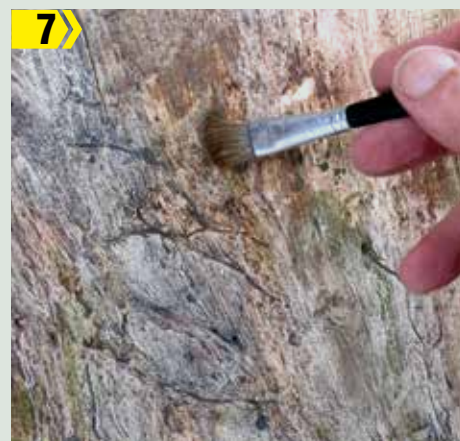
3» Mix wall filler to the consistency of double cream using a spatula. When mixing, add water a little at a time and mix before adding more.



6» Tilt the work at about a 45 degree angle. Start by adding an overall base colour wash over the cliff/rock face. I used acrylics and emulsion paints.



4» Brush the filler onto the foam. It's better to add several thin layers to aid drying and is a necessity if you're building to a depth greater than 10mm. This is a good time to blend joints between pieces of foam.



7» Whilst still wet, add light brown, dark brown, green and grey washes. Use plenty of water - you can always add more layers of wash, but you can't take it off as easily. Start at the top and let the colour work its way down. I retain the base colour in the palette and add other colours to blend better.



9» Choose a light colour for the highlights and drybrush this onto the rocks. Add paint to the brush, clean it on a rag, then drag the brush lightly over the surface so that only the raised surfaces receive paint.

10»



To simulate lichen and moss, take some off white and yellow-brown paint and flick it at the surface using an old toothbrush. Don't overdo this effect.

11»



Tease out pieces of PolyFibre and lay out on newspaper, then coat with hairspray.

13»



Stick the vegetation so that it hangs down the face in a random fashion. I used superglue and an activator for quick results. I used superglue and an activator for quick results. Glue one or two spots at one end, then stretch the PolyFibre until you have thin coverage, then glue that side down.

ABOUT THE AUTHOR



I've been modelling since my childhood and love steam power. A recent visit to the Foxfield Railway has given me an interest in industrial subjects. I like to discover fast techniques that produce realistic results – we all live busy lives!

12»



Transfer the piece to another sheet of newspaper and sprinkle on ground foam – I used Hornby R8874. Place in a safe location to dry.

Michael's top tips

- This job is messy, especially if you're cutting lots of foam. You may want to use a dedicated space that's easy to clean afterwards. A vacuum cleaner is essential for cleaning up.
- Insulation board is obtainable from your local wood yard or builder's merchant. Ask your supplier to cut it into smaller, more manageable pieces.
- Join foam sections together with a hot glue gun or specialised foam glue.
- Wear a dust mask when working with any material that produces dust.
- The finer the detail you add to a rock surface, and the lighter the colours you paint it, the more distant it'll look in a scene. Add less surface detail to rock faces close to the viewer.
- When applying the powder filler, you may find it easier if you cut down the bristles of your brush.

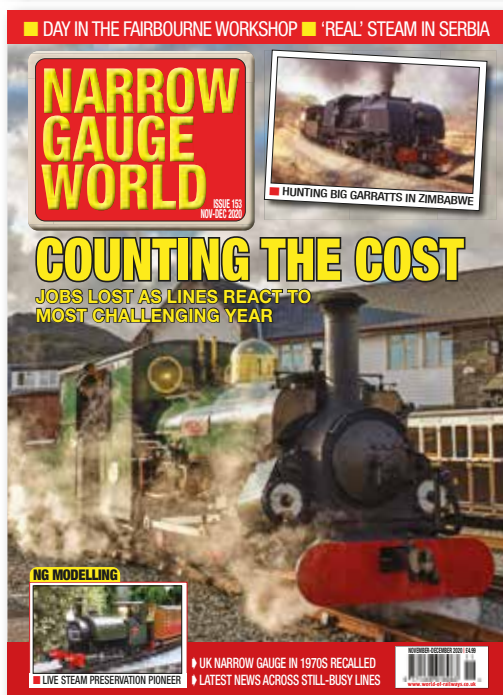
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FACTFILE

SCALE/GAUGE: 7mm:1ft scale/32mm gauge/O

SIZE: 9ft 2in x 1ft 4in

ERA/REGION: Late Victorian/early Edwardian era, Island of Sodor

LAYOUT TYPE: Fiddleyard to terminus

ELSBRIDGE

In the arms of a welcoming modelling group, and soon finding himself as its annual exhibition organiser, Neil Blair made this no-nonsense tram-way layout.

Words: Neil Blair **Photography:** Andy York



No.1 *Godred* arrives with a short goods only tram. The wagons in the train are a mix of scratch-built and kit-built. I'm not sure what the Ulfstead Mining Company open at the front of the train is doing this far up the tramway, perhaps it's collecting supplies for the mining company at Toryreck.



I've long had an interest in the railways of the Island of Sodor, thanks to my mum who got me into the books and models as a child. In recent years, this has focused particularly on the railways that were around before and ultimately grouped into the North Western Railway (NWR). The layout is my take on them and is based on a comment from the *Thomas The Tank Engine Man* biography of the Rev Awdry, where he talks about the Ffarquhar branch (back before it reached Ffarquhar when it was the Knapford and Elsbridge Railway) being built as a roadside tramway. This piqued my curiosity as I've long had an interest in roadside tramways, so I set out on a hypothetical what-if journey and 'Elsbridge' is the result.

Although I've been modelling in 7mm:1ft scale standard gauge for over 15 years, this is the first layout in that scale that I've completed. I've drawn on inspiration from both UK and Continental steam tramways such as Wantage, Wisbech and Upwell, Selsey, and the Sprimont to Poulseur tramway in Belgium. The last-mentioned was operated by six-coupled tram engines hauling stone traffic along with a four-wheeled tramcar – sound familiar?! These all helped to shape 'Elsbridge', as a steam tramway terminus tucked away on a cramped location near the centre of the town.

The early days

'Elsbridge' has been through many design

permutations since I first started planning for it several years ago. The current layout plan arose after I scrapped the previous attempt as I wanted something compact, where the whole station was on show and enabled most movements to be kept on scene too, without having to resort to running round and shunting using the fiddle yard. It's the classic loop and two sidings layout plan, but the headshunt also doubles as the platform road and is long enough to leave a tramcar at the end and still be able to run round.

The original intention had been to convert the end of our extended garage to house the layout, which would have given me a space of roughly 10ft by 12ft. Various factors meant that this didn't happen, and I'm glad it didn't, as I think I would have struggled to build a layout that size single-handedly. 'Elsbridge' lives in our spare bedroom, and I am fortunate enough to have a supportive husband who was happy for me to have the layout in the house. Space in the spare bedroom was always limited, but as the layout had to fit into the bedroom and only take up one wall, it had to be compact from the outset.

Scenic challenges

The rails in a sea-of-grass effect was something I was particularly keen to try to include on the layout if possible, so I'm quite proud of how well it worked. Having never done this before, it was a case of trying out what I wanted to do on a small section of track to

ABOVE: No. 3 *Gerda* trundles into the platform with an up mixed tram. Most of the structures on show here have been scratch-built from plastic sheet and strip, with embossed plastic sheet for the outer layers. I'm quite proud of the train shed, as I've wanted to model one for years. I really do need to finish off the lining on this side of No. 3...

RIGHT: No. 3 is in the process of running round its train, having just blocked the road to do so. Luckily the omnibus is owned by the tramway company, so there's no rivalry here to worry about between road and rail. Just visible is the water crane, an adapted Hornby Dublo 4mm:1ft scale model.



see which materials worked best for the look I wanted.

The trackwork (Peco Code 124 set track points and flexible track) was buried in DAS clay, which was then painted. I then covered the trackwork with Woodland Scenics coarse turf, using a blend of light and mid-greens as I wanted the effect of rails running in a sea-of-grass. The trees on the layout are from Primo Models, as I've found them to be both reasonably priced and realistic looking. Grass tufts and bushes/flowers are a mixture from Primo Models and Wargamers Scenics (on eBay – they are a wargaming specialist, but many of its scenic products are quite suitable for model railways, plus great value).

Painting figures and the backscene have been interesting to say the least! The backscene took a few attempts to get right – the sky on the first attempt was likened to something apocalyptic by friends. Thankfully, I found a useful article on World of Railways on how to paint a backscene, which was a great help. Figure painting has been a steep learning curve, as I'd never painted any before. The first few were a complete disaster and will never see the light of day again. I have found that painting them black as a base coat, and then using a limited range of muted

colours, has helped to tie them together and helped them to blend into the layout without standing out too much.

Buildings are a mix of kit (Port Wynnstay, 422 Modelmaking and Fair Price Models) or scratch-built, using a Plastikard core with embossed sheeting over the top. The kit-built buildings have all received varying degrees of modification, mainly to bring them into a house style for the tramway company so that it was obvious which buildings belonged to them. To help with this, the tramway buildings all have a common colour scheme, and most of them are of timber construction, except for the water tower and the stables, both of which are stone. One item of note is the water crane, which is an old Hornby Dublo 4mm:1ft scale model, the only alteration to make it suitable for 7mm:1ft scale was to provide a taller operating wheel.

Playing trains

The layout gets operated as and when I feel like it. Sometimes, I'll just have a quick operating session and run one to two trains depending on how much time I've got. At other times, I can happily spend a few

RIGHT: A view down the back siding during shunting when it's been emptied of wagons. At the far end of the site can be seen the tramway company stables, built in the early days of the company when they used hay motor power for working the tramway. These days, they're used mainly as stores, and for housing the few remaining horses used for deliveries around Elsbridge.

ROLLING STOCK

The majority of locomotives and rolling stock are scratch-built. The tram locomotives, of which there are four, all run on modified Bachmann On30 tramcar chassis, which are quickly and easily converted to 7mm standard gauge. The bodywork for them has been scratch-built from plastic sheet, strip and tube, and they've been fitted with custom DCC sound, courtesy of Paul Martin at EDM Models. There are a few ready-to-run locomotives (Dapol Terrier and Minerva Manning Wardle), but these have been repainted and modified as necessary. These represent locomotives that are occasionally borrowed from the neighbouring Wellsworth & Suddery Railway to cover for locomotive shortages on the tramway.

The tramcars, of which there are three, utilise the saloon body parts from Testors kits for the San Francisco Cable Cars, suitably modified, with the rest from plastic. Three kits give me enough parts to produce two tramcars, with six-windows a side.

Wagons owned by the tramway company have been scratch-built as well, with just buffers, wheels and running gear being commercial items. Wagons from the neighbouring WSR are a mixture of modified Slater's kits and 422 Modelmaking parts.



ABOVE: From right to left, we can see Nos. 1 to 4, showing the evolution of the design. The coffee pots Nos. 1 and 2 (*Godred* and *Sigrid*) operated the tramway for the first few years. After traffic increased, No. 3 *Gerda*, a slightly larger horizontal boiler locomotive, was designed. The ultimate design stage is No. 4 *St. Machan*, a chunky six-coupled locomotive designed for handling the heaviest services on the tramway.



WILLS
GOLD FLAKE
CIGARETTES

hours operating it and running sets of stock on it.

At the Trent Valley Group's Modelling Day in January, the layout proved that it can be operated for a full day without the operator losing interest. This is good because it's making its official exhibition debut at the 7mmNGA Exhibition on June 17, 2023, at Burton on Trent Town Hall.

Operation is via DCC using an NCE Powercab controller. Wiring is not my strong point, so it's been done as simply as possible. Amazingly, it all worked first time, which was a bonus. Point control is undertaken manually using the wire-in-tube method, with slide switches being used to change direction and polarity in one. These are all hidden along the front of the layout in the scenery. Couplings-wise, I've stuck with three-links, as I don't like the look of auto-couplers, and use of them would have required major modifications to the tram locomotives to enable them to fit. I was fortunate that my friend made me a useful uncoupling tool, which is a pen-torch with a small magnet mounted on the end of a length of brass wire.

Currently, there are three sets of stock that run and turnabout on the layout. These are limited by the number of cassettes available for the fiddle yard. First is the passenger tram, which is a pair of tramcars (All Third and First/Third Composite) and the luggage van. These arrive, are run round, and depart again. Even this simple manoeuvre can take several minutes because trains have to set back to use the run round loop, and the tram locomotives must take water on each journey owing to their limited water capacity. The mixed set consists of an All-Third tramcar and a pair of wagons, which are swapped with a pair already on the layout. Lastly, is a goods-only set of three wagons. Again, these are swapped with wagons already on the layout. Shunting and reforming of mixed and goods trains can sometimes take a good 20 minutes or so at least, depending on which location wagons are being shunted to/from. Train lengths have been deliberately kept short to maximise the length of layout that they can traverse (working from Gordon Gravett's principle that a train should traverse three times its own length on scene). Eventually, when I've built more stock, there'll be a couple more sets available to increase the operation.

Final thoughts

The main thing to do next is finish preparing the layout for its first official outing (the 7mmNGA Exhibition on June 17, 2023 at Burton on Trent Town Hall). For this, I want to finish building a few more tramway wagons and an additional coach, which will cover one of the pair of second-hand three-compartment All-Thirds acquired by the tramway in the early-1900s. After that, I want to build up more stock to enable me to run the layout in different time periods. I've made a start on a few wagons and a pair of tramcars for the later North Western period of ownership, covering up to the late-1920s, and I'm just contemplating now which of the tram locomotives will have an additional body built to swap the chassis over so that they can run in NWR livery.

If I was to pass anything on to fellow modellers, it



No. 3 enters the station site. At this point, the tramway had been running down the middle of the street for some distance to reach the station's location near the centre of the town.

INSET: A close-up shot of the Farmers' Cooperative Society loading bank at the front left of the layout. I was originally going to have a mill here, but after mocking it up decided that it dominated this end of the layout too much. The loading dock and cooperative lockup are simple scratch-builds from plastic sheet. All of the items located on it are removable so that I can swap things around. This is done by drilling a hole on the base of them and inserting a length of 1mm brass wire. This is then plugged into a corresponding hole on the platform, or in the goods shed.



would be that if it doesn't work, or you find you've lost interest, don't be afraid to start again. I'm glad I started again, even if I had a brief moment of panic once I'd taken the track up!

This layout has proved more enjoyable to operate and has kept my interest going for a long time. If I was to start again from scratch, I'd probably use 600mm wide boards, to give myself more room. I'd have liked to have squeezed an extra siding in, but on the current 400mm wide boards, it was just too cramped. The extra depth would also allow for a few more houses along the rear to help represent the station's cramped town centre location.

At some point, I'd like to build an additional module to cover the section of line where it ran down the middle of Elsbridge High Street to enable longer run and a chance to model more buildings.



ABOVE: No. 2 *Sigrd* departs with much whistle-blowing and bell ringing to warn road users of its impending arrival in their midst. The luggage van is loosely based on that built by the Great Eastern Railway for the Wisbech & Upwell Tramway, but with panelled sides to match the design of the tramcars.

ABOUT THE MODELLER

Name: Neil Blair

Age: 35

Number of years modelling: 25+

Name of your first layout: I don't think any of them were named really, as they never got far enough in the build stage.

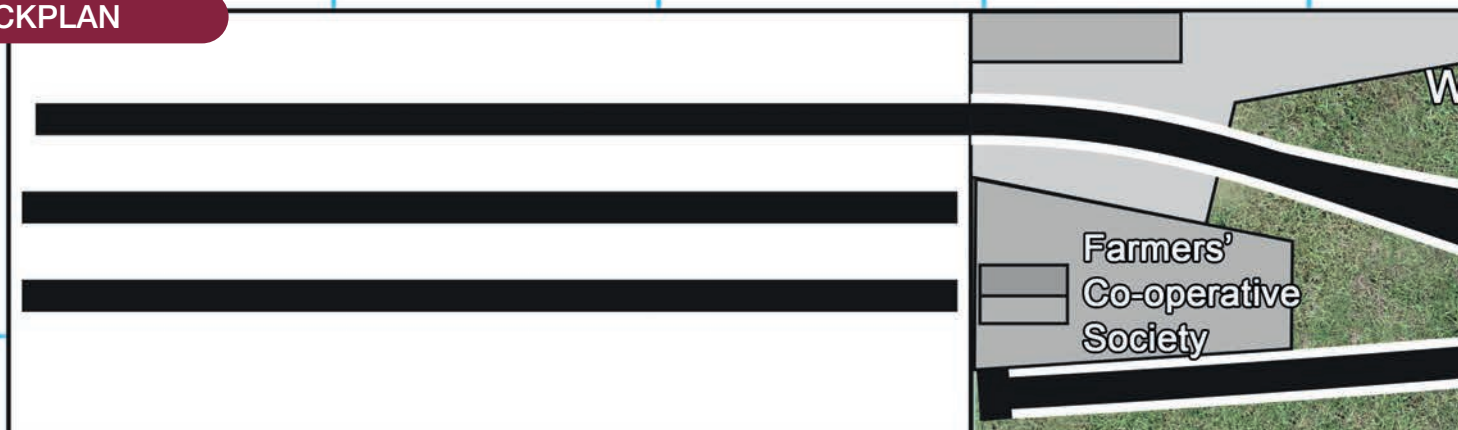
Favourite era/region: Pre-grouping Cumbria.

Favourite locomotive: Furness Railway No. 3 *Old Coppernob*

I've had model railways since I was young, although the last one that got finished was my large 4mm:1ft scale layout at my mum's before I left home to go to university in Derby. Since then, there has been a succession of part-built layouts and half-started ideas that didn't really get anywhere until about eight years ago when I changed departments and work, and through one of my new colleagues, became involved with the Trent Valley Area Group of the 7mm Narrow Gauge Association. I've made some great friends through that and became involved in the building of the group's layout, and with another member of the group, began to build a joint 7mm:1ft scale standard gauge light railway set in the Lake District. Shortly after joining the group, I had my arm twisted to become the Exhibition organiser for the 7mmNGAs annual Exhibition and AGM at Burton on Trent, and did that for a few years. I'm looking forward to taking 'Elsbridge' there this year as an exhibitor!



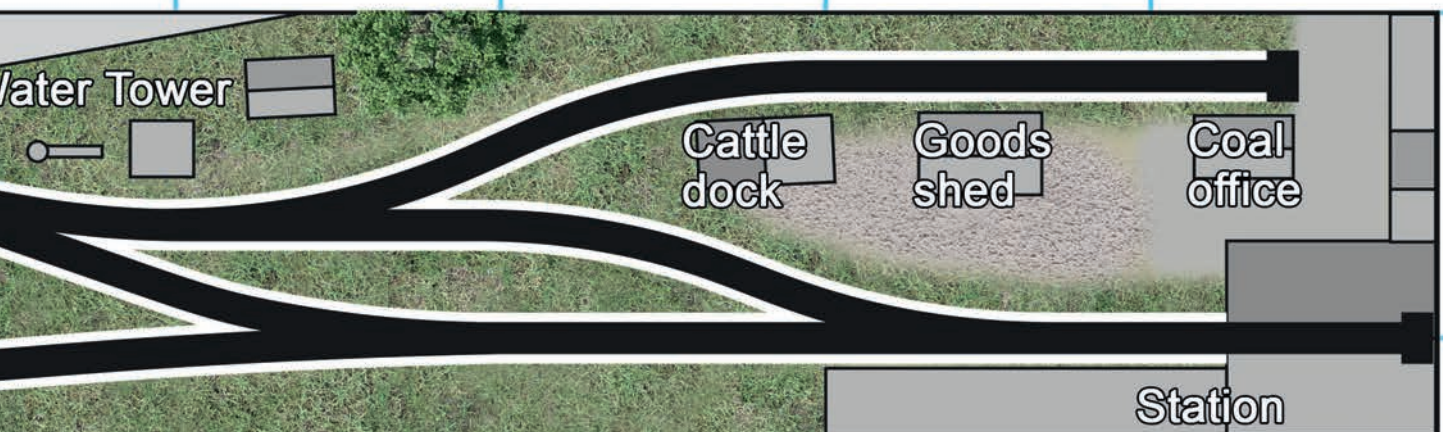
TRACKPLAN



1 square = 1 foot



ABOVE: Later in the day, No. 2 *Sigrid* has returned with the full passenger set and shunted its tramcars back under the overall roof to allow passengers to embark. Just visible in front of the milk churns is the small hole in the platform surface to allow the point to be changed – buried under the platform is a slide switch, which is changed using a short length of bar.



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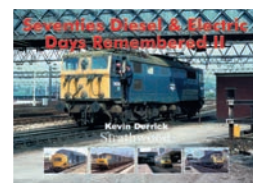
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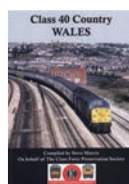
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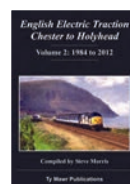
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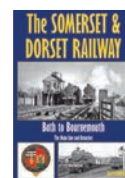
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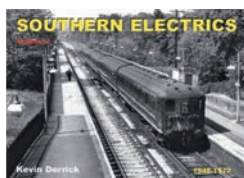
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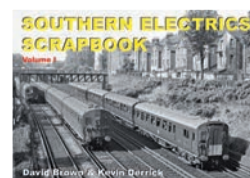
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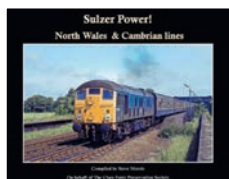
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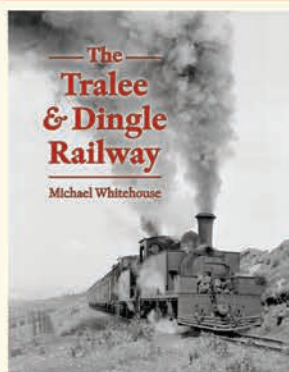
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NEW AND FORTHCOMING FROM LIGHTMOOR PRESS



The Tralee & Dingle Railway Michael Whitehouse

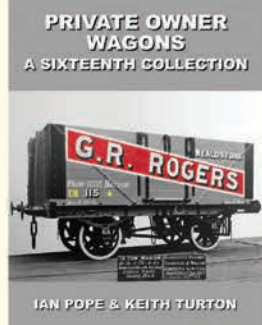
The Tralee & Dingle Railway still stirs the hearts and imagination. Many ferroequinologists*, including the author's father, flocked to the railway, but most only just in time. They saw the last rites of the monthly cattle train serving the Dingle fair in the early 1950s and experienced the thrill of a lifetime riding the narrow gauge train and even the footplates of the steam locomotives as they rushed the steep gradients and flew down the other side, brushing red fuchsias into disarray. All spiced by the general hazards of near-derelect rolling stock running over grass-covered and barely maintained track, often wet from Irish rain and mountain mists and over some of the most frightful curves and gradients ever engineered on a narrow gauge railway. The Dingle peninsula has some of the best scenery in Ireland to travel through which is a draw all of its own. Fact and folklore generated by this three-foot narrow gauge railway was all quite remarkable, as was its involvement in the developing Irish political environment. In this heavily illustrated book, Michael Whitehouse revisits the story of this remarkable railway using his father's photographs and research notes, together with considerably expanded text to place the railway in its political, economic and social context. (*A person who studies trains as a hobby!)

352 pages, 215 x 275mm, b&w and colour, gloss art, laminated board covers.
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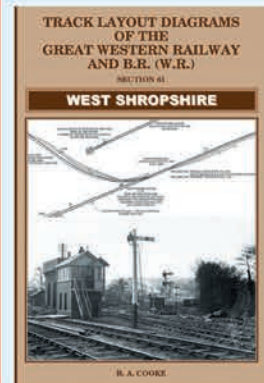
PRIVATE OWNER WAGONS A SIXTEENTH COLLECTION IAN POPE & KEITH TURTON

Once again the author's have joined together to create a volume covering a widely diverse selection of Private Owner Wagons. Coverage is across England and Wales and from beer to salt via coal. Each wagon operator is written up as fully as possible and illustrated with all known views of their wagons. The businesses range from the highly successful to the very short-lived, from large companies to the 'one man band'. Once again the volume gives an insight into the period when 'king coal' reigned. It was collieries, coal merchants and coal factors that made up the largest section of wagon owners. It adds yet more detail to the social history of a group of tradespeople, often overlooked, who played a huge part in the fabric of society with many coal merchant's having a big part to play in local politics and some rising to quite high office as documented in other volumes in this series.

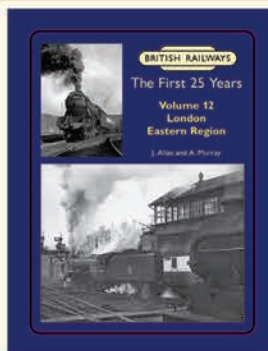
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British Railways The First 25 Years – J. Allan & A. Murray Volume 12: London Eastern Region

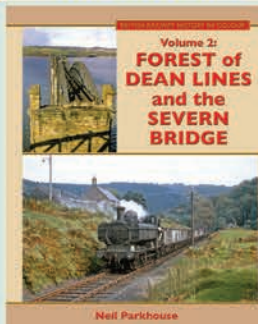
This volume covers the Eastern Region lines from the terminus stations at King's Cross, Liverpool Street and Marylebone out along the main lines as far as New Barnet, Ilford and Neasden respectively, the Metropolitan Widened Lines between King's Cross and Moorgate and the North London Line from Hackney to Canonbury. It also includes the former Great Eastern Railway branches to North Woolwich, Chingford, Enfield Town, Ongar and Palace Gates, and the ex-Great Northern Railway branch to Alexandra Palace. We visit the three principal motive power depots serving the former Great Northern Railway lines, King's Cross 'Top Shed', Hornsey and Finsbury Park, the massive complex that was the Great Eastern's Stratford Shed and Works, and the Great Central shed at Neasden. We also take in the freight traffic around the marshalling yards of Ferme Park and Temple Mills. All three termini saw extensive commuter traffic with 0-6-2Ts of different designs employed on the services from King's Cross and Liverpool. The Great Eastern 'Jazz' was the most intensive steam-worked suburban operation in the world. The Epping to Ongar line saw ancient 2-4-2Ts operating a push-pull service until it was electrified in 1957. Electrification came to the suburban lines from Liverpool Street in several stages, beginning in 1949 and completed in 1960 when the 'Jazz' was replaced by electric units. A mix of DMUs and locomotive-hauled trains took over at King's Cross. Dieselisation of main line services on both the Great Eastern and Great Northern lines began in 1958 and was complete by the end of 1963, whereas those on the Great Central ended in 1966 with run-down ex LM&SR 4-6-0s. King's Cross saw four different Type '2' diesel classes before standardisation on the Brush design. By the late 1960s the ubiquitous Brush Type '4' had become the core express motive power, supported on the Great Eastern by the English Electric Type '3' and in second place behind the small but iconic 'Deltic' fleet on the East Coast main line.

208 pages, 215 x 275mm, gloss art, laminated board covers. ISBN: 9781915069 19 1. Price £27.50 + £4 p&p

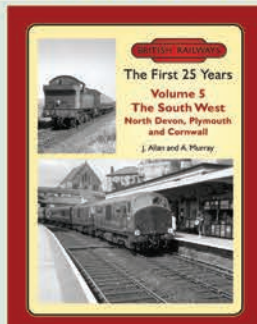


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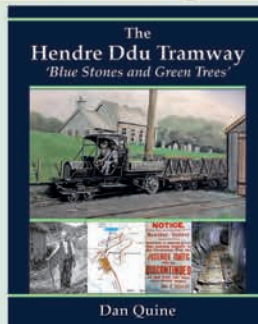
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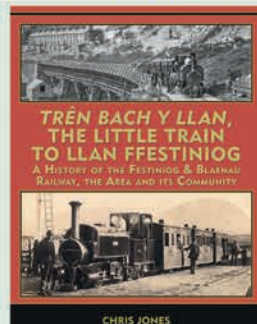
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Every passing month sees many model announcement surprises, but the biggest of the month came as Ellis Clark Trains entered the OO gauge market under its Railworks brand.

Retailer, Hattons is to see a few of its

4mm:1ft scale model exclusives return for second production runs with new liveries, following renewed market confidence. And, it has plans afoot for a new second-hand retail platform – details are yet to be revealed, but what we know can be read in our interview with Hattons' Dave Mylett.

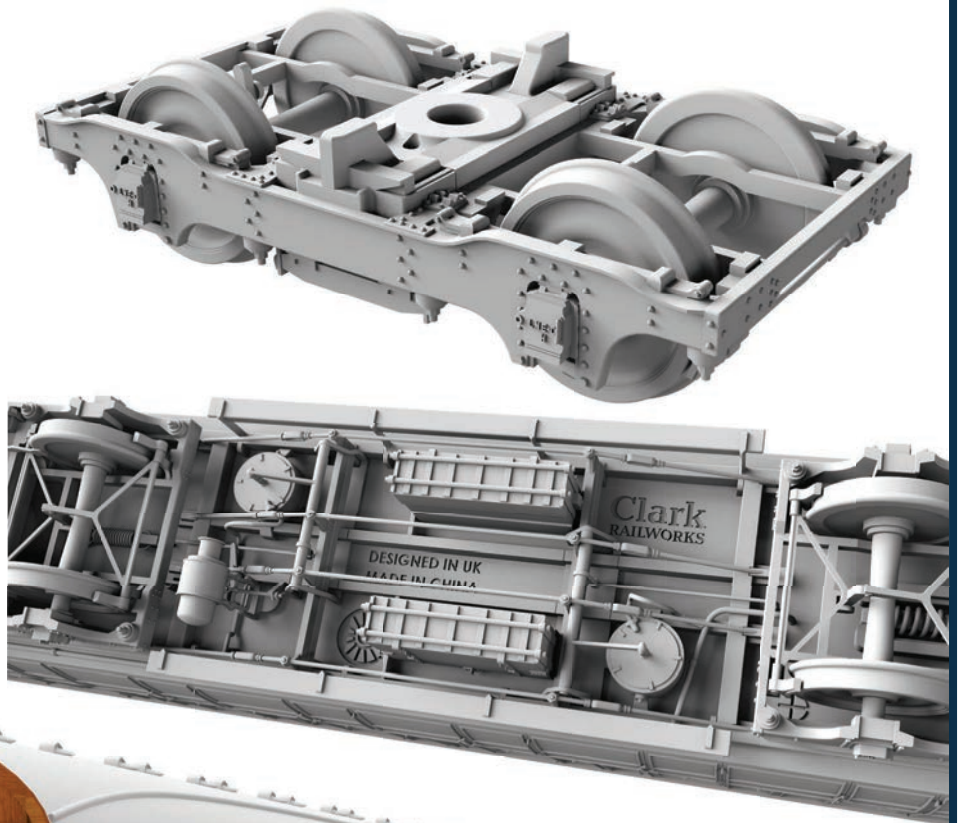
Meanwhile, Hornby's delivery of new models for TT:120 scale is 'full-steam ahead', as track packs and rolling stock items continue to arrive with customers in bulk – you can read our reviews in this issue, with more content on our website, World of Railways.

Ellis Clark breaks into 'OO' market

News came this month that 7mm:1ft scale (and larger) specialist, Ellis Clark Trains has been developing its first model for 4mm:1ft scale/OO gauge, depicting the Sir Nigel Gresley-designed GNR/LNER Quad Art coaches, as designed for use on commuter trains in and out of Kings Cross and on the Metropolitan Lines to Moorgate, and later seeing use as stand-by sets for Mk. 1 coaches.

Coaches are to be produced carrying a retail price of £325.00 per set, with 10 set variants to choose from, spanning the lives of the coaches in teak, BR crimson and BR maroon liveries. A complete specification list can be found on its website, while highlights include electrical pick-ups for interior lighting, and spare brake sets for EM/P4 gauges.

We are waiting for confirmation on when these new models are expected to be delivered.



Something fruity for 'OO'

A further batch of new models for 4mm:1ft scale / 'OO' gauge is to be produced by Rapido Trains UK. The manufacturer has expanded its tooling suite for its forthcoming GWR V14 and V16 vans, allowing it to manufacture models of the Y4 banana vans, examples of which are expected to arrive in early-2024, each carrying an RRP of £32.95.

Seven diagram Y4 variants are to be produced, covering GWR liveries (with lettering variants), with two in BR bauxite. Models are to feature new doors with locks and latches moved, steam heating pipes, and end ventilators modelled in the 'closed' position. Further imagery of these newly-proposed models can be seen on World of Railways.

Meanwhile, this month the manufacturer added to its recently-announced GWR opens and vans with a Diagram V17 No. 35601, WD grey (944028), Diagram O11 No. 21110, WD grey (943026), and Type 4 No. 564, Rhymney Railway grey (944037).

Rapido Trains is also to increase its livery offering for its models of the Hunslet 16in for 'OO', in another batch arriving late-2023, featuring five more liveries, including the much-requested model of *Beatrice*, as preserved.



'Slim Jim' exclusive

Kernow Model Rail Centre has taken delivery of a new exclusive OO gauge model representing Class 33/2 D6593 (3339), in as-preserved condition. Models are in stock, priced at £209.95, and utilise Heljan's latest tooling featuring newly-revised directional LED lighting, high-intensity headlight, and a 21-pin decoder socket.

The model of D6593 (3339) is available to order now at £209.95, and is fitted with a 21-pin DCC decoder socket, and represents the narrow-bodied locomotive in preserved BR green livery with high-intensity headlight.

D6593 entered service with British Rail in March 1962 as one of the last 12 members of the class, being 7in narrower to allow working through the restricted tunnels of the Hastings line. Its livery is suitable for modellers of the 1999 to present era. See more details on the KMRC website.



WOR THIS MONTH

Visit www.world-of-railways.co.uk to enjoy the following videos, interviews and features:



Watch videos of rail network upgrades from the West Coast main line, to Watford Junction and the removal of a station staircase. Ideal and rarely-photographed modelling inspiration, if ever there was!



Howard Smith interviews Ollie Simons, an avid modeller turned volunteer on the Colne Valley Railway about heritage work, why he purchased a BR 20T brakevan, while uncovering his latest purchase – a Ruston 48DS.



Quickview: Hornby Skaledale buildings and accessories. Howard Smith opens and comments on a selection of new arrivals from Hornby's highly-regarded Skaledale buildings and accessories range to see what you get for your money.

HATTONS: EXCLUSIVE INTERVIEW

Hattons revealed this month a return to manufacturing some of its model exclusives, with further batches of its Andrew Barclay 0-4-0ST and SECR 0-6-0T 'P' Class locomotives, plus Warwell and Beilhack snowploughs for 'OO' promised.

The new model batches are due for release in Q2, 2024, with a complete list of the liveries and prices of models to be produced available to view on World of Railways and Hattons websites.

Upon our interview with Hattons' Exclusive Commissions Manager, Dave Mylett, it was revealed that the retailer has been working on another project, due for release this summer...

Howard: How was your recent trip to China? Have factories evolved post-Covid?

Dave: It's always great visiting places and seeing how they've changed, but this trip was even more different to usual, but not as we were expecting. Covid 'regs' were pretty much as the UK was in 2021, but not prohibitive for movement or doing business and everyone was very welcoming.

For the factories, we noticed before we left that some weren't responding anymore. Luckily, these were some of the 'lower end' of the quality and reliability scale and it seems Covid has been too much for them to keep going through. At the other end of the scale, the ones

we could visit had all used Covid as a springboard to make changes and improvements. Some had expanded, some brought in new machines and technology, and others moved all together and set up new factories in new areas. Many are trying to make efficiency changes to reduce labour costs. It was great to see the development with this and gave us confidence that the manufacturing side of the industry is still improving in China.

I think, in terms of manufacturing, it's a good outlook for the future for technology, but raw materials and labour costs will still be major driving factors for prices on-board-ship.

H: Your latest run of P Class, and Andrew Barclay locomotives have a lower price than when first released, despite inflationary pressures on production and shipping. Does this reflect market confidence, or are other factors at play?

D: The way we price our projects is to amortise toolings on the first run. This means that all tooling costs are paid for and any further runs we just pay for the units and decoration changes. This helps us control costs because we know that if we decide not to produce any more models from that tool, we have still

Model	Number of new liveries	Price (each)
Bielhack snowplough	Two (twin-packs), covering BR yellow and Network Rail black	£70.00
Warwell	Eight, covering GWR, LMS, LNER, MOD 1960s-2000s, BR grey and BR brown	£25.00
Andrew Barclay 14in/16in 0-4-0STs	Nine, covering NCB blue and lined maroon, works grey, and colourful lined iterations	£85.00
SECR 'P' Class 0-6-0Ts	Seven, covering SECR lined green/goods green, Southern black/olive, BR black early crest (x2) and Bluebell variants	£85.00



made money on it. It also means that any further runs will have a buffer in the costings to help absorb things like price increases, exchange rates, or shipping costs.

H: What's your take on the outlook for 2023 and beyond re-model sales? How are you adapting to inflationary pressures?

D: We have been working on a new way for people to sell items through us. We have our pre-owned department and that is still very important to us, but we often have people selling one or two items that are just too small for us to take. Other sales sites, such as eBay are becoming confusing regarding fees and expectations and are not tailored to selling models. We are working on our own marketplace, using our vast database of models and our great track record of customer service to provide a safe and easy place for customers to sell their models through us rather than to us. Another way we will be different to other marketplaces is that sellers will send their items to us and our experts will check them before dispatching them to the customer. We are hoping to launch this during the summer and will provide more information on this closer to its launch.

Bala Lake loco miniaturised

Bachmann Europe has added a model of the Bala Lake Railway's preserved 'Alice Class' Quarry Hunslet 0-4-0ST *Holy War* to its 4mm:1ft scale/OO9 gauge range. Depicting the locomotive in its preserved state, with enclosed cab in lined blue livery, models are priced at £149.95 and proceeds from each sale will help the Bala Lake Railway.

The 0-4-0ST models will have 6-pin DCC decoder socket, coreless motor and die-cast boiler and running plate for extra weight.

Models can be ordered online now from the Bala Lake Railway's website. Read the full specification of models, with additional imagery, plus see a history of the prototype locomotive on World of Railways.



Heritage project receives Bachmann cheque

On Tuesday, April 18, 2023, Bachmann Europe Plc visited the North Eastern Locomotive Preservation Group (NELPG) at its Hopetown Workshop in Darlington to present models of the J72 Class locomotive *Joem* and a cheque for £504 to help with the completion of its overhaul.

The OO gauge/4mm:1ft scale model of No. 69023 *Joem* was released by Bachmann in 2020 as a Limited Edition for its Collectors Club. Available to purchase exclusively by Club members, just 504 models were made and £1 from each sale has been donated to help with the ongoing overhaul of the real locomotive, which is being returned to steam in time for the 200th anniversary of the Stockton and Darlington Railway in 2025.

The cheque was presented by Richard Proudman, Bachmann's Rail Brand Manager, to John Hunt, Chairman of NELPG.



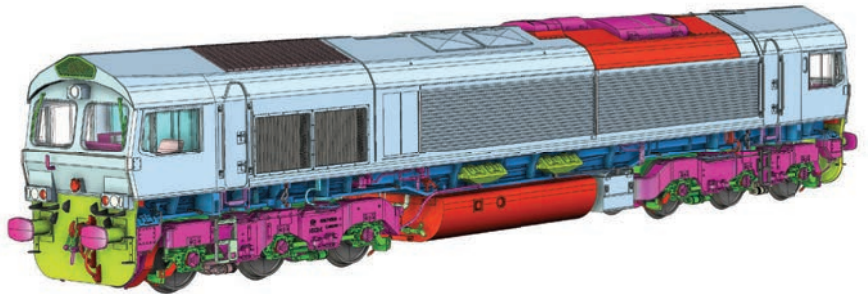
Bookend '66s' to join range...

New additions promised for first production run from modified ex-Hattons Class 66 tooling.

Accurascale is to mark the 25th anniversary of the Class 66 locomotive with OO gauge models of the first (66001) and last (66779) locomotives delivered to Britain.

Expected to be delivered in Q1 2024, with prices of £169.99 for DC/DCC-ready and £259.99 DCC sound-fitted, decorated samples are due with Accurascale in June. Joining its Accurascale Exclusives range, both locomotives will come with special presentation packaging and are only available direct via its website.

Seeking to maximise on its tooling acquisition, the manufacturer is offering the locomotives separately, most likely to make them more appealing to collectors and those wanting era-specific locomotives to run on their model railways.



“There were a few subtle differences between the first two and the rest of the production batch which have never been replicated before on a model. The lashing eyes to secure the locomotive to the ship were in a different position to later models.”

... alongside Accurascale exclusive Class 37

Upon the arrival of Accurascale's English Electric Type 3s/ Class 37s this month (read our review, page 104), joining its range is a manufacturer-exclusive model of ETS-fitted 37405, as-owned by Harry Needle Railroad Company.

The locomotive is to retail in 'special presentation packaging' with certificate, and will only be available direct from the manufacturer from Q1, 2024, at a price of £169.99 for DC/DCC-ready and £259.99 for DCC sound-fitted versions.

The model is said to match Accurascale's 'modernised' Class 37/4s from its first production run, complete with plated bodyside windows, LED tail lights, DRS multiple working socket, speedo cable and rectangular-style buffers.

With the inclusion of the Heavy Tractor Group (HTG) limited-edition model of its Class 37, Accurascale will see a total of 10 new Class 37s in run two with the addition of 37405.



Print-at-home figures

Tramfabrik has created a range of 1:76 scale free-to-download and 3D-print figures from its website. The figurines can be printed on resin 3D-printers, for those with access to one.

Its first releases include male and female steam locomotive drivers. The manufacturer says that its 3D models haven't been 3D-scanned, but designed on a computer, providing unique figures.



Custom layout names

Esme's Signs and Wall Art has shared imagery of recent custom model railway signage for its customers. The business is based across sites in Yorkshire and offers laser engraving/cutting, vinyl, and airbrushing products and services.

Bespoke and personalised signs can be found on its website or Facebook page, while custom railway-related signs can be made to order. Signs can be created in solid acrylic or veneered MDF.

YORK YARD NORTH

Hornby donates prizes to RBF

Hornby donated prizes last month to Railway Family Week – the Railway Benefit Fund's fundraising and awareness campaign. Prizes included Hornby mugs, a RailAdventure Class 43 HST train pack, Flash The Local Express Remote Controlled Battery Train Set, a Class 66, and re-built W1 4-6-4 locomotive.

Founded in 2021, Railway Family Week aims to bring together the entire railway industry to raise £50,000 for the Railway Benefit Fund (RBF). This year, Hornby offered a number of prizes to be won in a daily draw, which ran from Monday, April 24 to Friday, April 29.

Proceeds from every raffle ticket purchased supported the Railway Benefit Fund, a UK charity dedicated to supporting current, former and retired railway workers and their families. From financial grants and confidential advice to online tools and a legal helpline, its services are designed to provide support through life's unexpected events.



Cavalex: Class 56 in production

It's confirmed – production has commenced on Cavalex Models' Class 56 Co-Co locomotives for OO gauge, and prices are now set at £189.95 for DCC-ready models, and £289.95 for DCC sound-fitted variants.

DCC-fitted models are promised a custom sound project recorded and programmed by ESU, which interacts with independently-driven roof fans, each with its own coreless motor, to replicate the way that the real locomotive operates, under various loads and conditions.

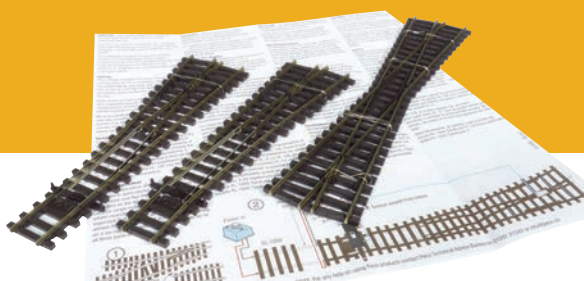
Models are promised a “newly developed speaker by ESU (one passive radiator and sugar cube combination unit)”, and hall sensors for adjustable automated flange squeal on sound-fitted models.

The feature should automatically provide the characteristic screech as the wheels enter sharp curves. Meanwhile, a ‘power bank’ is expected to provide “smooth uninterrupted running”, which, combined with floating centre axles, should allow “trouble-free road holding and power”.



Hornby TT:120 Track packs

Arriving with customers, Hornby's track system for TT:120 has seen new arrivals, with five ‘track packs’ to choose from, ranging from £19.90 to £32.80 each. The contents of each pack can be found on the Hornby website (Hornby exclusive), allowing customers to expand their sets to suit, choosing the correct one for their specific project.



Model Fences

York Modelmaking has expanded its selection of scale boundaries with more laser-cut fences for 2mm:1ft, 4mm:1ft and 7mm:1ft scales. With single and double-layered fences, and a selection of scale gates in the same styles to match, prices start from £8.50 for a 400mm length of fence in 2mm:1ft scale, with a pack of six gates in the scale priced at £2.50.



Hot new Hornby arrivals this month:

1 Reminiscent of the loveable Meccano ‘add-on’ sets in the past taking your set a step further, Hornby’s TT:120 track extension packs perform much in the same way, expanding the basic oval with sidings (sets one and two), until set three, which completes the second inner loop. Sets four and five add further sidings. Of course, many will want to deviate from the trackplans suggested to the side of each of the boxes, using the curved, straight, or points provided with each to create their own trackplan.

Neatly packaged, the contents of each one received fit together neatly, with smooth transitions from curved sections to straights, and vice-versa. Parts are pre-drilled for fixing to baseboards with track pins, alternatively, these can be fixed when ballasting with the glue mixture. Prices for packs range from £19.90 to £32.80.



2 The Rose and Crown Pub – one of many new Skaledale buildings and structures to arrive from Hornby with stockists this month is also one of its largest for 4mm:1ft scale. The building (R7359) is pre-painted, with transfer-applied signage. With 13 chimneys atop its three stacks, the brick-outline resin-cast structure has a lean-to extension to the side and one with bay windows to the front.

With evocative box art, you can watch Howard Smith’s first look video of the new Skaledale collection for 2023 on World of Railways.



3 Hornby’s release of its Hammant and Morgan HM7000 Bluetooth system this month commenced with a video demonstration of the system by its developers. You can catch up and watch the questions and answers video session on World of Railways. The HM7000 provides wireless control of HM DCC-fitted locomotives. These decoders take power from the rails (and can store it using the addition of ‘power banks’), but take their commands wirelessly over Bluetooth from your smartphone or tablet running the HM DCC app.

Now with retailers, our system is undergoing a thorough test, the report of which will feature in next month’s issue.



4 It’s bright and red – and though it never happened in reality, fans of Coca-Cola might be tempted by this new livery, applied to Hornby’s (ex-Lima) Class 121 model tooling, for ‘OO’. Historically, soft drinks manufacturers have had one of the largest marketing budgets. Fans of the soft drink can now run the single car unit on their layouts, or add it to (or start...) a collection of Coca-Cola ephemera.

In special Coca-Cola-branded Hornby packaging, the models (R30203) retail for £96.99, and provide a colourful addition to any model scenery.



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Quickview: Hornby Skaledale 2023 range

Howard Smith opens and comments on a selection of new arrivals from Hornby's highly-regarded Skaledale buildings and accessories range.

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DIGITAL EDITION EXCLUSIVE

**Quickview: Hornby TT:120 12T tank wagon
and LNER 'Toad B'**

Ahead of their review for the June 2023 issue of BRM, Howard Smith examines these new models from the manufacturer.

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Accurascale 37/4



Andy York

Accurascale's new Class 37 is now landing with customers who had pre-ordered one, bolstered by the reputation already earned with its 'Deltic' and Class 92 models, and it certainly doesn't disappoint.

Our review model depicts Class 37/4 37425 *Concrete Bob* in its attractive Regional Railways 'retro' livery, which was applied by the owner, DRS, in 2020 before the locomotive was recently offered for sale. As with previous models, the packaging reflects the quality of the product, accompanied by a good instruction leaflet, DCC sound function card for easy reference and a BR-style manual giving a history of the prototype and a driver's perspective of working with Class 37s by Nigel Mack.

Accurascale's initial models of the Class 37 range across sub-classes and era from early split headcode styles in BR green livery with small yellow warning panel through variants with plated and flush noses, body, roof, grille, buffer and skirt differences, as well as lighting types and configurations. The immensely varied detail differences among class members may not be comprehensively

covered within the initial products, but there is far more to come with a second tranche of models already announced for next year.

Everything about the model just looks right and feels of good weight, and the quality of livery application and printing of text and safety signage is as good as any out there in the market. Fitted detail abounds with fine handrails, lamp irons, beautifully sharp bodyside grilles and eye-catching etched detail such as the door kickplates, roof fan grille and sturdy nose aerials. The glazing is commendably lacking in prismatic edges; some comment has been raised about the joint between the windscreen surround and the roof and pillars, but that joint is present on the real thing, maybe slightly less pronounced than it scales on the model, but the effect is good. The detail pack contains bufferbeam detailing and cosmetic screw-link couplings, snowploughs and bogie chain brackets, plus a pack of etched nameplates for appropriate models that are already cut from the frets, which helps give a tidy and worthwhile result.

Below body level, there is an impressive amount of detail and distinction to the

bogies, with a commendable lack of fresh air between bogie and chassis without the movement being restricted.

A design delight with Accurascale models is the ease of access for fitting decoders, and the whole central roof area is easily removed and re-secured by the inclusion of small neodymium magnets. While the roof is off, the tidiness of assembly of the plug and socket wiring is evident, as is the size of the bass speaker included in this model.

In operation, without sound, the model is near silent in running and will have no problem with current collection on poor trackwork thanks to the stay-alive capacitors, which give several seconds of running without power. Thanks to the weight, the performance is impressive, capably handling a load of twenty Mk. 1 coaches.

Firing up the sound on DCC gives great pleasure; the depth of bass sound evident in the files is superb giving a good throaty roar to the model's performance. Hours of pleasure will be gained from putting the loco in drive mode and winding up the power.

How good is it? Well, you won't just want one...

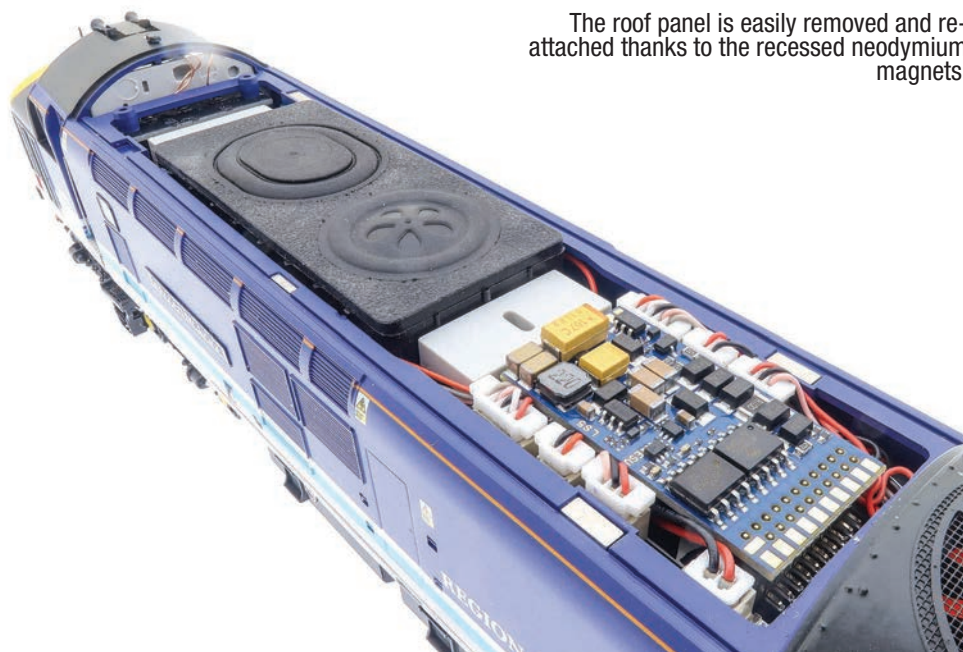
3D laser-scanning and extensive surveys have reproduced the dimensions and shapes to a high level of accuracy with a phenomenal level of detail, particularly around the all-important 'face' of the model.





Take time to explore the comprehensive crib sheet for all the lighting and sound functions offered by the model. Note also, the collection of bulkhead labels.

The roof panel is easily removed and re-attached thanks to the recessed neodymium magnets.



FACTFILE

Model: ACC232137425DCC

Accurascale Class 37/4 Sir

Robert McAlpine/Concrete Bob

Livery: Regional Railways 2020 'retro' livery

Era: 2020 – present

Construction: Die-cast chassis, plastic body with etched detailing parts

Weight: 690g

Minimum curve: Radius 2 (438mm)

DCC socket: 21-pin

WHAT WE LIKE

- ✓ Livery application and sharpness of print
- ✓ Metal chequerplate to door kickplates
- ✓ Design of bogie securing chains
- ✓ Roof panel and handle detail
- ✓ Sprung buffers with detail to rear of buffer heads
- ✓ Plug and socket connections for tidily routed internal wiring
- ✓ Lighting colour and brightness levels
- ✓ A deep bass to engine sounds
- ✓ Value for money



Hornby Dublo A4



Tony Wright

Linked in with Hornby's 100 years of producing model trains, by way of a celebration, the company is producing die-cast bodied locomotives in the 'style' of the original Hornby Dublo ones of many years ago.

The A4s need no introduction, for they rate as one of the finest express passenger steam locomotives ever made. In *Mallard*, they have the fastest steam loco of all time and in *Sir Nigel Gresley*, they have the fastest post-War steam locomotive.

This fine model doesn't represent the loco in any of its preserved manifestations, because it still has a single chimney. It's representative of the period from April 1952, when painted BR green, until December 1957, when it received its double chimney.

The subtle body-shape has been captured to perfection – many OO gauge A4 models don't get this right, and from every angle it's spot-on. The only thing which isn't captured perfectly is the turn-in at the base of the cylinders, but there has to be a means of separating the body from the chassis. The tender is plastic, and is an excellent

representation of the 1928 corridor type (ex-A1/A3), towed throughout its life.

A feature that sets this locomotive apart from the rest is the standard of finish – the BR green is dead right, and the lining is beautifully-applied, as are the numerals and BR device on the tender. The numberplate is arranged just like the prototype, supported by two triangles, not a solid wedge.

Mechanically, this A4 is the same as its plastic siblings, though with far superior bogie wheels. They appear to be the same as on the firm's A2/2 and A2/3 models; that is a real representation of an LNER 3ft 2in bogie wheel, not the previously generic sorts. All wheels are true-round, have consistent back-to-backs and on test on my 'Little Bytham' (see the WOR moving footage of this loco in action), ran perfectly through my mixture of hand-built and Peco points and crossings.

The pony wheels are flangeless to enable the model to negotiate train set curves, though flanged replacements are supplied for those with more generous radii. The locomotive is incredibly quiet and super-smooth. The motion is a fair representation

of the elegant original, including the lubricator drive off the RH rear driver's crankpin. The lubricator drive must be disconnected to remove the body – a bit of a faff, in my opinion.

Provision for DCC is in the tender (eight-pin), and there's now a far-superior mechanical/electrical coupling between it and the locomotive. It's now a fool-proof single action 'plug-together' device, and not the previous rather flimsy plug and socket connection, with a separate drawbar. There's even a firebox glow present, and provision for a smoke unit.

Cylinder drain cocks, a scale front shackle, brake rigging, and a bespoke crew are for the owner to fit. There's also an AWS bang-plate supplied, to be fixed behind the front coupling.

A truly outstanding model locomotive at a premium price tag, but of great appeal to collectors, given its limited production. It should appeal to those who can run it too, for its performance is something to be admired. Recommended without reservation.





Livery-wise, the only thing I'd really query is the green paint on the cab roof between the eaves and the rainstrips. This was an LNER feature – in BR days, the whole roof was black – a livery detail correct on the same firm's TT:120 A4.



There are the usual bits and pieces for the owner to fit, including cylinder drain cocks, a scale front shackle, brake rigging and even a bespoke crew.



Mechanically, this A4 is the same as its plastic siblings, though with far superior bogie wheels. They appear to be the same as on the firm's A2/2 and A2/3 models; that is a real representation of an LNER 3ft 2in bogie wheel, not the previously generic sorts.

FACTFILE

Manufacturer: Hornby Plc

Era: 5

Company/Operator: British Railways

Body and chassis: die-cast metal

Minimum curve: 438mm (R2)

WHAT WE LIKE

- ✓ Standard of finish is exquisite
- ✓ Numberplate '6' is the correct 'incorrect' Gills Sans font
- ✓ Excellent powerful performance, because of weight

WHAT WE NOTICED

Nameplates are printed plastic, not etched – I question the red background of these because evidence suggests this was only applied in the last few years of its BR career.



WORLD OF RAILWAYS

TV

DIGITAL EDITION EXCLUSIVE

On test: Hornby Dublo die-cast A4

We couldn't resist putting Hornby's new die-cast A4 4-6-2 locomotive at the head of a long rake on Tony Wright's 'Little Bytham' layout.

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Hornby LNER Toad B



Howard Smith

If there's a sign that British outline TT:120 isn't a 'fad', it's the diverse collection of models revealed upon its launch as a part of its range. Many have been quick to jump on the bandwagon shouting "it won't last" as they cling to their collection of existing stock, whatever the scale may be. But place yourself in the shoes of someone new to the scale – scale to gauge realism, at a more accessible price point sounds attractive...

Arriving this month, the first brakevan for the scale – the LNER 20T 'Toad B' – might not be the prototype I'd have opted for a range launch, but nonetheless, more popular models with BR transition era modellers such as the LNER/BR 20T brake are surely to arrive soon?

Moulded detail for this model has been carefully planned. Elements that we'd now find as separately-fitted on 4mm:1ft scale models, such as veranda safety bars and access handrails, or lamp iron brackets, here are integral. And they don't look any worse

for it – proof that in this marginally smaller scale, what the eye doesn't see...

The longer bodyside handrails are individually-fitted items, while side steps are clearly a later fit in the production process, too. These are neatly installed with a minimum of solvent adhesive, leaving no trace of the procedure on our review sample. Its design and assembly has been as neat and professional as you could have possibly wanted.

The astute modeller looking to detail or re-number models should be mindful of the differences between vehicles. Some vehicles had narrow- or wide-planked sides when built, and Hornby has selected the shorter length of RCH buffer shanks. Both original and re-built side duckets are represented by Hornby across its NE brown and BR grey-liveried variants. The prototypes on which these excellent models are based were far from identical, the earliest of examples constructed with veranda sanding gear and pipework to wheels. Recommended models.

FACTFILE

Manufacturer: Hornby Plc

Era: 2-3

Company/Operator:

Private-owner

Body and chassis: Plastic

Minimum curve: 267mm (R1)

Couplings: NEM-mounted

TT:120 standard

WHAT WE LIKE



Well-assembled with no flaws



Era-specific side duckets are correct





TT:120 Scale



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REVIEWS

Hornby 12T tank wagon



Howard Smith

Another arrival for TT:120 sees this tank wagon produced in a familiar livery. Carless Petrol tank with its red band has been seen before on Hornby's 14T tank wagon for OO gauge, while Oxford Rail's 12T wagon for OO gauge has too shared this decoration.

Chassis-wise, this model presents well, with inner- and outer-brake 'V' hangers presented in full to both sides, while brake lever supports are commendably fine with an impression of the through-holes for 'pinning down'.

This is a very free-running and lightweight model. The sign of the tank secured to the chassis with a screw is a clear indication of a future-proofed range, with options for tank variations. Our tank was screwed to the chassis with the top filler a little off-centre, but barely noticeably so. Pedantry on my part? Quite possibly...

Livery-wise, as can be seen in the image, lettering and numbering is sharp and legible down to the finest of solebar panels – but only under significant magnification. Buffer heads are true from all angles – rare with models at this size, which often require a little 'fettling', something I often let manufacturers 'off the hook' for.

The central tank strapping isn't quite as taut as we've come to expect from models in the manufacturer's OO gauge catalogue, but that's it – the only two very minor, arguably non-existent 'gripes' I could find. It's becoming more difficult with every model release to find what might be the smallest of faults to form an objective review – this latest from Hornby isn't making the task easier. Highly-recommended.

FACTFILE

Manufacturer: Hornby Plc

Era: 2-3

Company/Operator:

Private-owner

Body and chassis: plastic

Minimum curve: 267mm (R1)

Couplings: NEM-mounted
TT:120 standard

WHAT WE LIKE

- ✓ Tank design has raised detail for outer-riveted sections
- ✓ Drawbar hooks are perfectly formed and straight
- ✓ Finesse of solebar nomenclature and appendages
- ✓ Rivets are as close to scale as we could possibly want





Ellis Clark Trains Wickham Trolley



Howard Smith

If there's a prize to be won for the smallest RTR model of 2023 in 7mm:1ft scale/O gauge, this new release from Ellis Clark Trains surely takes first place. Cramming a mechanism that can provide reliable traction under the small vehicle – adding a sound decoder, and speaker – is a significant accomplishment.

Be warned, however, this is a model that demands careful handling, and dare I say it, is one of the most fragile I've ever had the pleasure of reviewing in the scale. It's the balance of manufacturing a model to exacting scale dimensions, without compromise of detail – a fine line to tread in model manufacture these days.

This is the manufacturer's first motorised RTR model for the scale, and I feel it has succeeded, even exceeded expectations, much like its RTR 'Presflo' wagons last year.

While models 'fly' off the shelves – this variant sporting BR maroon with a white roof having sold out – it highlights the importance of pre-ordering models again. With an impressive 17 livery variants on offer in this first batch, modellers of the late-1940s to 1970s, and preservation eras have their needs covered.

This variant sees BR(M) maroon with a white roof, numbered B40W, and represents the trolley as now preserved on the Somerset and Dorset Railway. Though tampo-printed elements on this livery are spartan, their application is crisp. The use of wood being extensive on the prototypes from the floor and sides of the wagons, to the floor, side steps and bench seating of the 'trolley' has always been a challenge to recreate. Here it's painted a mid-brown – options for weathering, perhaps?



Wagons are kept in-tow by these small pins, provided in an accessory bag with each model. The more wagons, the more pins... these work as they should.



The bench seating is commendably fine, with the steel frame of the prototype here, injection-moulded in plastic. Look closer, and you'll notice detail of the metal plates with bolts that secure the rear of the centre bench to its frame. The open nature of this model leaves no prisoners when fitting a crew. Ellis Clark is retailing a cast metal crew from Northumbrian Painting Services, increasing weight, hence traction. Of course, there's a healthy selection of 3D-printed crew from other manufacturers now, too.

Fitted with a headlight to each end – perhaps of some use when running at dusk, or poor weather conditions, or through tunnels – we don't expect it to have had a large impact on the prototype when running. Still, on the model, these are working and directional, with a glow sufficient to provide your would-be miniature crew with a smidge of confidence.

The Wickham Trolley comes boxed with a low-bed wagon. But these are available to purchase separately, too. Detailed as the main trolley is the wagons boast visible brakes, pinned down by a lever that could prove too tempting for those miniature figures 'up to no good' in a yard.

Wheels this small on a model in the scale are rare. For a road-railer project I modelled in the magazine years ago, I had to resort to ordering from the States. Ellis Clark has captured the cast nature, shape and diminutive size to perfection, though minimal painting of some rust to these nickel-plated items wouldn't go a-miss.

The model is also fitted with a Zimo (MS590N18) sound decoder with a 12x8mm mini dynamic speaker, 12x8mm resonance chamber and an exclusive sound project boasting 20 different sounds.

FACTFILE

Manufacturer: Ellis Clark Trains

Era: 4-7 and preservation

Company/Operator: BR (departmental)

Body: Plastic **Chassis:** Die-cast

Minimum Curve Radius:

1028mm (R2)

Couplings: custom pins

WHAT WE LIKE

- ✓ Detail bag includes curtains, turntable rails, toolbox & extra coupling hooks
- ✓ Laser-cut Wickham Dual Lane Shed kit also retailed by Ellis Clark
- ✓ Custom weathering options are available
- ✓ Extra flatbed or planked wagons can be purchased

PRICES

Wickham Trolley & Trailer:
£150 (£280 DCC sound)

Additional trailers: planked
£30 each, flatbed £28 each

WHERE TO BUY ONE

Ellis Clark Trains
Unit 2 Toller Court
Shortbank Road
Skipton
North Yorkshire
BD23 2HG

www.ellisclarktrains.co.uk





Hornby departmental 'Van C'



Howard Smith

First released to traffic in the summer of 2008, Hornby's OO gauge tooling of the Maunsell SR 'Van C' has aged very well, as evidenced by this latest iteration of the uneven plank variant, now carrying BR engineers' 'drab olive' as ADB975140 (R60081).

This depicts, rather faithfully, a prototype which, though constructed in 1937 as a 'brake', was recorded in 1971 as being repurposed by the engineers as a staff van. A photograph of the prototype can be found online, which purports to have been taken at Norwood Junction/Selhurst.

Unlike some of these vehicles, this prototype appears to have received little to no external modifications, making the use of Hornby's existing tooling ideal. Though difficult to decipher from the photograph online, the external ventilators appear to have been removed and plated over flush – I'll take Hornby's model as correct, however.

The window bars to the glazing have been carried out to a high standard, and these appear to have been tampon-printed. Their finesse is exemplary, as is the application

of other decorative items for that matter. A favourite is the red flammable warning notice placed to the doors either side, denoting no doubt propane gas bottles housed for cooking by the crew. Similar redundant and modified aging vehicles of this era had warning notices placed to their housing's underneath. It certainly 'modernises' the appearance of the design, as do the white-painted footsteps and door handles.

A slight error to the livery appears to be the application of a white dot in lieu of a smaller letter 's' adjacent to the 'ME' acronym denoting 'mechanical engineers'.

Dimensionally, the model remains 'spot on'. Our observations on the shape of the truss rods being rectangular rounded bar section rather than rod, remain – an understandable compromise for the strength of the plastic.

Overall, a great addition for modellers of the BR blue diesel era, which can be marshalled into engineers' trains, or equally left parked in departmental sidings, accordingly. Highly-recommended.

FACTFILE

Manufacturer: Hornby Plc

Era: 7

Company/Operator: BR (departmental)

Body and chassis: plastic

Minimum curve: 371mm (R1)

Wheel Profile: RP25

Couplings: NEM-mounted tension-locks

WHAT WE LIKE

- ✓ Livery application quality is exquisite
- ✓ We gave Hornby high praise for its tooling in 2008 – little has changed here
- ✓ Buffers are sprung, with turned steel heads
- ✓ Underframe detail with brake rodding remains impressive



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Accurascale Siphon G



Andy York

Accurascale has sent three of the main liveries for review that will be available in its GW Siphon G bogie wagon range, totalling 16 liveries. Three models here are all of diagram O.33, representing the original form of the smoother-sided non-passenger carrying coaching stock (NPCCS), which was a successor to the earlier outside-framed and horizontal planked types for their transportation of milk churns across their routes from country to city. 115 wagons were originally built with 60 of these adapted for ambulance trains for WWII, with plating over most of the upper louvres and adding windows. When these adapted vehicles returned to revenue service, they became Dia. M34 and subsequent batches produced between 1950 and 1955 included eight sliding louvre ventilators to the lower bodyside and were differentiated as Dia O.62. Accurascale has covered all of these bases with different body mouldings, so that variety can be incorporated into trains.

Across the principal liveries of GW

brown with shirtbutton crests, BR maroon, BR carmine and BR maroon, Accurascale has portrayed different markings and numbering as the use of the vehicles transitioned from milk traffic to parcel and newspaper usage. Add an ambulance carriage and an Enparts van into the mix and there's good variety. The later usage of the Siphon G was, for 34 of the type, for newspaper traffic until their withdrawal in the early to mid-1980s with TOPS codes of NMV and NNV, again provided for within the initial releases. Of course, predominantly found on the Western Region, vans were known to crop up anywhere around the network, particularly in parcels use.

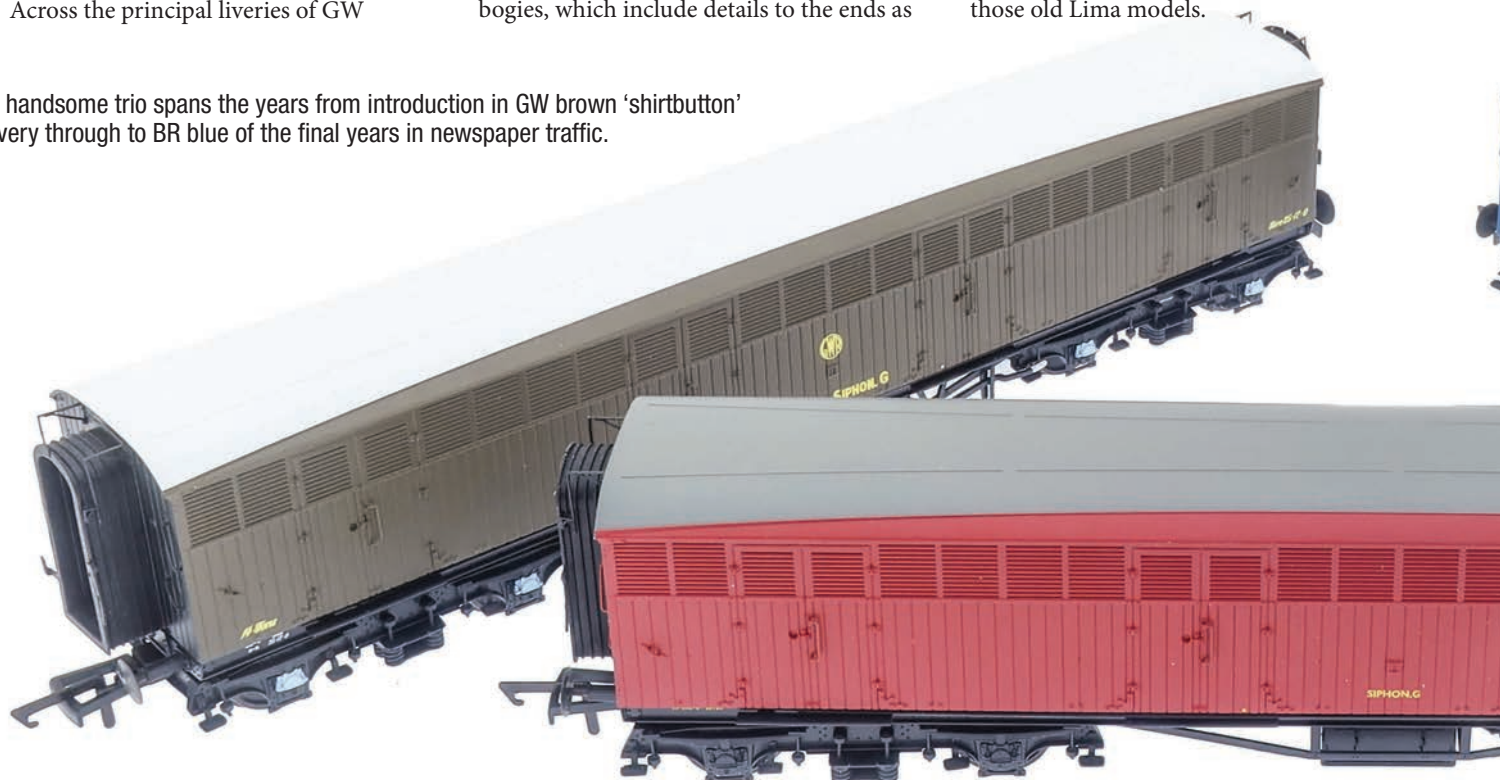
Although the body of the models seem quite 'flat' along with a plain roof (bar ambulance conversions with vents), there are separately-fitted details in the form of the door and end handles and the exquisite gangway corridor brackets. Below the solebar, the detail is superb with brake equipment and battery boxes and wonderful bogies, which include details to the ends as

well as the sides, with excellent brake linkage portrayals.

Returning to the ends of the carriages, the gangway connector is of a soft plastic which will be of interest to those close coupling trains. The accessory pack includes a pair of NEM-compatible magnetic couplings, which succeed in bringing the gap between gangways down from 9mm with tension-lock couplings to 2mm with the magnetic couplings, which improves appearances immensely. The magnetic coupling heads are quite small, as are the embedded neodymium magnets; they performed well on test but may not have sufficient strength if other vehicles in the train have substantial weight or friction.

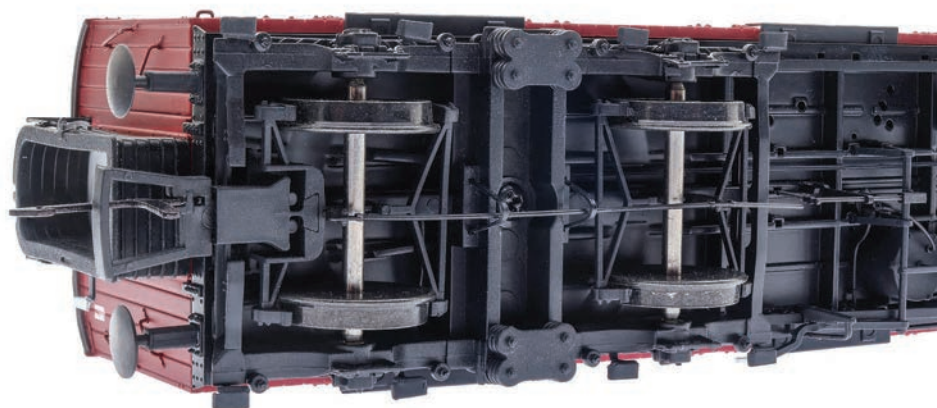
The last models we saw of this type were from over 40 years ago thanks to Lima, but Accurascale's model is in a different league, with more detail and finesse, and it's fantastic to see a model of a geographically widely-used type that spans over 50 years of railway life. It's absolutely worth replacing those old Lima models.

A handsome trio spans the years from introduction in GW brown 'shirtbutton' livery through to BR blue of the final years in newspaper traffic.





The soft plastic gangway end and supporting brackets are just one of the eye-catching features that show this is made to modern production standards.



The detailed bogie features brake blocks in line with the wheel treads, which can be removed if you fit EM/P4 wheelsets, for which space has been provided.

The bodyside brackets, hinges and handrails are superbly defined, as are the bodyside vents on this Dia. O.33 van.



FACTFILE

Models: (ACC2413-2924) Dia. O.33 - GWR brown: 2924

(ACC2411-W2938W) Dia. O.33 - BR carmine red: W2938W

(ACC2415-W2938) Dia. O.33 (NMV) - BR rail blue: W2980

Liveries: GW brown, BR carmine and BR Blue

Era: 1930 – early 1980s

Body and chassis: Plastic

Weight: 146g

Minimum curve: Radius 2 (438mm)

Accessories: Brake pipes, screw-link couplings and magnetic close couplers

WHAT WE LIKE

- ✓ Livery application and sharpness of print
- ✓ Soft plastic corridor gangway ends
- ✓ Bogie detail
- ✓ Underframe trusses, cylinders and battery boxes
- ✓ Bodyside moulded vent finesse
- ✓ Magnetic close-coupling

Revolution Trains

ZKV wagons



Howard Smith

Fitting new box bodies to older, re-purposed wagon underframes isn't a new concept. Even pre-grouping companies were doing it, and the tradition continues to this day, with the 'Lobster' wagons.

Here's another great wagon release by Revolution Trains, made commercially-viable no doubt largely because of its simultaneous release with its 35T Class A tank wagons, also for 'N'.

With the re-purposing of the Class A tank wagon chassis – as per reality, when fitted with new box bodies from 1975 – these wagons will suit modellers of the BR blue era when purchased in their revenue freight brown livery carrying the MTV TOPS code, while modellers of the late-1980s and 1990s can justify them too, carrying departmental brown with yellow stripe as ZKVs. Modellers of the 1993-onwards period can look to models in 'Dutch' livery, with blue-stencilled 'Mainline' branding.

From their initial use carrying sand and aggregates to later salt, plate iron and scrap,

and finally ending their lives transporting spoil, ballast and sleepers, the choice of wagonload for the modeller is varied, too!

It's the underframe detail that has us impressed every time with a Revolution Trains model. Rarely do we see any form of compromise, and here is no different with brake cylinders and apparatus represented to a high standard.

Wagons are being offered in triple packs of different liveries and numbers, and are available singly, too, making longer rakes of wagons with unique identities easier.

Visually, and dimensionally, the rolling stock looks right for 1:148 scale – you can check against a rule if you don't believe me. Smooth-running, with brake shoes as close to the wheel treads as they dare be, it's hard to find fault with anything.

Revolution Trains has cornered the air-braked rolling stock market for 2mm:1ft scale, and continues to announce and release products. A highly-recommended product, which proves that the market for more up-to-date rolling stock in the scale is there...

FACTFILE

Manufacturer: Revolution Trains

Era: 7-9

Company/Operator: BR revenue, BR departmental

Body and chassis: plastic

Minimum curve: 230mm (R1)

Wheel Profile: RP25

Couplings: NEM-mounted Rapido-style

WHAT WE LIKE

- ✓ Patch-painted sides where appropriate
- ✓ Stencilled TOPS code and numbers are era-specific
- ✓ Oleo buffers are well detailed, though polished shanks could be painted silver
- ✓ Underframe detail is impressive



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(Open)



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Train Pack 1



11222 - Mainline
Hunslet Crew Open Cab



11223 - Mainline
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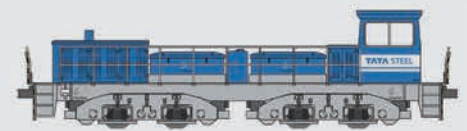
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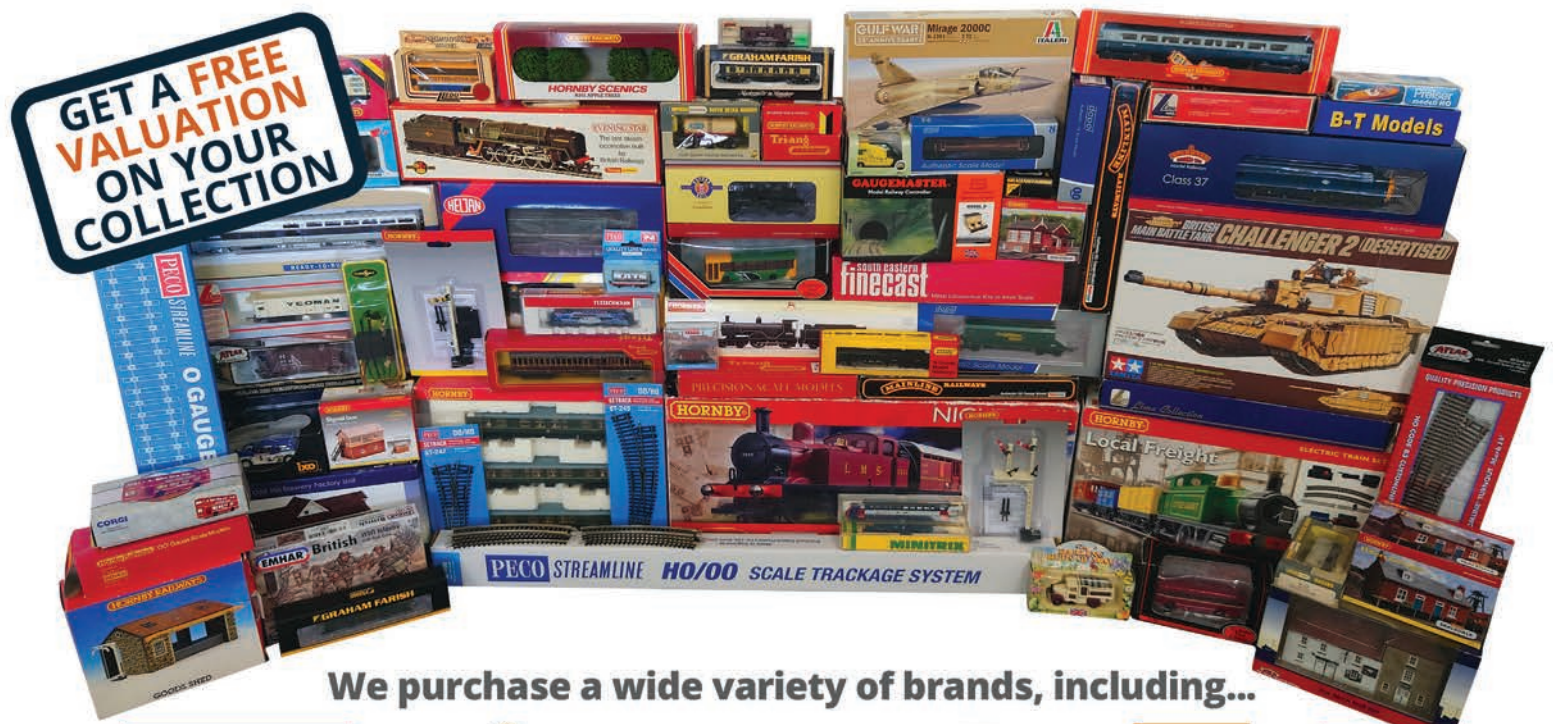
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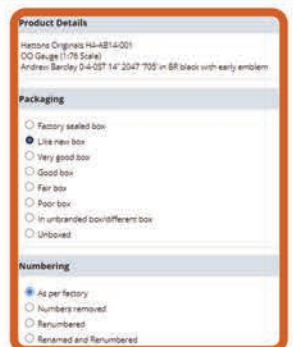
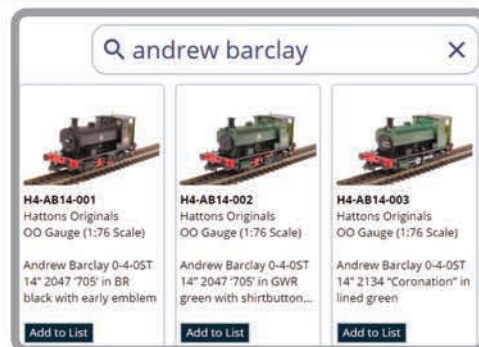
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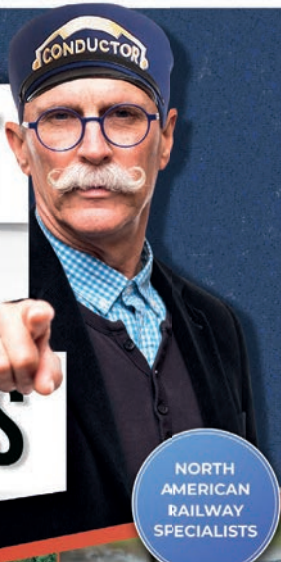
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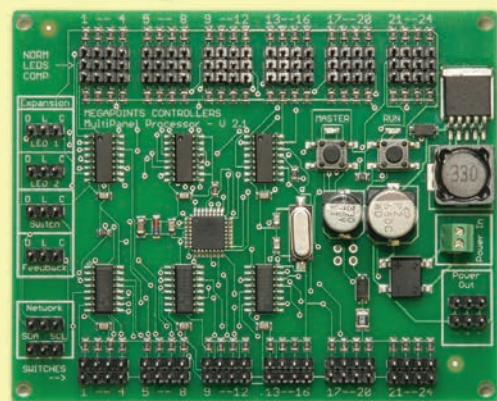
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July 29/30 - NEW MILTON - South Coast MRC Annual Exhibition, The Arnewood School, Gore Rd, New Milton, Hampshire, **BH25 6RS**. Sat/Sun 10-5.30.

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Sep 2/3 - STAFFORD GUILDEX 2023, Gauge O Guild Exhibition, Bingley Hall, Stafford Showground, Weston Rd, Stafford, **ST18 0BD**. Sat 10-5, Sun 10-4. www.gaugeoguild.com

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Sep 17 - OXFORD OXRAIL, Cherwell School, Marston Ferry Road, Oxford, **OX2 7EE**. Sun 10.30 - 4.

Sep 30 / Oct 1 - WAKEFIELD Model Rail Exhibition, Queen Elizabeth Grammar School Sports Hall, Northgate, Wakefield, **WF1 3QX**. Sat 10-5 Sun 10-4.30

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Oct 28/29 - LEEDS Model Railway Society Annual Exhibition of Model Railways, The Grammar School at Leeds, Alwoodley Gates, Harrogate Rd, Leeds, **LS17 9NA**. Sat 10-5 Sun 10-4.30.

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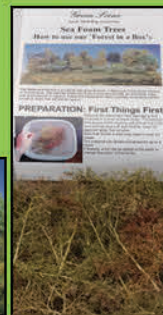
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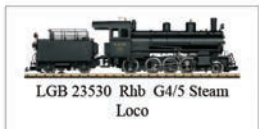
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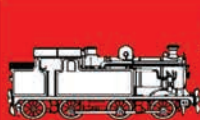


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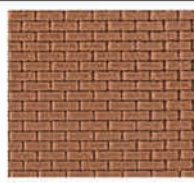
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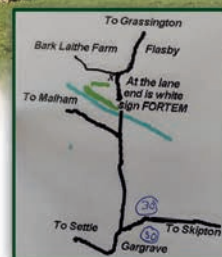
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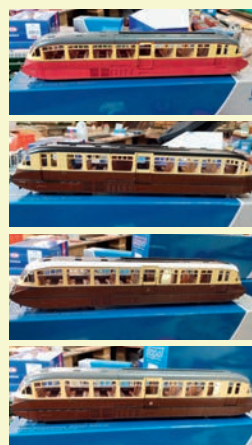
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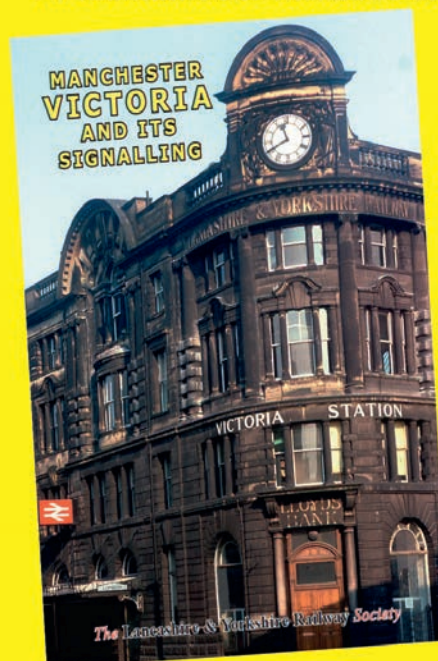
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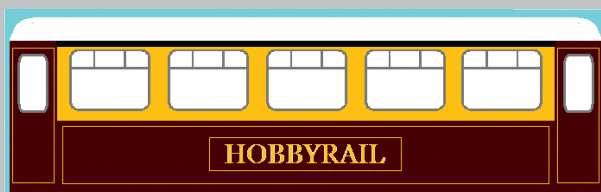
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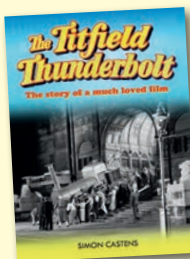
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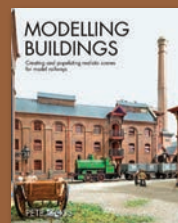
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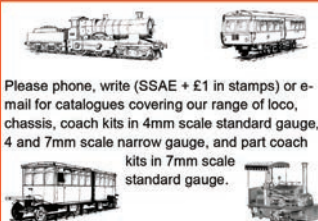
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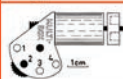
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The model railway show season is in full swing right now, and don't we know it! Members of the team have been out at exhibitions, either working or just visiting, every weekend for well over a month. Despite this, there are many events that look interesting, but we simply don't have the chance to attend.

Some wondered if the show scene would bounce back from the enforced break of Covid. It certainly has.

With exhibitions always held at the weekend, we wonder if it's time to consider a tweak to the calendar? Scrap Wednesday, the 'hump day' isn't much fun anyway, and insert another weekend. Yes, two weekends a week, that would do us. More modelling time, and twice the chances to visit a show to find inspiration from other people's models.

Some will point out there would be one or two minor practical issues, but surely the great brains who can understand how a black hole works can figure that out!

What was your favourite childhood sweet?

Andy: A tube of rainbow crystal kayli. Down in one!

Debbie: Dib Dab for me. Although I'm sure they only used to be 10p!

Howard: Milk chocolate – and nothing's changed.

Phil: Wagon wheels. I'm sure they are smaller today...

Ruth: Curly Wurly! I'm with Phil, they used to be so much bigger!

This month in pictures...



Phil built another garden railway in a day for the National Garden Railway Show and spent the day playing trains.



Over in Taunton, the RMweb members' day was a great success, with excellent cake on offer too.



Spotted in the National Railway Museum. Does this mean Platform 9 $\frac{3}{4}$ is real?



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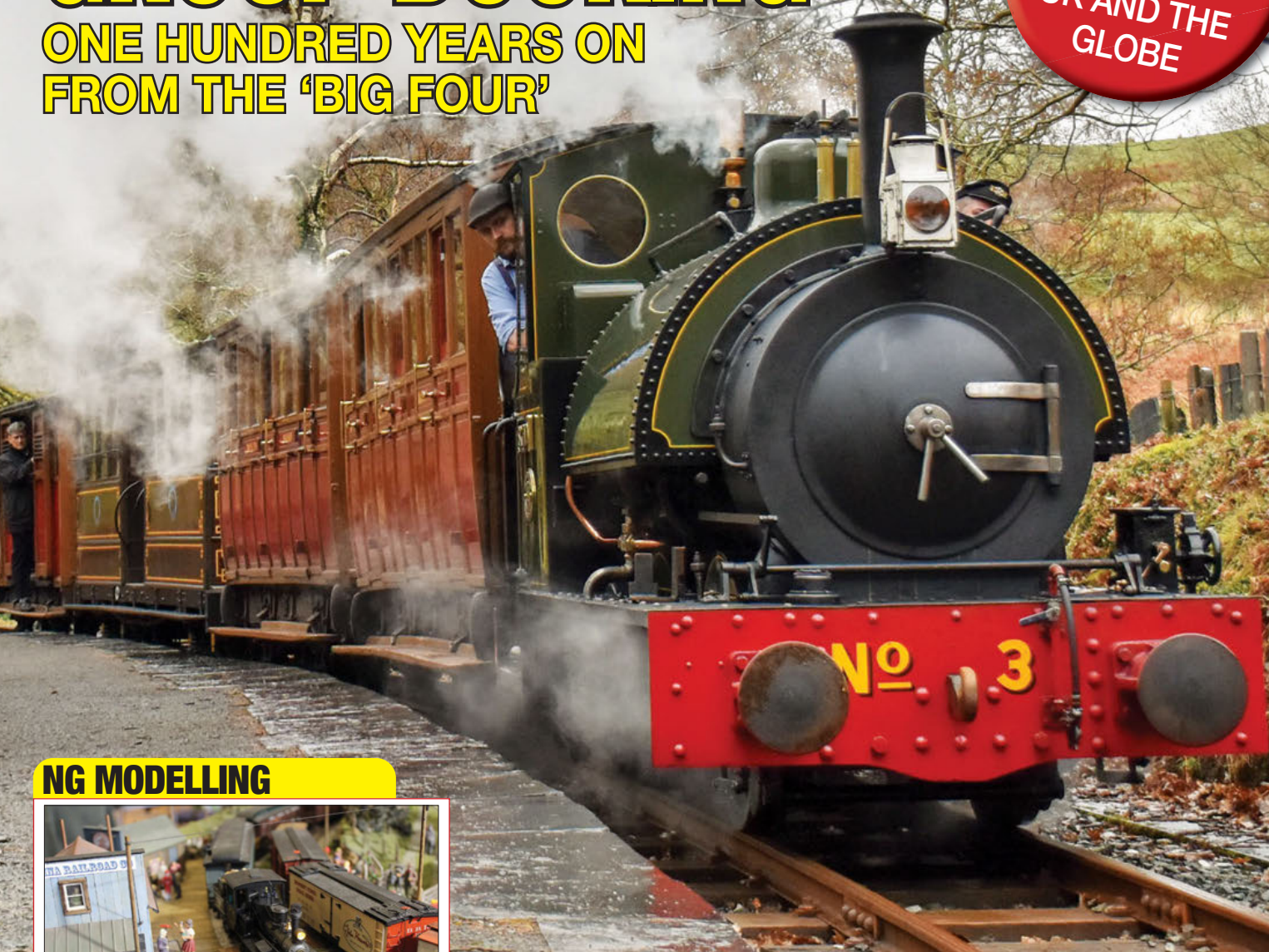


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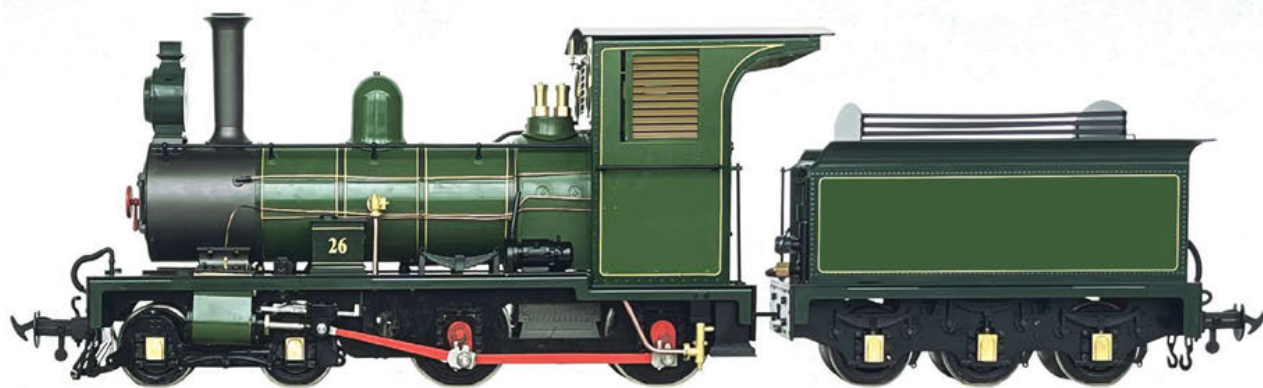
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Cover: Spot what's wrong with this photo! No doubt seasoned Talylyn enthusiasts will get it straight away, former Corris Railway Hughes 0-4-2ST 'Sir Haydn' the 'wrong way round' running towards Tywyn in the TR in 1951... This time it was all part of a photo charter on 20th March, of course attended by *Joey Evans*.

The inset shows Darjeeling-Himalayan Sharp-Stewart 0-4-0ST+T no 19B – see page 09 for news of a vital campaign seeking to ensure this locomotive stays in the UK.

Photo: Andrew Charman

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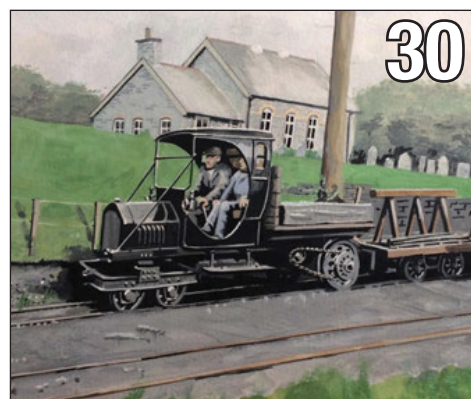
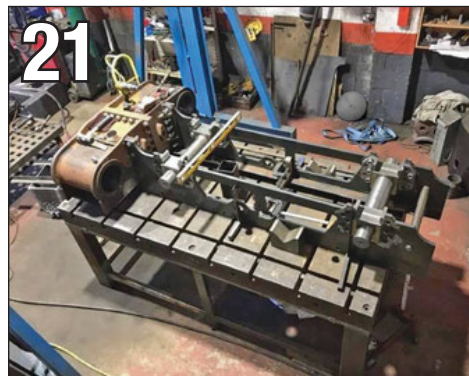


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All stop in Devon?



Above: A visit to Woody Bay, here for the Gala in 2021, is always a pleasure – major achievements such as new-build Baldwin 2-4-2 'Lyn' and the fine carriage stock have done much to recreate the atmosphere of a line none of us ever knew in its heyday. But if the revived Lynton & Barnstaple is ever to be more than a one-mile line, all concerned need to make changes...
Photo: Andrew Charman

Welcome to *NGW* 174, as we thankfully begin our summer monthly publishing schedule – so much has happened in the narrow gauge world over the past couple of months I've faced a real challenge fitting everything that needed to be included in the pages of this issue.

Sadly our lead story this month is as disappointing as it is significant. We of course are the greatest advocates for the narrow gauge and I try to avoid at all costs writing negative stories about our railways, but sadly it is very difficult to put a positive spin on the current situation at the Lynton & Barnstaple Railway.

Long have many, including *NGW*, championed the revival project in Devon, dubbing it the last big restoration, the headline project of the early 21st century as the Welsh Highland was the last major project of the 20th century. We've marvelled at the operation at Woody Bay, as new-build proper L&B locos haul beautifully restored carriages into a proper Southern narrow gauge station. Imagine what it will be like when it's more than a mile long, we write on a regular basis.

Sadly at present that prospect seems a long way off. You will read on the next page of the expiry of planning permissions effectively stalling the revival project, which is bad news in itself. But what is very

concerning is the atmosphere we are increasingly seeing amongst those charged with rebuilding the line.

Growing concerns

We can reveal that for some time now a small but significant (and growing) group of worried L&B supporters have been contacting *NGW* to air their concerns over slow progress of the project and a perceived lack of information from those in charge – fears that now appear to have been realised.

Similar concerns have been expressed on various online L&B support groups – forums which in normal times only generate perhaps a couple of messages per week are now seeing multiple posts per day.

The tone of such comments is becoming all the more worrying, typical examples being calls for votes of confidence in the L&B's Trustees and to set up entirely separate restoration bids further south. Not only is there a separate organisation to the Trust, Exmoor Associates, acquiring land for the railway's revival, but more recently a third group has been set up, the Yeo Valley Trust, to advocate restoration of the southern section of the railway. It's a confusing situation and as has been proven more than once in the past, multiple groups restoring bits of the same railway is seldom a good thing.

The one aspect that comes out loud and clear from all this is a lack of communication – a growing number of L&B supporters feeling they are not being sufficiently informed as to what is going on and are not being listened to. As a Trustee of a heritage railway myself I know that there are always some gripes from members feeling the Trustees don't tell them everything, and to be honest we simply cannot. But generally we are trusted to work for the benefit of our railway and if we don't meet members' expectations we expect to be replaced at an AGM. But at the L&B the situation appears to be moving far beyond gripes.

I've no doubt that the L&B Trustees have no hidden agenda and are committed, as chairman Peter Miles has stated on one of the forums, to a single aim, restoration of the Lynton & Barnstaple Railway. But it is also clear that they are in danger of 'losing the room'. The members need to have their say and as Trustee Chris Duffell has stated, now is surely the time to pause, take stock and have a no-holding back consultation with all the members to ensure that everyone moves forward together with one aim – steam trains running over many miles of the north Devon countryside...

Enjoy your *NGW*...

Andrew Charman



Planning permission expiry stalls Lynton revival project

The restoration of the Lynton & Barnstaple Railway has suffered a major setback following the expiry of planning permissions for the next phase of the Devon line's extension.

The permissions expired on 7th March with no work having been done, after the Lynton & Barnstaple Railway Trust was unable to overturn onerous conditions imposed when planning approval was granted by the Exmoor National Park Authority (ENPA) in 2018.

Dubbed Phase IIA, the plans sought to extend the currently nine tenths of a mile-long line a further 3½ miles southwest from its current limit of operations at

Killington Lane to Blackmoor Gate, where the Trust has recently acquired the former station (see separate story).

The plans, which included a new depot on the opposite side of the A39/A399 junction to Blackmoor Gate, were approved by ENPA in March 2018. These added to approvals already given by North Devon Council in 2016 to reinstate a mile of line outside the national park, running from Blackmoor Gate towards Barnstaple as far as Wistlandpound. These approvals remain in place and some work has been carried out on this section.

Some 144 restrictions were, however, placed on the ENPA approvals, including

what are known as 'Grampian conditions' – promoted by local objectors to the revival scheme, these insist that no work can start on the extension until all land required is in the Trust's ownership, all funding secured and all contractors appointed.

Missing link

The Trust owns most of the land on the section but has so far been unable to acquire a short section at Parracombe. A 'Return to Parracombe' appeal which was launched in July 2022 has so far raised only £700,000 of the estimated £1.6m required to fund the first section of just under a mile from Killington Lane to Parracombe Halt.

In May 2021 the Trust sought legal advice to vary the conditions by means of what is known as a Section 73 application, which would permit staged development of the extension. In a newsletter issued to Trust members in February this year it was stated that various delays, including the effects of Covid, staff shortages and "procrastination by ENPA" saw the S73 application not submitted until May 2022.

The application then faced a legal challenge from local opposition, claiming that the S73 would be illegal. This was refuted by legal advice that was sought by the Trust and an amended S73 proposal was submitted to ENPA in December.

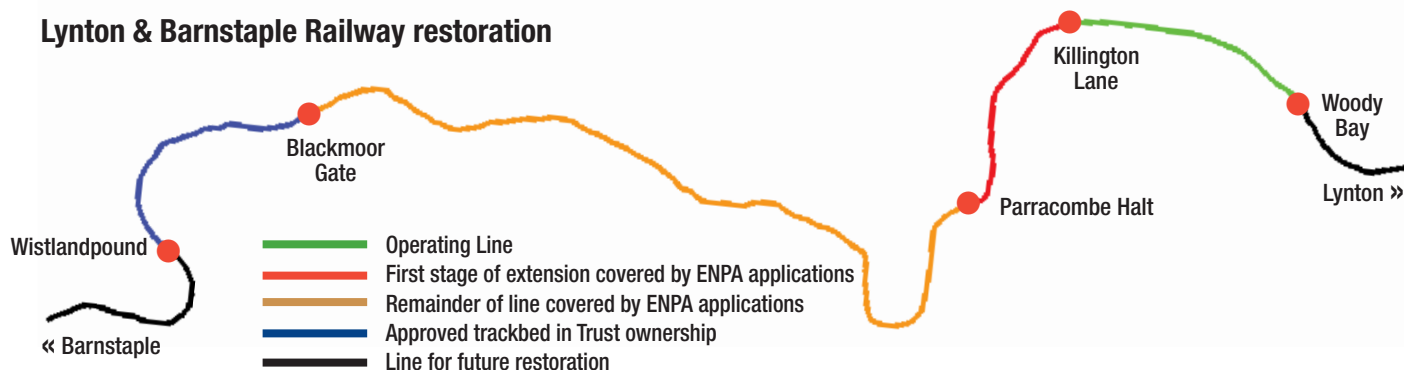
It then became clear, however, that any



Above: Much has been achieved at the Lynton & Barnstaple, as here at the Autumn 2022 Gala, but the next steps now look in serious doubt. *Photo: Stuart Chapman*

Left: The trackbed at Killington Lane, current limit of operations, stretches on invitingly at left but could remain undisturbed for some time yet. *Photo: Andrew Charman*

Lynton & Barnstaple Railway restoration



attempt to decide the S73 in the short period before the planning permissions were due to expire carried a high risk of refusal, which might impede future applications. So on legal advice the S73 was withdrawn, the permissions duly lapsing.

Angry reaction

The news sparked shock and some anger among L&B supporters, allegations made that the true situation had not previously been made clear to Trust members. Some called for replacement of the Trust board and a brief attempt was made to progress a vote of no confidence in the Trustees.

Others advocated changing the revival focus to either extend north from the current L&B headquarters at Woody Bay towards Lynton, or further south. The Trust owns no land north of Woody Bay but between it and Exmoor Associates, set up to acquire land for the revival, many stretches of trackbed to the south have been secured. Meanwhile the more recently established Yeo Valley Trust has been advocating restoration on the southern section of the once 19-mile line.

In March the L&B Trustees sent out a consultation to members, detailing four possible ways forward – abandoning attempts to extend in the National Park and instead extending south from Wistlandpound, heading north from Woody Bay towards Lynton, abandoning working with local authority planning and instead seeking a Transport & Works Order with deemed planning permissions, or progressing staged development of the existing plan initially on the land outside the national park that the Trust already owns and has permission to build on.

All were described as high cost and high risk with the exception of the staged development, which eight of the nine Trustees recommended to members. The ninth, Chris Duffell, who was only elected to the board in 2021 after irregularities in the voting papers forced the election to be postponed, abstained from the decision.

Mr Duffell then took to social media to explain what he described as many reasons for his abstention, including that the consultation document did not detail the costs of each proposal and whether the L&B Trust had the funds required, while the Trustees had shown no hint of contrition for the amount of funding expended on the now defunct permissions.

Chris Duffell added that his prime reason for abstention was that he disagreed with the four proposals in their current form and felt instead that the railway needs to pause and conduct a major review of its revival plans with the membership fully involved – the consultation did not include an option to reject all four proposals.

As *NGW* went to press the discussion and argument amongst L&B supporters over the project showed no sign of abating, with several predicting that matters will come to a head at the L&B Trust's AGM on 13th May.



Blackmoor station inn secured for L&B revival

The Lynton & Barnstaple Railway Trust has secured the Old Station House Inn at Blackmoor Gate, regarded as a crucial element in plans for the future reconstruction of the Devon 2ft gauge line.

An announcement on 16th March confirmed that contracts had been exchanged for the sale of the property to the L&B Blackmoor Company PLC, a subsidiary of the Trust, with the purchase set to complete on 17th April.

The pub and restaurant has been trading for many years in the former L&B station building which closed with the rest of the railway in 1935.

The Trust intends eventually to reinstate the building as a station while continuing to operate the current business – the new landlords emphasised that no radical

changes were planned, adding: “(we) assure all regular and occasional visitors to the Inn that it will continue to offer a warm welcome with quality food and drinks.”

A programme of investment is planned while respecting the historic nature of the station building.

Blackmoor Gate has a major status in the next stage of the L&B revival plans, with the railway intending to build a new depot on the opposite side of the A339 road to the station. Work on a new access road to the depot site, which the company owns, has recently begun.

Above: The distinctive Swiss Chalet design style of the Blackmoor Gate station building is still clear even with the later additions in its present guise as a pub and restaurant.

More shelter for Beddgelert passengers

The Welsh Highland Railway's Beddgelert station will soon have a proper building for the first time since the original WHR closed in 1937.

Snowdonia National Park Authority has approved FF&WHR plans for a new facility at the busy intermediate station – replacing a structure demolished when the line closed. Plans were originally approved 10 years ago but after starting work on the foundations funding issues curtailed the project.

The plans have now been re-visited and

modifications made to the design, including improving the environmental aspects and giving the building a lighter, lower visual aspect which fits in with its surroundings.

The new building will include a covered waiting area, staff room and toilets, a ticket office with a small retail area and coffee shop, and external covered seating. The existing covered shelter will be removed.

Below: Artist's impression of the new Beddgelert station.





Bagnall Sinembe arrives at Teifi line

A surprise arrival at the Teifi Valley Railway on 4th April was Bagnall 4-4-0T 'Sinembe' (2287/1926).

Nothing further had been heard of the loco since late 2013 when it was offered for sale in newly restored condition by Graham Walton-Binns, alongside sister loco 'Charles Whytock'. The latter subsequently was sold to a member of the Lynton & Barnstaple Railway, where it remains to this day.

The two were among 13 similar Bagnalls that worked at the Tongaat Sugar Company in Natal, South Africa – 'Isibutu', now at the Statfold Barn Railway, was another member of the class.

Mr Walton-Binns imported Charles Whytock and Sinembe with a third loco, 'A. Boulle', after they had been plinthed in Natal for some 30 years. From 1994 the first two were displayed at the Welsh Highland Heritage Railway – some work was done on Sinembe but it was then stored on a siding at Pen-y-Mount in

declining condition until in 2005 it was removed to Teeside for restoration.

A TVR spokesperson told *NGW* that basing Sinembe at the south Wales 2ft gauge line had been made possible by a mutually acceptable agreement with Mr Walton-Binns and his son Alex.

The initial agreement is over five years and while there are no confirmed plans at present to steam the loco, the biggest so far at the TVR, "this could change if our finances and volunteer base improve – Sinembe has been pressure tested but not steam tested."

The line plans a carriage shed extension to house carriages displaced by Sinembe. TFR resident steam loco, quarry Hunslet 0-4-0ST 'Alan George' (606/1894) is expected back in service next season following the completion of an overhaul.

Above: 'Sinembe' arrives at the Teifi Valley Railway on 4th April. Photo: TVR



Former Thorpe loco heading for Hayling

The Hayling Light Railway is set to welcome a new member to its motive power fleet in the form of a 1984-built Alan Keef diesel, works no 11.

Built for the 'Treasure Island Railway' at the Thorpe Park resort near Chertsey in Surrey, the loco has been donated to the railway by Thorpe Park owner Merlin Enterprises. It worked on the theme park line for more than 20 years, its final role being as a rescue loco on the later 'Canada Creek Railway' which closed in 2011.

The loco is currently in Keef's Ross-on-Wye workshops, undergoing overhaul and modification for its new role. It will be rebuilt with a steam-outline body in similar form to the HLR's existing loco 'Jack' and it is hoped that it will arrive on Hayling Island in time to celebrate the mile-long 2ft gauge line's 20th anniversary in September.

"We would like to give a big thank you to the staff and owners of Thorpe Park Resort, for their help in securing the loco, and in facilitating the recovery by our volunteer gang of track materials from their site," an HLR spokesman said. "This will help us to keep the Hayling Light Railway running for many more decades to come, bringing enjoyment to future generations of Hayling Island residents and visitors."

The HLR Trust is welcoming donations towards what will be an extensive and thorough restoration, but is equally keen to attract new volunteers in roles across the line. More details are on the website at <https://haylinglightrailway.wixsite.com/ehlr>

Above: The former Thorpe Park loco prior to restoration at its builders. It will be rebuilt to something close to the design concept below. Images: HLR



■ The extensive work currently underway at the Ffestiniog Railway's Boston Lodge depot has included a major extension to the Carriage Works including a new woodworking shop. The entire complex with its solar-panel roof is visible in this picture taken on 7th March. Great progress is also being made with the line's new Small Locomotive Shed, sited to the right out of picture. The foundations were completed in February with erection of the structure expected to be underway by the time of the Ffestiniog Railway Society AGM on 29th April. Photo: Chris Parry/FFWHR



Trust makes urgent bid to keep Darj loco in UK

Supporters of the Darjeeling Himalayan Railway have formed a Trust to try and keep the line's only locomotive outside India in the UK.

Sharp Stewart 0-4-0ST+T no 19B was built in Glasgow in 1899 as one of the original batch of four B-class locos for the DHR. It is due to be auctioned on 21st June along with the entire assets of the Beeches Light Railway, a private 2ft gauge line built around his Oxfordshire home by major rail industry figure Adrian Shooter.

Adrian rescued the locomotive from the USA and had it returned to steam at Tyseley works in Birmingham in 2003, then building the Beeches line to run it on.

Sadly he died on 13th December last year and now his widow is selling off the railway, which includes 19B, two bogie Darjeeling-style carriages built by the Ffestiniog Railway, and even a replica of one of the Indian line's station buildings.

The Darjeeling Tank Locomotive Trust has been formed by the locomotive's support crew who for many years assisted Adrian at open days held on the Beeches line. Further support has come from members of the Darjeeling Himalayan Railway Society, the very highly regarded UK group that does much good work to aid the DHR, and the Ffestiniog & Welsh Highland Railway.

The aim is to raise sufficient funding to allow the Trust to successfully bid for the loco at the auction and to ensure there is no danger of it being bought by a party outside the UK and being exported. There are high hopes that 19B will be offered as a single lot with the two carriages.

Assuming the bid is successful the Trust has secured agreement to base the loco at the Statfold Narrow Gauge Trust in Staffordshire. It will operate on Statfold's line and will also have access to the engineering facilities when the loco's overhaul becomes due.

The Trust would also wish to continue

Adrian's programme of taking the loco 'on tour' – 19B has run on several lines in the past including the Launceston Steam Railway, Leighton Buzzard Railway, Ffestiniog & Welsh Highland Railway and at Statfold.

19B has a current boiler certificate, though it is approaching its 10-year overhaul. The loco retains its original Lowmoor iron boiler, described by Trust members as the oldest operational locomotive boiler in the world.

The Trust is seeking to raise the funding required in various ways. These include loans of ideally £10,000 which will be paid back over a five-year period, and standing order covenants of £25 over the same five years to enable repayment of the loans. Donations of any amount will also be welcomed.

Details of the campaign and information on how to contribute can be found on the Trust's newly set up website at www.darjeelingtank.org.uk.

The auction, to be conducted by H J Pugh & Co, will include a wide selection of railway memorabilia. More details are in the advertisement on page 45 of this issue.

Above: Seen here on 13th February on the Beeches Light Railway, 19B's future in the UK may well depend on the fund-raising success of the newly-formed Trust. *Photo: Joey Evans*

Below: It is hoped the two Darjeeling-style carriages will be included in the auction with 19B as a single lot. *Photo: Andrew Charman*



BRIEF LINES

Marchlyn to Bressingham

Statfold-based Avonside 0-4-0T 'Marchlyn' will be the star visitor at one of the first Gala weekends of 2023 at Bressingham Steam Museum on 29th April-1st May. The loco is likely to work slate trains on the Norfolk museum's 2ft gauge line, one of four gauges in action over the weekend which is also likely to feature around 20 road steam locos.

Gala marks Esk centenary

Also enjoying a Spring Gala over the 29th April to 1st May weekend will be the Ravenglass & Eskdale Railway, celebrating the 100th anniversary of Davey Paxman 2-8-2 'River Esk'. A centenary exhibition will on display in the Ravenglass Railway Museum while the weekend will also feature all of the Cumbrian 15-inch gauge line's operational locos including museum resident, 0-4-0T 'Katie'.

Sybil closer to return...

Restoration of the West Lancashire Light Railway's former Dinorwic Quarry Bagnall 0-4-0ST 'Sybil' passed a milestone on 13th May when the loco's boiler completed a successful steam test.

...as Pixie progresses at FR

Meanwhile the overhaul of 'Pixie', another Bagnall 0-4-0ST formerly owned by the Reverend Teddy Boston, is making good progress at the Ffestiniog Railway's Boston Lodge works ready for the loco's return to the Richmond Light Railway. Tickets for the Kent line's annual charity steam fair on 12th August are now on sale at www.ticketsource.co.uk/rfr

Bess back at Ruislip...

The Ruislip Lido Railway's Gala on 13th-14th May is set to feature a visiting steam loco alongside the 12-inch gauge north London line's recently overhauled resident Penrhyn-style Hunslet 'Mad Bess'. More details of the event are at www.ruislipidorailway.org

...and the Doctor at Romney

Romney, Hythe & Dymchurch Railway Canadian Pacific Railway-style 4-6-2 'Doctor Syn' has returned to action on the Kent 15-inch gauge line this season, having been under overhaul over the last five years. Cracks found around the boiler clacks slowed the overhaul, but the loco hauled its first train on 2nd April.



Narrow gauge projects triumph again in the Heritage 'Oscars'

The annual awards presented by the Heritage Railway Association (HRA) have again produced a clutch of winners among the body's narrow gauge members.

One of the most popular recipient of an award this year, presented at the HRA annual dinner in Birmingham on 11th March, was the Sittingbourne & Kemsley

Light Railway. Among the smaller organisations shortlisted, the S&KLR took the Coiley Award for Steam Locomotive Engineering for its restoration of Kerr Stuart 0-4-2ST 'Premier', featured in *NGW* 167.

The Ffestiniog Railway's long-running programme of operating gravity slate trains was recognised with the Peter Manisty Award for exceptional achievements in the heritage railway world.

The programme now extends to more than 200 wagons and has its own bespoke shed at the FR's Minffordd engineering base. "The efforts of this team over more than 30 years are truly outstanding and it is more than overdue that their contribution to railway conservation should come into focus," FF&WHR general manager Paul Lewin commented.

The Morgan Award for rolling stock preservation

saw more success for the Lincolnshire Coast Light Railway, winning for its restoration of the 1927-built 'Queen Mary Coach' from the Nocton Estates Railway.

The Tallylyn Railway was again successful in the Marketing & Communications category, winning for its Covid-inspired venture into video production, while the Hampton & Kempton Waterworks Railway won the Infrastructure award for its new carriage and maintenance shed, which replaced converted shipping containers on the west London line.

Above: Delight amongst the members of the Sittingbourne & Kemsley Light Railway after they received an HRA Award for their restoration of 'Premier'.

Photos: HRA and Andrew Charman

Left: The Ffestiniog Railway gravity trains have long been a major attraction on the line.

Photo: Chris Parry/FFWHR

Below: The 'Queen Mary Coach', seen on the end of this Lincolnshire Coast Light Railway train, was another award winner. *Photo: LCLR*



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Rapid progress on Talyllyn overhauls

Overhauls being carried out on both of the Talyllyn Railway's original steam locomotives have been making good progress in recent weeks.

Reassembly and painting of Fletcher Jennings 0-4-0WT 'Dolgoch' has been proceeding rapidly despite the discovery of a crack in the left-hand connecting rod leading to a decision to manufacture two new rods. TR engineering manager Chris Smith told *NGW* he did not expect this job to unduly delay completion of the loco's overhaul as the rods could be among the last components fitted to it.

Fletcher Jennings 0-4-2ST 'Talyllyn' has been having its redesigned braking system fitted to the frames in the loco shed at Tywyn Pendre. The cylinders have been remachined and their new liners shrunk in.

Meanwhile Talyllyn's boiler is being retubed at the Mid Hants Railway in Hampshire. Southern-based TRPS members have been assisting with the boiler work.

The TR's former Corris Railway Kerr Stuart 0-4-2ST 'Edward Thomas' was set to head to the Vale of Rheidol Railway's workshops at Aberystwyth on 17th April for overhaul. Chris believes that the work is likely not to be extensive, only involving retubing the boiler and reboring of the cylinders. The VoR has previously overhauled Hughes 0-4-2ST 'Sir Haydn' for the TR.

Above: Talyllyn Railway engineering staff take a break from their reassembly of Fletcher Jennings 0-4-0WT no 2 'Dolgoch'. Photo: TR

Talyllyn's new top team enjoys a strong start

The Talyllyn Railway started its 2023 operating season with February half-term trains and a new team in charge following the departure of General Manager Stuart Williams to take up a new post in the Orkney Islands.

Two of the TR's senior managers, Lorraine Simkiss and Liz Porrett are now sharing the GM role with Lorraine taking responsibility for all income generation and human resource matters for staff and volunteers while Liz oversees operations, engineering and developments.

The two took over the reins after a strong 2022 season for the mid-Wales 2ft 3in gauge line, especially during December and the period between Christmas and New Year which was described as exceptional, with the 'Tinsel and Turkey' trains proving very popular.

"We said farewell to Stuart thanking him for all his hard work and effort and recognising the significant positive impact he had made to the railway in his time here and wishing him all the very best on his new adventure in Orkney," Lorraine said. "For Liz and I (it's) an exciting opportunity to lead and guide the railway amidst uncertain times.

"These changes are undoubtedly the way to keep us progressing as an organisation. When we want to thrive rather than just survive in these times; standing still is never an option."

TR chairman David Ventry added that the line had been delighted to appoint Lorraine and Liz to their new management roles. "They will provide continuity and stability in uncertain times, and we are confident that they will successfully lead the railway forward."



■ Talyllyn Railway engineering manager Chris Smith has praised the line's Young Members Group following a winter wagon painting working party which saw four of the line's heritage wagons repainted to a good standard.

"It was very well attended by the Young Members Group – they did a very good job and even tidied up after themselves!" Chris said, adding; "They enjoyed it and we will be doing it again – I was very impressed with our young members which bodes well for the future."

Photo: TR

Programme announced for Manx 150 celebrations

The Isle of Man Government's Department of Transport has published details of the programme of celebrations that it will stage this year to mark the 150th anniversary of the opening of the island's steam railway, and the 130th anniversary of the Manx Electric Railway.

Three distinct events will begin on Saturday 1st January, the exact 150th anniversary of the opening of the Dougal to Peel line, first section of the Isle of Man Railway.

Highlights will include the unveiling of an anniversary plaque at Douglas station, a locomotive line-up in the station and a double-headed departure from Douglas.

A week-long festival to be staged between 23rd and 30th July will showcase both the steam and electric lines but the centrepiece of this event is likely to be the display of two locomotives at locations formerly served by the railway.

Locos at closed stations

Former Manx Northern Railway no 4 'Caledonia' will be placed on a track panel at the former Knockaloe internment camp while IMR no 8 'Fenella' will be displayed in similar fashion at the former Peel station on the island's western coast.

A host of other events during the week will include parallel runs, motorman taster sessions and the appearance of special car and trailer line-ups on the Manx Electric Railway, behind-the-scenes visits and bus tours of closed lines, and a cavalcade on the MER, while the island's other lines – the Groudle Glen and Great Laxey Mine railways and the Douglas Horse Tramway, will also host special events.

Then on Sunday 10th September a commemorative day will be held to mark 130 years since the first section of the MER opened on 7th September 1893, between Derby Castle in Douglas and Groudle Glen.

A second plaque unveiling will take place at Laxey, which will also host a Victorian Fayre. Parallel runs and tours will again be part of the day's programme.

Full information on the entire anniversary programme are available in a special leaflet that can be downloaded from the island's transport website at www.rail.im



Rhiw Valley assets go up for sale after line closure

The locomotives, stock and track of the Rhiw Valley Light Railway have been put up for sale following the closure of the private 15-inch gauge mid-Wales line last October.

As reported in *NGW* 171, the line held its final open day on 2nd October and record numbers of visitors attended the event in splendid weather, the final runs being with resident steam locomotives 'Powys' and 'Jack' double-heading in both directions around the 1383-yard long line.

Owner Mike Woodroffe reluctantly decided to sell up following the passing of his mother Helena, widow of the line's founder Jack Woodroffe, who had basically subsidised the railway since Jack's death in 1990. While money was a factor, Mike said that running the railway had not been the same since Helena's passing.

The entire assets of the railway are now for sale, including the 0-6-2T Powys, built by Severn Lamb in 1973, 0-4-0 tender loco Jack, completed by Severn Lamb in 2003, and a petrol shunter, 'Monty'.

Also included are three bogie and two four-wheeled carriages and a four-wheeled guard's van, various wagons and accessories and an amount of engineering equipment and railwayania. But the track is not available, an offer already having been accepted for it.

Full details of the assets are available on the updated website at www.rvlr.co.uk and serious enquirers only can contact Mike on mike.woodroffe@gmail.com

Above: Both of the line's resident steam locomotives, 'Jack' (Left) and 'Powys', are included in the sale. Photo: Andrew Charman



■ The long-term project at the Ffestiniog Railway to restore a Kerr Stuart diesel that worked on the neighbouring Welsh Highland Railway has taken great strides in recent weeks, with work starting on the construction and fitting of the loco's distinctive bodywork.

Kerr Stuart 4415 is a unique loco, the oldest purpose-built diesel in the UK. When first built it underwent trials on the WHR and became the first of a class of locos of which is now the only survivor. The restorers have decided to replace the loco's McLaren engine with a Gardner unit, after restoration of the unit originally sourced for the loco proved too difficult and costly.

KS4415 will be on display at the WHR 100 celebrations on 23rd-25th June.

Photo: KS4415 team



Sanders fitted to Corris loco

The Corris Railway's new Hughes 'Falcon' steam loco no 10, which is being built by Alan Keef Ltd and was successfully steam-tested at the South Wales engineers in September, is to be fitted with sanding gear, a feature its inspiration, the three original locos built in 1878, never carried.

Considering the gradients and sharp curves of the original Corris route it is surprising that sanders were never fitted, even under the Great Western Railway's ownership during which surviving original loco no 3 was overhauled in Swindon.

Instead until the line's closure inclement weather would see the loco fireman or guard perched above the front bufferbeam, dropping sand from a box placed in front of the smokebox to the railhead using a watering can.

Such an approach would not be permitted in today's health & safety conscious environment, even if the modern-day Corris wished to carry on this way, so air sanding gear will be fitted to no 10 before it enters traffic.

The new loco's cab, bolted together for the first public steaming, has now been riveted. The upper rear section will retain removable bolts to improve footplate conditions on hot days and allow passengers in the front carriage to watch the crew at work. It will also recreate the look of the Falcon cabs in the early 1900s.

Other outstanding work at the end of February included the fitting of the air braking equipment and the electronics for the train communications system. The boiler and the cylinders need to have their cladding made and fitted.

No 10 is making good progress towards its final testing and entrance into Corris traffic later in 2023 but some funding still needs to be raised.

Donations can be made online at www.corris.co.uk or cheques, payable to Corris Railway, can be sent to Peter Guest, 38 Underwood Close, Callow Hill, Redditch, B97 5YS. Peter is also able to provide standing order payment forms.

Fairbourne Gala to star Prince & Albert

The Fairbourne Railway's traditional Gala weekend over the Spring Bank Holiday will see the Mid-Wales line's 15-inch gauge rails in action again, hosting one of the historic 'Barnes Atlantics'.

'Prince Edward of Wales', based at the Rhyl Miniature Railway, is one of the original Bassett Lowke class 10 4-4-2 locos – built in 1909 it was the first engine at Rhyl, working the north Wales line until it was replaced in 1920.

The loco later narrowly avoided being scrapped until it was bought by the Rhyl Steam Preservation Trust and returned to the line in 2014. It has since been restored to operation and rebuilt into the form it ran in between 1911 and 1969.

The loco carries the same name as the first engine to run at Fairbourne in 1916. This was a later more powerful version of the Rhyl engine – it was sold on in the 1920s and later severely damaged by fire. All six of what are known as the Barnes Atlantics, after an energetic early manager at Rhyl, have undergone many changes of identity over the years.

Fairbourne management also plan to have a 15-inch gauge Sutton Miniature Railway railcar at the Gala while on the 12¼-inch gauge diesel-electric loco 'Albert' will make a return visit from the Littlehampton Miniature Railway. It is hoped the loco will double-head passenger services with Fairbourne's recently acquired Simplex loco – regauged from 2ft, this has never previously hauled a passenger train.

Full details of the Gala are on the line's website at www.fairbournerrailway.com

Top: 'Prince Edward of Wales' will perform on the Fairbourne's 15-inch gauge line over the Gala weekend, and it will be joined by the distinctive former Sutton Miniature Railway railcar (below).

Left: The Fairbourne's most recent recruit, a regauged Simplex should see its first passenger trains over the weekend.

Photos: FR



Statfold to open five days a week

Enthusiasts who have never managed to visit the Statfold Barn Railway now have more opportunity as from 19th April the Tamworth museum, formerly only accessible on three enthusiasts days a year, began opening from Wednesday to Sunday each week.

At open days trains will be running on the 2ft and new 12¼-inch gauge lines and the Roundhouse museum collection will be open. Details are at www.statfold.com



Bredgar makes progress on Katie overhaul

Considerable progress is being made with the 10-year overhaul of Arn Jung 0-6-0WT 'Katie' (3872/1931) at the Bredgar & Wormshill Light Railway (writes *Samuel Jones*).

The loco had been a regular operator on the Kent 2ft gauge line before being withdrawn for overhaul at the end of last year. In recent weeks the removal of the chimney, blastpipe and steam turret have been followed by lifting of the cab so that the chassis can be overhauled.

Katie is Bredgar's longest-serving locomotive. It spent its working life in Western Africa before arriving at the Kent line in 1980.

The line has an extended opening programme this year with Wednesday 'diesel days' added to the usual monthly open weekends. The first steam event day will be on 7th May, dubbed 'Bluebells' as the line's woodlands surroundings are full of the flowers at this time of year. More details are at www.bwlr.co.uk

Above: Bredgar Arn Jung Katie makes her last public appearance at the Bredgar & Wormshill Gala in October 2022 before undergoing overhaul. *Photo: BWLR*

Buzzrail records festive record

The Leighton Buzzard Railway began its 2023 operating season on Sunday 12th March having enjoyed a record-breaking Christmas period in which a total of 6316 passengers were carried.

The first train service of 2023, the 10.30 departure from Page's Park was ceremonially flagged away by Leighton Linslade's Deputy Mayor Cllr Sheona Hemmings.

More UK and World news on pages 36-37

Greenway threat to Donegal revival?



Plans for a pedestrian, cycling and riding greenway on the County Donegal Railways trackbed through the scenic Barnesmore Gap could block future plans to re-open part of the route as a heritage railway (writes *Hugh Dougherty*).

Donegal Railway Heritage Centre members and supporters have been among those responding to the public consultation on the proposal for the section, between Ballybofey and Donegal Town.

The centre has been in talks with Donegal County Council about the possibility of including a narrow gauge railway track with the greenway, in similar fashion to Northumberland's Aln Valley Railway between Alnwick and Alnmouth.

The section through the Gap, running on a ledge cut into the hillside, had closed with the rest of the CDR on 21st December 1959 and the track was lifted in 1960. But the bulk of the trackbed has remained intact and distinct while its re-use as a mainline railway has been mooted as part of rail lobby group, Into the West's popular and growing campaign to reconnect Donegal to the Irish national system.

The former South Donegal Railway Restoration Society planned in 1991 to restore just over ten miles of the railway to link the site of the former Meenglas Halt with Barnesmore Halt, including the most scenic part of the line through the Gap.

The plan failed because of a proposed Ballybofey bypass scheme which would have used part of the old railway route past Lough Mourne, while a geological issue at the foot of the Gap and the objections of a resident close to the route sealed its fate.

The successor to the SDRRS, Donegal Railway Restoration CLG which operates the heritage centre, has since reaffirmed its aim of operating heritage CDR stock, with several options, including the Barnesmore Gap route, being actively considered.

Donegal Railway Heritage Centre manager Niall McCaughan has encouraged all our members, supporters and the general public to get involved in the consultation and said that there has been a good response. "It's important that the railway and its place in the Barnesmore Gap is recognised and it may be possible to build the greenway in such a way that it does not rule out a heritage line on the existing CDR trackbed," he said.

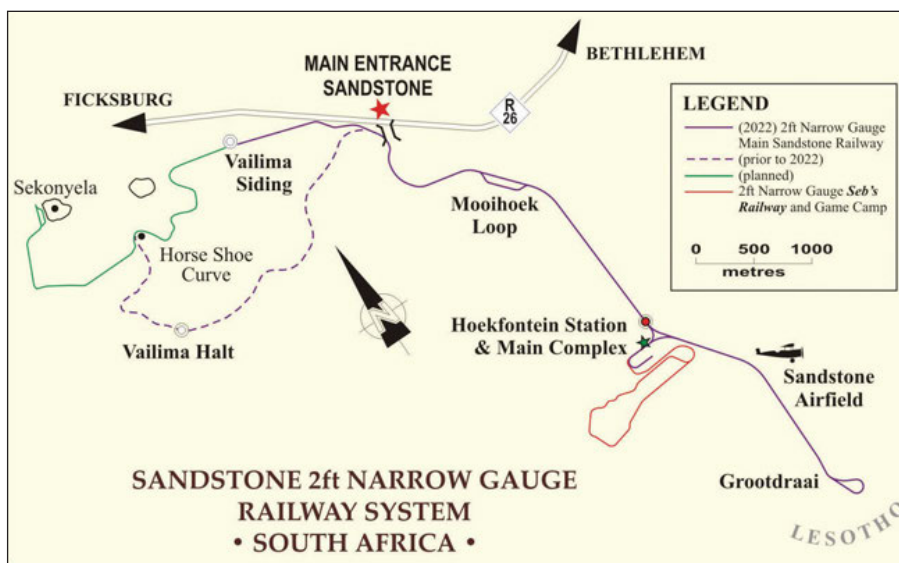
Above: Pictured in Barnesmore Gap on its return to the Donegal Railway Heritage Centre following restoration in October 2021 is CDR 2-6-4T no 5 'Drumboe' – the line of the railway can be seen on the hillside above the loco.

Below: What could be on offer. CDR 2-6-4T no 2 'Blanche' lifts its heavy mixed train through the Barnesmore Gap in May 1956.

Photos: DRHC



Sandstone extension to revive photo favourite



SOUTH AFRICA

The Sandstone Steam Railway near Bethlehem is to add a significant extension to its 2ft gauge running line, replacing a section of line lifted in late 2021 after damage by inclement weather.

The loss of the mountain section was expected to be keenly felt by enthusiasts at 'Stars of Sandstone' steam festivals, photo opportunities of double-headed NGG16 Garratt-hauled trains long a highlight of the events. But the weather damage left the section needing major repairs with no guarantee that similar damage would not re-occur. So it was lifted, the line ending at a point called Vailima Siding.

Picture perfect

Sandstone management decided instead to revive a rail project shelved many years ago – extending to a location called Sekonyela, one of the most photographed landmarks in South Africa and a magnet for steam photographers in the heyday of the Bloemfontein to Bethlehem line when operated by Class 25NC 4-8-4 locos.

Using 2ft gauge track lifted from the mountain section the line is being extended west towards Sekonyela, where it will terminate with a run-round loop in the farm of the same name.

The 4.3km long route follows the remaining piece of the mountain section as far as a sharp left turn called Horse Shoe Curve. The new line will veer slightly to the right halfway around the curve and then swing further right in a northerly direction towards Sekonyela farm.

Tracklaying was set to begin imminently with completion by the end of 2023.

Top: Sekonyela provides a distinctive backdrop during the last year of steam passenger services on the Bloemfontein to Bethlehem line in 1985 – South African Railways Class 25NC 4-8-4 passes Vailima with what is now Sandstone Estates at left. *Photo: R L Bucher*

Above left: Map of the Sandstone Steam Railway – the new extension to Sekonyela is at left in green.

Gales lead to Swiss derailments

High winds in **SWITZERLAND** on 31st March led to two separate derailments on metre-gauge lines, injuring 15 people.

At around 4.30pm one carriage of a train was blown onto its side near Lüscherz station and slid down a small embankment. Three people in the carriage, including the driver, were taken to hospital.

Around 20 minutes later and 40 kilometers from the first incident a train of the Regionalverkehr Bern-Solothurn RBS was derailed in Bürenen. Around 50 people were on the train and 12 were injured, one seriously.

Local authorities stated that weather measuring stations recorded gusts of 136km/h around the time of the incident.





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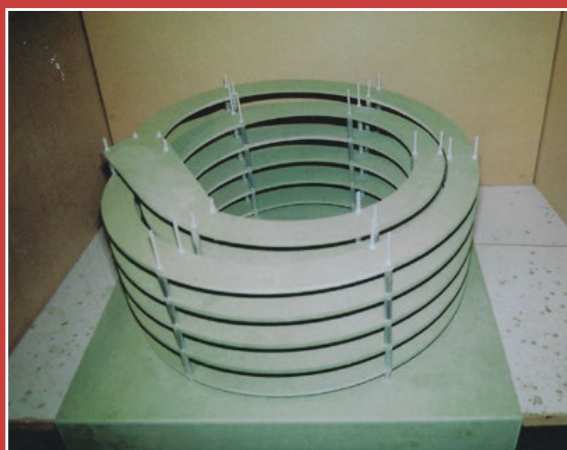


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21-year effort secures Indonesian survivors

Negotiations lasting more than 21 years finally bore fruit for the Stoomtrein Katwijk Leiden in Valkenburg on 29th March when two Dutch-built steam locomotives were loaded in Surabaya, Indonesia, for repatriation to the museum.

The two were both built by Du Croo & Brauns for the Indonesian sugar cane industry. Works no 81 of 1925 is a 90hp 0-8-0 tender loco with Klien-Lindner articulation and last worked at the Sumberhardjo sugar factory in North Java.

The second, no 159 of 1928, is an 80hp Mallet that was originally delivered to the Koenir sugar factory, owned by the Handels Vereeniging company of Amsterdam. Later

NETHERLANDS

the loco moved on to work at the Pesantren

Baru sugar factory.

Having first began trying to secure the locos around the turn of the millennium, the museum finally received approval for their repatriation from the Indonesian government in January. A crowdfunding campaign has now been launched, both to pay the estimated transport costs of 60,000 Euros (about £52,000) and to finance construction of a new boiler for one of them, costed at 70,000 Euros.

Also heading to the museum with the locos are four sugar cane wagons and a Dutch-made Simplex rail bike.

Photo: Stoomtrein Katwijk Leiden

New carriages debut on Alishan

The Alishan Forest Railway in TAIWAN has launched a new train set comprising six carriages constructed in cypress wood.

Taking two years and nine months to complete, the vehicles were built with steel frames and bodies of red and Taiwan varieties of cypress – a rare wood that only grows in Japan, the USA and Taiwan.

The red cypress wood is said to be both very uniform in consistency and highly

resistant to decay and insect damage.

The carriages include significantly larger windows than standard Alishan stock, domed ceilings and leather seats. The aroma of the wood is encouraged in the interior and Alishan management hope that passengers travelling in them will appreciate the connection between the 2ft 6in gauge line and its forested mountainous surroundings.

Photo courtesy Tsai Wen-chi, AFR, and AFR



Motor museum loco heads for Somme

The Petit Train de la Haute Somme in has welcomed a new locomotive to its collection following the arrival on 28th February at its headquarters in Froissy, northern FRANCE of a diminutive 0-4-0T built by French maker Weidknecht in 1912.

What is described as a "unique and small 60cm gauge locomotive" weighing only 3.8 tons, was originally employed at Grandes Tuileries de Bourgogne in Burgundy.

In the 1960s the locomotive was bought by Henri Malartre who ran a motoring museum at Rochetaillé-sur-Saône, near Lyon. While the museum is renowned for its superb collection with some of the exhibits displayed in a castle, the locomotive never found a real place and was displayed outside for almost 60 years.

This resulted in the Froissy museum asking if the loco could be placed on loan in order for a cosmetic restoration to be undertaken followed by display in the line's own museum, and the Lyon City Council eventually agreed to the move.

"It's now in our workshop, where we'll have a hard task to overhaul the connecting rods and cylinders to allow it to move on track," Froissy spokesman David Blondin told NGW. "It will be on display during our steam event on 28th May 28th and will find a good place in our museum after a complete repaint," he added.



■ The destination of Decauville 0-4-0T 'Edgar', built by North Bay Railway Engineering in 2017 for rail book and model specialist John Sutton and until recently at the Apedale Valley Railway, has been revealed. John has sold the loco to the Tacot Des Lacs, a preserved section of a former 600mm gauge industrial line in Bourron-Marlotte, north-central FRANCE.

Photo: Patrick Mourot/TDL

■ Derelict 2ft 6in gauge stock has been transferred from long-term storage at Dematagoda shed in SRI LANKA to undergo restoration for display in the southern Asian nation's new railway museum in the capital, Colombo. The British-built items moved include Hunslet 0-6-0 P1 class diesel no 530 and one of the three surviving Sentinel steam railcars. Examples of the former system's Hunslet J2 4-6-4T steam loco are already in the new museum.

The UK-based Narrow Gauge Sentinel Railcar Preservation Trust is still pursuing efforts to acquire one of the railcars for restoration in Britain.

Photos: Budhikka Sampath/NGSRPT



West Coast in wilderness to enable track upgrades

Businesses around the West Coast Wilderness Railway in Tasmania have seen tourism hit after the 3ft 6in gauge line was closed to allow major improvements.

The State Government announced a three-month closure over the continent's winter tourism season to allow major upgrades, made possible by \$16 million (equivalent to around £8.5m) of state funding.

The work on the 34.5km long 3ft 6in gauge rack line, a preserved section of the former Mount Lyell Railway, followed a derailment of a diesel loco on 28th February. The incident was minor with no injuries among the 50 passengers, but it sparked an investigation into the condition of the line's infrastructure.

Minister for Infrastructure and Transport, Michael Ferguson, said the upgrades would improve the reliability and resilience of the line and would be carried out to meet what the State Government predicts will be continued future growth in demand for the WCWR.

"Management have been carefully mapping out and progressively addressing the maintenance and upgrade needs of the railway and locomotives while trying to keep the trains

AUSTRALIA

on the tracks, but the age of the infrastructure and the work that needs to be done calls for a more intensive, programmed work plan during the quieter time of year," the Minister said.

"It means the shutdown will be during the low-patron winter season – nonetheless it was a hard decision to make, but a necessary one and strongly recommended by local tourism representatives to get the best result."

The shut-down has seen all services to the end of the line at Strahan cancelled, and these will not run until the project is completed. Services around the other terminus at Queenstown are set to stop from 5th June.

West Coast Wilderness Railway acting general manager Ian Robertson said the entire railway would be shut for a time, but one end might open before the other.

"By tackling the necessary upgrades head-on and in one go we will add decades to the operational life of the railway and the local economic benefit it brings," he said.

Local tourism operators appreciated the need to make upgrades, but also questioned why the work did not take place during the Covid period when the line was closed.



■ Harzer Schmalspurbahnen GmbH (HSB) 'Brockenlok' 2-10-2 no 99-7232 has returned to the line's base in Wernigerode, GERMANY after extensive repairs in Meiningen, Thuringia. The 1955-built loco was sent to the works after suffering a broken connecting rod in January 2022, and underwent a major inspection. The return trip saw 1897-built Mallet 99-5902 (at left in the picture) heading to Meiningen for overhaul after several years of storage. The intention is that the loco is available for services next year celebrating 125 years of the Harzquerbahn and Brockenbahn. The loco is one of four original Mallets on the Harz, three of which are considered operational.

Photo: Dirk Bahnsen/HSB

BRIEF LINES

Dutch O&K for Maldegem

The steam centre at Maldegem in BELGIUM has announced the programme for this year's Steam Festival on 6th and 7th May with two steam locos working the 2ft-gauge line alongside four on the standard gauge. Orenstein & Koppel 0-4-0T no 6900 of 1913, normally resident at the Leemspoor line at Rijsen in Holland, will be the visitor on Maldegem's narrow gauge line. More details are at www.stoomtreinmaldegem.be/en

Golden days at Pichi Pichi

The Pichi Pichi Railway in the Southern Flinders Ranges, South AUSTRALIA will celebrate its 50th anniversary this year. The 3ft 6in gauge 24-mile long line, totally run by volunteers, began its milestone season in March with a series of twilight services. On 22nd and 23rd July commemorative trains will run for members.

Darj on the up

The Darjeeling Himalayan Railway in INDIA has reported impressive traffic figures, up more than 61 per cent on the last pre-Covid year of 2018-19. Earnings over the 2022-23 financial year totalled 19.21 crores (around £1.9 million).

Mixed-gauge model theme

JAPANESE model railway maker Kato, which markets Ffestiniog Railway England 0-4-0s in 009 scale in partnership with UK maker Peco, is reported to be building a new factory with a private theme park attached boasting 762mm and 610mm gauge railways.

Readers can send in news from overseas lines for inclusion on these pages – contact details are on page 3

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Mountaineer and Gowrie



James Waite is impressed with progress in building replicas of 'first' and 'last' locos in the saga of the North Wales narrow gauge.

The Ffestiniog Railway has long been noted for the longevity of the locomotives built for it in the years before preservation. Two out of its four original Double Fairlies have survived, along with one of the two 'Large England' 0-4-0 tank and tender locos and, most remarkably of all, three of the four original 1863 Small England 0-4-0T+Ts.

The Small Englands are some of the earliest narrow gauge locos to have been built anywhere in the world, though they have over the years been substantially reconstructed and altered. The only former Welsh Highland Railway loco to survive is 2-6-2T 'Russell', built for the never-completed Portmadoc, Beddgelert & South Snowdon Railway and subsequently sold to WHR predecessor the North Wales Narrow Gauge Railway.

Early and late

'Mountaineer', the other Small England, was the first steam loco to arrive at Portmadoc and to run on the FR, but it ended its days as early as 1879. At the other end of the timescale the NWNGR's 1908-built Hunslet 0-6-4 Single Fairlie tank 'Gowrie' was the last loco to be constructed for any of these railways before preservation, but was withdrawn after only a few years' service. It was also the last Fairlie of

Above: 'Gowrie' in 5-inch gauge. Marion Allen's beautiful model on display during an open day at Statfold. Photo: Joey Evans

Below: Jonathan Clay's painting gives a good idea how 'Mountaineer' will look when built, although some details such as its colour have yet to be decided.

Photos in this feature by James Waite except where credited

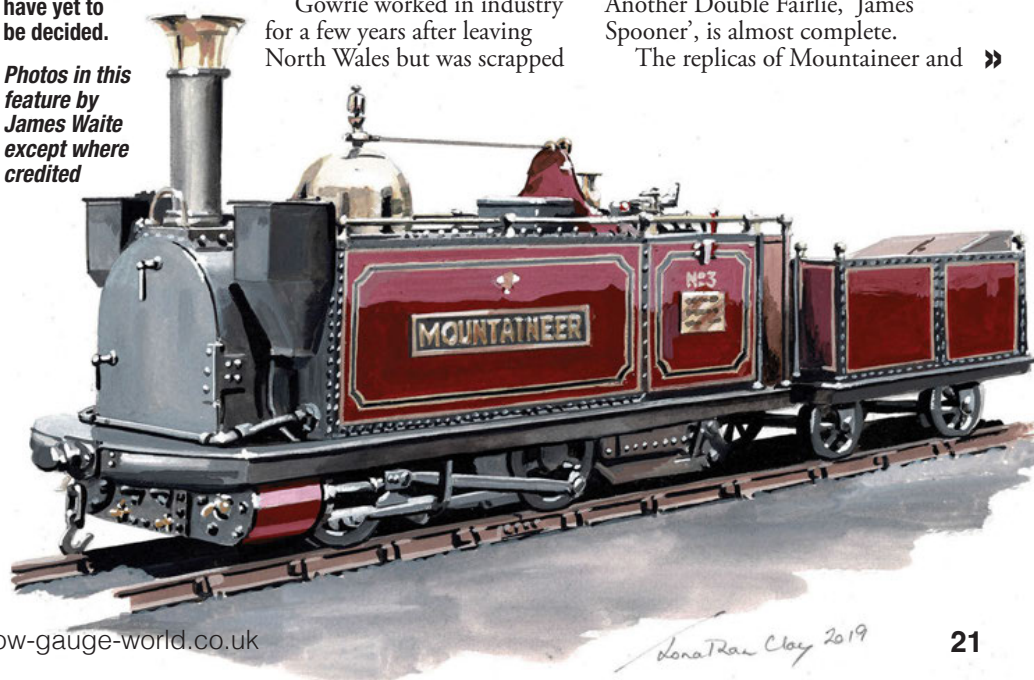
any type built for any railway – with the very considerable exception of the hundreds of Péchet-Bourdon constructed for the French army during the First World War and the single one built as a prototype for the Japanese army soon after it finished. They differed from Fairlie's product in having a single dome and simplified steam and exhaust piping, and Colonel Péchet and Professor Bourdon, their designers, took out their own patent in 1887. But for most people they were still regarded as small Double Fairlies.

Gowrie worked in industry for a few years after leaving North Wales but was scrapped

by 1931. It's ironic that we will only come to know these first and last locos thanks to the construction of the replicas.

The preserved Ffestiniog has a long record of building new locos of traditional appearance, albeit updated to modern specifications. The new Double Fairlie 'David Lloyd George' entered service as long ago as June 1992. In 1999 it was followed by 'Taliesin', a replica of the 0-4-4 Single Fairlie tank which was withdrawn in 1924, and in 2010 by the Lynton & Barnstaple Railway 2-6-2T 'Lyd'. Another Double Fairlie, 'James Spooner', is almost complete.

The replicas of Mountaineer and »





dereliction that tackling a similar-sized new-build machine hasn't been as much of a leap as one might think.

The launch of the project was followed by well-informed work to establish the most appropriate corporate structure which would both maximise the available income from donations and protect the project's trustees and subscribers. The Gowrie Locomotive Trust, a Charitable Incorporated Organisation or CIO, was established and its registration with the Charity Commissioners was completed on 23rd October 2019.

The eight trustees are led by Workshop X founder Martyn Ashworth, its project director, who has been intimately involved with the successful rescue from the USA of seven Penrhyn Quarry locos since 2006. He is also the owner of 'Ogwen', one of two Avonside 0-4-0Ts that were amongst the rescued locos, and its restoration was one of the Workshop X team's previous projects.

Other key trustees include Paul Stock, who had independently been planning to build a Gowrie replica, Joey Evans, whose excellent photos frequently grace the pages of *NGW*, Alan Freebury who has many years' experience of steam restoration and operation and Marion Allen who possesses both financial management skills and hands-on engineering know-how. Marion also owns a superb 5-inch gauge model of 'Gowrie' which she runs at fundraising events. It's a beautiful machine and must have brought in many converts to the cause!

An early assessment estimated a total build cost of £600,000, realistic in view of the size and complexity of the loco. Happily this was later



Above: The driving wheels and carrier frame of Gowrie laid out at Workshop X in approximately their final positions. This will be a big loco! Photo: Joey Evans

Left: Stuart Fletcher, whose experience extends to working with A4 Pacific 'Sir Nigel Gresley' on the main line, works on a vertical drilling machine on 15th February.

Gowrie are now under construction, though by dedicated groups rather than the FR and WHR. This article begins with Gowrie – the first project to be started when it was launched on 28th April 2018.

A large loco

The new Gowrie will be a large beast, with a weight of more than 19 tonnes. It's a most ambitious project for a volunteer group considering that the Ffestiniog's Double Fairlies weigh not much more than 20 tonnes.

The loco is being built at the well-known 'Workshop X' in Derbyshire, in which the volunteers have learned their craft from the restoration of existing 60cm gauge locos. Some of these, notably the Hudswell Clarke Ganges-class 0-6-0T which was rescued from Ghana, and the WDLR Hunslet 4-6-0T no 303, were in such an advanced state of



revised downwards to £450,000, thanks to the availability of generous benefits in kind, notably from the Statfold Narrow Gauge Museum Trust Limited, also a charity, and the very kind provision of key parts, materials and skills by professionals and suppliers at costs significantly below commercial prices.

A lucky find

One small saving has come with the recent discovery of the original brass dome cover from one of the three long-lost Hunslet 0-6-0Ts from the 4ft gauge Padarn Railway, probably 'Velinheli'. It has spent the past 50 years or so turned upside down and used as a flower pot at Port Dinorwic, now known as Felinheli, the Welsh version of its former name. The dome is of the same design as the one on Gowrie, and so the loco will now incorporate one original Hunslet part!

The charitable structure confers significant benefits; the Gift Aid regime available to the majority of donors increases the value of donations by some 25 per cent and the VAT exemption available to charities provides a reduction of another 20 per cent on the cost of bought-in materials and services funded by those donations. Donors who are higher-rate taxpayers can benefit personally since their donations are tax-deductible, and of course this benefit can also be passed on to the charity.

It is difficult not to be bowled over by the enthusiasm and skills of the volunteers at Workshop X, as well as what they have achieved. Gowrie is not their only current project, because as regularly reported in *NGW* no fewer than four other locos are in various stages of restoration.

To date more than £155,000 has been raised towards Gowrie – at an increasing pace, more than £100,000 being donated since May 2022. The power bogie frame, a substantial piece of engineering in its own right, has been completed along with the carrier frame and the driving wheels, plus many smaller parts. The cylinders and motion have been costed and the boiler design is being finalised.

Publicity, and the sharing of well-prepared information, are key to the project's fundraising. The Friends of Gowrie is an active supporters group. Their magazine *Gowrie*, published four times a year, is produced to a professional standard and is a model of its kind. There are also more frequent newsletters. The annual subscription is £20 per annum, more than worthwhile just for the magazine. Be aware, though, that it deploys persuasive fund-raising skills and readers will soon find

Bottom far left: Alan Freebury guides the power bogie frame as it is moved to a different part of the workshop.

Bottom left: Two professional riveters – Richard Jeffs and Jon Ballard – called on the day of James's visit. There was a look of sheer joy on Richard's face when the first rivet went in successfully!

Above right: The brass regulator housing of Gowrie and a replica nameplate. These replica plates are among several items available for purchase from the group.

Below: At NBR engineering the completed boiler of Mountaineer waits its chassis. Photo: NBRE



themselves wanting to make additional contributions!

Over the next year or so, large sums will be needed to fund the cylinders, motion and boiler but at the present rate of progress and with the extent of current crowd-funding there is every reason to believe that this will be achieved. The original planned timescale was five years. Covid blew this out of the window and the trustees now envisage that the loco will be completed by 2026 – provided the funds are available. If you can help do please contact them through their website (see below). Every contribution is valued, no matter how small.

Small but delightful

At just eight tonnes Mountaineer was a small machine compared to Gowrie. The company behind construction of the replica was

formed on 18th March 2019 and the project is very much the brainchild of David Wood and Major Brian Smyth. Brian used to serve with the Royal Engineers and has an excellent railway pedigree! David has recently retired from his long-term motorcycle business and can now focus full-time on Mountaineer.

The work is not being carried out by volunteers but has been contracted as a commercial project to NBR Engineering Services at Darlington, incidentally the town where Gowrie may have spent its last years since its final owner was probably J F Wake, a firm of loco dealers there.

The project was launched at the Warley Model Railway Show in November 2019, and progress since then has been remarkable. The boiler plates were ordered from Israel Newton and delivered to Darlington in the same month. The last tube in »





the completed boiler was expanded on 23rd March 2020, just four months later. But this was the fateful day when the UK's first Covid lockdown began, and the boiler's hydraulic test had to wait until May.

Since then the frame plates have been cut, the driving wheels cast and turned and the axleboxes and hornguides manufactured along with smaller parts. The distinctive motion, with oval-shaped rods and one-piece brass bushes more usually found in marine engines, will be complete before long and fundraising will soon be launched for the eccentrics and inside motion.

The project was held up for part of 2022 when no fewer than five cylinder castings were produced by outside suppliers until a usable pair became available in February 2023. The two cylinders, bolted together, form the frames' front stretcher and so their assembly has only now become possible.

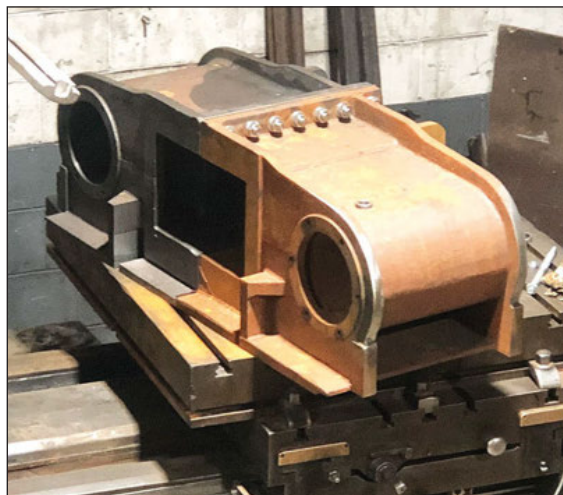
The rolling chassis should be completed this year, but the guiding principle has always been that nothing will be ordered for the loco until it is fully funded and for this

reason there has been no target completion date set.

A significant difference from the Gowrie project is that the company has a commercial for-profit structure and is not a charity – though as with many enthusiast projects it is unlikely that income will exceed the ongoing expenses associated with the loco once it becomes operational. This structure was adopted to enable construction to begin at once – charity formations can be a time-consuming process.

Under the company's rule-book formal decision-making is reserved to the founders in order to protect the project from any unwelcome take-over, but the intention is to consult bona-fide supporters as fully as is reasonably possible.

There is a support group whose members are called 'Mountaineers' and they will be offered the opportunity to purchase non-voting shares and significant benefits regarding access to the loco. It's a shame that Gift Aid isn't available; it was a value judgment and David's and Brian's decision was that it was worth foregoing it in order to start



Above left: The the driving wheels of Mountaineer in store at NBR Engineering at Darlington.

Above: At the fifth time of asking the cylinder blocks were completed to satisfaction. Photo: 1863 Mountaineer Project

Below: Significant progress – the very latest photo from NBRE shows the cylinders mounted on the chassis, which in this picture is upside down on the workbench. Photo: NBRE

work quickly. In this they have been supported by David Humphreys at NBR Engineering, an old friend, and NBR's bills have been considerably lower than might otherwise have been expected.

Just as it was

Mountaineer will be as original as it possibly can be while complying with good modern practice and requirements. It will therefore be a side tank – the original loco did not last long enough to undergo the modifications to the saddle-tank form in which the other three Small Englands have been familiar to all friends of the Ffestiniog for well over 100 years.

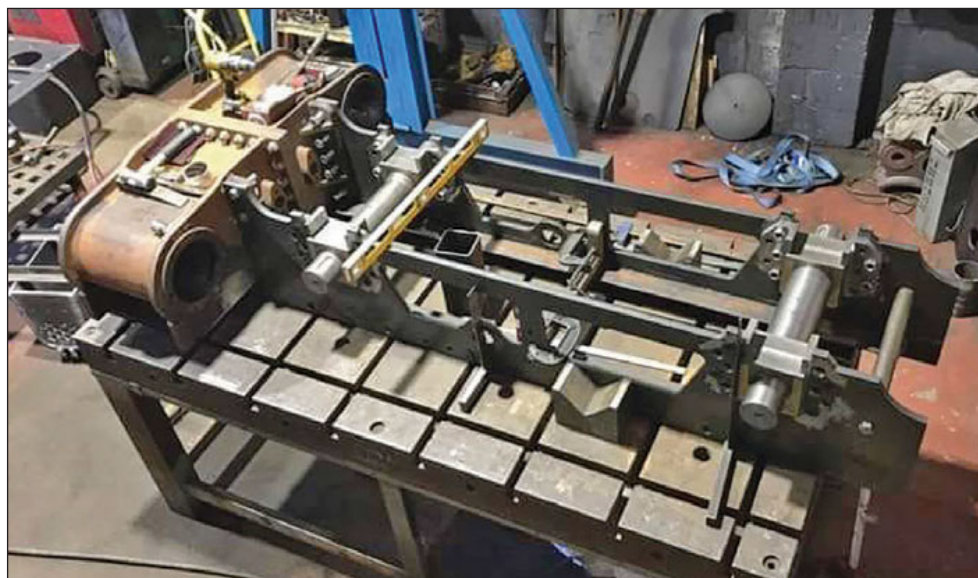
There is not a publication equivalent to the Gowrie magazine, but occasional project updates do appear on the company's website (see below) while there is also an active Facebook page.

Those running the project must have worked very hard, just as their colleagues building Gowrie have, and NBR Engineering rightly has a superb reputation for the quality of its products. How long will it be before we might conceivably see the two locos double-heading through the Aberglaslyn Pass? And, once this happens, is it too much to hope that someone will build a replica of one of the NWNCR's original Single Fairlies, and so make good the tragic loss of 'Moel Tryfan' at the start of the Ffestiniog's preservation era? **NGW**

Information & acknowledgements

James wishes to thank Martyn Ashworth and his colleagues at Workshop X, and David Wood of the Mountaineer project and also David Humphreys at NBR Engineering for all their help during the preparation of this article.

Details of the Gowrie Project are at www.gowri locomotive trust.com and the Mountaineer project at <http://1863mountaineerloco.co.uk>



Sunny start to a new season

Trains are running again across the UK narrow gauge and *NGW*'s photographers have wasted no time getting out to make the most of the very Spring-like weather...

Above: *Joey Evans* captured this view of Darjeeling 0-4-OST+T 19B in steam on the late Adrian Shooter's Beeches Light Railway on 13th February. All involved sincerely hope the sun isn't setting on 19B's time in the UK and you can help ensure the loco remains with us – see the story on page 9.

Below: During the same photo charter on the Tallylyn Railway that produced our cover picture the line's former Corris Railway Hughes 0-4-2ST 'Sir Haydn' posed for this shot that the black-and-white treatment certainly makes suitably dramatic. It's also novel as Sir Haydn was turned for the occasion and TR locos do not normally emerge from their shed front first...

Photo: Joey Evans

Overleaf: The Brecon Mountain Railway is a line that does not feature often in our pages, especially this location showing Baldwin 2-6-2 'Santa Teresa' approaching Dolygaer on the climb to Torpantau. *Ralph Ward* took this shot on 8th October 2022 but somehow it went astray in our system. When we found it we thought it was too attractive a view to ignore!



NARROW GAUGE WORLD







Above: Well that won't do the motion any good... The Tallylyn's other ex-Corris loco, Kerr, Stuart 0-4-2ST 'Edward Thomas' wasn't about to be left out of the photo charter fun and the recently restored Ty Dwr watering point provided a suitable location for a cameo on 20th March. *Photo: Joey Evans*

Below: The UK has a new railway! As reported on page 36, the Mease Valley Light Railway has now opened, adding to the many attractions of Stafford, but opening day was still a while away when this short test train headed out on the 12¼-inch gauge line on 2nd March. *Photo: Joey Evans*



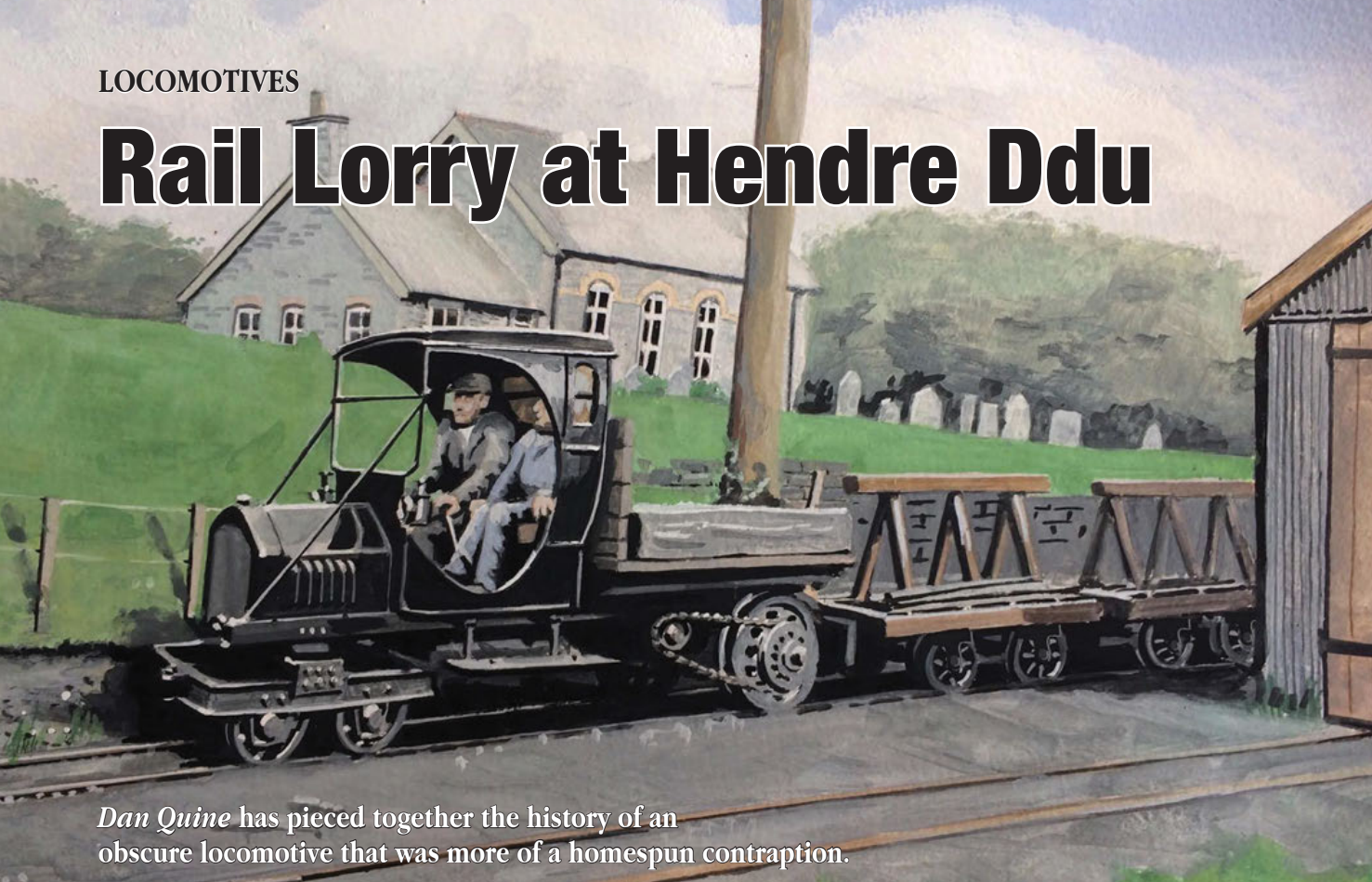


Above: Another narrow gauge railway that doesn't feature that often in our pages – the 2ft 8½in gauge Volks Electric Railway has been traversing the seafront of Brighton for a very long time indeed – *Leslie Kenward* pictured cars 7 and 8 and 6 and 10 crossing at Halfway station on 21st March, on driver-training duties just ahead of the line's 140th season.

Below: And finally, the very end – of the line! *Dave Wilson* caught the South Tynedale Railway's Hunslet 0-4-2T 'Green Dragon' resting at the limit of operations at Slaggyford on 8th April.



Rail Lorry at Hendre Ddu



Dan Quine has pieced together the history of an obscure locomotive that was more of a homespun contraption.

The Hendre Ddu Tramway was a 1ft 11in gauge line running about four miles west from Aberangell, in Mid Wales. It was privately owned throughout its history, being part of the Hendreddu slate quarry, though it carried a wide variety of goods and passengers.

The line passed through one of the most remote and beautiful valleys in Wales and for most of its existence provided the only transport to the outside world. From its opening in 1874 until the outbreak of the First World War it was worked by horses,

gravity and odd human-powered vehicles known as ceir gwyllt.

My book covering the history of the tramway was published by Lightmoor Press in December 2022. When compiling the book one of the most difficult subjects to pin down was the locomotive converted from a road lorry in the late 1920s. After a great deal of sleuthing, I was able to piece together the history of this oddball engine, and, working with renowned rail artist Jonathan Clay, to illustrate what it looked like. This article describes the locomotive and

Above: Artist's impression of the Willys-Overland loco on the Hendre Ddu Tramway. Image: Jonathan Clay

Below: A 1913 Willys-Overland Utility ¾-ton Truck. The frame, gear train and chain drive are identical to those of the Willys-Overland loco.

presents a scale drawing of it for the first time.

Lorry haulage & loco working

Although quarrying continued during the First World War, the tramway was mainly used to carry pit props from the extensive forests along its route. For every 100 tons of coal mined, two tons of pit props were consumed. German U-boats had choked off overseas timber supplies, so private forests were turned over to the war effort.

The wonderfully-named Henry Sharrock Higginbottom leased the timber rights around Aberangell in 1916. He owned dozens of collieries in the North West of England and along the Welsh borders which he needed to supply with pit timber.

Higginbottom laid miles of new tramway into the woods, connected to the Hendre Ddu Tramway. Hundreds of simple, four-wheeled 'timber bogies' were built to accommodate this traffic and 50 horses were used to haul the empty bogies back to the sawmills.

After the war timber was a profitable business and continued to be a major source of traffic for the Hendre Ddu into the mid 1920s. There are reports that Higginbottom moved one or more coal lorries to Aberangell to haul the timber bogies, and at least one of these stayed in the area after Higginbottom left in disgrace in 1922.

The First World War saw the first successful employment of mass-built



WILLYS UTILITY - ¾ TON TRUCK
WESTERN MOTOR CAR CO.
DENVER, COLO.

Willys-Overland locomotive

Built by Bill Milns in 1929 for the Hendre Ddu tramway

Drawn by Dan Quine, March 2023

Scale approx 16mm to the foot

“Sied Ddu was the only covered accommodation tall enough to house it. In early 1931 it was left outside overnight and the radiator froze and burst...”

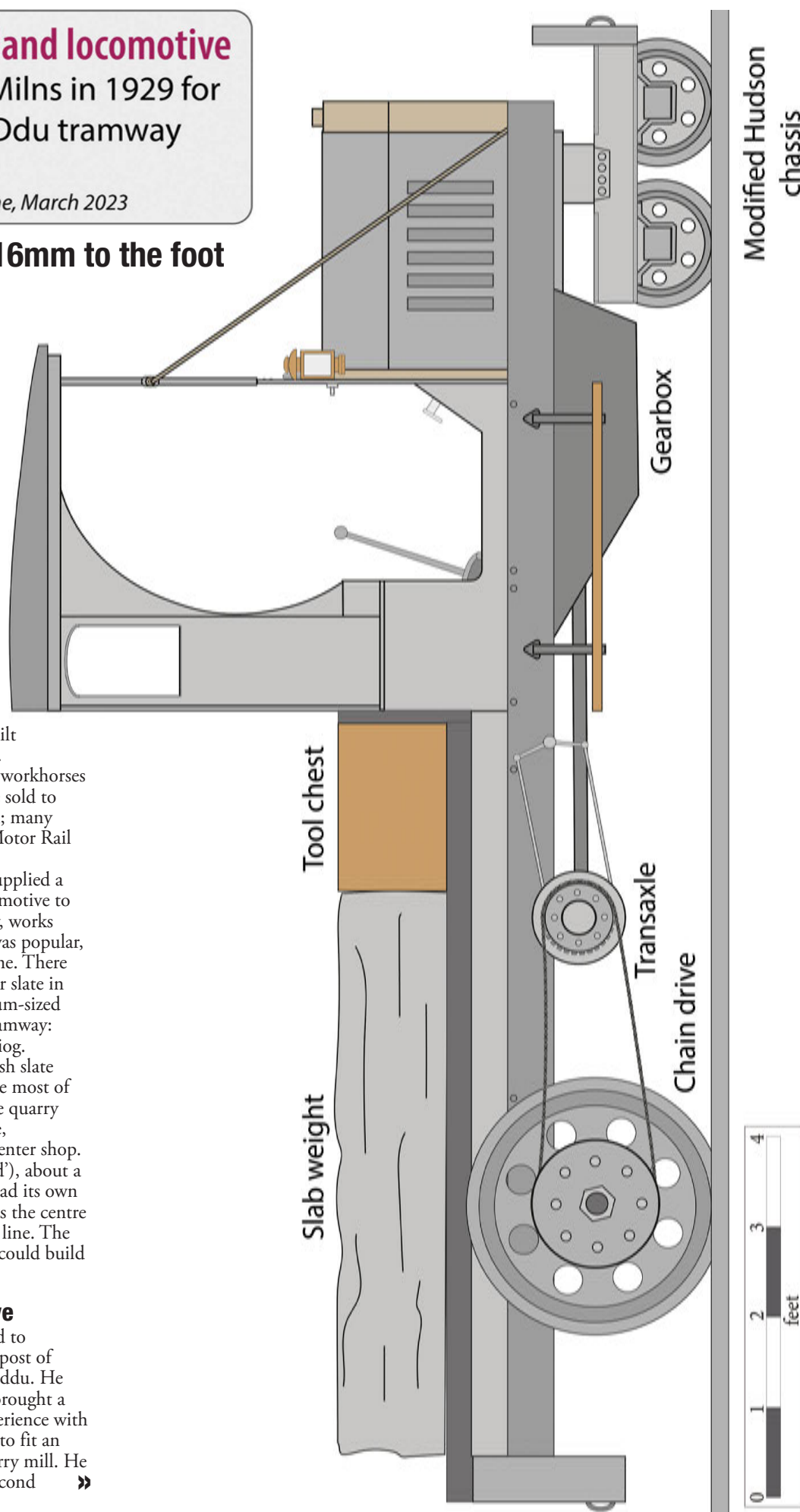
internal combustion locomotives, notably the Simplex petrol tractors built by Motor Rail of Bedford. Hundreds of these robust workhorses survived the war and were sold to railways around the world; many more were produced by Motor Rail in the decades after.

In 1921, Motor Rail supplied a bow-framed Simplex locomotive to the Hendre Ddu tramway, works number 2059. The loco was popular, if somewhat accident-prone. There was a boom in demand for slate in the 1920s, and two medium-sized quarries were using the tramway: Hendreddu and Gartheiniog.

Like many remote Welsh slate quarries, Hendreddu made most of its equipment on-site. The quarry had a fully equipped forge, blacksmith shop and carpenter shop. Sied Ddu (the ‘Black Shed’), about a mile west of Aberangell, had its own forge and in the 1920s was the centre of wagon building for the line. The company soon decided it could build a new locomotive.

The 1929 locomotive

In 1929, Bill Milns moved to Aberangell to take up the post of Chief Engineer at Hendreddu. He was an Englishman who brought a great deal of practical experience with him. His first project was to fit an overhead crane at the quarry mill. He then decided to build a second »





locomotive for the tramway and for this he used the major components of a 1913 vintage Willys-Overland truck, believed to have been the Higginbottom lorry.

The design of the locomotive may have been influenced by the Ford 'rail lorry' built by Colonel Stephens for his light railways. In 1927, the rail lorry was transferred to the Shropshire & Montgomeryshire railway, just 25 miles from Aberangell. It stayed there until 1929, so was running when the Hendre Ddu locomotive was constructed. It is quite possible that Bill Milns travelled over to Llanymynech to see the rail lorry in action. If he did, he appears to have taken its general layout as inspiration.

The new locomotive used the chassis, engine, gearbox, chain drive and bodywork of the lorry. The wheels were replaced with flanged iron wheels set to the tramway's gauge. The wheelbase of the road lorry was 11ft 6in which would have been too long to traverse the curves of the tramway. Therefore a front

bogie was constructed from a Hudson wagon chassis, as the few contemporary descriptions of the locomotive mention.

The glazed cab from the lorry was retained, and driver John Breese preferred the Willys-Overland locomotive for the extra weather protection it had compared to the cableless Simplex. The top of the cab was about 8ft above the rails.

The radiators on the Utility lorries didn't have a fan, which meant they only cooled the engine when running forwards. The tramway was designed for gravity operation, with a continuous downhill gradient from the quarry to Aberangell, so the locomotive ran facing uphill to generate airflow over the radiator when hauling wagons to the quarry. The return trains to Aberangell ran under gravity power, with the locomotive in reverse gear and only providing braking.

The rail lorry had a short working life. It was stabled at Sied Ddu, which was the only covered accommodation on the line tall enough to house it. In

Above: The 'rail lorry' that Colonel Stephens had constructed from a Ford Model T. The locomotive is at Llanymynech on the Shropshire & Montgomeryshire Railway and may have influenced Bill Milns.

Below: The winch in 1983, inside the main Gartheiniog pit. On the right is the six-cylinder Overland engine block. The roller chain is attached to the transaxle. The gear gate and lever is set for left-hand drive; the gearbox and the axle from the engine block to the rear transaxle are missing.

Photo: Donald Sills

early 1931 it was left outside overnight and the radiator froze and burst. The locomotive was dismantled and in 1935 the chassis, engine and gear train were used to create a winch at Gartheiniog quarry. This winch hauled wagons filled with waste up a short underground incline and the waste was dumped inside the largest chamber. Other parts of the locomotive were used to build a large gravity passenger car for the tramway.

Physical evidence

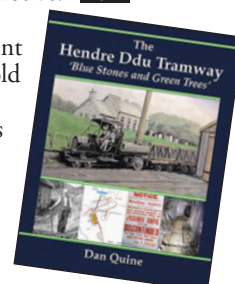
There are unfortunately no surviving photographs of the Willys-Overland locomotive. In 1983, Donald Sills discovered the remains of the winch and photographed them, shortly before they disappeared.

The surviving chassis forming the winch was 6ft 11¼in and formed of pressed steel channels bolted together. It had a six-cylinder Overland side-valve petrol engine with a chrome-nickel molybdenum iron block and the gear gate, which had four gears R, 1, 2 and D. The transaxle from the lorry drove a sprocket on the right side of the chassis. The roller chain was original, and assuming the sprocket from the rear wheels of the road lorry was reused on the locomotive, the approximate size and location of its rear wheels can be established. The radiator was of necessity missing at Gartheiniog. The winch would only have been used in short bursts to power the incline, so it would have run without overheating.

One feature of the locomotive noted in contemporary descriptions was a large slab of slate built into it. The locomotive lacked adhesive weight, with the heavy engine block at the front sitting over the unpowered bogie. The slab was therefore attached to the lorry's flat bed to add weight over the driving wheels. After the Second World War, a Bedford QL lorry was converted for rail use at Dowty Engineering and it had a concrete block chained to its flat bed for the same reason. This standard gauge loco survived into the 1970s.

The arrangement of the rail lorry is not known for certain. The drawing accompanying this piece is necessarily speculative, but gives the most likely appearance of the completed locomotive. **NGW**

■ The initial print of Dan's book sold out very quickly and a reprint has since been made – the book is reviewed on page 44.



The Grouping and the Narrow Gauge...



In the centenary year of the Grouping of Britain's railways, *Philip Williams* contrasts the fortunes of narrow-gauge lines that became part of the 'Big Four'.

A century ago, on the 1st January 1923, the 120 railway companies of the British Isles became four with the 'Grouping' occasioned by the Railways Act of two years earlier. Five narrow gauge railways found themselves absorbed into one or other of the so-called 'Big Four' companies, and another joined them later, these six sharing four different gauges between them. The London and North Eastern Railway (LNER) was the only one of the Big Four not to absorb a narrow gauge concern.

Two for the LMS

The London, Midland & Scottish Railway became the owner of two very dissimilar lines. From its inception in 1904, the eight-mile long, 2ft 6in gauge Leek & Manifold Valley Light Railway had been operated by the North Staffordshire Railway. It became by default part of the LMS at the Grouping, when the North Staffordshire was absorbed.

With no centre of population at either end, and relying mainly on the transport of milk as its one principal traffic, the L&MVR was never a financial success. The LMS appears to have cared little for it, closing it without ceremony in 1935 once the local creamery ceased production.

The LMS did, however, donate the trackbed to the local authorities, enabling it to become what is now a highly popular public right of way (though that of course has worked against a revival initiative ever since).

The LMS's second narrow gauge absorption was the oldest and greatest anomaly of them all. Since 1828 the

Above: The Leek & Manifold Railway did not benefit long from ownership by the LMS, closing in 1935. Photo: Peter Johnson Collection

Right: The Leek & Manifold line lives on in a highly popular trail, with Hulme End station building and loco shed repurposed for walkers. Photo: Andrew Charman

Below: The Nantlle Tramway survived in horse-worked form longer than its owner the LMS did. Photo: Peter Johnson Collection

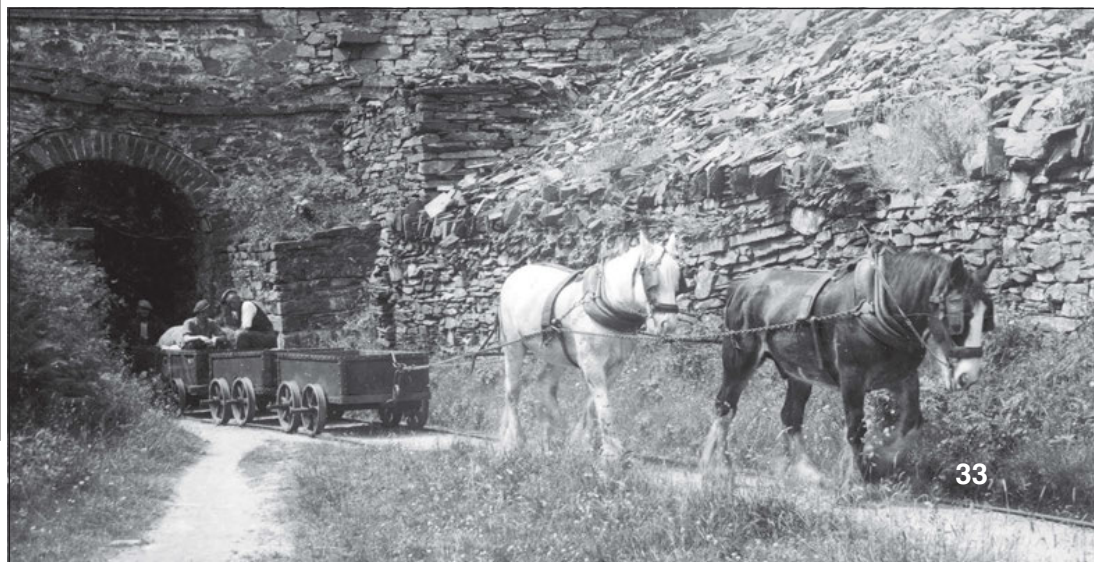
Nantlle Tramway had been carrying slate from quarries to Caernarfon along a 3ft 6in gauge line, originally using horse traction throughout.

As time went on, most of the route was converted to a standard gauge loco-hauled line, as part of the London & North Western Railway, but the two miles nearest the quarries remained as built and worked. Thus when the LNWR became part of the LMS in 1923, so did this short section of narrow gauge. Moreover, it survived in the same condition till

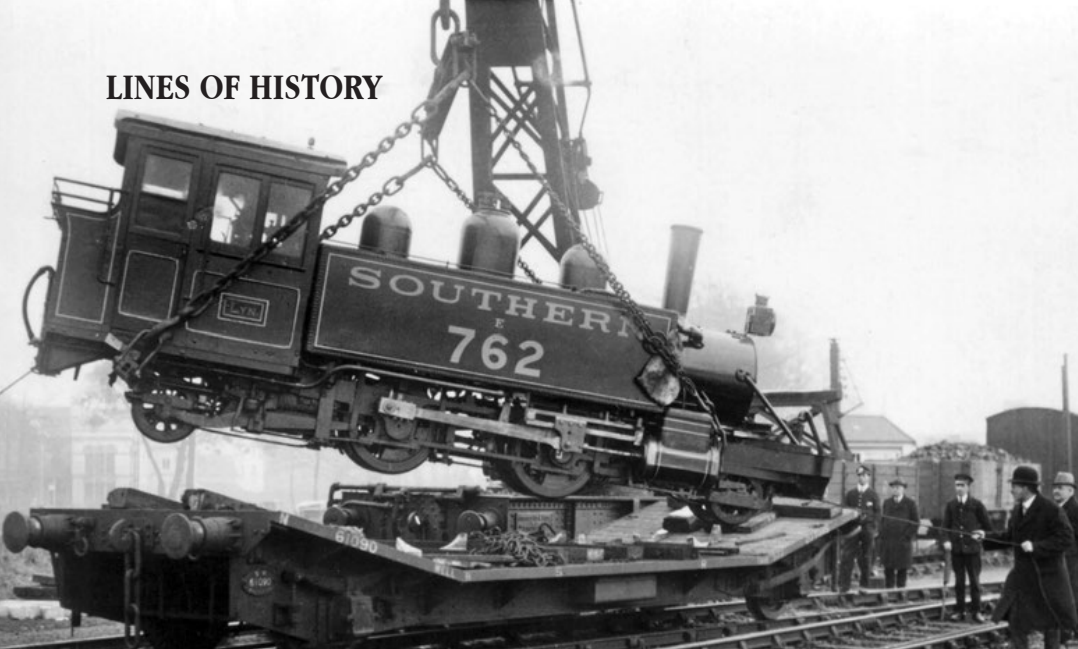
eventual closure in 1963, so for 15 years after Nationalisation in 1948, British Railways was owning and operating a section of narrow gauge, exclusively horse-worked line.

One for the Southern

In the south-west of England, the 21-mile long two-foot gauge Lynton & Barnstaple Railway had operated between those two towns since 1895. With its excellent permanent way and splendid locos and rolling stock, it was almost a main line in miniature. »



LINES OF HISTORY



Like so many other narrow gauge branch lines, however, it had cost too much to build and was impecunious. Traffic had not come up to forecasts, and it was not helped by the distance

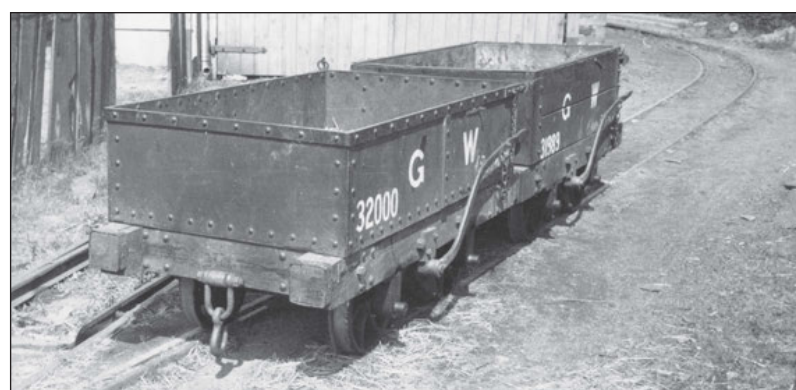
of the Lynton terminus from the twin villages of Lynton and Lynmouth it purported to serve.

By 1922, it was in discussions about a merger with the London &

Above: The Southern invested in the Lynton & Barstaple, adding a new loco and overhauling the others including 'Lyn' here, but the line closed in the same year as the Manifold. Photo: Peter Best/762 Club



Upper left: The GWR updated the Welshpool locos from their original specification seen here, but also ended the line's passenger service. Photo: W&LLR Archive



Left and below: On the Corris Railway the wagons displayed their new owners' initials but the locos never did, remaining as in this recreated scene featuring visiting original loco 'Edward Thomas'. Photos: Peter Johnson Collection and Joey Evans



South Western Railway (whose Barnstaple Town station it shared), but it has never been clear how far negotiations had progressed before the Grouping. In any event the L&B became part of the SR at that point, as of course did the LSWR itself.

The Southern behaved well towards the L&B in the early days. Infrastructure was renewed or improved, an additional locomotive was supplied, and there was also new rolling stock. But none of this altered the disinclination of locals to use the line, and the SR closed it in September 1935 after an unavailing campaign to save it.

As is well known, all the equipment was auctioned off shortly afterwards, with the stations and trackbed sold piecemeal to adjoining landowners. Nearly 70 years then elapsed before preservationists could take any practical steps to begin to restore the railway, even to its current transitional shape at Woody Bay.

Two plus one for the GWR

The Great Western Railway inherited two narrow gauge lines from its Cambrian Railways constituent at the Grouping, and later bought another (allegedly with great reluctance as part of a job lot). All three are operating at least partially today, so will be familiar to readers of *NGW*.

The late arrival was the 2ft 3in gauge Corris Railway, purchased with the Bristol Omnibus Company in 1930 by the GWR, which discontinued the line's passenger traffic in favour of buses in the following year and more or less ignored it thereafter. All the same, it was still operational at Nationalisation, though British Railways closed it almost immediately afterwards, rather than repair a flood-damaged bridge.

The two surviving locomotives were stored under tarpaulins at Machynlleth station, allegedly in an effort by Mr. Campbell Thomas, the stationmaster, to hide them from BR officials who might have intended to scrap them. However, there are so many pictures of them uncovered, that this must have been a futile effort even if true.

This writer's belief is that Mr Thomas knew the situation on the similarly-gauged Talyllyn Railway a few miles away, and took the view that sooner or later one or both of 'his' engines could be needed there – as duly came to pass when the pair were sold by BR to the Talyllyn preservationists in 1951. They have therefore carried the same numbers, 3 and 4, in four separate ownerships. Now, as *NGW* readers will know, an ongoing revival project is well under

way at the Corris, with replicas of both engines extant.

The Great Western's other two, formerly Cambrian, lines had been very different entities from the start, though opening within a year of each other. The 2ft gauge Vale of Rheidol Railway was situated amid a landscape of great scenic beauty and with a sizeable coastal town at one end, so its mineral traffic in the early 1900s was always subsidiary to a tourist passenger market.

The GWR did it proud during its period of ownership, maintaining (and improving) almost everything on the railway to its own high standards, and treating it as an integral revenue-earning constituent of the whole, although mineral traffic was discontinued entirely in 1931.

Only after Nationalisation did the VoR become an embarrassment (or anachronism) to British Railways for various reasons, and only determined efforts to halt a decline in maintenance and services by certain local BR management and staff enabled it to survive for long enough to be sold into private ownership in 1988. It is still among the smartest, if not necessarily the most characterful, of the surviving Welsh narrow gauge railways today.

The 2ft 6in gauge Welshpool & Llanfair Light Railway has always been an anomaly. There was never any potential revenue from minerals or slate in Montgomeryshire, now Powys, and it catered only for local passengers sharing its mainly agricultural traffic. The Cambrian Railways operated it from its inception in 1903 until the Grouping, following which the Great Western kept it open until Nationalisation, despite withdrawing passenger traffic in 1931.

As in the case of the Rheidol, but not the Corris, the GWR maintained and repaired the locomotives to a high standard, and indeed improved and 'westernised' them during its period of ownership, to help prolong the railway's life. A basic freight service survived until closure in 1956, when the engines were stored but not scrapped. The line was not lifted (possibly through the good endeavours of the same BR guardian angel who cherished the VoR), as by then the idea of heritage railways run mainly by volunteers was taking root at Tywyn and Porthmadog not far away. As is well known, the W&LLR and the Vale of Rheidol have both since joined the ranks of lines in healthy preservation,

Fortunate survivors

Thus of the six 'grouped' narrow gauge railways discussed above, only



"So many of them are still here, unlike the many other minor railways and branch lines killed off..."

two (both of those inherited by the LMS) appear to be lost forever. After a closure period of 70 years, the Southern's Lynton & Barnstaple has been gradually coming to life again, fulfilling the hope of its 1935 well-wisher; 'Perchance it is not dead but sleepeth'.

All three lines owned by the Great Western at one stage are still extant in preservation, which must say something for their management at the time, as there were and are many

differences between them. One wonders if the politicians and railwaymen who devised the Grouping took much if any notice of the few and apparently insignificant narrow gauge railways which would be caught up in the net. They might be gratified that so many of them are still here, unlike the many other minor railways and branch lines killed off in the period after Nationalisation. Railway enthusiasts are fortunate in what has survived. **NGW**

This page: Today the Vale of Rheidol, Lynton & Barnstaple and Welshpool & Llanfair railways all celebrate their former 'Big Four' owners. Photos: Joey Evans, Trevor Garnham and Andrew Charman





■ The brand-new steam locomotive being built for the 15-inch gauge Perrygrove Railway by North Bay Railway Engineering was nearing completion early in April. The loco was due to be officially launched and named at the Forest of Dean line on 28th April and we will have a report on the event in next month's issue. *Photo: NBRE*

Statfold opens its Mease Valley line

The Statfold Narrow Gauge Trust officially opened the first stage of the latest line at the Tamworth narrow-gauge mecca on 18th March, the 12¼-inch gauge Mease Valley Railway immediately proving very popular with visitors.

The new line heads away from a purpose-built terminus located behind Statfold's recently constructed transport museum and exhibition centre – the station boasting a turntable and an impressive stock shed with raised lines.

The line traverses a level crossing over the entrance to the also-new car park and heads across fields into an area of the Statfold grounds that has not previously seen any railway activity. Currently it ends in a run-round loop by what is known as 'Duck Decoy Wood' but there are apparently plans to continue the line in a large return loop, enabling trains to head back to the main station and their locos then turned ready for the next train.

After a very wet opening day during which the line suffered a minor derailment, the Mease Valley was in operation again at Statfold's Model Railway weekend on 1st-2nd April, when services were shared by Exmoor Steam Railway-built 2-4-2T 'Victoria' – constructed originally for a

railway project in Tintern, south Wales that never went ahead, and diesel hydraulic loco 'No.73' – this was built by Alan Keef Ltd in 2005 for a private line in Devon which proved to be short-lived.

As reported in *NGW* 173 the passenger stock includes two carriages built for the 'People at Work' exhibition in Germany in 1937 and three constructed for the Liverpool Garden Festival Railway which only ran in 1984.

Above: 'Victoria' heads away from the main terminus with another train on 1st April. The line passes an impressive selection of railway mileposts collected over the years.

Below: The line has impressive facilities.

Photos: Andrew Charman



Seaton carries out £200K bridge work

Seaton Tramway reopened for its new season in February following a winter maintenance programme that included a £200,000 project to refurbish three bridges on the Devon line's route.

The major project this winter was on three of the line's bridges – the 2ft 9in gauge electric line is built on a former standard gauge trackbed with some original railway infrastructure re-used, including some of the beams and girders that make up Bobsworth Bridge.

The work, undertaken by outside contractors, took six weeks to complete and included blasting the current beams, installing new bridge girders and applying a new coating to the entire bridge.

The major works were completed early to allow tram services to resume for the February half-term service and weekends through March.

Work done during the winter also included track realignment north of Colyford, between Tye Lane North and East Devon Bridge.



Two of the three steel-bodied bogie carriages that the Statfold Barn Railway recently bought from the South Tynedale Railway were in use as a shelter at the first enthusiasts day of 2023 on 18th March. The STR sold off the carriages having purchased and regauged six former Vienna tram cars after receiving a Heritage Lottery Fund grant.

Photo: Peter Johnson

Unusual loop under threat?

NGW reader **Mick Bass** recently rode the 'Reunification Express' sleeper service on the metre-gauge route from Hanoi to Ho Chi Min city in **VIETNAM**, and described it as "quite an experience, perhaps not to everyone's taste but offering a unique glimpse of the Vietnamese people going about their business and very hospitable they are."

Mick reports that the extension north of Hanoi up to Halong Bay remains part-built and can be seen by the roadside on the bus journey – opening has been prevented by an apparent lack of funding.

The Vietnamese Government has been offered a new purpose-built electrified and double track high-speed line, funded by the Chinese as part of the 'belt and road' initiative. "This has been rejected as they have seen how these projects have financially crippled less wary countries, so the ragged selection of diesels continues to wend their way up and down the single track line over 1600km.

"There is plenty of freight traffic hand-loaded into covered vans to be seen in passing loops en route but most stations actively discourage you from lurking on platforms taking photos," Mick adds.

He also reports that one interesting aspect of the Vietnam route, the unique loop out and around the station at Nah Trang which ensures all trains pass through the station without the need to run around, might be on borrowed time.

"There are plans afoot to move the station further out of town and so do away with the experience of leaving the station only to pass through the town's closely packed houses nmd to reappear a few minutes later at the station entrance."



The distinct loop at Nah Trang is under threat (above), but freight on the Vietnam lines appears bouyant (below). **Photo: Mick Bass**



Railmotors boost newly open Queensland line

The Atherton Herberton Historic Railway in northern Queensland, **AUSTRALIA** has recovered two former Queensland Rail railmotors for future use on its now operating heritage line.

The two, transported from the QR facility in Ipswich, are fitted with Rolls-Royce engines and in need of restoration.

The organisation owns 22km of 3ft 6in gauge line and in March began offering steam-hauled rides along part of the route using newly overhauled Peckett 0-4-0ST no 1069, built in 1905.

Preservation of the line began shortly after closure in 1990 but a Government decision to remove rather than raise a rail bridge to allow road coaches to pass under it divided the line and the revival effort – preservation at Atherton continued initially but was then abandoned in 2000.

In 2003 the effort was revived and

Above: The rescued railmotors will need full restoration.

Right: Steam services with the Peckett began in March.

Photos: AHHR



following a seven-year long \$170,000 programme to overhaul the Peckett, which last worked at the Mount Isa mine in 1955, and a wooden bogie carriage of 1913, services have begun this season. These run along 3km of the line between Herberton railway station and the town's Historic Village tourist attraction.

Future plans include the restoration of a second Mount Isa Peckett and the two railmotors that are planned to be used on extended services from Herberton to Moomin and eventually from Atherton to Herberton. This will require significant work including the rebuilding of bridges.

More details of the project can be found at www.athrail.com/

All change at top of the Harz

The Harzer Schmalspurbahnen GmbH (HSB) will have a new managing director from 1st July, Katrin Müller succeeding long-standing MD Matthias Wagener, who is retiring.

Katrin has been with HSB for 12 years and previously headed marketing and sales.

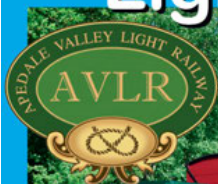
Supervisory Board Chairman Thomas Balcerowski expressed his great gratitude to the retiring MD. "Matthias Wagener took over responsibility for the HSB after the political turnaround and made it an important tourist beacon in the Harz Mountains," Balcerowski said, pointing out that Mr Wagener had initially taken on the role on a temporary basis in May 1997.

Below: All change: Matthias Wagener (left) prepares to hand over to Katrin Müller with HSB Supervisory Board Chairman Thomas Balcerowski.

Photo: Dirk Bahnsen/HSB



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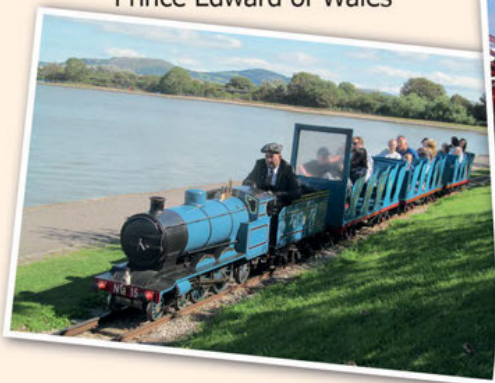
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Not the Last Trek North...

What wasn't eventually a finale for the Narrow Gauge North show was again a big success..

Even the sudden and unwelcome arrival of late winter snow could not stop the latest Narrow Gauge North show in Leeds proving another success on 11th March.

We reported in *NGW*173 that 2023 was expected to see the finale of the long-running show. Peter Kirkby, organiser for many years had handed over duties to his son Andrew who undertook much of the organisation for this year's event, but then sadly died in January – the show went ahead as a tribute to Andrew.

Visitors turned out in numbers, attracted by more than 20 layouts in various scales and a good smattering of traders. And at the end of the day some £2,400 was able to be donated to the Sue Ryder Care-Wheatfields Hospice.

More good news followed too, as Narrow Gauge North will now continue – the date for the next show has already been set on 9th March 2023, at the same venue of the Pudsey Civic Hall.

If any *NGW* reader has a layout that they would like to be considered for the 2023 show, they can email details to admin@narrowgaugenorth.co.uk

No-one will be more pleased that the show is continuing than *NGW* editor Andrew Charman. Having never managed to attend the event, and aware of its connection with the magazine which sponsored the first show in 1994, Andrew was determined to make it this year, only to get snowed in! Living up a hill in mid Wales is lovely, most of the year...

Above: Irish country station theme on the 7mm scale Donegal layout.

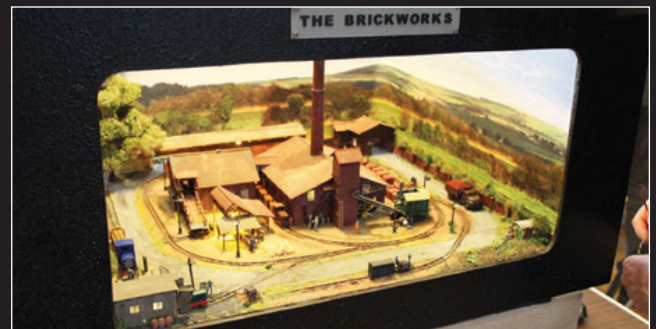
Upper right: Packed hall held plenty for the many visitors to enjoy.

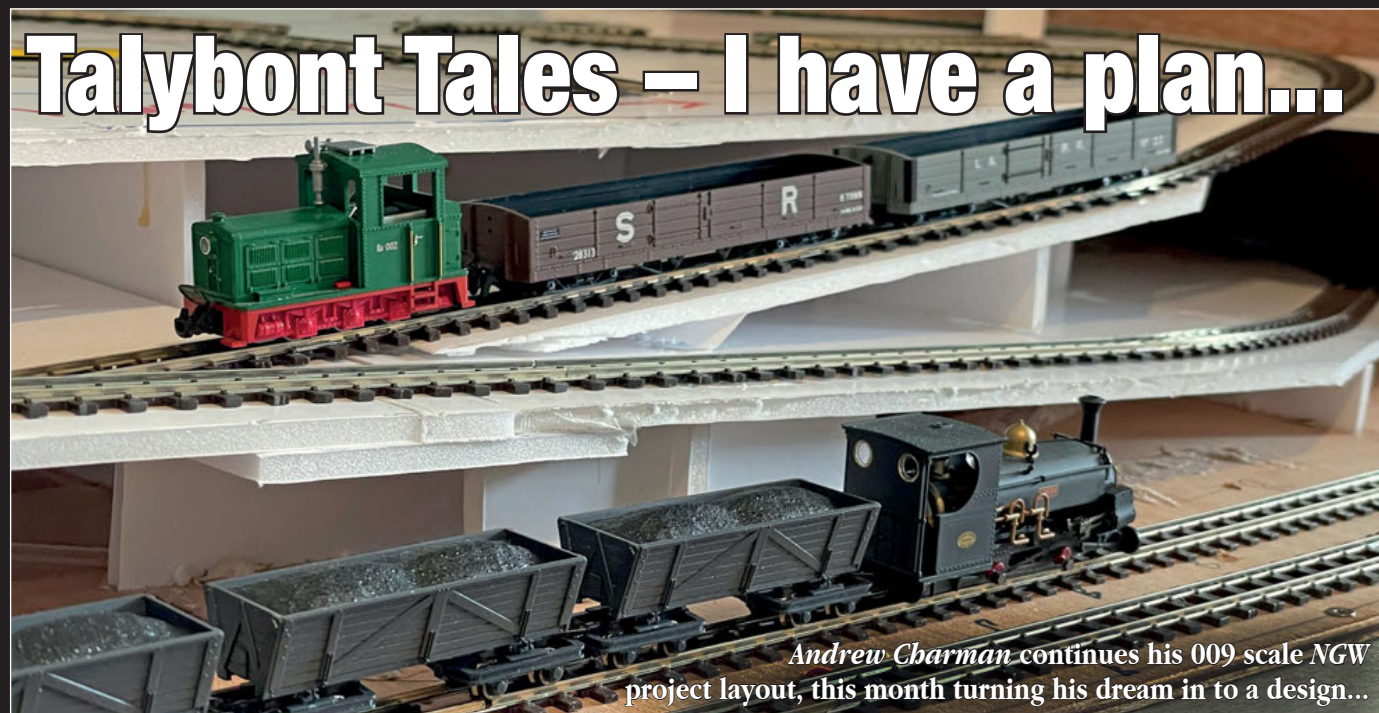
Lower right: Definitely in the small and delightful category, The Brickworks in H09 scale.

Below left: Detail makes all the difference...

Below: Shay action on the American-themed Karolina Falls in On30 scale.

All model photos by Thomas Whitelegg





Andrew Charman continues his 009 scale NGW project layout, this month turning his dream in to a design...

In *NGW* 173 we launched our 009 (4mm to the foot) project layout and built the 4ft x 2½ft baseboard for it. Next it was time to decide what was going on it.

Time spent on design is essential to a successful model railway – get it wrong and the layout will frustrate you and soon be abandoned. As mentioned last time, my design was based first on practical considerations – I needed a test track for review models received at *NGW* Towers. So the basis of the layout would be an oval to provide a continuous run, most of which I decided would be hidden and from which a second oval would climb to an upper level forming the scenic section.

My concept was an intermediate halt on a major narrow-gauge heritage line, the Talybont Railway (see panel for more). A former slate quarry once served by the railway has been taken over by enthusiasts and turned into a railway museum. This would enable me to have stock of various provenance on the model while the idea has some prototype inspiration – in the 1970s there was a plan to turn the former Pen-yr Orsedd slate quarry in Snowdonia into a museum and narrow gauge equipment was gathered there for a while.

First step was to figure out where the bottom-level trackwork, to include a small loop and fiddle yard siding, was going to go

on my board, and I admit I used the tools of my trade here, the same software I employ when designing pages for *NGW*. I created a new 'page', a scaled version of my baseboard dimensions, drew curves which I could move about the page, and downloaded plans of the points I'd be using from the Peco website, scaling them to an equivalent size. Peco is not the only manufacturer of 009 track but it is used by the vast majority of modellers, and helpfully correct-sized images of various bits of pointwork can be downloaded from the website. More on track matters directly...

As a result I soon had a plan for the base level, part of which is reproduced on this page. This would, however, prove to be very much a 'Mk1' plan... Meanwhile I couldn't, at this stage, get my head around planning the upper, scenic levels, and decided that these could wait until I'd built the lower level and then perhaps I'd scheme things out directly onto the board.

Full-size plan

By now you are thinking "I haven't got expensive desktop publishing software..." Well you don't need it – I made for myself an extra complication and could have done just as well by planning from the start using the way I transferred my plan to the board. You either print out the diagrams of the

pointwork full-size, or even better buy a couple of points, and use these to directly plan your layout onto the board...

There are a few essential considerations to make before you break out the felt-tip pens. Firstly, what will your minimum radius be? Sharp curves are of course a part of narrow gauge and an excuse for many modellers to cram quite a lot onto a small board, but it is recommended you don't go less than nine-inch radius, also widely known as 'first radius' in both 009 and N – 2mm scale standard gauge which uses the same 9mm track width.

Most locomotives and stock, including the Bachmann range, will go round nine-inch curves without suffering issues such as locking couplings, but I'm told that larger locomotives, such as the Heljan Lynton locos, can have problems.

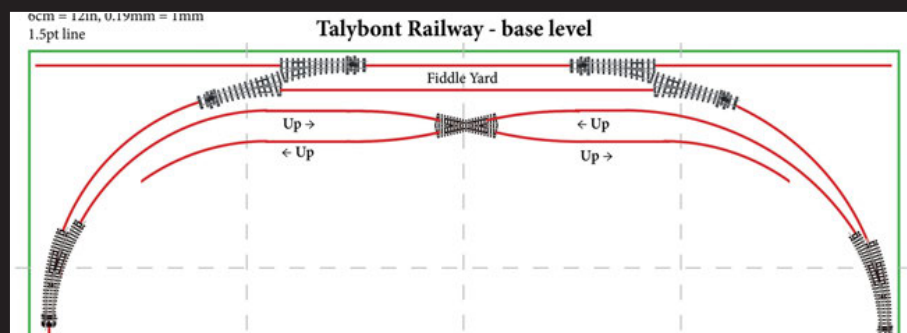
Choose your frog

This also leads into the thorny subject of electrics, which we will look at more closely in a future episode. What you need to know for now is that Peco produces two forms of 009 track and points – the 'Crazy Track' from the early days and a more realistic main-line track. The smallest point, of nine-inch radius, is made in Crazy Track style and is an 'Insulfrog' unit; change it from one direction to another and only the way it is set will receive track power – the other is

Heading: Second version of gradient proved simpler and with far fewer issues.

Left: Part of the original lower-level plan. Note that the hidden track, or that with sleepers covered by ballast, can make use of N-gauge track which has far more choice available. The Insulfrog curved points were eventually rejected due to their long dead frogs.

All photos/diagrams by Andrew Charman

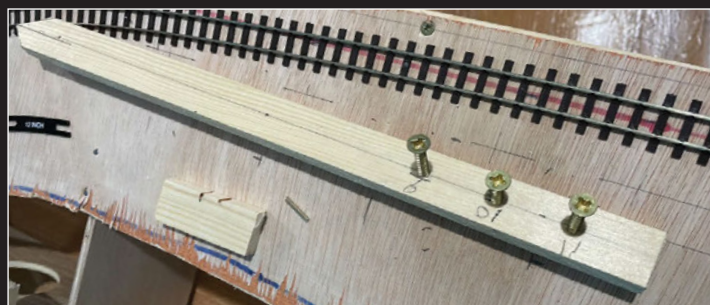


Right: Difference between dead-frog points (left) and live-frog equivalents.

Far right, upper: Tracksettas make creating smooth curves a doddle.

Far left, lower: Simple tool for drawing correct radius curves.

Below left: The first attempt at a gradient was eventually abandoned for the setup shown in the heading photo.



isolated, which on the face of it seems great.

The tightest point in Peco's normal 009 range is of 12-inch radius and there is also a Y-point, both of which are 'Electrofrog' – this means that the 'un-set' direction has its electric polarity changed but not turned off, meaning some extra wiring is needed.

So going for small radius and simple wiring would seem a no-brainer. Except that the Insulfrog points achieve their isolation by the central frogs being plastic – the picture above shows what I mean. In the old days short-wheelbased narrow gauge engines used to come to a dead stop on these plastic frogs, and while such issues are much less common today than they were in the 1970s when I was last working in 009 we also now have *very* short wheelbased locos such as Bachmann's quarry Hunslet...

I decided that I would only have dead-frog points in the fiddle yard, which no-one will see, and where locos were unlikely to go, such as some of the sidings in my quarry. I would take on the extra electrics, which in fact are not as daunting as it may seem, even though I was planning that my layout would be DCC-powered with locos controlled individually – a full explanation to follow in a future episode...

Smoothing the curves

The other useful thing to have when transferring your ideas to your baseboard is a Tracksetta. These inexpensive lengths of metal, made in a range of radii, are designed to fit into flexible track to produce smooth curves when pinning the track down. But they are also useful at the planning stage for marking out where your lines need to go. I also made a simple radius tool, comprising a piece of wood with a pointer cut at one end and holes drilled at nine, 10 and 11 inches from it. I simply screw a self-tapping screw into the hole and then pivot the pointer on it to draw a correct curve. Simple!

Having marked out my lower level, I then cut out the centre section of my baseboard top panel. I did not need this section and removing it would save some weight while providing access to the underside of the scenic boards to sort

electric feeds and such. But on reflection I should have cut four holes in the top board and left a central spine in both directions, as this would have helped with the rigidity of the baseboard as a whole. A lesson learnt, and this demonstrates what I stated last time – even if you have the space and time to build an extensive layout, it's best to start with a small section as you will make plenty of mistakes that will help with later layouts (I haven't told Mrs C yet that layout number two is sketched out in my head...).

I then laid the track on the base level and even ran a train! This will be described in detail next month, but my main reason was so I could plan the upper level and test just how steep a gradient up to it I could get away with...

All modellers will tell you that any gradient should be as shallow as you can manage, and I admit the ones on my layout are pushing the limit... Initially it looked as if I was in trouble. I thought I'd been clever by scheming in a crossover so that I could run the two ascending lines back towards the edge of the baseboard and free up more space on the upper level. But of course you cannot have a crossover that twists in two directions – it had to be level, losing several centimetres of climbing space.

Not only were the gradients either side too steep, the change from climbing to level and then to climbing again resulted in couplings routinely parting. After several days of experiments I eventually bit the bullet and rebuilt the whole section, without a crossover. As I said, you learn from your mistakes...

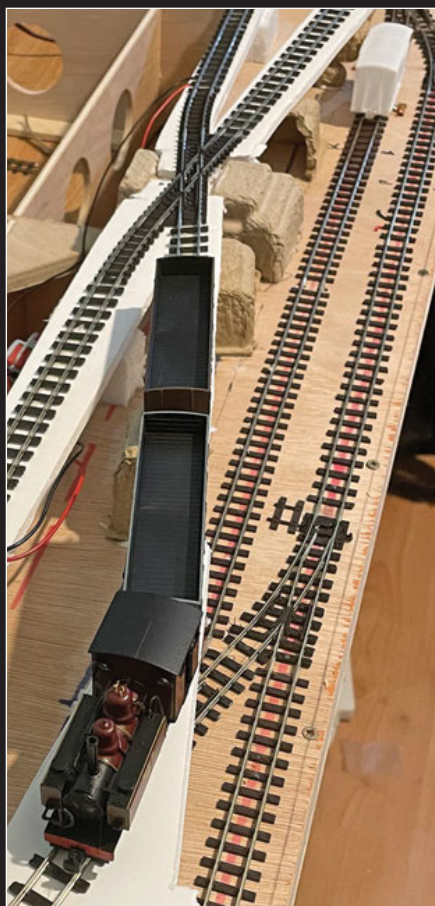
So at the end of all this, I had a working lower section and I could start adding the upper boards and scheming out the scenic upper levels. But before I talk about that, next month I'll go into detail about the unique art, and yes sometimes frustration, of tracklaying in 009 scale... **NGW**

■ Readers wanting to read the first part of this series can order a printed or digital back-number of **NGW173** at <https://www.world-of-railways.co.uk/store/back-issues/narrow-gauge-world>

Why Talybont?

All of my 'indoor' layouts over the years have been versions of a fictional line called the Talybont Railway. As a child on holiday in mid-Wales in the 1970s I saw the name outside a village – it sounded so close to Tallylyn, a railway we visited on most holidays, and suitably Welsh!

In fact the name means 'Tall Bridge' and there are several Talybonts dotted throughout Wales – there is even a tiny one just outside Welshpool close to **NGW** Towers. So Talybont Railway it is, and maybe one day I'll build the complete model line that's been in my head for much of my life!



009 – my part in the story...

As a follow-up to our feature last month, *Paul Towers* describes the formative years of 009, 'from the inside'...

The Editor writes: Our 'brief guide to the rise of 009 scale' in *NGW173* produced a gentle chiding from a good friend to the magazine, veteran railway modeller Paul Towers, that he rated not a mention!

In our defence it was never meant to be an in-depth history – but I have to admit that although I have known Paul for some years, not being a reader of *Model Railway News* when he was editor (I was a *Railway Modeller* man...) I was not aware of his prime role in the formative years of 009 – so I asked him to tell the tale...

Way back In April 1958 a friend and I were strolling around Westminster prior to going to the theatre in the evening. We stumbled across a model railway show and while we had both thought of building a model railway, we had no idea how to go about it. I had been interested in railways for a long time and was fascinated by narrow gauge having seen the Festiniog (as it was spelt then) and the Snowdon Mountain Railway (the latter hardly typical narrow gauge).

We saw a lot of layouts at the Model Railway Club's exhibition but one was a life-changing moment for me. This was the Milwehr and Llenli Railway, a magnificent layout in OOn2 (4mm scale running on 8mm track). Fortunately the *Railway Modeller* had the current issue on its stand featuring this layout.

Searching for NG

I started taking all three model railway magazines published at that time – disappointingly there was little narrow gauge but there were rare articles by P D Hancock on his home-based Craig and Mertonford (then OOn2) and Derek

Naylor's Aire Valley (OOn3 – 12mm gauge). This led me into trying my hand in that gauge as TT3 items were available to adapt. I struggled along with this and a few cast wagons for 8mm gauge for a few years.

In 1963 one of the ladies who worked with me showed me a copy of the *Daily Mirror* in which was a picture of a lady's high-heeled shoe with a circle of track and a diesel loco on it. I got straight on to the *Mirror's* picture department and they told me that it was something that had caught their eye at the Nuremberg Toy Fair.

It was from a firm named Egger and was called Eggerbahn – it was 00 scale on 9mm gauge track. I was on to it in a shot and my previous work was scrapped. I started collecting bits of Eggerbahn and commissioned a scratchbuilt model of a single Fairlie from Bec Models. It cost me all of £30 – a vast amount of money in those days. That model is now with the O09 Society.

I fiddled around with this until May 1965 when I was appointed Editor of *Model Railway News*. Funnily enough, more narrow gauge articles started appearing in the magazine until I took the bull by the horns – from then every month included a section called *Narrow Gauge and Industrial Miscellany*.

'Not proper'

I got a lot of stick as it "wasn't proper railways" but I took no notice. Then the magazine and its sisters moved to a new company in Hemel Hempstead and, by chance, at the other end of town was a firm called Richard Kohnstam and Co, importers of Eggarbahn. Dickie and I became firm friends and the first new Egger item that came in arrived at my office for review.



In 1966 Jack Shortland, my Assistant Editor, and I took a working holiday in North Wales and one evening we were invited out on a special trip on the Festiniog behind 'Blanche' so that the general manager Alan Garraway could take some publicity photos of her. When we got back to Boston Lodge he took us to see a little loco that they had – the quarry Hunslet 'Britomart'. I was immediately taken by this loco and her sisters.

The next day we went to the head office of the Dinorwic Quarry (as it was then spelt) to get permission to visit their works. This was immediately arranged and we duly arrived at Gilfach Ddu to be shown around by the then Manager. We only went around the workshops but we did see some more Alice-class locos and I was completely captivated. When we got back into the office I was on to Don Townsley and he produced a series of drawings comprising the Alice and Port classes and 'Jerry M'.

One person I got to know during this time was Mike Elvy who was very much involved with the Festiniog Railway. He wanted to sell a kit for a Festiniog loco and we spent a complete evening trying to think of a loco and a commercial chassis to



Above: The first advertising photograph by Egger. Photo by courtesy of the late Bill Luty

Left: Paul and his friend and joint operator Jo Vincent with what he claims will be his final layout in 009 scale, 'Llandyrau'.

go under it. On the way home I thought of the Alco 'Mountaineer' and a chassis to go in it. He contacted George Mellor (of *GEM Models*) and a kit was produced. I won't claim it was the first 009 British outline model but it was pretty well near it.

About this time I had, by chance, met up with a gentleman named David Mander and he wrote a couple of articles for me. This was after he entertained me at his house and introduced me to his magnificent Stronachloch Railway in his loft. This had started as a 00n2 layout but was converted to 009. He was a toolmaker by trade and all the locos were made by him. David died much too young – I was pleased that he left me all his locos and stock and the locos are now in the keeping of the 009 Society.

First effort

My first completed 009 layout was built for me whilst I was Editor of *MRN* by none other than Cyril Birch (of 'Ynys Gwyntog' fame) and was first shown at Hemel Hempstead in 1967. It was a 4x2 oval with a harbour in the front and was called Ecroux. The locos were Hunslets on Egger chassis built by the late Pete Brennand.

In mid 1967 I left *MRN* but continued modelling and started writing articles for Cyril Freezer in the *Railway Modeller*. The most significant was a series on David Mander's Stronachloch. I also wrote about Doreen and Andy Andrews's Torandor Valley Railway which was appearing at exhibitions at that time.

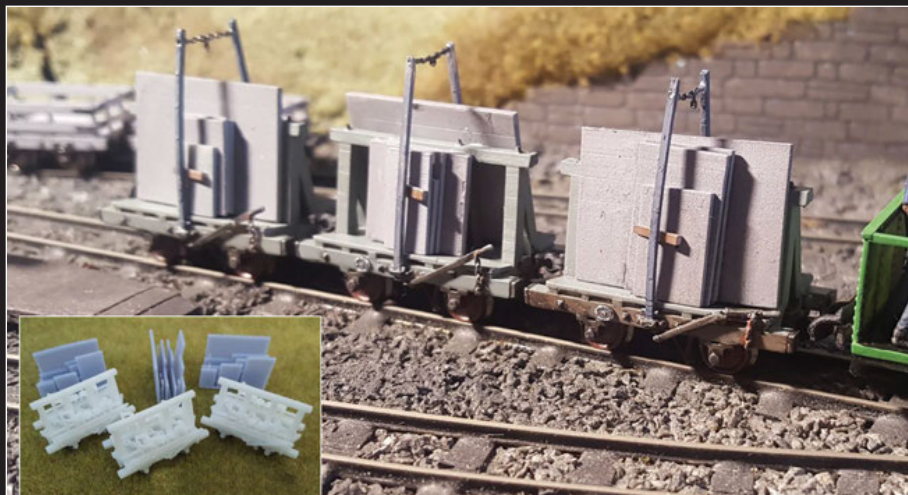
However 009 was by then suffering from many rabbit-warren layouts, an idea that Cyril Freezer devised in the *RM*. The trouble was, that although there were one or two good ones, the majority were jokes built by 'serious' modellers in OO who thought it was funny to have a minute layout with a train chasing its tail in and out of tunnels; layouts that did not bear any resemblance to a narrow gauge line. Two of my many 6ft by 1ft layouts were named Futtocks End and Bumble Hole.

Sometime in the mid 1990s the 009 Society made me an Honorary Life Member, apparently 'In Recognition of Extensive Work in Promoting Interest and Publicising Small Scale Narrow Gauge Railway Modelling'.

Worth waiting for

Now that the commercial side of railway modelling has started realising the market for 009, my dreams of many years have been fulfilled with accurate models of actual locos and stock.

My final 009 layout is 'Llandyrau', now in the care of my friend and joint operator Jo Vincent. It appeared at the Warley National a few years ago and other shows. I have now moved to 09 (7mm scale minimum gauge railways on 9mm gauge) but my next creation will be a small layout for my home use in 7mm standard gauge, modelling the Wantage Tramway – well out of the remit of this magazine! **NGW**



Slab sides from Brooks

In *NGW*172 we detailed the growing range of 009 scale 3D-printed stock available from new manufacturer Brooks 3D and in *NGW*173 we reviewed some of the company's Welshpool & Llanfair vans. Well now Nigel Brooks has been in touch with his latest releases, a set of slab wagons, in this case examples from the Corris Railway.

While a very familiar part of the slate industry scene these wagons can be very tricky to model in 009, so the Brooks kits are welcome. Nigel does add, however, that the kit includes all the individual slate slabs, poles, wedges and various chains, with spares, but these can be fiddly and delicate to add, so the kit is not one for beginners.

The kits cost £19 per set of three or £34 for a set of six – these wagons were seldom used singly. They can be ordered at www.brooks3dmodels.com

Nigel has also alerted us to his probable future releases which include several items 009 modellers will find useful. In likely order of release they include Snailbeach hopper wagons, Penrhyn rubbish wagons which will be able to be optionally ordered with correct double-flanged wheels, Ffestiniog slate wagons in the later London North Western and Great Western design, the iconic open Penrhyn quarryman's carriage, an open carriage from the Leighton Buzzard Railway and a Ruston & Hornsby diesel loco body kit for the 44/48 version that worked at RAF Fauld.

■ Roving model reporter *Phil Parker* recently spotted a couple of interesting creations on his travels. At right the under design Pechot-Bourdon will be the next model from Boot Lane Works (<http://bootlane.org.uk>) – it was due to be launched at the National Garden Railway Show on 15th April. "A bit bigger than their normal models, and I'll admit, quite tempting," Phil says.

Spotted for sale at a recent LGB day at Garden Railway Services in Princes Risborough was the remarkable one-off K27 below, in 3½-inch gauge and described by Phil as "a static, made from wood and junk – it was a stunning model, five feet long, and only £500. It was probably a good thing for me that it was already sold when I got there..."



The Hendre Ddu Tramway – 'Blue Stones and Green Trees' by Dan Quine

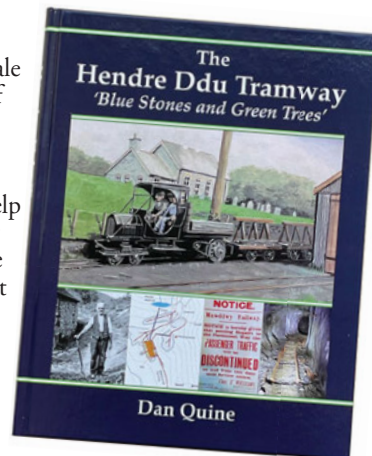
Many books are published on the narrow gauge but without doubt the most interesting are those that throw new light on obscure systems, those that have previously only been mentioned in brief or included in round-ups of more wide-ranging subjects. The authors of such works really have to work hard on their subject, the obscurity making it a much harder task to track down sources of information to produce a full story.

Dan Quine, author of this new work on the tramway that served one of the much lesser-known quarries of the Welsh slate industry, is certainly an author that does his homework. Dan is a regular contributor to *NGW* and indeed a piece based on the research for the book appears in this month's issue. It will give readers a good idea of just how detailed a story is related in this 220-page high quality hardback, the kind we've got used to from this publisher.

No avenue is left unexplored – the book opens with the first quarrying operations in 1865 and chapter by chapter describes the various stages that saw the slate working in the area served by the tramway expand and then inevitably decline, the final act being Forestry Commission tramways worked after 1948. The tramways and their operation are fully described, as is each individual quarry, and there is a chapter on rolling

stock which includes excellent scale drawings, despite the difficulty of figuring out just what the stock used on these lines looked like.

Plenty of top quality period photos and multi-colour maps help create a full picture of the subject in what is a definitive work. Note too that this is a reprint – the first print published last year sold out very quickly. All narrow gauge enthusiasts will find the book of great interest but you should not hang about getting hold of a copy, especially if you are a student of the slate industry... *AC*

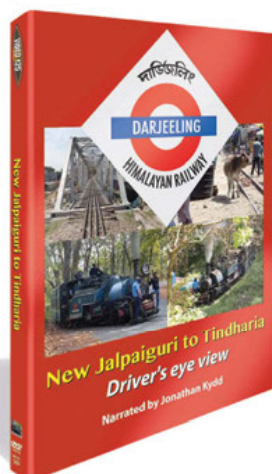


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■ Back in 2020 we reviewed a new and totally absorbing DVD from the Darjeeling Himalayan Railway Society, which provided exactly as its title suggested – a Driver's Eye View of the upper half of the railway, from Tindharia to Darjeeling, giving viewers a real impression of what it is like to crew a loco on this unique and famous Indian line.

We eagerly awaited the 'other half' and at last it's coming! Unfortunately it's just missed this issue's print deadline but we hope to publish a full review next month. We are assured quality and production are at the same high level as Part 2. Prices have been held at the same level as Part 2 at £25 for the DVD and £30 for the BluRay, with discounts for DHRS members. Details are at the DHRS shop, www.dhrs.org

Festiniog Law by Stephen Murfitt

You thought that just about everything that could be written on the history of the Festiniog Railway had already appeared in print – not at all! This rather different book focuses not on the surveyors or engineers but those who have had to tackle the often highly complex legal requirements that have arisen over the past 150 years or more.

You won't be surprised to hear that the author, Stephen Murfitt, is a lawyer himself, and honorary solicitor to the FR company, having first been introduced to the line when he trained as a volunteer locomotive fireman.

Stephen can write too – while the subject matter will definitely not be for everyone, and could so easily fall into the trap of being very dry and boring, it is actually presented in a very readable and interesting form. We are taken from heavyweight areas such as the creation of the statutory company and how litigation of 1868 added to the common law of Wales, through civil disputes, accidents, compensation cases and more. Of course the infamous battle over the Welsh Highland Railway reopening features as do the efforts to win compensation from the electricity authorities for flooding part of the route, but there is also much more to a title that your reviewer found surprisingly absorbing. *AC*

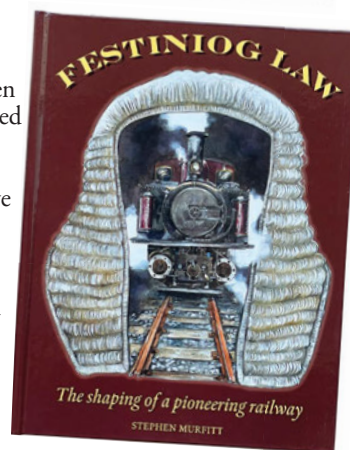
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Lots more on the 4ft

James Waite's article on 4ft gauge railways in *NGW* 173 set me trawling my photo collection. The Cambrian Railways' Porthywaen branch shared with its coast line at Portmadoc (now Porthmadog) in having a mixed-gauge crossing, with the 4ft gauge Whitehaven Quarry at the former and the 2ft gauge Welsh Highland Railway at the latter.

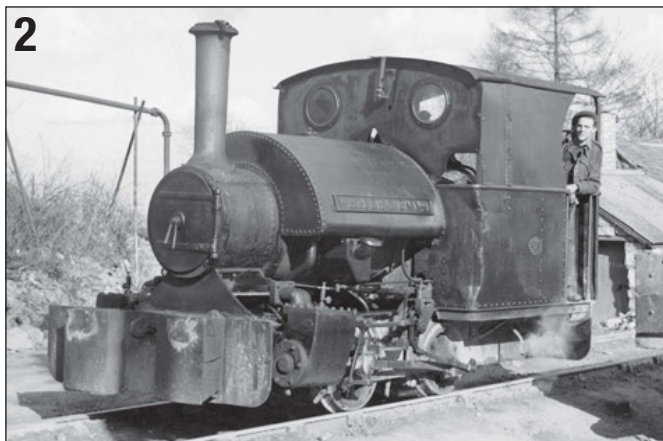
My first photograph (photo 1) shows a standard-gauge train on the crossing and the second (photo 2) one of the quarry's Bagnalls, 'Whitehaven No. 1', this was taken by George Alliez.

Regarding the Assheton-Smith

private railway, known by enthusiasts as the Padarn Railway, between Llanberis and Port Dinorwic (nowadays known as Velinheli) I have enclosed a photograph (Photo 3) of Horlock 0-4-0 'Jenny Lind' in steam at Gilfach Ddu. I think this image has only been previously published in a tightly cropped form and never from an original print.

In her book about the Penrhyn and Dinorwic railways (David & Charles, 1975) Susan Turner said that this was the only photograph of one of the Horlock locomotives in steam but on a lantern slide image of a slate train during shunting operations (Photo 4), the chimney of one of

2



Photos 1-5:
Narrow gauge historian and regular *NGW* contributor **Peter Johnson** has raided his archive to find more examples of 4ft gauge lines. For full details of each photo see Peter's letter.

them can be seen. Another lantern slide (Photo 5) shows two of the velocipedes used by the quarries before they got their own train.

Peter Johnson

Andrew C replies: Fascinating stuff Peter of rare but also fascinating examples of the narrow gauge...

Vivarais diesel conundrum

I was interested to learn from *NGW* 173 that the Brissonneau & Lotz locos which had been amongst the attractions of the Provence system had been obtained by the CF Vivarais but would query the statement that BB403 (or 040-003 if it's regained its original number) had been up until then the only diesel on the Vivarais.

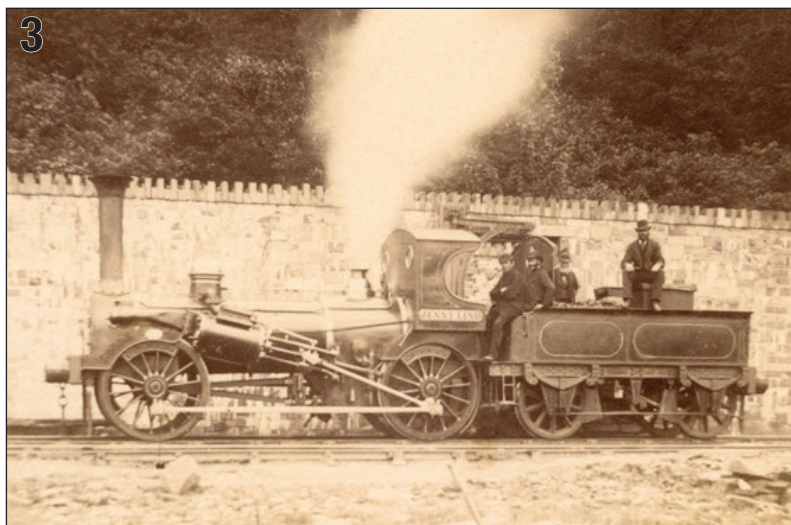
Even leaving aside the indigenous shunters X and Y, I understood from *Voie Etroite* for Feb-March 2012 that the CFC's 405 had been transferred to the Vivarais along with BB404 (as it was then numbered) and the former was certainly at Lamastre in September 2017, albeit in non-operational condition.

While writing, do you know if any of the last three Renault ABH railcars to run on the Provence line survive?

Guy Brigen

Andrew C replies: Your editor freely admits to French internal combustion locos not being his strong point! Can any reader answer Guy's query?

3



Got a point to raise or a question to ask of a narrow gauge nature? Send to editor Andrew Charman at the editorial address on page 3.



5



From a Different age...



Cape Bedfordshire...

Reader **Mick Bass** set us a challenge by claiming "I absolutely bet no-one has sent you a picture of working UK Cape-gauge steam for a while!"

Mick's picture was taken in 1979 November at Whipsnade Zoo in Bedfordshire. Built in 1896 by Sharp Stewart, the former Cape Government Railways 4-8-0 loco owned by wildlife artist David Shepherd and displayed at the zoo on a short length of 3ft 6in gauge track was being steamed, on what Mick describes as "a bit of coal and a whole pile of zoo-chopped trees. Ron Druce was the custodian and it was probably only steamed four or five times as it probably took a bit of persuading to get the boiler passed!"

As reported in *NGW* 169 in 2022 the loco took up residence at the National Railway Museum in York.



■ Finally an example of the 'seldom seen'. Phil Barnes, another regular in our pages, sent us the shot at right with the following explanation. "Since the beginning of this year, I have been doing a bit of volunteering at Amberley Museum and have found a niche on permanent way work – track inspections which lead to fixing problems with fishplates and rail securing bolts. We also often have a couple of sleepers to change (like you find).

The photo shows the p-way engineering train which is packed with all the gear that you need to maintain the trackwork. This is something that is not normally seen, other than when its 'services' are needed.

The rest of the time it lives in a shed – the train that enables everything to happen hidden away...



Unexpected Narrow Gauge...



A find at Hetton

I was recently exploring the remains of the Hetton Colliery Railway near Hetton in County Durham (*writes regular NGW contributor Matt Ditch*). I was aware that the original 1910-built railway waggon repair shop was still standing on an industrial estate within the town.

What was I was not aware of, was that a small section of narrow gauge track is still visible exiting the works. The building the track enters looks to be a later addition to the original structure – at as guess I would say it was for the repair of narrow gauge mining equipment.

The building is currently used by Trackwork, which manufactures track-related components. Sadly in 2021 the company announced that it intended to demolish the building based on its condition, although nearly two years later it does still appear to be in use by them.

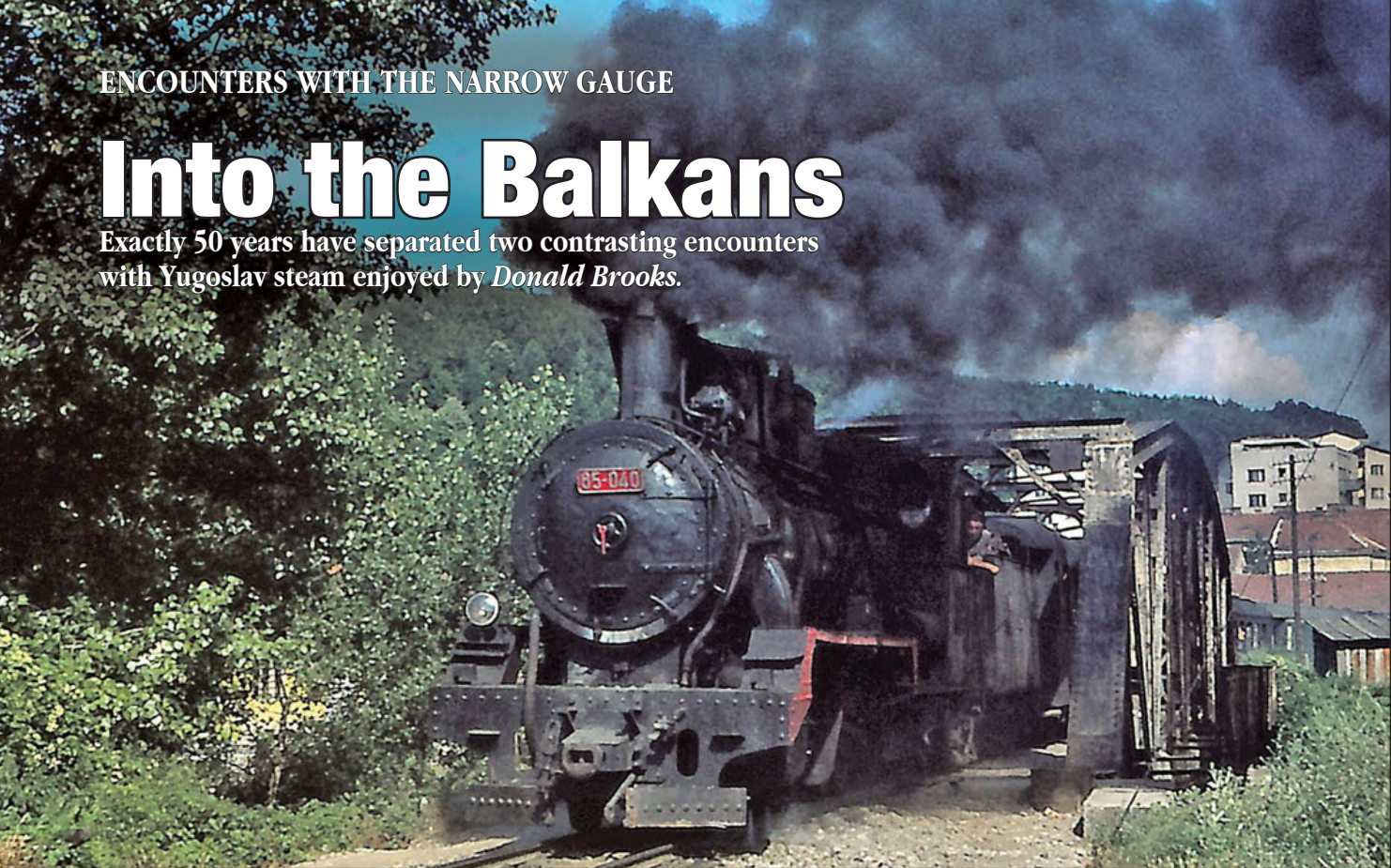
■ If any reader can provide more information please get in touch via the address on page 3.



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Into the Balkans

Exactly 50 years have separated two contrasting encounters with Yugoslav steam enjoyed by *Donald Brooks*.



Back in July 1973 a friend and I were travelling around Europe in search of steam. It was the second year of Inter-Rail and we certainly got value for money out of our tickets, seeing locomotives in action from Portugal to Greece and most points in-between. But for both of us the undoubted highlight was the 760mm gauge main line, then worked by a mix of steam and diesel traction, that connected Sarajevo in Bosnia with Titovo Užice in Serbia.

Not that we knew anything about either Bosnia or Serbia at the time. As far as we were concerned it was all simply Yugoslavia – we were blissfully unaware of the complex ethnic, national and religious background that makes the area so volatile.

Nor did we realise that Sarajevo – Užice was in effect two railways, an early 20th century line from Sarajevo to the Serbian border in support of

Austrian Imperial interests and the later Yugoslav connection through to Belgrade. Part of the Serbian section, the celebrated climb over the Šargan mountains, was reconstructed following closure in 1974 and still provides a heritage service today.

We saw a lot in 1973 but there was so much more we could have seen. We did not realise that the end was fast approaching for Yugoslavia's narrow gauge and we should at least have taken the diesel railcar over the spectacular route from Čapljina down to Dubrovnik and on to Nikšić. There was also the Steinbeisbahn main line from Prijedor to Lička Kaldrma, a fascinating journey even behind a class 740 diesel.

We were probably already too late to see one of Yugoslavia's major narrow gauge attractions, the class 97 rack and adhesion 0-6+4 locomotives constructed from the late 19th

Above: Yugoslav steam at its finest back in July 1973. 2-8-2 no 85.040, Slavonski Brod 27 of 1940, crosses the River Đetinja immediately after leaving Titovo Užice.

Below: 97-028, Floridsdorf 2149 of 1913, at the head of a train of very ramshackle wagons displayed in the grounds of the Slovenian Railway Museum, Ljubljana, this photo taken in July 2022.

Photos by Donald Brooks

century for services over the Ivan and Komar passes. The Ivan pass, well-known in the UK from Pat Whitehouse's book *On the Narrow Gauge*, had closed in 1966, replaced by a new standard gauge line to the Adriatic coast.

Unseen steam

The Komar Pass line, between Travnik and Donji Vakuf north-west of Sarajevo, remained open, though passenger services had been withdrawn the month before our visit. Diesel-worked freight survived for another year, with possible occasional use of the surviving 97s in support, but heading off into rural Bosnia by bus in the hope of seeing one would have been too lengthy and risky an enterprise. Greek steam beckoned and we continued our journey further south.

I finally encountered a 97 in the summer of 2022. On a day off from walking in Slovenia's Julian Alps, my wife and I took the train to Ljubljana to visit the railway museum. While its exhibits are primarily standard gauge, preserved in the open air with three goods wagons is 97-028, Floridsdorf 2149 of 1913, the whole train looking rather the worse for wear.

It's an unlikely location for a locomotive which never worked in Slovenia, a country which never had a rack railway. At least the loco survives, along with several of its sisters, including 97-019 in the even less likely setting of the Deutsches Museum's Lokwelt outpost at Freilassing in Bavaria. **NGW**



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