

WIRING BASICS | **CUT THE COST OF MODELLING** | **WEATHER LIKE A PRO**

BRITISH RAILWAY MODELLING

BRM
SEPTEMBER 2022



SOUTHGATE PARK

**MODELLING THE CAPITAL ON THE CUSP OF
RAIL TRANSFORMATION**

**4 GREAT
LAYOUTS
INSIDE!**

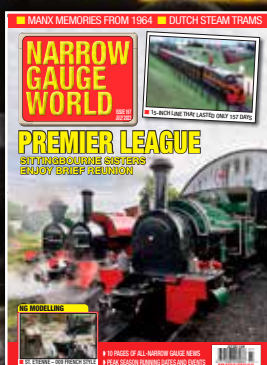
**WORLD OF RAILWAYS
TV**

4 BONUS ARTICLES

5 EXTRA VIDEOS

61 EXCLUSIVE IMAGES

**MAKING TRACKS 2:
WHAT'S NEXT FOR PETE WATERMAN AND CO?**



CLASS 319 4-CAR EMU

in N Scale from Graham Farish

The Class 319 4-Car Electric Multiple Unit is here! Available in N scale for the first time, these highly detailed models are brought to life by the exquisite livery application you've come to expect from Graham Farish. What's more, just like the real units, we've used the 319 as the basis for a Class 769 Bi-mode Multiple Unit too.

With four models available covering a variety of eras, which will you choose?



372-875 - Class 319 4-Car EMU 319004 BR Network SouthEast (Revised)

372-876 - Class 319 4-Car EMU 319382 Thameslink

372-877 - Class 319 4-Car EMU 319362 Northern Rail

ALL MODELS FEATURE:

- Unobstructed interior including seat and table detail
- Sprung Brecknell Willis or Stone Faiveley pantograph
- Integral conductive couplings between vehicles
- Functional multiple unit couplings at the outer ends
- Electrical pickup from all axles
- Directional lighting - switchable via DCC or the chassis-mounted switches
- Internal lighting • Speaker Fitted
- One decoder required for DCC
- Next18 DCC decoder socket - easily accessed via a removable underframe panel
- Powerful coreless motor and flywheel

BACHMANN EUROPE ARE PROUD WINNERS OF BRM MANUFACTURER OF THE YEAR 2021



CLASS 769 4-CAR EMU



372-850 - Class 769 4-Car BiMU 769008 Transport for Wales

With the same high specification as the Class 319s, and the fresh new livery of Transport for Wales, this newly-tooled Class 769 is accurately modelled with the pantograph removed and diesel engines fitted to each driving car!



Available now from Bachmann stockists. For more information and to find your nearest stockist visit: www.bachmann.co.uk

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R.T.R in unpainted brass/factory painted

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GWR/BR Parcels Railcar No.34.....	£550.00
GWR 1361 Early black chassis.....	£525.00
GWR/BR 1361 Late black chassis.....	£525.00
LY/LMS/BR Pug black.....	£450.00
LNER/BR J39 0-6-0 black.....	£815.00
GWR/BR 8750 Pannier black chassis.....	£525.00

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L&Y/LMS/BR Pug 0-4-0T.....	from £525.00
GWR/BR 1361 0-6-0ST.....	from £695.00
GWR/BR 8750 Pannier tank.....	from £695.00
GWR/BR Razor Edge railcar.....	£835.00
GWR/BR Parcels railcar.....	£810.00
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WORLD OF RAILWAYS

TV

FEATURE

Pete Waterman's 'Making Tracks 2' model railway

From July 18 to September 3, 2022, Chester Cathedral is home to Making Tracks 2 – Pete Waterman's layout homage to the West Coast Main Line. Howard Smith visited to see more.

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



PLAY

PRACTICAL

Making a removable wagon load

Phil Parker uses some GeoScenics products to fill his wagons and offers some top tips to making them look authentic.



PLAY

FEATURE

Driving a 5-inch gauge steam locomotive

Andy, from Garden Railway Specialists, talks Phil Parker through the controls of a 5 inch gauge live steam locomotive.



PLAY

p20



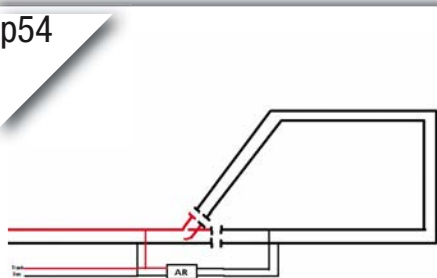
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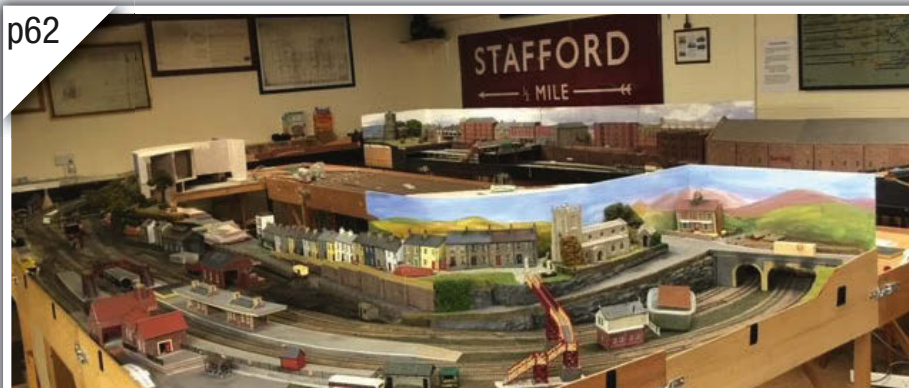
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PRACTICAL BRM

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Until September 3, Chester Cathedral is home to Making Tracks 2 – Pete Waterman's layout homage to the West Coast Main Line. Howard Smith visited to see more.

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BRITISH RAILWAY MODELLING

BRM

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HORNBY

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The Class 92 is a dual-voltage electric locomotive capable of running on 25kV AC from overhead lines via pantograph, or 750V DC from a third rail. It was designed specifically for operation through the Channel Tunnel between Britain and France. Since then, they have spread their wings, operating across the electrified sections of the national network and even to Eastern Europe.

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Models will feature dual motorised pantographs, a wealth of separate detail, running on a heavy die-cast metal chassis, powerbank capacitors, with availability in both DC/DCC Ready and DCC Sound Fitted formats, the latter including a twin speaker set up including an EM1 style 'Accurathrash' speaker, and ESU Loksound 5 digital decoder.

Due Q3 2022, our locomotives will be available and ready to perform from Q3 2022. Pre-order direct from Accurascale's website today www.accurascale.co.uk



The Class 92s to be released are:

92 001 EWS, Victor Hugo
92 003 Railfreight, Beethoven
92 009 DB Schenker, Marco Polo
92 010 Caledonian Sleeper
92 020 GBRf
92 022 Railfreight Distribution, Charles Dickens
92 032 GBRf, ImechE Railway Division
92 036 EWS, Bertolt Brecht

92 038 Caledonian Sleeper
92 042 DB Schenker
92 043 Euro Porte 2, Debussy

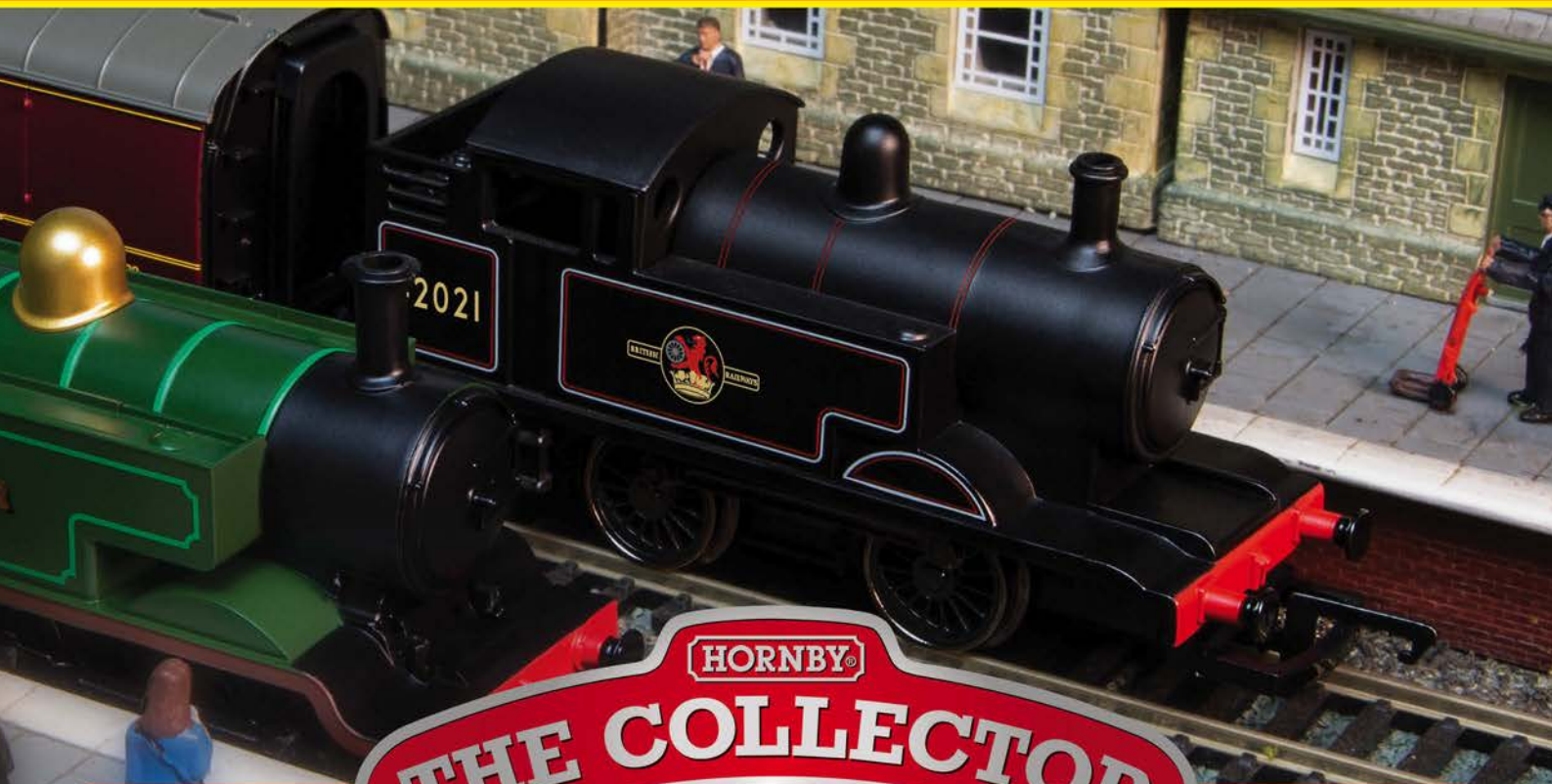
Retailer commissioned locomotives will also be available. These are; **92 015**, **92 017** and **92 023**.

1:76/4mm scale, OO gauge

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Welcome to September

It's not only our weather that's been heating up of late, the amount of new models arriving at our shores over the past few months has been on the increase, too. And more fantastic locomotives are on their way, including an all-new Class 37, which Bachmann announced – much to our surprise – at the DEMU exhibition last month.

The backlog of orders and new releases caused by Covid seems to be finally catching up, and it's great to see our hobby so busy.

What hasn't gone unnoticed is the increase in cost. Everything has become more expensive, from the price of RTR locomotives and scenic items to the cost of wood, electrics and building kits, all essential items when looking to build a layout.

We've had many letters from readers the past few months sharing their frustrations and, although we can't help lower price tags, there are a few tips we can offer to help reduce the cost of your modelling – see our article on page 84.

We've also been doing some work over

recent weeks on rejuvenating second-hand models and selling them on for a profit – look out for this series in future issues.

Hobbies are so much more than a device to fill gaps in time, they're great for escapism, and in the case of model railways, offer a wealth of social opportunities, too. Turn off the news and spend more time modelling is the best advice we can give you!

Back to BRM September, and there are four fantastic layouts for your perusal, including our cover layout, 'Southgate Park', which offers great tips for purchasing a second-hand layout. Also in 'OO', Paul Davis shares the highs and lows of building his first layout, 'Mantles Wood'. On another second-hand purchase, Arline Wilson took on the task of modernising 'Hadley, Stonebrook and Swale Railway' to make the layout more reliable for exhibition use, and finally, we visit the vibrant 'Hook Basin', which majors in scenic detail, while offering much operational fun.

This issue has a selection of practical features, including step-by-step builds on two iconic (and contrasting) buildings for your

layout – a church and a cinema. There's also top wiring and weathering advice, plus we catch up with The Stafford Railway Circle, and 'Making Tracks 2' at Chester Cathedral to see Pete Waterman and team's all-new 64ft-long layout creation – an ode to the West Coast Main Line on which he has travelled for more than half a century.

We've been inundated with new products at BRM headquarters, and in your September issue, we cast our expert eyes over the new Accurascale HCA and CDA wagons, the all-new TT range from Peco, the latest 'Magnesite' hoppers from IRM, plus, for OO9 modellers, we take a closer look at the new Snailbeach hopper wagons from Peco.

As always, if there is anything you would like to see us feature in future issues of BRM, please get in touch. Feel free to share images of your layouts and projects with us, too – we love seeing what you're building in your loft, garden shed, garage, spare room, workbench, and anywhere else! Enjoy your issue.

The BRM team

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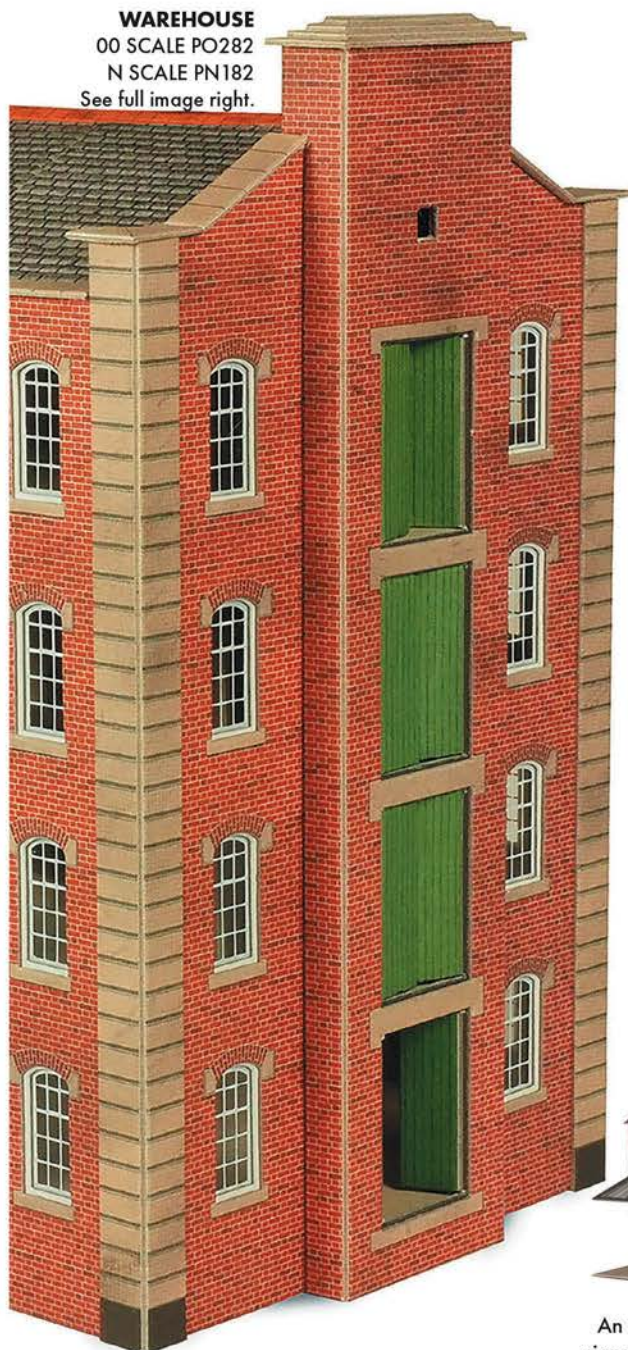
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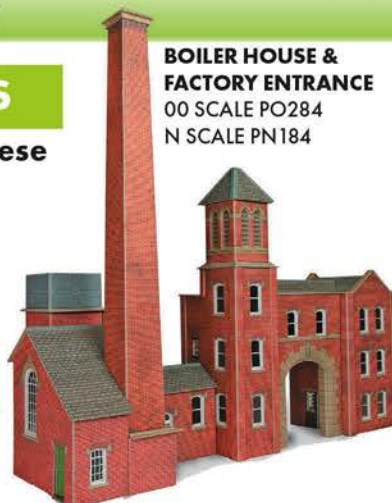
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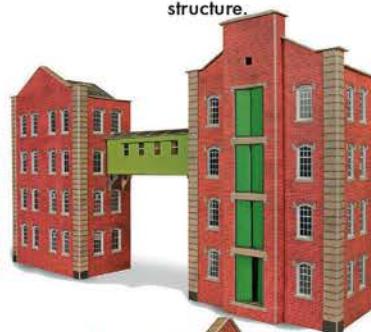
WAREHOUSE
00 SCALE PO282
N SCALE PN182
See full image right.



**BOILER HOUSE &
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00 SCALE PO284
N SCALE PN184



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Parish Church Kit.

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 00 SCALE PO253
 N SCALE PN153



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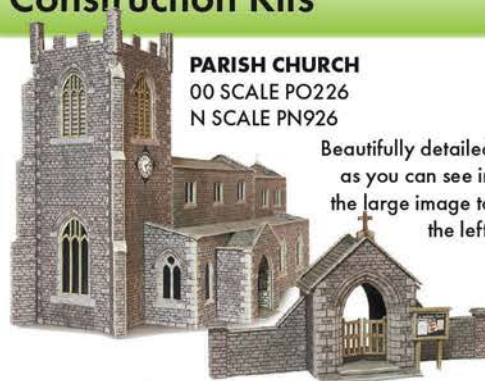


TOWN END COTTAGE
 00 SCALE PO267
 N SCALE PN167

Based on a lovely old house
 at the end of Lacock High
 Street in Wiltshire



VILLAGE SHOP & CAFE
 00 SCALE PO254
 N SCALE PN154



PARISH CHURCH
 00 SCALE PO226
 N SCALE PN926

Beautifully detailed
 as you can see in
 the large image to
 the left.



COACHING INN
 00 SCALE PO228
 N SCALE PN128



GRANGE HOUSE
 00 SCALE PO257
 N SCALE PN157

PLATFORMS

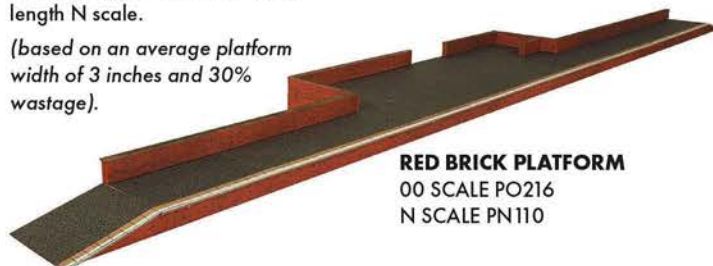
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 to 7ft in length 00 scale & 12ft in
 length N scale.

(based on an average platform
 width of 3 inches and 30%
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 00 SCALE PO216
 N SCALE PN110

Build to any shape. Straight, Curved, Island or Bays.

GARDENER'S COTTAGE
 00 SCALE PO258
 N SCALE PN158



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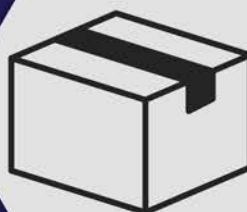
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L&MR 'Lion' 0-4-2
Stage: Engineering Sample Due: 2023
Price: from £152.96 Digital: NEXT-18
hattons.co.uk/rapidolion



Hawthorn Leslie 0-4-0ST
Stage: CADs Due: Q3 2023
Price: from £99.45 Digital: NEXT-18
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Hunslet 16" 0-6-0ST
Stage: Engineering Sample Due: TBC 2022
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hattons.co.uk/rapidohunslet



Class 78xx 'Manor' 4-6-0
Stage: In Production Due: Q3 2022
Price: from £140.72 Digital: NEXT-18
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Class 78xx Manor 4-6-0
Stage: Engineering Sample Due: Q3 2022
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascalemanor



HR Class I 'Jones Goods' 4-6-0
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OO Diesels & Electrics



Class 31
Stage: Engineering Samples Due: Q2 2023
Price: from £169.99 Digital: 21-pin
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Class 37/0, 37/4 & 37/6
Stage: Decorated Samples Due: Q3/4 2022
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Class 47
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Class 59
Stage: Painted Samples Due: Late 2022
Price: from £136.31 Digital: 21-pin
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BR NBL Prototype 10800
Stage: CADs Due: 2023
Price: £199.71 Digital: 21-pin
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Class 104 DMU
Stage: CADs Due: Q1 2023
Price: from £296.65 Digital: 21-pin
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OO Coaches & Wagons



Genesis 4 & 6 Wheel Coaches
Stage: Deco Samples Due: 2022/23
Price: from £30 Digital: 18-pin
hattons.co.uk/genesis



BR Mk2B Coaches
Stage: Deco Samples Due: Q4 2022
Price: £59.95 each Digital: Yes
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GWR Toplight City Coaches
Stage: Deco Bodyshells Due: 2023
Price: £51 each Digital: 6-pin
hattons.co.uk/dapoltoplightcity



Siphon G Vans
Stage: Early Samples Due: Q1 2023
Price: £54.95 Digital: N/A
hattons.co.uk/accsiphong



SECR 10t Goods Van
Stage: Deco Samples Due: 2023
Price: £28.01 Digital: N/A
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VIX Ferry Vans
Stage: Engineering Samples Due: 2022
Price: £39.06 each Digital: N/A
hattons.co.uk/rapidovix



Chaldron 4-wheel Wagons
Stage: Deco Samples Due: Q3 2022
Price: £44.99 (3-pack) Digital: N/A
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GWR 10t Iron Mink Vans
Stage: Engineering Samples Due: Q4 2022
Price: £28.01 each Digital: N/A
hattons.co.uk/rapidoironmink



45t OAA Wagons
Stage: CADs Due: Q1 2023
Price: £33.96 each Digital: N/A
hattons.co.uk/rapidooaa

N Gauge Steam Locos



LSWR Class M7 0-4-4T
Stage: Prototype Due: Q4 2022
Price: £83.2 Digital: Yes - PCB swap
hattons.co.uk/dapolm7

N Gauge Diesel Locos



Class 28 Co-Bo
Stage: Early sample Due: TBC 2022
Price: £534.65 Digital: NEXT-18
hattons.co.uk/rapidocobo



Class 59
Stage: Engineering Samples Due: Q4 2022
Price: from £136 Digital: NEXT-18
hattons.co.uk/dapoln59

N Gauge Wagons



JHA 102t Hopper Wagons
Stage: Engineering Samples Due: Q3/4 2022
Price: £33.96 Digital: N/A
hattons.co.uk/dapolnjha

O Gauge Steam Locos



Class J94 Austerity 0-6-0ST
Stage: CADs Due: 2023
Price: from £212.50 Digital: 21-pin
hattons.co.uk/dapolj94



Class B4 0-4-0T
Stage: Deco Samples Due: Q3 2022
Price: from £195.50 Digital: 21-pin
hattons.co.uk/dapololswrb4

O Gauge Diesels/ Electrics



Class 26
Stage: Deco Samples Due: Q3 2022
Price: from £509.15 Digital: ESU XL
hattons.co.uk/heljano26



Class 37/0 Re-Tool
Stage: CADs Due: 2023
Price: £534.65 Digital: ESU XL
hattons.co.uk/heljano37



Class 55 'Deltic'
Stage: Early sample Due: 2023
Price: from £594.15 Digital: ESU XL
hattons.co.uk/heljandeltic



Class 66
Stage: Engineering Sample Due: Q3 2022
Price: from £285.92 Digital: 21-pin
hattons.co.uk/dapolo66



Class 73
Stage: Engineering Sample Due: 2023
Price: from £636.65 Digital: ESU XL
hattons.co.uk/heljan73

O Diesel Multiple Units



GWR Streamlined Railcar
Stage: Painted Samples Due: Q3 2022
Price: from £297.50 Digital: 21-pin
hattons.co.uk/dapolorailcar



Class 117 2/3-car DMU
Stage: Early samples Due: 2022
Price: from £551.65 Digital: 21-pin
hattons.co.uk/heljan117

O Coaches & Wagons



Stroudley 4 Wheel Coaches
Stage: Deco Samples Due: Q3 2022
Price: from £96.77 Digital: Yes TBC
hattons.co.uk/dapolstroudley



Mk2 & Mk2A Coaches
Stage: Prototypes Due: Late 2022
Price: from £237.15 Digital: N/A
hattons.co.uk/heljanmk2



Mk1 Covered Carriage Trucks
Stage: Early sample Due: 2022
Price: from £152.15 Digital: N/A
hattons.co.uk/heljanccct



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OO Gauge (1:76 Scale)

Steam locos



H4-AB14-006 Andrew Barclay 0-4-0ST 14" 2069 "Little Barford" in Acton Lane Power Station blue wasp stripes (Hat) (RRP £99) **BARGAIN**...£84



R30001 Class 8P 'Princess Royal' 6203 "Princess Margaret Rose" in LMS crimson lake (Hor).....£229



R3854X Class 8P Princess Royal 4-6-2 6212 "Duchess of Kent" in LMS crimson lake - DCC fitted (Hor) (RRP £254.49) **BARGAIN**...£199



R3371 Class A4 4-6-2 4468 "Mallard" in LNER garter blue - Railroad range (Hor).....£121



R3541 Class Q6 Raven 0-8-0 2265 in LNER black pre-war lettering (Hor) (RRP £194.99) **BARGAIN**...£114



R3702 Class W4 Peckett 0-4-0ST "Daphne" in Tytherington Stone Co. purple (Hor).....£115



H4-P-013 SECR P Class 0-6-0T 5027 in ROD green (Hat) (RRP £99) **BARGAIN**...£84



R30070 Class 59 59101 "Village of Whitley" in Hanson Aggregates - Railroad Plus range (Hor).....£82



R30089 Class 67 67014 in Transport for Wales white and red (Hor) **NEW**...£175



4D-022-019 Class 68 68016 "Fearless" in DRS compass blue (Dap) **NEW**...£135.99



4D-022-020 Class 68 68018 "Vigilant" in DRS compass blue (Dap) **NEW**...£135.99



4D-022-021 Class 68 68027 "Splendid" in TransPennine Express (Dap) **NEW**...£135.99



4D-022-022 Class 68 68031 "Felix" in TransPennine Express (Dap) **NEW**...£135.99

Electric locos



R3569 Class 71 E5005 in BR blue (Hor) (RRP £206.99) **BARGAIN**...£99



R3568 Class 71 E5018 in BR green no yellow ends (Hor) (RRP £206.99) **BARGAIN**...£99



R3893 Class 91 91117 "Cancer Research UK" in GNER (Hor) £196.50

Bi-Modal Multiple Units



R3762 Class 8001EP five car pack 800201 in LNER "Azuma" (Hor).....£430.50

Coaches



R4493B 63' Hawksworth first corridor W8119W in BR crimson and cream (Hor) (RRP £60.49) **BARGAIN**...£33



R4188D ex-LMS Period II 12-wheel dining car M236M in BR crimson and cream (Hor) (RRP £60.49) **BARGAIN**...£23



LMSCoachPackA LMS Period III Non-Corridor 4-pack in BR crimson (Includes R4799, R4800, R4801 & R4801A) (Hor) (RRP £237.96) **BARGAIN**...£108



R4816A Maunsell restaurant kitchen and dining car 7865 in SR olive green (Hor) (RRP £58.49) **BARGAIN**...£29



R40164 Mk4 FO first open (accessible toilet) in GNER - 11317 "Coach L" (Hor) **NEW**...£40.50



R40165 Mk4 FO first open in GNER - 11417 "Coach K" (Hor) **NEW**...£40.50



R40146 Mk4 RFM Restaurant First in GNER - 10324 "Coach G" (Hor) **NEW**...£40.50



R40166 Mk4 TSO standard open in GNER - 12223 "Coach A" (Hor) **NEW**...£40.50



R40168 Mk4 TSO standard open in GNER - 12444 "Coach D" (Hor) **NEW**...£40.50



H4-RHTT-002 Rail Head Treatment Train "Water Jet" 2 wagons and water jetting modules (Hat) (RRP £118) **BARGAIN**...£99

N Gauge Diesel locos



2D-007-013 Class 66/4 66413 "Lest We Forget" in Freightliner / G&W orange (Dap) **NEW**...£96.77



2D-007-014 Class 66/7 66780 "The Cemex Express" in GBRI / Cemex (Dap) **NEW**...£96.77



2D-020-004 Class 153 single car DMU 153323 in Arriva Trains Wales (Dap) **NEW**...£105.53

Bi-Modal Multiple Units



K10-1671 Class 800/0 5-car BiMU IET 800021 in GWR green (Kat).....£178

O Gauge (1:43 Scale)

Steam locos



H7-A3-007 Class A3 4-6-2 60103 "Flying Scotsman" in BR green late crest and unstreamlined corridor tender (Hat) (RRP £750) **BARGAIN**...£579



H7-A4-004 Class A4 4-6-2 4489 "Dominion of Canada" in LNER Garter blue steel numbers/letters and unstreamlined corridor tender (Hat) (RRP £750) **BARGAIN**...£579



H7-A4-006 Class A4 4-6-2 60007 "Sir Nigel Gresley" in BR express blue unstreamlined corridor tender - "Record Breaker" (Hat) (RRP £750) **BARGAIN**...£579



H7-A4-008 Class A4 4-6-2 60009 "Union of South Africa" in BR green late crest and unstreamlined corridor tender (Hat) (RRP £750) **BARGAIN**...£579

Coaches



7P-001-305 Mk1 CK composite corridor M15928 in BR blue and grey (Dap) **NEW**...£175.07



7P-001-305U Mk1 CK composite corridor BR blue and grey - unnumbered (Dap) **NEW**...£175.07



7P-001-105 Mk1 SO second open E4215 in BR blue and grey (Dap) **NEW**...£175.07



7F-200-012 20 ton standard brake van in BR bauxite and yellow - B954768 (Dap).....£71.40



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31-171 L&YR Class 5 Tank No. 1042 L&YR Lined Black.....	£129.95
31-635B GWR 64XX Pannier Tank No. 6414 GWR Green, GWR lettering.....	£118.99
31-639 GWR 64XX Pannier Tank No. 6421 BR Lined Green E/E.....	£118.99
31-976B BR Standard 3MT Tank No. 82041 BR Lined Green L/C.....	£140.95
31-982 BR Standard 3MT Tank No. 82018 BR Lined Black L/C, weathered.....	£152.99



32-114B Class 08 Diesel Shunter No. 13052 BR Black E/E.....	£127.95
32-131A GWR 4575 Prairie Tank No. 5526 GWR Green w/Great Western lettering.....	£140.99
32-132 GWR 45XX Prairie Tank No. 4571 BR Lined Green E/E.....	£140.99
32-612A Class 90 Electric Locomotive No. 90 041 Freightliner Green.....	£195.95
32-613 Class 90 Electric Locomotive No. 90 026 BR InterCity Mainline.....	£195.95



32-614 Class 90 Electric Locomotive No. 90 019 "Penny Black" Rail Express System.....	£195.95
32-615 Class 90 Electric Locomotive No. 90 004 "City of Glasgow" Virgin Trains Original.....	£195.95
32-617 Class 90 Electric Locomotive No. 90 044 Freightliner G&W.....	£195.95
32-619 Class 90 Electric Locomotive No. 90 030 "Crewe Locomotive Works" EWS.....	£195.95
32-620 Class 90 Electric Locomotive No. 90 048 Freightliner Grey, weathered.....	£195.95
37-951E Conflat Wagon BR Bauxite (Early) with BR Crimson BD Container.....	£23.95
37-954A Conflat Wagon BR Bauxite (Early) with "Pickfords" BD Container.....	£23.95



371-004B Class 08 Diesel Shunter 08 721 "Starlet" BR Red Star Express Parcels.....	£123.99
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371-004BSF Class 08 Diesel Shunter No. 08 721 "Starlet", DCC Sound Fitted.....	£208.99
371-005A Class 08 Diesel Shunter 08 950 "Neville Hill 1st" BR InterCity (Swallow).....	£123.99
371-007A Class 08 Diesel Shunter No. 08 953 BR Engineers Grey, DCC Sound Fitted.....	£208.99
371-007ASF Class 08 Diesel Shunter 08 953 BR Engineers Grey, DCC Sound Fitted.....	£208.99
371-010 Class 08 Diesel Shunter No. 08 441 RSS Railway Support Services.....	£123.99
371-011 Class 08 Diesel Shunter No. 08 417 Network Rail Yellow.....	£123.99
371-011SF Class 08 Diesel Shunter No. 08 417 Network Rail Yellow, DCC Sound.....	£208.99
371-012 Class 08 Diesel Shunter No. 08 919 Rail Express Systems.....	£123.99
371-012SF Class 08 Diesel Shunter No. 08 919 Rail Express Systems, DCC Sound.....	£208.99



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37-975B Conflat Wagon GWR Grey with "GWR" AF Container, weathered.....	£25.95
37-978A Conflat Wagon BR Bauxite (Early) with BR Ice Blue AF Container.....	£23.95
38-021A CEA Covered Hopper Loadhaul, weathered.....	£32.95



38-049 MTA Open Wagon 395296 Ex-Loadhaul EWS Lettering, weathered ballast load.....	£32.99
38-272A BR 22 Ton Prestlo Wagon BR Bauxite (TOPS) Rugby Cement.....	£42.95
38-273 BR 22 Ton Prestlo Cement Wagon Blue Circle Cement Yellow.....	£42.95
38-345B BR FNA Nuclear Flask Wagon Flat Floor with Flask.....	£50.99
38-347B BR FNA Nuclear Flask Wagon Sloping Floor with Flask.....	£50.99
38-874 BR 12 Ton Vanwide Ventilated Van No. B783314 BR Bauxite (TOPS), weathered.....	£33.99
38-875 BR 12 Ton Vanwide Ventilated Van D6783392 BR Departmental Olive Green.....	£33.99
39-735DC Mk2F DBSO Refurbished Driving Brake Standard 9710 BR InterCity Swallow.....	£127.99
39-736DC Mk2F DBSO Refurbished Driving Brake 2nd Open No. 9704 Anglia.....	£127.99
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74901 20 Ton Brake Van ZTR No. KD1955054 S&T.....	£43.95
74902 20 Ton Brake Van No. AD1955009 NSI.....	£43.95
74903 20 Ton Brake Van CAR No. B9550423 London Underground.....	£43.95
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44-039 Office Block.....	£39.95
44-080 Weighbridge.....	£29.95
44-113 Wooden Goods Shed.....	£101.99
44-191 Red Star Parcels Office.....	£25.95
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44-0126 Lucston Station Platform.....	£55.95
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4D-022-019 Class 68 Diesel Locomotive No. 68 016 "Fearless" DHS Compass.....	£136.00
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4D-022-019D Class 68 Diesel Locomotive No. 68 016 "Fearless" DHS Compass.....	£161.95
4D-022-020 Class 68 Diesel Locomotive No. 68 018 "Vigilant" DHS Compass.....	£136.00
4D-022-020D Class 68 Diesel Locomotive No. 68 018 "Vigilant" DHS Compass, DCC Fitted.....	£161.95

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2D-007-013 Class 66 Diesel No. 66 413 Freightliner Orange & Black "Lest We Forget".....	£96.95
2D-007-013D Class 66 Freightliner Orange & Black "Lest We Forget", DCC Fitted.....	£123.95
2D-007-014 Class 66 Diesel Locomotive No. 66 780 GBRT Cemex.....	£96.95
2D-007-014D Class 66 Diesel Locomotive No. 66 780 GBRT Cemex, DCC Fitted.....	£123.95
2D-020-004 Class 153 DMU Locomotive No. 153 323 Arriva Trains.....	£105.95
2D-020-004D Class 153 DMU Locomotive No. 153 323 Arriva Trains, DCC Fitted.....	£131.95

GRAHAM FARISH N

371-004B Class 08 Diesel Shunter 08 721 "Starlet" BR Red Star Express Parcels.....	£123.99
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371-004BSF Class 08 Diesel Shunter No. 08 721 "Starlet", DCC Sound Fitted.....	£208.99
371-005A Class 08 Diesel Shunter 08 950 "Neville Hill 1st" BR InterCity (Swallow).....	£123.99
371-007A Class 08 Diesel Shunter No. 08 953 BR Engineers Grey, DCC Sound Fitted.....	£208.99
371-007ASF Class 08 Diesel Shunter 08 953 BR Engineers Grey, DCC Sound Fitted.....	£208.99
371-010 Class 08 Diesel Shunter No. 08 441 RSS Railway Support Services.....	£123.99
371-011 Class 08 Diesel Shunter No. 08 417 Network Rail Yellow.....	£123.99
371-011SF Class 08 Diesel Shunter No. 08 417 Network Rail Yellow, DCC Sound.....	£208.99
371-012 Class 08 Diesel Shunter No. 08 919 Rail Express Systems.....	£123.99
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371-013 Class 08 Diesel Shunter No. 13287 BR Green E/E.....	£129.95
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371-013SF Class 08 Diesel Shunter No. 13287 BR Green E/E, DCC Sound.....	£244.95
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371-015DSF Class 08 Diesel Shunter No. 08 818 BR Blue, weathered, DCC Sound.....	£219.95
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371-065 Class 03 Diesel Shunter Ex-D2054 British Industrial Sand White.....	£118.99
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4D-022-021 Class 68 Diesel Loco No. 68 027 "Splendid" Trans Pennine Express.....	£136.00
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ACC1032-HUO-O-L BR 24.5T HOP24/HUO Coal Hopper Wagon L No. B333850.....	£59.95
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7L-001-005 Junction Signal GWR Flight Hand with two arms, shorter post to Right.....	£89.95
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7P-001-105 Lionheart BR Mk1 Second Open SO Coach No. E4215 BR Blue & Grey.....	£175.95
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7P-001-105U Lionheart BR Mk1 Second Open SO Coach BR Blue & Grey, unnumbered.....	£175.95
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7P-001-305 Lionheart BR Mk1 Corridor Composite CK M15928 BR Blue & Grey.....	£175.95
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7P-001-305U Lionheart BR Mk1 Corridor Composite CK BR Blue & Grey, unnumbered.....	£175.95
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7P-001-305U Lionheart BR Mk1 Corridor Composite CK BR Blue & Grey, unnumbered

OO GAUGE

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30-425 Bachmann Midland Pullman Train Pack.....£525.00



31-426C Class 411 4-CEP 4 Car EMU No. 7122 BR SR Green.....£349.99



31-921A 31-921A LB&SCR H2 Atlantic Steam 32425 "Trevose Head" BR Lined Black.....£159.99
31-921ASF 31-921A LB&SCR H2 Atlantic Steam 32425 "Trevose Head" DCC Sound.....£229.95



35-126 Class 20/3 Diesel Locomotive No. 20 311 Harry Needle Railroad Company.....£124.99
38-125Y 35 Ton RTC Trib Train Van ZXR No. RDB 999900 BR Bauxite.....£18.99
39-265 BR Mk1 RMB Miniature Buffet Car No. M1865 NSF.....£28.99

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R3751 Class 87 Bo-Bo Electric Loco 87 002 "Royal Sovereign" Caledonian Sleeper.....£129.99



R3855 Princess Royal Class 4-6-2 Steam Locomotive No. 46211.....£139.99
R3855X Princess Royal Class 4-6-2 Steam Locomotive No. 46211 DCC Fitted.....£149.99
R3864 Star Class 4-6-0 Steam Locomotive No. 4003 "Lode Star" GWR Green.....£129.99



R3885 Class 60 Co-Co Diesel Locomotive No. 60 062 "Stainless Pioneer" DB Cargo.....£134.99
R3888 Class 56 Co-Co Diesel Locomotive No. 659 002 (ex-56 115) Floyd Zht.....£134.99
R3995 Clan Std GMT 4-6-2 Steam No. 72004 "Clan Macdonald" BR Green.....£159.99
R40006 ex-Mk1 SK Ballast Cleaner Train Staff Coach DB 975805 BR Departmental.....£25.99
R40007 ex-Mk1 SK Ballast Cleaner Train Staff Coach DB 975802 BR Departmental.....£25.99
R40008 ex-Mk1 SK Ballast Cleaner Train Staff Coach DB 975804 BR Departmental.....£25.99
R40060 GNR 4 Wheel Brake Baggage Coach No. 836 GNR Teak.....£21.99
R4351 Railroad BR Mk1 Corridor 3rd Coach No. M24439 BR Maroon.....£16.99
R4352 Railroad BR Mk1 Brake 3rd Coach No. M34655 BR Maroon.....£16.99
R4389 Railroad LMS Brake 3rd Coach No. 5200 LMS.....£17.99
R4520B 51ft Gresley Non-Vestibuled Suburban 3rd Coach E82190E BR Maroon.....£26.99
R4524 Railroad Brake Coach No. 5121 GWR Chocolate & Cream.....£17.99
R4534E BR Pull-Push Coach Pack Set No. 601.....£49.99



R4580 Eurostar Class 373/1 e300 Blue, Divisible Centre Saloons Coach Pack.....£39.99
R4626 Railroad MK1 BG Parcels Coach No. W80724 BR Chocolate & Cream.....£19.99
R4671 Railroad LMS 4 Wheel Coach.....£14.99
R4674 Railroad LNER 4 Wheel Coach.....£14.99
R4737 Maunsell 6 Compartment Third Class Brake Coach 3798 SR Malachite Green.....£27.99
R4793 SR 58ft Maunsell Rebuilt ex LSWR 48ft Six Compartment Brake 3rd No. 2628.....£29.99
R4797 Maunsell First Class Corridor Coach No. S72125 BR Crimson & Cream.....£24.49
R4799 LMS Suburban Non-Corridor Composite No. M16635 BR Crimson.....£29.99
R4808 BR Mk2D Coach Open Brake Second BSO No. E9481 BR Blue & Grey.....£23.99
R4811 BR Mk2E Coach Open Brake Second BSO No. 9502 Intercity.....£23.99
R4818 Railroad Composite Coach No 5055 Southern Malachite Green.....£12.99
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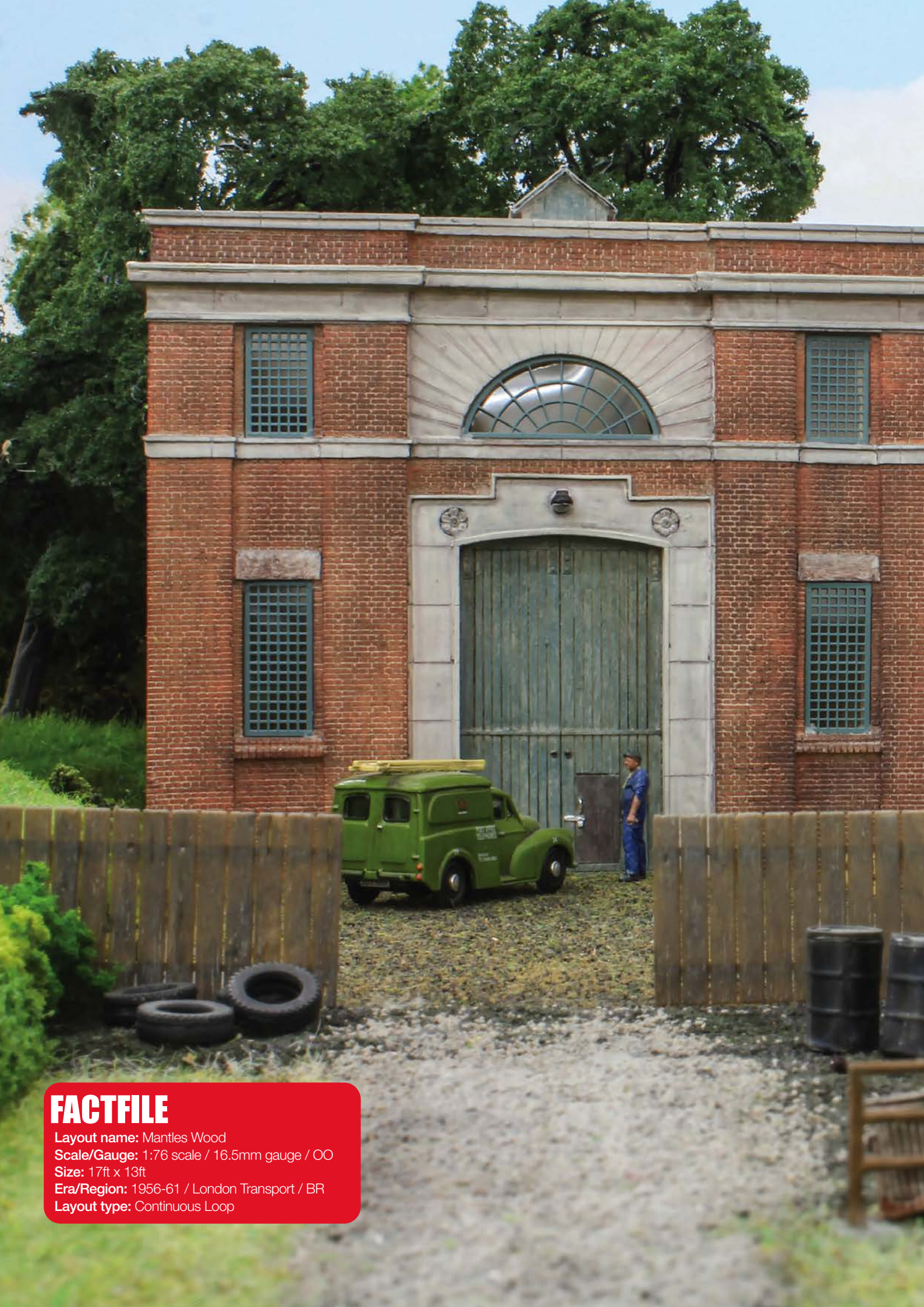
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FACTFILE

Layout name: Mantles Wood

Scale/Gauge: 1:76 scale / 16.5mm gauge / OO

Size: 17ft x 13ft

Era/Region: 1956-61 / London Transport / BR

Layout type: Continuous Loop

MANTLES WOOD

Paul Davis wasn't defeated by his layout vision, despite admitting to limited modelling skills. Thanks to proprietary models, his efforts have flourished.

Words & photography: Paul Davis



Ex-LNER L1 67778 waits in the siding, having been taken off a London-bound train from Aylesbury.



A Thompson L1 calls at Mantles Wood with a stopping train from London to Aylesbury formed of Gresley stock.



I should never have gone to Pendon.

We moved into the first of two houses in High Wycombe late 1975. An outing to Pendon was a 'must' one Sunday early in 1976. Even half a century ago, it was totally absorbing. The quality of workmanship was both inspirational and overwhelming – 'I could never do anything like that'.

Needless to say, further visits were made over the next 40 years and I came away with a few pointers, like how to use view blockers, the importance of keeping tones muted, plus that buildings should 'grow' out of the ground and not be plonked on.

I'd long wanted a layout of my own and felt I might have at least one decent layout in me, but how on earth could I achieve something like that? Of course, Pendon is a team effort; I'd just have to cut my cloth accordingly.

Our first house in Wycombe wasn't really suitable for much of a layout, but I did start collecting stock, as funds permitted. At the time, I was thinking of somewhere the Western met the Southern, though, over time, my ideas changed. I'd long been interested in Rickmansworth where Met electrics gave way to steam, plus, of course, BR trains also ran.

Layout origins

In 1989, we moved to our second house in Wycombe and one of its attractions was having a proper loft conversion, I could finally have a railway room!

The 1990s were problematic in terms of money. Sky-high mortgage rates in the early-1990s meant funds were short, but eventually, things got easier. In 2000, I was able to take early retirement and get to work on a layout. Now I had the time, but less money. Isn't it always the way?

My late wife was always very supportive of men having hobbies, so I never had any problems here. I didn't feel I could recreate Rickmansworth, so created a fictitious line called Aylesbury Junction, which some may remember from earlier years on RMweb. It was intended to be a line created jointly by the Metropolitan and LNWR to fend off competing new builds from others.

Tricky trackwork

I really wanted something in four-rail, but was not happy with the commercial offerings, and my skills aren't great. If I have any skill at all, it is in spending a lot of time trying to create 'the right look' without needing any great proficiency.

I wasn't happy using Code 60 rail for third and fourth rails. In real life, the running rails

on London Underground were 100lb/ft and the third/fourth rails 120lb/ft, so using Code 60 is nonsense. However, I am no scratch-builder. It also seemed to me that some visual trickery would be needed, given that OO gauge is 4ft 2in wide and not proper width, so achieving 'the look' was always my aim.

At a glance, the third and fourth rails can look the same width or maybe a tad narrower due to the conductor shoes scrubbing the tops of the rails. This led me to start using Code 100 rail for all four rails. However, in time, I changed to using Code 83 for the outer rail, but juggled with the position to make it look right (to my eyes anyway).

On my first layout, the rails were superglued to track pins. I didn't have too much trouble with them coming unstuck over winter, but nowadays, I solder them to track pins. My normal track is Peco Code 100 and, after using my method, I am happy it's something anyone with patience could do without any great skills. The main lesson would be to lay/solder just a few inches at a time, in some cases, having two or three work sites around the layout and working towards each other.

A house move

In 2014, we sold up and moved to west Wales. I was permitted to use the 17ft x 13ft garage for my next layout, which is turned into a proper room. I was advised not to skimp on insulation and that turned out to be very good advice – I've not regretted it. Even in winter, the room warms up pretty quickly.

I would have been confident enough to start with open frame baseboards, however, a local carpenter made the baseboards in less than four days. It would have taken me four years! He knew about MDF and made a good job, so I let him surface the layout with solid MDF, which was then topped with fibre board.

All the time, I was thinking about what the layout should be. I still had a hankering for Rickmansworth, but even with a 17ft length of garage this would still barely be a third of what I'd need. And so 'Mantles Wood' was born. To this day, 'Mantles Wood' is still the demarcation point between TFL and Network Rail, having been the demarcation between London Underground and British Railways for the period of my layout.

The premise is that the area was to have been a dormitory town for expansion of industry in the Aylesbury area between the wars and LU had electrified beyond

Rolling stock



I have a vast amount of rolling stock (far too much) stretching back to the 1970s, though most of those are retired. Occasional items are 'Rule 1' but for the most part, I run things that would have been seen in the Chilterns, albeit with some licence (eg Quad Arts, which I can't resist, and tube stock on mileage accumulation/driver training runs). I really need a Chesham set to go with my ex-GCR/LNER C13, but that means either 3D-printed set or a brass set, neither of which I've worked on, and both would be a big ask.

Over the years, I have had a lot of pleasure rehabilitating old Airfix, Mainline and some Lima models to give me practice, especially where their errors aren't too noticeable.



A short working from Mantles Wood to Neasden heads off led by Q23 driving car.



Metropolitan Bo-Bo No.3 *Sir Ralph Verney* arrives with a train for Aylesbury. The electric locomotive will give way to steam here. Note the pick-up beam on the leading carriage, which was to prevent 'gapping' by the electric locomotives. The elderly couple seem to be waiting a long time for their London-bound train.



The engineers are pondering how best to unload their supplies...



Trackside allotments were a part of the railway scene in the 1950s. This is one of three on the layout.

Rickmansworth to a new station at Mantles Wood. Traffic never materialised and the war came. After the war, Town and Country planning came in, so the station was caught in limbo.

In this way, I can have locomotive-hauled stock from London with Met Bo-Bo electrics giving way to steam haulage, plus having BR traffic, which for me, is ideal.

Creating the scene

Nearly all buildings on the layout are commercially available (some with modifications) – I'm not much of a scratch-builder. The wooden water tank by the locomotive stabling point is scratch-built by myself, very loosely based on Brill. Otherwise, it's just a case of minor modifications to make them different from ones straight from the box. One of the goods sheds has a lean-to by Ten Commandments

added, and the crew building by the stabling point has a brick base added. The Townstreet brick water tower has the windows bricked up to represent Metropolitan Railway's house style.

Back in the 1980s, I was impressed by a German visitor to Wales who'd built a layout called 'Abermymach' where he'd used modified kits from the likes of Kibri and Vollmer to represent a Welsh GWR scene. I realised that, in the late Victorian era, a lot of architecture in UK, USA and Germany was quite similar except for the roof areas, which were the main point of difference, and I have always borne this in mind.

The main station building is a Kibri German administration building. The platform canopies are Kibri Detmold station, and I left the roof off one section as bomb damage was still very evident in the mid-1950s. The goods sheds and crew relief shed

are all Walthers but look quite English as there's not much to show they are American.

I've tried not to cram too much in, on the basis that 'less is more', while keeping operational possibilities in the visible section.

During planning, I often placed a camera at track level to check perspectives and, as a result, some of the best views are at or about track level, though sadly, I can't do focus stacking to produce photographs to give some views full justice. I have left most things moveable on the main platform. The main station building and large tree in front are removable so I can take photographs from more angles.

Where possible, I use view blockers, muted tones and, of course, all buildings 'grow' out of their surroundings. Finally, I was able to implement what I'd learned so long ago. Maybe, I should have gone to Pendon – just not spent so long doing something about it!



A bucolic view of the goods sheds, which never saw much use. Buildings are a mix of Walthers and Ten Commandments.



A train of Q stock led by Q27 driving car arrives with a short working. 67778 is still waiting to cross over to the down side.

This is about as busy as it gets at 'Mantles Wood'. Despite having frequent services, the station never achieved its potential. I daresay the old ladies on the down platform are discussing Mrs Dale's Diary. There wasn't a lot happening in the 1950s.



The advert and timetables are from Sankey Scenics. The LT signs I found online but the supplier doesn't seem to be there anymore.

Most model railways are set in the summer, and mine is the same! The large tree is from Caynix, as are other trees on the layout. Its prices compare favourably for really good trees when you think of the costs of a single carriage these days, let alone a locomotive. The hedges are from a variety of sources with the intention of keeping variety and colours and belief in something that isn't real.

There are 12 road vehicles on the layout, a mix of Oxford and Base Toys, unweathered as I can't seem to get weathering right on vehicles. I have a three-wheeled van from a white metal kit which isn't in any of the pictures. It reminds me of the bread van that used to come round in the early-1950s.



Some station seats are from EFE, others from the Kibri Detmold kit, which was the gift that kept on giving.

The roads are generally hardboard, sprayed in 50 shades of grey and with a dusting of grey powder to give texture. The bus parking area has embossed plastic sheet representing cobblestones. Off scene, dirt roads were created with Woodland Scenics fine ballast, always mixed, to give a blend rather than straight from the packet.

There are nearly three dozen figures on the railway, following the principle of 'less is more'. I've tried to make the groupings believable, and no particular area looks crowded. The 'family scene' in front of the station uses Preiser figures. The bus crew are Monty's Figures. All others on the layout are either Modelu or the sadly-departed Falcon Figures.

The fence is from Ten Commandments. I wanted something to represent a quick and cheap way that the railway might have installed fencing when the bay fell out of use, with the intention of doing a proper job later, which they never did.

Final thoughts

I couldn't have made 'Mantles Wood' without the products from The Forth Rail on Shapeways. More recently, I have switched to Hunt magnetic couplings for most stock, which has made running sessions more pleasurable as age catches up with my eyes. Modelu crews and Railtec Transfers have also been among my recent go-to suppliers for renumbering and detailing locomotives.

The main advice I would give to others is to make a start. Whenever I try a new technique, I find it takes three goes. First attempt is poor, second is better and, at the

Paul's layout-building tips

1. Spend money on good model trees if your tree-making skills aren't successful
2. If you have an external railway room, use the best insulation you can afford
3. It's worth procrastinating a little to understand how to do things with limited skills
4. Don't be afraid to get things wrong
5. Go to Pendon Museum, if you can!

same time, I finally figure out what I should really do, then the third attempt is quite good. Take your time and try to do things well, even if you do them a few times over.

I've thought about whether I might want to model somewhere else, but with

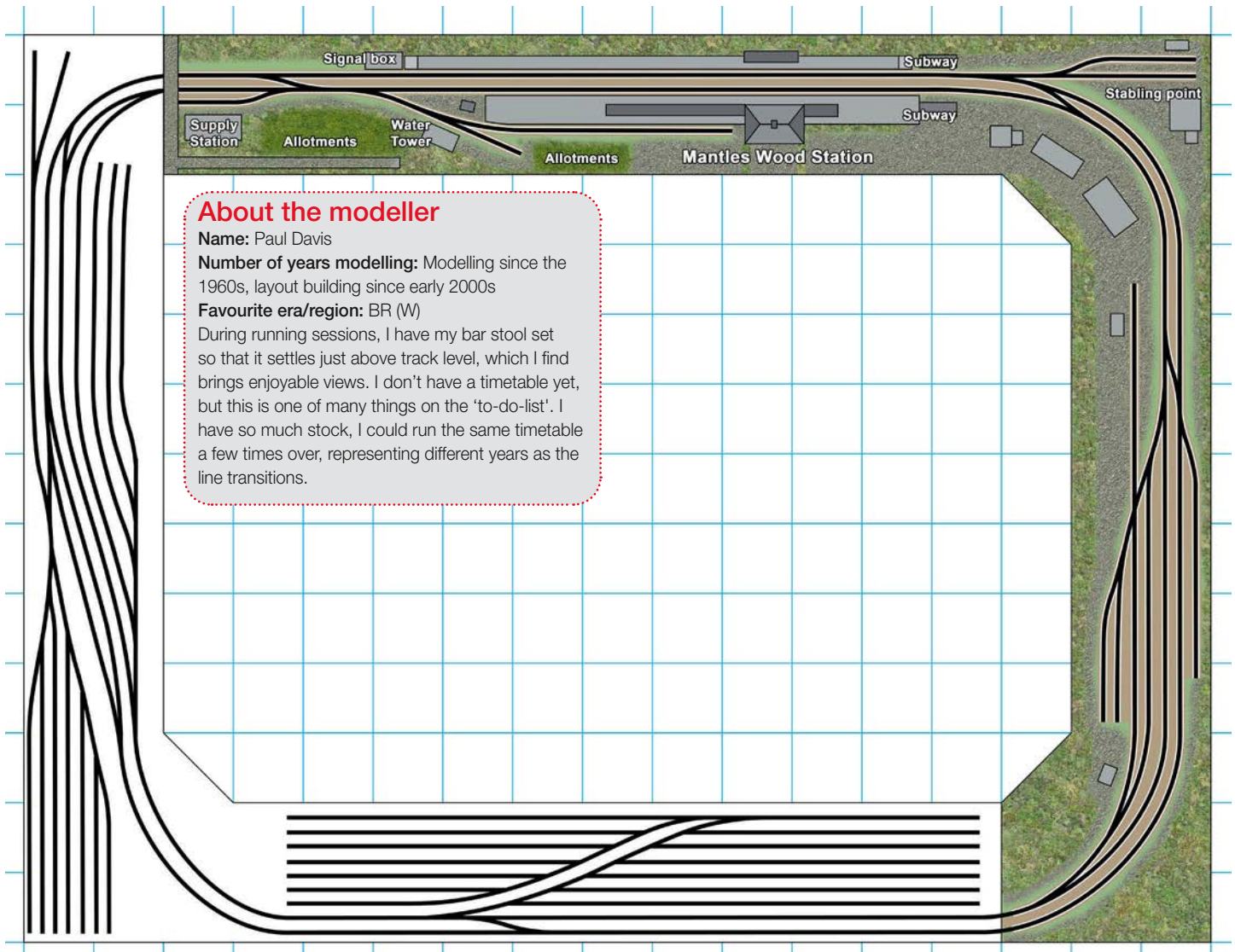
over 100 locomotives and units, I can't think of anything else that would utilise all my electric and steam stock. Somewhere closer to London might be possible, but making use of all the goods stock as well as passenger stock is a problem.

The train of Q stock makes ready to depart from the bay. The modest goods sidings here have been lifted, having never fulfilled their potential.





Ex-GWR pannier L99 brings locomotive coal in a single wagon with brake van in tow. This Mainline pannier provided a good base for developing my skills at capturing the mid-life patina of these locomotives.



About the modeller

Name: Paul Davis

Number of years modelling: Modelling since the 1960s, layout building since early 2000s

Favourite era/region: BR (W)

During running sessions, I have my bar stool set so that it settles just above track level, which I find brings enjoyable views. I don't have a timetable yet, but this is one of many things on the 'to-do-list'. I have so much stock, I could run the same timetable a few times over, representing different years as the line transitions.



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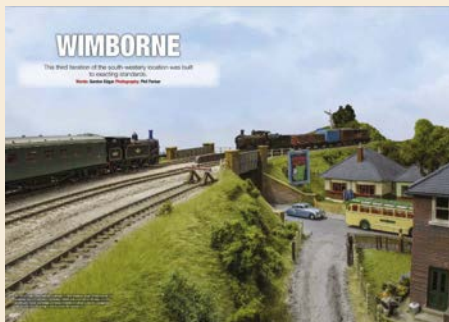
WIMBORNE

Can you pass on my compliments to the Wimborne Railway Society, and, in particular, to the article's writer, Gordon Edgar, for an absolutely enthralling article, expertly written in simple and direct style without fussy technical detail.

As a 69-year-old recent returner to railway modelling, I was absorbed by not just the detailed background information but also the descriptions of operations at exhibitions. While I have no intention to exhibit, the accounts of stock and train movements have now given me ideas for my own modest layout. A big shout out also to Phil Parker for capturing the best of the layout in his stunning pictures.

I also thought that the later sections of the article – 'Highlights and Challenges', and the 'About Us' feature – spoke so clearly and eloquently of the group's dedication, interest and enjoyment of their hobby, that I felt like contacting them straight away to join up. Unfortunately, I live in Luton, Bedfordshire! Bravo to the whole team at WRS, and thank you BRM for publishing them.

Richard Furmanski



RENDEZ-VOUS AT PENDON

Over the weekend of September 10-11, 2022, Pendon Museum will celebrate the reopening of the Midland and South Western Junction Railway. This cross-country route between Cheltenham and Andover traversed GWR territory and eventually was taken over by the latter company. Never very profitable, it closed in September, 1961.

Now, faithfully reproduced in 4mm:1ft scale, the line has been brought back to life and will reopen to traffic 61 years after closure. Naturally, we had hoped to celebrate this last year, but a certain pandemic intervened. Come and see services restored and trains once again cross Great Western territory in our version of the Vale of White Horse. There will be a range of services on view from a 1920s MSWJR-liveried train to a 1936 diesel railcar.

In addition, a full service will be running on the Vale railways set amongst the beauty of the Vale scene portraying a way of life unrecognisable today.



LAYOUT BASICS WITH PHIL

I thought Phil's Layout Basics webinar was good and informative, and demonstrated the need for patience and careful speed of working – everything in its time. So, what next?

How about:

1. Wiring turnouts?

There are so many ways and considerations. If you are running DCC should you use a separate power supply? Buy another 'box' to control ancillary issues such as turnouts and layout lighting?

2. Changing the parameters of locomotive running characteristics to give typical and smooth running

3. Locomotive maintenance

Some of these issues will not be covered at one visit; but could start with relatively simple issues and progress from there to, say, route control.

The principle of Zoom meetings is really good, although I know from personal experience it can be difficult and time-consuming to prepare and deliver, but well worthwhile.

Dave Haigh



BRM replies: Thank you for the valuable feedback, Dave! We're always looking for new ideas for World of Railways and BRM articles, plus webinars and virtual events, too. Watch this space!

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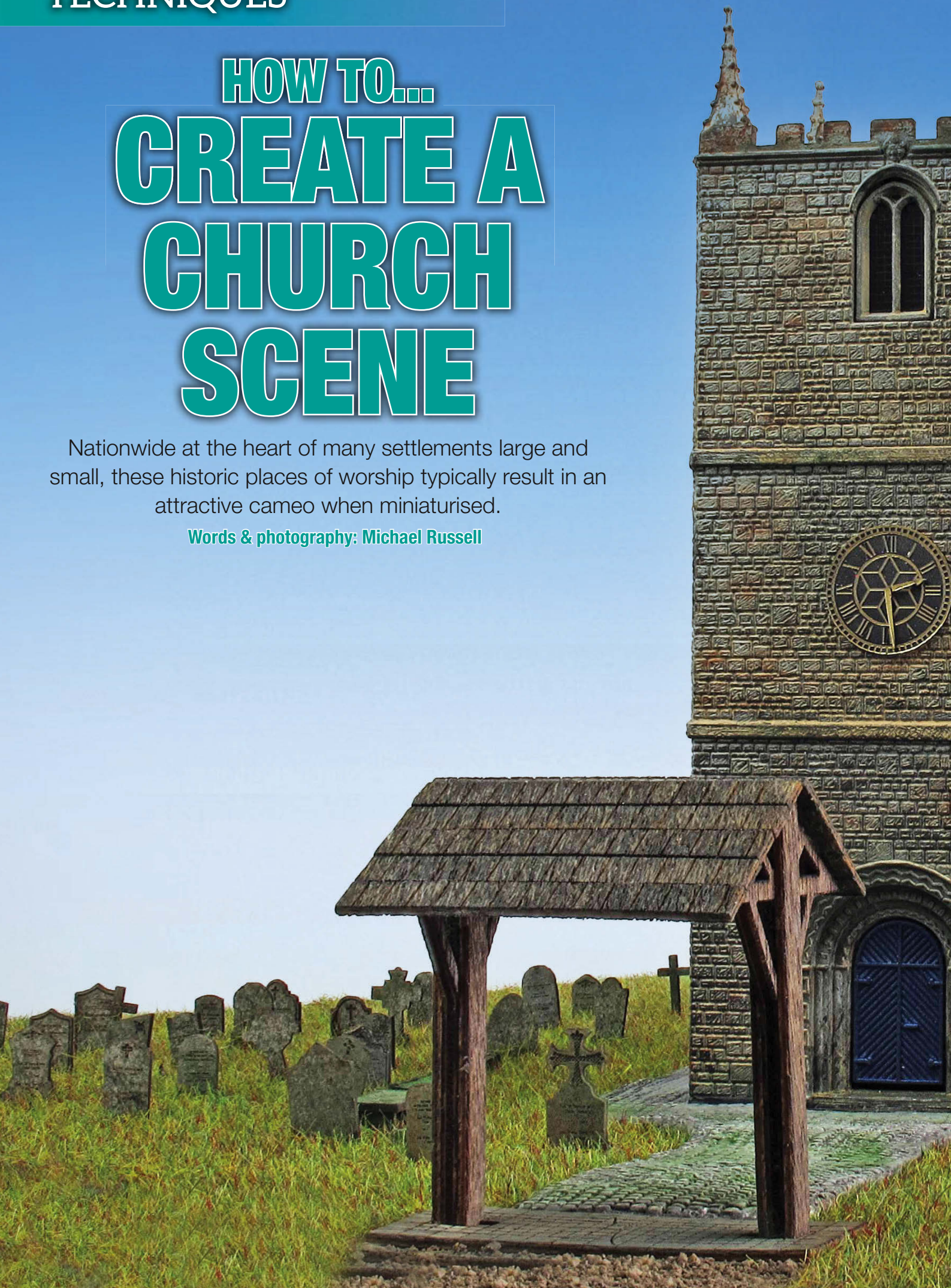
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HOW TO... CREATE A CHURCH SCENE

Nationwide at the heart of many settlements large and small, these historic places of worship typically result in an attractive cameo when miniaturised.

Words & photography: Michael Russell





Despite a drop in attendees, churches are still ubiquitous in Britain. In some locations, churches are the dominant building and are large compared to the size of the villages they serve, such as around the Fens and King's Lynn. There are many denominations, and it helps to know a little about them to model them correctly. Churches also vary depending upon era and their architecture varies accordingly.

Graveyards are interesting subjects, and there are spectacular ones to visit if you are looking for inspiration. Cemeteries from the Victorian era contain a great deal of

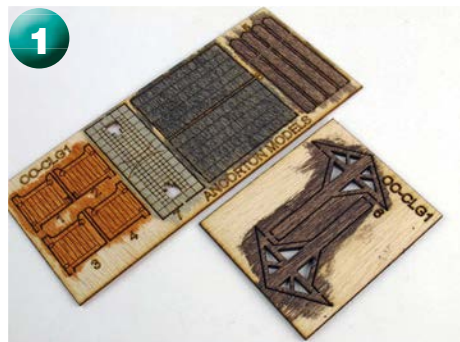
symbolism, which is fascinating to research and model. You can depict the affluence of an area on your layout that you're modelling by the size and quality of monuments in a graveyard.

Don't forget that many churches have been sold off for redevelopment and have been repurposed for housing or commercial use. A church kit such as this would make a good starting point for such a project.

We'll also cover a solution to the problem of light showing under buildings, something that looks unrealistic if you're fitting interior lighting for visual impact.

TECHNIQUES

Building the lychgate



1 Colour the parts while still in situ on the runners. The material is strong and won't warp. I used shades of acrylic diluted with plenty of acrylic thinners. Ensure the mix is liquid enough not to fill the grain. You could also use ink or dye.



2 When building the lychgate, the posts are a tight fit in the base. Use pliers with a plain jaw to squeeze the ends slightly to make them fit. Use the same technique with the gate posts.

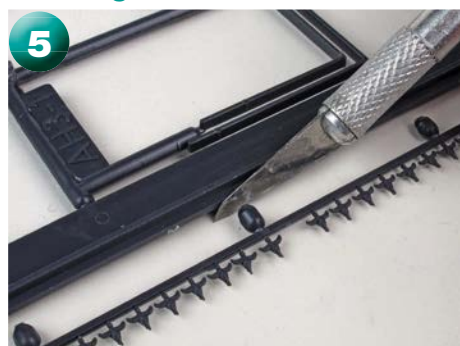


3 Cut the tiles into lengths along the rows using a sharp knife. Touch up the cuts using the paint/ink/dye you used previously. To avoid splitting the wood, make several light passes using a rule as a guide.



4 Stick the tile lengths into place using PVA glue with no gap between them. The slight unevenness caused by fitting the roof in this way produces a more realistic finish.

Building the church



5 Cut fragile parts from the runners with a knife and store these securely away. It's easy to damage the delicate roof trim parts, you'll see that part of mine is missing.



6 Cut all the wall sections from the runners with a knife and don't feel the need to cut too close to the part as you can tidy that up afterwards. Don't bend parts off the runners.



7 Use a rough file on the runner attachment points and then, when you approach the component, swap to a fine file and use an action that runs along the length of the part. Also, check for flash around the edges and windows apertures and remove with a file.

Think ahead

When making plastic model kits, it pays to think ahead. For example, sometimes it's easier to paint individual items, or sub-assemblies, before final assembly. We need to consider how we're to subsequently assemble these painted items.

The glue used in the construction of plastic kits works by melting the surfaces it is applied to and fusing them together. This can only happen if the glue has direct contact with the plastic. The danger is that paint can disrupt this and act as a mask that prevents correct bonding.

There are five ways to get around this issue. The first, and most obvious way, is not to paint surfaces that we wish to glue in the first place. The second involves masking the areas that we don't want to paint. When spray-painting, masking is usually inevitable and, even when applied using a brush, masking has advantages. For example, it allows us to achieve sharp edges and to avoid having to think too hard about where to apply paint while actually painting.

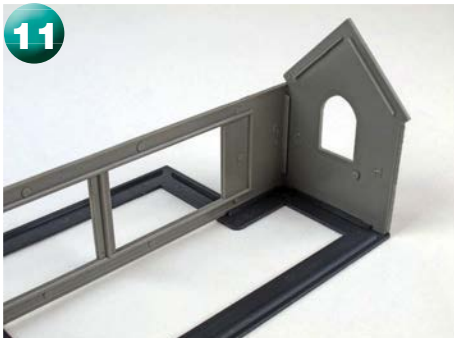
The third way is to wipe paint off while it is still wet. This works best when you use paint thinners to remove wet paint, but it is easy for this to contaminate paint that we don't want to disturb. The fourth way is to scrape the paint off mechanically once it has dried. This option works well on flat, even surfaces.

The fifth way is to leave the paint on all surfaces and to use a different glue. PVA works surprisingly well, although it takes longer to dry on non-absorbent surfaces, such as plastic. It is ideal when sticking clear plastic components together, too.

I tend to use a combination of all five techniques depending upon the circumstances. I establish the methods that I will employ by reading instructions before assembly commences and by looking at the relevant parts at the same time. On complex kits, this can be quite involved, but the process gets easier with experience. Many model railway buildings are fairly simple to construct and are a great way to learn the art of building plastic model kits.



There are a number of pin marks on the mouldings made when releasing the parts from the injection machine. File these flush with the surface, especially where other parts will be bonded.



I commenced the build by sticking edges marked G together first. The base is a great aid to achieving a square build. Glue the corner together first and then, with the walls held in the correct location, run a small amount of Poly Cement along each side so that it is drawn in by capillary action.



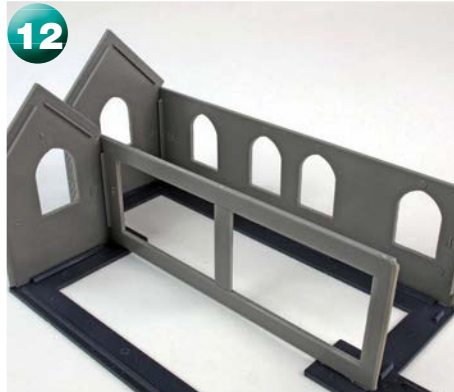
Trial fit the roof pieces at this stage and ensure that they fit snugly in place. The roof should go between the gable ends down to the mounting points. I had to remove some material from one end with a file to get a good fit.



Before painting, it is a good idea to remove debris from the model in the way of plastic filings, dust and hairs. Use a soft paintbrush for this. Mask areas you want to keep paint-free using tape or Humbrol Maskol.



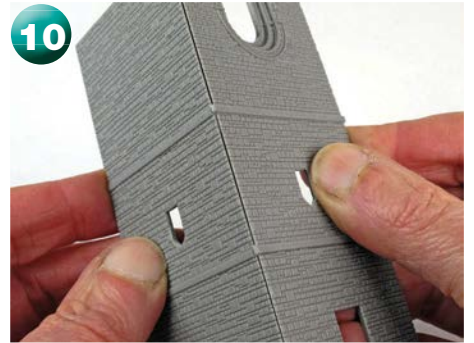
Line parts up around the base and familiarise yourself with them. The parts are marked with letters on the inside, you should ensure that you stick letters to the corresponding letters to get the order correct.



The first two walls are an anchor for the rest, so let them set for at least an hour or two. Now glue corner 'F' and corner 'H' and leave to set again with the base resting on a flat surface.



The roof trims can now be fitted. They are delicate, so be careful when cleaning them up. I found that I got a better fit by filing the top of the ridge tiles flat.



Do a dry run of each joint before sticking them together. Hold parts next to each other and check that they fit together snugly. The mating surfaces should lie flat to each other with no obstacle in the way.



I stuck the three sides of the tower together using the roof/floor as a guide to squareness. Stand the tower on a flat surface to get the tops level. Gluing long runs together, such as the sides of the tower, is more of a challenge.



I decided not to fit the church tower to the base at this stage because this reduces the size of the opening through where the windows will be fitted later. It is important to think ahead during construction.

Michael's top tips

Long glue runs: Some long lengths of plastic to glue in this kit are more of a challenge. In such circumstances, you need a quick, even and controlled application of glue. A precision applicator is a great help.

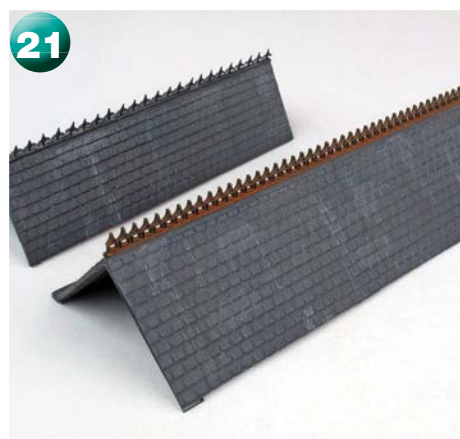
Paint handles: It is important to have a good handle on a part or sub-assembly when painting, especially when spraying. To make the roofs accessible, I glued small offcuts of plastic card inside them. This is easy, in this case, as the roof pitch is 45 degrees. Afterwards, you could leave the handle on for strength, or cut it off.

Shading: Shading is carried out to make up for the lack of light indoors and to add depth to the model. There are two types. Pre-shading is the method of applying an undercoat that will alter the appearance of the topcoat. This is why aerosol cans containing a topcoat often advise which primer should be used with them. Post-shading is carried out after the topcoat has been added and is usually a shadow colour or a darker shade of the topcoat.

Painting the church



I pre-shaded the model by applying a grey primer. I then sprayed white at an acute angle from above, followed by black at an acute angle from below. You should end up with a stark 3D effect. This will get covered by the topcoat, but the undercoat will affect it in a subtle way.



Give the roofs an overall coat of 50:50 dark grey and thinner. When dry, add random patches of lighter and darker shades of the base coat. Dry-brush streaks of light grey and dark grey down the roof. Add a highly-diluted wash of brick red to the roof ornamentation.



I made the windows in Microsoft Publisher using the insert shape, line feature. Use a line thickness of ½ point and six pixels between the horizontal and vertical lines to form a grid. Use the group and align tools to aid you and print out onto overhead projection transparency film.



Give the model a wash of a sandstone colour or other to match the prototype in the area you are modelling. Ensure that you thin the paint about 50:50 with acrylic thinners so that the base layer isn't entirely blocked out.



Remove masking material and glue the windows and doors in place. I used Humbrol Poly Cement since I had retained plastic-to-plastic contact. Windows must be flush to the wall and central in their holes.



Colour in the squares of the glazing in a random fashion using coloured pens. Again, accuracy isn't vital and some clear panes may be left. Cut the glazing to fit the windows and install using Glue 'n' Glaze from Deluxe Materials. One side of the film I used was matt, and so I fitted this to be visible from the exterior as it looks more realistic.



Dry-brush the model with earthy colours from dark to light greys and dark to light browns. You need to make the effect look random, which is not our normal way of working as humans tend to work in patterns. Don't worry about covering whole stones exactly as this isn't necessary.

Final assembly



Fit the roofs in place so that the larger piece is installed first. This engages into a slot in the wall at the end furthest from the tower. Ensure that the roofs are pushed down onto the support ledges.



You will need three clock faces for the tower. When assembling, take care to set the hands to the same on each face. Be careful to get 12.00 at the top and the clocks at the same level on each side of the tower. Although the faces are self-adhesive, it is safer in the long run to use cyanoacrylate glue to hold them in place.



Use weathering powders to take away the freshness of the finish. Green is the most useful, and you should apply it in corners and anywhere that is in proximity to greenery, such as a tree. Mix a grey from smoke and white and apply around the base.

Putting it all together



Place your church into its final position on the layout/diorama and draw around it using a pencil or other marker. I drew a line outside the perimeter to represent a path and also a path leading away to the lychgate.

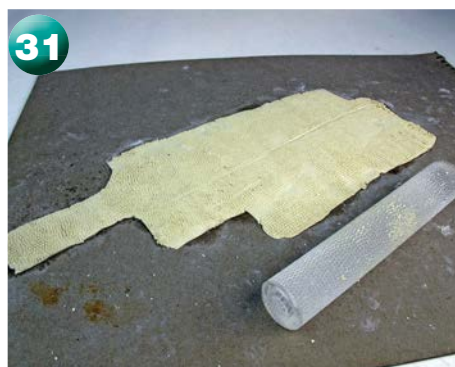


Before the clay starts to dry, position the church and use a sharp knife to cut around the outside walls. Remove the clay from the centre, leaving a border around the shape of the church.

Making the graves



Use weathering powders on the gravestones. Try to get the effect as random as possible and don't be afraid to use your fingers to spread the powders around and rub it into the material. Wear a mask as you will produce dust.



Roll out air-drying clay over the marked shape and cut away excess that falls outside it. Try and get an even covering. Use the textured roller to form the stone setts. You can use water, petroleum jelly, or talcum powder to prevent the mixture sticking to the roller, although I found that I didn't need to use anything.



Once the clay has dried, test fit the church. The clay will have shrunk and cracked slightly so fill potential gaps with fresh clay. You can use PVA to strengthen weak areas, or glue pieces back into place.



To add variety, cut up the graveyard runner surround and make flat graves and ground markings. Glue small strips to the end of flat pieces to make graves that resemble tables. If you need more material, try using the Coping Stones kit.



Staedtler Lumocolor fine tip permanent markers (black, yellow, red, green, blue, orange, purple), overhead projection transparency film, DAS air drying clay www.hattons.co.uk

Gaugemaster (GM483) Fordhampton Church Kit, (GM169) Static Grass Glue www.gaugemasterretail.com

Scale Model Scenery (LX394-OO) Laser-Cut Gravestones, (LX392-OO) 10mm Coping Stones, (LX239-OO) Parish/Village Noticeboards, (LC059-OO) Self-Adhesive Clocks www.scalemodelscenery.co.uk

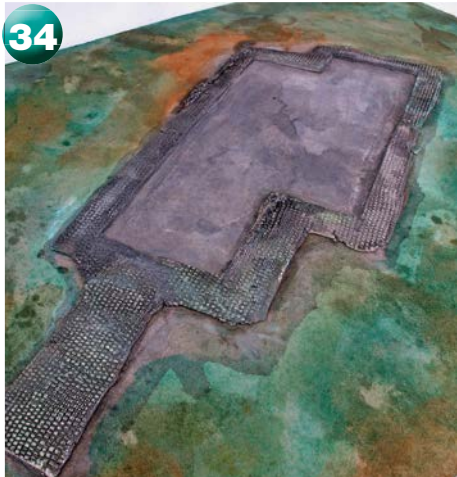
(OOCLG1/95827) Ancorton Models Church Lychgate kit, (43509) Das Modelling Clay, (47040) 250ml Expo Fast Set PVA, (47021) Standard Grade Super Glue www.expotools.com

Humbrol (AV0015) chrome oxide green, (AV0011) black, (AV0012) white, (AE2720) 20ml Precision poly Disp, Aerosols: (AD6001) grey primer, (AD6033) matt black, (AD6034) matt white, (AC7433) acrylic thinners, (DB0033) 33 black acrylic paint, (DB0034) 34 white acrylic paint, (DB0056) 40 pale grey, (AB0186EP) 186 brown, (DB0067) 67 Matt Tank Grey, (DB0063) matt sand, (AB0030EP) Dark Green, (AC5217) Maskol, (AG5110) Masking Tape, Hornby (R7178) Scatter Grass Grass Meadow www.hornby.com

Deluxe Materials Glue 'n' Glaze www.deluxematerials.com

Green Stuff World (2410) Sett Pavement roller www.greenstuffworld.com

TECHNIQUES



34 Colour the sett stones and all the base with dilute washes of acrylic paint. I used very dilute black on the sett stones and in the centre of the church. I used various shades of brown and green on the rest of the ground.



35 Glue the gravestones in place using PVA glue. Align these roughly into rows, but don't be too neat. Stick some of the headstones at wild angles, both side to side and fore and aft. Cutting the bases of some of the gravestones at an angle will help.



36 Brush on an even mix of PVA glue watered down 50:50 with water, then apply static grass. Cover an area of about 15cm.sq.in each pass so that you have time to apply the grass before the glue dries.

Conclusion

This is a straightforward kit to build that goes together nicely with no major difficulties. The only thing that would be challenging for a beginner would be the long glue runs involved when sticking the sides together. I'm particularly pleased with the windows as the stained-glass effect is subtle as it is in the prototype. Interior lighting would really bring the building to life.

The yew tree is synonymous with churches and graveyards, as churches are often built on the sites of older places of worship. Unfortunately, I have had to leave this out as there doesn't seem to be any off-the-shelf models available even though it is a common native species.





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SOUTHGATE PARK

Purchasing a second-hand layout, minus wiring diagram, was the first of many challenges for the duo of Shane Wilton and Mark Miller.

Words: Shane Wilton **Photography:** Andy York



33102 pulls into the station from London Waterloo with a down express while a 4 VEP looks on.

‘Southgate Park’ is set in the period 1986-1991, which was a time of transition with the advent of Network Southeast, when Mk. 1 EMUs and locomotive-hauled trains dominated. Bold new liveries mixed with down-at-heel blue and grey stock capture this period. Red paint was in vogue and, if it didn’t move, it was painted red! Benches, bins and lamp posts all got the treatment, as well as station canopies. Freight trains were still by the wagon load and the first and second-generation diesel electrics we no longer see in vast numbers were prevalent.

The layout is a two track main line served by a 12 road fiddle yard with six through roads for each running line. The inner circuit has a number of short stabling roads to provide additional storage. The scenic section covers 16ft with two running lines passing down the middle. A bay platform provides an additional platform face, giving three in total. The station avoiding line passes to the front of the scenic sections and provides access to three EMU stabling sidings. A double track section of London Underground runs along the rear, entering and exiting the scene via tunnels.

This layout is the fourth to bear the name ‘Southgate Park’; the first was at my family home, in the garage, and based on North London. My dad came up with the name as it sounded typically London-esque without being area-specific. The second was a move into the garden following the conversion of the garage into a bedroom. This was a very large and extensive railway that lasted from 1986 until 2000, when it was closed and dismantled when my parents moved house. Version three came 11 years later and was never completed and eventually dismantled when my wife and I moved house. The current version of ‘Southgate Park’ came into being when Mark Miller and I purchased ‘Northolt International’ from Tom Harwin in March 2017, when it came up for sale. We were very impressed by it and still are every time we set it up, which is testament to the work of the original owner and creator.

Switching control

Construction of the layout is very traditional, being 12mm plywood framing with 12mm plywood tops. Softwood timber is used for the supporting legs with adjustable feet, which are permanently attached to the layout and fold up for transport. Each board has metal dowels to ensure positive alignment, with the



The classic BR(S) look with a 4 CIG and a pair of 4 VEPs stabled waiting for rush hour to commence.



individual sections secured together with 10mm diameter bolts and wing nuts. This was the method used by the previous owner and we have found no reason so far to change this. Electrical continuity between boards is maintained with substantial jumpers connected by plug and socket terminal block connectors – again, a method used by the previous owner and has proved very resilient.

As we didn't build the layout, the biggest challenge we faced was finding out how to put it together and how it all works! 'Southgate Park' is the first exhibition layout I have been wholly involved in and it has certainly been an experience so far. The other major challenge is getting it to a reliable electrical state, Mark undertook most of the work to convert it from DC to DCC, which is not as straightforward as it seemed due to many isolating sections, much wiring with no diagram, plus age-worn fishplates that lost their integrity. We are slowly working through the issues and upgrading as we go. We are currently replacing the mechanical microswitches on the turnouts with autofrogs, which prove to be much more reliable and resilient.

Having such a large layout is challenging at first. Maintaining it and getting it to work was a job. We are still experiencing niggling electrical issues but are gradually finding each cause of these and working out ways to best eliminate them. It's never the same problem twice!

Currently, the layout is operated using the Roco Z21 Start, which normally controls my stay-at-home layout. I was a latecomer to DCC control, and this is the only system I have used on a regular basis. I particularly like the smartphone app, which gives the operators freedom to roam. While the Z21 Start does what we require, consideration is being given to having its own dedicated Z21 system, possibly one of the higher-spec versions that have a higher power output as we have concerns with the Z21 Start not having enough power for continued exhibition use.

The layout doesn't run a timetable, as this method of operation doesn't appeal to us. We aim to fill the yard up with trains in a set sequence and start with train one in road one, followed by the train behind it, train two. Each following train is drawn up to the head of the road on which it sits, allowing the train ahead – train one – to complete its circuit, drawing up behind the train that is now waiting – train two. This sequence

Get the Urban look



I'm particularly pleased with the overall look of the layout – it captures the feel of London's suburban railway network and is identifiable as such without any trains on show. Mark is a particular fan of the work we have done to change the station board, with the addition of the bridge with low relief buildings. It has worked out really well and anyone would think it's always been there.

Being an urban location, scenery in the sense of the word is stark and functional – no cricket pitches and rolling fields here! Imagine the railway is set below street level and set in brick-lined cuttings, there are no trees on the layout and greenery is limited to weed and scrub growth. Discarded newspapers and drinks cans litter the trackbed in the platforms. I guess this is a disappointing aspect of everyday life we would like to ignore but it's there for all to see, so needs to be modelled. We have, however, managed to add a few planters to the street scenes to brighten everyone's day!

One of the favourite aspects of creating this is the research and the treats that are thrown up; photo hosting sites such as Flickr being a particularly handy resource with its useful search function. A number of social media groups also have knowledgeable members within them.



Celebrity green 50007 *Sir Edward Elgar* heads south-west with a Waterloo-Salisbury Network Express as 50046 waits departure with a working from Exeter.



is repeated for each road until all six roads for each up and down running line have been cycled through, although, in practice, it doesn't always go to plan! The most important thing is to keep trains running at the front of the layout as this is what people come to see!

'Southgate Park' is new to the exhibition circuit, so we are hoping for a few more years of operation and will continue as long as the invites keep coming!

Rolling stock

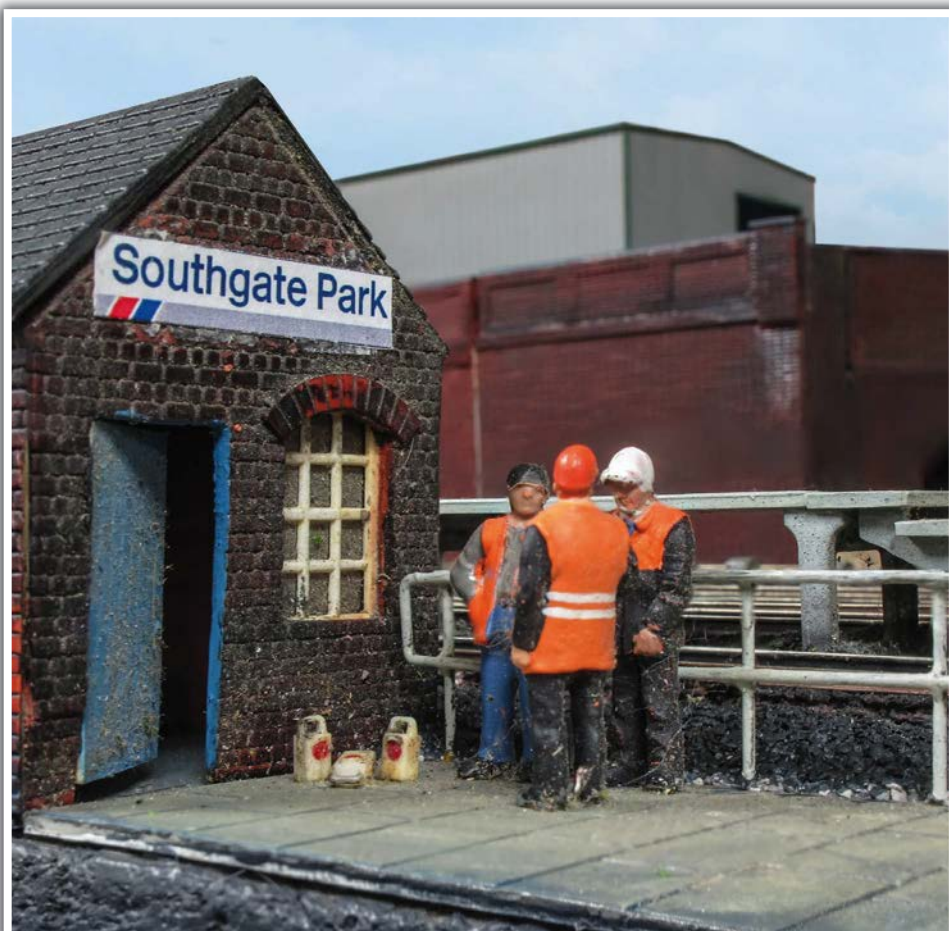
Being a Southern Region-based layout, the main bulk of the stock consists of the classic BR/SR Mk. 1-style EMUs from the late-1950s and 1960s, with some 1980s Mk. 3 stock. You will recognise Bachmann suburban two-car electric units of EPB and HAP designations along with a number of DEMU variants. Outer suburban and express trains are made up of four-car VEP, BEP, CIG and BIG EMUs from Bachmann, Hornby and the workbench.

Alongside the classic EMUs, there are a number of locomotive-hauled formations based on West of England express services out of Waterloo, in the main, hauled by a fleet of Class 50s in a number of liveries. First-generation DMUs, mail trains, air-braked Speedlink and MOD trains, stone, oil and china clay trains make up the revenue freight services, and finally, a number of engineers' trains and departmental EMUs make up the stock list. We have aimed to portray typical train formations using a mix of ready-to-run stock and kit-built items, depending on availability. All stock has been weathered or modified in some way to create individuality among the ever-growing fleet.

A number of buildings were built by the previous owner and formed key features of the layout. The most noticeable is the Royal Mail building, which towers over the London end of the layout.

Next steps

The layout is approaching 20 years old and this is starting to show in places, but nothing a little TLC can't resolve. A few areas of track need relaying as they have begun to separate from the cork underlay, causing undulations, which can affect running qualities. Most of the changes to the layout were completed ahead of the public debut in October 2021, but there are still small improvements and refinements to be made, such as replacing some of the cast metal street lights with non-working brass versions, which are more robust and finer in appearance.



Maintenance staff discuss the day's plans, or perhaps something more important, outside the store building at the stabling sidings. Note the small collection of tail lamps.



50024 *Vanguard*, in its unique revised NSE livery, draws into Southgate Park, oblivious to the trackworker waiting to cross the line.

Southgate Park Station



As part of our alterations, once we took ownership, the station area underwent a radical makeover. In its 'Northolt International' incarnation, the station never had a main building, this being imagined to be off scene. After some deliberation, we opted to use the Bachmann Scenecraft station building in 1930s Art Deco-style, as this suited the Southern Region style adopted during pre-war modernisation. This also gave us the opportunity to give the station a proper entrance and exit for the passengers to actually leave the platforms! A one way road passes the front of the station entrance with a bus station to the rear of the layout. The main station building is surrounded by Bachmann Scenecraft buildings, which give a convincing London area feel to the layout. The platforms are adorned with full canopies, giving waiting commuters protection from the elements. The origin of the canopies is nothing more than the classic Airfix kits, clad with Wills roofing sheets and L-cut Creative valancing.



A blue and grey 4TC waits in platform 1b as an early livery 4 CIG pulls into platform 3.

We are also looking at installing working multiple aspect colour light signals. Extra details on the buildings such as TV ariels and CCTV cameras are planned as well as signage, general clutter and signs of life.

The next major project is to have the London Underground section at the rear of the layout operational with a set of 1962 stock plying its trade. A lot of work has already been carried out, such as the removal of the dive-under and replacement with two running lines. We still need to complete the off-scene trackwork and build working 1962 stock.

The mention of stock leads nicely into rolling stock projects. Coming soon, and maybe complete by the time this issue reaches the shops, is the iconic Wessex Electric Class 442. Affectionally known as 'Plastic Pigs', this model is an extensive rebuild and upgrade of a model first completed in 1998 and is much overdue for an overhaul. Other stock plans include increasing the EMU fleet with a Mk. 3-based Class 455, another BEP, CIG and a BIG, which will allow more eight-car trains to be formed. More SR-based engineering trains are a must, as there were some very distinctive wagons around at this time, unique to the Southern Region such as 'Crab' and 'Lamprey' two-axle opens. However, the problem we are facing is what do we leave off the layout as the yards are filling up rapidly!



FACTFILE

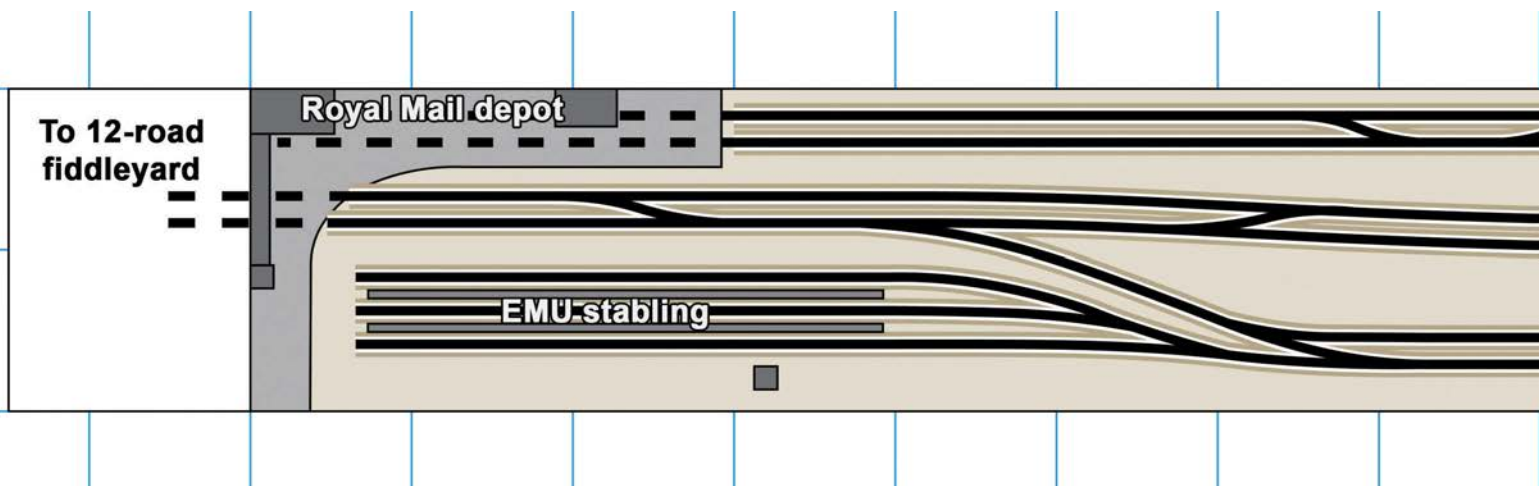
Layout name: Southgate Park

Scale/Gauge: 1:76 scale / 16.5mm gauge / OO

Size: 24ft x 8ft

Era/Region: 1986 - 1991 / Network SouthEast Southern Region South West division

Layout type: Continuous run





Meet the Modellers

Name: Shane Wilton

Number of years modelling: As long as I can remember, getting on for 50 years!

Name of first layout: 'Springford', built by my dad in the late-1970s, measuring 8ft x 4ft and provided several years of fun

Favourite era/region: BR 1980s to mid-1990s

Favourite locomotive: The noisy ones!

Since the mid-1980s, I have modelled layouts based on the south west area of London, ever since days out with my dad to exotic places like Clapham Junction, Queenstown Road and Battersea. I would only ever see SR EMUs on days out like this and the advent of Network SouthEast in 1986 was a revelation, with the bright liveries and an exciting time of rejuvenation. I'm not alone in stating that a lot of railway modellers recreate those times that gave us happy memories.

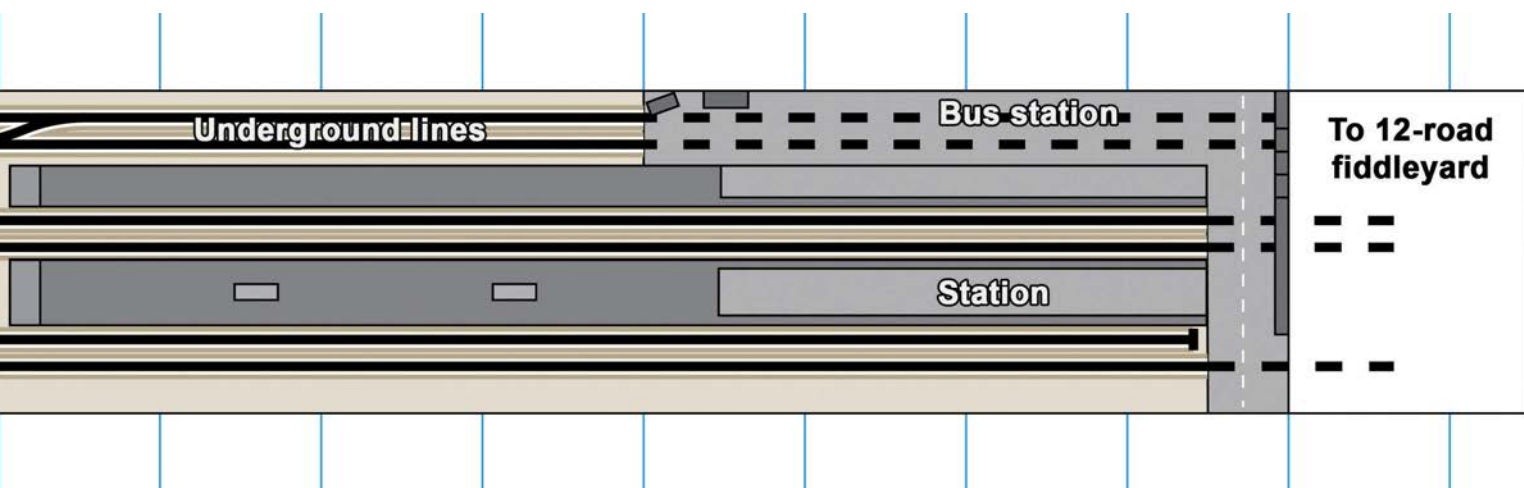
Name: Mark Miller

Number of years modelling: Since I was six, but only really serious in my late teens

Name of your first layout: Honestly, there's been that many I couldn't tell you

Favourite era/region: BR 1980s to mid-1990s

Favourite locomotive: Class 50 particularly 50049, which was the first locomotive I drove. I spent many a year building layouts, changing era, and now, with 'Southgate Park', I'm finally at a place where I'm happy with the location and stock to model. That said, I feel a relative novice when it comes to EMUs, so I look more at the Southern Region diesels to model for the layout.





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Welcome to Network SouthEast Upham Market

Mark Thatcher drew inspiration from his recollections of East London commuter traffic in the 1970s and 1980s with this urban recreation.

Words: Mark Thatcher **Photography:** Andy York



Factfile

Layout name: Upham Market
Scale/gauge: 4mm:1ft / 1:76 scale / OO gauge
Size: 13ft x 18in
Era/region: 1980s East London
Location: Fictitious
Layout type: End-to-end

ON EXCLUSIVE



A two-car class 101 DMU pauses briefly to re-fuel. In the street scene above, the double-decker bus is correctly numbered as the 165 service and is bound for Havering Park!

I certainly do not want to wish my life away, but had I been born in 1951, rather than a decade later, I think I would have traded post-war austerity for having witnessed a few good main line steam years, and would have also avoided 'generation swipe' for a decade longer too. I would have definitely been running home from school to Gidea Park station in Essex, with my gricer's notebook in hand. But it was not to

be. My earliest memory of trains was being pushed across a footbridge in my pram in 1962 and seeing great plumes of smoke from a locomotive passing underneath at speed. Many years later, my grandfather told me it was a 'Britannia' class.

As I got older, whilst I had no particular interest in railway modelling per se, I did have a general interest in modelling.

At 16, I secured probably the best Saturday

job (with extra hours over Christmas) at Selfridges, working in the men's suit department, though I had to borrow a rather ill-fitting suit for the interview. I would catch the 07.30am service, which would get me to Liverpool Street at around 08.10, and then hop on the tube to Tottenham Court Road, leaving me enough time to walk up Oxford Street and window shop.

The train journey sticks in my mind – BR



A Bachmann class 108 DMU departs from platform 1A. Hard against the buffers on platform 2 is a Royal Mail DPU taking on mail. The platforms are just long enough for a four-car rake, but are better suited to three or two cars.

ON EXCLUSIVE

blue slam-door stock, with people alighting before the train had even stopped, the chaotic nature of the line, with maintenance work always being carried out, and the rows upon rows of terraced houses backing onto the cut. I also remember the smell changing for the worse after Bethnal Green and the grimy red brick arches and underpasses leading into Liverpool Street itself. When stepping off there, the cacophony of noise

and the clattering of hundreds of rushing feet, it was a far cry from Gidea Park. I felt I had been plunged into a whole new world.

I know I can't model smells, but I reckoned I could manage the rest, I just needed to be patient and wait for the right

time, and the right layout that I could use for this project. In fact, I think over the years I have tackled almost every scale, even creating a T gauge airport diorama. But I always had a hankering to model exactly what I remembered when I started



I had much fun recreating my spin on the busy Romford Street scene. However, luck fell into my lap in this respect as Kingsway Models offers an exact replica of Romford's Station Arcade in its extensive range of cards kits, which largely focus on London-centric buildings.



Station parade

As I mentioned earlier, I wanted to recreate a busy east London street scene that I remember so well. I removed all the scenic components from the roadway and then set about thinking how to replicate Romford! On my street scene, the pavement was re-laid, slab-by-slab from Metcalfe's paving kits. Also, I did not have to look too hard to find Kingsway Models, which makes a vast range of card kits based in and around London. Included in its range was an exact replica of Romford's 'Station Parade' row of shops. How perfect this was! Unlike pre-stamped card kits, you will spend a little more time cutting out the individual recesses for windows and doors. This is both fiddly and time-consuming.

All in all there are 132 window and door recesses on this parade, but the time and effort bore great results and this is also a little different to more commonly seen rows of shops on other layouts and very true to the prototype. I also added the Queen Victoria public house as the corner building to the parade.

commuting into Liverpool Street from Gidea Park station in the mid-1970s through to the late-1980s.

The donor layout

I try to keep to the things I am passable at and avoid all else! It is actually unfair to describe the layout I purchased as a donor layout. It was far more than that. But I knew, if I attempted to build something from scratch, my track-laying skills are weak at best, and ballasting would have driven me to distraction. I waited for the right layout to come up that had the shape and form I was looking for, yet could be adapted and cluttered to match my memories of the line. As soon as I saw Ethan Tillott's layout (made with a little help from his dad, Matt), I knew it was for me as it ticked all my boxes.

It was an end-to-end layout, which is what I wanted. As a naturally lazy person, I felt I would get bored with a continuous loop layout and at least with this massive shunting puzzle, I would have something to hold my attention. The track was already laid and DCC-controlled and much of the line was complete, with a Hornby Skaledale High Brooms station being the main feature at the terminus at one end, and a four-road engine shed that could become a train maintenance depot with three further roads entering a tunnel, which could act as a hidden fiddle yard at the other end.

But there was a drawback. Ethan and Matt had built this to fit into their available space and it was never designed to be moved. The 13ft layout was in one long section. Looking at the trackplan, it was clear that chopping

it into two smaller lengths would not be an option as there were points everywhere!

So there was nothing for it. My wife kindly agreed to drive us down to North London in a hired van to collect it. It only just fitted in with inches to spare! Getting it out the other end was an equal challenge as it only just fitted into the designated model-building room (AKA the dining room) and took four people to ease it into place. Inevitably, due to the move and the flex along the length of the base board, several dropper connections became disconnected, but a close friend spent four hours on his back underneath the layout, fault-finding and re-soldering various errant wires. No mean feat for a 78-year-old!

Backscenes

Once the layout was in an operable condition, it was time to assess how I wanted to make it my own. I knew I wanted to add a lot of lineside detail, probably too much for some, but this is how I remembered the line back in the 1970s and 1980s. The original backscene was from Peco and had a distinctly post-industrial revolution feel about it. It was also too low, as I knew I would be developing a new high street scene above the hidden fiddle yard. The houses here were just represented by flat cut-outs of the same background. Talking to Matt later, he admitted that he and Ethan had run out of steam in this respect and were more eager to play trains, but this was one area I definitely wanted to improve upon.

As the line would look busy, I decided to opt for a clean, plain background, just in a very light blue, to keep the focus on the foreground. Apparently, this was very much a post-war trend as many modellers opted for a similar approach back then. So a sheet of 8ft x 4ft MDF was sourced and cut to size, with the one joint annoyingly falling right in the middle of the road bridge that acts as a scenic break between the track and the row of shops.

Track work and signalling

The track is all Peco Code 100 and features 13 express points, which give a gentle flow to the movement of the stock across the various roads. All the points are controlled by the "hand of God", but as the layout is too delicate and long to exhibit, this suits me as I operate it from the front. There is also one diamond crossing, which uses a polarity throw switch to change between the up lines from the platforms to the fiddle yard and the



Below Station Arcade there is much general detritus next to the permanent way, just as I remember it from the 1980s. There never seemed to be a day go by when some work wasn't going on down the line.

down lines to the TMD.

There are two roads from the terminus station that either branch up to the three-road fiddle yard or down the two roads towards the TMD. Behind the station platforms there are a further four roads where stock can be sent to the fiddle yard and from there traversed back into the station platforms.

The layout came with an NCE Procab DCC controller and, while I already had a Gaugemaster Prodigy Wireless set in stock,

I was not going to look a gift horse in the mouth. I raised my concern that this 2A controller would run out of juice but Matt told me not to worry. He was right and now I have added a second controller slaved into the master unit, which offers greater running flexibility.

There are nine working illuminated signals on the line, some two-aspect, some three-aspect, all controlled by rotary switches mounted to the front panel of the layout. These and the yard lights were

DIGITAL EDITION

The Class 108 DMU is returning from Upham Market Traincare bound for Upham Market station and ready to be pressed into service.





Lighting and effects

As this layout is usually operated under subdued lighting, I knew scale lighting was going to be important to create the right look and feel. That was another reason for the lighter backscene.

To light the interiors of the Station Parade, I picked up some tiny LED rope lights with a battery box, for only 50p per set at an amateur radio show of all places. These are arranged in a rather higgledy-piggledy manner just to vary the amount of brightness in the building. The street lights and Belisha beacons came from the brilliant Kytes Lights, who also create some fantastic emergency vehicles and convert Oxford Diecast models with flashing lights. Two of these are on the layout, too. The illuminated buffer lamps are from Train-Tech and just clip on between the rails. They are actually a great indicator of connection problems as if they go out, we know that a road has a short or a power drop! So, now I had a fire scene with The Queen Vic pub on fire. As there is no fire without smoke, I pressed a Steam Master dry steam effect unit that I had in stock into service. These use a vapourising condenser to evaporate water to create a realistic steam effect. It's great to watch the steam rise behind the pub then gently drift over the road and billow down the parapet of the bridge while a service is trundling away below.

from Layouts4U. The layout only needs three 13amp sockets, one for the Procab controller, one for the 12V signals and one for the station and yard lights. Other lights and effects are powered either by 12V or 3V batteries secreted behind the backscene.

Buildings

The line was already populated with some buildings from the usual suspects. The aforementioned station was complemented by a Skaledale bridge to connect both platforms. I actually had to slice this in two, and remove 2cm from the middle as it was too wide for the platforms! The water tower, Portakabins and re-fuelling point were

from Bachmann Scenecraft, and the railway arches were from Wills kits. Ethan had done a fine job building and weathering these, but his show-stopper was the four-road engine shed (now re-named Upham Traincare Depot), which was very well accomplished. It is actually four Wills engine shed kits added together.

Aside from the street scene, the remaining buildings to help fill in the backscene behind the station were the back walling, gates, low relief offices and warehouse units from Scale Model Scenery. Security fencing and the telephone mast were also purchased from this company. I initially had the notion not to add anything to the backscene, but I had

Embellishments

I probably had more fun in this department as I just let my imagination go wild. Clutter everywhere – just as I remember it! Once again, Ten Commandments and Scale Model Scenery were instrumental in this respect. The former provided authentic NSE platform seating, a Portaloo and BR BRUTES (the trolleys often seen on station platforms) as etched-brass kits. The latter supplied various palette loads, including branded Buildbase bags of sand. A cycle rack, pillar boxes, telephone boxes, ticket barriers, ticket machines and kiosks were all from Peco and I am sure there is a ton of other stuff that I will remember I added long after this article has gone to print.

created a scenic imbalance insofar as the top of the line looked nice and busy by the parade of shops, but the station area looked quite desolate. So, with only 10 days to go before the photo-call from BRM, I bit the bullet and added these. Matt and Ethan used Wills railway arches extensively, but another three were added to the far right of the backscene as it did not look right being left in sky blue.

Also, as the backscene was being replaced, I took the opportunity to add a row of bright

white LEDs into the roof of the tunnel – as I now had access to it. These are rated at 12V and run off a small battery, which I use for one of my model boats. They come on a roll, can be cut to length, and have a sticky back to them, so are fiendishly easy to install. These cost only a few quid from that well-known auction website.

Signage and figures

I love to add as much detail as I can and noted that Scalescenes produced all the correct Network Southeast signage I could wish for. These are purchased as a

downloadable PDF file and you simply enter the name of your chosen station and the changes cascade down through the document, ready for you to print out.

The road markings and street signs are made by Roger Smith Signs and are available from Ten Commandments. I see so many great layouts spoiled by wiggly road lines, so for me, these were an essential addition.

There are over 100 figures on the line. The school party, line workers and road sweepers are from Bachmann, but as I wanted busy streets and platforms it would have been uneconomical to buy these in bulk. In any case, Ethan already had these extra figures to hand and included them. I mounted these



This great shot of the permanent way shows the superb job that Ethan has done on the ballasting and track detail. The nine signals on the line are controlled by simple rotary dials mounted to the front of the layout.

ON EXCLUSIVE

onto small clear plastic bases so I could move them around wherever I wanted.

Stock

I have around 30 DCC locomotives to hand, mostly with sound, which suit this layout. I can run a passable set of BR blue locomotives and stock, or indeed a number of yellow Railtrack Class 20s and Class 37s, which are very appropriate for night time running sessions and track repairs. It is the Network Southeast stock that is my favourite though, as I just like the livery. I can just about squeeze my Bachmann four-car VEP into the platforms and then make it disappear into the fiddle yard, but in

practice, I prefer to run single MLVs and a two-car Class 128 DMU. I also really rate the single-car Heljan Royal Mail DPU with the awesome sound chip from Olivia's Trains.

Worthy of mention also is the Bachmann Class 108 DMU. If I had to pick a favourite it would be the Bachmann Class 205

'Thumper', complete with door-slam sound effects that I remember so very clearly. This sound chip by Legomanbiffo is seriously impressive.

Coaching stock has been provided by another good friend who has converted these Bachmann carriages with working interior lights and 'stay alive' capacitor lighting, so they don't flicker over the point



work. These look great!

In one of my wilder moments, I had even resorted to running one of my four sets of London Underground's four-car S-Stock sets on the farthest-most road just for a bit of fun. These were made by Bachmann in limited quantities especially for the London Transport Museum and have long-since sold out. This S-Stock is far too new to fit in with the predominant NSE rolling stock, but I take a liberal view that this is my railway so I run what takes my fancy. They look pretty nice alongside the existing NSE stock in any case, even though these come from two different eras!

A typical running session involves one operator controlling operations to and from the station platforms and the TMD and occasionally to the first row of the fiddle yard to simulate a train on a longer excursion. The second operator rearranges the carriages and other diesel locomotives between the rear four roads adjacent to the station, up to the rear two roads of the fiddle yard.

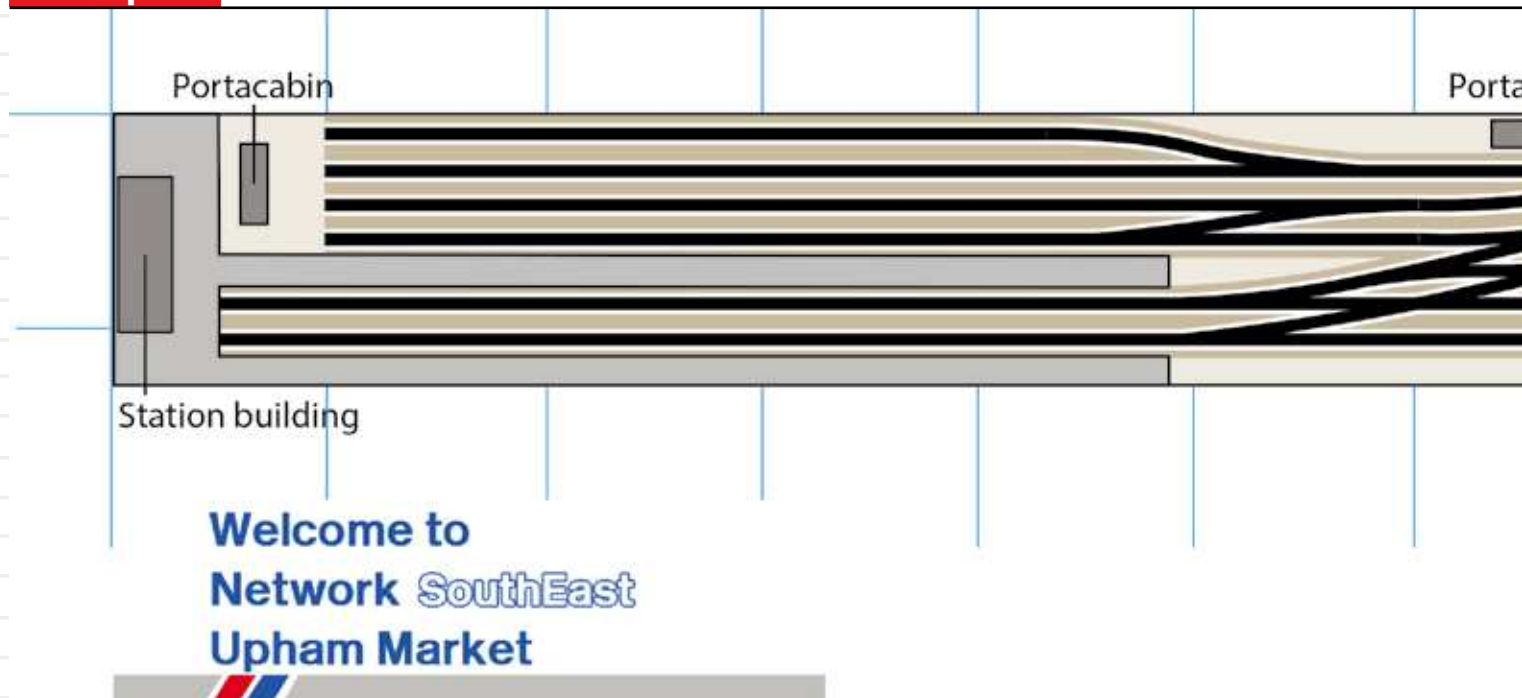
Final thoughts

So, while this is a first-time effort in this scale for me, it is also a collective effort and it would never have been completed without Ethan's great kick-start, assistance and encouragement. I am still in close contact with Ethan and Matt to this day and I think they like to see the odd update here and there. I hope the collective work



Railfreight 47301 in front of a Class 31 wearing the same livery rest in the storage sidings behind Upham market station. The low relief buildings add interest and the light blue back scene boards really looked too plain without them. Most are from Scale Model Scenery and were finished just in time for the photo shoot too!

Trackplan

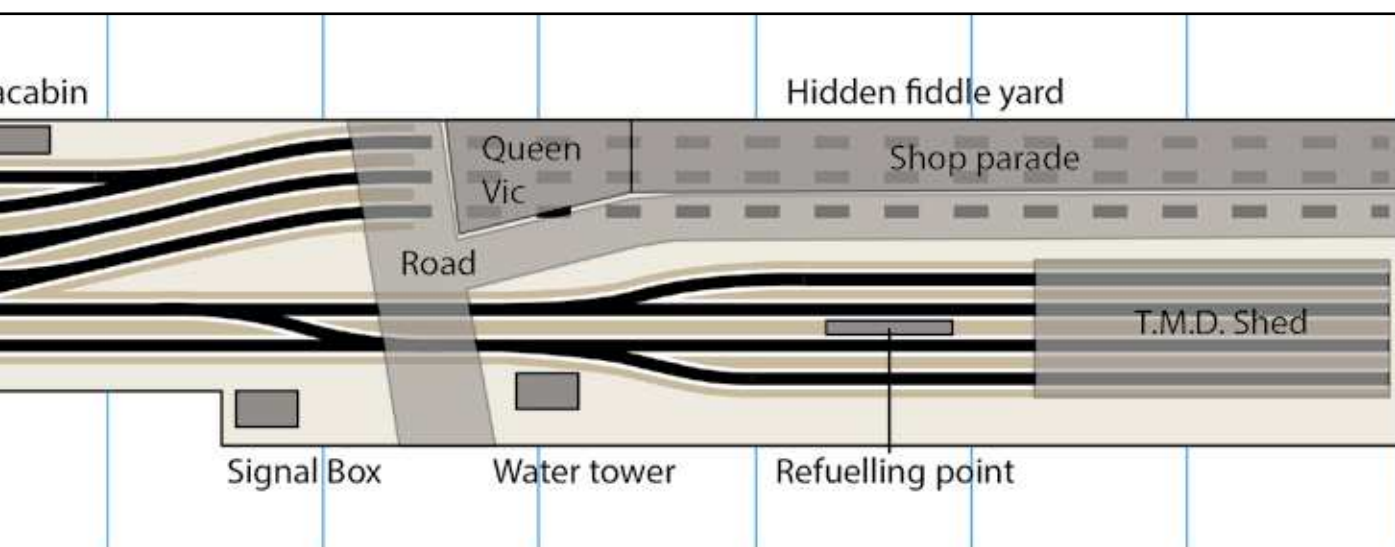




The four-road Upham Traincare depot was expertly made by Ethan and Matt utilising four engine shed kits from Wills. The ubiquitous railway arches are also from this supplier.

of the team this end has carried on the high standards that they set. Thanks also to Dave "The Steam" Bull for sorting out the wiring, Daniel Walker for creating those great lit carriages, Dan Cousins for helping me complete the scenic elements (just) in time and Jordan Greeves, mainly for making sure I did not buy another budget locomotive at a Toy & Train fair that was not at least DCC-ready! Of course, thanks also to my wife, for driving to help pick 'Upham Market' up, helping us all install it and for letting me disappear for hours on end into my grotto to get it to the stage that you see here today.

So have we pulled it off? I guess that is for the broader model rail fraternity to judge. At the start of this particular model railway journey, I had a vision of what I was hoping to achieve, based solely on my recollections of the line. I studiously avoided undertaking any further research as I felt that could cloud my judgement and mess up my recollections. So, for me, this is simply a 3D-picture of what I witnessed during my frequent rail commute, as only I remember it. Looking at the layout while writing this, it evokes great memories, so on that level alone it has been worth the effort. Of course it is un-prototypical on so many levels. Gidea Park was a through station, not a terminus, and Romford's 'Station Parade' of shops was at ground level. I guess that is what we call modeller's licence!



HOW TO... WIRE A DCC RETURN LOOP

Words & photography: John Gay

Compact auto-reverser modules automate power switching, preventing a short-circuit on layout return loops. John Gay explains how the small devices are installed.



This is a guide to understand the reasons for installing a DCC Auto Reverse Module. We'll discuss problems that you might have in the process and some wiring and installation tips.

Firstly, let's explain why we need to talk about wiring a return loop. "It's just DCC", I hear you say, "So, we just need two wires." This is true until you get your two wires crossed!

Let's explain the problem here. For example, out of my DCC system, I have black and red wires to feed my track. I then use a little saying to myself: "Black is back". This is my way of remembering that, when I am standing in front of my railway, the back rail of the track is always fed by the black wire.

Now, imagine if you were to take your train and stick a label on the side of it.

Five tips for tidy wiring

1. A main DCC Bus (twisted bus wire) running under the layout with terminal tags keeps wiring tidy.
2. DCC droppers soldered to the underside of track hide the wire better. When ballasted, it's very hard to notice.
3. Dropper wires from the track can be soldered to terminal tags.
4. Good quality wire strippers make easy work when wiring under a layout. DCC Concepts (DCT-BWS) are a useful product.
5. Extra wire can be added using West Hill Wagon Works Cable Management Clips sold as a four-pack.



Track cutters
Drills (various sizes)
Atten (ST-2080D) soldering iron
Wire strippers
Small screwdriver set
PVA glue (to fix track)



Digikeijs (DR5013) Digireverse (from Digitrains)
Peco (SL-100F) Code 75 flexible track
Peco (SL-E189) large radius left-hand point
Peco (SL-111) finescale insulated rail joiners
Twisted pair wire and Terminal Tags



Fig 1: Train heading into reverse loop.



Fig 2: Train returning from loop.

Looking at the train (Fig 1) below, the label is closest to me in my sight and is on the side of my 'red' rail. I set my train off down the track towards my return loop. The train will run round the loop and on its return, is the label is on the other side of the train, not visible (Fig 2).

The label is now on my back rail, which is fed by my black wire. Sounds pretty simple, doesn't it? And what you would expect. With this in mind, my front red rail has now become my black back rail. If we were to wire this up normally, this would cause a track short circuit. Your DCC system would trip out and you would lose power. So, at some point on the train's journey, we need to change over the track power, where red rail power goes to the back rail and black rail power goes to the front rail. We can't do this on the whole layout, it has to be done in the reversing section of track that we can control with some sort of changeover unit.

The shorting out problem would become evident at the entry or exit to the reverse loop. With the point set straight on, you can run your train round the back, but when you change the point to make the return trip, you would have a short circuit on your track power. Traditionally, with analogue DC control, you'd need to stop the train in the reversing loop and switch the track power manually to change the power to the track wires, and reverse the direction switch on the controller to keep the train running in the same direction. Then you'd be able to change the point and run your train off the loop back onto the main layout. With DCC, you can achieve seamless running as there is something a little better to help us with this problem.

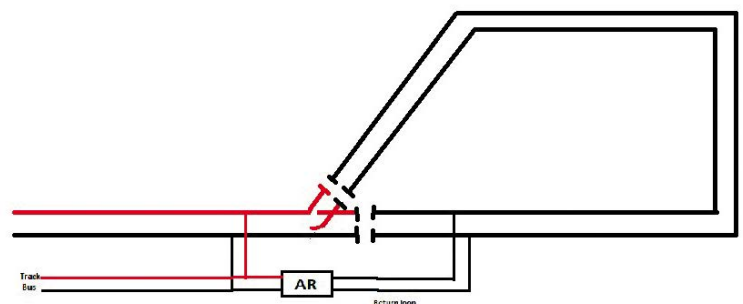
A DCC auto-reverser is useful. These units automatically change track polarity in an instant in response to sensing the dreaded short circuit. The key point here is that because track polarity doesn't govern train direction with DCC, this switching operation can be done on the fly without stopping the train. The decoder in the locomotive will barely notice the momentary change in track power configuration under its wheels and will continue to run in the direction it was instructed to originally. Just make sure the point is set correctly for its exit from the loop before it gets there!

Diagram showing basic wiring setup of an auto-reverser unit.



Here are four examples of DCC auto-reverse modules. These are the Digitrax AR1, DCC Specialties PSX-AR, Digikeijs DR5013 and the Tam Valley Dual Frog Juicer. Some are basic, switching track power polarity only, others are more complicated, having extra functionality and features too, such as point motor control, to change the point on your loop and block occupancy to show that there is a train in the loop.

Basic ones, such as the Digitrax AR1 and TVD Dual Frog Juicer, are simple to use and wire, as shown opposite.



The PSX-AR from DCC Specialties can be wired the same as the other units, but it has a few extra features. It can change the point motor attached to your return loop. It's compatible with stall-type motors, such as Tortoise or Cobalt, or solenoid from the likes of Peco or Seep. Other versions of this auto-reverser can also provide feedback (PSX-ARFB).

The Digikeijs DR5013 is the unit I have chosen for a few reasons:

1. Automatic changing of the return loop's point with feedback.
2. It can report back to my DR5000 that the section of track is occupied.

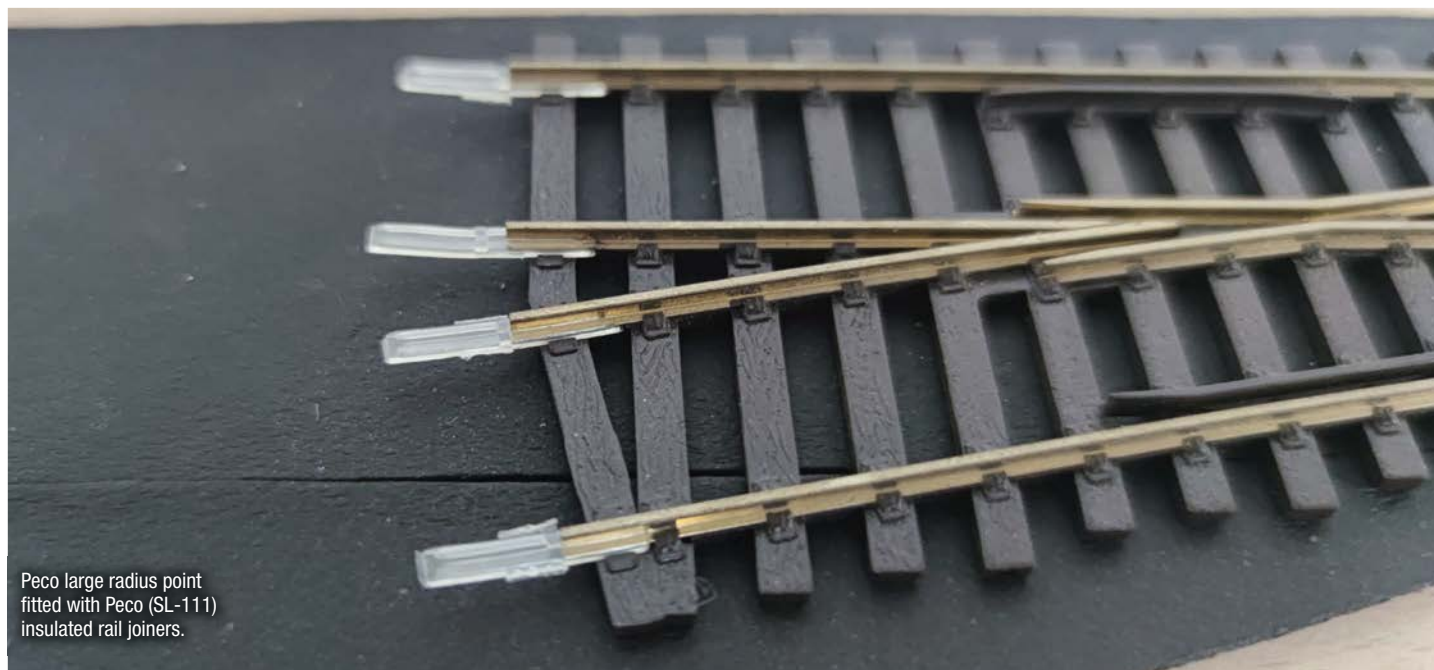
TECHNIQUES

Setting up to use an auto-reverser

Step 1

If we are changing track polarity, we need to isolate the return loop track from the rest of the layout.

Install isolated rail joiners or cut the track to give a small gap between the two rails. This must be done to both rails on both ends of the loop or section in question, so all four rails in total, as illustrated below. I'll be using isolating rail joiners to make my return loop. Beyond this point, the track splits and enters a loop, with power fed by the auto-reverser.



Peco large radius point fitted with Peco (SL-111) insulated rail joiners.

Step 2

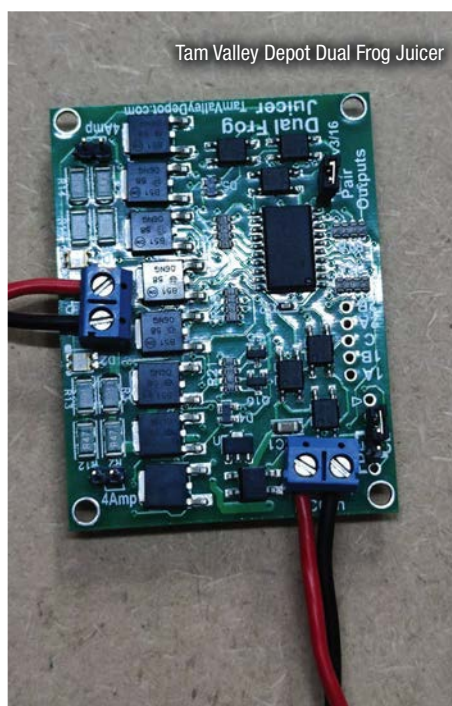
For the purpose of this article, we will ignore the Loconet and the two track sensor wires. Let's focus on the red and black wires – the input and output. All auto-reverser units have a track bus input and output.



Digikeijs DR5013

Shown here, at the base of the unit, we have the two track input wires (Track Input K & J), and at the top of the unit there are output wires to the isolated return loop track (Track C & T).

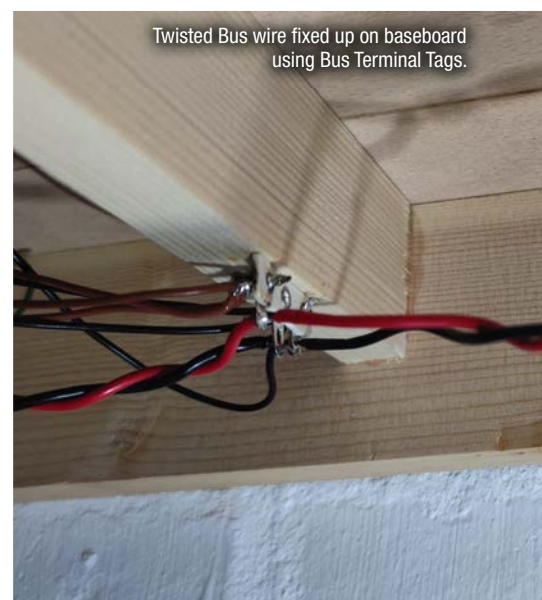
A more basic unit would look something like the image adjacent, with a track bus input and output to the return loop track.



Tam Valley Depot Dual Frog Juicer

Step 3

We need to attach the two output wires from the auto-reverser unit to the isolated looped section of track. I recommend soldering your dropper on the underside of your rail – that way, when you weather and ballast your track, the droppers are hardly noticeable. Underneath my baseboards I use twisted Bus Wire, and every 30cm this is fixed up to the baseboard with a bus terminal tag. This makes it neat and tidy and easy to add droppers from my track to the bus terminal tags



Twisted Bus wire fixed up on baseboard using Bus Terminal Tags.

Step 4

Now, time to run some trains! Test run trains in each direction around your return loop to make sure the newly-installed auto-reverser is working correctly.



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Layout wiring tips

Howard Smith demonstrates some key wiring techniques while working on the RM project layout 'Runcorn Salt Union'.

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PLAY

DIE-CAST UPGRADES

Words & photography: Phil Parker

Altering a road vehicle collection to give a workaday appearance is Phil Parker's latest mission, using techniques equally applicable to rolling stock.



Trains are great. Criss-crossing the country, they transport bulk loads with wonderful efficiency, until the last mile or so. Once freight gets to the railhead, there's no option but to use road transport for the final delivery.

We might love trains, but layouts with post horse-drawn vehicles also need lorries. Lorries need loads.

Model lorries are readily available, but it's rare to find one supplied with a load. Those that are tend to be low-loaders hauling something interesting, though unusual. We need a selection of mundane loads. The sort of things that you'd see every day on the roads.

Handily, the techniques we use for loading railway wagons can be adapted to work just as well for lorries. I've built four different loads, and, since I only have two lorries to hand, most of the loads can be swapped around, which will allow me to vary the scene on my layout.

As one of the vehicles is from British Road Services, created in 1947 by nationalising the road haulage industry, it's appropriate to swap loads. After all, the real vehicles will have been used for whatever needed to be moved. Don't forget, the railways operated under Common Carrier rules until the 1960s, so were unable to refuse any load offered, so their vehicles would be stacked



Road Wagon Loads kit - £15.00
Geoscenics
www.geoscenics.co.uk

The Army Painter
www.thearmypainter.com

with a huge variety of goods. Unless you glue vehicles down, swapping loads is a fast way to make changes on a layout.

Working lorries also get dirty, so before loading up, I like to weather models. Most die-casts look far too toy-like with their shiny paint to fit into a model railway scene properly, but toning this down isn't hard and the results make any layout look a lot better. The standard of decoration on a modern model is amazing, so let's get the best out of it.

Vehicle weathering

1



Straight from the box, an attractive though shiny die-cast lorry from Base Toys. It's very pretty, and would look nice on a collector's shelf, but on a model railway? It's more toy than model.

4



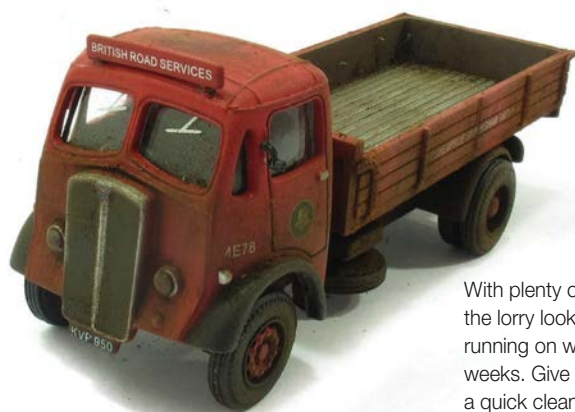
Normally, there isn't a need to repaint the model, just dull the shine with a spray of matt varnish. I'm using an aerosol from The Army Painter range, which seems especially effective if applied in thin coats.

7

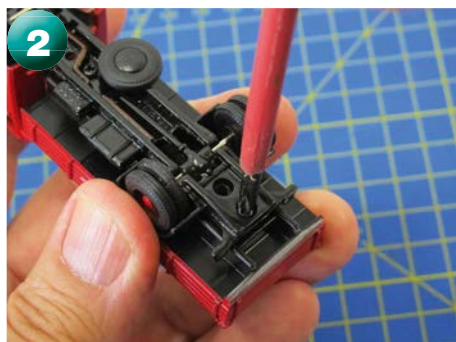


If the worst happens, and you break the glazing when removing it, the glass can be replaced with Deluxe Materials Glue'n'Glaze. It works better on vehicles with flat glass, but as long as you keep the liquid horizontal while it dries, the results look OK, and certainly better than no glazing at all.

10



With plenty of dirt applied, the lorry looks like it's been running on winter roads for weeks. Give the windows a quick clean, and it could be ready for the layout.



Taking the model apart simplifies the painting stage. On this model, there are cross-head screws holding the body to the chassis. At the front, the axle needs to be removed to get at the screw – pull the wheels off the ends and it slides out.

5



For many, simply taking away the shine from the model will be enough. It certainly looks a lot more realistic with its matt paintwork.

8



The load bed area of model lorries often looks too clean. Normally supplied painted a mid-brown colour, dry-brushing the planks with pale grey and cream looks more realistic for the bare wood normally found here.



Inside, the glazing plastic is held to the cab with plastic rivets. Drilling them slowly with a large bit allows parts to separate. This trick works if your model is held together with rivets instead of screws.

6



Sometimes, it's not possible to take a model apart. Spraying varnish over the glazing would fog it, so this needs to be masked. I find that Blu-Tak worked into the edges of the aperture with a small screwdriver works well. Keep it very thin at the edges so that varnish reaches inside the aperture. Poking the Blu-Tak with a cocktail stick will remove it later, without scratching the plastic.

9



The matt-painted body is perfect for weathering powders as its microscopically rough surface will hold them without the need to seal it afterwards. Simply scrub a mix of dark brown and smoky black into the surface with a stiff brush. By working over a plastic tray, I collect spare powder, which is then put back on the model, reducing waste.

11



For a lorry working in warmer months, give the body a wash with water and a cotton bud. This will turn the weathering powder liquid and leave it in crevices where dirt collects on the real thing.

TECHNIQUES

Adding loads

1



Geoscenics produces a useful range of loads suitable for road and rail vehicles.

4



Moving to a load covered with a tarpaulin, I need to make a convincing shape on the back of the model. Strips of balsa wood look like packing cases once hidden by the sheet.

7



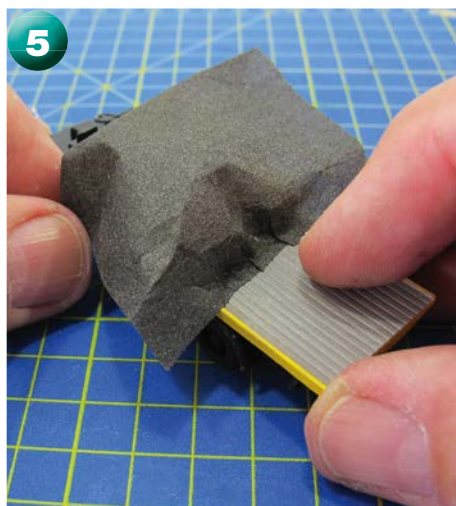
Barrels, packing cases and oil drums in the range are high-quality 3D prints. Painting is easier if they are glued to a board with double-sided tape first – far easier than trying to hold each one in your fingers. It makes spray painting a breeze.

2



Starting with a coal load, cut a piece of the Insert board slightly smaller than the bed of the lorry, colour it black and drop it in, over clingfilm. Pour coal on top and then flood with watered-down PVA, exactly the same as you'd do when ballasting. The clingfilm will stop everything sticking to the model.

5



Covering up is a bit like wrapping a birthday present. UHU sticks the tarpaulin material to both the metal and wood. It grabs fast, too. Real sheets will sag where unsupported by the load, so try to model this if possible.

8



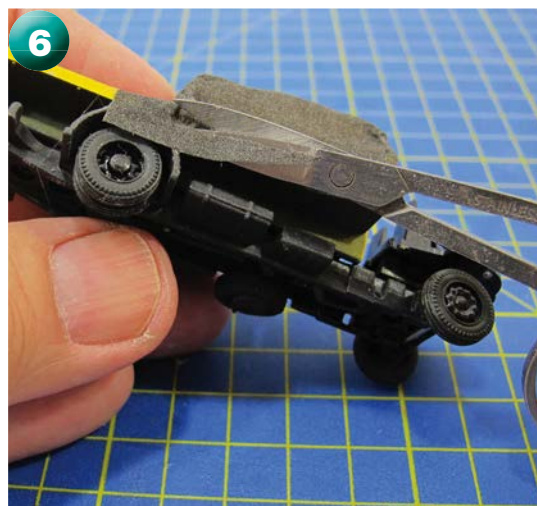
To make the load removable, a false floor is made from scribed Plastikard sheet. Painted to look like the wooden load bed, it's invisible once in place, especially with the boxes, drums and barrels glued on. Mixed loads like this were common right into the 1960s as the railways were obliged to take all traffic offered to them.

3



When dry – this can take a few days – the load can be removed. A little rub around the edges with fine sandpaper should ensure that the pile of coal slips in and out of the lorry easily.

6



Working with a bigger sheet than you need makes the job easier. Any excess can be trimmed away with small scissors. All that is left is to superglue some of the supplied rope over the top to look like it's holding the tarpaulin in place.

9



The simplest load is planks of wood. I fit these to a piece of balsa wood a similar colour to the supplied strips. Doing this makes the load look bigger and let's the material go further.

BRM readers receive an introductory 20% discount voucher on these lorry loads using code BRMRoad1 at www.geoscenics.co.uk

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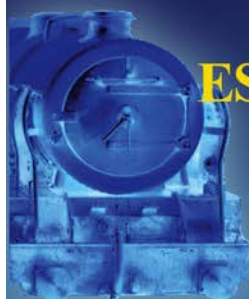
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PLAY

Stafford Railway Circle



In a new monthly feature for BRM, we talk with a model railway club to find out their projects and membership benefits. This issue, we speak to Terry Robinson of the Stafford Railway Circle about his favourite aspects of the hobby, and what it takes to organise an exhibition.



BRM: When was the club founded?

Terry: Stafford Railway Club was founded in 1957 and its activity was mainly visiting steam sheds across the UK. A current member often relates the story of a visit to Swindon Works while they were building the Class 52 'Westerns' and seeing a GWR 'Castle' in the Works.

Although called Stafford Railway Circle, our modern clubrooms are based in Stone, about six miles north of Stafford with good access and ample parking.

BRM: What age do you have to be to become a member?

Terry: To become a full member, you must be 18. However, we actively encourage younger members. Under 18s must have a guardian or parent with them.

BRM: How many working layouts does the club have and in what scales/gauges?

Terry: We have the following club-owned layouts:

- Three OO gauge layouts – 'Whiteacres', 'Stackton Tressell' and another new layout under construction called 'Meyrick Road'
- 'Freshwater' was a station on the IOW, which is modelled in EM
- The club also has two new layouts under construction as yet unnamed, one in N

and the other in O

- Finally, in 7mm:1ft scale, we have our 0.16.5 narrow gauge layout called 'Seagone'

Some members are unable to build a layout at home for many reasons, so we have the Modular Group, which enables you to build a 4ft section of a layout that can be joined to other members' modules on construction/running nights at the clubroom.

BRM: Are you looking for members skilled in specific disciplines?

Terry: I think it's fair to say anyone can come along, and if they have any particular

skills, they would be most welcome.

It's much more likely that somebody new to railway modelling seeks out a club to further their modelling skills and knowledge. There is no better way than joining your local model railway club to enjoy the hobby.

BRM: Do you have a favourite era or region to model?

Terry: My main interests would be the Great Western Railway and more recently the Southern Railway in the West Country.

BRM: What is your preferred track to use and why?

Terry: All the layouts I have built or purchased have used either Code 100 or 75 from Peco. I wish I had the skills to build my own track, but I know my limitations.

BRM: We know you're an advocate for DCC, but do you have a favourite system?

Terry: The technology of DCC won me over from its earliest days, and although many club members thought it would never catch on, they have all changed their minds in recent years, especially with the introduction of sound.

My DCC journey started with the original Lenz system in a wooden case, but soon moved on to using the Gaugemaster Prodigy system. More recently, I have changed again, this time to NCE.

I'm often asked which is best; the answer is to try them all and make your own decision.

BRM: Do you have a favourite layout from over the years?

Terry: Yes, some layouts I've seen over the years have left fond memories. 'Yatton Junction', 'Dewsbury Midland', 'Bournemouth West', 'Black Country Blues' and 'Runswick Leamside' are some of my favourites.

BRM: Tell us about the Stafford Exhibition being held in September this year?

Terry: For the last 20 years or more, we have held our annual exhibition on the first weekend in February at the Stafford County Showground in the three smaller halls.

Two major changes from 2022 onwards are that all future exhibitions will be held on the last weekend in September and will now be in Bingley Hall on the same site. This has almost doubled the available floor space with much-improved facilities.

Stafford is probably the largest

independent model railway exhibition in the UK, with a regular attendance of about 5000. The exhibition is financed by the club, without sponsorship.

We are hoping the change of date and much larger venue will encourage an even larger attendance, the last two exhibitions having been cancelled due to Covid restrictions.

BRM: You must be happy to be back, then?

Terry: I am only one of a very dedicated team that over the years has made Stafford Railway Circle a successful club with an annual exhibition to be proud of.

Planning an exhibition of our size is an ongoing job for the exhibition team, but it really starts in earnest about six months before each exhibition.

This includes confirmation of all the layouts, traders, planning the advertising, and organising the hotel accommodation for layouts. Also, organising the electrics, making sure all the equipment is safe and PAT tested, arranging van hire, and collecting the barriers from the stores and Warley MRC.

Exhibitions are, I believe, the lifeblood of model railway clubs, and the last few years they were sadly missed, so I say to each and every one of you, please support and attend your local exhibitions, and come along to the Stafford Exhibition.

BRM: What would you tell people considering joining a club?

Terry: For me, being involved with the club has brought many benefits, from learning modelling skills, understanding how the real railway works and long friendships with other members. Joining a club has many benefits once you have dipped your toe into the water. The first night at the club can be a bit daunting, not unlike your first day at school or a new job, but persevere as it's well worth it.

All clubs have a vast amount of experience from its members for you to draw on, and are more than willing to help advise on the way they would tackle specific issues you need help with.

Quick facts

Members: 60+

Club nights: Tuesdays and Fridays 1900 - 2200

Current annual membership fee: £70 p/a

Website: www.staffordrailwaycircle.org.uk

Meet: Terry Robinson

I came back to railway modelling in my late twenties in 1976, after picking up a copy of a model railway magazine to read over the Christmas break. I discovered that Chester Model Railway was my nearest model railway club and became a member working on the 3mm group layout.

After a house move in 1977, I joined Stafford Railway Circle; little did I know that some 45 years later I would still be a member.

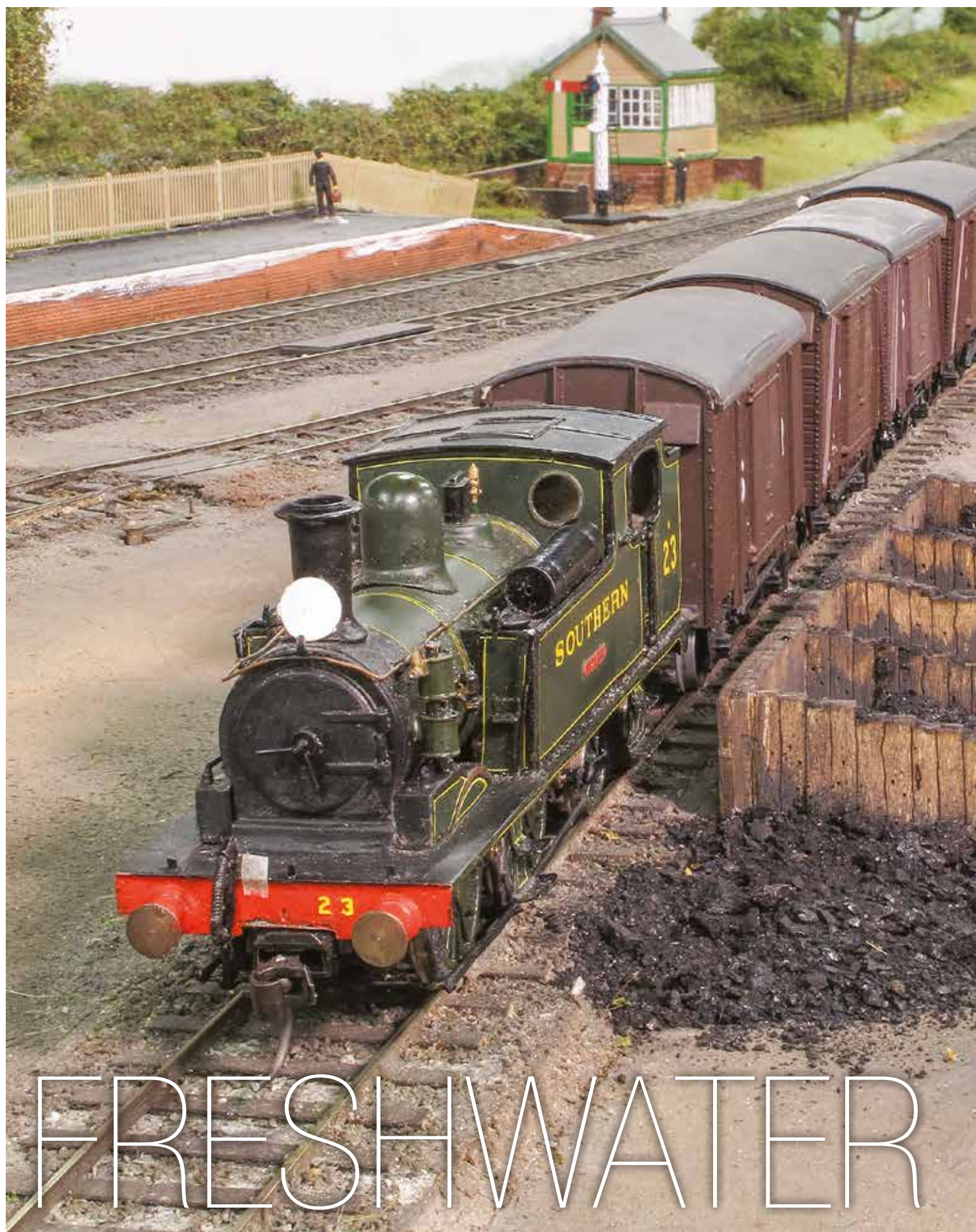
I became involved in the running of the club and was very much part of the two major changes for the club.

The first was moving our annual exhibition to the County Showground in 2003, and the second was to find new clubrooms, the subsequent move in 2010 to a modern clubroom on two levels with all the facilities.

The help I have received over the years is priceless, and I hope I've passed on many tips to the next generation of railway modellers who join Stafford Railway Circle.



DIGITAL EDITION



FRESHWATER



1 David Ford and Tony Parker explain how Stafford Railway Circle refurbished and completed this attractive layout built by the late Fred Shilton.

FACT FILE

SCALE/GAUGE: 4mm:1ft scale, 18.2mm gauge EM
SIZE: 13ft 11in by 2ft 6in-3ft 6in
ERA/REGION: Southern Railway 1925-35
LOCATION: Isle of Wight
LAYOUT TYPE: Fiddleyard to terminus
POWER/CONTROL: 12V analogue DC
WORDS: David Ford & Tony Parker
PHOTOGRAPHY: Andy York

Fred Shilton's EM gauge layout of Freshwater (IoW) station was entrusted by his family to the Stafford Railway Circle when he passed away recently. Our task was refurbish and complete it to exhibition standards.

The layout was designed as an approximation of the station area and to be a permanent fixture, mainly to display his collection of scratchbuilt locomotives, coaches and wagons. Extra baseboards and a new fiddleyard have been slowly constructed, re-wired and an additional scenic section has been created by SRC members. The layout is now portable and has started to attend exhibitions.

Freshwater station on the Isle of Wight opened in 1889. It was the largest station on the Freshwater Yarmouth & Newport Railway (FYNR) line with a single platform and a two-storey building at the end of a short approach road. As well as Freshwater, the station served surrounding areas of Alum Bay and Totland. Its main traffic was passengers and coal, with some goods and cattle.

It would seem that the Isle of Wight is on another planet, as research into the FYNR has proved difficult. Even now after talking to many people and accumulating books, maps and internet information we are still in conflict with just exactly what happened at Freshwater in 1925-35 - the time that Fred chose to model. His trackplan was based on various, somewhat conflicting, original plans and drawings. There was a single platform with a run-round loop and headshunt, two goods sidings and a cattle dock plus a coal unloading and storage shed.

Whilst this seems straightforward, we have found that between 1925 and 1928 the Southern Railway altered the station area. It removed locomotive and carriage sheds and a cross track, added and lifted sidings and points and moved the signalbox. The platform had up to four changes in length, but the dates at which they occurred are vague. Depending on the result of your research it could all have happened at once or over three, five or even seven years and in no particular order!

Early timetables show a service pattern of eight or nine trains a day of passenger, mixed and goods from opening right through the Southern years. We will run more than this in a running session!

LEFT: Adams 02 0-4-4T No. 23 *Totland* brings a train of Southern 12t vent vans into the goods yard. Some hard manual labour is underway to fill sacks for the coal merchant.

The layout was designed to be a permanent fixture, a place to display Fred's collection of scratchbuilt locomotives and wagons, but it's now portable and has started to attend exhibitions.



2

CONFLICTING INFORMATION

We decided to keep approximately within Fred’s chosen dates and retain the track as he laid it. Many said that work to amend the track layout may have started in 1924, with the changing of rail from flat-bottomed to bullhead type. We have maps from 1888, 1905 and showing the proposals in 1924 and some showing no change until 1927 plus photographs from 1892, 1906 and throughout the 1920s and 1930s. It seemed to have settled down after that and there were no further changes until the line closed on September 21 1953.

However, to save us lifting and relaying track and buildings we kept the signalbox and the formation in its original position. We changed the long building in the station yard from an unusual ‘coal house’ into a warehouse and turned it around to show a vehicle entrance door. An oil storage tank is now at the end of this building and much nearer to the road.

Some buildings at the junction of School Green Road,

Freshwater fleet - locomotives

Company	Type	No.	Name	Year	Livery	Notes
FYN	Terrier *	2		1913	pale green	Started as 'A1', rebuilt as 'A1X' and renumbered W8
Southern	Terrier	8	<i>Freshwater</i>	1932	dark green	'A1X' - See above ex-LBSCR
Southern	Terrier	12	<i>Ventnor</i>	1903\$ 1930	dark green	'AIX' ex-LBSCR
IWC	Beyer, Peacock	4	<i>Ryde</i>	1876	maroon	IWC provided all motive power until 1913
Southern	E1	2	<i>Yarmouth</i>	1932	dark green	Ex-LBSCR
Southern	O2	23	<i>Totland</i>	1925	dark green	Ex-LSWR
Southern	O2	24	<i>Calbourne</i>	1925	dark green	Ex-LSWR

\$ arrived on IOW

* Preserved



3

Coach sets

Company	Type	No.	Liv- ery	Notes
Various	50ft	4	green	Varied sources at different times, during the life of an independent FYNR as well as the Southern. Kept in sets on the layout
	28ft	5	green	
	28ft	4	teak	

Goods wagons

Company	Type	No.	Livery	Notes
IWC & SR	coal	7	various	There are other wagons of various types and colours of course, mostly brown or grey. These are RTR items modified to EM
SR	vans	4	Dark brown	
SR	brake	4	Dark brown	
IWC	open		Brown/grey	

ON EXCLUSIVE



4

OPPOSITE FYN 'Terrier' No. 2 is one of several ex-LBSCR locomotives transferred to the Isle of Wight.

LEFT: One of the Southern Railway's ubiquitous sectional concrete provender stores has been built in the goods yard.

THIS PAGE: Period details are carefully placed around the layout to help set the era and location.



This layout was donated to the Stafford Railway Circle by the family of the late Fred Shilton, who was a long-time member, and is exhibited in his memory.



Hooke Hill and Stroud Road near Bow Bridge have been added to give more character to the station area. They are scratchbuilt, based upon local architecture and although rotated by 90 degrees they are positioned mainly as buildings in the map for 1927.

The scenic section of the layout is 10ft 11in (3,330mm) and the fiddle 3ft 0in (914mm) long with the width varying between 2ft 6in (760mm) and 3ft 6in (1,066mm) wide at the station end. One major change we instigated is to have a road overbridge to the north-east of the station throat (right hand side) as we did not have sufficient room to do the road and level crossing some 800ft from the platform. This enabled us to cover the entrance to the fiddleyard - we don't like holes in the walls!

Fred built the layout to EM gauge with 'live frog' handbuilt points. We have modified the electrics and used the auxiliary switches of the H&M motors to indicate their position on the control panel and used

ABOVE: Southern 'Terrier' No. 8 *Freshwater* trundles in with a passenger train from Newport formed of 50ft stock.

ABOVE RIGHT: An overbridge has been brought closer to the station site to act as a break between the scenic section and fiddleyard.

RIGHT: No. 8 rests before running around its train at the terminus.





7

microswitches to alter the frog polarity. All the points are initiated via centre-off spring switches.

Analogue control is used throughout, with Gaugemaster model 'UD' panel-mounted twin controllers. There are six sections, all able to be driven by either controller. Dual-colour LEDs show which controller is driving which section.

Couplings are by Kadee with magnets fitted between the rails at strategic positions to allow for 'hands-free' shunting. Whilst they can take a considerable amount of fiddling to set up they seem to work well in practice.

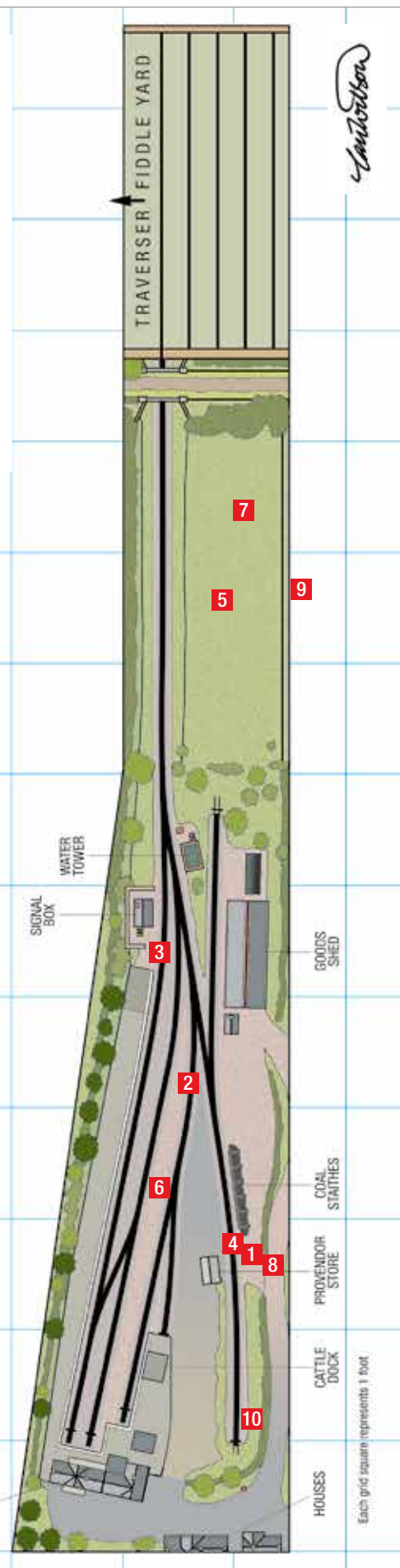
Two Dapol operating signals are used, one as a Platform Starter and the other as Home by the station throat. Static ground signals have been fitted by the run-round loop and the long siding. They are all of Southern Railway design.

The fiddleyard, hidden behind a backscene and front fascia is a sliding board with six tracks. Toggle bolts at the end of each track both secure their position and pass the electrical supply. The board around each track



6

TRACKPLAN FRESHWATER





■ back story the Freshwater Yarmouth & Newport Railway

The first railway to be built on the Isle of Wight opened between Cowes and Newport in 1862. In 1868 there was a proposal to build a line from Newport to Freshwater at the western end of the island. This came to nothing but in 1880 the Freshwater, Yarmouth & Newport Railway Company was authorised by Act of Parliament to build a 12-mile single track line with three passing places from the Isle of Wight Central Railway at Newport to Freshwater.

Construction started at the western end in 1886 with the line reaching Yarmouth two years later. It opened to goods traffic on September 10 1888. There

were some passenger excursions but the line didn't officially open to passenger traffic on 20th July 1889.

Intermediate stations were at Carisbrooke, Calbourne, Ningwood and Yarmouth and in July 1889 a private station was opened at Watchingwell for Sir John Simeon of Swainston. Eventually this became a public station and it appeared in public timetables from August 1923.

From the start the service was provided by the Isle of Wight Central Railway (IWC) which provided staff and rolling stock while the FYN was responsible for maintaining the line. This did not prove to be a satisfactory arrangement and after bankruptcy in 1896



formation has been roughly painted brown and grey so that viewers do not see unpainted wood through the bridge, which can be distracting. The fascia boards are set so that viewers can see the operation of the fiddleyard, as we know they like to view the stock.

The table opposite lists the main items of locomotive and rolling stock at the time the line is set, all built by Fred. They have come from many varied sources, particularly the 'Terriers' (A1X) and 'O2s' which were typically used by the Southern Railway and its predecessors on the island. Unfortunately, the Southern and the FYN had a habit of renumbering and renaming their locomotives to the extent that different locomotives carried the same number!

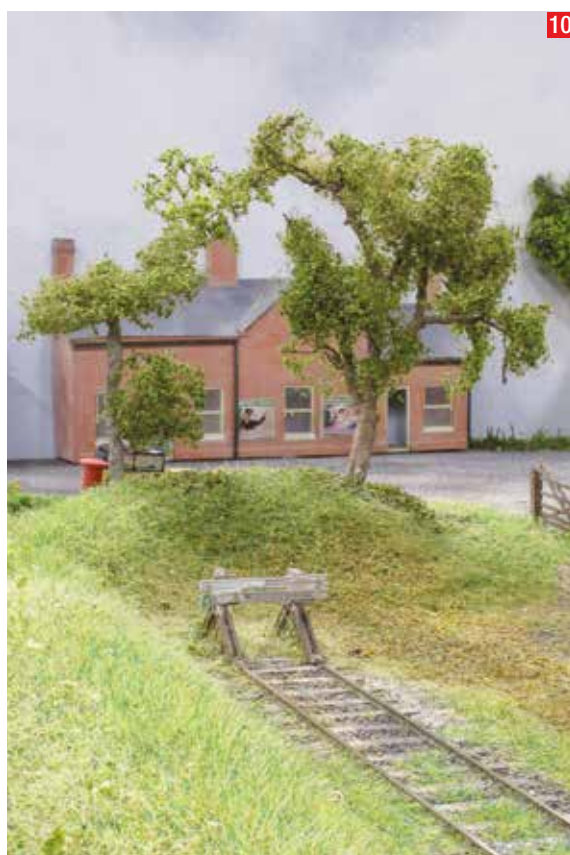
Equally the coaches appeared on the island at various times and came from the constituent companies of the Southern Railway - the LSWR, LBSCR and SE&CR. In addition, we have other vehicles of Great Central, Manchester South Junction & Altrincham (MS&JA), North London and Metropolitan Railway design.

Stafford Railway Circle members hope that we have done justice to Fred Shilton's scratchbuilding skills. We'd like to take this opportunity to offer our thanks to his family for their trust in allowing us to research details and refurbish of this unique piece of railway history and, most of all, play trains on the Isle of Wight. ■

LEFT: FYN No. 2 meets an earlier form of heavy goods traction in the goods yard. Wagons are a mix of scratchbuilt and modified RTR models.

RIGHT: Stafford Railway Circle members have updated and completed Fred's scenic work ready for its exhibition outings.

BELOW: A late-19th century passenger train hauled by IWC Beyer, Peacock 2-4-0T No. 4 *Ryde*.



10

the partnership was eventually dissolved in 1913 when the FYN bought its own locomotives and rolling stock. The company also built its own station 200 yards west of the IWC station at Newport. Initially passengers had to walk between the two stations but by June 1914 trains were allowed to run between them.

The line was not financially successful with the company operating in bankruptcy until August 1923 when it was taken over by the Southern Railway as part of the Grouping of Britain's railway companies in to the 'Big Four'. Shortly after, the FYN station at Newport closed and all trains ran to the IWC station.

Improvements were made with some through trains running between Freshwater and Ventnor but following Nationalisation in 1948 the line's days were numbered. In 1952 its closure was proposed and, following a public inquiry, approved with the last train running on September 20 1953.

After closure the station bookstall was relocated to the town and is still in use. The signalbox, which was moved to Freshwater from Newport by the Southern around 1924/25, became a bus shelter and was relocated to Wootton on the IoW Steam Railway in 1987.



9



MAKING TRACKS 2

Until September 3, Chester Cathedral is home to Making Tracks 2 – Pete Waterman's layout homage to the West Coast Main Line. Howard Smith visited to see more.

Words & photography: Howard Smith

Constructed in record time for its size, this is a sequel – of sorts – to Pete Waterman's 'Making Tracks', exhibited in Chester Cathedral last year and featured in BRM August 2021. The location for this exhibit comeback remains the beautifully historic landmark, opened in 1541, as does the subject matter – the West Coast Main Line, an artery that Pete has ridden thousands of times when commuting to London.

This isn't a re-hash of the previous OO gauge layout, though – it's all new. So new in fact, that went I visited on the day of its first public display, Pete could be found with a tube of UHU glue in one hand, and patch of grass in the other, adding scenery.

"There's a bit of grass that we missed around that mast, there", he says with a smile, pointing towards a section of the intricate, highly-respectable overhead catenary, modelled in full, with tensioner pulleys and weights. And, from a distance, it's the overhead catenary that dominates on this layout, set against the green scenery and sky-blue painted backdrop, interspersed with Pete's favourite structures of his familiar arterial route. From canals, to bridges, fly-overs, multiple gradients and cut-throughs – it's obvious that designing the baseboards onto which sits this enviable trackplan was handled by an expert.

Behind the layout, raised high above the fiddleyard and facing the public is a mimic panel, indicating the position of trains. It entices the viewer to raise their head from the layout, reminding them that behind the workings of this extensive operation lies a clever automated block signalling system, coded to detect the position of trains. The panel is controlled by a program that runs on a laptop and communicates instructions to an Arduino microcontroller. It was the brainchild of techno-wizard Phil Moreton. A brief look at his code shows the level of commitment to the project! "It needs a little tweaking", he tells me, "but it's 99% there..."

A team of about 12 has been responsible for assisting Pete in the creation of the layout, and their names and input are

proudly displayed at the end of the layout.

Though running to signals is recommended, the average punter at Chester Cathedral wouldn't notice. So, during busy times of operation, deft hands at the controls are required to see a minimum of four trains circulating at the same time – entertainment galore for the viewer, though many looks of concentration on operators' faces!

Constructing the layout in less than a year has been a feat for Pete, and his team. For one, there wasn't enough room to assemble the entire layout during construction. Secondly, the structures required weren't available 'off the shelf'. These had to be designed in CAD, tested, laser-cut, and assembled. A smaller section of the lattice bridge crossing the tracks to the left edge of the layout is now available as a kit from Chester Model Centre, main sponsors of the event, alongside Hornby. Add to this, software coded from scratch to control signals, and you can see the mammoth task that has been accomplished.

Despite descending on the team during its first day of public operation, things were running remarkably well considering the length of track and complexity of the

inclines being asked of the OO gauge RTR models. Time for a chat with Pete:

Howard- Well, here we are, in the beautiful Cathedral at Chester – a bit of a juxtaposition though, don't you think – a lovely old organ here, but the sound of horns is emanating from a cutting-edge DCC-controlled layout with sound? It's a great location!

Pete- [jokingly] I hope you're not referring to me as the old organ? [laughs] Yes, it's not a traditional model railway audience, and so we're doing two things – we're bringing a totally new audience to the Cathedral, and a new audience to model railways. Last year, the Cathedral was bowled-over with the number of visitors and their diversity – that's fantastic. We were so pleased because we heard many times, "Grandad, can you get your model trainset out of the loft?" That's what you want to hear!

H- I couldn't agree more. A few hours ago, I saw you were sitting with mates in front of the layout, enjoying the trains go by – it really is that sort of layout, isn't it?



Models are put through an arduous test for the duration of the exhibit, circulating around the 64ft x 12ft OO gauge model. Hornby A1 *Tornado* is in charge of a railtour. The manufacturer is sponsoring the event, alongside main sponsors, Chester Model Centre of 71-73 Bridge Street Row, East Grosvenor Shopping Centre, CH1 1NW.

P- Yes, and you know what? We don't get that time because we're usually busy modelling. Whereas here, when you have operators, it allows you the chance to just watch things go by. We're always too busy building these things, and you never get a chance to sit back, look at them and think, "this is just like the good old days..."

H- So, it's a condensed section of the West Coast Main Line...

P- A very condensed section of the West Coast Main Line...

H- ...featuring some of your favourite bits?

P- Yes, well, I 'spotted' at Hilmorton Junction, Rugby, in the '50s – I used to sit under the Great Central bridge because the signalman couldn't see me! That's the closest you could get to the line. The 'Super Ds' used to come in from Market Harborough and Northampton, and of course, you could sneak your pennies on the line!

When I saw the reaction to last years' layout, I thought, what's as exciting as the tunnels? And I instantly thought – Hilmorton Junction, because when the Up trains shoot out from below, combined with the flyovers... you have all sorts of things in the way. And on the model, when you see a 'Pendolino' come out of there at a scale

120mph, the kids get all excited. I wanted to pay tribute to BR, and the LNWR, so I thought, if we put back the original lattice bridges, alongside the modern bridges, it would be a great cameo, combining ancient and modern. It's not historically 'right' for either period, but it looks different and has some quite technical bits and pieces...

H- I see you're a fan of architecture as much as locomotives, then?

P- Oh yeah... I'm crazy about architecture, that's what I'm saying, the LNWR stuff that I was brought up with is fantastic – the blue brick and the lattice bridges. But, I also love the '60s stuff where the motorway influence starts to come in. Flyovers, bridges... I'm a sucker for all that.

H- Different architecture makes a layout more interesting...

P- Well, I think it does, and you've got to research it. And, I think that's part of the hobby. When did they build it, when did they knock it down? Or when did they change it, and why did they change it?

We had a problem building some parts of the junction on the layout, but thanks to Railnuts [modelling group], we resolved them. We came across the problem one night, so posted it online. In the morning, I woke up and a guy from Network Rail had posted, "here's your problem, because I've got all the drawings, and this is where you've gone wrong."

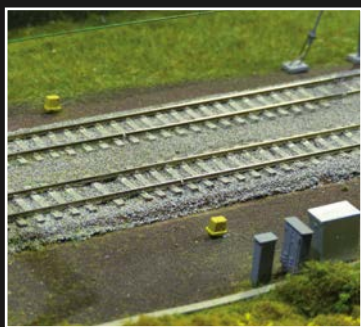
Originally we'd 'Googled' it, and looking from above, we thought it was a right-hand point, but it turns out it was a 'Y', and of course, that instantly changed the whole geometry of the junction. Ahh, there you go!

H- For a public not overly versed in railway matters, it's great to see such a variety of glamorous, and not-so-glamorous trains. In the space of a few minutes, I've observed a Blue Pullman, APT, *Tornado*, and a few seconds later, a pair of Class 37s running light engine.

P- Yes, I mean, we're trying to reflect the diversity of the railways. So we've deliberately got two 'Super Ds' on 50 wagons so we can say, "when I was your age, this is how we moved freight. Today, we put it in containers and move it at speed, but when I was your age, I sat underneath that bridge and watched these trains trundle past at 15



All in the detail...



Observing the prototype closely has resulted in smaller details like these wheel counters represented lineside, in yellow. Equipment boxes and ballast bags can also be found.



Overhead line electrification dominates the view from above – I've never seen it modelled on such a large scale before, though. Truly impressive.



Signalling is a joy to watch. Sitting lineside, it creates a sense of anticipation for a train soon to shoot – or trundle – through the scene. The variety of stock is entertaining.



DCC power distribution on a layout this large is important. Modelling the power distribution of the prototype hasn't been overlooked either, with transformers and rectifiers.

miles-per-hour." Nothing does 15mph these days.

H- From what I've seen, the kids that have visited so far are very excited, jumping up and down at the sight of a train, pointing out things they've spotted, perhaps inspiring them to want a model railway. How do you think this environment differs from that of a traditional exhibition?

P- Well, at exhibitions, people are too interested in showing you what they can do. We're not interested in that. We're interested in bringing people to the hobby, you know, get people behind and saying, "come on, don't be frightened, do it yourself".

I can give you a great example of this – we had a lovely lad who came last year who's absolutely fascinated with the APT. He came up from London, twice, I think, with his dad. He was so excited that he got his dad to build him a 14-car set. He was due to bring that set here today to run it. But, with the weather being so hot, they're going to do it next week.

That's what it's all about... He went away and said, "Dad, let's build a bigger one of those!" And his dad did. That's like me in 1952, when I went to my first model railway exhibition at the Jaguar Social Club at the Jaguar Model Railway Society. I remember looking at all the switches and the signals with them all going up and down and thinking, "wow, this is what I want to do!"

H- It would be remiss if I didn't shout about the unmissable LED matrix display behind the layout showing train movements – that looks like a work of art!

P- Yes, that's Phil (Moreton). This is a new idea – we've done it, and for the next one, we'll improve it even further. We already

know how to improve it and make it even more useful in the future. I think that when people see this – once we've perfected it – they'll say, "why don't we have one of them?" It involves the public more and more, and that's what's really good. You can give your tablet to the public and say, "right, follow your train around, just make sure you watch all the signals."

Of course, I could have spoken with Pete much longer, and asked far more questions about the layout – and I did, you can watch more in the video of Making Tracks 2 on this month's episode of World of Railways TV, free to BRM paper and digital subscribers. However, rather than give every last detail about the layout away to which Pete and his

small team have invested so many hours, I'd encourage you to visit the Cathedral instead – if you can – and see the work of art for yourself.

Chester Cathedral is impressive, but add this temporary exhibit into the mix, and you find yourself with a fun family outing that's sure to please all ages!

What? Making Tracks 2

When? July 18 – September 3, 2022, 10.00 – 17.00 (layout doesn't operate Sundays)

Where? Chester Cathedral, St Werburgh Street, Chester, Cheshire, CH1 2DY

How much? £2.50pp

Contact T: 01244 500959

W: www.chestercathedral.com



Attendance was steady throughout the day, and unlike a crowded traditional exhibition, there was room to examine the model more closely – largely helped by its size! The 64ft x 12ft footprint is the same as the previous layout, though plans are already underway to expand further for a new venue in 2023 – "just watch this space", says Pete.



Factfile

Layout name: Making tracks - WCML
Scale/gauge: 16mm gauge, 1:76 scale, OO
Size: 64ft x 20ft (approx.)
Era/region: 2000s WCML
Location: Watford to Shugborough (with a N.Wales detour)
Layout type: Continuous loop

ON EXCLUSIVE

PETE WATERMAN – ‘MAKING TRACKS’ – WEST COAST MAIN LINE

Enjoying the sight of trains passing-by, Pete Waterman and his team created this layout in record time, for display at Chester's famous Cathedral.

Words & Photography: Andy York



Before we see any of the landmarks on the project, the layout is unmistakably of the West Coast Main Line with brick retaining walls and overhead line equipment framing the four-track formation.

For a year, few of us have had much of an inkling on when model railway exhibitions may start to come to life again, but one of the first, and most spectacular opportunities will be thanks to Pete Waterman OBE and his team of modellers. A major display will be open to visitors from July 17 to September 3, inside the vast space of Chester Cathedral.

During a long winter of lockdown, Pete and the team have certainly been busy creating not just one of the largest exhibits the hobby has seen, but one that features a lot of modelling skills and techniques infrequently seen on any layout, let alone one designed to be moved and re-erected.

Project origins

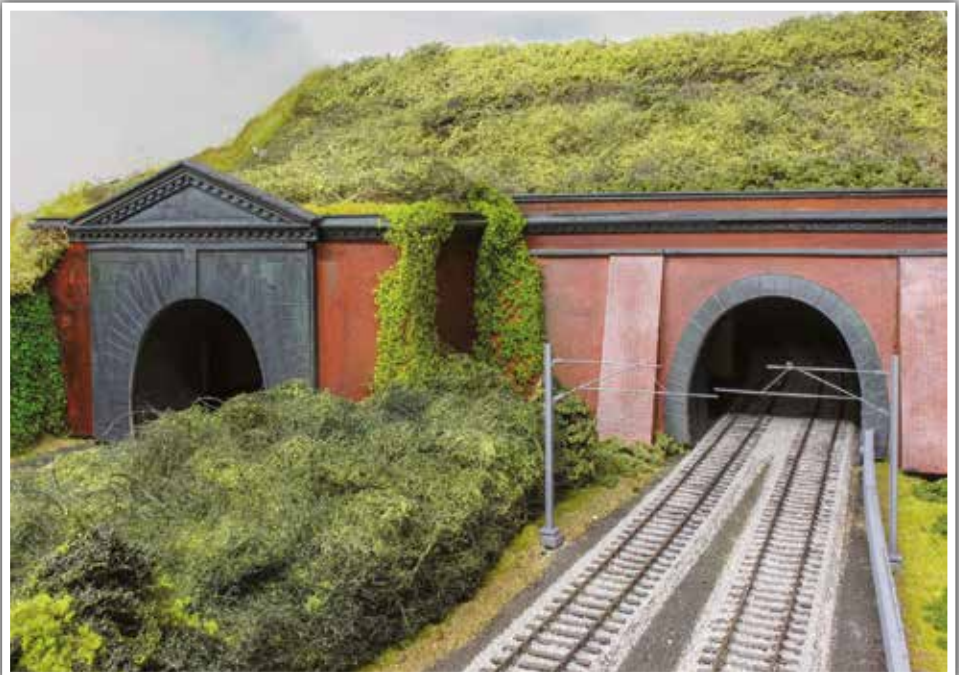
Chester Cathedral, sitting in the heart of the busy tourist city, is staging an exhibition, 'Making Tracks', as its 'Year of Journeys' theme looking at human journeys and adventures, plus commemorating Thomas Brassey, whose memorial sits within the cathedral precincts.

Chester Cathedral's Mark Baxter explains how Pete came to be involved with the project, "Pete Waterman OBE is the Chairman of the Thomas Brassey Society, and the enthusiasm of Pete and his 'Railnuts' team is outstanding, and a plan very quickly came to be, steered by Pete. The knowledge and expertise of Pete and Railnuts have been invaluable – the construction of the model has been inspiring, and their plans for how it will travel to the cathedral similarly innovative". The team at the cathedral is looking forward to the event, which starts on Friday, July 16, "It will stretch for about 90% of the Nave floor – which I think will certainly be a sight! I'm also interested to see the different reactions to the model, from model railway enthusiasts to families. I think everyone has a love for model railways, don't they?" The West Coast route is a journey Pete has made countless times and his observations have led to a great model.

Star attraction

At first sight, the scene is unmistakably one of the West Coast Main Line with distinctive architecture that pleases those who know the detail, while painting a broad-brush piece of art for those who are travellers on the route. There's some geographical modellers' licence involved with the arrangement of key features, but broadly, the scene is from north (left) to south (right).

The first feature on the far right of the



The classical form of the original (left) London & Birmingham Railway tunnel portal north of Watford undoubtedly made statements to investors and travellers about the grandeur of the route. To the right, the later tunnel is more austere, once cost had trumped style as traffic on the line increased.

scene is the north portal of Watford Tunnel, echoing the erstwhile Doric Arch at Euston Station some 20 miles south of the scene. The original 1837 double-track arch of the London & Birmingham Railway stands at the rear of the boards with the slow lines at the front of the scene running through the later London & North Western Railway tunnels.

Although the main parts of the layout are of a four-track formation, the trackwork is far from being four absolutely parallel lines, with the track gently and subtly curving from one grouping to another. The

transition of trackwork from the Watford portals to the single line portals of the slow lines through Northchurch Tunnels towards the centre of the layout is a striking example of subtlety, proving this is a model of a railway, rather than a mammoth train set.

The expanse of the scene is centrally divided by the short Northchurch Tunnel section – just north of Berkhamsted – and is topped by the only two buildings on the layout, a pair of inter-war detached cottages.

The tunnels have been transposed on the model to keep the pairing of the fast lines through a twin-track bore.



The low-down view of the southern portals of Northchurch Tunnel ahead of the wiring shows the proliferation of hardware around the modern railway, with electrical trunking on both sides of the line and raised equipment boxes associated with the communications mast.



The short (300m) tunnels at Northchurch are topped by the only buildings on the layout, two detached inter-war houses with charming gardens.

Emerging from the left-hand side of Northchurch Tunnel, the tracks come closer together within a deep cutting with wide and tall but narrow bridges akin to the section north of Roade in Northamptonshire. Here, it is evident how much modelling thought and time has been put into the catenary system with single posts and double and quadruple track portals. The portals were designed for laser-cutting, cut and constructed by the team to give a pragmatic and effective approach to replicating complex structures. From normal viewing distances, they certainly hit the mark. Clever planning dictated the position and separation of the portals, once again based on the varying lengths of Peco overhead wire sections of 200, 260, 340, 380, or 500 millimetre sizes, giving choices to work around the major structures.

As we move left (or northwards), the pairs of tracks start to diverge with the fast lines heading into a portrayal of the eastern end of Shugborough Tunnel to the rear of the layout, where the hidden tracks continue to the end of the layout before arcing round



Shugborough Tunnel's eastern portal was intended to portray an Egyptian entrance arch.

DIGITAL EDITION

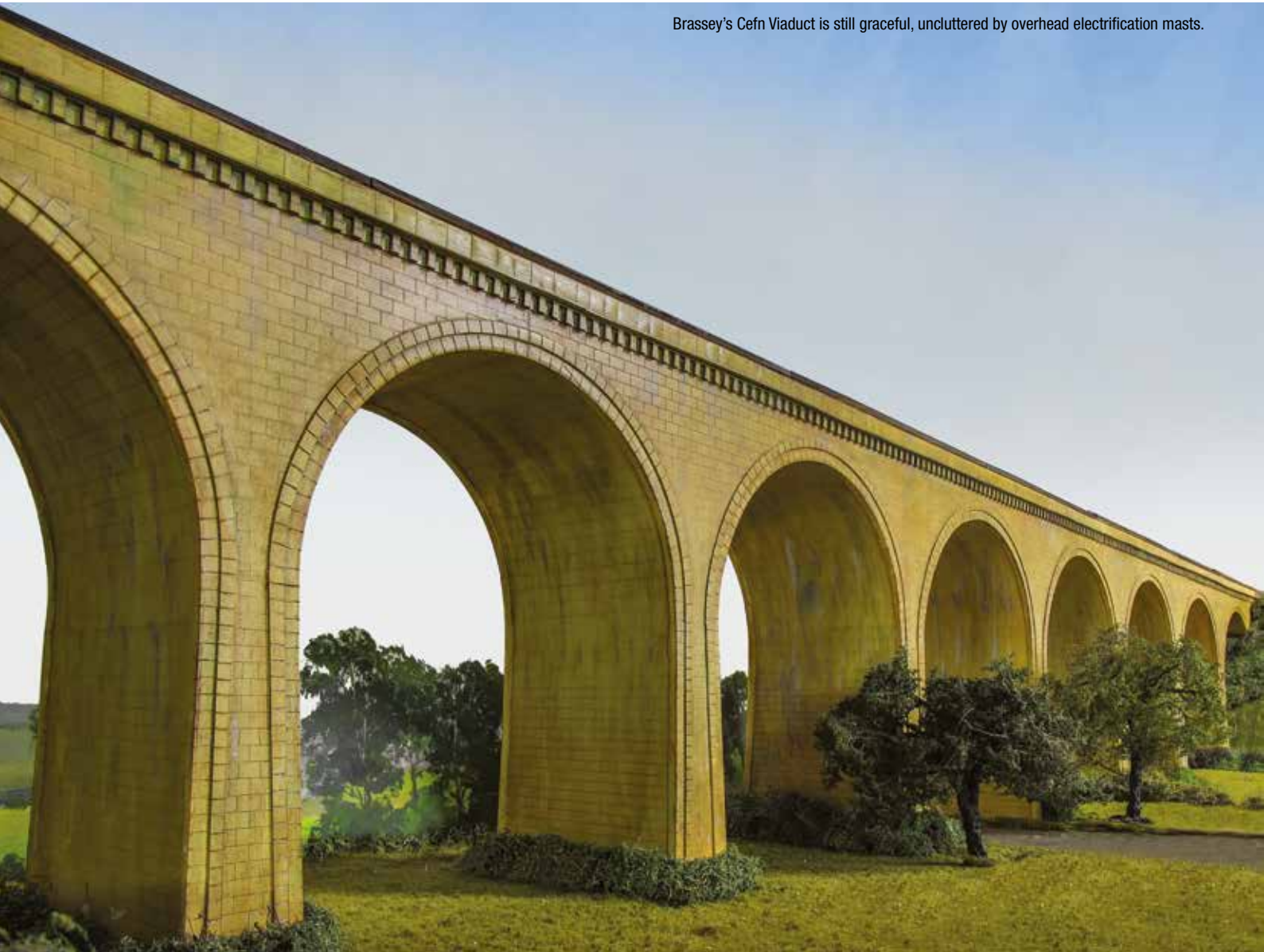
to the storage sidings. Before this, there is a ladder junction across all four tracks allowing for the separation of the two routes onwards, meaning that Down trains on the slow lines may cross onto the rear tracks.

The diverging route in the foreground includes interesting terminals of the overhead wires beyond the ladder junction, before the non-electrified route over the impressive Cefn Viaduct scene. Cefn Viaduct spans the River Dee near Chirk on the Shrewsbury and Chester Railway taking the line 100ft above the valley floor and was completed by Thomas Brassey, a resident of Chester, in 1846. The viaduct took Pete many hours to create, despite being able to laser-cut significant portions of the stone facings. The valley gives an opportunity for some scenic modelling including individual trees where a little more time is taken with seafoam sprigs on wire trunks and branches covered with scatter, plus the scenic backscene looking up the valley of the River Dee after some artistry from Mike Raithby.

Beyond the viaduct, the line exits stage left through the western portal of Shugborough



Brassey's Cefn Viaduct is still graceful, uncluttered by overhead electrification masts.



Thomas Brassey (1805-1870)

The link between the layout and Chester Cathedral lies with Thomas Brassey. A quite remarkable man and probably less recognisable or well-known than he ought to be considering his achievements, rising to become one of the richest self-made Victorians.

Many know the names and faces of the Stephensons and Brunel as engineers, but Brassey was a major contractor, carrying out work through his agents, supervisors and labourers. The son of a farmer from a village just south of Chester, Brassey was home-schooled until attending Kings School in Chester. At the age of 16, he became an apprentice to a surveyor and through road-building and construction in Cheshire came to meet Thomas Telford and George Stephenson and worked up to become a partner in the surveying firm. After a few unsuccessful tenders, his first contract came with a 10-mile section of the Grand Junction Railway in Staffordshire including the seven-arch Penkridge viaduct.

At the age of 32, his first contract was complete and his work accelerated. Within 15 years, he had built 2,000 miles of railway, a third of the mileage in Britain. Across the Channel, Brassey heavily influenced early French railways, building three-quarters of them during his life. His ability to deliver works on time and on budget brought international recognition with works across the continents of Europe, Asia, Australasia, North and South America until, by the time of his death, in 1870, he had built 5% of the railways of the world. Failures were few and any losses were covered from his own funds, he paid his staff well and visited sites in person. There were no fancy offices despite employing an average of 80,000 workers a year, and he earned the respect of many. His work went beyond the railways to build drainage systems and docks around the world and he was a major backer of Brunel's Great Eastern ship. A family man, his three sons went on to serve the military and parliament. At the time of his death, he was worth over £5,200,000 – an extraordinary amount for the time, and one of the century's wealthiest self-made men was commemorated with a bust in St. Erasmus Chapel in Chester Cathedral. Surely, he merits a greater place in history?

Penkridge viaduct, part of Thomas Brassey's first contract, spans the wide, shallow valley of the River Penk. A southbound Virgin Voyage service heads toward Penkridge station in June 2012..



Tunnel with its substantial Romanesque castellated stonework and returns to the rear storage yards.

A 70-foot-long layout is no mean feat at the best of times, but to come up with solutions that mean it can be effectively supported and transported are another thing and, quite literally, the foundation of the scene. A series of study table frames, built to controlled dimensions with adjustable feet, were constructed to support each of the board frames bridging over spacing between each table. Each table frame has bolt fixings, allowing them to be dismantled, transported and reconstructed on site. The largest section, which portrays Cefn Viaduct

is 12ft in length, plus at least 3ft in height – an impressive model in its own right. This means that with the other boards, a large-capacity van is required for the big move!

The baseboard sections are based on multiples of the Peco catenary system contact wire lengths so that the board joint coincides with catenary portals or masts and the associated wiring to allow complete modules to remain intact, minimising the set-up work in this regard on arrival at the cathedral. As with the real railway, much of the overhead wiring is held under tension, both cosmetically and in practice with terminal wiring taken down through the board and secured with a bolt

where necessary. Away from the scenic sections of the model, there is no overhead equipment, but approach framing will be fitted inside each tunnel to guide locomotive pantographs into position below the wires on the visible scenes.

As an extremely frequent traveller along the route, Pete observed that little in the way of individual trees could be discerned as the miles fly past, and has adopted the impressionistic approach to lineside vegetation. We see miles and acres of varied greens and rarely do the eyes get a chance to rest on an individual tree. Therefore, much of the vegetation on and above the cutting sides is formed from rubberized horse hair

A Down train on the mainline to Glasgow will not encounter another tunnel after Shugborough until entering Glasgow, the price being paid by more circuitous routes and stiffer climbs than the East Coast route.



ON EXCLUSIVE

covered with clump foliage and scatter materials to give the variety and consistency of tones along the scale mile of scenery.

Many of us would recoil at the thought of ballasting near on four scale miles of track and pointwork, but Pete breaks this down into logical and manageable steps. Beneath the track, 4mm thick cork strips give an appropriate bed for the modern railway, glued with PVA and weighted down until set. The track is laid onto the cork, and a router is used to form the bevelled ballast shoulder in the cork strips. The router is governed by a guide between the rails, placing the router bits at the appropriate offset from the sleeper ends. It seems the

team was able to quickly create the shoulder using this method. After painting the track, Woodland Scenics ballast was sprinkled with fingers and thumbs in several hours because there's little pointwork, which can take longer to work around. Pete tells us it wasn't too time-consuming.

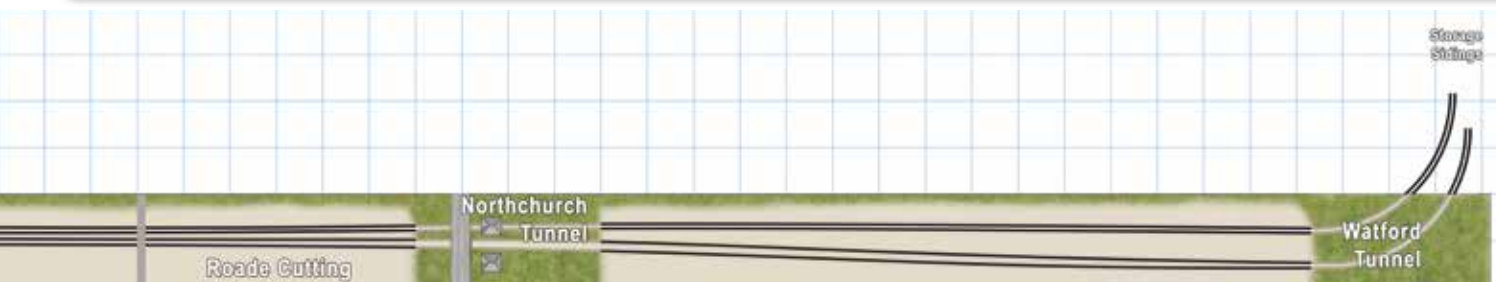
The layout is well worth a trip to see – the cathedral is located in the heart of the historic city and is just under a mile walk from the station. Making Tracks will be open Monday to Saturday from 10am - 5pm (last entry at 4.15pm) and runs from Friday, July 16 until Friday, September 3, 2021. The event isn't open on Sundays to allow for normal worship and essential maintenance

The Railnuts team

Dave Douglas – Woodwork and CAD design
Kevin Treeby – Trackwork and electrics
Aaron Moran – OHLE and Points
Tony Crowther – Scenics
Paul Morgan – Scenics
Ralph Robertson – Houses and bridges
Andy Gyde – Scenics and CAD work
Mike Raithby – Painting
Pete Waterman – 'Jack' of all trades!

on the model to take place. The event is free to visit, but donations to the cathedral are welcomed. Don't miss our video tour of this layout in this month's BRM TV, plus bonus footage available in the digital edition.

During this testing phase of the layout's creation before the big move to Chester Cathedral there seems to be some interesting workings!



WORLD OF RAILWAYS

TV

DIGITAL EDITION EXCLUSIVE

Pete Waterman's Making Tracks I

We interview Pete Waterman about his stunning model railway layout, Making Tracks.

WATCH THE VIDEOS HERE

(You must have a wifi connection to stream video content)



PLAY



HADLEY, STONYBROOK AND SWALE RAILWAY

Modernising this layout for a smoother, more reliable exhibition experience, is an ongoing process for its new owner, Arline Wilson, with enjoyable results.

Words: Arline Wilson **Photography:** Andy York



A porter opens a gate for cyclists to cross the line while *Snowdon* (4-6-2 converted from a Tri-ang 'Princess') takes on water and coal.

This is a tale of how a new railway modeller became interested in model railways and then developed some of the skills required to build a layout. I am a retired female, with three grown-up children, a husband who has always been interested in railway modelling, and a cat

who purrs loudly as she enjoys watching the trains go by.

I first became interested in model railways about eight years ago, when I was needed to help operate my husband's layout, 'Blakecaster', at exhibitions. At one of these exhibitions, there was a layout for sale...

Originally built by Mr. Ernest Machin in 1991, I acquired the layout in 2016, when he offered it for sale because he was finding it difficult to exhibit due to his age.

When I bought the layout, it consisted of five boards, each measuring 3ft 6in x 15in, and was operated by a Hornby Dublo



FACTFILE

Layout name: Hadley, Stonybrook and Swale

Scale/Gauge: 1:43.5 scale / 16.5mm gauge / O-16.5

Size: 20ft x 6ft (6m x 2m)

Era/Region: Late 1940s, region of Shropshire and Staffordshire Moorlands

Layout type: End-to-end

controller attached to the boards by a series of crocodile clips. There was no fiddle yard. The points and the signals were controlled by rod-in-tube.

My first task was to rewire the layout. Each of the five boards had its own power provided by crocodile clips, so

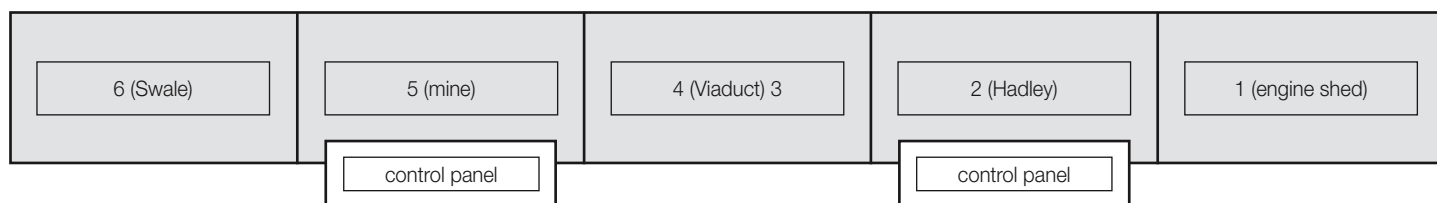
it was sensible to treat each board as a separate section, but I also wanted to have independently-powered sections within each board to make operating easier, with isolated sections at each end to be able to park locomotives.

I planned to have two controllers because

of the length of the layout, one at each end. I also wanted to be able to control the whole of the track from either end because it's easier to control the stopping position of a train for uncoupling that is travelling towards you than one that is travelling away. This meant I had to work out the wiring and



The Beyer-Garratt (based on the Tasmanian Railways Beyer Peacock Garratt) takes passengers over the stony brook to Hadley. Beside the stream, a fisherman, watched by a young boy sitting on the rocks, ignores the 'no fishing' notice.



switches for track power from either end. I realised that it would be useful to see, at a glance, which end was in control at any time, so I added bicolour LEDs. I then opted to have a separate cable to each board because that would make testing the boards simpler, but what could I do with the viaduct board in the middle? The problem was solved by cutting through the track in the centre of the viaduct so that each half of the centre board had its own cable. Finally, I numbered the boards one to six from the engine shed.

Learning new skills

I had recently joined the Stafford Railway Circle and we were just starting to build a new OO gauge layout, so I had already

learnt how to solder wires to tag strips. This skill became useful as I spent the next three months doing the wiring for HSSR, with occasional help from my husband, Peter, when something didn't quite work as expected. My Christmas presents that year included a transformer, two Gaugemaster hand controllers, and a soldering iron! Shortly after the control panels were completed, the layout was invited to an exhibition in January 2017.

At this first exhibition, a lot of time was spent walking up and down the layout to change the points, so I realised how useful it would be to have point motors. I didn't have to cut away any of the baseboard because the points were currently controlled

by rod-in-tube, so all I needed to do was remove the rods. I left these partly fitted under the boards, but glued out of the way as a back-up plan. I used Peco solenoid motors and simply added the wires for the motors to the tag strips I had already fitted under the boards. The CDU for the layout was fitted under the centre viaduct board.

Route selection method was chosen where feasible, but this meant I had to add diodes where any button was controlling more than one point at the same time. For example, to get from Hadley platform to the head shunt. I hit a problem as I found one button would not change as many as three points at the same time. However, one of the experienced members of Stafford Railway Circle was able

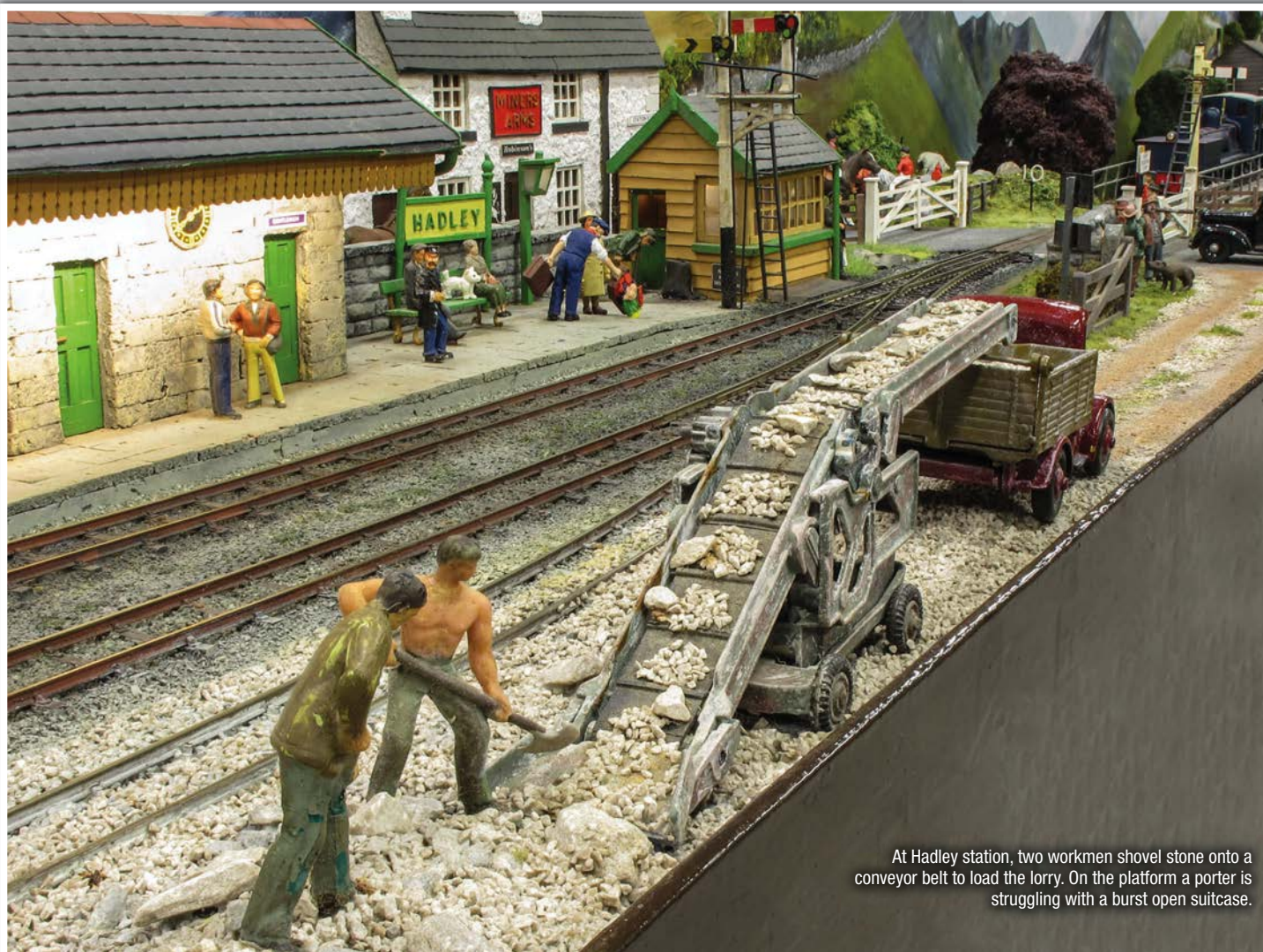
A workman is in trouble for mis-spelling 'Stonybrook'. His supervisor is holding the missing letter 'O'.



The fictitious layout of 'Hadley, Stonybrook, and Swale Railway' HSSR is set in the late 1940s. It provides a rail link between the Cleve Hills of Shropshire, which are visible in the distance, and the terminus of the Staffordshire Moorland Light Railway SMLR at Blakecaster, which has limited road connections to the outside world, its main connection being the narrow gauge railway laid along a narrow ridge across the infamous Patchway Bog.

The town of Swale, which takes its name from the low-lying moist ground that surrounds it, is close to the marshy edge of the Patchway Bog. The single track line that used to run on the ridge over the bog to Blakecaster has unfortunately partially sunk into the quagmire due to the recent heavy rains. The workers in Stonybrook Mine are currently working hard extracting stone and gravel to be transported by road and farm tracks from Hadley to help repair the submerged track to eventually allow that route to be reopened.

Swale is connected to Hadley by a viaduct over a stony brook. Occasionally, fishermen make their way down the steep slopes to the brook but rarely catch anything. The tea room at Hadley Station does a roaring trade with ramblers who are attracted to the area by the interesting walks over the nearby hills. Some stay at the Miners Arms, which provides very comfortable accommodation. The station master at Hadley is well known for his love of cats; you can see some of them near the station enjoying the company of the passengers on the platform, or hunting for mice underneath the tea room. The engine shed beyond the station area provides servicing and repairs for all the rolling stock in this area.



At Hadley station, two workmen shovel stone onto a conveyor belt to load the lorry. On the platform a porter is struggling with a burst open suitcase.

to explain that I needed to add extra wires to the return from the points as the thin wires in the cable could not carry enough current.

The next exhibition invitation was in June 2017, but a few days before the exhibition, one of the point blades broke, so I had to do a temporary repair by soldering the blade in a fixed position. This meant I could not use the siding, so it was a bit of a challenge for the operator at that end of the layout. Once the exhibition was over, I had to learn how to remove and replace a point without damaging the scenery, but it proved a lot simpler than I had expected. One of the club members explained how to cut the rails of the broken point and prise it out, carefully remove a chair at each end of the adjoining track, slide onto those rails new fishplates, place the new point in place, and then slide back the fishplates onto the rails on the new point.

By then, I was looking at other ways I could improve the layout. The original surface for the fields was just sawdust painted green so I decided to add static grass. I also had fun adding more figures.

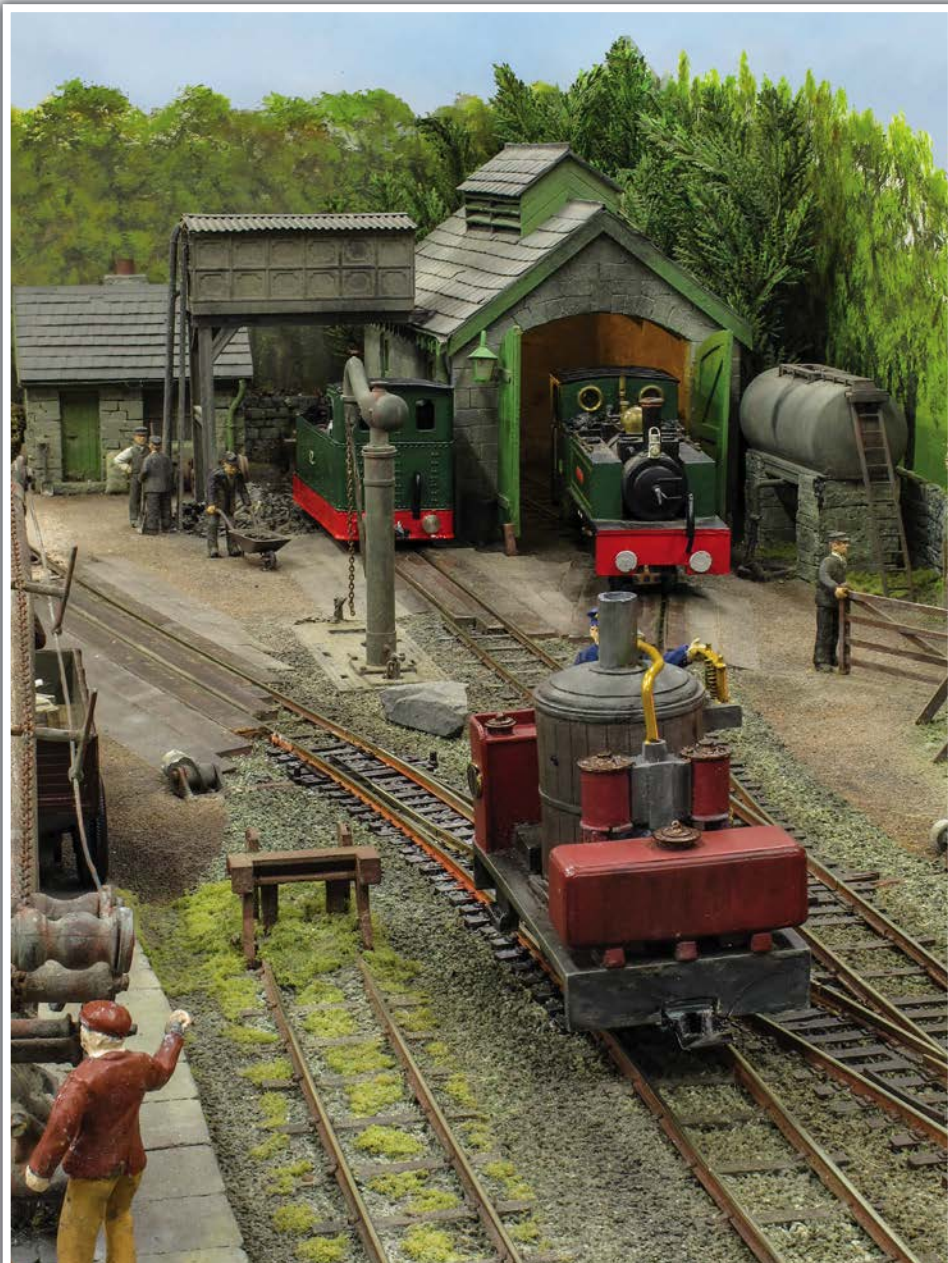
Operating the layout

I had found that the lack of a fiddle yard was a disadvantage at an exhibition. So, with my husband's help, we built two extra boards to provide a fiddle yard at right angles to the original layout. To drive a train onto the fiddle yard meant I had to cut a hole in the end board by Swale Station. This was achieved by drilling a series of holes in the hardboard to outline the shape and then, using a keyhole saw, join the holes together. The result was not very neat but is mostly out of sight under a bridge.

I decided that working out a sequence for operating would be useful as it gave me an opportunity to show off the rolling stock. It was fun and quite complicated working out a sequence that didn't end up having locomotives or rolling stock in the wrong place. By then, it was clear that a team of three people worked best as one person could look after the fiddle yard.

Shunting has been a challenge as the layout uses tension-lock couplings and getting the couplings the right height on the rolling stock so that they uncouple or stay coupled as required has been difficult.

I was becoming accustomed to preparing the layout for exhibitions, but I soon realised that running trains to check the whole layout before each show was not so easy, as it is 20ft long in total. On two occasions,



Etna (built from a Smallbrook Studios kit) is on its way to the head shunt. In the background, *Snowdon* waits in the engine shed while No. 12, a Peco tram locomotive, takes on water and coal.

I managed to set up the full layout in the house, but it meant blocking a doorway and I had to crawl underneath the layout to get out, so I decided I needed to do something about it.

The viaduct rarely needed testing because it is simply single line track, so I looked at what I could do to omit it. Unfortunately, I had put the CDU underneath the viaduct, so I could not test the points without that centre board. I decided to move the CDU to a box that could be located on the floor, and then I was able to connect the other four boards together to check everything was working without needing the centre viaduct board.

Narrow gauge stock

Narrow gauge railways commonly have smaller locomotives and rolling stock than their standard gauge counterparts, and sharper radius curves don't look out of place. O-16.5 is a useful scale/gauge to model in because the track gauge is the same as for OO gauge, but the scale is 7mm:1ft. This means that scenic items are O scale items, but for rolling stock, we can use a OO gauge chassis. Many locomotives use a chassis taken from a OO gauge locomotive and there are some specialist manufacturers of body kits for them.

Much of my rolling stock was bought with the layout. The locomotive in the engine



A row of houses overlook Swale station. A young boy sitting on his father's shoulders watches the trains.

Adding Lighting

I wanted to add lighting under the platform canopies and inside buildings. I used LED strips with built-in resistors for both platforms, and the engine shed. The signal box was more difficult as I didn't want to break it open, but a club member suggested using wire in tube from underneath the baseboard to fit an LED in the ceiling. Painting the tube the same colour as the inside of the signal box hid the tube perfectly, but drilling a hole from underneath to the correct position took two attempts, even with careful measuring, so I found a figure to glue on top of the hole that had landed outside the signal box instead of inside! The tea room already had a grain of wheat bulb fitted, so that was easy to add to the lighting circuit.



shed, *Snowdon*, was converted from a 4-6-2 Tri-ang 'Princess'. No. 12 in the water/coal is a Peco tram. Locomotive No. 8 arriving at Hadley Station is a Peco Hunslet tank.

The vertical boiler locomotive, seen in the head shunt and later pulling a goods train, was made from an *Etna* Smallbrook Studios kit, which is cast in urethane resin and fitted onto a Hornby 0-4-0 chassis. Unfortunately, the chimney was too tall to go under the bridge at Swale Station, so I had to saw off about half an inch!!

The scratch-built Garratt on the viaduct, based on the Tasmanian Railways 2ft gauge 0-4-0+0-4-0 Beyer Peacock Garratt, is powered at both ends by two Hornby 'Smokey Joes'. The coal bunker, water tank, chassis, and cab were constructed from Plastikard sheet. The chimney is part of a ballpoint pen, and the smoke box door is a button.

The passenger coaches were built by combining Peco coach kits. Behind the Garratt, the first coach was two pairs of coach sides modified to produce a balcony car, the second coach uses one and a

half coach sides, plus the loading doors from a Peco box van to provide a luggage compartment. Peco coach sides were also used when building the steam railcar.

The wagons are from a range of sources. Some are Chivers Finelines (now Five79) kits; others are from Peco kits and some are scratch-built on a OO gauge wagon chassis.

Future plans

The layout has now been to several exhibitions over the last few years, mostly in the Midlands area so far. It is supported on trestles and has been criticised a few times for being too low, but I like children to be able to see the trains fairly easily. I give any interested children a quiz sheet with 10 things to look for on the layout. Finding the fox is often difficult!

Three more points have broken – always during an exhibition, so running repairs had to be improvised. This is probably due to the points being quite old and the Peco solenoid point motors are quite violent when changing them.

Another wear-and-tear problem is the

soldering on the Tag strips. As I was fairly new to soldering, the inside of the control panels was not very neat, so fault-finding, when a wire has become detached, was difficult. Therefore, I decided to rewire both control panels, which are now much neater than my first attempt. The next exhibition booking I have is for Telford in October 2022, and there are a few shows in the diary for 2023, so I hope to continue to exhibit for several years.

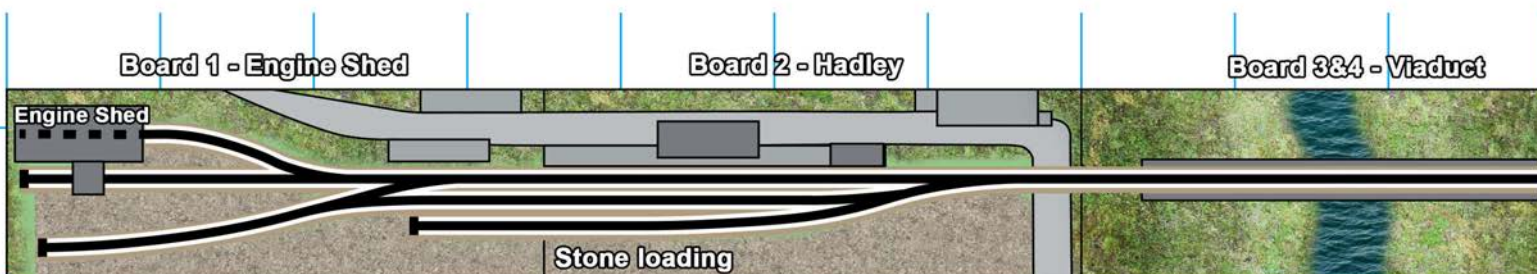
My current work in progress is another layout in the same scale, set on the Welsh/Shropshire border, but I am aiming to have a fiddle yard in the middle with a station at each end, plus a canal at one end and a slate mine at the other end.

The annual Stafford Railway Circle exhibition has moved from early February to a new permanent date, the last weekend in September. The 2022 show had originally been intended to have a narrow gauge theme, but the Covid cancellations have delayed these plans. Perhaps our 2024 show will feature one of my narrow gauge layouts?

Watch this space!



Etna at Swale station is bringing in goods together with men to work in the mine.





Can you find the fox hiding from the hunters who wait at Hadley level crossing?



Swale High Street and a postman is about to enter the cafe for a 'cuppa' before emptying the letter box.

Board 5 - Mine

Board 6 - Swale

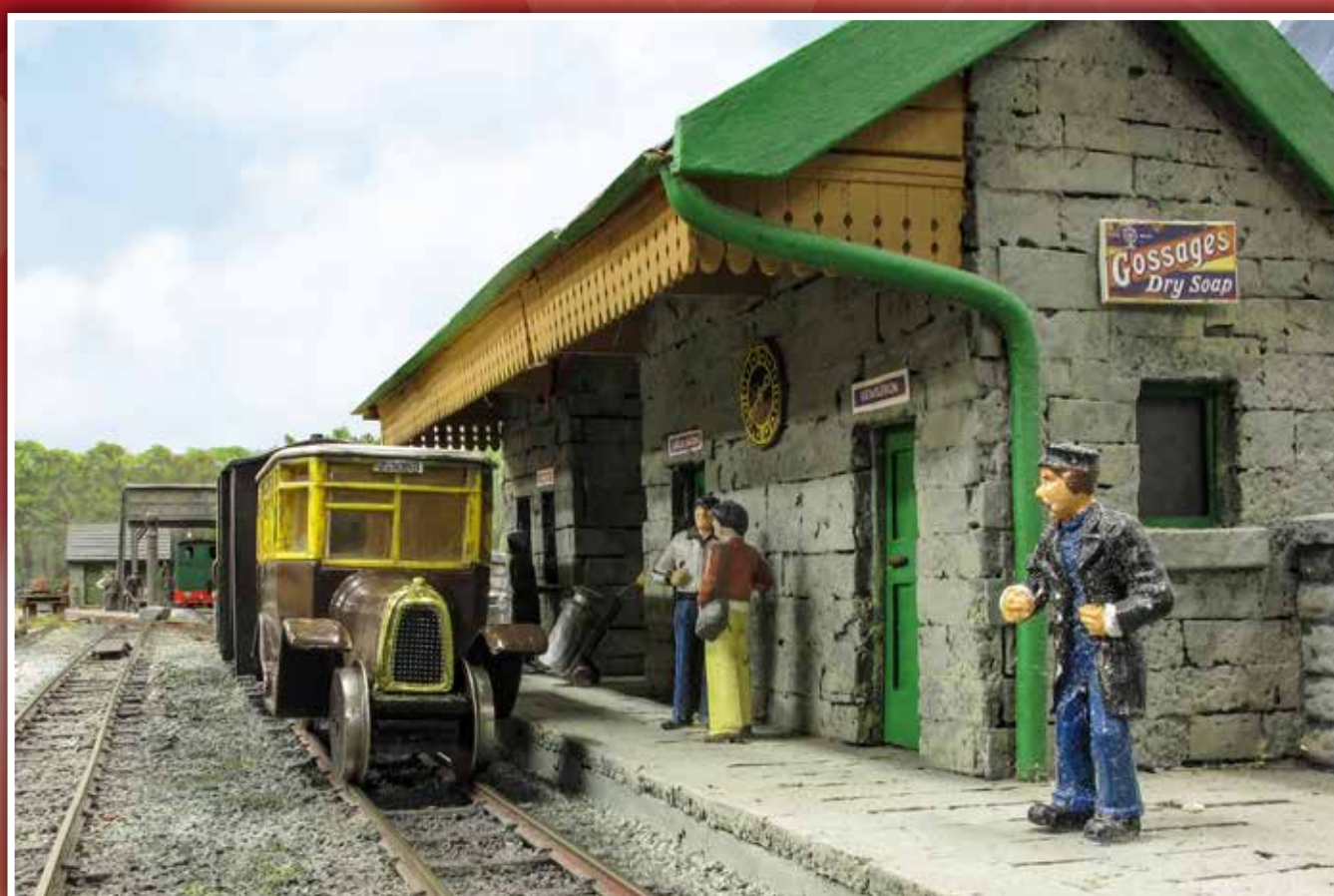
Station

Fiddleyard



DIGITAL EDITION EXCLUSIVE

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HOW TO... BUILD A CINEMA

Distinctive architecture of yesteryear, seldom seen on a layouts demands closer attention. Michael Russell finds this Art Deco-style kit an entertaining, and satisfying build.

Words & photography: Michael Russell



This kit builds into a cinema for the corner of a high street in a town or city. It features classic Art Deco styling that can still be seen today, although this particular prototype has, unfortunately, been demolished to make way for a supermarket. It would look great on any layout from the mid-1930s onwards, with just a few slight changes for the modern era. It would be easy to personalise it, too; for example, just by changing the signage.

Parts are printed onto high-quality mountboard, which means they are about

1.5mm thick. This makes for a strong building. This is not the easiest card kit I have built. However, if you have built a card kit before, you will be ready for one of these. If you have printing facilities, there are excellent and generous free kits available on the website for you to try out first.

I chose to use a PVA-based glue as I don't like glue fumes, but this means it takes longer to build the model. To minimise the possibility of spoilage, to maximise strength, and to minimise drying time, chose one of the tacky variety and not the thin kind

supplied for ordinary craft use. There is one point in the build where an impact adhesive is a better option and this is mentioned.

This article is not a replacement for the kit instructions, which should be your primary source of information, rather they explain ways to make the build easier and workarounds for issues that I experienced.

The challenge of this kit is the curved section of the front and errors in the build are likely to manifest themselves in the fitting of the last main item – the upper roof – this is an area that requires particular attention.



Start by looking through parts to get an overview of where you are heading. Read instructions and look at all the diagrams provided. Pay special attention to areas that look complicated.



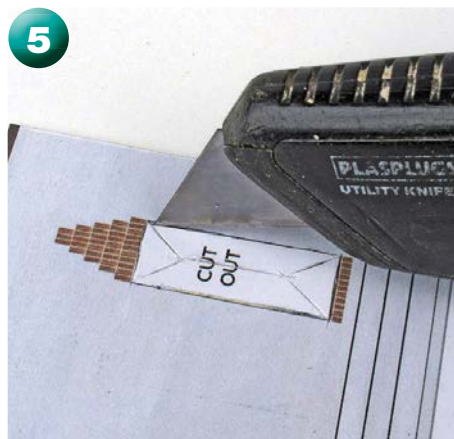
Cut the posters off the bottom of the instructions and store them so that they don't get damaged. Take digital photographs of all the card sheets so that when the cutting starts, you still have all the initial data available in one place for reference.



As you cut parts out, you will find that you have increasingly smaller pieces left still to use. To prevent these getting mixed up with offcuts, use a container to store anything that you want to keep until it is needed later in the build.



As parts are needed, I like to cut them out roughly using scissors, but don't do this on long runs if it means bending the part. I then cut them out neatly using a knife and rule. I find that this allows me to relax and concentrate on the cuts without the fear of damaging surrounding parts.



When cutting out the windows, cut away from the corners, turning the card around each time as necessary. It does help if you are ambidextrous!



As each part is cut out, colour the cut edges with a soft pencil, such as a 3B. This is a small task that makes a huge difference. You could also use appropriately coloured pencils, pens, or paint, but I didn't find this necessary on most parts.



Colour the edges from the reverse so that, if you should make a slip, no mark will be made on the printed surface. If you mark the surface, use a quality kneadable eraser to remove it.



This kit didn't need any of the slow glue and wrap procedure of many card kits, but it is useful to use a seam roller to ensure that parts are stuck down evenly. Make sure that the parts are located accurately first and watch that you don't move them during rolling.



When you come to fit recessed windows and doors, apply glue, then gently lower the surface so that you can get the part central to the hole. Only press firmly when you are happy with the placement.

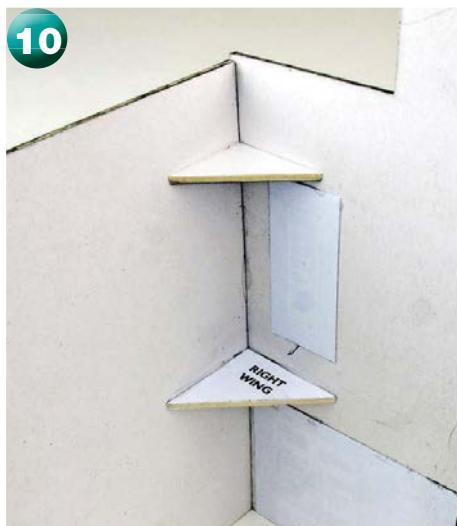
Working with card

When cutting card there are a few points to bear in mind. First, you need to make several light passes with the knife for each cut to reduce the risk of damaging yourself or the kit. At the same time, you need to press down firmly on the rule. This might prove difficult to the beginner, as the natural inclination is to press down with equal force on both arms. Until you get used to it you will need to concentrate when making the cuts.

Second, card and paper blunt cutting edges fast. You can tell when your blade is getting blunt because it will start to tear. Have spare blades available so that you can swap regularly. You can also use a small desk stone to sharpen the blade every few cuts. Use a DIY knife rather than a craft one for heavy-duty card cutting.



TECHNIQUES



You can make corner braces from the spare card left over as part of the runner material and I prefer two per corner for strength. You can even use the labelled portions of the card to identify parts of the building.



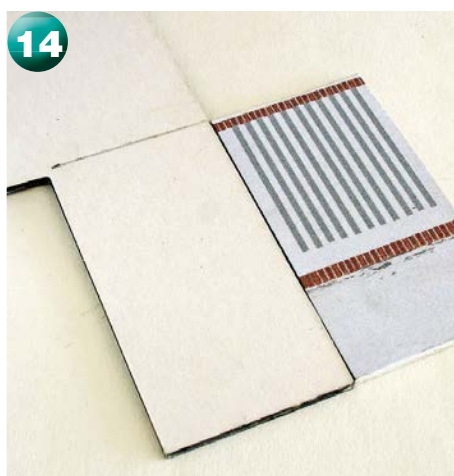
Some parts are unidentifiable when removed from the main card. You can write on the reverse of the part to work around this issue. I still wouldn't cut all parts out to start though, but just as needed when the steps come around.



When fitting the tower sides and backs, I used a number of brackets to increase strength and to get things square. Cut additional brackets from offcuts of cardboard from the kit, but ensure they are square before using them.



When gluing parts together, you can use modeller's masking tape to gently hold them in place until the glue has set. Be careful when removing it not to tear the surface and don't get glue on the tape.



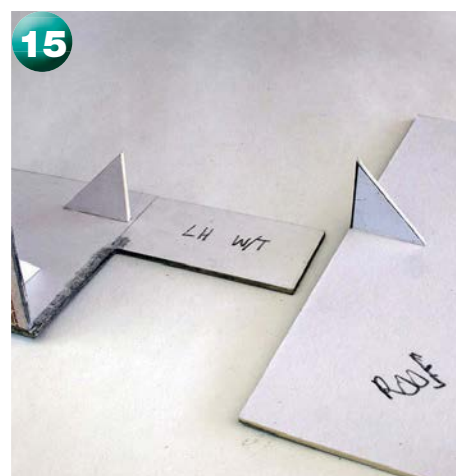
When working out the depth at which the main roof should be fitted, use the tower sides as a guide. You will find that the roof sits just below the main sides, and almost flush.



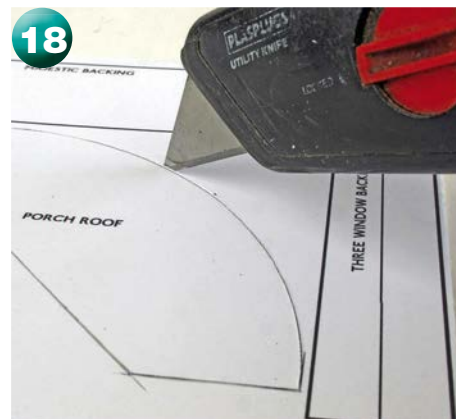
This is what the kit looks like after stage 6 of the instructions. Ensure that you build the model on a flat surface to keep it true – I used a mirror. Inaccuracies will show up later in the build, possibly leading to frustration.



The fire escape doors are a nice feature to add if the back of the cinema will be seen. Mark this out using a pencil and employ the bricks as a guide. Use the 3B pencil to widen the door frame in case your cutting is inaccurate.



When fitting the main roof, I used one bracket on each side, so four in all for strength and to get a level fitting. Glue the brackets in place and let them set before fitting the roof in place.



When cutting curved surfaces, use the very tip of the blade and work slowly using very gentle pressure for the first cut. Make further cuts again using gentle pressure until you are through. It is important to get these curved surfaces accurate so take your time.

QUICK TIPS

Cutting surfaces

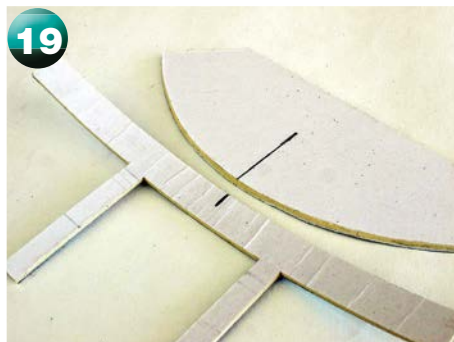
You can use any sacrificial, flat surface to cut out the parts on, however, I would recommend one of the 'self heal' variety. These are made of a soft substance that will not deflect the knife blade due to previous cut marks.

Preventing damage

The printing on these buildings isn't waterproof and can easily be damaged. Do not drink or have water around your work area. If you wish to make them splash-proof once built, then use a matt varnish or a fixative spray such as that used by artists when working with pastels.

Health and safety

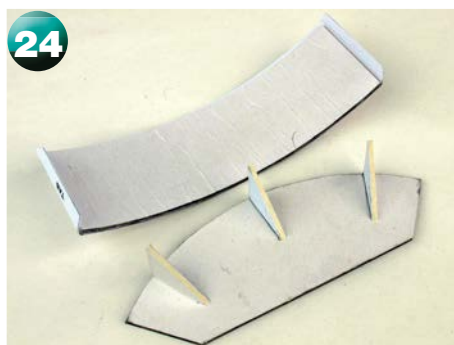
The 3B pencil employed in the build is quite soft and this will get on your hands. This could spoil the look of the building and the lead it contains is a health hazard, so wash your hands regularly and always when you finish a modelling session. Open a window for ventilation if you use contact adhesive.



When fitting the porch, and similar items together, you need to centre one part to another. To do this, measure the length of the part, half it, and then draw a centre line. Align the centre marks when fitting the parts together.



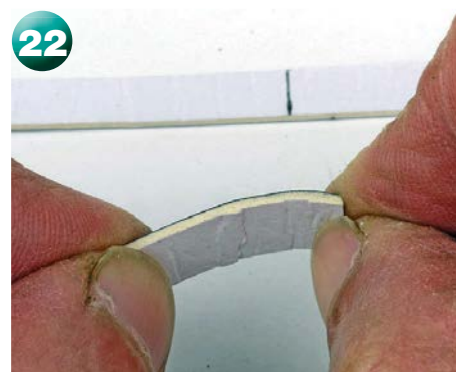
When you are about to start step 9, shade the outer edge of the porch with pencil as the upper stories are recessed as they get higher, like a cake. It is a good idea to shade joints beforehand as it is much easier than to do so retrospectively.



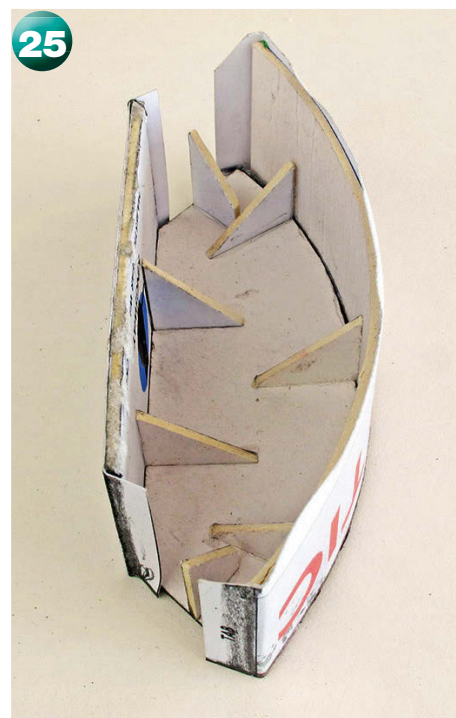
For extra strength, I used three brackets on the majestic panel joint to the upper roof. Make sure that the roof is stuck inside the panel to form the correct curve and that it is flush with the top. Stick in three stages starting with the centre and then each end in turn.



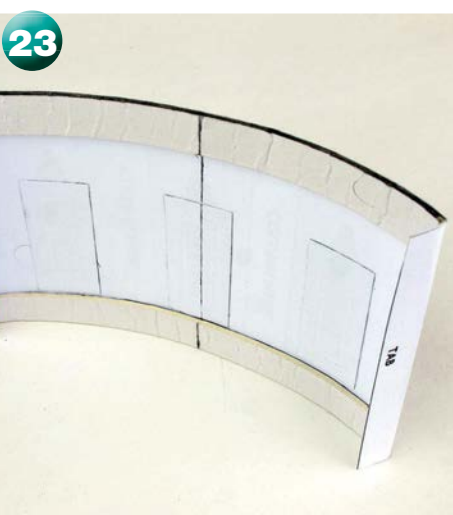
This is the position after step 7 of the instructions. Since the porch alignment will affect the storeys that are built above it, it's important to fit this accurately. Ensure that the porch roof sides butt up against the tower wall sides.



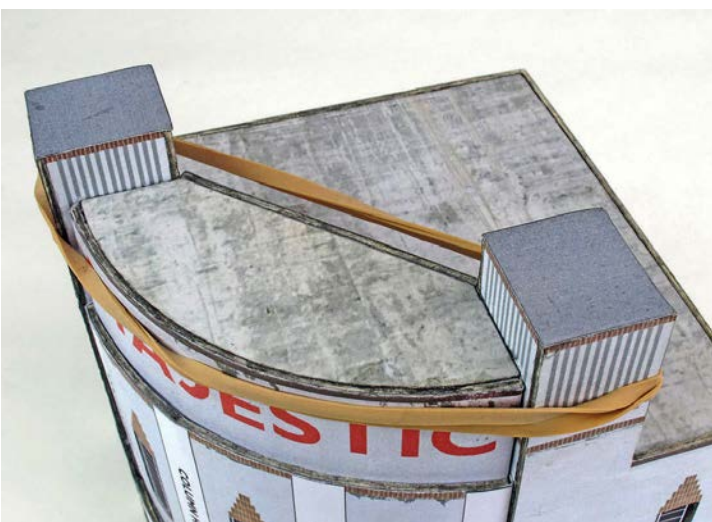
When preparing curved parts, such as the backing strips, ensure that you bend the card along the whole length, right to the ends, as it must form an even curve.



I deviated from the instructions and used brackets to assemble the majestic panel and the upper roof to form a sub-assembly. This gives priority to the roof alignment and makes allowance for small build errors.



Ensure that you glue the backing strips at the very top and bottom of the three panel window and that they are centred correctly. The majestic panel fits behind this panel and the depth of card is needed for accurate alignment.



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When fitting the majestic sub-assembly in place, ensure that it meets with the tower sides. I used rubber bands to keep parts together tightly.

TECHNIQUES



When fitting the columns, rubber bands are useful to hold everything in place should you use PVA glue. Ensure that you fit the columns the correct way up as indicated on the card surround.



Glue the majestic letters in place using the pre-printed letters as a guide. It is worth fitting these as it greatly improves the look of the building. A plain panel part is also provided if you wish to use your own lettering.



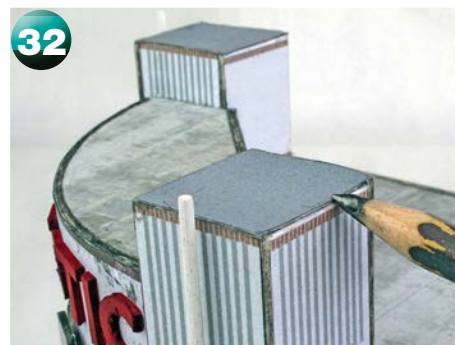
Cut out the canopy carefully to get an even curve because this will make for a good fit and a stronger bond. Glue the canopy end pieces in place first before adding the edging. Fit the canopy to the building using a contact adhesive.



Paint the cocktail sticks with white acrylic. When dry, use a razor saw to cut off the pointed ends, touch up the damage with more paint and then glue in place, being careful to align them vertically with the building stripes.



Cut the letters out using a fresh knife blade and colour in the sides using a red felt tip pen. I couldn't find the correct shade of red to match the front, so used the new colour all over, which looks better than a having mix. You could use acrylic paint instead.



Go over the building and look for white edges that are still showing. Use a 3B pencil to cover them. It is worth doing this as nothing screams out 'model' on a card or paper building than white edges showing.

Conclusion

This isn't the easiest kit in the range to build because it has curved sides. If I did the build again, I'd graduate some of the pencil shading on the edge of buildings. For example, for the light-coloured building sides that meet the brickwork, I would use a lighter tone, by using the pencil with less pressure and extend the three stripes around the side using a black biro.

Overall, a good-quality kit and a great feature for any layout,



CMM Majestic Cinema kit
www.kingswaymodels.co.uk

Humbrol (34) white acrylic paint
www.uk.humbrol.com

Rocket Card glue
www.deluxematerials.co.uk



Steel rule
DIY knife with spare blades
Mirror
Highly recommended/useful
Desk stone
'Self heal' cutting mat
Engineer's square
3B pencil
Seam roller
Masking tape
Digital camera
Small storage container
Scissors
Red felt-tipped pen or red acrylic paint
Kneadable eraser
Razor saw

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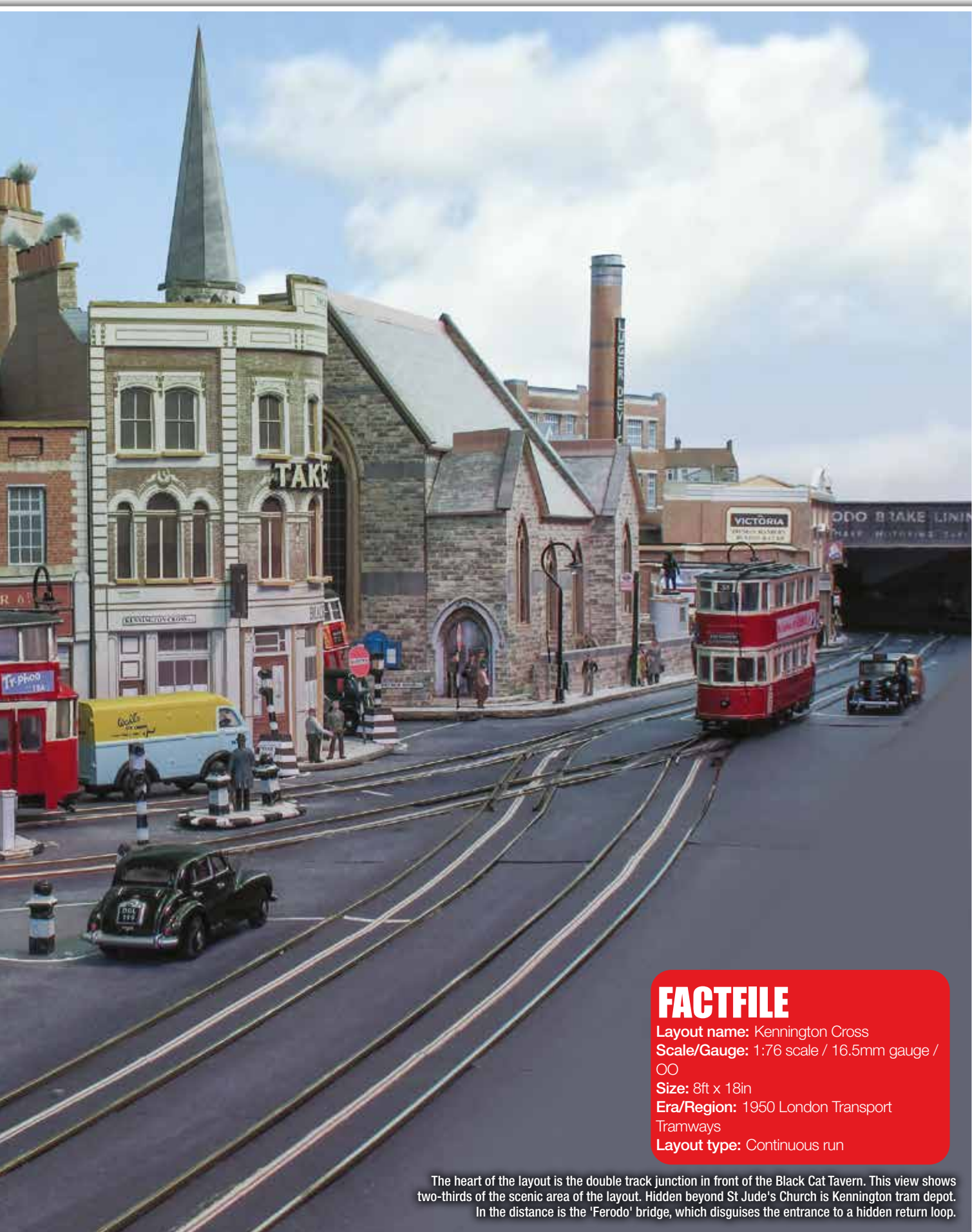
KENNINGTON CROSS

Recreating London's lost tramways in a believable scene is made easier when you own a range of card kits. John Howe explains his passion for the Capital's former electrified transport.

Words: John Howe **Photography:** Phil Parker



Kennington Cross Underground station will not be found on any map, but the building is typical of those designed by Charles Holden, that were often used on the southern section of the Northern Line. Postwar road traffic grew quickly, and portable Police-controlled signals were seen at many major junctions. A UCC Feltham is followed by an E1 class car.



FACTFILE

Layout name: Kennington Cross

Scale/Gauge: 1:76 scale / 16.5mm gauge / OO

Size: 8ft x 18in

Era/Region: 1950 London Transport

Tramways

Layout type: Continuous run

The heart of the layout is the double track junction in front of the Black Cat Tavern. This view shows two-thirds of the scenic area of the layout. Hidden beyond St Jude's Church is Kennington tram depot. In the distance is the 'Ferodo' bridge, which disguises the entrance to a hidden return loop.

It was especially important to me that the street scene should clearly resemble London in 1950, with its characteristic buildings. However, the setting is entirely fictional despite the name. The real Kennington was a major hub for various tram routes.

The layout is housed in the same small bedroom in which I built the experimental one. While that was sited around the walls, 'Kennington Cross' is in 'cabinet style'. This makes the inclusion of lighting easier to arrange and provides protection while travelling, not least from Peak District weather. For practicality, the layout controls are built into the baseboard at one end. The layout stands on trestles when exhibited.

South of the river

Hours spent leafing through period photographs of London trams revealed much interest, especially when compared with the modern scene. Sometimes, much of the original setting has since been obliterated, but often the buildings still stand. Close examination reveals many important period details that fix the scene into a particular point in history. In such surroundings, advertising signs and posters are especially noticeable, as is street furniture. The lamp posts, traffic signs and signals, and 'Keep Left' bollards were items that clearly set the period.

A once common item was the subterranean Public Conveniences, often accessed by steps descending from an island in the middle of the road. An example is placed opposite the entrance to the Underground station. There is a police box with an adjacent wartime air raid siren, nearby. There is also a single tree and some foliage in St. Jude's churchyard, but the layout is unapologetically urban. A bombsite would have been a nice feature to have, but I found it very hard to resist adding yet another building to any empty space. The road level is built up around the track with layers of mounting card, and the surface is represented by a dark grey card. Pushing this down on the rails gives an impression to aid cutting. I represent the centre conduit rails and slot with printed adhesive paper. This narrow strip can be stuck to the surface and curved as required to follow the track.

I especially enjoy doing all that I can to create a scene authentic to 1950s period London. In recent years, I have collected the set of the Middleton Press *Historic Tramway Albums*, which provide photographs covering the full extent of the London system. Close examination of the images



My primary aim was to represent a typical London street scene. I have previously scratch-built (non-functional) lamp standards myself from wire and oddments. However, I came across the range of lighting from Layouts4u some years ago. Some of the designs in the range are a bit toy-like, but there are examples that appear reasonably accurate – the fact that they worked was a bonus. The prices are very tempting too – half a dozen working lamps for less than £10. They are supplied with a resistor so can be wired into a simple 12V DC supply, or by removing the resistor to work on 3V. The layout (day) lighting on the layout is by LED strip, so that is switched off and the street lamps are switched on.

It would have been nice to light the tramcars, but alas very difficult. Rather like buses, the trams were generously lit internally – particularly noticeable in any photograph. I know that light panels are available but the plastic kits from which the trams are built are not the easiest to assemble. I have built many over the years, and possibly each has gone together slightly differently! I would also be concerned about 'light leaks'.

Providing a working headlight would be tricky as the platforms are open to view and it would be difficult to conceal the lamp inside. The model tram market is nowhere near as advanced as that for model trains. A ready-to-run traditional double deck twin bogie, English tram, with a reliable working chassis to modern standards would be great to have. Tramway modelling has always been something of a niche section of the hobby, perhaps not least because of the difficulties of representing the overhead wire – not a problem for myself due to the widespread use of the conduit system of current collection used in London.

In the past, I have lit some buildings on permanent home-based layouts, but those on 'Kennington Cross' aren't lit. I think that would be my first thought of providing more lighting.



The overbridge at Kennington Junction station is deemed to carry the Southern Region railway lines. It, and the entrance building, are a much-compressed version of the real Vauxhall station.

gave many ideas of what could be included. When I built my earlier tram layouts, I was reduced to repainting second-hand Lesney model cars. Things are considerably easier now, with a vast range of vehicles available. Of course, I have to ensure not to breach the July 1952 date when the London Trams finally said goodbye to London.

Tricky trackwork

Apart from being fairly fearless at butchering flexible track, my abilities do not enable me to build bespoke track sections. Therefore, for the junction I used two Peco 'Y' points for their short length, together with a short crossing. Other points used on the layout are of the small radius, live frog variety. Through trial and error, I have acquired some skill at bending flexible track to small radii, down to 3.5ins. The need to include many isolating sections means that the rails need to be carefully shaped by hand, often while separate from the sleeper base. I glue the track to the baseboard using Bostik All Purpose adhesive, and use drawing pins through the sleepers to hold it in place while setting.

The layout uses traditional 12V DC controls. There are two Gaugemaster Combi controllers; one 'Up' and one 'Down'. Multiple switched sections are provided with the switches mounted on small panels on the front of the baseboard. The trams run at a reasonably consistent speed with each other. Well-timed pauses at tram stops can help regulate the intervals, if required. The points at the double junction are electrically-operated. The other points on the layout, accessing the hidden storage in the depot, are controlled by push rods. With ten cars currently available, I find that four can provide a reasonably intensive service with a fifth usually posed on the depot forecourt. Adding the fifth to the roster demands concentration as the interval between the cars becomes smaller! The remaining five cars are stored offstage and can be added to service as others are removed.

Rolling Stock

Most of the trams are built from the Tower Models plastic kits of the London E1 and UCC Feltham. There is also a model of car 1 – this was an experimental tram built by the LCC in 1932, known as Bluebird due to its original livery – which is built from a 3D-printed body kit. To achieve reliable and consistent running, and also to enable the use of the tight return loops, I use small four-wheel mechanisms from



At the movies

The layout probably shows that, after the tramcars, my interest is mainly in buildings. The Majestic cinema is based on the one that stood in Mitcham. My experience showing the earlier exhibition layouts showed me how much visitors appreciate an authentic scene. At an exhibition at the LT Acton Museum Depot, I showed my 'Dog Kennel Hill' model, which had three or four blocks of South London LCC council flats included. A young guy was on duty at the show as a security guard and spent most of the day looking bored stiff. Eventually, he wandered over for a closer look and suddenly smiled and said "That's my nan's flat!". He had no interest in the trams at all, but the distinctive scene was familiar to him.

As a young 'bus spotter' (I'm not old enough for even trolleybuses, let alone trams!), I made long trips using the economical 'Red Rover' tickets and, although I took little interest at that time in the buildings that I passed, they were the scenery that I was used to and what I base my models on.

My range of card model kits contains several cinemas. It is hard to resist adding more, as they are so characterful. A suitable cinema, pub, or shop is an easy way to set the scene. All are based on real buildings; most of them from around the London area.



Churches make large models. St. Jude's is a comparatively modest-sized church but makes an impressive centrepiece for the layout. In the side street, a wartime 'Utility' Daimler bus awaits the time to leave its stand. After July 1952, all of the trams had been replaced by diesel buses.

the now defunct Bachmann Underground Ernie range. The cars have distinct front and rear ends with driver and conductor figures. The Felthams and Bluebird have the further distinction that the doors at the trailing end are open. A typical end-of-line terminus would be a nice feature to have on the layout, but the thought of running cars

backwards would upset me!

I have some baseboards ready for another layout, which I plan to build. This will be very similar in concept but set in the earlier LCC (London County Council) period around 1925. At this date, the cars were in a distinctive primrose and purple lake livery. While buildings of this period could

well have lasted into the 1950s, I hope to be able to give them a distinctive look that is compatible with the age. The advertising posters and signs, street furniture, road vehicles (including some horsedrawn) will all contribute to this. In an effort to make the layout easier to transport, I have made the new boards slightly shorter.

Buildings



Almost all of the buildings used on the layout are from my own Kingsway Models range of uncut card kits built in standard or modified form. These models are based on real buildings, mainly from London. A unique item is the central rail overbridge, which is loosely based on a compressed version of that at Vauxhall – once a busy tram hub. The use of standard OO gauge Peco track and points meant that the space available for buildings didn't always allow the structures to be constructed 'square' – just as in real life – and some were modified to suit the specific site. In order to fit in the return loop at the right-hand end of the layout, the hidden track passes through some of the buildings. The size of the loop enables a satisfyingly long gap before the same car reappears from under the railway bridge.



ON EXCLUSIVE



Experimental 'Bluebird' LCC (Route Extra)

Built by the LCC as a prototype of a proposed new fleet in 1932, Bluebird was unique. With a steel and aluminium body, air brakes and powered doors, it had many advanced features. Originally painted in a blue and ivory livery it was sold to Leeds in 1951.



EH 85 (Route 38)

Ex-East Ham. One of a batch of 20 similar cars built in 1927/8 to the standard LCC pattern. Like many of these cars, 85 lasted to the end of the system in 1952 in service at Abbey Wood depot.



E1 557 (Route 40)

This car was one of 50 rebuilt using trucks and equipment from single deck cars of classes F & G in 1930. Identified by upper decks similar to E3 cars and also with a wide centre window pillar to the lower saloon, these were the final E1 cars to be built. Withdrawn in 1952.



E1r 1514 (Route 54)

Delivered as a standard E1 in 1911, car 1514 was later modernised as part of the 'rehabilitation' programme in 1936. External distinguishing features include the flush lower side panels and inset destination blinds. Withdrawn in 1952.



E1 1571 (Route 2)

A standard E1 from the 1911/2 batch, withdrawn in 1952.



W 2054 (Route 8)

Ex-Walthamstow car built in 1932. Known as 'Rockets', these cars were known for their high speed and noise. Withdrawn in 1952.



UCC Feltham 2111 (Route 20)

Ex-MET car built in 1930/1 by the Union Construction Company in a modern style with air-powered doors and brakes. Sold to Leeds in 1951.



UCC Feltham 2145 (Route 8)

Ex-LUT car built in 1930/1 by the Union Construction Company in a modern style with air-powered doors and brakes. Sold to Leeds in 1951.



E1 1597 (Route 10)

This car began as E class 420, built in 1906. After an accident it received a top deck in the style of an E3 and was renumbered 1597 and reclassified E1. It ran until 1951. The lower deck remained original with its slimmer end pillars.

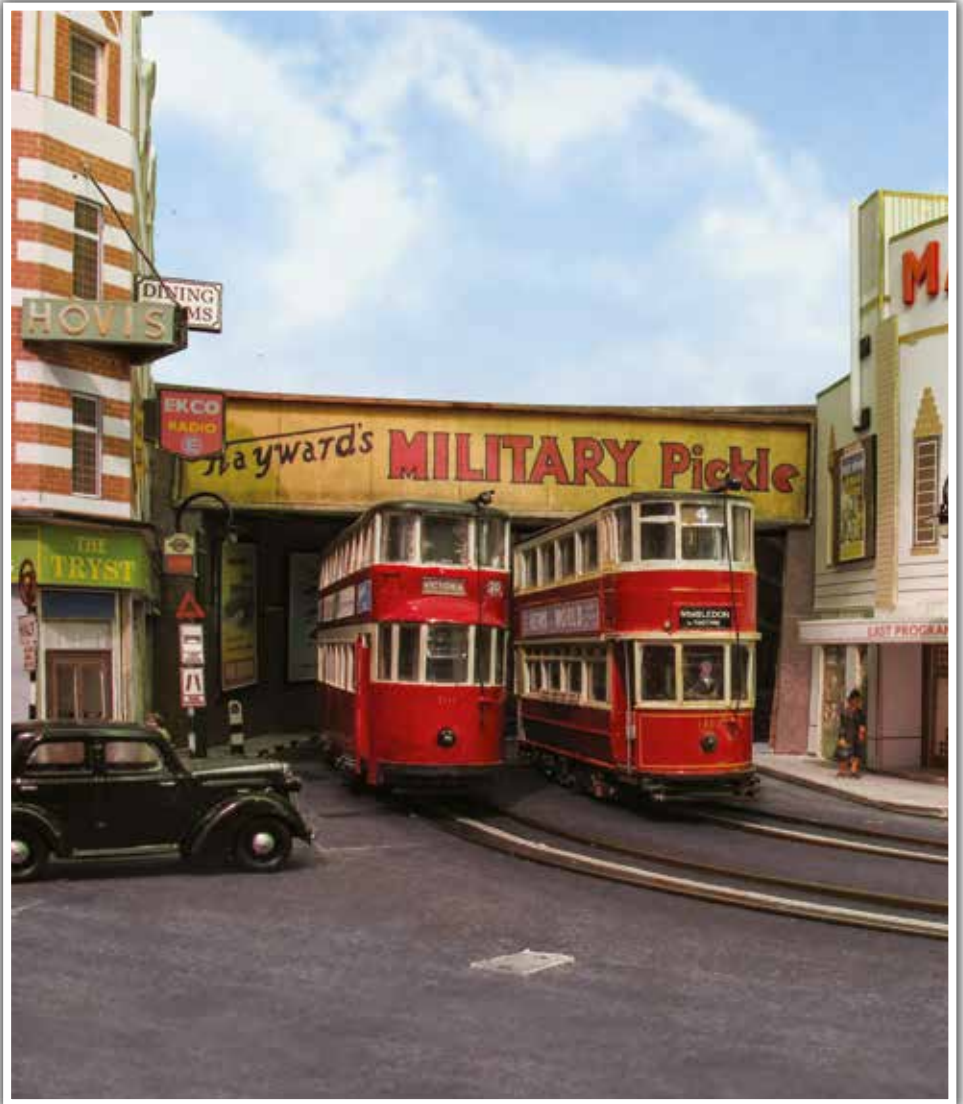


E1 1802 (Route 4)

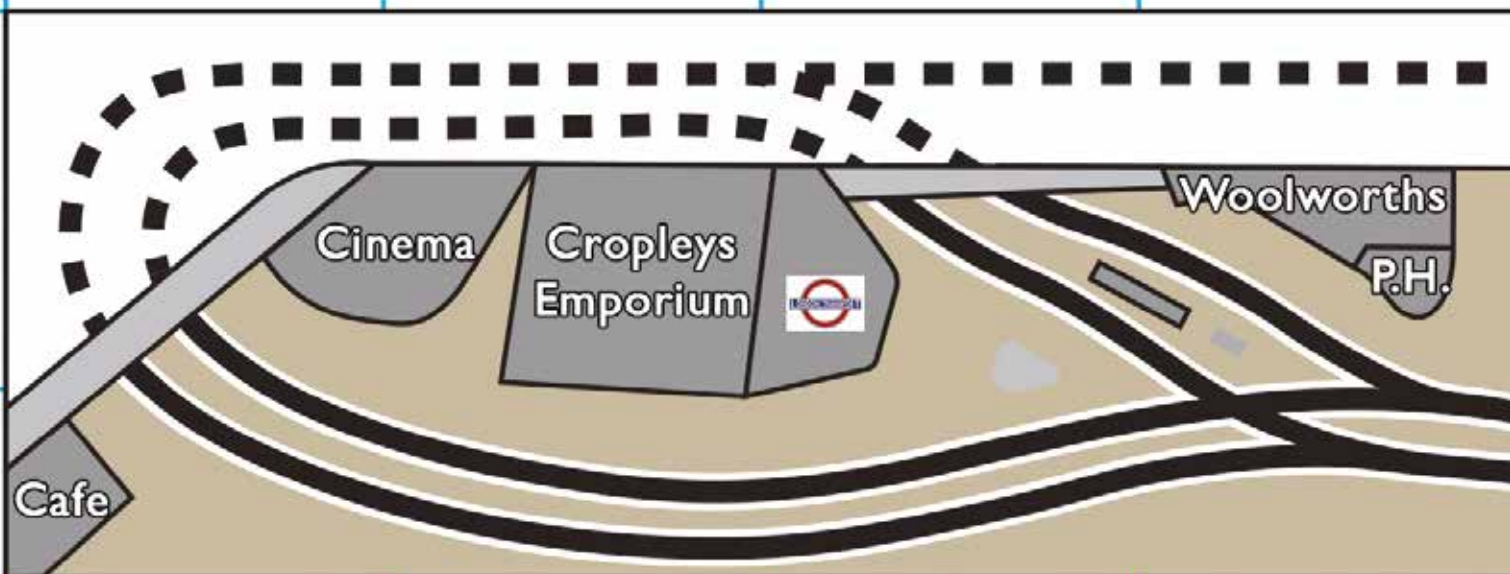
A standard E1 from the 1922 batch which were allocated to Clapham for many years. Withdrawn in 1951.

I have already built a couple of 'B type' London buses in preparation. In this period, trams appeared to be a rather more advanced means of transport – much more welcoming and robust than the buses of the day. They had the advantage of being larger and usually having roofs to keep upstairs passengers dry. In comparison, buses were open-topped and smaller. Over the next 30 years, the buses would improve greatly. By 1952 (the time of my 'Kennington Cross' layout), it seemed obvious that buses, fuelled by abundant and cheap oil, should replace trams. A persistent press campaign at the time had pressed the view that removal of the inflexible tramcars and their rails, would remove London road congestion once and for all!

To build your own versions of the buildings on this layout, visit www.kingswaymodels.co.uk



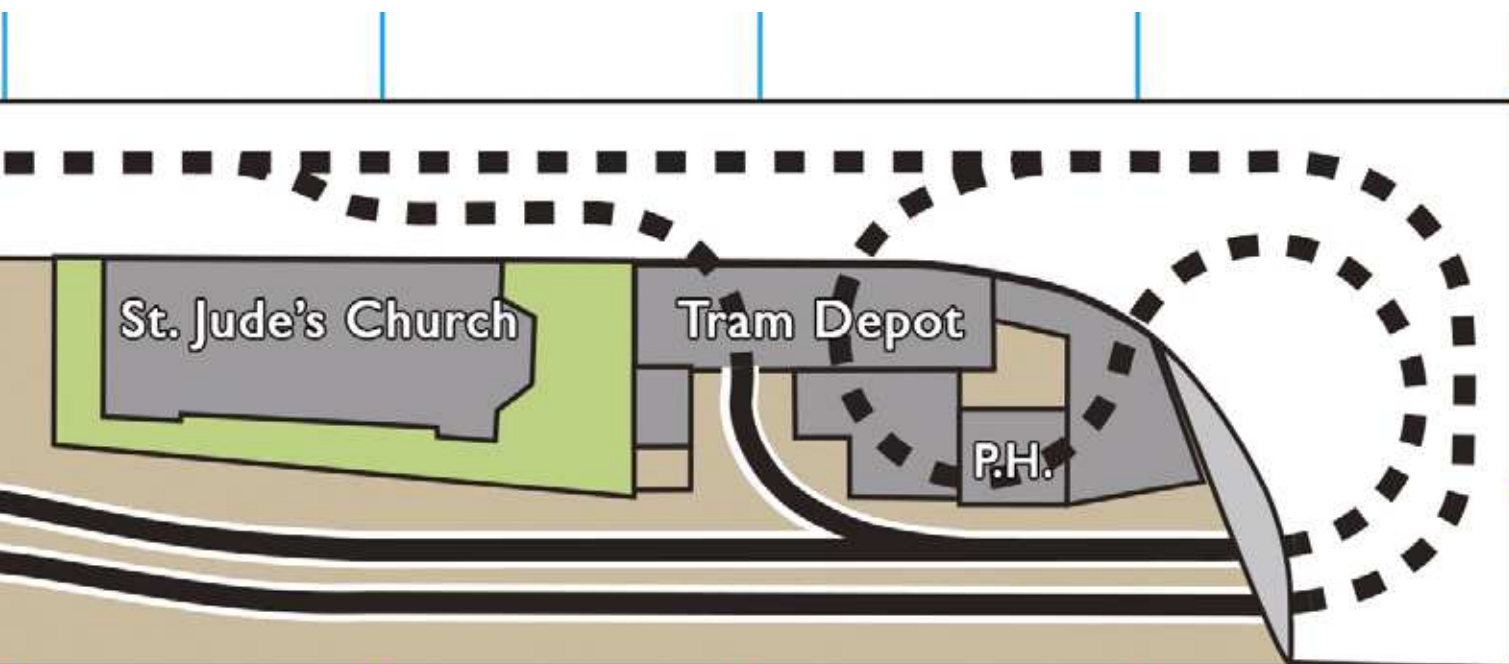
The Hayward Brothers ran their pickle business in Kennington and advertised on the trams for many years. The Tryst tearoom might have been one of the cafes that were listed by London Transport as being a source of refreshment for the tram crews. They had long working hours. The conductor would quickly jump off the car to leave the 'billy-can' there, and then collect it on the return journey, full of hot tea.



ON EXCLUSIVE



A Feltham passes in front of Woolworths, just behind the Police phone box with the adjacent air-raid siren still in place. The island between the tracks also has an entrance to the subterranean public conveniences. The dark blue Evening Standard delivery van is about to deliver to the station newsstand. Like the prototype, the model was converted from an Austin FX3 taxi cab (from the Classix range).



CONTROLLING COSTS

Rising inflation is increasing the cost of living, and our hobby. Howard Smith proposes solutions to continue enjoying the pastime of model railways.

Hornby LMS Stanier 'Black Five' 45017 salvaged from two non-running locomotives – one had a poor running chassis, the other, damage to its bodywork. Total cost... £35.



Across the board, from energy bills to fuel, groceries, consumables, and our hobby – prices are rising considerably faster than the average pay rise. Pension pots are worth less in real terms, too. And as I type this, the Bank of England may be forced to raise rates, and follow the European Central Bank. 2022 does have a feel of the 1970s about it, particularly when you add recent – and most-likely future – strike action into the equation. Doom and gloom? No; but it is a significant shift in the economic cycle.

Model railways is about escaping such drama. It's a hobby to be enjoyed, into which we can become absorbed, and put day-to-day worries at the backs of our minds. And, as the wider media has come

to learn and publicise during recent times, hobbies are good for our mental wellbeing. Ours is a broad church – it attracts enthusiasts with all manner of finances. If you're feeling the squeeze, trying to save, or simply looking to cut the cost of the hobby (and who wouldn't), here are a few considerations so that we can continue to enjoy our pastime.

Payment plans

Model manufacturers and retailers haven't overlooked the economic pressures of recent times. Many – more so recently – have introduced interest-free payment plans on items upon checkout, online. These allow the cost of higher-value items such as a locomotive with the full options

list ticked, for instance, to be split into equal payments, across intervals. While this often applies only to new items, it can help with the affordability of a higher-value order.

Buying seconds

Not everything always goes to plan on the production line of a model release, and sometimes things are omitted, glued on back-to-front, the wrong way around, or have livery defects. Known as 'seconds' these models can sometimes be purchased from a manufacturer's stand at an exhibition at reduced cost on the original recommended retail price advertised. It pays to check these if you can, thoroughly, to see if you can live with

the defects, particularly if this concerns the motorisation of the model. For cosmetic defects such as component location, or liveries, these can often be remedied and hidden with a little weathering.

Buying broken/damaged

Whether you're looking on the classifieds section of RMweb, or an online auction website, don't be put off by the mention of 'damage' or 'missing parts' in the description of a model. It's wise to scrutinise such models further because many won't purchase these, leading to lucrative bargains. Is the model an easy repair? Could 'non-runner' simply be a broken wire to the motor terminal, or dirty pick-ups? Is the seller knowledgeable and familiar with the cleaning ritual of locomotive wheelsets, or simply wanting rid of a bereaved relative's collection? Can the body of a non-running locomotive be married to the working chassis of a similar locomotive suffering body damage?

Replacement parts for modern RTR might be harder to source than for models pre-2010, but if the damage is only a few missing handrails, or other easily-broken exposed parts, these can often be substituted for 'home-brew' replacements with a minimum of tools and some wire.

Pre-order

Pre-ordering helps manufacturers predict the appetite for an RTR release, leading to better cost control, and exposing themselves to less risk. For this reason, many manufacturers offer discounts for pre-ordering models to entice modellers to commit early on, ahead of model production. Pre-ordering not only guarantees delivery of smaller batch-driven limited-edition exclusives from retailers, it also secures discounts typically in the region of between 5 and 30%.

Bulk buys

Trainload wagons look better in rakes. For this reason, manufacturers and retailers often provide customers with an incentive to purchase more than one item. A percentage discount on some purchases, typically increasing up to a maximum limit in line with the number of items bought is sometimes possible. Though you might not

want to keep all the wagons for yourself, if you know someone else interested in the same wagons too, why not place a bulk order and both save?

Junk or treasure?

Nothing quite keeps costs as low as something that's free. Sometimes, certain items depreciate to the point where the original owner simply wants rid. Perhaps they have a new DCC system, but have four DC controllers lying about? Of course, we'd recommend you'd keep at least one DC controller to test models before fitting a decoder – but you get the gist. Ask around and it's surprising how many people have something surplus to their requirements. From left-over project off-cuts of plywood lingering in a garage or shed to older die-cast vehicles lurking in the darkness of lofts – many are prepared to donate items to enthusiastic individuals.

Cash-in/exchange

Collecting is fantastic, to a point. The point at which a collection becomes a hoard only you can decide – again, it all depends on your finances and room to display what you love the most. We'd say it becomes a hoard when a thick layer of dust is evenly spread across models – it's a sign that those models aren't being run enough, or given the love they deserve. Time to thin out, move on and upgrade? We'd understand if they hold sentimental value, of course, but please give them a dust all the same.

We can hoard many things, some of which we believe might be useful 'one day'. But at some point, that drawer filled with un-built wagon kits, since replaced with RTR items you've purchased, despite promising yourself you'd get around to building them 'one day' is now surplus. Where rolling stock or locomotives are concerned, unless these have sentimental value, it might be worth selling to upgrade, using the proceeds toward a new project. You can list these online, or, visit your local model shop and see if an offer can be made.

Compromise

Does your small shunting plank layout require the latest DCC system, or would a more-affordable entry-level unit suffice?

For larger layout builds, consider building half of it now, and look to expand in the future, when possible. Some layouts cost significantly more than others, depending on how many buildings or kits you intend to assemble. Scratch-building keeps costs low and a largely grassed landscape with little track won't cost anywhere near as much as one with complex track, intricate infrastructure and large buildings.

Team up

Do you really need your own layout? Why not join a club, run your stock on a layout there and share costs with other members? Building something in a group, particularly a large layout, makes it more cost-effective per head, while the speed of a model build can often be accelerated, too. Club members can often pool stock and locomotive resources to produce an equally pleasing spectacle at a fraction of the cost.

Conclusion

Whichever way you plan to continue enjoying the hobby, there are small changes in the way we look at larger ticket items that can help reduce its cost. Don't forget to combine purchases to save on postage and always shop around for the best deals – never shy away from making offers on second-hand items, either.

Though scratch-building is time consuming – and might be a steep learning curve – it can be very cost-effective for larger structures.

Perhaps you have a great tip to help reduce the cost of the hobby? If so, we'd like to hear it! Contact us at brm@warnersgroup.co.uk, and we can share it with readers.

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FACTFILE

Layout name: Hook Basin

Scale/Gauge: 1:25 scale/16.5mm track/18in gauge

Size: 8ft 6in by 1ft 6in

Era/Region: Early 1930s, North Oxfordshire

Layout type: Scenic Terminus to Fiddle Yard

HOOK BASIN

Encouraged by a friend to scratch-build a freelance locomotive, Richard Williams soon turned his attention to this photogenic layout in the same scale.

Words: Richard Williams **Photography:** Phil Parker



H&M Locomotive No. 4 shuffles wagons in the station yard. The sheeted loads are paper towels, soaked in PVA glue, draped over a foam former. Tie down ropes are rigging cord from model boats, and given a wash of sepia ink to tone them down.

Wind the clock back some 14 years, and picture in your mind two railway modellers, sitting waiting for a train on the way back from an exhibition, discussing the design of a freelance locomotive one of them had built. My friend had extended the proprietary chassis slightly to fit the body, but we then mused if the same type of chassis could be used without modification. A sketch was produced of the shortened locomotive, and, to the builder's amazement, it 'worked'. The drawing was handed to me with the comment, "Well, you'd better build it, then".

The locomotive turned out a few weeks later as 'Hook and Morton Tramway Loco No. 2', although it's affectionately known as *Tyro*. It was my first foray into scratch-building, and the start of 'Hook Basin'.

First steps

Tyro needed somewhere to run, and being in the slightly esoteric scale of 1:25, meant building most things from scratch. 1:25 scale has some advantages, especially when combined with the idea of 15in or 18in gauge stock, which is what was, in part, driving the idea.

It's 12mm:1ft scale, or '1mm:1in', which is useful for 'measurement of eye'. 16.5mm track could be used as 'close enough' scale, and this meant that reliable donor mechanisms were also available reasonably cheaply.

1:24 is a 'Dolls' House scale' and some detail parts were available fairly easily for buildings and scenery. 'G' scale scenic parts could also be used, if visually appropriate.

There is a lovely size about the scale for visual interest. You get a fairly large locomotive and stock, but due to their small scale size, they don't need much space in which to be effective.

Having an interest in the narrow gauge operations around the Ironstone workings of North Oxfordshire, and taking inspiration from the working 18in gauge lines of the War Department, the basis of the first board was sketched out on paper, then transferred to full-size planning using lining paper, marked out in a 6in grid of squares. Not long after that, the baseboard was constructed from 6mm plywood. It's an open box of plywood, with the joints pinned and glued, and supported by stripwood in the corners. By moving the joints from underneath the baseboard surface, to the sides of an identical size sheet, a separate larger box was made that slotted over the baseboard that forms both the proscenium

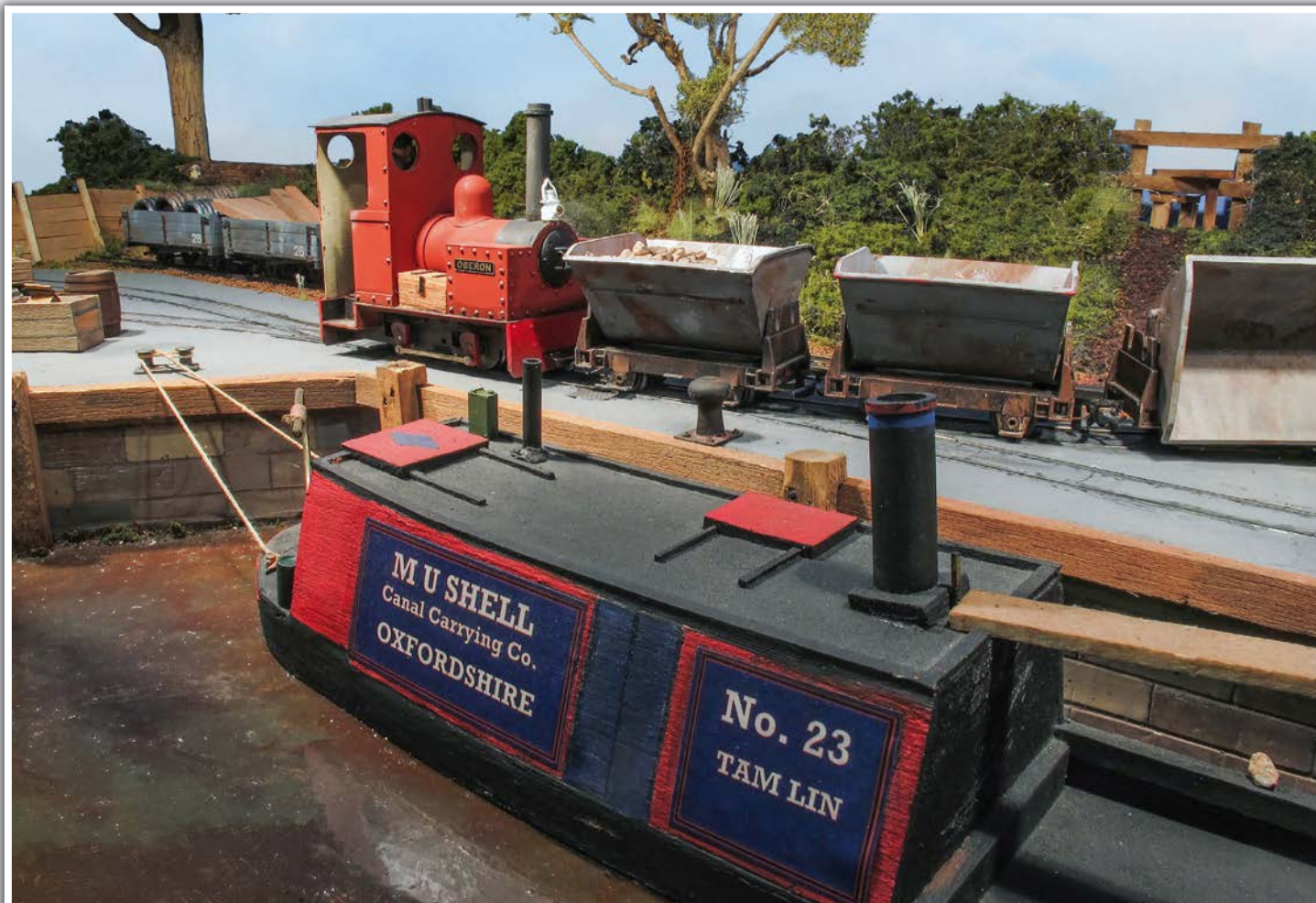
“ It's fun to have something a bit different, and I'm looking forward to the next reaction from someone who doesn't realise that the skips actually tip! ”



The 'historical' justification for the line, and the reason for the layout's existence. 'Ironstone' being loaded into the narrowboat is a key part of the layout operation, delighting both viewers and operator.



Tyro, H&M No. 2 arrives with the passenger train. The 'zinc' roof on the station building is just the dull side of tinfoil, then given coats of sepia wash to dull it further and provide weathering. The inset trackwork (lower) uses check rails of Code 60 glued to the sleepers, before filling between them with DAS clay. This gives a neater finish and more reliable operation.



Oberon caught mid-shunt at the canalside. To prevent striking the chimney on the unloading point, the skip rake is half unloaded, then shunted to allow the empty skips to be used as barrier wagons. The narrowboat is a shortened version of a full 72ft boat to fit on the scene, but still comes in at a scale 56ft long.

arch and travel case. A hinged front cover was attached, which doubles up as a display panel. Good wood glue and a powerful staple gun are definitely your friends here, and the resulting boards are light, strong, and resistant to knocks.

The Canal scene uses the diagonal cross bracing put in to stiffen the layout as part of the scenic area, forming the canal-side walls. In a compact (4ft x 18in) space, the trackplan allowed for two trains to alternate, with the goods doing some shunting to the canal siding, and the mineral train loading the ironstone into the waiting canal boat.

In this form, the layout was first exhibited, and, as a then-newcomer to the exhibition circuit, I was pleased with the interest it had. The first part had been built with the option of extension, and much thought was given to how/if the ironstone shunt could be achieved, as the operator is 6ft away and has their view partially obscured by the central scenic break.

The station board is slightly larger than the canal board (4ft 6in x 18in), but this additional length allows locomotives to

run round their respective trains without needing to exit the board. The station board also required a new fiddleyard be constructed, as the stock increased with additional goods wagons and a new passenger train.

Rolling stock

The two tram locomotives, numbers 2 and 4, are both scratch-built, but run on Hornby 'Smokey Joe' chassis. Their basic construction is of a pair of styrene boxes – one for the skirts, running board and bufferbeams, and one for the locomotive body. The lower box has a cut-out that allows the chassis to clip in on the original chassis mounts, but I also back it up with a bolt and captive nut!

To these basic boxes, various details are applied from more styrene strip, coat hanger wire, 7mm:1ft scale locomotive castings and 4mm:1ft scale smokebox doors.

The tram body helps hide the crudity of the mechanisms, but also allows space to hide lots of lead sheet. Coupled with a feedback controller, they move with a grace

that belies their origins.

The quarry locomotive, *Oberon*, is outwardly based on a Bagnall Inverted Saddle Tank, similar to ones used at the Royal Armouries in Woolwich. This uses a Fleischmann Oe 'Magic Train' diesel chassis, as the outside frames and external flycranks provide visual movement and differentiate against the skirted trams.

The skips for the quarry train are also Magic Train items, repainted and slightly modified to allow them to tip all the way over.

The rest of the stock is a collection of open styrene boxes on commercial chassis, mostly out of 1.5mm styrene. Goods wagons have planking scored in with an Olfa cutter, or occasionally a small screwdriver when I've put the cutter down and can't find it again. Detail is built up from styrene strip and rod. The chassis are mostly the medium-length Tri-ang or Hornby wagons, like the cattle van or 'Bolsover' mineral wagon found in the 1980s starter sets, but some wagons use the longer wheelbase examples as found under TTA tank wagons. More scale-appropriate

brake gear is built-up from more styrene strip and offcuts of 1.5mm sheet for the brake shoes.

The coach rides on a pair of Thompson bogies from Bachmann, their slab-sided nature looking appropriate under the centre vestibule vehicle.

Scenic work

While the majority of the techniques used with the rolling stock transfer across the various scales well, I found a lot of the scenic materials available now were far too fine for 1:25 scale. Grass cover ended up being built from layers of ground foam, clump and flocks, and paintbrush bristles were used for longer grass. Bushes were formed from rubberised horsehair, with more flock and ground foam applied. This, in turn, led to the technique I used on the trees.

The tree trunks were made from cuttings of buddleia and privet from the garden, which had been baked in the oven to remove any unwanted 'hangers-on'. Branch extensions were applied as necessary from twisted wire, and teased out rubberised horsehair used to hint at a twig network. Then, a layer of dark green ground foam

Operation

'Hook Basin' operates to a schedule when at shows, which takes about 25 minutes to work through, and results in all the trains being back at their starting point ready to go again.

I'd planned the operation, and how it would work before I'd bought the wood for the baseboards. This was also useful in helping me work out how much fiddleyard storage I'd need, and how much stock I'd need to build.

The sequence has three key elements:

1. The passenger, which arrives, runs round and after taking on water, departs again
2. The goods train, which will depart with a rake that's different from the one it arrived with, but can be used to shuffle wagons about at the whim of the station staff
3. The stone train, arriving from the quarry and then unloading into the narrowboat

The sequence overlaps these phases, each train 'handing over' to the next on scene. This means that sets of couplings are tuned to work in particular sets and with particular locomotives, so that they are reliable under exhibition running. In theory, the locomotives can swap with each other, and by and large, this is true, but I try and avoid it.

was applied, before the final layer of finest 'dyed sawdust' flock for the leaves. The base foam layer was needed to add bulk, depth and under-shadow to the 'leaf' layer. The first trees didn't have the foam and looked wrong.

Engineering scenics mostly used balsa strip, cut down to create fences, retaining walls, and rubbing timbers on the canal, as well as the canal boat. The inset trackwork used Das clay, suitably stained and painted.

It was a case of experiment and adaptation in some cases, but the overall effect has

appeared to work well. I've tried to vary the colours and textures seen with the greenery to hint at it being an amalgam of several different plants. This required a dive into my childhood model railway supplies, as well as more modern offerings.

Getting the look

A challenge I have for myself, starting with 'Hook Basin', is to answer the following question: Can the viewer determine the historical order in which the buildings were built? In trying to answer that question, it



A view from a narrowboat! The canal crane is a Duncan's Models 7mm:1ft scale model, used without much modification, bar the plinth on which it stands for additional height. It used to pivot, but I had to fix it in place to prevent damage in transit. Canal stonework shows signs of a 'rough shunt' by arriving boats.

forces me to think about not only where the buildings go on the layout, but how they're finished visually.

While the canal scene has two main buildings, I wanted to try different external finishes for the station area.

In all cases but one, the buildings were first roughed out in thick card, as a readily available supply was local to me. Once I was happy with the size, and composition of the building, I'd transfer the final mock-up onto clean card, ready for cladding.

The wharf office is actually the third one built. I kept revising the design following feedback, and it was easier to start again, rather than adapt what I'd already done. This is still in card, with planking 'scribed' in with a ballpoint pen. Windows are 7mm:1ft scale signal box items, with the frames built up from styrene strip.

The old stables, harking back to the possible horse-drawn origins of the tramway, was finished as stone-built using DAS clay, scribed when dry, but then with a more modern concrete render of exterior Polyfilla applied over the stonework. However, enough time has passed that even the render is fighting a battle with the elements.

The main building for the Chandler's is brick-built, but this is terracotta Das, with the brickwork scribed in by hand. I get comments about my sanity for doing so, but in reality, the process was quite swift, taking only a few hours one evening.

The station building is the odd one out, being entirely built of scribed balsa without a mock-up being done first. The zinc roof on it is kitchen tinfoil, shiny side down and further dulled with inks.

More tinfoil, put through an artist's paint squeezer, creates the farm store. I found that the slightly thicker foil from takeaway cartons was better for the sides, but for the curved roof, I needed thinner as it was more flexible.

The engine shed uses more balsa and 7mm:1ft scale resin walling for the stone foundations.

Looking ahead

The first items of stock for 'Hook Basin' were started towards the end of 2008, with the layout being first shown in 2009. Many shows have since been enjoyed, by the visiting public viewing it, and by the operators and I running the trains.

'Hook Basin' still brings me enjoyment to operate and seems to have life yet. It's fun to have something a bit different, and I'm



That moment of anticipation! The operator can't see the wagon tipping from the usual operating point, so you're listening for the thud and whoosh of stone down the chute. The canal water is just a layer of hardboard, shiny side up, painted with murky browns and greens, then coated with several coats of varnish.

Richard's top exhibiting tips

- Keep a note of issues at shows, but don't try and fix things there, you'll only make it worse. It's easier to pull a misbehaving locomotive and repair it at home, or just not use a particular siding where you're not testing under the gaze of the public
- Testing running at home, if possible, is a godsend. Find the bugs before it's too late! 'Hook Basin' can just be put up in my living room, but it is worth doing every so often, especially after any serious work
- Carry spare point motors, and arrange the wiring to allow swift replacement – give yourself access to swap them out. This is especially true for the station throat point that everything has to run through from the fiddle yard!
- Travel seems to be extra hard on wiring looms, especially where they're soldered to solid points – allow a little give and support for the loom in these areas. The classic 'it worked at home' seems to be key here.
- The layout, that has run faultlessly all day, will stop working the moment someone points a video camera at it. This seems to have no known solution.



A rare moment of stillness on the layout! The locomotive shed was put there in case I ever get round to building another locomotive for the layout. However, it does allow a pause in the sequence at times as the locomotives take on coal and water. The uncoupling ramp for the modified tension-lock couplings can be clearly seen here, but in operational use they are much less visually intrusive.

looking forward to the next reaction from someone who doesn't realise that the skips actually tip!

'Hook Basin' is as complete as it will be. I can't store or transport any more sections for it. I hope to give it a bit of a clean and dust soon to keep it in good condition. I may also

take the opportunity to remove or update some of the modelling that doesn't work.

I'm building a few smaller layouts, mostly as the mood and time allow. Two are in OO9, one in 1:55 scale. This has also led to dabbling in 3D design and printing using a resin printer, and new baseboard

construction techniques. As with anything I build, the topics are esoteric, being a fictional island in the Skagerrak (between Denmark and Norway), early 2000s Cornish china clay working in OO9, and some Steampunk-based silliness inspired by HG Wells 'First Man in the Moon' in 1:55 scale.





No. 4 shunts wagons onto the canal side. The clearances between the locomotive and the wall of the old stables are very tight. Also seen is the fastest worker on the layout – I've never seen him move, but the skips tip over well!

About the modeller

Name: Richard Williams

Age: Early 40s

Number of years modelling: 30+

Name of first layout: 'Hardraw', my teenage OO gauge effort in the house cellar.

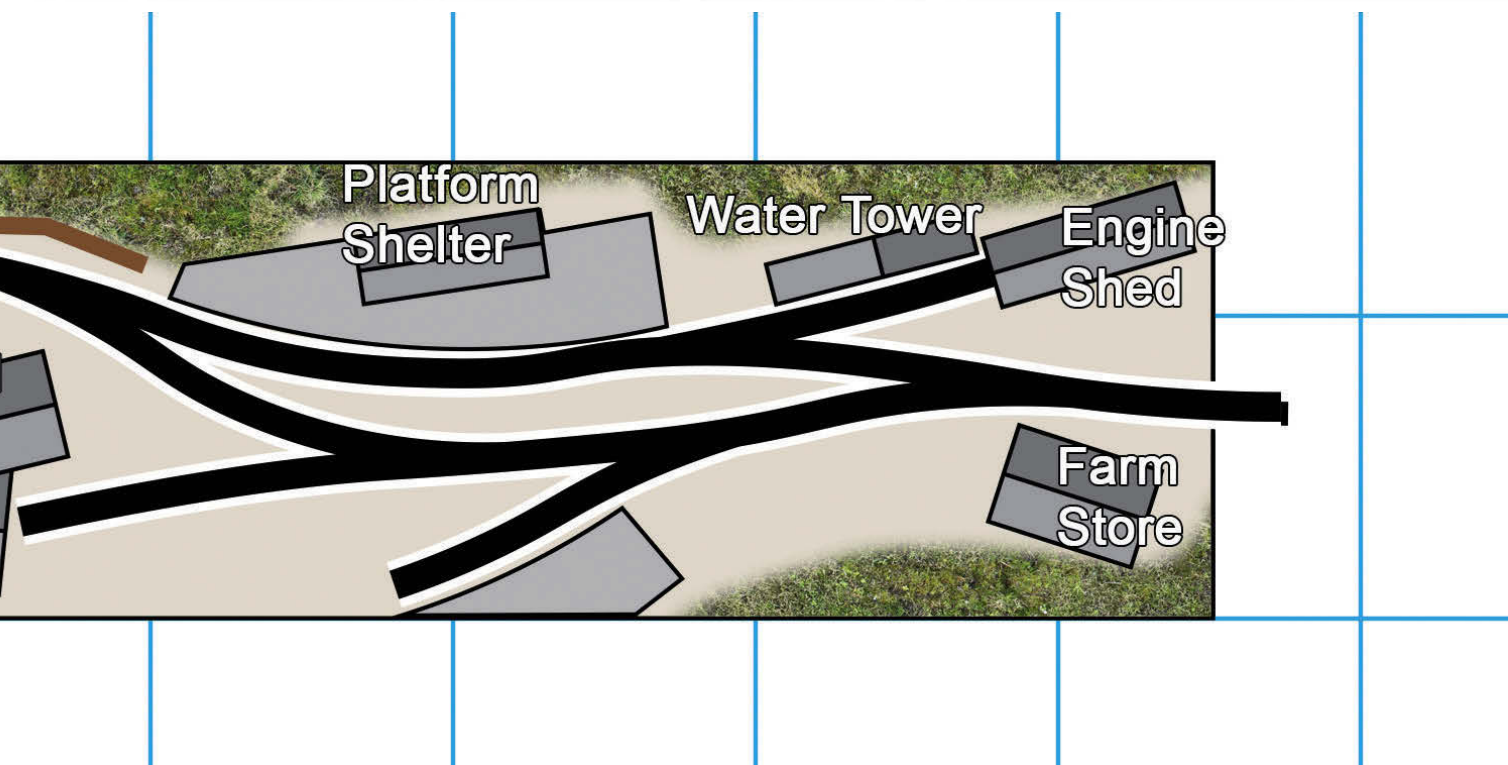
Favourite era/region: Varies, but generally, something narrow gauge!

Favourite locomotive: War Department Hunslet 4-6-0T.

I now live not far from the old heart of the GWR in Wiltshire, but my interest in railways goes back to watching the commuter trains to London as a toddler. Aided by holidays in Wales, I found a particular interest in narrow gauge railways, and anything esoteric or off the beaten track will often pique my interest. I started modelling as an early teenager in OO gauge, as so many do, but switched to narrow gauge modelling during university. I can also be found playing trains in the garden – the lure of proper steam engines is too great!

HOOK BASIN

1/25

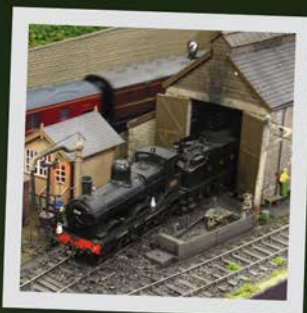
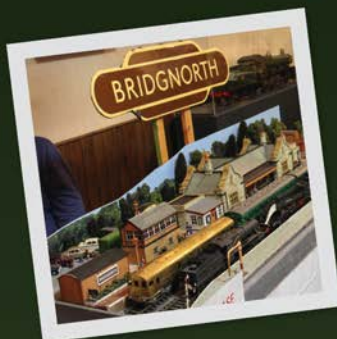




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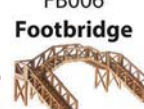
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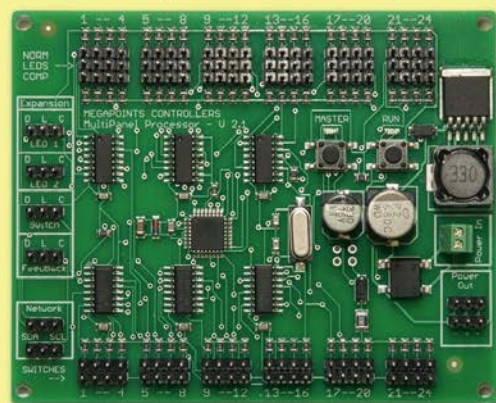
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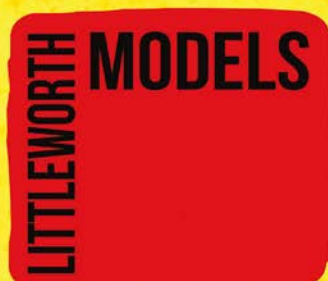
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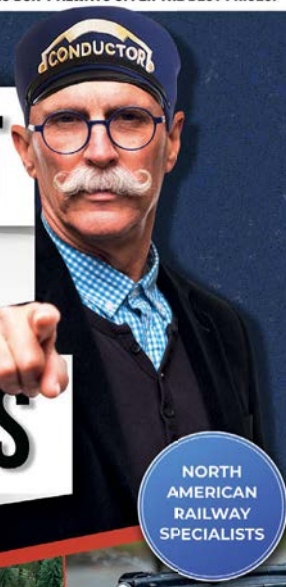
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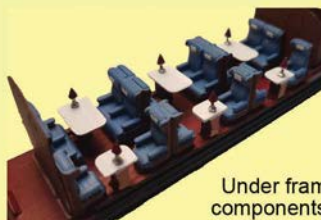
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Tickets are still available to book for the Sunset Scotsman evening canape train, as well as up close viewing in our engine shed, the opportunity to visit the footplate, and experience the corridor through the tender. Tickets can be booked online at www.bluebell-railway.com



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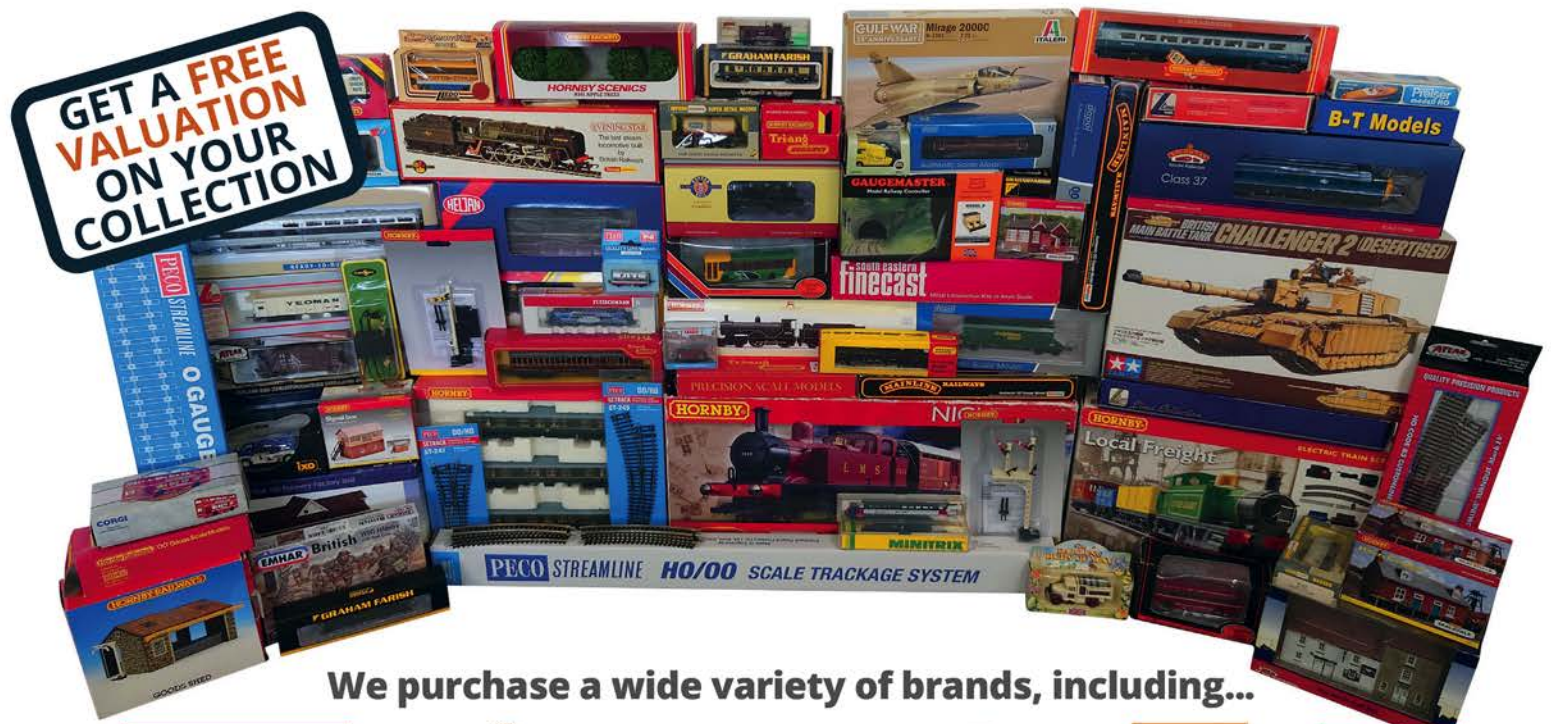
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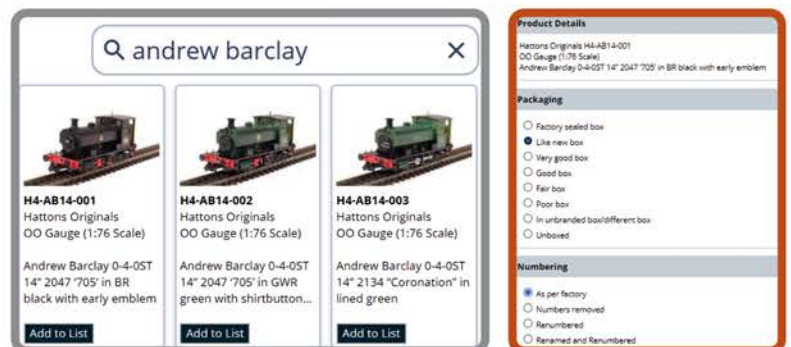
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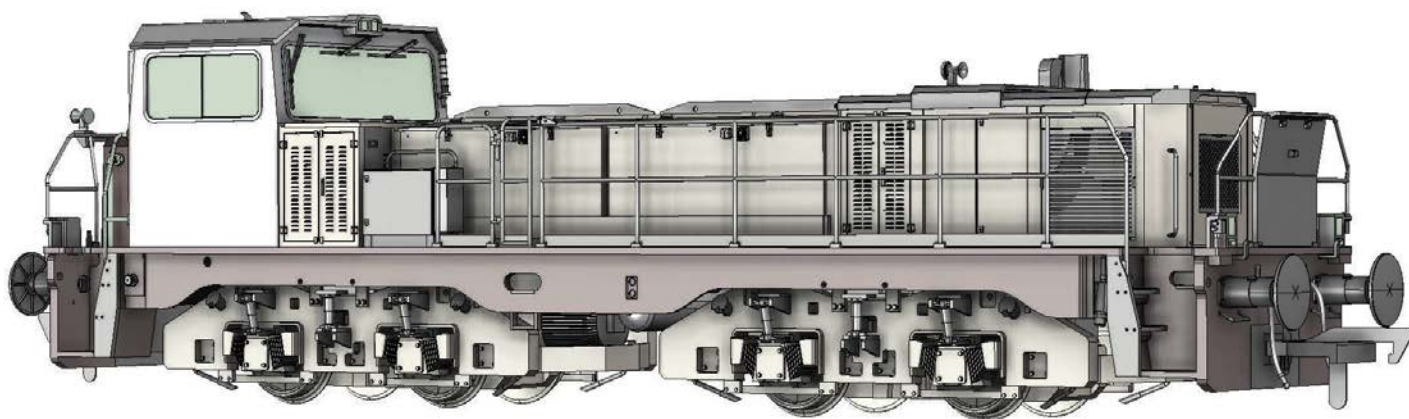
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BUMPER SUMMER UPDATE FROM REVOLUTION TRAINS



Manufacturer poised for new OO gauge announcement as Class 59 sounds recorded and decorated samples of Class 128 for 'N' received in 'state of play' unveiling.

Revolution Trains has clarified progress on its range of models under development, and has revealed that a new OO gauge announcement is to be made at the DEMU Showcase exhibition on July 23/24.

"So far, 2022 has been a period of some consolidation," said Revolution's Ben Ando. "The arrival of the IWA vans and timber carriers in OO and N at the beginning of the year, and our N gauge Class 320/321 units in early spring, saw us packing and posting like never before. However, we are now making good progress on the other items already announced and in development," he added.

In summary, progress on its current projects can be followed, below:

Class 128 parcels unit (N)



The manufacturer received decorated samples mid-June and these have reportedly been assessed. Revolution Trains says it will close the order book for these immediately after giving customers the chance to see the models at The International N Gauge Show in September, allowing production to begin.

Class 18 battery hybrid shunter (OO)

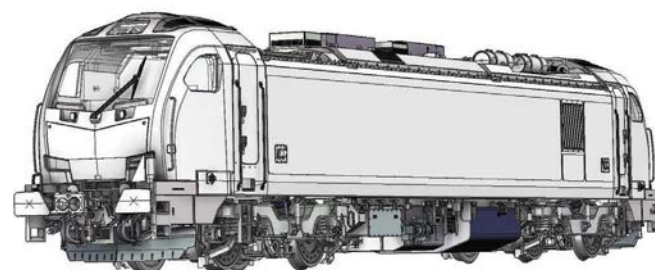


CAD work has been completed on this model, with the co-operation of Clayton Equipment Ltd, Beacon Rail and Tata Steel. First samples of these models are expected early September, 2022. The Tata models have different body arrangements and bogies from the Beacon versions, and the Beacon versions have different lighting arrangements. Revolution Trains has said that all differences are being accurately represented.

Class 59 diesel (N)

In May 2022, Revolution Trains was invited by Freightliner, owners of all the Class 59 locomotives, to visit its Mendip VMF (Vehicle Maintenance Facility) to carry out sound recording for its forthcoming Class 59 model. The recordings are being processed prior to installation in its DCC sound-fitted models. Haulage tests are also said to be complete and the electronic design has been completed to ensure maximum lighting and operational functionality. Decorated samples are expected shortly.

Class 93 tri-mode locomotive (OO)



Revolution Trains has been informed by Stadler that its first locomotive should be completed ready for static and dynamic testing in late September/early October, 2022. Revolution Trains has been invited to visit its plant in Valencia for a viewing.

The manufacturer commented, "It is important to check the 'built' locomotives against the drawings before we commence the costly tooling process. Our CAD is 80% complete and the model options include pantographs that can be raised or lowered, and working roof fans, as well as full exterior and interior lighting.

Class 313/314 EMU (N)



A first EP sample was received in late spring and has been tested. The next step will be decorated samples with the expectation that production of these models will go into production late-2022.

FNA-D nuclear flask carriers (N and OO)



CAD of both N and OO gauge variants of these models has been approved and tooling has started. Models represent the present day flasks, introduced from 2014, which have now displaced the older style flasks that were in use since the 1970s. The model has been produced with the support of DRS Ltd and samples in both scales are expected by the Warley National Model Railway show at the NEC, which Revolution Trains is expected to attend.

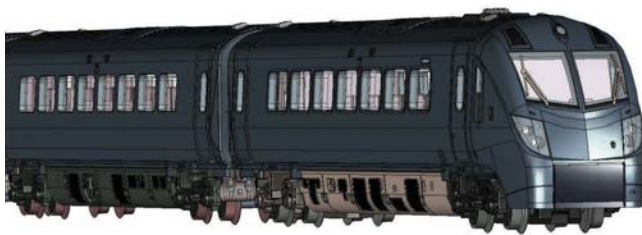
Cartic-4 (N and OO)



First samples of these models have been received and tested. The manufacturer has said that while their operation is satisfactory, minor modifications will be made to the articulation units to make assembly more straightforward for customers. CAD is nearing completion for the OO versions.

For further images of the above models, and to view current pricing, visit the Revolution Trains website.

Class 175, 180 (N)



CAD of these models is said to be almost complete, with the manufacturer now beginning CAD design work on its OO gauge versions. Models will be available to pre-order shortly at a low EarlyBird discount price.

Revolution Trains said, "Conscious that our customers do not have limitless funds, we are attempting to pace our releases to assist with budgeting."

Its 1938 tube stock CAD work was delayed due to Covid preventing access to research sources. However, this has now been achieved and has resumed, with finalised CADs expected soon, and tooling "likely to start shortly".

DAPOL RELEASES NEW IMAGES OF HOPPERS FOR 'O'



Dapol has shared images of decorated samples of its forthcoming O gauge HBA and HEA coal hoppers, expected to arrive in shops late July to early August 2022.

Models announced are as follows:

- (7F-047-002) HEA Coal Hopper Railfreight Red / Grey 360000
- (7F-047-003) HBA Coal Hopper Freight Brown 360114
- (7F-047-004) HEA Coal Hopper EWS Maroon 360354
- (7F-047-005) HEA Coal Hopper Freight Brown 360292
- (7F-047-006) HEA Coal Hopper Mainline Blue 360620
- (7F-047-007) HEA Coal Hopper Transrail Grey 360208
- (7F-047-008) HBA Coal Hopper Freight Brown 360626
- (7F-047-001) HEA Coal Hopper Railfreight Red / Grey 360104

In 1975, the prototype 32.5T HBA (360000) was unveiled at Shildon Works. The hopper body was painted initially in Bauxite.

An order for 2000 wagons to be built at Shildon was placed. Two small changes to the design were made. The ladders fitted at each end were altered from being centrally-mounted to being offset to the left. Bruninghaus springs were fitted to later builds, allowing the maximum speed to be increased from 45mph to 60. This modification must have proven successful as a plan was put in place to retrofit the earlier wagons to the same standard. The TOPS code of HEA was introduced to distinguish these, with the last 80 or so completed by mid-1985.

By the latter half of the 1980s, the traffic that the wagons were designed for was declining, and new roles for the fleet had to be found, and an early example was carrying rock salt, while another was scrap metal.

Many HEA wagons were converted for other uses and the conversion process increased in pace, therefore the number of HEAs declined steadily throughout the 1990s.



RETAILER EXCLUSIVES UPDATE

Rails Dynamometer car in maroon – first look



Rails of Sheffield has shared images of its exclusive new production batch of the LNER Dynamometer Car, to be released in BR maroon livery. Previously available from the retailer in teal guise, the new batch of models will allow more modellers to access the vehicle for the later British Railways era.

Production of models is expected to be complete in August, in a limited number, which reportedly has sold well, leaving limited numbers available. Models are priced at £139.95 (product code 935004) and can be purchased from the Rails of Sheffield website.

Based on detailed research of the prototype, archive photographs and official drawings, Rapido Trains UK's model exclusive to Rails of Sheffield has extensive

interior and underframe detail, including depictions of the recording equipment and the additional speed recording wheel between the bogies.

The North Eastern Railway's test car, built at Darlington Works in 1906, is now on display at the National Railway Museum, and recorded the 126mph run of LNER A4 No. 4468 Mallard on July 3, 1938.

No. 23591 passed into LNER ownership after the Grouping of 1923, and its Chief Mechanical Engineer, Sir Nigel Gresley, used it extensively to test his new designs and modifications against established locomotives. It was even used behind the UK's only steam-diesel hybrid locomotive!

For more details, visit the Rails of Sheffield website.



Exclusives join KMRC shops

1902 RCH seven plank open wagon for O gauge and GWR Saxby & Farmer Type 12b signal box for OO gauge in latest exclusive arrivals with the retailer.

Kernow Model Rail Centre has released an exclusive commission from Bachmann Scenecraft in 4mm:1ft scale (OO) of the ex-Liskeard & Looe Railway (later GWR) Saxby & Farmer Type 12b signal box at Coombe Junction.

The 'box was one of two supplied to the railway and opened in 1901. The independent Liskeard & Looe had its own platform at Liskeard station, situated at 90 degrees to the main line. The line fell away sharply turning through 180 degrees, reaching Coombe Junction, where trains had to reverse to continue their journey to Looe. The 'box originally contained a 26 lever Saxby & Farmer

Duplex frame, which was later replaced with a Great Western Railway frame of the same size in 1956. The 'box continued in use until modernisation caught up with it and it closed on May 8, 1981, the rationalised layout being controlled by two ground frames.

The highly detailed Scenecraft model of Coombe Junction signal box in 4mm:1ft scale is cast in resin, painted and has a removable roof to allow an interior to be fitted, if required.

Chris Treise, Managing Director said, "The Coombe Junction signal box joins our growing range of Exclusive Scenecraft signal boxes and other buildings with a local connection to either of our branches".



Wagon for 7mm:1ft scale

Meanwhile, for 7mm:1ft scale modellers, an exclusive O gauge 1902 RCH seven-plank open wagon in the livery of Guildford brick and tile manufacturer 'T. Mitchell' has been announced, and is also in stock. Thomas Mitchell was originally the proprietor of a brick and gravel merchant, that by the early 1900s had become Thomas Mitchell and Sons, brick and tile manufacturers, with a large brickworks at Guildford Park. By 1902, it had its own 10T private owner wagon for the transportation of coal to the works in black with white lettering.

KMRC's detailed model in O gauge is being produced by Dapol Ltd, and is based on its RCH 1887 specification seven-plank open wagon. The model features a 9ft die-cast chassis with a compensation beam and is fitted with open-spoked wheels, while the body is injection moulded with separately applied parts. Sprung metal buffers and sprung coupling hooks with three-link couplings feature.

Matthew Higgs, Guildford Branch Manager said, "I'm pleased that we are able to announce our Guildford T. Mitchell wagon. With the growth of O gauge recently, this wagon was an obvious choice for an exclusive model

with it being local to our shop in Guildford and should complement some of our future projects rather nicely."

For current pricing and availability of the above models, visit the Kernow Model Rail Centre website, or visit its Guildford or Camborne stores.



Accurascale and Rails partner for Class 89 exclusive

Accurascale and Rails of Sheffield have revealed an exclusive model of the BR Class 89 'Badger' prototype electric locomotive in OO gauge/4mm:1ft scale, to be sold exclusively direct from Rails of Sheffield and Accurascale.

CAD is now said to be complete, and the locomotive has entered the early stages of tooling. The first tooled sample is expected in time for the Warley show in November 2022, with decorated samples following in early 2023 and delivery slated for Q3, 2023.

Accurascale and Rails of Sheffield first mooted this exclusive model in 2020 as an "expression of interest" among the public to see if there was sufficient interest. Demand was reportedly healthy, but due to the COVID 19 pandemic, access to the real locomotive for surveying and scanning was impossible. However, following the easing of restrictions in 2021 and beyond, Accurascale was able to continue working on the model, including a survey facilitated by Mervyn Alcock and the staff at Barrow Hill, while Wabtec Brush was able to provide copies of the original drawings.

The new model will use the same sound setup as its existing diesel and electric locomotives, as well as the DCC-operated rising and lowering pantograph. Models will feature a "powerful and silky smooth" five-pole skew-wound motor with twin flywheels, helical cut gears, a flange squeal sensor, and full lighting package including illuminated cab dials. The Class 89 model will be based on Accurascale's Class 92 platform, with all-wheel-powered six-axle bogies and a pantograph that can be raised or lowered almost silently under DC or DCC control.

The project is the second partnership between Accurascale and Rails of Sheffield, and builds on the successful cooperation with the Class 55 'Deltic', which included a number of special editions commissioned by Rails in partnership with the Deltic Preservation Society and Locomotion Models. However, the Class 89 will feature five different livery variants, all of which can be bought from both Rails and Accurascale direct online.

Speaking about the project, Rails of Sheffield MD John Barber said "We are very excited to be working with the Accurascale team on this new OO gauge project. The Class 89 has been on many people's wishlists for a long time, so we are thrilled to be recreating a model of such an iconic engine in miniature together."

Accurascale's PR and Marketing Director Fran Burke expressed his excitement about the project; "The Class 89 is one project our production team and I have been very excited about for a long time. For people of our age it has been a legendary locomotive and one we will bring a premium, high-quality feel to. We are thoroughly excited about this locomotive and our latest partnership with Rails of Sheffield and we feel that there could be many more of these partnerships in the future, bringing modellers many more exciting models with it."

Livery variants are as follows:

- October 1986 to November 1988 BR InterCity Executive livery
 - December 1988 to July 1992 Named Avocet on December 16, 1988 and debuts in InterCity Swallow livery
 - March 1997 to January 1999 Reinstated for service March 3, 1997 and repainted into GNER blue with white logo (loco had been purchased in 1996 by Sea Containers)
 - February 1999 to June 2007 Returns to service after repairs at Brush in GNER blue with gold logo
 - April 2019 to present Classic InterCity Swallow livery with Modern OHLE Flashes
- The model specification is as follows:
- Highly-detailed OO scale model, 1:76.2 scale
 - Minimum Radius 438mm (second radius Set-track)
 - Die-cast metal chassis
 - RP25-110 profile OO wheels
 - Separately-applied etched metal/plastic detail parts, including grab handles, steps, wipers
 - Scale width wire handrails
 - Etched metal pre-painted nameplates provided for customer to install
 - Full underframe tank detail with brackets and pipework
 - Fully sprung metal buffers and bufferplate, factory-fitted pipework and screw



couplings (for display)

- Dummy knuckle coupler provided in polybag
- Kinetic NEM coupler mounts at correct height and mini-tension-lock couplers
- DCC ready [21-Pin MTX Socket] or Factory-Fitted DCC Sound options
- Five-pole motor with twin flywheels and metal helical gear box for maximum performance and slow speed running
- Gearing arranged so locomotive can achieve minimum scale top speed of 125 mph (200 km/h)
- DCC ready with PowerPack capacitor for uninterrupted power
- All wheel drive (sprung centre axle) and all-wheel pick-up
- Directional lighting, DC and DCC
- Switchable red and white marker lights with day/night options
- Separately switched cab lighting and illuminated, driver's desk, auto/off on movement
- Scale-sized Brecknall-Willis High Speed pantograph raised and lowered by DCC control (auto up and down operation in DC with power on/off)
- Customised Dual-Speaker Technology with large 'Accurathrash' Style Bass Speaker and smaller 'iphone' style cube for higher frequencies (on DCC Sound-fitted models only)
- Hall sensor for flange squeal effect (DCC sound-fitted models only)

Pre-ordering is now open and orders can be secured via the Accurascale and Rails of Sheffield websites for a deposit.



ZEN V12 DECODERS – LATEST ARRIVALS FROM DCC CONCEPTS

Promising easier installation and setup, advanced ABC control, automated shuttle functions, with 'stay-alive' control, the manufacturer's range is its most advanced to-date.

Now with DCC Concepts and its stockists is a new range of Zen Blue+ decoders. Latest range additions offer extended and improved automatic braking abilities, with "simple to use one step locomotive set-up." The range is compact too, designed to fit where some of its competitors can't.

The DCC Concepts Zen Black three wire stay-alive range separates energy storage from its power management, making it easier to install a 'stay-alive' into locomotives. Connection to 'stay-alives' uses a plug-and-play three-wire setup.

Its Zen V12 Black pack (DCD-SAA-3W) offers a selection of 'stay-alives' in four sizes. These are compatible with its Zen Black, Blue+, Lenz, ESU and other decoder ranges with a three-wire 'stay-alive' connection.

Zen decoders are manufactured in an automated facility. Electronic parts have a 'best before' date because solder-able surfaces need to be oxidation-free for reliability, so parts deliveries are always 'just in time'. PCBs used in assembly are said to be created "within hours of assembly" by machinery with "superb accuracy".

DCC Concepts explained further, "critical things such as machine-applied fluxes and solder baths are replaced well before recommended dates, so that dry joints can't happen and defect-free assembly and subsequent reliability can be guaranteed. Once assembled, all of our decoders are tested at least three times before we package them for sale, so we know that they all work perfectly when we sell them."

To explore the full range of DCC Concepts Zen Blue+ and Black, visit its website.



MULTI-SCALE PICKET LINE FIGURES



With rail strikes in the news, this new 3D-scanned figure pack is timely and useful for layouts depicting the current scene.

Containing five figures

and available in any scale, this is the perfect set to deploy if technical problems prevent your trains from working.

Four figures are holding placards while one regales us with a megaphone.

While the recent disputes inspired the commissioning of this set from the manufacturer, the figures could be just as easily used on a 1970s layout, positioned outside car plants and railway stations or a few years later in front of a coal mine. Figures could also be used to replicate any sort of protest.

BACHMANN LAUNCHES NEW CLASS 37 AT SHOW

Bachmann Europe Plc surprised visitors to the Diesel and Electric Modellers United (DEMU) Showcase event held in Sutton Coldfield on July 23/24 with new models of the English Electric Type 3, Class 37 diesel-electric locomotives for its Bachmann Branchline 00 gauge range. First models were on sale during the event from the Bachmann Stockists in attendance.

Class 37 models have featured in the Bachmann Branchline range for the last two decades, but these models owe nothing to their predecessors and have been designed from scratch, using original drawings, diagrams, site visits and surveys of real locomotives.

Building on the impressive specification of its award-winning Branchline Class 47, the new Class 37 shares many common features such as the five-pole motor with twin flywheels, all-axle drive and electrical pick-up from all wheels. Each axle runs in a pair of separate metal bearings, located in the die-cast metal bogie towers, which are fitted to the die-cast metal chassis block. The Dual Fitted speaker system returns, offering

authentic sound reproduction and this is pre-fitted to every model, while the Class 37 features a new 'Yard Light' mode, allowing red tail lights and/or marker lights to be displayed at both ends of the locomotive when operating on DCC. Using the model on DCC also provides cab lighting and engine room lights at the push of a button, while analogue operation sees marker lights, tail lights and headlights – where applicable – illuminate as desired.

Each Class 37 offered will be produced in three formats, with standard versions ready for use on analogue control out of the box and with a Plux22 DCC decoder socket for consumers wishing to fit their own DCC decoder. Bachmann recommends its (36-570A) Plux22 locomotive decoder. Sound-fitted models will be offered and these versions are pre-fitted with Zimo's (MS450P22) sound decoder.

Sound-fitted deluxe models share all the features of its sound-fitted models, with additional innovations for which the company received the Innovation of the Year prize in the 2021 British Railway Modelling Awards – namely the motorised radiator fan, driven by an independent motor, operated as part of the sound

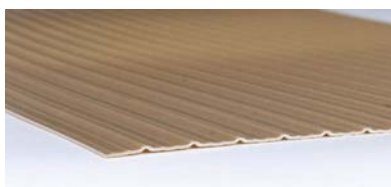
STEEL CLADDING PROFILE FROM SLATER'S PLASTIKARD

Embossed styrene with modern corrugations is expected to become popular with modellers of recent times.

Slater's Plastikard has a new embossed styrene sheet in its range. Coded 0455 and designed to closely replicate steel cladding, the 298mm x 174mm grey/brown coloured sheets feature raised rib and bolt detail.

Available in 4mm:1ft and 7mm:1ft scales, priced at £4.00 each, sheets are easily cut with a sharp scalpel or craft knife and can be bonded to other similar styrene materials with a solvent adhesive to clad a shed, modern farm building, or warehouse model. The material is often seen on roofs, too.

Embossed detail is more obvious on the outer face, though its inner-face is equally useable if painted and left 'as-is' for a shed interior. Sheets are easily flexed, allowing it to be wrapped around building curves if needed.



project, and cab windscreen glazing that has undergone a specialist treatment to replicate the tinting seen on the prototype.

Bachmann has produced for the first-time multiple sound projects to be used in its sound-fitted and sound-fitted deluxe models. No fewer than four sound projects can be specified for any particular locomotive and these encompass the original Class 37/0s, original locomotives with thermostatically controlled fans, Class 37/4s and DRS-modified machines.

Sound-fitted and sound-fitted deluxe models will operate on both DCC and analogue from the box. On analogue control, authentic sound effects can be enjoyed when power is applied, including a convincing engine start up procedure before the model moves, while the motorised fans on sound-fitted deluxe versions will also operate according to the sound project fitted to that particular model.

The models revealed on sale depict 37430 *Cwmbran* in BR large logo blue livery (35-335/35-335SF/35-335SFX), while the Bachmann Collectors Club has announced a limited edition model of Class 37422 *Cardiff Canton* in EWS livery (35-335K/35-335KSF/35-335KSF).



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PLAY

PECO TT:120 RANGE

Words & photography: Howard Smith



“We’re reaching saturation point in the ready-to-run 4mm:1ft scale market”, or words to that effect, many vocal modellers have said, many times. And, it’s true that there’s a finite list of models that will sell in sufficient numbers to make them financially viable. As we’ve seen increasingly, if the numbers stack up for a particular case, the chances are more than one manufacturer has such a project under development. What are manufacturers to do? Perhaps the solution lies in a new scale.

Announced this summer by Peco was a

new range of track and accessories for TT (a long-standing acronym for ‘Table Top’) in 1:120 scale, and already items are with stockists. The opportunities for TT are plentiful – behind the now opened new door to this scale lies a land of rich bounty, ready to be claimed. Expect to see the same prototypes and accessories we’re familiar with in OO gauge, only to a scale that many have labelled as ‘perfect’ between N gauge and OO gauge.

NEW TIMES, NEW SCALE

British TT of old in the 1950s and early

1960s was manufactured to a scale of 1:101.6, or 3mm:1ft, but when used in conjunction with 12mm gauge track, it resulted in a scale compromise that British modellers had to accept. Fast-forward to 2022, and the expectations – and demands – of modellers have increased. And so, a newly-tooled range to 1:120 scale has arrived. For the first time, British modellers have access to true-to-scale off-the-shelf models and accessories without compromise.

Kindly sent for review by Peco is a selection of its new products in the scale,

datafile

BASICS

Manufacturer: Peco**Catalogue Refs and RRP:**

(SL-1208) Joiner Sleepers RRP: £5.75

(SL-1240) Buffer Stops RRP: £4.95

(LK-12100) GWR Signal Box RRP: £14.95

(LK-12101) Country Station Building RRP: £18.95

(LK-12102) GWR Goods Shed RRP: £18.95

(LK-12103) Platform Edging (brick type) RRP: £8.95

(LK-12104) Platform Edging (stone type) RRP: £8.95

(LK-12105) Platform Ramp Edging (brick type) RRP: £6.95

(LK-12106) Platform Ramp Edging (stone type) RRP: £6.95

(LK-12107) Platform Brick-Type Pavements RRP: £5.75

(LK-12108) Platform Concrete Type Surface RRP: £5.75

(SL-U1295) Medium Radius Right Hand Turnout Code 55 Nickel Silver Rail RRP: £17.95

(SL-U1296) Medium Radius Left Hand Turnout Code 55 Nickel Silver Rail RRP: £17.95

Gauge/scale: 12mm gauge/1:120 scale/TT**Era:** Pre-grouping to present**Company/Operator:** GWR/BR/post-privatisation

Track to Code 55 is the same high standards that we're familiar with from the manufacturer. Per standard, instructions on wiring its products are provided.

enough to build a small layout with diorama, in fact. From its new Unifrog range of track, we find Code 55 nickel silver turnouts in medium left- and right-hand radii, while a pack of four injection-moulded buffer stops (SL-1240) will seem familiar to those who've built its similar kits in N and OO gauge, requiring a minimum of cutting out, and assembly with a solvent adhesive. Usefully, two holes have been made in a sleeper, large enough to secure the bufferstop with track pins, which can be later hidden with rail. This should keep models on the layout should an over-enthusiastic operator be on the controls.

Injection-moulded in plastic, too, are packs of four platform ramps in either stone-built or brick-built types. Their design allows them to slot together easily using tabs with its packs of six platform edgings, also available in the choice of two materials. Combining a pack of platform straights and ramps results in two platform faces measuring 948mm – a comfortable length for a locomotive and five bogie coaches, for instance. Interestingly, also in the range, we find a choice of brick-type and textured-effect platform tops. The concrete-effect platform tops are lightly textured, but might require a little straightening before gluing in place. I'd argue that the brick-type pavements are of greater use.

A CUT ABOVE

The most welcome additions to this range, above what's considered to be the essential of track, is a small selection of classic rural railway infrastructure. A Country Station Building, GWR Signal Box, and GWR

Goods Shed, all combine laser-cut wooden parts, with detailed plastic mouldings. These are sharp, well-cut and appear accurate.

The GWR signal box is representative of the prototype seen at Highley on the Severn Valley Railway, which many modellers might choose to visit for inspiration when detailing the interior. Meanwhile, the GWR Goods Shed bears a resemblance to the Ratio GWR Goods Shed for 4mm:1ft scale.

FUTURE-PROOF

This move away from injection-moulded plastics has to be welcomed and I'm very keen to see the results when painted and assembled. Given that many layouts have a short lifespan, it provides a more sustainable form of modelling. The only injection-moulded parts on these buildings are chimney pots, which, owing to their intricacy, can't be laser-cut as single items.

Packaging for all items is card, too – and interestingly, as such, makes for an eye-catching display in a retail environment. Where possible, this is already made from recycled material, and is widely-recycled. It's a great introduction by the manufacturer, and something I'd welcome to see across its entire range of products. Other manufacturers whose products are supplied in endless plastic bags should take note.

ONLY THE BEGINNING

In launching this fantastic new range, Peco has sowed a seed for other manufacturers to follow suit with RTR releases. For the modeller, this makes modelling larger locations in the same space possible. Tempting? Very...



Laser-cut from wood, with a sheet of clear acetate for windows, and injection-moulded chimney pots, this is the first multi-media kit we've seen from the manufacturer.



Future-proof. Recycling and sustainability of products is only set to increase in the coming years. Peco has given its new range thought, choosing materials wisely.

ACCURASCALE HCA AND CDAS

Words & photography: Andy York



Following our review of Accurascale's HAA MGR wagons in BRM July 2022, we are now seeing the arrival of the canopy versions of the hoppers in HCA format, with the aerodynamic overhang designed to minimise the loss of powdery coal at speed in transit. These models are suitable for the early 1970s onwards, in early liveries through to post-privatisation in the EWS-owned examples shown in the review.

Overall, the MGR hopper design was considered a huge success and it was developed into wagons for other commodities. The most famous is the china clay version (TOPS code CDA), which was introduced in 1987-8, 124 wagons being built at Doncaster Works and delivered wearing ECC English China Clay branding with blue frames. A further 15 were rebuilt from HAA hoppers in 1989. CDAs are still in use on these trains in EWS and DB liveries, and some still sport evidence of their original blue cradles and hoods.

As mentioned in the earlier review, take care when removing these intricate wagons from their boxes as you will

immediately realise each one has a clingy plastic wraparound, which ensures that the chassis detail is well-held in place during transportation. Take care when cutting this wrap open to avoid damaging surfaces or wagon parts.

We are reviewing the early brown freight livery pack of three HCA wagons and the EWS-liveried pack of CDA wagons, all with an exemplary level of finish to the galvanised wagon body and the numbering and data detail with era-appropriate overhead

electricity warning signs.

The HCA pack of three wagons features the early style body with internal strengthening bars and early number and data panels, which, combined with the brown cradle, points these models to the early 1970s. A great addition to the already produced HAA wagons because mixed rakes could be observed. As with the real wagons, which had the addition of the overhanging canopy, the model's canopy is a separate part. There are occasional variations in the



Beneath the HCA wagon, Accurascale has replicated the cross-braced hopper doors and their stops in addition to the fine chassis detail. It should be noted that further work is required to these wagons to fit finescale wheels.



Each wagon has excellent detail and superb decoration and, in an era where common and basic open wagons are approaching similar price points, tremendous value for money at the equivalent of £25 each.

standard of fitment of the part, but these wouldn't be noticeable when weathered – as they should be! The centre two straighteners are taller than on the HAA to provide bracing for the canopy, too.

In addition to the early set reviewed, Accurascale has produced a post-privatisation pack with EWS branding and red cradles – I'm sure other liveries will appear in due course.

The CDA packs of three wagons have been produced in EWS and later DB liveries with two packs including different number and decoration versions in each livery. Pack two, as pictured, includes a wagon with a blue canopy (a hangover from original English China Clay's ownership) while pack one includes decoration variety, with one wagon retaining partial blue framework from earlier times.

The most distinctive aspect of the CDA is the protective cover and mechanism to the roof, and this has been replicated extremely well with some lovely moulding of the draped cover with high points and creases. The ties to one side of the canopy are particularly fine whilst the top roller bar and the hinge mechanism are all present. The ends of the wagon include equipment boxes for the roller canopy, which distinguishes the CDA from other hoppers. The CDA

variant doesn't have the large cams above the solebar because there's no requirement for compatibility with auto-tripping gear to open the hopper chutes, as was the case in merry-go-round operations. Therefore, these wagons don't need to be 'handed' in a train.

Both sets of hoppers retain the bottom-emptying hatches, the HAA with diagonal door reinforcement and the CDA with rectangular reinforcement. The chassis has sprung Oleo buffers with a steel finish to the shanks, something rarely seen on models in this scale.

While the wagon is upside down, it's evident that Accurascale's approach to normally unseen detail carries onwards with accurate replication of discharge door stops, suspension and brake gear elements, making them a delight to examine from every angle. Take a close look at the clasp brakes sitting around the wheelset and the brake blocks in line with the wheel treads.

The cradle frame and die-cast chassis feature specific and very clearly defined print for markings and build plates (which are etched parts), and the overhead warning flashes are crisp. There's certainly nothing to fault with this model decoration-wise.

At this point, it is relevant to state that the wagons are handed and all should have the

datafile

BASICS

Manufacturer: Accurascale

Catalogue Refs:

(ACC2595HCA-EWS1) freight brown – Pack 1

(ACC2596HCA-EWS2) EWS – Pack 1

(ACC2535CDA-DB1) DB – Pack 1

(ACC2536CDA-DB2) DB – Pack 2

(ACC2030CDA-EWS1) EWS – Pack 1

(ACC2531CDA-EWS2) EWS – Pack 2

RRP: £74.95 (set of three wagons)

Gauge/scale: 16.5mm gauge, 1:76 scale

OO

Era: 5 to 9

Company/Operator: BR/EWS/DB

Weight: 55g

Chassis: die-cast **Body:** plastic

Accessories: Dummy Instantan link couplers, brake pipes

Minimum Curve Radius: 438mm (R2)

Wheel Profile: RP25

Couplings: NEM-mounted tension-locks

cam handles for hopper discharge on the same side of the train – the auto-tripping gear would only be on one side at the power station unloading road.

Each of the packs retail for £74.95 even with the additional elements beyond the previously-reviewed HAA, and at less than £25 per wagon, demonstrates Accurascale's commitment to value for money.

The models include a detail pack with scale dummy Instantan couplings and bufferbeam pipes to be fitted by the user if coupling choices permit. The fitting of the pipes is the neatest I have seen, with a peg that mounts into a socket on the rear of the bufferbeam on each side of the coupling mount – easy and tidy!

Overall, there is much to commend the models for and little to criticise. I look forward to seeing some of these exceptionally free-running wagons weathered and loaded on layouts.



IRM MAGNESITE HOPPER WAGONS

Words & photography: Andy York



Following on from the success of previous IRM short-wheelbase wagon platforms such as the ballast hopper, we now see the superb Magnesite hopper wagons on the same platform.

These vehicles served a specific traffic flow of magnesium carbonate (magnesite) from Bennettsbridge in Co. Kilkenny to Quigley's works at Ballinacourty in Co. Waterford, a distance of some 50 miles, for use in the production of heat-resistant bricks for lining blast furnaces for the export market. The plant was completed in 1969, and from 1970 received one train per day of 12-22 hopper wagons hauled by an 'A' Class locomotive (reviewed in BRM September 2021). Rising costs, decreased tax incentives and a slump in the value of the raw material saw the traffic end in 1982, with production moving to Asia.

Following on from the modular approach, which CIÉ took with its wagon fleet in the 1960s and 1970s, IRM has been able to capitalise on its common chassis, first used under the ballast hopper wagons. A whole new body tooling for these distinctive wagons was created, featuring the intricate loading hatches, discharge mechanism and profile of the hopper body.

IRM has produced four triple wagon packs with different numbers and markings,

all in the same bauxite livery, in addition to a single wagon product, limited to 350 pieces, which features handwritten enthusiasts' 'graffiti' marking the last train, now 40 years ago, making a total of 13 different wagons.

The models are set off beautifully with very fine, but robust, etched metal walkways and ladder detail, as well as an array of chassis detail to create a wagon to excellent standards of fidelity. Irish Railway Models has taken this opportunity to refine the existing chassis tooling further, so that features such as the brake levers and their ratchet/pin-down loops have been improved with additional finesse and detailing. Further additions to this tooling include the correct style of axleboxes used on the magnesite wagons and similar stock during this era. The improvements on the underframe detail are certainly noticeable.

NEM pocket-mounted tension-lock couplings are fitted as delivered, but cosmetic Instantan couplings are included to improve appearances substantially. As with all IRM products, sufficient clearance has been designed to fit finescale wheelsets to 5ft 3in Irish gauge.

The price for triple packs is €124.95, and the special edition single wagon is €44.95, with a 10% saving when you buy two or more packs.



datafile

BASICS

Manufacturer: Irish Railway Models

Catalogue Refs:

(IRM1160MAG-A) Pack A (triple)

(IRM1161MAG-B) Pack B (triple)

(IRM1162MAG-C) Pack C (triple)

(IRM1163MAG-D) Pack D (triple)

(IRM1167MAG-1) (single)

RRP: €45.40 single, €126.25 triple

Gauge/scale: 16.5mm gauge / 1:76 scale / 00

Era: CIÉ 1970-1982

Company/Operator: CIÉ/Irish Rail/Iarnród Éireann

Weight: 41g

Body and chassis: Plastic

Minimum Curve Radius: 438mm (R2)

Wheel Profile: RP25

Couplings: NEM-mounted tension-lock

Accessories: Cosmetic Instantan couplings

PECO SNAILBEACH HOPPER WAGONS

Words & photography: Phil Parker



The Snailbeach District Railways operated in Shropshire as part of the Colonel Stephens' empire, from 1877 to 1947. Initially hauling lead ore to a main line interchange at Pontesbury, this was later replaced by roadstone. The line suffered a protracted demise, a Fordson tractor, operated by new owners Salop County Council, even replaced locomotive haulage for the last few years.

Peco's tooling is the latest addition to its OO9 range. Comparing it to the plan in Eric S. Tonks' book on the line, dimensionally, it appears to be spot on. Even the very finely made – and free running – metal wheels are a scale 1ft 7in diameter. Correctly, brake gear is fitted to one side only.

The plastic mouldings are very crisp. You could even argue that the model looks too good, compared to photographs of the prototypes, which tended to have suffered a hard life. A small downside is the lack of weight, and there's little room to hide weight, unless hidden under a load.

Livery is grey with white lettering. SDR is spread the length of the body, although photographs show this varied, some wagons confining it to the central section. However, for No. 32, the wider version is correct, as is the use of different typefaces for letters and numbers.

For a model 37mm long, the industry-standard 10mm on couplings at each end aren't ideal. Were I to run a rake of hoppers, it would be tempting to only use couplings on the ends, swapping to chains between the other wagons. This isn't a criticism of the Peco model, but it deserves better than the chunky 'Bemos' fitted – these are in NEM sockets, so can be easily removed.

Modelling the SDR is made easier thanks to Baldwin locomotive power and ex-WD wagons, useful for the post-WW1 period and available for the scale. Dundas Models makes a kit for the line's open coal wagons.

Peco has recognised that not everyone is an SDR modeller, so the hopper is available in plain grey and brown liveries for those who want to run it on their own lines, too. It's certainly not out of the question that these wagons could have found new homes.

While kits for this prototype have been available in many scales, hopper assembly is a challenging task if you want a good running chassis. For that reason, I'm sure this wagon will be well received by modellers.

If you'd like to know more about these wagons, visit the Shropshire Mines website www.shropshiremines.org.uk where details of a restored wagon can be found.



datafile

BASICS

Manufacturer: Peco

Catalogue Refs:
(GR-340A) – SDR Grey
(GR-340UB) – Plain brown
(GR-340UG) – Plain grey

RRP: £24.00

Gauge/scale: 9mm gauge 1:76 scale

Era: 2-4

Company/Operator: Snailbeach District Railways

Weight: 5g

Body and chassis: Plastic with metal wheels

Couplings: Bemo

Minimum Curve Radius: 438mm (R2)

Wheel Profile: RP25

Couplings: NEM-mounted 'Bemos'

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Quickview: Peco OO9 gauge Snailbeach District Railway wagons

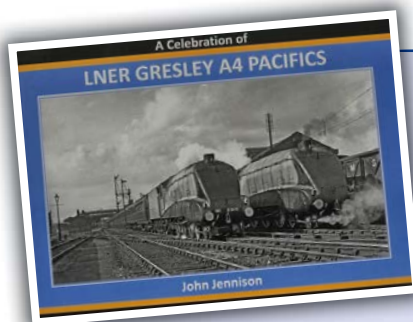
Howard Smith examines three new hopper wagon models from Peco of those used by the Snailbeach District Railway.

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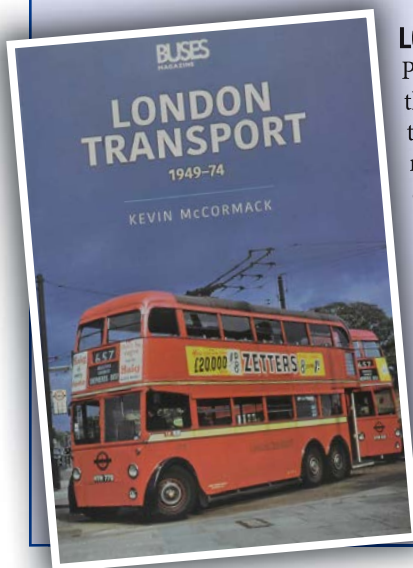
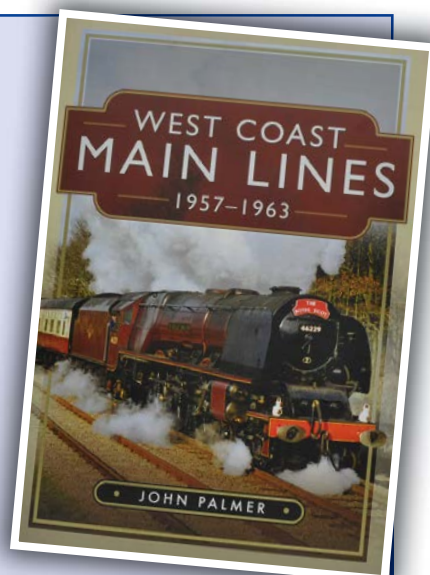
A CELEBRATION OF LNER GRESLEY A4 PACIFICS, by John Jennison, Irwell Press. PRICE: £28.95

As usual, with most of Irwell's titles, I'm sent the manuscript beforehand to proof read, make comments and to 'expand' in some cases the captions to the photographs. I hope this 'expression of interest' doesn't preclude or even exclude me from writing an objective review. It's the latest in this highly-regarded series of 'Celebrations', each one focusing on famous classes. Previous ones have featured Gresley's non-streamlined Pacifics, Stanier's 'Princess Coronations', the 'Britannias' and the SR 'West Country' and 'Battle of Britain' light Pacifics, all following the established format of whole-page imagery, creating the highest standards of reproduction. Like all those others, it is a veritable 'feast for the eyes.' The

imagery is truly outstanding, with some of the finest shots of this incredible class I've ever seen. Granted, I have seen some of these images before, but many are new; and many were taken by W.J. (Bill) Verden Anderson, in my opinion, the finest railway photographer of all time. The pictures show the A4s throughout their lives, on all the roads they regularly worked on. Though the images are in B&W, modellers will find them of the greatest assistance, and anyone contemplating making/modifying an A4 should obtain this book immediately; it complements entirely all the other established works on these great locomotives, some published by Irwell, too. I'm credited with 'Tony Wright has provided valuable assistance with the captions' in the Introduction, and I'm delighted to have been of assistance. That said, I'm less delighted that some of my comments/corrections have not been incorporated. Yes, I know the Up and the Down directions at Grantham, that 'The Elizabethan' never ran in March (of any year) and that *Kingfisher* had lost its corridor tender by September 1966! But these are minor quibbles; those of a pedant, and I'm precious about my input being credited. They should not detract in any way from the reader's enjoyment of this wonderful new volume. Just love it for what it is, enjoy it for what it is and use it for what it is; it's good value for money and should appeal to all – even those (unlike me) too young to have seen these 'Streaks' in their post-War heyday on the ECML. Thoroughly-recommended.

WEST COAST MAIN LINES 1957-1963, by John Palmer, Pen and Sword. PRICE: £35.00

There's the usual large selection from the Pen and Sword house this month. Other titles include the first joint biography of the two Brunels, a pictorial tribute to Glasgow's trams (marking the 60th anniversary of their withdrawal from service) and a pictorial tribute to Virgin Trains. But, I've chosen to review the book on the West Coast Main Lines because it appears to be aimed at exactly the likes of me; those born immediately post-War, who grew up as 'baby-boomer' trainspotters, 'copping' what was seen on the lines in question during the time depicted. It's exactly up my street! It's also rather good. The origins of the lines described are considered, setting the historical scene, as it were, including shots showing the lines prior to BR. The informative and accurate text is complemented by several maps, diagrams and appendices, as well as very well-reproduced photographs, many of which are in colour. All have been printed to a high standard on first class, gloss stock. There's even a small selection of colour shots of some of Stanier's locomotives in more-recent preservation, though oddly, no dates are given. Other shots showing preserved locomotives are also dotted among the pages, though I'm not too sure of the relevance of these, especially with regard to the work's timescale. That said, I'm sure a colour picture of *Cornwall* in L&NWR service doesn't exist. I loved the 'You'll Never Believe What's Been Through' Chapter – all trainspotters will relate to this; a reported, and often disbelieved account from a fellow 'spotter, claiming to have seen a really rare locomotive. There's even a list containing examples of such incredible events, possibly gleaned from a spotter's grubby notes. Recommended for model-makers and students of railway history.



LONDON TRANSPORT 1949-74 by Kevin McCormack, Pen and Sword. PRICE: £14.99

Plenty from Key this month – supplied via Pen and Sword, including works on 'modern' scenes illustrating the railways of South East Sussex and its surroundings and HSTs on the Western Region. But, I've chosen the London book because it's of much greater personal interest. It follows the established format of near-100 softback, A5 pages of full-colour illustrations, with accurate, informative and, in some cases, extended captions. In fact, it's a visual journey of nostalgia, at least as far as I'm concerned. Though I can't recall seeing any London trolleybuses, LT's other transport assets were very familiar to me. The Tube/Underground systems were of particular interest; and, will be of particular interest to model-makers, too, particularly since LT railway stock is now much more available than in the past, especially RTR. The Met electric Bo-Bos are also featured – made RTR by Heljan. The locomotive-hauled (antique) stock might need a bit of kit- or scratch-building, however. For those who are not sure how much of a difference there was in between the Tube's loading gauge and the standard loading gauge, the shot on page 8 makes a fascinating comparison. For architectural modellers, the contemporary street scenes are of particular use, especially with regard to 'street furniture' and advertising. As with all the other books in this format, it represents exceptional value for money, particularly since the photographs haven't been published before. I thoroughly recommend it.

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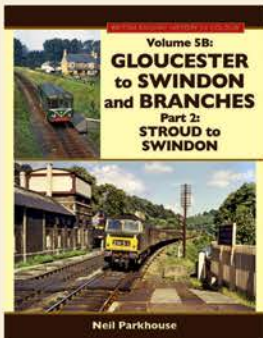


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Vol. 5B: GLOUCESTER to SWINDON and BRANCHES

Part 2: STROUD TO SWINDON Neil Parkhouse



The second and concluding part of our journey from Gloucester Central to Swindon. Having reached Stroud in Part 1, we first complete our study of that station before heading off up the valley towards Brimscombe and Chalford. These were two very different stations, opened over fifty years apart, one designed by Brunel and the other to a then standard GWR design, both of which were closed on the same day in 1964 when local passenger services were withdrawn. Much of this service was provided by the auto trains running to and from Gloucester, which served the valley for sixty years until bus and private car ownership brought about their demise. However, BR having given notice originally in 1962 about ending the service, its last years of operation were well photographed and all of the intermediate halts are illustrated here, along with numerous scenic views of the autos scurrying up and down the valley. Interspersed with the auto workings were a very varied range of other trains, from London expresses to heavy freights, most of which required banking up the fearsome climb to Sapperton Tunnel and the summit of the line. Banking engines were usually stationed at Brimscombe, where a shed was provided from broad gauge days, but assisting locomotives for heavy expresses requiring piloting waited at Stroud, attaching to the front of the train when the stop was made there. Our period covers from the late 1950s to the mid 1970s, through the last years of steam on BR(WR) and the early diesel-hydraulic era. Again the locomotive variety is impressive: 'Castles', 'Halls' and 'Granges', '9Fs' and '8Fs', 'Prairies', pannier tanks and '14XX' 0-4-2Ts, along with 'Westerns', 'Hymeks' and 'Teddy Bears'. And railbuses! Once we reach Kemble we take time out to travel the branches to Tetbury and Cirencester, on which the railbuses reigned supreme for the final six years, stopping at all of the halts and studying the infrastructure of both termini. And then, we complete the last leg to Swindon, crossing into Wiltshire and carrying a while at the delightful Brunellian station at Miney, before arriving at our destination, past the engine shed which lay alongside the line from Gloucester. And everything on this gorgeously scenic journey, especially the lovely mostly long stations – Stroud and Kemble alone remain – are as usual illustrated in glorious colour, from spring greens to autumn golds, and Swindon green to Rail blue!

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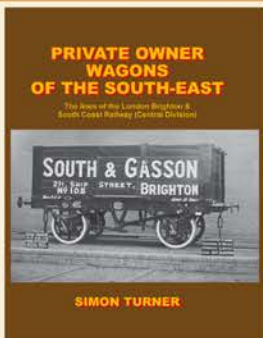
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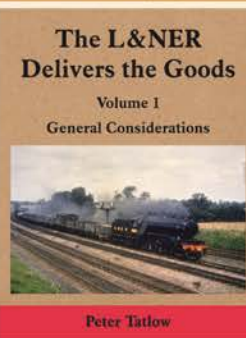
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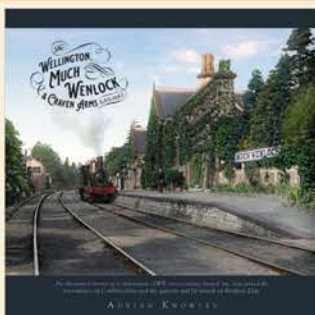
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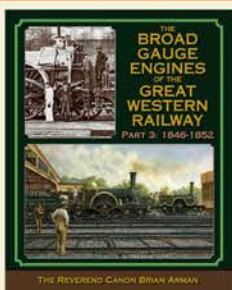
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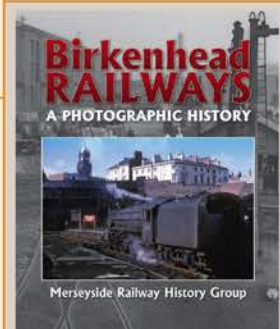


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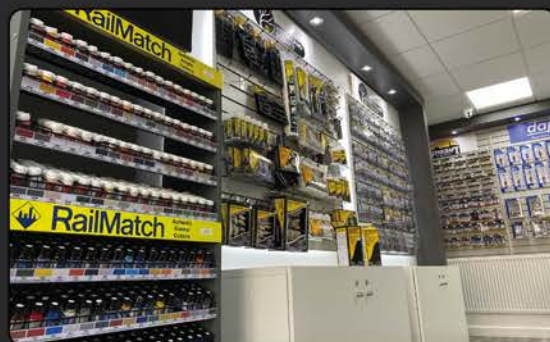
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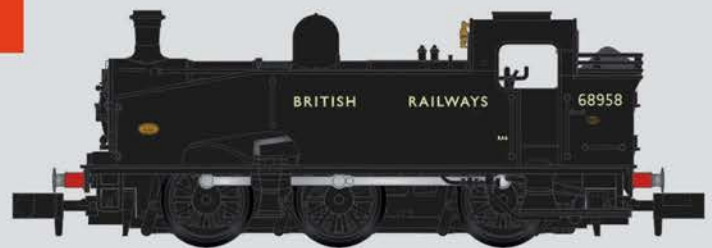
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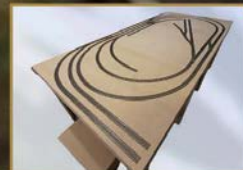
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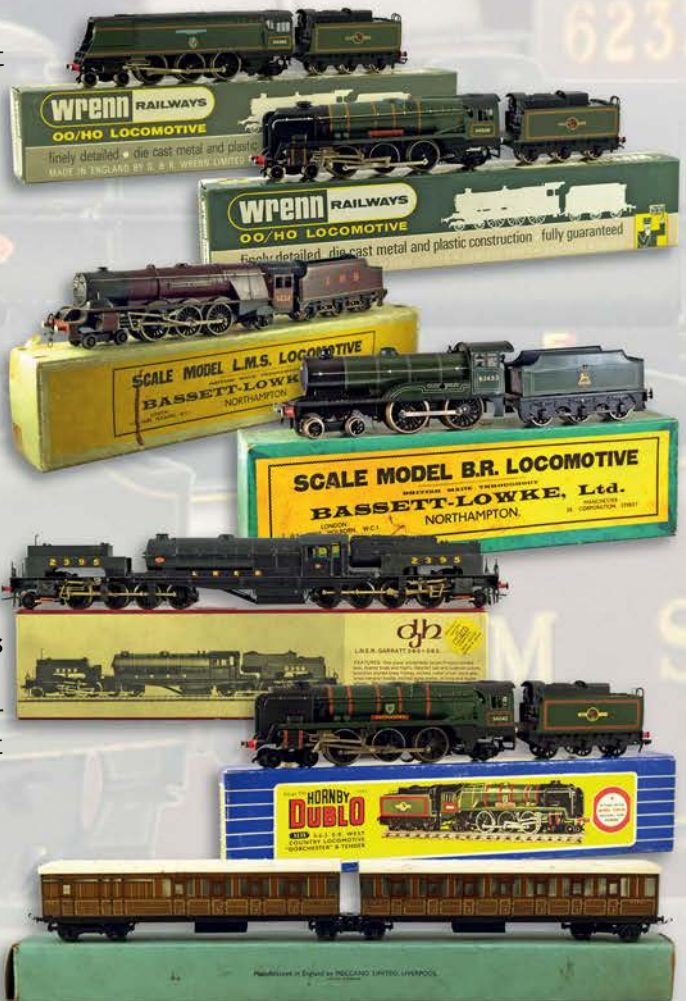
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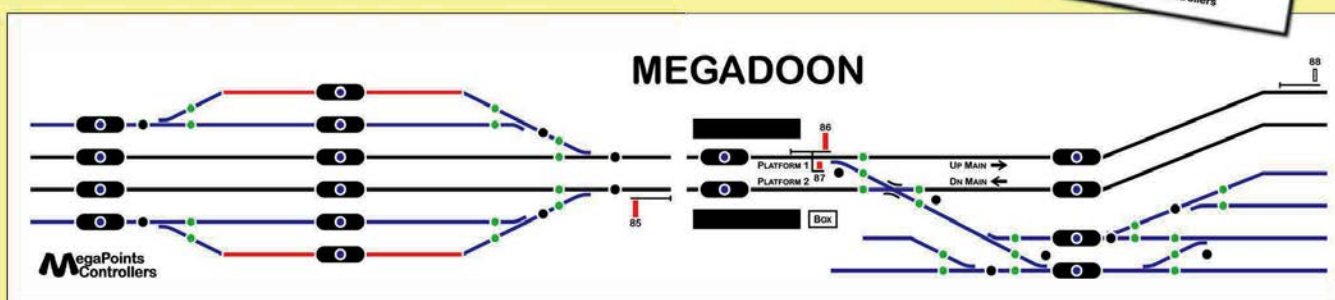
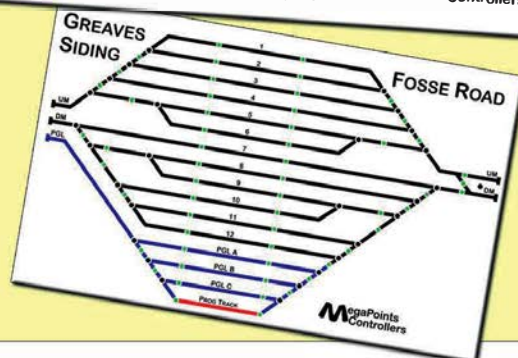
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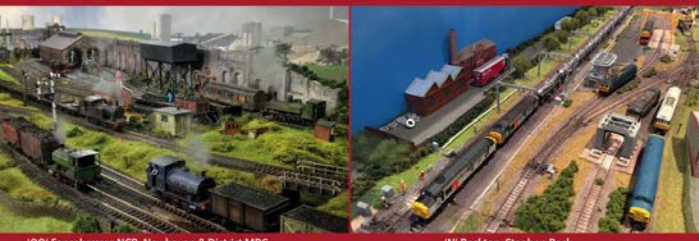


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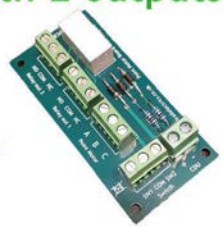


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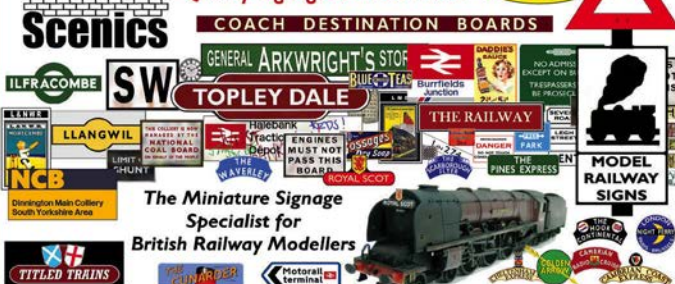


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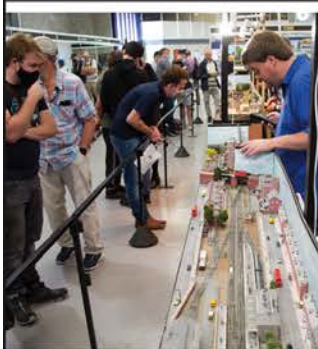
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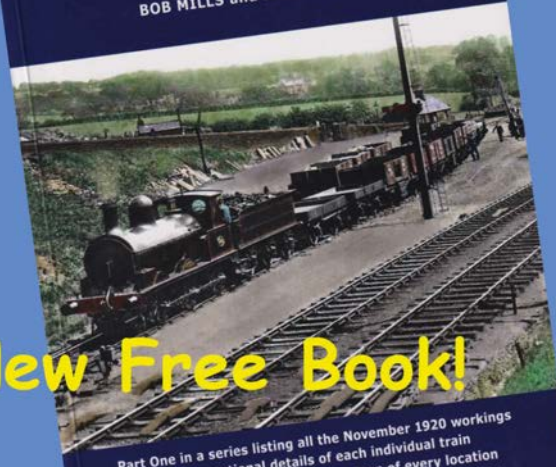
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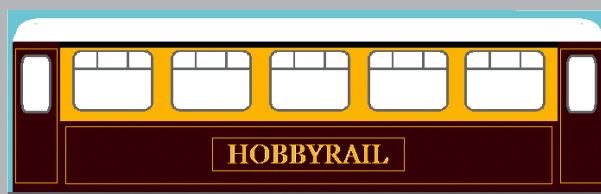
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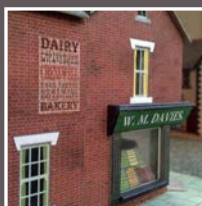
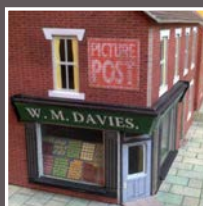
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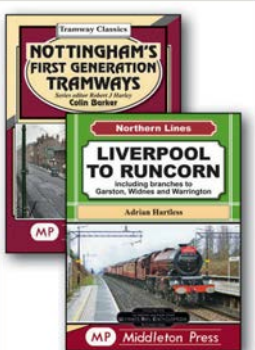
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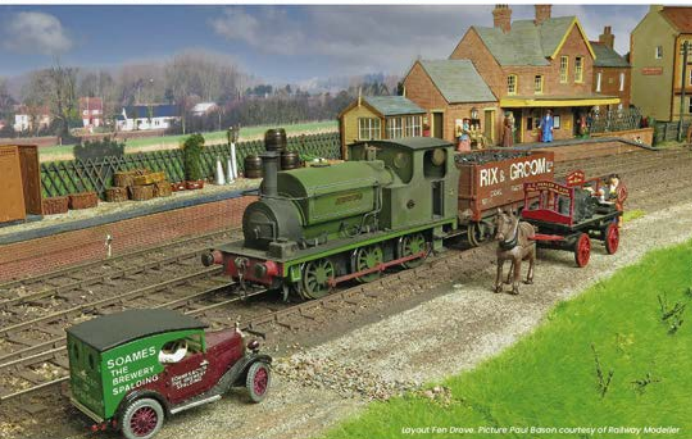
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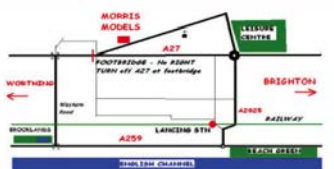
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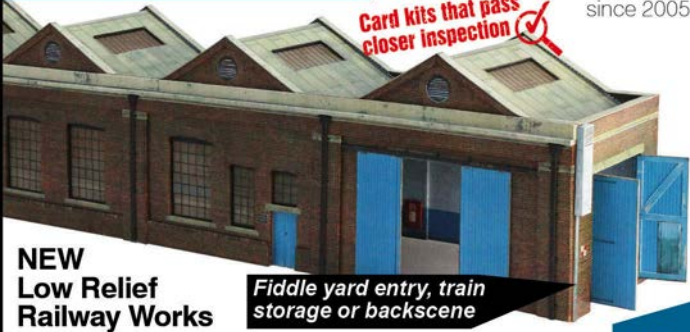
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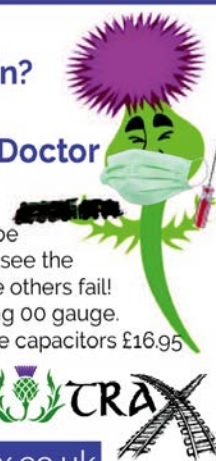
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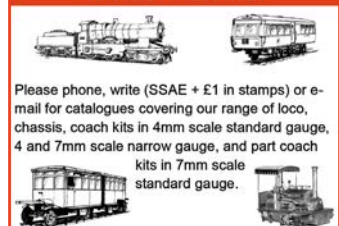


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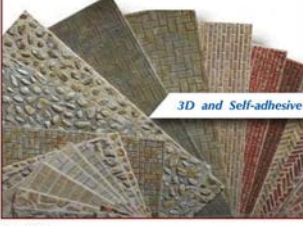
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Buffer Stop

A weird day at work

Do you know how much muck a 10 ¼ inch gauge steam locomotive can chuck out of its chimney?

Phil and Howard found out the hard way recently. Balanced on a flat wagon, hauled by said locomotive, they soon found themselves covered in a mix of oil, water and smoke. Halfway around the line, there was a stop so they could wipe the emulsion from the camera lenses.

This isn't the sort of thing any of us thought we'd spend a Friday morning doing a few years ago, but times change. Now, our focus is online, while still producing magazines. We produce a lot of videos for World of Railways, and World of Railways Plus members get even more, too!

It all started in 2014, when we put the first DVD (remember them?) on the front of BRM. On the first disk, filmed in a make-shift studio, we built models and tried not to look like idiots on camera. Now, we get to go out and about – we still look like idiots, though.

Taking you behind the scenes or to places you wouldn't ordinarily go is interesting. We've met many fantastic people this way, and are keen to meet many more. If you're involved with a railway organisation, be it real or model, and you'd like a visit, give us a shout. With a bit of luck, we'll even have managed to wash the soot out of our official sweatshirts by then!

You can find our Stapleford Park miniature railway adventures, and loads more on WOR TV, free to BRM subscribers, with plenty more exciting content on WOR Plus: www.world-of-railways.co.uk.

Howard and Phil have been inhaling steam locomotive smells at Stapleford Park miniature railway. What's your favourite smell?

Andy: Bacon, fried in dripping!

Debbie: A traditional fish and chip shop!

Howard: For once, it's everyone else talking about food! I like that fresh smell after rainfall in summer.

Phil: I like the smell of steam engines, but freshly baked bread wins out.

Ruth: Freshly cut grass for me!



This month in pictures...



Debbie and her family had a great weekend down in Kent and couldn't resist a visit to the Romney, Hythe and Dymchurch Railway.



Phil's 'Layout in a day' went down a storm at the National Garden Railway Show. Sadly, the beer was only borrowed.



Howard watches Curwen 4-4-2 John H Gretton raise steam at Stapleford Park – see more on WOR TV, with bonus videos on WOR Plus.



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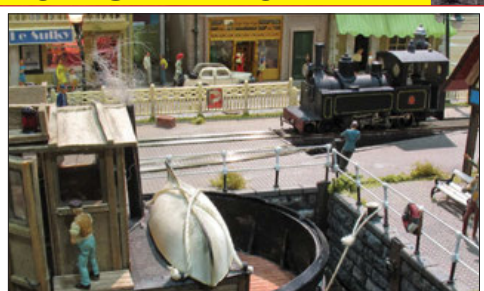
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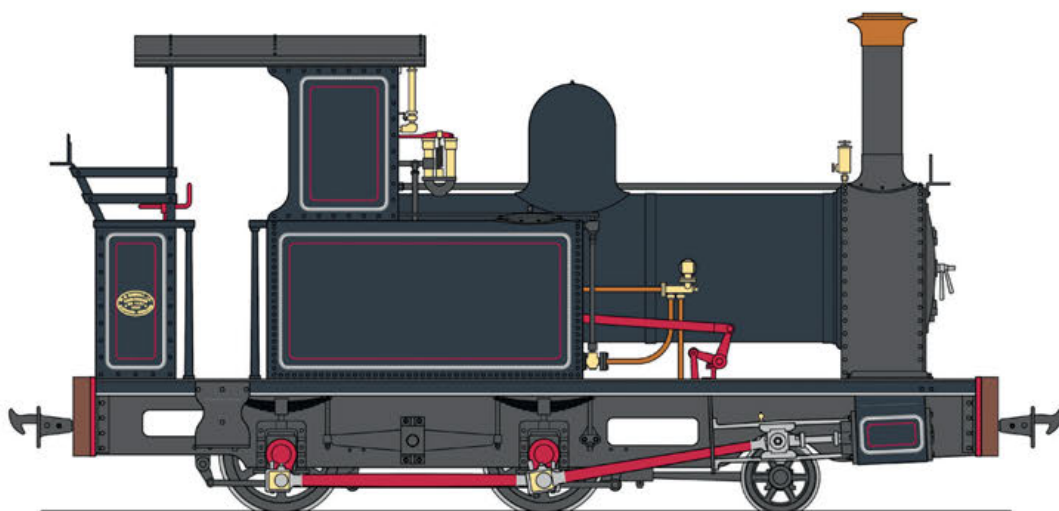


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The model is gauge-adjustable (32mm and 45mm), gas-fired and fitted with a water top-up valve, water gauge with blow-down valve, lubricator and pressure gauge; constructed of stainless steel and etched brass with a copper boiler, the model will, as usual, be covered by our two-year warranty. The anticipated pre-order RRP is £1695.00 (subject to the usual provisos), available late 2022 or early 2023. Fitted with Accucraft chopper couplings, the model will be supplied with two chimneys, the original balloon stack and the more traditional type. Available in five liveries, full details can be found on the Accucraft UK website.



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Cover: Three sisters together, but briefly – the Sittingbourne & Kemsley Light Railway staged a Brazil bonanza on 21st May when its Kerr, Stuart 0-4-2ST 'Premier' returned to service after overhaul. The inset picture shows the Gateshead Garden Festival Railway, a 15-inch gauge line that ran for only five months but carried more passengers than many lines do in years...

Photos: Andrew Charman & Donald Brooks

NARROW GAUGE WORLD is published nine times per year.

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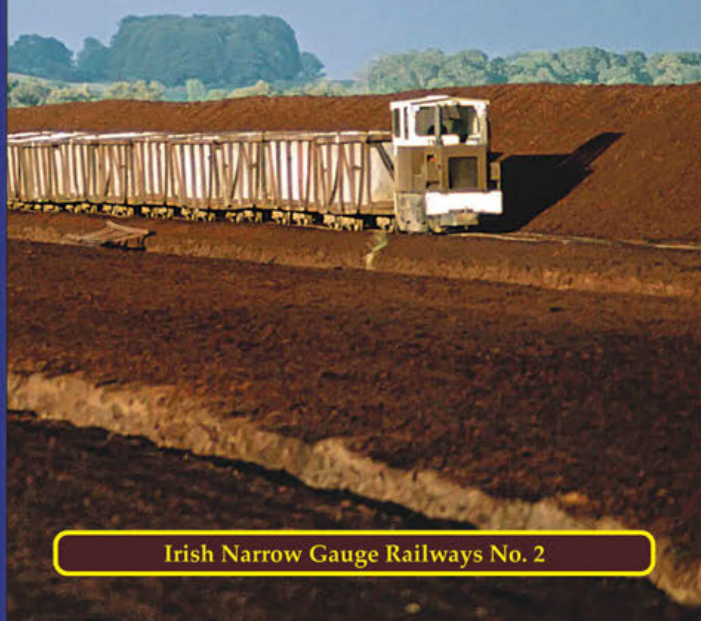
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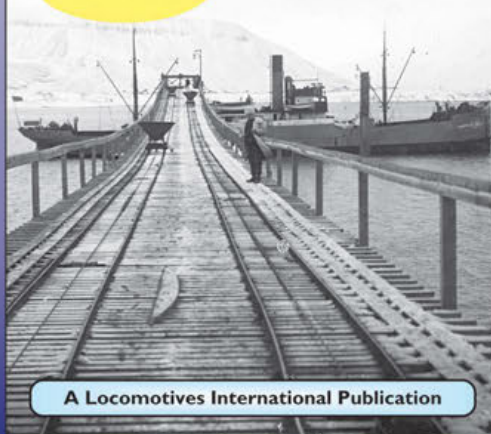


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Oh what an atmosphere...



"I replied I'd actually be more than 230 miles further east..."

Photo: Returned loco, original look, and still an industrial location. **Sjors van Dongen** pictured Brazil 0-4-2ST 'Premier' out in the country during the loco's first weekend back in service after a long-awaited overhaul on 21st-22nd May.

Some days I just love my job editing this magazine (well to be honest even when trying to send final pages in the early hours of the morning I love my job!). A prime example is the lead feature in this month's *NGW* 167, which also features on our cover.

Going into the weekend of 21st-22nd May I had many fellow enthusiasts saying, "See you at the Welshpool & Llanfair's Beyer Bash," and reacting with some surprise when I replied I'd actually be more than 230 miles further east.

Back last year I'd been told by the Fuller family, stalwarts of the Sittingbourne & Kemsley Light Railway, to keep the weekend free for the return of the line's Kerr, Stuart Brazil 0-4-2ST 'Premier' to service after quarter of a century on the sidelines. It had to be this weekend, because fellow Brazil 'Leader' had a boiler ticket that was about to expire, so this would be the only chance for a good while to have three of the highly recognisable class of locos in steam together.

So it was a good news story, but I definitely don't need such a hook to attract me to the Sittingbourne & Kemsley. Okay, this line doesn't

run through dramatic, even attractive scenery. Its locomotives and particularly its rolling stock are not exactly classically elegant examples of the golden era of British engineering, but instead spartan designs that were created solely to do a job of work in industry. And as you will read in my feature on the Three Brazils Gala, this is also a line that has left me with a streaming nose in the past...

Yet, the S&K is also a line that oozes its own atmosphere, one that feels very authentic, what those of us who were born too late imagine proper industrial narrow gauge lines looked like in their declining years. How accurate that image is, well only our elders know, but I'll keep on going back to Sittingbourne – even if it is a very long day trip...

Common sense on coal?

We are pleased to report in this issue that railways can now order proper Welsh steam coal again, following the repair of Fros-y-Fran colliery's washing and grading equipment. The coal is of course now more expensive and being able to get it is potentially only a temporary reprieve from the issues facing our railways

over future fuel, but it is a start.

Is, however, the narrative changing? For a long time the common-sense message, that at a time we still need coal it is far better to dig it out of UK mines than transport it halfway across the world in emissions-pumping ships, has been lost on politicians desperate not to upset the green lobby.

Now, however, one or two of these politicians are being brave enough to suggest in public that allowing further coal extraction might not be such a retrograde move as we have been told.

Summer stay home

Summer is approaching, the railways are into their busiest period, and hoping after the struggles of the last two years it really will be busy.

So, as we see all over news the troubles at airports as travellers see their holiday excitement dissipated by hours sitting in queues hoping their plane won't be cancelled, why not – not bother? Have a UK holiday, and make sure it includes lots of visits to railways. You know it makes sense!

But seriously – enjoy your *NGW*.
Andrew Charman



Welsh coal supplies resume – but possibly only for 2022...

Heritage railways both narrow and standard gauge have welcomed the resumption of the availability of Welsh Steam Coal from Fros-y-Fran colliery, but with a warning that it is only a short-term fix to the coal supplies issue.

As reported in previous editions of *NGW*, the south Wales mine – the UK's last remaining pit supplying steam coal – caused a major panic on 28th January when it suddenly ceased all supplies following a breakdown in its washing and grading plant.

With the mine already due to close in November, it was concluded that repairing the equipment was not economically viable. Railways were forced to seek alternative supplies of coal at much greater cost, while stepping up trials of potential sustainable substitutes to coal.

The situation was further aggravated by the Russian invasion of Ukraine, Russia being a major exporter of steam coal.

Now, however, the Fros-y-Fran equipment has been repaired, the mine working with Hargreaves Ltd which has an exclusive contract to supply the lump coal to the UK market.

Mining will still end in November, however, when the final two areas of the current operation are exhausted and with the loss of 150 jobs, unless Fros-y-Fran is

successful in its application for a two-year extension. Most of the 350,000 tonnes of output that will be produced in this period will go to steel plants in Port Talbot.

The resumption of supplies has also not eased the financial pressures on railways – increasing costs, particularly for the fuel used to power the mine's equipment, is forcing up the price of the coal.

On 25th May the Heritage Railway Association (HRA), together with the National Traction Engine Trust and the Heritage Fuels Alliance met with mine representatives to discuss the situation in what was described as a very useful visit.

The HRA and seven representatives of Welsh railways had already taken their case to the Welsh Parliament on 17th May, meeting Senedd members to discuss the problems over continuing coal supplies and the major role the railways play in Welsh tourism – there are 19 heritage lines in Wales welcoming around 1.2 million visitors and generating £52 million a year for the Welsh economy while across the UK the figure is nearer £600 million.

The Welsh Government will make the decision on extending the Fros-y-Fran licence – hopes have been raised after the Welsh Government climate minister Julie James told the BBC Wales *Politics Show* on 15th May that it was important for some

coal mining to continue.

Meanwhile further trials have taken place with another possible substitute sustainable fuel, both the Tallyllyn and Fairbourne Railways testing biomass briquettes from Phoenix Speciality Oils Ltd, made from the waste produced by rapeseed oil production.

The Fairbourne first tried out the fuel late last year, but the briquettes, which are an odd green colour, produced unacceptable levels of smoke and smell.

It was later discovered the batch used had been in store for some time and Fairbourne general manager Murray Dodds reported that the new batch had yielded more promising results. "This lot is fresh, and we've learnt to store it dry," he said.

The Tallyllyn Railway also held a successful trial with the briquettes using its 0-4-2T 'Tom Rolt'. Engineering manager Chris Smith reported plenty of steam and a lack of clinker or ash.

NGW has been in contact with Phoenix and we hope to bring readers a more detailed insight on this fuel soon.

Above: Fairbourne Railway driver Tom Curtis seemed happy to be trying out a Rapeseed-based coal substitute in early June, even though the actual briquettes look anything but traditional... Photo: Murray Dodds/FR

■ Just as we were about to send the final news pages of this issue to press we received pictures from *Alan Poxon* of Adrian Shooter's Daerjeeling Himalayan Sharp-Stewart 0-4-0ST no 19B, normally resident at his private Beeches Light Railway in Oxfordshire, in action at the Statfold Barn Railway's 'Sweet Indian Steam' Gala, which was held over the weekend of 11th-12th June.

We expect to carry more coverage from the Statfold Gala in our next issue.





■ The Welshpool & Llanfair Light Railway celebrated the 120th anniversary of its two original locomotives on 20th-22nd May with an unprecedented gathering of other locomotives produced by Beyer, Peacock at its Gorton, Manchester works.

Welshpool Raven Square station played host to four gauges – 2ft 6in gauge 'The Earl' worked passenger services (seen here poking out behind the signal box) while its out-of-ticket sister 'The Countess' was displayed close to the 2ft gauge pioneer Garratt today owned by the Ffestiniog & Welsh Highland Railway and resident at Statfold, 3ft gauge Isle of Man Railway 2-4-OT 'Fenella' (3610/1894) and two standard-gauge locos, former Gorton works shunter no 1827 of 1879 from the Foxfield Railway, and steam tram 'John Bull' (2464/1885) which lives at Crich Tramway Museum. The 0-4-0ST was displayed in steam, running on temporary track. *Photo: Sjors van Dongen*

Taiwan diesel heading for Llanfair?

The Welshpool & Llanfair Light Railway hopes to welcome a substantial new member to its motive-power fleet this winter, in the form of a Mitsubishi-built diesel loco from Taiwan.

The plan, which would see the 1972-built loco placed on a three-year loan to the W&LLR, is the latest evidence of an increasingly successful relationship between the Welsh 2ft 6in gauge line and the engine's owners, the Alishan Forest Railway.

DL-34 weighs 25 tons, is rated at 520hp and described as one of the most powerful locomotives built for 2ft 6in gauge service.

It was designed to take loaded trains up gradients as steep as 1 in 16 on the famed Taiwan line, and on curves of similar sharp radius to those on the W&LLR. Its power will make it capable of being able to rescue a failed steam locomotive and its train from any section of the Welsh line.

Putting the loco into service will enable the W&LLR to take both its existing main-line diesels, Drewry 'Chattenden' and Diema TSC175 – the latter built for work

in Taiwan – out of service for long-needed heavy overhauls.

The Alishan is best-known for its iconic Shay steam locomotives and announcing the planned loan W&LLR company secretary Michael Reilly, who as a former diplomat in Taiwan led the project, admitted that romantics amongst the line's members might have preferred to see a Shay on loan instead, but added; "DL-34 is a much more practical proposition."

A 'Sister Railway Agreement' was agreed between the W&LLR and the Alishan in 2017 and has since seen much co-operation between the two, including visiting delegations in both directions, information exchanges and an exhibition about the W&LLR at the Taiwanese line.

W&LLR chairman Steve Clews said that the line was most grateful to the Alishan and the Taiwan Forestry Bureau, which runs the AFR. "DL-34 will be a welcome and valuable addition to our roster," he said. "We hope once the pandemic has receded sufficiently

Taiwanese visitors will be able to come and see the loco for themselves."

While the W&L and AFR have agreed details of the loan, however, **NGW** understands that there are still major obstacles to overcome before the loco appears in Wales, principally the cost and current lack of availability of shipping DL-34 halfway round the globe.

The Alishan initiative led to a second Sister Railway Agreement between the W&LLR and the Taiwan Sugar Corporation, former home of Diema loco TSC 175 and which now runs five heritage lines. This culminated in the Welsh line's Barclay 0-4-0 'Doug' visiting Taiwan as a guest at TSC's annual Sugar Festival in December 2018.

Photo: Heading for Wales? DL-34 pictured on test in Taiwan following a recent overhaul.

Photo: AFR

Peter Pan to pump museum

Well-travelled Ker Stuart Wren 0-4-0ST 'Peter Pan' will be adding yet another line to its resume on 29th August when it appears at the annual Gala of the Westonzoyleand Pumping Museum Light Railway.

The loco, owned by Graham Morris and celebrating its centenary in 2022, will be hauling passenger trains on the 2ft gauge line, the first time a steam locomotive has run at the museum.

The line traverses the Grade II-listed pumping station in Bridgewater, Somerset, which houses one of the largest collections of stationary steam engines and pumps in the UK.

More details of the Gala will be on the museum website at www.wzlet.org

PDF NEWSPAPERS and MAGAZINES: WWW.XSAVA.XYZ



Russell to return for WHR Centenary celebration



The only surviving original Welsh Highland Railway locomotive, Hunslet 2-6-2T 'Russell' is to return to its former home at the end of the month as a year of celebrations marking the centenary of the WHR begin.

The loco, now owned by the Welsh Highland Heritage Railway, has already been back to the WHR – the revived line runs alongside the WHHR's Gelert Farm, Porthmadog base on what was once the WHR's Beddgelert transhipment siding.

In 2019 the loco ran on the section of WHR from the WHHR's Pen y Mount Halt to Beddgelert. On 30th/31st July this year it will work a series of heritage trains between Dinas and Rhyd Ddu – a first in preservation and 100 years after the first

original Welsh Highland Railway trains ran over this section.

Russell was built in 1906 for the Porthmadog, Beddgelert & South Snowdon Railway. This line was never completed and the loco was taken into the stock of the North Wales Narrow Gauge Railway, running between Dinas, near Caernarfon, and South Snowdon – today's Rhyd Ddu.

In 1922 a new line was built from Porthmadog to connect with the NWNGR at South Snowdon, the whole line then becoming the Welsh Highland Railway. The WHR closed in 1937 but was rebuilt by the Ffestiniog Railway between 1997 and 2011.

The trains at the end of July will be the first in a series of events telling the story of

the WHR, jointly organised by the Ffestiniog Railway Company, Welsh Highland Heritage Railway, the Welsh Highland Railway Society and the Welsh Highland Railway Heritage Group.

The year will culminate with an event on 23rd-25th June next year, celebrating the opening through to Porthmadog. This will be based at the southern end of the railway and is set to include all three railways and some of the original locomotives that still operate on the line.

Above: Russell and an authentic Welsh Highland Railway heritage train cross the river at Pont Croesor during the locomotive's last run on its former home line in 2019.

Photo: Chris Parry/FFWHR



ENGINE, SNOWDON RAILWAY TRAIN, AT LLANBERIS STATION



Schoolchildren join party for Snowdon's Padarn

The Snowdon Mountain Railway has marked the 100th birthday of its locomotive no 6 'Padarn' with a trip for local schoolchildren.

The loco was one of the second batch of engines built by the Swiss Locomotive & Manufacturing Co for the 800mm gauge rack line, costing £3,000 (equivalent to around £120,000 today) and entering service in 1922.

Originally the loco was named 'Sir Harmood' after the then chairman of the SMR, Sir John Sutherland Harmood-Banner. Its name was changed in 1928 to that of the lake adjacent to the railway's home town of Llanberis, Llyn Padarn.

To celebrate the centenary schoolchildren from Ysgol Dolbadarn in Llanberis were taken for a trip up the mountain to the 2022 terminus of Clogwyn, powered by Padarn.

SMR commercial manager Vince Hughes said that for many of the children it was their first ride on the line and they thoroughly enjoyed the journey up the mountain to Clogwyn.

"The fact that this Swiss locomotive is still operational after 100 years of service can only make you appreciate how well the Victorians constructed them – out of the seven original locomotives the railway purchased three are still in operation today," he added.

Left: SMR no 6 'Padarn' pictured shortly after delivery to the Snowdon Mountain Railway in 1922, and on its 100th birthday celebrated with local schoolchildren.

Photos: SMR



Talyllyn celebrates its gold-hearted members

The Talyllyn Railway ran its annual Founder's Day train on 14th May with travellers including members of the pioneer preserved line's 'Heart of Gold' group.

The group takes its name from the railway's nickname, which itself comes from the name of a film released in 1965 and mostly shot in 1953 in the early days of the Preservation Society.

Heart of Gold Group members make a regular monthly donation to the Society and receive exclusive benefits including the invitation to travel on the Founders' Day train and a special edition of the Talyllyn Railway enamel badge.

TRPS president David Mitchell sent the train off by cutting a ceremonial tape, using the same ribbon first employed 71 years ago when the Talyllyn reopened as a heritage line.

The double-headed train was hauled by the two former Corris Railway 0-4-2STs no 3 'Sir Haydn' and no 4 'Edward Thomas'. The trip included a pause for no 4's refreshment at the recently restored original watering point of Ty Dwr where the loco took water.

TRPS chairman, Jonathan Mann, said that it was wonderful to be able to celebrate Founders' Day in a more normal manner this year. "A good number of members attended, including one gentleman who had with him a photograph taken when he was a young boy, standing in the Guard's van on the Railway in

1947 – this scene was quickly recreated with him standing once again in the van for a photograph, much to his delight."

Later this year the TR plans to run a number of special trains available to anyone interested in learning more about the slate heritage of the line.

On the 'Pioneer's Train', an experienced guide will travel throughout the journey, pointing out interesting features and answering questions. Details of these events will be on the website at www.talyllyn.co.uk

Above: The recreated watering point of Ty Dwr proved a centre of interest.

Below: TRPS president David Mitchell recreated the opening ceremony with the same ribbon that was used in 1951.

Photos: TR



■ Several railways took the opportunity to adorn their locos in suitably celebratory front ends over the period of the Queen's Diamond Jubilee holiday – **Matthew Pye** caught the Ravenglass & Eskdale Railway's Krauss Pacific 'Whillan Beck' at Dalegarth on Friday 3rd June.



BRIEF LINES

More land for L&B

Lynton & Barnstaple Railway sister organisation Exmoor Associates has announced the purchase of another section of trackbed for the line's revival. The 489-metre long section, known as Budd's Wood, is adjacent to the Chelfham viaduct.

Ruston ready for Exmoor

Restoration of Ruston 22/28HP diesel 179880 of 1936, which spent many years on display at the Lynton & Barnstaple Railway's former Lynton station (NGW160) has now been completed and the loco has been gifted to Exmoor Associates ready to play a role in future extension of the L&BR.

New carriage at Old kiln

The Old Kiln Railway has taken delivery of a new bogie carriage. Built for the Ramsgate Tunnel Railway, which operated between 1936 and 1965 and was to the same 2ft gauge as the Old Kiln, the carriage has had a new body built by the Moors Valley Railway. It will need some work before entering service on the Surrey line.

Fowler boiler progress

Latest progress in the restoration of 1924-built Fowler 0-4-2 no 16341 at 'Workshop X' in Killamarsh has seen corroded sections of the boiler cut out and new plated sections made ready for welding in place. Owned by the proprietor of The Steam Workshop model engineers, the loco was repatriated from Australia by Jeremy Martin of the Richmond Light Railway and sold to the current owner in 2019.

Boiler lifts aplenty

Further activity at Workshop X in recent weeks has seen the boilers of both 3ft gauge Hudswell-Clarke 0-4-0ST 'Handyman' which is being cosmetically restored for the Statfold Narrow Gauge Trust, and Orenstein & Koppell Luttermöller 0-10-0T 10956 lifted from their frames. The O&K's boiler proved a particular challenge, the Killamarsh team discovering it was both riveted and welded to the frames...

For Statfold read Amerton

Our editor had complete brain fade in our last issue, not once but twice describing the Bala 50 event's visiting Hunslet Wren 0-4-0ST 'Jennie' as a Statfold resident, when of course he meant the Amerton Railway. He profusely apologises...



Manx lines gear up for Summer Festival

The railways and tramways of the Isle of Man will be signalling a proper return to normality at the end of July by staging a five-day Summer Transport Festival.

All five of the currently operating island lines will be taking part in the event between 27th-31st July though the main action will be on the Steam Railway and the Manx Electric Railway.

Highlights on the former include the return to service of newly overhauled Beyer, Peacock 2-4-0T 'Maitland', a night event at the Port Erin Railway Museum, tours of the running shed and signal box at Douglas station, and pie & mash and Marrakesh dining trains. The popular closed lines bus tour will also feature during the week.

The Manx Electric Railway will operate several parallel runs on its double track, a number of unusual tram and trailer combinations, tours of the car sheds at

Derby Castle and Laxey, and even the chance to try one's hand as a motorman.

There will also be a rare opportunity to see inside the Snaefell Mountain Railway depot at Laxey, while both the Groudle Glen Railway and the Great Laxey Mines Railway will also be getting involved in the festivities, the Groudle staging a Victorian Saturday and Gala on Sunday.

Full details of the Festival, with all the events listed, are in a brochure which can be downloaded in PDF form from the Events & Experiences section of the www.rail.im website.

Above: new-build Bagnall 'Brown Bear' waits to pull a special train on the Groudle Glen Railway during the Queen's Platinum Jubilee celebrations on 5th June. And the loco will be playing its part in a summer celebration on the island at the end of this month. Photo: GGR



Annie gets an overhaul and a new owner

The Bagnall-style 0-4-2T 'Annie' has been transported from its home at the Groudle Glen Railway to John Fowler Engineering in Cumbria to undergo a major overhaul.

The 2ft gauge loco was built in 1998 by Groudle Glen stalwart Richard Booth, to the design of a locomotive that Bagnall supplied to the Gentle Annis Tramway in New Zealand in 1922.

Annie has been out of service for some time awaiting major work that has now been made possible by a significant donation made by the Williamson family.

The funding has enabled a new boiler to the original design to be ordered from Bennett Boilers in Somerset. A new cast pony truck has been ordered while the overhaul will also include work to the locomotive's motion and new side tanks.

Meanwhile Richard Booth has told *NGW* that he has recently given Annie to the Sea Lion Locomotive Trust. A separate concern to the Groudle Glen operating company and a registered charity, the Trust also owns original GGR Bagnall 2-4-0T 'Sea Lion' and Baguley diesel 'Maltby'.

Above: 'Annie' touches down at John Fowler Engineering ahead of its overhaul.

Photo: Jack Dibnah/JFE

Below: The patterns used to cast the trailing truck for 'Brown Bear', pictured here, will be re-used on a new truck for Annie.

Photo: Richard Booth

■ Among lines back in operation this season is that at the Threlkeld Quarry Museum in Cumbria. James Kindred caught Bagnall 0-4-0ST 'Sir Tom' in less than glorious conditions on 31st May.



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Sisters reunited at Evesham

The annual Steam Gala of the Evesham Vale Light Railway will take on additional significance this year as the Worcestershire 15-inch gauge line celebrates its 20th anniversary.

The event on 9th-10th July will welcome two visiting locos, 0-6-2T 'Powys' from the Rhiw Valley Railway in mid Wales and 'Bonnie Dundee' from the Cleethorpes Coast Light Railway.

Powys, a David Curwen design built by Severn Lamb in 1972 has never previously left its Welsh home and at the Gala will be reunited with sister Severn Lamb loco 'Dougal', resident at Evesham.

Bonnie Dundee started life as a 2ft gauge 0-4-0WT built by Kerr Stuart in 1900 for Dundee gasworks. When the gasworks closed in 1959 it was sold into

private ownership and then in 1976 went to the Ravensglass & Eskdale Railway for restoration, during which the loco was regauged and side tanks added.

A further rebuild in 1996 saw the loco adopt its current form as an 0-4-2 with tender. It is now on long-term loan to the Cleethorpes line and will be having its first outing since the completion of a recent overhaul by John Fowler Engineering at Old Hall Farm in Bouth.

The Gala will feature an intensive service on both days with a variety of different locomotives hauling trains.

Rover tickets will be available on both days and as part of the anniversary 20 complimentary tickets will be placed in the Adult ticket rack on each day.

Photo of 'Powys' above by Andrew Charman

Jubilee first for the Queen Mary

An almost 100-year old carriage carried its first-ever fare-paying passengers over the Jubilee weekend of 4th-5th June.

As reported in *NGW* 164, the Lincolnshire Coast Light Railway has been restoring the 'Queen Mary' carriage, to increase train capacity and help with a short platform at one of its stations.

The 2ft gauge vehicle was built originally for the Nocton Estates Railway in either 1926 to 1927 to transport guests

at shooting parties held on the estate.

The vehicle got its name from estate workers, reputedly as a result of the haughty manner of some of the guests who were transported in it!

So many visitors wanted to travel on the LCLR over the holiday weekend that on the second day it had to operate a three-carriage train, the first time this has been necessary since the line moved to Skegness Water Park in 2009.



■ Green for go: The Rhyl Miniature Railway has repainted its 1921-built Barnes Atlantic 'Railway Queen' into the colours the 15-inch gauge locomotive wore when it ran at the Belle Vue Railway in Manchester from 1928 until the 1980s.

Photo: Justin Bell/RMR



Back on track at Fintown

The Fintown Railway re-opened for the first time since the Covid Pandemic on 15th June (*writes Hugh Dougherty*).

Former County Donegal Railways railcar no 18 is now running on the 3ft gauge line five days a week (not Mondays or Saturdays) until mid-September.

The Gardner-engined railcar 18, built by Walkers of Wigan and the Great Northern Railway of Ireland for the CDR in 1940, has been on loan to the railway by its owners, the North West of Ireland Railway Society, since Easter 2003, and will mark 20 years at Fintown in 2023.

The line's Seamus McElwee said that Fintown staff were delighted to be operating again this year and hoping for a good season having been badly hit by the Covid lockdowns.

"Our local credit union has been very kind and that's allowed us to run again this summer, but we need as many people as possible to visit us and to ride in a genuine Donegal railcar over our three-mile section of the Glenties line, amid superb scenery, to bring in much-needed revenue to ensure our future survival," Seamus added.

Full details of train times are at www.facebook.com/fintownrailway

Photo: Back on track for 2022: Railcar 18 arrives at Fintown *Photo: Hugh Dougherty*

■ Just as we closed for press news came through that the Cavan and Leitrim Railway had gained permission to operate passenger services on its line at Dromod, following proactive engagement with the Commission for Rail Regulation. C&L spokesman Darragh Connolly described the decision as "a momentous occasion in the history of the preserved railway."

Members of the McInerney, Pentony and Cleary families were the first groups to board the train service at Dromod on 4th June and the railway is now open at weekends. We will have more on this progress in our next issue.



■ The Fairbourne Railway staged a successful annual Gala over the weekend of 28th-30th May. Diligent work by the railway's engineering department in finishing an axle and crank replacement on Darjeeling 0-4-0ST 'Sherpa' ensured it was able to share duties on the 12.25-inch gauge line with fellow residents 2-6-4T 'Russell' and 2-6-2T 'Yeo'.

Exmoor Steam Railway-built 'Christopher' (right), visiting from the Littlehampton Miniature Railway, also ran on the main line while two more Exmoor-built locos – 'Monty' (below) from the Evesham Vale Light Railway and 'Anne' (below right) from the Perrygrove Railway, topped and tailed on the 15-inch gauge demonstration line.

Photos: Andrew Charman



Right: When Ratty met Romney – not for the first time a Romney, Hythe & Dymchurch Railway Gala featured visitors from fellow 15-inch gauge line the Ravenglass & Eskdale Railway. At New Romney during the 95th Anniversary Gala on 15th May the Cumbrian line's diesel 'Shelagh of Eskdale', which has been helping out in Kent for some time, and Heywood 0-8-2 'River Irt' lined up with residents Krupp 4-6-2 'Black Prince', Davey Paxman 4-8-2 'Samson' and Krauss 0-4-0 'The Bug'.



Right: Typical of such events, the RH&DR ran a 'Cumbrian Mountain Express' featuring none of its Cumbrian visitors! Samson, Black Prince and between them the reversed Yorkshire Engine Co 4-6-2 'Winston Churchill' did the duties.

Photos: Steve Town – see also Gallery pages



Froissy 50th – better late than never...



The Chemin de fer Froissy-Dompierre, run by APPEVA (Association Picarde pour la Préservation et l'Entretien des Véhicules Anciens) finally got to stage its delayed 50th anniversary gala over the weekend of the 26th-29th May.

The group had actually clocked up half a century since the first heritage trains ran on the line in 1921, but the Covid pandemic prevented the Gala being held last year.

Located in the Somme Department of France, between Paris and Lille, the 600mm gauge line is the last survivor of the many miles of trench railways that were built in the First World War to service both Allied and German front lines.

FRANCE

The railway dates from 1915 when the French Army built a line along the Somme canal – Froissy provided an interchange with the metre-gauge Réseau Albert line. From 1916 to 1918 the line was on the Allied front line with 1500 tonnes of arms and provisions passing over it each day.

Following the war the line remained and was initially used for reconstruction and later for transporting sugar beet, being extended more than once with a zig-zag added to gain height to the Santerre Plateau. It continued working until 1972, by which time the two extended sections to

Chaulnes and Péronne had been removed.

APPEVA had been formed in 1970 with the aim of preserving a narrow-gauge line and soon settled on Froissy. The first APPEVA train ran in June 1971 over a kilometre of route from Froissy to Cappy.

The line to the top of the zig-zag was reopened by 1974 and two years later to Dompierre, reaching the 4.3 miles that forms today's line. A new museum was opened in Froissy in 1996.

It is a highly interesting route – as well as the zig-zag and the roadside section to Dompierre, trains also negotiate a 200-metre long tunnel on a curve.

The Gala event saw several of the Froissy line's resident steam fleet in action and two visitors from the Pithiviers museum railway run by AMTP (Association du Musée des Transports de Pithiviers). These were a 1938-built La Meuse 2-6-0 that formerly worked at the Maizy sugar refinery, and a Henschel-built Brigadelok (1551/1917).

Attractions for visitors included passenger trains and a demonstration goods train with former sugar-beet wagons, harking back to the line's post-war history.

Top: Volunteers from Pithiviers pose with their La Meuse loco at Dompierre during the evening 'fun train'.

Above left: On the Friday, locos in service over the weekend gathered around the turntable at Froissy with their crews. From left to right are a Brigadelok (Henschel 1551/1917) from Pithiviers, Froissy residents Decauville 0-6-0 (1652/1916) and Coferna 0-6-0 diesel (1941) La Meuse 2-6-0 (1938) from Pithiviers, Vulcan 0-8-0 (3852/1925) and Borsig Brigadelok 'Genevieve' (10334/1918), both from Froissy.

Left: The visiting La Meuse and Brigadelok from Pithiviers at Cappy.

Photos: Charles Insley – Charles's 009 model railway with a French theme was displayed at the Gala and is featured from page 40.



■ The first full year of revived logging trains on the Zillertalbahn in AUSTRIA has proven a major success according to the line. We reported in *NGW* 157 how the trains, which had not run for eight years, had been reinstated in the hope of cutting road traffic. Timber specialist the Binderholz Group used the line to transport cut trees from Jenbach to its factory in Fügen for processing – the first year has seen 4,649 wagons of timber travelling in a total of 582 trains, representing 276,300 gross tonnes, or 19,000 truck journeys, removed from the area's road network. Photo: Zillertalbahn



Boost in tourism hastens upgrades to Matheran line

Longer trains and online booking services are being proposed for the Matheran Hill Railway near Mumbai, INDIA to boost post-Covid tourism.

Services over the full 21km long 2ft gauge line, which dates to 1901, have been suspended since 2020 following cyclone damage, with only the section from Matheran to the first intermediate station of Amman Point operating.

Reconstruction work is expected to be completed by the end of 2022 with the upgraded line shortening journey times by 30 minutes, while Indian Railways is also looking at a number of further improvement measures.

For the first time travellers will be able to book tickets for travel on the line through the national operator's central online booking system. This is being made possible by the laying of fibre-optic connecting the stations.

There are to be more train services to the Hill station at Matheran, while research is underway to determine whether trains can be lengthened from six to eight carriages. This is despite the Matheran line being a typical Indian hill railway with several sharp ascents, zig-zag reverses, cuttings and tunnels.

According to the *Mid-day* Matheran newspaper, representatives of Matheran Municipal Council met the divisional railway manager to request that the weekend service of 10 trains a day be extended throughout the week. "It is the only major means of transport and attraction and will help boost the economy," Mumbai council president Prerana Sawant said.

Railway officials reportedly assured the council that trials would take place to determine whether increasing the train length to eight carriages was practical.

Tram museum counts cost of Covid



The Museum of the Rotterdamsche

NETHERLANDS

Tramweg Maatschappij (RTM) at Ouddorp has become yet another organisation to feel the effects of the recent Covid pandemic, impacting its finances.

Paul Conlan, who visited the 1067mm gauge (3ft 6in) museum on 4th June, reports that work to build a Backer & Rueb replica tramway loco No 37 is progressing very slowly. "So far the cylinder block has been placed in the frames and the wheels, axles and boiler are ready but little else," Paul told *NGW*.

The loss of income from Covid closures has forced the RTM to prioritise funding for other projects including Orenstein & Koppel 0-6-0 tram locomotive no 56, built in 1920 and carriage no 397. The locomotive only needs

primer coating and painting and it is hoped it will enter service before the end of the 2022 operating season.

Also adding to the costs is an extension to the museum currently under construction and which when completed will allow more space for exhibits as well as improved toilet facilities for visitors. It is hoped that the work on the extension will be completed in 2023.

The RTM also hopes at some point to extend its running line from West Report to the village of Scharendijke in Schowen Duiveland but Paul adds that this is a project for the future.

Above: RTM M1805 'Meeuw' (Seagull) approaches West Report on the running line, with the Grevelingenmeer in the background. Port Zélande can be seen in the distance at the right.

Below: Carriage No 397 undergoing restoration in the RTM's workshops at De Punt.
Photos: Paul Conlan



Loco traumas but MTVS on track

Recent times at the Museum of French Steam Tramways and Secondary Railways (MTVS) at Crèvecœur-le-Grand, north of Paris, have seen ups and downs, according to the museum's *Olivier Janneau*.

The most significant 'down' involved the restoration of the museum's 1897-built Corpet-Louvet 0-6-0T no 691, which had been making good progress.

Much work had been done preparing the loco for its official hydraulic test, including a boiler retube, descaling and ultrasonic tests. But days before the arrival of the inspector a hydraulic test by the museum resulted in cracks appearing in the loco's firebox.

"This is the end of the story for the present," Olivier said. "The firebox will have to be repaired or replaced and there is no funding available for the moment."

Much better news has come from the Communauté de Communes de la Picardie Verte (CCPV), the local authority that owns the next section of the project's running line.

"The president came to our general meeting at the beginning of April, to announce that from 2022 the CCPV will give us a grant equivalent to three lorries of new sleepers enabling us to extend the track by 1.2 km per year."

Olivier added that the MTVS is now awaiting the signing of the agreement so that authorisation can be sought to permit rehabilitation of the line, a process that should take around four months.

"Then, from 2023, we will start to remove

FRANCE

the current track and replace it with metre-gauge track. Our next goal is Oudeuil station, which is currently walled in and 4.1km away – the station, owned by the CCPV, is to be rehabilitated and will be the temporary terminus of the train with a refreshment bar and local products, reception for tourists and information on local paths and bike rides."

Olivier added that the station will for the first time be equipped with a turntable, a water tower and a wagon weighbridge.

The MTVS has also recently received more items from Switzerland, including a track tamping machine it has been awaiting a year and which was sourced from the Yverdon/Sainte Croix railway.

In May a chassis formerly under a passenger carriage was delivered from the Swiss line between Nyon and Saint-Cergue, the culmination of a process started back in 2007.

"This was recovered mainly for its bogies, which will be fitted to a chassis that will be built to suit the Haute Saône carriage currently being restored.

"The whole will go under the body of the Jura car number 56 which was saved in 2007 – we will then have three carriages from the same manufacturer in three different versions, one in sheet metal (Vienne BB7), one in varnished wood (Haute Saône B5v), and the last, Jura no 56, in green wood.

Photo: MTVS trains are set to run a lot further in future with a further 4.12km route to Oudeuil now in the group's sights. Photo: Olivier Janneau/MTVS

Wagon workshop build progresses

Members of the Upsala-Lenna Järnväg (ULJ) (Upsala-Lenna Railway) in SWEDEN are making solid progress with the erection of a carriage and wagon workshop at their base in Faringe – the walls of the wooden building were erected before the end of May with the roof set to follow rapidly.

The 891mm line – a gauge unique to Sweden – runs for some 21 miles and is a surviving section of the once much more extensive Roslagen network.

Can't find a diesel? Shrink a standard...

The Wiscasset, Waterville and Farmington Railway Museum in Maine, USA, has taken delivery of a standard-gauge diesel locomotive, which it intends to rebuild and modify to the museum's 2ft gauge.

The diesel, a General Electric 45-ton unit built in 1941, was delivered to the museum's base at Sheepscot in mid-May and unloaded onto a specially constructed standard gauge track panel, having been acquired for "scrap price."

The museum has been searching for three years for a suitable locomotive to supplement its existing diesel; "We needed something to plough snow, pull work trains, handle trains on the Mountain Extension, and (perish the thought) rescue a stalled steam train."

The plan is to overhaul the two power bogies and rebuild them to 2ft gauge, create a new powertrain based on one of the loco's two diesel engines and traction motors, narrow the loco's width to fit the line and reduce its weight by around 10 tons, and fit snowploughs at either end.

There is also the possibility of at some point installing an alternative battery propulsion system.



■ Last month we featured an incredible one-man new-build project in South Australia, engineer Glenn Smythe constructing an example of the lost Z-class of 3ft 6in gauge 4-4-0 tender locos. Well Glenn has already been in touch with a photo update, in that the loco is now 'standing on its own two feet' after the second of the driving wheels was lifted into place. Of course we will keep readers informed of further progress on this inspiring project.

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Brazil in the Premier League

Andrew Charman heads to Kent for the long-awaited and yet brief reunion between three sister locomotives.

It was as I accepted the invitation to climb onto the footplate of the Sittingbourne & Kemsley Light Railway's Hudson Hunslet diesel workhorse 'Victor' (4182/1953) that I realised that never had I previously ridden a heritage railway train so early in the morning – even on my home Welshpool & Llanfair Light Railway.

It was Saturday 21st May at Sittingbourne Viaduct terminus in the town on the north Kent coast, and I was meeting the 7.30am train to Kemsley Down as one of the early visitors for a very special event. While many railways celebrate the return of a locomotive to steam after a long restoration, Kerr, Stuart 0-4-2ST 'Premier' was not only entering service for the first time in more than

quarter of a century, it was sharing duties with its two almost identical sisters 'Leader' and 'Melior'.

And this was indeed an event not likely to be repeated for many years. For as Premier made its return, Leader was working its last weekend before its boiler ticket expired – with the loco being third in the S&KLR's current overhaul queue it won't be back in action any time soon.

Premier has been restored to an appearance it would have presented when built in 1905 under works number 886. Appropriate – as one of the earliest examples of the Kerr, Stuart Brazil class, the locomotive became fleet number one of the brand-new 2ft 6in gauge railway built by Edward Lloyd to serve his equally

Above: The sister Brazils raise steam on the Saturday morning of the anniversary event. Left to right are 'Melior', 'Leader' and star of the show 'Premier'.

Below: The rebuild of Premier resulted in an appearance recalling its early days working on the paper mill line.

Photos by Andrew Charman except where stated, taken on Saturday 21st May.

new paper mill at Sittingbourne. Sister Kerr, Stuart Leader (926) arrived shortly after.

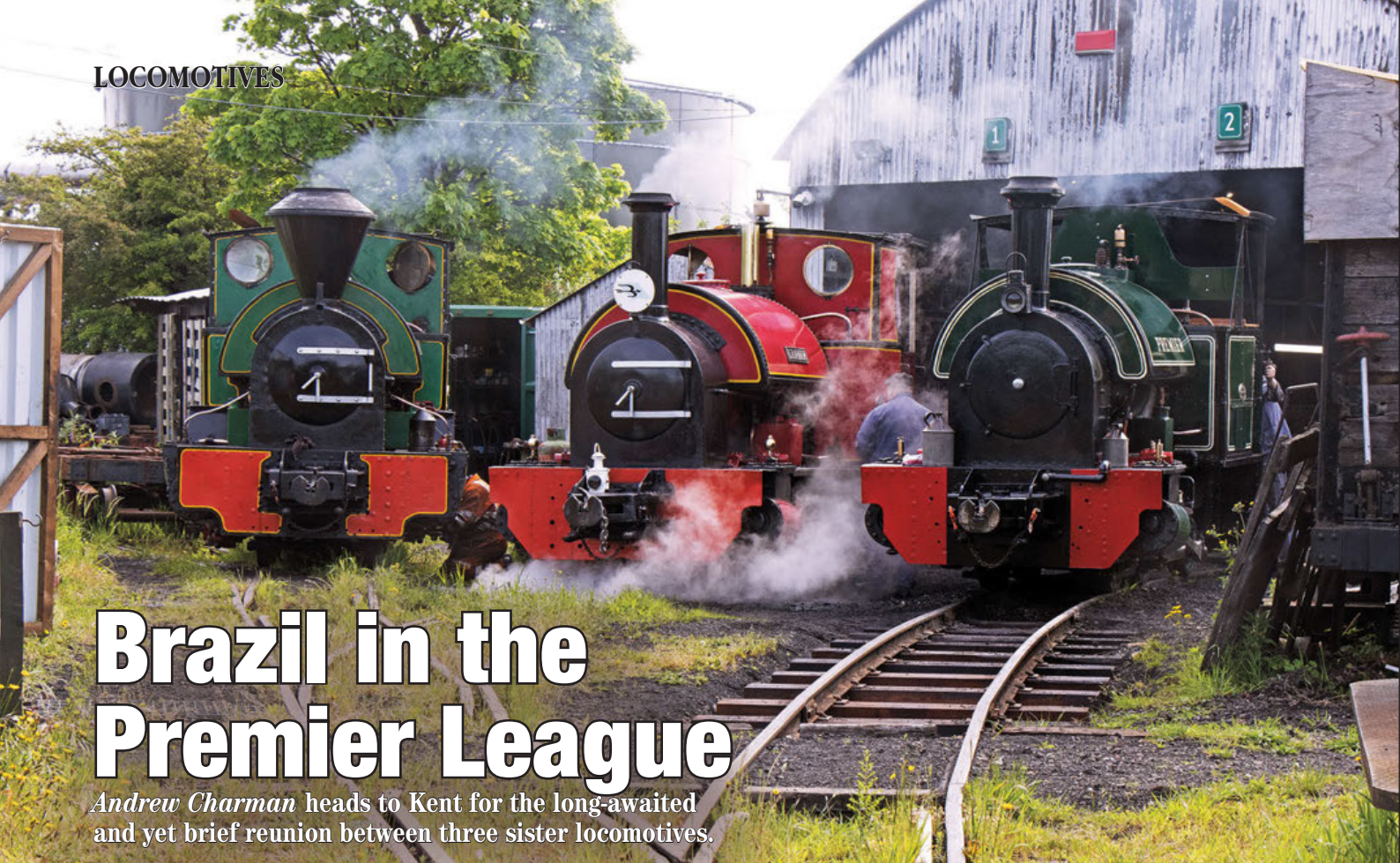
Doubling the fleet

The railway grew over the years as the paper complex expanded and in 1948 became part of the Bowaters Group. By the time a switch to road transport resulted in closure of the railway in the Autumn of 1969 there were two more Brazils in the fleet. 'Excelsior' (1049) had arrived in 1908 and 'Melior' (4219) in 1924 – the latter an improved version of the design with Hackworth motion.

By the time of closure efforts were already underway to preserve the southern section of the railway and the Sittingbourne & Kemsley Light Railway opened in 1970, initially operated by members of the Locomotive Club of Great Britain.

All four Brazils were preserved – Premier, Leader and Melior remained at Sittingbourne, while Excelsior was among four Bowaters locomotives sold to the Pleasurairail operation which was planning to build new lines at stately homes and other attractions. The four went to a line that was under construction at Whipsnade zoo in Bedfordshire, and three of them, including Excelsior, remain there today.

Back at Sittingbourne, Premier had remained in working order right up to the end of Bowaters operation and was heavily used in the early years of preservation, alongside the



Bagnall 0-6-2Ts 'Superb' and 'Triumph'. Leader, withdrawn in 1969, was acquired by a consortium of S&KLR members who restored the loco, while Melior, working but in poor condition at the end of the Bowaters era, needed heavy repairs and did not enter service on the preserved line until the mid 1980s.

Premier last steamed on the S&KLR in 1996. A decision was made in 2005 to try and seek a Heritage Lottery grant for an overhaul and the railway wrote to the local Swale Borough Council in the hope of getting a letter of support to aid the lottery bid. Instead the council sent a £10,000 donation. "That paid for the new saddle tank and effectively kick-started the restoration," S&KLR chairman Liz Fuller told *NGW*. Unsurprisingly the current Mayor of Swale, councillor Simon Clark, was among Saturday guests to the Three Brazils Weekend.

The railway was unsuccessful in its lottery bid and eventually raised the entire cost of the restoration, estimated at around £115,000, itself. Two major benefactors proved a big help, as did a pair of specific appeals made among the membership for 'Premier Stays' and 'Premier Parts'.

The most extensive work necessary was to the boiler, carried out by the standard-gauge North Norfolk Railway and including a new inner firebox and smokebox, and a complete retube. The bottom end also needed substantial attention including re-turning the wheels.

Much studying of photos in the railway's museum was carried out in an effort to return the loco to its appearance on arrival in Sittingbourne in 1905 – it now carries a deep green livery quite unlike the more familiar apple green of S&K locos today worn by Melior, and with heavy lining and its name in a large serif font on the saddle tank. Weather protection for the crew is spartan too, the sides and rear of the cab open to the elements.

Visiting workers

The celebration weekend was held in two parts, starting with an event for members and friends of the S&KLR on Saturday. Crews on the locos included members of other railways, among them the Kent & East Sussex, Bluebell and even the Welshpool & Llanfair, the two 2ft 6in gauge lines having struck up a strong relationship since Sittingbourne Bagnall 0-6-2T 'Superb' appeared at the Welsh line's Steam Gala in 2017 and 2018.

Sunday was a public day and a very successful one. On both days an intensive timetable saw 11 steam services from Kemsley Down and 10 from Sittingbourne Viaduct, with »



Kerr, Stuart's standards...

Kerr, Stuart & Co was founded in Glasgow in 1881 but moved to Stoke-on-Trent in 1892, with all its locomotives being built in the Staffordshire town.

The firm became well known for its standard locomotive designs, the classes that boasted trailing trucks allowing the use of large fireboxes becoming a trademark feature of Kerr, Stuart products. The Tattoo and Brazil 0-4-2 saddle tank narrow gauge locos, both of which appeared at the start of the 20th century, were typical of these designs, with inside or outside frames depending on the gauge they were ordered for.

The Brazil, reputedly named because the first example was built for a customer in the South American country, was in many ways a larger version of the Tattoo, with 9 x 15in cylinders giving it a rating of 84hp compared to the 36hp of the 7 x 12in cylindered Tattoo. There was also a class between them, with 8 x 12in cylinders and named Darwin.

Some of the earliest Brazils were 'Premier' and 'Leader', built for the new 2ft 6in line at Sittingbourne paper mill and delivered in 1905. Sittingbourne had two more Brazils, 'Excelsior' in 1908 and when a new mill was opened in 1924, 'Melior'. This was different

to its sisters as Kerr, Stuart had updated its motion to the Hackworth design. All four engines survived to be preserved.

Other Brazils went to various customers across the globe, as did many examples of the smaller Tattoo class. Two Tattoos that saw service in the UK were 'Stanhope', built in 1917 for Rossyth dockyard, later employed at Penrhyn Quarry and today preserved at the Apedale Valley Railway, and the Corris Railway's no 4 supplied in 1921, which was later preserved at the Tallylyn Railway as no 4 'Edward Thomas'. The Corris has since built its own Tattoo locomotive, no 7, completed in 2005.

After Kerr, Stuart collapsed in 1930 the goodwill to its designs was bought by Hunslet and the very last steam locomotive built by the original Hunslet Engine Company, in 1971, was an improved version of the Brazil class. This was 'Trangkil No.4', a 2ft gauge example which ran on sugar lines in Indonesia. It was repatriated in 2005 for preservation at the Statfold Barn Railway, where it regularly operates today – Statfold today owns the rights to produce new Hunslet steam locomotives, and is custodian to the Hunslet and thus Kerr, Stuart archives.

Two stages in the life of Premier. In the picture above, taken by David Mitchell, the loco is pictured with Bagnall 0-6-2T 'Superb' in Bowaters service at Kemsley Mill on 21st September 1966.

Editor Andrew Charman took the shot at right on his first visit to the S&KLR in April 1986. Premier at left was with Melior, then under restoration.





locos swapped at Sittingbourne. Visitors made the most of the rare opportunity to see the three Brazils in steam together, one of *NGW*'s regular correspondents telling us that "the trains were rammed full!"

While Premier was the star of the weekend it did not quite take all the limelight – sister Leader was the centrepiece of a small ceremony on Sunday afternoon. As mentioned this loco has always been owned in preservation by a consortium of S&KLR members and the ceremony saw it officially handed over to the railway's ownership.

Above: They may be sisters but each has its own distinct profile.

Above right: Spot the track! Proper industrial narrow gauge as Premier approaches Kemsley Down.

Below: Premier is ready to head its train back to Sittingbourne as Melior arrives.

Having completed its final weekend of duties, Leader faces some time on the sidelines. Its overhaul is expected to cost at least £150,000 and won't begin until the completion of the 10-year overhaul of Bagnall 0-6-2T 'Superb', which last ran in 2018, and sister Bagnall 'Triumph' – currently stored in Kemsley yard but with a sign next to it stating that the loco is too under overhaul.

Enduring appeal

Your editor was born far too late to see the line in its Bowaters years (something I regret!). I made my first

visit to Sittingbourne in 1986 and have always enjoyed returning to the Kent line, even though having prior to 2007 been the closest narrow gauge railway to my Surrey home, it's now one of the furthest from *NGW* Towers in mid Wales.

The overall memory from that first visit some 36 years ago was of an authentic industrial railway, a rare thing by the 1980s though it had to be said that much of the surroundings of the line appeared to be a bit of a wasteland of disused industry – all rusting pipes and gantries.

Certainly when five years later I



took my then new wife on a day out to the line, she wasn't quite as impressed with the scenery compared to the railways ridden on our first holiday to Wales earlier that year...

We rode in the semi-open former Chattenden & Upnor Railways carriages, which had spent 17 years in Wales on the Welshpool & Llanfair before being bought by the S&K in 1978 to work just 12 miles away from their former home. I leant out to take photos of the loco literally parting the greenery growing on either side of the route – and as this was early in the season suffered severe hayfever as a result! That did not exactly help Rosemary's enthusiasm for the industrial narrow gauge...

The line's surroundings have changed over the years – much of the industrial desolation has made way for new building, Asda and Morrison's superstores, a new main road crossing over the railway, while the control of the lineside vegetation is rather better these days.

Challenges met

That the railway is here today becomes all the more impressive when you consider that it is run by one of the smaller groups in UK preservation and one that has faced many challenges, most notably almost two years with no trains in 2009-10 when the paper mill was closed and the land that was leased to the railway passed to new owners.

Not until 2012 did trains return to the full line, including the half-mile long concrete viaduct that



Above: It was a special weekend for Leader too, the loco working its last trains before expiry of its boiler ticket.

Below: Premier and train traverse the signature feature of the S&KLR, the concrete viaduct that runs for more than half a mile.

leads to Sittingbourne station. And this viaduct, a listed structure, has been a constant headache, with more than £100,000 invested in its upkeep over a 10-year period.

Despite this the railway has progressed, celebrating its 50th anniversary of preservation in 2019 while also managing, thankfully, to retain that industrial atmosphere. The trackwork, for example, is these days to the highest standards in the line's preservation history – in an age of close monitoring by authorities such as the Office of Rail and Road it has to be. Yet as the picture on the

facing page clearly demonstrates, Sittingbourne trains still make their way along two lines of rails snaking through a mainly green corridor – this is proper industrial narrow gauge, and long may it remain so. **NGW**

More Information

Sittingbourne & Kemsley Light Railway trains run on Sundays, as well as Wednesdays in August and the Summer Bank Holiday Monday, until the end of September when the line stages its traditional two-day end-of-season Gala. More details are on the website at www.sklr.net



Dutch Steam Trams



James Waite profiles some highly distinctive lines and their locomotives.

In 1879 the Dutch government introduced a law authorising the construction of steam tramways. They could be built on the roadside verge, so avoiding the land purchases that were needed for conventional railways. Some were standard gauge and enjoyed physical connection with the main line railway network, but to further economise others adopted the narrow gauge.

The trams had to run slowly but the low construction costs resulted in the tramways becoming an important means of transport in many parts of the country and it wasn't long before their distinctive four- and six-coupled locos became a familiar sight within

the Dutch countryside, often with enclosed casings and with skirt tanks which helped protect the motion.

The steam-tram concept also found its way to the Dutch colonies, lines built including one to 3ft 6in gauge in Java, in what is now Indonesia, and a metre gauge line in Surinam in South America.

The box-shaped machines probably owe their origin to the British Merryweather company. A standard-gauge example, no 2 of the Rijnlandsche Stoomtramweg Maatschappij (Merryweather 110/1881), is now the oldest surviving tram locomotive anywhere in the country.

Above: 0-6-0Tm no 54 (Orenstein & Koppel 8065/1915) stands at Ouddorp on 24th August 2005. At the time it was painted in the plain green livery that it carried in the last years before the 1965 closure. Five of these locos were built in 1915.

Photos by James Waite, taken 18th August 2018 except where stated.

For many years from 1932 this loco was displayed in the Dutch national railway museum at Utrecht and survived extensive damage during a bombing raid in 1943, but since 2017 it has been a static exhibit at the standard gauge Hoorn-Medemblik heritage line. It was one of five supplied for the opening of one of the country's first tramways which ran between Leiden and Katwijk, to the east of The Hague.

In 1881 the Arnhem municipality organised a competition to find the most suitable loco for its line, and Merryweather, Krauss, Hohenzollern and SLM took part. The Krauss loco won, but Mr JG Rueb, one of the judges, much preferred the Merryweather, largely because it was fitted with inside cylinders which were better protected from roadway dirt. This led directly to his firm Backer & Rueb, based at Breda, entering the market with a refinement of the Merryweather design, and the firm soon became the pre-eminent builders of the type.

Their locos were distinctly small, often just 3.2 metres long. They were powerful and could be supplied for any gauge between 750mm and standard. Joy valve gear was often fitted since it was mounted higher up than other types and, again, less prone to damage from dirt. One pleasing touch was the wavy ribbon design of their worksplates, though this was apparently based on a similar plate on a Hohenzollern machine.





However they faced stiff, and somewhat unfair, competition. At the time the Netherlands imposed no import tariffs on complete locomotives, but they did on the parts which Backer & Rueb incorporated into its locos, with the result that foreign builders enjoyed a price advantage.

Amongst others these included SLM, which used the indirect drive designed by its proprietor Charles Brown. The cylinders were mounted high up and the drive was transmitted by rocker shafts. Germany was no stranger to steam tramways either and among its builders Henschel, and to a lesser extent Hohenzollern, gained significant shares of the Dutch market. Henschel's standard loco was somewhat larger than Backer & Rueb's, typically 4.3 metres long.

One of the largest systems was the 3ft 6in gauge Rotterdamsche Tramweg Maatschappij, which served the islands and countryside to the southwest of Rotterdam. It was a progressively managed concern, pioneering the multi-modal container concept because so many of its routes involved ferry crossings. As a result it proved unusual amongst tramway companies because it became a significant ferry operator.

The system was badly affected by the floods which devastated much of

Facing page below: James was given a cab ride on his first visit to the Tramweg Maatschappij in Rotterdam in August 2005. No 54 runs along the dyke on its way to Port Zélande.

Above: No 57 (9194) was part of the second batch of three O&K locos delivered in 1920. Seen in the engine shed at Ouddorp, it has never run in preservation.

Above right: A rear view of no 54 at Ouddorp, emphasising its unusual proportions.

Below: No 54, repainted in its earlier fully-lined green livery, has just left Port Zélande. The attractive leading carriage was built by Haine St Pierre in 1905.



WAITE'S WORLD

the Dutch coast in 1953, but it soldiered on with a reduced network until its last line closed in 1965, helped by a healthy seasonal traffic in sugar beet. Diesels first appeared in 1924, but the system retained several steam locos until the closure.

Although it operated a significant network of bus services in its later years it continued to run passenger trams, and was still investing in further diesel-powered ones as late as the early 1960s.

Heritage operations

By the time of its closure the RTM had become the last functioning narrow gauge steam tramways in the country, and it survived long enough for preservationists to take an active interest. In the following year they started a heritage operation out of the old RTM harbour station at Hellevoetsluis, one of its principal ferry ports. But hostility from the local council eventually forced them to seek a new home in 1989 near the coast of the island of Goeree-Overflakkee at Ouddorp, once the terminus of an RTM branch.

The heritage railway now runs along the Brouwersdam, a dyke which separates the North Sea and the Grevelingenmeer. It was built between 1965 and 1971 as part of works to protect the coast from a repetition of the 1953 floods.

When I first visited in 2005 the line's southern terminus was at Port Zélande, a resort area midway along the dyke. The railway has become a hugely popular tourist attraction, helped considerably by the presence of the Center Parcs holiday complex next to Port Zélande station. It has now been extended to the village of Scharendijke on Schouwen-Duiveland, close to Renesse, home to a noted medieval church with a tower which is a striking feature of the view from all around, including the RTM's trains. Renesse was once served by another RTM branch, but it was severely damaged in 1953 and did not reopen.

Thanks to their early start the preservationists were able to rescue a comprehensive collection of the RTM's equipment, including the four surviving 0-6-0 steam tram locos, one from Henschel and three from Orenstein & Koppel, built between 1913 and 1920. They are distinctive machines, and are fitted with skirt tanks although they lack the square casings.

The RTM once owned 16 classic Backer & Rueb 0-4-0Tms built between 1899 and 1904, along with six similar ones built by Werkspoor in 1906, but they all disappeared between 1946 and 1956. The preservation society is building a replica of no 37, the last survivor, »





though it is likely to be several years before it is completed.

Also preserved are some historic diesels and no fewer than 13 of the RTM's beautiful period carriages, fitted with large windows and containing splendid saloons, along with many freight wagons.

Geldersche tramways

The De Graafschap district, to the east of Arnhem in Gelderland province, was also noted for its narrow gauge tramway system, which included three lines that crossed the border into Germany. They were built by several companies, some to 3ft 6in gauge and others to 750mm.

With considerable encouragement from the Dutch government they all came under common management in the 1930s by the Geldersche Tramwegen, the largest of the companies, while retaining their separate ownership structure.

The new organisation soon embarked on a much-needed

Above: Geldersche Tramwegen loco no 7 'Vrijland' at the National Smalspoormuseum – May 2007.

Above right: The distinctive plates on Geldersche Tramwegen no 13 'Silvolde', including Backer & Rueb's wavy ribbon worksplate – May 2007.

Below: PNKA 0-4-0Tm no B1903 stands beside the water tower at Tulungagung, eastern Java, in August 1978.



rationalisation scheme. At least one isolated 3ft 6in line was abandoned, and the lines deemed worthy of retention were converted to 750mm gauge. Unlike the RTM the lines replaced their passenger services by buses, though they were restored for the duration of the Second World War. Freight operations lasted until 31st August 1957.

They, too, dispensed with the overall casings on their later steam locos, but happily one of the classic Backer & Rueb machines and two Henschel ones have been preserved. The latter originated on the Tramweg Maatschappij Zutphen-Emmerik as its nos 3 'Hummelo' (Henschel 6014/1902) and 7 'Vrijland' (Henschel 6848/1904), and were renumbered 603 and 607 by GT in the 1930s.

They were the last of seven of these locos in service. Three were withdrawn in 1935 and 1936 and the other two on the closure of the Zutphen line, when nos 603 and 607 became the outright property of GT. In 1955, Hummelo was bought back by Henschel to become a valued exhibit in the company museum at Kassel, where it is still displayed by the firm's successors. Vrijland moved to the Utrecht museum late in 1957.

GT nos 12 'Ulft' (R&B 116/1895) and 13 'Silvolde' (R&B 182/1900) were two non-standard locos supplied by Backer & Rueb, distinctive features including a boiler pitched 100mm lower than normal

to reduce their centre of gravity. This was achieved by waisting in the lower firebox to fit between the frames.

These locos were always owned by the GT, along with four standard machines built between 1887 and 1891 which were all set aside in 1929 and written off in 1934. Ulft suffered a similar fate in 1935, but Silvolde became the GT's works shunter at Doetinchem after its years in regular service were over.

The loco was still there when GT celebrated its 75th anniversary in 1956 and marked the occasion by introducing heritage trains, consisting of an original carriage, no AB48, and van no GZ41 hauled by Silvolde. The trains proved so popular that they returned the following year, making their last run between Doetinchem and Doesburg on the day of the closure.

The entire train joined Vrijland at the Utrecht museum in 1975 but went into store 20 years later. In 2000 they moved to the National Smalspoormuseum at the Valkenburgse Meer, a former sandpit which now forms a recreational lake close to Leiden, original home of the standard gauge Merryweather loco. Vrijland followed them there in 2003.

They all remained static exhibits until 2019, when grant funding allowed a start to be made on restoring Vrijland to working order. The work includes a new boiler and narrowing its gauge to 700mm to suit the museum's operating line. While this work continues currently trains are worked by a number of industrial locos. They are of great interest, but Vrijland will be an especially valued member of the working fleet when it becomes the first classic Dutch narrow gauge tram loco to run again.

Java and Surinam

The tramways in Java and Surinam outlived their counterparts in the Netherlands, and possessed some locos fitted with the classic boxy casings. I found PNKA 0-4-0Tm no B1903 (Hohenzollern 1120/1898) in steam at Tulungagung in eastern Java in 1978, though its sole function appeared to be to supply steam for a water tower pump and I didn't see it move. It hasn't survived but three similar locos are preserved, two Beyer Peacocks and another Hohenzollern.

The Surinam railway probably operated until the 1980s. Several of its locos survived until about ten years ago but have all now been scrapped with the sole exception of 0-6-0 skirt tank 'Para' (Backer & Rueb 300/1916). The RTM and Geldersche machines are the only survivors of the narrow gauge tram locos in the Netherlands. **NGW**



Weathering the conditions...

The photographers who regularly submit their content for our pages found plenty of trains to focus on over the past month – and a range of climate conditions to cope with too...



Above right: *Karl Heath* was in just the right place to catch some interesting and attractive light as Hunslet 0-4-OST 'Elidir' (493/1889) raised steam on a sunny morning at the Llanberis Lake Railway on 28th May...

Right: ...while *Ben Bucki* was about to get soaked by a rare (in the editor's experience) Welsh thunderstorm as he photographed fellow Hunslet 'Britomart' (707/1899) on a goods working at the Ffestiniog Railway on 3rd June.

Overleaf: The 'Lady Ann' doesn't appear to be going anywhere soon as Lynton & Barnstaple-style 2-6-2T 'Yeo' sets off back to Fairbourne from Penrhyn Point during the Fairbourne Railway Gala on 30th May. Visiting Littlehampton Miniature Railway 2-6-2T 'Christopher' awaits its next turn in the loop.

Photo: Andrew Charman





**NARROW
GAUGE
WORLD**





Above: For Austria read Powys... A photo charter booking on the Welshpool & Llanfair Light Railway on 16th May paired ZB2 'Zillertal', on hire from the Zillertalbahn in Austria, with a highly appropriate train comprised of the four-wheeled Austrian carriages that have been donated to the Welsh line by the Zillertalbahn over the years, starting in 1968.
Photo: Ralph Ward

Below: Four days earlier *Matt Ditch* was again enjoying the authentic industrial surroundings of the Beamish Living Museum in County Durham, focusing on the recently restored Andrew Barclay 0-4-0WT 'Glyder'.





Above: The Romney, Hythe & Dymchurch Railway is a master of Gala entertainment, very much demonstrated by this shot from the Kent 15-inch gauge line's 95th Anniversary Gala on 14th-15th May. This parallel run on the double track between Hythe & New Romney naturally featured locomotives 9 and 5, Canadian-style Yorkshire Engine Co 4-6-2 'Winston Churchill' and Davey Paxman 4-8-2 'Hercules'.

Below: The very different proportions of the UK's two leading 15-inch gauge lines, the 'narrow-gauge' Ravenglass & Eskdale Railway and the 'scale' Romney, Hythe & Dymchurch Railway are clearly indicated here during the latter's 95th Anniversary Gala. The R&ER's Heywood 0-8-2 'River Irt' was piloting – and overshadowing – the Romney's Davey Paxman 4-6-2 'Green Goddess' near Dungeness.

Photos: Steve Town



Tracks by the Tyne

Donald Brooks looks back at the Gateshead Garden Festival railway that was open for just 157 days.

It was a good year in 1990 for the 15-inch gauge in the east of England. By the time the Bure Valley Railway opened in Norfolk on 10th July, another new line had already been in operation for a couple of months up in Tyne & Wear. And although the Bure Valley almost went out of business following financial difficulties after its first season, there was never any intention that the line at Gateshead would outlive the year.

The perceived success of the 1984 Liverpool International Garden Festival (*NGW159*) led to four further festivals, at two-year intervals

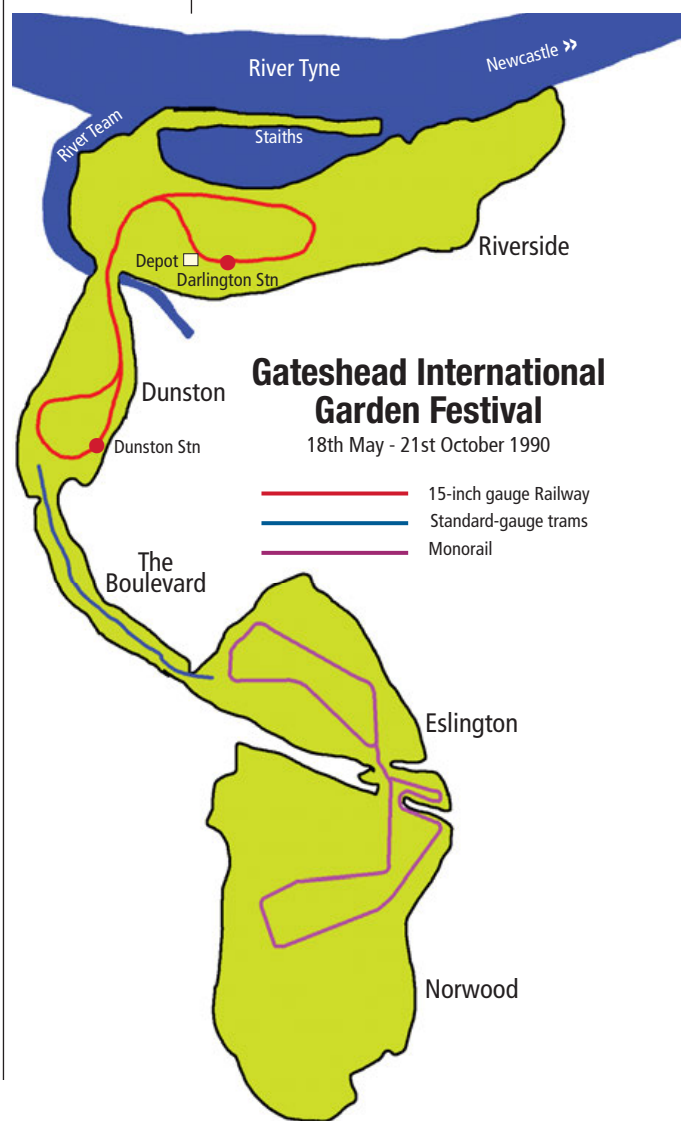
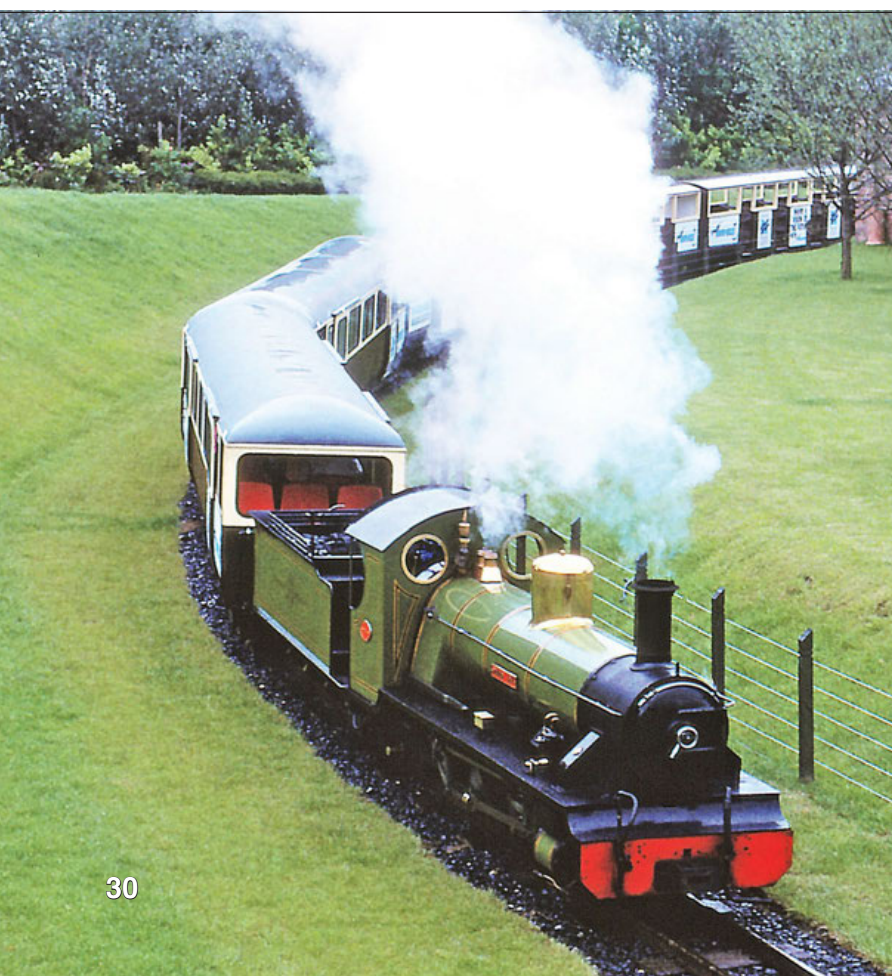
– Stoke-on-Trent (1986), Glasgow (1988), Gateshead (1990) and Ebbw Vale (1992). All were intended to reclaim the derelict post-industrial landscapes in which they were held and to contribute towards wider regeneration of the host town or city.

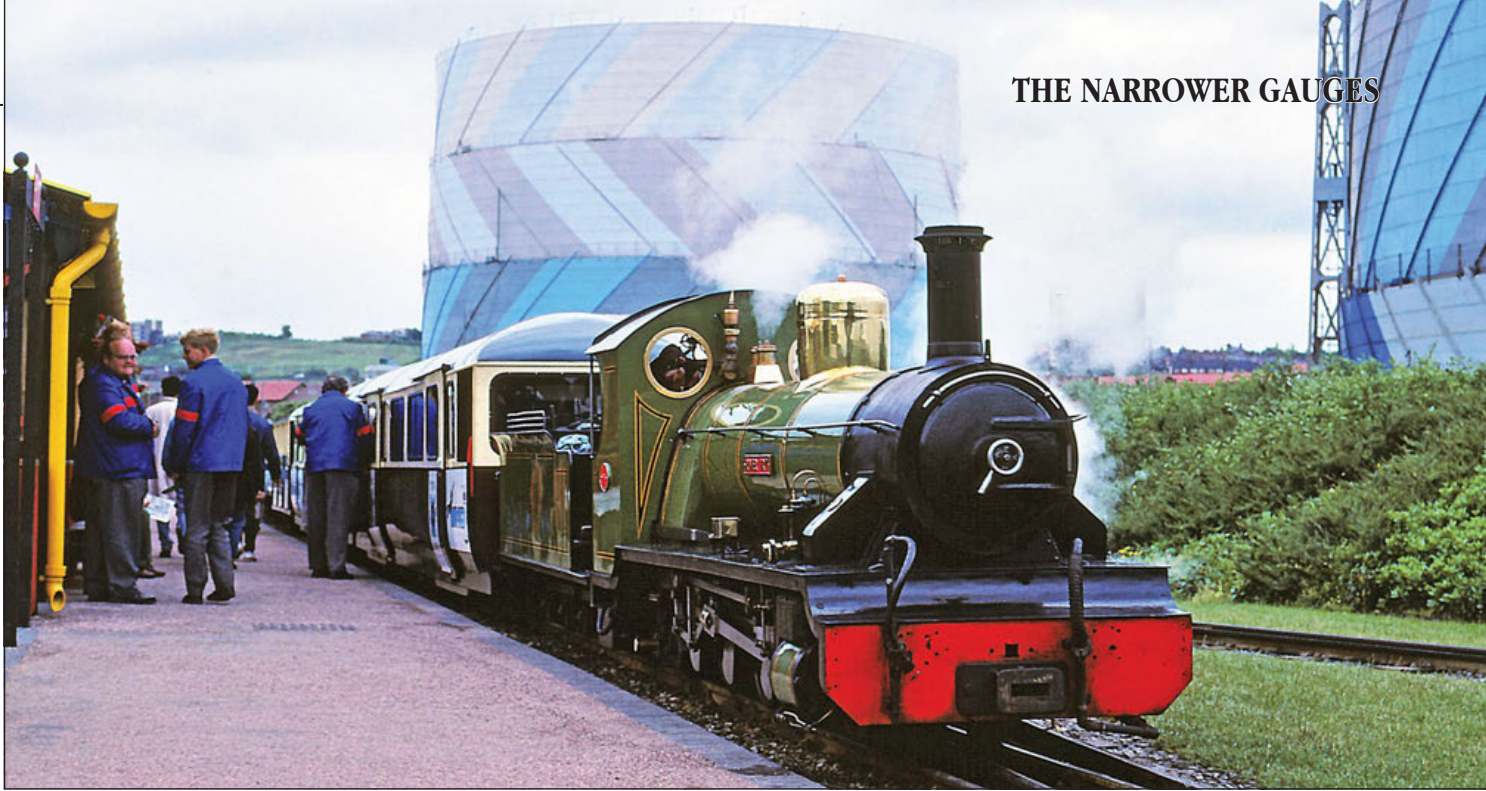
Following the example of Liverpool each of the subsequent festivals incorporated one or more railways, designed to provide a ride around the site, to fulfil a transport function or both – and in all cases the main line was narrow gauge. Severn Lamb provided new motive power for both Stoke and Glasgow. Stoke used

Above: 'John Southland' runs alongside the Tyne towards Darlington station. Dunston Staiths, given a £1m renovation to use as a festival promenade, is in the background.

four 1ft 11³/₄in gauge four-wheel diesel-hydraulic locos of uninspired design, while Glasgow went for three 60cm gauge machines based on the Great Northern Stirling Singles, with diesel engines in their tenders.

The hillside Ebbw Vale site was served by a 3ft 6in gauge funicular, with the short 7¹/₄in gauge Swansea Bay Railway also providing rides in





the Swansea City Council Garden. It was the Gateshead railway, though, that most closely resembled Liverpool by adopting the 15in gauge and using live steam on its circuit.

A unique site

The Gateshead Festival was held alongside the Tyne, not far west of the city centre bridges to Newcastle and close to Dunston railway station. Whereas the four other festivals had been based on coherent sites, Gateshead was unique in that it was divided into four zones, separated by a railway, main road and the River Team (a tributary of the Tyne), requiring good internal transport to bring it all together.

The Riverside zone bordered the Tyne and was dominated by the 1893 Dunston Staiths, used by trains from County Durham to load coal into ships on the river and said to be Europe's largest wooden structure. A bridge across the River Team led south to the smaller Dunston zone, from which a long avenue known as The Boulevard connected with Eslington and finally Norwood.

These last two zones were served by a novelty monorail circuit with fairground-style caterpillar trains. Uniting the whole site from north to south was a Dotto road train, while a standard gauge tramway ran the length of The Boulevard, using cars borrowed from Crich museum.

The 15in gauge railway served the two northern zones, Riverside and Dunston, with a station in each. It was roughly two kilometres long and consisted of a short single-track section, crossing a 100m bridge over the River Team to connect long balloon loops in the two zones. The station in Riverside was known as

Facing page far left: 'River Irt' curves at the southern point of the Dunston loop.

Left: Map showing how the railway fitted into the various garden festival zones.

Above: River Irt in Darlington station – the gas holders of the Redheugh Gasworks are dominant in the background.

Below: River Irt on the points of Darlington station loop. The track in front of its smokebox led to the shed.

Photos by Donald Brooks, July 1990

Darlington and was not far from the Riverside entrance to the site, accessible by bus and special ferry service from Newcastle.

All internal transport was included in the £6 site admission charge. Although it was mainly the road train and the tramway that performed an actual transport function, the 15in gauge railway could be used by anyone wanting to make a single journey between Riverside and Norwood.

The Ravensglass & Eskdale Railway (R&ER) was heavily involved in the Garden Festival line from its inception. The R&ER provided new rail for the main circuit and secondhand lengths for the sidings, which led to a spacious two-road shed off the Riverside station loop. Both stations had a single platform and the route round the Dunston zone was all plain track, with neither loops nor sidings.

For tracklaying, which started in the summer of 1989, the R&ER provided its four-wheeled battery-

electric locomotive 'Greenbat', Greenwood & Batley 2782 of 1957, along with a flat wagon and charging equipment for the locomotive.

Like Liverpool, the line was not easy to work, with sharp curves and gradients as steep as 1 in 40, including climbs in both directions to the Team bridge. Trains followed a figure-of-eight route, running clockwise in Riverside and anti-clockwise in Dunston. Their drivers were recruited locally, while training was provided by the R&ER.

Four locomotives

Greenbat returned to Ravensglass on 13th March 1990, following the arrival of three R&ER locos which were to work the Gateshead line. Two steam engines, 0-8-2 'River Irt', the oldest working 15in gauge steam loco in the world, and 2-6-2 'Northern Rock', built at Ravensglass in 1976, were accompanied by 1969 4-6-4DH 'Shelagh of Eskdale'. Both River Irt and Shelagh were veterans of the Liverpool Garden Festival and both »





share a Heywood heritage, with Shelagh incorporating components from the 0-6-0T 'Ella' and River Irt rebuilt from 0-8-0T 'Muriel'.

The fourth locomotive in service at Gateshead came from the Romney, Hythe & Dymchurch Railway (RH&DR). This was Bo-Bo diesel 'John Southland', built by TMA Engineering in 1983 primarily to work school trains serving Southlands Comprehensive in New Romney, now the Marsh Academy. These trains, which ran from 1977 to 2015, were initially steam worked, the principle of diesel operation being established by borrowing Shelagh from the R&ER. John Southland was later renamed 'J B Snell' after the RH&DR's former general manager.

Like the locomotives, much of the rolling stock had a garden festival past. Gateshead purchased ten former Liverpool bogie carriages and hired a further five from the RH&DR. Completing the roster were five Maxi carriages, built new at Steamtown Carnforth, which went to the R&ER after the Festival closed.

The 20 carriages were formed into

three mixed sets of six or seven cars each. Two or three trains were in operation simultaneously, running at least every ten minutes and controlled by radio using a system similar to the one pioneered in the UK by the R&ER. For line operation the two steam locos and 'John Southland' were preferred, with Shelagh usually restricted to shunting duties.

Back at Ravenglass the R&ER was celebrating the 75th anniversary of the first stages of the line's reopening as a 15in gauge route in 1915, following the closure of the original 3ft gauge railway two years earlier. The main festivities were held at the end of August, with visiting Bassett-Lowke engines typifying motive power from the early days.

Despite three 'home' locomotives being at Gateshead the R&ER still succeeded in operating all its scheduled services over the season, helped by hiring 4-6-2 'Black Prince' from the RH&DR. Although bringing a Romney engine to Ravenglass to fill a gap created by moving R&ER locos to Gateshead may seem an unnecessarily

Above: With two Maxi carriages at the head of its train, River Irt pulls away from Darlington and heads for the bridge over the Team. The building immediately behind the train is the railway's shed and works.

Below: One of the short tunnels is approached by John Southland as it passes the ornamental maze on its way through Riverside. The dull weather may have accounted for the light loadings on the trains.



complicated series of moves, it certainly added interest for the 15in gauge enthusiast and gave the R&ER an extra visiting engine for its anniversary year.

The Gateshead festival site had a strongly industrial past, with Dunston Staiths not the only reminder. Riverside was the location of Redheugh Gasworks and the group of gasholders just outside the boundary of the festival provided a distinctive background. Dunston had also once been the site of a major railway yard, Norwood sidings, but no trace survived.

Throughout its route the 15in gauge was well integrated into the new landscape, its route including three short tunnels and a staffed level crossing as well as both over and under bridges.

Five successful months

The Gateshead Garden Festival opened on 18th May and ran for 157 days, closing on 21st October. The site, in which over £50m had been invested, saw more than three million visitors, though their experience differed slightly from Liverpool in that Gateshead had fewer specifically garden-related features and more of the character of a general exhibition in an attractively landscaped setting.

Like all the festivals the event's legacy remains open to debate, though Gateshead today, with the Sage music centre, BALTIC Art Centre and the celebrated Angel of the North sculpture, is a much more vibrant place with a more confident sense of identity than 30 years ago.

On the final day of the festival River Irt, Northern Rock and John Southland provided a three-train service, but within three days two had returned home. Northern Rock travelled directly to the Bure Valley for the line's inaugural gala weekend. Shelagh remained at Gateshead a little longer to work the track-lifting trains and by December all the new rail had been transported to the R&ER for reuse. The borrowed rolling stock also went home and the ten carriages owned by the festival were put up for sale.

The Gateshead railway may not have been quite as high profile as its Liverpool predecessor and there may not have been such long queues for its trains, but it still gave a lot of pleasure to both general visitors and enthusiasts alike. Today the Riverside and Dunston festival areas have been redeveloped, mainly for housing. It is still possible to cross the bridge over the Team used by the railway, now part of a pedestrian and cycle route, but otherwise only photographs and memories remain. **NGW**

Remarkable Railcars...

Phil Horton outlines a surprising saga that embraces Yugoslavia, Portugal, Peru – and possibly Mozambique!

Until the end of the 1960s Yugoslavia was served by an extensive system of narrow gauge railways, most of them built by the Austrians to their military gauge of 760mm. The longest of these linked the capital Belgrade (now in Serbia) with the Adriatic Coast at Dubrovnik (now in Croatia). A series of other 760mm gauge lines connected with it including a branch to the naval depot at Zelenica on the Gulf of Kotor.

My encounter with the Zelenica branch was described in *NGW* 24 of Feb/March 2003. I had found myself there while on a camping trip across

Above: Yugoslavia: Class B802 diesel railcar waits at Hercegnovi on the Adriatic coast in August 1967.

Below: Class B802 railcars were sold to Portuguese Railways in 1980 and operated as Class 9700 units on the Corgo line. In May 1991 9714 and 9713 are near Carrazedo with a train for Vila Real. Photo: Maarten van der Velden

Europe from London to Istanbul with four university friends. The highlight for me, but not necessarily my friends, was seeing the heavy mid-day mixed train heading from the coast up into the mountains. This train was hauled by two ancient ex-Austrian 0-8-2 'compounds' built by Krauss at Linz at the turn of the 19th century.

We stayed in the area for about a week so I became familiar with the daily sequence of trains. One that I considered to be less impressive than the others arrived at the harbour-front station at Hercegnovi at around lunchtime, worked by a modern stainless steel two-car diesel unit.

I discovered much later that this was one of JZ's Class B802 four-car diesel units, built in Yugoslavia by Duro Dakovic with Fiat engines during the early 1960s. They were often worked as two-car sets as seen at Hercegnovi. This meant that there was a driving unit at only one end so at Zelenica this had to be uncoupled to use the turntable there. It was to be more than 40 years before I encountered any of these units again.

Railcars in Portugal

While the Yugoslavian lines all closed in around 1970, elsewhere in Europe the narrow gauge lasted much longer. As regularly featured in *NGW* some survive to the present day. European »





Left: Two Class 9500 railcars, 9501 and 9502, built on the chassis of earlier Yugoslavian Class 9700 units. They are at Regua, Portugal, with a service from Vila Real in May 1998.

Lower left: A single Class 9500 at Mirandela bus interchange in May 1998. It was on a shuttle service to Carvalhais, withdrawn in December 2018.

Bottom left: Two of the older Class 9100 units, 9102 and 9103, used on the Tamega line. They are next to Livracao shed in October 2002 and at left is one of the Class 9500s about to replace them.

Below: Peru: No 903, one of the unrebuilt Class 9700 units sold off by Portuguese Railways, at Machu Pichu on a tourist train. Photo: David Almeida/Wikipedia Commons

Facing page: Two more views from Maarten van der Velden on the Congo line – 9713 and 9714 in the hills near Alvacoës (upper right) and at right nos 9719 and 9703 at Alvacoës, 7.25km from Regua.

Photos all taken in Portugal except where stated. Uncredited photos by Phil Horton

at work on the Tamega, while the Corgo line was operated by modern-looking CP single-unit Class 9500 railcars. What was left of the Tua line was worked by Class 9020 Bo-Bo diesel-electric locomotives, built by Alsthom in 1976.

As we had only time to travel on one of the branches we opted for the Tua line and so enjoyed a run with loco-hauled stock behind one of the diesel locomotives. At the terminus we found another Class 9500 unit running in the green livery of the Metro de Mirandela and operating a shuttle service to Carvalhais, 4km along the former Tua line to Bragança. A newly opened bus station adjacent to the railway at Mirandela formed an excellent transport exchange hub.

I returned to the Douro Valley in October 2002 as part of a steam-hauled rail tour organised by the Portuguese Traction Group for the Warwickshire Railway Society. On the Tamega line Class 9100 railcars were being replaced by the more modern Class 9500 units.

During our 1998 visit to Regua my friend and I had found a dozen or so long-retired 2-4-6-0 Mallet tanks still arrayed around the mixed-gauge turntable but by 2002 several had been purchased for preservation. Their places were taken by a number of very down-at-heel looking railcars. Despite my general rule of photographing everything in sight, I failed to take any shots of these units.

Yugoslavian origins

It was only when back on our special train that someone mentioned these railcars had originally come from Yugoslavia. They were in fact ex-JZ Class B802 railcars that were sold to Portugal in 1980! They were converted to metre gauge to become CP Class 9700 but in Portugal they had proved to be unreliable and several were fitted with new engines.

They nevertheless lasted until 2001 and it was surprising that we did not see any during our 1998 visit.

countries once noted for their narrow gauge included Portugal but, apart from a very short visit to Porto in 1973 when the lines were still predominantly steam-worked, it was not until May 1998 that I returned to Porto with a friend for a more lengthy stay.

By then much had changed on Portuguese Railways (CP) but travel on metre gauge trains was still possible, albeit with diesel traction. These included commuter trains from Porto (Trindade) station to Lousado and Senhora da Hora which still run today. Porto is also the gateway to the Douro Valley, well known for its Port-producing vineyards.

The broad-gauge (5ft 6in) line ran the whole length of the north side of the valley to Barca de Alva before

crossing into Spain. En route it passed four side valleys which in the 1970s boasted lengthy metre gauge branch lines. (*Steam operations on all four were described in Waite's World in NGW161 and 162 – Ed*)

Each line was named after the river valley it followed. The first, which left the main line at Livracao, was the 'Tamega line', the second at Regua, the 'Corgo line', the third at Tua, the 'Tua line'. The fourth at Pocinho had been the 'Sabor line' but this had closed completely in 1988.

Three years later the Tamega, Corgo and Tua lines had all lost their northern halves and only ran as far as Amarante (13km), Vila Real (25km) and Mirandela (45km) respectively. During our visit we found Class 9100 units, built by NOHAB in 1949 still



Latterly at least, only the two power cars appear to have been used. This was not the end for some of the units as they were exported to Peru to work the 914mm gauge Inca Rail service to Machu Pichu. Others are reported to have been sold to Mozambique.

Further research has revealed that in the mid-1990s the chassis of nine of the Class 9700 railcars had been used to build the Class 9500 units. This involved entirely new bodywork along with more powerful Volvo engines with a driving compartment at each end. Five were painted in red livery for the Tamago, Corgo and Tua lines and four in green livery for the Metro de Mirandela as seen during my 1998 visit.

The new units were to have a short life as the Tamago and Corgo lines both closed completely in 2009. The Tua line closed in 2012, apart from the Metro de Mirandela which lasted until December 2018. Some of the Class 9500 units are reported to have also since been sold to Peru while some Class 9020 diesel-electrics have gone to Madagascar.

In sourcing photos for this article I came to regret my decision not to take any of the Class 9700 units at Regua in 2002. Fortunately Dutch enthusiast Maarten van der Velden has kindly provided me with several splendid photos of the Class 9700 at work on the Corgo line in 1991. I did however, without knowing it, capture the rest of their evolution from the JZ Class B802 unit in 1967 to the CP Class 9500 some two decades later.

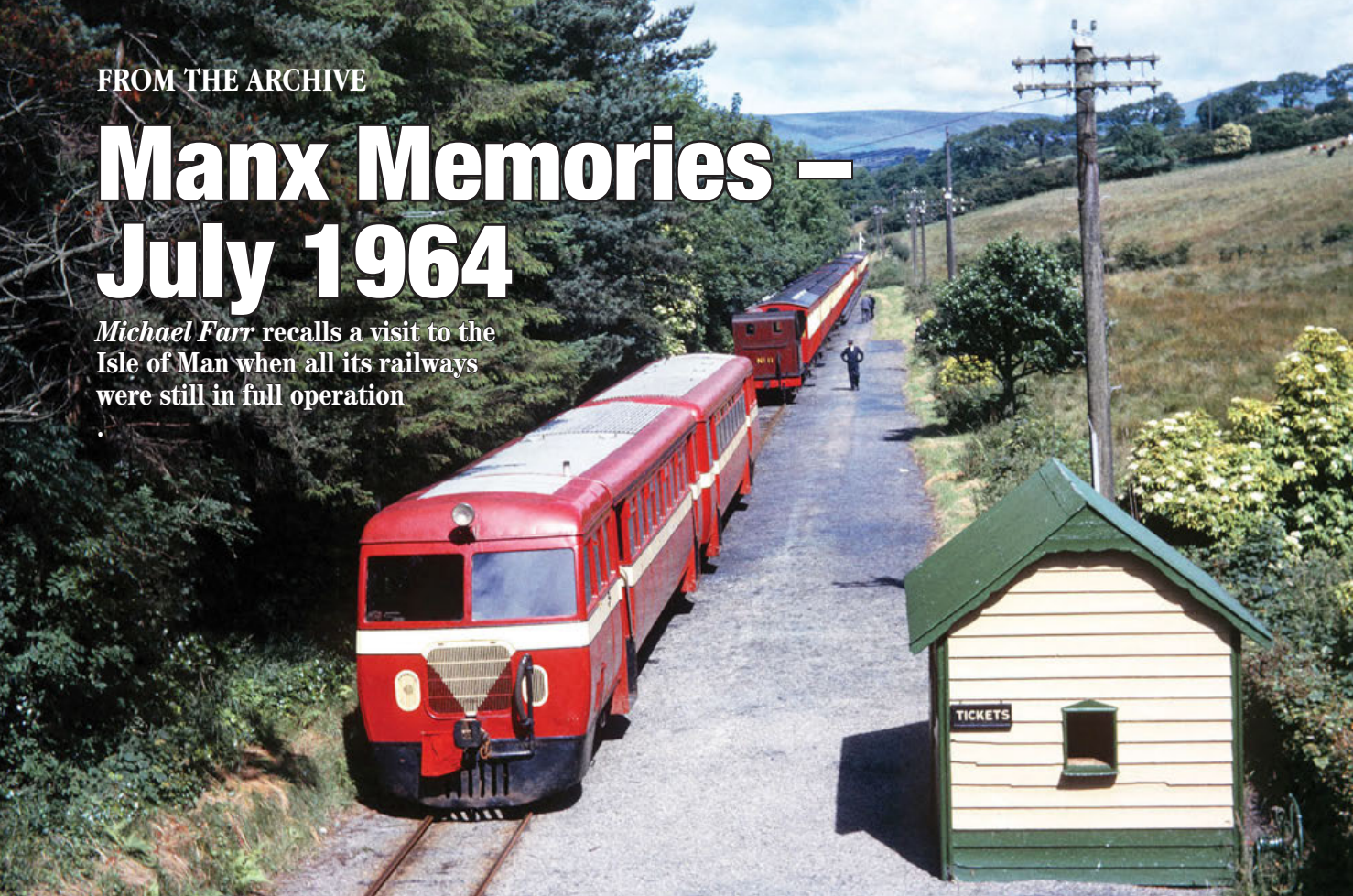
A photograph of Inca Rail unit no 903 at Machu Pichu is available on Wikipedia Commons but it would be interesting to know whether any **NGW** readers have come across these units working in either Peru or Mozambique, or a Class 9020 diesel in Madagascar? **NGW**



FROM THE ARCHIVE

Manx Memories – July 1964

Michael Farr recalls a visit to the Isle of Man when all its railways were still in full operation



At the end of my memories of my first Manx holiday in May 1963 (NGW157), I stated that I'd vowed to return the following summer while the peak timetable was operating. For 1964 I chose to visit just after the summer timetable had begun on 6th July, before the busiest holiday weeks and after the TT Races.

July's weather was much warmer and the increased service meant I could explore more fully, alighting at intermediate stations with a good chance of catching a following train to visit the next station along the line. For this facility I had to pay an additional 2s 6d for my runabout

ticket, which was now fifteen shillings (about £11 in today's money).

Arriving on Saturday 11th July, I was able to experience the solitary Sunday trains to take visitors to the open-air service at Kirk Braddan. In days of yore these carried many hundreds of passengers but by 1964 the service was operated with one locomotive and the railcars.

Two rakes of carriages were lined up in the Port Erin platforms at Douglas onto one of which Beyer Peacock 2-4-0T no 11 'Maitland' (4663/1905) coupled. Even before the 10.10am departure, the carriages were well filled and we set off for

Above: Former County Donegal railcars 19 and 20 wait at Kirk Braddan on 12th July 1964 to work the first of two trips back to Douglas after the nearby open-air church service. Note no 11 'Maitland' and its train behind.

Photos by Michael Farr

Braddan where passengers used the stepboards to alight on the ground-level cinder 'platform'.

Once everyone was heading off to the church grounds, Maitland continued on its way to run round the train at Union Mills. It then shunted the carriages onto the main line beyond the station and returned at speed, light engine to Douglas. Here it must have coupled to the second train set very quickly and left for Braddan in the path of the former 10.40am departure. Once again passengers detrained and the loco carried on to Union Mills.

Diesel traction

Shortly afterwards a purring sound heralded the arrival of the railcars with a few latecomers and the booking clerk from Douglas, who immediately opened up the tiny office at Braddan. He then hung poster boards on the road bridge advertising return trains (a fare of sixpence) in the hope that some of those who had walked to the service might be glad to ride back to Douglas for their lunch.

Meanwhile, Maitland had run around the second rake of carriages and coupled them to the first rake, bringing both trains back to Braddan. The rear set was then detached beyond the station limits and the front portion pulled forward just short of the railcars.

After the service the early birds were ushered into the railcars which



returned to Douglas. Next the front train was filled and, likewise, ran non-stop to Douglas. Maitland was detached from the carriages and returned light engine to Braddan to pick up the second set. When this was full it returned to Douglas where the loco was put on shed without delay.

Meanwhile the railcars made one last trip to Braddan to collect any latecomers and also the booking clerk who wasted no time in shutting up the station. Within minutes a silence fell on Douglas station as if no train had moved that day. This meant I was free to spend an afternoon on the horse trams.

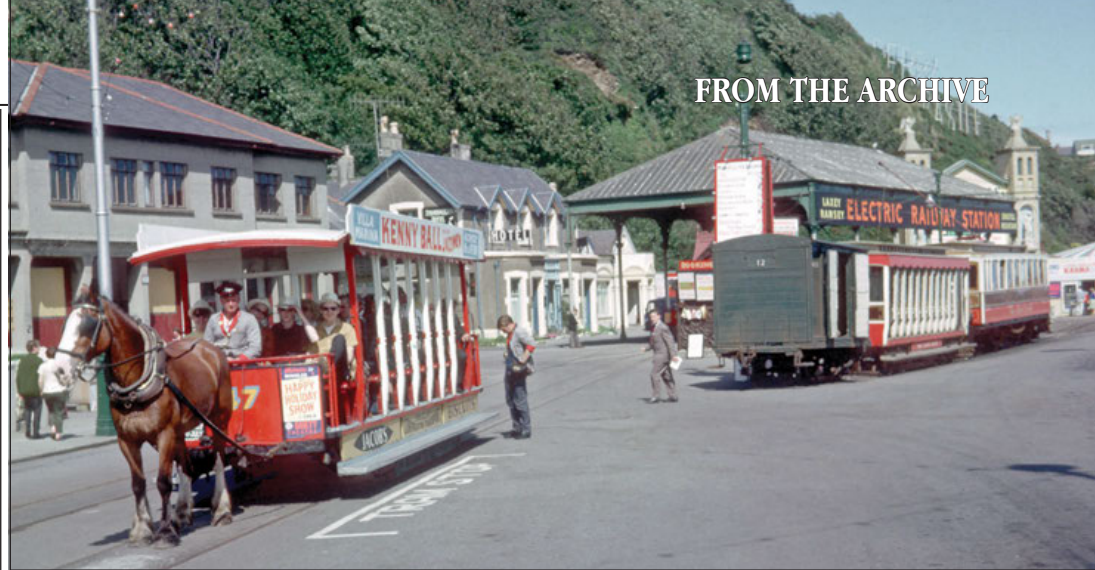
Four delightful days

For the summer of 1964 the former County Donegal diesel railcars were rostered for only one short trip to Kirk Michael and back, leaving Douglas at 2.10pm and returning at 4pm to bring home those who had visited the railway's pleasure grounds at Glen Wyllin. These were advertised as 'A beautiful Glen on the Western Shore' with a boating lake, children's playground and 'Luncheons, Teas and Snacks at reasonable charges'.

Due to my love of the railcars I sampled this working on my first weekday afternoon, 13th July. My abiding memory of the journey home with all the windows open was a party of young Irish nurses who led community singing at the tops of their voices!

On Tuesday, when I descended the steep hill to the Central Prom, I turned left instead of right and headed towards the Derby Castle terminus of the Manx Electric Railway to Laxey and Ramsey. I was lucky enough to be given a tour of Derby Castle workshops and later got up Snaefell on the 3ft 6in gauge trams, although the summit was unfortunately in mist.

Wednesday 15th July was to be



Facing page, below: Unhurried atmosphere at Kirk Braddan as Maitland waits to follow the railcar back to Douglas with the first of two rakes of carriages.

Above: Returning from Kirk Braddan the afternoon was spent on Douglas Horse Tramway. Manx Electric cars from Ramsey are connecting with a horse tram at Derby Castle.

Right: There was time to travel over the Manx Electric Railway, at Laxey meeting a Snaefell Mountain car on the left.

Below: On 13th July the two Irish railcars are at Kirk Michael on the afternoon trip to the railway's pleasure grounds near the station.



another day on the steam railway, travelling east and north. I caught the 10.25am from Douglas to Ramsey to alight at St John's so I could photograph and then board the following 10.40am to Peel in order to have a ride behind the railway's largest 2-4-0T, no 16 'Mannin' (6296/1926).

First thing on the Thursday I sought out the bus to Union Mills, a former station between Kirk Braddan and Crosby which no longer appeared in the timetable. This was the very first Manx station building I had modelled, taking the details from »





a photo in Ian Macnab's 1945 book on the railway. The previous year I had realised the curving platforms were attractively sited between high trees and so I planned to take photos of the two morning trains as they passed through.

The entrance gate was padlocked so I scaled the low wall. As I walked down to the tracks a thin plume of smoke indicated someone was working on the trackside. I was grilled by the trackworker who politely told me I was trespassing but I happened

Above: The former County Donegal railcars at Douglas on 18th July, no 20 leading. They will take a special party to Ramsey – an unusual destination for the diesels.

Left: No 5 'Mona' waits in a siding on 15th July as no 16 'Mannin' brings the 10.40am Douglas to Peel into St John's.

Below: On the same day Mannin was on light duties, hauling the two-carriage shuttle from St John's to Peel. It is preparing to return from Peel at 12.10pm.



to mention I had been with the railway's Mr Kincade. He had given me permission to visit the sheds at Douglas, but the ganger thought I had been given the run of the railway!

Thus I was able to photograph no 8 'Fenella' (3610/1894) on the 10.25am to Ramsey and no 12 'Hutchinson' (5126/1908) with the 10.40am to Peel as they threaded their trains around the reverse curve through the trees. I bade a polite farewell with thanks to the ganger and returned to Douglas in time to be able to experience the more intense service on the south line.

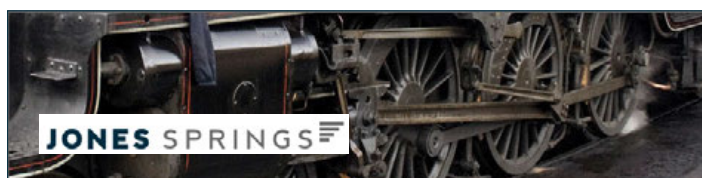
Final shots

Friday was a free day – with a 3pm flight home on the Saturday I had time to spend the morning at Douglas station where I found the railcars waiting ready to take a society special to Ramsey, my only opportunity to photograph car no 20 in the sun.

My return to Bristol from Ronaldsway coincided with the arrival of a large number of visitors. Dan-Air pressed into service its fish-bellied Bristol freighter fitted with seats, from which the Glaswegians flooded onto the runway using the vehicle loading ramps.

My own flight was delayed as the airline wanted to switch aircraft so that its luxury walnut-panelled Dakota built for a rich middle-eastern gentleman could be transferred from the Glasgow service to the Cardiff and Bristol route whose passengers were presumably expected to be more appreciative of the extra luxury! **NGW**





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St. Etienne-en-Caux

We spotlight the superb French-themed 009 layout built by Charles Insley.



You know how it goes, you wander around a model railway show and then something which is just a bit above the norm catches your eye. That was just the case for Phil Parker, editor of *NGW*'s sister magazine *GardenRail*, during the Bristol show at the end of April when he laid eyes on 'St. Etienne-en-Caux', a feast of

French-inspired excellence in 4mm scale 009 (9mm) gauge, and had to photograph it for our benefit!

The layout is the work of Charles Insley and was actually completed back in 2010. When we contacted him Charles told us that the inspiration for the layout's construction came firstly from a family holiday in northern France in 2007,

during which he visited the CF Baie de la Somme (CFBS) and the Chemin de fer Froissy-Dompierre.

Plenty of inspiration

He adds, however, that several other reasons sent him down the French modelling route; "reading my dad's copies of *Model Railway News* as a child and being captivated by the articles by the late



Left: This photo clearly shows just how compact the entire layout is, yet it manages to pack in so much to admire, with the attention to detail in creating small cameos an essential element adding so much extra to the finished model.

All photos in this feature by Phil Parker



Dennis Allenden on modelling French railways; and particular layouts that I've seen at shows and featured in the modelling press such as Christopher Payne's 'St Pierre et la Rue Perrin', Jacky Molinaro's 'Vis-en'Artois', Bruno and Alain Duchesne's 'Souvenir de Royan' and 'Isle de Fa' layouts and Peter and John Smith's 'St-Jean sur Mer',

Following the holiday in 2007 Charles started reading the French narrow gauge magazine *Voie Libre* "and never really looked back..."

The metre-gauge CFBS was a key inspiration – the station building for St-Etienne Ville is based on that at Le Crotoy and the harbour-side scene of St-Etienne Quai follows the theme of that at St Valey-sur-Somme, though Charles adds it is very compressed. But he decided that his layout would represent a 60cm gauge line, in 009/H0e scale, as he is a life-long modeller in 009.

The layout is built on baseboards constructed on a frame of 15 x 45mm timber unusually topped with 8mm balsa sheet; "It's so light and easy to work," Charles says.

The track is Peco 009 Streamline, but to create the bespoke look of the tramway-style route along the quayside and through the street Charles hand-laid code 55 rail on copper-clad sleepers.

There are two stations, St-Etienne Ville and St-Etienne Quai, and each board is independently controlled, the layout effectively run as two separate entities.

Buildings are a mixture, most of them scratchbuilt along with some kits by MKD and a couple of farmhouse kits that are marketed as props for wargamers in 1:76 scale.

Long marine life

Particularly impressive are the boats tied up at the quay – though sharp-eyed readers might notice that one of them, a coaster, has a very Welsh name... The 'Rachel Parry' of Portmadoc (old spelling). was originally built for a Welsh 009 layout



Charles had in the 1990s. He comments; "She about 25 years old, and is now reduced to timber traffic between the south-west and Normandy!"

Just as impressive, however, are the small details – this is a layout teeming with tiny cameos that really provide the atmosphere of a French harbour town. »

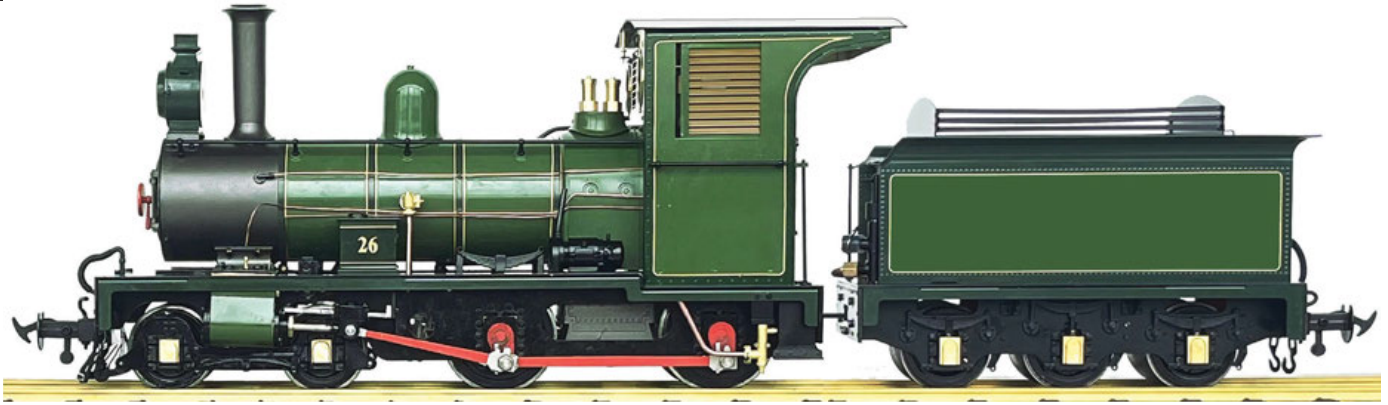




As for the rolling stock, this is a mixture of kitbuilt, kitbashed and scratchbuilt items – Charles tells us that models of French prototypes are rather thin on the ground...

He adds that the Bristol show seems to have heralded the second coming of the layout – it has since been to a four-day show as part of the 50th anniversary gala of the CF Froissy-Dompierre/P'tit Train de Haute Somme, (an enormous amount of fun, but tiring, according to Charles) and it may possibly be appearing at several more major UK exhibitions in the coming months. So **NGW** readers should have plenty of opportunity to view St. Etienne-en-Caux for themselves – take it from us, it's well worth a look... **NGW**





■ It's very much a large-scale selection of new modelling product on the shelf this month, thanks to new stuff appearing at the recent Llangollen Garden Rail Show, and with the National Garden Railway Show happening at Peterborough on 25th June there could be some more delights for those of a garden railway persuasion in our next issue. But come on smaller-scale narrow gauge manufacturers, let's see your new releases!

Topping this month's selection is the first 'sneak peek' of Accucraft UK's forthcoming 'Lawley' 4-4-0 live-steam locomotive in 16mm scale. The full-size locos ran on the Beira Railway in Mozambique – 42 were ordered from the Falcon Engine & Car Works in England (previously Hughes, builder of Corris Railway no 3) and Glasgow Railway Engineering in Scotland between 1895 and 1898. After the Beira Railway converted to 'Cape Gauge' (3ft 6in) several of the Lawleys were later purchased by South African Railways, eventually becoming class NG6. Two are today preserved at the Sandstone Steam Trust.

Accucraft's model is described as 'museum quality' – it is gauge-adjustable between 32mm and 45mm and gas-fired, the mechanical specification to the builder's long-established standards. It will be available in either Beira green, SAR green or SAR black, at an anticipated price of £2995.00. Certainly it will present a different shape on 16mm lines – full details are on the website at www.accucraft.uk.com and Accucraft adds that the loco will look particularly appropriate with the range of SAR passenger and freight laser-cut rolling stock kits produced in MDF or plywood by Resurgam Rolling Stock (<http://resurgamrollingstock.co.uk>).



Penrhyn open season at Wood Valley

The spartan open carriages built by the Penrhyn Quarries in north Wales for transporting their workers have long been popular kits in 16mm scale, produced in the past by the likes of Brandbright, I P Engineering and Timpdon. Now there is a new kit available from Wood Valley Works.

The original vehicles were constructed between 1879 and 1927 and unusually not numbered but lettered – the first batch (A-J) came from De Winton of Caernarfon and seven more (K-Q) were built at the quarry workshops in Felin Fawr.

There was no effort made to provide much comfort – in original form the carriages were unsprung and having ridden in a recreation at the Bala Lake Railway your editor can confirm that the ride is, memorable... Several survived into preservation and the Tallyllyn Railway constructed their first preservation carriages from them, initially simply fitting basic roofs, so there are lots of possibilities for the enterprising modeller...

The kit is in MDF and comes complete with wheels, axles, brass bearings and a range of 3D-printed detailing parts such as buffers, coupling hooks and safety chains. The brake gear works, and at the price this is an inexpensive way to assemble a rake of rolling stock.

The Wood Valley Works website is also well worth a look for interesting 16mm items, even offering a kit of the novel brake van converted from Penrhyn loco 'Sanford'!

Produced by Wood Valley Works
Tel: 01270 876882 E-mail: sales@woodvalleyworks.co.uk
Web: www.woodvalleyworks.co.uk Price: £32.00 each plus post

Early Darj from Plateway

There are plenty of options these days when assembling a Darjeeling Himalayan Railway train in 16mm scale but these two new releases from Plateway Models are a bit different to the norm, being of the Indian line's first-generation bogie carriages built between 1906 and 1915.

The unusual aspects to these vehicles were their heavily rounded corners, which will no doubt turn heads in a model setting. They also have central floors at a lower level than at the ends.

There are three types available, a first/second-class open composite, second/third composite and guard/luggage/open. All are supplied as comprehensive kits – the bodies are laser-cut ply with different layers to create the relief between doors, windows and panels, with such details as internal seating all supplied, as are the bogies and wheels. All that is needed to complete each model are paint, glue and decals.

Produced by Plateway Models
E-mail: PlatewayModels@gmail.com
Web: www.platewaymodels.co.uk
Price: £100.00 each plus post





Hopper effort in 7/8ths

Model Engineers Laser is best known for supplying laser-cut metal components to larger-scale model engineering users but among smaller items also produces parts in 7/8ths scale (18-inch gauge on 32mm, 2ft gauge on 45mm track) for the Hunslet 0-4-0 'Jack'. The firm is also working on a design for the Kerr Stuart Skylark 0-4-2T (one of the editor's favourite locos – and he models in 7/8ths...).

That probably inspired the latest item for 7/8ths, based on a design by noted modeller John Campbell. These hopper wagons were used by the Snailbeach District Railways in Shropshire, where they were hauled by Baldwins – and a Kerr Stuart Skylark.

As the company name suggests the finely detailed kit is modelled in laser-cut plywood and brass, and is designed to suit Model Earth Design Glyn Valley Tramway axleboxes and wheels from Slaters.

MEL planned to launch the wagon at the National Garden Railway Show in Peterborough on 25th June.

Produced by Model Engineers Laser

Tel: 07927 087172

Email: sales@modelengineerslaser.co.uk

Web: www.modelengineerslaser.co.uk

Price: £50.00 plus VAT



Memories of the Bord na Mona Railways

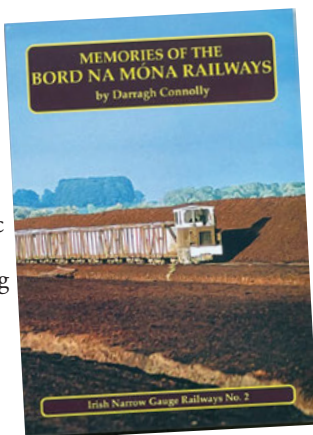
Darragh Connelly

■ Darragh Connelly is a well known figure on the Irish narrow gauge scene and has now joined forces with prolific publisher Mainline & Maritime to produce this book. It marks the passing of what many consider the last remaining industrial narrow gauge empire, the extensive and constantly changing 3ft gauge lines serving the Irish peat bogs of Bord na Mona.

The informative A4 softback, running to 64 pages, provides a great deal of detail on what was a massive operation, mostly through good quality colour photos used nice and large, typically over half a page and combined with highly detailed captions.

No detail is missed, from the train formations out on the bogs, to the yards and workshops, the locomotives (the various diesel classes are all carefully analysed), rolling stock (again, individual items pictured) and even the arrangements made when passengers were carried, involving some midly odd conversions from buses... Modellers will particularly find the close-up views of the locos inspiring for future projects.

With the Bord na Mona operation rapidly disappearing after many years of warning, this is a highly topical publication that provides a fitting tribute to what was a fascinating industrial system, the like of which we will never see again. **AC**



The Railways and Tramways of Laxey

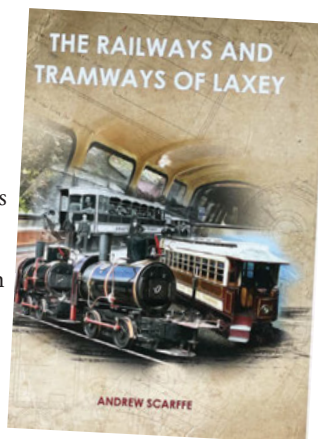
Andrew Scarfe

■ This is not a new book, published originally in 2014, but the publishers have now supplied the author with extra stock allowing the book to go on sale at what is an absolute bargain price, as well as helping one of the Isle of Man's most unusual lines.

In this A4 hardback, running to 175 pages, author Andrew Scarfe provides us with a very detailed description of how the rich lead mine close to the village of Laxey was exploited in the late 19th century. In one year the Great Laxey Mine produced more than 11,000 tons of zinc ore, more than every other UK lead mine combined, and to transport it a 19-inch gauge railway was built and worked by two distinctive and tiny locomotives built by Stephen Lewin and named 'Ant' and 'Bee'.

Obviously the Great Laxey Mine line takes centre stage in the book, which describes not only the original history but the restoration of the line to today's heritage attraction, including the building of replicas of the two locos. But every other tramway in the district is also analysed in detail, even schemes that came to nothing, and the connecting Manx Electric and Snaefell Mountain tramways.

There are many photos including a host of period views, maps of lines and track layouts, drawings of the locos – this would be a good book at its original price, at £5 it's an absolute steal... **AC**



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E-mail: iain@mainlineandmaritime.co.uk
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First French in China

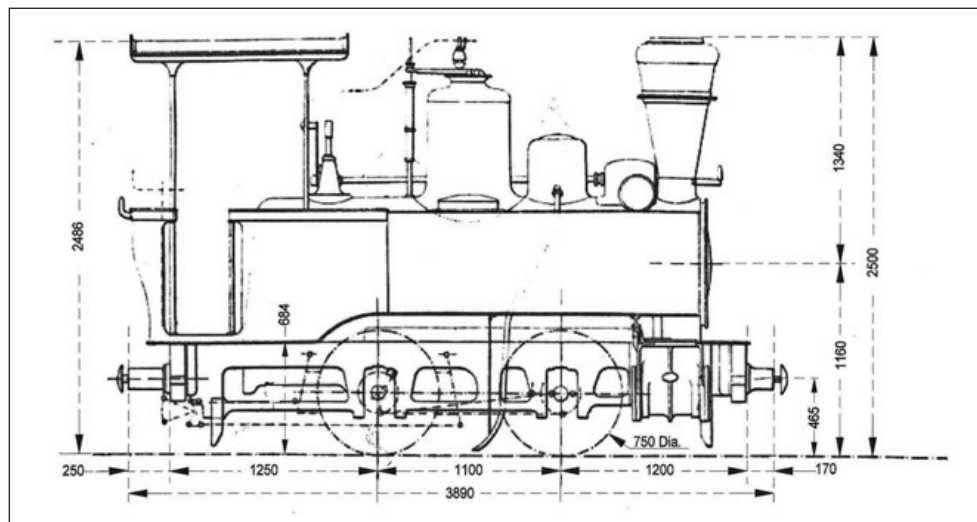
I was delighted to see the article by Dick Paterson titled *First French Railway in China* in *NGW* 165. This article is nicely illustrated and gives a good basic description of efforts to introduce railways to China.

I am afraid that the final part of the article with regards to Jardine's relationship with Decauville is not supported by hard evidence. I spent considerable time in the Cambridge University library studying the Jardine records and could find no evidence that the company bought the Decauville railway.

The Tien-Tsing Decauville Railway is described in considerable detail in English-language Chinese newspapers. I was lucky to find these six or seven years ago. These tell us that the Decauville railway was offered to the Chinese for installation at the Imperial Palace in Beijing but the price was too high and a French entrepreneur, a Monsieur Thévenet offered a suitable railway at a much lower price. The newspapers report that the Decauville track (and rolling stock?) went to Tien-tsing town and was afterwards sold on.

There is some evidence that the locomotive was returned to France. This locomotive was built by Couillet in 1884 and given the Decauville number 26 and name 'Fedora'. It was eventually sold to Soc. des Mines de Bormettes in 1889. This indicates that it was the property of Decauville from 1884 to 1889.

The railway offered by Mr Thévenet is described in *La Genie Civil*, 15th September 1888. Peter Crush has been researching Chinese Railways for many years and was the source of this article. **Jeff Lanham**



The diagram above is from *La Genie Civil* in September 1888, as mentioned in Jeff Lanham's letter and has been cleaned up by Dick Paterson. It shows an 800mm gauge loco supplied for a railway in the Chinese Imperial Palace Gardens.

The feature in *NGW* 166 on the Bicton Woodland Railway reminded Michael Farr, a regular *NGW* correspondent, of the time he tried the cab of the loco 'Woolwich' for size (below) – see his letter.



Dick Paterson replies: I am grateful for the response from Jeff Lanham to my article. The additional information is most welcome. His bone of contention seems to be that Jardine did not buy the Decauville equipment for the demonstration railway at Tien-Tsing. If Jardine did not pay for it, who did?

According to Maggie Keswick, (a descendant of Jardine Matheson's co-founder William Jardine) in *The Thistle and the Jade...*, Gaston Galy, Decauville's representative for China, had signed an agreement with Jardine Matheson to co-operate in an attempt to sell miniature railway equipment to the Chinese Government. Was Jardine canny enough to persuade Decauville to supply the railway at its own risk and expense against the hope that it would lead to substantial business in China?

The significance of the article in *Le Genie Civil* is that there were two 'Toy' trains in China in the late 1880s: the 600mm gauge demonstration train depicted in my article and an 800mm gauge train in the Imperial Palace Gardens.

This was supplied through Mr. Thevenet with a 0-4-0 locomotive by J. F. Cail of Chaillot, Paris and five luxurious bogie carriages 11 metres long by 2.2 metres wide built in Lyon. These are the carriages pulled by eunuchs in the Palace Gardens because the Empress did not like the sound of the steam train. Health and Safety at Work?

This is an interesting field, but the more we dig, the more loose ends we will find.

Andrew C adds: All fascinating stuff – as Dick says in his covering letter his objective has always been to lay out what we think we know and what is missing, and to hope that it prompts someone to offer the missing bit of the jigsaw, or correct a misunderstanding, as did Jeff.

Bicton memories

Thank you for publishing my letter on the Bala line and photos in *NGW* 167. It is inevitable when one has been interested in railways for 80 years that you read through a magazine and think; "I was there!" On page 18 I found another short but favourite line – Bicton Gardens.

In my early BBC days Bicton fell in the South and West area so I followed up the news of the opening and soon made friends with Mr James, the agent; I was subsequently invited to special events, such as the opening of the branch line to the Hermitage to record interviews.

When our two boys arrived Bicton became a favoured family outing and before long I built (notionally for them) a small portable layout of the terminus, from measurements taken on site. I always thought the balloon-shaped track layout was ideal for a larger model railway.

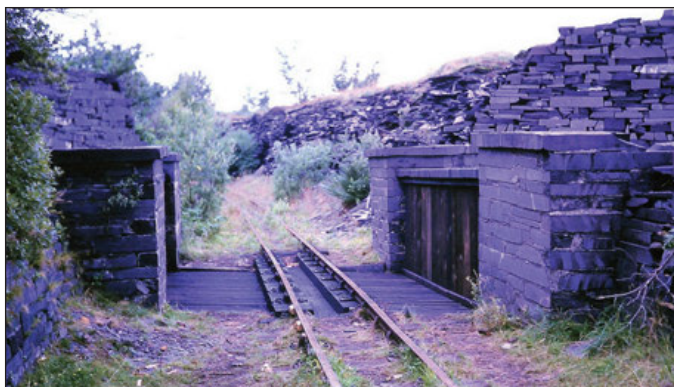
I used 3/8in scale on 00 gauge, Triang/Hornby wagon underframes and bogies for the rolling stock and Triang Transcontinental diesel power bogies for 'Carnegie', the bogie diesel, and also as the complete power unit for the small four-wheeled diesels. The steam loco 'Woolwich' was cobbled together from a Triang Polly and a lot of Plastikard!

When we moved to Hampshire I gave the layout to the Bicton Gardens Museum, but not the beautiful hand-built model of Woolwich made for me by Dennis Martin of Bedford, who had seen the loco at the Arsenal. I still have this and can't bring myself to paint it blue for fear of spoiling it.

I took the model to Bicton and arranged a photo-shoot with the local paper as the loco's regular driver at that time was also called Dennis Martin! The press photographer, Freddie Collins, also took the attached photo of me in charge – the Lord help the passengers!

Michael Farr

From a different age...



Could Penrhyn have been preserved?

The letter in *NGW* 165 from Alisdair McNichol, disagreeing with a historical report published in the previous issue on the state of the Penrhyn Railway after closure, prompted Robin Willis, membership secretary of the Penrhyn Quarry Railway Society (www.penrhynqrs.co.uk), to get in touch with more information.

According to Alisdair the report reprinted in *NGW* 164 from the Narrow Gauge Railway Society's *Narrow Gauge News* of March 1962, in which the Penrhyn main line was described as being in "very poor shape indeed", was wrong – he made a survey of the line in Spring 1964 and described it as "a little overgrown in places but mostly very solid."

Robin has now provided us with a copy of a survey that was undertaken by the Merseyside Model Railway Society in April of the same year, and printed in 2009 in the PQRS's excellent quarterly and mainly historical journal *Y Llechen–The Slate*.

The Merseyside members were prompted into action by a letter in the *Railway Magazine* from the University College of Bangor Railway Society, enquiring after the prospects for preserving the Penrhyn.

They started their survey at Port Penrhyn and worked their way along the entire line to Felin Fawr works and into the quarry. They carried 'tick sheets' with space for comments, examined every over and under-bridge, and measured the gauge of the track at 11 different sites. They also considered heritage railway requirements, such as a station at each end, car parking and storage for locos and rolling stock.

The result was an extensive report which space allows us only to provide a flavour of here. It was duly submitted to the Bangor Society and nothing more was heard of it! No-one involved knew at the time that the quarry owners were negotiating to sell all its assets to building giant McAlpine's, which a preservation effort would not have aided.

The report covered permanent way, rolling stock and facilities. The track was said to average 1ft 11-inch gauge but varying from 1ft 10 $\frac{1}{8}$ to 1ft 11 $\frac{1}{16}$. Sleepers

were "good on mainline, okay at Quarry, poor at Port Penrhyn." Most of the mainline fishplates were present, though around 1 in 20 were missing at the Port and 50 per cent at the quarry.

Some good, some not

The alignment was said to be good on the main line but poor at the Port, while the quarry was described as "shocking"... One stretch of track near Felin Hen had been slightly buckled by a fire.

Only "three or four" stub points were working – the others required "digging out and oiling." Several "normal" points were found, "all in good condition."

The ballast was described as "old dirt but well drained," except in one cutting. Track was overgrown on the quay, only slightly on the main line and clear at the quarry.

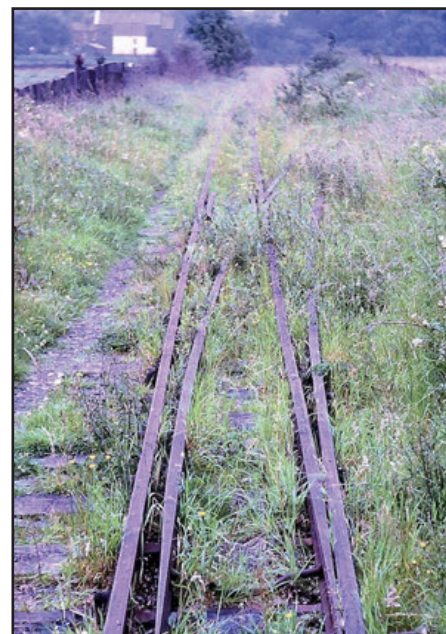
The surveyors concluded that the bridges were mostly in good condition, only one having a not-too serious crack in the bricks. Fencing was in good condition too.

In terms of facilities, the Port had a two-road steam shed (still there today), a carriage shed and another shed that could be turned into a repair shop, while the loading bank could be used as a platform and another building as a booking office. There was plenty of car parking space too, though it was concluded passenger access at the quarry end was poor.

The team even surveyed all the machine

tools they found, analysing whether they could be re-used in a preserved operation. Only one was dubbed "useless".

The full survey certainly makes for interesting reading. "The reader may form the opinion that the railway had been neglected but could easily have been repaired to become an iconic preserved railway," Robin comments. Indeed – what might have been... **NGW**



These photos from July 1963, a year after the Penrhyn line closed were all provided from the PQRS archive.

At top left is St Anne's Bridge at Felin Fawr and top right the track and bridge at Felin Hen – the scene has vastly changed in these areas over the years with the bridges having long gone.

Above right is one of the points at a 'pass-bye' (passing loop), and at right the quarry workshops at Felin Fawr – the end of the infamous 'scrap line' is just visible.



That was the year that...

Narrow gauge news stories from the archives and their legacy...

From 20 years ago

Corris Railway Corris, Gwynedd (2ft 3in gauge)

On 3rd June 2002 the Corris Railway ran its first public train for 72 years, comprising Ruston & Hornsby 4wDM no 6, a single coach for 16 passengers, and a brake van. The train operates an hourly service between Corris station and the shed at Maespoeth, where a short talk on the history of the line is given. (NGN, July 2002)

East Hayling Light Railway Hayling Island, Hampshire (2ft gauge)

After an unexpected delay, tracklaying has now commenced. By the end of May, most of the track and pointwork in Beachlands station had been laid, but not ballasted. Once it is completed and the rolling stock can be shunted in the yard, work on the mainline to Eastoke will begin. (NGN, July 2002)

Lynton & Barnstaple Railway Woody Bay Stn, North Devon (60cm gauge)

The railway's board has announced that tracklaying will commence at Woody Bay in August, once the supply of new sleepers, rail fixings and some brand-new rail has been delivered.

Public operations will commence on 18th April 2003, when visitors will be able to ride down the line on a specially constructed works train, as far as bridge 67 (currently under construction).

(NGN, July 2002)



Definitely an encouraging issue of Narrow Gauge News! All three lines soon progressed and tracklaying at The L&B actually started that June as in the picture at right, featured in NGW130 – the young Murray Dodds to the rear now manages the Fairbourne Railway. The three lines are now long-established and while the East Hayling line makes few headlines the Corris and Lynton & Barnstaple are both moving towards future major expansion, though both yet to extend much beyond the length of lines described in 2002.

Welshpool & Llanfair Light Rly Llanfair Caereinion, Powys (2ft 6in gauge)

The railway celebrated the centenary of 'cutting the first sod' on Saturday 4th May... the Earl of Powis re-enacted the Cutting of the First Sod on the lawn outside the Raven Square terminus in company with chairman Ken Fenton, and with music provided by members of the Newtown Silver Band. The last time this band had played at the railway was when the last train under British Railways ran in 1956. Amazingly, several members of the band were present on both occasions! (NGN, July 2002)



Your editor remembers this event very well! Inspired by the suitably period outfits worn by those performing the first sod recreation, when the centenary celebrations a year later included an 'Edwardian Weekend' I persuaded my entire family to dress accordingly (right). I don't think my three children have ever quite forgiven me...

Extracts from **Narrow Gauge News**, the news journal of the Narrow Gauge Railway Society – for more details of the Society and how to join up, go to www.ngrs.org.uk or see the advert on page 45.



Diary & Special Events

■ No responsibility can be accepted for dates on these pages and anyone planning a visit for a ride or to an event should check the line's website and/or social media feeds for the latest update before travelling. Not all trains operate over full lines – on some days for example the Ffestiniog Railway only runs from Porthmadog to Tan-y-Blwch.

Special Events in June

Only events of interest to rail enthusiasts are included in this guide.

■ **Wed to Sun in July, Devon Railway Centre Open Day** Museum nr Tiverton, 2ft gauge line. <http://devonrailwaycentre.co.uk>

■ **Every Saturday, Great Laxey Mines Railway operating;** novel 19-inch steam line on Isle of Man. www.laxeyminesrailway.im

■ **2nd July, West Lancashire Light Railway Volunteers Day;** Behind the scenes tours aimed at potential new recruits

■ **2nd July, Amberley Museum Steam Show;** Bagnall 'Polar Bear' steam loco on railway, road steam operating around site

■ **9th-10th July, Evesham Vale Light Railway Gala;** Visiting steam locos from Cleethorpes and Rhiw Valley, intensive service

■ **9th-10th July, Leek & Rudyard Railway Isle of Mull Gala;** Recalling Mull & West Highland Railway using line's original stock.

■ **16th July, Romney Hythe & Dymchurch Railway 95th Anniversary Special;** Train celebrating reopening on 16th July 1927

■ **16th-17th July, Cleethorpes Coast Lt Railway Summer Steam;** All locos in operation, visiting locos, miniatures and models

■ **24th July, Gartell Light Railway Open Day;** 2ft Somerset line, frequent timetable. 01963 370752, <http://newglr.weebly.com>

■ **27th-31st July, Isle of Man Railways Summer Transport Festival;** all steam and electric lines, details at www.rail.im

■ **27th, 30th-31st July, Groudle Glen Railway;** for Isle of Man Heritage Transport Festival, all-day Wed, Victorian Sat, Gala Sun

■ **30th-31st July, Welsh Highland Rly Centenary Celebration;** Heritage trains between Dinas and Rhyd Ddu

■ **7th Aug, Rhiw Valley Light Railway Open Weekend.** 15in gauge line close to Welshpool & Llanfair. www.rvlr.co.uk

Tramways

- **Douglas Horse Tramway** www.iombusandrail.im/heritage/ No reopening date announced at press time
- **Great Orme Tramway:** Conwy 01492 577877, www.greatormetramway.co.uk Open daily between 10am and 6pm
- **Manx Electric Railway, Snaefell Mountain Railway:** Isle of Man, 01624 662525, www.iombusandrail.im/heritage/ Daily services, Summer Transport Festival 27-31 July
- **Seaton Tramway:** Devon 01297 20375, www.tram.co.uk Daily services from 10am

Miniature Railways

- **Audley End Railway** (10.25in): Essex 01799 541354, www.audley-end-railway.co.uk Daily services
- **Beer Heights Light Railway** (7.25in): Devon 01297 21542, www.pecorama.co.uk Daily services.
- **Lappa Valley Railway** (15, 10.25, 7.25in): Cornwall 01872 510317, www.lappavalley.co.uk Open daily.
- **Littlehampton Miniature Railway** (12.25in): Sussex www.littlehamptonminiaturerailway.com Weekends and 26th-31st
- **Moors Valley Railway** (7.25in): Hants 01425 471415, www.moorsvalleyrailway.co.uk Daily services
- **North Bay Railway, Scarborough** (20in): Yorks 01723 368791, www.nbr.org.uk, Daily services
- **Rhyl Miniature Railway** (15in): Clwyd 01352 759109, www.rhylminiaturerailway.co.uk Friday-Sunday, daily from 15th
- **South Downs Light Railway** (10.25in): Sussex 07518 753784, www.south-downs-railway.com Weekends, 27th

Visit our forum: <http://www.narrow-gauge-forum.co.uk>

What's On	1st	2nd	3rd	4th	5th	6th	7th	8th	9th	10th	11th	12th	13th	14th	15th	16th	17th	18th	19th	20th	21st	22nd	23rd	24th	25th	26th	27th	28th	29th	30th	31st
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Steaming to a Hungarian Spa



Donald Brooks recalls a 1994 trip over a Hungarian line that has recently reopened.

While the Covid-19 years of 2020/21 provided a consistent diet of bad news for rail enthusiasts, with passengers draining away from the main-line networks and heritage lines forced to adjust to completely new systems of working, there was the occasional good news story.

One of Europe's major railways, MÁV (Magyar Államvasutak or Hungarian State Railways), provided an example on 1st July 2021 when it reopened a narrow gauge branch closed in 2002. The line in question, to Csisztaföld on the Balatonfenyves system, exudes real narrow gauge charm, with short trains trundling through the flat rural landscape, albeit on newly relaid track.

The Balatonfenyvesi Gazdasági Vasút was one of several light railways designed to open up underdeveloped areas for agriculture after the Second World War. Construction of the 760mm gauge network on the

southern shore of Lake Balaton started in 1950 and continued for most of the decade, ultimately extending to roughly 50km. The first five kilometres south from the lakeside standard gauge interchange at Balatonfenyves to Imremajor were built on the route of an earlier 600mm gauge horse-worked line.

Motive power was provided by C50 four-wheel diesel tractors, built by Északi in Budapest from the 1950s. The first closures came as early as 1968, but some routes survived because of the poor state of the local roads, leaving a system of three branches for me to encounter when I first visited in 1994.

The main route, which has continued to see passenger trains throughout apart from a short break for reconstruction, runs for 14km in a mainly southerly direction to Somogyszénpál. Branching off eastwards at Imremajor another route

Above: The end of the line. C50 5713 runs round at Csiszta gyógyfürdő. In the background is 490.056, which spent some hours here with the nostalgia train.

Below: 490.056 approaches Balatonfenyves near the end of its journey with the return nostalgia service from Csiszta. This ran daily in July and August, with a special fare, and otherwise only when chartered.

Photos by Donald Brooks, summer 1994

continued to a further junction with lines running south to Táska and east to Csiszta gyógyfürdő, both 15kms from Balatonfenyves. The Csiszta branch was extended for one kilometre as late as 1987 to serve the thermal baths referred to in the second element of the station name.

Steam specials

The Táska branch was very much a poor relation with just a couple of daily services negotiating its overgrown tracks, while Csiszta saw extra summer services for tourists from the lake, including steam specials. These were worked by 0-8-0T 490.056, Budapest 5848 of 1950, with a train of special stock.

I travelled on the 12.00 diesel service from Balatonfenyves, which provided a return trip along the Táska branch on its way to Csiszta. During my five minutes at Csiszta, before the train returned directly to the lake, there was just time to photograph 490.056 during its own protracted layover. Back at Balatonfenyves well before the steam train returned, I walked out to capture a further photo as it approached.

A serious derailment in 2001 led to discussions about the network's future, ultimately bringing about closure in September 2002 apart from the short section to Imremajor. Following rebuilding, passenger traffic was quickly restored to Somogyszénpál, but 19 years elapsed before Csiszta saw trains again. Larger Mk48 diesels are now regular performers on the improved track while steam is also expected to make a comeback. **NGW**





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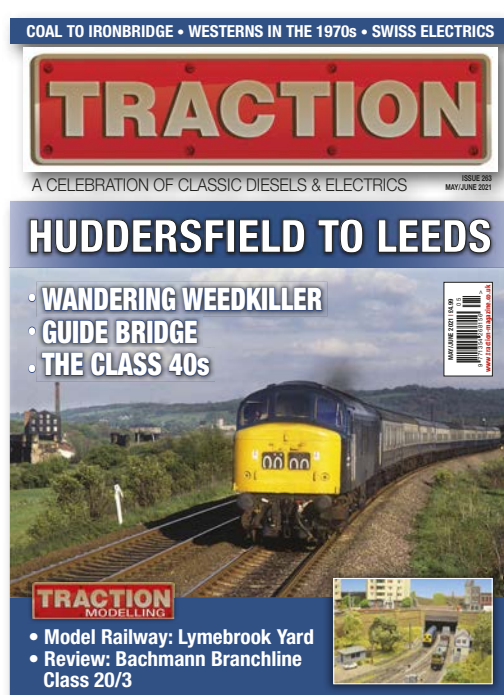
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