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LAYOUT

Heaton Lodge Junction

We speak to Simon George about this mammoth project and take a tour of the layout.

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



PLAY

PRACTICAL

Boats for railway modellers

Where can a railway modeller find suitable boats for their layout?

Phil Parker looks at some options.



PLAY

FEATURE

Observing bridge and viaduct details

Touring bridges and viaducts, culminating in a visit to the UK's longest masonry viaduct in Rutland, Howard Smith shares ideas to help make your model structures more accurate and interesting.



PLAY

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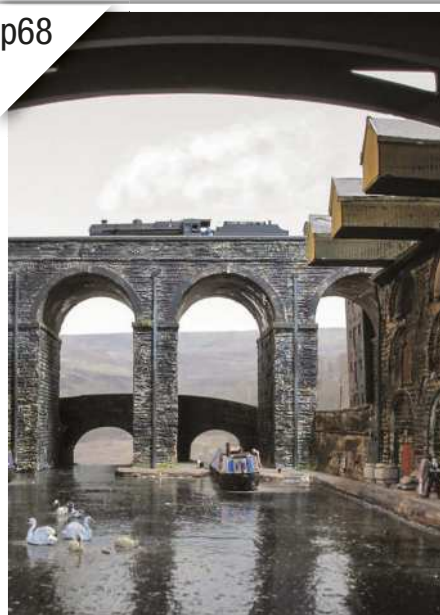
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Arguably the most impressive of railway features and of many different styles, these raised structures make equally imposing models, as Clive Baker shares.

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The new iTraveller 6000 Train set from Hornby, the latest model figure range from ModelU, new liveries for the Oxford Rail 12T tank wagon, plus a range of new military tents from Gaugemaster are just some of the latest products to arrive at BRM's headquarters.

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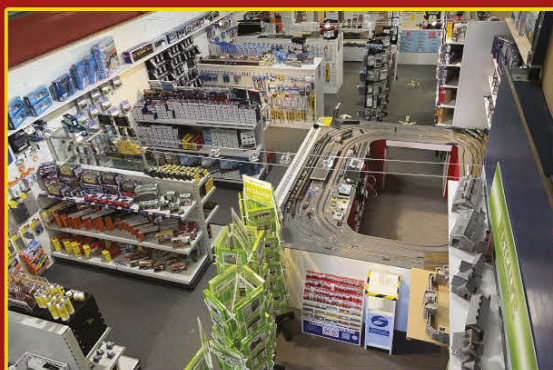
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Welcome to September

When first-time layout builders look for a project, understandably, they usually start small. Clearly Simon George missed the brief, who after seven brave years of construction, has completed his heroic 'Heaton Lodge Junction' layout – possibly the largest indoor O gauge layout we're ever likely to see tour the country.

What started as a project to recreate his youth, quickly grew to a 200-baseboard monster, measuring more than 180ft in length and requiring a setup time measured in days, not hours! We've visited 'Heaton Lodge Junction' many times over the course of the build and have been in awe at the scale of this project and how it quickly offers immersive viewing. You really have to see it to believe it – and, now you can! Britain's biggest model railway will be going on tour to the viewing public soon. Look out for dates in BRM or on our website, World of Railways.

Before embarking on 'Heaton Lodge Junction', Simon spent the best part of 18 months learning and practising the techniques needed to build his first model

railway. In September, we'll offer the same on our website, World of Railways, as we run our first Skills Week, to help other would-be builders gain all the information they need to start their own projects.

The free-to-attend online event from September 20 runs until Friday, September 24, with a different theme on offer each day, covering all major modelling topics including trackwork, weathering, scenery and layout control. It's a week not to be missed – tell your friends, too!

Another two great layouts in this issue arrive in the shape of 'Watery Lane Sidings' – an extension to 'Charlie Strong Metals', a highly-accomplished scrapyard layout, previously featured in BRM February 2021. Our final layout is the branch line terminus of Rushwick, one of many layouts created during lockdown. Cunningly, its creator has recycled and re-used products when possible – something we can all learn from.

On top of the layouts, we're also taking a closer look at arguably the most impressive railway feature, the viaduct, in this issue,

with a build of Arten Gill by skilful modeller Jeff Grainger. Plus, railway historian, Clive Baker takes us for a tour around the country, visiting some iconic structures that can inspire many a modeller. Meanwhile Andy York picks some of his favourite layouts to feature viaducts.

Also inside your September issue, skilled modeller, Jamie Warne, takes us through everything you need to know about choosing the right kind of lighting for your model railway, Phil Parker tackles an N gauge card kit and Howard Smith demonstrates introductory weathering techniques.

Learning new skills is all part of the fun of railway modelling and, as Simon George demonstrates, there really is no limit on what can be achieved. If you'd like to brush up on some of your modelling techniques, make sure you join us at the World of Railways virtual Skills Week (September 20-24). Sign up to our weekly newsletter at www.world-of-railways.co.uk to get regular updates. We'll look forward to welcoming you.

Happy Modelling!

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Although it's tricky to visit the shops at the moment and collect the latest issue, there are a number of ways you can get BRM delivered straight to your door or inbox!

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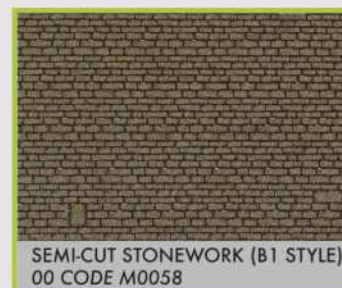
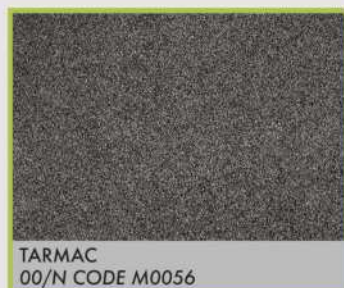
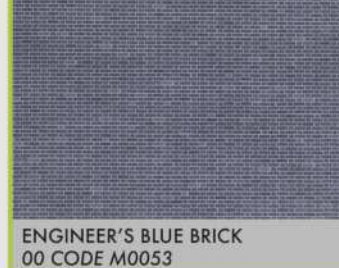
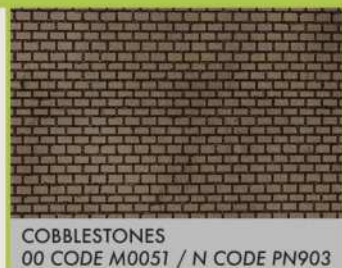
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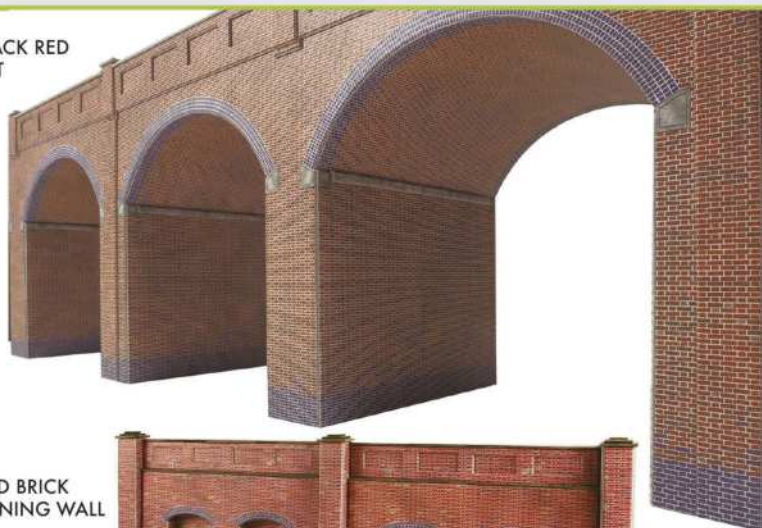
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31-741 Bachmann OO Gauge MR 1532 (1P) Tank 1303 LMS Black (Original)	£149.95	£119.96
31-741SF Bachmann OO Gauge MR 1532 (1P) Tank 1303 LMS Black (Original)(DCC Sound Fitted) Real Coal and Weathered by TMC	£289.95	£231.96
31-741 Bachmann OO Gauge MR 1532 (1P) Tank 1303 LMS Black (Original) Real Coal and Weathered by TMC	£189.95	£151.96
31-742SF Bachmann OO Gauge MR 1532 (1P) Tank 58072 BR Lined Black E/Emb (DCC Sound Fitted) Real Coal TMC Weathered	£289.95	£231.96
31-761 Bachmann OO Gauge GNR Class C1 4-4-2 Atlantic no.272 GNR Green Real Coal & Weathered by TMC	£239.95	£191.96
31-911SF Bachmann OO Gauge H1 Atlantic 4-4-2 Portland Bill 2038 SR Malachite Green (DCC Sound) Coal and Weathered by TMC	£349.95	£279.96
31-921A Bachmann OO Scale LB&SCR H2 Atlantic 32425 'Trevose Head' BR Lined Black (Early Emblem)	£219.95	£175.96
31-921ASF Bachmann OO Scale LB&SCR H2 Atlantic 32425 'Trevose Head' BR Lined Black (Early Emblem)	£309.95	£247.96
31-921ASF Bachmann OO Gauge H2 Atlantic 4-4-2 Trevose Head 32425 BR Black E/Emb (DCC Sound) Coal and Weathered by TMC	£349.95	£279.96
31-921A Bachmann OO Gauge LB&SCR H2 Atlantic 4-4-2 Trevose Head 32425 BR Lined Black E/Emb Coal and Weathered by TMC	£259.95	£207.96
31-921 Bachmann OO Gauge 4-4-2 H2 Class Atlantic 32424 Beachy Head BR Black E/Emb Real Coal Plates Fitted & TMC Weathered	£234.95	£187.96
31-922SF Bachmann OO Gauge LB&SCR Class H2 Atlantic 4-4-2 no.422 LBSCR Umber (DCC Sound) Coal and Weathered by TMC	£359.95	£287.96
31-922 Bachmann OO Gauge LB&SCR Class H2 Atlantic 4-4-2 no.422 LBSCR Umber Real Coal and Weathered by TMC	£269.95	£215.96
32-002A Bachmann OO Gauge GWR 49XX Stanway Hall 4971 BR Lined Black E/Emb Real Coal Plates Fitted & Weathered by TMC	£217.45	£173.96
32-067 Bachmann OO Gauge Class 43 Warship 842 Royal Oak BR Blue with Deluxe Weathering by TMC	£194.95	£155.96
32-166 Bachmann OO Gauge SE&CR N Class 2-6-0 no. 1406 (Sloping-Top Tender) SR Black (Sunshine) Real Coal TMC Weathered	£199.95	£143.96
32-178A Bachmann OO Gauge LMS 5MT 'Crab' with Welded Tender 13174 LMS Lined Black (Original)	£144.95	£108.71
32-205A Bachmann OO Gauge GWR 8750 Class Pannier Tank 8771 BR Lined Black E/Emb Real Coal and Deluxe TMC Weathered	£149.45	£119.56
32-205A Bachmann OO Gauge GWR 8750 Pannier Tank 8771 BR Lined Black E/Emb Real Coal and Weathered by TMC	£159.95	£127.96
32-376A Bachmann OO Gauge Class 37/4 (Refurb) 37422 Robert F. Fairlie BR Regional Railways Nameplates Fitted TMC Weathered	£204.96	£163.97
32-389TLD5 Bachmann OO Gauge Class 37/4 37416 'MT Fuji' BR Mainline (DCC Sound)	£279.95	£223.96
32-394DS Bachmann OO Gauge Class 37/5 Refurbished 37521 Colas Rail Freight DCC Sound Parts Fitted (1 end) TMC Weathered	£307.45	£245.96
32-441SF Branchline OO Gauge Class 24/1 D5149 BR Green (SYP)(DCC Sound) with Deluxe Weathering by TMC	£329.95	£263.96
32-485 Bachmann OO Scale Class 40 Centre Headcode D365 BR Green (Small Yellow Panels)	£188.95	£151.16
32-529C Bachmann OO Cl. 55 Deltic D9010 The King's Own Scottish Borderer BR 2-Tone Green Nameplates Fitted TMC Weathered	£219.95	£175.96
32-532A Bachmann OO Gauge Class 55 Deltic 55003 Meld BR Blue Nameplates Fitted and Weathered by TMC	£219.95	£175.96
32-533 Bachmann OO Gauge Class 55 Deltic D9001 St. Paddy BR Two-Tone Green FYE Nameplates Fitted and Weathered by TMC	£214.95	£171.96
32-575A Bachmann OO Gauge LMS Ivatt 4MT Double Chimney 3000 LMS Black (Revised)	£139.95	£83.99
32-612 Bachmann OO Gauge Class 90 90042 Freightliner Powerhaul	£194.95	£155.96
32-683RJSF Bachmann OO Gauge Class 45/0 Lytham St. Annes 45022 BR Blue (DCC Sound Fitted) with Deluxe Weathering by TMC	£299.95	£239.96
32-734BSF Bachmann OO Gauge Class 66/0 66117 DB Cargo (DCC Sound Fitted) Weathered by TMC	£259.95	£207.96
32-734B Bachmann OO Gauge Class 66/0 66117 DB Cargo Weathered by TMC	£179.95	£143.96
32-734CSF Bachmann OO Gauge Class 66/0 66100 Armistice 100 1918-2018 DB Cargo (DCC Sound) Plates and Weathered by TMC	£294.95	£235.96
32-734C Bachmann OO Gauge Class 66/0 66100 Armistice 100 1918-2018 DB Cargo Nameplates Fitted and Weathered by TMC	£194.95	£155.96
32-738Z Bachmann OO Gauge Class 66 66136 YWU-LONDON DB Livery 'Exclusive TMC Edition' with VALUE Weathering by TMC	£189.95	£142.46
32-739SF Bachmann OO Gauge Class 66/4 66419 Freightliner G&W (DCC Sound Fitted) Weathered by TMC	£262.45	£209.96
32-740SF Bachmann OO Class 66/7 66789 British Rail 1948-1997 GBRf Blue Large Logo DCC Sound Plates Deluxe TMC Weathered	£284.95	£227.96
32-741 Bachmann OO Gauge Class 66/7 66783 The Flying Dustman GBRf Biffa Red Nameplates Fitted and Weathered by TMC	£184.95	£147.96
32-741SF Bachmann OO Gauge Class 66/7 66783 The Flying Dustman GBRf Biffa Red (DCC Sound) Plates Fitted TMC Weathered	£264.95	£211.96
32-753DS Bachmann OO Class 57/0 57007 Freightliner Bond Freightliner Green (DCC Sound Fitted) Nameplates TMC Weathered	£304.95	£237.45
32-755A Bachmann OO Gauge Class 57/3 no.57314 Arriva Trains Wales (Revised) Weathered by TMC	£214.95	£171.96
32-756ASF Bachmann OO Gauge Class 57/6 Restormel Castle 57602 GWR Green (FirstGroup)(DCC Sound) Weathered by TMC	£304.95	£243.96
32-756A Bachmann OO Gauge Class 57/6 Restormel Castle 57602 GWR Green (FirstGroup) Weathered by TMC	£214.95	£171.96
32-763A Bachmann OO Gauge Class 57/3 Chad Varah 57302 DRS Compass (Original) Nameplates fitted and Weathered by TMC	£219.95	£175.96
32-765SF Bachmann OO Gauge Class 57/3 no.57313 WCR Maroon (DCC Sound Fitted) Weathered by TMC	£324.95	£259.96
32-765 Bachmann OO Gauge Class 57/3 no.57313 WCR Maroon Weathered by TMC	£234.95	£187.96
32-778RJ Bachmann OO Gauge Class 37/0 37275 Östanslän Pioneer ÖR Railfreight Metals Sector	£179.95	£143.96
32-830A Bachmann OO Gauge LMS Ivatt 2MT 2-6-0 no.6418 LMS Black (Revised) Real Coal and Deluxe Weathering by TMC	£204.95	£163.96
32-900C Bachmann OO Gauge Class 108 2-Car DMU BR Green (Speed Whiskers) Weathered by TMC	£289.95	£231.96
32-908 Bachmann OO Gauge Class 108 2-Car DMU BR Blue & Grey Weathered by TMC	£289.95	£231.96
32-983 Bachmann OO Gauge Class 66/7 Evening Star 66779 GBRf Brunswick Green Etched Plates Bells and Weathered by TMC	£235.25	£188.20
32-991 Bachmann OO Gauge Wickham Type 27 Trolley Car BR Maroon Ballast Load and Weathered by TMC	£116.95	£93.56
32-992 Bachmann OO Gauge Wickham Type 27 Trolley Car BR Departmental Yellow	£89.95	£71.96
32-992 Bachmann OO Gauge Wickham Type 27 Trolley Car BR Engineers Yellow Weathered by TMC	£109.96	£87.97
32-994 Bachmann OO Gauge Wickham Type 27 Trolley Car BR Green Ballast Load and Weathered by TMC	£116.95	£93.56
35-051 Bachmann OO Gauge LNWR Webb Coal Tank 7841 LMS Black Real Coal Glossed & Weathered by TMC	£164.95	£127.96
35-126 Bachmann OO Gauge Class 20/3 20311 Harry Needle Railroad Company Weathered by TMC	£209.95	£167.96
35-175 Bachmann OO Gauge ROD 2-8-0 1918 Railway Operating Division Black Real Coal & Deluxe Weathered by TMC	£234.95	£187.96



Item Name	RRP	TMC Price
35-527 Bachmann OO Gauge Class 121 Single-Car DMU BR Network SouthEast (Revised) Weathered by TMC	£187.45	£149.96
38-032A Bachmann OO Gauge HHA Bogle Hopper Freightliner Heavy Haul Weathered by TMC	£64.95	£48.71
38-243 Bachmann OO Gauge MBA Megabox High-Sided Bogle Box Wagon EWS (with Buffers) Weathered by TMC	£59.95	£44.96
38-245A Bachmann OO Gauge MOA Low-Sided Bogle Box Wagon EWS Weathered by TMC	£54.95	£43.96
38-726 Bachmann OO Gauge WD 50T Warflat Bogle Wagon WD Bronze Green With Cromwell MKIV Tank Weathered by	£68.70	£51.53
38-801 Bachmann OO Gauge Ransomes & Rapier 45T Steam Breakdown Crane GWR Grey with Deluxe Weathering by	£309.95	£247.96
38-827Z Bachmann OO Gauge (ex-Double Bolster) Plate Wagons (Twin Pack) Freight Grey E310418/B920098 Weathered	£59.95	£47.95
39-002 Bachmann OO Gauge BR Mk2A BFK 2-Coach Pack HST Barrier Vehicle BR Blue VALUE Weathered by TMC	£103.95	£83.16
39-184B Bachmann OO Gauge BR Mk1 BG Brake Gangwayed Royal Mail Letters Coach Weathered by TMC	£68.95	£55.16
39-190 Bachmann OO Gauge BR Mk1 BG Brake Gangwayed Generator Van Network Rail Yellow VALUE Weathered by TMC	£59.95	£47.96
39-200D Bachmann OO Gauge BR Mk1 BG Brake Gangwayed Coach High Security Rail Express Systems Weathered by	£68.95	£55.16
39-225A Bachmann OO Gauge BR Mk1 BCK Brake Composite Corridor Coach BR Blue & Grey VALUE Weathered by TMC	£55.95	£44.76
39-242 Bachmann OO Gauge BR Mk1 FO First Open BR Blue & Grey Weathered by TMC	£58.95	£47.16
39-273 Bachmann OO Gauge BR Mk1 GUV General Utility Van BR (SR) Green VALUE Weathered by TMC	£55.95	£44.76
39-301A Bachmann OO/HO Scale BR Mk1 SK Pullman Second Kitchen Grey & Blue (With Lighting) Weathered by TMC	£78.95	£63.16
39-310C Bachmann OO Gauge BR Mk1 Pullman PSP Second Parlour '350' Umber & Cream (Grey Roof)	£79.95	£44.98
39-312 Bachmann OO Gauge BR Mk1 SP Pullman Second Parlour Coach Blue & Grey (With Lighting) Weathered by TMC	£87.45	£69.96
39-413 Bachmann OO Gauge BR Mk2A BFK Brake First Corridor BR Regional Railways - Weathered - Includes Passenger	£74.95	£40.77
39-421B Bachmann OO Gauge BR Mk1 POS Post Office Sorting Van Post Office Red (with Nets)	£69.95	£55.96
39-422 Bachmann OO Gauge BR Mk1 POS Post Office Sorting Van Royal Mail Travelling Post Office Weathered by TMC	£74.95	£59.96
39-470A Bachmann OO/HO LMS 60ft Porthole Corridor Brake 1st Coach BR Crimson & Cream Weathered by TMC	£54.95	£43.96
39-471 Bachmann OO Gauge LMS 60ft 'Porthole' Brake First Corridor BR Maroon	£54.95	£43.96
39-476 Bachmann OO Gauge LMS 60ft 'Porthole' First Vestibule BR Maroon	£54.95	£43.96
39-528A Bachmann OO Gauge SR CCT Covered Carriage Truck BR Blue Weathered by TMC	£50.45	£40.36
39-575 Bachmann OO Gauge BR Auto Trailer Crimson & Cream (Discontinued)	£68.95	£55.16
39-575 Bachmann OO Gauge BR Auto Trailer Crimson & Cream Weathered by TMC	£78.95	£63.16
39-576 Bachmann OO Gauge BR Auto Trailer Maroon (Discontinued)	£68.95	£55.16
39-577 Bachmann OO Gauge BR Auto Trailer Crimson (Discontinued)	£68.95	£55.16
39-579 Bachmann OO Gauge GWR Hawksworth Auto-Trailer BR Maroon	£79.95	£44.97
39-580 Bachmann OO Gauge GWR Hawksworth Auto-Trailer BR (WR) Chocolate & Cream	£74.95	£44.97
39-580 Bachmann OO Gauge GWR Hawksworth Auto-Trailer BR (WR) Chocolate & Cream Weathered by TMC	£85.45	£68.36
39-603 Bachmann OO Gauge SE&CR 60ft Birdcage Brake Third Lavatory Coach SR Malachite Green Weathered by TMC	£89.95	£67.46
39-604 Bachmann OO Gauge 60ft Birdcage Brake Composite Lavatory Coach SE&CR Wellington Brown Weathered by TMC	£89.95	£67.46
39-610A Bachmann OO Gauge SE&CR 60ft Birdcage Composite Lavatory Coach SE&CR Dark Lake Weathered by TMC	£94.95	£71.21
39-685 Bachmann OO Gauge BR Mk2F RFB Restaurant First Buffet Coach BR Blue & Grey Weathered by TMC	£69.95	£55.96
R3086 Hornby Railroad OO Gauge LNER A1 Class 4-6-2 no.4472 Flying Scotsman (Era 3) Real Coal & Weathered by TMC	£151.00	£113.24
R3098 Hornby HO/OO Gauge Tornado Loco Late BR Etched Nameplates Special Edition	£119.99	£95.99
R3223 Hornby HO/OO Gauge BR 2-8-0 42XX Class	£119.99	£95.99
R3233 Hornby OO/HO Gauge LNER 4-4-0 D16/3 Class 8825 Real Coal Parts Pack & Weathered by TMC	£164.99	£131.99
R3235 Hornby OO/HO Gauge BR 4-4-0 D16/3 Class E2524 Early BR (1948) Coal Parts Pack & Weathered by TMC	£164.99	£131.99
R3332 Hornby OO Gauge BR 4-6-0 King Edward VIII 6029 BR L/Crest Real Coal and Weathered by TMC	£204.99	£163.99
R3333 Hornby OO Gauge BR 4-4-2T Adams Radial 415 Class - Early BR	£131.99	£105.59
R3359 Hornby Railroad OO Gauge Rothery Industries Ex-GWR 101 Class 0-4-0T 391 (Era 4/5) Real Coal & Weathered by	£74.99	£56.24
R3360 Hornby OO/HO Gauge Railroad Ex-SR 0-4-0 No. 087 'Mosley Tarmacadam' Weathered By TMC	£62.50	£50.00
R3376 Hornby OO Gauge BR, Class 71, Bo-Bo, E5022 - Era 6	£170.99	£136.79
R3380 Hornby OO Gauge LNER 0-6-0 7510 J15 Class Real Coal & Weathered by TMC	£149.99	£119.99
R3417 Hornby OO Gauge BR Class K1 2-6-0 no.62065 BR Black L/Crest (Era 5) Real Coal and Weathered by TMC	£145.99	£116.79
R3421 Hornby OO Gauge BR DRUMMOND 700 '30698'	£124.99	£99.99
R3534 Hornby OO Gauge GWR 6000 King Class 4-6-0 6023 King Edward II (Era 3) Real Coal & Weathered by TMC	£226.00	£169.50
R3552 Hornby OO Gauge GWR 6800 Grange Class 4-6-0 6860 Aberporth Grange (Era 3) Real Coal and Deluxe Weathering	£200.99	£160.79
R3568 Hornby OO Scale BR, Class 71, Bo-Bo, E5018 - Era 6	£170.99	£136.79
R3568 Hornby OO Gauge BR Class 71 Bo-Bo E5018 (Era 6) Weathered by TMC	£200.99	£160.79
R3585 Hornby OO Gauge Railroad BR Class 90 Intercity 90135 Weathered by TMC	£124.99	£99.99
R3614 Hornby OO Gauge LMS Patriot Class 4-6-0 Rhyl 5521 (Era 3) Real Coal Semi-Glossed then Weathered by TMC	£219.99	£175.99
R3615 Hornby OO Gauge United Alkali Co. Ltd, Peckett W4 Class, 0-4-0ST, 560 - Era 2	£100.00	£80.00
R3622 Hornby OO Gauge BR J36 Class 0-6-0 no.65311 Haig (Era 4) Real Coal and Deluxe Weathering by TMC	£199.99	£159.99
R3631 Hornby OO Gauge BR H Class 0-4-4T no.31265 (Era 4) Real Coal and Weathered by TMC	£169.99	£135.99
R3634 Hornby OO Gauge SR, Lord Nelson Class, 4-6-0, 851 'Sir Francis Drake' - Era 3	£169.99	£135.99
R3634 Hornby OO Gauge SR Lord Nelson Class 4-6-0 no.851 Sir Francis Drake (Era 3) Real Coal & Extra Detail Weathering	£214.99	£171.99
R3635 Hornby OO Gauge BR Lord Nelson Class 4-6-0 30863 Lord Rodney (Era 4) Real Coal & Weathered by TMC	£209.99	£157.49
R3640 Hornby OO Gauge Willans and Robinson Peckett Class W4 0-4-0ST Niclausse 882 (Era 2) with Deluxe Weathering by	£169.99	£135.99
R3657 Hornby OO Gauge Loadhaul Class 60 Co-Co 60070 John Loundon McAdam (Era 9) Weathered by TMC	£212.49	£169.99
R3660 Hornby OO Scale DCR, Class 56, Co-Co, 56303 - Era 10	£169.99	£135.99
R3670 Hornby OO Gauge Train Packs Class 72xx Freight Pack - Era 5	£194.99	£155.99
R3681 Hornby OO Gauge LMS Princess Coronation 4-6-2 6241 City of Edinburgh (Era 3) Coal Ash & Satin Textured	£250.00	£183.48
R3682 Hornby OO Gauge BR, Princess Coronation Class, 4-6-2, 46225 'Duchess of Gloucester' - Era 4	£204.99	£163.99
R3694 Hornby OO Gauge Associated Portland Cement Peckett B2 Class 0-6-0ST Westminster (Era 5) Coal and Deluxe	£152.50	£122.00
R3705 Hornby OO Gauge John Dewar & Sons R&H 48DS 0-4-0 No.458957 (Era 8) Weathered by TMC	£111.25	£83.44
R3706 Hornby OO Gauge War Department R&H 48DS 0-4-0 Army 802 (Era 4/5) Weathered by TMC	£114.99	£91.99
R3711 Hornby OO Gauge BR Princess Royal 4-6-2 Princess Marie Louise 46206 (Era 4) Real Coal and Weathered by TMC	£229.99	£183.99
R3713 Hornby OO Gauge BR Princess Royal 4-6-2 Princess Arthur of Connaught 46207 (Era 5) Real Coal & Weathered by	£229.99	£183.99
R3719 Hornby OO Gauge GWR Class 5101 Large Prairie 2-6-2T no.4154 (Era 3) Real Coal and Weathered by TMC	£169.99	£135.99
R3723 Hornby OO Gauge BR Class 61xx Large Prairie 2-6-2T no.6145 (Era 4) Real Coal and Deluxe Weathering by TMC	£199.99	£159.99
R3742F Hornby OO Scale DB Cargo Romania, Class 92, Co-Co, 91 53 0 472 001-3 Ōmihai EminescuŌ - Era 10	£109.99	£87.99
R3751 Hornby OO Gauge Caledonian Sleeper Class 87 Bo-Bo 87002 Royal Sovereign (Era 10) Weathered by TMC	£202.49	£161.99
R3752 Hornby OO Gauge Railroad S.Wilson Paper Mills 0-4-T no.2112 (Era 2/3) Real Coal & Weathered by TMC	£69.99	£52.49
R3767 Hornby OO Gauge BR Terrier 0-6-0T 32655 E/Emb (Era 4) Real Coal and Deluxe Weathering by TMC	£134.98	£107.98
R3769 Hornby OO Network Rail Class 43 HST Power Cars 43013 Mark Carne CBE 43014 The Railway Observer (Era 11)	£339.99	£271.99
R3773 Hornby OO Gauge BR Provincial Class 156 Set 156401 DMS 57401 and DMSL 52401 (Era 8) Weathered by TMC	£174.99	£139.99
R3781 Hornby OO Gauge K&ESR Terrier 0-6-0T No.5 Rolvenden (Era 2) Real Coal and Deluxe Weathering by TMC	£132.49	£105.99
R3823 Hornby OO Gauge LB&SCR 45 'Merton' Centenary Year Limited Edition - 1998	£104.99	£83.99
R3830 Hornby OO Scale BR, Thompson Class A2/2, 4-6-2, 60501 'Cock o' the North' - Era 4	£189.99	£151.99



35-527 Bachmann OO Gauge Class 121 Single-Car DMU BR Network SouthEast (Revised) Weathered by TMC
£149.96 Regular Price £187.45



R3098 Hornby HO/OO Gauge Tornado Loco Late BR Etched Nameplates Special Edition
£95.99 Regular Price £119.99



R3332 Hornby OO Gauge BR 4-6-0 King Edward VIII 6029 BR L/Crest Real Coal and Weathered by TMC
£163.99 Regular Price £205.00



R3380 Hornby OO Gauge LNER 0-6-0 7510 J15 Class Real Coal & Weathered by TMC
£119.99 Regular Price £149.99



R3568 Hornby OO Gauge BR Class 71 'E5018' BR Green
£136.79 Regular Price £170.99



R3622 Hornby OO Gauge BR J36 Class 0-6-0 no.65311 Haig (Era 4) Real Coal and Deluxe Weathering by TMC
£160.00 Regular Price £199.99



R3640 Hornby OO Gauge Willans and Robinson Peckett Niclausse 882 (Era 2) with Deluxe Weathering by TMC
£136.00 Regular Price £169.99

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Hill Farm, Beck Hole, Whitby, YO22 5LF

OO Gauge Steam Locos



Class 5101 'Large Prairie' 2-6-2T
Stage: Painted Samples Due: TBC
Price: from £115.56 Digital: NEXT-18
hattons.co.uk/dapollargeprairie



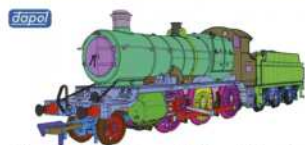
Class 78xx 'Manor' 4-6-0
Stage: Engineering Sample Due: Q3 2021
Price: from £135.96 Digital: NEXT-18
hattons.co.uk/dapolmanor



Class J26 0-6-0
Stage: Early Development Due: Q4 2021
Price: from £93.50 Digital: 8-pin
hattons.co.uk/oxfordj26



Class 78xx Manor 4-6-0
Stage: Engineering Sample Due: Q4 2021
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascalemanor



Class 43xx 'Mogul' (Tool Update)
Stage: CADs Due: Early 2022
Price: from £142.76 Digital: NEXT-18
hattons.co.uk/dapolmogul



Class O2 'Tango' (Tool Update)
Stage: CADs Due: Early 2022
Price: from £169.15 Digital: 21-pin
hattons.co.uk/helijantango

OO Diesels & Electrics



Class 59
Stage: Painted Samples Due: Mid-2021
Price: from £131.71 Digital: 21-pin
hattons.co.uk/dapol59



Class 45
Stage: Early Sample Due: Q3 2021
Price: from £143.65 Digital: 21-pin
hattons.co.uk/helijan45



Class 47
Stage: Prototype Due: Q4 21 - Q1 22
Price: from £131.71 Digital: 21-pin
hattons.co.uk/helijano47



Class 86/4 & 86/6
Stage: Early Development Due: Q3 2021
Price: from £161.46 Digital: 21-pin
hattons.co.uk/helijan864



Class 37/0, 37/4 & 37/6
Stage: Engineering Samples Due: Q2 2022
Price: from £169.99 Digital: 21-pin
hattons.co.uk/accurascale37

OO Diesel Multiple Units



Class 104
Stage: Early Development Due: TBC
Price: from £180 Digital: 21-pin
hattons.co.uk/helijan104

OO Coaches & Wagons



Genesis 4 & 6 Wheel Coaches
Stage: Painted Samples Due: Q3 2021
Price: from £30 Digital: 18-pin
hattons.co.uk/genesis



JHA Hopper Wagons
Stage: Painted Samples Due: Mid-2021
Price: from £38.25 Digital: 6-pin
hattons.co.uk/dapoljha



BR Pilchard Wagons
Stage: Deco Samples Due: Q3 2021
Price: £13 Digital: N/A
hattons.co.uk/oxofrdpilchard



GER 10t Covered Van
Stage: Deco Samples Due: Jul 21
Price: £13 Digital: N/A
hattons.co.uk/oxfordgervan



LNER 10t Banana Van
Stage: Prototype Due: Jul 21
Price: £12.50 Digital: N/A
hattons.co.uk/oxford10tbanana

N Gauge Steam Locos



LSWR Class M7 0-4-4T
Stage: Prototype Due: Q4 2021
Price: £83.26 Digital: N/A
hattons.co.uk/dapolm7



Bulleid Light Pacific 4-6-2
Stage: Early Development Due: TBC
Price: from £169.96 Digital: NEXT-18
hattons.co.uk/dapollightpacific

N Gauge Diesel Locos



Class 59
Stage: CADs Due: Q3/ Q4 2022
Price: from £136 Digital: NEXT-18
hattons.co.uk/dapoln59

N Diesel Multiple Units



Class 142 Pacer (New Batch)
Stage: In Production Due: Q3 2021
Price: from £123.12 Digital: NEXT-18
hattons.co.uk/dapol142

O Gauge Diesels/ Electrics



Class 26
Stage: Prototype Due: 2022
Price: £509.15 Digital: ESU XL
hattons.co.uk/heljano26



Class 47
Stage: Early Sample Due: Q4 2021
Price: £594.15 Digital: ESU XL
hattons.co.uk/helijano47



Class 56
Stage: Early Sample Due: Q4 2021
Price: £594.15 Digital: ESU XL
hattons.co.uk/helijan56



Class 73
Stage: CADs Due: Mid-2022
Price: £531.25 Digital: ESU XL
hattons.co.uk/helijan73

O Diesel Multiple Units



GWR Streamlined Railcar
Stage: Painted Samples Due: Q4 2021
Price: from £297.50 Digital: 21-pin
hattons.co.uk/dapolrailcar



Class 121 'Bubble Car'
Stage: Painted Samples Due: Q3 2021
Price: from £254.15 Digital: 21-pin
hattons.co.uk/dapolo121

O Coaches & Wagons



Stroudley 4 Wheel Coaches
Stage: Prototypes Due: Q4 2021
Price: from £93.50 Digital: Yes TBC
hattons.co.uk/dapolstroudley



Mk2 & Mk2A Coaches
Stage: Prototypes Due: Q1 2022
Price: from £237.15 Digital: N/A
hattons.co.uk/helijanmk2

OO Gauge (1:76 Scale)

Steam locos



R3861 Class 8P 'Merchant Navy' 4-6-2 35017 'Belgian Marine' in BR green early emblem (Hor) **NEW** ... £170.50
R3448 Class B17 4-6-0 61619 'Welbeck Abbey' in BR green early emblem (Hor) (RRP £120.99) **BARGAIN** ... £109



OR76J27001 Class J27 0-6-0 1010 in LNER black (Oxf) **NEW** £94



OR76J27002 Class J27 0-6-0 65837 in BR black early emblem (Oxf) **NEW** ... £94
H4-P-016 SECR P Class 0-6-0T 31556 BR black early emblem (Flat) (RRP £399) **BARGAIN** ... £84



R3828 The Aberdonian Railtour train pack Class A1 4-6-2 60163 'Tornado' in LNER apple green British Railways lettering and 3 Mk1 coaches (Hor) **NEW** ... £180

Diesel locos



3463 Class 33/0 33023 in BR blue grey roof and orange cantrail stripe - weathered (Hel) **NEW** ... £143.65



3465 Class 33/0 33025 in West Coast Railway Company maroon (Hel) **NEW** ... £135.15



3459Hel Class 33/0 33029 in Direct Rail Services blue (Hel) **NEW** ... £135.15



3466 Class 33/0 33052 'Ashford' in BR blue (Hel) **NEW** ... £135.15



3462 Class 33/0 D6508 'Eastleigh' in 1990s BR green small yellow panels (Hel) **NEW** ... £135.15



H4-66-037 Class 66 66587 in Freightliner/ONE pink 'AS ONE, WE CAN' (Hat) (RRP £150) **BARGAIN** ... £119



H4-66-036 Class 66 66623 in Freightliner/G&W orange (Hat) (RRP £150) **BARGAIN** ... £119

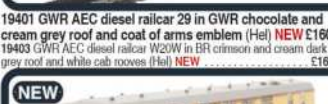


H4-66-033 Class 66 66743 in GBRI/Royal Scotsman (Hat) (RRP £150) **BARGAIN** ... £119

DMUs



19400 GWR AEC diesel railcar 22 in GWR chocolate and cream white roof and shirbutton emblem (Hel) **NEW** ... £160.65



19403 GWR AEC diesel railcar W22W in BR crimson and cream dark grey roof and white cab windows (Hel) **NEW** ... £160.65



19402 GWR AEC diesel railcar W21W in BR crimson and cream grey roof (Hel) **NEW** ... £160.65



19405 GWR AEC diesel railcar W26W in BR green speed whiskers grey roof (Hel) **NEW** ... £160.65



19404 GWR AEC diesel railcar W32W in BR green speed whiskers white roof (Hel) **NEW** ... £160.65

Wagons



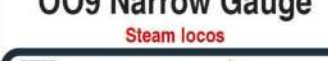
4F-010-009 JNA 'Falcon' bogie ballast wagon in Network Rail yellow - NLU29008 (Dap) **NEW** ... £25.50



4F-010-010 JNA 'Falcon' bogie ballast wagon in Network Rail yellow - NLU29042 (Dap) **NEW** ... £25.50



4F-010-011 JNA 'Falcon' bogie ballast wagon in Network Rail yellow - NLU29009 (Dap) **NEW** ... £25.50



4F-010-012 JNA 'Falcon' bogie ballast wagon in Network Rail yellow - NLU29144 (Dap) **NEW** ... £25.50

Track - Code 100 Setrack

R8072 Left Hand Standard Point (Hor) ... £9.50
R603 Long Straight - 670mm (Hor) ... £4

Track - Code 100 Streamline

SL-100 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track (Poc) ... £3.10
SL-100 Pack of 25 (Poc) ... £76

Track - Code 75 Finescale

SL-100F 1 yard (91.5cm) length of Nickel Silver Finescale flexible track (Poc) ... £3.20
SL-100F Pack of 25 (Poc) ... £78

OO9 Narrow Gauge

Steam locos



9982 Lynton & Barnstaple Baldwin 2-4-2T 'Lyn' in L&B dark green (post-1906) (Hel) **NEW** ... £203.96



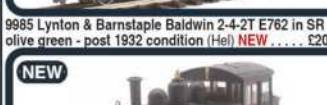
9981 Lynton & Barnstaple Baldwin 2-4-2T 'Lyn' in L&B dark green (pre-1906) (Hel) **NEW** ... £203.96



9983 Lynton & Barnstaple Baldwin 2-4-2T E762 'Lyn' in SR ex-L&B dark green (Hel) **NEW** ... £203.96



9984 Lynton & Barnstaple Baldwin 2-4-2T E762 'Lyn' in SR olive green 1029 - 1932 (Hel) **NEW** ... £203.96



9985 Lynton & Barnstaple Baldwin 2-4-2T E762 in SR olive green - post 1932 condition (Hel) **NEW** ... £203.96



9980 Lynton & Barnstaple Baldwin 2-4-2T in plain works black (Hel) **NEW** ... £203.96

N Gauge

Steam locos



25-002-010 Class V 'Schools' 4-4-0 30915 'Brighton' in BR green late crest (Dap) **NEW** ... £127.52



25-002-006 Class V 'Schools' 4-4-0 30939 'Leatherhead' in BR green early emblem (Dap) **NEW** ... £127.52



25-002-009 Class V 'Schools' 4-4-0 924 'Haileybury' in SR olive green (Dap) **NEW** ... £127.52

Bi-Modal Multiple Units



K10-1674 Class 800/0 5-car BEMU IET 'Azuma' 800209 in LNER (Kat) **NEW** ... £178



K10-1671 Class 800/0 5-car BEMU IET 800021 in GWR green (Kat) **NEW** ... £178

Track - Code 55 Finescale

SL-300F 1 yard (91.5cm) length of Finescale Nickel Silver flexible Wooden Sleeper track (Poc) ... £3.20
SL-300F Pack of 30 (Poc) ... £93
SL-300F 1 Yard of Nickel Silver Flexible Track Concrete Sleepers (Poc) ... £3.50
SL-300F Pack of 30 (Poc) ... £99

O Gauge (1:43 Scale)

Steam locos



H7-A3-006 Class A3 4-6-2 60077 'The White Knight' in BR green late crest and streamlined non-corridor tender (Hel) (RRP £750) **BARGAIN** ... £579



H7-A4-009 Class A4 4-6-2 60008 'Dwight D Eisenhower' in BR green late crest and streamlined non-corridor tender (Hel) (RRP £750) **BARGAIN** ... £579

Diesel locos



4030 Class 50 50036 'Victorious' in BR large logo blue - weathered (Hel) **NEW** ... £619.65



4029 Class 50 in BR large logo blue black roof - unnumbered (Hel) **NEW** ... £594.15



4021 Class 50 in BR large logo blue grey roof - unnumbered (Hel) **NEW** ... £594.15

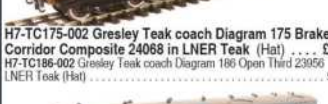
Coaches



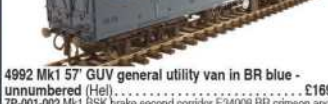
H7-TC115-002 Gresley Teak coach Diagram 115 Corridor Third 23896 in LNER Teak (Hat) **NEW** ... £179



H7-TC115-001 Gresley Teak coach Diagram 115 Corridor Third unnumbered in LNER Teak (Hat) **NEW** ... £179



H7-TC175-002 Gresley Teak coach Diagram 175 Brake Corridor Composite 24068 in LNER Teak (Hat) **NEW** ... £179



H7-TC186-002 Gresley Teak coach Diagram 186 Open Third 23956 LNER Teak (Hat) **NEW** ... £179



4992 Mk1 57' GUV general utility van in BR blue - unnumbered (Hel) **NEW** ... £169.15



7P-001-002 Mk1 BSK brake second corridor E34006 BR crimson and cream (Dap) **NEW** ... £175.07



7P-001-001 Mk1 BSK brake second corridor E34010 in BR crimson and cream (Dap) **NEW** ... £175.07



7F-037-004 Conflat 'A' flat wagon B735200 in BR bauxite BD type container in BR bauxite 'Door to Door' (Dap) **NEW** ... £175.07



7F-037-103 Conflat 'A' flat wagon B735230 in BR bauxite (Dap) **NEW** ... £175.07



7F-037-102 Conflat 'A' flat wagon B735201 in BR bauxite (Dap) **NEW** ... £175.07



7F-037-005 Conflat 'A' flat wagon B735364 in BR bauxite BD type container in BR bauxite 'Door to Door' (Dap) **NEW** ... £175.07



7F-037-002 Conflat 'H7' flat wagon 39612 in GWR grey BD2 type container in GWR brown 'Door to Door' (Dap) **NEW** ... £175.07



7F-037-001 Conflat 'H7' flat wagon 39660 in GWR grey BD2 type container in GWR brown 'Door to Door' (Dap) **NEW** ... £175.07

Any or Multiple Scales

Digital decoders

R8249 8-pin 4-function 2-sided 0.5A (1A peak) decoder (Hor) ... £16
R8249 Box of 6 (Hor) ... £92
R8101 TTS DCC Sound Decoder 8 pin plug - Class 31 diesel (Hor) ... £39

Digital controllers

524-042 NCE Power Cab 2 Amp Digital DCC controller (NCE) £159.50



DCC-GB120IP Cobalt ip slow-motion digital point motor - pack of 12 (DCC) £255

Point motors



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31-186ASF LMS 5XP Jubilee Steam No. 45654 "Hood" BR Lined Green L/C.....£254.99



- 31-191 LMS 5XP Jubilee Steam No. 45604 "Ceylon" BR Experimental Green.....£169.99
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32-940 Class 150/2 2 Car Sprinter DMU 150 216 GWR Green w/Passenger Figures.....£271.95
32-941 Class 150/2 2 Car DMU No. 150 220 Notem.....£250.95
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37-354D BR 13 Ton Steel Sand Tippler BR Grey (Early) with Load.....£22.95
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37-425B 16 Ton Steel Slope-Sided Mineral Wagon BR Grey (Early), weathered.....£22.95
37-428 16 Ton Steel Slope-Sided Mineral Wagon "Rother Vale" Black.....£19.95
37-429 16 Ton Steel Slope-Sided Mineral Wagon "WD Barnett & Co." Red with Load.....£22.95
38-776A 20 Ton Class B Anchor-Mounted Tank Wagon "Shell BP" Black, weathered.....£29.95
38-777A 20 Ton Class B Anchor-Mounted Tank Wagon "Esso" Black, weathered.....£29.95
38-779 14 Ton Class B Anchor-Mounted Tank Wagon "Tina" Black.....£25.95
38-780 14 Ton Class B Anchor-Mounted Tank Wagon "Regent Oil" Black.....£25.95
39-462 LMS 57R "Porthole" Brake Second Corridor Coach M27001M BR Blue & Grey.....£67.99

DAPOL OO

- 4F-010-009 JNA Falcon Open Wagon No. NLU29008 Network Rail Yellow.....£25.95



- 4F-010-010 JNA Falcon Open Wagon No. NLU29042 Network Rail Yellow.....£25.95
4F-010-011 JNA Falcon Open Wagon No. NLU29099 Network Rail Yellow.....£25.95
4F-010-012 JNA Falcon Open Wagon No. NLU29144 Network Rail Yellow.....£25.95

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- GM458 Fordhampton Alphabet & Numeral Set.....£3.15
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GM486 Fordhampton Hotel Kit.....£31.50
GM487 Fordhampton Electricity Gantry Crane.....£19.50
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HELJAN OO

- 19400 AEC Railcar No. 22 GWR Chocolate and Cream with white roof.....£160.95
19401 AEC Railcar No. 29 GWR Chocolate and Cream with dark roof.....£160.95



- 19402 AEC Railcar No. W20W BR Crimson and Cream with dark grey roof.....£160.95
19403 AEC Railcar No. W21W BR Crimson and Cream with light grey roof.....£160.95

- 19404 AEC Railcar No. W32W BR Green with Speed Whiskers & white cab roof.....£160.95
19405 AEC Railcar No. W25W BR Green with Speed Whiskers & dark roof.....£160.95
3458 Class 33/0 Diesel Loco No. 33 025 BR Civil Engineers Dutch, weathered.....£143.95
3459 Class 33/0 Diesel Locomotive No. 33 029 DRS Blue.....£135.99
3462 Class 33/0 Diesel Locomotive No. D6508 "Eastleigh" BR Retro Green.....£135.99
3463 Class 33/0 Diesel Locomotive No. 33 023 BR Blue.....£143.95
3465 Class 33/0 Diesel Locomotive No. 33 025 West Coast Railways Maroon.....£135.99
3466 Class 33/0 Diesel Locomotive No. 33 052 "Ashtford" BR Blue.....£135.99

HORNBY OO

- R3914 Railroad Class 37 Co-Co Diesel Loco No. 97 304 "John Tiley" Network Rail.....£74.99
R3915 Railroad Class 121 Single Car DMU No. 960015 Network Rail.....£71.49
R3952 Class 390 Pendolino Train Pack 390 001 Avant! West Coast.....£224.49
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R3999X Princess Royal Steam Loco 6205 "Princess Victoria" LMS, DCC Fitted.....£212.99
R30001 Princess Royal Steam 6203 "Princess Margaret Rose" LMS Crimson.....£188.99
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R30006 Terrier 0-6-0T Steam Loco 32646 BR Black BRITISH RAILWAYS.....£89.99
R30008 Terrier 0-6-0T Steam Loco 32640 BR Black E/E.....£89.99



- R30036 Railroad Bagnall Diesel Shunter Train Pack.....£61.99
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R40021 Mk1 Brake Corridor Composite Coach W21083 BR WH Chocolate & Cream.....£31.99
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391-031 Baldwin 10-12-D Tank 590 Welsh Highland Railway Lined Maroon.....£135.99

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9983 Lynton & Barnstaple Baldwin 2-4-2T E762 "Lyn" SR Maunsell Green.....£203.95
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9985 Lynton & Barnstaple Baldwin 2-4-2T E762 SR Maunsell Green.....£203.95

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- 372-934DJS N Class 2-6-0 Steam Loco No. 1823 Southern Railway, DCC Sound.....£220.99
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OO GAUGE

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39-177C BR Mk1 BG Full Brake Coach No. M80565 BR Crimson & Cream	£27.95
39-186 BR Mk1 BG Full Brake Coach QFX AD8 975612 Departmental Olive Green, wind	£27.99
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39-312 BR Mk1 SP Pullman Second Parlour Coach No. E352E BR Blue & Grey	£36.99
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WORLD OF RAILWAYS

HORNBY HOLIDAY CHALLENGE GETS OFF TO A FLYING START



It seems that holiday time is model railway building time, according to Hornby, and its recently-launched Summertime Layout Challenge is proving very popular. The competition challenges modellers to build a summer-themed layout. Entries are being judged by Pete Waterman OBE and celebrity mechanical maestro, Fuzz Townsend who will view all layout entry images, with their top three from each category going to a social media vote.

Hornby's Jaimie Gilham, said, "We have had a fantastic response to the #HornbyHoliday Summertime Layout Challenge since launching it and can't wait to see what participants have come up with! Mike Day (also of Hornby) has been busy creating his version of a summertime layout to help give people inspiration and creative layout tips. The next few months are looking to be filled with lots of exciting layout builds and we look forward to sharing them with our judges. The winner from each category will win a whopping £1000 worth of Hornby products, too, so it's definitely worth considering entering."

Head to the Hornby website to find out more.

LOCOMOTION MODELS PARTNERS WITH CAVALEX MODELS FOR MGR WAGONS



Locomotion Models has revealed plans to release models of the first and last Merry-Go-Round (MGR) coal wagons in a partnership with Cavalex Models.

Images have been shared of pre-production samples, though these aren't necessarily representative of the final models. Some parts are missing, including the four cross braces on the original bodied version. Further images of the forthcoming wagons are said to be issued soon. The wagon pack is priced at £65.00. Wagons will incorporate the following features:

- Highly-detailed underframe
- Zinc alloy chassis
- Easy conversion to P4/EM, with 26mm axles used as standard
- NEM coupling pockets
- Sprung buffers

Visit the Locomotion Models website for further details.

DAPOL ANNOUNCES CLASS 59 IN N



The Chirk-based manufacturer has announced a new Class 59 for N gauge modellers, which is already at the tooling stage. This latest model incorporates Dapol's re-designed chassis and electronics and the same five-pole motor that is used in the N Gauge Class 50 and Class 68.

Dapol announced OO gauge versions of the Class 59 earlier in the year, with pre-ordering still open. The models are anticipated to arrive by the end of the year.

Like its OO gauge models, tooling will cater for all three sub-classes: 59/0, 59/1 and 59/2. Six liveries are to be produced, covering various eras for the class, in Foster Yeoman, Hanson, Aggregate Industries, ARC, DB Schenker and National Power branding.

Sound-fitted versions are available for pre-order through the Dapol website. DCC-ready and DCC-fitted versions are available to pre-order through the stockist network.

Prices are currently listed as £160 for DCC-ready, £190 for DCC-fitted and £270 for the DCC Sound versions. It is hoped that the N gauge models will arrive in the UK before the end of 2022.

REVOLUTION TRAINS SHOWS COACH AND WAGONS SAMPLES



First engineering samples of the Mk. 5 Caledonian Sleeper (CS) and Mk. 5a TransPennine Express (TPE) coaches have been received and are now being assessed for mould quality, fit of parts and operation.

These exciting new trains were introduced towards the end of the last decade, and the Revolution versions feature full interiors, photo-etched grilles and wire details, kinematic close coupling and carefully designed free-running inside bearing bogies.

Caledonian Sleeper operates its Mk. 5s in rakes of 16 vehicles as Lowlander (London-Glasgow/Edinburgh) and Highlander (London-Aberdeen/Fort William/Inverness) services.

The CS models feature accurate bodyshells and interiors for seated, club, standard sleeper and accessible sleepers, switchable cabin lighting as standard and inter-car 'Dellner' couplers to replicate those fitted to the prototype.

Revolution Trains is offering CS cars in sets of four priced at £180 for pre-orders – in both Lowlander and Highlander versions – to enable customers to build up complete, accurate rakes.

The TPE Mk. 5a vehicles operate in push-pull mode in fixed rakes of five cars comprising DT(coach A) - TS(coach B) - TS(coach C) - TS(coach D) - TF(coach E) powered by a Class 68 and branded 'Nova 3'.

The T1 car has buffers and standard drawgear at the outer end so it can be coupled to the locomotive, while the DT car has a driver's cab and aerodynamic outer end with buffers.

All cars have operating interior lights as standard and directional lights are fitted to the Driving Trailer. The Revolution models are being sold in complete five-car train packs in two number sets with a £225 pre-order price.

Once the performance of these test models is complete, painted samples will be prepared for final checking before models go into production.

Decorated samples of the forthcoming MMA/JNA box wagons in N gauge have also been received in DB red, Tarmac/Ermewa grey, VTG dark blue, GBRf blue, VTG/Mendip Rail silver, Cappagh mid blue and Touax red oxide colours.

Once these samples are checked and approved, Revolution expects to close the order book and commence production toward the end of August.

The Ealnos wagons will be available until orders close for the pre-production price of £36.50 each (£40.50 with flashing tail lamp).

KERNOW MODEL RAIL CENTRE STEAM RAILMOTOR UPDATE



The first images of the 'impressions' from Kernow Model Rail Centre's new tooling for the GWR Steam Railmotor in OO gauge have emerged. The retailer's initial Engineering Prototypes have been assembled and are said to arrive with it for review and testing, shortly.

Having completed the CAD for tooling in October of last year, the retailer is producing models of both diagram 'O' and diagram 'R' types offering modellers greater variety on their layouts. The Diagram 'O' and 'R' types differ where the size of boiler room windows, double-leaf passenger doors and bogie types are concerned.

Prior to assembly, as with every RTR model, the Steam Railmotor was little more than a sprue of plastic parts. Fit and finish of these will be checked by KMRC, shortly.

The prototype was laser-scanned at Didcot for a more accurate rendition of the body shapes and details. The model is expected to have working directional oil lamps and interior light and is said to be DCC-ready with a 21-pin decoder socket. Five versions are to be released.

SEVERN VALLEY RAILWAY TO HOST STEAM IN LIGHTS EVENT



Tickets are about to go on sale for the Severn Valley Railway's spectacular Steam in Lights services for 2021, running from mid-November through until late December.

The booking office is expecting an early surge in demand as visitors seek to secure their seats for the festive extravaganza. Watch the railway's short promotional film for its Steam in Lights events on our website, www.world-of-railways.co.uk.

This year, Steam in Lights features a new narration for the family-friendly experience, which begins at the illuminated Bridgnorth station. A live performance on platform 2 with entertainment from Grandma Cackler and her grandson is expected to take place, before the arrival of the steam-hauled train, adorned with tens of thousands of colourful lights.

Before departure, guests will have the opportunity to purchase festive treats and drinks from the refreshment room to enjoy on their journey. Once settled in a private compartment or at a socially distanced table, passengers will embark on a journey past twinkling scenes, full of lights, forest creatures and mysterious characters, accompanied by music and narration.

This year, the number and range of lineside displays has been expanded. "We've already had many enquiries about what the railway is offering for Christmas 2021," said Lewis Maddox, the SVR's events manager. "We're launching Steam in Lights as early as possible. This year is going to be more amazing than ever, with even more of the wonderful and magical displays that our visitors enjoyed so much last year. We know this is going to be a popular experience, and pre-booking is the only way to make sure of a seat!"

Departures from Bridgnorth are on selected dates between November 19 and December 23, with up to three evening departures. Bookings are covered by the SVR's Covid Guarantee. Visit its website for further details and information.

OSBORNS MODELS REVEALS LATEST VEHICLES IN 1:148 SCALE



Designed and 3D-printed in-house by the manufacturer, the model shop further expands its road vehicle offerings for N gauge modellers.

As part of its Arch Laser brand of road vehicles for 1:148 scale, suitable for N gauge layouts, Osborns Models has introduced a 3D-printed dropside truck, based around a Ford Iveco cab. The vehicle has been decorated using its in-house painting service, with a load to simulate a scaffold truck.

Suitable for layouts around the turn of the century, the vehicle is also available in other guises with different bodies, including a box van body and as a flatbed to serve as a car transporter.

Maurice Pearce of Osborns Models commented, "There are many vehicles available on the 3D design website Shapeways, but apart from being relatively expensive they also need post-print treatments to make them smooth enough in N scale. We are currently near the limit for detail vs. scale for kitchen table manufacturing and manufacture everything in-house including packaging where we can."

MIDLANDS AND LONDON MODEL ENGINEERING EXHIBITIONS CANCELLED



MIDLANDS MODEL ENGINEERING EXHIBITION

Both the Midlands Model Engineering show in Warwickshire for 2021 and the London Model Engineering Exhibition in 2022 have been cancelled by the organisers.

Statement from Meridienne Exhibitions Ltd:
It is with deep regret that due to the ongoing uncertainties of Covid-19 pandemic; we have to announce the cancellation of the 2021 Midlands Model Engineering Exhibition, which was due to be held at the

Warwickshire Event Centre on the 14th-17th October.

This difficult decision is taken despite a real determination by the Meridienne Exhibitions team, trade, clubs, societies, exhibitors and other supporters, all striving to continue to deliver the usual high quality and successful event during this very difficult time.

Over the past few weeks, we have been in the excruciating position of considering every possible scenario to see how we might be able to proceed, but sadly, the risks of holding the event now far outweigh the reasons for going ahead. The core decision is based on the escalating cases of COVID-19, and the risks that widespread illness and self-isolation could have on everyone involved. We have navigated our way over the past 16 months through obstacles, but now feel that the odds are stacked against us and we are no longer in a position to be able to proceed safely with the unknown government Covid-19 requirement for autumn/winter ahead.

It follows that with our decision to cancel the Midlands Exhibition we have also, again regretfully, decided that it is not practical nor financially viable to proceed with the London Model Engineering Exhibition at Alexandra Palace in January 2022.

Having presented model engineering and other exhibitions for well over 40 years, these decisions represent a tremendous disappointment for all but hopefully, the situation will be different in later 2022 and we may again present a model engineering exhibition.

We look forward to seeing you all again soon.

MARKET DEEPING CLUB TO ASSIST IN FILM PRODUCTION



Market Deeping Model Railway Club is to loan its 'Cannons Cross' layout for a short film. *Dream Big*, written and directed by Pip Swallow, under French Fancy Productions is a bittersweet comedy about a young woman who discovers she can control the world through her model railway.

Pip's first short, a sitcom pilot, *Great Expectations* won Edinburgh TV Festival's New Voices Award for 'Best Pilot', and also screened at Tribeca, Underwire, Cambridge and Norwich Film Festivals. Her second short as writer and directorial debut is the comedy-horror, *Day of the 20,000 Gammon*, currently in post-production.

The *Dream Big* script has been developed with the support of the British Film Institute, which is continuing its support with part production finance. French Fancy Productions needs to raise a further £5,000, and is looking for Associate Producers to donate £1000 or more to the film's production.

WORLD OF RAILWAYS

TV

DIGITAL EDITION EXCLUSIVE

Rails and Accurascale locos on the MRC test track

An exclusive look at Rails LNWR Precedent, Caledonian 812 Class and Gas Turbine, plus Accurascale Deltic with Lawrence Robbins of the MRC and Howard Smith.

WATCH THE VIDEOS HERE

(You must have a wifi connection to stream video content)



PLAY



Factfile

Layout name: Heaton Lodge
Scale/Gauge: 7mm:ft / 32mm gauge / O
Size: 192ft x 30ft
Era/Region: 1980s BR Midland Region
Layout type: Continuous circuit

0 gauge

HEATON LODGE

With more than 100 baseboards, and measuring over 180ft in length, Britain's Biggest Model Railway is now complete and ready to hit the road.

Words: Simon George **Photography:** Andy York



The everyday meets special – a Class 56 heads west with an empty MGR train as *Evening Star* heads for York with a positioning move for a special service.

It's been a while since 'Heaton Lodge Junction' has featured in BRM, but I'm pleased to say there's been a huge amount of progress since.

First off, the scenery has finally been completed. It required the best part of 50kgs of static grass to cover the entire 200ft length, as well as a bucket full of crushed earth to give that 'matt' appearance.

To top it off, I've used theatrical snow spray to lightly give the impression of frost, which gradually increases in intensity as you travel west. The thinking behind this is it's a chilly winter morning sometime in December 1983 and the rising sun hadn't quite melted the western extremities.

River Calder

The River Calder and the two large rail bridges that cross it (the later built girder one being disused) are now also in situ. The late, great Allan Downes constructed these for me back in 2015, and up until last year, they had been gathering dust in the workshop. I've

detailed the girder bridge with rivets, graffiti and rust, while the original Manchester & Leeds Railway viaduct – still in use on the model and the prototype – has scribed clay 'stonework' painted with watercolours and finished with ice and mould effects. I don't know what Allan would think of them now, but I remember vividly him calling me a vandal for covering his plastic stonework with my scribed clay effort!

The 10ft by 2ft river ended up using over 20kgs of casting resin with about a quarter of a cup of Woodland Scenics Yellow Silt and Murky coloured dyes mixed in to give the impression of depth.

There have been no fewer than three separate castings to give the 'depth' I wanted to achieve. However, the second one didn't go as planned.

Right in front of some cameras, which are filming a documentary about 'Heaton Lodge Junction', I'd rushed things and inadvertently mixed the wrong hardener to the resin, which resulted in a cloud of steam and smoke, the resin instantly setting like concrete. While trying to keep a straight face, at least it looked impressive!

'Triangle' scenic section

One part of the layout that I didn't plan is the western 'triangle'. Originally, where the Huddersfield-bound double tracks peel away south from the Manchester to Leeds Lancashire and Yorkshire line, I'd simply decided to utilise the widening gap in the middle as an operating space. However, it was too good an opportunity to miss not to give



this section of the layout a huge impression of width.

Having read Pete Waterman's book on his superb permanent 'Leamington Spa' layout and his comments on the sheer width of it giving him difficulties, I thought hard about how to overcome this.

Eventually, I hit upon the idea of constructing this 20ft x 20ft section of the layout on five 'trolleys', which I constructed from aluminium box section. These roll into place with the scenery bolted on top. Part of the river is also built on the trolleys, so derailments in the long 30ft tunnel, which carries the Huddersfield-bound dive-under lines can be accessed pretty easily.

There are small gaps in the scenery where the trolleys buffer up together - no pun intended- but these are invisible from the viewing side.

Fiddle yard

To be totally candid, the 150ft fiddle yard was an absolute nightmare to build. I much prefer

scenery construction to track laying and the nine months it took to build and wire weren't much fun. It was the only time I considered packing in the whole project, such was its sheer excruciatingly repetitive nature. It really took its toll.

Using around a mile of Peco O gauge bullhead track and 60 odd Peco points, each of the four main lines has its own fiddle yard (two side-by-side and the other two beginning at the end of these) with seven 40ft roads each. This then allows 28 trains to be comfortably stabled, including three of the longest trains on HLJ - the huge 35ft long MGR freights.

That said, now that the fiddle yard is totally finished, I've actually quite taken a liking to it! The sight of such a humongous amount of track in one place is a real spectacle. So, in phase two of 'Heaton Lodge Junction', I'm planning to completely detail this vast amount of track too. Albeit fictitious, it should take on the look of a busy 1980s marshalling yard in the next few years,

Our first impressions

'Heaton Lodge Junction' is big. You just won't believe how vastly, hugely, mind-bogglingly big it is. You might think the Vale scene at Pendon is big, but it's small in comparison.

I had read the statistics - 180ft long and more than 50 baseboards. I'd even been to visit the layout in its early days when only a few boards were complete. None of this prepared me for what I found when Andy and I opened the door of the warehouse-come-modelling room it now lives in.

On my left was around 20ft of the layout. To the right, tracks head off into the distance. Just how far only came home to me as I walked along it. Even the biggest layout at an exhibition is covered in a few paces. This one seems to keep going.

Standing by the derelict factories, the next surprise is that, when looking to the far end of the layout, a train heading your way is invisible. O gauge locomotives might be big, but what you spot first is the headlights, and they are a long way away.

I've seen many model railways, and the sheer scale here is completely new to me. If you get the chance to see 'Heaton Lodge Junction' on its tour, do it. This is a spectacle you don't want to miss!

Phil Parker



A wintry West Yorkshire landscape is the setting for Simon's layout as a 'Whistler' heads onto the scene from the Calder Valley line with the Redbank parcels service.

complete with signal gantries, miles of (more) concrete trunking and eight huge metre-high lighting towers scratch-built from brass rod.

Sponsorship

Talking of long trains, there is only Heljan's range of heavyweight diesels that can haul the monster freights that are becoming 'Heaton Lodge Junction's' hallmark. It made perfect sense when Heljan kindly offered to become the layouts sponsor, providing much of the motive power for the layout.

Each Heljan diesel is sound- and decoder-fitted by expert Lee Edmondson before it comes back to me for renumbering and weathering. While I love kit-building, I've had to weed out almost all the kit-built rolling stock I painstakingly constructed – as well as the locomotives – as I found they just don't have the sheer resilience required to run reliably for hours at a time. It takes around six minutes for a scale 50mph freight train to circumnavigate 'Heaton Lodge Junction'. Long enough to make another cup of tea!

Back in March, I was lucky enough to get my hands on one of the prototype Heljan Class 56s, which hauled a 50 wagon MGR train from 10am until being finally shut down at 6pm. That's a real distance of around seven miles, which oddly enough is about four times the length of the real Heaton Lodge Junction!

Timescale

It's taken around seven years to complete the layout, working most evenings for four years, then the remaining three years full-time. I've been lucky enough to keep the standard of modelling pretty consistent throughout because I crucially practised modelling techniques for around 18 months prior to starting work on the model. This was purely so I could hit the ground running.

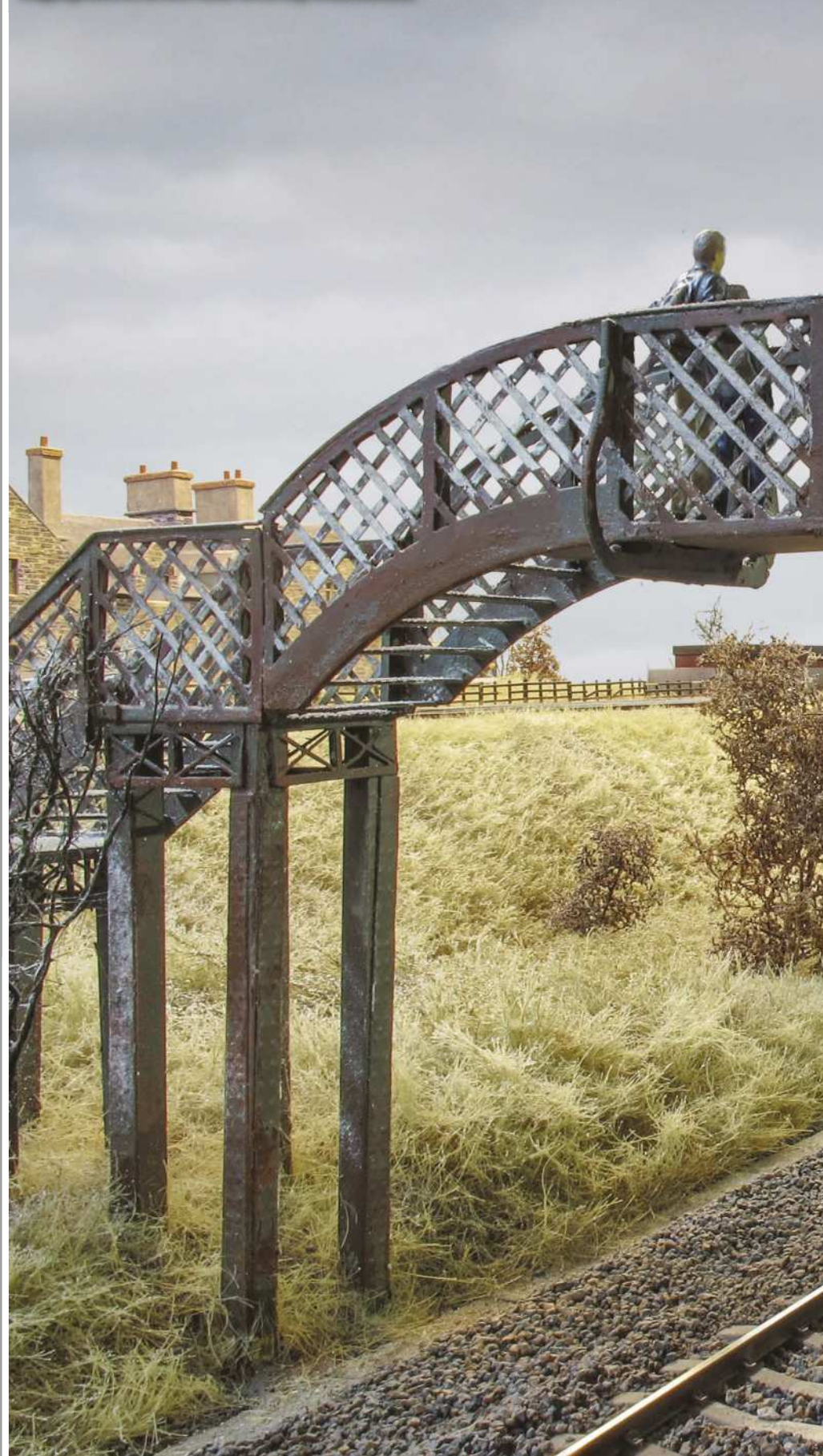
At the outset, this whole project was simply a way of recreating my past – albeit on a huge scale. My ultimate goal was to be able to generate that same anticipation as the lights of a heavy freight came into sight from over a mile away.

I'm glad to say, in my opinion, I've definitely achieved that. There are times when I glance down the length of the line with the headlights of a locomotive in the far distance and for a few seconds it's exactly like being lineside on a winter's day in 1983.

Exhibiting

I'd completed around 60ft of the full 192ft length of the scenic side when it struck me

The hearts of the chilled spotters on the lattice footbridge are warmed by the sight of the Redbank parcels as a Transpennine Class 124 DMU heads away, dropping down to the dive-under on its way to Huddersfield.





that, given this model was transportable (albeit with three articulated trucks), there could be a way of turning it into a business as I was fairly certain even the general public may want to view something so large and detailed. Given the usual model railway exhibitions couldn't allow for a full four-day set up (and a similar timescale for breakdown) it would mean organising my own exhibitions to allow this.

It would be near impossible to operate a full sequence of train movements for a 'working' day, every day for a month or so at a time, so we need to build in operational automation with our DCC system, which will see a continuous cycle of trains to entertain visitors. We're still working on this to try and make life easier, otherwise, we'd need an army of operators, but we will still be able to intervene and have manual control of trains. Visitors will see trains typical of the area in the 1980s, including the occasional steam special workings, but I'm sure the timespan may slip a little if there's something else appealing to run.

For the general public's consumption, I



Detail abounds on the layout, the smashed glazing on this Mini is created from broken microscope slide slip cover glass.



Thawing icicles drop from the culvert arch into the murky waters as 37222 rattles above.

Several structures on the layout are by the late Allan Downes – here, the waterworks buildings slowly decay.



A Manchester to Leeds Transpennine service has rasped its way up the bank from the dive-under and approaches Heaton Lodge Junction.



reckoned a re-brand from 'Heaton Lodge Junction' into 'Britain's Biggest Model Railway' would be necessary and then the decision was made to organise the model's first exhibition over a full three weeks in December which is somewhat of a departure from the usual three days!

Enlisting the help of the local council, I was kindly offered a large empty market hall at reduced rates in its home city of Wakefield, bang in the middle of the city centre in fact. This fitted comfortably into their policy of trying to entice more people into the high street. The aim is to have a more family-orientated exhibition, which has the railway as its centrepiece, but with a mixture of food, drink and possibly craft stands to widen the appeal for families on the run-up to Christmas.

This first three-week exhibition will be somewhat of a 'pilot' where I aim to correct inevitable issues that might arise but come 2022, it'll be full steam ahead with city centre venues and dates centred around holidays.

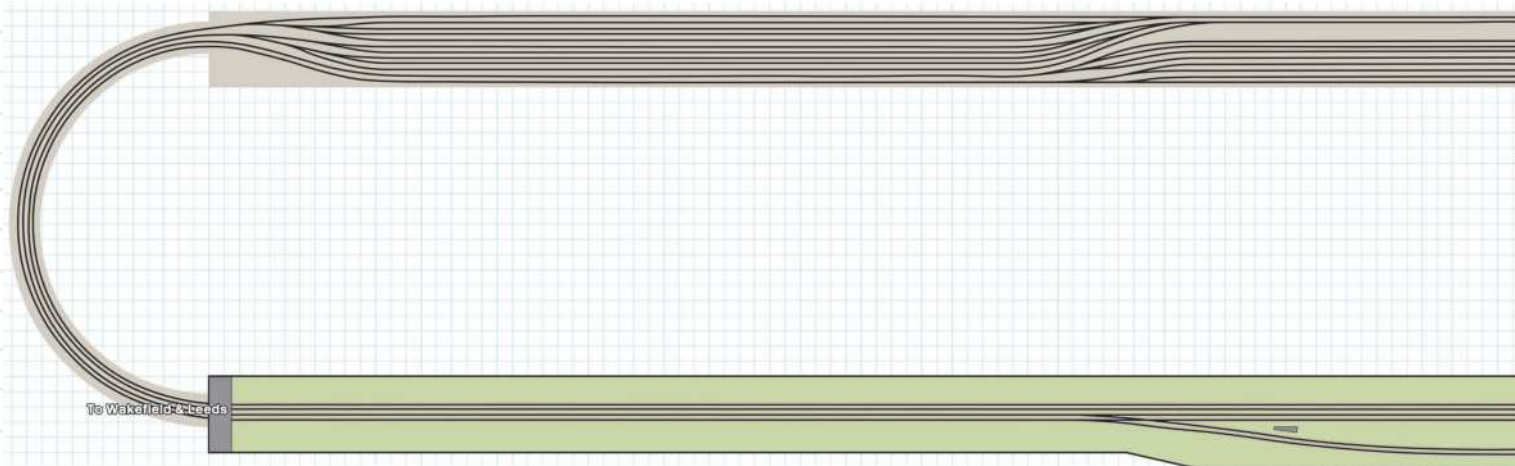
I've always harboured a passion for playing a small part in improving the image of this wonderful hobby, as well as making it more accessible to the public and it is with this in mind that the chosen locations will be accessible and appealing to family audiences as much the enthusiast. We will publish further details in due course.



A Heljan Class 37 with the vehicle portion of a Westbound Motorail service will ignite the nostalgia for motorists of the 1980s.

Trackplan

HEATON LODGE



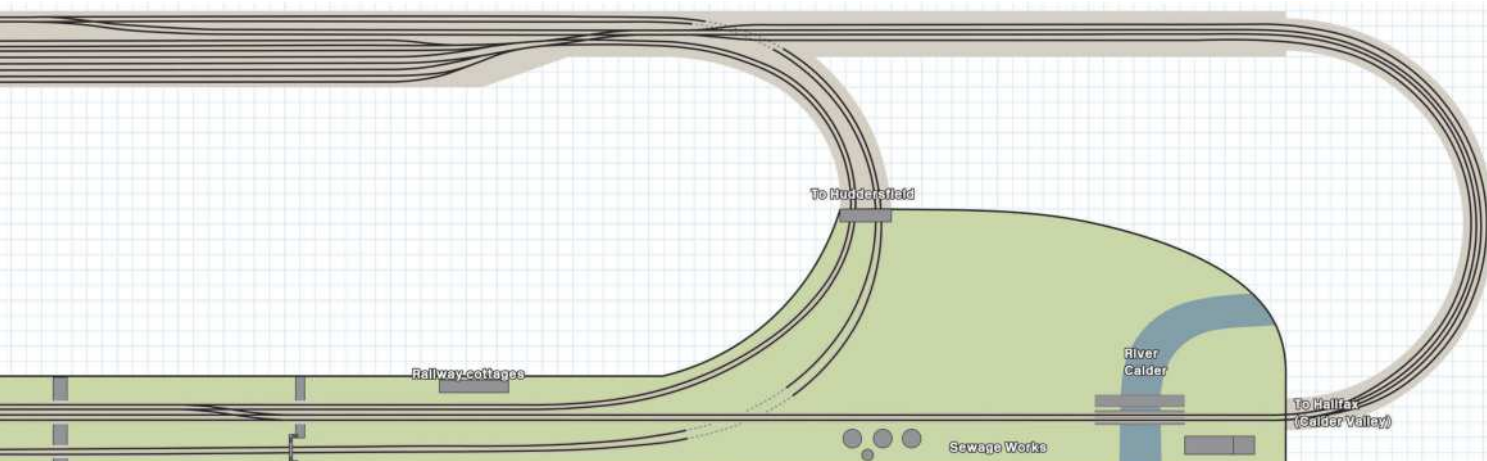
The departure of autumn into a frosty winter is all around these two hardy spotters overlooking the layout's easternmost point.



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BRM meets...

BRITAIN'S BIGGEST MODEL RAILWAY

Andy York meets Simon George and his monumental Heaton Lodge Junction project for an update on how modellers will be able to see the layout.

AY: Simon, you are now branding your 180ft long O gauge project depicting Heaton Lodge Junction, a West Yorkshire junction of lines near Mirfield where the Huddersfield route diverges from the Calder Valley line as 'Britain's Biggest Model Railway'.

SG: Hopefully, yes, it really will be Britain's biggest model railway. The reason for that is that I really want to turn the template for railway exhibitions on its head. So instead of visiting a model railway exhibition, I want to take this model directly to the public. From the end of 2020, the intention is to position it outside various shopping malls up and down the

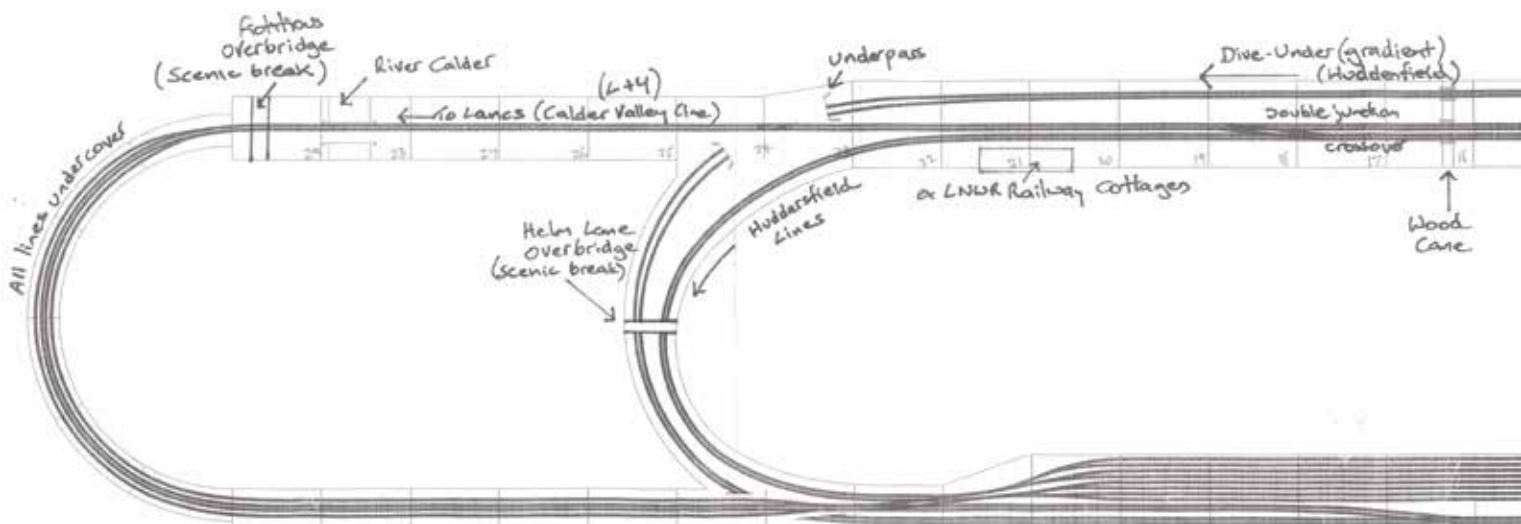
country in its own marquee. It will only be on show during school holidays, so when the public are walking past it on the way to do some shopping, hopefully kids will be pulling their parents in asking to visit 'Britain's Biggest Model Railway'. At the same time hopefully getting new people interested in the hobby, which is another one of my passions.

AY: We are going through a time of increased exposure with the likes of Channel 5's Great Model Railway Challenge and others stimulating an interest in creative hobbies. Model railways is possibly being viewed as a little less nerdy than

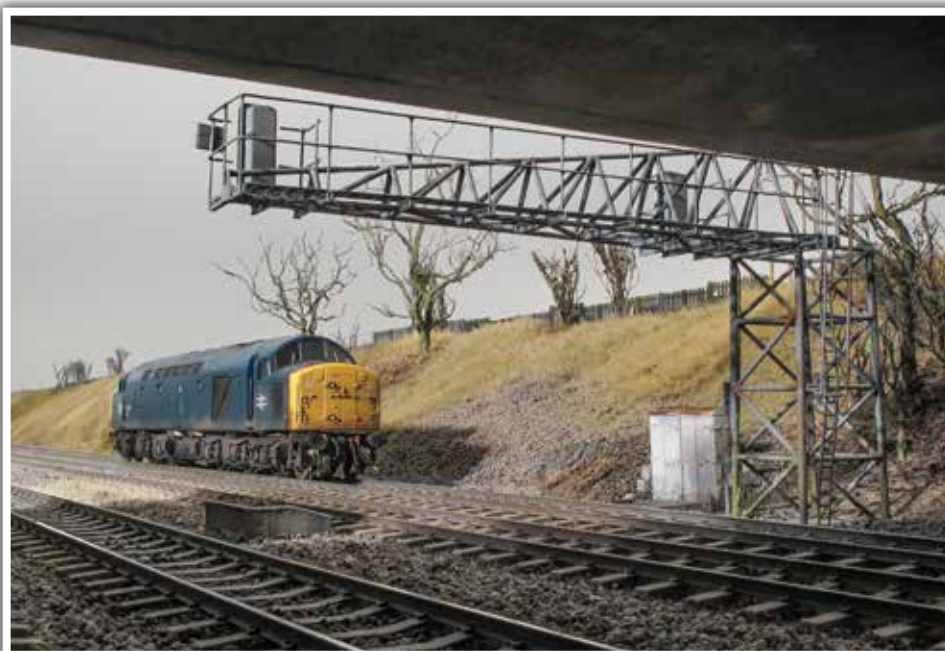
it was 18 months ago. New people are showing an interest in what we do.

SG: Certainly, I think if you look at something like Miniatur Wunderland in Hamburg. To a modeller, it's a huge train set with landscapes, towns and an airport that gets 400,000 visitors a year, each paying €15 to go and see it. With Heaton Lodge being branded 'Britain's Biggest Model Railway', hopefully the publicity surrounding that and it being positioned in locations where there's a lot of people walking past, people will be tempted to come in and see.

AY: The name is as really big hook to catch people. This is worlds apart from



ON EXCLUSIVE



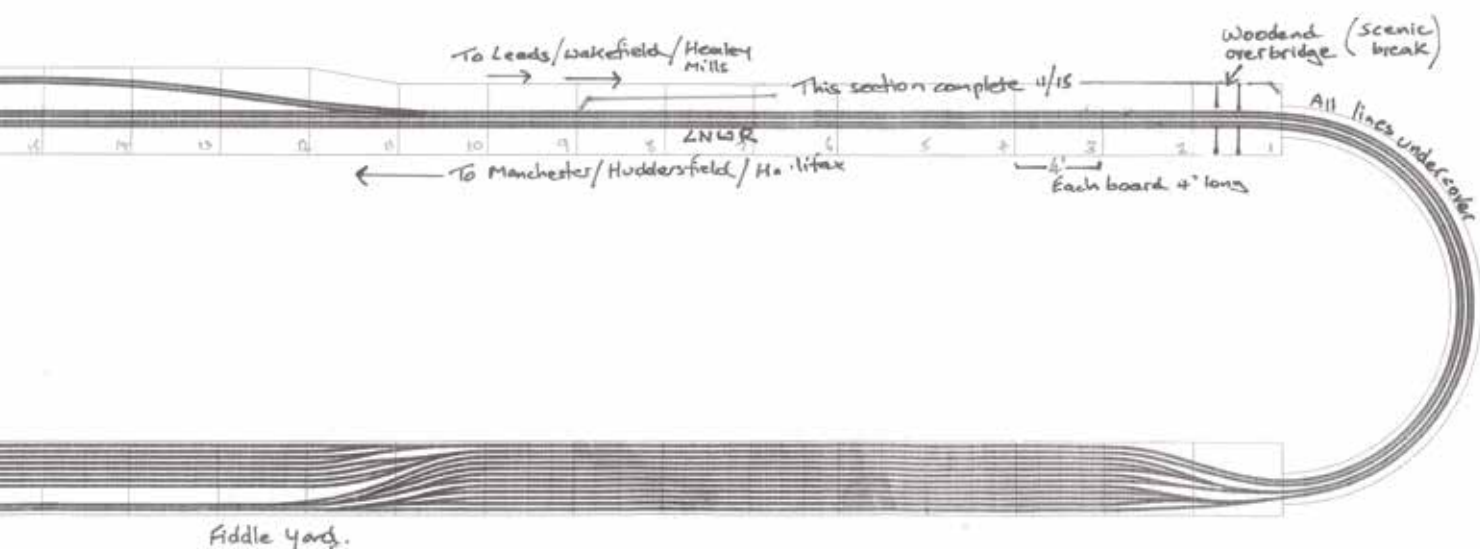
keen to ask you "How did you do that?" So there is a lot you can communicate to visitors about the practical skills involved.

SG: Definitely, yes. As I said to you earlier, I'm not actually a railway modeller; this is the first layout I have built. I did tinker with model railways when I was 12 or 13, but I haven't really done anything since, and that kind of helped me in a way because I didn't buy a book off a shelf and follow the same old processes and techniques to get to what you see here now. There are a lot of things I found out through trial and error and I like to utilise natural materials such as the dead leaves found around the layout, which are actually real leaves ground up, so there are lots of ways you can replicate various things to be found on the layout and I'm quite keen to pass on all those things as well.

a tatty model railway in a seaside town's back street. For us, as modellers, there will be really something to see; this is proper top-notch modelling, not just shaking a few boxes to get something running around.

SG: No, hopefully this will show people what level you can take it to if you really want to, they will then leave via a model shop with train sets and other products geared up to getting new people involved with the hobby. There needs to be some return on the investment, as this has been an expensive exercise as well as taking up an awful lot of time.

AY: There are so many aspects to this, skills-wise, that you have been through while building this layout and I know from the topic on RMweb that many people are



AY: This project is about individual skills, it's not a team of modellers or club; this is one man's mad, but brilliant, project. It's part of your youth, history and memories – a very personal project where you have brought those to life.

SG: You've got it in a nutshell, that's what I wanted to achieve. Even if it's not a success on the public circuit, all the way through my retirement I've got a great model railway to play with.

AY: I'm sure there will be many people who are keen to come and see it and hopefully you can get the word out through wider channels as this really does merit the widest possible audience to show what can be achieved. You're planning to have the layout, largely complete, at Warley show in 2020; there are not many exhibition halls which are going to be able to fit this in are there?

SG: Exactly, so that's why it's only ever going to go to Warley show at the N.E.C., simply because that's the only place big enough for it to go in. Once I was well into building the layout, I came up with the business plan of taking the model direct to the public - that's why it's going to be in its own marquee outside big shopping attractions.

AY: I remember certain attractions as a kid nearly 50 years ago, such as the model railway in the cellar at Alton Towers with a few black and white photographs as the only memory. It could well be, in 50 years' time, people will say "Do you remember going to see Britain's biggest model railway?"

SG: I often wonder what would happen to the model in the distant future. It's in its infancy at the moment and it's not yet wholly finished, but when it is, a long time into the future, where would it end up? Who knows?

The interesting thing is that back in the 1960s, British Rail used to have two huge O gauge layouts that they used to take around the country during the period of transformation from steam to diesel or electric just to inform the general public the changes that were happening. So, I'm not the first person to have done this, I've just twisted the concept a bit and, instead of people coming to visit the layout, I want to take it straight to them.

Class 56 diesel-electric locomotive number 56104 of Tinsley TMD approaches Heaton Lodge East Junction on the Up Slow line with a Healey Mills to Fiddlers Ferry MGR working – David Ingham



WHERE IS HEATON LODGE JUNCTION?

Just west of Mirfield, in what used to be the West Riding of Yorkshire, two trans-Pennine main lines diverge. At Heaton Lodge Junction, westbound trains can take the Standedge Tunnel route via Huddersfield or head for the Calder Valley line.

It was at its busiest during the first half of the last century. It was also where the LNWR's 'New Line' to Leeds began. Opened in 1900, it left the Huddersfield - Leeds line at Spen Valley Junction, about half a mile south of Heaton Lodge, before burrowing under the Calder Valley route in two short single line tunnels. Climbing steeply alongside the existing four tracks, it then headed for Leeds, crossing the Calder on a lattice girder bridge.

Traffic was heavy until the 1960s, when rationalisation began with the closure of the 'New Line' in 1965. Five years later, BR embarked upon a full scale remodelling of the junction, removing two of the four Calder Valley lines, plus a further two heading to Huddersfield.

At around the same time, experimental LMS-installed speed signalling and semaphores were also replaced, together with Heaton Lodge Junction's (originally LYR) signal box. Colour light signalling, including two large gantries, was then installed, all controlled from Healey Mills powerbox located about eight miles east.

The first mile or so of the 'New Line', including the underpass and Spen Valley Junction (now renamed Heaton Lodge South), were brought back into use. It was realigned to curve in the opposite direction and connect with the Calder Valley line. This, BR figured, reduced conflicting movements with heavy freight traffic to and from Lancashire by eliminating a flat crossing.

WORLD OF RAILWAYS

SKILLS WEEK

Brush up your modelling skills with World of Railways Skills Week

A week-long event in September is to be hosted at www.world-of-railways.co.uk, which will come jam-packed with best practice advice, video demonstrations and step-by-step guides.

The free-to-attend online event kicks off on September 20th and runs until Friday, September 24th, with a different theme on offer each day, covering all major modelling topics including trackwork, weathering, scenery and layout control.

Each day will also showcase a couple of layouts previously featured in BRM Magazine, which will provide extra inspiration for any would-be modeller.

Whether you're new to railway modelling, or looking to hone your skills, they'll be something on offer for everyone.

**Day 1 – Monday September 20th:
Getting to grips with track**

**Day 2 – Tuesday September 21st:
Setting the scene**

**Day 3 – Wednesday September 22nd:
Layout control**

**Day 4 – Thursday September 23rd:
Adding detail**

**Day 5 – Friday September 24th:
Rolling stock**

It's a week not to be missed. If you haven't done so already, pop the date in your diary and sign up to our weekly newsletter at www.world-of-railways.co.uk to get regular updates.

*Stay tuned as we reveal
more exciting details
of the World of Railways Skills Week soon!*



HOW TO... LIGHT YOUR LAYOUT

Words & photography: Jamie Warne

Having gained valuable experience through previous installations, Jamie Warne offers practical solutions to place your model railway in its best light.



- 3mm and 6mm plywood scraps
- 32mm x 32mm PSE softwood
- Red and black wire (22 gauge)
- LED high density tape (5m reel)
- LED driver of suitable power
- Aluminium mini-trunking kit (with brackets, end caps, and diffuser covers)
- 2.1mm male & female jacks



- Carpentry square
- Clamps
- Coarse sandpaper
- Cordless drill and suitable bits
- Electrical screwdriver
- Handsaw and tenon saw
- Pencil and measuring tape
- Scissors
- Selection of 10-50mm screws
- Soldering iron
- Solder and flux paste
- Wire strippers and cutters
- Wood glue

Optional:

- Bandsaw and/or circular saw
- M4 bolts (50mm) and nuts



I've seen so many people ask how to light layouts effectively and easily, that I thought it was time I set the record straight, especially when it comes to the choice of components, design of the lighting rig, and its construction. Truth is, you don't need a degree in electronics (I certainly don't have one!) to master the basic principles, but it is easy to overlook some of these aspects.

As Giles Favell noted in his article in the Spring 2020 issue of BRM, lighting a layout is a very important aspect of layout design. If your audience can't properly see your layout, then you've effectively wasted the many hours you've spent pouring your heart and soul into building it. This goes for home layouts, too! There will be some inevitable crossover with Giles' article, but I'm going to build on that by showing you how to produce a lightweight, versatile, and relatively low-cost lighting rig. Specifically, I'll be going into more detail about LED tapes and their associated components.

It's not quite as simple as buying an

'all-in-one' kit that includes a transformer and a reel of LED tape. OK, you could buy one, and they would probably work fine; but these kits tend to be equipped with transformers or power supplies that aren't purposefully designed for LEDs, but are rather more generic 12V supplies. LEDs can be a little bit fussy when it comes to their power requirements – that's why LED drivers exist. On a basic level, these ensure higher efficiency, and can help prolong the life of the LED tape.

Now, it's all very well having a new lighting rig for each layout you have, but wouldn't it be easier and cheaper to design a system whereby as many components as possible can be used for other layouts? Sure, some layouts may be wildly different in size and shape, but that doesn't make it impossible. The beauty of 12V LED tapes is that, if you buy a big enough driver, you can keep on extending the system. If you combine the tapes with aluminium trunking of varying lengths, as well as

create a standard mounting and connection system, it becomes incredibly easy to create any number of set-ups in all kinds of configurations and locales, with limited additional cost.

Quality lighting rigs don't need to be heavy, complicated to make, nor ridiculously expensive. In fact, for this project, I'm using leftover materials from old, dismantled layouts (PSE softwood and scraps of plywood). All I had to buy was a 5m reel of LED tape (£12), some aluminium profiles with brackets, end covers and diffusers (another £12), the LED driver (about £35), and finally, some 22-gauge wire and 2.1mm male/female jacks (£14 in total, which included 10 pairs of jacks).

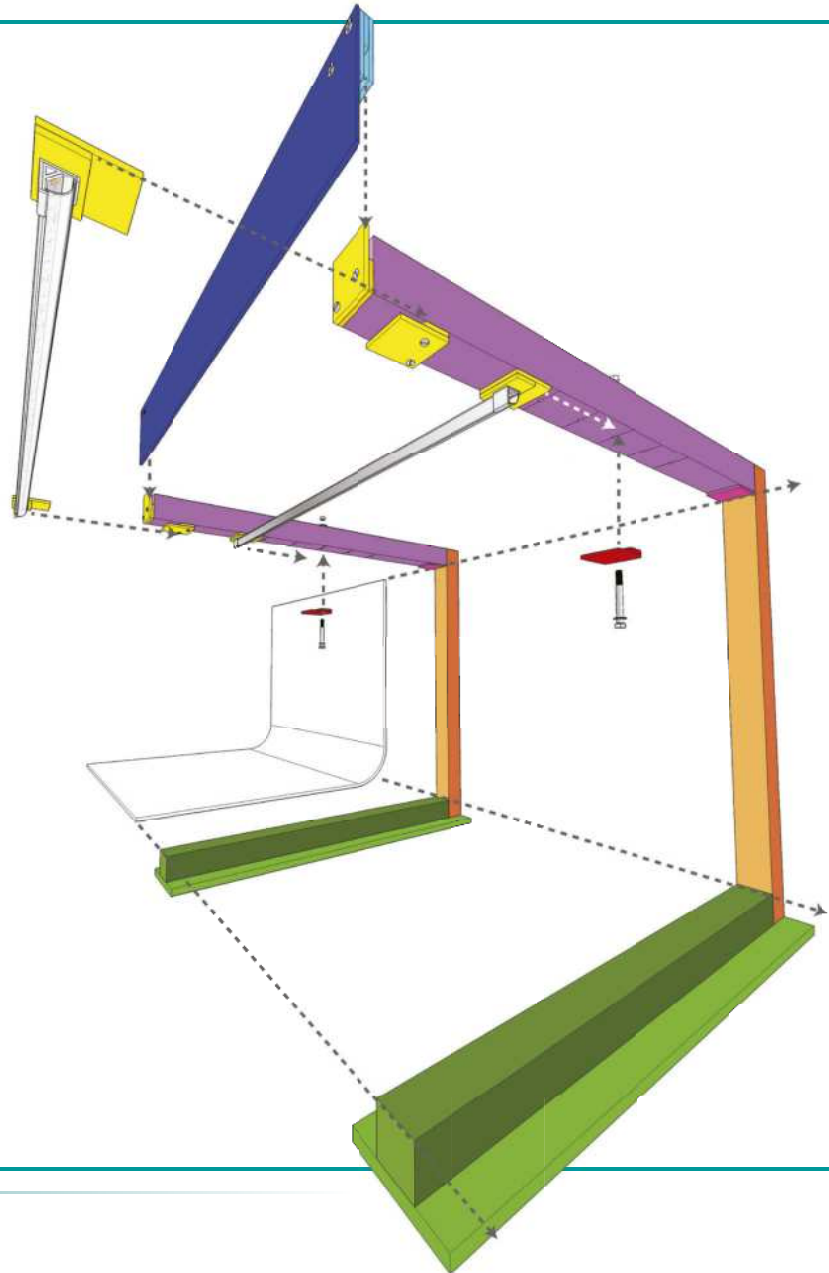
The driver that I purchased is 150W, and thus can actually handle up to 15m of high-density LED tape. So, given another £24 to buy two more reels of LED tape, we have enough versatility here to light any number of future layouts up to 5m/16ft in length (assuming a 3-run set-up).

The design

There are two frames, each made from three 32mm x 32mm PSE softwood lengths (purple, orange and dark green on the 3D model). Attached underneath is a 6mm ply ski (light green) for stability. The 3mm ply pelmet (dark blue) and both runs of lighting (LED tapes sitting in aluminium profiles) are removable via brackets (yellow) made from more 3mm ply scraps. Each bracket is made from two identical 'L' shapes that interlock, and each 'L' shape is a 32mm x 32mm ply square, with a 32mm x 16mm spacer.

Additionally, the lighting run closest to the (orange) upright can be moved forwards or backwards using the (red) brackets; these have an M4 bolt that corresponds with holes in the (purple) arm, which are spaced 50mm apart. While not really necessary for layout lighting, as I'll be using this rig to photograph various dioramas I need a flexible system to ensure that nothing is in shadow. Lastly, a white cardboard backdrop sits on the (dark green) frame, and slots behind the stopper (pink 3mm ply squares under the frame arm).

Ultimately, this lighting rig design will be lightweight, easy to build, low-cost, and versatile. Indeed, I use the same construction principles for my layouts!





I recommend... LED tape

- ✓ Robust
- ✓ Cheap
- ✓ Versatile
- ✓ Compact
- ✓ Safer
- ✓ Widely available
- ✓ Energy efficient
- ✓ Lightweight
- ✓ Wide beam angle
- ✓ Long-lasting
- ✓ Colour temperature options

Fluorescent tubes are fragile, inefficient, and not as long lasting. Along with energy-saving bulbs, they also tend to cast the 'wrong' colour, and can often be quite dim. Energy-saving bulbs have the added disadvantage of being very expensive! Spotlights tend to get rather hot, and they also don't provide a wide spread of light. This means that you'd need a very powerful (blinding, even!) spotlight set some distance away from the layout to pull it off efficiently. Not impossible, but hardly ideal!

DENSITY

Whether you want standard or high density tapes depends on the brightness desired.

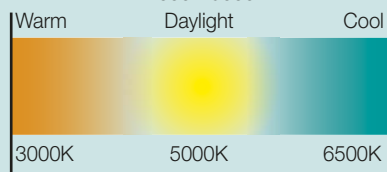
NOTE: the larger gaps between LED chips in SD tapes can create dark spots.



COLOUR TEMP

Quote in Kelvins, this determines how warm or cool your light is. For tonal variation, try a mixture of daylight and warm daylight tapes.

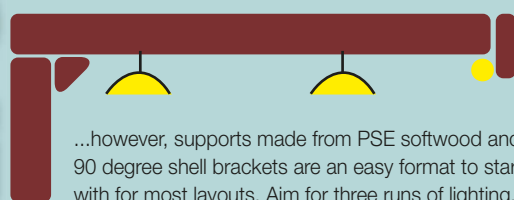
4000 - 5000



I recommend...

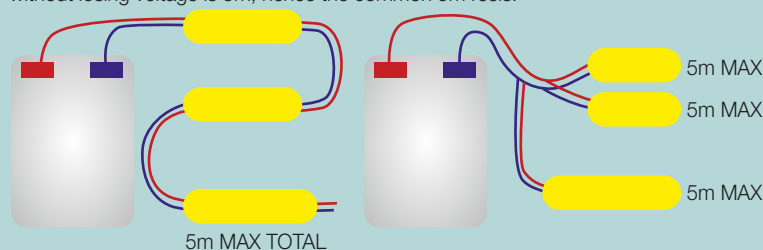
...using high density LED tapes of between 4000 to 5000 Kelvins. Avoid RGB LED tapes as they are harder to achieve a natural colour.

Your lighting rig design will depend on the size, shape, circumstances/portability of your layout, and the space you have to store/transport it all...



...however, supports made from PSE softwood and 90 degree shell brackets are an easy format to start with for most layouts. Aim for three runs of lighting.

While LED tape is convenient, the usual maximum length you can wire in series without losing voltage is 5m, hence the common 5m reels.



Ideals: ✓ Relatively lightweight ✓ Collapsible (if possible) ✓ Multiple lighting runs ✓ 'Plug and Play'

A dedicated driver will power LED tape at maximum efficiency. You can always buy a larger driver than you need, but not a smaller one.

STEP 1

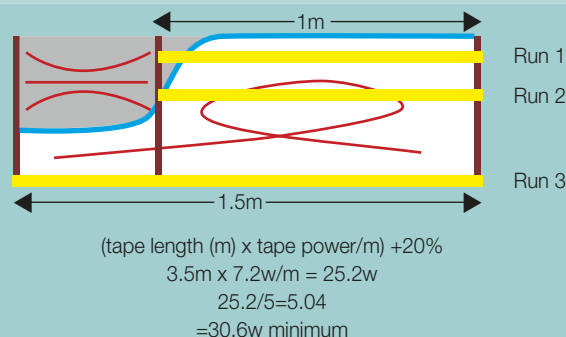
Check whether the LED tape you bought needs a constant current or constant voltage driver.

STEP 2

... then use the following to find what size driver power (min) you need:

$$\left(\text{LED tape length (m)} \times \text{Power of LED tape/m} \right) + 20\%$$

Eg.



CONNECTORS

To join runs of lighting, 2.1mm jacks are a really simple and reliable method. You can also either make your own splitter cables (for wiring in parallel) or buy them pre-wired. All this means you can adjust your setup to suit multiple layouts or uses!

$$\text{Current} = \frac{\text{Power}}{\text{Voltage}}$$

$$\frac{60\text{w}}{12\text{v}} = 5\text{A}$$

Minimum Rating
 Ensure the wire used is of sufficient gauge for the current. You can use online calculators or tables to help you make the right choice.

WIRES

Gauge (AWG)	Ampacity (A)
12	30
14	25
16	18
20	11
22	7
24	3.5
26	2.2

TRUNKING

I use aluminium profile kits to mount LED tapes to supports; they are lightweight, often come with diffusers to soften light, and also help act as a heatsink.

OVERHANG

Extend your lighting rig over the front of your layout to ensure that any objects in this area are adequately lit!

TECHNIQUES



First, cut the six 32mm x 32mm (or similar) PSE softwood components to the length needed for the two rig supports. If you don't have a circular saw, a handsaw will do the job just fine.



With the frames cut, drill pilot holes to prevent the wood splitting. A slight diagonal placement should also prevent twisting. Countersink the holes before driving in the screws.



Cut the 6mm or 9mm plywood 'skins' to size. Mark out the centre line on the frame bottom and the skis, and then drill, countersink, and screw it into place.



Mark the 3mm plywood to form all the homemade brackets we'll need. For this rig, that's; 10 32mm x 16mm spacers, and 14 32mm x 32mm squares (two are backscene stoppers, and two are used later for the pelmet).



Cutting these small components can be tricky. If you have access to a bandsaw, that's probably the easier route. Otherwise, a hacksaw or tenon saw will probably do the trick. Try and use one with fine teeth.



The fascia can be cut since we have the 3mm plywood to hand. As I can't saw wood straight, personally, I'd use a circular saw; but here, my Dad is using a handsaw – he is a carpenter, after all!



Don't forget to sand all edges throughout the build. Plywood is particularly notorious for splintering (especially modern stuff), so at the very least do a pass with some coarse sandpaper.

SAFETY FIRST

When using power tools, always take safety precautions; not only wearing PPE, but also ensuring work is clamped down, and that you are using guides where possible. If you are worried about fingers getting close to blades, use a notched offcut of wood to carefully push the component on the opposite side to the guide; this will ensure it is held tight against it.



The homemade 'L' brackets are now assembled and 10 are required. Wood glue is plenty strong enough for the job. I'm using mini-clamps to hold them while the glue cures.



The two 'U' brackets on the back of the fascia are formed from two 32mm x 32mm squares and a single 32mm x 16mm spacer. Attach to the fascia with wood glue and short screws.



10

Four of the brackets will need a pilot hole drilled in the centre of their spacer. I'm attaching brackets for the aluminium mini-trunking to clip into, with brass screws.



11

Hole placements will now need to be marked on the underside of the frame 'arm'. Start in the middle, and space the markings at your chosen intervals (I chose every 50mm) both directions. As always, drill pilot holes first.



12

While we've got the drill out, it's a good time to screw the background stopper in place, if you need one. Don't forget to mark out and leave the 3mm gap between the upright and the stopper.



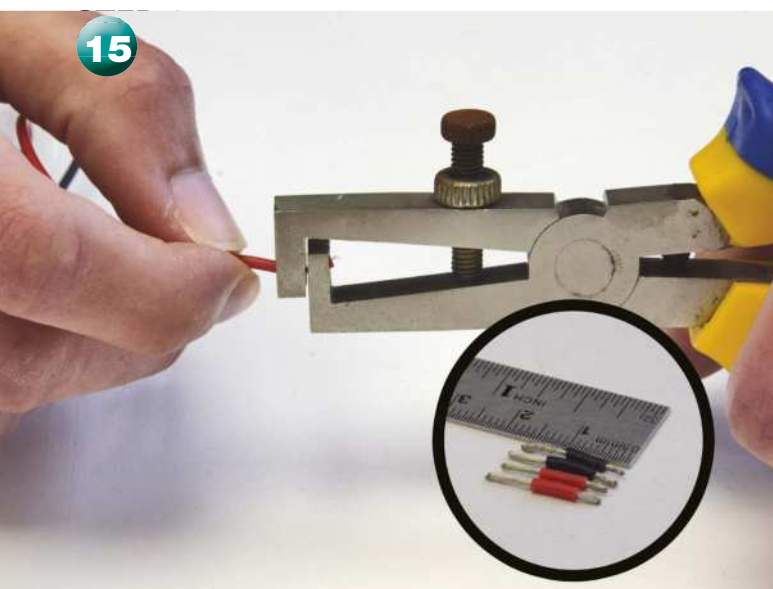
13

The holes can be enlarged to match the diameter of the bolts you intend to use. These larger diameter bits can give quite the kick, so make sure you clamp your frames down!



14

With the woodwork done, we can now focus on the lighting. Conveniently, LED tapes can be cut to length; simply cut carefully along the dotted line – here every three LEDs – with a pair of sharp scissors.

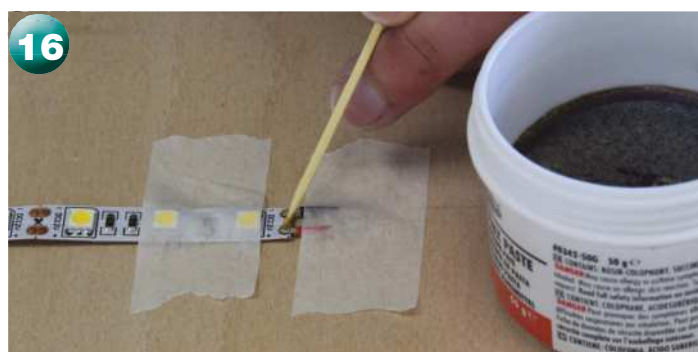


15

Strip 3mm from one end of the 22 gauge red and black wires. Cut each into 16mm lengths, then strip about 6mm off the other end. Don't strip more than is necessary. Two pairs are required per LED tape.

TOP TIP

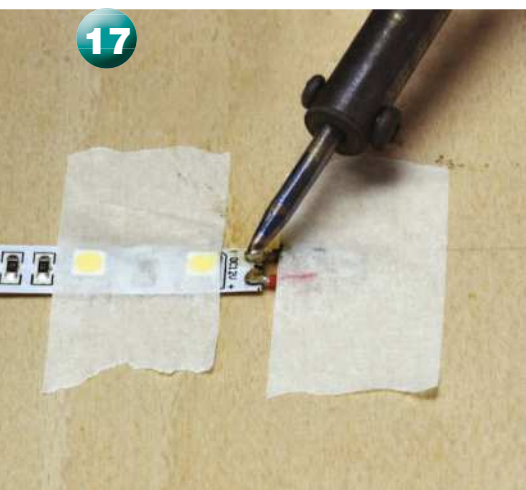
Holding onto and stripping short bits of wire can be difficult if you don't have a mechanical stripper (these automatically grip and pull the insulation off). A trick I use is to put the wire in the stripper, clamp and twist it to cut the insulation as normal, but instead of pulling on the wire to release the insulation, I release the stripper and use fine-nosed pliers to carefully peel off the insulation instead.



16

Turn on the soldering iron. Meanwhile, secure the LED tape to a scrap of wood with masking tape. Do the same with the 3mm bare wires, securing them onto the LEDs' copper contact points. Apply flux to the wires and contacts.

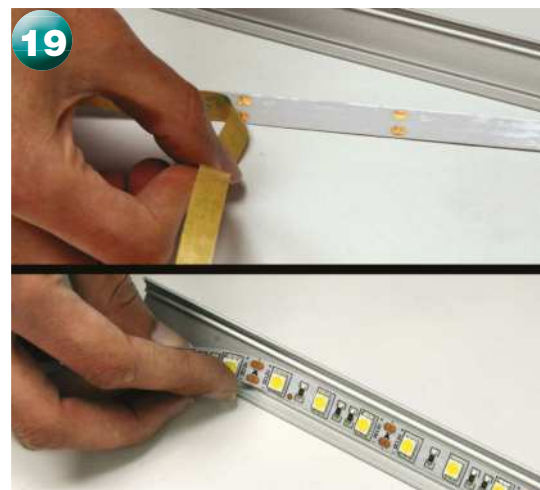
TECHNIQUES



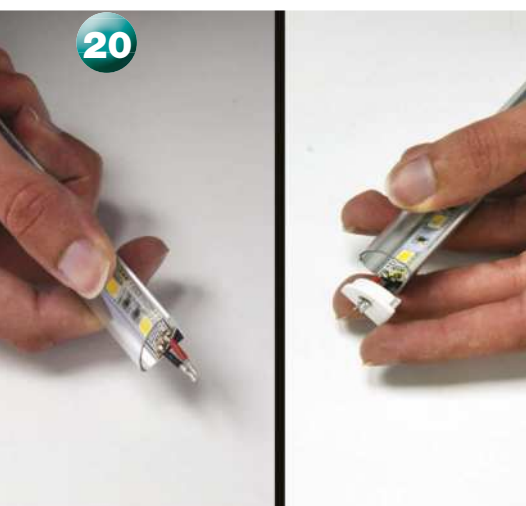
With a well-tinned tip, briefly press the soldering iron against the wire; long enough to melt the flux and draw the solder onto the components to form a neat, shiny joint.



Now add a little flux to the 6mm stripped end of the wires. With a cleaned and newly-tinned tip, dab the bare wires to form a stiffer wire more easily screwed into the 2.1mm connectors.



Remove the backing from the LED tape to expose the sticky tape. Starting from one end of the aluminium profile, carefully apply it to the centre; ensuring there are no lumps.



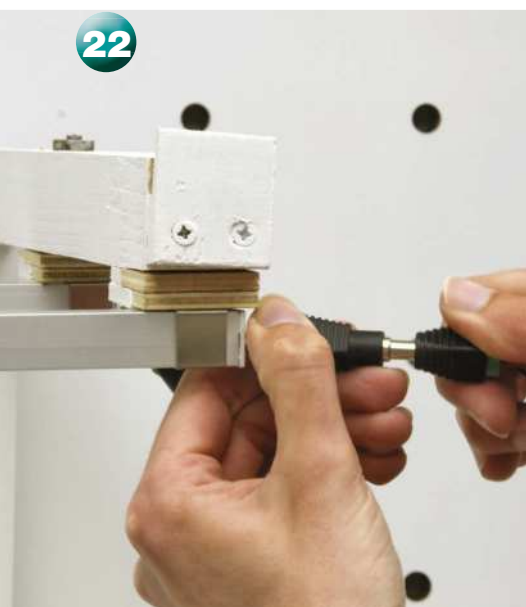
Clip on the diffuser, easier starting from one end, then thread the wires through the holes in the end plates. The plates are a simple push-fit.



Screw on a male 2.1mm jack to one end, and a female to the other; making sure the positives and negatives match the tape! Then snap the aluminium profiles onto their brackets, as shown.

TOP TIP

Always clean your soldering iron tip with brass wool; before, after every three joints, and when you're done. Additionally, it should always be 'tinned' by applying a small amount of solder immediately after cleaning. This will keep it from oxidising, and will result in neater and easier soldered joints. A shiny tip and soldered joint is what you're after, as it means no impurities.



The lighting runs can be attached to the frames. You can either buy a 2.1mm jack splitter cable to connect the LED tapes, or make your own as I have done.



Clip the pelmet/fascia onto the two brackets on the end of the supports, fit the backdrop into place, and then connect the driver. For exhibition layout rigs, you may want to add some cable tidying along one of the supports, and have somewhere you can screw the driver to out of sight. Note that this rig is easily stored on the pegboard in my studio; readily accessible for photographic purposes!

Find more great modelling advice at www.world-of-railways.co.uk/techniques

00 GAUGE MODEL RAILWAYS



Class 91 LOCOMOTIVES

Introduced in 1988 the class have been a familiar sight on the East Coast Mainline (ECML). Drawing on knowledge gained from the APT project, the Class 91 Bo-Bo locomotives were designed with an operating speed of 140mph in mind. Unfortunately, due to promised upgrades to the ECML not arriving speeds were limited to 125mph just as with their HST predecessors.

The Class 91 features 2 driving cabs, the first being wedge shaped and the other being flat. This was due to the unrealised intention to use the Class 91 for freight work at night as well as high speed passenger trains in the day. With the introduction of new Hitachi IEP sets on the ECML, Class 91 faces a more varied future being cascaded down to other routes.



R3890 BR, Class 91, Bo-Bo, 91002 'Durham Cathedral' - Era 8

DCC
READY 8 PIN
CONNECTION



R3891 LNER, Class 91, Bo-Bo, 91118 'The Fusiliers' - Era 11

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DCC
READY 8 PIN
CONNECTION



R3892 VTEC, Class 91, Bo-Bo, 91111 'For the Fallen' - Era 10

DCC
READY 8 PIN
CONNECTION



R3893 GNER, Class 91, Bo-Bo, 91117 'Cancer Research UK' - Era 10

DCC
READY 8 PIN
CONNECTION

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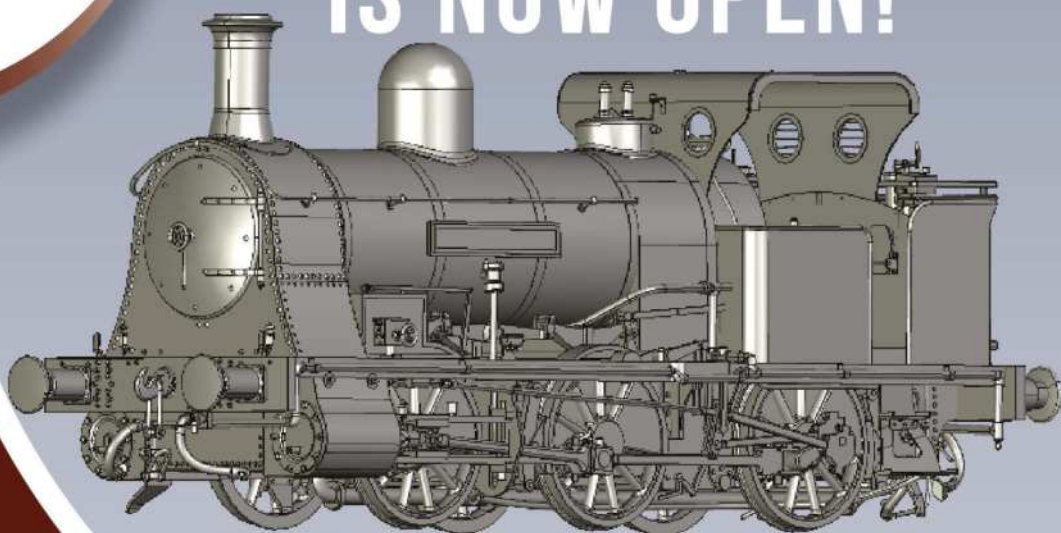
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BELLEROPHON

Bellerophon was one of six almost identical locomotives built for the Haydock Collieries between 1868 and 1887, Bellerophon herself being completed in 1874. KR Models will produce a DC and DCC Fitted OO gauge model. The 0-6-0 configuration will be very complex with the Stephenson/Gooch valve gear.

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TOWERS OF STEEL

To follow on from the article 'How to build a pylon' (BRM, March 2021), the subject of power grid infrastructure has been much overlooked within modelling circles. These towers have been a part of the British landscape for more than 90 years, and cover a large range of different voltages and designs from 11kV to 400kV.

As a child living in Oxfordshire, they were a prominent feature and sparked (pun intended) the imagination as to where the lines were running to and from, and the tower designs and substations where they congregated.

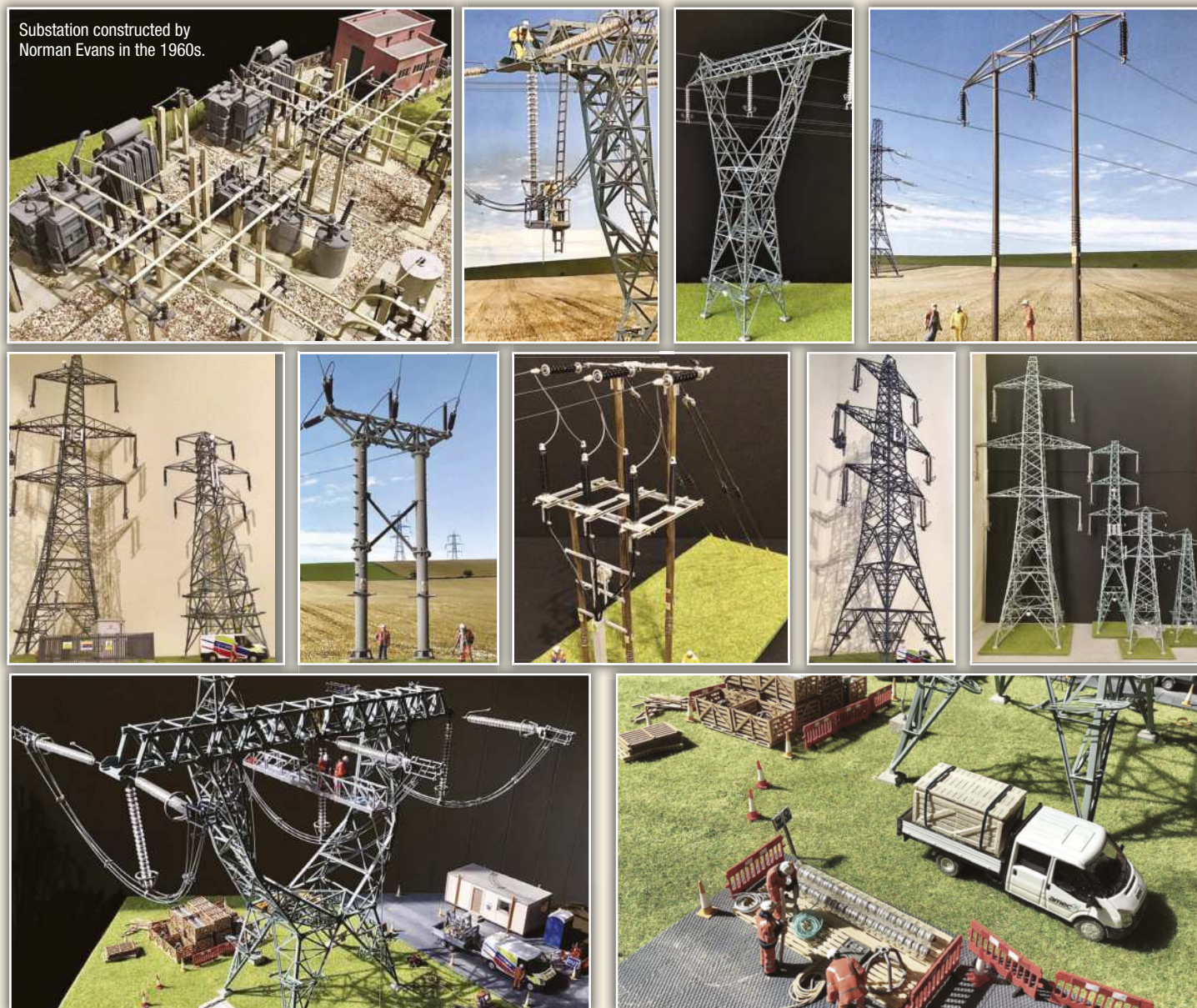
Fast forward to the present day and the fascination continues. Now the challenge was in offering something different, yet relevant to model railways. The aim was to showcase a variety of British designs all in 4mm:1ft / 1:76 scale. Via the internet and various associates within the industry, accessing the original CEGB/National Grid blueprints/engineers' drawings enabled accurate scale models that could be incorporated on layouts and dioramas. With so many designs available, each model posed its own building challenges, and diagrams were made to scale and used as templates. Plastruct and evergreen plastics were cut and glued to form these rather complex structures, while insulators comprised of acrylic or metal washers, as well as plastic tubing.

When completed, each model was airbrushed with enamel paints. In the case of the utility poles, acrylic sprays were applied to give a durable and realistic finish. Additional detailing including Noch birds embellished each model with a little more character.

Although a steady hand is required, only basic equipment such as a craft knife, cutting mat, brushes, rule, protractor, super glue, Plastic-Weld, and masking tape were used to produce each tower or pole. The only model I didn't construct was a rather impressive 33/66kV 1950s 1:76 scale substation, which was acquired while visiting the Astolat Railway exhibition's club stand in 2018. Built by electrical engineer, Norman Evans, in the 1960s from metal and wood, it brilliantly captures all the detailing and intricacies the prototypes possess to a tee.

Paul Rees

Substation constructed by
Norman Evans in the 1960s.



EVERYONE LOVES CHOCOLATE



I'm a committee member of the Railway Enthusiasts Club (REC) at Farnborough in North East Hampshire and have been making a representation of the Fry's Chocolate Train.

In my previous article (Inbox, September 2020) I was pondering on how to finish it, and not able to get satisfactory lettering, I decided on a compromise. The eagle-eyed of you might have noticed that there is a livery problem with the train. There are two photographs in my previous article that show the train from the same angle and they look different.

This has enabled me to compromise – isn't modelling all about compromise? Despite their dissimilar font, I purchased three sets of Slater's lettering – two in 5mm and one in 3mm. I painted them black, then touched the top surface with a golden yellow paint. These have been mounted as per the first photograph using PVA in the vain hope that I can remove them at a later date, and I hope they look good.

I made vacuum pipes from violin string. I still have to sort out the bogie mountings. I'm not really sure how I'm going to do this as I need to trap the nut before fixing down the roof. The opinion from my club colleagues is that I should use self-tapping screws.

I had fitted a NEM socket to the outer of one of the vans but this obstructed the bogies, so it is either three-link or my favourite Sprat and Winkle couplings! I have to thank my 'guiding lights' whom I have bothered mercilessly, namely, Kevin Fry, Dave Smith, Mike Walshaw and Martin Lilley, all of whom are members of the REC at Farnborough. It shows why you all should join your local Model Railway Club – all skills are welcomed!

Charles Weager

ONLINE MODELLING SHOWCASE

The Great Eastern Railway Society (GERS), although known as a railway historical society, was originally founded by modellers seeking to establish a common resource group. The GERS has always had a strong modelling community and has recently sought to rekindle member interest in modelling.

Since January, we have created a members-only modelling forum, revised the modelling part of our website and have now published an online 'showcase' of East Anglian and East London modelling. The 'showcase' comprises 26 layouts – many never-before-published – a cornucopia of modelling projects and some examples of 'virtual modelling' – the ultimate no space modelling technique!

The GERS would be pleased to receive copyright-free material that has been published but which has no permanent home to which public access can be gained. For some, these models can be as important a part of railway history as the real thing. Find our showcase on our website www.gersociety.org.uk.

Paul Godwin

SEARCHING FOR PLANS

Hello, I'm Dhanushka and I'm from Sri Lanka. I'm a lover of British locomotives and have many models of them, all ordered from Hattons. I want to build British lineside buildings, but I don't have any dimensions. On the internet, I can't find any accurate dimensions, either. Could you please tell me of a reliable source to find plans?

Dhanushka Kariyawasam

Howard Smith replies: Dhanushka, it's a pretty common problem to have and though measuring a building in the UK isn't an option for you – even if it still existed – a calculated guess is an often-employed technique by modellers. You can roughly calculate dimensions by counting bricks in photographs or using the height of doorways. If it's scale plans you require, that really depends on the line you're modelling and the railway company to have constructed the building(s). Books still remain the best resource for model plans, but again, these tend to be railway company-specific.

Perhaps it's a more generic-looking building you'd like? In which case, let's put this to readers – do you have a favourite book containing model building plans? Let us know, send an email to us brm@warnersgroup.co.uk.



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Factfile

Layout name: Watery Lane Sidings
Scale/gauge: 4mm:1ft / 16.5mm gauge / OO
Size: 3ft 10in x 15in
Era/region: 1970s, West Midlands
Layout type: End-to-end

WATERY LANE SIDINGS

Dave Hall further refined his already highly-accomplished scrapyards scene, while avoiding the stereotypical operational quirks of smaller layouts.

Words & Photography: Dave Hall

The former Small Heath gas works W5 Peckett passes Danny Wizzbang's mobile café as it pulls in a train of empty minerals from the BR exchange siding, to be loaded with scrap.



A 220hp Yorkshire diesel-hydraulic shunts the Metal Box sidings. The locomotive was modified from a Craftsman etched kit for a BR Class 02.



In my article on 'Charlie Strong Metals', in the February 2021 edition of BRM, I said a little about my plans for the future of the layout, which included an extension and a new fiddle yard – this follow-up describes that new part of the layout. I mentioned having an engineering works and a coal merchant's siding, but an interest in particular types of wagon caused a change of plan.

The original plan changed for no other reason than that I wanted to run a train of covered vans, and so the idea of having a branch of Metal Box, and adding modeller's licence to the products that they made meant that I could also run coil-carrying wagons.

Metal Box was a real company and did exactly what it 'said on the tin'. It made metal packaging, such as biscuit tins and paint tins, with different branches making different lines. The imaginary Small Heath branch makes paint tins. It takes only a small step of the imagination to think that an oil drum is basically a large version of a paint tin and so it also produces oil drums, which allows me to run wagons carrying strip coil.

The siding that was going to be a coal merchant's yard became one leading to a Shelby Group workshop. Here, maintenance on the lorry fleet, including those of Charlie Strong's scrapyard, is carried out. The rail access to this building serves the same purpose as that for the lorries, only for maintenance and overhaul of locomotives. The reason for choosing the workshop rather than the coal merchant was twofold. Firstly, and as a practical device, it provides a substantial building to act as a view blocker for the connection to the scrapyard board. Secondly, it gives an excuse to run more locomotives! By having locomotives in for overhaul from other parts of the group of companies, I can justify having far more than is realistically necessary for a small scrapyard. As the servicing facilities at the scrapyard are minimal, I have also added a canopy for servicing.

Setting the scene

The extension and fiddle yard follow the same practice with regard to the construction of the baseboards, the track, and the electrics as on 'Charlie Strong Metals', so there is really nothing to add over what was said in my previous article, save for that instead of sand for ballast, I have used a mixture of real stone, coal, coke and ash. These materials were pulverised using a



pestle and mortar before being sieved.

Once again, there really isn't much in the way of proper structures as most of them are low-relief, forming the backscene. The Metal Box factory consists of Skytex resin mouldings that were bought unpainted and have had some windows covered over by sliding doors that open onto a loading platform. The platform itself was made from

card and painted with concrete colours and washes.

Above the platform is a canopy, constructed using Wills corrugated asbestos, that was carefully filed to taper it at the edge in order for it to appear thinner. The other part of the factory is also low-relief but was

made from scratch, using Wills corrugated asbestos and breeze block sheets.

Within the Metal Box compound is a fuel oil storage tank, which provides fuel for heating and processes within the factory. The basis of this tank is a toothpaste tube. Not the squeeze type, obviously, but one of the hard plastic pump-type tubes. Wrapped around the tube is a layer of thin Plastikard,



A Shelby Haulage Atkinson Borderer is parked on the waste ground while its driver goes for a brew at Danny Wizbang's mobile café. In the background, a Vanguard diesel-hydraulic comes off the shed road to shunt vans that have been left by a BR trip working.

which was scored to represent panels of welded construction. At the top of the tank, there is a guard rail, made from 0.45mm brass wire. More wire was soldered to a Severn Models etched brass ladder to form a safety cage, which is attached to the side of the tank. The final part of this structure is a bund wall, which I made from card and painted and weathered to appear as concrete.

The haulage yard and locomotive repair shop has the only proper building of substance. The walls were made using card and Wills brick sheet, with card toppings appearing as concrete blocks. The roof sections are removable and are held in place by magnets. The doors on this building are of the roller shutter type and are made from thin corrugated plastic that wraps around a brass bar so that the door can be opened and closed. To open and close it requires a hook on a pole, rather like a shunting pole and this is not an easy task, so I leave the door open, however, I may motorise it in the future.

The other building in the haulage yard is a covered locomotive servicing area. I have made this to appear as something of an afterthought that was built low-cost, using materials from the scrapyards.



The Thomas Hill Vanguard 4wDH, works fuel oil tanks for the Metal Box works. The locomotive was constructed from an etched kit manufactured by RT Models.

The haulage yard and locomotive workshop, with Charlie Strong's Scammell Pioneer recovery truck in the yard.



Operation

Having this new board has opened up more possibilities and interest in operation of the scrapyards, thanks to the run-round loop, in addition to providing a greater variety of traffic in itself.

Inward traffic for the Metal Box factory consists of steel coil, carried in a variety of Coil C, Coil J and Bogie Bolster E wagons. Tinplate arrives in Shock-absorbing vans. Fuel oil is carried in tank wagons. Finished products leave in covered vans and in containers, carried on Conflats.

The extension has also brought in the use of British Rail locomotives, however, the tight curves mean these are restricted to Classes 03 and 08. These BR locomotives also bring with them the use of brake vans, which add an extra layer of complexity to the shunting moves. The BR locomotives are not allowed to enter the scrapyards board, which adds yet another layer.

On the other hand, some of the industrial locomotives are allowed to work into the BR exchange sidings (the fiddle yard), but only those that are plated as registered with the British Transport Commission. This was something that happened in real life and BTC plates are available from RT

Models and Narrow Planet. All of this makes operation varied and interesting, without it being a shunting puzzle.

Locomotives and rolling stock

Since my previous article, the locomotive fleet has expanded and keeps on expanding. Operating a scrapyards layout gives motive power opportunities that are simple to justify and all one has to say is that "it was bought for scrap". Many rail-connected scrapyards used locomotives that were bought for scrap but were found to have useful life left in them until they broke down and were then cut up, to be replaced by another that would eventually meet the same

fate. I don't keep them all on the layout at once but will change what is used from time to time, with at least four on the layout at any one time.

Some of the locomotives are not in fact new additions to my collection but are models that I built or bought some time ago, for other layouts. They have been added to the pool available for use on 'Watery Lane'/'Charlie Strong's' by the subsequent fitting of DCC sound. Including the BR locomotives, there are some 16 available for use on the layout, a mixture of kits and ready-to-run. The latter may be ready-to-run but they are never ready-to-use on the layout. Everything must be weathered and I

The locomotive fleet

Ruston 48DS 4wDM – Hornby RTR. Repainted.
 Ruston LSSE 0-6-0DE – Modified from Heljan BR Class 07
 11T Hibberd 4wDM – Nonneminstre whitemetal kit.
 16in Barclay 0-4-0ST – Hattons RTR. Repainted.
 16in Barclay 0-4-0ST – Hattons RTR. Modified.
 14in Barclay 0-4-0ST – Hattons RTR. Repainted.
 Peckett W4 0-4-0ST – Hornby RTR. Modified and repainted. Planet Industrials cab.
 Peckett W4 0-4-0ST – Hornby RTR. Repainted. Planet Industrials cab.
 Thomas Hill Vanguard 4wDH – RT Models etched kit.
 Yorkshire Engine Co. DE2 0-4-0DE – Judith Edge etched kit.
 Yorkshire Engine Co. 220HP 0-4-0DH – Modified from Craftsman etched kit for BR Class 02
 BR Class 03 – Bachmann RTR. Renumbered
 BR Class 08 – Bachmann RTR. Renumbered.



like to alter things so that they are never out-of-the-box models.

The previously mentioned Coil C and Coil J wagons are not available as either kits or ready-to-run models, so I had to improvise. As far as the prototype goes, British Rail simply fitted coil-carrying cradles into old Diag 1/004 and 1/007 Pig Iron wagons and renamed them Coil C. They similarly cut down the sides on 26T and 27T tipplers to create Coil J.

My Coil C wagons use the bodies of very old Trix Pig Iron wagons and are fitted to the underframes of Hornby 27T Iron Ore tippler wagons, and in one case the underframe of a Parkside 26T tippler. The Coil Js were made by cutting down the sides of a Parkside 26T tippler, for one, and for the other, a Bachmann sand tippler was cut down. New

capping rails were made from plastic strip, and cradles for all were made from plastic strip and brass wire.

All other wagons are generally available kits or RTR.

The future

It is often said that a model railway is never really finished and that's probably true, but there is nothing major to add and no alterations are planned.

Anything else will be small details and additions to the rolling stock, as far as 'Charlie Strongs/Watery Lane Sidings' are concerned, although, I do have plans to construct a reasonably large loft layout that will incorporate a blue era BR line and an industrial line with coal industry associations...



A BR Class 08 and brake van arrive to collect empty 'Shocvans', to be tripped to Small Heath yard, before onward movement to Ebbw Vale, where they will be loaded with tinplate.

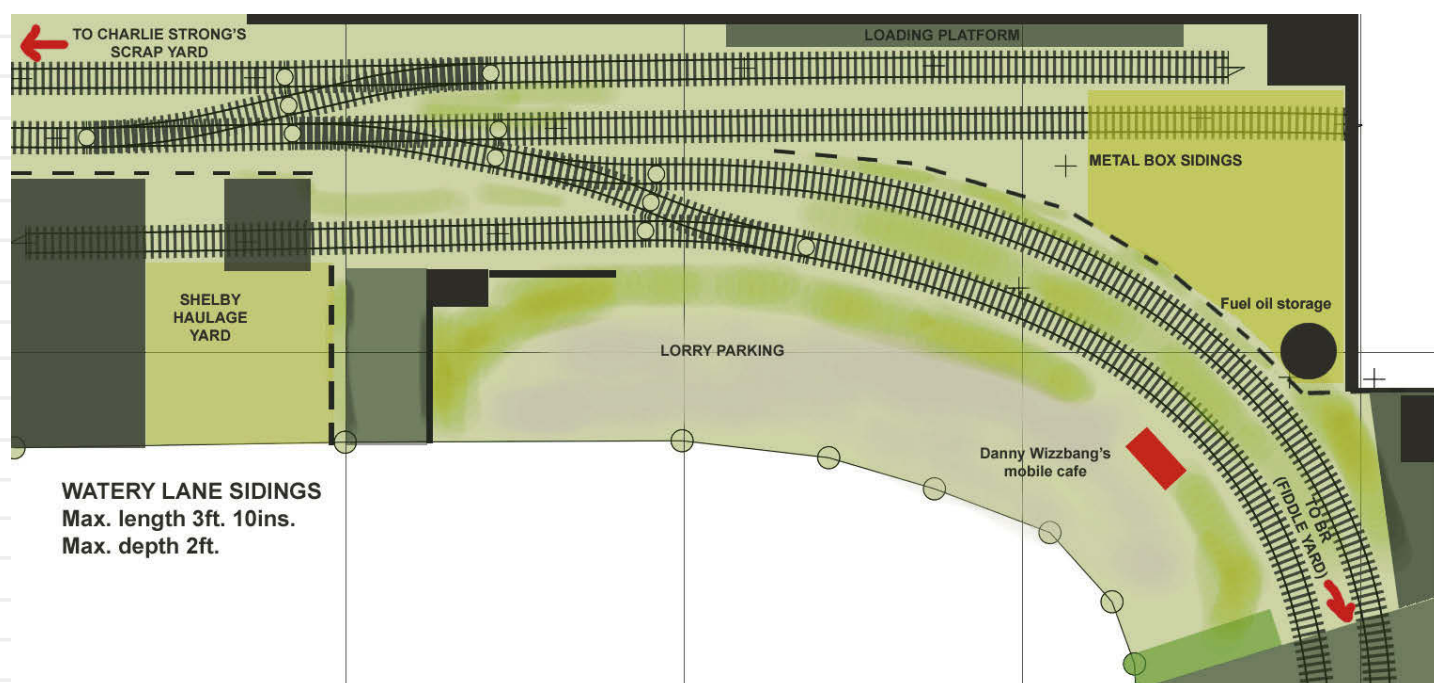




A BR Class 03 and brake van departs Watery Lane, while the driver of the Ribble Cement ERF gets something to eat before the long journey back up the M6.

Trackplan

WATERY LANE SIDINGS

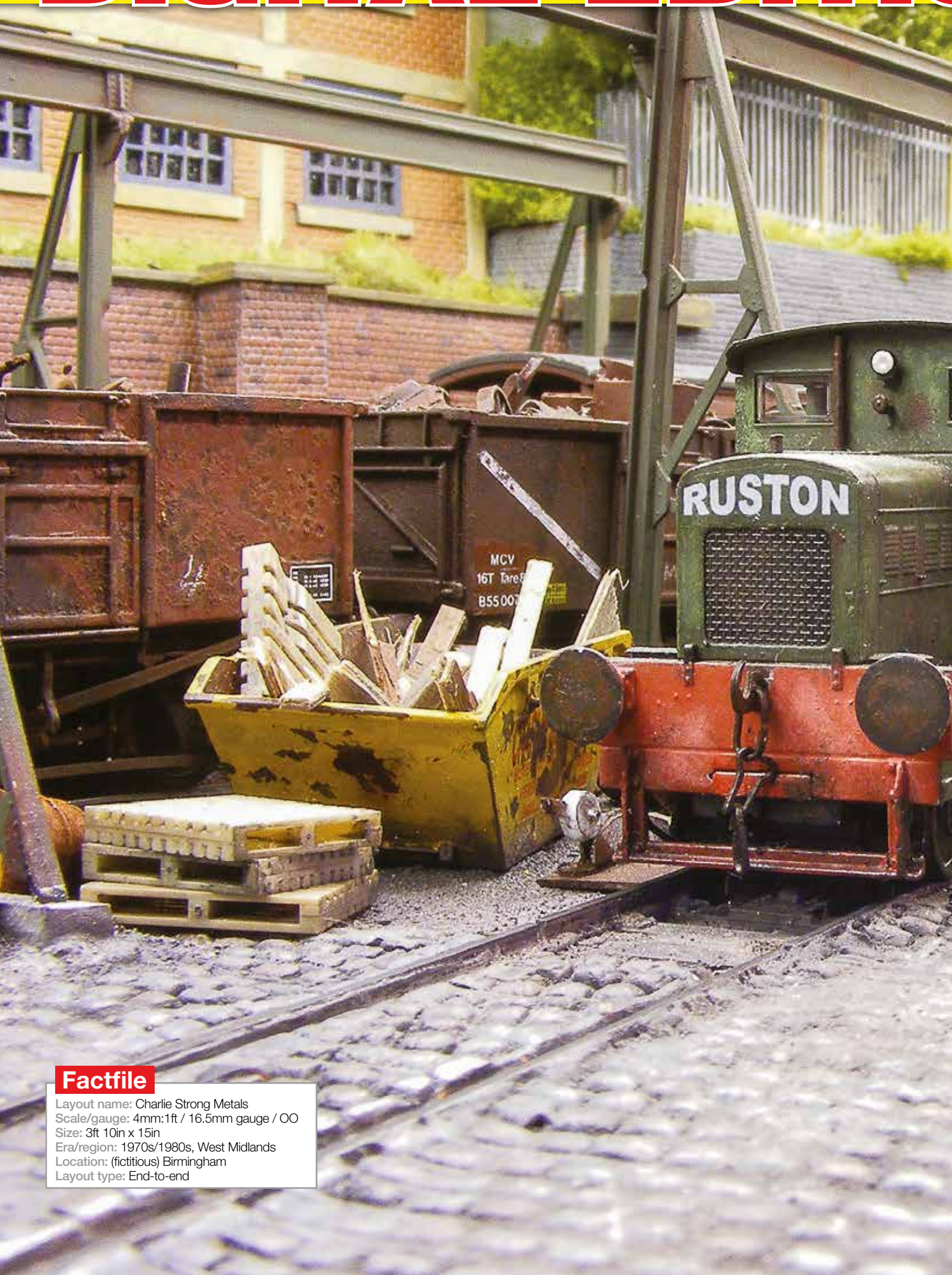




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Factfile

Layout name: Charlie Strong Metals
Scale/gauge: 4mm:1ft / 16.5mm gauge / OO
Size: 3ft 10in x 15in
Era/region: 1970s/1980s, West Midlands
Location: (fictitious) Birmingham
Layout type: End-to-end

ON EXCLUSIVE

CHARLIE STRONG METALS

Predictably, Hornby's Ruston & Hornsby 48DS locomotive has stimulated new layouts, including this highly-immersive metal recycling scene by Dave Hall.

Words & photography: Dave Hall



A Ruston 48DS propels empty mineral wagons to be loaded under the main gantry crane. In the background, another rake of minerals are loaded under the smaller gantry.

The 48DS, one of the latest ready-to-run industrial locomotives from Hornby, released in 2019, was a welcome addition to the growing ranks of industrial locomotives being produced by mainstream model manufacturers. It was something that I never thought would be produced as a ready-to-run model. I have had an interest in these, the smallest standard gauge locomotives built by the Lincoln firm of Ruston & Hornsby, for many years, so purchasing a pair of the new models was something I couldn't resist.

Rather than have my models sit on a shelf, or make occasional (but out of place) runs on my pre-grouping layout, I decided to build a layout especially to run these small locomotives on and chose a scrapyards as it is an industry that brings back memories of my visits to yards such as Booth's, in Rotherham, when spotting industrial locomotives in the 1980s and 1990s.

The layout is fictional, even though it is supposedly set in a real place, but it bears no resemblance to Small Heath in Birmingham. The name 'Charlie Strong Metals', comes from the BBC television series *Peaky Blinders*, where a character of that name has a scrapyards. Other *Peaky*'s influences on the layout include the Shelby distillery and The Garrison Tavern.

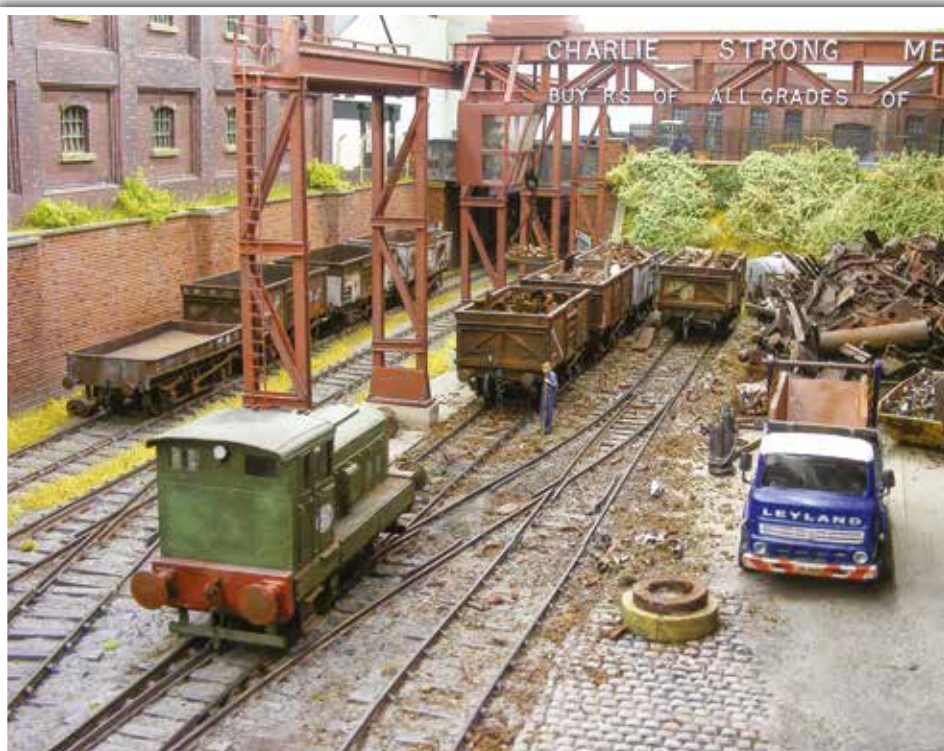
The foundations

The baseboard is a mere 3ft 10in x 1ft 3in, with a fiddle yard of 3ft 6in in length. It occupies a space above my desk, but below bookshelves that are integral with the desk. When designing Charlie's yard, I made the entry/exit tracks far enough back so that a hole could be cut in the bookshelf support without compromising its strength so as to allow a fiddle yard to be fitted at the side above my computer.

The baseboard is of very simple construction and uses 9mm ply for everything but the backscene, which is 5mm, reinforced along the top edge by a length of 12mm x 18mm pine. The underside of the baseboard is an open box, with no cross members, which makes wiring and point control easy to fit. The track area is completely flat, which made construction of the board very easy. The raised scenery, where the road runs along the right-hand end, is made from ribs of stiff foam board that was once an estate agent's sign. The ribs were covered in corrugated cardboard and papier mâché, made from newspaper and PVA.



One essential scenic device is the scrap pile and spilled scrap in the loading area. This was the most difficult thing to get right and I'm still not entirely convinced that I have made a good enough job of it. The main pile is a lump of insulation foam that was roughly hacked to shape and sprayed with black paint. The next stage was to glue to, and embed into it, hundreds of pieces of spare kit parts, cut up redundant wagons, lengths of Plastruct, pieces of rail – just about anything that may look like scrap. The pile was painted rusty colours and weathering powders were added. The ground spillage was made by using Plastikard shavings, sawdust, tea leaves and real steel swarf, all held in place by PVA glue that was dripped on in the same way one would ballast track.



The Ruston 88DS prepares to couple to loaded wagons under the main gantry crane.



The John Fowler 150hp diesel waits at the fuel pumps. DERV (Diesel Engine Road Vehicle) grade for the road vehicles and red diesel for the locomotives and on-site plant. In the background is the Shelby Distillery bonded store.

Digital control

The electrics are very simple because, being designed for DCC, there are no section switches. Copper tape is stuck to the underside of the board and wire droppers are soldered to every length of rail, leaving the rail joiners to line up the track joint with no reliance being placed on them for conductivity. There is, however, a DPDT switch that allows DC operation, but this is really only used to test new locomotives.

Trackwork is all Peco, with the points being Code 75 SLE 191/192 and the plain track being bullhead. In order to save space, on some of the points I have cut them short at the toe end and, on those under the main gantry crane, I have shortened the rails on the frog side of each.

Experience of operating my two previous small OO gauge layouts showed that

electrically-operated points are unnecessary as everything is within easy reach and so these were an expensive complication that I could do without for this layout. The points are operated by lengths of steel wire running through holes drilled in the baseboard sides and are supported underneath by electrical connector blocks. Short lengths of wire, bent through 90-degrees, are held fast to the operating wire by more electrical connector blocks and protrude through holes in the baseboard and into the point tie bars. These lengths of wire bear on the operating arms of microswitches that change the frog polarity as the point is changed. To give the operating wires a neat touch, I turned some small brass knobs to go on the ends.

Lighting is provided by a strip of LED lights that I acquired very cheaply from a friend who is an electrician.

Scenery and structures

The scene is meant to represent a former through line and railway goods yard that has been long closed and has been taken over by the scrapyard, hence the stone setts in the yard and the high retaining wall at the rear. This set up was inspired by the scrapyard of C.F. Booth in Rotherham that is on the line of what was once a main line railway and goods yard.

I wanted to create a boxed-in urban environment that made the layout area as self-contained as possible without the need for either a photographic or painted backscene. So, two sides of the box are almost entirely filled by low-relief structures. These are a mixture of scratch-built, kit parts and, in the case of the distillery warehouse, resin mouldings from Skytrex. There is only one building on the layout and this is a

simple scratch-built store-cum-engine shed, made from Wills corrugated sheets.

The other structures are a pair of scratch-built gantry cranes and a scrap press/baler. The press/baler was made entirely from Plastikard and is of no particular prototype; it is instead based on various photographs and drawings found on the internet. The large gantry crane is of no particular prototype but was based on gantry cranes that I have seen, plus some internet photographs. It is built from Plastruct and some old Code 100 steel rail. The Plastikard bases for each of the supports have a small neodymium magnet embedded in them and the base of the supports has a corresponding magnet. This is to enable the crane to be easily removed and replaced for track cleaning or transportation of the layout. It also prevents accidental damage if a hand or sleeve should catch it as the structure will simply detach rather than break. The gantry is fitted with magnets for the same purpose, but to also allow for moving the crane into different positions. It isn't a working model.

Operation

Such a small layout has limited operating interest, but the addition of DCC sound to the locomotives makes the driving of them more interesting and realistic. I have used sound in my O gauge locomotives, but putting the equipment into such small models as the 48DS was a challenge that I chose not to take on and instead had the work done by a professional. I have fitted DCC sound to the other locomotives, however. It is important to me to have the correct sounds, or if a sound file is not available, to have one that is as near as possible with regard to the engine and transmission. It isn't simply the sounds coming from the locomotives that add interest and realism, it's the way the Zimo decoders are set up to give inertia and braking. Unlike on some budget decoders, or a DC system, it's not simply a case of turning the dial down on the controller and the locomotive stopping dead. It becomes quite satisfying to be able to gauge when to brake to not run into the wagons that you are coupling up to.

On my previous two OO gauge layouts, I had used Dingham couplings, but I have always preferred the more realistic look of three-links and as another exercise in cost-saving and simplicity of wiring, I went with these. I make my own links and hooks and that is yet another cost-saving. As the stock always faces the same way, everything has



An overview of the entrance to the yard, with a Peckett 0-4-0ST in the foreground and a Ruston 48DS. In the background are condemned wagons for scrapping. The stone setts and tarmac infill are made using DAS clay.

ON EXCLUSIVE



brass links on one end and, on the other, the end link is iron wire. This enables the use of a small magnet on the end of a length of brass wire, which is fitted to a pencil torch, to be used as a shunter's pole. I find this much easier to use than trying to hook the links with a bent length of wire.

The operation of the yard is quite simple and mostly involves bringing in empty wagons from the British Rail connection/exchange siding, positioning them under the crane and filling them with scrap. I guess some extremely skilful and adventurous modellers would build a working crane and devise some method of loading the wagons, but I'm not that skilful so I place loads into the wagons by hand. It isn't an exhibition layout and I am usually the only person to see the 'Great Hand from The Sky', so I don't care. After all, The Hand has to come down to couple and uncouple the wagons, anyway.

Apart from wagons to be loaded with



The view from under the Garrison Lane road bridge, with the Ruston 88DS passing a rake of 16T minerals that await loading.



A well-worn Ruston 48DS. It started out as the army version, but has been partially repainted and custom transfers added, followed by a heavy dose of weathering.

scrap there is a variety of other wagons that come in to be scrapped. These are shunted into the rearmost siding and, during an operating session, the one in the cutting area is removed by hand and the line of wagons is shunted along the siding.

An operating session begins by moving out loaded wagons from the sidings and off to the exchange siding to be collected by a BR trip working, or in other words, to the fiddle yard. A 10-sided dice is thrown, which gives the number of empties that the BR locomotive has left – these are then positioned to be loaded. When the empties are in place, and the loads placed in them, the session ends.

The future

Plans are afoot to replace the existing fiddle yard with an extra scenic section and to build a new fiddle yard on the end of that. The new board will probably include a coal merchant's siding and a small engineering works. Provision will also be made for a run-round loop so as to make operation more interesting and to allow BR locomotives onto the scene as they deposit and collect the wagons to and from the rest of BR system.



Locomotives and rolling stock

The stock for carrying scrap is mainly the ubiquitous 16T mineral wagon, but I have seen photographs of other types, such as 13T steel highs, being used for scrap traffic and so have examples of those, too. The minerals are a mixture of modified Airfix and RTR Bachmann – all wagons have been heavily weathered by myself. Although dated, I prefer the Airfix kits over RTR as they are easier to adapt and convert to other variations. You may think all 16T minerals are the same, but they aren't!

The locomotive roster has inevitably expanded from the original two 48DS Rustons and even includes steam. Even into the 1980s, one scrapyard used steam to shunt then-modern wagons such as POAs. Not all the locomotives are on the layout at the same time, and usually only two are used per operating session.

Ruston 88DS 4wDM – built from a Judith Edge kit. This one formerly worked on my old layout, 'White Peak Limestone & Tarmacadam Ltd.' but has been fitted with sound, re-weathered and re-branded to work on this layout.

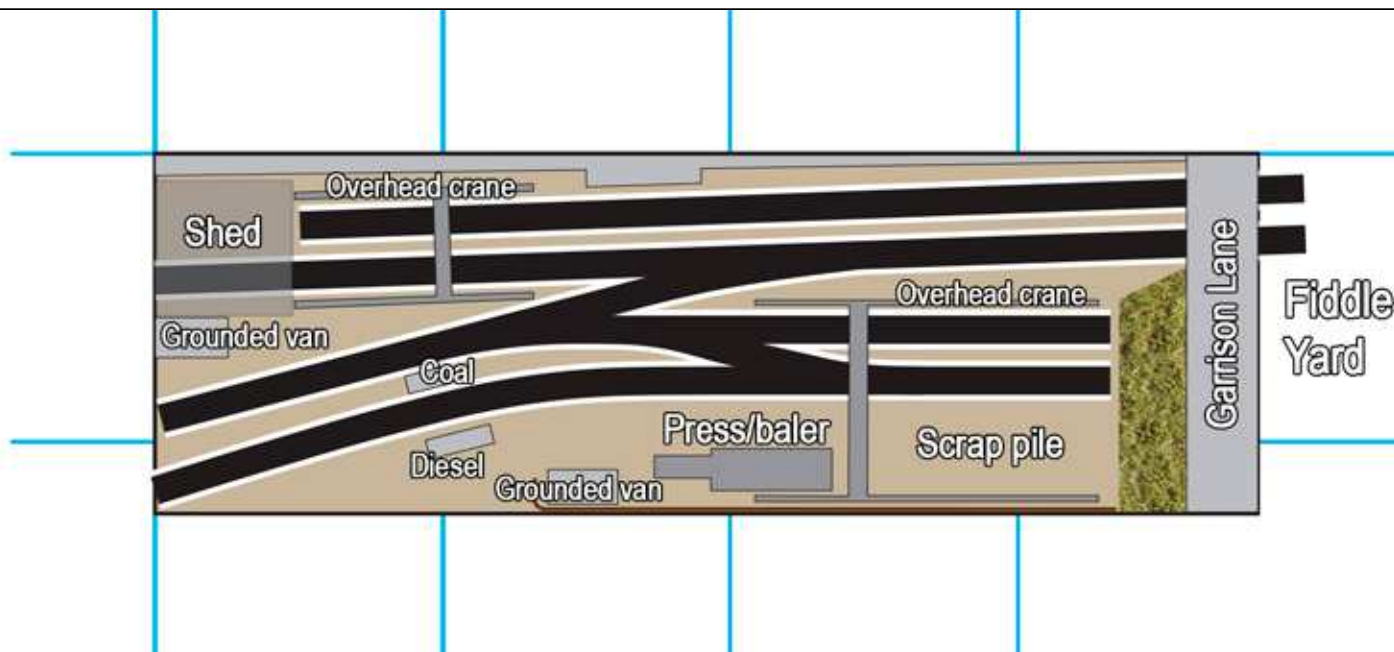
Fowler 150HP 0-4-ODM – built from a Judith Edge kit. This one is a new build and is also sound-fitted. It uses a High Level Loloader 90:1 gearbox and a budget Chinese 10/15 motor.

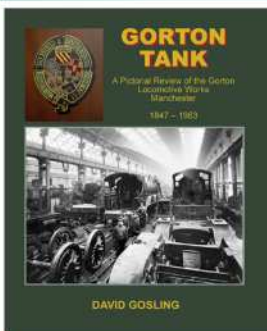
Peckett W4 0-4-OST – This is a Hornby RTR model and was formerly used on another of my old layouts, 'Nant-Y-Mynydd' (a Welsh colliery line). It has been rebuilt with a dropped footplate and low-height cab, plus the addition of sound.

Peckett W4 0-4-OST – This locomotive is modified to appear as a newer variation with the addition of a 3D-printed cab and buffer beams from Planet Industrials. I have fitted larger buffer heads, detailed and weathered it, as I do with all of my locomotives.

Trackplan

CHARLIE STRONG METALS





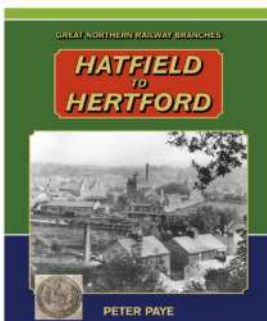
GORTON TANK

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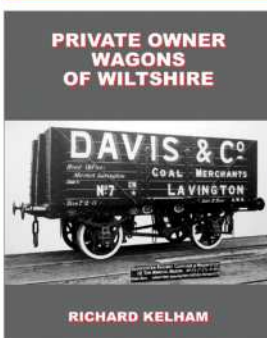
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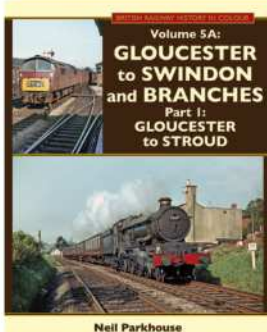
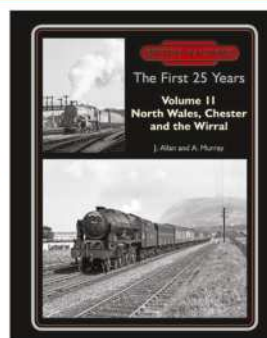
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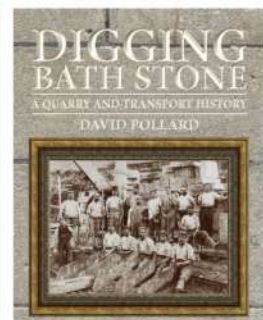
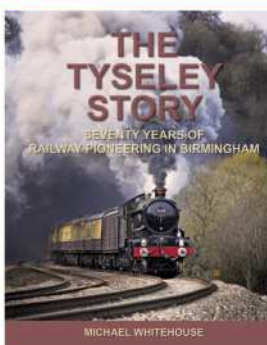
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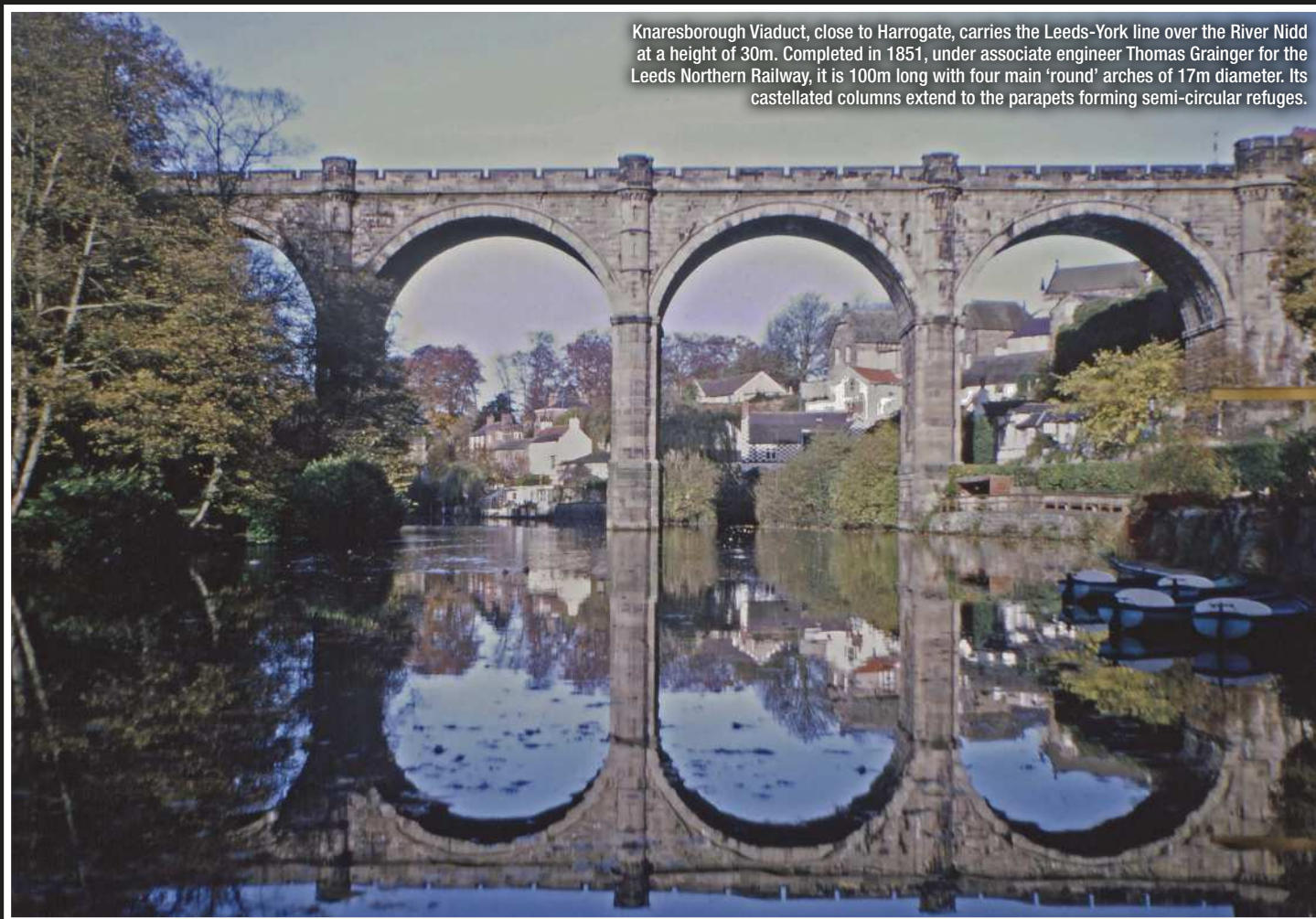
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THE RAILWAY VIADUCT

Arguably the most impressive of railway features and of many different styles, these raised structures make equally imposing models, as Clive Baker shares.

Words & photography: Clive Baker

Knaresborough Viaduct, close to Harrogate, carries the Leeds-York line over the River Nidd at a height of 30m. Completed in 1851, under associate engineer Thomas Grainger for the Leeds Northern Railway, it is 100m long with four main 'round' arches of 17m diameter. Its castellated columns extend to the parapets forming semi-circular refuges.



The word 'viaduct' is derived from the Latin – 'via' meaning 'way' or 'road' and the latter syllable from that of the word 'aqueduct'. The Oxford English Dictionary refers to 'a long bridge-like structure, especially a series of arches, carrying a road or railway across a valley or dip in the ground'.

In selecting suitable examples, I have stretched the dictionary definition a little to include some variants performing the same purpose. The diagram on the next page

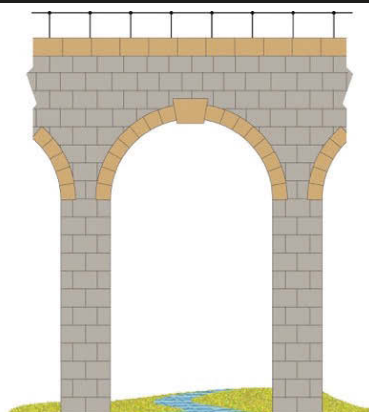
demonstrates the various styles discussed in this article.

When hearing the word, my mind immediately turns to the iconic form of 'Headstone Viaduct' at Monsal Dale in Derbyshire; erected by the Midland Railway (MR) in 1863, it once carried that company's line between Derby and Manchester, crossing the River Wye at a height of 21m.

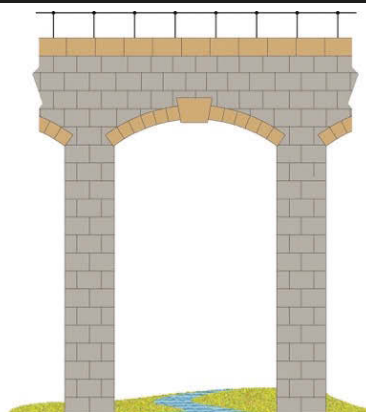
In 1951, British Railways (BR) considered this viaduct to be suitable promotional

material as the subject for a poster intended to whet the appetites of would-be visitors to the Derbyshire Peak District; engaging the skills of artist Peter Collins ARCA, prints of the resulting artwork are still available, and one cannot help thinking that this massive piece of civil construction, built from local limestone, enhances its natural surroundings rather than being a 'blot on the landscape'.

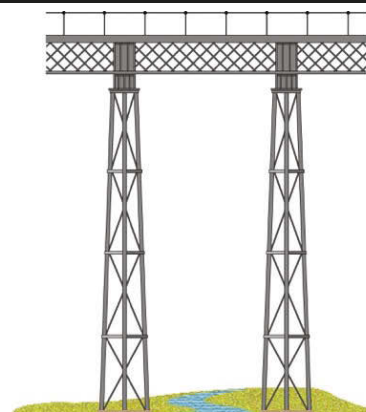
The castellated columns of Knaresborough Viaduct, crossing the River



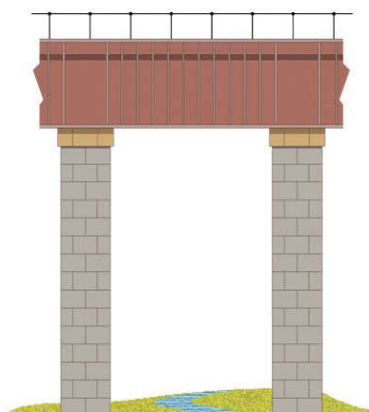
ROUND ARCH



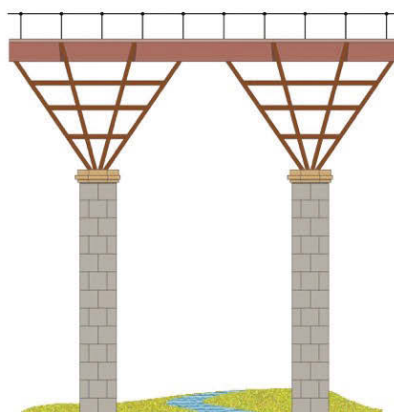
SEGMENTAL ARCH



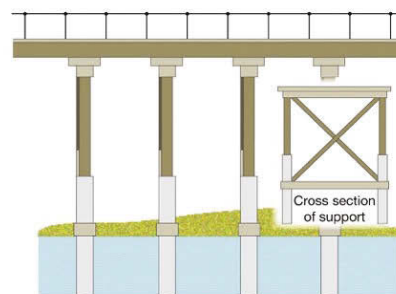
WROUGHT IRON LATTICE



STEEL GIRDER
supported by stone piers



BRUNEL TIMBER VIADUCT
supported by stone piers



TIMBER VIADUCT
lower parts of uprights encased in concrete

Styles of arch referenced in this article

Nidd, situated on the Leeds-York line close to Harrogate, were also considered poster material when BR commissioned artist Jack Merriot to produce the feature-painting to attract passengers to Yorkshire.

Paintings depicting the 24 arches that comprise the iconic Ribbleshead, or otherwise known as 'Batty Moss' Viaduct, were commissioned to tempt the public to visit the Yorkshire Dales and Cumbria. Various scenes of this Victorian masterpiece of engineering have been used by more than one agency, not necessarily all railway operators.

Viaducts also form part of urban settings – the approaches to Birmingham's New Street and Moor Street stations for instance, but the famous artist L. S. Lowry was clearly inspired by the brick arches that dominate the Stockport skyline. Considered one of the largest brick-built structures in Britain, this particular viaduct, erected in 1840 for the Birmingham & Manchester Railway to cross the River Mersey, is understood to have been the inspiration for a number of Lowry's works.

Rumour has it, artists of a different genre have taken inspiration from railway



Pont du Gard, originally constructed by the Romans to carry an artificial water course across the Gardon River in France, remains a World Heritage Site. Its three tiers of arches, 273m in length, rise to a height of 48m.

viaducts. The entertaining duo Flanagan and Allen, while appearing in a Derby theatre, took a walk one afternoon close to the viaduct that once carried the Great Northern Railway through that area of the town. Taking shelter from a rainstorm, beneath the viaduct, they wrote the old-time favourite *Underneath the arches*.

Roman times

As already mentioned, the latter syllable of the word is derived from 'aqueduct', the railway variant being a development of spans supporting waterways. Such structures can be traced back to Roman times. Pont du Gard crossing the Gardon River is the most visited heritage site in France. Close to Avignon, this vast structure remains today as a testament to those much-travelled engineers. Built as an aqueduct with three tiers of arches, it remained as such until the 6th century, before becoming a tollgate during the Middle Ages, then finally a road during the 18th and 19th centuries.

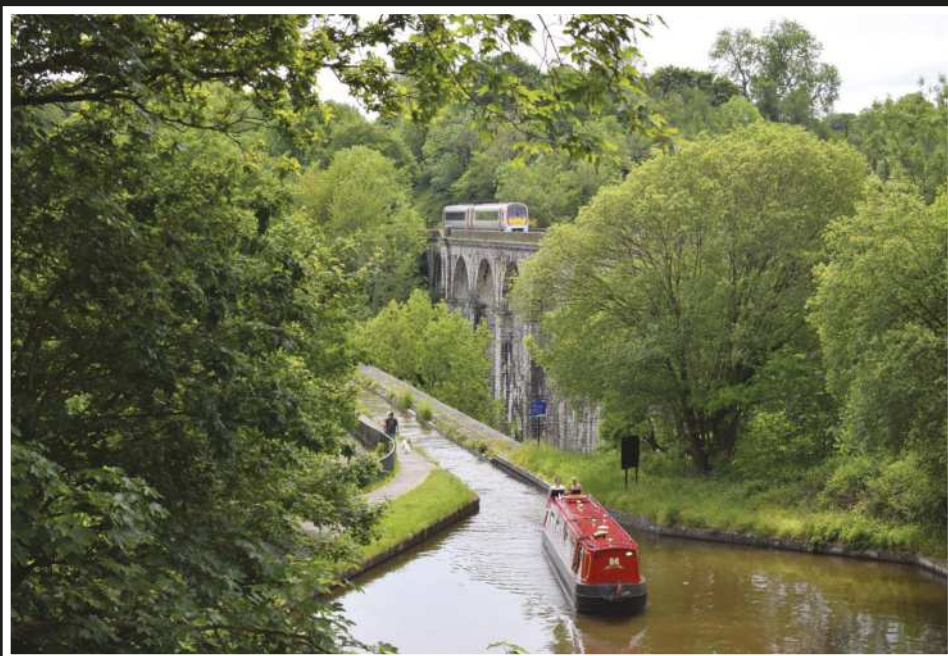
Victorian Heritage

Sankey Viaduct is considered to be the first in the world to carry a railway. Engineered by George Stephenson and opened as part of the route between Liverpool and Manchester in 1830, this structure is Grade I listed and remains in use on the now busy, electrified commuter route linking the two cities.

The building of viaducts during the Victorian railway age was not a new science, the canal engineers having created aqueducts based on the Roman practices almost a century before the railway age. The architecture of Sankey viaduct follows, to a great extent, that of aqueducts.

Crossing the River Ceiriog at Chirk are examples of both aqueduct and viaduct – two structures, standing side by side linking England with Wales. Engineered by William Jessop and Thomas Telford, building of the aqueduct commenced in 1796 with completion in 1801. Consisting of stone 'round' arches, the aqueduct still conveys narrowboats on the Llangollen arm of the Shropshire Union Canal at a height of 21m above the valley floor.

Parallel to the aqueduct, but having a greater height of 30.5m, the original viaduct conceived by Scottish engineer, Henry Robertson with contractor Thomas Brassey, was erected between 1846-48 for the Shrewsbury and Chester Railway, but replaced just 10 years later by a new



As a narrow boat enters the pound having traversed Chirk Aqueduct, passing from England into Wales and about to enter Chirk Tunnel, a Class 170, two-car DMU crosses the viaduct heading towards Shrewsbury.



Drakelow Viaduct carries the ex-MR line from Burton upon Trent to Leicester across the River Trent; opened in 1849, the five 'segmental' arches featured in this image are just one of three portions that make-up an approximate ¾ mile stretch that crosses the wide, shallow flood plain. To the extreme left of this image is one of four pill boxes added to the viaduct as a defence against invasion in 1940. Short in height, this stone-built viaduct bears the hallmarks of MR construction. The geometry of a segmental arch is often favoured when designing a shallower structure as a greater headroom can be achieved within the width of the arch.

Once serving the South Derbyshire collieries and Drakelow power station, traffic along this line is presently sparse, but the route does survive and remains in double track formation. This image captured in 2011 features new London Transport rolling stock built at the Derby-based Bombardier plant, the stock is being transferred to Old Dalby testing facility. Motive power features four Class 20 locomotives, top and tailed, together with barrier wagons.

structure. The surviving build has a total of 16 stone 'round' arches, three extra spans at each end replacing former timber units.

At a later date, the Shrewsbury and Chester Railway, built principally to convey coal from the collieries at Ruabon and Wrexham, merged with the Great Western Railway. Today, the viaduct is traversed by a regular service, operated by 'Cross Country', between Birmingham International and

Holyhead.

Not all the viaducts featured in this article are built of stone, brick or concrete. Some, as in the case of Bennerley Viaduct, are constructed of wrought iron components. Erected by the Great Northern Railway linking Derbyshire with Nottinghamshire, close to Ilkeston, it is now preserved and Grade II listed. The make-up of others such as Wyllins Viaduct on the Isle of Man is a



Folkestone Harbour Viaduct presents a contrast to other examples, crossing the inner harbour and once linking the town with an interchange facility for cross-Channel ferries. Promoted by three directors of the South Eastern Railway Company (SER) and completed in 1844, its chief engineer William Cubitt had also been responsible for the design of Foord Valley Viaduct on the SER's main line. Consisting of 13 red brick 'segmental' arches with solid centre section, the viaduct initially served to carry coal trains to the jetty. Once a rail connection had been established at Boulogne, international rail/marine services were able to commence from 1849, only to be displaced as the most popular route to France by that of the Dover-Calais route in 1882. The harbour crossing also includes a swing bridge; to enable the use of heavier trains, the bridge was replaced in 1893 and again, in 1930 designed by George Ellson, chief engineer of the Southern Railway. In 2012, following a decline in cross-channel traffic, Network Rail designated the harbour branch 'permanently out of use', however, the viaduct arches were repaired in 2017 to facilitate a walkway accessing refurbished leisure/museum amenities at the one-time harbour station.

combination of stone and steel.

Unlike Monsal Dale and Bennerley viaducts, others remain in regular use and have a timeless quality. Reaching back to the Victorian railway age, viaducts have, with regular maintenance, remained a part of the infrastructure, their life spanning all eras of rail traffic from *Rocket* to 'Pendolinos'.

Elizabethan times

Building railways that passed through scenic areas necessitated a larger number of tunnels and viaducts. Apart from branch-lines in the wake of the Beeching Report, some main-lines also succumbed to closure, in particular the MR Derby-Manchester route in 1968. At that time, another ex-MR main line was under threat of closure; renowned for passing through areas of outstanding natural beauty, claims were made that due to the high cost of maintaining the large number of tunnels and 22 viaducts, the Settle and Carlisle (S&C) line should close.

During the 1950s, a device was designed to enable easier inspection of viaducts and bridges. Mounted on a flat railway wagon,

it resembles an inverted hydraulic crane with a self-levelling platform, and enabled inspectors to examine closely the condition of the under-arch masonry. At the time of its introduction, I recall the equipment being featured on the BBC Children's Hour programme 'Railway Roundabout'.

Passing over Ribbleshead Viaduct, now a Grade II listed building, must be the highlight of a trip on the Settle and Carlisle line, but one reason given for closing the line was the questionable stability of the structure following a century of use. Apart from other remedial work, singling of the section of track on the viaduct proved to be one solution and, following a long campaign, this fine line with its numerous viaducts remains open, particularly to satisfy the growing needs of the tourist industry as well as important Anglo-Scottish services.

During 2021, this iconic viaduct will be receiving attention that should ensure its future for many years to come. Publications show images of scaffold erected around the piers to enable engineers to carry out repairs to brickwork, drainage, pipework and the

removal of vegetation.

When the London North Western Railway Company laid its line from Ashbourne to Buxton, the northern section opening in 1894, the work involved building two viaducts. Spring Gardens Viaduct virtually cuts the town in half; with a height of 27m, it dwarfs adjacent buildings. To the south of the spa town that sees more snow per year than most others in England, and on the same line, is Dukes Drive Viaduct. Although singled following closure of the Ashbourne route to passenger workings in 1954, the line to the south remains open as far as Hindlow for quarry traffic. A straight viaduct of 13 arches, two enthusiasts selected this location to photograph the final steam working to Hindlow in March 1968.

From south to north

In the south-west, linking Devon with Cornwall, Calstock Viaduct stands sentinel over its namesake village carrying the Plymouth to Gunnislake branch over the River Tamar. Instead of the builders using local stone, the viaduct consists of concrete

blocks cast on site.

The viaduct closest to France must be the brick-arched structure carrying the Folkstone Harbour Branch linking the town with the cross-channel ferry terminal. In steam days, the sight of a boat train being hauled across the tidal harbour by as many as three ex-South Eastern Railway 'R1' Class 0-6-0T locomotives, with a fourth banking from the rear, would be a regular event. In later years, as the Anglo-French traffic decreased, excursions hauled by heritage motive power could be witnessed before closure in 2012.

Crossing the Mauddach estuary on the ex-Cambrian Railway line, the traveller can observe some of the finest coastal views that Wales has to offer, as the train traverses Barmouth Bridge. Generally described as a bridge, the word viaduct would be more appropriate. Completed in 1867, it is considered the only major timber-built bridge remaining in use. Although closed 1980-86 while work to eliminate damage to the piers caused by marine worms, a major restoration programme to secure its future is presently underway.

Tall viaducts are no strangers to the mill towns of Lancashire and Yorkshire, the steep valleys creating fast-flowing rivers proving to be ideal prime-movers for the mill machinery. I recall observing some of the final steam workings in 1968 – double-headed Standard class 5s hauling excursions across the 22 arches of Saddleworth Viaduct

and, on another occasion, a similar working passing over George Stephenson's 13-arch Lydgate Viaduct. Over a decade later, when steam power returned to the main line, the graceful 31 'round' arches at Crimble Valley set the scene for some special heritage workings.

Railways arrived in the northern area of Scotland some years after the net had been cast in the more populous southern regions. Laying tracks through the more demanding terrain required a high proportion of tunnels and viaducts; mainly of single-track formation, curvature tended to be more severe.

A spectacular example is Culloden or 'Nairn' Viaduct, encountered shortly before a traveller's journey from the south ends at the former Highland Railway headquarters at Inverness.

Continuing a journey further on the Far North of Scotland line, Oykel Viaduct, which crosses the Kyle of Sutherland is encountered to the south of Invershin station. To the south of the viaduct is Culrain station, originally opened as a request halt. The distance between the two settlements is considered one of the shortest on the UK main line system.

One cannot ignore one infamous viaduct situated on the West Highland line between Fort William and Mallaig. Disregarding its association with wizardry literature, the structure of Glenfinnan Viaduct is unusual as, due to the unworkable nature of the local

stone, contractors Robert McAlpine & Sons opted to use cast concrete. Opened in 1901, the 21 spans form a graceful curve through the spectacular landscape surrounding the River Finnan.

Narrow gauge to the west and offshore

Narrow gauge railways or tramways, often built to a tight budget, are not always associated with major civil construction such as a viaduct, but two examples could be found on the Isle of Man carrying the ex-Manx Northern Railway, 3ft gauge line, over two ravines between St. John's and Ramsey. Glen Wyllin Viaduct, like its counterpart at Glen Mooar, comprises tapering stone piers topped with steel spans. However, the two structures differ. Glen Wyllin's steelwork was solid while that of Glen Mooar, lattice.

I have left my most travelled-over viaduct till last. The two centre piers of Dolgoch rise majestically above the ravine, the structure surviving in its original condition carrying heritage trains along the 2ft 3in gauge line operated by the Talylllyn Railway Preservation Society.

Model interpretation and shallow valleys

Having enjoyed railway modelling from an early age, layouts featuring a viaduct have always impressed me to the extent that I have made two attempts to capture the spectacle of a train elevated above the



ground. Inspiration has come from two layouts, in particular.

Walkham Viaduct, originally consisting of 16 timber spans mounted on masonry pillars, was constructed for the South Devon Railway's Tavistock branch in 1859. Engineered by I. K. Brunel, individual timbers, part of a structure mounted at the tops of the piers supporting the decking, could be replaced without the need to close the line.

Although the timber spans of this ground-breaking feat of engineering were replaced by steel units in 1910 and the whole structure demolished in 1965, Walkham Viaduct remains immortalised in 4mm scale at Pendon Museum. Witnessing this scenic masterpiece with a Great Western train traversing the viaduct, backed by a vista of Dartmoor can do non-other than inspire a modeller to go home and do likewise.

At Monsal Dale (Headstone) and Miller's Dale, both viaducts on the same route a short distance apart, featured on a permanently housed O gauge layout at the Matlock Bath premises of the well-known model suppliers, Slaters. Available for public viewing from 1977 until the company's move to Darley Dale, the models could be compared to the prototype viaducts just a few miles up the road. Set in the pre-grouping era, I would delight in watching replicas of Midland Railway Johnson and Deeley classes hauling trains through the

spectacular Derbyshire scenery, while stocking up with Plastikard and other modelling products.

'The World's End', in more recent times, is a superb model based on the castellated architecture of Knaresborough Viaduct and must have inspired many modellers.

When attempting to include a viaduct in one of my own layouts, I have noted that the change in baseboard levels and the depth of a backcloth can present issues, particularly if modelling the dramatic landscape of Derbyshire or Yorkshire dales, Welsh valleys or Scottish glens where frequently a viaduct is situated adjacent to a tunnel mouth. For instance, if modelling Headstone Viaduct, in 4mm:1ft scale, a height of at least 30cm would be required from baseboard to track level without adding a backcloth. I have often considered a model in 2mm:1ft scale to be a more successful solution to achieve the effect of a train passing through a landscape, after bursting from a tunnel and traversing a viaduct.

Opting for a location in the shallow valley of the River Trent enabled me to construct a hopefully realistic-looking model in the available space. The selected prototype at Drakelow, sited on the ex-MR, Burton upon Trent to Leicester route, suited the criteria.

When modelling a specific viaduct, scratch-building would be the necessary option, but for a generic masonry build, card kits produced in both 4mm:1ft and 2mm:1ft scales are available from suppliers

such as Metcalfe, and moulded examples from Hornby, Ratio, Kibri and Wills. To model a wrought iron structure such as Bennerley would be demanding, involving much repetition, but laser-cut components could offer a solution or the modeller turn to 3D printing.

The archways of some viaducts have been frequently utilised as business premises for automotive engineers, or simply storage units. This usage always adds interest.

In conclusion

Viaducts seem to be the one railway feature to survive with minimal change since their construction in the days of railway mania. Overhead catenary has been added to some; an example being Stockport, and one cannot help wondering how L. S. Lowry's brush would have been manipulated to depict such modifications.

Some have become redundant and their function changed to suit today's leisure needs by converting it to a footpath or cycleway. Others vanished from the landscape, heavy explosives often the only means to remove such well-built pieces of civil engineering.

One example on the Talylyn Railway has become readily identifiable, and an impression of it makes up that railway's logo; appearing on every piece of marketing produced by the, apart from the railway's historical locomotives and rolling stock, the viaduct is the icon of the line.

Bennerley Viaduct, erected in 1878 to carry the Great Northern Railway route between Derby and Nottingham, crossed the River Erewash, Erewash canal and the Midland Railway 'Erewash Valley' line, close to Ilkeston; linking the counties of the two aforementioned cities, the viaduct had a length of over a quarter of a mile and with a height 17m, remains the longest wrought iron viaduct in the country. At the planning stage, the use of masonry arches was considered unsuitable due to the effect of underground mine working in the area. It is likely that wrought iron became the choice of material to showcase the local iron industry. Following closure of the line in 1968, it has been preserved as a Grade II listed structure. With voluntary support from the 'Friends of Bennerley Viaduct' it is due to become a public walkway with access ramps at each end.



HOW TO... BUILD A VIADUCT

Words & photography: Jeff Grainger

Experienced layout builder, Jeff Grainger, shares his intricate construction of this mammoth structure, based on a Settle and Carlisle prototype.



Ask any rail enthusiast to name the features that characterise the Settle and Carlisle (S&C) and you'll likely find stunning natural scenery and man-made structures such as viaducts and stone walling high on the list. Being a Cumbrian, keen fell walker, and with my background in physics, I find all of these very appealing. Given the chance, I set out to model key elements of the line.

My previous two 'Kirkby Luneside' layouts were my initial attempt to capture the glory of the S&C in model form. Both these layouts comprised a station and a scenic section, the emphasis of the latter being a large fell area, giving plenty of opportunities to incorporate dry-stone walling, a viaduct and the obligatory sheep!

The ongoing, current layout – Gill Head – is my latest S&C project, this time, with the central feature being, not a fell, but a viaduct. I can only hope that the end result will do justice to a line that fires the imagination of all who've seen it.



One is spoilt for choice with S&C prototype viaducts to model, the only constraint with many is the sheer size of the things! My favourite has always been the beautiful Arten Gill viaduct in Dentdale, 11 arches and 220yds in length, with the road deck nearly 120ft (36m) above the gill running through its centre. A full, 11-arch model of this structure in 4mm:1ft scale might not be possible, given constraints of space, but my aim was to build as much as I could, with all the relevant landscape – gill, hillside, walls and vegetation. CREDIT: Robert Higgins



2

A much lower 'L'-girder table was built to support the viaduct, with the top of the cross-joists around 55cm above the ground. The viaduct, when built, was going to come in at around 25kg, and with at least this weight again of plaster on the viaduct valley, relatively sizeable timbers were used: mostly 3in x 2in (69mm x 44mm).

The timber was CLS, left-over from a garage conversion. The radii for the track crossing the viaduct were set at around 100ins, and pencil and string were used to mark out the 12mm ply used for the base and road deck. The ply is 11cm wide, with the distance along the larger (east) edge being about 7ft (220cm).

The Anderson and Fox book was used to work out the positions of the centres of each pier, both on the inner (west) and outer (east) faces. The core of each pier was a piece of birch ply, 45cm high, 11cm wide and 12mm thick. The two King piers are substantially wider, and these received thicker formers. The base, road deck and pier formers were glued and screwed.

Several front-on images of the viaduct were measured on-screen and used to draw-up the shape of the piers, west and east onto 3mm birch ply sheets. These were then cut out and glued and screwed onto the 12mm cores. 3mm was used because it is thin enough to flex round the viaduct curve and easy to cut with a craft knife.

Piers one, two (King), three, four, five (king), six and nine were modelled, which, along with the end abutments allows for eight arches.



3

The distance between the inside pier faces on a curved viaduct must remain constant, so the 4mm ply formers are wider on the rear (east) face than on the west. To ensure consistency of width, and to add extra stability, I added plaster filler to the inside pier faces. This was rubbed down when dry before any stones were added.



4

At this point, I had the 'skeleton' of my viaduct and needed to decide how to apply the stone fascia to the piers. For the viaducts used on Kirkby Laneside, I'd used Slaters 7mm dressed stone sheets, but the intricacy of detail on the new construction – and the number of Plastikard sheets it would take – meant that a different approach was needed.

To the general merriment of followers of the Gill Head layout – and cries of insanity – I decided to cover the piers in Das-built stones. Individually fitted, brick by brick...!



5

Das air-dry clay is a wonderful modelling material, readily available in 1kg packs. I use white clay, but for this job, the colour was irrelevant. A portion of Das, rolled into a sausage shape between the fingers is easily forced into shape by moulding it in a simple Plastikard jig. The jig has a rectangular cross-section and can be built to the width and depth required and the resulting dried length – it takes around a day to set – can be cut to length to produce stones/bricks for the fascia.

A study of the stonework in the main body of the piers showed a predominance of blocks around the 4ft x 2ft and 3ft x 2ft sizes, with a fair number of 1ft thick slabs, of varying lengths. The first two scale to 16mm x 8mm and 12mm x 8mm for the Das stones. These stones were obtained by cutting the 8mm wide strips, moulded in the jig, to the correct lengths.

The pier stonework also shows a pronounced variation in its surface – it is far from smooth. To model this, about 60% of the Das stones were made 4mm thick, around 35% were 3mm thick with a few other variations 'thrown in'. When applied, the thickness variation adds a pleasing look to the pier fascias, especially when lit by a low light.



6

The pier height, ground to start of arch ring, was about 80% of the total viaduct height, or around 100ft. Accounting for small gaps between each course, this meant around 45 stone courses per pier – but practically, this was less for most piers as a fair part of their length would be underground.

Individual Das stones were fixed to the ply using PVA glue. Care had to be taken to keep the stonework in line across adjacent piers. Initially, I marked a series of lines, 4mm apart, onto the ply, but after a few hundred bricks the constant width of the 4mm wide Das stones kept things in order. A small level was used to check each layer of bricks as matters proceeded. This got very messy at times, with the level – and my hands – coated in PVA!

One other constraint was the need to quoin the edges of each layer. The stone blocks on all edges are interlaced in a regular pattern – a bit of a pain to start with, but a job that rapidly became familiar.

7



Aside from sometimes being tedious, fixing thousands of Das stones onto the piers was a relatively easy task. The more complicated aspects of the build started when arch ring and arch liner level was reached.

It would have been an easy option – and in my view a total cop-out – to fit plain arch liners, using Plastikard bent to shape and painted grey, perhaps with a stone pattern drawn onto them. However, given the investment in time with the brickwork, and the fact that all the liners would be in full view, I decided to go the whole hog and continue with the Das brick's theme.

A thin ply jig was built for each of the eight arch liners and 0.060in Plastikard was fitted, curved to the correct shape.

8



2mm thick Das stones were then glued to the Plastikard using PVA to produce something a bit more realistic looking, and in keeping with the piers.

The construct was then screwed directly to the road deck to hold it firmly in the correct place.

9



For the arch rings, I compromised a little. A typical Arden Gill ring has about 60 'voussoirs', but I settled for around 45 – for ease of fixing. Again, Das stones were moulded to the desired size and PVA-glued onto a backing of 0.020in Plastikard, previously cut to the correct curve. The rings were then set in place. Patience was required to building 16 of these, but needs must.

10



In many ways, the spandrel area above the arch rings was the most fiddly task in the construction as the stonework is different here, with much smaller blocks, more akin to large bricks. 2mm thick Das strip was used to manufacture around 4000 small stones, typically 8mm x 3mm, interspersed with larger blocks, as per the prototype. Workbench comfort was facilitated by building each spandrel area onto a 0.020in Plastikard backing. Bricks were glued with PVA onto the plastic, and, once set, the plastic was stuck to the viaduct with Evo-stik.

11



With all the spandrels in place, a layer of wider coping stones was set on top of the parapet – about 480mm above base level. Phew! After three months and around 19,000 stones (no, I didn't count them individually!), the stonework was complete and it was time to paint it.

12



I must have studied dozens of photographs of the prototype – especially to try to get a representation of the colour of the stonework. Of course, much depends on the lighting, angle of the sun, or cloud cover, so I erred on the side of caution and didn't overdo the grey-ish tone of the stones. I prefer to use acrylic paint and a grey mix was made up from Woodland Scenics Stone Grey, Slate Grey and Burnt Umber, along with some lighter Dove Grey. The paint was probably mixed in a dilution of at least 20 parts water to one part paint mix. Trials were carried out over a number of days to try to match the dry grey colour to a number of Arden Gill photographs. When I was satisfied with the colour, 'washing' began. The surfaces of the Das stones on the viaduct were liberally smeared with PVA as a result of the construction process. To aid with paint uptake, the stones were very gently rubbed down with fine emery paper. This produced a lot of dust and took a number of hours, but it probably helped the end result.

A dilute wash of white emulsion (10 water: 1 paint) was applied to the stones as a base coat and allowed to dry. Over three days, 15 washes of the dilute grey mix were painted onto the structure, allowing a few hours to dry in between each wash. I had no pre-set target for the number of washes, but stopped when I thought the grey was dark enough – again, judged against images of the prototype.

When the final wash was dry, two white pointing washes were run over the brickwork. The viaduct was now ready to take its allotted place on the layout!

For further details, visit Jeff's thread on RMweb. Search for:

Gill Head:
Kirkby
Luneside's neighbour

13



The double-thick base of the viaduct was screwed to the 'L'-girder table, matching the height of the road deck to the surrounding baseboard tops. A layer of 4mm cork sheet was glued to the viaduct road deck and SMP finescale track fixed on top. Once set, with soldered electrical connections in place, a test vehicle made the first powered traverse of the structure. I've thoroughly enjoyed building my representation of Arden Gill. My only regret is that I only had space for eight arches, not the full 11. Scratch-building is fun!

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Layouts with viaducts

Viaducts, by their very nature, are large in the real world and are some of the grandest successes of the Victorians. On layouts, they're impressive, but they can take a lot of planning, skill and materials to produce; especially when scratch-built.

Andy York selects some of the finest we've featured in BRM.



WALKHAM VIADUCT – PENDON MUSEUM'S DARTMOOR SCENE

Guy William's 13ft long model of Brunel's Walkham Viaduct, which was near Tavistock in Devon, forms the centrepiece of the museum's Dartmoor scene. Formed from over 2,500 pieces, the model was transported to its home in suitcases more than 65 years ago. See more of the Dartmoor scene in BRM December 2012.



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'SCORBITON' – KENDAL MRC

The most common purpose of a viaduct is to span a valley and river. Kendal MRC's layout features an impressive stone-built river crossing reminiscent of the Dee or Severn. Behind the main running lines are more recent river crossings, a feature often found where traffic capacity increased beyond the original railway. See more of 'Scorbiton' in BRM February 2019.



'TETLEYS MILLS' – DAVE SHAKESPEARE

Life in a Northern Town. Many layout viaducts are signature pieces in rural surroundings, but there were also many substantial viaducts in urban settings. The impressive span on 'Tetleys Mills' by the formidable, late Dave Shakespeare is one fantastic example. See more of 'Tetleys Mills' in BRM January 2011.



'BLUEBALL SUMMIT' – ANDREW BARTLETT

There's little in this picture that indicates the model is N gauge, but the gentle curves of the viaduct arching way above the surrounding village buildings make a pleasing showcase for the procession of trains running through the South Devon scene. See more of 'Blueball Summit' in BRM March 2020.



'DENTDALE' – WAYNE WEBB

The Settle and Carlisle route gives the railway enthusiast some of the finest viaducts, as shown in Jeff Grainger's article about building Arten Gill in this issue, which is also represented in this impressive centerpiece of the N gauge layout built by Wayne Webb, Bob Taylor and Tony Frazer. See more of 'Dentdale' in BRM April 2014.



'BEWDLEY' – GRAHAM HEALD

Wribbenhall Viaduct to the north of Bewdley station on the Severn Valley Railway is still enjoyed by thousands each year with its panoramic view of the town and valley. Graham's model is on a more pronounced curve, which makes the geometry and building of the stone structure even more impressive. See more of 'Bewdley' in BRM November 2019.



'BLACK COUNTRY BLUES' – STAFFORDSHIRE FINESCALE GROUP

Not all viaducts are huge structures, but even the five-arch representation of Thomas Brassey's Penkridge viaduct on 'Black Country Blues' still took care of a few feet of layout space. Close attention was given to the dimensions, structure, finish and detail of the original and the model – an elegant combination of stone Staffordshire Blue engineering bricks. See more of 'Black Country Blues' in BRM October 2012.



'THE WORLDS END' – PETE GOSS

There are few finer architectural modellers than Pete Goss, and his model of Knaresborough Viaduct spanning the River Nidd is probably one of the finest examples of his work. Taking 18 months to build, the model accurately captures the Gothic decoration and detail of the original. It's a beautiful view from the train, but you don't get the reward of seeing the viaduct even though Pevsner was damning of its architecture to match the nearby castle. See more of 'The Worlds End' in BRM December 2017.



A Class 142026 'Pacer' crosses
paths with 37 411 *Ty Hafan*.

THE WORLDS END

With its impressive viaduct centrepiece, realistic buildings, raging civil war cameo and contemporary setting, Peter Goss' layout is packed full of amazing details, and the closer you look the more there is to see.



the viaduct could be a model by itself at five feet long, and if a small model railway of Knaresborough were built at home, the viaduct would almost certainly be omitted due to space and height restrictions

This, my third model railway project, The Worlds End, visits North Yorkshire and the town of Knaresborough at the turn of the twenty first century. The model trains featured represent those running through the station over the last 15 years.

The layout developed from around 2008 along the lines of copying an everyday scene with perhaps an historical leaning. My original thoughts were for a simple design that hopefully hadn't been done before. I wanted to imagine the whole thing from the outset before breaking it down into smaller bite-sized components. It was to be an architecturally interesting arrangement with a high standard of model buildings.

Martin Welch's book, 'The Art of Weathering' and 'Architectural Modelling' by Dave Rowe inspired my painting and modelling techniques, which I then adapted. However, it was an earlier visit to a model railway show (probably York) that set me off down the model railway path in the first place.

Modelling considerations

I believe most railway modellers would not entertain Knaresborough as a viable modelling subject, as it has limited operating potential, particularly for the shunting and goods train enthusiast. The terrain, if built to scale, could possibly be too demanding and time-consuming to construct, and the viaduct could be a model by itself at five feet long.

These negatives were completely opposite to my positive thoughts, and only reinforced my idea for a comprehensively detailed architectural model of a market townscape with a railway running through it.

My model of the Knaresborough viaduct to scale could extend over one complete specially extended and dropped baseboard. A quick estimation based on street maps showed the viaduct and station to be around nine feet long (684 real feet in length).

Ground level differentials could be anywhere up to at least a scale 2 feet high which represents 152 real feet height. The viaduct is 90 odd feet high to start with.

The egg shape of the layout came about by default. I let the building arrangement design dictate the layout shape, rather than fitting to a prescriptive regular arrangement. The narrow town scene on the left, the deep river gorge and viaduct centered and the wide wooded countryside and castle to the right making a balanced visual contrast.

Building arrangement

Several buildings were drawn up on the computer, with street scenes assembled and layout plans developed. Train sets were listed, time-lined and discussed with my railway enthusiast friend Stephen before construction commenced.

The model buildings included a row taken from Finkle Street, which were mirrored to fit behind the station. Other buildings were chosen from further into the town, including the Blind Jack pub cluster of three large buildings and a corner building from Market Place, the single storey small shop over the tunnel mouth and buildings from High Street, the Raw Gap building cluster, the small timber-framed rented building at the base of viaduct and Mother Shipton's office building.

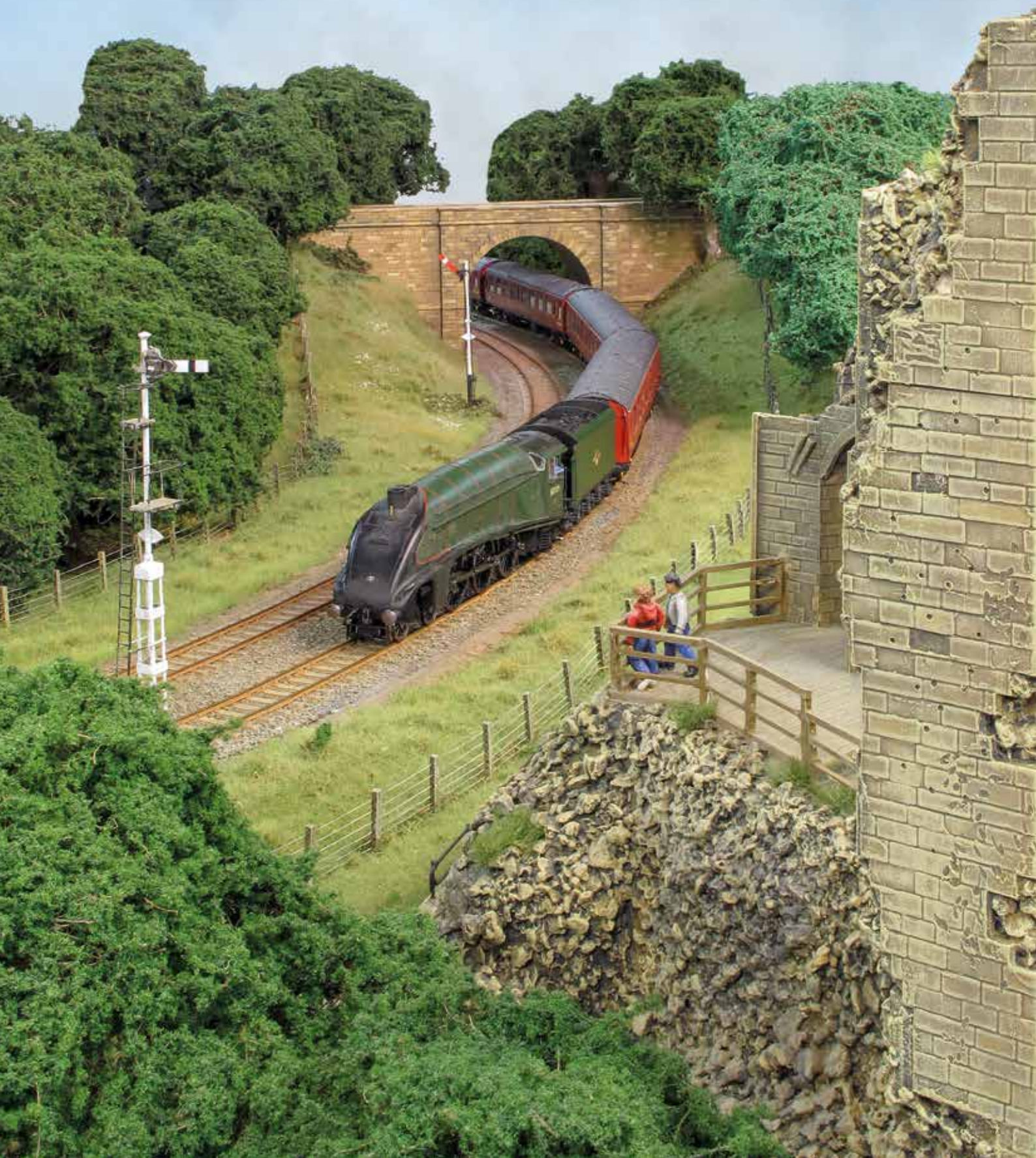
Paint what you see – not what you think you see

I believe in copying real life wherever possible, so steps can be taken to enhance detailing just by looking. Brickwork can be carefully painted and detailed following colour photographs. An illusion I use is to paint false shadows at door and window reveals, using Humbrol 67 tank grey before fitting any windows. I got this idea after looking at photographs and realising that colour is not always what it seems – for instance, dirt is not black unless it is coal. As such, I dry brush dirt or wash as darker

Factfile

Layout name: The Worlds End
Scale/gauge: 4mm/OO
Size: 19ft x 11ft operating area
Era/region: Modern, North Yorkshire
Layout type: Continuous run
Words: Peter Goss
Photography: Andy York

4-6-2 A4 No. 60009 *Union of South Africa* speeds past the ruins of Knaresborough Castle - just one among many grand sights.



shades of grey very subtly. For The Worlds End and the previous 'Rowlands Castle' and 'Etton' layouts I kept to lighter tones on purpose. Comments have often been made at shows on how light my modelling is.

Buildings crispness

Keeping the Plastikard and card-based building structures crisp is very important – keep those corners tight and at right angles with no gaps please. After constructing a building corner solidly from inside, lightly scrape the external corner carefully with a sharp knife or fine emery paper held on a little wood block. This technique blends the corner into one sharp edge before maybe applying a little plastic weld with a paint brush to seal the corner externally ready for painting.

Gaps at the bottom of a building is also a big one for me. After gluing a building into

place, fill any remaining gaps with a little PVA and apply some litter or ground cover. Alternatively, I sometimes bury the building into a small ground recess. This helps hide any gaps in the first place.

Camera details

Take close-up pictures of your model and look at the details on screen. It needs to be

clean. No matter how well it is painted, if the modelling is not square and tidy it will never look right as untidy right angles and corners tend to jump out. Taking time to carefully construct and detail items before painting pays dividends. This applies to rolling stock as well as buildings. In these days of high resolution, cameras make pictures three or four times larger than life, which doesn't

all my trains are full of passengers, even if they are all sat on one side of the train. Locomotives all have drivers as well. Using model humans brings to life any scene, without a doubt



Even in model form, this pub garden really does look good enough to drink in.

help. Using magnifiers to construct and paint means that when viewed normally the models can look much tidier.

Highlighting

Take a look at the clothes someone nearby is wearing. The shirt or jacket will have a base colour, but look again. The crevices are darker and the parts facing the sun are much lighter. Re-creating this effect is a huge factor and, once grasped, can be applied to buildings, wagons and anything with raised details. The viaduct parapet wall stonework, for example. Painted in various stone shades from light stone to dark grey, the parapet coping stone is then edged with a small brush using a light cream colour. This is not noticed initially but when pointed out becomes obvious.

Viaduct

The viaduct took around 18 months to construct. It has solid wood uprights with a chipboard track deck that was then clad in double thickness mount board with plain

Plastikard 'stones' constructed as complete sub-models that fitted together around the solid timber. The underside of the arches was the last thing to build. Painting was carried out at sub-assembly stage before filling gaps and paint blending on completion.

River Nidd

I used acrylic water effect paste from Green Scenes, which is brushed on to an acetate sheet, then swept into ripples with a wooden coffee stir stick. The paste itself dries clear and glossy. The underside of the acetate was painted a green/blue colour to start with before being stuck down with acrylic grab adhesive. The boats and other details were glued down with before the water ripples arrived.

Figures

I couldn't find suitable modern-style figures for the layout, so I consulted with a wargames equipment designer and we decided to start our own range. The figures used are from our own range of 'Pete Goss

Figures'. All were cleaned up of any flash and casting pegs. The bottoms of the feet were filed flat so that the figures would almost stand up on their own. I drilled and superglued short lengths of 0.5mm brass wire to one foot, or if legs were too fine, to the bottom of a coat flush with a leg. This is used for locating the figure in a wood block while the paint is drying and acts as a dowel to help keep the figure in position on the layout.

All the figures were sprayed with Halfords white etch primer, and painting was carried out using Humbrol acrylic colours. Faces were detailed with eye and mouth lines, and all were given slightly rosy cheeks. Games Workshop thin acrylic washes were applied using brown for creams, fawns, browns, reds; and black wash. Once dry, all colours were highlighted or dry-brushed with lighter shades before enamel matt varnishing. The figures are then glued into position.

The Sealed Knot battle cameo

The model battle scene was created using

Perhaps a touch cleaner than the prototype, this version of Knaresborough's viaduct is nevertheless a stunning reproduction of the real thing.



ON EXCLUSIVE

A Civil War battle rages, although only for the entertainment of watching tourists. Peter has used his military modelling skills to great effect here.



plastic wargames figures. All figures had their weapons removed, as they would be liable for paint falling off, then they were fitted with new pikes and muskets. The pikes are brass wire with one end bashed flat and filed to a point. Muskets are scratch-built out of plastic card. The hands of all figures were re-made out of Milliput so that they grip the weapons. Photographs were used to obtain authentic figure clustering.

Northern Belle

The 'Northern Belle' train was made up entirely out of second-hand stock, apart from the lead locomotive which was new but in a different livery. The coaches were all completely stripped down and Shawplan details and flush glazing added together with a good re-spray in 50/50 mix GW wagon brown/BR Maroon to give the distinctive plum colour. Hundreds of transfers were added before applying a few sprays of gloss/satin mixed Humbrol varnish.



The River Nidd makes for a restful counterpoint to the mammoth viaduct overhead and the busy scenes elsewhere on the layout.

The Class 67 enjoyed a number of nicknames, including 'Cyclops', for obvious reasons.



Couplings were all removed and replaced with a horizontal swinging hook brass wire from one bogie fitting down into a hole on towards the outer end of the next bogie. Nothing more was required. This gives a solid movement to the train with no play in couplings whatsoever.

The Scarborough Spa Express

All my trains are full of passengers and locomotives all have drivers. Model humans bring life to any scene, without a doubt, but also bring perspective and reality into view. People hanging out of open carriage windows is another trait I use to great effect. Huge amounts of comments relate to this at shows making it all worth-while.

The proprietary Bachmann and Hornby stock SSE coaches are re-sprayed in a 10/90 mix of BR maroon/signal red to get the WCR maroon colour. Only 10% maroon is required as it quickly kills the 90% signal red dead. This maintains the brightness of the red colour quite nicely. Couplings are similar to Northern Belle stock but used to a greater advantage as the Bachmann coaches have telescoping couplings which can still be used with a brass wire hook and hole arrangement. On a straight run the coaches are visually very tightly coupled.

Would I like to build it all again?

It took a lot of time and effort but I think it was definitely worth it. The effect is exactly how I imagined it would turn out. We are now at the limit of stock capacity and running sequencing at shows, so there is no more to do on the model other than to just enjoy and maintain it. It is too big to erect at home, so I just rely on invitations to shows to put it up. I don't think I could build another large project like this just on my own, though, and I will need more hired help for the next project! ■



The very pub the whole layout is named after. Yes, we thought it was missing an apostrophe too, but that really is how it's written.



MEET THE MODELLER



I have only been railway modelling for the last 20 years or so in earnest. Prior to that I was mainly involved in other interests, including some aspects of military modelling, hence the military connections on my model railway projects.

Previous model railway layouts; 1) 'Etton' – a 1960s rural setting based on an imaginary single line branch connection running through the Yorkshire Wolds in East Yorkshire. An end-to-end station to fiddle layout, which was not initially intended to be exhibited as it was a self-indulgent project. The layout was eventually extended by eight feet at the station end to include a detailed village scene covering a second fiddle yard. 2) 'Rowlands Castle' – a much larger project, continuous double track run based on a South Hampshire village scene during the World War II 1944 D-Day build up. This layout now resides in Rowlands Castle village itself and can be viewed during the summer months at Stanstead House for the next year or so.



What we think...

I'm the first to admit that I am still learning the railway modelling ropes, but such expertise is not required to be blown away by The Worlds End. Its substantial centrepiece grabbed my attention from the outset, and made this layout an obvious choice for this month's cover, but the more you look at it the more there is to see. His realistic rendition of Knaresborough looks good enough to live in, and there are so many little scenes to discover therein. I'm also impressed that Peter has decided to design and manufacture his own figurines. With all of that, and much more besides, does it really matter that the trains run in a basic oval? I don't think so.

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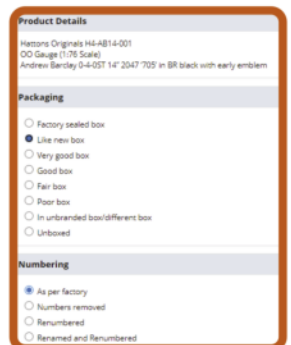
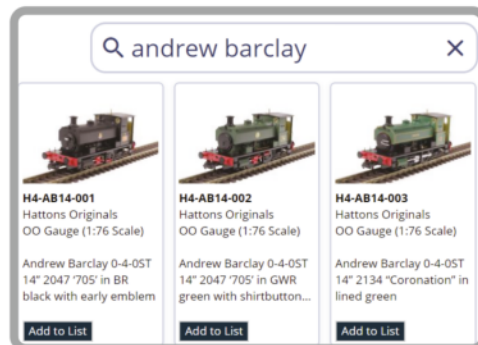
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HOW TO... BUILD A CARD INDUSTRIAL UNIT

Words & photography: Phil Parker

Phil Parker builds this accessible N gauge modern warehouse kit from Metcalfe Models.



The theme for this build is, 'Keep it simple'. Sometimes we can get bogged down in a layout project, and a quick build can be refreshing. Card kit assembly can take a few evenings, but the results are admired forever.

I'm no stranger to the Metcalfe range. Back in our Spring issue, I built Town End Cottage in 4mm:1ft scale and couldn't resist customisation. Moving to N gauge calls for a slightly different approach, though.

In 4mm:1ft scale, I'd be inclined to scribe mortar joints in the walls. A quick measure of a blockwork building locally, while trying not to look suspicious, tells me that this is less than 5mm deep. That's 0.03mm in 2mm:1ft scale, so I think we can ignore that.

The corrugated roof is a different matter, but even so, the smaller scale makes life easier. No need to replace the flat card with something textured, just emboss the corrugations, and take advantage of the excellent coloured printing.

Apart from this, there is the usual

colouring of the die-cut edges and a tiny bit of corner filling to do, but nothing major or time consuming. Good news for layout owners, as you don't often see these buildings on their own, so multiple kits will be needed to fill the space.



Metcalfe Models
www.metcalfe-models.com
(PN185) Industrial Unit

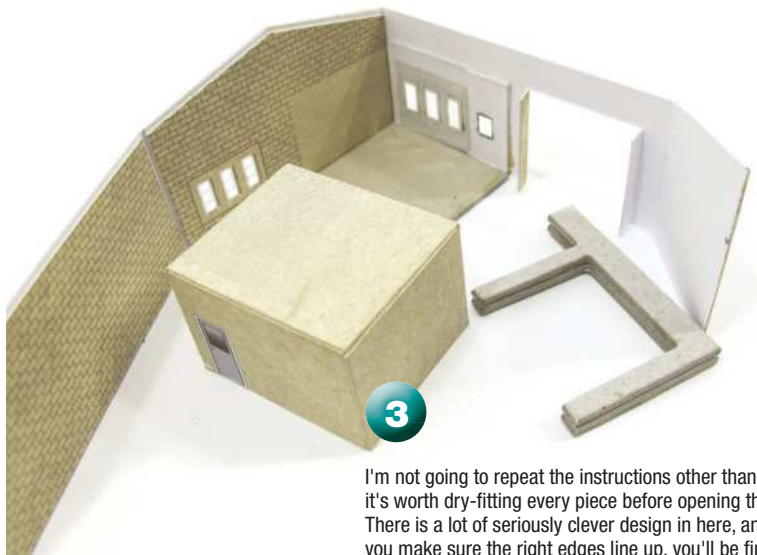


In the packet are two, folded, die-cut, printed card sheets, some plain grey strengtheners and a sheet of windows. All the modeller has to add is glue. I use Deluxe Materials Rokit card glue.



2

All the exposed white edges need to be touched in with colour. Humbrol 121, Pale Stone, is a pretty much perfect match for the walls and 173, Track Colour, for the brown bits. Dabbing it on with the side of a flat brush seems effective, and if any escapes on to the printed side, it can be quickly wiped away with a finger.



3

I'm not going to repeat the instructions other than to say it's worth dry-fitting every piece before opening the glue. There is a lot of seriously clever design in here, and if you make sure the right edges line up, you'll be fine. That office folds up out of one sheet, complete with rebates to locate the walls squarely.



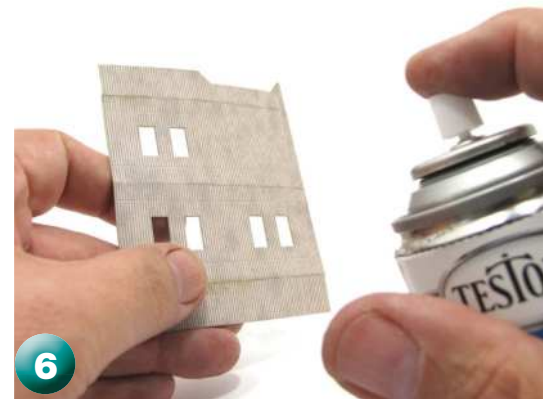
4

More nifty rebates locate the inner roof supporting card. It's important that this sits flush with the top edges of the walls. I find it easier to put it in place and then add drops of Roket card glue to the inside corners from the end of a screwdriver, letting capillary action draw it into the joint.



5

The corrugations on the ends and roof benefit from gentle embossing with a rounded tool (a dried-up ballpoint pen will work) to add texture. I had considered replacing these with plastic corrugated card, but in N, the relief required isn't great. Be careful to score the card and not push through the printed surface, though.



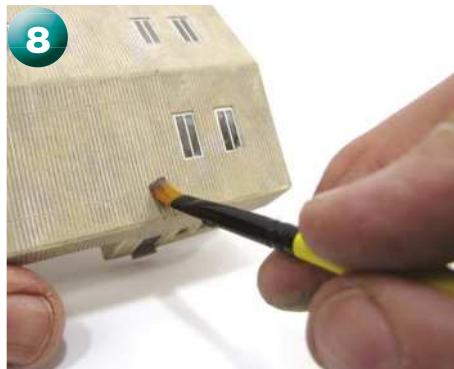
6

Since I plan to weather the roof with washes of paint, it's a good idea to give it a few coats of matt varnish to stop the liquids soaking in and cockling the cardboard. Do this before fitting the windows, or you'll mist these up.



7

Even painting the fold lines along the edge of the roof doesn't entirely hide them, but a little household wall filler applied into the corners does the job. I always keep a knife blade that has lost its sharp edge for this sort of job – it allows very precise application of filler.



8

Modern buildings don't seem to get very dirty, so I'm weathering the roof with nothing more than a patchy wash of Precision Paints weathered concrete colour. Depending on where the finished model ends up on the layout, a bit more dirt might be required, but it's easier to add than remove.



9

The finishing touch is a nameboard from the pun-filled selection included in the kit. Don't forget to colour the edges with some paint or a felt-tip pen, then fix in place with a tiny amount of PVA. You don't want the glue splurging out around the edges.



De-branding an Oxford diecast van

You can't have a modern warehouse without at least one white van, but the only model I could find was in Network Rail colours.

The printing seems to resist most chemicals, but a rub with some T-Cut, a car paint restoration liquid that is very slightly abrasive, eventually removed the logos. It also took away a little of the paint, but that adds to the look – many of these vans show a few battle scars after all.



Shopping list

- | Metcalfe Models PO263-4 OO/HO Corner Shop, M0054 – OO/HO Red Brick sheets
- | Craft knife
- | Steel rule
- | Scribing tool
- | Small screwdriver
- | Deluxe Materials Roket Card Glue
- | UHU
- | Wire
- | Tissue paper

OPEN ALL HOURS

Phil Parker channels his inner Arkwright with the build of a budget corner shop card kit from Metcalfe Models.

Subtle details make a difference on a model. The correct posters on a wall or cars in the street - even the clothes your figures wear define the era of your layout.

BRM Editor, Andy, presented me with a Metcalfe Models kit for a corner shop just as I finished reading the novel, 'The Trouble with Goats and Sheep' by Joanna Cannon. Set in the long, hot summer of 1976, the story included a detail that took me back to my childhood - orange shop windows.

In the days before air conditioning, many shops would stretch orange cellophane across the inside of windows to protect goods from the glare of the sun. As I

slurped my Walls Red Arrow lollipop (with free aeroplane!), I would have to strain my eyes to see what was inside. The decision was made - my miniature shop was to be set in 1976.

I'm a big fan of card kits. They're a cheap way to fill a baseboard with buildings for a start. Better still, if well designed, assembly is great fun. Although they aren't a five minute build, time spent is repaid with a lovely model.

My challenge is to modify the kit to look as good as possible with simple tweaks. By tackling those corners with readily available materials and working on the more obvious flat surfaces, I think the end result looks pretty respectable. ■

I'm a big fan of card kits. They're a cheap way to fill a baseboard with buildings

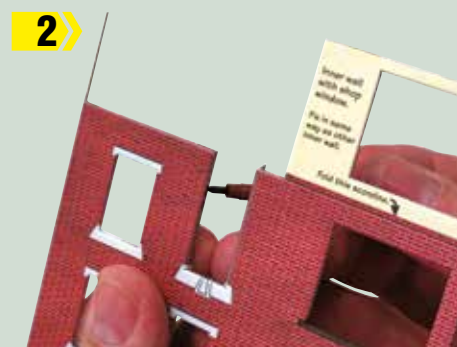


Young Phil Parker, circa 1976, orange shop windows not pictured.

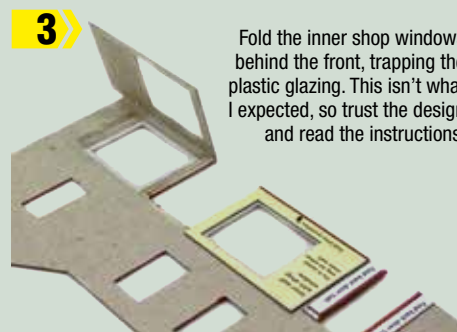
ON EXCLUSIVE



Metcalfe Models' corner shop is supplied on thick, pre-printed cardboard sheets. All parts are die-cut, requiring a small nib to be cut through to release them from the sheet. Additionally, there are printed plastic windows and comprehensive instructions.



The classic upgrade for any card kit is to colour exposed edges using a felt-tipped pen. Colour from the back of the sheet to avoid getting ink on the printed front.



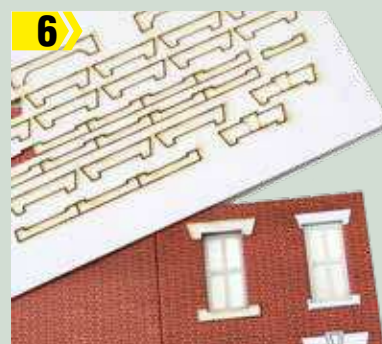
Fold the inner shop windows behind the front, trapping the plastic glazing. This isn't what I expected, so trust the design and read the instructions.



There are two window sizes, large and small, but the holes in the wall don't look very different. The printed side is matt and should face out. Clear all-purpose glue holds the plastic in place.



Printed curtains are provided and should be fitted with a scrap card spacer so they aren't on the back of the glass. The downstairs windows also need net curtains. A single ply of tissue paper works perfectly.



Laser-cut sills and lintels are found on a separate sheet. Fitting these provides depth to the detail, which makes them an essential addition no matter how good the printing.



I like to add interior walls so that you can't see through the building. Card offcuts from the kit or old packaging are perfect for this job. Test fit everything - a little trimming was required by some windows.



Despite being angled, the shop door fits perfectly between the two sides. Decorative mouldings surround the windows. I made a mistake by not colouring the visible brick edges - black paint sorts this.



Assembling the shop sign looks complicated but once you fit the first spacers, it's easy. A small pink piece of card provides a guide to the correct distance for each above the window. It's not stuck but used as a jig.

10»



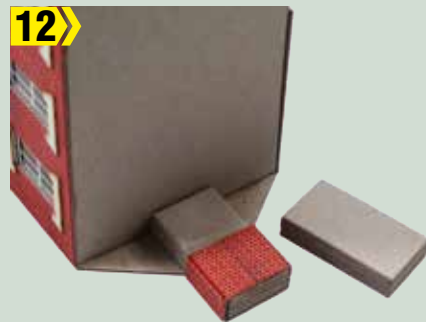
Several shop names are provided printed on thick card. More are on the thin card sheet to cut out and fix over the originals. Check the fit, I've coloured the ends and sides of the old name with black pen to hide gaps when the new name is added.

11»



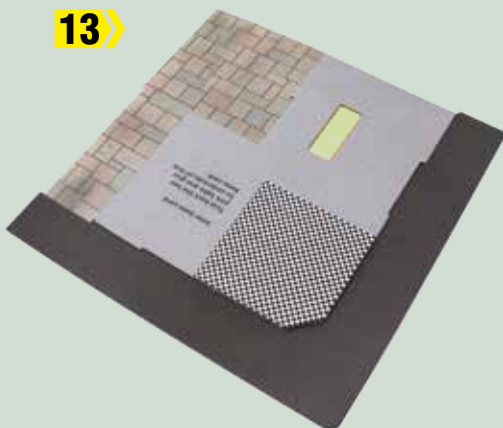
Orange cellophane used to be stuck to the back of shop windows to protect the goods from strong sunshine. Unable to find suitable sweet wrappers, I'm painting the inside with Humbrol clear paint.

12»



Chimney stacks are built from plain card layers and wrapped with the thick card sides. A strong method of construction but one that relies on accurate alignment of the parts for a square result. A slow drying glue isn't a bad idea to allow for adjustment.

13»



A couple of tabs in the main floor fold back to provide alignment guides when fitting it in the large sheet. Another layer is then added for the shop floor. You could print a different surface to add variety, although it's hardly obvious from outside the model.

14»



Various shop interiors are provided although there are only two choices of back walls. A couple of small walls made from scrap card hide the gap behind the window displays when looking through the door.

15»



With the main part completed, a gable end is assembled in the same way. The inner walls for the passage aren't flush with the bottom of the wall but raised slightly to let the wall fit into a gap in the base.

16»



Viewing a model from normal angles makes the roof more obvious than it is in real life. Printed sheets look too flat to be convincing, but scribing along the courses and between each slate improves things.

17»



Both roof parts fit nicely, but I should've bevelled the inner edges of one of the gables with a fine sanding stick to have a less prominent join.



18»

Bricks printed to match the Metcalfe range are available in Builder Sheet packs from the company. Each contains four thin and four thick brick card sheets which include useful curved lintels and roof tiles.



19»

I've cut out the castellated design, ensuring that only whole brick faces appear. All the edges are coloured with felt pen and the part is test fitted. A tiny smear of glue holds it in place.



20»

I feel that fancy corners would be excessive for the chimney stacks so these are wrapped in a single piece. While a join is still visible, it's not obvious and could be kept away from the viewing angle.



21»

Chimneys get dirty, so a stiff brush and Humbrol Smoke weathering powder adds the all-important grime. As the card surface is very smooth, matt varnish, lightly sprayed and left to dry, helps adhesion.



22»

Lead flashing stops rainwater seeping down between bricks and slates on real buildings. In model form, it's represented using 3mm wide strips of tissue paper and fixed with a tiny amount of PVA glue. A quick coat of paint finishes the job.



23»

Printed pavement sheets and self-adhesive individual slabs are provided. The printed version matches the courtyard paving and the colours are superb, so I scribed the gaps between the slabs and used it.



24»

No guttering is included, but it can be made from 4mm-wide strips from the edge of the thick card sheets. Colour with a black pen, then fix with the slightly curved edge from the die-cut sheet outwards.



25»

Finally, the model is bedded into the ground with grass fibres blown into place from a bottle. Since this is a summer scene, I'm using mainly beige with only a hint of green.





Factfile

Layout name: Rushwick
Scale/Gauge: 4mm:1ft / 16.5mm gauge / OO
Size: 7ft 6in x 1ft 3in
Era/Region: BR (W) early 1960s
Layout type: Fiddleyard to terminus

RUSHWICK

The fruit of lockdown labour, this branch line terminus is a lesson in prototype observation, recycling and using what is to hand. Nick Palette explains.

Words: Nick Palette **Photography:** Andy York



Diesel-hydraulic D6326 is shunting coal wagons, having left the brake van in the platform. Passengers are awaiting the arrival of the next train.

As a regular exhibitor until March 2020, with the cancellation of all model railway shows, I suddenly found myself with plenty of time to spare. No longer was I preparing layouts during the week and spending the weekends away from home. So, the obvious solution was to build another layout.

Given that it wasn't easy – and still isn't in some cases – to obtain supplies of basics such as track, I had to rely on what I had to hand. Fortunately, over many years, I have accumulated a good stock of Peco points in both Code 100 and 75, and I was able to source what I needed in Code 75. I tend to buy track in boxes of 25 yards so, again, I was fortunate that I had enough to do what I wanted.

Recycling parts

For the foundation, I had bought a layout from a friend, which I had dismantled to salvage parts but I was left with the fiddle yard board, which measured 4ft 6in by 15in. Having built a layout in a similar space before, 'Ashbourne Midland' (featured in BRM January 2018), I was confident I could do it again. I needed a fiddle yard/staging area, so I decided to use a three-track traverser that I already had for another layout. This makes use of metal drawer runners and does the job. It also ticks another box – having a common fiddle yard for several layouts; an ambition I have had for several years.

I had at my disposal two left-hand medium points, a large 'Y', a right-hand curved point and a catch point. By laying the points out on the board, I came up with the configuration shown in the trackplan.

It took me a week or so to get the track down and wired up. Part of the process involved cutting holes in the cork underlay for uncoupling magnets. I am an advocate of Kadee couplings on all layouts that I build, which includes British, European and American outline. I find that they are easy to install these days as most, if not all, British stock has the NEM socket. Uncoupling can be done either by magnets in fixed locations or by hand using something as simple and accessible as a bamboo skewer. Also, because they have a horizontal action, a piece of rolling stock can be lifted from a train very easily as opposed to the vertical action of other types of couplings. A lot of people might argue that they don't look very realistic on British stock, but how many prototype tension-lock couplers do we see?



The branch passenger train has arrived at Rushwick in the hands of large 'Prairie' 4160.



LAYOUT FOCUS

The locomotive has uncoupled from the coaches and is running forward to take water prior to running round the train in readiness for the return journey.



'Prairie' 4160 has disposed of the horse box and is coupled to the coaches, ready for departure.



Ready to run

I am a firm believer in using items that are readily available off the shelf. I can say that everything on the layout, with the exception of some of the scenic items, fits that bill. One gets involved in discussions at exhibitions when people ask questions about aspects of the layout and I hope I inspire them to go away with greater confidence, realising that everything is within their grasp. Having said that, I am full of admiration for those who have the skill and devotion to scratch-build.

On the subject of homemade scenic items, I have always felt that model trees are often too small, especially many of the commercial offerings on the market. I, therefore, took to scratch-building trees for this layout using various techniques gleaned from the internet, especially YouTube, and books, in particular, the *Wild Swan* books by Gordon Gravett. While I'm not professing to be an expert, I feel that making your own trees to fit a particular situation gives the layout individuality. With care, acceptable trees can be made at a much lower cost compared to buying ready-made examples. Just as we would for any other item on the layout, such as a building or item of rolling stock, take the time to go out and look at the prototype

– there's plenty of them out there!

Staying with the scenic elements of the layout, there are two things that I think stand out that we are all very fortunate to have these days. The first, is static grass. I have used it for several years now on layout projects and it is amazing how effective it can be. The initial outlay for a static grass applicator can be quite high, but the payback is well worth it. One thing I do when laying static grass is to make sure I have a blend of colours in the applicator. Grass is not a uniform colour and can be quite subdued so it pays to observe and not make things too uniform or bright! Once again, there are plenty of tutorials on the internet.

The other thing is the availability of photo backscenes. They add tremendous depth to a scene and also add realism given the use of photographs of the real world! However, a word of warning. Look closely at the trees in the photographs – I only recently realised there is a tree without leaves on the Peco backscene I used here, whereas all my three-dimensional trees are in full leaf. I came across a commercial photographic backscene a few years ago that had a mixture of leafless trees and trees in full leaf – neither one thing nor the other. Also, make sure

your backscene is appropriate to the area modelled. For example, high rugged hills and mountains are not good for the South East of England!

DCC operation

I have used DCC control for the last 15 years or so and rely on a trusty Lenz 100 system. All the points use solenoid motors powered by a Digitrax DS64 stationary decoder. I was offered a Roco plug-in Route Controller – no longer available and hard to find – back in 2019, which is a separate hand-held controller just for points and other accessories. This leaves the main throttle for locomotives and saves swapping from one mode to the other every time you want to change the points.

Locomotives and rolling stock are from the usual sources, mainly Bachmann, Dapol and Hornby. Originally, I had considered setting the layout in Hampshire in the 1930s using SR stock. This would have meant buying a completely new set of locomotives and stock, so I decided, in the end, to use what I had most of, that being BR Western Region stock of the early 1960s, around the transition from steam to diesel.

As to its exact location, that is not clear.

Given the use of a Class 22, that really puts it somewhere within reach of those depots to which they were allocated – Bristol, Newton Abbot or Laira, although some were moved to Old Oak Common later.

The main performers are, as well as the Dapol Class 22 diesel, a Hornby large 'Prairie' and a Bachmann Class 121 'Bubble Car'. Until I work out a way to change it, the 'Bubble Car' is showing destinations of Twyford and Henley. However, it does bring back memories of days at school in Twyford in 1963 where the Henley branch ran right next to the playground!

Show ready

The layout is intended for exhibition, hopefully in the not too distant future, where a sequence of three trains, two passenger and one freight will be run. I have spare locomotives and stock to vary things if needed.

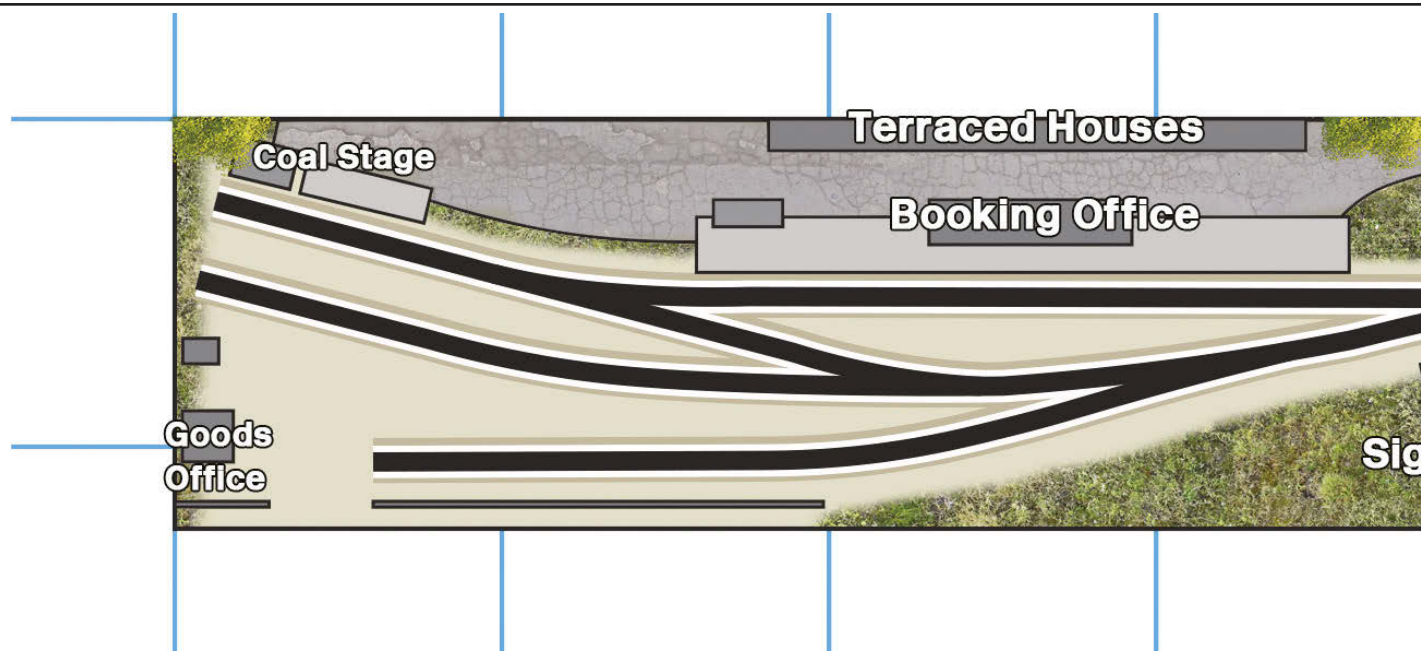
As a serial small layout builder, 'Rushwick' will no doubt fulfill the role of exhibition layout for 18 months or so while thought is given to the next project. Nothing definite yet but there's rumblings of the Far North Line in a similar period. There is a green Class 24 and Class 26 lurking in my storage locker waiting for the opportunity to run.



A view of the office and coal stage at the end of the layout. Also shown are some of the scratch-built trees using branches from garden cuttings as trunks.

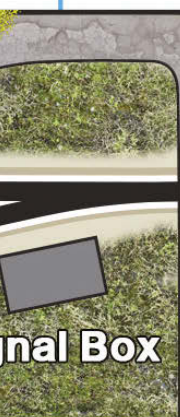
Trackplan

RUSHWICK





An overall view of the small goods yard showing the coal siding, with the locomotive facilities in the background.



Signal Box

HOW TO... WEATHER WITH POWDERS

Words & photography: Howard Smith

Howard Smith uses Hornby's Class 156 to demonstrate a beginners accessible upgrade technique for any item of motive power or rolling stock.



Last month I demonstrated airbrushing as a painting and weathering technique for models on a Peco turntable. Weathering not only customises models to make them more individual, but it can also tell a story, creating an illusion of wear and tear, depicting age and making models appear to have existed for longer.

The most accessible technique is using weathering powders – dense pigments that are applied by brush, and readily attach themselves to textured surfaces. Weathering powders are available in a number of useful shades for modellers, from a number of manufacturers. The most common – and useful – shades to have in stock when modelling are smoke, white, rust (in various shades) red oxide, and dirt browns. From these, variations can be created, like by mixing white and smoke to create an ash colour, for instance.

Though useful for rolling stock, weathering powders can and should be used around the layout on anything from small garden paths, to roads, smoke-stained chimneys, bridges or tunnels. Often, if constructing a model railway, buildings can be painted over a period of time, not always

using the same shades of paint. Weathering powders if subtly applied, can harmonise colours and help blend what might be slightly different shades of paint to give a more cohesive look.

Hornby's Class 156 model is a tooling of Lima origins, released under the Hornby brand again in 2006. This year, it was released in the commemorative 100-year anniversary of the RAF, in Northern livery (R3772). The underframe detail of this model is a one-part injection-moulded part and is a weak-point when placed alongside more recent Hornby models. The clean black-painted plastic draws attention to the underframe, and though it lacks ploughs at both ends of the model too, modifying it is outside the scope of this article. Instead, let's approach this from a cosmetic improvement perspective.

Many a beginner will question whether applying paint to a newly-purchased model is a wise idea, particularly when modelling confidence is low, but know that weathering powders can be removed more easily than paint by wiping with a damp cloth, or damp cotton bud, provided they haven't been sealed or fixed. Let's start the improvement.



Humbrol Rust weathering powder
DCC Concepts Smoke and Brake Dust weathering powders
Testors Dullcote



Weathering powders

Available in a variety of different colours between manufacturers, weathering powders all act in the same way. Key to their application is a brush and textured surface. Matt surfaces are easier to weather than gloss, or satin. If the powders don't stick to the model, consider applying a coat of matt varnish first.



This Hornby (ex-Lima) Class 156 DMU is a good runner being new, but lacks underfloor detail. The prototype was seen running not far from BRM headquarters, in Stamford and the livery is sure to stand out on a layout.



Aftermarket kits are available to replace the chassis detail, but I'll attempt to disguise it through weathering, making it appear more natural. The unpainted wheels draw attention to the simplistic underframe and must first be painted.



Removal of wheels on the un-powered vehicle is best achieved by applying leverage with the back of a pair of pliers to the wheel face and carefully moving the pliers away from the bogie frames. The frame should slowly distort for each to be freed.



Each of the shiny nickel wheels is lubricated in the factory before assembly and so can't be painted before a thorough degrease. Some blue roll with a little white spirit can be used to clean each, removing greasy deposits.



Humbrol 32, an off-black colour, is stirred thoroughly before use. If left standing for any length of time, shaking alone for a minute won't distribute pigments from the bottom of the tin within the carrier fluid properly.



The first coat of paint is applied, but two coats of enamel paint are required to provide sufficient coverage. If using a water-based paint, a primer will be required to provide adhesion for subsequent top coats.



It's important not to paint the treads or inside faces of wheels on the power car. Wheels on the power bogie can be rotated by running the unit on powered track.



Wheels on the non-driven bogie are easily rotated, but steer clear of pick-ups. Axles can be slid for access to the wheel fronts, or better still, remove the clip-in retainer with a screwdriver.



9

Share images of your weathered models with us at brm@warnersgroup.co.uk

To prevent dirtying the sides of the unit, which are regularly cleaned, a length of low-tack masking tape is applied. This will allow the brush to run up to the line neatly, without fear of over-dirtying the reasonably clean white vinyls.

TECHNIQUES



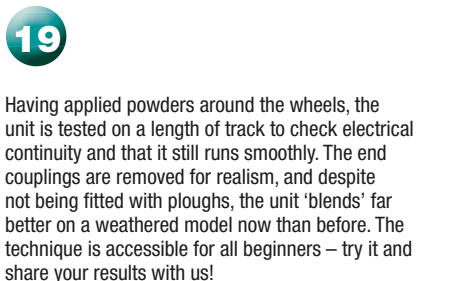
I've had this tub of weathering powders from Liberon Modelisme for more than 15 years and it is an excellent shade for underframe dirt. Sadly, no longer available, DCC Concepts manufactures a close shade with its brake dust powder.



The inner-ends of the units aren't washed often either, hence applying powder here makes sense, too. Note how it accumulates where detail and relief is located, just like the prototype – an effect harder to achieve with airbrushing.



Stepping away from the model a little bit helps gain a sense of the level of application. I feel this is enough for a model where the above-waistline is clean. Any dirtier and it might not look right, despite the prototype having a heavy accumulation of dirt.



Having applied powders around the wheels, the unit is tested on a length of track to check electrical continuity and that it still runs smoothly. The end couplings are removed for realism, and despite not being fitted with ploughs, the unit 'blends' far better on a weathered model now than before. The technique is accessible for all beginners – try it and share your results with us!



Applied with a large, but soft bristle brush that readily absorbs the powder, excess is first dabbed off on the side of the container to prevent waste, then the brush is tested on blue roll to check the amount that will be left on the model.



DCC Concepts' Black weathering powder is ideal for soot deposits. I don't want this unit to look like it has been too long in service, so a subtle application of the off-black pigments with a smaller brush is ideal. Think about the direction of the plume when running, and apply fore and aft.



I never seal weathering powders on model architecture, but on rolling stock that is handled, greasy deposits can leave marks behind. I recommend sealing the powders with Testors Dullcote, a solvent-based matt lacquer. Paper protects the rest of the model.



Working the brush around the underframe in all directions up to the yellow line, the powder is gradually deposited. Work over a paper towel as the powder will spill and easily stain. Over-application can be removed with water.



The exhaust pipe and silencer are prone to rust, so a smaller brush is used to apply Rust from DCC Concepts to the area. Overspill will be largely hidden after the first application of sealant.



Coupling the units together, I check for consistency with the corridor connection and exhausts, applying a little more to ridges in the roof that are prone to catching more dirt.



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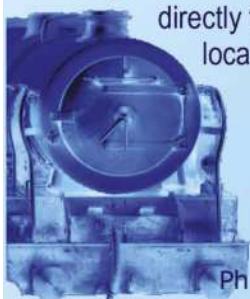
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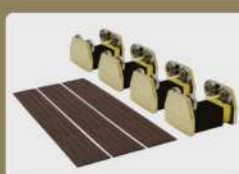
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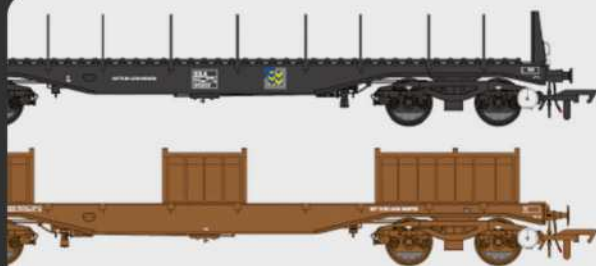
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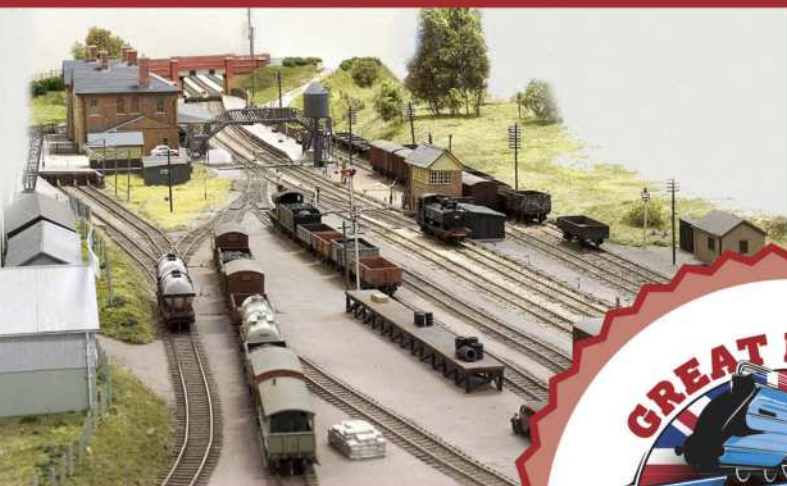
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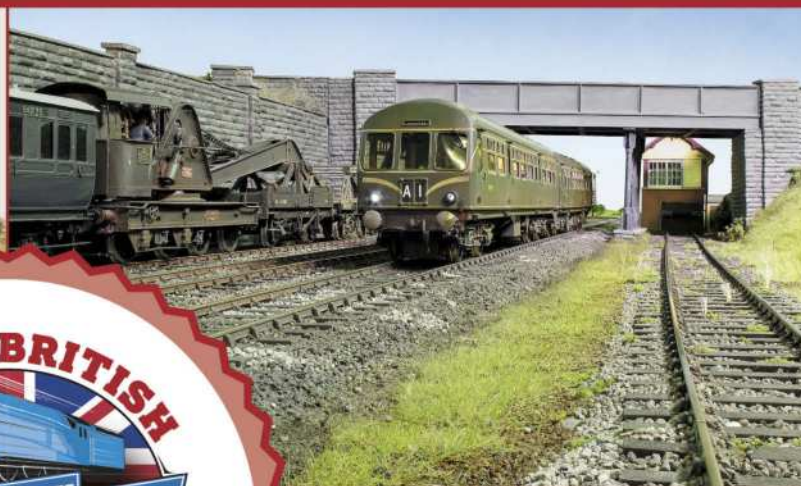
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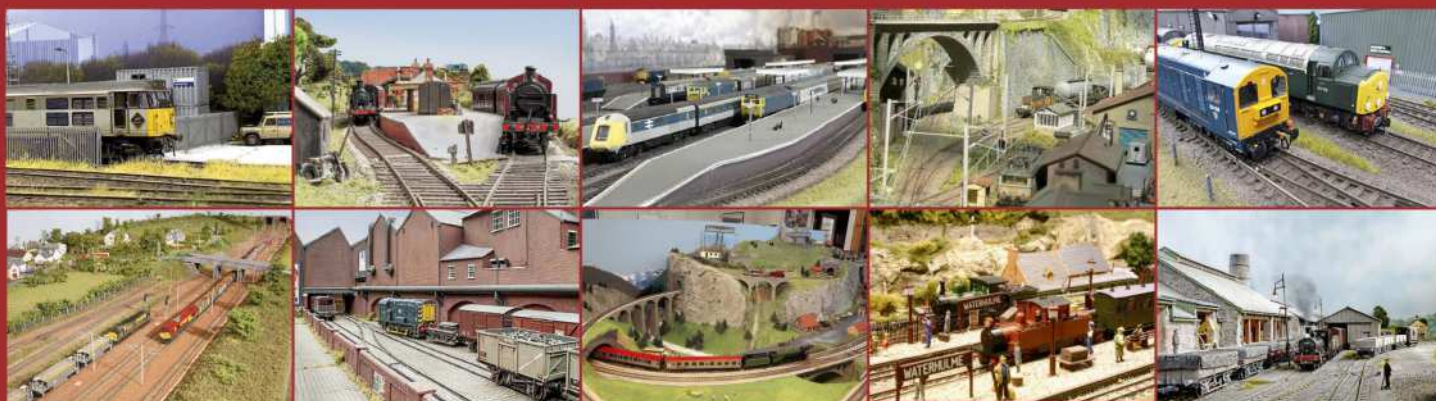
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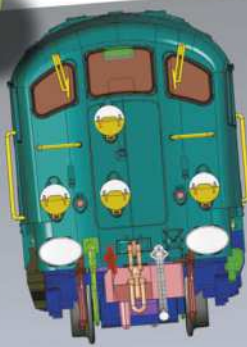
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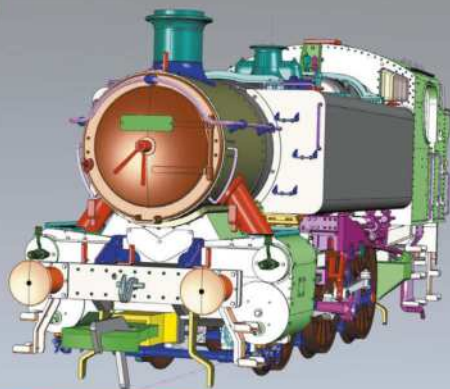
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HORNBY (R4964) CORONATION SCOT RFO COACH 7507

Arriving with Hornby stockists as part of its range of Stanier LMS Coronation Scot coaches in the distinctive blue livery with silver stripes is this Restaurant Open First, depicting coach 7507. Located third in the formations from the London end, the RFOs played a vital part in each of the three nine-coach consists. With the exception of the FKs and BTKs, built new for the service, the LMSR's Wolverton Works carried out interior refurbishment to a luxurious standard of selected existing coaches.

Price: £47.99

W: www.hornby.com

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GAUGEMASTER FORDHAMPTON (GM440) FORDHAMPTON MILITARY TENTS



As part of the Fordhampton Military range from model retailer, Gaugemaster, these versatile tents would be an asset to any Army on manoeuvres. Moulded in a khaki-coloured injection-moulded plastic, the tents can be enhanced with weathering, or even by adding a camouflage finish with paint. Alternatively, for a civilian look, perhaps re-purpose and paint them a different colour for a campsite?

Price: £13.50

W: www.gaugemasterretail.com

OXFORD RAIL 12T TANK WAGON

Modelled on the 12T tank wagons to RCH diagram 72 of 1907, Oxford Rail's wagon is yielding many different liveries in model form, as applied to the standard design used by many privately-owned companies. The biggest variation among the wagons was the liveries they carried, being used to advertise ownership and the many products carried, from fuels to beer.

Price: £18.95

W: www.oxforddiecast.co.uk

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HORNBY SKALEDALE (R7272) STONE BUS STOP AND (R7271) MODERN TIMBER GARAGE

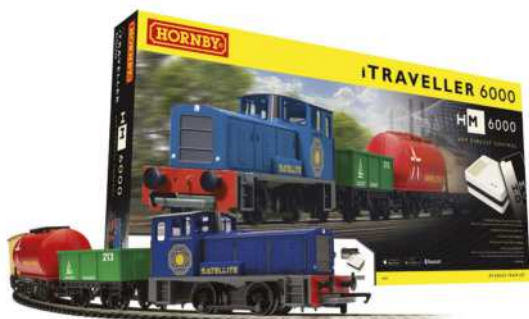
Whether in a small town or village, this pre-painted bus stop in the style of a stone-built structure features brick, tile and wooden planking detail, with notice signs by the entrance. Measuring 82mm in length, and based on a typical Cotswold structure, it could be sited around many other regions, too. Meanwhile, its modern timber-built garage is typical of the many bespoke hardwood structures now constructed to complement traditional architecture.

Price: £17.99 (each)

W: www.hornby.com

HORNBY (R1271M) ITRAVELLER 6000 TRAIN SET

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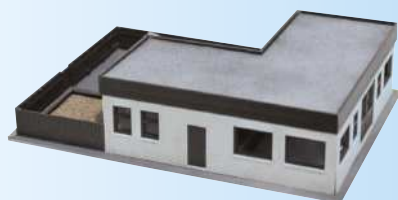
Featuring Hornby's new Hamant and Morgan cutting-edge wireless app-based model railway controller, its iTraveller 6000 Train set contains all you'll need to start in the hobby. Included is an oval of track and siding, 0-4-0 diesel-outline locomotive, rolling stock, re-railer, buffer stop and track mat. The HM6000 control unit included requires a smartphone or tablet device onto which its app is installed to enable operation. Visit the Hornby website to expand with track, locomotives, rolling stock, or accessory control with its HM6010 Accessory Controller.

Price: £129.99

W: www.hornby.com

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GAUGEMASTER (GM485) INFANT SCHOOL



This versatile kit is based on a small infant school, but its design could be used for many other applications, such as offices or municipal buildings. The injection-moulded plastic kit comes with playground accessories such as a slide, swings and features a sandpit. Plastic glue is required to assemble the 67 pre-coloured parts moulded in three colours. Glazing is supplied, but for best results, we recommend painting components. Instructions are provided.

Price: £15.00

W: www.gaugemasterretail.com

MODELU PHOTOGRAPHERS



3D-scanned from real people, each figure is available in scales from N to 16mm. Wearing period-correct clothes, each is posed taking a photograph. Pictured is (1670-076) Photographer with Box Brownie, (1671-076) Photographer with Box Brownie, (1672-076) Photographer with Kodak No. 2 and (1673-07) Photographer with Leica Mk. 2. Figures are supplied unpainted, on supports from the 3D-printing process and scale at 22mm tall for 4mm:1ft scale – around 5ft 5in.

Price: from £4.50

W: www.modelu3d.co.uk

SCALE MODEL SCENERY (KX058-00) SKATEPARK / BMX PARK

00



Widespread across the UK in urban areas to entertain younger generations, skateparks are very popular. This kit is designed to be accessible to construct with laser-cut MDF cores and high-resolution paper wraps with plain and graffiti versions included. The kit features two quarter pipes, a jump box, a fun box and two grind rails. Also included are two sheets of concrete texture paper to form the base for the skatepark. Laser-cut safety railings are included for extra detail on the quarter pipes.

Price: £6.99

W: www.scalemodelscenery.co.uk

SUPERCASST VICAR, BRIDE AND GROOM AND CHARLES DICKENS FIGURES

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Kent-based model shop, Mike Pett Miniatures has added to its Supercast range of model figures. Joining its expanding range of cast metal offerings is a Vicar, Bride and Groom, plus Charles Dickens, book in hand. The last-mentioned can also be supplied mounted on a totem plinth for an additional cost. Figures are supplied ready-to-paint, though for an additional fee can be painted by Supercast in-house.

Price: Figures £5.50 (each), plinth: £9.99

A: 12 Chatham Street, Ramsgate, Kent, CT11 7PP

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21 TON MINERALS

Words & photography: David Ratcliffe

Vacuum-braked 21T Mineral B312762, built at Derby in 1962, was recorded at Manchester Ashton Rd C&W in April, 1983.



The 21T Mineral wagon had its origins on the GWR, where it had been developed in the 1920s to improve the economics of coal haulage in South Wales, while from 1950, British Railways introduced some 7,450, the first 2,500 of which were unfitted and constructed by several private wagon builders.

The first 1,000 unfitted wagons had riveted bodywork and pressed pattern end doors, while the other 1,500 had welded bodies and welded fabricated doors. All had standard RCH underframes, 21ft 6in over headstocks, with a 12ft wheelbase.

From 1961, building switched to vacuum-braked vehicles, all of which were constructed at either Derby or Shildon. Numbered from B310000 onwards, all were of welded construction and like the unfitted wagons had two doors each side and an end door. However, the vacuum-braked 21T were also fitted with small flap doors

above the main side doors, and although the underframes were dimensionally the same as the earlier wagons, they were fitted with the BR clasp eight shoe vacuum brake, roller bearings and 1ft 8½in pneumatic buffers. The first 1,000 vacuum-braked 21T wagons (diagram 1/119) were fitted with a manual changeover lever for adjusting the amount of brake force to be applied, depending on whether the vehicle was loaded or empty. Subsequent vehicles, which were allocated to diagram 1/120, were fitted with SAB (Self-Adjusting Brake) gear, which automatically adjusted the brake force applied according to the loading on the wagon's springs.

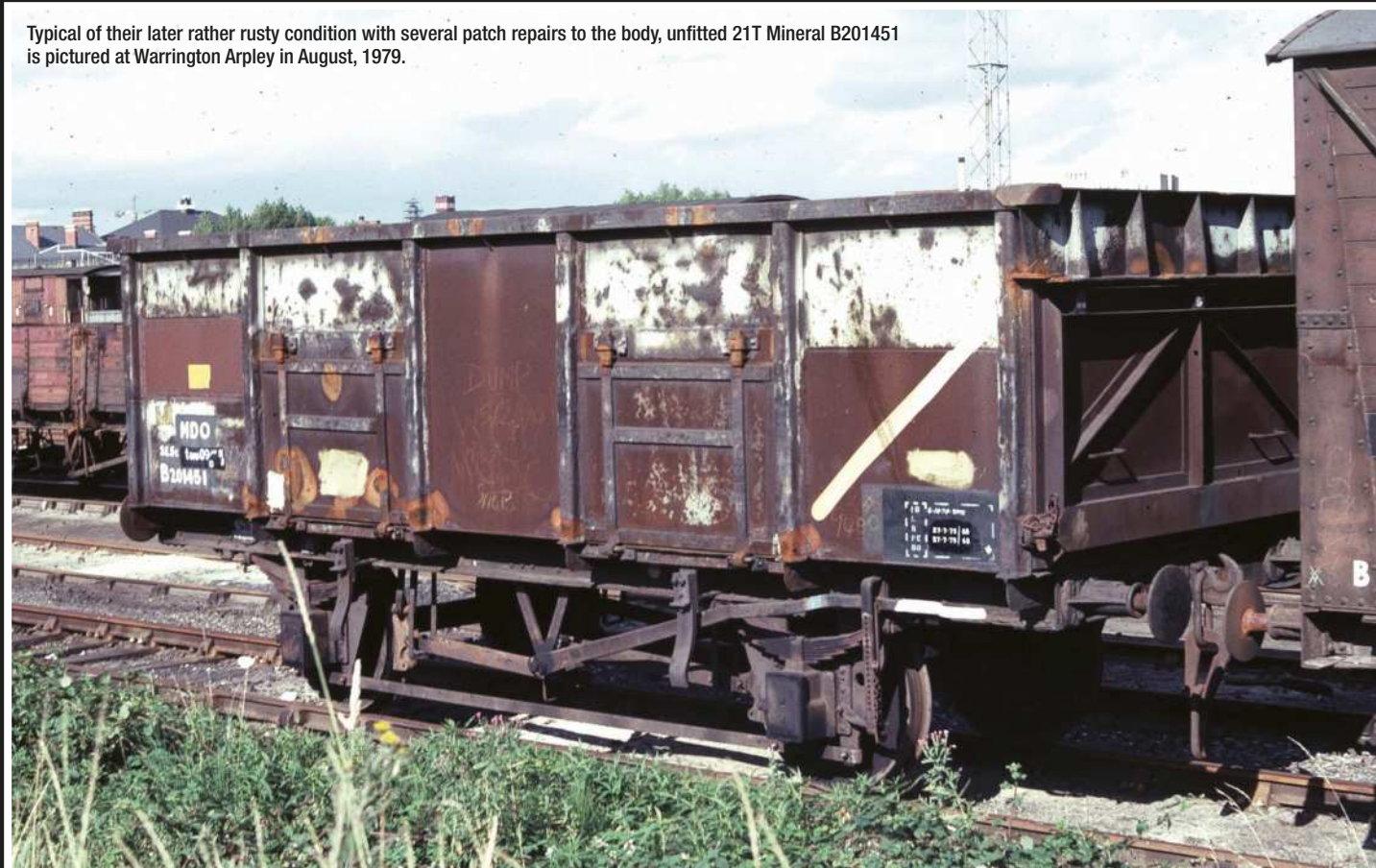
All the 21T Minerals were painted in BR standard livery. The unfitted wagons had grey bodywork with white lettering on black patches and a black underframe, while the vacuum-braked wagons had bauxite bodies with white lettering directly applied, again with a black underframe.

From the mid-1960s, some also received 'COAL 21' or 'COAL 21VB' codes, while from 1973 they received MDO (unfitted) or MDV (fitted) TOPS codes. All also had a white diagonal stripe on the body side indicating the end with a door.

Initially, the unfitted wagons were concentrated in South Wales working from pitheads to docks, power stations and steelworks. Similarly, the vacuum-braked 21T wagons went into service between collieries and large industrial customers, although they worked more widely and could also be seen in Kent, the Midlands and Scotland. For many years, most operated in block trains, but by the early 1980s much industrial coal traffic had been transferred into hopper wagons and the occasional 21T Minerals could then be seen in domestic coal traffic to local depots.

The last use for the MDOs in any numbers was on coal workings to Swansea Docks, where the unloading facility could

Typical of their later rather rusty condition with several patch repairs to the body, unfitted 21T Mineral B201451 is pictured at Warrington Arpley in August, 1979.



not end-tip vacuum-fitted wagons, while the MDVs were increasingly used to carry other minerals including rock salt from Over & Wharton, roadstone from Caldon Low, gypsum from Hotchley Hill, and agricultural lime from Ferryhill. From the 1980s, the vacuum-braked wagons also saw considerable use in scrap metal traffic working to the BSC works at Deepcar, Lackenby, Llanwern and Ravenscraig and to Allied Steel & Wire's Cardiff Tremorfa works.

BR's decision to switch to an air-braked network for revenue traffic in 1984 resulted in over 70 MDVs being subsequently fitted with a through air-pipe for continued use in scrap traffic in northeast England and the Sheffield area. In 1989, some 29 MDVs were sold to D. Christie to work as a block train from its scrapyards at Camlachie to Ravenscraig. These wagons were renumbered as CHR4609-4637 and recoded as PMVs but their continued main line use as private owners proved to be short-lived and the first 20 had been withdrawn and gone into internal use at Ravenscraig by March 1990. The remaining nine, now repainted grey with large 'MC' lettering, worked for several more months on behalf of MC Metals from Springburn to Ravenscraig before they were withdrawn.



When no longer required for scrap traffic, three of the 21T Minerals that had been recoded PMV and repainted in MC Metals livery were acquired by the Scottish Railway Preservation Society. PMV No. CHR 4632 (previously MDV B313378) is seen at Boness in May, 1993.

Ton Minerals

Number series	Diagram	Builder
B200000-0999	110	Metro-Cammell 1950 - riveted
B201000-1499	107	P&W McLellan 1950
B201500-1999	107	Charles Roberts 1951
B202000-2499	107	Birmingham RC&W 1951
B310000-0999	119	Shildon Works 1961
B311000-1949	120	Derby Works 1961
B312000-3499	120	Derby Works 1962
B313500-4499	120	Shildon Works 1962
B314500-4999	120	Derby Works 1963

Notes:

1. All bar diagram 110 had welded bodies.
2. B311950 to B311999 were cancelled and the underframes used to build the 50 COIL A VB wagons.

ACCURASCALE

21T MINERAL

Words & photography: Andy York

datafile

BASICS

Manufacturer: Accurascale

Catalogue Refs:

(ACC1080-MDOA) COAL21/MDO - Grey pre TOPS - Pack A

(ACC1081-MDOB) COAL21/MDO - Grey pre TOPS - Pack B

(ACC1082-MDOC) COAL21/MDO - Grey pre TOPS - Pack C

(ACC1083-MDOD) COAL21/MDO - Grey TOPS - Pack D

(ACC1084-MDOE) COAL21/MDO - Grey TOPS - Pack E

(ACC1090-MDVA) COAL21VB/MDV - Bauxite Pre-TOPS - Pack A

(ACC1091-MDVB) COAL21VB/MDV - Bauxite Pre-TOPS - Pack B

(ACC1092-MDVC) COAL21VB/MDV - Bauxite Pre-TOPS - Pack C

(ACC1093-MDVD) COAL21VB/MDV - Bauxite TOPS - Pack D

(ACC1094-MDVE) COAL21VB/MDV - Bauxite TOPS - Pack E

(ACC1085-MDONP) Planet Industrials MDO (ACC-1095-PMV1) MDV/PMV Triple Wagon Pack in MC Metals Livery

RRP: £74.95 (set of three wagons)

Gauge/scale: 16.5mm gauge, 1:76 scale OO

Era: 5 to 8

Company/Operator: BR

Weight: 50g per wagon

Chassis and Body: Plastic

Minimum curve radius: 460mm (R2)

Couplings: NEM-mounted tension-locks

Accessories: Cosmetic instantan couplings, vacuum pipes and brake rigging on MDV

pack of cosmetic instantan couplings and spare brake rigging assemblies with the MDV versions.

Some of the packs are sold out before arrival of the wagons, but several packs are still available from Accurascale and its stockists. I'm certain they'll sell quickly given the quality of the product and the usefulness of the prototype.



Introduced in 1950, almost 14,000 of these mineral wagons were built for BR. Two basic designs were used, both having twin side doors and one end door in their 21ft 6in long bodies, mounted on 12ft wheelbase under-frames. The 21T wagons (MDO and MDV) were closer to their 16T predecessors in height and lasted longest both in production and in service.

As is the Accurascale way, there's choice for the customer giving the potential to make up varied trains. At launch, there are five packs of three wagons in unfitted grey livery, five packs in fitted bauxite livery plus special-edition packs for Rails of Sheffield and Planet Industrials. In both the grey and bauxite liveries, there are three packs with pre-TOPS numbering and two packs with TOPS markings. Not just limiting the differences to the numbers, there are slightly different styles to the lettering and markings from wagon to wagon, so there is a typical variety to a rake of wagons, as there was in the real world.

The Planet Industrials' pack of wagons is a representative rather than accurate industrial livery, while the Rails of Sheffield pack sports a livery during MC Metals'

ownership, where the PMV wagons were used to transport scrap metal to Ravenscraig steelworks.

There are principal variations between the wagons, with spindle buffers and oil axleboxes for the unfitted 1/107 wagons and Oleo buffers, roller bearing axleboxes and door hangers for the fitted 1/120 wagons – all correctly captured on the model.

The wagon body is an accurate rendition of twin-door version of the MDO/MDV with the level of detail we've come to expect. The grey of the unfitted MDO version is of the lighter style – there was a variety of shades of grey out there – but I'm sure that will be largely academic once the models are suitably weathered.

Beneath the body, the chassis detail is very fine with factory-fitted detail parts, particularly on the vacuum-braked MDV, and the model sports sprung buffers.

Accurascale makes it easy for the finescale modeller as ever, with the capability to fit EM or P4 wheelsets with ease to replace the 12.6mm three-hole RP25.88 darkened profile sets with 14.85mm back-to-backs and 26mm over the wheelset pinpoints.

Each of the wagons comes with a detailing

ANE MODEL DC/DCC CONTROL UNIT

Words & photography: Howard Smith

DC or DCC? Choose wisely! It's strange to think that many newcomers are faced with making this decision as they start, knowing little about the pros and cons of each method of control. How do you decide the route you'll take so early on without making a costly mistake? Perhaps the solution is in purchasing a control system that can handle the operation of DC and DCC locomotives?

Distributed in the UK via electronics specialist, DMG Electech, and supplied with a three-year warranty for peace of mind, the LD101 Dual-mode DCC/DC Command Station from ANE Model now benefits from knowledgeable UK-based customer service and aftersales support.

Connectivity is straightforward as expected, an RJ-11 cable linking the compact control box to the command station, power being provided with the wall transformer included, while two rail outputs with screw terminals are fitted to the command station. The power input to the unit from the transformer dictates its output, this being 15VDC at 4A, more than enough for the average N or OO gauge DCC layout that isn't laden with accessories.

When testing a new device such as this I never read the instructions first, partly because it's a good test of the user interface to see if it's intuitive, but also because I rarely read instructions anyway! Upon opening the box, plugging the respective cables into their sockets is fool-proof – there's only one way – and the unit fires into life pressing the obvious button labelled 'power'. Switching between DC and DCC modes was simple with the press of a button, and the selected mode is confirmed by LEDs on the command station.

Throttle control is provided by a rotating knob, an indent allowing thumb control without looking at the unit, particularly useful if shunting on the

layout, for instance. The throttle knob isn't limited in its travel, hence will continuously rotate, speed steps on DC or DCC being controlled electronically. It requires some adjustment if changing from regular throttles. I'd recommend adding a small washer to the rear of the control knob to lift it away from the unit as it rotates.

Accessory control and directional control for both DC and DCC is via two buttons labelled 'off' and 'on'.

Decoder functions F0 to F28 are accessible from the unit, and which one of these is selected is displayed in the four-digit, seven-segment display. Unlike some DCC controllers, where functions can be allocated to specific buttons, or are ready to be pressed, this unit requires a little patience, but the trade-off is an uncluttered handset. Modifying a rubber mobile phone cover to fit around the rear edge of the box might make the sharper edges of it more comfortable for longer operating sessions.

This isn't a basic DCC control system – CV programming can be achieved through the unit, the control knob being at the heart



of reaching each CV and setting its new value. Through comprehensive printed colour instructions, advice is given on getting started, function control, how to set up a consist, programming consists, accessory decoder control, recall functions, and more, all with useful flow charts, removing a lot of the technical 'jargon' for the beginner.

With the simple push of a button, layout operators now have access to a control system that offers DC or DCC outputs – without fuss. Bar my personal modifications suggested, I have but praise for this new system delivering above expectations for its entry-level DCC control system price.

datafile

BASICS

Manufacturer: ANE Model (distributed in UK via DMG Electech)

Catalogue Ref: (LD101) Dual-mode command station

RRP: £119.99

Gauge/scale: N or OO (recommended)

Weight: 64g (handset)

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DMG Electech DCC/DC Command Station

Howard Smith provides an introductory tour of this dual control model railway controller, with DC and DCC control outputs at the press of a button.

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PLAY

KERNOW MRC 10T ROAD VAN

Words: Howard Smith Photography: Tony Wright



A ‘road van’ is the terminology used to describe a brake van, capable of transporting goods. The London & South Western Railway’s (LSWR) 10T Road Vans were its most numerous, almost 500 being constructed from its introduction in 1884, until 1905, to Diagram 1541.

Of typical sturdy hardwood construction over a steel chassis with a 10ft 6in wheelbase, the vehicles featured hinged side doors to access goods from platform level. Their design varied little over the years, key detail changes being round or ribbed shanked buffers, and straight or cranked step board supports.

Kernow Model Rail Centre has laboriously researched the prototypes and introduced its own branded models of these vehicles, its tooling catering for the aforementioned variations. Impressively for a first release of the models, 10 variants are being produced, covering LSWR livery with red ends, SR

pre- and post-1936 brown liveries and BR grey. Versions suitable for mainland and the Isle of Wight are available, making this model launch comprehensive.

Laser-scanned from survivor, 56046, on the Isle of Wight Steam Railway, the model unsurprisingly scales out very well. The injection-moulded plastic model is light, but a small rectangular cast weight screwed to the inside of the body helps increase its weight to 46g – sufficient to keep the model sturdy when running, but not detrimental to performance if hauled in a longer rake of wagons by a smaller locomotive.

The depth of sides and ends is immediately noticeable, such is the design of the vehicles with external bracing. Ironwork with bolt detail is represented, too, from the angled bracing of the frame, to the individually-fitted lamp brackets. Sturdy wire handrails (which don’t feel like they will fall off) are finely executed, their ends

– just like the prototype – appearing to be flattened where a bolt would join them to the bodyside. Injection-moulded detail to the bodysides has made this possible, and the models look all the better for it. The side door handles have been moulded integral with the doors. Modellers seeking greater detail could swap these for individually-fitted items, but such is the excellent application of livery to the models, I’d be tempted to leave them.

While on the subject, the liveries are where these models excel. All have been faithfully researched and the colours chosen – particularly for the LSWR variants – match proprietary colours very well. Even the underside of the verandah roofs have been painted with a crisp edge finish, as the satin red meets the brown of the sides.

Flipping the model over, brake gear with accurate ‘inverted’ pull rodding is depicted, each of the brake beams represented either

side of the axle. Careful design and planning have made this possible, while leaving space above for the NEM tension-lock couplings to pivot. Further consideration for the modeller has been given, as these sockets can be removed with a screw, allowing three-link couplings to be fitted to the hooks with relative ease.

Comparing buffer types among models shows how research has played a large part in this project. Such a small detail is noticeable by savvy modellers, and it highlights the standards that we now expect of manufacturers. The same is of the step boards, the majority of models being fitted with the straight boards, but cranked step board supports were fitted to the majority of vehicles transferred in 1925 to the Isle of

Wight for greater clearance. These are fitted to Kernow's models of 56045, S56050 and S56055.

Almost seven years after it was announced, it is only since the retailer has brought all managing of the project in-house, removing the delays caused by third parties, that this project has finally come to fruition. Kernow Model Rail Centre is now a larger business since the model was proposed, boasting another shop, with further models having seen the light of day, and more are expected.

These comprehensive new models join its growing portfolio of LSWR models and will be ideal for use with its Adams O2 0-4-4T locomotive, while providing another useful vehicle to fuel the growing interest in the pre-grouping era. Highly-recommended.



In light of further research, Kernow Model Rail Centre changed a small number of the running numbers and liveries, first announced in 2014.



Flush glazing to both ends of the model looks accurate, with bars for added authenticity.

datafile

BASICS

Manufacturer: Kernow Model Rail Centre

Catalogue Refs:

(SB003A) 56046 in SR brown livery with red ends (Isle of Wight)

(SB003B) 10131 in LSWR brown livery with red ends (Mainland)

(SB003C) DS 54538 in BR grey livery with 'Return To Three Bridges' branding (Mainland)

(SB003D) S56055 in BR grey livery (Isle of Wight)

(SB003E) S56050 in BR grey livery (Isle of Wight)

(SB003F) S54663 in BR grey livery (Mainland)

(SB003G) S54466 in BR brown livery (Mainland)

(SB003H) 54611 in SR pre-1936 brown livery with red ends (Mainland)

(SB003J) 54551 in SR post-1936 brown livery (Mainland)

(SB003K) 56045 in SR post-1936 brown livery (Isle of Wight)

RRP: £34.99

Gauge/scale: 16.5mm gauge, 1:76 scale OO

Era: 2 to 5

Company/Operator: LSWR / BR

Chassis and Body: Plastic chassis and body

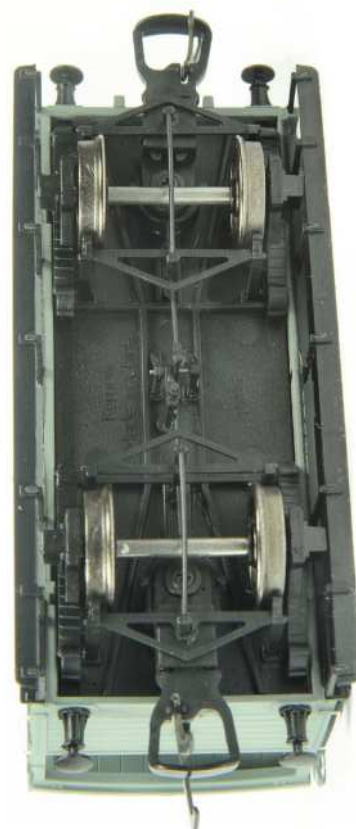
Weight: 46g

Minimum curve radius: R2 (438mm)

Wheel Profile: RP25

Couplings: tension-lock

Accessories: N/A



The sign of any good model of a brake van – properly-modelled brake rigging for those low-level camera angles. Even when on its roof, this model doesn't disappoint.

DCC CONCEPTS ROLLING ROAD

Words & photography: Howard Smith



I'm privy to seeing many fantastic model railway products arrive on my desk each month, but every once in a while, a new item will catch you by surprise. The clever designers in Settle have released a valuable series of tools for the more serious railway modeller, suitable for 9mm, 12mm, 16.5mm, 18.2mm, 18.83mm and 32mm track gauges.

The rolling road is often misunderstood by modellers, dismissed by some as something of a luxury. If you only have a small handful of locomotives, perhaps this is true, but for modellers who have more than a few, their use can't be ignored. A rolling road is very useful if building locomotives from kits, allowing faults such as wheel or axle wobbles to be corrected. It is also useful for letting a locomotive 'run in' so that bearings can be checked for tolerances, and gears can mesh, as intended.

Where a locomotive is in perfect running order, a rolling road still has its uses, particularly if a locomotive is being converted from DCC to DC, or vice-versa. A new sound decoder installation can be checked against a sound file to see if acceleration or deceleration and the sounds emerging from the locomotive correspond, for instance, all from the workbench. If looking to purchase a second-hand locomotive, a rolling road will also help

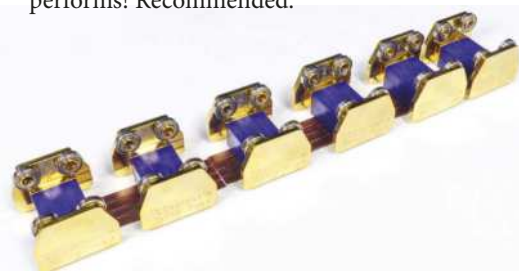
check that all functions work as intended, including the performance of the locomotive at its top speed, without a large test track.

DCC Concepts new rolling road range is wonderfully-crafted, each unit housed within precision-cut sturdy foam packaging. Kindly provided for review was a sample of its 32mm gauge six-axle unit. Each axle unit consists of two gold-plated sides, secured to a plastic carrier spacer set to the required gauge, which contains a strong neodymium magnet. Supplied inside the packaging are three pre-painted metal strips, etched to represent four sleepers, placed side-by-side. If glued between rails, as intended, each axle cradle will be secured into place, allowing adjustment fore and aft to suit your model's wheelbase.

When tested, the units performed admirably, the larger diameter rollers handling the weight of large Heljan locomotives with ease. Furthermore, because of the size of these, they didn't sound 'tinny'. Owing to the separation of rollers – necessary to prevent a locomotive from potentially 'riding up' them under heavy acceleration – I had to adjust the wire sand pipes on a kit-built Class 08, to prevent them from shorting on the unit. On most RTR locomotives, these are plastic items, anyway.

Regarding price, the 10-axle 9mm and 16.5mm gauge units, and six-axle 32mm gauge units are priced comparatively close to small RTR locomotives in their respective scales. While this might put some off, these are quality items designed to last a lifetime. Surely, it's better to have 14 locomotives that run beautifully, than 15 that are rough runners? For the space-starved modeller, those with more than a handful of locomotives, or a model railway club, this new range is worthy of the investment. You'll only ever need the one, but I can almost guarantee that as soon as it leaves the

box, everyone will be standing around it, examining what's running on it, and how it performs! Recommended.



Magnetic strips hold the units firmly into place, the blue spacers a perfect match for 32mm gauge track.



Housed neatly in a dust-free blue faux-leather box, a magnetic catch keeps the lid in-situ.

datafile

BASICS

Manufacturer: DCC Concepts Ltd.

Catalogue Ref:

(DCM-RRO.6) O gauge Premium Edition six-axle

(DCM-RRO.4) O gauge Premium Edition four-axle

(DCM-RRH.10) OO gauge Premium Edition 10-axle

(DCM-RRN.10) N gauge Premium Edition 10-axle

RRP: £99.95 (each), £149.95 (O gauge six-axle)

Gauge/scale: 32mm gauge

Construction: Plastic spacer with gold-plated sideframes and roller bearings

Weight: 104g (per roller)

Wheel Profile: Suitable for all types

Accessories: N/A

ACCURASCALE COIL A STEEL WAGONS

Words & photography: Andy York



Riding on the coat-tails of Accurascale's MDO/MDV, we get treated to something a little special and unusual in the shape of the Coil A – BR diagram 1/412 to Lot 3450 steel coil wagon.

Facing demand for transporting different types of steel in the late-1950s and early-1960s, British Railways at its works in Derby diverted 50 21ft 6in chassis, which were intended for 21T mineral wagons and adapted them by adding an 18in long well, lined with wood to enable steel coil loads to be transported. The cradle was fitted with three movable bars that supported the nylon hood.

When first built, the 'Coil Wagon' brand was carried on the wagons until the mid to late 1960s, when they were then branded 'Coil A VB' and later 'Coil A' to differentiate the two designs. The A code was added to the earlier design once the first Coil B entered traffic. Under TOPS, the designation KAV was used, which was later changed to SFV and SFW (for the surviving wagons which were air-piped) from 1983 onwards. 1/412 Coil Wagons were first allocated for cold reduced coil traffic from John Summers, at Hawarden Bridge during 1962 and by 1975, 35 vehicles were in Pool 3019, working Hawarden Bridge to steel

terminal locations. By 1971, examples of Coil A wagons could also be found at South Shields, and at Middlesbrough by 1975, by which point 15 were allocated to Pool 1307 on hire to the British Steel Corporation at Motherwell. Thus, although not numerous, they were wide-ranging.

They were also found working to British Leyland at Swindon, Brierley Hill and to Ford at Dagenham. By 1984, the 29 remaining vehicles were used on South Wales workings, particularly to Newport Docks where most of the survivors collected but also to Hamworthy Docks at Poole. In 1986, several examples worked to Avonmouth but by early 1992, the 21 surviving vehicles were all operating out of Llanwern, but only to the end of the year, by which time the last examples were scrapped.

Despite these wagons having run with the tarpaulin in place only, Accurascale, with its drive for detail, has modelled the interior beneath the canvas cover, which can be easily removed to allow for cameo scenes in loading sidings.

The chassis is to a high standard, but the body and fittings are an absolute delight of detail. I particularly like the natural unevenness of the tarpaulin, which is one of the most obvious features when the wagon is

formed in a train. Such trains can be a short block or individual wagons in a mixed rake.

Three packs of three individually-numbered wagons will be available, covering various stages of pre-TOPS and then the TOPS era KAV-coded wagons to reflect their operational lives from the early 1960s up to their withdrawal in 1992.

It's an unusual wagon, but well-executed and I can certainly find an excuse for some.



datafile

BASICS

Manufacturer: Accurascale

Catalogue Refs:

(ACC1100-COILAA) Wagon Pack A

(ACC1101-COILAB) Wagon Pack B

(ACC1102-COILAC KAV) Wagon Pack C

RRP: £74.95 (set of three wagons)

Gauge/scale: 16.5mm gauge, 1:76 scale OO

Era: 5 to 8

Company/Operator: BR

Weight: 50g per wagon

Chassis and Body: Plastic

Minimum curve radius: 460mm (R2)

Couplings: NEM-mounted tension-locks

Accessories: Cosmetic instantan couplings, vacuum pipes

IRISH RAILWAY MODELS A CLASS

Words & photography: Andy York



Although Irish-outline models may not be your thing, this review signals what the forthcoming Accurascale Deltics, Class 37s and 92s maybe like as the two businesses are intrinsically linked.

60 of these Co-Co locomotives were ordered by CIE from Metropolitan-Vickers and were built at Dukinfield Works, Manchester between 1955 and 1956 as part of a plan to implement mass dieselisation of the Irish railway network.

Initially, the locomotives were fitted with an eight-cylinder, two-stroke Crossley unit developing 1,200 horsepower and deployed across the country, working express passenger and freight services including cross-border trains. However, significant reliability problems soon emerged, including cylinder defects, unbalanced engines and flashovers as well as excessive water temperature causing shutdowns. The subsequent reliability issues lead to CIE placing all future locomotive orders with General Motors, and in the late 1960s, CIE managed to persuade GM to sell them

replacement power units to re-engineer the troublesome A Class.

The rebuilds took place between 1968 and 1971. They were rebuilt with a 12-cylinder EMD 645E engine seeing a slight increase in power over the original Crossley units. With

their new engines, the As settled down and provided over two decades of further service into the 1990s, with the last members of the class being withdrawn in 1995 after the introduction of the new 201 Class.

Taking the model from the box the



immediate impression is of solidity and quality. Although it's a Co-Co wheel arrangement, it's slightly shorter than a Class 25 to give a little context. Livery application and lettering is faultless and there's a wealth of detail to admire, especially on the bogie sideframes, and finesse through etched side grilles and roof fan grille.

Included in the box is an enjoyable A5 booklet telling the story of the A Class, a personal perspective from a railwayman and how to get the most from the features and functions. It's nicely topped and tailed with period advertisements for the locomotive and its contributing entities.

At launch, there are 17 models covering the liveries seen through the life of the locomotives, from silver as delivered through green, black, black and tan and Irish Rail orange liveries in various numbers. The differences arising from the re-engine and re-build are catered for, too. The early locomotives have no headlights above the cab windows, a single exhaust port and shorter silencers on the roof. The roof modifications after rebuilding are catered for within the roof panel, which is very easily removed, attached by four neodymium magnets. Once you've had the satisfaction of that, you'll wonder why more models can't be configured this way for accessibility. On the 21-pin blanking plug can be found six small dip switches to give the analogue user control of the white marker lights, red tail lights and headlights at each end, which can be flicked over with a small screwdriver.

I have also been lucky enough to test the model out with DCC sound. There is to be different files for Crossley and EMD engine models. The swap-out of the blanking plug and 21-pin decoder takes seconds, and the roof panel reassuringly snaps back into place thanks to those magnets. Beautiful.

Should you need to remove the body to fit a crew to the cabs, this is also simplicity itself with four recessed screws and the body then lifts away. While you have the roof off, you'll undoubtedly spot three capacitors at the fan end of the chassis, more of that anon.

The model's performance on track is nothing short of phenomenal. It's perfectly fine on analogue with smooth and silent control, but it starts to come into its own on DCC. Those capacitors are now working and give power for around five seconds if contact with the track is lost, even though it is all wheel pick-up and drive, which means it can perform a party trick of running at around 15mph over a sheet of A4 paper laid over

the rails! Moving onto what's really special about this diminutive powerhouse, I have had 23 Mk. 1 coaches behind it on the test track, which it handles with ease. At speed step 1 out of 128, the model still moved that ridiculous length train without any sign of reduction in speed and with no trace of slipping.

The models are on their way as I type this – the Western World is struggling to get goods shipped from China, with container shortages and variable shipping delays, but IRM have side-stepped this and the models are reaching us by train, on The Silk Road all the way from China to Barking Freight Terminal – is this a first?

Treat yourself to something a little different. I'm having one as there's a family connection to Metrovick at the time of their manufacture and this bodes very well for the standards and capabilities of those Accurascale locomotives for the UK market.

Although it's an IRM product, UK buyers can order direct and have locomotives shipped from within the UK, so you won't fall foul of current tax and shipping complexities.

datafile

BASICS

Manufacturer: Irish Railway Models

Catalogue Refs:

(IRM1100) A1 - Special Edition Silver with box – £199.99

(IRM1101) A30 - Silver

(IRM1102) A42 - Green

(IRM1103) A46 - Lined Green

(IRM1104) A55 - Black

(IRM1105) A12 - Black with Yellow

(IRM1106) A15 - Black & Tan

(IRM1107) A39R - Black & Tan

(IRM1108) A23R - A Class Locomotive-Black & Tan

(IRM1109) 018 - Supertrain

(IRM1110) 027 - Supertrain

(IRM1111) 007 - Irish Rail Supertrain

(IRM1112) 056 - Irish Rail Supertrain

(IRM1113) 048 - Irish Rail

(IRM1114) 017 - Irish Rail

(IRM1115) 054 - Irish Rail

(IRM1116) 036 - Irish Rail

RRP: £169.99 DCC-ready

Gauge/scale: 16.5mm gauge, 1:76 scale
OO

Era: 5 to 8

Company/Operator: CIE/Irish Rail

Weight: 500g

Chassis and Body: Plastic chassis and body

Minimum curve radius: 438mm (R2)

Wheel Profile: RP25

Couplings: NEM-mounted tension-locks



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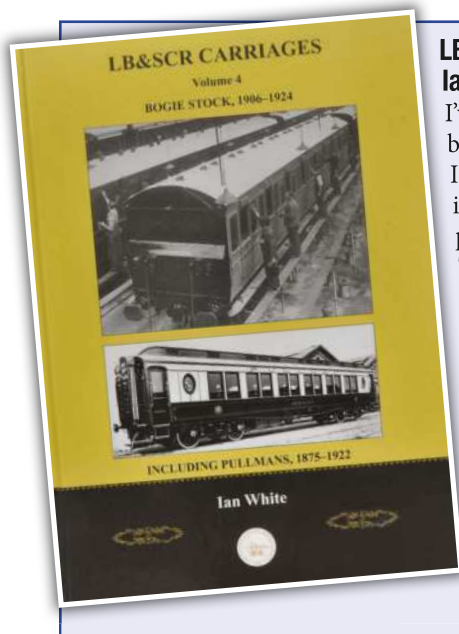
Andy York takes a closer look at the new A Class and puts the model to the test on track.

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PLAY



LB&SCR CARRIAGES VOLUME 4 BOGIE STOCK, 1906-1924 INCLUDING PULLMANS, 1875-1922, by Ian White, the HMRS. Price: £35.00

I've been a past-President of the HMRS and have always held in the highest esteem everything published by the society. This book is no exception. It really will become the standard work on the subject, because I've seen nothing else like it; nothing as comprehensive and certainly nothing as thorough. It's divided up into seven chapters, describing arc-roofed standard carriages, motor trains, electric motors and trailers, proposed designs, both American and British Pullman cars of the period and WWI Ambulance trains. There's also a supplement and 11 appendices as well as references and indexes. Quite an achievement! The standard of reproduction is excellent, all presented on high-quality stock. That reproduction enables the hundreds of photographs and drawings to be perfectly interpreted. There are even some coloured pictures of 7mm:1ft scale models of American Pullman cars, as well as coloured shots of the real things awaiting restoration; a huge task, it would seem. The variety on show is incredible, and the story doesn't end in 1924. There are pictures of surviving carriages in both SR and BR days, including a wonderful image on page 126 of a D-/67, No. DS291, in crimson and cream livery, taken in 1958, still with its number at the left-hand end. This wonderful vehicle, riding on six-wheeled bogies, appears on the next page, this time in Southern Region green; sadly, with a condemned sign on it. One of the type is being preserved on the Bluebell Railway. It really is an incredible work, and it complements perfectly the other books related to the Southern published by the HMRS. As with all the others, this latest volume is to be highly-recommended.

RHAPSODY IN BLUE WESTERN REPAINTS THE COMPREHENSIVE GUIDE by Adrian Curtis, Track Record Press. Price: £19.99

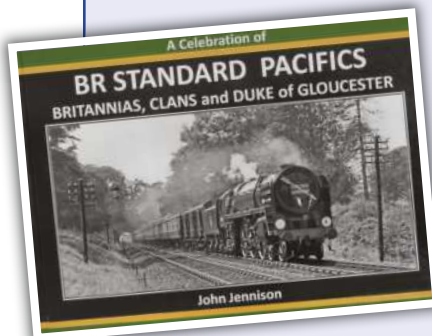
If ever a man knew (or knows) everything worth knowing about the WR hydraulics, then it's Adrian Curtis. It certainly is comprehensive as it relates to the painting regime undertaken by Laira (Plymouth) Depot on most of BR's 'Western' Class 52 diesel-hydraulics between 1972 and 1976. Not all the 74-strong class made the Laira repaints – a dozen avoided them, and these are listed in the final chapter of this 160-page volume. All the ones which did are given at least a page to themselves (some, more). The (almost all-colour) photographs show the locomotives in just about every condition sported by their blue livery. Some will provide inspiration for some really serious weathering! Produced on good quality stock, the photographs have reproduced well in the main. A comprehensive account is written for every class member illustrated, including any embellishments applied. The research needed to get all these details right must have been incredible, though not unexpected given the author's pedigree in these matters. Obviously, for anyone contemplating the making of models of the class, this work will be an invaluable resource, particularly if depicting the locomotives' final years. I found the time rather depressing for the class because, even with repaints, prior to that some individuals were incredibly shabby – was this to do with Laira's cleaning regime? The contemporary 'Deltics', although at times very dirty, never displayed the flayed-off paint effect seen on the last of the hydraulics. I have to say (as with most other classes), I preferred the 'Westerns' in their original liveries, and there must be a book out there showing these. I found this account very interesting, if rather lugubrious towards the end of the locomotives' lives. Highly-recommended.



A CELEBRATION OF BR STANDARD PACIFICS – BRITANNIAS, CLANS AND DUKE OF GLOUCESTER, by John Jennison, Irwell Press. Price: £27.95

This is a publication where I have to express an interest, having proof-read it and expanded on some of the captions. This is the third in this wonderful series (following on from the 'celebrations' regarding the 'Princess Coronations' and Gresley's non-streamlined 'Pacifics' – and there are more to come), and it follows the A4 landscape format, using, in the main, whole page photographs. And, what photographs! With some of the best photographers' work on display, allied to outstanding reproduction, this is a visual feast to delight the eye. Though I haven't checked exactly, I'd doubt if any class member is missing from the illustrations. The locomotives are seen throughout their lives, on all the duties that were allocated to them. For students of rolling stock, the trains hauled by the locomotives are of interest, too! The standard of reproduction is exceptional and the captions are accurate and informative, making it an invaluable

resource for any potential model-makers. All the variations in appearance throughout the classes' lives are recoded (with the exception of *Lord Rowallan* latterly fitted with oval front buffers and *Iron Duke* with its big tender). The 'Brits' success on the ex-GE main line is well described and the SR's 'celebrity' pair is well-illustrated. The class's rather inauspicious start on the WR is also mentioned. Whether anyone would go as far in their modelling as to replicate the likes of 70011 and 70049 (on pages 106 and 107), shorn of the nameplates and fitted with crude, hand-painted, wooden front numberplates, I don't know, but it would be a remarkable exercise in weathering. All in all this is a magnificent book, fully up to the standard of the previous two in the series, and it's essential for any enthusiast's bookshelf. It's exceptional value for money and I recommend it entirely.



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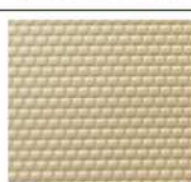
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Thank you for the terrific job on resurrecting the Class 37 which I had obtained second hand, it having been spoilt when it was renumbered and coated with varnish of some type.

You not only removed the varnish and the poor numbering, but also resprayed the affected areas and renumbered it again. Then, after all that, applied a good weathered finish. As you say "it's the little things".

Terrific, thank you. There is a real danger I'll be back! Mr Foster

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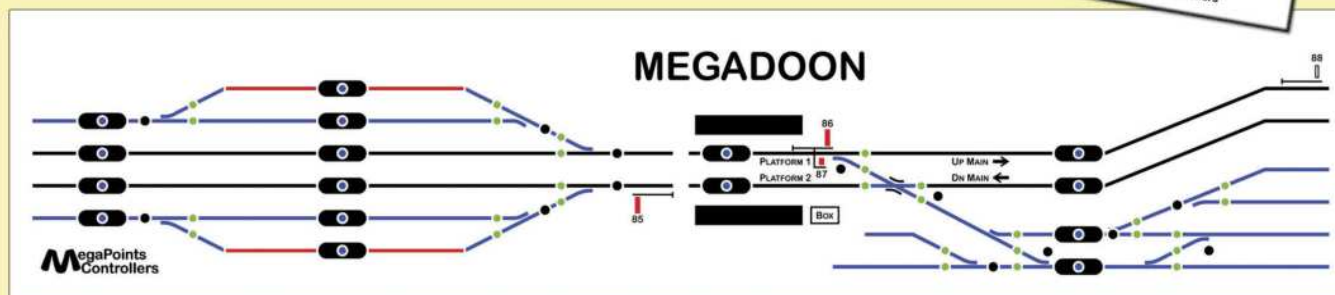
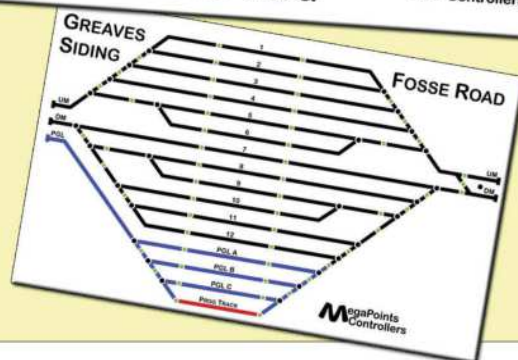
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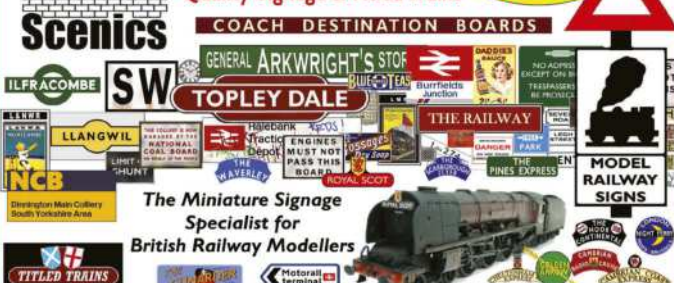
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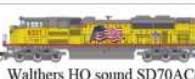
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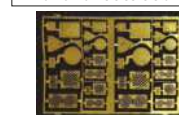
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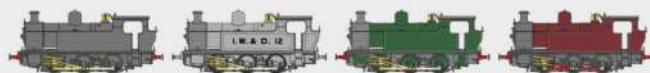
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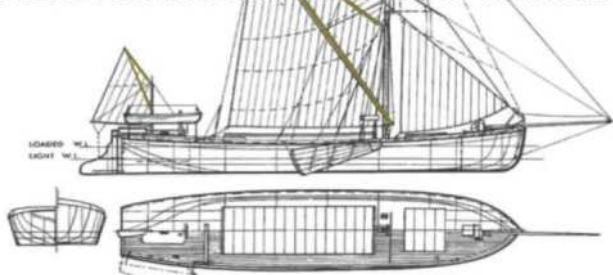
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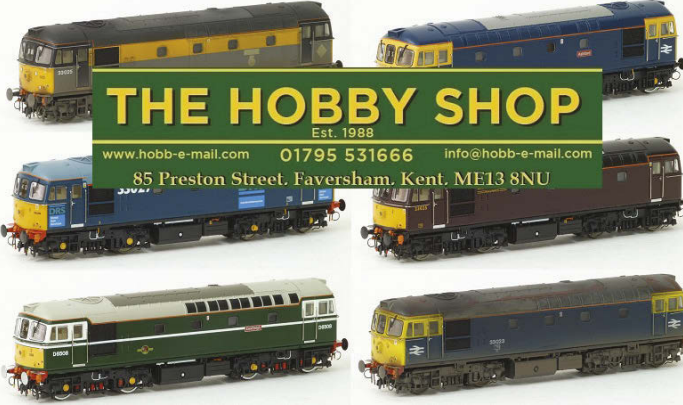
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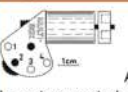
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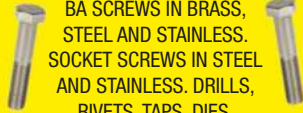
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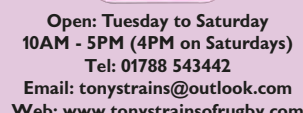
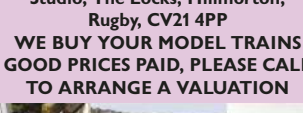
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Buffer Stop

Summertime, the modellers' favourite season

Once you get over the massive size of 'Heaton Lodge Junction', the thing that strikes you is that Simon has chosen to set the model in winter. Even on a warm day, you shiver slightly watching the trains go by in the frosty landscape.

Compare that to the majority of layouts that are set in high summer. Why do we do this?

Is it just easier to model trees in full leaf when all those fiddly twigs and branches are hidden from view?

Or is it nostalgia from a time we now see through old photographs? Taking pictures when wielding the family Instamatic you were told to make sure the sun was behind the camera so the subject could be seen, and don't bother wasting expensive film on dull days as the photographs won't look right.

The joy of our hobby is we can keep a little piece of the long, happy days of summer in our models all year round.

What do you think? Are there any autumnal layouts out there?

Standing in front of an ice cream van, what do you order?

Andy: A cider lolly; suck it until it's transparent!

Debbie: I love ice cream, but anything toffee-flavoured is a winner for me.

Howard: I'll have the lot. Not that I'm greedy or anything...

Phil: Classic Mr Whippy 99 for me. With that red sauce stuff that's never seen a real raspberry.

Ruth: Lemonade ice lolly for me!



This month in pictures...



Kings Cross station. The perfect place to play a game of "Where's Howard?" (Answer: In a coffee shop waiting for Phil...)



Glass trophies and postage aren't a happy mix, which is why it's taken so long to present the 2019 BMRA Layout of the Year Award to the MRC's Tim Watson for Copenhagen Fields.



Andy's tripod needed a little gaffer tape repair on a recent photo shoot. We are nothing if not determined to bring you the best photographs.



This is Phil Parker, reporting to you from a glamorous car park for World of Railways Plus.

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7D-009-002 - Class 121 W55027 BR Green SYP



7D-009-003 - Class 121 W55029 BR Blue/Grey



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7F-049-001 - VEA 12t Goods Van (Vanwide) Freight Brown 230026

7F-049-002 - VEA 12t Goods Van (Vanwide) Freight Brown 230034

7F-049-003 - VEA 12t Goods Van (Vanwide) Railfreight Red/Grey 230117

7F-049-004 - VEA 12t Goods Van (Vanwide) Grey (Yellow Ends) 230142

7F-049-005 - VEA 12t Goods Van (Vanwide) BR Red/Grey 230078

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7F-049-201 - VEA 12t Goods Van (Vanwide) Freight Brown B 783773

7F-049-202 - VEA 12t Goods Van (Vanwide) Freight Brown B 784641 VMV

7F-049-203 - VEA 12t Goods Van (Vanwide) Freight Brown B 784479

7F-049-204 - VEA 12t Goods Van (Vanwide) Freight Brown B 783994 VMV

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4F-011-033 - Ventilated Van LMS Grey 117870

4F-011-034 - Ventilated Van LMS Grey 117870 Weathered

4F-028-001 - 45ft Container High Cube Twin Pack Argos/Co-operative

45011 7 450065 2

4F-028-002 - 45ft Container High Cube Twin Pack Argos/Co-operative

45011 7 450065 2 Weathered

4F-028-013 - 45ft Container High Cube Twin Pack P & O Ferry

008460 2 008037 7

4F-028-014 - 45ft Container High Cube Twin Pack P & O Ferry

008460 2 008037 7 Weathered

7F-053-010 - 5 Plank Wagon Corrugated

End BR Diagram 39 Grey B497621

7F-053-011 - 5 Plank Open High Bar

Bauxite B485082

7F-054-004 - BR Meat Van Bauxite

870050

7F-056-018 - BR Standard Plywood Van

Diagram 213 Bauxite B764481

7F-056-019 - BR Standard Plywood Van

Diagram 213 Bauxite B771500

7F-056-020 - BR Standard Planked Van

Diagram 208 Bauxite B768104

7F-056-021 - BR Standard Planked Van

Diagram 208 Bauxite B777491

7F-057-005 - BR Insulated van Diagram

0251 Blue B872150

2P-006-004 - Mk3 Sleeper Jervis

Departmental DB 977989

2P-006-006 - Mk3 FGW Dynamic

Lines 10601

2P-006-007 - Mk3 First-Scotrail

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2P-006-008 - Mk3 Serco

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2F-047-009 - CCT BR Southern

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- **WANDERING WEEDKILLER**
- **GUIDE BRIDGE**
- **THE CLASS 40s**



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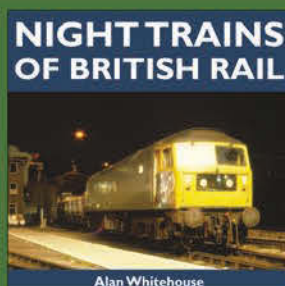


Andrew Wiltshire

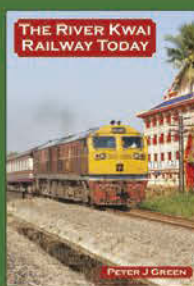
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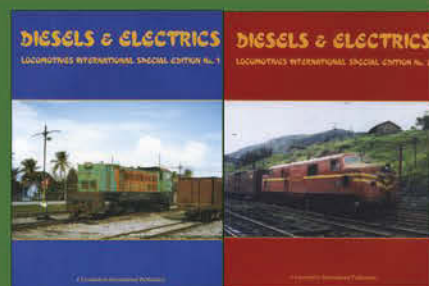
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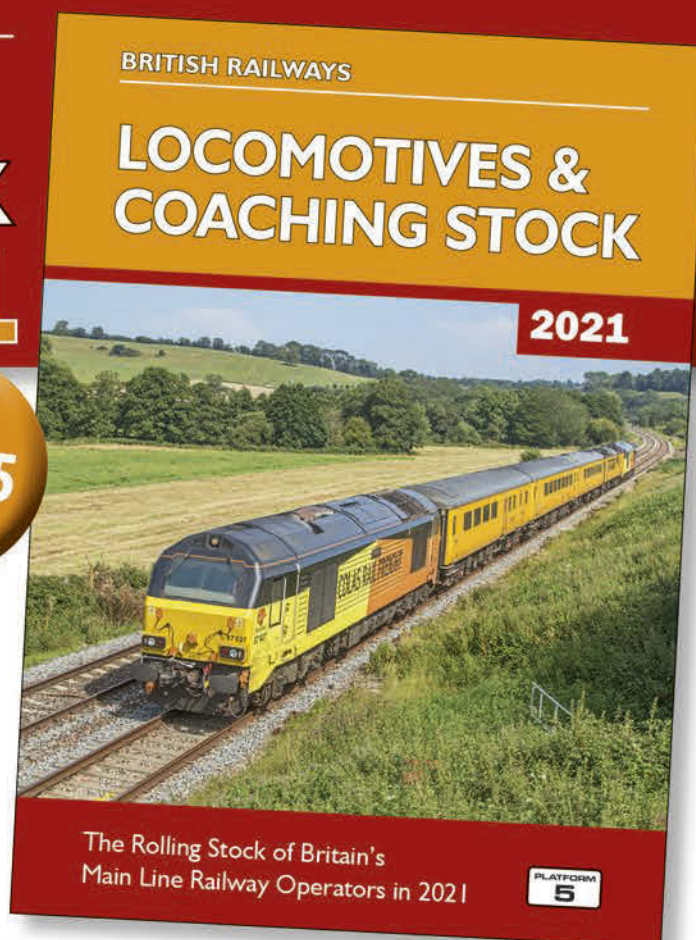
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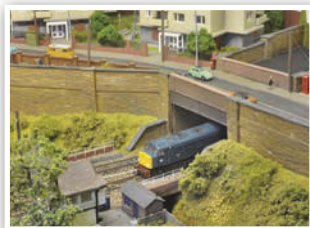
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**The 'Westerns' in the
1970s**
by Graham Callaway



The decline of coal traffic on Britain's railways in the last few years has been rapid and dramatic, with once busy locations losing much of their railway importance. To commemorate the end of this traffic David Hayes looks at one such power station at Ironbridge in Shropshire in the years until the mid 1980s; the later years will be covered in the next issue.

In his article 'The Wandering Weedkiller' Keith Widdowson recalls a day chasing and photographing a Class 20 hauled weedkiller train around Kent in spring 1989.

Class 40s had a great following among enthusiasts in their later years in the 1980s and Steve Ireland, like many others, set out to photograph them; here we present just a few of his many images of the class.

The Swiss railways have always been at the forefront of electric locomotive developments. Colin Boocock begins an examination of the post war electrics starting with the 1944 built BLS Bo-Bos from which all modern electric locomotives have been derived.

Gavin Morrison's multi-part photo study of the Manchester to Leeds Trans Pennine main line concludes with the section between Huddersfield and Leeds.

Jeff Nicholls takes us on another of his family holidays to Scotland when he combined visits to depots in Motherwell and Glasgow with trips around Fort William and Oban.

The editor spent many happy hours watching trains at Guide Bridge in the early 1960s but missed out on visits there in later years. Ray Briscall's feature describes this Manchester junction during the late 1970s when the Woodhead line was still busy.

Chasing after the 'Westerns' in the 1970s became a passion for Graham Callaway and his friend as they attempted to see as many of the class as they could before withdrawal. He tells his engaging story with its successes and disappointments.

In TRACTION MODELLING the featured layout is Steve Farmer's layout Lymebrook Yard in N Gauge and is a compact layout set in Staffordshire in the 1970s. There is plenty of railway action and some delightful urban scenes.

Stephen



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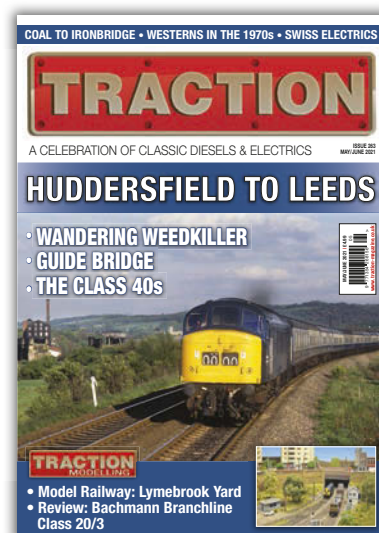
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On 13th May 1976 45073 heads the 17:10 Newcastle to Liverpool at Heaton Lodge Junction.
GAVIN MORRISON



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Unknown date in 1971: Two tone green Brush Type 4 No. 1823 (later 47342) takes the Cannock Road spur from Bushbury Junction with a loaded 6L64 MGR for Ironbridge. The letter 'L' in the headcode display implies that this was running as a special to the power station, possibly laden with coal from Littleton or North Staffordshire. SIMON DEWEY



Ironbridge Coal: Part 1

David J. Hayes looks back at the coal and rail freight operations associated with this Shropshire power station before its closure in late 2015

As has often been the case, what was once an everyday sight can quickly be consigned to history and become nothing more than a distant memory. Who would have thought that the UK would be devoid of a nationwide wagonload network or that the once familiar sight of block trainload deliveries of coal to major power stations using the efficient air braked Merry-Go-Round (MGR) concept introduced in the mid 1960s would now, some 55 years later, be an almost extinct species. The MGR type of operation has suffered from the relentless reduction in recent years of coal fired power stations and the switch from burning coal to biomass fuel (wooden pellets) by the few that still survive.

One power plant that burned coal for decades and then switched to a more environmentally friendly renewable fuel source before closing was Ironbridge Power Station in Shropshire. Although small in comparison to the likes of Drax and Ratcliffe, Ironbridge still consumed vast quantities of coal, which was conveyed using the MGR delivery method. Such

workings were once an everyday part of the rail freight scene in the West Midlands, especially in the Wolverhampton area where the majority of workings to and from the power station could be seen.

The accompanying maps show the railways around Wolverhampton and the wider area as of the mid 1970s, and also pinpoint some of the more localised coal sources to be found in the region at that time, the majority of which dispatched MGR coal services to Ironbridge. Some of these continued to supply the power station throughout the 1980s and well into the 1990s.

Ironbridge Power Station: a brief history

Situated on the banks of the River Severn in the beautiful and historical surroundings of the Ironbridge Gorge, Ironbridge Power Station (also known as Buildwas Power Station) dated from the early 1930s when Ironbridge 'A' was officially opened in October 1932. This was followed almost 37 years later by the opening of Ironbridge 'B' in June 1969. Ironbridge 'A' reached

its maximum output capacity of 200 megawatts (MW) in 1939, Ironbridge 'B' achieving its maximum potential of 1,000 MW in February 1970.

Ironbridge 'A' was partially closed in October 1980 and ceased generating completely the following year. Demolition commenced in 1983, although part of the site was granted 'listed' status and thus saved. Ironbridge 'B' continued generating up until 20th November 2015 when it then closed, having reached its maximum limit of power generation as stipulated under a European Union directive. Decommissioning commenced soon after, with initial demolition works taking place in the summer of 2017 and the cooling towers were brought down at 11am on Friday 6th December 2019.

During its 83 years of power generation, Ironbridge Power Station went through various ownerships. Not surprisingly, railborne coal delivery patterns also varied considerably over the years, the power station consuming coal from various English, Welsh and Scottish sources, and later from cheaper foreign import sources.

Circa late 1970s: 47214 approaches Cannock Road Junction having descended from Stafford Road Junction with an empty working from Ironbridge formed of 25 HAA hoppers. The MGR will soon set back onto the lines visible to the right where the locomotive will run round before heading northwards via Bushbury Junction. The line the train has traversed was once of mixed gauge construction, allowing broad gauge trains to access the former mixed gauge GWR goods facilities at Victoria Basin/Herbert Street (see map), which opened in 1858 to facilitate the trans shipment of goods between the two gauges (Wolverhampton was once the northernmost limit of the GWR's broad gauge empire). Herbert Street Goods was rebuilt in 1930 and operated by National Carriers from 1967, but closed in March 1972. SIMON DEWEY



Unlike some of the larger UK power stations, where a large 'balloon' loop of track(s) allowed for MGR trains to circumnavigate the power station whilst discharging their payload of coal, which was then either conveyor-belted direct to the power plant for burning or to a nearby stockpile, the trackwork configuration at Ironbridge dictated otherwise and locomotives had to run round their trains first prior to unloading.

This article covers the 45 year period from 1970 to 2015 and, in addition to looking in detail at coal operations and touching on oil and biomass traffics delivered to the power station, documents rail activity associated with Ironbridge during the British Rail corporate blue era and the various pre- and post-BR privatisation freight operating sectors and organisations.

Ironbridge coal supply: the 1970s

Ironbridge was supplied throughout the 1970s by MGR trains with coal sourced both locally and from a little further afield. The majority were powered by slow speed control equipped Class 47s, although members of the Class 50 fleet were no strangers on Ironbridge MGR turns from the North Staffordshire area and were thus a familiar sight on the West Coast Main Line (WCML) through Stafford during the mid 1970s, as mentioned and illustrated in the editor's 'Fifteen hours at Stafford in 1975' article in TRACTION 259.



October 1982: An unknown Class 47 performing the empty MGR reversal manoeuvre at Cannock Road Junction shows to good effect the rationalised track layout, which dates from January 1970 (there were once extensive sidings in this area for the stabling of empty coaching stock). This was once the approach to Wolverhampton Low Level station for trains from the Shrewsbury and Stafford directions. The line continuing straight ahead and curving round to the left was once part of the GWR main line from London Paddington to Birkenhead Woodside. SIMON DEWEY



March 1983: A loaded and empty Ironbridge MGR 'meet' at Cannock Road Junction. Having run round its train on the Bushbury spur, 47302 propels its loaded MGR into the Cannock Road head shunt (once the main line approach to Low Level station). It will then draw forward and pass 47298, and its empty MGR, as it ascends the incline to Stafford Road Junction where it will join the Wolverhampton to Shrewsbury main line. Once clear, 47298 will then perform a similar sequence of reversal and run round manoeuvres before heading northwards via Bushbury Junction. THE LATE BRIAN ROBBINS (SIMON DEWEY COLLECTION)

As will be seen from the map, the nearest source of coal supply to the power station at that time was Granville colliery, near Donnington, which was reached by a branch line from Stafford Junction, near Wellington. This branch was once part of a through route to Stafford via Gnosall (closed to passenger services in September 1964), but is still in use today to access Telford International Railfreight Park.

Loaded MGR trains from Granville to Ironbridge were required to run round at Wellington and again at Madeley Junction (not to be confused with Madeley Junction between Crewe and Stafford on the WCML), where the branch to the power station diverges from the Shrewsbury to Wolverhampton line. Other 'local' sources of MGR coal supply to Ironbridge were workings from the Cannock area, such as Essington Wood, Littleton and West Cannock.

Those MGRs arriving at Ironbridge Power Station from a little further afield at this time included services from Cockshute Sidings (staging point), Daw Mill, Ford Green (Norton colliery), Kidsgrove (Park Farm opencast disposal point), Kingsbury

Sidings (believed to be coal mainly from Baddesley), Silverdale and Trentham; Trentham was the coal output and rail forwarding point for Florence and Hem Heath collieries, which were connected underground.

The empty MGR formations from Ironbridge didn't always return direct to a colliery or opencast disposal point for reloading and were sometimes sent to such locations as Crewe, Longport, Madeley Chord, Pratts Sidings, Stoke and Washwood Heath until next required.

Table 1 details the coal delivery train plan for Ironbridge as of October 1975, equating to 64 loaded MGRs per week (12 most weekdays, 11 on a Monday and five on a Saturday morning). All services at this time were diagrammed for Class 47 haulage, the exception being the 6G44 oil train from Stanlow, which was booked for a Class 40. How many of these trains actually ran in reality is debatable, though, as all the services were designated as 'conditional' and therefore subject to short notice cancellation on a day to day basis, based on whether there was sufficient demand or traffic available for movement. Power

station coal delivery schedules could be very unpredictable and were thus amended accordingly on a weekly basis despite what was shown in the working time table and local trip booklets.

The Class 47s used for certain Ironbridge workings were also utilised for MGR turns to Rugeley Power Station. For example, the locomotive allocated to the afternoon 6T94 MGR trip from Littleton had earlier worked a morning 6T94 MGR duty from Littleton to Rugeley. Likewise, the Class 47 used for the afternoon 6T95 MGR arrival from Daw Mill had earlier found employment on a Cannock line 6T95 MGR turn from Hednesford to Rugeley. After berthing the empty 30 wagon MGR set at West Cannock colliery, the Brush 4 then ran light engine to Daw Mill to take up the Ironbridge duty.

With the exception of those MGRs from Daw Mill and Granville, all the other MGR services were required to run round in the Wolverhampton area. Although some of these run rounds were booked to take place at Wolverhampton High Level station, the majority were carried out at Cannock Road Junction on what was once the former Great Western Railway route

leading into Wolverhampton Low Level station (see Wolverhampton area map). Likewise, although the majority of MGR empties from Ironbridge ran round at Cannock Road Junction, some ran round at Wolverhampton High Level.

The MGR lengths were somewhat short compared to later years when formations to be found on Ironbridge duties could contain up to 32 or 34 hopper wagons (workings from Daw Mill and Granville had actually been formed of 30 hopper wagons in the early 1970s). Although loaded trains were designated as Class 6, back then this didn't always relate to 60 mph operation. Despite being air braked, the MGR workings would have been restricted to a maximum of 45 mph when loaded.

New Type 5 workhorse

During late summer 1977, a 'taster' of the future motive power for Ironbridge MGR operations was provided by the appearance of the new Type 5 Class 56. No. 56033 had been based at Saltley since mid August for driver familiarisation and was deployed on out and back MGR workings from Kingsbury Sidings to Ironbridge.

The favoured diagram for these driver training runs appears to have been 6G43, the 09:05 SSuX from Kingsbury and the 12:45 return. The same Class 6 headcode was used for both the loaded and return empties, and also for a late afternoon out and back weekday departure to the power station, although it's not certain as to whether the afternoon duty was used for driver training purposes and, therefore, may have been worked by a Class 47 locomotive (see Table 2 for full diagram details). Perhaps a former Saltley driver could enlighten us?

Based on a picture of the 12:45 return working published in the March 1978 issue of 'The Railway Magazine', the MGR wagon set appears to have been rather short at just 20 HAA hoppers. The train, hauled by 56033, was seen coming off the Ironbridge branch at Madeley Junction on Bank Holiday Monday 29th August 1977. Another picture taken two days later of the same loco and train crossing the River Severn on the Albert Edward Bridge appears in the November 1977 issue of the same publication.

By the late 1970s, Littleton colliery, reached by a branch from Littleton Sidings situated on the Stafford to Wolverhampton main line near Penkridge, had become a major source of coal supply for Ironbridge Power Station. By October 1978 there were eight loaded MGR departures scheduled to run each weekday to Ironbridge, all of which were booked for Class 47 haulage and each of which was booked for a 20 minute run round at Cannock Road

(RIGHT) Tuesday 14th April 1987: 58044 is halfway through unloading its delivery of Baddesley coal at Ironbridge. The photo was taken with permission and courtesy of the CEGB. JOHN WHITEHOUSE

TABLE 1: Ironbridge Delivery Details: October 1975

Train	Service Details	Arr	Length
6G38	04:06 MSuX from Stafford *	06:00	25
6T23	04:52 SuX from Daw Mill	07:00	26
6G44	03:55 MSuX from Stanlow	07:35	Oil Train
6T21	06:15 SuX from Littleton #	08:01	25
6G38	07:00 SSuX from Silverdale #	10:01	25
6G42	07:13 SO from Ford Green *	10:31	25
6T22	09:35 SuX from Granville	11:22	27
6G42	09:10 SSuX from Ford Green #	12:43	25
6G38	11:16 SSuX from Silverdale #	13:50	25
6T95	12:35 SSuX from Daw Mill	14:46	26
6T94	14:40 SSuX from Littleton #	16:10	25
6T23	15:55 SSuX from Daw Mill	18:22	26
6G38	16:08 SSuX from Silverdale #	18:53	25
6G36	17:51 SSuX from Trentham #	20:07	25

* run round at: Wolverhampton High Level station

run round at Cannock Road Junction



Saturday 11th September 1976: Wolverhampton High Level station witnessed a fair amount of Ironbridge MGR activity even at weekends. 47290 eases an empty working from Ironbridge into the station. The Shropshire power plant at this time was booked to receive Saturday coal deliveries from Daw Mill, Ford Green (ex-Norton), Granville, Littleton and Stafford (believed to be ex-Silverdale). Those from Ford Green and Stafford were booked to run round here rather than at Cannock Road. The MGR seen here could well be 6G58, the 08:18 SO empties from Ironbridge to Daw Mill, which was due through Wolverhampton at 09:57. TOM HEAVYSIDE



TABLE 2: 6G43 SSuX Class 56 Driver Training Trip

Location	Arrive	Dep./Pass
Saltley Holding Sidings		08:30*
Kingsbury Sidings	08:45*	09:05
Walsall		09:58
Wolverhampton		10:19
Madeley Junction		10:46
Ironbridge Power Station	11:06	12:45
Madeley Junction		13:07
Wolverhampton		13:42
Walsall		13:58
Kingsbury Sidings	14:50*	15:00*
Saltley Holding Sidings	15:15*	15:45*
Kingsbury Sidings	16:00*	16:30
Walsall		17:14
Wolverhampton		17:36
Madeley Junction		18:03
Ironbridge Power Station	18:23	20:15
Madeley Junction		20:37
Wolverhampton		21:04
Walsall		21:20
Kingsbury Sidings	22:05*	22:15*
Saltley Holding Sidings	22:30*	

Note: *Arrival times at Kingsbury Sidings and timings for 'Light Engine' movements to and from Saltley depot are an approximation (it is assumed that the loco ran to Saltley after working the morning Ironbridge diagram). It is possible that the Class 56 used for the morning driver training runs was replaced with a Class 47 loco for the late afternoon 16:30 departure to Ironbridge Power Station. Separate tripping resources were used for dealing with empty wagons for loading at Baddesley and Birch Coppice, and for returning them loaded back to Kingsbury Sidings.

Junction (see Table 3).

The Class 47 for the 6T21 diagram was also utilised to work a MGR turn from Granville to Ironbridge, while the Class 47 allocated to the 6T23 duties was utilised to trip Speedlink deliveries of confectionery from Bescot to the Rowntree's distribution warehouse at Penkridge (connected to the Littleton branch) at the beginning of the day. After fulfilling its booked Ironbridge itinerary and berthing the MGR empties, it returned the empty 'chocolate vans' back to Bescot in the evening in readiness for trunk Speedlink hauling to York.

It appears that faulty green or red carded coal hoppers extracted from MGR sets and left at the power station to await attention, were dealt with by local tripping resources. One such turn utilised the Class 25 locomotive allocated to the Bescot to Wellington trip (T19). The Type 2 would visit Ironbridge to collect any 'green carders' and take them to Bescot from where they would most likely be forwarded to the wagon repair depot at Burton-on-Trent or perhaps to Toton wagon shops. MGR hoppers used on the Ironbridge circuit requiring attention at Burton were previously gathered and forwarded from Wolverhampton's Bushbury Sidings.

The early 1980s scene

By the 1980s, the working time tables and local trip booklets showed Ironbridge MGR duties designated as Class 7 (45 mph) when loaded and as Class 6 (60 mph) when empty, this being based on wagon sets being formed of the standard HAA coal hopper. However, some hopper wagons were later equipped with improved braking and suspension systems, thus permitting them to travel at Class 6 speeds when loaded and empty. These were coded HBA, HDA, HMA and HNA, some of which were fitted with canopies to reduce coal dust 'blow off' during transit.

The source of coal supply for Ironbridge during the early 1980s was pretty much the same as that for the 1970s, although there were some omissions, such as the workings from Kidsgrove (Park Farm), Ford Green (Norton) and Granville, all of which had closed in 1976, 1977 and 1979 respectively. Regular MGR coal deliveries continued to arrive at Ironbridge from Daw Mill, Kingsbury (ex-Baddesley), Littleton, Silverdale and Trentham, and these would soon be joined by MGR coal hauls coming from Barrow Hill. Those from Barrow Hill travelled from Wichnor Junction by way of the Lichfield to Walsall freight only line, which closed as a through route in March 1984 (see TRACTION 233 & 234).

Littleton was still an important coal source and, as of May 1983, was scheduled to dispatch 20 loaded MGR trains each week to Ironbridge. In addition, there were 15 each booked from Kingsbury and Silverdale, ten each from Barrow Hill and Daw Mill, and five from Trentham, equating

TABLE 3: Littleton Departure Details: October 1978. All SSuX

Train	Dep.	Cannock Rd Jn.	Arr. Ironbridge
6T21	06:20	06:45-07:05	08:00
6T22	07:05	07:30-07:50	08:51
6T23	08:45	09:09-09:29	10:15
6T24	09:45	10:10-10:30	11:20
6T20	11:10	11:35-11:55	13:00
6T23	13:37	14:00-14:20	15:27
6T24	15:05	15:30-15:50	17:10
6T20	16:40	17:05-17:25	18:13

to 75 loaded MGR trains per week. But, again, how many of these actually ran in reality is another matter.

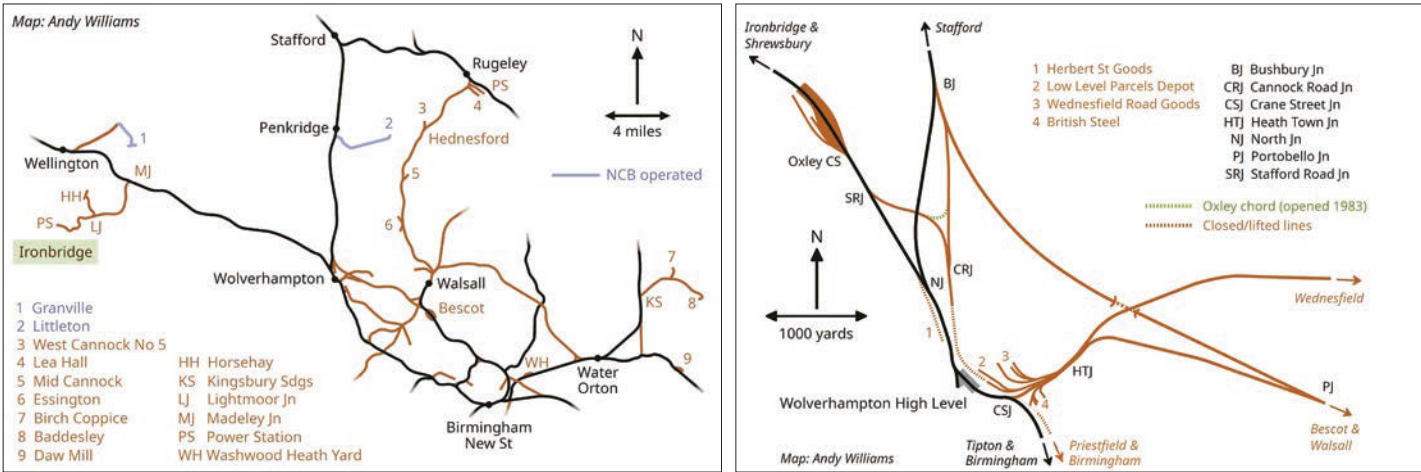
Those from Littleton and North Staffordshire, and their return counterparts, were to benefit from the opening of the Oxley Chord in August 1983 (see Wolverhampton area map), which eliminated time consuming run rounds and reversals at Cannock Road Junction. In later years, the Oxley Chord would be used by coal trains from the Chatterley Valley opencast disposal point in the Potteries and imports from Liverpool Docks. Further coal workings using the Oxley Chord would later include long hauls from Scotland (imports and opencast) and trains of imported biomass from Liverpool. Block oil train deliveries from Stanlow to Ironbridge also benefitted from this new piece of track, and could now run by way of Stafford rather than via Shrewsbury, thus avoiding a run round at Madeley Junction in order to access the Ironbridge branch.

By the mid 1980s, the dominant sight of Class 47s on Ironbridge MGR duties had been broken and were now joined by Classes 20 (working in pairs), 56 and

TABLE 4: Ironbridge MGR Trip T67 SSuX (Class 56 or 58): May 1986

Location	Arr.	Dep/ Pass	Headcode
Saltley Holding Sidings		14:35	0T67
Kingsbury Sidings	14:50	15:10	6T67
Baddesley Colliery	15:40	16:40	7T67
Kingsbury		17:10	
Walsall		18:05	
Wolverhampton		18:25	
Madeley Junction		18:55	
Ironbridge Power Station	19:15	20:30	6T67
Madeley Junction		20:50	
Wolverhampton		21:15	
Walsall		21:35	
Kingsbury Sidings	22:25	22:40	0Z00
Saltley Holding Sidings	22:55		

Note: Some passing times are an approximation



Wednesday 12th November 1980: 47228 takes an MGR onto the Ironbridge branch at Madeley Junction as a 'Peak' hauled cement train, possibly a special from Penyffordd to Birmingham Curzon Street, approaches from the Shrewsbury direction. MIKE HEMMING



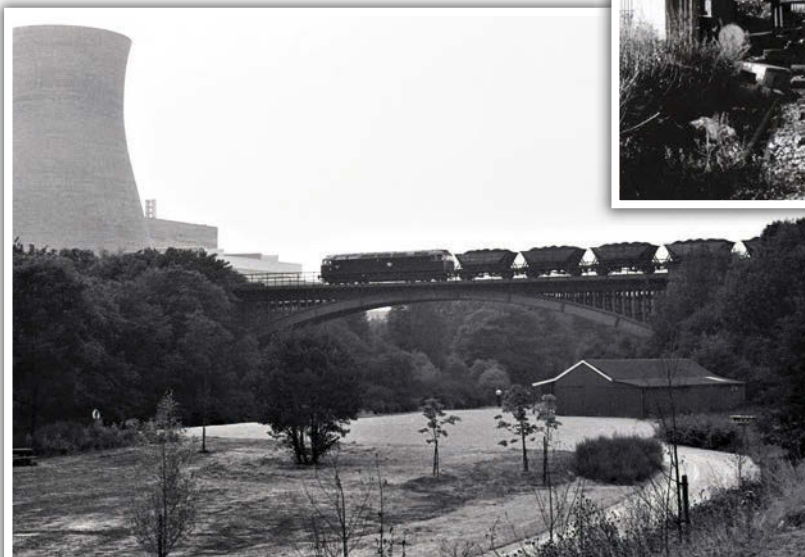
58. Table 4 details a typical 'out and back' Ironbridge MGR trip from the Warwickshire area booked for Type 5 power as of May 1986. By this date, the loaded train started from Baddesley rather than Kingsbury Sidings, the locomotive being used to work the empties to Baddesley to be loaded (this was a movement previously performed by separate tripping resources allocated to the branch serving Baddesley and Birch Coppice).

Article to be continued in the next issue of TRACTION

(RIGHT) Thursday 29th September 1977: Saltley's Class 56 driver training loco, 56033, emerges from the short tunnel section just north of Walsall station with what is believed to be 6G43, the 12:45 SSuX MGR empties from Ironbridge to Kingsbury Sidings. The service is seen approaching Ryecroft Junction where it was due to pass at about 14:00 (see Table 2 for full timing schedule). JOHN WHITEHOUSE



(RIGHT) Saturday 6th December 1980: Sometimes, empty MGR workings from Ironbridge Power Station that would have normally travelled via Portobello Junction after passing through Wolverhampton station were known to be diverted along the Stour Valley main line route. Such a working is seen here passing Watery Lane Shunt Frame, Tipton, behind an unrecorded Class 56. It is quite possible the service may have even been routed through Birmingham New Street if heading for Baddesley, Daw Mill, Kingsbury or Washwood Heath. DAVID WILSON



(LEFT) Unknown date in 1976: A Class 47 hauls a loaded MGR across the River Severn on the 1863 dated Albert Edward Bridge on the approach to Ironbridge Power Station. MIKE HEMMING

(RIGHT) Circa 1985: By the mid 1980s, pairs of Class 20s could be found on Ironbridge MGR duties. Such a pairing, led by 20128, is about to descend from Stafford Road Junction to Bushbury Junction via the Oxley Chord (opened August 1983) with empties from the power station. SIMON DEWEY





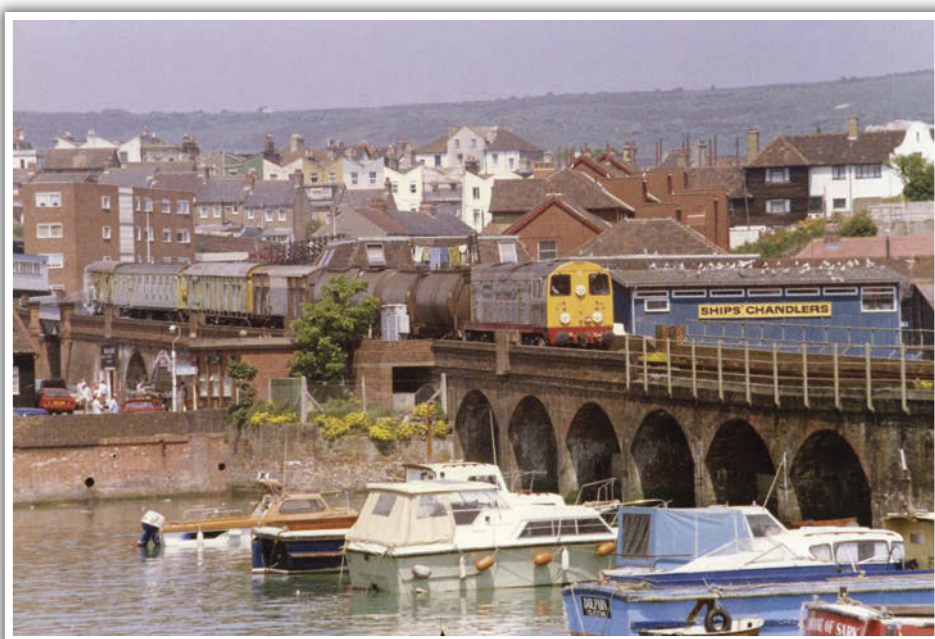
Taken from the rooftop of our Waterloo office, 20904 leads 20901 out of Charing Cross on Monday 22nd May 1989. The scene has somewhat changed over the years with the 'bullring' being infilled by a multiplex cinema. PAUL WINTER

The Wandering Weedkiller

Keith Widdowson raced around Kent on a photographic mission in order to record the visit of the Normix-Chipman weedkilling train in the May of 1989.

Six Class 20 English Electric were acquired by Hunslet-Barclay earlier that year and were immediately contracted out to Chipman's for use throughout Britain on weedkilling duties. An eye pleasing livery of two tone grey with a red sole box line was bestowed on the selected six Bo-Bos, which were renumbered 20901-6. The two involved on that May day, I believe it was Tuesday the 23rd, were 20901 (formerly D8041/20041) and 20904 (formerly D8101/20101). I failed to keep any timings but research shows that it may well have started out of the Chipman base at Horsham.

At the time I was the mixed traction diagramming manager at Waterloo – responsible for all non-EMU traction and crewing arrangements throughout the former South Eastern Division of the Southern Region. As the Class 20 drivers had no route knowledge of the lines covered, it fell to my section to diagram the necessary conductor drivers. And, the clerk who actually compiled the duties, suggested that because of the train's



Slowly easing their way over the inner harbour viaduct and swing-bridge into Folkestone Harbour station itself are 20901 (leading) and 20904 (trailing and out of sight). ANDY VEITCH

'unusualness,' he would like to take a day's annual leave and chase the train by road. Liking the idea myself, I also took a day's leave, travelling down by train to his then home station of Gillingham to join him.

The weather was, fortunately, kind to us and, after a glorious ride through the 'Garden of England', we parked near Folkestone Harbor station. Firstly viewing the train from over the harbour itself we then raced, by foot, to a couple of vantage points up the 1 in 30 incline, and what a throaty noise they made on the ascent! After returning to the car we cut across to Shepherds Well station (we had sussed out the limited photographic options open to us prior to embarking on this journey) whilst the train reversed at Dover Western Docks.

The timekeeping, from my recollection, was never more than a few minutes out and so once again, taking advantage of the train's visit and reversal at Sheerness, the final photographic opportunity was taken on the four tracked section between Newington and Rainham. That was it and, with our mission accomplished, Andy dropped me back at Gillingham station for my train home to St Mary Cray. A day with a difference!

Sending the seagulls into orbit, 20904 is seen from Radnor Bridge Road overbridge climbing up the 1 in 30 incline between Folkestone Harbour and Folkestone East. In the days of steam boat trains three R1 tanks had to be used for the climb before they handed over to main line locomotives for the journey to London. KEITH WIDDOWSON



Andy, being more fleet of foot, had made it to a footbridge across the incline nearer Folkestone East carriage sidings and his shot depicts the train breasting the top of the incline. ANDY VEITCH

We now moved to Shepherds Well station where the 20901 leads the train through the station. KEITH WIDDOWSON



Amongst the Kentish orchards, between Newington and Rainham, 20904 now leads, having reversed once more, this time at Sheerness. ANDY VEITCH

On Saturday 27th May the two Class 20s brought the weedkiller train into the central side terminus platforms at London Bridge. It is believed that, after reversal, the train was routed over the South London Line to Victoria. Due to restrictions at the time banning locomotives over the South London Line, two de-icing/sandite units took it forward. The leading unit, 016, was designated as a 'Rail Cleaning Unit' at the time, subsequently being renumbered 930016 and then 930102 before being withdrawn in 2002 and scrapped in 2003. The Class 20s followed the train to Victoria via Crystal Palace to reunite with their train. ANDY VEITCH



The driver of Darlington built 40113 (originally D313) lets its 16 cylinders rip near Prestatyn whilst in charge of 1D32, the 10:29 Birmingham New Street to Holyhead on 4th August 1979. Robert Stephenson and Hawthorn at their works in Darlington constructed the 20 locomotives numbered D305 to D324 in 1960 and 1961 whilst the rest came from English Electric's Vulcan Foundry at Newton-le-Willows. JOHN IRELAND



Class 40s at work

With the rundown of the Class 40s in the late 1970s and early 1980s, Steve Ireland set out to photograph and travel behind these veterans whilst there was still a chance. Railway enthusiasts have a good track record of marking the end of loco class, station or line on BR, a sense of capturing final moments and memories and to be able to look back and say, "I was there." Steve picked up his Pentax camera, driven by the need to track down and record as many Class 40s on film as he could. These pictures represent a small selection of those adventures.



40169 leads 46229 'Duchess of Hamilton' at Long Preston on 17th May 1980, hauling the SLOAs 'The Bristolian' from Bristol to Carnforth. Due to BR imposing a lineside fire precaution between Carnforth and Hellifield, 40169 was attached for this leg to haul the Duchess plus its train. The use of the Class 40 would, no doubt, have caused mixed reactions on the day amongst steam and diesel enthusiasts both on board and at the lineside. JOHN IRELAND



The race is on. Dringhouses in York on 3rd September 1980 sees two differing nose end variants running side by side. 40057 is on the main down with a steel train and 40153 on the Leeds down in charge of the Heysham to Haverton Hill chemical tankers. JOHN IRELAND



40166 from Longsight depot was amongst the final few 'forties' with a working headcode panel and, over 5 years after their use was abolished on BR, we see the former D366 whistling past Wortley near Leeds displaying the 1E34 headcode of the 08:53 Blackpool North to Newcastle on 29th August 1981. The story goes that the headcode was wound up correctly at Manchester Victoria by a very obliging driver. Well done sir, whoever you were! STEVE IRELAND



Wigan's 40093 heads east at Chinley on the morning of 13th April 1982. The wagons behind are the well known ICI limestone steel hoppers dating from 1935 which, for over 60 years, plied their trade between Tunstead Quarries near Peak Forest in Derbyshire and the soda ash plant in Northwich, Cheshire. Hauled originally by 8F steam engines and then by Class 40s, 45s, 37s, 20s and 25s, their vacuum only braking system was not compatible with the new Class 60s when introduced in the early 1990s and so their days were numbered. JOHN IRELAND

40129 enters Bolton from the Manchester direction on the evening of 31st May 1982. The visit of the Pope to Manchester saw an intensive service on the Blackpool to Manchester route that day with most trains going over to locomotive haulage by Classes 25 and 40. Inevitably these trains were nicknamed 'Popexes'. STEVE IRELAND



40187 is bathed in late evening sunlight on 21st July 1982 as it takes the curve towards Crigglestone West at Healey Mills in charge of a heavily loaded train of scrap in 16 ton mineral wagons and even more fully filled coal hoppers plus brake van. 40187 was withdrawn a month later and disposed of by BREL Doncaster. JOHN IRELAND



Storm clouds are brewing over Manchester as 40060 pulls up for a signal check just west of Victoria station heading a tanker train for Stanlow on 11th October 1984. This loco had an extended life as a departmental locomotive and was later renumbered 97405. Its luck ran out when it became the only one of the four departmental Class 40s not to enter preservation. The chimney of Boddington's brewery stands prominent. JOHN IRELAND



40015 heads a delightful mixed freight northbound at Moore near Warrington on the evening of 19th September 1983. Named 'Aquitania' in June 1962, the plates had been removed over ten years earlier but the back lighting in this end of summer scene shows the replacement studs in the original name location. STEVE IRELAND

The end is nigh at Derby as the last ever passenger run of 40044, and one of the last Class 40 service moves on BR, sees enthusiasts cram into the first few coaches of the 16:16 Sundays only Birmingham New Street to York on a cold and snowy 20th January 1985. After arrival at York it then ran light engine to Crewe Traction Maintenance Depot and was switched off forever. STEVE IRELAND



Where it all started! In 1944, the Bern Lötschberg Simplon Bahn (BLS) launched the Ae4/4 class of eight Bo-Bo electric locomotives, each of which was able to deliver 1,000 bhp through each of its four axles. They lasted a long time, which enabled No. 258 to be photographed sixty years later on 26th March 2004 heading a local train at Spiez, bound for Zweisimmen. The class was eliminated later that year, apart from one museum locomotive (No. 251) that is retained for special events. COLIN BOOCOOCK



The development of post war Swiss electrics: Part 1

A revolutionary locomotive built in 1944 paved the way for almost all the electric locomotives built in Switzerland and the rest of Europe ever since. Colin Boocock relates their history.

As a young enthusiast I read about it in my teens when I bought a copy of Cecil J Allen's book 'The amazing railways of Switzerland'. In 1944, the Bern Lötschberg Simplon Bahn (BLS for short) had placed in service the world's first electric locomotives that sustained 1,000 bhp per axle and with all axles driven by independent, bogie mounted ac traction motors. The class was of eight locomotives of just 80 tonnes weight, something which seemed amazing at the time but is commonplace today. They were classified according to the Swiss system as Ae4/4, the first '4' registering the total number of axles, and the second '4' the number of motored axles. The BLS gave them the numbers 251 to 258.

The BLS railway serves a north-south axis through Switzerland and crosses the Alps by means of a steeply graded

main line that reaches its peak inside the Lötschberg tunnel. In more recent decades a base tunnel takes most of the fastest and heaviest traffic, but the spectacular railway through the original tunnel is still open for local trains and for car-carriers that enable drivers to avoid driving over the pass which often snows up in winter. At 4,000bhp, these eight Bo-Bos outclassed all the rigid-wheelbased or articulated older electric locomotives that had come before them. They were initially used on express passenger trains that had originated at Bern or Basel, or even in Germany, and were heading for the Simplon tunnel which would take them into Italy.

Other Bo-Bo types followed, and we shall explore them in this article, but while looking at these eight historic locomotives I need to refer to one development that came in 1959. Three double-locomotives

of the same type, practically two Ae4/4s coupled back-to-back with the inner cabs eliminated, were built specifically for heavy freight, classified Ae8/8, and 8,000 bhp output. Three such pairs were built at first, numbered 271 to 273. So useful were they that, in 1965, Nos 253 to 256 of the original Ae4/4s were rebuilt to Ae8/8s and given the numbers 274 and 275. The Ae8/8s lasted on freight work until 1996, but the original Bo-Bos were dogged survivors, the last keeping going until 2004, a life span of sixty years!

By the time the Ae8/8s were all in service, the BLS was receiving its new Re4/4 class, but before we get that far ahead we must check on what the Swiss Federal Railways (SBB/CFF/FFS) had been doing.

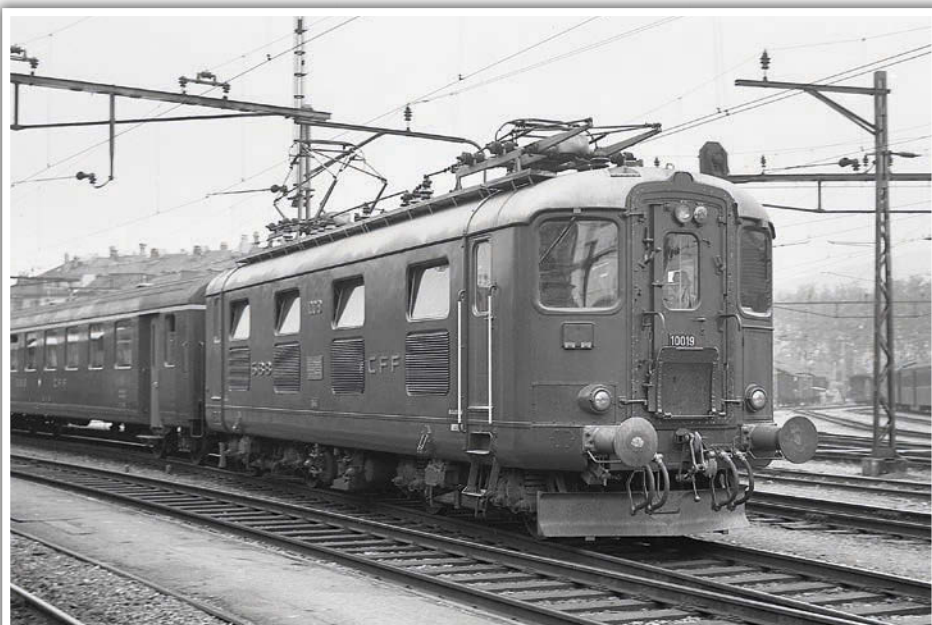
The SBB had a Re4/4 class which it introduced in 1948. The classification 'A'

(RIGHT) The SBB's first Bo-Bo class was the Re4/4 type of which 10019 was seen to be active at Luzern on 27th August 1959. The class was for push-pull passenger services, for which gangwayed cab ends were fitted. They were lightweight machines of 2,500 bhp. COLIN BOOCCOCK

was supposed to cover locomotives with a top speed between 85 to 110 km/h; above 110 km/h the class prefix became 'R'. There was at least one major exception to this. The letter 'e' indicated an electric locomotive. Subsequent developments added a Roman numeral to the class name, such as Re4/4II. The original SBB Re4/4 class became what we knew more recently as Re4/4I. The first batch of 26 locomotives had gangway ends, cabs at each end and were intended for push-pull working of passenger services. I saw one or two on my earliest trips to Switzerland in 1956 and 1959. The class was said to be a development of class RFe4/4 motor luggage vans, and as such they were not hugely powerful, producing just 2,500 bhp, but as they weighed only 57 tonnes that was as much as they could sensibly handle. The second batch of these Re4/4s had plain cab fronts and were intended for fast lightweight express trains such as the early Trans Europ Express services (TEE). While most of the class were painted the dark SBB overall green, 10033, 10034, 10046 and 10050 were repainted in 1972 in the TEE colours of red lower panels and cream upper panels; they looked smart, but eventually reverted back to dark green. Examples of these equally long-lived locomotives finished up as carriage shunting and transfer locomotives before being withdrawn as late as 2004.

Without exception, all the locomotives described in this article were built by the Swiss Locomotive Works (SLM) at Winterthur. Supplies of electrical equipment were shared between Brown Boverie (BBC), Oerlikon (MFO) and Sécheron (SAAS); all are now parts of BBC.

Following on from Bo-Bos, the next logical step for SBB was to grow the size and produce a locomotive with six powered axles. The Ae6/6 of 1952 was just that, a Co-Co weighing in at 125 tonnes and producing nearly 6,000 bhp. These broke the rule about maximum speed because their class number Ae6/6 suggests their top speed to be 110km/h or less whereas in fact they were rated for 125km/h. These locomotives were intended for international express trains over the steeply graded Gotthard route, a line that paralleled the BLS and had similar characteristics but which was owned by SBB. The Ae6/6s were highly regarded and all carried the names and crests of towns and villages in Switzerland. They started life in dark green, later receiving red which suited them well, though the last few to survive to the 21st century to handle local freight trains had reverted to green again. The last went in 2013 after a life of around sixty years!



The second batch of Re4/4s, that later became reclassified Re4/4I to differentiate them from later locomotives, were built with full width cabs. Some were used on Trans Europ Express (TEE) services, which No. 10042 was certainly not working when seen on a local train at Neuchatel in the 1980s. DAVID FORD



No. 10050 was one of the Re 4/4's that was repainted in the red and cream TEE colours. It is seen here at Delémont on 7th July 1993 working a local train. PHIL RICHARDS Creative Commons



The new 'grand dame' of SBB in the 1950s was the Class Ae6/6 Co-Co design which was used on heavy international passenger services across the Alps. Dark green No. 11422, which carried the name Vaud and the town's crest, was being released from its train at Luzern on 27th August 1959. COLIN BOOCOCK



In 1959, the BLS received four giant double Bo-Bo locomotives for the Lötschberg line's heaviest freight trains. These were basically each a pair of back-to-back Ae4/4s with the wheel arrangement Bo-Bo+Bo-Bo. No. 274 is seen near Kumm, north of Spiez, on 11th July 1984. PETER ALDER



Ae6/6 11424 is seen with northbound freight at Flüelen on the 17th August 1988. Note that the aluminium trim has been removed due to corrosion. DAVID FORD



Seen repainted in the red livery adopted by SBB in the 1980s, Ae6/6 No. 11424 brings a northbound freight through Flüelen on the 17th August 1988. DAVID FORD

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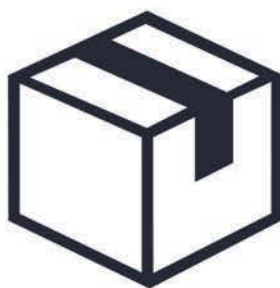
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TRACTION MODELLING

Photography by Tony Wright



in partnership with **BRM**

LYMEBROOK YARD

Making excellent use of height, Steve Farmer's compact layout imagines a small suburban station on the North Stafford Railway line.

Steve's 'history' of the line, which is set in the late-1970s, is that the North Stafford Railway built a direct line from Newcastle under Lyme to Crewe in the mid-19th Century. En route, a small suburban station was built, which the model now represents. Originally double track, the section beyond to Crewe was singled at the beginning of the British Rail era.

Train movements are all replicated from Steve's memory. The yard sees trip workings and various traffic flows. The trip locomotive does its own shunting, there being no yard pilot present. Passenger workings and through-freights are typical of the period, though several other favourite locomotives and units are run, which would have been less likely – modeller's licence!

A sheet of the right-sized MDF was cut into three sections. Normal ply was used for the layout ends, and the flexible MDF was fixed in place. Battens were added underneath for support and pieces were cut out to accommodate overbridges and raised sections, some of which are not permanently fixed to allow for maintenance and stock-retrieval, if necessary. They're attached to the layout using wing nuts and bolts set into wooden blocks.

The overbridges and retaining walls at either end are deliberately different – stone for one end and brick for the other, both made more natural with careful weathering.

For the track, Peco Code 55 with Electrofrog small radius points at the front of the layout were used. Dropper wires were soldered on the rails and wired through the frog to avoid relying on blade contact. The lines were brought to the rear on Peco Code 80 Setrack 9in curves, out of sight.

The fiddleyard can hold nine trains ranging in length from a single-car DMU to a Class 44 and nine 16T mineral wagons and a brake van, or any other trains of similar length.

All the motive power is drawn from Steve's collection of blue-era stock and features Classes 03, 20, 25, 31, 37, 46, 47 locomotives, and Classes 101, 104, 105, 108, 120, 121, and 128 for the DMUs.

Rolling stock is from various Dapol, Peco and Farish ranges, plus many kits from N gauge stock suppliers. All have been weathered and toned-down.

After the track was laid, Peco platform walls were fixed in position, all requiring curving to match the track radii. Slater's Plastikard was used to make the top, Steve then covered it with a further layer of embossed paving Plastikard.

The backscene was painted white and then a blue wash applied and dabbed off with a tissue to give a cloud effect. The ground around the brook and at the front was made from polystyrene blocks, glued, hacked to shape, then covered in filler, painted green and finished with static grass from the Greenscene range.

Roads are thin card glued onto the ply base and painted with enamels. The road markings were achieved by employing water slide transfer sheet white lining, cut to length and applied individually.



Greenery in the form of bushes and trees is a mixture of 4D Modelshop and Woodland Scenics for the trees, and various types of bushes and undergrowth for the other land cover. The brook was represented by painting the base brown with green streaks. It was then built up with layer upon layer of gloss varnish, allowing each layer to dry before applying the next and continuing, even after the layout was finished, to keep refreshing the look.

The blocks of flats are modified Kibri kits that have had their height reduced and a scratch-built roof added. The bottom storey on the kit is a shop with clear windows; these were replaced with embossed Plastikard to represent concrete cladding, aiming to represent a typical 1970s block of flats and an open space that authorities provided around them.

The front of the large goods warehouse was scratch-built from Plastikard, with etched windows supplied by N Brass Locomotive. The stone exterior

complemented the retaining walls. The shops to the right are a Walthers' kit.

Bachmann Scenecraft terraced house fronts were used for the dwellings across from the station entrance, but they were raised on a balsa block to give height and allow for front gardens leading down to the road. The gardens were detailed using fine grass flock from Woodland Scenics.

The station building is two Bachmann structures cut, shut and finished off as a small suburban station. The access steps leading down from the road were taken from a Gaugemaster ex-Kestrel kit with a scratch-built base.

Vehicles are from Oxford Diecast, Base Toys and Parker kits, all have been matt-varnished to remove their gloss sheen.

At a show, passenger trains vary from DMUs to locomotive-hauled three-coach sets. Freight-wise, there's usually a through Wagonload working, hauled by a Class 44, plus a Speedlink working and a few trip diagrams.



Bachmann Branchline Class 20/3



With their expanding and diversifying operations after 1998, Direct Rail Services (DRS) had 10 Class 20s, later expanding to a fleet of 15, rebuilt by Wabtec between 1998 and 1999. These locomotives were distinguishable from their predecessors by the plated nose and cab ends with reinforced cab windows with Wipac lights fitted, plus increased fuel capacity with tanks in front of the cab and in the nose. Multiple Unit connectors were added to the ends allowing operation with other Class 20/3 locomotives. The fleet of Class 20/3s owned by DRS saw frequent work across Britain in pairs (or with Class 37s) on nuclear flask trains, the company's speciality. DRS supplied Class 20s for use with the Rail Head Treatment Train in winter, too.

It's almost 20 years since Bachmann introduced a Class 20 to its Branchline range,

which became its first sound-fitted model in 2005, but, make no mistake, this model isn't a reworking of that well-regarded model, but a newly-tooled one with a view to delivering the features that modellers of today wish to see in a new product. The old benefits of a beautifully-running model that looked the part haven't been lost, and the model has performed to the same standards as my old fleet on test.

Starting inside-out, the model has an all-new chassis block, running plate and bogies. The five-pole motor has been laid lower to facilitate more space at the top for the circuitry. The board has a pre-fitted speaker, which sits behind the engine-room grilles, plug and socket connections for all wiring and a PluX22 decoder socket meaning that sound-fitting will be as simple as it can be once the two screws securing the body have been removed.

Beneath the chassis, the white tanks can be removed for analogue users to configure lighting modes via DIP switches. Operation of the cab light, which is a yellow LED with directional function, can be switched as day or night mode with a

purser white light.

The new body tooling covers the cab-roof radio pods and ventilators, and the distinctive cab and nose ends of a Class 20/3 with the removal of doors and headcode panels to give a clean look with the plated surface. At the base of the cab end and above the bufferbeam at the nose end are the prominent Wipac light boxes with headlights and taillights. Centrally-mounted at both ends are the multiple working connector sockets in their downward-facing mountings and, at a higher level, the forward running light. The grille vent at the top of the cab end doesn't seem to quite match the appearance of the prototype images that I have seen of 20306. The aforementioned cab lighting really shows up the level of internal detail. To fit a crew, the cab interior can be removed from inside but it requires a little persuasion. The detailing pack includes the usual bufferbeam piping, cosmetic screw-link couplings and snowploughs. The sprung metal buffers are in polished steel, representing how the locomotive was initially delivered.

Body decoration is up to Bachmann's usual high standard and this model, 20306, is in the early style of DRS branding with pale blue panels over the base Oxford Blue colouring. The locomotive retained this livery from its introduction with DRS in 1999 through to its withdrawal in 2013.

For those who prefer the later 'Compass' branded locomotives, Bachmann is releasing 20312 in this livery at the same time. That seems a good enough reason to indulge in two models so that trains can be double-headed or 'topped and tailed'.



Manufacturer: Bachmann Branchline
Catalogue Refs:

(35-125) Class 20/3 20306 DRS Blue
(35-126) Class 20/3 20311 Harry Needle Railroad Company
(35-127) Class 20/3 20312 DRS Compass (Original)

RRP: £164.95 DCC ready

Gauge/scale: 16.5mm gauge, 1:76 scale OO

Era: 9

Company/Operator: DRS

Weight: 341g

Chassis and Body: Plastic chassis and body

Minimum curve radius: 438mm (R2)

Wheel Profile: RP25

Couplings: NEM-mounted tension-locks

Accessories: Cosmetic screw-link couplings, multiple working and brake pipes, snowploughs



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NEWS

Bachmann unveils spring 2021 announcements

Highlights from its latest quarterly announcement see the re-introduction of popular Bachmann Branchline 4-6-0 locomotives, new and updated toolings, DCC sound upgrades and new liveries.

Headlining its new RTR locomotive announcements is an all-new Class 20/0. Benefitting from the latest developments in RTR models, the new tooling sees the locomotive back-dated to its original condition, and modellers will benefit from directional lighting, cab lighting, with the option of DCC sound.

Sound-fitted variants of its Class 24/1 and Class 121 DMUs are to join its range, alongside its Class 20/3 locomotives (joining its previously-announced standard versions).

Differences between its Class 20/3 locomotives in DRS blue and later compass livery go beyond the logos to bogie pipework detail, cab antennae and bogie painting differences. Locomotives are to be offered with or without DCC sound.

Joining the Graham Farish N gauge range of models will be 66783 *The Flying Dustman* in GBRf Biffa Red livery, while its range of rolling stock for the scale will be bolstered by a further version of its forthcoming WD 'Parrot' Bogie Wagon, this time depicting the 50T Bogie Bolster B.



Class 92 exclusive arrives with Kernow Model Rail Centre

Revolution Trains' new Class 92 locomotives, produced by Rapido Trains, are arriving with stockists. As part of an exclusive limited edition for retailer, Kernow Model Rail Centre, further variety can be added to your locomotive fleet with its exclusive model commission of 92017 *Bart the Engine* in the instantly recognisable Stobart Rail livery.

Models are DCC-ready and fitted with Next-18 decoder sockets. Previously named *Shakespeare*, 92017 was outshopped in Stobart Rail's blue and white livery and named *Bart the Engine* on October 27, 2009. Operated by Direct Rail Services, the locomotive is based at Crewe International Electric Maintenance Depot.



Faller celebrates 75th anniversary with 200 new products

Faller GmbH is celebrating its 75th anniversary this year, in which customers and fans will be presented with around 200 new products from the Faller and Pola G in-house brands, due to appear in stores in the first half of this year.

Further building models from continental European countries with regional influences are to be added, from Black Forest building sets to a motorised windmill in Z gauge, plus many accessories.

In other news, its car system is to be updated. The digital vehicle and traffic control system is getting its most extensive update to date, featuring radio-based hardware in a handy USB stick, with optimized graphics and a new software interface with 'dark mode' for use if operating an illuminated layout with room lights switched off.

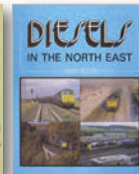


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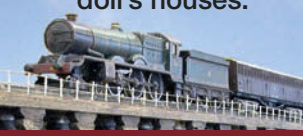
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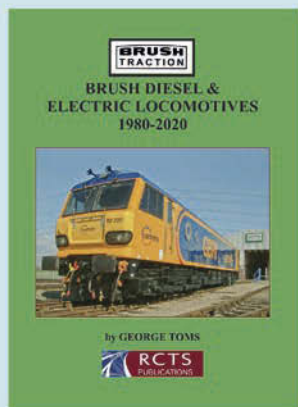


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New for 2021 our latest book - Brush Traction Brush Diesel and Electric Locomotives 1980 - 2020

We present our latest publication, this time on Brush Traction, a major builder of locomotives for export and within the UK. Written by a life-long employee of Brush Traction this work covers the period 1980 to 2020 and covers all aspects of the Brush output of Diesel and Electric locomotives during that period plus the many changes experienced in work practice, management control and ownership of the brand name. In addition to the export orders this work covers the large orders for BR Class 60, Class 92 and Eurotunnel. Later chapters explore the changes in outlook which led to the development of re-engineering as the main stay of work undertaken by the works at Loughborough. Detailed appendices are provided for all those locomotives built in 1980-2020 plus completely up-to-date tables on those built pre-1980 (Class 31, 47 etc). Presented casebound in 184 pages with 73 accompanying photographs.

Take the opportunity to purchase this book at the introductory price of £24.50 plus £3.00 for p/p.

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WESTERN LOCOMOTIVE ASSOCIATION - D1013 WESTERN RANGER BOGIE OVERHAUL APPEAL

The Western Locomotive Association has been making good and steady progress with the overhaul of D1013 Western Ranger.

Over the last ten years, the locomotive has benefitted from thousands of hours of volunteer help. Work carried out so far has included a full electrical re-wire, component overhauls including pre-heaters, fuel pumps, cooler groups, cooling fans, cardan shafts, exhausters and air receivers, internal paintwork and cab desk rebuilds, not to mention current work to overhaul its two massive 65 litre Maybach MD655 V12 engines.

D1013 was built and introduced into service in 1963 and clocked up 1.32 million miles before withdrawal in 1977. Its last overhaul was over 45 years ago!

When the decision was made to withdraw D1013 from traffic, it was agreed that any



work carried out should be to the same standard or better than BR would have achieved, and to a mainline standard.

Attention has now turned to the bogie wheelsets which will need a full overhaul in order to give the locomotive a new lease of life for the next 150,000 miles over the next 40 years or so.

Much of the required work will need to be outsourced to specialist engineering companies to ensure it complies with safety standards and is to the latest specifications.

How much will a bogie overhaul cost?

It is currently estimated that the project and the return of D1013 to service is going to cost in the region of £200,000.

We need your help

This is a massive undertaking for a volunteer group and so as well as applying for funding from the Heritage Lottery Fund and seeking sponsorships, we need additional help from private donations.

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9th August 1986. The 08:03 Liverpool Lime Street to Newcastle is seen inside Huddersfield station headed by No. 45070. Today the centre roads, on which a two car Birmingham RCW set is standing, have been removed. Unfortunately, as far as photographers are concerned, today's eastbound Trans Pennine Express services do not stop at this platform, instead using the other side of the island platform.



Trans Pennine Loco Hauled: Part 3 Huddersfield to Leeds

Gavin Morrison continues his journey across the Pennines, this time continuing from Huddersfield to Leeds. This section of the route is about to see considerable change over the next few years with the restoration of four tracks on the section of line between Huddersfield and the outskirts of Dewsbury.



12th July 2003. Royal train Class 47/7 No. 47798 is in charge of this Pullman special from Birmingham to Inverness seen passing Bradley Junction as it heads towards Leeds. This is one of the sections that is to be rebuilt as a four track line. The crossover for the single track line to Bradley Wood Junction can be seen about half way down the train. This chord links the Huddersfield to Leeds line with the Calder Valley line and is used by local trains between Huddersfield, Halifax and Bradford Interchange.



13th May 1976. The area around Heaton Lodge Junction has undergone considerable reorganisation several times since the 1960s. (see TRACTION 241 for a map) Originally, the line on which the train is about to run was part of the former LNWR "New Line" which ran from Heaton Lodge to Farnley Junction on the outskirts of Leeds. From Heaton Lodge it ran under the former L&YR Calder Valley line before heading towards Leeds on a steeply graded line. It allowed trains to avoid the congested section around Mirfield. After the closure of the "New Line" in the 1963 a spur was built linking the Calder Valley line to the remnants of the "New Line". It is on this new section, which opened in 1970, that we see Class 45 No. 45073 heading the 17:10 Newcastle to Liverpool. The abutments of a bridge on the "New Line" can be seen above the white gradient post. At the reorganisation of Heaton Lodge Junction in 1988 the westbound track towards Huddersfield which passed under the Calder Valley line was lifted. The eastbound line is still used by trains.

5th July 1961. Photographs taken on the "New Line" are relatively rare as, by the 1960s, the line saw relatively little use and was closed as a through route in 1963. However, Trans Pennine services were often routed this way rather than via Dewsbury. Gateshead's EE Type 4 No. D240 is seen passing the site of Gildersome East station (closed 11th July 1921, but open for goods until 30th July 1963) heading the 09:45 Newcastle to Liverpool Lime Street express. Close study of the photograph reveals that another locomotive hauled train is heading in the opposite direction; this is almost certainly the 09:00 Liverpool Lime Street to Newcastle. The signalman has been very prompt returning the signal to danger after the passing of that train.



9th February 1986. On a snowy day No. 45101 is passing the location of the former steam shed at Mirfield with the 09:51 York to Liverpool Lime Street. At this time the line through Mirfield was still four tracks rather than the current three.



2nd July 1982. The photographer has climbed to the top of the fire escape of one of the old mills to capture this image of No. 40099 heading eastwards with the Saturdays only 09:00 Llandudno to York. The old steam shed can just be seen in the background and was used by a haulage company; this area is now a housing estate.



3rd May 1990. No. 47475, the only member of the class to receive Regional Railways livery, is passing Thornhill Junction with the diverted 10:04 Newcastle to Liverpool. The train has run from Leeds via Normanton and Wakefield Kirkgate. Ravensthorpe station on the line towards Dewsbury and Leeds is behind the train. This area will change dramatically in the future with the construction of a flyover and Ravensthorpe station's location will also be moved.



3rd December 1981. On a wet morning 'Deltic' 55009 calls at the Dewsbury with the 08:49 York to Liverpool Lime Street. At this time on Thursdays this train was often 'Deltic' hauled in order to test the locomotive before a railtour duty at the weekend.

29th June 1976. On a fine June evening No. 46021 crosses Batley viaduct heading the 17:10 Liverpool Lime Street to Newcastle.

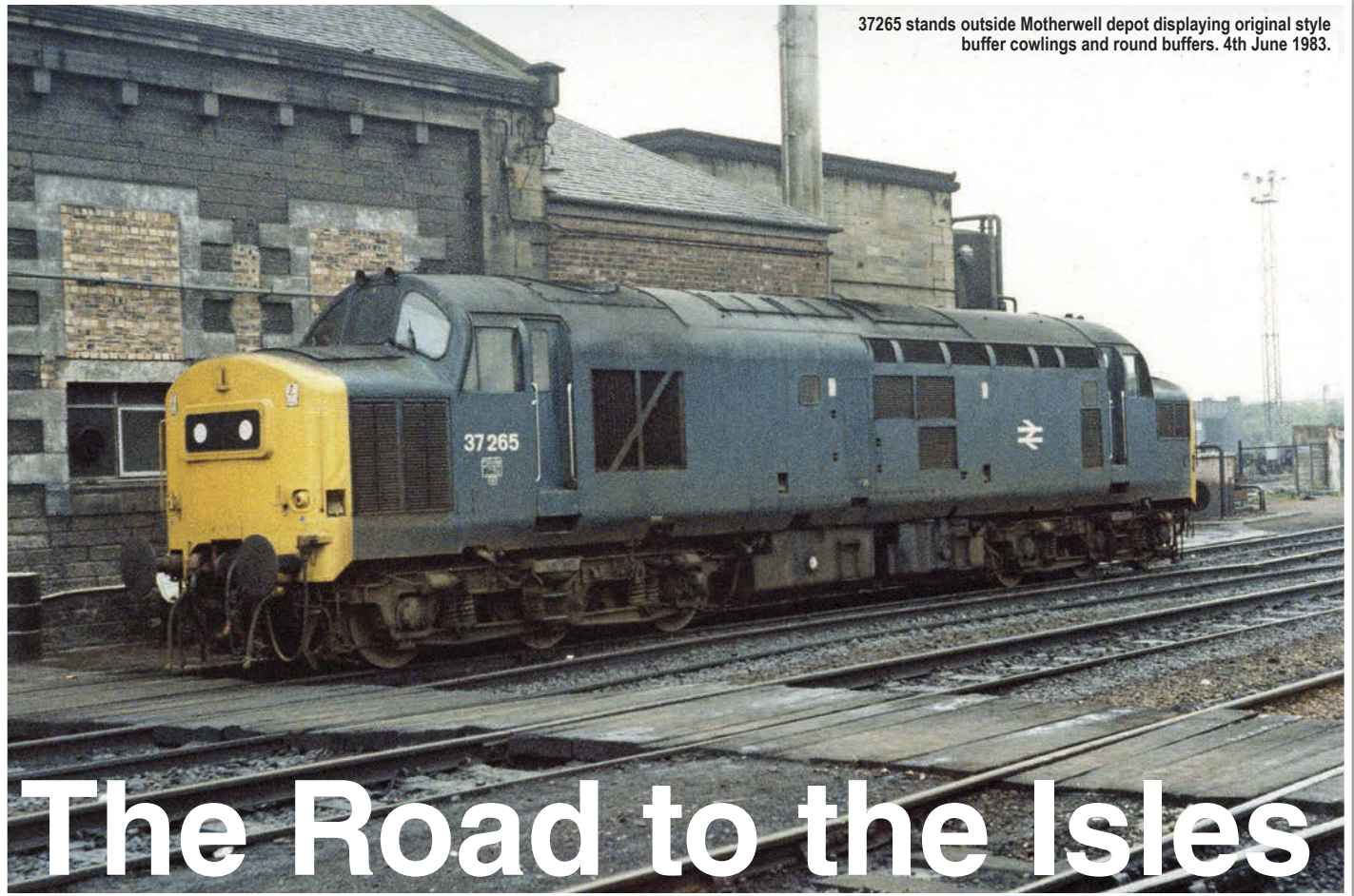


28th July 1975. No. 47051 is approaching Morley Low station with 1M77, the 16:10 Newcastle Liverpool Lime Street before entering the 3369 yards long Morley Tunnel. Morley Low station is nowadays known simply as Morley.

8th March 1961. It is hard to believe this picture was taken sixty years ago. Even though this feature has been about loco hauled trains across the Pennines we just had to include this photograph of one of the handsome Trans Pennine 6 car diesel multiple units (the later Class 124s). It is seen leaving Leeds City with a Hull to Liverpool Lime Street service less than three months after their introduction on this demanding route. Over to the left are the platforms of the "Wellington" part of the station, which were used mainly by trains on the former Midland Railway lines into Leeds.



37265 stands outside Motherwell depot displaying original style buffer cowlings and round buffers. 4th June 1983.



The Road to the Isles

At Whitsuntide in 1983 Jeff Nicholls and his family headed off to the Highlands again, this time in a white Fiesta, the untrustworthy Lada having been sold off.

This time the destination was Oban for a week on what seems to me the least interesting of the scenic Scottish routes with relatively little of railway interest once you get there. Perhaps this is a little harsh, but a diet of Class 37s only can get a little monotonous (well, it could then, nowadays it would be like Manna from Heaven!) Interest on the trip would come mostly from visiting two new (to me) depots, Polmadie on the way up and Eastfield on the way down. At both depots I was made extremely welcome.

Motherwell Again

Following what was to become a long standing tradition, the first stop was made at Motherwell where my permit was examined by the foreman with an almost total lack of interest, I got the feeling I could have shown him anything and got round. This time I was lucky enough to find three '27s' on shed, two of them side by side in the old steam shed building just asking to be photographed.

By this time I was heavily into 4mm scale so-called Modern Image modelling so I was pleased to contrast two of the '37s' present. 37028 had been refurbished with its buffer beam surrounds removed, front doors fitted with a bar welded across, oval buffers and split headcode boxes plated over. 37265 on the other hand retained its buffer beam

skirts and headcode box unplated, though displaying twin white on black dots. No less than ten of the English Electric Type 3s were present as well as nine Class 20s, four '08s' and a pair of '47s', one of which, 47210, took me down to only five of the class needed for the full set. Counting the aforementioned '27s' this brought the ratio of Sulzers to English Electrics to 5:24. Interesting or what?

Polmadie and my only 'Real' Class 06

I had decided to go round Polmadie on the way to Oban and Eastfield on the way back, though I didn't have a permit for the former depot. Fortunately, the foreman was a very accommodating chap and, as I had arrived by chance at the same time as a small group of enthusiasts, he agreed to

37028 is inside the depot at Motherwell on 4th June 1983.



27042 and 27055 nestle inside the old steam shed at Motherwell on 4th June 1983.



On 4th June 1983 06008 waits its fate outside Polmadie depot along with 08730 which is missing its centre wheels. Discernible under a magnifying glass on the original print, the '06' carries the old British Railways lion and wheel emblem above its number.



take us all round the depot which I seem to remember was locked. The first loco we clapped eyes on was 06008 waiting its turn for scrap, having been withdrawn almost three years earlier. In another few months it would be cut up on site at the depot. In front of it stood 08730 with its centre wheels removed for attention. These were the only locos that were in photographable positions but inside the depot lay another treat for me - 47273, now I only needed four of the class! Two of my last six in one day was an excellent result. As a matter of interest, my very last one was ex ScotRail 47706, which took me until 1992 to capture, passing through Eastleigh of all places.

Also on shed were a further four more Scottish Class 47s, five '20s', one '27', one '37' and a total of five '08s' but, without a doubt, the star of the show was the diminutive 0-4-0 Barclay, the only member of the class I ever saw until 06003 entered preservation.

And on to Oban and Fort William

The Oban line was exclusively the preserve of the '37s' and, whilst nowadays this might be regarded as bliss, I tended to regard them as usurpers of the Class 27s so wasn't too bothered about seeing and photographing them. The only photo I took was of 37043 'Loch Lomond' stabled up in the station with the famous McCaig's Folly in the background. If only I knew then what I know now.....

One of the days of the holiday saw us in the Fort William area where I photographed one of the delights of Scotland that seemed so rare south of the Border, a single Class 20. Working into Fort William on a trip freight was 20148. On occasions this snowplough fitted 1000 hp machine worked passenger and mixed passenger/goods trains to Mallaig and back. On the small depot at the Fort 37051 and 37081 'Loch Long' waited their turn to work trains to the south.

20148, with snowploughs, leads a trip working into Fort William on 9th June 1983.

37043 'Loch Lomond', complete with snowploughs, while away the hours in the shadow of the famous McCaig's Folly, a monument built during the 1930s to relieve local unemployment. There are some interesting old cars in this view taken on 7th June 1983.



My first visit to Eastfield

The depot took a bit of finding, being situated in a bit of a rough area, putting it mildly, and we didn't like to stop and ask directions! The omens for the visit were excellent as the first loco seen before even approaching the foreman's office was 26002, and the second 47464 which I stopped to photograph as it sported twin snowploughs. At the time, modelling wise, I was suffering snowplough mania and had fitted virtually all my diesel fleet with the little yellow adornments - a pair of '20s', a pair of '25s', a pair of '26s', a '27', a '33', two '37s' etc.....

Once inside the large depot my permit was greeted quite cordially and I was more or less given the run of the place and photographed 27030 amidst typical shed 'clutter'. The grand total of forty eight locos were seen on the depot, including 40173 which had been withdrawn almost two years earlier and was dumped against some buffer stops. The most interesting loco I wanted to photograph was 37112 which sported Eastfield's special version of wrap round yellow ends, black cab window surrounds combined with small numerals and logos as opposed to the large ones which were beginning to spread slowly across the land. Sod's Law allowed me to photograph one end only but, interestingly to the modeller

in me, it still carried round headed buffers. Another large logo livery loco in evidence was 47712 'Lady Diana Spencer' which, frustratingly, was totally unphotographable.

An hour's very enjoyable visit to Eastfield was rounded off by photographs of 20028, complete with delicious very large snowplough, and 27207 which was making its way on shed as I was leaving.

Homeward bound and a hell of a fright

So ended my third visit to Bonnie Scotland, not as successful number wise as my previous two visits: 113 locos 'spotted,' but of these exactly 50 were 'cops', so quality stuff!

I wasn't quite finished with the railways though: driving along the A74, down the Clyde valley, the road twists and turns, following quite close to the West Coast Main Line at some points. Imagine my split second horror then when I glanced in my rear view mirror and found it almost filled with the yellow front end of an '87' bearing down on me, or so it seemed, at a rate of knots. It was only a kind of optical illusion but it gave me a bit of a turn. Naturally, I recovered quickly enough to catch the number, just for the record, 87016 'Sir Francis Drake'.

27030 is inside Eastfield surrounded by shed paraphernalia on 11th June 1983.



This is the number 1 end of 37112 in 'Eastfield' livery seen in Eastfield depot on 11th June 1983.

(RIGHT) On 11th June 1983 27207 moves onto the shed at Eastfield, passing a DMU carrying 'Trans Clyde' markings.



Now that's what you call a snowplough! 20028 is at Eastfield depot and the question is had it moved since winter?



(BELOW) 47464 stands outside Eastfield depot with withdrawn 40173 in the background on 11th June 1983.



40118 runs light into Guide Bridge in July 1977. The loco holding sidings can be seen in the distance with Class 25, 40 and 76 awaiting their next duties.



Around Guide Bridge

Ray Briscall looks at this junction station on the outskirts of Manchester in the 1970s.

In the 1970s Guide Bridge was home not only to a substantial four platform station, but also formed a major junction and had its own goods facilities and loco stabling yard whilst there was also a large yard at Dewsnap about a mile to the east. It was situated on the main line from Manchester Piccadilly to Sheffield; the much lamented Woodhead Line. Also converging here were lines from Stockport via Denton in the south and from Ashton Moss and from Stalybridge to the north and north east respectively.

During the late 1970s, I visited several locations in the area including Guide Bridge itself, Dewsnap Sidings, Hyde North and Broadbottom and witnessed some of the still very busy freight traffic. Passenger trains to Sheffield and beyond, via the Woodhead Tunnel, had finished back in 1970 but there were still the 1954 vintage Class 506 1500 v DC electric units that provided the Manchester to Glossop and Hadfield service. There were DMUs as well and I used to frequent the service from Manchester to Marple Rose Hill.

Specials

Even though the Woodhead route was closed to regular passenger services, there were still occasional specials and I was fortunate enough to travel on three of these: the Roch Valley Railway Society tour to York and the Keighley & Worth Valley Railway train, hauled by Class 40 No. 40042, on 30th September 1978 as well as the LCGB 'Doncaster Rover No. 2' on 21st

January 1979, hauled by the green liveried No. 40106, and lastly the LCGB 'Woldsman' rail tour 21st April 1979 which had Class 25s Nos. 25319 and 25297 into Guide Bridge to be replaced by Class 76s Nos. 76014 and 76030.

The occasional steam special could be seen, such as the 'Leander Envoy' 24th February 1979 which was hauled from Leicester to Guide Bridge by Class 47 47051 where 'Jubilee' No. 5690 'Leander' was attached to take the train to York and then on to Carnforth where 'Leander' was being transferred to Steamtown.

Freight

The stabling roads at Guide Bridge were mostly used by Class 76s but diesels and Class 506 EMUs were stabled there as well. Fuelling facilities for diesel locos were also provided. Freight was handled by Class 76s and diesels including Classes 25, 40 and 47. Much of the freight traffic was South Yorkshire coal which fed numerous power stations in the North West, a major customer being Fiddlers Ferry. Many of the coal trains stopped at Mottram for the '76s' to be exchanged for diesels to continue to Fiddlers Ferry via Godley Junction and Stockport. Some, though, continued through Guide Bridge to Ardwick for local distribution. Coal, although the major source of freight traffic, was not the sole contributor as steel, express parcels, gas tanks and even mixed freight trains could be seen. Dewsnap sidings, though somewhat run down, still housed

a considerable variety of freight vehicles whilst some interesting condemned coaching stock could sometimes be found there.

Today

So what is left today? The portion of the Woodhead route from Manchester through Guide Bridge still exists as far as Glossop and Hadfield for local commuter services while the Manchester to Marple DMU services also continue to call. Trans Pennine Express services from Manchester Piccadilly to Hull and Manchester Piccadilly to Huddersfield pass through and take the line to Stalybridge. There is also the once weekly 'parliamentary' Fridays only service from Stockport to Stalybridge. The line from Ashton has been lifted but the other lines from Stockport and Stalybridge still exist. The station itself is now just 2 platforms with a much rationalised track layout, the stabling roads are long gone and Dewsnap sidings have been built over but there is still a yard located between the Stalybridge line and the Ashton canal. This is used by SB Rail for maintenance of on-track machines such as tampers. Freightliner also have a maintenance depot which looks after wagons used for stone trains from the Peak District and container wagons used for the 'binliner' refuse trains originating in the North West of England. A small number of freight trains operate through Guide Bridge, mainly stone and 'binliner' trains but also biomass trains for Drax power station.



Motor car M59406M arrives at Guide Bridge with a Glossop to Manchester Piccadilly train in July 1977.



76029 leads 76008 with a westbound steel train near Broadbottom on the 13th September 1979.



There are no fewer than nine Class 76s in this view of Guide Bridge Stabling Point including two where only the pantographs can be seen. Identifiable are 76008, 76009 and 76013. The date is 21st April 1979.



Driving trailer M59607M brings up the rear of a Glossop to Manchester service as it passes the junction with the line from the Marple direction at Hyde North on April 20th 1981.



On the evening of July 26th 1978 40108 takes a train of parcels vans westwards through Guide Bridge.

76049 heads non stop through Guide Bridge with an eastbound parcels train.



76040 brings a train of coal hoppers through Guide Bridge heading towards Ardwick on July 26th 1978.

The sidings at Dewsnaps, just to the east of Guide Bridge, often contained damaged or withdrawn rolling stock. Here two steam heating vans, ADM 44407 and 44413, together with a burnt out Mark 1 kitchen-buffet M1509, await their fate on 20th April 1981. In 1960 twenty former LMS third class open vehicles (diagram D1745 lot 185 built 1926) were converted to provide train heating for steam heat only stock when being hauled by an ac electric locomotive. They were used largely between Liverpool, Manchester and Crewe. As more electrically heated stock became available their usage on passenger trains ended and most found further use as stationary pre-heaters at stations and in carriage sidings. These two coaches were scrapped by Marple & Gillott at Attercliffe in Sheffield about December 1981.



The 'Western' that started the Graham's passion for the class: D1053 'Western Patriarch' is at Newton Abbott on 8th June 1974. STEVE IRELAND



The 'Westerns' in the 1970s

On the 27th of May 1975, Graham Callaway scribbled '33 110, location: Fratton' in his notepad and so began more than two decades of 'train passion'.

With the school summer holidays fast approaching, my Mother's pronouncement that "It'll keep 'em quiet for six weeks" seems as funny now as it felt acutely annoying then. To be fair, if it wasn't for my parents' backing in the early years then I probably wouldn't be writing this now.

Although 33110 is accredited as being my first loco spotted, the class of engine that really caught the imagination of a wide-eyed twelve year old was, without doubt, the 'Westerns'. I saw all the 'Deltics', even coping a few as haulage, and the best part of the Class 50 fleet is safely captured on film in my slide library. But when it comes to pure nostalgia, there's only one class for me. I only saw sixteen members of the class in action and what follows is purely a personal reflection of events that occurred over twenty years ago through the eyes of a child.

My brother Pete and I started spotting in the May, and by June we had completely exhausted the possibilities of our home town, Portsmouth. We had clubbed together our pocket money and bought a 40p 'Locoshed' book, so we were more than

aware of the other motive power around. Having saturated Portsmouth for a month we were pretty sure that 'copping' a Class 40 at Fratton was a high on impossibility. (Ironically, 40173 did put in an appearance on a railtour in February 1978.) Bursting at the seams with pent up frustration, we approached 'good old Dad' for suggestions of where we could go to see more trains.

"When I was a kid, Eastleigh was a Mecca for trainspotters, why not try there?" Cracking idea: only two problems. One, where the hell is Eastleigh, and two, we're broke.

"Eastleigh is up near Southampton and here's a quid for the train fare."

The 4th of June 1975 found us encamped on the platform end at Eastleigh, greedily taking in everything that moved on rails. We gorged out on Classes 07, 08, 33, 47, 73 and 74 before encountering the crème de la crème. With our departure time approaching, D1053 'Western Patriarch' trundled up from the Fareham line with a rake of empty stone wagons. You would have thought we'd won the pools by the way we were jumping up and down with excitement. Although the train was only

doing about 20 mph, we decided on the 'belt and braces' approach to getting the 'cop'. "You get the number, I'll get the name". Unfortunately, whichever one of us was 'you' completely failed in his task and in our excitement we both took the name. As already mentioned, our sole record book was a 'Locoshed' and for those who remember, 'namers' were only identified with an asterisk, it required a surreptitious look at an Ian Allen 'Combined Volume' on the bookshelf of W. H. Smith to verify the number.

South Wales

The next sighting of a 'Western' came in the August of the same year. For the first time ever, we were off on a family holiday: two nights in a caravan in the garden of a hotel owned by family friends. The 'Conifers Hotel' was situated at Cwmbran, just outside Newport, Gwent and therefore afforded us our first sight of, amongst others, Class 25 and 45. Our parents always gave us the opportunity to watch the trains wherever we were, and South Wales was no different.

Our arrival day found us safely

ensconced within the confines of Newport station for a couple of hours in the evening. Notebooks and pens were working overtime trying to keep up with the seemingly endless flow of Class 37 hauled coal trains and by the end of our session we had accumulated over thirty numbers. The following day was spent shopping in Cardiff (with a brief interlude on the station) followed by another evening visit to Newport. Just after 'cabbing' a Class 25 (I think it was 25085), we were greeted with the sight of D1071 coupled up with 47185 working one of the longest coal trains I had ever seen. It may have been prejudice or just a latter day fleet of fancy, but I'm sure 'Renown' was doing all the work, certainly the accompanying cacophony of noise suggested that all was not well with the '47'.

Still on a high from the sighting, we entered the buffet for the last of the day's tea. The station announcer was informing all passengers of the next London bound arrival, and I can still recall asking my Dad if it might be another 'Western'.

"It's not likely son, but how about this one on the parcels train." Turning to look out of the window, we witnessed the huge bulk of D1011 'Western Thunderer' rumbling by with its eastbound parcels train. We bombed out of the buffet to try and get an even closer look at the engine but unfortunately she got the 'right away' just as we arrived. Our disappointment lasted a mere moment as we realised that we had seen two 'Westerns' within five minutes of each other, a feat we would never repeat if you exclude double headed workings.

Whilst we got out and about a fair bit, including a visit to Didcot where we could see the 'Westerns' bombing through the station from our vantage point on the coaling stage (although were unable to read the name or number), our next recordable sighting came on the 27th of April 1976.

Reading

I think it must have been half term at school because we made a Tuesday afternoon visit to Reading. We purchased a Reading Station Trainspotter's Platform Ticket (that particular day's one had a picture of 'Warship' on it) and settled down to collect the proverbial hatful of numbers. Apart from the ubiquitous Class 47s and 50s we also managed a pair of 37s, three 31s and a 33. The ticket ran out at four p.m. so we retired to the car park at the east end of the station. Perched on the small footbridge that crossed the road, our first 'cop' was D1028 'Western Hussar' on a West of England working. Feeling well chuffed at the sighting, we rushed back to the car to beg for an extra hour in the hopes of seeing another 'Western'. With permission granted, and even Dad coming back for a look, we returned to the bridge and desperately scanned the horizon for

another 'Western'.

Much to our consternation, the next sighting was tantalisingly unavailable. A 'Western' had taken up position of station pilot between platforms 6 and 8. Although she carried her number in her headcode panel, an ill parked 'brute' mail trolley covered it from our viewpoint. Our disappointment was tempered by a flying visit by D1036, the 'Emperor', bombing through the station on an express train for Paddington.

I think Mother must have been getting bored sitting about, because Dad announced that we had another fifteen minutes before we had to join him back at the car. Although we prayed to all the Gods we could think of, we saw no more 'Westerns' and returned to the car as ordered. Our Father was nowhere to be seen, but Mother had a knowing glint in her eye. Within a few minutes he returned with the information we were desperate for.

"It's Patriarch," he said. He had gone to the station especially for us and found out the name of the pilot engine. Although not a 'cop', we were still 'made-up' at getting another number in the book. As we all clambered into the car, the tell tale Maybach roar echoed around the station environs. Reading through the wheel sets of the EMU in the bay, we made out the name WES-TER-N EM-PR-ESS as D1037 set out on the last leg of her journey to the Capital. A lucky sighting maybe, but still a fantastic way to finish the day.

D1021 'Western Cavalier' was the next sighting for me, my only class member spotted without Pete. A group of school pals had arranged a visit to Eastleigh and I tagged along to try and 'up' my class totals for the Class 33s and the Class 73 electro diesels. The 28th of May 1976 became a day long remembered for my own stupidity! Having seen 'Cavalier' on a stone train, I returned home and bated Pete mercilessly about my 'super-cop'. Taking this as a personal challenge, he arranged a visit to Reading and Swindon, refusing point blank to take me with him. My single 'Western' looked a bit sick against the six runners he managed to cop on his trip: I think I learnt my lesson!

To Swindon

Having served my penance, Pete arranged a repeat of his Swindon trip and this time let me come along. The 16th of August found us camped out all morning at the western end of platform 5 on Reading station. Surrounded by a good twenty or so fellow spotters, we were confident of more 'Western' sightings: what we actually got was a procession of Class 47 and 50 hauled trains. Feeling a little discouraged, we set off for Swindon in the hopes of tracking down a few class members and perhaps getting a glimpse of what lay in the graveyard of Swindon Works.

Arrival at Swindon saw us take a lengthy

walk, via all kinds of dubious back streets, to where we thought we might see some of the condemned engines. We finally ended up somewhere to the north of the yard, and although we could see the rusting hulks of those once fine engines, we were too distant to make out the painted on numbers. Feeling worse than when we were at Reading, we set off back to the station, this time following the road signs. Having passed under the railway, we could now see D7029, D818 and D1015 posed around the turntable.

Although it was quite a majestic sight, better still awaited us at Swindon station. Waiting patiently for our train back to Reading, D1058 'Western Nobleman' passed through on a freight train heading toward London. 'Now this is how it should be' we thought. A quick bomb back to Reading, and there, waiting in the pilot's spot was another 'Western'. Sprinting through the walkway, in case the much desired engine should depart, we finally discovered the identity of the engine: D1053. Even though it was the third time we had seen 'Patriarch' it was still a sight to behold. The blue paintwork was definitely fading and all in all it looked in a pretty care worn state, but we loved it and that was all that mattered!

No more 'Westerns' came through the station during our stay (although we did see four of the named Class 47s: 'Orion', 'Thor', 'Odin' and 'Sir Daniel Gooch') and with departure time beckoning, we made our way to platform 2A and the 3H DEMU journey home. Passing the depot, we could see another 'Western' just sticking its nose out of the shed. The trees were much sparser then, allowing us a 'one shot' attempt at ascertaining the number. D1065. "Consort," we yelled in unison. Coming slightly more abreast of it, the dilapidated state of the paintwork confirmed our sighting.

Westbury

Two days later, and we were watching trains again. A family visit to Cheddar Caves culminated in a visit to that Mecca of 'Western' watchers; Westbury. Although it was a completely spur of the moment decision by our father, it turned out to be one of our most remembered evenings. All was not well with the car and it finally gave up the ghost slap bang in the centre of Warminster. Our parents phoned the AA and then gave Pete and me their blessings to continue the journey on foot. Being young and happy go lucky we joyfully set out for Westbury, following the road signs as directed. A Class 31 was seen on a freight train but, much to Pete's chagrin, the number remained unrecorded. "If you run across that farmer's field, you'll get it," says Pete. My reply was, and still is, completely unprintable. Just on the outskirts of Westbury, a car horn heralded the arrival of the rest of the family and we

completed the remainder of the journey in our 'well again' Ford Corsair.

It was a fine summer's evening and Westbury station was an idyllic place to be. Apart from the occasional car passing over the bridge, the only sounds that have survived in my memory were the birds singing, the bells from the signal box and the 'clunking' of semaphore signals. We witnessed a couple of '47' hauled passenger workings (081 and 247), a brace of '33s' on a stone working (118 and 115) and a Class 31 (305) running light, before the clunk of the home semaphore dropping to its line clear position heralded the arrival of another train.

Long before its arrival, we were certain that we were about to see another 'Western'. The surrounding hills reverberated to the 'Maybach Music', the crows took to the air in startled fashion, and we waited in huge expectation. In true Hollywood fashion, the huge bulk of D1056 'Western Sultan' materialised out of the setting sun and slowly ground to a halt at the platform. A brief period of door slamming, an almost mournful blow of the guards whistle and a tired looking 'Sultan' lifted her train out of the station and on towards the capital. We were spellbound. To most people it was only a train: to us it was a moment in time, a time neither of us has forgotten.

The station quickly returned to its former tranquillity, leaving its staff and two very happy boys to await its next arrival. 50030 put in an appearance, as did 33111 before we were given the "Another ten minutes" routine. Once again the air gradually began to fill with the unmistakable roar of a 'Western' and our hopes and prayers were answered. The train rolled to a halt just off the platform end leaving one rather panic stricken 'Western basher' beseeching us to give him the number of his haulage.

"Nobleman," we replied, and what a sight she was. Although only five months from the scrap yard, D1058 seemed to effortlessly lift her train from the station and pounded off into the dusk, leaving only her acoustics to accompany us to the car. As we left the car park, we saw the signal drop for another train. Another 'Western'? We never found out. Mum, quite fairly in hindsight, was desperate to get home and we still had a couple of hours of driving to do. I bet it was another 'Western' though!

On the 5th of September, Eastleigh depot held its annual open day and we felt duly obliged to attend. We already had advanced notice of a proposed visit by a "Western" on a special from Swindon and so we approached the depot full of high hopes. The actual event was a little disappointing: no visiting motive power on display and only the usual diet of Class 33s, 73s and 74s to scribble in the book.

The undoubted highlight of the day was the arrival of D1009 'Western Invader' on the promised railtour. She shed

her load of passengers on a makeshift platform at the back of the yard and, if memory serves correctly, stayed in her completely inaccessible, and therefore unphotographable position for the rest of the day. Still, she was a 'cop' and that's all that really mattered to us at the time.

Having enjoyed our first visit to Westbury so much, we saved like mad and arranged another visit in October. Time was fast running out for our beloved 'Westerns', but we were still hopeful of seeing some in action. The 30th of the month saw us clamber aboard a 3H DEMU unit at Cosham station for the long and tortuous journey to Wiltshire. Highlight of the trip westward was trying to spot the camouflaged soldiers and tanks deployed on the vast expanses of Salisbury plain. The camouflage was obviously pretty good because we didn't spot any!

Arrival at Westbury station immediately brought us back to the reason for our outing. Stabled on the shed amongst a few Class 47s sat a 'Western'. She turned out to be none other than our old friend 'Patriarch', but what the hell, she was a runner and that's what we were there to see. Although we obviously had no idea at the time, D1053 was only to last another 15 days in traffic so the sighting had even more significance with the passing of time.

The passing of a few months between visits certainly had an effect. Not for us the tee shirt clad laziness of a balmy summer's evening; scarves, gloves and coats were order of the day as the wind chill effect reduced even weather proof kids to a foot stamping, hand blowing, walking snowman state.

Having witnessed a couple of Class 50 hauled passenger workings, the cold was forgotten as we watched a 'Western' hauled stone train approach the station from Merehead Quarry. D1048's appearance inspired a host of photographers to appear from previously unseen hidey holes and 'Lady' basked in its popularity before reversing the train into the yard and retiring to the shed. The photographers returned to their warm niches whilst we stayed on the platform and watched yet more Class 50s and 47s come and go. 31254 and 31209 broke the monopoly, working a passenger service from somewhere westward, but they were surpassed by another stone train and another 'Western'.

Once again the photographers reappeared to hog the platform edge as D1041 'Western Prince' ground to a halt prior to reversing to the yard and thence retreating on shed. 'Prince' had become a bit of a celebrity due to her dilapidated paintwork and lack of works attention, and the assembled photographers paid their own homage by reeling off frame after frame in the attempt to preserve its image for posterity.

As the morning wore on, we witnessed

some of the photographers wandering across the sidings to the shed and committing more memories to celluloid. Since Pete was just starting out on his photographic career, we felt that we were equally allowed to join in on the impromptu 'shed bash'. Armed with Pete's Zenith camera we spent the next hour walking in almost reverential silence around the three time-worn beasts of burden that we coveted so much. All three were parked with engines running, and although only idling, the power was there to be both seen and felt.

We decided to chance our arm at Temple Meads for the afternoon, in the forlorn hope that we may witness some 'Western' hauled passenger services. Class 45s, 47s and 50s dominated Bristol that afternoon and we left in the gathering gloom sure in the knowledge that we had copped some 'Peaks' but just a little disappointed at the lack of 'Western' action.

Westbury on the homeward journey was reached in pitch blackness, and sure enough there were four 'Westerns' on shed. We never found out the number of the fourth member of the class, but it was certainly the last one we saw on anything other than a special.

Into 1977

1977 dawned and we were already aware that 26th of February meant the end of an era. We had managed (by much begging and pocket money advances) to secure seats on the 'Western Memorial' railtour of the 29th January. The day dawned with much expectation but not a little concern that our cross London connections were a little tight. I'm sure the tour was a wonderful success for all its passengers, with D1023 'Western Fusilier' putting on a masterful display of haulage. However, due to our insistence on picking the tour up at Paddington and not Reading, and a friend's unforeseen paranoia about the Underground's escalator system, we contrived to miss our only chance of 'Western' haulage.

We spent the day trawling round the capital's termini, collecting lots of numbers but not the one we really wanted. The original plan of action was to await the special's return, but by about 7 pm on a freezing winter's evening we decided to put it all down to experience and call it a day.

The 19th of February was the day that the only 'Western' special to pass through our home station of Cosham was due to run. We decided to witness its passing at Eastleigh in the morning, return home for lunch, then walk into Cosham to see it make its way back to the capital.

As any local will testify, Eastleigh station on a Saturday morning is a pretty lonely place. The buffet is shut and the only railway traffic is the seemingly endless procession of EMUs. We waited patiently, not fully sure of the 'Southern Belle's'

passing time but were eventually rewarded with the sight of D1013 'Western Ranger' hurtling headlong (complete with red-backed nameplates) toward Weymouth. With another 'cop' safely confined to the notebook, we headed home with the promise of another sighting later that afternoon.

The afternoon witnessed the most spotters ever seen at Cosham and we duly took our place amongst them, wedging ourselves into the last vacant spots on the footbridge. We were already fully aware of the 'belling' system at the station (1 bell: eastbound, 2 bells: westbound, 3 bells: non stop) so with three bells rung, the crossing gates down and all eyes westward, we awaited 'Rangers' arrival. We were not to be disappointed. D1013 fairly flew by and I managed to capture my only semi worthwhile photograph of a 'Western' in action. The shot, taken on Mum's 127 'Brownie' style camera, was a studiously composed effort. As I look at the slightly blurred print now, I can see an engine long since withdrawn (though happily preserved), a rake of Mark 1s long gone, and a signal box that has also failed to last the passage of time. Of such things memories are made!

The end

One week later and we were off to pay our last respects to what must surely remain the most popular class of diesel locomotive. Still trying to quench our insatiable thirst for 'cops', we decided on South Wales as the location for our sighting of 'Western Tribute'. The arduous 3H DEMU journey to Temple Meads was rewarded with the sighting of the 'Campaigner' in tandem with 'Lady'. We were unaware that D1010 and D1048 were being used as back up engines to the main tour and although not 'cops', still made for a magnificent sight. They were also the first of three surprises on that day.

For reasons I still cannot comprehend we were sure in our own minds that the 'Tribute' was pathed to run firstly to Plymouth and then onto Swansea before her return to Paddington. We decided that Severn Tunnel Junction would make an excellent viewing location, thoughts of the double headed special bursting out of the Tunnel and powering on westward spurring us into motion. A DMU conveyed us into Wales and our first stop at Cardiff. The Welsh capital was pretty quiet train wise and so we aborted our stay in favour of another daytime pass of Ebbw Vale, collecting yet more 'cops' of the Class 37 fleet in the progress.

Newport station provided us with the second surprise of the day. I assume there must have been a rugby international at Cardiff Arms Park that day because we were treated to the most unexpected sight of two Class 40 hauled specials passing through. 40028 and 40120 following each

'Western Patriarch, again this time at Reading 1A55 the 09.55 Paignton to Paddington on May 12th 1973. DAVID WHARTON



Graham didn't see any 'Westerns' on express trains in South Wales but here we see D1001 on a passenger train at Cardiff pm the 6th August 1976. DAVE HIGSON



D1048, D1041 and D1053 are seen stabled at Westbury on the 30th October 1976. DAVE HIGSON



other in quick succession, literally jam packed with what I think were Scottish rugby supporters. With Pete's estimated time of arrival for the special drawing near, we grabbed another cuppa from the buffet and made yet another DMU journey to Severn Tunnel Junction.

The whole place seemed strangely deserted, but having the station to ourselves seemed to be a most suitable way of saying goodbye. Apart from the fifteen numbers we collected off the depot, the only other trains we saw were the occasional DMU and Class 47 hauled express workings. With 'Tributes' arrival now overdue, every time we heard the rails 'sing' we set our cameras eastward and hoped for our 'Westerns'. 37238 flew through light engine and then came surprise number three. D1023 and D1013 came hurtling through the station heading eastward! 'Fusilier' and 'Ranger' were heading for Plymouth and we were at the wrong end of the platform, facing the wrong way. No shots were taken but the memory remains.

I have never seen reference to it anywhere else, but at the rear of the last coach someone had hung a sign. It was a simple black text on a white background rectangular board and bore the legend "THIS REALLY IS THE END". I should like to place on record our own personal thanks to whoever was responsible for the message: I don't think it could have been more succinctly put. We watched as the train disappeared into the distance and then the realisation dawned. That was it: no more 'Westerns', no more euphoria at another sighting, no chance of haulage, no chance of capturing photographic memories.

The journey home was an almost surreal event. We recalled previous sightings. We stuck our heads out of the window at Westbury but somehow it didn't seem to be the same. We talked about the following week's visit to Plymouth, but realised that in truth we were going to be a week too late. On that day the magic died.

We managed to resurrect it a little with the passing of the Class 40s, but the 26th of February 1977 witnessed a passing of something special to a minority of the Great British Public. We were part of that minority and felt it just as keenly as any other even though we were relatively inexperienced at 'bashing', photographing or just plain watching.

I know the 'Deltics' had a massive following, and we were part of the tribe, but the 'Westerns' will always hold a special place in the hearts and souls of all who were there. I like to think that the feelings engendered by the class was the start of the 'pro diesel' feeling. Steam had died but the 'Westerns' filled the void. The interest shown encouraged a new breed of spotter and a preservation movement that still grows with each new class of life expired engine.

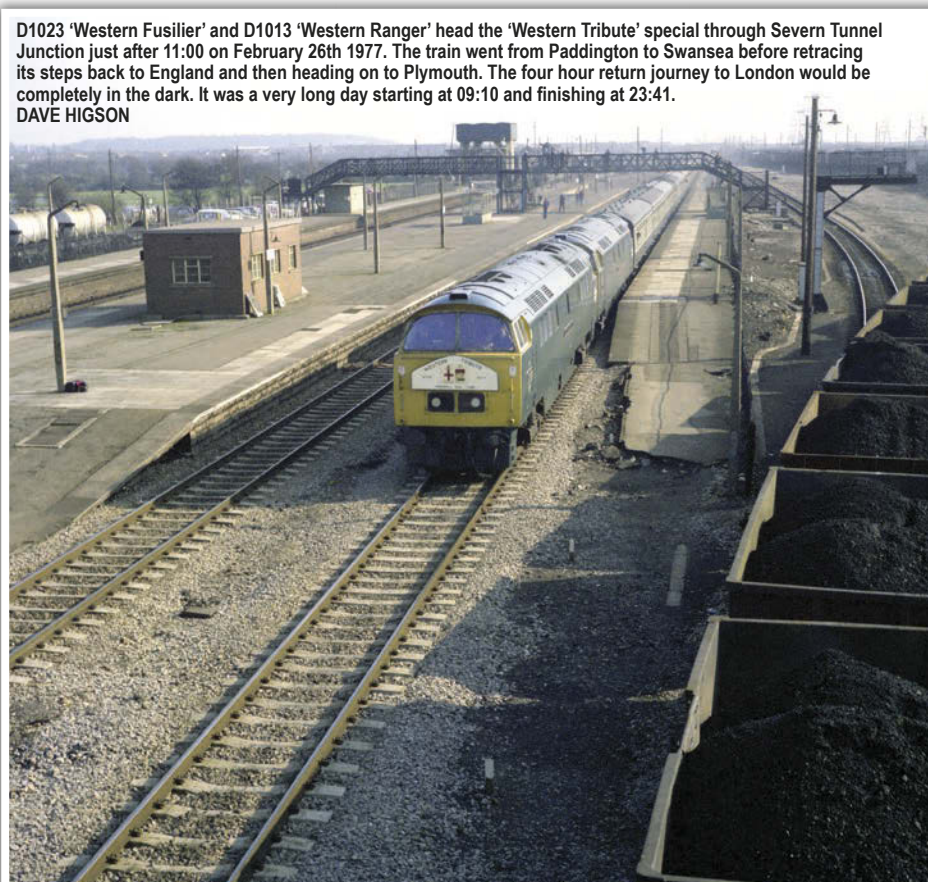
With apologies to its author, I recall a part of a poem from somewhere that's as strongly felt today as it was when first published:

***Kings and Castles may haunt the dark
Yet so will Western Patriarch.***

As a footnote, the 14th of May 1977 saw us aboard the RPPR 'Silver Jubilee' railtour. Using the 'Royal Pair' of 31s (414 and 416), part of the itinerary was a visit to the Swindon graveyard. Records show that 22 'Westerns' were present, but of D1053 there was no sign. Perhaps she really does haunt the dark!



Graham and some of his friends had booked tickets to travel on the 'Western Memorial' special but unfortunately did not arrive at Paddington until the train had departed. D1023 'Western Fusilier' is seen at Bristol Parkway on the outward leg of the tour. The special then travelled through the Severn Tunnel and via Hereford and Shrewsbury to Crewe. After a break at Crewe the train headed back to Shrewsbury via Chester before retracing the outward route back to London. 29th January 1977 DAVE HIGSON



D1023 'Western Fusilier' and D1013 'Western Ranger' head the 'Western Tribute' special through Severn Tunnel Junction just after 11:00 on February 26th 1977. The train went from Paddington to Swansea before retracing its steps back to England and then heading on to Plymouth. The four hour return journey to London would be completely in the dark. It was a very long day starting at 09:10 and finishing at 23:41. DAVE HIGSON



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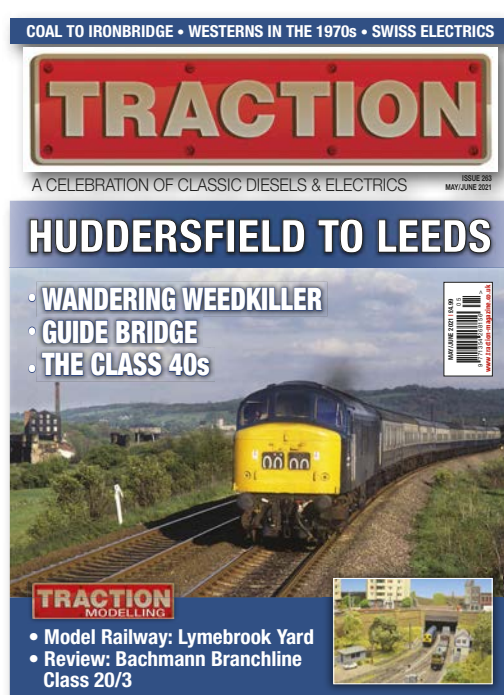


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