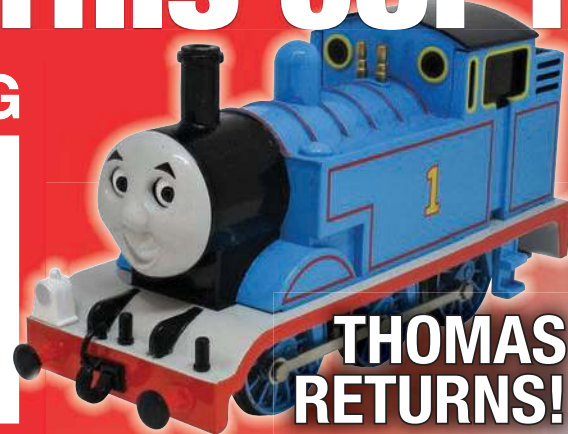


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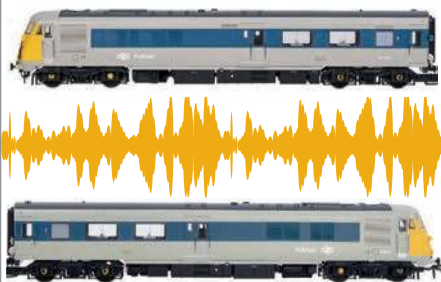
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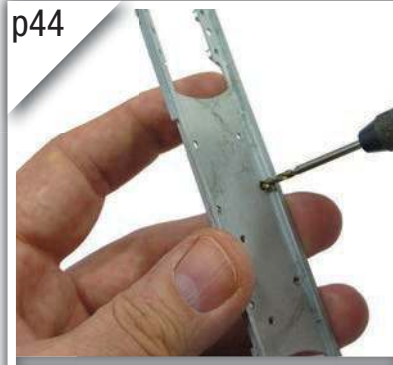


PLAY

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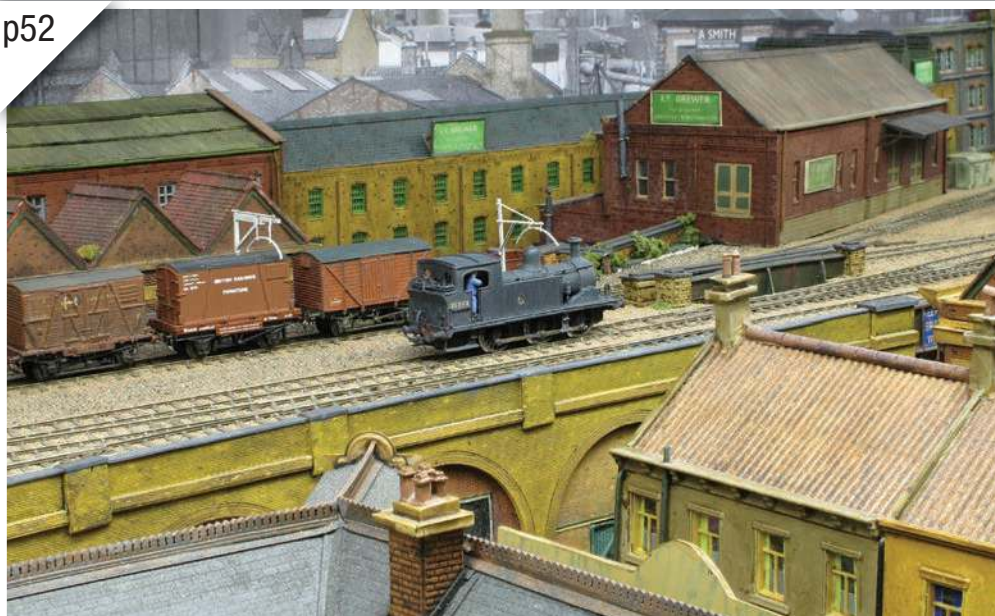
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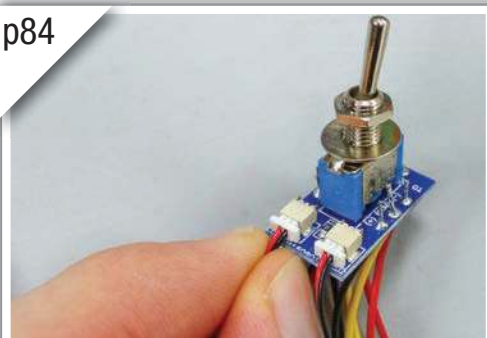
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BRM

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D6702

37/0 - British Railways green [small yellow panels]



D6704

37/0 - British Railways green [full yellow ends]



37001

37/0 - British Rail corporate blue



37602

37/6 - Direct Rail Services 'compass'



37605

37/6 - Direct Rail Services original



37606

37/6 - Direct Rail Services 'compass'



37607

37/6 - Direct Rail Services original



37608 - 'Andromeda'

37/6 - Rail Operations Group/Europhoenix



37609

37/6 - Direct Rail Services 'revised compass'

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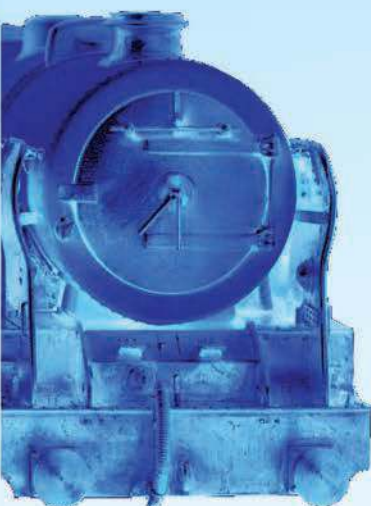
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Welcome to Spring

We have thought about creating a Capital-themed issue for a while and what better time to do it than to coincide with the forthcoming London Festival of Railway Modelling at Alexandra Palace on 21/22 March? Are the railways of London and the South-East proportionally represented in modelling terms compared with branch-line termini, rural routes, motive power depots and goods yard? Probably not, as to model London, content generally requires the space to run long main line services or busy intricate environments, which is normally enough to limit modellers' ambitions in that regard, but it's fair to say that this month's featured layouts tackle it in different and appealing ways.

Kier Hardy's magnificent Hornsey Broadway project gives us full-length trains in abundance and variety in large form and it's a great pleasure to have been able to capture that. Lightermans Yard takes us into South London backwaters, giving the viewer a landmark backscene that could

not be anywhere else but the south bank of the Thames. Terry Tew once again brings a compact but busy slice of West London in the form of Rossiter Rise, showing that you don't necessarily need Kier's space to go to town. All three are proper 'modellers' layouts' and they've inspired Phil to look at a straightforward but different practical project, while our features on lighting and wiring can help anyone motivated to tackle busy layouts in urban landscapes.

Anyway, we're looking forward to a great show at Ally Pally. It has one of the grandest outlooks from any exhibition venue in the country and there's something for everyone in the list, from the best finescale layouts through to content from the Great Model Railway Challenge – that's appropriate for the show with probably the widest diversity we see in the exhibition calendar, with plenty of families coming on the Sunday, especially if it's a nice spring day! We look forward to seeing you there if you can make it.

Bachmann announced its Thomas range

for the UK and Ireland market at the Toy Fair in January, which means a whole new host of toy stores will be bringing the children into the hobby. Hopefully they'll stay with us and in decades to come will be gracing the pages of magazines with their creations.

Hornby will surely be delighted with their results in the British Railway Modelling Awards this year, taking the coveted Overall Manufacturer of the Year title for 2019. The top three in each category are listed on Page 102 and you can see the full results for all categories on RMweb.

We have had a lot of positive feedback on the March issue of BRM in particular, so thanks to all correspondents. We love the content in this month's magazine and we've got some other great layouts coming up this year. If you would like to see your layout featured in BRM, please do get in touch; we are always happy to see something fresh!

BRM Team

Featuring on page 30, skilled clocksmith, Kier Hardy, created this exceptionally-well observed slice of North London.





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31-520SF Class 159 DMU
Bachmann 3-Car DMU 159013 BR Network SouthEast (Revised) - Sound Fitted
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31-520 Class 159 DMU
Bachmann 3-Car DMU 159013 BR Network SouthEast (Revised)
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38-727 Bogie Flat Wagon
31-676A Bachmann Class 85 E3057 BR Electric Blue
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32-441 Class 24/1 D5149
Bachmann Class 24/1 D5149 BR Green (Small Yellow Panels)
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32-442 Class 24/1
Bachmann Class 24/1 24137 BR Blue Livery
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35-500 Class 117 DMU
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35-501 Class 117 DMU
Bachmann Class 117 3-Car DMU BR Blue & Grey
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35-525 Class 121 DMU
Bachmann Class 121 Single-Car DMU BR Green (Speed Whiskers)
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31-460A E&CR C Class
Bachmann SE&CR C Class 583 SE&CR Lined Green (Original)
Was £224.95 **NOW £191.21**



R3805 Class 5MT
Hornby BR Class 5MT 4-6-0 Black 5 Locomotive 45379 - Era 11
Was £175.00 **NOW £157.49**



R3737 A4 Class
Hornby BR A4 Class 4-6-2 60022 'Mallard' - Era 4
Was £179.99 **NOW £161.99**



R3679 Peckett W4 Class
Hornby Port of London Authority Peckett W4 Class 0-4-0ST No. 74 - Era 3/4
Was £99.99 **NOW £89.99**

N GAUGE



372-331 3MT Tank 82020
Graham Farish N Gauge BR Standard 3MT Tank 82020 BR Green (Late Crest)
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371-641 Class 70 Colas
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371-640 Class 70 70015
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31-062 Bachmann J72 Tank
68696 BR Black (Late Crest) Real Coal
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31-063 Bachmann EI Tank
NER EI Tank 2173 NER Lined Green
Real Coal & Weathered by TMC
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R3756 Hornby Crosti 9F
Railroad BR Crosti Boiler 9F Class 92028
BR Black Real Coal & Deluxe Weathered
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31-460A Bachmann C Class
C Class 583 SE&CR Lined Green Real
Coal Glossed & Deluxe Weathered
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2730 Heljan Class 27
27001 BR Green Full Yellow Ends (V3)
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R3746 Hornby Class 31
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no.31102 (Era 7) Weathered by TMC
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31-368DS Bachmann Class 03
Class 03 03026 BR Blue DCC Sound
Fitted Deluxe Weathering by TMC
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32-030DS Bachmann Class 20
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38-802 Bachmann Steam Crane
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ly Emblem) Deluxe Weathering by TMC
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38-803 Bachmann Steam Crane
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mental Red with Deluxe Weathering by TMC
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32-610 Class 90 - 90005
Bachmann Financial Times BR InterCity
Swallow Plates Fitted & TMC Weathered
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87014 Class 87
Hornby Intercity Knight of the Thistle
DCC Fitted Parts & TMC Weathered
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32-791DS Class 37/0
Bachmann D6739 BR Green Split Hcode
SYP DCC Sound Fitted TMC Weathered
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4D-011-009 Railcar
Dapol Streamlined Railcar GWR Twin
Cities Choc/Cm no.16 TMC Weathered
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R3605TTS Class 60
Hornby DB Schenker Co-Co 60044
Dowlow (Era 10) Weathered by TMC
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32-287 Class 101
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38-803 Breakdown Crane
Bachmann OO Ransomes
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£212.46



39-277 GUV BR Blue
Bachmann BR Mk1 General Utility Van
Was £47.95 *NOW* £28.78



R3771 Hornby A4
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R3738 Hornby A3 4472
LNER Flying Scotsman Double Tender
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31-481 Bachmann G2A
LNWR 49106 BR Black - Weathered
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31-627B Bachmann 3F Johnson
MR Tender 3520 LMS Black (Original)
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31-004A Bachmann O4
LNER Robinson 63762 Weathered
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CURRENT LIMITED EDITION PROJECTS

G5 0-4-4T Locomotive - This project is currently at the CAD stage, **expected Early 2021**, for more information on models being products please contact us or view the website.

24.5T Mineral Wagons - This project is also at the CAD stage, **expected late 2020**, for more information on models being products please contact us or view the website.

32-482Z & 32-482ZSF Bachmann Class 40 - OO Scale model locomotive of D233 'Empress of England' in BR Blue livery with Indicator Discs, **IMMINENT RELEASE**.

66025421 Heljan Class 25 25102 - BR Two Tone Green Full Yellow Ends, unusual livery as this class 25 survived 10 years into BR Blue still in two tone green! **IMMINENT RELEASE**.

TMC WEATHERED PRODUCTS N GAUGE



372-428 Graham Farish
WD Austerity 3085 LNER Black (LNER
Original) Real Coal & Weathered by TMC
Was £217.45 *NOW* £192.76



372-777 Graham Farish C Class
SE&CR C Class 31227 BR Black E/Emb
Real Coal & Weathered by TMC
Was £174.95 *NOW* £154.75



2S-008-011 Dapol Class A4
A4 Silver Fox 2512 LNER Silver Grey
Valanced Real Coal & Weathered by TMC
Was £186.30 *NOW* £171.76



2D-010-011 Dapol
Class 67 Royal Diamond 67029 DB Silver
Weathered by TMC
Was £157.96 *NOW* £145.25



371-651A Graham Farish
Class 57/0 57008 Freightliner Explorer
Freightliner Green Weathered by TMC
Was £174.95 *NOW* £153.25



Dapol Renumbered Class 50
Dapol Class 50 Monarch 50010 BR Large
Logo Blue Weathered by TMC
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THIS MONTH AT

Compiled between 9th January and 6th February 2020

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Latest News



Oxford Railgun Trainpack

The latest version of this popular model is now in stock, this time featuring a sound fitted loco with firing sound for the railgun.

www.hattons.co.uk/tmah63



Heljan OO Class 33 Update

Due in mid-2020, Heljan have now confirmed the RRP for their re-tooled OO Class 33s with hi-intensity headlights. Pre-order yours today

www.hattons.co.uk/tmah64



Heljan O Class 40 Update

The RRP and selling price for Heljans re-tooled O Gauge Class 40 has now been released. Pre-order yours now for £466.65 each.

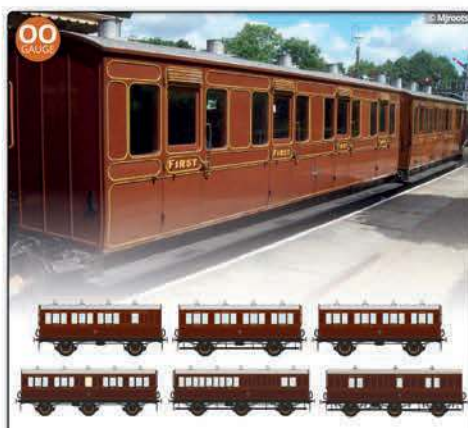
www.hattons.co.uk/tmah65



Heljan O Class 56 Update

Another RRP confirmed by Heljan recently was for their O Gauge Class 56. Available to pre-order now for £594.15 each. See more online.

www.hattons.co.uk/tmah66



Genesis - LBSCR Artwork

We have now released the livery artwork for the LBSCR mahogany liveries versions of our OO Genesis coaches. See more on our website.

www.hattons.co.uk/tmah67



Heljan OO9 2-4-2T "Lyn" Update

Heljan have also confirmed the RRP for their forthcoming OO9 model of the L&B 2-4-2T locomotive. Pre-order yours today!

www.hattons.co.uk/tmah68

Top 5 Best Sellers



Oxford Diecast
76CP001 - £5.50



Oxford Rail
OR763TO005B - £62



Hornby
R3603TTS - £139.50



Dapol
4D-014-001 - £129.15



Hornby
R6909 - £21

Forthcoming Releases from Hornby & Dapol

All available to pre-order now at: www.hattons.co.uk

HORNBY® 2020 Range Announcement - see details at www.hattons.co.uk/hornby2020

LMS Princess Royal 4-6-2

Pre-order from £171. DCC-fitted £184.50
Due in Stock February 2020



R3709 6201 "Princess Elizabeth" in LMS crimson lake
R3711 46206 "Princess Marie Louise" in BR express passenger blue
R3713 46207 "Princess Arthur of Connaught" in BR maroon

LNWR W1 'Hush-Hush' 4-6-4

Pre-order for £180
Due in Stock January 2021



R3840 10000 in LNER battleship grey
R3841 10000 'British Enterprise' in LNER battleship grey
R3842 10000 in LNER apple green
R3843 10000 in LNER Garter blue - rebuilt
R3844 60700 in BR green with early emblem - rebuilt

LNWR Thompson Class A2/2 & A2/3

Pre-order for £171
Due in Stock December 2020



Class A2/2
R3830 60501 'Cock o' the North' in BR green with early emblem
R3831 60505 'Thane of Fife' in BR green with late crest
Class A2/3
R3832 500 'Edward Thompson' in LNER apple green
R3833 514 'Chamossaire' in LNER apple green
R3834 60512 'Steady Aim' in BR green with early emblem
R3835 60523 'Sun Castle' in BR green with late crest

Standard Class 2MT 2-6-0 78xxx

Pre-order for £162
Due in Stock January 2021



R3836 78047 in BR black with late crest
R3838 78010 in BR black with early emblem
R3839 78000 in BR unlined green with late crest

Class 91

Pre-order for £153
Due in Stock December 2020



R3890 91002 'Durham Cathedral' in Intercity Swallow livery
R3891 91118 'The Fusiliers' in LNER livery
R3892 91111 'For the Fallen' in Virgin Trains East Coast livery
R3893 91117 'Cancer Research UK' in GNER livery

Class 370 APT-P

Pre-order from £356
Due in Stock December 2020



R3873 370003 & 370004 - 5-car pack - £437
R3874 370001 & 370002 - 7-car pack - £356
R3948 Non-Driving Motor 49004 (powered) - £99

Additional coach packs also available.
Codes between R40011 and R40014.

dapol Forthcoming Items

See the full range of forthcoming items from Dapol at www.hattons.co.uk/dapolpreorders

N Gauge & OO Gauge

Streamlined Battle of Britain/ West Country 4-6-2 in N Gauge

Pre-order from £169.96
Due in Stock: TBC



2S-034-001 "Fighter Command" in SR malachite green
2S-034-002 34066 "Spitfire" in BR green with late crest
2S-034-003 34110 "66 Squadron" in BR green with early emblem
2S-034-004 21C113 "Okehampton" in SR malachite green
2S-034-005 34030 "Watersmeet" in BR green with late crest

Class 5101 'Large Prairie' 2-6-2T

Pre-order from £115.56
Due in Stock: TBC



4S-041-001 5109 in GWR green with Great Western lettering
4S-041-002 6126 in GWR green with Great Western lettering
4S-041-003 5108 in GWR green with shirtbutton emblem
4S-041-004 5150 in GWR green with GWR lettering
4S-041-005 5190 in BR black with BRITISH RAILWAYS lettering
4S-041-006 4134 in BR lined green with early emblem
4S-041-007 6167 in BR lined green with late crest

OO Gauge

Class 43xx GWR Mogul 2-6-0

Pre-order from £135.96
Due in Stock: June 2020



4S-043-001 6336 in GWR green with Great Western lettering
4S-043-002 6385 in GWR green with shirtbutton emblem
4S-043-003 GWR green with BR smokebox numberplate
4S-043-004 7324 in BR black with early emblem
4S-043-005 6364 in BR lined green with early emblem
4S-043-006 7310 in BR lined green with late crest

Class 59

Pre-order from £131.71
Due in Stock: August 2020



4D-005-000 59002 "Alan J Day" in Foster Yeoman livery
4D-005-001 59103 "Village of Mells" in ARC livery
4D-005-002 59206 "John F Yeoman" in DB Schenker livery
4D-005-003 59204 "Vale of Glamorgan" in National Power livery

O Gauge

Class 66

Pre-order from £276.25
Due in Stock: March 2021



7D-066-001 66001 in EWS red and gold
7D-066-002 66504 in Freightliner 'Powerhaul' livery
7D-066-003 66709 "Sorrento" in GBRf / MSC livery
7D-066-004 66421 in DRS plain blue
7D-066-005 66789 "British Rail 1948 - 1997" in BR large logo blue with GBRf branding
7D-066-006 66783 "The Flying Dustman" in Biffa red with GBRf branding

BR Mark 1 coaches

Pre-order for £169.15
Due in Stock: February to March 2020



BSK, SO, SK and CK coaches available in multiple liveries, including: BR crimson and cream, BR green, BR chocolate and cream, BR maroon and BR blue & grey.

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Tel: 0151 733 3655

Pre-orders accepted
via credit or debit card only
Pre-order prices subject to manufacturer RRP.

Genesis 4 & 6 Wheel Coaches

Due in stock: from Q1 2021

Produced by: *Hattons*

Order yours online:
www.hattons.co.uk/genesis

Project Genesis faithfully represents the trains of the Era 2 & 3 period and brings modellers the opportunity to run a detailed train of coaches in liveries not normally seen in ready-to-run form.

We are producing 6 body styles between the 4 and 6 wheel coaches. These will be used to represent a lot of different coaches when allied to the painting and printing we are applying to them.

The individual styling has been made to include the most common features from some of the most widespread and longest lasting coaches to help make them recognisable to customers.

GWR chocolate & cream



H4-4BT-101A	4 wheel brake 3rd 203 - £30/£36
H4-4BT-101B	4 wheel brake 3rd 197 - £30/£36
H4-4F-101	4 wheel 1st 165 - £30/£36
H4-4C12-101	4 wheel composite 96 - £30/£36
H4-4T-101A	4 wheel 3rd 394 - £30/£36
H4-4T-101B	4 wheel 3rd 306 - £30/£36
H4-6BT-101A	6 wheel brake 3rd 154 - £30/£36
H4-6BT-101B	6 wheel brake 3rd 148 - £30/£36
H4-6C123-101	6 wheel tri-composite 84 - £30/£36
H4-6T-101A	6 wheel 3rd 526 - £30/£36
H4-6T-101B	6 wheel 3rd 539 - £30/£36
H4-6S-101	6 wheel 2nd 105 - £30/£36
H4-6CL-101	6 wheel composite lavatory (1st/3rd) 42 - £30/£36
H4-6FB-101	6 wheel full brake
H4-46Pack-101	Pack of 4 coaches (4BT, 4T, 6C123, 6BT) - £110/£135

GNR lined teak



H4-4BT-201A	4 wheel brake 3rd 56 - £30/£36
H4-4BT-201B	4 wheel brake 3rd 79 - £30/£36
H4-4F-201	4 wheel 1st 235 - £30/£36
H4-4T-201A	4 wheel 3rd 1432 - £30/£36
H4-4T-201B	4 wheel 3rd 1645 - £30/£36
H4-6BT-201A	6 wheel brake 3rd 135 - £30/£36
H4-6BT-201B	6 wheel brake 3rd 145 - £30/£36
H4-6T-201A	6 wheel 3rd 2694 - £30/£36
H4-6T-201B	6 wheel 3rd 3275 - £30/£36
H4-6C13-201	6 wheel composite 93 - £30/£36
H4-6F-201	6 wheel 1st 2040 - £30/£36
H4-6CL-201	6 wheel composite lavatory 864 - £30/£36
H4-6FB-201	6 wheel full brake - £30/£36
H4-46Pack-201	Pack of 4 coaches (4BT, 4T, 6C12, 6BT) - £110/£135

LNWR Plum & spilt milk



H4-4BT-301A	4 wheel brake 3rd 6925 - £30/£36
H4-4BT-301B	4 wheel brake 3rd 7342 - £30/£36
H4-4F-301	4 wheel 1st 4568 - £30/£36
H4-4C13-301	4 wheel composite 3654 - £30/£36
H4-4T-301A	4 wheel 3rd 896 - £30/£36
H4-4T-301B	4 wheel 3rd 731 - £30/£36
H4-6BT-301A	6 wheel brake 7523 3rd - £30/£36
H4-6BT-301B	6 wheel brake 7005 3rd - £30/£36
H4-6C123-301	6 wheel tri-composite 4004 - £30/£36
H4-6T-301A	6 wheel 1987 3rd - £30/£36
H4-6T-301B	6 wheel 1892 3rd - £30/£36
H4-6F-301	6 wheel 1st 4690 - £30/£36
H4-6CL-301	6 wheel composite lavatory 4020 - £30/£36
H4-6FB-301	6 wheel full brake - £30/£36
H4-46Pack-301	Pack of 4 coaches (4BT, 4T, 6C123, BT) - £110/£135

SECR Crimson Lake



H4-4BT-401A	4 wheel brake 3rd 3069 - £30/£36
H4-4BT-401B	4 wheel brake 3rd 3058 - £30/£36
H4-4F-401	4 wheel 1st 1041 - £30/£36
H4-4C12-401	4 wheel composite 2062 - £30/£36
H4-4T-401A	4 wheel 3rd 3361 - £30/£36
H4-4T-401B	4 wheel 3rd 3262 - £30/£36
H4-6BT-401A	6 wheel brake 3rd 6525 - £30/£36
H4-6BT-401B	6 wheel brake 3rd 6536 - £30/£36
H4-6T-401A	6 wheel 3rd 2956 - £30/£36
H4-6T-401B	6 wheel 3rd 2960 - £30/£36
H4-6F-401	6 wheel 1st 1015 - £30/£36
H4-6FB-401	6 wheel full brake - £30/£36
H4-46Pack-401	Pack of 4 coaches (4BT, 4C12, 6T, 6BT) - £110/£135

LMS crimson (Midland style)



H4-6BT-501A	6 wheel brake 3rd 6523 - £30/£36
H4-6BT-501B	6 wheel brake 3rd 6498 - £30/£36
H4-4X6CL-501	ex 6 wheel composite lavatory 9531 - £30/£36
H4-6T-501A	6 wheel 3rd 5705 - £30/£36
H4-6T-501B	6 wheel 3rd 5054 - £30/£36
H4-6FB-501	6 wheel full brake - £30/£36
H4-46Pack-501	Pack of 4 coaches (6BT, 4X6CL, 6T, 6T) - £110/£135

LNWR pre-war brown



H4-6BT-601A	6 wheel brake 3rd 4118 - £30/£36
H4-6BT-601B	6 wheel brake 3rd 4122 - £30/£36
H4-6CL-601	6 wheel composite lavatory 486 - £30/£36
H4-6T-601A	6 wheel 3rd 42956 - £30/£36
H4-6T-601B	6 wheel 3rd 43008 - £30/£36
H4-6FB-601	6 wheel full brake - £30/£36
H4-46Pack-601	Pack of 4 coaches (6BT, 6CL, 6T, 6BT) - £110/£135

SR olive green



H4-4BT-701A	4 wheel brake 3rd 3654 - £30/£36
H4-4BT-701B	4 wheel brake 3rd 3594 - £30/£36
H4-4C13-701	4 wheel composite 5750 - £30/£36
H4-4T-701A	4 wheel 3rd 1501 - £30/£36
H4-4T-701B	4 wheel 3rd 1717 - £30/£36
H4-6BT-701A	6 wheel brake 3rd 3602 - £30/£36
H4-6BT-701B	6 wheel brake 3rd 3615 - £30/£36
H4-6CL-701	6 wheel composite lavatory 5790 - £30/£36
H4-6T-701A	6 wheel 3rd 1524 - £30/£36
H4-6T-701B	6 wheel 3rd 1770 - £30/£36
H4-6FB-701	6 wheel full brake - £30/£36
H4-46Pack-701	Pack of 4 coaches (4BT, 4T, 6C13, 6BT) - £110/£135

LBSCR mahogany



H4-4BT-1601A	4 wheel brake 3rd - £30/£36
H4-4BT-1601B	4 wheel brake 3rd - £30/£36
H4-4C13-1601	4 wheel composite - £30/£36
H4-4F-1601	4 wheel 1st - £30/£36
H4-4T-1601A	4 wheel 3rd - £30/£36
H4-4T-1601B	4 wheel 3rd - £30/£36
H4-6BT-1601A	6 wheel brake 3rd - £30/£36
H4-6BT-1601B	6 wheel brake 3rd - £30/£36
H4-6CL-1601	6 wheel composite lavatory - £30/£36
H4-6F-1601	6 wheel 1st - £30/£36
H4-6FB-1601	6 wheel full brake - £30/£36
H4-6T-1601A	6 wheel 3rd - £30/£36
H4-6T-1601B	6 wheel 3rd - £30/£36
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£2.75 per single diecast item

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OO Gauge (1:76 Scale)

Steam locos



H4-AB14-004 Andrew Barclay 0-4-0ST 14" 1863 in Caledonian Railway lined blue (Hat) £99



H4-AB14-007 Andrew Barclay 0-4-0ST 14" 1927 'No.1' in Burnley Corporation Gas Department blue (Hat) £99



H4-AB14-001 Andrew Barclay 0-4-0ST 14" 2047 '705' in BR black early emblem (Hat) £99



H4-AB14-008 Andrew Barclay 0-4-0ST 14" 2069 'Little Barford' in Adlon Lane Power Station blue wesp stripes (Hat) £99



H4-AB14-003 Andrew Barclay 0-4-0ST 14" 2134 'Coronation' lined green (Hat) £99



H4-AB16-002 Andrew Barclay 0-4-0ST 16" 2043 'No 6' in NCB green (Hat) £99



H4-AB16-003 Andrew Barclay 0-4-0ST 16" 2226 'Katie' in lined maroon (Hat) (RRP £99) **BARGAIN** £89



266217 Beyer Garratt 2-6-0 0-6-2 47979 in BR black LMS lettering and revolving coal bunker (Hel) £212



266219 Beyer Garratt 2-6-0 0-6-2 47988 in BR black early emblem and revolving coal bunker - lightly weathered (Hel) £221



266220 Beyer Garratt 2-6-0 0-6-2 47993 in BR black early emblem and revolving coal bunker - heavily weathered (Hel) £221



266215 Beyer Garratt 2-6-0 0-6-2 4982 in LMS black revolving coal bunker (Hel) £212



266214 Beyer Garratt 2-6-0 0-6-2 4984 in LMS black original coal bunker - lightly weathered (Hel) £221



266222 Beyer Garratt 2-6-0 0-6-2 4986 in LMS workshop photographic grey revolving coal bunker (Hel) £212



1303 Class 1361 0-6-0ST 1365 in BR black early emblem (Hel) £135.96



R3727 Class 21 L&Y 'Pug' 0-4-0ST 11244 in LMS black (Hor) £63



R3383TTS Class 4073 Castle 4-6-0 5050 'Earl of St Germans' in BR green early emblem - TTS sound fitted (Hor) £169.50



OR76AR006 Class 415 Adams Radial 4-4-2T 3520 in Southern Railway green (Oxf) £94



4780 Class 47xx 2-8-0 'Night Owl' 4704 GWR green shiributton emblem (Hel) £154



4782 Class 47xx 2-8-0 'Night Owl' 4707 GWR green post-war GW lettering (Hel) £154



R3804 Class 49xx 'Hall' 4-6-0 5972 'Hogwarts Castle' in Hogwarts Railways red - Harry Potter range (Hor) £99



R3803TTS Class 49xx 'Hall' 4-6-0 5972 'Hogwarts Castle' in Hogwarts Railways red - TTS sound fitted - Harry Potter range (Hor) £122.50



R3805 Class 5MT 'Black Five' 4-6-0 45379 in BR black late crest - as preserved - Ltd Ed of 1000 (Hor) £157.50



R3616 Class 5MT 'Black Five' 4-6-0 5089 in LMS black (Hor) £153



R3643 Class 7MT 'Britannia' 4-6-2 70046 'Anzac' in BR green late crest (Hor) £162



R3638 Class 7P6F 'Streamlined West Country' 4-6-2 34010 'Bideford' in BR green late crest (Hor) £166.50



R3682 Class 8P 'Princess Coronation' 4-6-2 46225 'Duchess of Gloucester' in BR express passenger blue (Hor) (RRP £204.99) **BARGAIN** £144



R3681 Class 8P 'Princess Coronation' 4-6-2 6241 'City of Edinburgh' in LMS post-war lined black (Hor) (RRP £204.99) **BARGAIN** £144



R3715 Class 8P 'Streamlined Coronation' 4-6-2 6224 'Princess Alexandra' in LMS coronation blue (Hor) £171



R3677 Class 8P 'Streamlined Coronation' 4-6-2 6229 'Duchess of Hamilton' in LMS crimson lake - as preserved (Hor) (RRP £189.99) **BARGAIN** £139.50



R3639 Class 8P 'Streamlined Coronation' 4-6-2 6244 'King George VI' in LMS crimson lake (Hor) (RRP £189.99) **BARGAIN** £139.50



R3617 Class 8P Rebuilt Merchant Navy 4-6-2 35030 'Elder Dempster Lines' in BR green late crest (Hor) £144



R3756 Class 9F Crosti 92028 in BR black early emblem - heavily weathered - Railroad range (Hor) £112.50



R3663TTS Class A1 4-6-2 60163 'Tornado' in LNER apple green British Railways lettering - Railroad Range - TTS sound fitted (Hor) £122.50



R3811 Class A1 Terrier 0-6-0T 48 'Leadenhall' in LB&SCR improved engine green (Hor) **NEW** £81



R3781 Class A1 Terrier 0-6-0T 5 'Rolvenden' in Kent and East Sussex Railway blue (Hor) £81



R3812 Class A1 Terrier 0-6-0T W10 'Cowes' in Southern Railway olive green (Hor) **NEW** £81



R3627 Class A3 4-6-2 60103 'Flying Scotsman' BR express passenger blue (Hor) £162



R3737 Class A4 4-6-2 60022 'Mallard' in BR blue early emblem (Hor) £162



R3522 Class A4 4-6-2 60026 'Miles Beevor' in BR green early emblem (Hor) £149



R3701 Class A4 4-6-2 60028 'Walter K Whigham' in BR experimental purple (Hor) £153



R3451 Class B1 4-6-0 61032 'Stembok' in BR black early emblem (Hor) £148



R3544 Class B12/3 4-6-0 8527 in LNER apple green (Hor) **NEW** £145



R3622 Class J36 0-6-0 65311 'Haig' in BR black early emblem (Hor) (RRP £139.99) **BARGAIN** £99



R3621 Class J36 0-6-0 722 in LNER black (Hor) (RRP £139.99) **BARGAIN** £99



R3603TTS Class LN 'Lord Nelson' 4-6-0 30850 'Lord Nelson' in BR green late crest - TTS sound fitted (Hor) (RRP £194.99) **BARGAIN** £139.50



R3635 Class LN 'Lord Nelson' 4-6-0 30863 'Lord Rodney' in BR green early emblem (Hor) (RRP £169.99) **BARGAIN** £124



R3634 Class LN 'Lord Nelson' 4-6-0 851 'Sir Francis Drake' in Southern Railway olive green (Hor) (RRP £169.99) **BARGAIN** £124



OR76N7001 Class N7 0-6-2T 1002 in Great Eastern Railway wartime grey (Oxf) £98



OR76N7001XS Class N7 0-6-2T 1002 in Great Eastern Railway wartime grey - DCC Sound fitted (Oxf) £187



OR76N7003 Class N7 0-6-2T 69612 in BR black early emblem (Oxf) £98



OR76N7002 Class N7 0-6-2T 8011 in LNER black (Oxf) £98



R3729 Class O1 2-8-0 6359 in LNER black (Hor) £99



R3730 Class O1 2-8-0 63806 in BR black early emblem (Hor) £99



H4-P-001 SECR P Class 0-6-0T 178 in SE&CR full lined green (with brass) (Hat) £99



H4-P-002 SECR P Class 0-6-0T 753 SE&CR full lined green (with brass) (Hat) £99



R3500 The Sir Nigel Gresley Collection (Hor) £474



R3402 Queen of Scots Train Pack Class A4 4-6-2 in LNER garter blue and three pullman coaches - Ltd Ed of 1000 (Hor) £324



OR76B00M01 Railgun Pack railgun 'Boche Buster' and 'Dean Goods' 0-6-0 2330 in ROD khaki (Oxf) **NEW** £247



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



1721 Class 17 'Clayton' D8502 in BR green small yellow panels (Hel) £118.96



1723 Class 17 'Clayton' D8534 in BR blue (Hel) £118.96



MM0230 Class 201 no 230 'River Bandon' IE original Enterprise (Mur) (RRP £145) **BARGAIN** £128



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



R3576 4-wheel Sentinel 4wdH 19 in green chevrons (Hor) £64



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4D-025-004 Class 21 D6111 in BR green small yellow panels (Dap) **NEW** £129.15

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 4D-025-003D Class 21 D6116 BR green small yellow panels - DCC fitted (Dap) £154.66
 4D-025-002 Class 21 D6120 in BR green (Dap) £129.15
 4D-025-002D Class 21 D6120 in BR green - DCC fitted (Dap) £154.66



4D-025-001 Class 21 D6121 in BR green (Dap) £129.15
 4D-025-001D Class 21 D6121 in BR green - DCC fitted (Dap) £154.66
 2656 Class 26D 26010 in BR railfreight grey red stripe (Heli) £110.46



2730 Class 27 27001 in BR green full yellow ends - heavily weathered (Heli) £127.46

NEW

2718 Class 27 27029 in BR blue Highland Rail emblem (Heli) **NEW** £118.96

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 2717 Class 27 27033 in BR blue Haymarket depot emblem (Heli) **NEW** £118.96
 2732 Class 27 5357 in BR blue (Heli) **NEW** £118.96
 2719 Class 27 5380 BR two tone green full yellow ends - weathered (Heli) £127.46
 2731 Class 27 D5369 in BR green small yellow panels (Heli) **NEW** £118.96

NEW

4D-014-001 Class 29 D6114 in BR two tone green small yellow panels (Dap) **NEW** £129.154D-014-002 Class 29 D6129 in BR blue (Dap) **NEW** £129.15

R3745 Class 31/6 31602 "Driver Dave Green" in Network Rail yellow (Hor) £157.50
 32-790 Class 37D 37049 "Imperial" Railfreight Coal Sodor triple grey (Bac) £154.50



R3757 Class 47/7 47798 "Prince William" in EWS Royal Train claret - Railroad range (Hor) £72



4D-003-016 Class 52 "Western" D1043 "Western Duke" in BR chromatic blue small yellow panels (Dap) £129.15



R3774 Class 67 67024 in Belmond British Pullman umber and cream (Hor) £144
 31-590 Class 70 70015 in Freightliner air intake modifications (Bac) £184.50
 4D-006-010S Class 73D E6004 BR green grey solebar - DCC sound fitted (Dap) £258



BARGAIN

4D-006-006 Class 73/1 electro-diesel 73138 in Civil Engineers "Dutch" (Dap) (RRP £146.22) **BARGAIN** £91
 OLIV001 Class 73/2 73337 in Gathwick Express - Olivas Trains limited edition (Dap) (RRP £140) **BARGAIN** £84



R3707 Ruston 48DS "Queen Anne" in Longmorn Distillery match wagon (Hor) £72



R3705 Ruston 48DS 458957 in John Dewar and Sons red match wagon (Hor) £72



R3706 Ruston 48DS Army 802 in War Department green match wagon (Hor) £72

**Diesel train packs**

R3750 Belmond Pullman Train Pack Class 67 67021 in Pullman and three pullman coaches (Hor) £270



H4-B-J-002 Janus bundle 0-6-0 Janus diesel shunter in ICI maroon three matching open wagons (Hat) (RRP £129.90) **BARGAIN** £69.50



R3697 Northern Belle train pack Class 57/6 57601 "Windsor Castle" and three Mk2D coaches in Northern Belle (Hor) £135

**Electric locos**

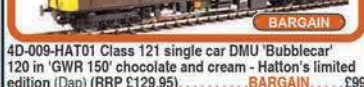
7601 Class 76 EM1 Woodhead electric E26051 in BR green half yellow panels - Ltd Ed for Olivas Trains (Heli) £160



R3739 Class 87 87001 "Royal Scot" and "Stephenson" in BR blue - as preserved (Hor) **NEW** £154
 R3656 Class 87 87019 "Sir Winston Churchill" Virgin Trains red and black (Hor) £153



R3740 Class 92 92023 in Caledonian Sleeper (Hor) £76.50
 R3741 Class 92 92043 in GBRf (Hor) £76.50

**DMUs**4D-009-HAT01 Class 121 single car DMU 'Bubblecar' 120 in 'GWR 150' chocolate and cream - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £994D-009-HAT04 Class 121 single car DMU 'Bubblecar' 121020 in Chiltern Railways blue - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £89

4D-009-005 Class 121 single car DMU 'Bubblecar' 121027 "Bletchley TMD" in Silverlink purple and green (Dap) (RRP £145) **BARGAIN** £79



BARGAIN

4D-009-HAT07 Class 121 single car DMU 'Bubblecar' 55022 in original Network South East - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £99



4D-009-006 Class 121 single car DMU 'Bubblecar' 55026 in BR blue and grey Highland Rail stag (Dap) (RRP £145) **BARGAIN** £79



4D-009-007 Class 121 single car DMU 'Bubblecar' 55032 in BR blue and grey Welsh Dragon emblem (Dap) (RRP £145) **BARGAIN** £79



4D-009-008 Class 121 single car DMU 'Bubblecar' 55032 in Midline West Midlands (Dap) (RRP £145) **BARGAIN** £79



4D-009-HAT06 Class 121 single car DMU 'Bubblecar' 960010 in Railtrack 'coaching stock' maroon - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £89



4D-009-HAT03 Class 121 single car DMU 'Bubblecar' 960011 in Railtrack blue and green - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £99



4D-009-009 Class 121 single car DMU 'Bubblecar' 97723 in Railtrack red and white (Dap) (RRP £145) **BARGAIN** £79



4D-015-HAT01 Class 122 Gloucester RCW "Bubblecar" single car DMU TDB975023 in Route Learning BR blue - Hatton's limited edition (Dap) (RRP £129.95) **BARGAIN** £99



4D-015-009 Class 122 single car DMU 'Bubblecar' W55006 in BR green small yellow panels (Dap) £123.25



4D-015-008 Class 122 single car DMU 'Bubblecar' W55018 in BR green speed whiskers (Dap) £123.25



R3773 Class 156 2-car 'Super Sprinter' DMU 156401 in Provincial (Hor) £112.50



4D-011-100 Streamlined Parcels Railcar 17 in GWR chocolate and cream Express Parcels branding (Dap) **NEW** £125.76
 4D-011-100D Streamlined Parcels Railcar 17 in GWR chocolate and cream Express Parcels branding - Digital fitted (Dap) **NEW** £151.90



4D-011-101 Streamlined Parcels Railcar W17W in BR crimson Express Parcels branding (Dap) **NEW** £125.76
 4D-011-101D Streamlined Parcels Railcar W17W in BR crimson Express Parcels branding - Digital fitted (Dap) **NEW** £151.90



NEW

4D-011-006 Streamlined Railcar 10 in GWR chocolate and cream shirbutton emblem (Dap) **NEW** £125.76
 4D-011-006D Streamlined Railcar 10 in GWR chocolate and cream shirbutton emblem - Digital fitted (Dap) **NEW** £151.90
 4D-011-005 Streamlined Railcar 12 in GWR chocolate and cream shirbutton emblem (Dap) **NEW** £125.76
 4D-011-005D Streamlined Railcar 12 in GWR chocolate and cream shirbutton emblem - Digital fitted (Dap) **NEW** £151.90
 4D-011-009 Streamlined Railcar 16 in GWR chocolate and cream twin cities crest (Dap) **NEW** £125.76
 4D-011-009D Streamlined Railcar 16 in GWR chocolate and cream twin cities crest - Digital fitted (Dap) **NEW** £151.90



4D-011-007 Streamlined Railcar W11 in BR chocolate and cream (Dap) **NEW** £125.76
 4D-011-007D Streamlined Railcar W11 in BR chocolate and cream - Digital fitted (Dap) **NEW** £151.90



4D-011-008 Streamlined Railcar W8W in BR crimson and cream (Dap) **NEW** £125.76
 4D-011-008D Streamlined Railcar W8W in BR crimson and cream - Digital fitted (Dap) **NEW** £151.90

**EMUs**

R3606 5-BEL Pullman Brighton Belle EMU end vehicles in Pullman umber and cream small yellow panels (Hor) £160



R3699 Class 2-HIL 2 car EMU in BR green (Hor) £126



R3813 Class 395 'Javelin' 4-car EMU 395013 'Hornby Visitor Centre' in Southeastern - Ltd Ed (Hor) **NEW** £144



R3700 Class 401 2-BIL 2 car EMU in SR green (Hor) £126

**Track Maintenance vehicles**

R8697 76 ton Cowans and Sheldon breakdown crane ADRC96200 in BR engineers yellow (Hor) £58.50



B800 Non-motorised OO Track Cleaner motorised cleaning heads & vacuum in blue (Dap) £64

**Train sets - analogue**

R1167 'Flying Scotsman' starter train set 4472 'Flying Scotsman' loco in LNER green and three Gresley teak coaches (Hor) £157



R1176 Class 373 'Eurostar' starter train set (Hor) £126



R1265 Hornby Family Fun Project - OO gauge model railway starter pack (Hor) £180
 R1215 Hornby Junior starter Train Set - 'Express Train' - battery powered (Hor) £25

Prices valid until 25/03/20

All prices correct
when going to press.
Errors excepted.



R1202 Mallard Pullman Train Set Class A4 4468 'Mallard' in LNER blue and pair of Pullman coaches (Hor) £144



R1230 Starter train set - 'GWR High Speed Train' (Hor) £108



R1234 Starter train set - 'Hogwarts Express' - Harry Potter range (Hor) £180

R1233 The Coca-Cola Christmas Train Set (Hor) £72

R1243 The Welshman train set GWR King and four Collett coaches - Hornby 'Signature' Collection (Hor) £351

Coaches



R4886A Bulleid 59' composite corridor S5718S in BR green (Hor) £43.50



R4899 Clerestory composite corridor 3229 in GWR chocolate and cream (Hor) £33.50

R4525 Collett restaurant car 9578 in GWR chocolate & cream - Railroad Range (Hor) (RRP £21.99) BARGAIN £10

R4830 Gansley 81' 6" full brake 4234 in LNER lark (Hor) £49.50

R4904 K type Pullman 'New Century Bar' Pullman under and cream (Hor) £53.50

R4935A Mk1 BSK brake second corridor 99312 in Hogwatts Railways maroon - Harry Potter range (Hor) £37



R4355 Mk1 BSK brake second corridor W34888 in BR chocolate & cream - Railroad Range (Hor) (RRP £21.99) BARGAIN £12

R4934 Mk1 SK second corridor 99716 in Hogwatts Railways maroon - Harry Potter range (Hor) £37



R4934A Mk1 SK second corridor 99721 in Hogwatts Railways maroon - Harry Potter range (Hor) £37

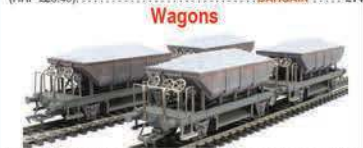
39-051E Mk1 SO second open M4929 BR maroon (Bac) (RRP £47.95) BARGAIN £33



OR763T0005B Mk3a TSO second open SC12014 and SC12030 in ScotRail - twin pack (Oxt) NEW £62

R4668B Mk4 TSO second open 12425 East Coast (Hor) (RRP £28.49) BARGAIN £14

Wagons



K4103 'Dogfish' ballast wagons in Civil Engineers 'dutch' - weathered - Pack of 4 - Ltd Ed for Kernow Model Rail Centre (Hel) £99.99

4F-012-025 12 ton ventilated van 6788101 in BR bauxite (Dap) NEW £9.50

4F-012-026 12 ton ventilated van 6788101 BR bauxite - weathered (Dap) NEW £10.50

37-225H 16 ton steel mineral wagon top flap doors BR grey - weathered (Bac) £18

4F-036-019 20 ton bulk grain hopper 701358 in LMS bauxite (Dap) NEW £17

4F-036-020 20 ton bulk grain hopper 701358 LMS bauxite - weathered (Dap) NEW £18

37-326C 30 Tonne JGA Bogie Hopper in RMC - weathered (Bac) £45.95



R6909 ex-LMS D2068 20 ton brake van M731456 in BR bauxite (Hor) £21

R6913A ex-LSWR 24 ton brake van 55003 in SN brown red ends (Hor) £22.50

R6913 ex-LSWR 24 ton brake van 55002 in SN brown red ends (Hor) £22.50



R6915A ex-LSWR 24 ton brake van S55032 in BR grey (Hor) £22.50



R6915 ex-LSWR 24 ton brake van S55040 in BR grey (Hor) £22.50



4F-053-003 IKA 'Megafret' twin container wagon 3368 490 9 460-0 in blue - weathered (Dap) £48.88

R6926 KFA Intermodal wagon in Tophook Rail - No Containers (Hor) £36

R6927 KFA Intermodal wagon Touxer Maritime and Hyundai containers (Hor) £36

R6907 LMS D1919 20 ton brake van 730176 in LMS grey (Hor) £21



R6911 LSWR 20 ton brake van 9646 in LSWR bauxite red ends (Hor) £22.50

R6883 Pack of three GWR horse boxes in GWR brown (Hor) £55



H4-RHTT-001 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules (Hat) £118



H4-RHTT-004 Rail Head Treatment Train 'Sandite' 2 wagons and sandite modules - weathered (Hat) £126



4F-027-014 Silver Bullet bogie tank wagon in NACCOI ECC - 3387 789 8 053-6 (Dap) NEW £27.16



4F-027-013 Silver Bullet bogie tank wagon in NACCOI ECC - 3387 789 8 077-5 (Dap) NEW £27.16



R6933 Tank Wagon in Coca-Cola® (Hor) £11.69

Digital controllers



R8213 DCC Select unit (boxed) (Hor) £98



36-504 EZ Command Dynamics Ultima DCC system (Bac) £280

Digital control accessories



R8232 DCC Electro point clips (20 in a pack - 2 used per point) (Hor) (RRP £7.35) BARGAIN £3

Switches and levers

R046 2-way lever frame switch 'On-Off' (Hor) £9

Lighting equipment

CL31 Coach lighting effect - cool white flickering tube (Tra) NEW £22

AL4 Modern flashing red tail light - automatic effects - battery powered (Tra) NEW £13.50

Cables & Wires

PL-80 Power feed joiners - Code 100/124 (x4 pairs) (Pac) £6

Model railway spares

SL-111 Finescale insulated rail joiners/insulators (Code 70, 75 & 83) for OO & HO gauge - Pack of 12 (Pac) £2

SL-110 Finescale rail joiners/insulators (for code 70, 75 & 83) for OO & HO gauge - Pack of 24 (Pac) £2

SL-111 Insulated rail joiners/insulators (for OO, HO & O gauge) code 100 rails incl. Hornby, Peco & Peco Streamline) - Pack of 12 (Pac) £1.80

SL-10 Metal rail joiners/insulators (for code 100 rails incl. Hornby, Peco & Peco Streamline). For OO, HO & O gauge - Pack of 24 (Pac) £2.20

19KADEE NEM302 Kadec coupling - Long (110.70mm) - Pack of four (Kad) £5

Wheel sets

R8218 14.1 mm coach wheels (pack of 10) (Hor) £14.50

R8264 14.1 mm 2 hole coach wheels (pack of 10) (Hor) £14.50

Buildings - railways

554Ratio AFP brick-built wartime signal box - plastic kit (Rat) NEW £24

LK-20X Double length overall arched station roof (Pec) £16

LK-58 Modular loco depot inspection pit kit (Pec) £7.50



PO235 Platform kit in stone - card kit (Met) £11

R8009 Station terminus building pack (Hor) £42.50

PO340 10' two-bay platform canopy - card kit (Met) £13.50

Man-made scenic structure - railway-related



89121 27.4 metre (90ft) Motorised turntable - ready for installation. For HO/OO scales. DCC ready (Hel) £210

LK-55 70ft Well type turntable (Pec) £42



R083 Pack of 6 Buffer Stop (Hor) £10

R083 Single (Hor) £2

R460 Straight Platform (Hor) £3.50

Signals

4L-001-005 Signal - motorised, GWR bracket right hand junction bracket signal - two arms (Dap) £55.21

4L-001-006 Signal - motorised, GWR left hand junction bracket signal - two arms (Dap) £55.50

4L-002-001 Signal - Motorised, LMS home (OOSIGN3) (Dap) £26

4L-001-003 Signal - Motorised, Round post GWR home (Dap) £26

Bridges, Tunnels & Walls



PO244 Retaining wall in red brick - 4 sections per pack - card kit (Met) £12

People - civil



36-407 1950s train crew - pack of six (Bac) £8.50

Track - Code 100 Setrack

R8090 1 single yard length of Semi-Flexible Track (Hor) £3.50

R8090 Pack of 24 (Hor) £79

R805 1st radius double curve (8 make a circle) (Hor) £2.60

R805 Circle of 8 (Hor) £20

R806 2nd radius curve (Hor) £1.60

R807 2nd radius double curve track (8 make a circle) (Hor) £2.50

R807 Circle of 8 (Hor) £18

R809 3rd radius double curve track (8 make a circle) (Hor) £2.60

R809 Circle of 8 (Hor) £20

ST-2035 4th radius Standard Curve Track (S12 x 8) (Pec) £18

R8262 Double 4th radius curve (8 make a circle) (Hor) £3.20



R070 Electrically Operated Turntable (Hor) £64

R8222 Extension Pack B. Contains 1 x R8073, 2 x R800, 4 x R807, 1 x R806, 1 x R803 (Hor) £26

R8223 Extension Pack C. Contains 1 x R8073, 2 x R800, 4 x R807, 1 x R806 (Hor) £26

R8226 Extension Pack F. Contains 1 x R8072, 1 x R8073, 2 x R800, 2 x R806, 1 x R803 (Hor) £24.50

R7277 Layouts made easy - large corner layout complete track pack (Hor) £618

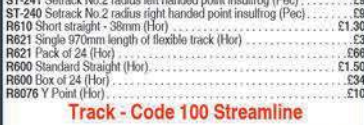
R7278 Layouts made easy - medium sized oval layout complete track pack (Hor) £374

R8074 Left hand curved point (Hor) £14

R8077 Left hand Express point (Hor) £14.50

R8072 Left hand Standard point (Hor) £9.50

R8075 Right hand curved point (Hor) £14



R8073 Right Hand Standard Point (Hor) £9.50

ST-245 Setrack curved double radius left handed point insultrug (Pec) £14.50

ST-244 Setrack curved double radius right handed point insultrug (Pec) £14.50

ST-201 Setrack double straight (Pec) £2

ST-241 Setrack No.2 radius left handed point insultrug (Pec) £9

ST-240 Setrack No.2 radius right handed point insultrug (Pec) £9

R810 Short straight - 38mm (Hor) £1.30

R821 Single 970mm length of flexible track (Hor) £3

R821 Pack of 24 (Hor) £66

R800 Standard Straight (Hor) £1.50

R800 Box of 24 (Hor) £34

R8076 Y point (Hor) £10

Track - Code 100 Streamline



SL-100 Pack of 25 1 yard (91.5cm) length of Code 100 Wooden-sleeper nickel silver flexible track (Pec) £66

SL-100 Single (Pec) £3

SL-102 1 yard (91.5cm) length of Nickel Silver concrete-sleeper flexible track (Pec) £3

SL-102 Pack of 25 (Pec) £70

SL-99 3 way medium radius point - insultrug (Pec) £27



SL-E88 Large Y point - electrofrog (Pec) £11.50

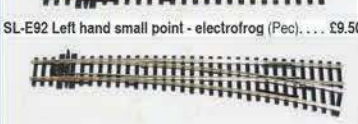
SL-E86 Large Y point - insultrug (Pec) £11.50

SL-E87 Left hand curved point - electrofrog (Pec) £12.50

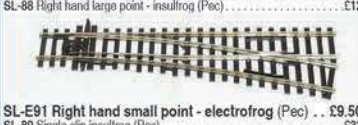


SL-E89 Left hand large point - electrofrog (Pec) £12

SL-E89 Left hand large point - insultrug (Pec) £12



SL-E92 Left hand small point - electrofrog (Pec) £9.50



SL-E86 Right hand curved point - electrofrog (Pec) £12.50

SL-E86 Right hand curved point - insultrug (Pec) £12.50

SL-E88 Right hand large point - insultrug (Pec) £12



SL-E91 Right hand small point - electrofrog (Pec) £9.50

SL-E91 Single slip insultrug (Pec) £32

Track - Code 75 Finescale

SL-108F 1 yard (91.5cm) length of Code 75 Wooden-sleeper nickel silver flexible track (Pec) £4

SL-108F Box of 25 (Pec) £98

SL-102F 1 yard (91.5cm) length of Finescale Nickel Silver concrete-sleeper flexible track (Pec) £3.30

SL-102F Pack of 25 (Pec) £79



SL-100F Pack of 25 1 yard (91.5cm) length of Nickel Silver Finescale flexible track (Pec) £72

SL-100F Single (Pec) £3



SL-E189 Finescale left hand large point electrofrog (Pec) £12.50



SL-E186 Finescale right hand curved point - electrofrog (Pec) £13

SL-E188 Finescale right hand large point - electrofrog (Pec) £12.50

Track underlay

SL-50 Roll of foam track underlay sleeper indentation. Suitable for Setrack and Streamline 5m long (16' 4") (Pec) £10

Publications - model and prototypical railways

R8159 Hornby 2020 range Catalogue - 66th Edition - 100th anniversary limited edition (Hor) NEW £9.50

Buses



UK6515 ADL Enviro400 MMC - 'Metroliner' (Nor) £51

UK6513 ADL Enviro400 MMC - 'Stagecoach South' (Nor) £51

UK6511 ADL Enviro400 MMC - 'Stagecoach West Scotland' (Nor) £51



Opening Hours
Store: 09:00 - 17:00 Mon - Sun
0151 733 3655



07:30 - 18:00 Mon - Sat
09:00 - 17:00 Sun

NEW



76VOL08REC Volvo FH Boniface Recovery Truck
Manchett's (Oxt) **NEW** £20.50
76WM007 Whitley Mondial Ice Cream Van Piccadilly Whip (Oxt) **NEW** £6

Military ground vehicles

NEW



76LRFCA003 Land Rover FC Ambulance 24 Field Ambulance, Bosnia (Oxt) **NEW** £9.50

Tools

SL-43 Loco Lift and storage unit (Pec) £12

O09 Narrow Gauge Steam locos



9963 Lynton & Barnstaple 2-6-2T 190 'Lyd' in Southern Railway green - as preserved (Hel) £186.96



9952 Lynton & Barnstaple 2-6-2T 761 'Taw' in Southern Railway green (Hel) (RRP £189.95) **BARGAIN** £149.50



9960 Lynton & Barnstaple 2-6-2T E188 'Lew' in Southern Railway green (Hel) £186.96

Coaches

GR-505 4-wheel first class coach in Glyn Valley Tramway green (as preserved at Talyllyn Railway) - buffers (Pec) £23

Track - Code 80 Setrack

ST-400 Setrack narrow gauge starter track pack - first radius (Pec) £68
ST-406 Setrack O09 left-hand point - insulfrog (Pec) £9.50
ST-405 Setrack O09 right-hand point - insulfrog (Pec) £9.50

Track - Code 80 Streamline

SL-400 1 yard length of O09 narrow gauge flexible track worn wooden sleepers (Pec) £3.50
SL-400 Box of 25 (Pec) £83
SL-404 1 yard length of O09 narrow gauge track 'mainline' wooden sleepers (Pec) £3.50
SL-404 Pack of 25 (Pec) £92



SL-E492 Left hand point electrofrog (Pec) £9.50



SL-E491 Right hand point electrofrog (Pec) £9.50

N Gauge Steam locos



2S-009-007 Class 2884 2-8-0 3822 in BR black late crest (Dap) £124
2S-010-007 Class 49xx 'Hall' 4-6-0 4970 'Skeety Hall' in GWR green Great Western lettering - Digital fitted (Dap) £149.88
2S-007-013D Class 57xx Pannier 0-6-0 9744 British Railways green - DCC fitted (Dap) £100



2S-007-016 Class 57xx Pannier 0-6-0 8767 in BR green British Railways lettering & later cab (Dap) £74
2S-007-011 Class 57xx Pannier 0-6-0 6746 GWR green shuntbox emblem (Dap) £74



2S-008-011 Class A4 4-6-2 2512 'Silver Fox' in LNER grey valances (Dap) £124.38



2S-008-009 Class A4 4-6-2 4490 'Empire of India' in LNER garter blue valances (Dap) £124
2S-008-009 Class A4 steam loco 60029 'Woodcock' in BR green late crest - DCC fitted (Dap) £149.88
2S-020-003 Class B17 4-6-0 1671 'Manchester City' LNER apple green (Dap) £109



2S-002-003 Class V Schools 4-4-0 929 'Malvern' in Southern Railway malachite green (Dap) £123

Diesel locos

2D-012-008D Class 22 diesel loco D6311 in BR green small yellow panels & disc headcodes - DCC fitted (Dap) £151.39
2D-002-003 Class 50 50037 'Bluebird' original Network SouthEast (Dap) £123.21



2D-002-002 Class 50 50040 'Leviathan' in BR large logo blue (Dap) £123.21
2D-004-001D Class 56 56019 in Railfreight red stripes - DCC fitted (Dap) £140.11
2D-007-011D Class 66/0 66002 in EWS DB branding and six megafire container wagons - Digital fitted (Dap) **NEW** £220.15
2D-010-008D Class 67 67006 'Royal Sovereign' in Royal Train claret DB logos - Digital fitted (Dap) **NEW** £134.26



2D-010-010 Class 67 67021 in Belmond British Pullman umber and cream (Dap) £108.76
2D-010-010D Class 67 67021 in Belmond British Pullman umber and cream - Digital fitted (Dap) **NEW** £134.26



2D-010-009 Class 67 67027 'Charlotte' in Colas Rail Freight (Dap) £108.76
2D-010-009D Class 67 67027 'Charlotte' in Colas Rail Freight - Digital fitted (Dap) **NEW** £134.26
2D-010-011D Class 67 67029 'Royal Diamond' in DB silver DB logos - Digital fitted (Dap) **NEW** £134.26
2D-022-000D Class 68 68019 'Brutus' Trans'Europe Express - Digital fitted (Dap) £145.35

HSTs



2D-019-006 Class 43 HST 4-car book set in East Coast silver (Dap) £160.89

DMUs

2D-009-003 Class 121 'Bubble Car' DMU W55033 BR green speed whiskers (Dap) £114
ND116E Class 142 'Pacer' 2-car DMU 142021 Tyne and Wear PTE (Dap) £115.18



2D-021-003 Class 156 156403 in Regional Railways express Central Trains branding (Dap) £114.71

Coaches

2P-012-800 Pack of three Maunsell coaches - Set 398 - brake third, brake third and compartment third in BR crimson and cream (Dap) £71

Wagons

NR-P426 7 plank open wagon 'London Brick Company and Forders' LFP (Pec) **NEW** £9.50
2F-022-006 Cargowagon bogie ferry wagon 33 80 279 7543-6 in grey and blue yellow stripes (Dap) £19



2F-026-006 HIA aggregate limestone hopper 369020 in Freightliner green (Dap) £21.50

Model railway spares

SL-311 Insulated Rail Joiners/Fishplates for N & O09 gauge (12 per pack) (Pec) £1.70
SL-310 Rail Joiners/Fishplates for N & O09 gauge (24 per pack) (Pec) £2

Track - Code 80 Setrack

ST-12 No.1 radius double curve (Pec) £1.70

Track - Code 80 Streamline

SL-302 1 yard (91.5cm) length of Concrete Sleeper Nickel Silver Flexible track (Pec) £3
SL-302 Pack of 25 (Pec) £64



SL-300 Pack of 25 1 yard (91.5cm) length of Wooden-Sleeper Nickel Silver Flexible track (Pec) £64
SL-300 Single (Pec) £3



SL-E389 Left hand large point electrofrog (Pec) £10.50



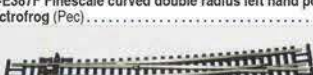
SL-E396 Left hand medium point electrofrog (Pec) £9.50
SL-395 Right hand medium point insulfrog (Pec) £9.50

Track - Code 55 Finescale

SL-302F 1 Yard of Nickel Silver Flexible Track Concrete Sleepers (Pec) £3.50
SL-302F Pack of 30 (Pec) £91



SL-E387F Finescale curved double radius left hand point electrofrog (Pec) £11



SL-E389F Finescale left hand large point electrofrog (Pec) £11
SL-U396F Finescale left hand medium point unitfrog (Pec) £10.50



SL-E392F Finescale left hand small point electrofrog (Pec) £9.50



SL-E391F Finescale right hand small point electrofrog (Pec) £9.50

Cars

NEW



NMOS006 Morris Minor Saloon Clipper Blue (Oxt) **NEW** £4

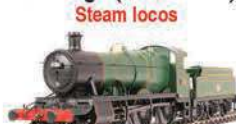
Commercial vehicles

NEW



NFT024 Ford Transit MK5 Royal Mail (Oxt) **NEW** £4.50

O Gauge (1:43 Scale) Steam locos



4311 Class 43xx Mogul 2-6-0 4358 in BR lined green early emblem (Hel) £662.15



7S-010-013 Class A1 'Terrier' 0-6-0T 751 in SECR green (Dap) £191.25



7S-010-013S Class A1 'Terrier' 0-6-0T 751 SECR green - DCC sound fitted (Dap) £340



7S-010-012 Class A1X 'Terrier' 0-6-0T 32650 in BR lined black early emblem (Dap) £191.25



H7-A3-003 Class A3 4-6-2 2745 'Captain Cuttle' in LNER black unstreamlined non-corridor tender (Hel) £750



H7-A3-001 Class A3 4-6-2 2750 'Papyrus' in LNER Grass green unstreamlined corridor tender - 'Record Breaker' (Hel) £750



H7-A3-002 Class A3 4-6-2 4472 'Flying Scotsman' in LNER Grass green unstreamlined corridor tender - 'Record Breaker' (Hel) £750



H7-A3-005 Class A3 4-6-2 60035 'Windsor Lad' in BR green early emblem and unstreamlined non-corridor tender (Hel) £750



H7-A3-004 Class A3 4-6-2 60072 'Sunstar' in BR Express blue unstreamlined non-corridor tender (Hel) £750



H7-A3-006 Class A3 4-6-2 60077 'The White Knight' in BR green late crest and unstreamlined non-corridor tender (Hel) £750



7S-005-001 Class Y3 Sentinel 4wVB 42 in LNER black (Dap) £148.75



7S-005-005 Class Y3 Sentinel 4wVB 68164 in BR black early emblem (Dap) £148.75

7S-005-007 Sentinel 4wVB 14 'Mistral' in National Coal Board (Dap) £148.75

Diesel locos

2051 Class 03 shunter D2011 in BR green no yellow ends and conical exhaust (Hel) **NEW** £335.75



2055 Class 03 shunter D2033 in BR green wasp stripes and 'flowerpot' exhaust (Hel) **NEW** £335.75



2052 Class 03 shunter in BR green no yellow ends and 'flowerpot' exhaust - unnumbered (Hel) **NEW** £335.75



2057 Class 03 shunter in BR green wasp stripes and 'flowerpot' exhaust - unnumbered (Hel) **NEW** £335.75



2590 Class 05 shunter in BR green no yellow ends (Hel) (RRP £395) **BARGAIN** £295



7D-008-017 Class 08 shunter 08709 in EWS red and gold (Dap) £191.25

7D-008-013 Class 08 shunter 08717 in BR blue (Dap) £191.25



7D-008-015 Class 08 shunter 08740 in Railfreight Triple grey (Dap) £191.25



7D-008-009 Class 08 shunter D3002 in BR green late crest and wasp stripes (Dap) £191.25

Visit

www.hattons.co.uk/new

for all new releases



SL-E792BH Left hand point bullhead rail and electrofrog (Pec) £43
SL-E786BH Right hand O gauge curved turnout (Pec) £54



SL-E791BH Right hand point bullhead rail and electrofrog (Pec) £43
ST-725 Setrack Bullhead 2nd radius standard curve 410mm (16") (Pec) £58
ST-700 Setrack Bullhead straight 394mm (15.5") (Pec) £58
ST-0751 Setrack second radius Unifrog left hand point (Pec) £43
ST-0750 Setrack second radius Unifrog right hand point (Pec) £43
SL-E797BH Y Point bullhead rail & electrofrog (Pec) £43

Track - Code 143 Flat Bottom Streamline

SL-E792BF Finescale Left Hand Point With Flatbottom Rail, Electrofrog (Pec) £45
SL-E791BF Finescale Right Hand Point With Flatbottom Rail, Electrofrog (Pec) £45

Buses



43WFL001 Weymann Fanfare Southdown Motor Services (Oxf) £37.50

Cars



VA10317 Ford Cortina Mk3 2000E - Black (Cor) NEW £27



VA11913 Ford Cortina Mk4 2.0 GL - Diamond White (Cor) NEW £27
CO2730 Volkswagen Campervan - Peace Love and Music (Cor) NEW £22.50
CO2731 Volkswagen Campervan - Peace Love and Music (Cor) NEW £22.50

Any or Multiple Scales

Analogue controllers

COMBI Combi 12V 1Amp Single Track Controller Transformer (Gau) £34
D Double/Twin track power controller (Gau) £26



Q Quadruple 4 circuit power controller (Gau) £149

Analogue point control



PLS-100 SmartSwitch Set for 4 servo control & operation, switches, control board & digital readout for Analogue operation (Pec) £61
PL-15 Twin Microswitch (Pec) £7.50

Digital decoders

Imperium1 'Imperium' 21 Pin MTC 6-function DCC decoder (Dap) £19.51
DCR-21Pin-Direct V2 21-pin 6-function 1.1Amp direct plug decoder back EMF (Hat) £16.50
DCR-8PIN-Direct 8-pin 2-function 1.1Amp direct plug decoder back EMF (Hat) £17
BPDC22 8 & 21-pin 4-function 1A (1.8A peak) small OMNI decoder (measures 29 x 27 x 10mm) x 2 (Gau) £79
DCR-8PIN-HarnessMini 8-pin (harness) 2-function 1.1Amp small & thin (mini) decoder back EMF (Hat) £16
DCR-8PIN-HarnessMini Box of 5 (Hat) £72
DCR-8PIN-HarnessMini Box of 10 (Hat) £128



DCR-8PIN-Harness Box of 10 8-pin (harness) 4-function 1.1Amp decoder back EMF (Hat) £120
DCR-8PIN-Harness Single (Hat) £15
DCR-8PIN-Harness Box of 5 (Hat) £58
R8249 8-pin 4-function 2-sided 0.5A (1A peak) decoder (Hor) £16
R8249 Box of 6 (Hor) £92
DCR-8PIN-Direct 8-pin compact direct 4-function 1.1Amp decoder back EMF (Hat) £17
DCR-8PIN-Direct Box of 5 (Hat) £77
DCR-8PIN-Direct Box of 10 (Hat) £136
BPDC29 OMNI 8 Pin Direct Plug Decoder (5 Pack) (Gau) £84
R8117 TTS DCC Sound Decoder 8 pin plug - Princess Coronation 4-6-2 steam loco (Hor) £36



R8102 TTS DCC Sound Decoder 8 pin plug - Class 37 diesel (Hor) £36
R8121 TTS DCC Sound Decoder 8 pin plug - Class 66 diesel (Hor) £38
R8106 TTS DCC Sound Decoder 8 pin plug - Gresley Class A1 and A3 4-6-2 steam locos (Hor) £16
R8107 TTS DCC Sound Decoder 8 pin plug - Gresley Class A4 4-6-2 steam loco (Hor) £36
R8120 TTS DCC Sound Decoders - Pack of 2 8 pin plugs - Class 43 HST (MTU) (Hor) £59.50
R8123 TTS digital sound decoder - Class 50 diesel (Hor) £38



R7239 TTS digital sound decoder - Class J36 steam loco (Hor) £38
DCD-ZH218-6 2m Black - 8 and 21 pin 6 function digital decoder (DCC) £23

Digital control accessory decoders



DCD-AD8FX COBALT 8 output Accessory Decoder for Cobalt or other motor-drive point motors (DCC) £43
36-561 EZ Command Dynamics 4-output point decoder (similar to Hornby R8247) (Bac) (RRP £45.35) BARGAIN £24.50

Digital controllers



DCC02 Prodigy 'Advance 2' starter DCC controller package (Gau) £279
DCC49 Prodigy 8 Amp booster unit (Gau) £169
DCC014 Prodigy Advance 2 Backlit Walkaround controller (Gau) £124
DCC01 Prodigy Express 1.6 Amp DCC controller - complete starter pack (Gau) £159.50
DCC06 Prodigy Express 1.6 Amp Digital controller WiFi capability - complete starter pack (Gau) £224

Digital control accessories

DCD-PWRAUX Alpha power upgrade bundle - 5 amp booster and Alpha control box (DCC) £129



DCD-PWRAUXAEC Alpha power upgrade bundle - 5 amp booster and Alpha control box Alpha Central accessory controller (DCC) £214
DCC-AEC Cobalt Alpha Central - 12-way switch control panel (DCC) £111
BPDC80 Pack of three DCC outofpolarity reversal switches - for electrofrog points (Gau) £14.50



524-037 SB5 5 amp Power Booster DCC system for use Power Cab controller (NCE) £177

Point motors

PL-9 5 Mounting Plates for use PL-10E (Pec) £2.80
DCP-CB12P Cobalt 12 slow-reaction analogue point motor - pack of 12 (DCC) £204
DCP-CB51P Cobalt 5 slow-reaction analogue point motor - pack of 5 (DCC) £102
BPPM29 Pack of five Surface mounted seep point motors (Gau) £36
PL-10 Point Motor (Switch Machine) (Pec) £6
PL-10 Bag of 6 (Pec) £34
PL-10E Point Motor Extended Pin (use PL-9) (Pec) £6
PL-10E Bag of 6 (Pec) £34
PM-2 Seep point motor (Gau) £2.50
PM-2 Pack of 6 (Gau) £20
PM-1 Seep point motor accessory switch (Gau) £5



PL-11 Side/surface mounted point motor (Pec) £8
TT300 Slow motion DCC point motor (Tra) £20



HAT-PM-01 Solenoid point motor including pin extension and 2-way arm for use points and signals. Exclusive to Hatton's (Hat) £6.50
HAT-PM-01 Pack of 6 (Hat) £35

Switches and levers

PL-13 Accessory Switch for fitting to turnout motor PL10 (Pec) £3



PL-26B Passing Contact Switch - Black Lever (Pec) £6
PL-26R Passing Contact Switch - Red Lever (Pec) £6
PL-26Y Passing Contact Switch - Yellow Lever (Pec) £6

Electronic components

WM5 Wall mounted transformer - 12V DC 2amps - suitable for Peco turntable motor (PL-55) (Gau) NEW £16

Lighting equipment

GM538 3mm LED bezels for control panels - pack of 12 (Gau) NEW £3.80
GM539 5mm LED bezels for control panels - pack of 12 (Gau) NEW £3.80

Man-made scenic structure - railway-related

SE6 Smart Screen enclosure - mounted double sided post (Tra) NEW £9
SE5 Smart Screen enclosure - mounted round post (Tra) NEW £9
SE4 Smart Screen enclosure - mounted right hand round post (Tra) NEW £9
Grass, ground, lichen, hedges and foliage
JMAT10S Landscape Mat - No.10 - 48" x 12" (Jav) NEW £2.30

Track underlay

JCS18L Cork underlay sheet - 1/8" thick - 24" x 36" (Jav) £9

Glues

AD-57 Rocket Card Glue - Clean, Neat & Instant Card Modelling Glue (Ideal For Superquick & Metcalfe) - 50ml (Daf) £5.50

Tools

PL-71 Loco servicing kit - servicing cradle, infills, Powertube, brush & scraper (Pec) £19



2006 Class 20 in BR blue full yellow ends, 1980s style warning flashes and headcode discs - Exclusive to Hatton's (Hel) (RRP £599) BARGAIN £395
2015 Class 20 Railfreight Red Stripe headcode boxes (Hel) (RRP £599) BARGAIN £427



2558 Class 25/3 in BR blue - unnumbered (Hel) £446.25



2556 Class 25/3 in BR green full yellow ends - unnumbered (Hel) £446.25
2555 Class 25/3 in BR green small yellow panels - unnumbered (Hel) £446.25



3400 Class 330 33012 in BR blue - weathered (Hel) NEW £466.65



3402 Class 330 33030 in EWS red and gold (Hel) NEW £466.65



3403 Class 330 33035 'Spitfire' in Network SouthEast blue (Hel) NEW £466.65
3395 Class 330 in BR green full yellow ends - unnumbered (Hel) NEW £466.65



3393 Class 330 in BR green no yellow ends - unnumbered (Hel) NEW £466.65
3394 Class 330 in BR green small yellow panels - unnumbered (Hel) NEW £466.65



3401 Class 330 in Civil Engineers 'Dutch' grey and yellow - unnumbered (Hel) NEW £466.65
3404 Class 330 in Direct Rail Services blue - unnumbered (Hel) NEW £466.65



3740 Class 37/4 in BR green - unnumbered (Hel) £509.15



4205 Class 42 Warship 829 'Magpie' in BR blue (Hel) (RRP £699) BARGAIN £495



4024 Class 50 50007 'Sir Edward Elgar' in GWR green (Hel) £594.15



4027 Class 50 50015 'Valiant' in BR civil engineers 'Dutch' grey and yellow (1990s Railtour condition) - Exclusive to Hatton's (Hel) £594.15



4020 Class 50 in BR blue - unnumbered (Hel) £594.15



4022 Class 50 in original Network Southeast - unnumbered (Hel) £594.15



4023 Class 50 in revised Network Southeast - unnumbered (Hel) £594.15



5285 Class 52 D1035 'Western Yeoman' in BR green small yellow panels (Hel) £509.15



5287 Class 52 D1039 'Western King' in BR maroon full yellow ends (Hel) £509.15



5290 Class 52 D1043 'Western Duke' in BR chromatic blue small yellow panels (Hel) (RRP £599) BARGAIN £470



5506 Class 55 'Deltic' D9001 'St Paddy' in BR green full yellow ends - weathered (Hel) £509.15



7P-004-010 GWR Diagram 'N' 59' Autocoach 39 in GWR chocolate and cream twin cities crest (Dap) £170



7P-004-008 GWR Diagram 'N' 59' Autocoach W37W in BR crimson and cream (Dap) £170
4913 Mk1 TSO second open BR chocolate and cream - unnumbered (Hel) £169.50



7F-048-001 HAA MGR coal hopper 350274 in Railfreight red cradle (Dap) £46.71



7F-048-002 HAA MGR coal hopper 355203 in Railfreight red cradle (Dap) (RRP £56.95) BARGAIN £46.71
H7-WW-701 Warwell wagon 50t diamond frame bogies MS.1 War Department (GWR) (Hat) £85



9500 Modern Diesel Depot Kit - unpainted (Hel) £75.65



SL-700BH Box of 12 1 Yard O Gauge Flexible Nickel Silver Track (Bullhead Nickel Silver Rail) (Pec) £75
SL-700BH Single (Pec) £5.50
SL-E790BH Double slip (8 degree angle), Electrofrog (Pec) £5.50
SL-E787BH Left hand O gauge curved turnout (Pec) £54

Buildings - railways

Signals

7L-001-001 Signal - motorised, GWR home - square post (Dap) £47.56
7L-002-001 Signal - motorised, LMS home - wooden arm (Dap) NEW £47.56

Track - Code 124 Bullhead Streamline

METCALFE

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GRANGE HOUSE

00 CODE PO257 - £12.50
N CODE PN157 - £8.00



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N CODE PN158 - £8.00



CROFTERS COTTAGE

00 CODE PO259 - £9.50
N CODE PN159 - £7.50



MANOR FARM BUILDINGS

N CODE PN951 - £9.00
00 CODE PO252 - £11.50



MANOR FARM HOUSE

00 CODE PO250 - £10.50
N CODE PN150 - £9.00



MANOR FARM BARN

00 CODE PO251 - £10.50
N CODE PN150 - £9.00



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00 CODE PO255 - £12.50
N CODE PN155 - £10.00



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00 RED BRICK CODE PO263 - £11.00
00 STONE CODE PO264 - £11.00
N RED BRICK PN116 - £8.50
N STONE PN117 - £8.50



VILLAGE SCHOOL

00 CODE PO253 - £10.50
N CODE PN153 - £11.00
(N GAUGE COMES WITH PLAYGROUND)

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00 RED BRICK CODE PO261 - £10.50
00 STONE CODE PO262 - £10.50
N RED BRICK PN103 - £8.50
N STONE PN104 - £8.50



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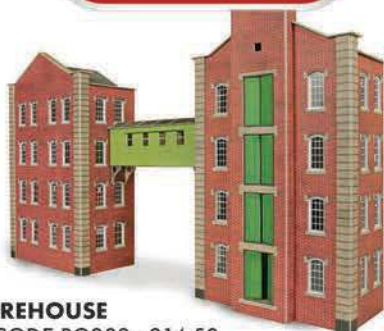
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N CODE PN183 - £11.00



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00 CODE PO285 - £13.00
N CODE PN185 - £7.50



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00 CODE PO284 - £14.50
N CODE PN184 - £13.00



FIRE STATION

00 CODE PO289 - £19.00
N CODE PN189 - £12.00

RAILWAY



WAYSIDE STATION

00 CODE PO238 - £18.00
N CODE PN138 - £10.00



WAYSIDE SHELTER

00 CODE PO239 - £14.00
N CODE PN139 - £10.00



GOODS YARD CRANE

00 CODE PO540 - £6.50
N CODE PN840 - £5.00
00 pictured below. N version comes without hook.

00 GWR SIGNAL BOX

00 CODE PO330 - £15.00



00 GWR BENCHES

00 CODE PO501 - £5.75

MINI KITS



PICNIC TABLES

00 CODE PO510 - £5.50
N CODE PN810 - £5.50



MARKET STALLS

00 CODE PO530 - £6.50
N CODE PN830 - £5.50



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00 CODE PO522 - £5.75
N CODE PN823 - £5.75

PLATFORM KIOSK

00 CODE PO517 - £5.75
N CODE PN817 - £5.50



00 PARK BENCHES

00 CODE PO503 - £5.50



00 CHILDRENS PLAY AREA

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WORLD OF RAILWAYS

BACHMANN ANNOUNCES NEW 2020 RANGE

Bachmann has revealed 20 new products across its Branchline and Scenecraft OO scale ranges that are expected to be available during the next three months.

These new arrivals follow the OO gauge J72 steam locomotives, Ransomes & Rapier 45T Steam Cranes, Class 158 Diesel Multiple Units (DMUs), and the first Class 24/1 diesel locomotives that were released in the last quarter of 2019.

In addition to the 20 products revealed this week, more than 30 previously announced items from Bachmann Branchline, Graham Farish and Scenecraft have also been confirmed for release during Spring 2020.

RANGE	SCALE	ITEM No.	DESCRIPTION	RRP
Branchline	OO	31-464A	SE&CR C Class 1573 SR Lined Black	£199.95
Branchline	OO	32-393A	Class 37/7 Refurbished 37800 <i>Cassiopeia</i> Europhoenix (ROG)	£179.95
Branchline	OO	32-393ASF	Class 37/7 Refurbished 37800 <i>Cassiopeia</i> Europhoenix (ROG) [S]	£269.95
Branchline	OO	39-600A	SE&CR 60ft Birdcage Brake Composite Lavatory SE&CR Dark Lake	£ 84.95
Branchline	OO	39-610A	SE&CR 60ft Birdcage Composite Lavatory SE&CR Dark Lake	£ 84.95
Branchline	OO	39-620A	SE&CR 60ft Birdcage Brake Third SE&CR Dark Lake	£ 84.95
Branchline	OO	33-131	BR 35T Bulk Grain Hopper BR Freight Brown (BRT) - Weathered	£ 31.95
Branchline	OO	33-132	BR 41T Bulk Grain Hopper BRT 'Grainflow' Green - Weathered	£ 34.95
Branchline	OO	33-133	BR 35T Bulk Alumina Hopper BRT Blue - Weathered	£ 31.95
Branchline	OO	33-300H	GWR 20T 'Toad' Brake Van GWR Grey	£ 20.95
Branchline	OO	33-308A	GWR 20T 'Toad' Brake Van BR Grey (Early) - Weathered	£ 22.95
Scenecraft	OO	44-088C	Bluebell Booking Office Crimson and Cream	£ 99.95
Scenecraft	OO	44-089C	Bluebell Station Canopy Crimson and Cream	£ 57.95
Scenecraft	OO	44-090C	Bluebell Waiting Room and Toilet Crimson and Cream	£ 89.95
Scenecraft	OO	44-091C	Bluebell Office and Store Room Crimson and Cream	£ 59.95
Scenecraft	OO	44-088G	Bluebell Booking Office Green and Cream	£ 99.95
Scenecraft	OO	44-089G	Bluebell Station Canopy Green and Cream	£ 57.95
Scenecraft	OO	44-090G	Bluebell Waiting Room and Toilet Green and Cream	£ 89.95
Scenecraft	OO	44-091G	Bluebell Office and Store Room Green and Cream	£ 59.95
Scenecraft	OO	44-558	Corrugated Metal Shed	£ 10.95



HELJAN APPOINTS GAUGEMASTER AS UK AFTER-SALES AGENT

Heljan has informed its customers that its UK spares and returns arrangements are to change from March 1, 2020. From that date, all enquiries regarding Heljan spare parts and all UK returns of faulty models will be handled by Gaugemaster.

Howes Models of Oxford handled Heljan's spares and returns requests for 20 years. The move sees it replaced by the Sussex retailer. Heljan thanked Howes for its efforts over the past two decades.

The spares and returns service is currently suspended to allow the handover to take place. Heljan has asked customers not to contact Gaugemaster until March 1 when it will begin assisting with enquiries. As of that date, Heljan is promising service seven days a week, by telephone during shop hours, and 24/7 via the Gaugemaster website.

CAVALEX TO ABANDON OO GAUGE CLASS 91, BUT CONTINUE IN N

After the announcement that Hornby is to produce a Class 91 locomotive in OO gauge, Cavalex made a statement on its website:

"Following the news on January 6th that Hornby is now to produce its own version of the Class 91, we have taken time in the last two weeks to evaluate our situation. Our intention with this project was to bring a highly-detailed locomotive to the market that has been overlooked for a great number of years. Given the small market for electric locomotives and especially the Class 91, with its limited area of operation and services, we have decided at this time that, despite the overwhelming support received by the community, it would not be viable in the short term to progress with our Class 91.

This has not been an easy decision and we have assessed all avenues available at this point in order to try to satisfy the excitement and expectations of the modelling community shown over the last few months.

It is with some sadness that we have to park the 4mm version of this iconic locomotive for now, however we do still intend to progress with the 2mm version as previously planned.

We wish Hornby every success with their model and again wish to thank everyone that has supported us in this project and hope that we can provide equally-anticipated models for all of you moving forward."

The project had reached the stage where a detailed 3D-printed 4mm scale model was shown, as seen at the Chiltern Model Railway Exhibition.

READ OUR FULL
COVERAGE OF
THE 2020
LONDON TOY FAIR
ON PAGE 90



ACCURASCALE CLASS 55 DELTIC PROJECT UPDATE

It's been three months since Accurascale's first pre-production prototype of the Deltic broke cover and arrived for assessment from China. While Accurascale was very happy with the shape and overall look of the model, it was felt that there were several areas to improve. These included:

- Shoulder grills
- Bogie adjustments
- Roof fans
- Roof fan grills
- Handrail adjustments on the front
- Glazing
- Buffer cowling and shank

After a period of testing, it was decided that some improvements in running performance were required, too. Accurascale originally thought that these changes would be done in a short period of time, though after a series of discussions with the factory, it has become apparent that they will take longer than was envisaged. The changes are underway, though Chinese New Year will delay work for almost six weeks. A revised delivery date of late May 2020 is now stated for decorated samples, which will include mechanism improvements and the cosmetic improvements that have been requested on the above list. Accurascale's high demands meant that some retooling was required and this is taking place in China.

Once Accurascale and its commission partners are happy with decoration samples, production will commence. This will give a delivery date of models to customers of late-November 2020. Accurascale has apologised to customers for the delay and has said it "wants these models to do the mighty Deltic justice, and they have to be right. The finished models on layouts and collections will be worth the wait."



ISINGLASS MODELS LAUNCHES BRAKE VAN

The firm has announced a new Gresley 32ft 3D-printed kit release.

The new kit includes sides, ends, roof, torpedo vents, frame, drop steps, 'W'-irons, axleboxes and springs. Aimed at being easy to assemble, the 3D-printed resin works with superglue and accepts cellulose paints without special cleaning required.

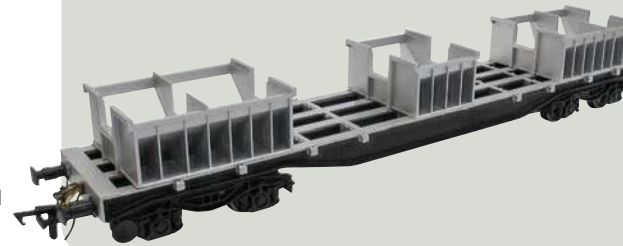
Also released is a diagram 109, 61ft 6in third class sleeper with the Great Northern 'quad-art'. More kits are due for release at our London Festival of Railway Modelling show at Alexandra Palace in March.

CAVALEX BBA AND BLA WAGONS READY FOR PRODUCTION

The final samples of Cavalex BBA steel-carrying wagons were delivered just in time for the Chiltern show last month.

Built between 1973 and 1981, the BBA was a slightly longer version of the earlier BAA steel wagon, but with a reduced 75T capacity. The heavily-constructed long steel carrier was designed to carry ingots, slabs, long lengths of steel, steel coil, and billets.

Between 1994 and 1995, more than 250 BBA wagons were modified with the removal of the ends and floor and the fitting of coil boxes and recoded BLA. This was for steel strip coil traffic in South Wales for Dee Marsh, Llanwern, Trostre, and Ebbw Vale.



VISITORS FLOCK TO 2020 LONDON MODEL ENGINEERING EXHIBITION

Thousands of visitors headed to Alexandra Palace in January to attend the 24th annual London Model Engineering Exhibition.

The exhibition showcased the full spectrum of modelling and was packed with over 2,000 models built by individuals, clubs and societies. On show was traditional model engineering such as locomotives, steam and internal combustion engines and steam road vehicles, as well as scale model ships, aircraft, and tanks to remote-controlled trucks and James Bond and Star Wars models.

More than 45 clubs and societies attended, each vying to win the prestigious 'Society Shield' as voted by the clubs and societies. First place was awarded to Harlington Locomotive Society, for its superb recreation in miniature of a railway locomotive works. Second place was awarded to the West London Meccano Society, and in third place was Eastleigh Young Engineers.

Exhibition Director, Chris Deith said; "The exhibition is regarded as the leading show of its kind in terms of size, content and attendance and it's thanks to the continued support of the exhibiting clubs, societies and specialist suppliers that it is possible to present such a varied event. A huge thank you also to all the visitors who travel from far and wide to attend the event, many of whom return year after year."

Visitors were able to take part in hands-on model building activities, see technical demonstrations, watch the Tamiya radio-controlled trucks tackle an obstacle course or take a passenger ride behind a steam locomotive courtesy of the Polly Owners Group. Dates for the 2021 show will be announced shortly.



BRM TV

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EXCLUSIVE

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NEW PRODUCTS

OMEN MINIATURES SCENIC AND FIGURE ACRYLIC PAINTS



Omen Miniatures has developed two new ranges of acrylic paint. The figure painting range is designed around its range of miniature figures and has 25 key colours suitable for clothing, from Faded Vest Orange to Jacket Blue, Flesh, Light Blue, Signal Red, Dark Brown or Leather. Its scenic range now has useful shades from Wooden Grime to Underframe Dirt, Worn Tarmac, Oily Black, Tree Green or Mortar, for instance. All paints promise a durable smooth texture and matt finish. The paints are water-based and non-toxic. Paints are available at exhibitions or online from Omen Miniatures or Skytrex.

Price: £3.00 (per pot)

W: www.omen-miniatures.com or www.skytrexmodelrailways.co.uk

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SCALE MODEL SCENERY (LX205-00) LADDERS

Add detail to industrial buildings, farm silos or chimney stacks with this pack of two laser-cut wooden industrial ladders with safety cage from Scale Model Scenery. The intricate parts require delicate handling, and assembly is recommended with tweezers and a PVA adhesive or superglue for rapid construction. Cut from 0.8mm laser-board, with step-by-step instructions to build two ladders up to 210mm x 12mm x 22mm, they're suitable for use with buildings up to 195mm tall. With some modifications, they can be joined together to make longer ladders, too.

Price: £5.49

W: www.scalemodelscenery.co.uk



LIFECOLOR (LC-CS47) SHELL CASE ACRYLIC PAINT SET

Though aimed at military modellers for painting shell cases, this pack of six matt metallic colours from Lifecolor can be used to paint metallic surfaces. The six shades are Polished Steel Modern Shell, Laquered Steel Late WWII German Shell, New Shell Brass Shade 1, New Shell Brass Shade 2, New Shell Brass Shade 3 and Spent Shell Burned Brass. Putting their intended uses to one side, these paints are ideal for the painting of many brass fittings on steam locomotives from cylinder cocks to backheads, steel valve gear, and hydraulic buffers.

Price: £17.99

W: www.airbrushes.com

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SKYTREX 20FT CONTAINER

Cast in resin this 20ft container can find many uses on your layout. From the corner of a yard to store tools, to ports, loaded on the back of an HGV, or loaded on a container wagon, many thousands can be found across the country and overseas for the transportation of goods. Many have found secondary use and can be found in different states of repair, with faded shipping logos or patches of rust – ideal effects for the modeller to recreate.

Price: £5.00

W: www.skytrexmodelrailways.co.uk

BACHMANN EUROPE (36-2020) MODEL RAILWAY 2020 CATALOGUE



For 2020, Bachmann has combined its British-outline ranges in a single annual brochure as a reference point for its customers and collectors. More than 200 colour-coded pages identify the Graham Farish, Narrow Gauge, Branchline, Scenecraft and accessories ranges. The combined volume illustrates ongoing items that are available to purchase from your local retailer, or products that have been included in previous catalogues for delivery at a future date. Not included are unseen or newly-tooled products. Enquire at your local Bachmann stockist or visit its website.

Price: £7.95

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MODELU 3D-PRINTED FIGURES

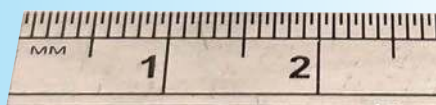


Modelu has unveiled scans of people to be 3D-printed in new ranges for 2020. The new figures commence with eight 'modern image' driver poses designed for Colas, DRS, Freightliner or GBRf Class 66s. The manufacturer has designed these for Hattons new Class 66 locomotive, but they can be adapted to suit motive power from many other manufacturers. Now in stock are 50 new 'Pendon' figures, including seated passengers. Visit its website to order 3D prints in your scale of choice.

Price: From £4.50

W: www.modelu3d.co.uk

VISIT WWW.WORLD-OF-RAILWAYS.CO.UK FOR THE LATEST PRODUCT NEWS



RK EDUCATION (RKPSU MICRO)

This micro-sized variable output PSU measures the size of a stamp and runs from 16VAC or 12VDC. The single output voltage can be adjusted using the small variable resistor. The unit has been designed for lighting and can power up to 150mA (approx. 10 LEDs). Small enough to place inside buildings.

Price: £22.99

W: www.rkeducation.co.uk

GENERAL STEAM NAVIGATION LOCOMOTIVE RESTORATION SOCIETY



A batch of OO gauge box vans from Dapol will help the restoration of locomotive 35011 *General Steam Navigation*, and of PS *Medway Queen*. The wagon design fits the Medway Queen Preservation Society theme of fictional wagons that might have been seen at Gillingham Pier if there had been a rail connection, but is also relevant to the General Steam Navigation Locomotive Restoration Society. The two vans are available in red or blue from either society.

Price: £12.90 (each)

W: www.35011gsn.co.uk and www.medwayqueen.co.uk

OXFORD RAIL STRUCTURES (OS76R002) GWR SIGNAL BOX

OO

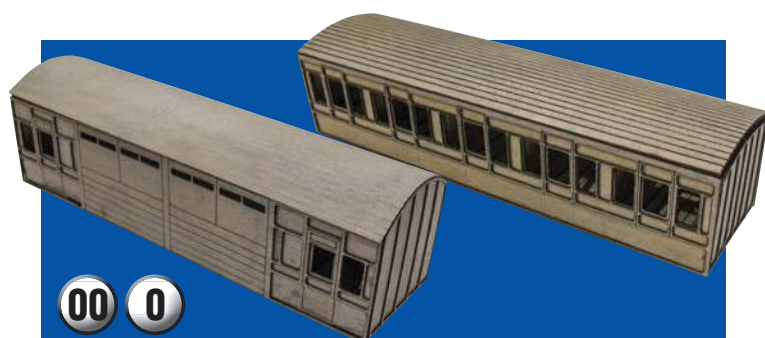


Arriving with Oxford Rail stockists, this new range of resin-cast, detailed, painted and pre-glazed buildings can be used to rapidly populate your layout or diorama with structures.

Of GWR outline, this 130mm x 95mm x 54mm signal box makes a great addition to a traditional branch-line terminus. The interior could be detailed by adding a floor, levers, bells and indicators. Plenty of useful reference material can be sought online or from the many publications on signal boxes.

Price: £26.99

W: www.oxfordrail.com



OO

POPPY'S WOODTECH PRE-GROUPING COACH KITS

Pre-grouping modellers have access to a new range of laser-cut wooden coach body kits to fit to a chassis of their choice. Laser-cut specialist, Poppy's Woodtech has unveiled a new range of 4mm:1ft scale body kits pre-cut from plywood. To aid assembly, a jig to provide the correct curvature of the roof is included. To date, an NER six wheel Third and Third Brake are available, with a Highland six-wheel Third and Locker Composite also available. Etched Clemonson underframe kits and 3D-printed detail sets are available to finish the models. The kits are also being produced in 7mm:1ft scale.

Price: Body kits: £27.50, NE & HR six-wheel coach £29.50

W: www.poppyswoodtech.co.uk



OO

REVOLUTION TRAINS HOA BOGIE HOPPER WAGONS

Arriving with its customers and available via its website, Revolution Trains' new bogie hopper has been released in EWS Construction, Cemex, DB Schenker, Ermewa and VTG liveries. Nearly 200 of these wagons are in service with three operators in five different liveries and are used for stone trains from quarries in central England, the Mendips and have also seen service on Anglo-Scottish sand trains for glass-making. Head to www.world-of-railways.co.uk to see our unboxing video.

Price: £TBA

W: www.revolutiontrains.com

HORNBY (R3679) W4 'PECKETT' NO. 74 PORT OF LONDON AUTHORITY

OO



Hornby's model has arrived in the guise of locomotive No. 74 in Port of London Authority blue. Built for William Baird & Co. for use at its Gartsherrie Ironworks in Coatbridge, Peckett Works No. 489 of 1890 was passed to R.S Hayes of Bridgend for disposal, but sold to the Port of London Authority (PLA) in 1943. It would continue in PLA service as No. 74 before being withdrawn in December 1957.

Price: (RRP) £99.99

W: www.hornby.com or see your local stockist

BRM TV

BRITISH RAILWAY MODELLING

DIGITAL EDITION EXCLUSIVE

HORNBY'S 0-4-0ST

Hornby's industrial 0-4-0ST arrives in a new livery depicting Port of London Authority No. 74. Which of its new liveries would you choose and why?

EXCLUSIVE

WATCH THE VIDEO HERE
(You must have a wifi connection to stream video content)



GET THE LOOK

1: To run along an unfenced light railway track, the Class 04 diesel is fitted with side-skirts and a cowcatcher. This model is based on an Airfix (now Dapol) plastic kit running on a Model Power Chassis. Nowadays, it would be easier to use a Bachmann model fitted with parts from Branchlines to hide the wheels.

2: The bridge crossing the waterway had to be built in-situ, because it's part of the baseboards. Sides are constructed from scratch using Plastikard along with the railing supports. These are drilled to accept the wire that the railings are made from.

3: Every village needs a classic K6 telephone box. This one is a Harburn Hobbies resin ready-to-use model, but Langley offers a more detailed etched kit or Merit supplies a plastic version.

4: Years ago, modellers had to build road vehicles from whitemetal kits, but now we can use one of the many die-cast models available. Once painted with matt varnish, combined with windows made from Deluxe Materials Glue'n'Glaze, they're an easier way to fill roads.

5: Ratio plastic telegraph poles are a useful addition, but rural areas wouldn't be home to many circuits in the late-1950s because few homes enjoyed a telephone. Remove most of the arms before planting them on the layout. If you're modelling a prototype, check with contemporary photographs for a guide.

6: The backscene was hand-painted by Neil Ripley. Trees are scratch-built using multi-strand wire that is twisted into shape and covered in Artex, with added foliage. Working in agriculture has given him plenty of time to observe the prototype's shape.

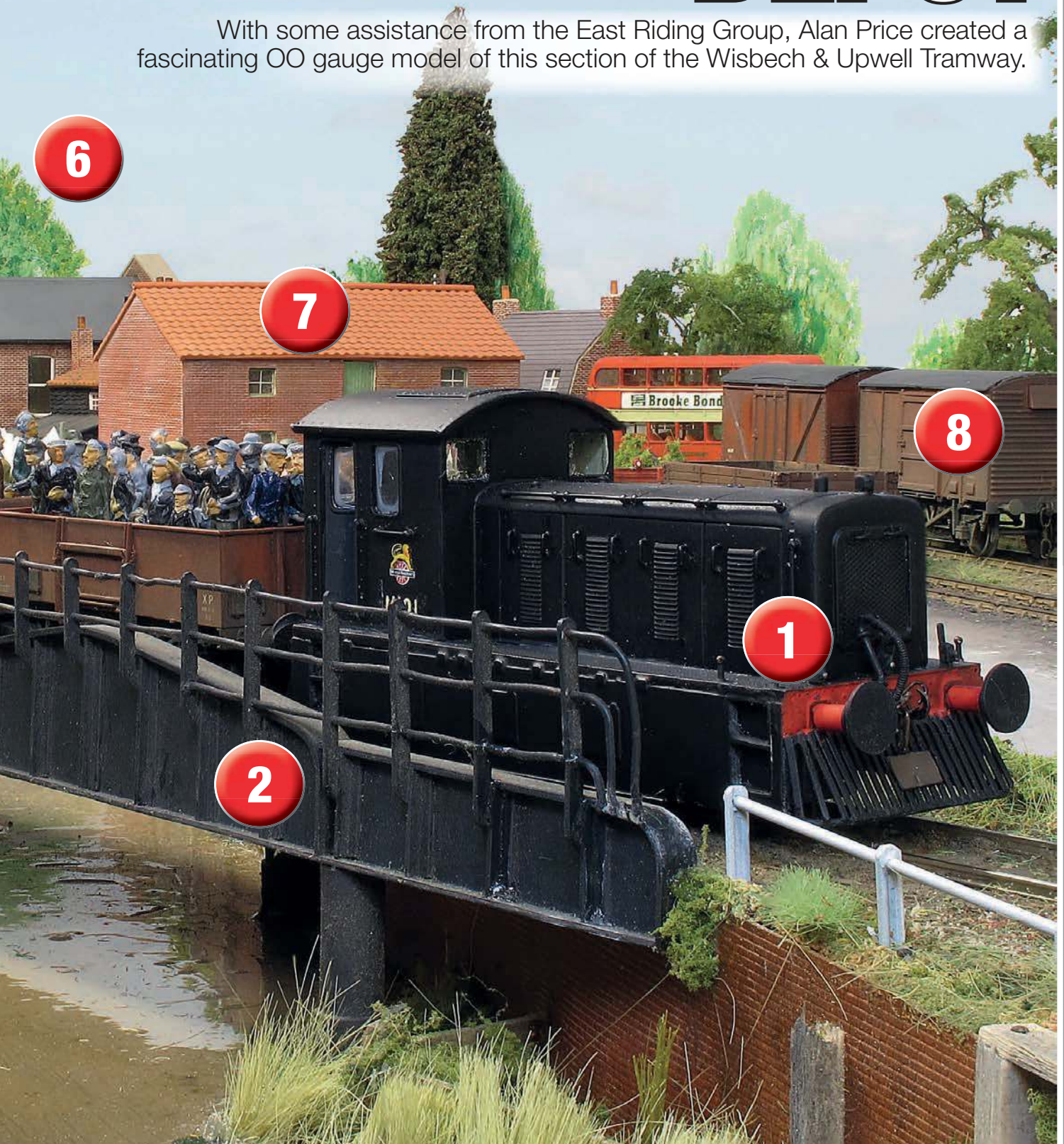
7: The buildings are based on prototypes. Construction starts with a card mock up placed in position to check the size. A Plastikard shell has embossed brickwork laminated to it. Some models are simply washed with paint to represent rendering or whitewashing, while others, where brickwork is more defined, have the mortar colour applied first and then individual bricks picked out in various shades. Roofs are either from Wills sheets or York Modelmaking laser-cut self-adhesive roof tiles.

8: Much of the rolling stock, such as these vans, are from the Parkside range of plastic kits. At harvest time, the line would often see trains made up of 60 vehicles.



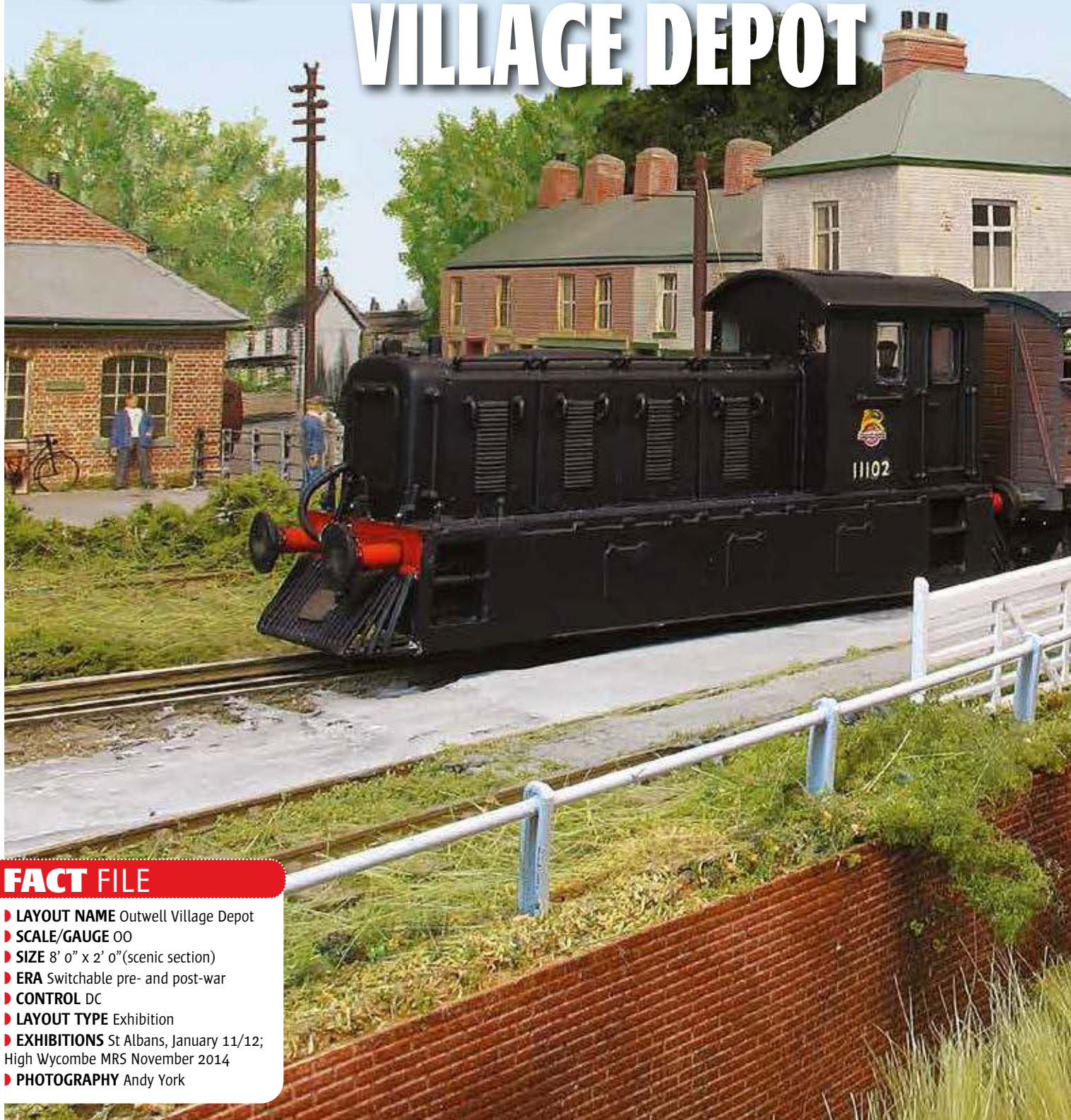
OUTWELL VILLAGE DEPOT

With some assistance from the East Riding Group, Alan Price created a fascinating OO gauge model of this section of the Wisbech & Upwell Tramway.



OUTWELL

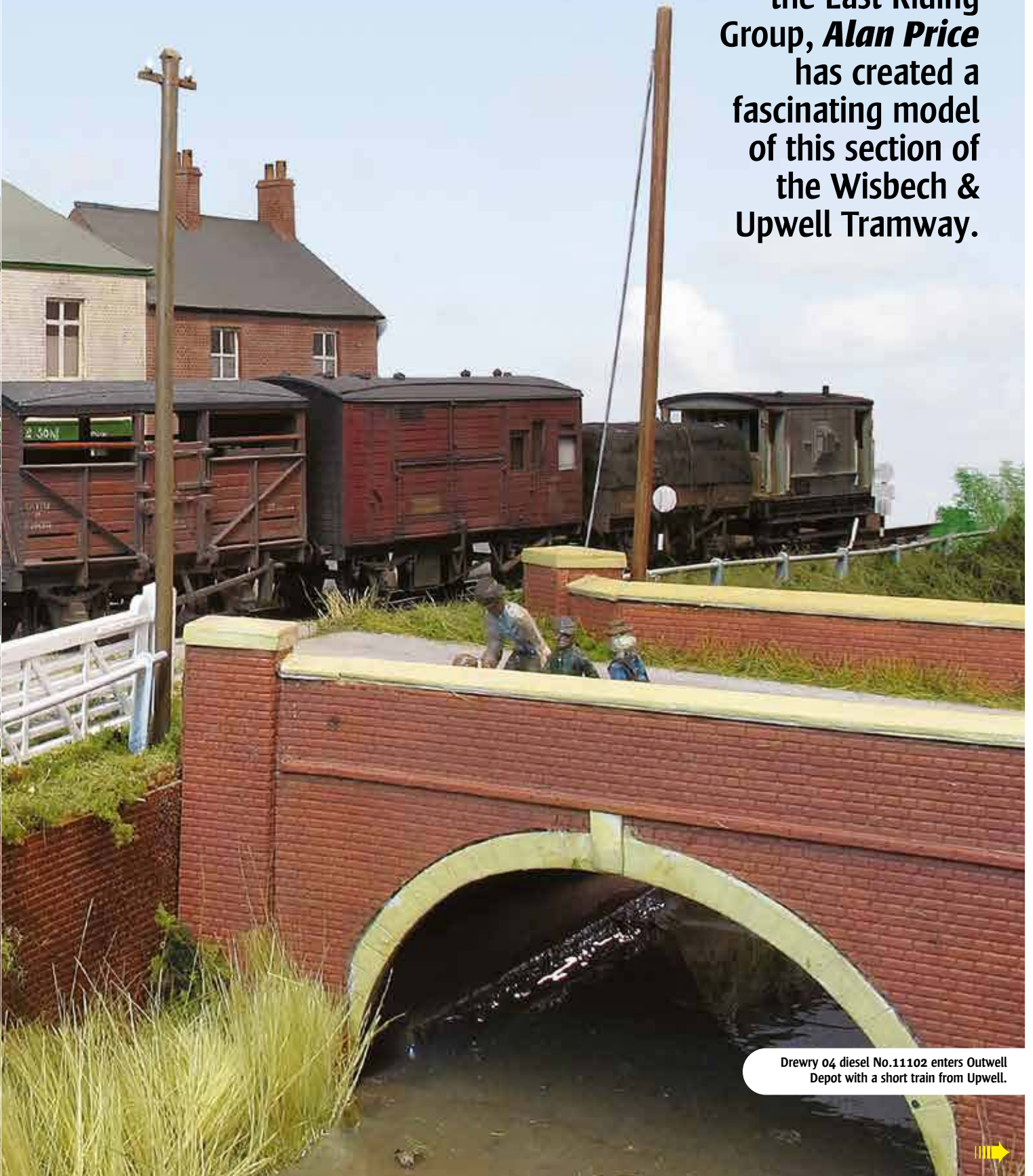
VILLAGE DEPOT



FACT FILE

- **LAYOUT NAME** Outwell Village Depot
- **SCALE/GAUGE** OO
- **SIZE** 8' 0" x 2' 0" (scenic section)
- **ERA** Switchable pre- and post-war
- **CONTROL** DC
- **LAYOUT TYPE** Exhibition
- **EXHIBITIONS** St Albans, January 11/12; High Wycombe MRS November 2014
- **PHOTOGRAPHY** Andy York

With some assistance from the East Riding Group, **Alan Price** has created a fascinating model of this section of the Wisbech & Upwell Tramway.



Drewry 04 diesel No.11102 enters Outwell Depot with a short train from Upwell.





Class J70 0-6-0 steam tram crosses the canal, heading for Wisbech with a train of vans; the first vehicle is the 'Fruit Traffic Office Van'.

1945-1950 saw most activity - it was not uncommon to see double-headed trams with up to 60 fruit vans in tow.

East Anglia might seem somewhat like the back garden of England, but has long been a productive region for produce. The Great Eastern Railway decided to build, as an experiment, a standard gauge line from Wisbech to Upwell but to the 1870 Tramways Act. This enabled the line to be built with less regulation and less cost than a conventional railway. With its proximity to the road, locomotives were somewhat unconventional with the workings and running gear enclosed and subject to speed restrictions of 12mph, with further

restrictions of 8mph when crossing roads and 4mph when passing through facing points.

The tramway opened on August 20, 1883, but only as far as Outwell. On September 8, 1884 the line through to Upwell opened to traffic. Passenger services ceased as early as December 31, 1927 as a result of competition from road transport. Goods services were far healthier, continuing for another 38 years. The years 1945-1950 saw most activity, when it was not uncommon to see double-headed trams with up to 60 fruit vans in tow. This buoyant trade saw investment in new diesel trams, making this quiet backwater the first fully dieselised branch in the country, but due to road competition, the line finally closed on May 23, 1966.

The model

You might not think it, but the layout began way back in 2000. After being involved with a York club layout, Neil Rushby and myself decided to build a layout of our own and looked for a suitable location which would satisfy our interests, mine being LNER steam, Neil's being BR diesel. When Neil came across an article about the Wisbech & Upwell Tramway, we considered that both our interests would be well catered for, and the largest intermediate depot on the line, Outwell Village, most suited our needs in both space and operation.

Occupying just two 4' x 2' boards and flanked with a fiddle yard at each end, this gave us a layout of manageable size, yet offered authentic through running. By coincidence the length of the scenic section turned out to be about scale length which was a bonus, as it gave a good perspective of the size of the prototype. Baseboards were constructed from 4" x 3/4" timber, the depth allowing for the canal and well creek. Tops were 6mm ply with cork laid on top for the track bed. Despite the depth of the boards they are still reasonably light, an advantage when carrying them to shows. The fiddle yard boards are all ply, coming from an old layout of Neil's.

Trackwork is SMP code 75, all points hand-made with copper clad sleepers. The chosen gauge is OO as this suited the stock we already had. Point operation is 'wire in tube', actuated by a rocking

KEY DETAIL



BRIDGES

The two bridges that cross the waterways had to be built *in situ*, as they are part of the baseboards. The rail bridge has a ply base as this is part of the board tops. The sides were constructed from scratch using 'Plastikard' as were the railing supports, these were drilled to accept the wire that the railings are made from. The road bridge again is part of the baseboards, with a ply sub-frame covered with embossed 'Plastikard' brickwork from the Slater's range.



lever driven by a toggle switch wired to change the frog polarity. Sand was used as ballast for the track, sprayed with Humbrol Dark Earth. The yard area and roadways were laid down using photo mount card. The fiddle yard boards have Peco track and points for robustness and ease of build.

The layout attended shows from early in its build and developed as we went along. The intervening years saw Neil move to Wales leaving me to slowly progress with adding structures and stock as time allowed. However, although it had been very well received at shows I felt it needed a little more to give it atmosphere. This brings us to where the layout stood at the 2011 Hull show.

It was there that I had a conversation with another Neil - Neil Ripley - as to what he thought I might be able to achieve in bringing more atmosphere to the scene. My thoughts were that the model needed to be brought up to date scenically and be made to better stand out on a circuit awash with branch line layouts.

As it turned out, both Neil and fellow East Riding Finescale Group modellers had themselves considered building a model of Outwell Village Depot some years back (and indeed Neil had supplied his research to us to use in the development of this layout in its early days). As such he was well versed in what could be done to this model. What you see here is what transpired from that conversation.

Domestic circumstances means that I have no real room to work on the layout at home, except in the living room which, as you might imagine, doesn't go down too well with the good lady! So

finding how accommodating the lads of the ERFG can be was a bit of a wish come true. With the layout quickly moved to their 'playroom' in Hull, and after some discussion with the rest of this group of experienced modellers, a list was drawn up of things to be done.

The backscene was the first to be tackled as it was felt that the original was too high and was finished in blue, the wrong colour. This had the effect of drawing the viewer's eyes away from the model itself, so it was removed, trimmed down a few inches and painted white. I had always thought the inclusion of St Clements church would add greatly to the atmosphere of the layout, as it was a prominent feature of the area and appears in the background of many of the classic photos of the line. Therefore Neil, as the resident artist in the group, was tasked to transform the newly neutral backscene. As can be seen in the photographs, he made a superb job of it which not only enhances the layout but gives it real depth.

While this was going on I busied myself with making more trees to go along the rear using multi-strand wire twisted into shape and covered in Artex, with foliage added in the usual manner.

With my job in agriculture I had plenty of time to study differing types of tree and how they grow. Always good to have something to work to when trying to create a realistic model!

Another shop/house seen in early shots of the line was also added, which I built using the same method as for the other buildings. This was sited to the rear of the yard office on the main street. Though not strictly correct for depicting the later years of the line (it had been demolished), it has the effect of nicely blending the scenic area with the painted backscene, and gives a nice continuation down the street.

In addition to the extra trees, vegetation and the distinctive fencing of the prototype (scratch-built in brass by Mally Baker) just seemed to appear in place as the lads got stuck in on the weeks when I was unable to visit the shed! Other details such as reeds, ducks and swans on the waterway, plus the semi-sunken house boat (once a feature of the real location and replicated in plastic card on a suitably modified resin hull) were also added, along with a selection of modified and detailed road vehicles and figures to fit in the main period when the layout is set. The layout suddenly seemed re-vitalised and sprang to life in the way that I had always wanted it to.

Structure and running

The scenic modifications were not the only upgrade given to the layout. On the advice of the gang, the two original scenic boards were reworked with easy to use integral folding legs in place of the multi-piece separate set previously employed. This, combined with lightweight plywood covers, makes the model far easier to transport and display.

It is also my belief that you must try to keep something moving at all times. Now I know that does not happen on the real railways, but to keep the public's interest it is a must to provide a good level of entertainment. The old fiddle yards, only having two single ended storage roads each reached by a single point, meant that trains had to be handled and turned by hand. With three link couplings on the stock not the easiest of tasks!

Taken with the suggestions proffered by the

KEY DETAIL BUILDINGS

The buildings on the layout all stood at their location at sometime, and are all made using the same method. Construction starts with a scale drawing of the item to be built. Sizes are gauged from doors, windows, etc, with reference from the many photos in the numerous publications of the prototype. The next step is to make a card mock up, which is then placed in its intended position to check the size is correct - any adjustments are made before the final model is created.

All buildings have a plastic card shell with embossed brickwork laminated to it. Different painting techniques have been used on the buildings, depending on what finish is required. As can be seen from the photographs, some have had just a wash to represent rendering or whitewashing while others, where brickwork is more defined, have the mortar colour applied first and then individual bricks picked out in various shades. This gives (I think) quite a good representation of the brickwork. Roofs are either from Wills sheets or, as in the later additions like the shop behind the yard office or the small cottage at the Wisbech end of the layout, from the York Modelmaking range of laser-cut self-adhesive roof tiles.





A short train leaves Outwell with a view of the depot and St Clements church in the background.

group and the well thought-out fiddle yard arrangement on the group's other layouts, I wondered if something similar could be used on Outwell. At the time I was unable to get to the group's room for a short time due to work commitments but when I did finally return I was delighted to find Colin Stark, the resident carpenter from Black Cat Baseboards, had constructed two new fiddle yards with three storage roads entered *via* a turntable. This greatly reduced the handling of stock, increased the storage capacity and made the layout capable of one man operation if need be. Each fiddle yard has its own controller, so the other operators can turn and set trains ready for their next turn of duty. This helps to keep periods of non-movement down to the bare minimum. A switch on each board allows the main controller to take charge of trains when the main operator is ready to accept them.

The whole picture

At exhibitions we want to show off our models, and to this end I try to create a pleasing and tidy exhibit. We had a look at the lighting and decided to replace the four old bulbs with two strip lights. This meant a new set of lighting boards so again Colin came up with the goods while John fitted and wired the new lights. This whole arrangement is hinged in the middle so it folds and protects the lights in transit. The whole of the fascias have been painted black.

We spend many hours making and building our models so to put a shabby frame around them at shows is counterproductive, a good deal of care and thought should be put into how the finished display will look. I think we have done a reasonably good job with what we have done with Outwell. All the alterations that have been done are the result of the group's experience on the circuit - making the model far easier to transport and much easier in set up and break down. After all, at the end of a long weekend the last thing you want is to spend hours packing up and stripping down for a long journey home.

The future

There is not much more to do to the scenic section of the layout, maybe the odd figure here and there, so my attention is turning to the rolling stock. There is a special guard's van in the pipeline, along with two more coaches and the weathering of the original stock is also being looked at so as to bring the finish in line with the revitalised layout. **BRM**

THANK YOU

At this point I must thank my good lady for putting up with me disappearing to Hull to work on the layout and going to shows. Thanks also to the operating team that go to shows with me, which does include Chris, my wife, on occasions. Also thanks to the East Riding Finescale Group: Neil, Colin, John, Jeff, Mally, Alex, Craig, Tony, and Steve who have made me feel very welcome as part of their fold and who also assist at shows.

Thanks must also go to the exhibition managers who have invited Outwell to their shows and to the public for their comments and interest in the layout. Indeed, after its first showing following its refurbishment, it looks as though I may be booked for many more shows over the years to come.

KEY DETAIL MOTIVE POWER & ROLLING STOCK

This was one of the main factors that attracted us to model this location. I have always had a liking for the LNER while Neil's interests lay with BR. As both ran on the metals of the tramway, what could have been better? Also the types of locomotives used on the tramway were out of the norm, which again got our interest.

Initially, with my LNER bias, it fell to me to supply the steam trams for the layout. I scratch-built the two Y6 trams and as you may have guessed, I made the bodies from plastic card, with the sides scribed to represent the planking. Below the footplate the sides and cow-catchers are made from brass.

For motors, I used an old Lima unit in one, which Neil had in his spares box, while the other originally had a Hornby motor, but this has since been replaced with a 'Black Beetle' motor bogie, to give more reliable running (I hope). The initial diesel tram was Neil's interest. This was finished as the green liveried 04 number D2201 and is a much modified and back-dated Airfix body mated with a Model Power chassis.

The last of the original locos for the layout, a Nu-Cast kit for a single ended Y1 steam Sentinel, can also be seen running on the layout. One of these

was trialled on the line prior to a pair of double cab variants being ordered. However, they were found to be poor performers for the work they were to carry out and were subsequently transferred away.

The main traffic on the tramway was predominately fruit vans, and these come from the Parkside range. Opens and flats could also be seen at times, again these are a mixture of ready-to-run or kit-built with the latter being drawn from Coopercraft, Pocket Money or 51L. Most of the stock has been slightly weathered by a light application of black powder paint dusted on dry.

The tramway used Fruit Traffic Office Vans during the summer months. Starting at Wisbech they would drop a clerk at each depot down the line to Upwell. Their duty was to waybill as much traffic as possible before the van returned. This vehicle has a scratch-built body from plastic card fixed to a chassis that Neil had.

Passenger stock is from D&S Kits. These are brass etches and come as a pair of bogie coaches and a four-wheel version. Both have verandahs at each end and are low slung due to there being no platforms on the tramway, except at Wisbech East Station.



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GAUGE

LAYOUT FOCUS OUTWELL TRACKPLAN



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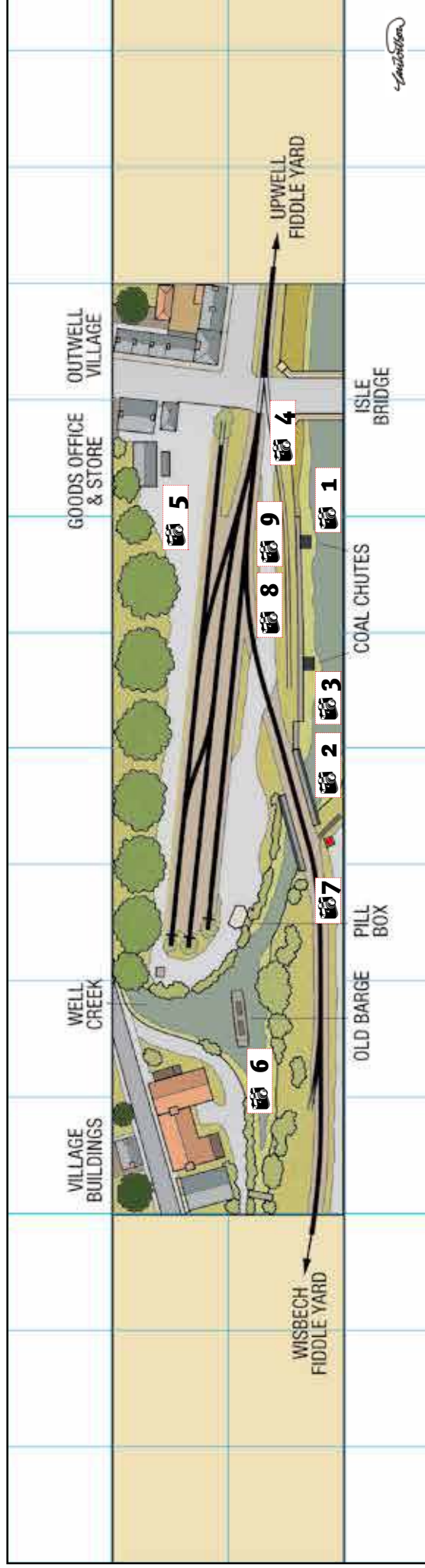
3



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5



OWNER FACTFILE



- NAME Alan Price
- RMweb NAME Alan
- AGE 59
- OCCUPATION Farm worker
- LOCATION Wilberfoss, East Yorkshire
- YEARS MODELLING 20 years
- OTHER INTERESTS Motorsport, racing



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Factfile

Layout name: Hornsey Broadway
Scale/gauge: 4mm: 1ft / 1:76 scale / EM
Size: 24ft x 3ft
Era/region: 1970s / BR(s)
Location: London
Layout type: Fiddleyard to main line junction

EM gauge

Hornsey Broadway

Inspired by trainspotting trips and carefree days of his youth, skilled clocksmith, Kier Hardy, created this exceptionally-well observed slice of North London.

Words: Kier Hardy **Photography:** Andy York



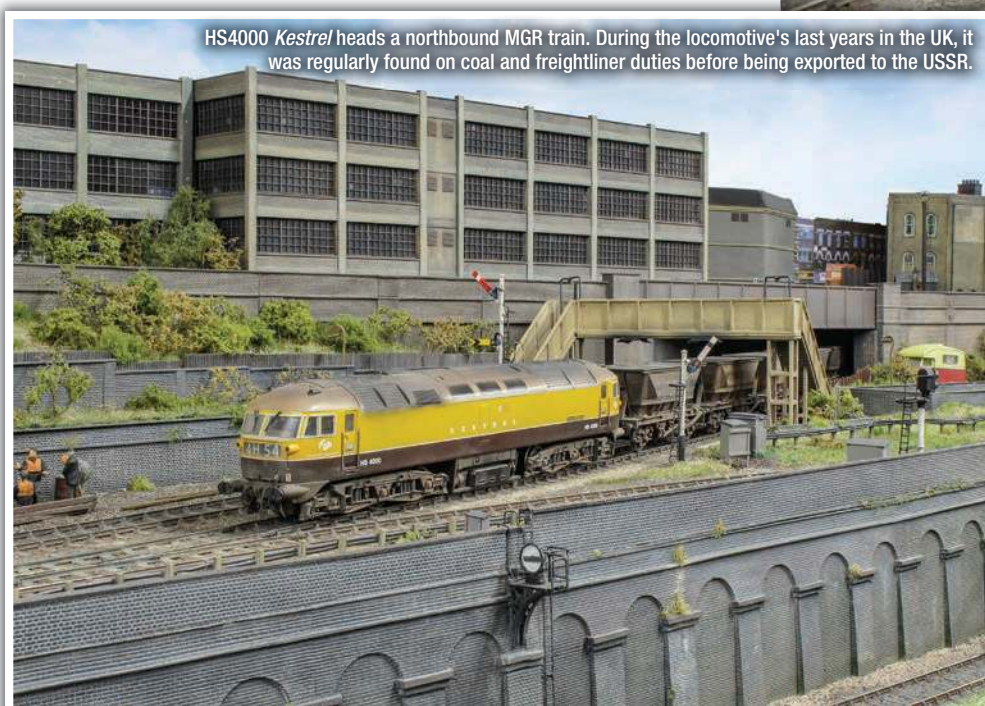
A Class 501 EMU negotiates the flyover and joins the Midland lines on the approach to Hornsey Broadway station, with a service from Broad Street. Meanwhile BR Derby type 2 (Class 25/0) 5173 is held at the signals on the Up Slow line with a mixed freight heading for East London.

A Little Poem

After a day of maths and double science,
We're looking forward to our Saturday alliance,
Putting away those school books of reference,
So time can be devoted more to our preference,
To forget about that hard week of swotting,
And look forward to a day of spotting.

Of diesels and electrics in green and blue,
On a weekend that's what we like to do,
Happily walking the line-path home,
In the distance can be heard that familiar drone,
This time of day no doubt the 1L34,
Not another Deltic, what a bore!

In the morning for our tickets we'll queue,
Whilst in the platform will be our DMU,
Getting ready to leave the station,
Filling us full of anticipation,
For an exciting day touring The Smoke,
To include Stratford and Old Oak,
With notebook and pen ready to write,
I think I'd better get an early night.



HS4000 *Kestrel* heads a northbound MGR train. During the locomotive's last years in the UK, it was regularly found on coal and freightliner duties before being exported to the USSR.

Stick with me and we'll take a trip back down memory lane, 50 years ago approximately.

Memories from the tender age of 11 take me back to the time when my family lived in Hertfordshire. The station at Potters Bar was a 10-minute walk away, where a few mates and I would meet after teatime to jot down numbers and watch the constant procession of trains passing through. Frequent suburban services in the hands of Brush Type 2s with non-corridor compartment stock were a particular favourite and, even out of view under the bridge, you knew what sort of train it was by the wheels

thumping over the rail joints. As well as a few evenings at the lineside, we would regularly take a train into Kings Cross on a Saturday to visit the main line termini and various diesel depots. Then after a full day of scribbling numbers in our notebooks, the evening would be spent in my front room underlining the cops of the day.

Layout inspiration

It's the memories of those spotting trips and carefree days of my youth that have brought about the creation of 'Hornsey Broadway', not just to feature the trains we saw, but also the sights and sounds of Greater London

and, in particular, the architecture, the surroundings and the street scenes.

It's hard to imagine in the 21st century that a small group of youngsters would be left to their own devices touring some of the most run-down parts of the capital and trespassing on railway property, but as I recall, it all seemed exciting at the time and a great way towards filling up my Ian Allan



Stock

Prior to construction, my existing locomotive and rolling stock fleet was more suited to the provincial areas of the country and West Yorkshire in particular. Gradually most of that stock has been replaced or altered to show examples that would have been seen in North London between 1969 and 1971, formed mainly of ready-to-run models customised and modified at the very least, with a contingent of kit-built stock for added variety.



An English Electric Type 4 drops down towards Hornsey Tunnel with an express service for Kings Cross. Brush Type 4 D1100 is in charge of a freightliner train heading for Stratford in East London.

ABC locospotters' book.

Those days are long gone, so with little chance of acquiring a time machine, the building of this train-set has rekindled memories and also forms a historical portfolio of how things were during the 1970s.

Prototype

Observation and research of the real railway is important to any modelling project so as to bring about some credibility in a made-up scene. Prototypical formations of freight and passenger stock have been produced for the layout using original BR documentation for reference, with advice and supplementary stock for the layout being provided by railway professionals and friends. There are always new projects in the pipeline, as well as maintenance and improvements to existing stock to keep things fresh and interesting.

Railway modelling can sometimes be a lonely pursuit, so the social interaction with other like-minded individuals on a regular basis forms a good knowledge base and provides the compatibility of visiting rolling stock and excursions to other layouts. Back in the day, I don't ever remember seeing lights on the front of trains during daylight hours, so anything fitted to a model locomotive that detracts from the realism, or which could affect its performance was removed and consigned to the bin, including flimsy methods of electrical current collection seen on some models today.

Design brief

The construction of this 24ft-long project commenced in 2009 to replace the West Yorkshire-themed layout 'Wibdenshaw', which already fitted into an existing format of storage yards and off-scene hardware. Having a good recollection of the ex-Great Northern lines out of Kings Cross with its distinctive flyovers, gradients and brick retaining walls, the non-railway aspects form a key part of this layout project as well as having something interesting to operate and watch the trains go by. There is no one prototypical place that I could copy that would satisfy all my desires, so with a bit of imagination, the scene has been developed using a variety of structures and buildings to give the impression of a North London district.

The London & North Eastern Railway constructed a route from Finsbury Park to Highgate, Edgware and Barnet, which

became known as The Northern Heights Railway. There were plans to extend through to Bushey Heath, and had it not been for the intervention of WWII and the subsequent restrictions involving the Greenbelt Act, the area would have been very different to what it is today. Some of the formation was actually built but never used and after the War much of it was handed over to London

Transport for use on the present-day Northern Line. It's not difficult to imagine that a change in history could well have resulted in the line being used as an inter-regional route and the potential for a Barnet Loop re-joining the East Coast Main Line at Brookmans Park.

With a feasible historical background to the project, the scene was set to model



some typical North London structures and architecture around an oval four-track format.

Various sketches were prepared and the 'committee' consulted. Perhaps I should add at this stage that not all of my plans were given the thumbs-up straight away, so alternative suggestions were taken on board to satisfy those who would become involved



Another English Electric Type 4 with headcode discs hauls a rake of Mk. 1 coaching stock. On the high level branch line above is a Brush Type 4 heading for the locomotive depot at Hornsey Road.



An English Electric Type 4 drops down towards Hornsey Tunnel with an express service for Kings Cross. Brush Type 4 D1100 is in charge of a freightliner train heading for Stratford in East London.

in its operation as the project proceeded. First and foremost, the layout was to be fully portable with just a handful of outings to shows planned. With 'Wibdenshaw' occupying the space at home required for the new layout, it became apparent that we only ever got to see the pretty side when it was away at shows as the viewing side was normally hard up against a garage wall. The backscene was a bit of a view blocker to say the least, so the new order of the day for 'Hornsey Broadway' would be for it to face inwards at home, and outwards when away. This arrangement has complicated the

wiring and control panel layout somewhat, but has been worth the time and trouble, especially as it spends most of the time in an operable condition at home.

Construction

My previous EM gauge layouts have been constructed using plywood for strength and rigidity, so this practice was repeated by building six baseboards of open frame construction (each 4ft x 3ft) with risers to accommodate the trackbed. The flyover forms the main feature of the layout with regard to aesthetics and geometry, allowing enough clearance to employ gradients of

1:80 rising and falling over a distance of 20ft back to datum level. The remaining 4ft of flat area is host to some complicated pointwork which forms Hornsey Midland Junction. The formation of the main lines, passing loops and the facility to switch from one track to another gives added operational interest, with the opportunity to feature multiple routes diverging within the scene. To avoid topographical compromise of the scenic area, one third of the main running lines are subterranean with the area above providing the space for a large motive power depot. The tunnel mouths are at the lowest point of the layout and help reinforce the characteristics of the ex-GNR route descending towards Kings Cross.

Due to the inaccessibility of certain sections of the permanent way, it has been built with epoxy-filled and strengthened

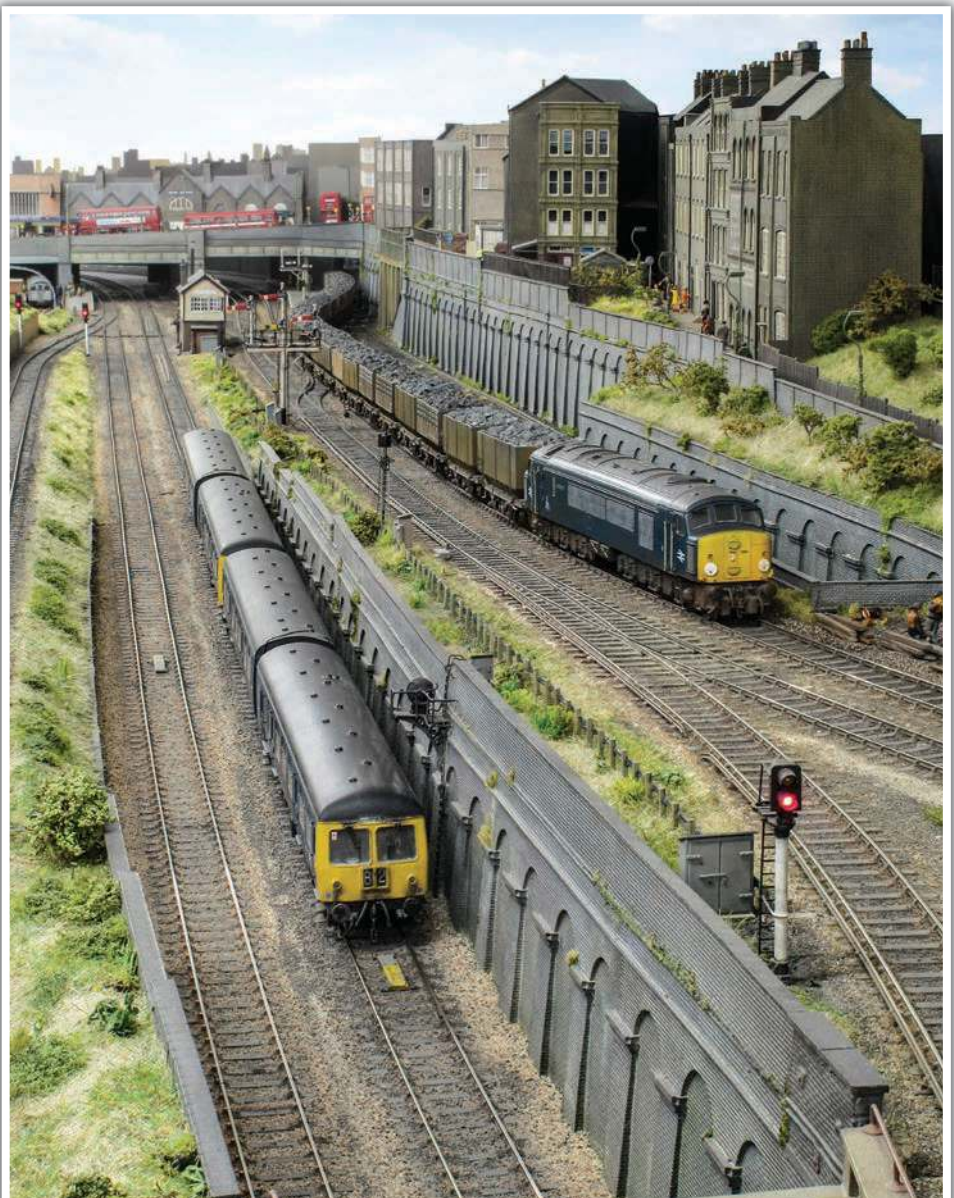


A busy scene at Flower's garage as various cars undergo servicing and repair in the well-equipped workshop.

check rails at every dowelled baseboard joint, pre-soldered to copperclad paxolin strips. There's a lot of track and pointwork to maintain, with long heavy trains passing over it on a regular basis, so I felt the most important factor had to be reliability over prettiness. It was a conscious decision to revert back to using copperclad sleepers for all the pointwork on the scenic area, as this speeded up construction considerably and I have always found this method robust and easy to adjust. Look closely and it will be obvious, so with some careful ballasting and weathering, this aspect of the trackwork isn't so noticeable as it blends in with the ready-to-lay C&L sections as part of a much bigger scene. Remote point control utilises cheap, cheerful and chunky H&M solenoid units with additional microswitches for polarity changing and auxiliary feeds, each one fed from a CDU bus loop through heavy-duty relays, so as to extend the life of the panel switches.



A pair of English Electric Type 1s are given a clear road from the station in charge of a coal working destined for East London.



A four-car Cravens DMU service for Kings Cross heads along the Up Fast, while a 'Peak' heads for East London with a loaded train of coke wagons.

The story continues in the next issue of Traction Magazine... on sale April 3.



The London Transport building was made by Pete Johnson using Plastikard with laser-cut windows. To finish off, the signage artwork was drawn up on a computer and printed before securing it in place. London Transport logos were a common sight at some stations during the mid-20th century, later giving way to 'Underground' signage. Those familiar with Perivale under-ground station will recognise many of the features of this structure. With the exception of the Williams & Glyn's Bank (modified Bachmann), all of these buildings are scratch-built to represent typical London architecture.

The main station building is based on examples that were built by the Great Northern Railway, typical of many found in the North London area and in keeping with the theme of 'The Northern Heights'. Again, the signage gives a clue to the era, the British Railways dark blue enamel soon to give way to corporate BR white and black.

WILLIAMS & GLYN'S BANK



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HORNSEY BROADWAY



Except buses



Except buses

Road surfaces use fine grade wet & dry abrasive paper and the markings are made from strips of masking tape cut on a flat glass plate, some pre-coloured with a felt tip pen to represent double yellow lines. Broken lines are fixed in place as a continuous strip, then cut in situ with a sharp knife.

It's apparent that the area around here isn't flat, so I felt it important to highlight gradients on both the railway and the roads, including Bridge Street. The infrastructure and retaining walls bring another dimension to the scene, with the higher ground towards the back of the scene, utilising the buildings and structures as backscenes in their own right.

Bridge Street, Hornsey Broadway

This view shows a typical weekday scene at Bridge Street, Hornsey Broadway, where it's possible to catch a bus, train or the tube directly to most parts of the Metropolis. Whatever the means of getting here, there are shops, businesses, and social attractions to cater for most. For us young trainspotters, it's the start of a 20-minute walk to the motive power depot to spot some locomotives.

There's plenty of street furniture in the scene, some of it such as lamp posts and telephone boxes, ready-to-plant from the likes of Ratio and Gilbow. Belisha beacons and signposts are constructed from a sleeved brass tube, finished off with yellow map pins and lithograph signage. Additional scratch-built items such as litter bins and bus shelters are again made from brass. Kibri produces the barriers which are often found around traffic intersections and pedestrian crossings. They have been painted and finished to look like galvanised railings.



To highlight this busy scene, the figures have been acquired from a number of different sources including Airfix and Bachmann. Many hours have been spent customising and painting so as to fit in with the era. Vehicles and buses are also populated using Prieser HO scale people, as their slightly smaller size makes them ideal for this purpose.

Layout control

The control system used on the layout is certainly nothing new, just providing a variable supply of DC volts using bomb-proof hand-held controllers mainly from Gaugemaster. They're always getting dropped by butter-fingered operators (myself included) and the worst-case scenario is 10 minutes at the bench with a soldering iron to put things right. Simplicity of the control system is one of the most important aspects of operation, so there's a great deal of satisfaction in being able to flick a switch, push a button and then spend time watching the trains pass by, instead of trying to communicate with a microprocessor about different addresses and numbers. Not everything involved with the layout control uses mid-20th century technology, as there's quite a few semaphore signals on the Midland side of the tracks, their servo motors being driven by a Megapoints controller via switches on the control panel.

Scenery

One of the most enjoyable aspects of building 'Hornsey Broadway' has been the scenic aspects and structures which are to be found beyond the boundary, inspired by those days out during the 1970s. Countless sheets of brick embossed plastic and mashed up building kits from different parts of the globe have been brought together to form this slice of North London, with observation being the key.

An early exposure to public transport, along with my enthusiasm for buses, brought about a good opportunity to model them in a typical interchange scene as found in many parts of the Metropolis. The station area has been designed to show a cross-section of stationary buses in a busy environment, as well as providing a scenic break at the north end of the layout.

Road vehicles are represented to suit the era, mainly from the Oxford Diecast range. Every vehicle is treated to some form of customisation, fitted with drivers and passengers as well as the shiny paint-work toned down with matt varnish. Modelsounds stand-alone bass speakers with customised MP3 files on a 6-minute loop represent vehicle engine noises, slamming doors, muffled voices and squealing brakes. It's certainly not in your face, but does bring another dimension to the scene with background ambient sound bringing a busy station area to life.

My final layout?

In summing up I would consider myself very lucky to have been associated with so many railway modellers over the years, from those early days as a member of the Gloucester Model Railway Club, which provided the help and encouragement

to build my first exhibition layout. It's also been a pleasure to be associated with various modelling groups and individuals responsible for iconic layouts such as 'Hayley Mills', 'Russell Bridge', 'Worcester Road' and 'Canada Street', which have provided inspiration by the bucket load. Special mention should also go to Mike Walker for his dedication in organising model railway events in the Cheltenham area, raising many tens of thousands of pounds for charity, and highlighting the fact that we're also here to help each other in many ways.

'Hornsey Broadway' will most likely be my last foray into the big trainset league, but there will no doubt be many more years of improving and adding to the rolling stock roster, or revisiting structures on the layout that can be improved upon. Finally, special thanks are due to Paul James and Greg Brookes for their practical skills, knowledge and contributions to the project.



Quality car sales at Flower's car lot – a good selection of second-hand cars at very reasonable prices, ranging from a Austin A35 for the bargain price of £50 to a top-of-the-range Jaguar for £490.





Meet the Modeller

Kier Hardy of Hereford has a good modelling pedigree with his last layout, 'Wibdenshaw', an exhibition favourite for many years. He runs an excellent website, emgauge70s.co.uk, which features his modelling and those of his EM gauge modelling friends with excellent practical articles on projects. Kier's workshop is a delight to visit. He specialises in the repair and restoration of clocks and timepieces and it's a pleasure to visit. A real craftsman.

What I love

by Andy York



Underground

Although the short section of underground line is non-operational, the detail is superbly observed with the lineside cabling and cable arch.



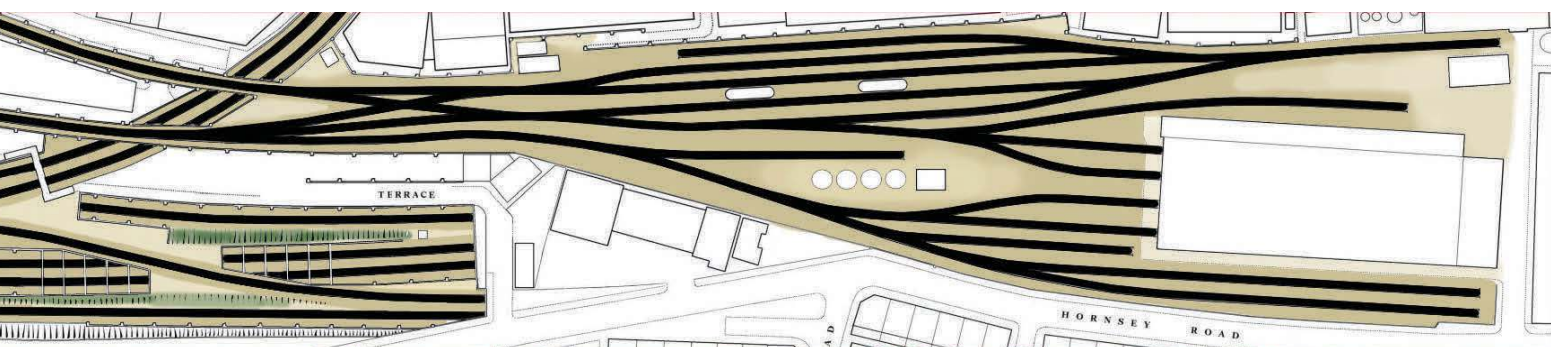
Shed-bashing

The Hornsey Road depot scene, which is described in Traction Modelling next month, evokes an era of trudging London streets to cop a few locomotives at Stratford and Old Oak Common.



Everyday life

The layout includes many aspects of everyday suburban London life, including period signs and styles.



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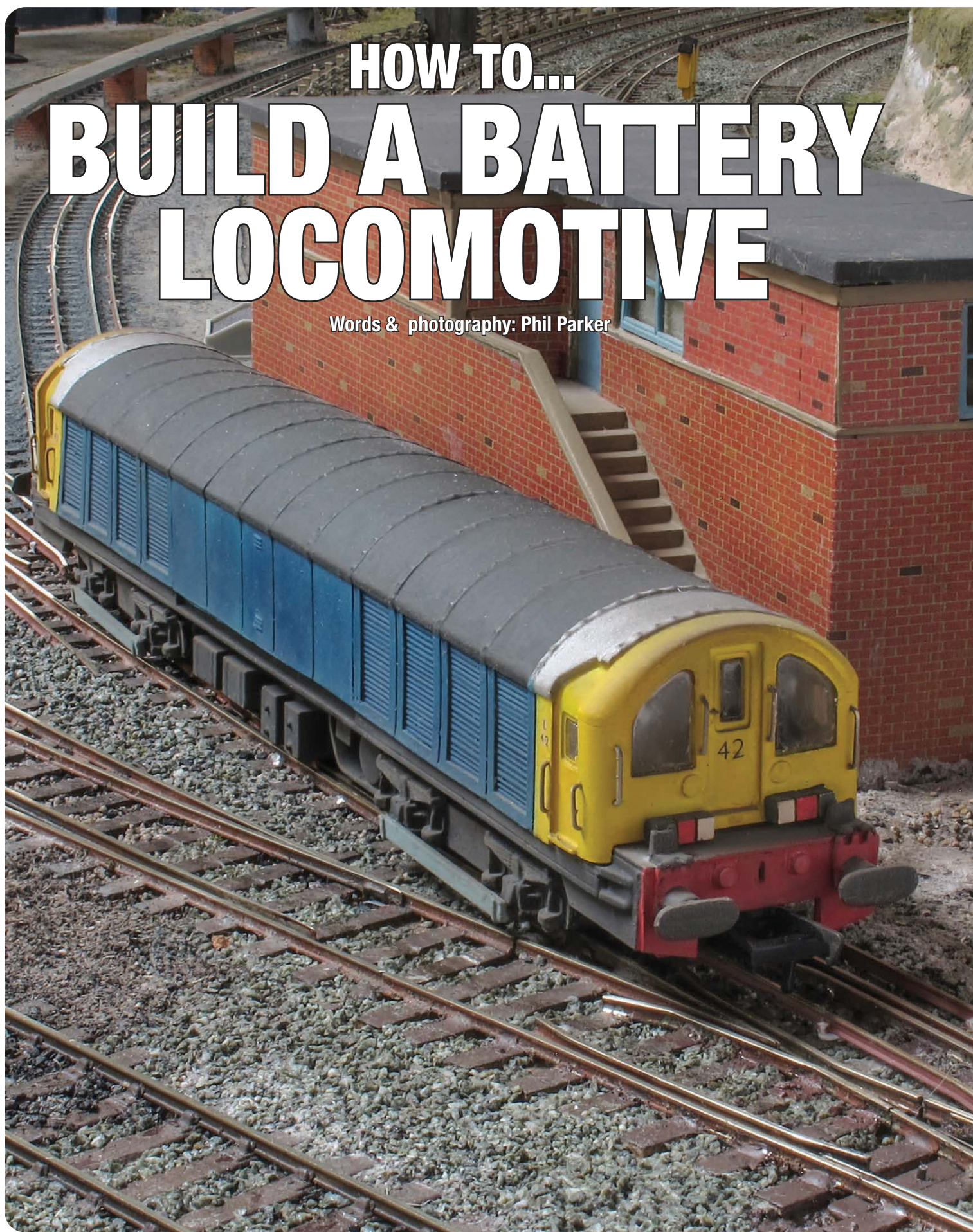
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HOW TO... **BUILD A BATTERY LOCOMOTIVE**

Words & photography: Phil Parker





We all know that London Underground trains are electrically-powered, picking up power from rails in the centre and at the side of the track. Obviously, this isn't a good idea when there are permanent way staff working nearby. In these instances battery-powered locomotives are required.

The first examples were built in 1905, but the 1936 batch set the template for future examples being designed to the tube line loading gauge. In total, 52 locomotives were acquired in six batches from four different manufacturers. As you would expect, every batch was slightly different and the machines have been modified and upgraded throughout their lives.

If these prototypes interest you, there is an excellent 'London Underground battery-electric locomotives' thread on our forum, RMweb, started in 2012, with prototype photographs and even a plan of the 1963 example. Several enthusiasts and an engineer on the real things have joined the conversation and I'm sure they will welcome newcomers with open arms.

There's also a discussion of the various kits available. I'm using a resin-body kit from Radley Models. It's simple to build and uses a ready-to-run power bogie.

Assembly can be with glue or solder. I chose the latter for the whitmetal parts for speed, but epoxy or superglue would be just as effective.

The kit started life with Harrow Models before being passed on to Radley and it has been upgraded along the way. The instructions show a whitmetal body, but this has been replaced with a single piece resin casting in current kits. They also suggest resin bogie sideframes, but there are whitmetal ones in the box.

According to experts, the model is a bit of a hybrid, being based on a 1947 prototype, but with features from the 1937 batch. The best photographs I found show a 1965 (I think) machine that has received a few

upgrades, most obviously the replacement of the end lights with modern WIPAC versions.

Many liveries have been applied. Overall maroon from the early days or a complete coat of hi-viz yellow today. I like the yellow and rail blue in the photographs so chose this, even though it's slightly more complicated to paint and mask.

Thanks to all the variations of the prototype, my model isn't perfect. You'd need more modifications and photographs for that, but it went together well and looks the part. Tension-lock couplings prevent me from trying to add all the end detail that is a feature of these machines, but I'm not complaining as this is where underground machines get very complex.

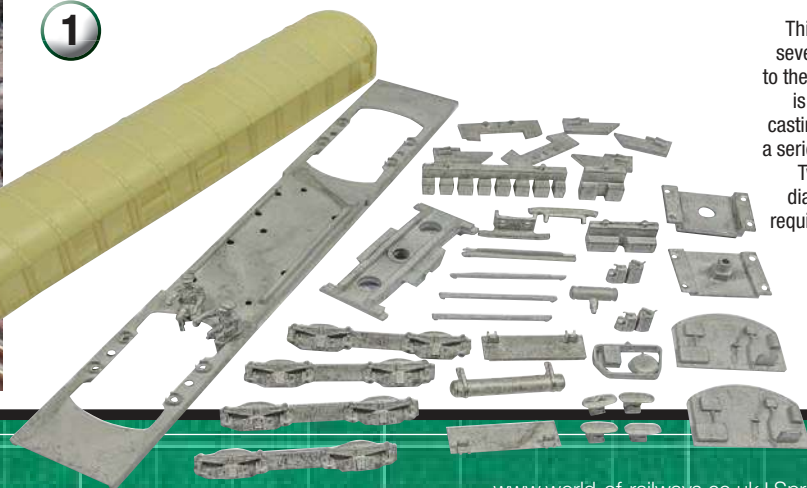
Strictly speaking you should build two kits, because battery locomotives normally work in pairs, one at each end of a train to avoid running round, but the 1964-onward examples are passed for single operation. They only operate on batteries once they reach the site, picking up power from the rails for most of the run.

The machines can work over the entire network and are normally stabled at Ruislip Depot when not in use. They don't appear to escape from the Capital very often, but Clayton Equipment has tested its version on the Ecclesbourne Valley Railway, so they get a holiday sometimes if you are looking for an excuse to run one.



A prototype spotted on the Ecclesbourne Valley Railway in 2019 by Paul Barrett.

1

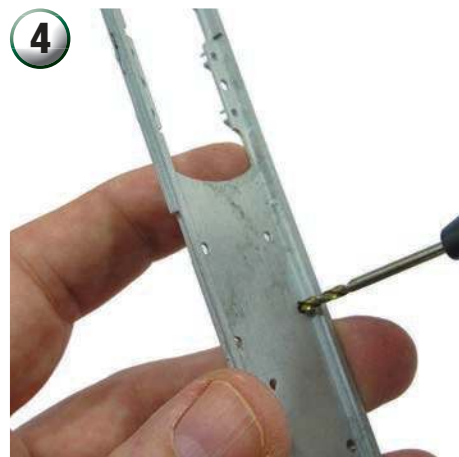


This kit has been through several versions according to the instructions. The body is a nice one-piece resin casting and the underframe a series of whitmetal parts.

Two axles with 10.5mm diameter wheels are also required, plus a power unit.



A Tenshodo SPUD (Self-Propelled Underfloor Device) is the magic unit that powers this model. Supplied ready-to-run, the only problem with them is that conversion to DCC is difficult.

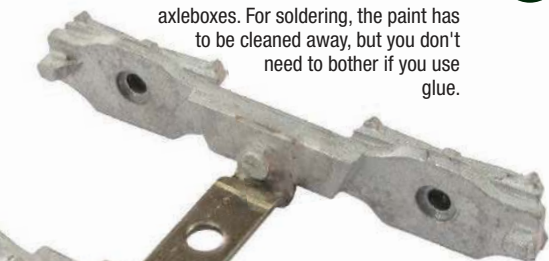


Because of limitations inherent in the casting process, holes in whitmetal normally need to be opened out a little with a drill bit or round needle file so the pegs on the castings fit nicely. You don't want a power tool for this, finger twiddling gives better control.

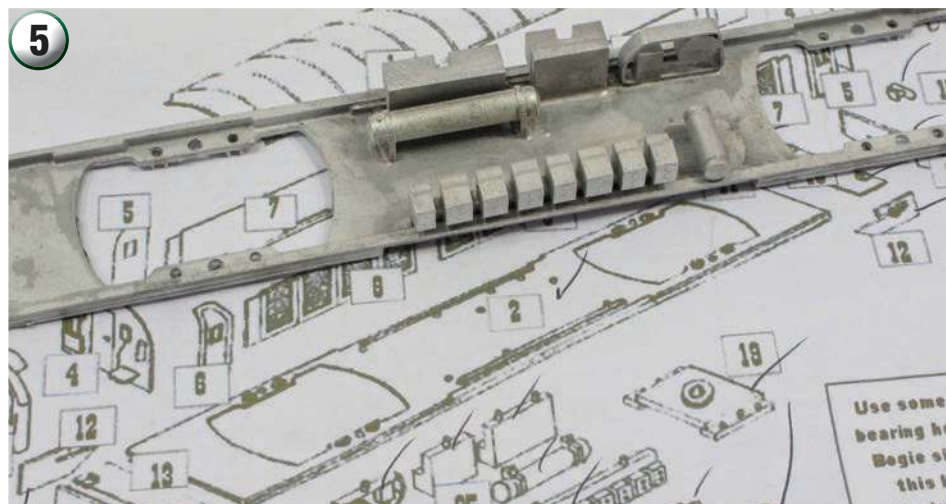


The front end slots together well but benefits from a dry run to ensure the correct side pieces are fitted the right way around.

In the SPUD box are a couple of brackets to clip in the bottom for holding sideframes. This job needs the fancy, curly end version as it holds the frames so the axles are in line with the axleboxes. For soldering, the paint has to be cleaned away, but you don't need to bother if you use glue.



The castings are pretty clean but there is a bit of flash between some of the boxes on the underframe. Folding a piece of emery paper in half allows for this to be cleaned out. Polish any moulding lines off the buffer faces and other obvious areas at the same time.



An exploded diagram in the instructions shows where all the bits go. Make sure you have the chassis the right way up when fitting them; remember the picture shows the top with parts fitted underneath. It's all too easy to get this wrong as I've found out in the past.

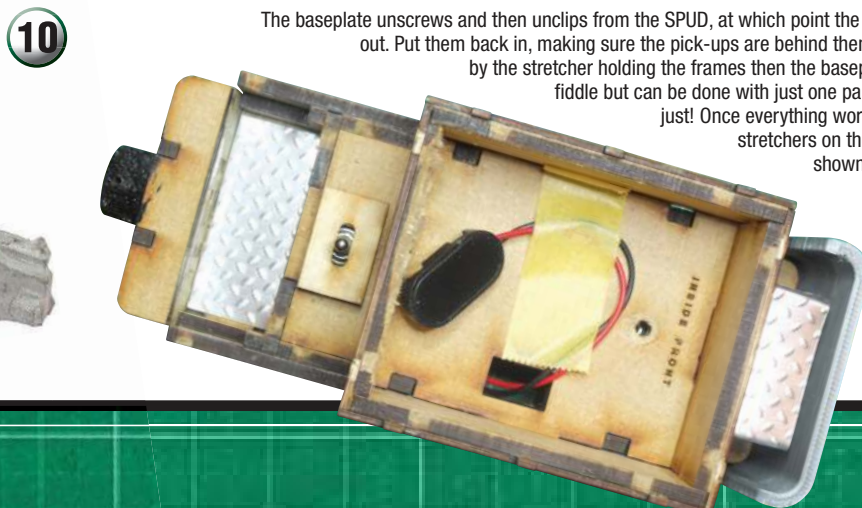


The unpowered bogie should be fitted with brass bearings fixed in place with glue. Since I'm soldering the parts together, I'm using epoxy but superglue will work if you aren't heating things up. Make sure the bearings are properly seated in their holes or they will bind on the axle ends.



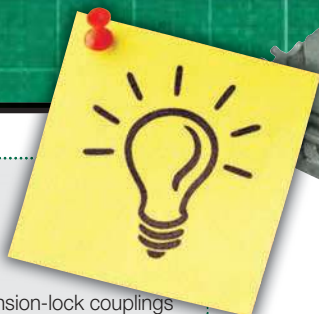
To finish the bogie, put the wheels in place, then trap them between the sides. Sadly, you can't get them out later, but as long as the build is nice and flat this won't be a problem.

The baseplate unscrews and then unclips from the SPUD, at which point the wheels fall out. Put them back in, making sure the pick-ups are behind them, followed by the stretcher holding the frames then the baseplate. It is a fiddle but can be done with just one pair of hands, just! Once everything works, add the stretchers on the ends (not shown here) with glue.



TOP TIPS

It's too easy to gum up tension-lock couplings when weathering a model, especially if you use an airbrush. Masking with tape will stop the paint, but leftover adhesive can be just as bad. Aluminium kitchen foil can be formed around the couplings, needs nothing to hold it in place and stops paint and powder getting where they shouldn't.



Brass nuts are fitted in the main chassis to hold the bogie supports. I'm gluing them with a smear of epoxy. Test the threads are clear by running a bolt through, but only after the glue is fully-cured.

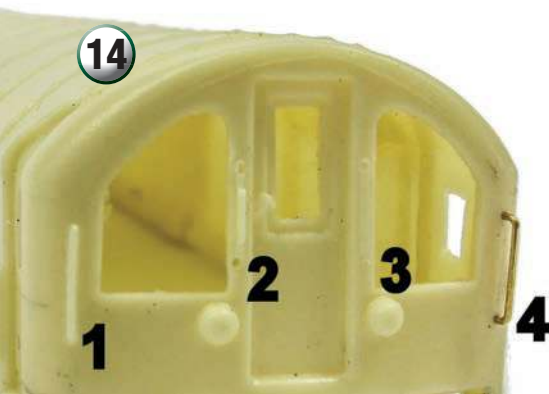
12



Read the RMweb London Underground thread at www.brmm.ag/london-underground

The supports bolt in and bogies bolt to these. It's a nice system that allows for the parts to be easily removed in the future for painting and maintenance.

14



Moving on to the body, the moulded handrails need to go. My method is as follows: 1) Moulded handrail. 2) Trim the ends with a sharp knife and use these as a guide to drill 0.45mm holes. 3) Remove the rest of the handrail. 4) Superglue the replacement made from 0.45mm brass wire in place.

17



To extract a resin body from its mould, a greasy release chemical is used. This will repel paint and so needs to be cleaned off using washing-up liquid and lukewarm water. Don't use hot water as it can distort the resin. Rinse afterwards so there are no traces of the detergent left, then leave the model to dry.

15



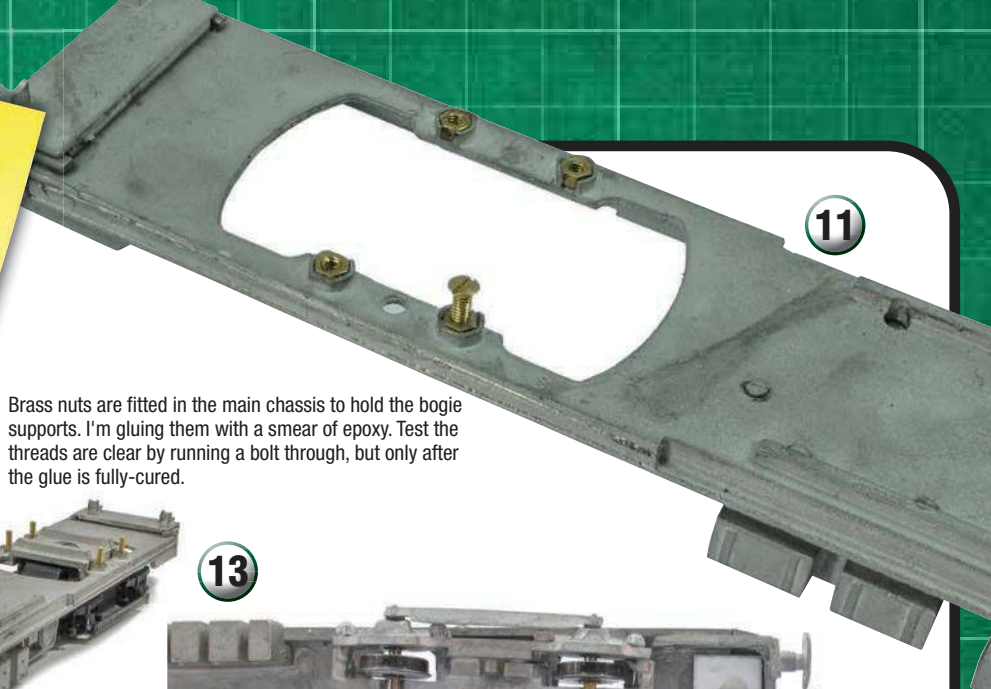
The modern locomotives are fitted with WIPAC lights that aren't in the kit. Without measurements, I've made a best guess and created them from 1.5mm square plastic strip. To make each lens, I cut half way through the strip with a fine-bladed saw, each cut being 1.5mm apart. A mitre block keeps things tidy and the cuts vertical.

18



Painting anything yellow can be a nightmare because it's a colour that doesn't cover well, even on a cream-coloured resin. A thin coat of white aerosol primer ensures a stronger and brighter colour.

11



13



No provision is made for couplings other than 3-links, so I add a pad of 1mm thick Plastikard at each end then glue Bachmann cranked couplings to this. It's not ideal as the swing at the ends is quite large on tight curves, but there doesn't seem to be another solution.

16



When fitting the new lights, the old versions are sanded back to represent blanking plates. Buffers can be fitted in the conventional position, or folded up on to the footplate.



19



To achieve a weathered blue on the sides, I paint the model with rail blue and then mix this with Humbrol 147 (Light Grey) to produce a paler shade that I dry-brush over the base colour. This is repeated with a little more grey in the mix each time to provide a variety of shades.

20



Finding the correct transfers for this era of model proved impossible. The closest I could find was Gill Light 1 and 2mm tall characters from Fox Transfers. The end numbers really need to be bold but at least the results are better than my signwriting. Because my model is generic, I've numbered it 42 which doesn't actually exist, so I can't be wrong!

21



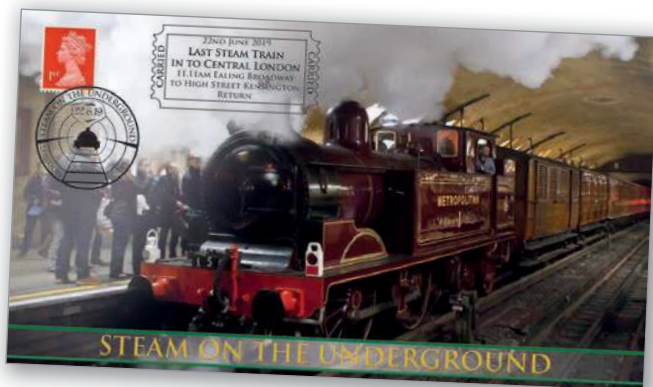
After a light dusting of weathering from the airbrush, the glazing is fitted. It's a nice set of vac-formed clear plastic parts that can be glued in place with a suitable adhesive. If you've replaced the handrails with wire, check there aren't bits of wire sticking through the body and getting in the way. I had to clip a few off to get the glazing to sit properly.



The finished model. My London Underground battery locomotive build.

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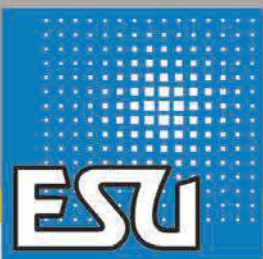
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INBOX

Q&A
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LAYOUT IN A BOX

I was very interested to read your article in the February 2020 issue of British Railway Modelling about the 'Layout in a Box'.

I also have a layout in a box, which is called Ainswick Road. The layout represents a small traction maintenance depot on the edge of a British city, primarily used for refuelling but with facilities for light maintenance and inspections. It is set in the period 1990s – 2000, so a range of locomotive liveries can be seen. The depot also has deliveries of fuel by tankers and parts/stores by vans.

The layout was designed to test whether a micro fold-up layout could be built no bigger than one sheet of A3 paper when folded away. The baseboard is of 9mm ply with softwood and MDF edges. Track is OO Peco code 100 with pushrod point control.

The scenery and buildings have been made to plug-in to the layout like a 3D jigsaw. Unpacking and setting up takes around 20 minutes. The scenery, controller, control panel, fiddle yard and stock will fit into a 40L hinge-lid plastic box, so the layout is easy to transport and can be taken by bus or train! The layout can be run as DC or DCC.

John Davy



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Your forum for sharing information, questions, praise, grumbles, projects, layouts and your thoughts on the hobby then and now.

A RETURN TO WALES



I have had a lifelong interest in model railways since the age of four years old and I am 62 in May this year. Beckland End and Tye Bridge were both purchased in August and November 2018 and Eastfield in summer 2019. I have always loved narrow gauge railways due to the fact that one day, I would like to return to Wales to tour round the narrow gauge lines, but as I am disabled, this will have to involve many months of future planning.

David Southey



HAPPY CUSTOMER

I'd like to say how much I enjoy the BRM magazine especially the DVD, which I have playing while working on my layout to encourage me to keep going. A big thank you to you and all the team at BRM for the excellent work that you do..

Robert Winchester

FRED BLACKMAN REMEMBERED

My father-in-law, Fred Blackman, died on 22 January 2020 at a hospice near his home in Norfolk, with his daughters Angela and Judith by his side.

Fred had been involved with the design, development and manufacture of printed circuit boards in the 1950s, working with people who had developed the techniques during the Second World War. When the chemical milling process subsequently became available and the ability to double side etch, this gave Fred, a keen railway modeller, the idea to apply the techniques to model railways.

In the late 1960s, Fred came to a weekly South East Essex Railway Society meeting in Leigh-on-Sea with the basic parts for an etched brass 4mm scale LNER Class N7 0-6-2T and some Gresley coach sides. However, these ideas did not progress very far, until a move from Essex to Surrey in 1971.

Moving to Surrey resulted in a meeting with Pendon Museum including Guy Williams, Tony Reynolds and Paul Karau, who saw the potential. Numerous kits subsequently became available under the Mallard Models brand in both 4mm and 7mm scale, such as SECR birdcage stock, Gresley stock, LMS panelled stock, GWR carriages, Siphon H and 'Monster'. At that time, finding accurate historic information to produce accurate drawings was the main problem and the high number of GWR items reflected the involvement of the Pendon Museum.

Mallard Models was very much a 'cottage' industry. Fred's younger daughter's bedroom doubled up as his design studio, complete with drawing board. The tiny spare room was the packing room, where his late first wife Stella and daughters packed the kits, and the loft space was the darkroom. Later, he opened a model shop in Camberley, where he also moved kit production.

After a period in hospital in the 1980s, Fred decided to close the business, which was sold to his friend Dave Smith and the kits subsequently became available under the Blacksmith Models brand, which was subsequently sold to Martin & Annette Breakspear, trading as Cooper-Craft. Unfortunately, the kits are apparently no longer available.

Fred was born in Bow, East London in 1935. The family soon moved to Romford where he spent many hours at Romford Station. He was most proud of his GER/ LNER Class D16/3 4-4-0 model, the very first 4mm scale etched brass locomotive kit, which he produced in the early 1970s. The design included a new concept of a separate chassis to the mainframes, which pivots within them, with the aim to keep the driving wheels in constant contact with the track.

The funeral service took place on 24th February 2020.

Paul Goldsmith

BOILER EB WAGON SET

In the history of wagon building, 1965 really stands out as the year when the biggest railway wagon ever was built, the Boiler Set. The then Central Electricity Generating Board required movement of huge pressure vessels from both Renfrew and Wolverhampton to the new generation of power stations being built.

The largest vessels weighed typically 290T and were 120ft long. Collaboration between BR and the CEGB brought the building of the wagon set to Ashford works, in Lot No. 3257. The capacity of the full boiler set was 295T.

The complete set consists of two load carrying beams, two swivel cradles and four multi-axle bogies :- B902805 (outer), B902806 (inner), 902807 (inner), 902808 (outer). Vacuum brakes were fitted to the outer bogies only. When loaded, temporary brake pipes had to be attached along the load to maintain the continuity of the train braking system.

I briefly considered making the four bogies from scratch in styrene, but as the floor needed to be immediately above the axles, I could foresee problems with that. So I went into the spares box and extracted two Tri-ang R242 Trestles, and removed the bogies from those.

The moulded axle boxes and springs were ground-off to a smooth face. Three bogies required the headstocks grinding off level with the remainder of the bogie. Following this, the inside supports and swivel was completely cut out. The undulating shape at the top was then ground-down to shape. This work may sound tedious, but at the end you have a square bogie, and the axles will be at 90° - simple.

Pin point bearings were held in place with a dab of superglue. I lifted the middle ones slightly higher in the elongated wheel, which is designed to give the Tri-ang axles a little flexibility over rough track. After a few moments the Wizard wheels can be inserted. They do need to be inserted at this point, for when the new floor is in place it will not be possible to flex the side plates. Four floors are made from 1mm styrene, with recesses for the wheels and drilled at the centre for the swivel. A countersunk M2 bolt should be used and the nut dabbed with glue. Once over the centre axle there's no going back! As the floor has minimal gluing spots I used solvent, then made fillets with thick epoxy resin glue.

The side plates are capped, with additional strengthening over the centre axles. Cosmetic axleboxes, again from Wizard, are glued on, together with 9-leaf heavy-duty springs. These are cut from Comet castings, designed for the tender of a Class 2 loco. Couplings are refitted on bogies one and four.

Harold Smith



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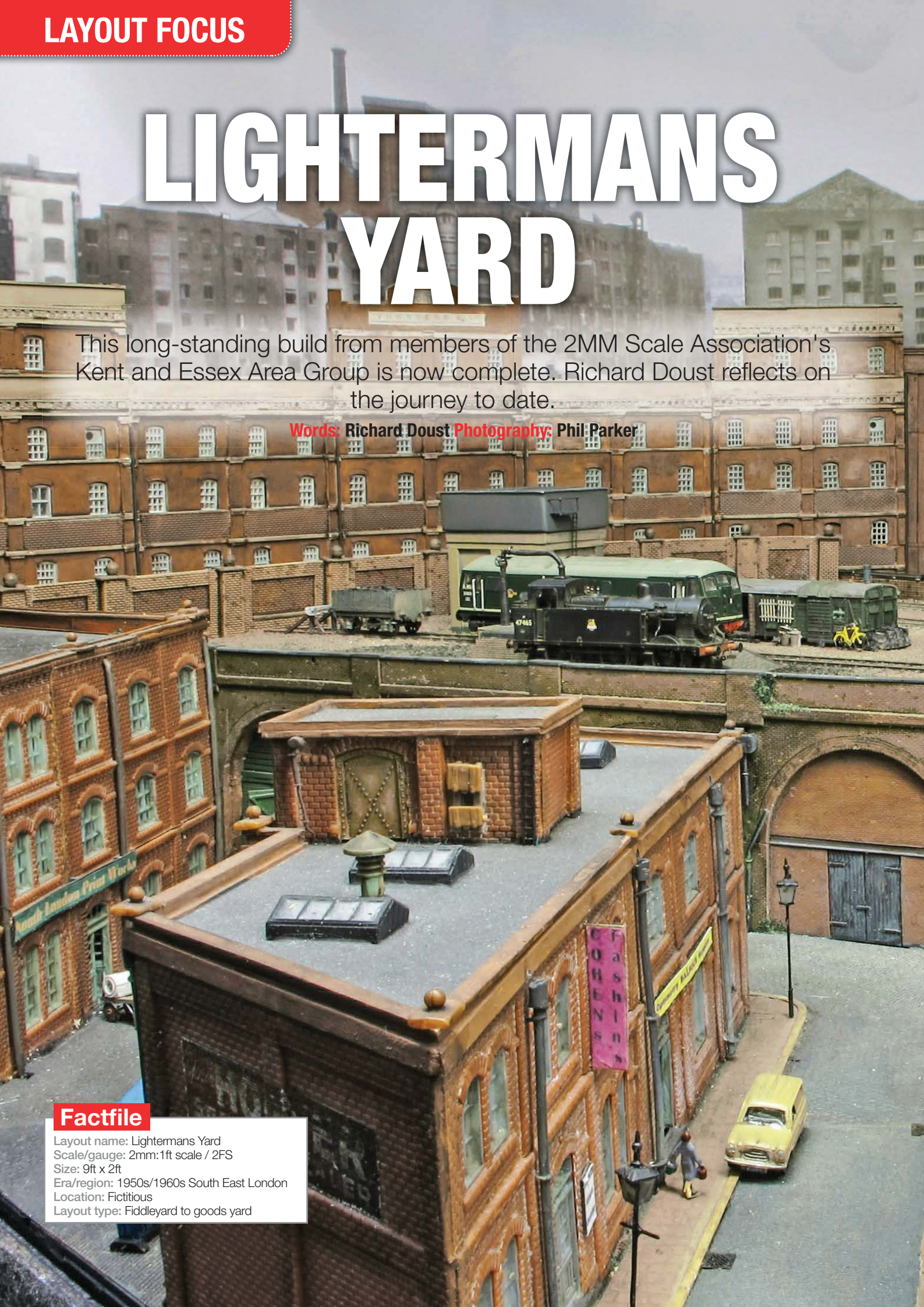
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LIGHTERMANS YARD

This long-standing build from members of the 2MM Scale Association's Kent and Essex Area Group is now complete. Richard Doust reflects on the journey to date.

Words: Richard Doust **Photography:** Phil Parker



Factfile

Layout name: Lightermans Yard
Scale/gauge: 2mm:1ft scale / 2FS
Size: 9ft x 2ft
Era/region: 1950s/1960s South East London
Location: Fictitious
Layout type: Fiddleyard to goods yard





The warehouse showing the cold store extension that was added when a disused railway, crossing over the yard entrance, was dismantled. The different colour brickwork and more modern stonework are evident.

In the early 1990s, a 2mm Scale Association group was formed by Andrew Cox and Arthur Adams in Swanley, Kent. The group became the South-East London Area Group (SELAG) of the 2mm Scale Association, and eventually became the Kent and Essex Area Group (KEAG) as we are now known.

In an effort to establish a group project, Andrew Cox suggested a layout to which everyone could contribute. The design chosen was based on an Iain Rice design, 'Cannonsgate Goods'. For some time this consisted of track boards and an ingenious cascade DC system of which Andrew was the master. After he retired from the group, the decision was made to rewire the layout to DCC, and conversion started in 2010.

Early members of Andrew's group who are also current mainstays of KEAG are Alan Smith, Keith Gloster and Pete King. Keith, Alan and David Smith have built or

modified most of the locomotive and rolling stock to 2mm Finescale standards.

Pete Townsend took over as co-ordinator of the Kent and Essex Group from Andrew Cox. 'Lightermans Yard' was just a set of baseboards with a central raised track bed, with beautiful trackwork constructed by Keith Gloster. These were surrounded by flat 'jigsaw' pieces of plywood that were destined to become the foundations for the scenic parts of the layout.

The layout represents a small fictitious goods yard, somewhere in South East London. Timescale is the late-1950s to early-1960s, near the end of steam, with an occasional diesel to be seen.

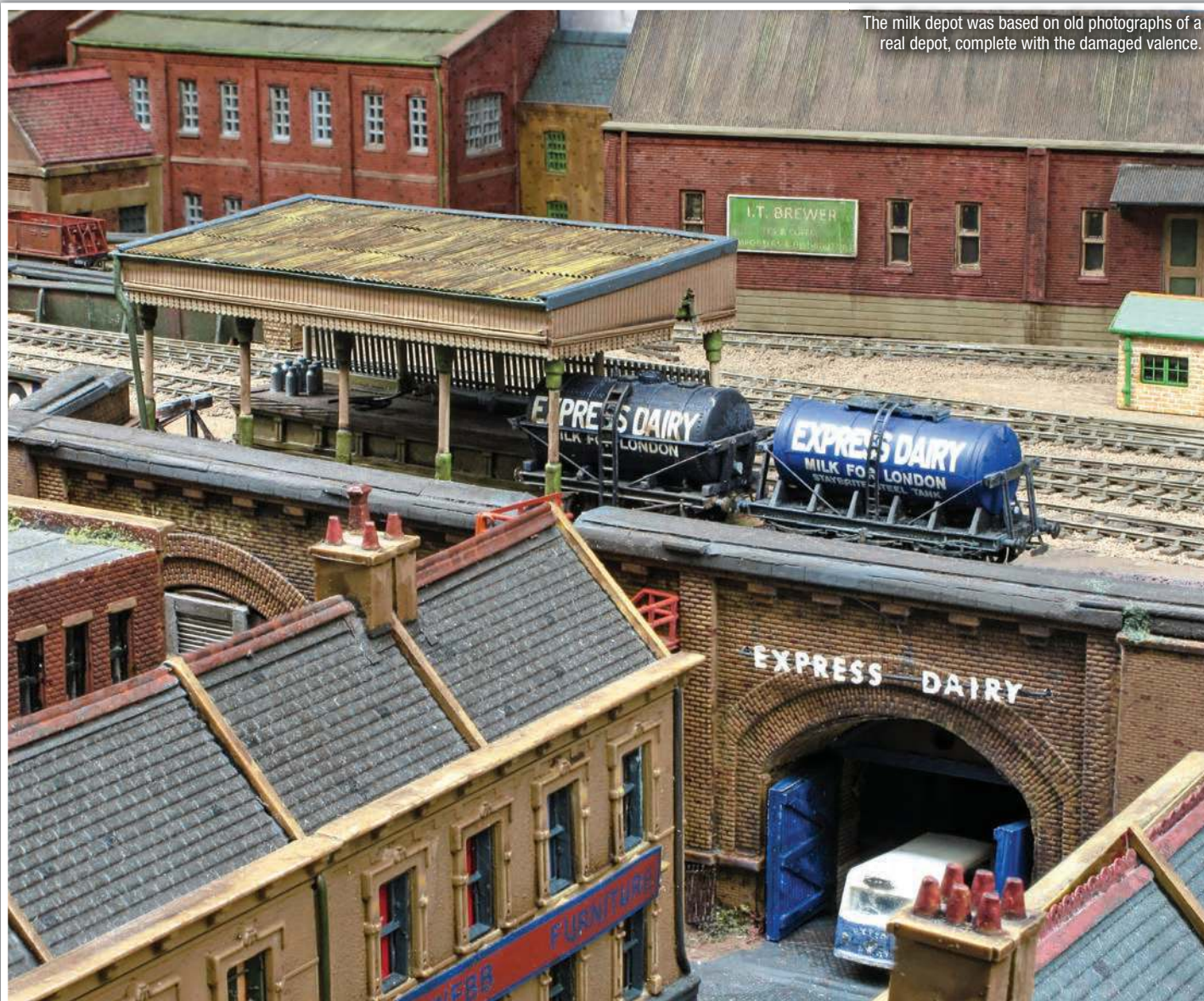
Building work

Following one of our monthly meetings, Pete took home one of the scenic boards. Though he had no experience of making buildings or any other type of scenery,

he had quite a collection of building kits from manufacturers like Walthers, DPM and Faller, which had been purchased many years before for a proposed N gauge layout that never materialised because he discovered 2mm Finescale. Pete started to look at these kits to see how they could be used and Anglicised. The price labels on the packets were from shops including Alan Brett-Cannon, Hadley Hobbies, Victors and Kings Cross Models. Some were even priced in £.s.d, so it shows how long he had them.

The result was that the following month Pete turned up at our meeting with the first board covered in buildings and other scenic details to gauge other members' reactions. Getting a favourable response, he then proceeded to take a board home each month, returning with a completed section each time until the scenic part of the layout was complete.

Most of the buildings have detailed



The milk depot was based on old photographs of a real depot, complete with the damaged valence.

interiors. The Lighterman's Arms, for example, has a detailed bar with bottles, glasses and pumps, tables and chairs – even a dartboard! No-one ever sees them, but we know they are there. All the buildings are a combination of bits from kits and scratch-building.

Pete also managed to find, at a local second-hand furniture shop, three large flight cases in which the scenic boards are stored. Two of these cases double as stands for operators of the layout, allowing them to see over the back scene.

Changing hands

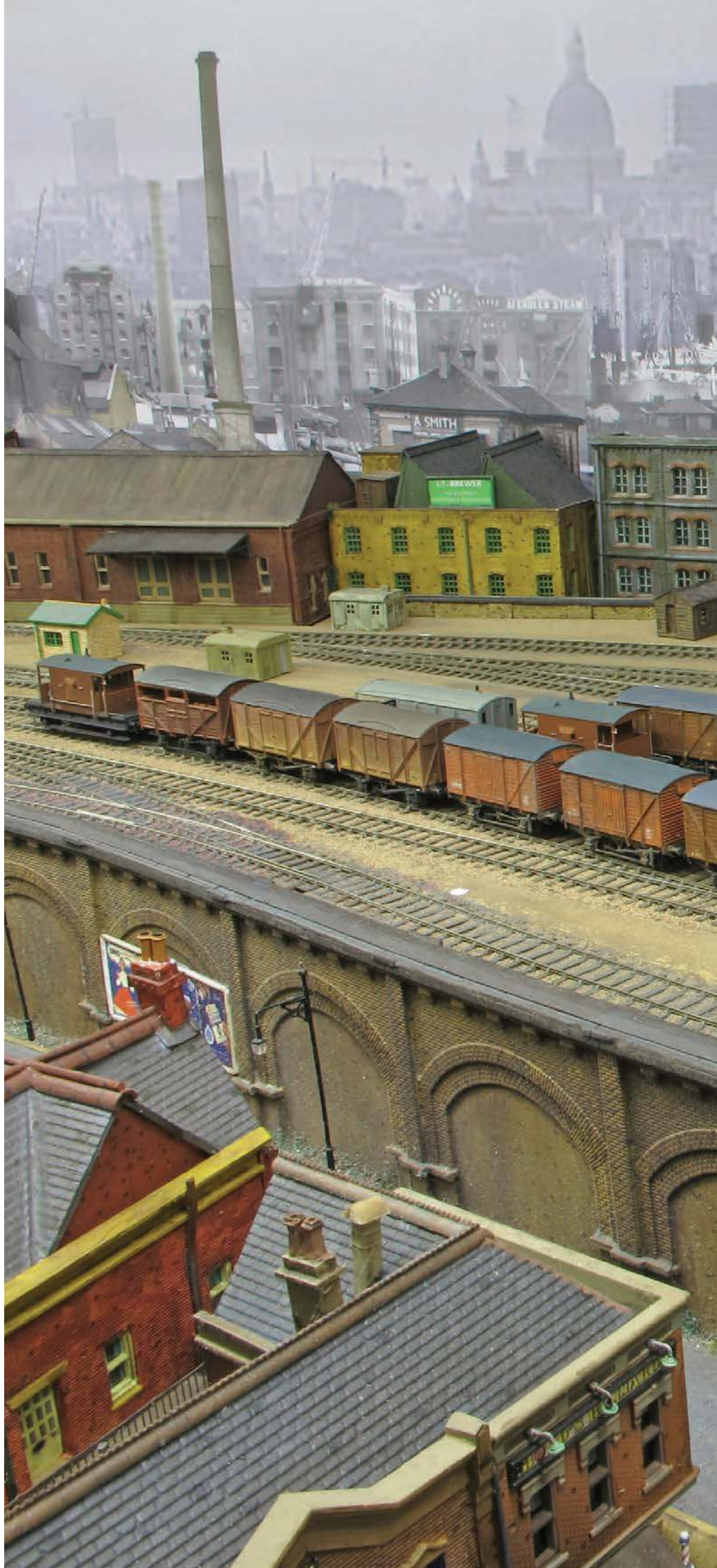
Pete's last contribution to the layout, after he had moved to Somerset and handed over the group reins to Pete King, was the large warehouse. The shell had been built by Alan Smith, but Pete then added the interior, platforms, all the extra details and lighting.

Pete King joined the 2mm Scale Association at the end of 1999. He was encouraged by Andrew Cox to come along to the local area group meeting, which by coincidence, was also his group. At the beginning of 2000, Pete took the plunge and was immediately made welcome by a small group of friendly faces all huddled around, as it turned out, the trackwork of 'Lightermans Yard'.

Although Pete was not involved in any of the planning, he understood that it was Andrew's idea in order to have a group project. The layout was just bare bones when Pete first saw it and it remained that way for many years. As has been mentioned previously, the trackwork had been wired by Andrew and, ingenious though it was, seemed to cause permanent confusion to everyone else!

Pete King's contribution, so far, has been helping with the rewiring to DCC, building a G6 locomotive and some wagons, making some small scenic items (bufferstops, foot crossing at the dairy) and some track ballasting. Otherwise he has carried out various repairs to the scenics (replacing missing chimney pots, etc.) He recently brought all of the rolling stock couplings to one common standard in order to improve that aspect of operation and is currently carrying out chassis and wheel replacements on some of our more elderly stock to improve its running.

I'm a relative newcomer to the scale and the group, and became involved with the scenic buildings with a scratch-built model of a curved warehouse for one end





A lot of the shops and industrial buildings around the yard bear the names of past and present members of our group. An industrial unit on the backscene can be seen as A. Smith and the low relief building to the right has the name Gloster Wharf.

of the end of the layout. Pete Townsend provided a master section for resin casting, modelled from drawings I had made of the former Decca factory at Queenstown Road, Battersea. The cast sections were modified for realism. My further contribution is the scenic background of London viewed from the South East, a photographic collage of typical London buildings on a long digital printout.

Exhibition ready

Since then, and with the collective support and contributions of the current KEAG members, 'Lightermans Yard' has developed into a working, fully scenic, exhibition layout.

The track is hand-built, using Code 40 rail soldered to PCB sleepers, with etched chairs. Stock is supplied by various group members, and is a mixture of RTR N gauge stock converted to 2FS and kit or scratch-built 2mm Finescale models.

The most recent addition to 'Lightermans Yard' is adding stay-alive units to the layout locomotives. This has proved a great advantage in working the layout.

In operation, trip locomotives, a mixture of steam and diesel, bring in a train from the fiddle yard, uncouple and retire to the locomotive yard. A shunter, or shunters, will then separate the train and distribute the vans and wagons via the yard feeds to the warehouse, and the brake vans to their road. The occasional milk train will move its tankers to the dairy siding. The yard locomotives will then assemble a train and deliver it to be picked up by a trip locomotive and taken off-scene.

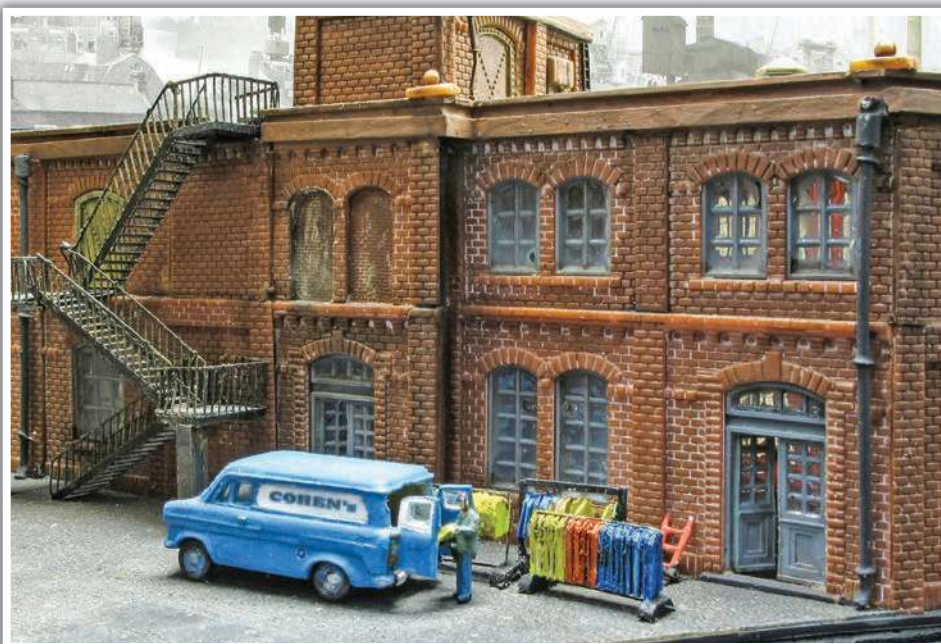
We always enjoy our outings to display 'Lightermans Yard' at exhibitions, where it has scored highly with visitors and other 2mm Association members.

The layout is far from finished, construction having occupied members of the KEAG for many years, and we think it still has some distance to go. As for a new layout, there is a strong feeling that that has already been started by Keith and Alan's Somerset and Dorset model of 'Evercreech Junction'!

See progress
of Lightermans
Yard at:
www.2mmkeag.blogspot.com



The headshunt end of the yard that also shows the curved low-relief warehouse.



The layout features many cameos of this kind, which audiences seem to find quite captivating.



In this scene men are unloading sheets of wood into the wood yard entrance. A peek round the back of this building reveals a circular saw for cutting wood to customers' requirements.



Looking across the busy scene to the locomotive yard above the occupied arches. This view also shows how the backscene fades into the distance.



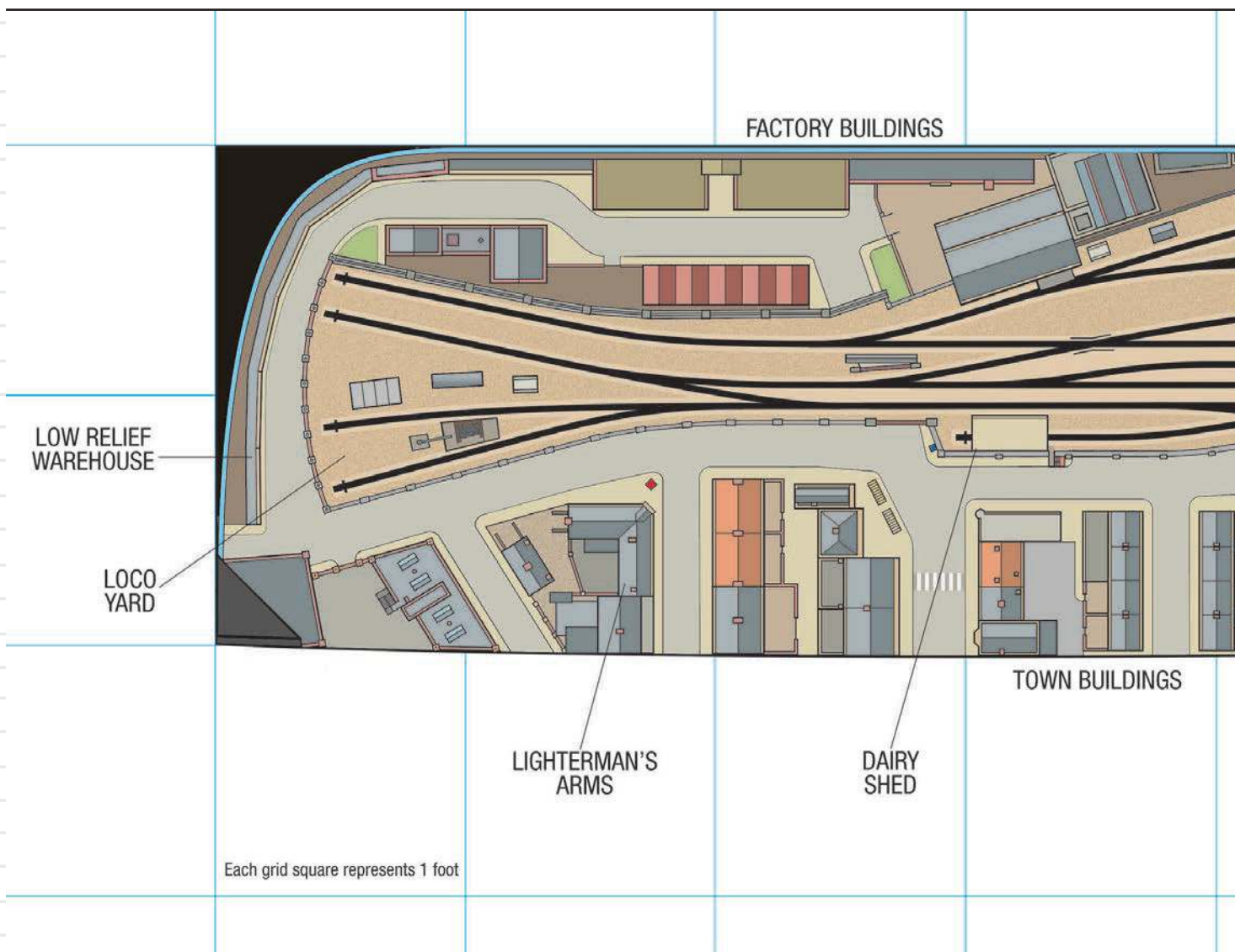
In late 1950s London there was still much evidence of WWII bomb damage. This building has had to be demolished, leaving the adjoining buildings supported by huge wooden props.



Never a moment's rest for the Lightermans Yard locomotives.

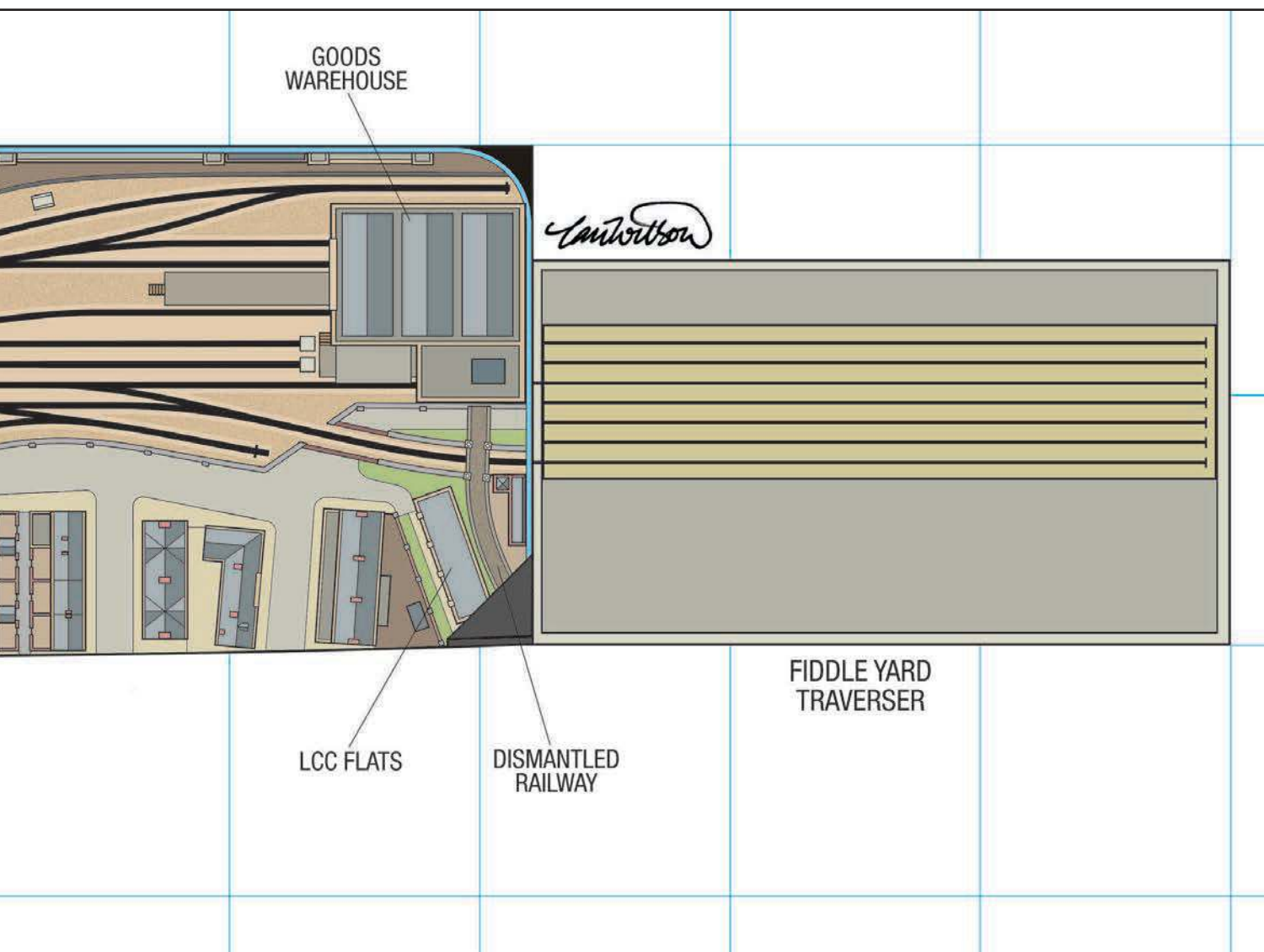
Trackplan

LIGHTERMAN'S YARD





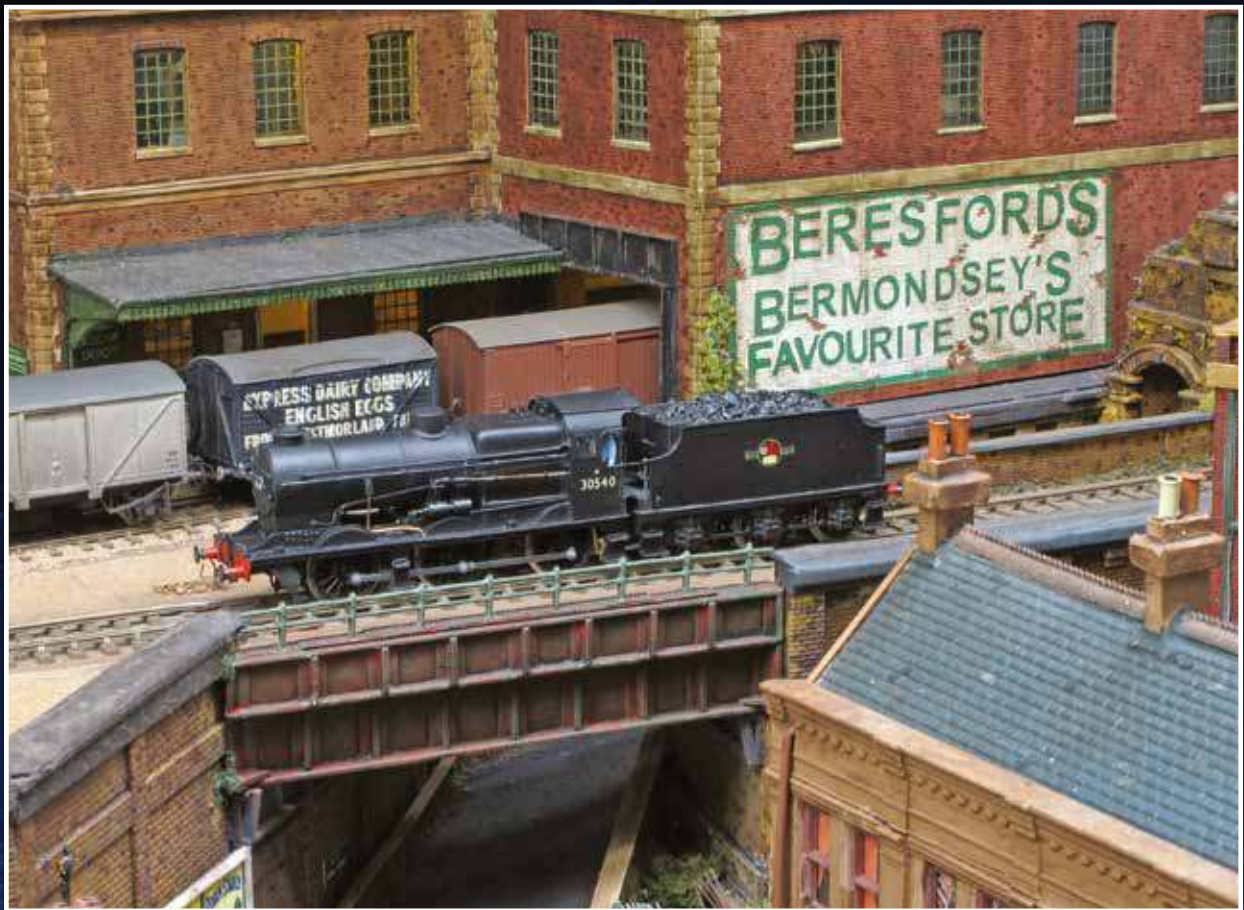
One of our challenges to younger viewers is to spot the pigeons. In this view one can be seen above the Daz poster.





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Layouts of London

As the 'Ally Pally Show' approaches, it's opportune to look at different styles to modelling London. After we compiled this feature 'Copenhagen Fields' was also voted 'Layout of the Year' by BRM and RMweb readers and you can come and see just why!

I swore I'd never live and work in London after my student years. Busy suburban services from Kent and a daily choice of the Circle Line from Bayswater or the Central Line from Queensway into the City were uncomfortable, somewhat odious at times and oppressive in the heat of summer. But it is one of the great cities of the world, of course.

We've chosen some of our favourite layouts from over the years that ooze atmosphere from different eras from the colourful world of pre-Grouping in smoky suburbs, through to the current day.

From the early days of the railway, the companies created their London termini to reflect their stature and wealth in ornate facades, all around the edge of the city but hardly within it. They're all huge, certainly in terms of being able to model such scenes, but it doesn't stop intrepid modellers doing so. As the city expanded, the stations became enveloped, and the lines themselves generated suburbs and sprawl.

It takes space to create the most impressive of scenes such as Kier Hardy's Hornsey Broadway layout, featured in this issue, but Rossiter Rise also proves that you

can create something that is very definitely London in a small space framed by canyons of blue brick retaining walls and the backs of multi-storey offices and industry. London scenes need height as much as they do length and depth.

Copenhagen Fields returns to The London Festival of Railway Modelling at Alexandra Palace on 21/22 March this year so there is no better opportunity to put our claim that this is the best London layout ever to the test. Rossiter Rise will be there, too, to inspire you that you don't need to model a whole panorama to get the London look.

See this layout in the July 2018 issue of **BRM**. Download today from www.pocketmags.com/BRM.



KENSINGTON ADDISON ROAD

The West London Line connecting Willesden and Battersea holds much operational appeal with its capacity to handle inter-regional workings through the eras. Twickenham MRC has opted for the early 1920s, featuring the variety and colour of the pre-Grouping era and is the only layout that I know of which features a prototypical elephant!

The same location inspired Worthing MRC to create a version in the early years of the 21st century with 'Loftus Road', which again majors on inter-regional workings within recent memory. The layout includes some wonderful everyday street scenes, all scratch-built and with a healthy dose of London traffic congestion.

See this layout in the August 2016 issue of **BRM**. Download today from www.pocketmags.com/BRM.



EARL'S COURT

This issue includes Terry Tew's latest layout in the form of the wonderful 'Rossiter Rise', which along with his previous layout, 'Earl's Court', shows us that it's perfectly feasible to create slices of unmistakable London atmosphere in a small space with both layouts being less than 6ft long. 'Earl's Court' is another interpretation of West London and the suburban workings. His stock rosters are always interesting with plenty of kit-built and adaptations to create lesser-known prototypes.



KINGS CROSS

Gainsborough Model Railway Society's grand O gauge layout recreates the glory days of the L&NER in 1200sq.ft from Kings Cross to Leeds. The jewel in the crown is the London Kings Cross terminus with its glazed arched overall roof. The platform end scene evokes memories of the 10.00am departure of the Flying Scotsman service. The model is open to the public several weekends of the year starting with Easter this year.



COPENHAGEN FIELDS

Surely there can be no grander project that is readily identifiable as London than The Model Railway Club's fantastic 'Copenhagen Fields'. Under Tim Watson's leadership, the layout features a wide panoramic landscape of North London through to the most minute details in street life. Add in the fact that virtually all the stock is scratch-built to exacting standards and runs on hand-built 2mm finescale track, and I think that this must take any prize for the best layout of a London scene.

See this layout in the March 2017 issue of **BRM**. Download today from www.pocketmags.com/BRM.



NORWOOD ROAD

Simon Paley's background in signalling in the rail industry shows in his South London scene, which includes the spread of a modern tram network alongside a suburban station with bi-directional running and a wealth of detail around the lines. Colourful modern liveries show the variety that is still there on the network today (if we choose to look up from our mobile 'phone screens).

See this layout in the July 2017 issue of **BRM**. Download today from www.pocketmags.com/BRM.

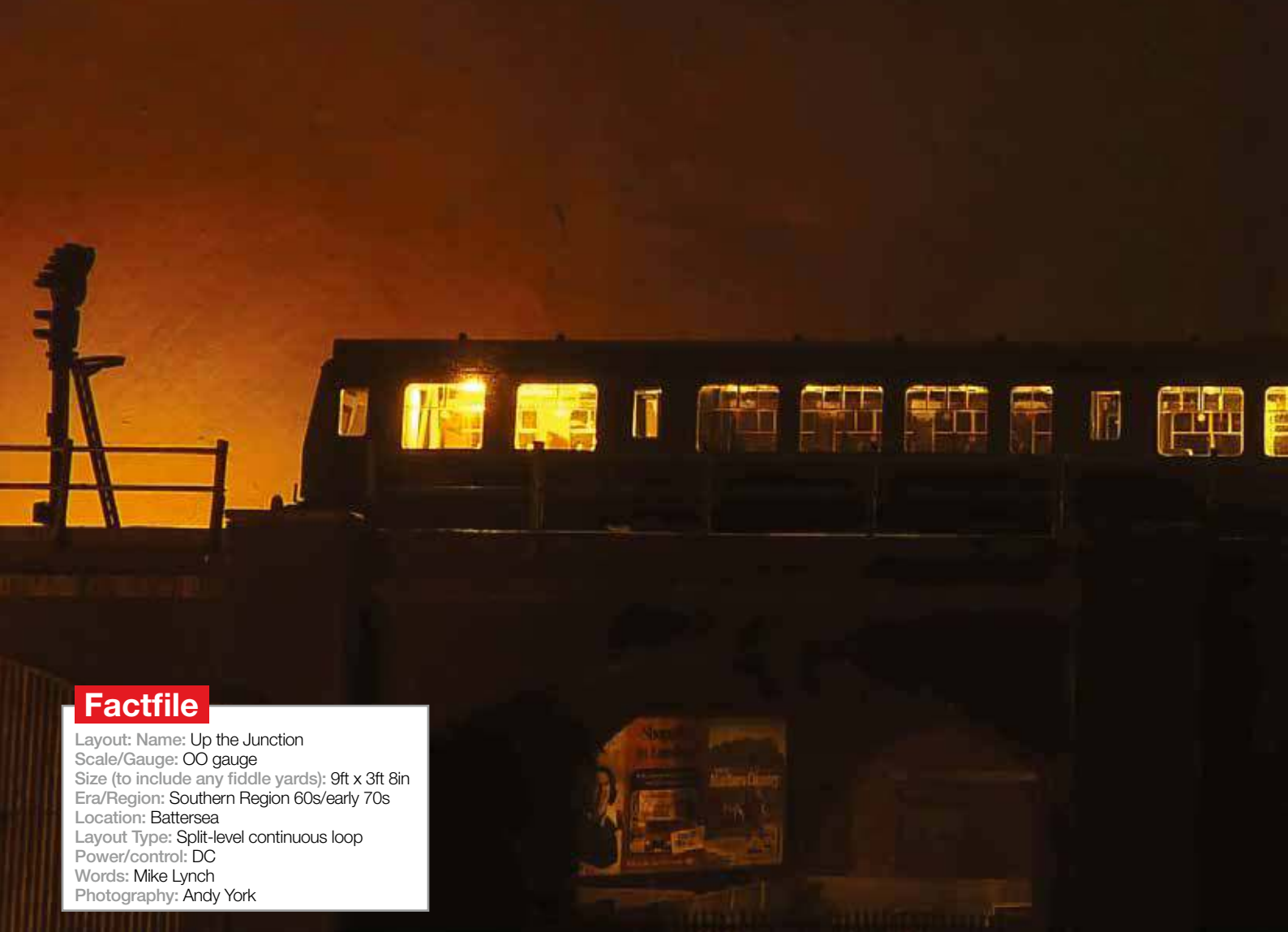


UP THE JUNCTION

Mike Lynch's project always delivers atmosphere, again in small spaces, and his South London slice of suburbia with elevated lines over brick arches housing dodgy car repair workshops, high rises and back streets you wouldn't want to walk at night in the early 1970s, provides it to perfection. Busy commuter services rattle over the arches behind a quiet permanent-way yard.

UP THE JUNCTION

Putting atmosphere at the forefront of his design, Mike Lynch's budget layout reflects his passion for the transitional era around the Southern Region.



Factfile

Layout Name: Up the Junction
Scale/Gauge: OO gauge
Size (to include any fiddle yards): 9ft x 3ft 8in
Era/Region: Southern Region 60s/early 70s
Location: Battersea
Layout Type: Split-level continuous loop
Power/control: DC
Words: Mike Lynch
Photography: Andy York

Being a fan of micro layouts it's rare to see any of the stock to do much more than move up and down a few feet at any time. There's a circle of third radius track to run in new trains, but after a few circuits things get more than a little boring.

Given that I'd always wanted to see them glide along a decent scene at a good lick, I decided to find a few spare feet of space to set up a modest run around. I don't like tight radius curves on layouts, so decided on a modest 4ft x 2ft urban section; the feeding loops being placed out of sight behind the

back scene. To offset the lack of running space and for operational interest, a small shunting yard was planned for the front of the layout with the main lines placed atop a viaduct behind.

GATHERING IDEAS

The BR transitional era setting would be loosely based on the area around Battersea, inspirational photos of which were found in David Larkin's brilliant series of British Rail Wagon books. This dictated a Southern theme, but because a Victorian railway

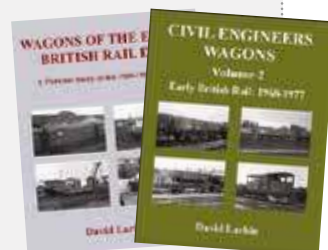
viaduct is something that is found in most cities the layout could be more generic. Therefore, many locomotives from many regions - including a beloved HST set - could run through it whenever desired. The 'Yard' was thus initially conceived and construction began in earnest.

MOOD SETTING

I'm more interested in atmosphere than accuracy; something that's reflected in most of my layouts. However, a scene still has to be convincing so there's a heavy reliance

Interested in wagons?

Discover more with the prototype images and informative captions in the pictorial survey series of wagon books by author David Larkin. Split into volumes by freight traffic type and covering the Big Four, British Railways and British Rail eras, there's a wealth of information to the modeller. Keep alongside your modelling bench when adding detail or weathering.



the layout reflects my fascination for the transition era. I get a great deal of fun mixing up coaching, wagon and train combinations; even though not entirely prototypical

LAYOUT FOCUS

on photorealistic textures and good quality card kits to help achieve this. Several Arched Bridge kits from Scalescenes were used for the main viaduct; joined together in an 'L' shape formation. The configuration neatly provided the scenic break for the lower yard. Scenic breaks are the bugbear of all micro layouts and it's always hard to disguise these ugly openings successfully. After careful planning, a freelance factory building, water tower and a Scalescenes signal box hid these to the left.

CHANGE OF PLAN

The flats are quite deep and I found myself adding an extra ten inches to accommodate them. This created a dilemma about what to do with the rest of the empty space at the rear. I'd already inserted a small scrapyards scene under the first two arches and had intended to place low-relief structures behind this, but this arrangement would jar with the tower block. I then had the bright idea to add an additional Scalescenes viaduct running from behind the water tower and along the back of the flats. The layout had now suddenly morphed into a junction cameo and allowed for three trains to run in addition to the shunting yard - plenty of scope for playing trains. The new arrangement also created a greater sense of depth for the operator and viewer.

A beloved HST emerges from the Scenic Break on the left-hand side and demonstrates that the layout can accommodate any BR period train from any region. It's unbelievable to think that the Class 43 is still running after 40 years faithful service. In a recent article celebrating the anniversary it was suggested that the Class has another 20 years useful service yet to come!



I'm more interested in atmosphere than accuracy. This night time running picture captures the orange glow of Sodium street lights in an urban setting. The illumination for the block of flats is provided by an ordinary light bulb – a much easier way to light a building instead of messing about with individual LEDs.



A Hornby Class 20 pairing about to shunt a Hornby Steam Crane; the crane has been detailed and given a new jib runner. These runners were often converted from old coach stock.



KEY DETAILS



The Warehouse

A freelance warehouse with a rail-served loading bay was constructed to the right. This was placed in front of the viaduct and, provided the necessary cover for the yard.

“the layout was extremely cheap and quick to construct. With money saved, maybe the financial controller will let me purchase that much desired Hornby Q1 to complete the scene



Hornby 2-HAL, Bachmann Class 416 and Lima Class 73 share the running loops on the layout.

Meet the Modeller



I grew up in Birmingham, about half a mile from Perry Barr Station, so trains are definitely in my blood. The BR corporate era was what I remember most, plus the many train trips to town at weekends with mum. New Street Station was one of the most exciting places to be and there was a great deal of pleasure had in watching AC electrics glide alongside various diesel locomotives - the blue exhaust smoke occasionally illuminated by sparks from the overhead wires.

Those visits left a powerful impression on me. However, whilst I modelled the ubiquitous Airfix kits during my youth, it wasn't until I lived in Ireland that I began to indulge a desire for model railways. Whilst my Southern scheme is an odd choice for a 'Brummie', I'm working on a micro layout for my collection of Hornby Class 86s - an attempt to recapture the magic of those childhood experiences.

STOCK

The BR Blue era stock is comprised of a Hornby 2-HAL, a Bachmann Class 416 and fabulous 4-CEP. In addition, a Heljan Class 33 and a detailed Lima Class 73 share the running lines. The motive power for the yard is a Bachmann Class 08 with the occasional appearance of a pair of Hornby Class 20s. With a simple change of advertising hoardings and correct period cars, the layout can accommodate stock from the early sixties and a Green Hornby 2-HAL runs alongside a Heljan Class 33 whilst a Heljan Class 16 shunts in the yard.



BEST OF TIMES

The layout reflects my fascination for the transition era. I grew up during the period and new tower blocks seemed to be rising up from the rubble of old Victorian areas nationwide. Britain was still emerging from the austerity period, yet was still leading the way with innovation in technology and industry seemed to continually turn out stunning engineering firsts - the development of the HST being just one example. It felt like a much more hopeful time and the future looked brighter somehow. But maybe I'm being a little too nostalgic.

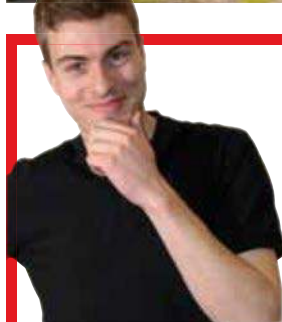
KEY DETAILS



The flats

Behind the arches, to take care of empty space, a block of flats was used. This was sourced from Kingsway Models who carry a fantastic array of card buildings. The kit is brilliantly proportioned, although a little bland, and provided me with a bit of scope for detailing.

The outside was sprinkled with fine sand to convey a concrete finish; this was glued down with a watery mix of PVA. I also wanted the flats to light up during night time running, so the window openings were cut out and interior details created using various Scalescenes textures. Holes, of various diameters, were cut into the back of the interior walls for lighting. The light source was provided by an ordinary light bulb, sealed inside a box, and affixed to the back of the interior wall section. It's a method that saves a lot of time, expense and messing about with LED circuits.



Howard says...

Look at many layouts that grab your attention and see what sets them apart from the rest. Is it the detail? The authenticity? The atmosphere? Mike's layout uses the latter to its best advantage and he's successfully built a layout to a budget by making the most of printed card kits. The Southern Region is captured with railway arches and high-rise flats, whilst a nod to an extinct era of the railways is provided with the disused goods shed and water tower.

BRM TV

BRITISH RAILWAY MODELLING

DIGITAL EDITION EXCLUSIVE

UP THE JUNCTION

Grab a three-minute slice of South London suburbia on Mike Lynch 's busy commuter service, complete with brick arches housing dodgy car repair workshops, high rises and gloomy back streets.

EXCLUSIVE

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



SPOTLIGHT



GET THE LONDON LOOK

You can use more than locomotives or rolling stock to define a region or period of time on your layout, as Howard Smith explains.

Words: Howard Smith

A busy 1960s scene on London's Oxford Street outside the Selfridges store. Red Routemaster buses and BMC vehicles as far as the eye can see...

Unless you're modelling fiction, the setting of a layout's location is important. Even if loosely inspired by a line or region, beyond the architecture, accuracy is often in the small details. Such attention to minutiae helps place the viewer in a time warp, leading to a more convincing scene.

Everything should be considered, from road signs to theatre shows, road vehicles – even the costumes being worn by the figures on your layout. These can open avenues for 'would-be' conversations around your layout, perhaps between two figures over a fence, or scenarios – a delivery of milk or the bins being collected, for instance.

In this Capital-themed issue, I've compiled this list of objects that could be set around a layout depicting a slice of London. Creating an unmistakable atmosphere with the range of die-cast or white-metal kits available has never been easier.

BLACK CAB CAMEO

If the 'big red bus' has a rival, surely it's the black cab? Though widespread throughout cities nationwide, the black cab is most renowned for roaming the streets of the Capital. Having undergone a few iterations, the most familiar of design to have roamed our streets is the Austin FX4. Produced for 43 years, from 1958, first under the BMC name, then British Leyland and finally Carbodies/LTI, the long-lived icon has cemented itself as part of London's furniture. Fitting under the Hornby SkaleAutos umbrella and produced from tooling by Oxford Diecast, this FX4 has a distinctive side wrap for a newspaper. In more recent times, cabs have been painted all manner of colours.



My vision: Taxis look better in numbers – why not create your own taxi rank and introduce variation? Given that the taxi sign is in clear plastic, this could easily be back-lit on some models, where others could be placed on a timer. Advertising can be customised to your taste, though consider adding passengers and drivers for realism. Cut the odd window out too, if you're feeling adventurous.

FIGURES

You can set the tone of a neighbourhood quickly just by selecting characters that reflect its settings. Is it an area of trade with small shops and shoppers, or a local area of industry where workers cycle to clock on and off shift? Perhaps even an affluent area of banking? The figures you choose and the way you present them can easily set the tone of a model. Figures are available from many suppliers, but some of my favourites are those manufactured by Modelu.

My vision: This selection of businessmen could be placed outside a tall building in a variety of poses, some entering, others exiting the premises.



RED PHONE BOX



Dotted around the country, red phone boxes are in rapid decline, though in the most popular era for our models between 1950 and 1970, they were commonplace. Finding models of these isn't difficult, with examples available from Modelscene, P&D Marsh Models, Peco, Hornby Skaledale and Bachmann Scenecraft to name a few. Simply visit your local model shop and I can almost guarantee you'll find one off the shelf. Ensure you choose the right box, however – designs have changed subtly over the years, the most radical being the KX series. They are ideal for placing on a bare stretch of pavement.

My vision: Phone boxes can be used to tell a story – perhaps a family of tourists is making a long-distance call, or a car has broken down and the driver is calling for recovery? Is a delivery driver lost and phoning the depot? Or could a private investigator in a long mac be using it to hide undercover?



COBBLES

Before the turn of the last century, many of London's streets weren't paved, but laid with cobbles – even well into the middle of the 20th Century. If your layout is set in the pre-Grouping era, there's a fair chance that a lot of the backstreets of your layout would be cobbled, too. There are a few methods of achieving this, from scribing plaster, or using ready-made printed card sheets to injection-moulded plastic ones, or a personal favourite, self-adhesive textured sheets from Redutex. Cutting them to shape is simple and they can be layered on flat surfaces, walls or roofs.

BUS CAMEO

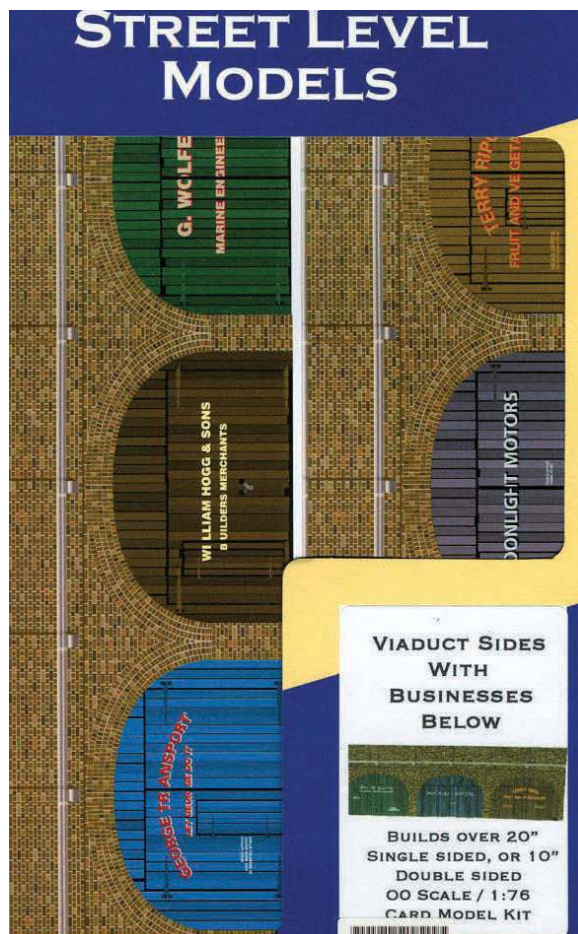
Corgi has a vast catalogue of model buses. Nothing quite resonates with the streets of London as a giant red bus. From early 'Routemasters' to the two Corgi buses pictured, a Volvo from Blue Triangle Buses Ltd in the colours of East London Transit with 'Go Ahead London' branding, or the New Routemaster 'Boris bus' with Coca Cola branding, both are ideal for post-2010 London layouts. For extra authenticity, take note of routes and branding applied to these. Would such a bus appear near your location? Print-at-home water-slide transfers can be used to adapt either to suit, if required.

My vision: The bus is at a stop, passengers disembark with a queue ready to alight. Doors have been cut and glued in their open position. The front axle is bent to pose the wheels as 'turned'. Flashing LED indicators inform traffic that it has stopped. Inside, disassembly of the model has seen the fitting of figures, some standing downstairs, or sitting reading newspapers, others heads down reading.



UNDER ARCH VIADUCTS

Space is at a premium in the Capital. Valuable space underneath viaducts has a history of being used to house businesses. From garages, to timber merchants, joiners, used tyres sales, or scrap merchants, options for the modeller are varied. Timber fronts, steel fronts or roller shutter doors can be used to create the odd-ball effect of these self-developed spaces on the fringes of towns and cities. Kits are available from KS Laser Designs and Street Level Hobbies among others, with ready-to-plant options from Bachmann and Hornby.



HOUSE UPGRADES

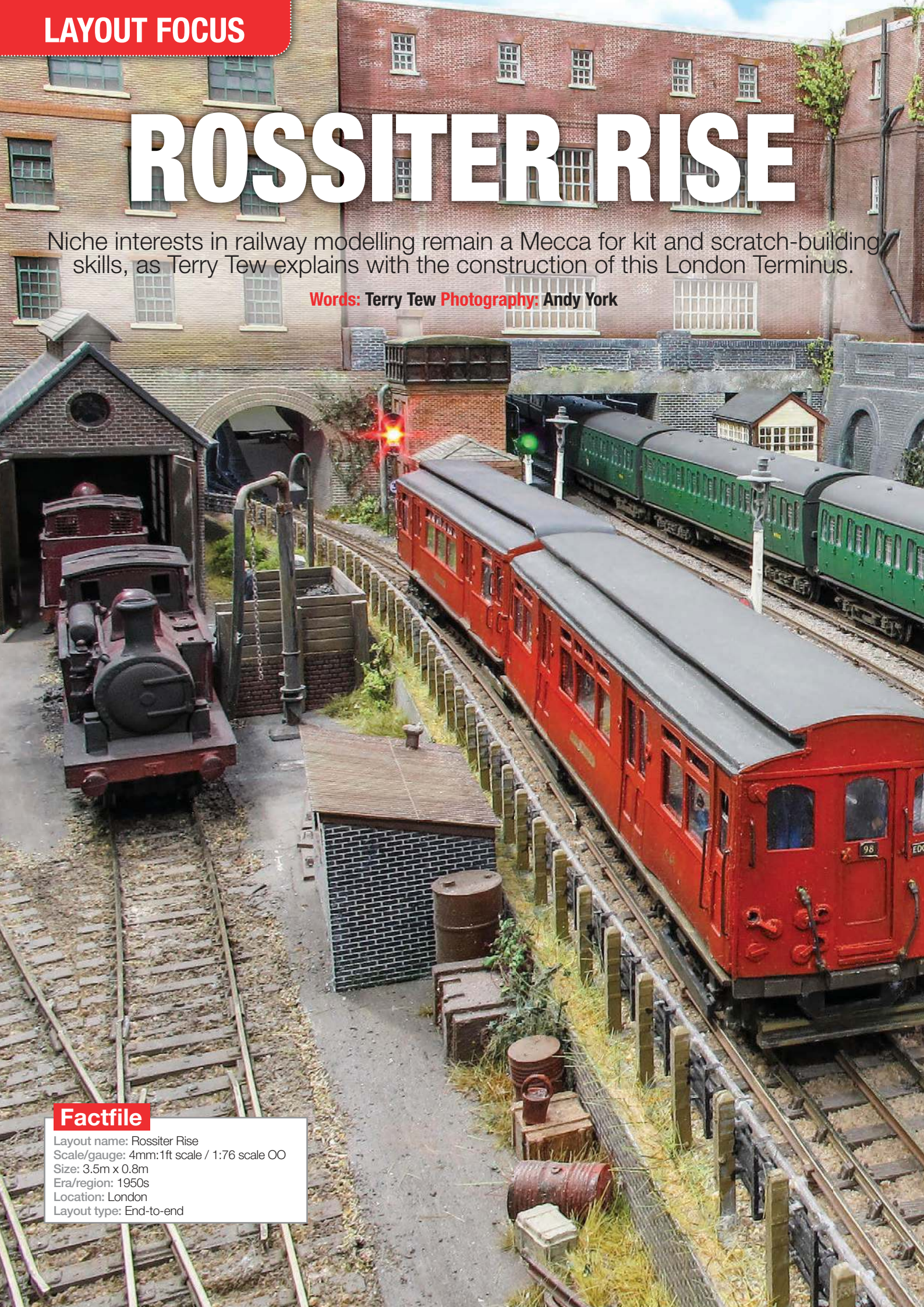
Nothing quite shouts desirable neighbourhood like a bold front entrance, with steps leading to a gloss black front door, sandwiched by tall white-painted columns. Tall town house kits can be sought from a variety of suppliers, Metcalfe and Fallor to name just two, but to upgrade the entrances you'll need to look further afield. Scale-link manufactures a number of small upgrade kits that can be used in conjunction with town house buildings to add an impressive entrance way, from etched railings, to pillars or pilasters (canopies).



ROSSITER RISE

Niche interests in railway modelling remain a Mecca for kit and scratch-building skills, as Terry Tew explains with the construction of this London Terminus.

Words: Terry Tew **Photography:** Andy York



Factfile

Layout name: Rossiter Rise
Scale/gauge: 4mm:1ft scale / 1:76 scale OO
Size: 3.5m x 0.8m
Era/region: 1950s
Location: London
Layout type: End-to-end



General view of the station showing a two-car 'Q' Stock train in the LT bay while a BR Eastleigh three-car EMU (later Class 501) arrives on the through lines. A few LT locomotives simmer outside the steam shed.

Rossiter Rise is a fictional location somewhere in West/Northwest London and is set in the mid/late-1950s. It was conceived mainly as a means of running and exhibiting my ever-increasing collection of London Underground stock.

I constructed the baseboards myself using a mixture of various thickness of ply and MDF. The three sections sit on square metal tubes supported by three tressles. The centre main scenic section is 136cm x 78cm and created in the cameo style, being fully enclosed and including a lighting hood. The left-hand board is 90cm in length and incorporates a traverser/fiddle yard. This is visible to the public and is operated from the front. The board to the right is 122cm long and hides a fiddle yard.

To the front of the scenic section is a small London Transport Depot. This is loosely based on several locations including Drayton Park on the Northern City Line. The depot incorporates a two-road depot/shed, which has been constructed from a modified Bachmann Scenecraft item. There is also a single road steam shed, made from a plastic kit, with my own resin roof sections added to it.

Behind this, and at a slightly higher level, is a single line London Transport (LT) bay platform. Continuing towards the back of the layout are two through platforms. These are 'electrified' using the four-rail system common to LT and the ex-LNWR/LMS North London Lines.

Finally, there is a single road non-electrified through bay. The trackwork is a mixture of Peco Bullhead and C&L Finescale.

Building work

The low-relief buildings to the left of the layout are scratch-built using brick-embossed styrene sheets and (mainly) windows from the American 'Tichy Train Group'. One of these is modelled in a semi-derelict condition.

The over-bridge station building to the right of the layout is constructed from Auhagen parts with a scratch-built roof. This is inspired by the station at Primrose Hill, close to Euston.

The terrace along the rear of the layout is scratch-built and is based on the houses in Rillington Place, where the notorious 'Christie' murders took place. These are typical of many properties in London and I was able to use stills from the (colour) film as a guide.

They are made from resin, including



ACV/BUT three-car set stands in the bay platform. These vehicles were synonymous with the Watford Junction-St Alban's Abbey and Harrow & Wealdstone-Belmont branches.



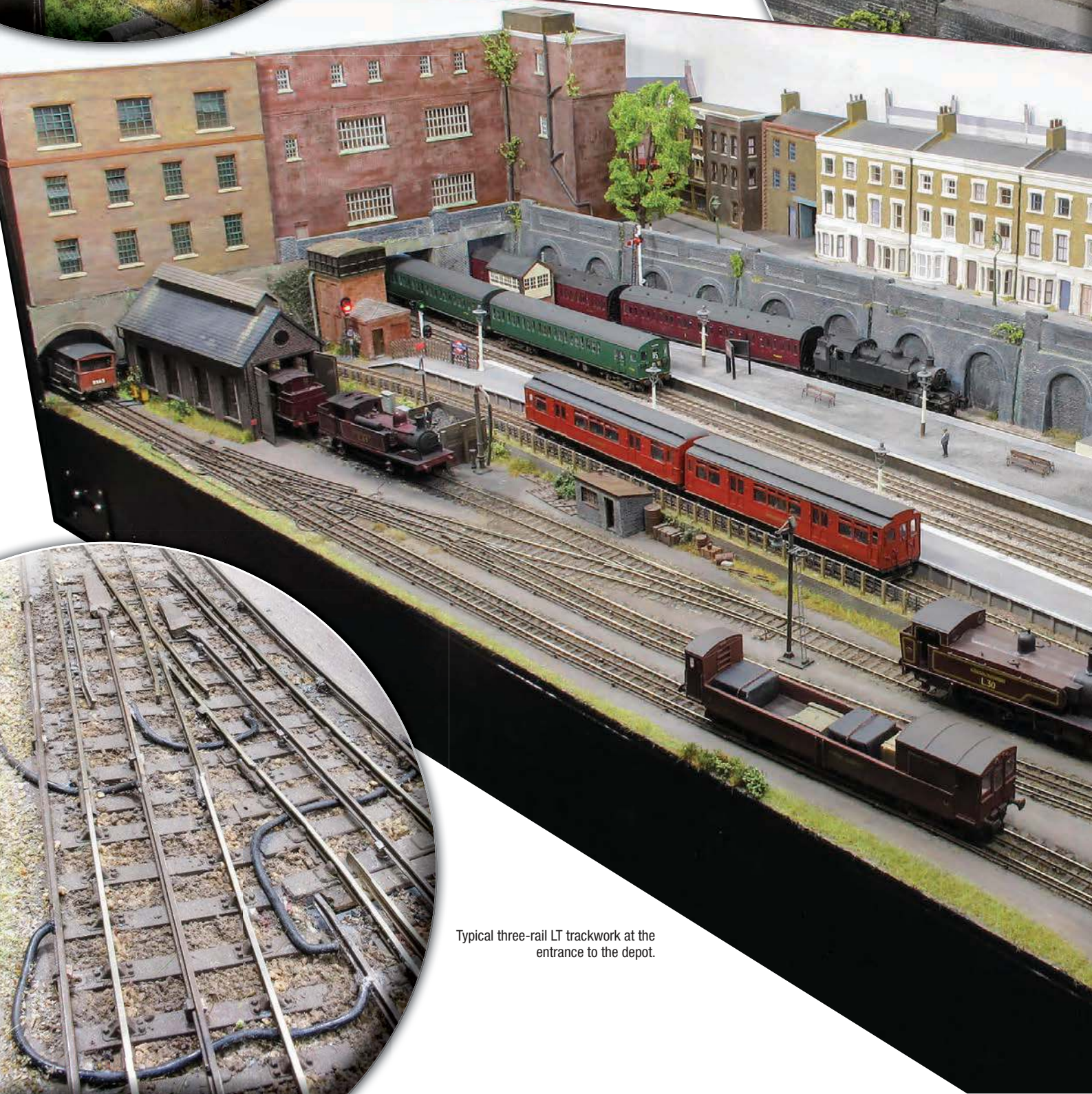
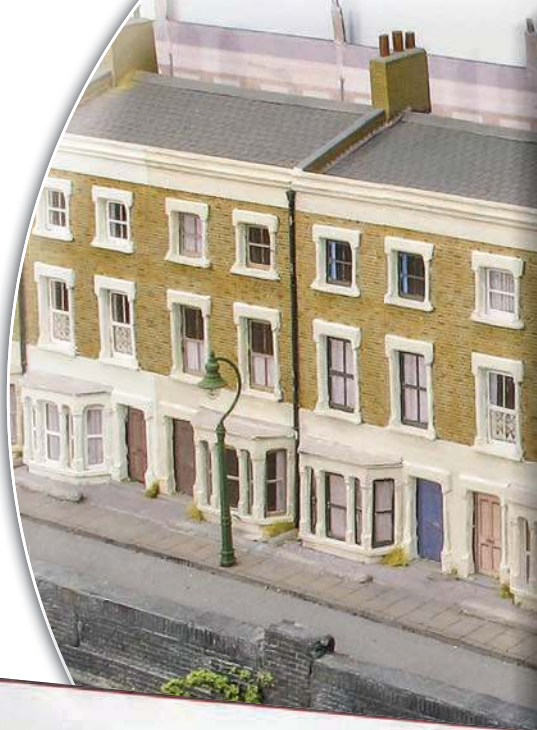
A two-car 'Q' Stock set waits in the LT bay platform. There is much activity in the depot, including kit-built 0-6-2T L.49 and L.52, Radley Models L.9 and 3D-printed L.30.

An ex-LNWR Oerlikon set arrives at 'Rossiter Rise'.





Working signals. To the left is a 3D-printed LT signal. To the right, a scratch-built ex-LMS searchlight, which were unique to the North West London suburban services.



Typical three-rail LT trackwork at the entrance to the depot.



The row of terraced houses were scratch-built using my own resin castings and are based on similar properties that once stood in Rillington Place, where the infamous 'Christie' murders took place.



the roofs and fire-breaks. The buildings to the left of the terrace are constructed from modified American kits. The block of flats to the right are from the 'Skytrex' range.

Incorporated into this back scenic section are a couple of examples of forced perspective. One of the houses has the front door open, giving an internal view. There is also an opening giving an internal view of a metal fabricating company. The illusion of depth has been created by adding an extension box behind the rear of the layout, which incorporates diminishing size profile and decoration, plus a smaller scale photograph as the final thing the eye sees.

The station building uses sections from the 'LNWR Grand Junction Components', plus a scratch-built roof. The roof and valance are modelled on Belmont Station on the ex-LMS Stanmore branch. Also seen on the layout is a signal box and small permanent way shed, which are both etched brass kits from the 'Severn Models' range.

Daily operation

There are three working signals. A Dapol semaphore, a London Transport colour light and an ex-LMS Searchlight. The LT signal is 3D-printed and the ex-LMS scratch-built.

Operation can be undertaken by a single person but is optimised when two people are working in tandem. One works from the front to control the LT depot and bay. The second works from behind and controls the other movements. An NEC Powercab and slave unit is used. The aim is to always keep things moving.

I would like to thank Tony Dean and Harry Pollard for their input and for assistance with running the layout. Last, but certainly not least, my partner Linda for her support, patience and encouragement.

The station building is typical LNWR. The roof and valance is scratch-built and based on Belmont in the London suburbs.



ROLLING STOCK

Stock used is varied with the majority being non-RTR.

LT DEPOT

F class 0-6-2. SE Finecast body on Bachmann chassis.

Ex District Hunslet 0-6-0 tank. 3D printed body on a Bachmann chassis.

Bachmann LT pannier.

Heljan Met Bo-Bo loco.

L.9 Electric (Used for transporting items between depots). Radley Models.

Two Battery Locos. Ex Harrow Model Shop whitmetal kits.

Ballast Motor Driver. Detailed Radley Models kit.

Plus a large number of scratch-built, kit-bashed and kit-built engineering wagons and brake vans.

Also to be seen is stock owned by my erstwhile co-operators Tony Dean and Harry Pollard, including E & F Class locos, Panniers, T Stock, etc.

LT BAY

2/3 car Standard Tube Stock set. Radley kits motorised using a much-modified 'Underground Ernie' chassis.

2 car Q Stock set. Modified and detailed Radley kits, including my own styrene overlays, which produce prototypical flush glazing.

Also 'in the pipeline' is a 3 car T Stock set and a 2/3 car H Stock (ex District C/D Stock) set.

Although not in evidence much these days 'Short Workings' were once very common on the London Underground. During the 1940s and 1950s, short formations were used on such services as The Acton Shuttle, Edgware Road to Addison Road (Kensington Olympia), The Northern City Line, The Aldwych Branch, Chesham Branch etc. etc.

The models used on Rossiter Rise typically reflect such services.

THROUGH PLATFORMS

Ex LNWR Oerlikan set. 3D printed bodies on various chassis.

Ex LMS GEC Stock set. Cut & Shut Dapol suburban coaches powered by a Replica Railways' chassis.

BR 1957 (later class 501) set. Converted from a Bachmann 2-EPB.

Through Bay platform

Bachmann Ivatt 2-6-2T in push/pull mode with two converted Dapol coaches, one being a driving trailer.

BUT/ACV 3 car 'Flying Brick' set. 3D printed bodies driver by a Heljan W&M railbus chassis. These bodies, as well as the Oerlikon set, are designed by Simon Dawson and available via Shapeways.

Also appearing on this track is a selection of freight and light engines, which can include almost anything from the era/area modelled.

The majority of stock is DCC sound-fitted.





Ex-Metropolitan District Railway Hunslet 0-6-0T, a battery locomotive and a bogie flat wagon rest outside the depot.



Ballast Motor Car L.64, portrayed in the earlier engineering grey livery, is shunting an ex-Metropolitan brake van and a sump wagon. Both were scratch-built.



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HOW TO... **OPTIMISE YOUR LAYOUT LIGHTING**

Words & photography: Giles Favell

Theatrical consultant, Giles Favell, shares his years of experience in set and layout design to bring you a masterclass in the art of painting with light.



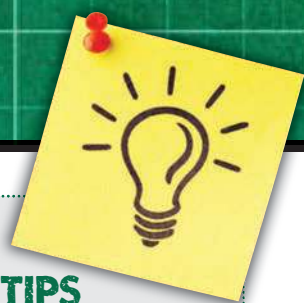
I have spent more than 40 years working in and for theatres, first in Stage Management and then Production Management, before becoming a Stage Engineering Designer and Theatre Consultant – designing the theatres themselves, rather than the shows that perform in them. In my earlier years I lit a few shows (which involves the drawing-board design, and then the rigging, focusing and plotting of shows) and re-lit many more! However, suffice to say that my career is not based on stage lighting, but on practical theatre production.

Both in theatre and in model railways we strive to create a realistic, plausible world – one full-size, and the other to a much smaller scale, but both intended to draw us in, and free us from the distraction of the ‘real’ world.

One of the most important aids to a good theatre performance is lighting. A wonderful set can look flat, dull and drab if poorly lit, and indeed a fairly average set can be brought to life in an enchanting way by very good lighting. If you can’t see an actor’s face clearly, you will find it much more difficult to hear or understand what he/she is saying (odd, but true) – and that means good lighting and a good controlled environment wherever possible.

In the same way, a layout can be visually helped by good lighting – especially at an exhibition, but also at home.

With a model railway, we should consider what we are trying to achieve. Most often, we are presenting an ‘out-door scene’ in ‘daylight’ to be viewed from the front and slightly above. Distractions serve to weaken or break the illusions that we are striving to create. In order to have as few distractions as possible, many of us provide a back-scene, (the equivalent of a theatre’s back-cloth or Cyclorama), which provides a sky or distance, but more importantly, hides the view of inappropriate real-life that would otherwise be seen. Similarly, a nice fascia is often provided to smarten the layout front in the same way. I advocate as high a back-scene as is possible. I use at present a separate cloth, about 150cm high, set behind the layout by about 30cm (easy at exhibition – not so easy at home!). This is matt front-projection cloth, printed with a photo image of gloomy cloud cover, and this allows the eyes to concentrate lower on the layout, where I want them.



DIRECTIONAL LIGHTING

Aside from the colour, we have to consider the beam angle and positions of lighting. Generally, if you walk outside, light coverage is even, unless you go into shadow – and that is what we should try to emulate. When lighting in theatre, we have someone ‘walk’ and they have to move everywhere on stage, always facing front, while the Lighting Designer is watching carefully to make sure there isn’t any shadow or change of light as they move around. This is advisable for a layout as well, using a hand close to the track as a substitute ‘walker’.

The text-book position for a light is 45 degrees to the side and 45 degrees in the vertical plane – and one from each direction. This enables the good illumination of the actor, whilst also ‘sculpting’ the face or whatever is being lit.

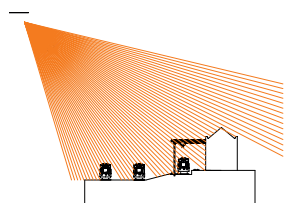
All too often we see extremely nice layouts that are poorly lit towards the front, because the only available lighting is from above – as a result of rigging difficulties – which does nothing to light the sides or face of the object we are viewing. Although top lighting and side lighting can add dimension to the picture, they are no substitute for a bit of front ‘fill’ lighting. 45 degree front lighting is a good compromise, and gives depth and character, while showing detail.

In theatre practice, all these directions are used together, and carefully mixed to create the vision that one sees on stage. Although one cannot rigidly adhere to this when lighting a model railway, it is as well to be aware of the principles when making your decisions.



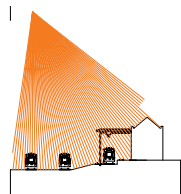
TOP TIPS

- Use the same light for your modelling (especially painting!) as you will use to light your layout. Different lights make the same object look different...
- Ensure shadows always come from the same direction.
- Season, time of day and weather will all dictate your lighting – if using different coloured LEDs, have them on dimmers so you can fine-tune or change your lighting.
- Daylight comes at us from all directions – even on a sunny day. Light is reflected from impurities in the atmosphere, the ground, or structures. The sunnier the day, the more uni-directional the light is.
- ‘Specials’ - lights that are focused to shine through doors and windows - provide shafts of sunlight or moonlight... These add to the mix, enhancing the layout.
- Paint any visible ‘off-stage’ areas black, or a very dark colour – it makes the intended focus much clearer.



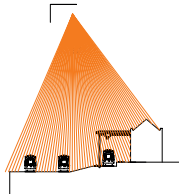
(A)

Drawing A shows how the face viewed is illuminated by use of a forward lighting position.



(B)

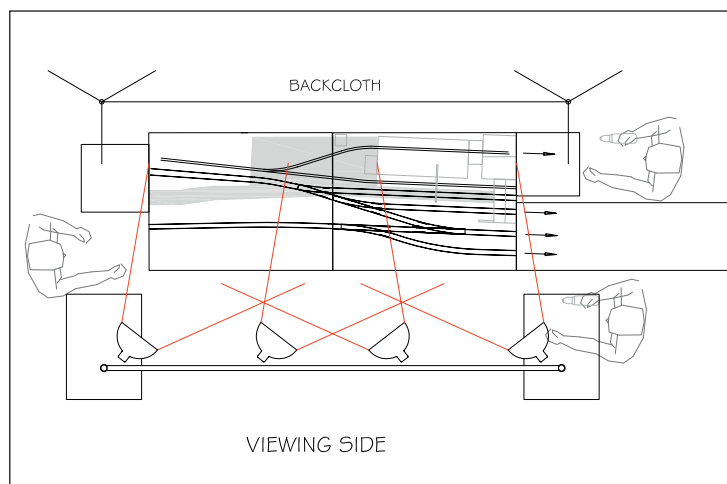
Drawing B shows how stock and scenery at the front of the layout is top-lit, leading to shadows on the vertical faces. Scenery further back is better lit.



(C)

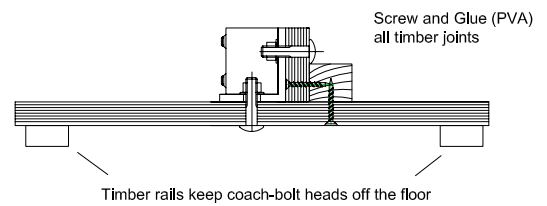
Drawing C shows everything in the front on the layout as being back-lit – fine when combined with front light, but not of itself good lighting.

My preference for lighting a layout would be individual incandescent lamps, reproducing the 45-degree principle, with some ‘key’ light from one side or behind to fill and tell the story. However, this is not practical at any size because of power, heat and available positions. When deciding on the lighting for ‘Denton Brook’, I chose to use photographer’s lamps, fitted with 35W 5500K compact fluorescents, and mounted in aluminium dome reflectors from Ebay (at £10 each). This enables each lamp to be focused at roughly 45 degrees in both planes, and the lamps have quite sufficient spread (or beam angle) to ensure there was even coverage without dark patches. This is quite a lot of light to throw at a layout (by traditional standards) but I’ve not had any complaints so far.



LIGHT RIGGING

To rig these in front of the layout, I bought a 3m 'Disco' lighting truss with stands from Ebay (about £90 at the time). The truss was good and robust, and only needed metal tags welding, to mount the lamps. Without welding, I would have bought and fitted 38mm 'P' Clips, using a longer bolt. Rubber-lined stainless steel types are available from Amazon and Ebay as well as many other suppliers. The only other change to the stands for the truss I made was to remove the three splayed legs, and instead used a socketed base. This was made from steel square hollow section, welded to angle, which in turn was bolted to an 18mm plywood base to create a 'boom base' or 'tank trap'. A boom is a vertical scaff pole used for lighting, and the first type of boom bases looked like WWI tank traps. This rig positions the lights above the heads of the audience, in front of the layout, and gives sufficient distance to even out the spread of light. It is surprisingly compact in the car when de-rigged, but is still significantly larger than some other solutions. Similar bases for a light-weight application such as this could be made using Kee Klamp fitting base flanges with Integrated toe-board C18 Size 7, through-bolted to 18 or 25mm ply.



Base - approx 750 x 500mm
Material: 18 - 25mm ply (best quality)
Timber rails 50 x 25 P.A.R.
2-way batten 50 x 50 P.A.R.

Kee Klamp base plate is set towards back, to allow for lights cantilevered on front of truss. If rig is balanced, locate socket centrally.

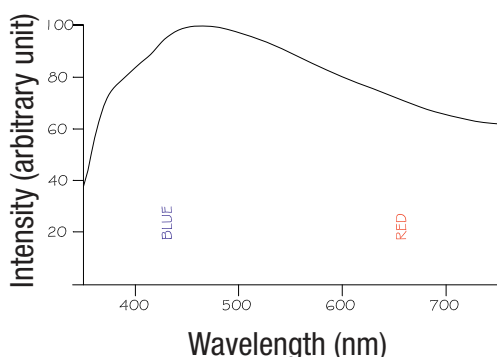
NEVER EXCEED 3:1 Height to Base ratio.

This base is rectangular for use with truss, which provides rigidity of structure. If simple bars are used, then base must be 750 square (for max 2250 height)

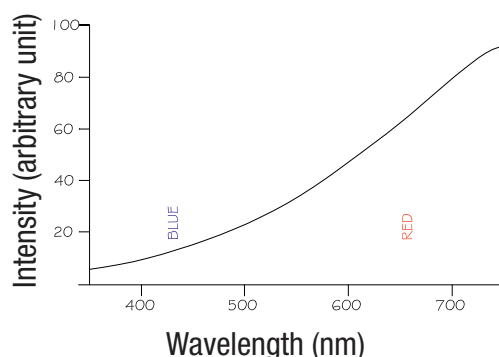
PORTRAYING DAYLIGHT

Real daylight is broadcast across a wide spectrum of wavelengths and, although it may look typically white, it is made up of all the colours of the spectrum to a greater or lesser extent, which is why there is so much depth and 'life' to daylight. Artificial light sources, however, broadcast on much narrower wavelengths, making objects look flatter and more lifeless than in daylight. Halogen lamps produce a good colour range, albeit on the warm side. Traditional incandescent lamps also broadcast over a wide range, but are warmer than halogens. Both these types produce a great deal of heat, however, and aren't energy-efficient. Fluorescents can be unpleasantly 'spiky' in their output, giving 'flat' colours as we know so well. LEDs broadcast a little more than their primary colour, but still aren't perfect.

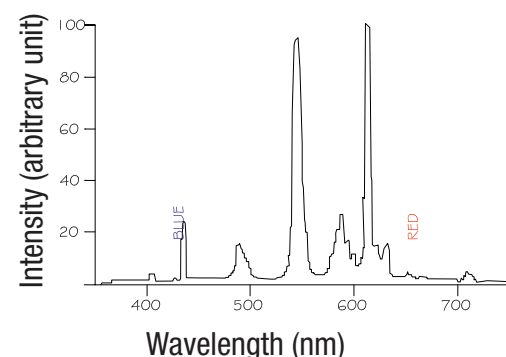
DAYLIGHT



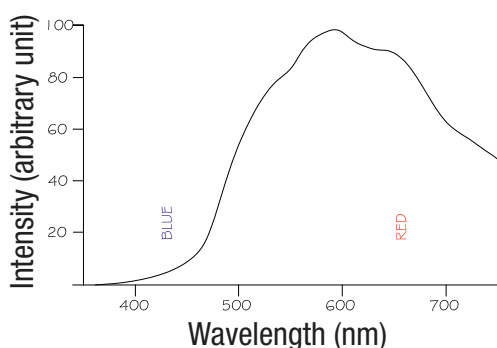
INCANDESCENT



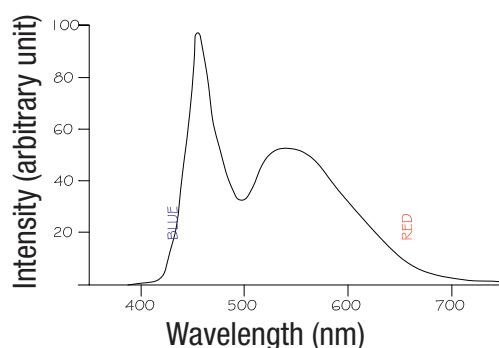
FLUORESCENT



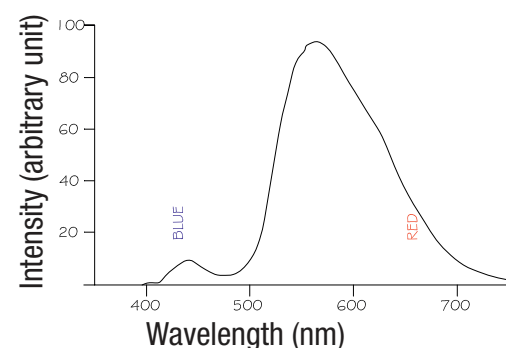
HALOGEN



COOL WHITE LED



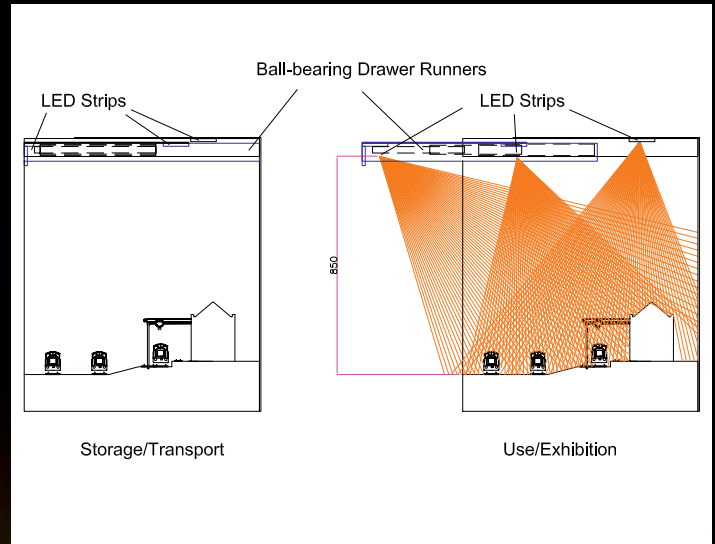
WARM WHITE LED



If it is not appropriate to have vertical booms in front of the layout, then one is left with the alternative of a cantilevered system with verticals to the rear. Ideally, these would be welded steel or similar, to give the best strength-to-weight ratio. However, most people do not have this option. A timber alternative, made from 3in x 1in softwood, with 9mm plywood plates would work, allowing the removal of the arm for storage and transport. Although the lights we would put on the end are not excessively heavy, it is very important that the support – and in particular the joint – should be strong enough for there to be no risk of collapse. A joint must be carefully glued with PVA and screwed in construction, noting that screw holes should be pilot-drilled to avoid the timbers splitting.

I recommend the use of pin hinges to mount a fascia or lighting pelmet. These are conventional back-flap or butt hinges made without the pin fitted, and supplied with a slightly smaller, looser pin. One half of the hinge is screwed to one element, and the other half is screwed to the second piece. Once put in position, the pin is inserted to temporarily join the elements together. Lots of theatre scenery is held together with these things! These are manufactured by Doughty and Triple E.

The last suggestion of form I shall make is for those layouts which are presented with a proscenium arch, and may be fully boxed-in with a viewing 'window' in the front face. These may be able to accommodate a pull-out lighting position, mounted on ball bearing drawer runners (40cm would be a convenient size as a minimum). These would be mounted on the side frames of the layout 'box', and carry almost an inverted drawer, to suit the lights and fascia required.



This would push in flush with the box and be secured with a small bolt for storage and transport, and simply pull out for use. These drawer runners (as shown above) are surprisingly strong, available from numerous suppliers and would be quite suitable for the purpose.



DIFFERENT BULBS

Incandescent bulbs are getting much harder to come by, use a lot of power (relatively) and produce a lot of undesirable heat, but do produce a nice light. Either GU10 halogens would be my choice if going down this route, or PAR38 for a larger layout – but select the suitable beam angle to avoid creating dark spots.

As mentioned, compact fluorescents of the appropriate colouration are viable, if used with shades or masking, and from an appropriate position. They are available in a range of white colours, but can have a tendency to 'flatten' a picture – particularly if used straight on. Look for photographic equipment when purchasing. A maximum of 35W per bulb would likely be appropriate.

LED options: LED GU10s are available in 4000k (neutral white), 2700K (warm white – rather yellow) and 6000K (cool daylight). These are very low wattage, producing very little heat, and are infinitely better than the versions sold even five years ago. They are available in 36 degree, and most usefully, in 100 degree. Coupled with a suitable fitting, they can be used for primary lighting, or as key lighting (the sun, for instance) from one side or behind to enhance and complement the primary lighting.

LED battens: A replacement for the old fluorescent tube are now available. These come in many lengths, colours and light outputs. A Ledus LED 4ft weather-proof 4000-4500K Natural White 45W (450W incandescent equivalent) was bought for trial, and compares very favourably in terms of colour rendering (the warm end of daylight) and strength and spread of output (a fairly even 120 degrees). If you're looking at a single light-source for simple lighting, this might be it. I would add a second light source of around 6000k – likely a GU10 – to add depth and contrast. Battens such as these have a wider spectrum than the fluorescent tubes, and I prefer them. They're less delicate, too.

LED panel lights: These are becoming more common and available in many colours including 4800K. LED panel lights are also available in round, square or rectangular up to about 122cm long and some dimmable, they give very even coverage, and would be suitable for home use.

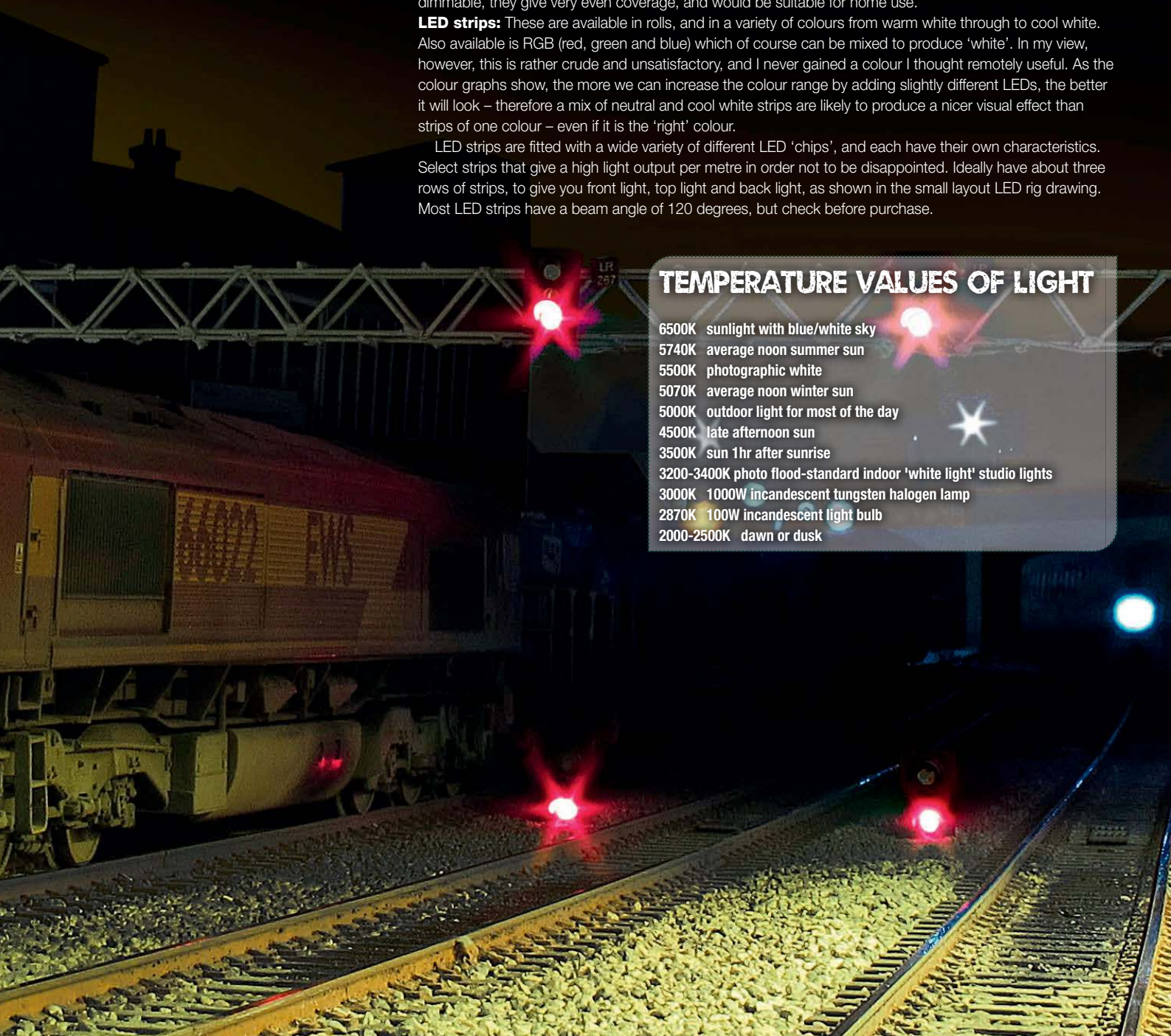
LED strips: These are available in rolls, and in a variety of colours from warm white through to cool white. Also available is RGB (red, green and blue) which of course can be mixed to produce 'white'. In my view, however, this is rather crude and unsatisfactory, and I never gained a colour I thought remotely useful. As the colour graphs show, the more we can increase the colour range by adding slightly different LEDs, the better it will look – therefore a mix of neutral and cool white strips are likely to produce a nicer visual effect than strips of one colour – even if it is the 'right' colour.

LED strips are fitted with a wide variety of different LED 'chips', and each have their own characteristics. Select strips that give a high light output per metre in order not to be disappointed. Ideally have about three rows of strips, to give you front light, top light and back light, as shown in the small layout LED rig drawing. Most LED strips have a beam angle of 120 degrees, but check before purchase.



TEMPERATURE VALUES OF LIGHT

6500K	sunlight with blue/white sky
5740K	average noon summer sun
5500K	photographic white
5070K	average noon winter sun
5000K	outdoor light for most of the day
4500K	late afternoon sun
3500K	sun 1hr after sunrise
3200-3400K	photo flood-standard indoor 'white light' studio lights
3000K	1000W incandescent tungsten halogen lamp
2870K	100W incandescent light bulb
2000-2500K	dawn or dusk



RUNCORN SALT UNION

PART ONE: Wiring for Digital Command Control

Words & photography: Howard Smith

A process often misunderstood, Howard Smith commences wiring this new layout project, highlighting best practice techniques.



This layout project has me excited. I've seen videos and images of this industrial location adjacent to the Wirral, a history of which can be read in last month's issue, presented by Traction Magazine's Stephen Rabone. Key elements that piqued my interest for its setting in the late-1980s and 1990s was the use of BR locomotives, running around track with such tight radii that they were prone to wheel slip, hauling just a few wagons.

The site has greatly changed today, being re-developed by Ineos, though its industrial usage continues. Modelling the location to deadlines in the confines of an office

has restricted the scope. I only have a few months to construct this, and finding time to spend on it is difficult. Perhaps many modellers find themselves in a similar situation? Modelling smaller projects has its advantages, though I've never modelled an industrial site before. Understanding the complexity of these, it'll require more detail than lazily adding static grass to a landscape, planting trees and calling it 'done'.

There needs to be compromise. Let's curb ambitions for a second here. Modelling every rivet and achieving something that looks truly impressive isn't the aim of the game – there simply isn't the time. If I can

create architecture and operation that's convincing from a distance however, I'll be pleased. Smaller details and refinements are simple to add later, as ideas and observations prevail – a similar story on most layouts.

I'm starting the article with the track laid. If you're new to the hobby and are interested in learning how this is done, find a step-by-step tutorial on our website world-of-railways.co.uk/techniques. Four points are from Peco, track and one other point is from Hornby. All is to Code 100. I've used what's been lying around the office, ordering in what's required, just as you'd do at home. 'Re-use, recycle' is my motto. Most of the rail will be hidden by plaster, painted to represent concrete. It's a blessing that'll hide a multitude of sins, including the rail height.

The layout has a simple trackplan with five points which should make wiring simple. The two baseboards in an 'L'-shape are from White Rose Modelworks and have an angle support piece. The layout is DCC-controlled, hence wiring it requires only dropper cables and a bus wire. Electronic items are from DCCconcepts.

Though I could opt for DCC-controlled points using its Cobalt IP Digital motors, DC modellers might want to run the occasional locomotive, so I'm sticking with its Cobalt Alpha IP point motors, using one of its DC split power supply kits. This will allow for control of the layout using either DC or DCC.

The principles for wiring a DC layout are similar, though rather than connect the dropper wires to a bus, they must be isolated via a switch on the control panel if you want to stop trains at a location. Let's get wiring!

SHOPPING LIST

DCCconcepts

- Switch Pack (CBSGR)
- Cobalt Turnout Motor Installation Template kit
- Cobalt IP Analog point motors (DCP-CB6iP)
- 25m of 2.5mm twisted bus wire (DCW-TW25-2.5)
- Dropper Wire 50m 26 x 0.15 green (DCW-DSGRN50)
- 1.5mm power bus wire (DCW-RD50-1.5 and DCW-BK50-1.5)

W www.dccconcepts.com

TOOLS

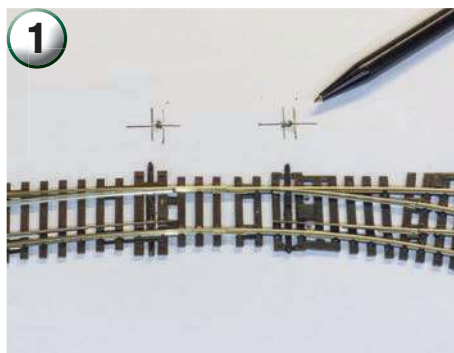
- Wire strippers and cutters
- 25W soldering iron
- Solder and flux paste
- Mini drill with slitting disc
- Cordless drill
- Track rubber



Each motor is screwed into place with the small screws provided. I've positioned these two facing each other for space reasons to avoid them clashing.



I never trust rail joiners for electrical conductivity. Soldering them using a 25W iron, solder and flux is a reliable method. Don't solder the rails touching the frog – ensure a gap for isolation is maintained.



The track is laid on white paper. Over the cork-topped boards, it's ideal for sketching the location of point motor centres, roads, buildings or fences.



The centres of the point blades are drilled with a 12mm 'spade' drill bit, supplied with DCCconcepts' Cobalt Turnout Motor Installation Template kit. A cordless drill makes quick work through the MDF boards.



The unrealistic excess is cut from the tie-bars. These can be used for side-mounting point motors where space is tight, if desired. A craft knife makes quick progress.



Ensure rail tops are cleaned after soldering. This is DCCconcepts Jumbo track cleaner – it's very large, obviously, hence lasts a long time. Flux can be removed using a toothbrush, water and washing up liquid, if required.



Before pinning the track, I remove the over-centre springs from the underside of the points. These maintain the blades in the correct position when using solenoid point motors. I'm using more realistic slow-action motors and their removal will make for smoother operation.



Using the plastic template also supplied with the kit, this is inserted into the hole and the grooved holes are used to locate the smaller drill bit. This is used to pre-drill the screw holes for the point motors.



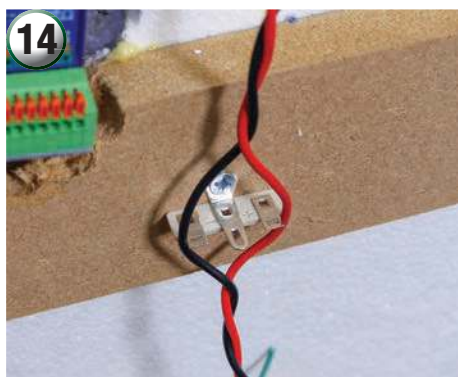
Peco Electrofrog points have a wire leading to the frog section of the point. This must be isolated from the surrounding rails. See how on this episode of BRM TV.



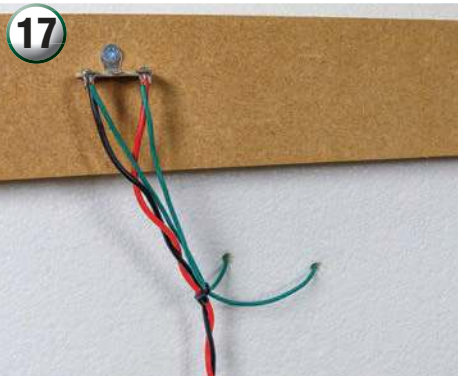
Holes are drilled for dropper wires using a 3mm diameter bit, sufficiently wide to prevent wires catching the sides of the hole when being pushed through.



11 Dropper wires are soldered discreetly to the tracks. See where these should be placed and how in this episode of BRM TV.



14 The purpose of the bus wire is to provide power to the track and accessories around the layout. It's placed around the layout adjacent to the tags allowing accurate measurements to be taken.



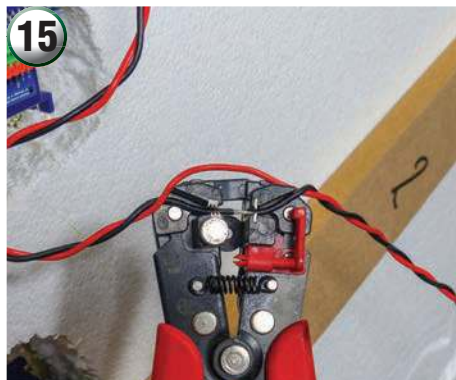
17 It's important to keep wires tidy. It helps find faults at a later date and prevents dangling loose wires that are prone to being caught.



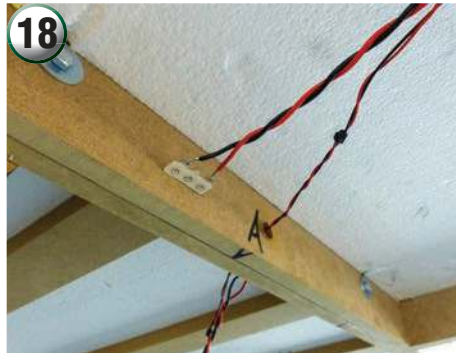
20 Power to the frog is supplied courtesy of this wire. Its polarity will be determined by the direction of the point. Cobalt IP Analog motors have two internal switches that facilitate this changing of polarity.



12 It's important to leave a gap between the frog rails and the track that follows. Cosmetic sleepers can be slid under rails where gaps are deemed too large later, though most of the track will be hidden by concrete.



15 Wires to be soldered to the tags can be stripped in-situ and needn't be cut. This helps keep wires strong and makes their attachment easier.



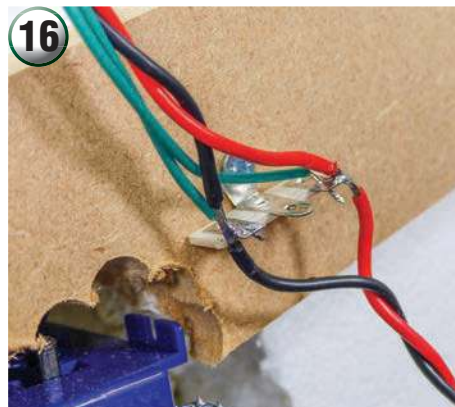
18 Between the boards a plug solution is required, though not knowing how many accessories I'll add later – hence the number of wires required between boards – I'm saving the choice of plug for later.



21 The rails prior to the frog don't change polarity. Rather than rely on thin contacts at the switch rails, I solder a piece of stripped and tinned wire to the underside of these rails.



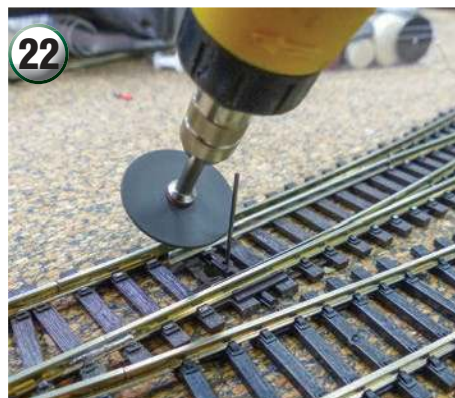
13 Returning to the underside, these bus terminals from DCCconcepts (DCC-Tag25) are easily screwed to the underside for keeping wires neat. These are kept close to the track.



16 A dab of flux followed by solder to dropper wires, bus wires and bus terminals sees them firmly attached.



19 Returning topside, work commences to make the point frogs more reliable. The wires beneath switch rails and the frog are cut to isolate the frog from current arriving at the toe of the point.



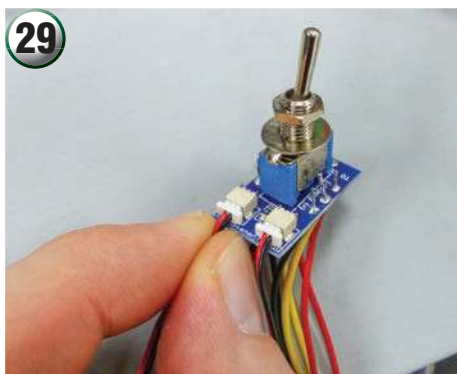
22 I cut the hardened steel point motor actuator using a mini-drill with a slitting disc. The metal will heat up quickly, melting the plastic, so cut in small steps.



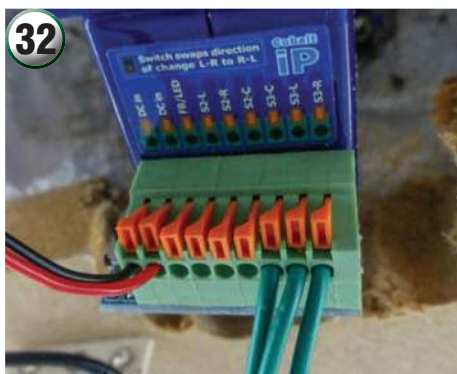
Before proceeding, I check the electrical conductivity of these newly-soldered joints and ensure that electricity flows – or doesn't – correctly. See how in this episode of BRM TV.



Included with DCCconcepts (CBSGR) switch pack are two plug-and-play green LEDs per switch. These are connected, ensuring the plugs are orientated the correct way. DCCconcepts produces the same pack of six switches with red LEDs, if desired.



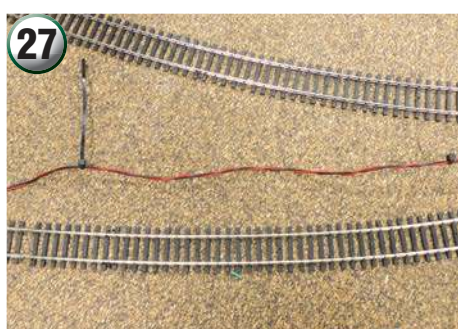
Control of points is provided courtesy of DCCconcepts 12VDC split power supply kit. The two transformers included plug into the wall sockets. At the other end, a 12V common, positive and negative wire exit.



Inserting wires into the motors is easy – the push terminals are strong and reliable. Note the L (left) and R (right) input feeds from the track to the switch which determine the polarity of the frog.



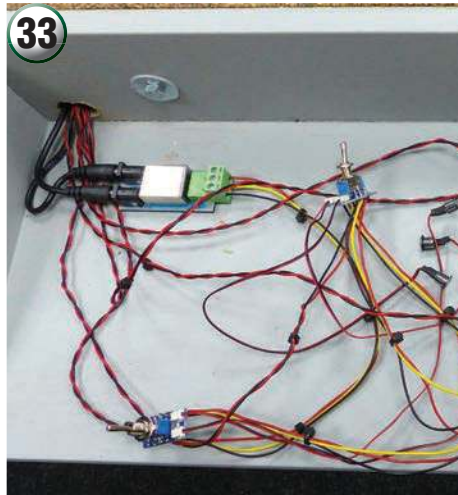
I'm controlling the points with these switches. I'm tinning the eight holes with solder before adding the wires.



The wires that run between each motor are measured by creating a loom. This is made by pushing drawing pins into the board and twisting pairs of wires to length around them.



Power is applied to test the function of point motors, switches and LEDs. Some of the point motors spring into life, some don't. Investigation is required.



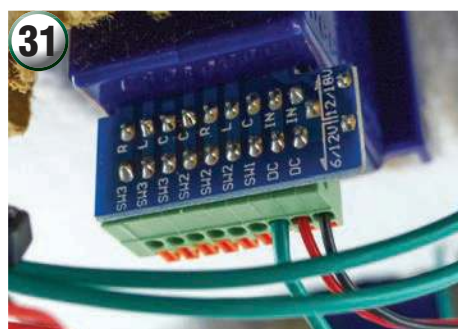
The wires are neatly zip-tied for ease of tracing. With track and point motors tested, I'm persuading our designer to create a control panel design. More on this in a future issue, though for now the layout is wired and I can breathe a sigh of relief.



The tinned wires are pushed through the holes which are heated with the iron. They take easily and the bond is strong, confirmed by a strong tug on the wire. The wires to be soldered are a 12V common, positive and negative. These daisy-chain in parallel between switches.



One end of the wires is soldered to the switch exit. The direction of current changes depending on the direction of the switch. This forces each point motor to change direction.



Underneath, on each point motor, a switch must be flicked down into the 6-12V setting on motors that weren't changing. If using a voltage between 12 and 18V, the switch should be in the up position. Note the DC feeds in to the right and wire from the frog leading to the common terminal of the integrated switch.

BRM TV

BRITISH RAILWAY MODELLING

DIGITAL EDITION EXCLUSIVE

RUNCORN SALT

Howard Smith and Phil Parker talk about creating quick card versions of your layout buildings as a way to iron out potential problems. Though with his talk of tinting windows, we're wondering if Phil is getting into the 90s car tuning scene?

EXCLUSIVE

WATCH THE VIDEO HERE
(You must have a wifi connection to stream video content)



DCC concepts

thinking outside the square

Cobalt-SS

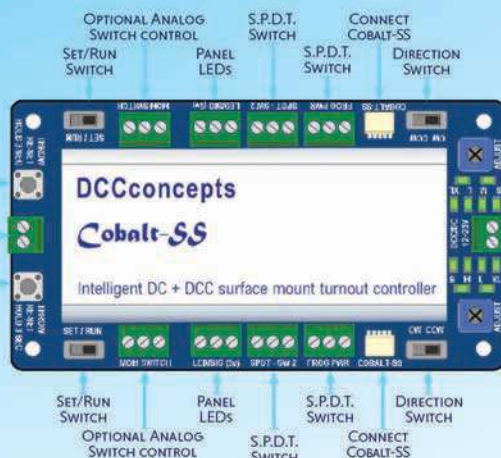
Cobalt-SS. The ultra-small motor drive turnout motor, ready to go "as is" to use with DC or DCC. So small that it can be almost hidden in the ballast, yet strong enough for all common scales. Very stable stepper motors provide the drive power... and all motor connections are plug-and-play.

Cobalt-SS works with all common scales & gauges. All you will need is in every pack. Each output on the control board is able to operate a full crossover.

This picture of Cobalt-SS is presented here full size



RESET TO DEFAULT
CONNECT TO DC OR DCC
RESET TO DEFAULT



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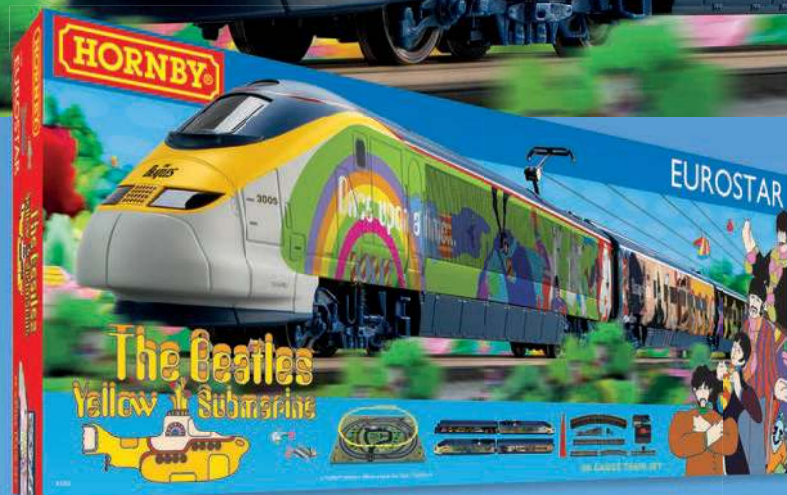


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The Beatles Yellow Submarine



Eurostar's first use of the Class 373 for an external agency marketing campaign took place in September 1999 when the Sets 3005/3006 were transformed with a vinyl wrap that recreated scenes from The Beatles' film 'Yellow Submarine'. This was just one of the promotional events that marked the release of the remixed music track that Autumn.

Along with the remastered version of the film that was released in cinemas and on video, the film which had not been available for over a decade included scenes that were edited out of the original, while the album featured new mixes of songs, such as 'With A Little Help From My Friends' and 'Lucy In The Sky With Diamonds', the first such time a Beatles album had ever been remixed.

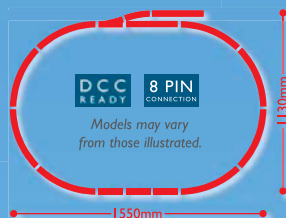
R1253M EUROSTAR TRAIN SET

ROLLING STOCK

Eurostar, Class 373 Power Driving Car + Eurostar, Class 373 Dummy Driving Car, 2 x Eurostar, Class 373 Passenger Saloons, with non-working pantographs

TRACK & ACCESSORIES

3rd Radius Starter Oval, with Track Pack A (includes point and buffer)
Train Controller (R8250), Wall Plug Transformer (P9000W), Power Connecting Track (R8206), Re-Railer
Hornby MidMat (1600 x 1180mm)



Also available Eurostar Class 373 Train Pack and Coach Pack



R3829 EUROSTAR TRAIN PACK

Eurostar, Class 373 Power Driving Car + Eurostar, Class 373 Dummy Driving Car, 2 x Eurostar, Class 373 Passenger Saloons, with non-working pantographs



R4000I EUROSTAR COACH PACK

Eurostar, Class 373 Divisible Centre Saloon Coaches x 2

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PLEASE NOTE: The product images shown above for R1253M, R3829 and R4000I are a representation of the decoration on side 1 and side 2 of each model contained within each pack.



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LONDON TOY FAIR 2020

The toy industries' annual January showcase in the Capital yielded great railway interest, as Phil Parker reports.

Words & photography: Phil Parker

Oxford Rail

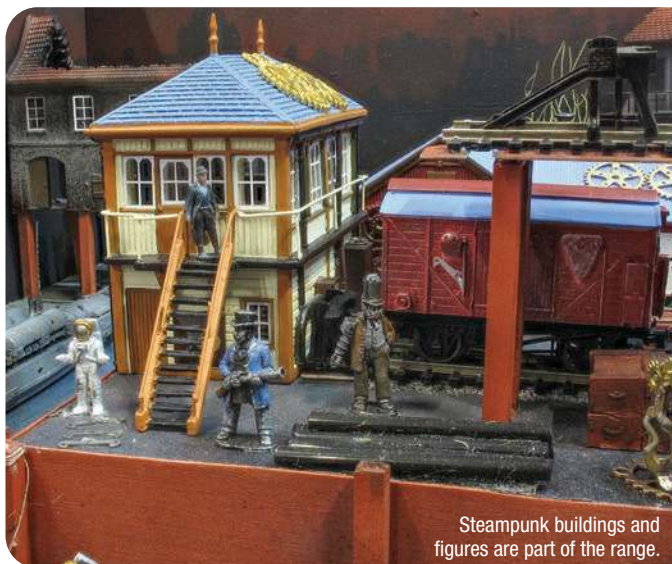
2020 looks set to be a busy year for Oxford Rail, with its announcement of a new LNER J26 0-6-0. A logical development from the firm's soon-to-be-released J27, the model will be available in three different versions - BR (Early) as No. 65767 and No. 5738 and BR (Late) as No. 65736. All three locomotives will have the option of sound.

An unexpected addition to the range will be a Standard Cowans Sheldon 15T Crane, to be offered in four different guises as BR Stoke M.P.D No. RS1023/15, LMS Wellingborough No. 243, BR Stewarts Lane DS. 316, LNER Sunderland No. 901628. With an RRP of £19.95, feedback on our forum, RMweb, has been positive.

While marshalled in a train, the jib would rest on a runner wagon. Oxford Rail has looked at these and they vary in design far more than the cranes. Fortunately, many companies used a 4- or 5-plank private owner wagon with a wooden cradle to support the jib. Oxford Rail is to produce this with an RRP of £14.95.

Oxford Rail revealed new liveries for its private owner tank wagons, new 10T covered vans, plus a 'Pilchard' 20T ballast and sleeper wagon. We also saw 4mm scale pre-production castings for a Mercedes Sprinter van and Mk. 3 Ford Escort and Capri, Audi S3 and Quattro cars.

Cowans Sheldon 15T Crane.



Steampunk buildings and figures are part of the range.

Bassett-Lowke

Steam punk came to the Toy Fair in the shape of Hornby's new range, which enjoyed its own amazing stand decked out as a wood-panelled room with a fireplace and Victorian furniture.

On one side was a layout showcasing the range and demonstrating the possibilities for those with imagination. Its creator, Laurie Calvert, built it using many parts from the Airfix catalogue with special additions to fit in with the production models.

The range includes more than model railway items, branded packs of Humbrol acrylic paint and the return of the Airfix chocolate vending machine from the 1970s are also available. To populate your model, 3D-printed and appropriately-dressed figures will be available.

The resin construction and unusual design of the locomotives results in models being slightly beefier and heavier than traditional OO gauge locomotives.

Appropriate for the world of Victorian steam-powered science and empiricism they will live in, the models built around these will require more generous clearances on their layouts before running them.

Each has a good, solid feel and plenty of weight. The detail might be unconventional, but it's certainly eye-catching and well-modelled.

Hornby

For its 100th birthday, the Hornby team had increased the display space on the stand to accommodate the large number of announcements we've seen across its ranges. A large surprise is a Harry Potter train set in gauge 1. Produced in conjunction with classic American train maker Lionel, the set features No. 5972 *Hogwarts Castle* and a pair of coaches.

Battery-powered, a remote control handset works the speed and direction of the model, plus operating a selection of sounds including clips from the films. Plastic track forming an oval 73in by 50in is ideal for both indoor and outdoor use. RRP for this set is £150 and it should be out in the autumn.



Gauge 1 GWR 4-6-0 'Hall' Class *Hogwarts Castle*.

Keep up
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latest news at
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Bachmann Europe

Bachmann used the London Toy Fair to launch its OO gauge Thomas the Tank Engine range to the retail trade, just in time for the famous locomotive to celebrate its 75th birthday.

The initial range will consist of: *Thomas*, *Percy*, *James*, *Gordon* and *Toby*. Each locomotive has moving eyes that look from side to side as the model travels around the layout.

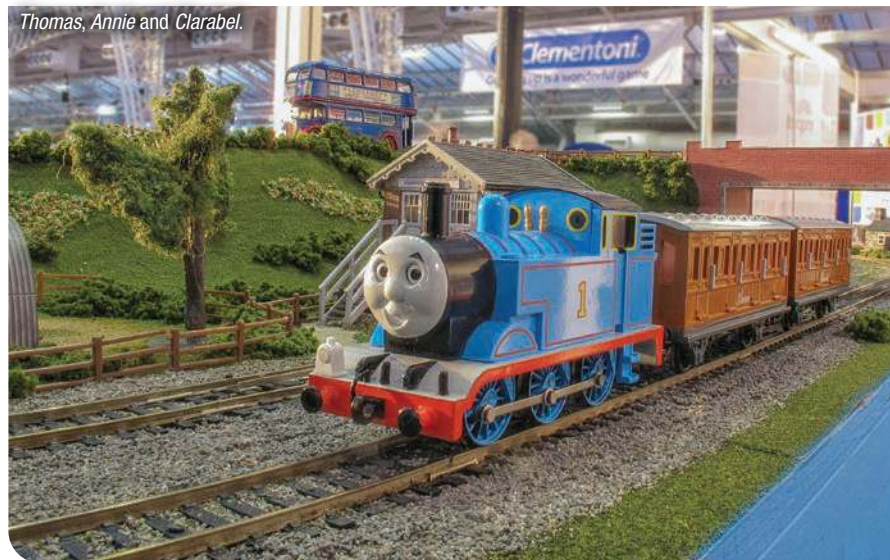
With a view to the future, a Next 18 DCC socket is fitted into all locomotives. Bachmann believes that most youngsters are digitally-savvy and will be quicker to look beyond analogue control than many adults. It also raises the tantalising possibility of sound chips including speech for each character. We wonder who will record this?

Rolling stock will also be part of the range: *Annie*, *Clarabel*, *Henrietta*, *Gordon's* express coaches as well as plank and tank wagons. We understand that the full range of characters, currently part of the Bachmann USA range, will launch in the UK. All models are based on the current TV series, so should be easily recognisable to children, and the models are marketed as being suitable for those aged three and above.

Models will appear on our shelves in the autumn, in time for the vital Christmas market. Train sets feature a simple DC controller and E-Z Track, a simple-to-use and robust system with built-in ballast, making it ideal for layouts built on the carpet as well as more advanced set-ups.

In addition to the OO gauge models, Bachmann will continue to sell the popular G gauge range consisting of *Thomas*, *Percy*, *Toby*, *James* and *Emily*.

Thomas, *Annie* and *Clarabel*.



What is the London Toy Fair?



The Toy Fair is the UK's largest dedicated toy, game and hobby trade show that takes place at the end of January each year at London Olympia. With over 270 companies exhibiting thousands of products, it attracts buyers from all the major retailers and a number of independent traders. Everything seen here will be on Christmas lists in a few months' time.

As soon as it ends, most of the stands relocate to Nuremberg, Germany and then New York, U.S. Toys are big business!

CREATING THOMAS THE TANK

As the cheeky character celebrates its 75th anniversary, Phil Parker retraces the history and evolution of this inter-generational and most famous of locomotives.

Words & photography: Phil Parker



“Thomas was a tank engine who lived at a big station. He had six small wheels, a short stumpy funnel, a short stumpy boiler and a short stumpy dome.” - so it was that *Thomas the Tank Engine* was introduced to us on page four of the second book in the Railway Series.

Thomas was born in 1943 thanks to a bout of measles afflicting two-year-old Christopher Awdry. His father, the Rev. Wilbert Awdry OBE, invented some stories about trains as a way of entertaining his poorly son and made him a model of the locomotive that became *Edward*.

After the war, his wife encouraged him to seek a publisher for his stories and the first book, *The Three Railway Engines*, appeared

courtesy of Kaye & Ward in 1945. As well as *Edward*, this book introduced us to *Gordon* and *Henry*.

The series could well have stopped there, but Christopher wanted a model of *Gordon*. Sadly, in the post-War period, materials were hard to come by and building an express locomotive wasn't possible. Instead, the Reverend built a tank locomotive from 'odds' and 'ends'. The youngster named it *Thomas* and his father then had to devise more adventures for the new arrival. These were the basis for the book *Thomas the Tank Engine*, which appeared in 1946.

By now, public demand persuaded Awdry to keep writing the series. In his eyes, locomotives and rolling stock were

all children with distinct characters. In one interview he explained “There was no doubt in my mind that steam engines all had definite personalities. I would hear them snorting up the grade and little imagination was needed to hear in the puffings and pantings of the two engines and the conversation they were having with one another.”

Thomas and *Percy* were cheeky, misbehaving whenever possible. *Gordon* was proud, *James* troublesome, *Henry* vain. All get their comeuppance and learn valuable lessons. Unsurprisingly for a vicar, there is a moral to each tale.

Just as importantly for the author, the reader learns about real railway operation. Awdry was involved as a volunteer guard with the nascent Talylyn Railway (TR) as well as other preservation societies. He wrote a non-fiction book on the Birmingham and Gloucester Railway, and served as the Editor of *Industrial Archaeology of Gloucestershire*. When appearing on *Desert Island Discs*, two of the records he chose were steam train soundtracks.

He was also a railway modeller, exhibiting his own *Thomas* layout at the International Model Railway Exhibition (IMREX) several times. In addition, there was an extensive layout based on Barrow-in-Furness in the attic at Emneth.

Awdry didn't just create characters, he invented an entire island for his trains. Sodor is situated between mainland Britain and the Isle of Man. An enthusiast's paradise, all the locations are based on real places and railway lines, especially those on the narrow gauge where the TR and Ravenglass & Eskdale Railway are the Skarloey Railway and Arlesdale Railway respectively. This was made explicit and young readers were encouraged to persuade



ISLE OF SODOR

In the Irish Sea, between Barrow-in-Furness and the Isle of Man, you will find the Isle of Sodor.

"Discovered" by Wilbert and his younger brother George Awdry, they created the entire island, complete with history and landscape, for the railway stories to take place. The name derives from the Manx Diocese of Sodor and Man, the boys deciding that while there is an Isle of Man, there wasn't an Isle of Sodor – at least until they invented it.

62 miles wide and 51 miles long, Sodor is considerably larger than Man. The capital is the city of Suddery, but the largest city is Tidmouth, which makes a number of appearances in the books.

The North Western Railway is the main transport provider, with a main line from Vicarstown, where it connects with the UK via a bridge. Nine standard gauge branches connect the main towns. There are four narrow gauge railways including the Culdee Fell railway that climbs the island's highest mountain in the same way as the Snowdon Mountain Railway does in Wales.

It was important to Awdry that the railway maintained a consistent look. Readers had spotted inconsistencies between illustrations so he used the background history to prevent this. Of course it didn't stop every mistake, hence the creation of *The Fat Controller*, who could be blamed for making changes.

their parents to pay a visit to the lines on school holidays.

Books weren't the only place *Thomas* appeared. In 1957, Awdry narrated two stories for vinyl records. A few years later, popular television presenter Johnny Morris records a series of albums for Delyse Records. Whereas the author simply read the books, Morris enhanced the story with his own sound puffs and chug sound effects, in a similar manner to his work on the TV series *Animal Magic* around the same time. This must have gone down well because Morris reading the story *Edward and Gordon* would also go to the desert island with its author.

The popularity of the books had reached the BBC and in 1953 it decided to televise them using Hornby OO gauge models on purpose-built sets. Episode one, *The Sad Story of Henry* was broadcast live on Sunday, June 14 from the Lime Grove studios. Live TV drama was common as video recording facilities were at best primitive so sadly, no version exists today.

By all accounts, the programme was a bit of a disaster. An incorrectly set point derailed *Henry* and a giant hand is suddenly seen putting him back on the track. Awdry complained about this and the unrealistically jerky movement of the trains. A planned second programme was cancelled and the author and publisher decided to concentrate on producing books, instead.

It would be over 20 years before another

attempt was made to bring the characters to our screen. In the meantime, comedian Ted Ray had read five books on the BBC's *Jackanory* but he was simply sitting in a stationmaster's office with occasional illustrations appearing on screen.

Andrew Lloyd-Webber remembered the books from his childhood and by 1973, enjoyed the clout and finance to believe he could create a series and sell it around the world. Awdry worked directly with Lloyd-Webber as both he and the publishers weren't confident about the plans. Part of the problem was that the deal required all the rights and control of the characters to be

signed over, essential (apparently) for deals to be done in the U.S.. As the good Reverend predicted, "Once the Americans get hold of it, the whole series would be vulgarized and ruined."

The plan was for a series featuring 2D cut-outs in the style of the illustrations in the books. These would be animated in a way that would be familiar to anyone who watched *Ivor the Engine* as a child. A pilot episode was produced in 1976, but the sale of the show to the U.S. never happened and the project didn't proceed further. Lloyd-Webber went on to write *Starlight Express*, based on a very similar concept to the

James the Red Engine. Based on L&YR Class 28 mixed-traffic tender locomotives. According to *The Island of Sodor: Its People, History and Railways*, *James* is an experimental member of the class fitted with a front pony truck, larger driving wheels and a Fowler tender. (Bachmann model)



THE MODELS

The very first models from the Railway Series, were created by Awdry. *Thomas* initially looked like an LNER J50 with smaller tanks, painted blue with 'NW' on the sides. Sadly, this model was lost in the U.S..

This push-along model was replaced by an RTR locomotive. Awdry explained, "I bought *Thomas* in 1948 when I was writing *Tank Engine Thomas Again*, and wanted to start modelling once more after a lapse of some 20 years. *Thomas* was one of Stewart Reidpath's standard models with a heavy, cast white metal body."

Lack of spares caused this to be replaced by a modified Tri-ang 'Jinty' and then later a Hornby LBSC tank in 1980.

Commercially, the first model in the shops came from Meccano. 1965 saw the production of an O gauge clockwork *Percy*, complete with a yellow truck and red goods van.

Surprisingly, this didn't sell well and the model later appeared with the face replaced with a red smokebox. Perhaps this is why it was the last O gauge train set made at the famous Binns Road factory?

Hornby took on the licence in 1985, eventually producing 22 different models. Many were based on existing tooling, sometimes modified to look more like the character.

Thomas was easy – the locomotive was based on an LB&SCR O-6-OT 'E1' and there was already one in the electric range. However, it also produced clockwork models of *Thomas* and *Percy*, which were produced from scratch with very different, arguably closer to the TV series, proportions to the electric models. Initial productions of *Thomas* have faces resembling those from the books. After a year, this was retooled to match the TV series.

Now a special clockwork/pushalong train set for the very young - **PERCY THE SMALL ENGINE**

The Rev. W. Awdry's lovable character brought to life by **MECCANO**

Percy is a very special, famous Small Engine.

Children everywhere share the amusing adventures of this cheeky little chap in the charming 'Railway Series' books by the Rev. W. Awdry. And now you can have Percy all to yourself, just like he looks in those stories—same face, same colours, everything! Because Percy's here with his own train set!

The 'Railway Series' books by the Rev. W. Awdry are available from all good bookshops.

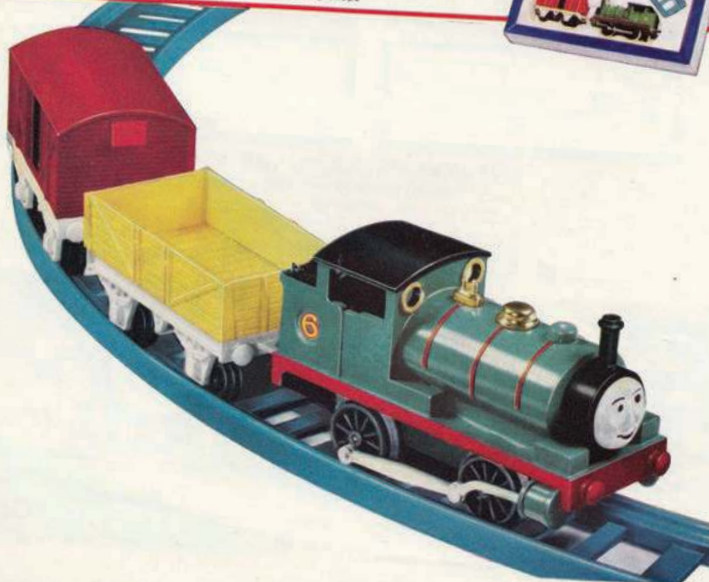
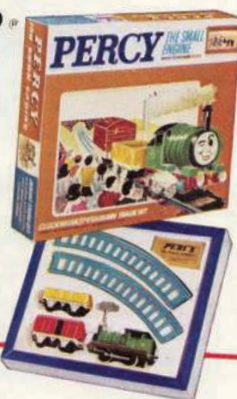
YOUR PERCY TRAIN SET HAS ALL THIS:-

- * Percy himself (large as life!) with a new long-running clockwork motor that can be free-wheeled backward and forward without damage.
- * Open Goods Truck and Closed Goods Wagon with sliding doors.
- * A brand new idea – rails simply click together, and are specially designed to make it easy to put Percy on the track. Track is 36" in diameter, and includes a brake device to stop the train automatically.
- * Everything constructed in tough, gaily coloured plastic – safe, light, practical.
- * Percy will also fit your standard "O" Gauge track.

COMPLETE WITH TRUCKS AND CLICK-TOGETHER PLASTIC TRACK

59/6

From Hamleys, Harrods, Gamage, Selfridges and all good toy shops



Other locomotives required more work. *James* is the Tri-ang '3F' with an extension to smokebox and chassis. *Edward* is an LNER 'D49' fitted with a taller chimney and cylinders removed.

Rolling stock was based on existing models. Pedants pointed out that coaches *Annie* and *Clarabel* had two axles from Hornby and in the TV series, whereas the books gave them bogies.

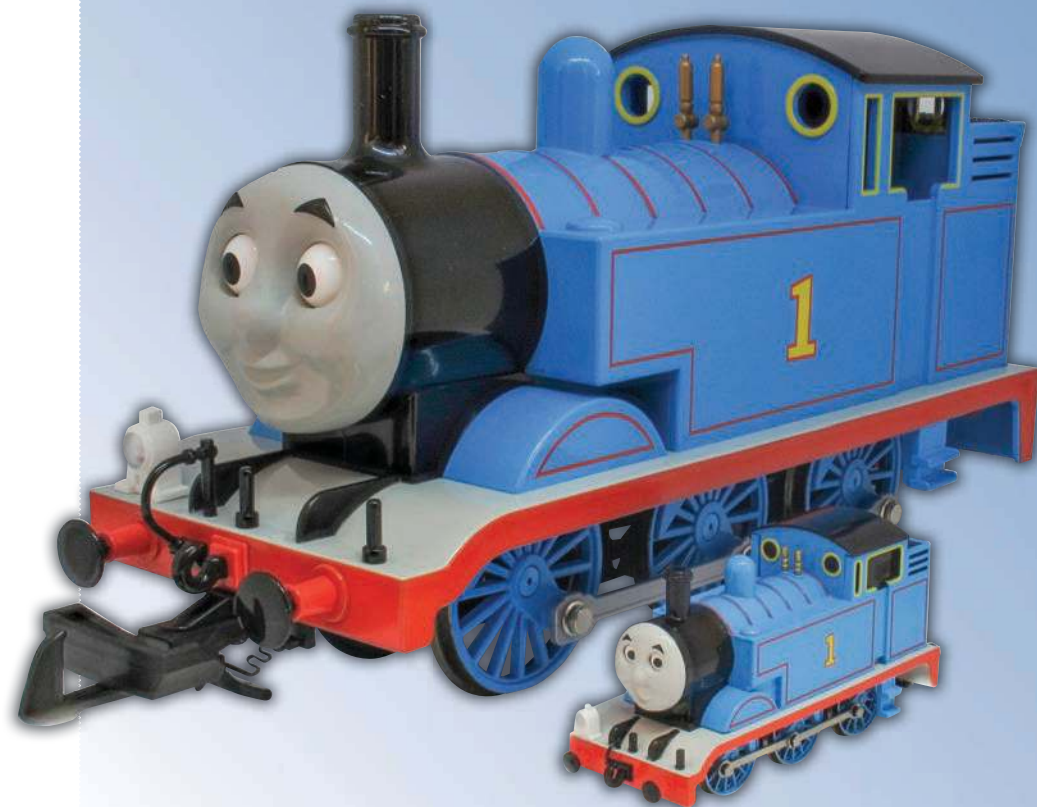
Märklin produced a German range of models in 2006 using chassis designed for its three-rail system and Hornby bodies. Hornby gave up the licence in 2018.

Bachmann USA has long held the licence in the U.S. and produces a large number of models in OO gauge, although labelled HO for the local market. As well as the standard gauge items, OO9 models of the Skarloey Railway appeared in 2015.

G scale models appeared in 2009 and because the Hornby licence only covered OO/HO models, they were allowed to be sold in the UK. All the models were created from scratch to represent the character from the TV series and usually include moving eyes and in OO gauge, are fitted with Next 18 DCC sockets.

While the OO gauge range couldn't officially be imported, models found their way across the Atlantic in small numbers. The OO9 locomotives are particularly popular and kits exist to adapt them to TR prototypes.

Bachmann has the licence and revealed the first examples from the range at the 2020 London Toy Fair. These should appear on our shelves in the autumn. An N gauge range is also under development.



Seen in the TR museum, the entire Ffarquhar layout.



railway series and called his organisation 'The Really Useful Group' after a description of *Thomas*.

Thomas finally became a hit thanks to TV Producer, Britt Allcroft. Researching a documentary on the Bluebell Railway, home to *Stepney* from the series, she met Awdry and read the books. The stories quickly cast a spell. "It really didn't take me long to become intrigued by the characters, the relationships between them and the nostalgia they invoked," she later said.

Five years were spent securing the finance to get the series into production and 1984 saw stories from the first eight books filmed using gauge one models on giant sets. Märklin chassis were fitted with purpose-built bodies to represent the characters.

26 episodes were filmed, narrated by Ringo Starr. All but one were from the books, the final *Thomas' Christmas Party* being a brand new tale written by Allcroft and David Mitton. It was subsequently adapted into a book by Awdry.

Series two in 1986 adapted more of the Rev. Awdry's stories, plus one from Christopher Awdry, who had taken over writing the series in 1983, adding 20 books until 2011.

At this point, the series was re-worked for the American market as a segment within the show *Shining Time Station*. For this, an



THE TALYLLYN RAILWAY MUSEUM

Located at Tywyn Wharf station, the Narrow Gauge Railway Museum is collection of artifacts relating to narrow gauge railways in the British Isles.

Member No. 79 - Rev. Awdry's connection to the TR resulted in the contents of his study finding their way to the museum after his death. A section of the room has been recreated in the museum, along with a display of artifacts including a model of the Ffarquhar Railway.

For more information, visit the museum website: www.narrowgauerailwaymuseum.org.uk.

There will be a special display celebrating the 75th birthday at the museum later this year.



Recreating Sodor on RMweb

We know that *Thomas* and chums are based on real locomotives, and many of the stories are based on real events, but what happens if you take the models and turn them into a realistic scene?

Tom Foster (TomF) on RMweb has been doing just that. His models treat the North Western Railway with the same level of realism that we might apply to a GWR or LMS subject. You can see more on his thread Ffarquhar - Recreating Awdry's 'Railway Series'.

alternative narration had to be provided to accommodate American English. Ringo Starr continued with his duties for the first series, being replaced after this by George Carlin.

By 1992, the UK series was back in production but featuring fewer Awdry stories and more written by Allcroft and Mitton. The excuse was that the originals introduced many new characters that would require more models and locations, greatly increasing the production costs.

Sadly, this meant that the railway realism started to dissipate. *Henry's Forest* saw the locomotive stop to look at some trees, a contravention of British Railways Rule 55,

which states that the driver should contact the signalman if the train has to stop for more than three minutes in a section. Allcroft didn't appreciate Awdry pointing this out, nor his complaint that the line ran far too close to the trees and a stray spark could have started a fire.

Series four returned to the books and introduced the narrow gauge trains, but the next series abandoned original material entirely. New characters were introduced, some based on ones in the books but others to provide a basis for more "edgy" tales and dramatic plot lines.

At the turn of the millennium, *Thomas* made it to the big screen as the star of his

first film *Thomas and the Magic Railroad*. Aimed at the US market, British viewers found themselves having to deal with American terms, but it still covered its costs, making a tiny profit.

By this point, very little of the railway realism remained. One of the main characters is Diesel 10, a Class 42 'Warship', fitted with a preposterous crabbing arm on the roof. The Rev. Awdry didn't live to see this, passing away in 1997.

By 2009 and series 13, production had moved away from physical models to CGI. The appearance of the locomotives changed slightly and additional voices joined the narrator.

Toy giant, Mattel, obtained the rights to the series in 2012 and continued production of TV shows and films. Recently, *Edward*, *Henry* and *Toby* have been relegated to the supporting cast of characters to make way for new lead locomotives. One of the problems for a 21st Century production company is that nearly all the original characters were male. Only *Mavis* and *Daisy* provided gender balance. The locomotives were 'in charge' and male. Coaches were pulled around, and were female.

While these changes might have generated a lot of controversy in the press, it's hardly surprising that such a long-lived and loved series of stories has needed to evolve over time. Despite this, *Thomas* and his friends are still popular with children worldwide. On its 75th birthday, there doesn't seem to be any sign of this changing either, ensuring a healthy supply of railway enthusiasts for the future.



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THE ORIGINS OF THOMAS

Phil Parker takes a trip into Fenland to visit one of Thomas's ancestral homes and the church in Emneth where the Reverend Awdry was the Vicar. We were also given access to the Reverend's study, where many of the legendary books were penned.

EXCLUSIVE

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



HOW TO... MAKE A PORTAKABIN KIT

Words & photography: Howard Smith

Howard Smith constructs three multi-media kits for BRM's new project layout.

Picture the scene – you need three identical buildings on a layout. Should you scratch-build them or construct them from a kit? Like many model buildings, choice is as varied as the opinion from modellers on what is the most suitable media to use.

If using multiples of a building, I think it's important to stick with the same type for appearance. If you can scratch-build an item, ensure you maintain the consistency across multiple buildings. If using a ready-to-plant resin building with relief in the stone walls, avoid placing a flat-sided card kit adjacent. This is where you become an artist, arranging your model composition for best visual effect.

Pushed for time with this layout build, three multi-media kits from Scale Model Scenery were sourced. These can be made into impressive-looking models – the paper, card and Perspex sheet design lending itself well to the smooth, flat-sided nature of these buildings. For a more textured object – a brick wall, for instance – I'd have opted for embossed plastic sheet.

Your choice of modelling media should be flexible. Observe which works best for a given subject and don't be afraid to mix and match. Embellishing your latest card kit with styrene or brass details is perfectly acceptable, too. So long as you avoid clashing details and remain consistent throughout, you'll be on the right path.



Follow this model
build online at
www.world-of-railways.co.uk/techniques

Scale Model Scenery produces many kits, but I wouldn't recommend its (KX041-00) 1970s Portable Office Building as your first. Results can be excellent, though a wise first move for best results is to practise with essential techniques on a less-involved kit.

Handily, as with all kits from this manufacturer, comprehensive instructions are included, in colour. These easy-to-follow and numbered steps make assembly easier to understand.



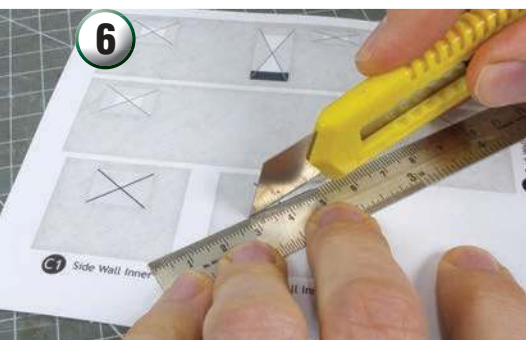
Unlike some card kits, this kit requires printed paper parts to be cut and glued to laminated card pieces. Using a fresh blade in a craft knife, I cut through the tabs holding parts in situ.



Glues for card kits are varied. Some use PVA, others a multi-purpose adhesive such as UHU. I'm using Noch's rapid-drying laser-cut (61104) glue. It dries quickly, so work swiftly to prevent lumps.



Card pieces are laminated to form a superstructure onto which the glazing, then paper, is glued. To prevent parts from warping as the glue dries, a glass acts as a weight. Parts can be gently flexed to straighten, if needed.



Internal walls are provided with the kit, should you decide to lift its removable roof to detail the inside. The window aperture flaps are cut diagonally, working from the corners, inwards. Using a steel rule and cutting mat, it's simple work.



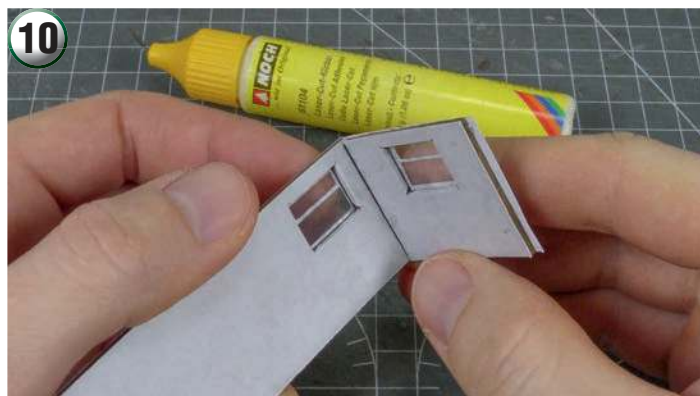
The black-printed windows are cut from the printed sheet, from the corner angles, working inwards. Keep cuts straight for appearance. Cutting windows is optional and the windows can be left as printed, if desired.



The Perspex sheet provided has printed window frames to aid with alignment. These have printed light weathering, too. The sharp edges of the steel rule can scratch, so I'm cutting these with scissors.



With Perspex sheet and outer wrapper glued, the components are left until dry. Some warping might be observed, in which case, parts should be gently twisted back to shape.



Final assembly is rapid, and hiding corner joins is most of the battle. For this, the order in which components are glued, and to which end, must be carefully observed.



A black marker pen can be used to hide the edges of the paper. The same technique is used for the side supports where the card is coloured using a few coats of the felt-tipped pen. Paper kits are difficult to rectify if damaged – patience is key.



With roof lifted, the vinyl blue flooring and ceiling panels are easily seen – reasons for me wanting to detail and add lights inside, later. A day's enjoyable modelling.

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Control Panels

GMC-U Single Track Controller with Simulation
Most Suited to OO/HO/N/Z Scale Layouts



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BEST FOR
Realistic Running

GMC-W Single Track Walkabout Single Track Controller
Most Suited to OO/HO/N/Z Scale Layouts



Fitted with 1.5m of cable, this controller allows you the freedom to move around your layout while still controlling your layout. It has a single 12V DC track output.

BEST FOR
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AWARDS



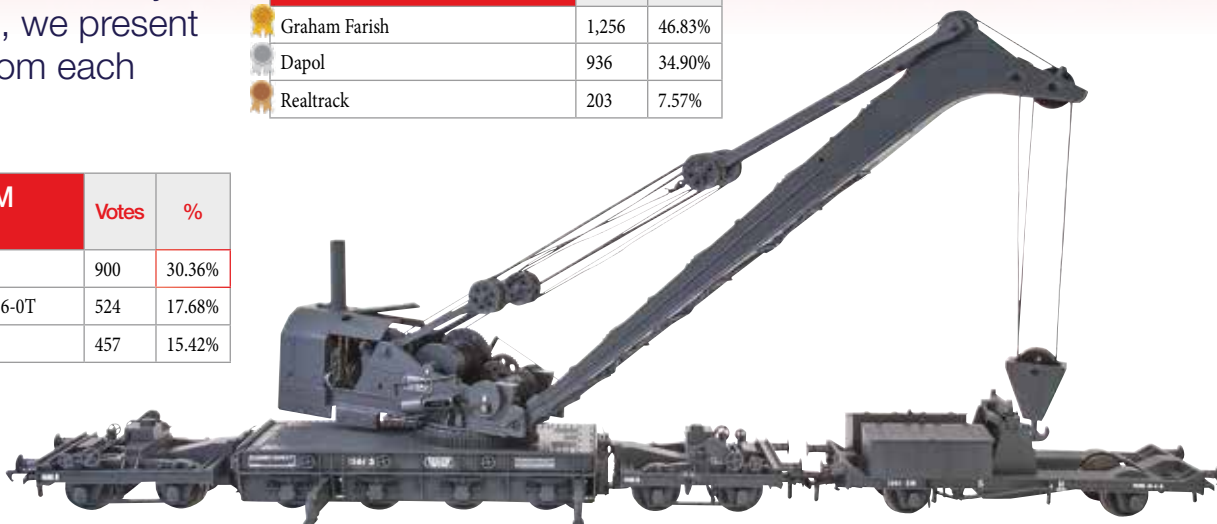
N GAUGE LOCOMOTIVES	Votes	%
Graham Farish SECR 'C' 0-6-0	863	35.66%
Farish Western Pullman	598	24.71%
Dapol N Class 50 Co-Co	510	21.07%

N GAUGE ROLLING STOCK	Votes	%
Osborn's 45T Ransomes Rapier Crane	802	34.27%
Graham Farish Birdcage coach range	694	29.66%
Realtrack PCA cement wagon	296	12.65%

The British Model Railway Awards are the 'Industry Oscars'. Here, we present the winners from each category.

N GAUGE MANUFACTURER OF THE YEAR	Votes	%
Graham Farish	1,256	46.83%
Dapol	936	34.90%
Realtrack	203	7.57%

OO GAUGE / 4MM SCALE STEAM	Votes	%
Hornby Peckett 'B2' 0-6-0ST	900	30.36%
Hornby Stroudley 'Terrier' 0-6-0T	524	17.68%
Hornby J36 0-6-0	457	15.42%



OO GAUGE/4MM SCALE WAGONS	Votes	%
Bachmann 45T Ransomes & Rapier Crane	1,085	31.57%
Hattons Rail Head Treatment Train	437	12.71%
Oxford Rail Gladiator rail-mounted gun	283	8.23%

OO GAUGE MODERN TRACTION	Votes	%
Hornby Ruston & Hornsby 48DS 0-4-0	816	24.95%
Hattons Class 66	693	21.19%
Kernow MRC D6xx 'Warship'	552	16.88%

OO GAUGE / 4MM SCALE MANUFACTURER OF THE YEAR	Votes	%
Hornby	1094	28.90%
Bachmann Branchline	670	17.70%
Accurascale / Irish Railway Models	543	14.34%

OO GAUGE COACHING STOCK	Votes	%
Hornby BR Mk 2F carriages	921	34.93%
Hornby 'bow-ended' 57' Collett range	914	34.66%
Hornby 59' Bulleid coach range	802	30.41%





O GAUGE LOCOMOTIVES

Votes %

Hattons Gresley A3 4-6-2	870	34.23%
Heljan Class 50	401	15.77%
Heljan Class 37/4 Co-Co diesel	371	14.59%

O GAUGE ROLLING STOCK

Votes %

Accurascale BR 24.5ton HUO hopper	746	33.56%
Darstaed BR Mk 1 Suburban coach range	490	22.04%
Darstaed BR Mk 1 Main line coach range	437	19.66%

O GAUGE MANUFACTURER OF THE YEAR

Votes %

Dapol	696	26.19%
Heljan	662	24.91%
Hattons Model Railways	661	24.87%

OVERALL MANUFACTURER OF THE YEAR

Votes %

Hornby	1,100	27.34%
Bachmann Europe inc. Graham Farish	722	17.95%
Hattons Model Railways	507	12.60%

RETAILER'S CUSTOMER SERVICE AWARD

Votes %

Hattons Model Railways	1143	28.50%
Rails of Sheffield	1003	25.01%
Scale Model Scenery	557	13.89%

WEBSITE OF THE YEAR AWARD

Votes %

UK Model Shops	929	29.72%
Model Railway Engineer	635	20.31%
LNER info - LNER Encyclopedia	370	11.84%

RMWEB MODELLER OF THE YEAR AWARD

Votes %

Jamie Warne (SouthernRegionSteam)	431	19.96%
Simon George (HeatonLodge40)	350	16.21%
Tim Watson (CF MRC)	291	13.48%

EXHIBITION OF THE YEAR AWARD

Votes %

Warley National Exhibition - NEC	1085	34.13%
London Festival of Railway Modelling - Alexandra Palace	479	15.07%
Model Rail Scotland	294	9.25%

LAYOUT OF THE YEAR AWARD

Votes %

Copenhagen Fields - The Model Railway Club	604	19.51%
Bournemouth West - Roger Sunderland	593	19.15%
Arun Quay - Gordon Gravett	351	11.34%

INNOVATION OF THE YEAR

Votes %

Bachmann Ransome & Rapier 45T crane	1018	27.81%
Scale Model Scenery for constant product development	951	25.98%
Rails of Sheffield 3D print van	540	14.75%



See more and join in the conversation on our forum RMweb.co.uk.



Humbrol



GOLDEN VALLEY HOBBIES

Cakebox Challenge Results



Our challenge for modellers was to produce a diorama that would fit in a cake box and include two railway items. Entries were diverse and the winners were decided by public vote. Our thanks to Humbrol and Golden Valley Hobbies for providing some great prizes!

THEME - HOLIDAYS

Winner

'Camping Carriage' by Chris Evans

'Heacham Station' in North West Norfolk was allocated its first Camping Coach in 1960. Later in the decade, when passenger traffic on the branch line towards Wells-next-the-Sea decreased, a second coach was allocated.

With the end of passenger services on the branch line, the turntable pit was filled in and the camping carriages used the former turntable spur. When the Kings Lynn to Hunstanton line finally closed in 1969, it appears that BR forgot there were camping coaches here and they were left for several years after the lines were lifted. Eventually, the coaches *Montana* and *Maid of Kent II* were removed by road. As they had survived past the major scrapping period, both coaches went into preservation.

Chris's model shows a family arriving for a holiday at Heacham after the line by the Wells platform had been lifted in the mid-1960s. The beach is only a couple of hundred yards down the road, there's a pub next door and a chippy half way to the beach.

Building the model was a chance to try out new modelling techniques and materials for Chris. His favourite part of the build was fitting out the coach interior.



Runners up

'Taxi Rank Corner at Waverley Station - Summer of 1957' by Marlyn Price

A photograph in an old 'Scotsman' newspaper article, of queues at Waverley Station, reminded Marlyn of school summer holiday journeys, travelling north to stay with grandparents in the Highlands. An old taxi cab was booked, which took them to Waverley Station and the first stop was always the John Menzies newsstand to buy comics for the journey.



'Andy's Seaside Memory' by Andy Brooks

Andy was inspired by memories of riding donkeys on the beaches at his favourite seaside village, Hemsby. There was always an ice cream van and you hadn't been on holiday until you enjoyed a cornet.



THEME - INDUSTRY

Winner

'Colliery Terrace' by Chris Chewter

Chris wanted to model the human aspect of Industry, showing where the people lived. Anyone who has been to South Wales will know the thousands of identical stone terraces spread across the valleys and beyond.

Look inside the cab of the Andrew Barclay, modelled using an ARC resin kit that is sadly no longer available, and you can see the loco crew enjoying a quick brew. Tilt the diorama to look down the alleyway to see one of the workers walking home. A tin bath is hanging up on the wall of the outbuilding and the mangle stands ready in the back yard.

Each house has the essential rabbit hutch, reflecting when miners kept rabbits to eat when resources were tight. You can see one of the miners discussing something with his wife, while it must be washing day next door.

The diorama is scratch-built from card with Scalescenes.com building papers. Details are from Langley Models and most of the figures from ModelU.



Runners up

'Take a little walk to the edge of town...' by Ben Bucki

Spon Lane Mechanical Engineers is a fictional industrial concern in the West Midlands. The gradual collapse in local heavy industry has seen its customer base declining and the premises becoming more neglected. An unusual design, Ben has created a box offering limited viewing angles and incorporated lights and sound for a small-scale theatrical effect.

'The lost days of Ironstone' by Pat James

Loosely based on the Wellingborough ironstone tramway in Northamptonshire, Pat used the challenge as the perfect testing ground for some ideas ahead of a larger layout. Modelling a metre gauge system, he used 12mm track with a 'Peckett' locomotive kit-bashed from an old Airfix Austerity kit incorporating styrene sheet and brass motion parts.



MELANGOOSE

Sympathetically evolving a layout acquisition to taste can make for difficult decisions, though for Nigel Crow his ideas made this transformation simple.

Words: Nigel Crow **Photography:** Tony Wright





'Celebrity' 37425 passes underneath the local Western National bus bound for Porth Meryn.

Factfile

Layout name: Melangoose
 Scale/gauge: 4mm:1ft / 1.76 scale / OO
 Size: 8ft x 8ins
 Era/region: Privatisation, 2001-2005
 Location: Cornwall
 Layout type: Fiddleyard to terminus

From the late 1990s to the early 2010s, I was a trader on the model railway exhibition circuit. As a regular at certain shows, you get to know certain folk within the clubs at the shows you attend. The Taunton Model Railway Exhibition organised by the Somerset Modellers Club at Huish College was always a friendly affair. One of its key members, Dave Smart, had built a Cornish branch line layout called 'Melangoose'. The layout was built so you could run stock from an 0-6-0T 'Pannier' tank locomotive with an autococh to a Class 66 with modern china clay hoppers.

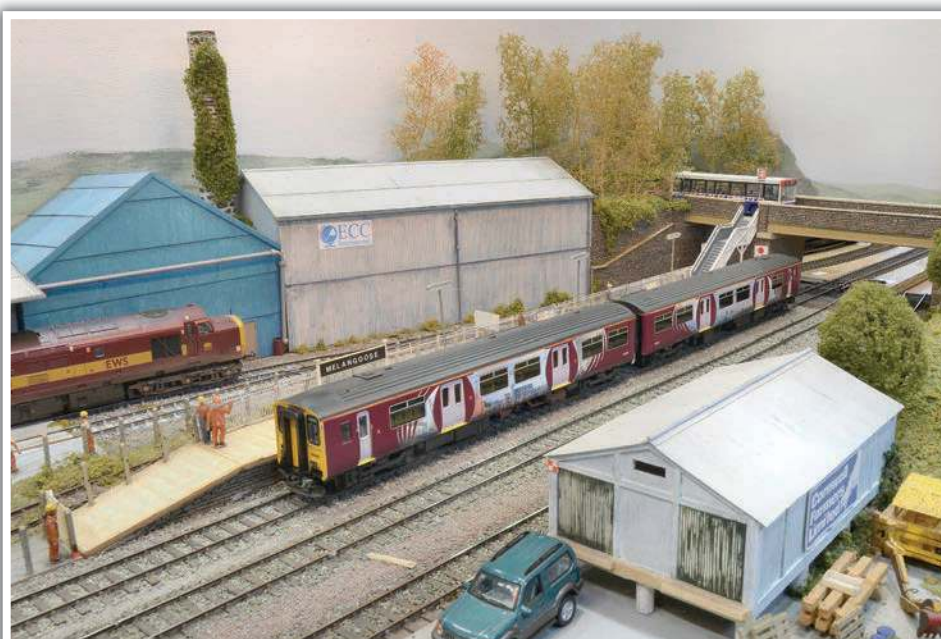
I remember seeing the layout a few times at Taunton and was very impressed with how much had been created in a small space, yet it had such varied operation. I had been to Cornwall several times in the early 1990s and was a fan of the china clay trains that I had seen at various locations on my visits to St Blazey, Bodmin Road and Lostwithiel.

The lovable thing that I found with main line and branch line railways in Cornwall, was that the railway infrastructure hasn't changed for decades. It is like being in a time-warped away from modern railways. Memories of Class 37s on short freight trains came flooding back to me, and I had a new favourite layout on the exhibition circuit. In 2007, Dave told me he was planning another layout as he was moving properties and had more space at his disposal. So, a few months later, 'Melangoose' was transported to its new home with me.

New era

'Melangoose' is very compact in length, incorporating a china clay works, a cement terminal and a small platform to add passenger interest. The baseboards are made from plywood. The scenic board is 5ft 2in long and the fiddle yard, which consists of a single track on a sector plate, adds another 3ft 6in. Cleverly, two storage tracks are housed within the china clay works, with a third track at the rear of the layout. The track is SMP on the scenic section and Peco in the bits that you can't see, and was analogue-control with an operating panel in the front of the layout.

My initial idea was to run the layout as if it still existed into the sectorisation era, but in 2009 Hornby brought out its Class 153 in the Wessex Trains Devon and Cornwall railway livery – a game changer for me. The new idea was to model the privatisation scene with the last days of the Class 37s handing over to the new generation EWS Class



150265 in its pictorial Wessex Trains livery makes a change from the usual Class 153s



37674 St Blaise Church and the local ground staff have their work cut out shunting.

66s, which were late to arrive in Cornwall because of axle weight loading. The major wagon type for china clay trains since 1988 has been the CDA. These are a part of the merry-go-round hopper family and Hornby produces a model of the wagon. Over the last 10 years, rolling stock options have been added to with super-detailed versions of the Silver Bullet slurry tanks (ICA), Imerys Bulk hopper (JIA), Traffic Services Polybulk (IRB) and Clay Tiger (PBA).

With such variety of freight flows available, the missing link was the Wessex

trains Class 150. I had a re-branded Regional Railways-liveried unit with Wessex Trains on the side, but I wanted the purple livery with the colourful pictorial vinyls. A Bachmann Class 150 was painted in purple with lilac doors and homemade matt vinyls were applied, taken from Martyn Read's superb photographs. The result gets a lot of remarks, although this is my take on the livery as no transfers have been available.

Exhibition time

In 2009, I was asked to bring 'Melangoose'

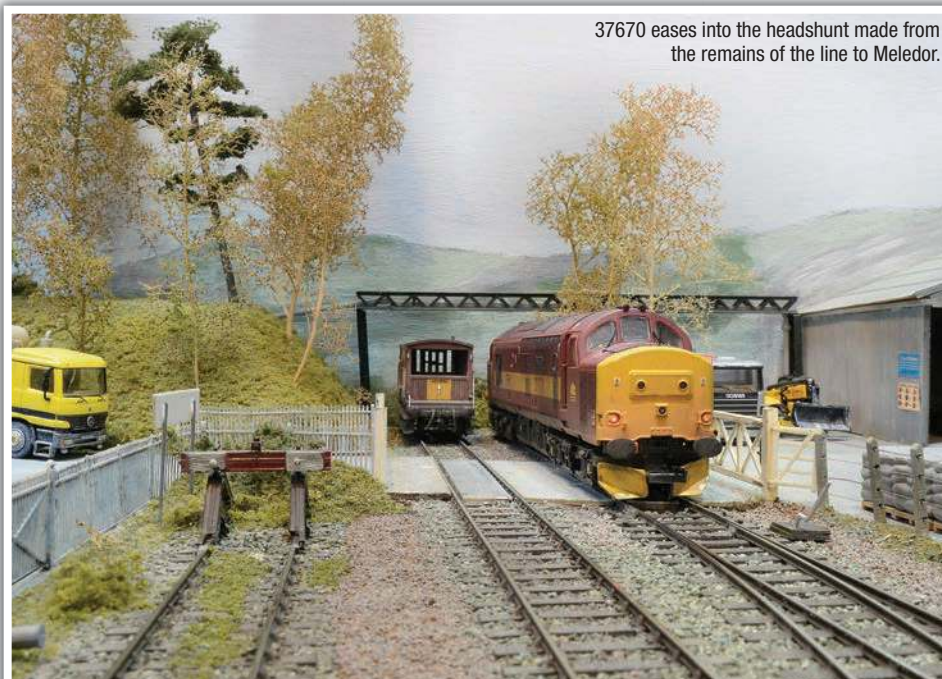
for fun, to Salisbury railway station celebrating the 150th anniversary of the current station. This was a great day and I was asked to attend the Salisbury Model Railway Exhibition. Things became serious... I had operated Kevin Broadhurst's 'Leaway Park' for many years and Steve Adcock's 'Russell Bridge', so I knew what to expect. The weekend went very well, but I was thinking about introducing DCC to the layout because most of my friends had gone down this route.

Operational change

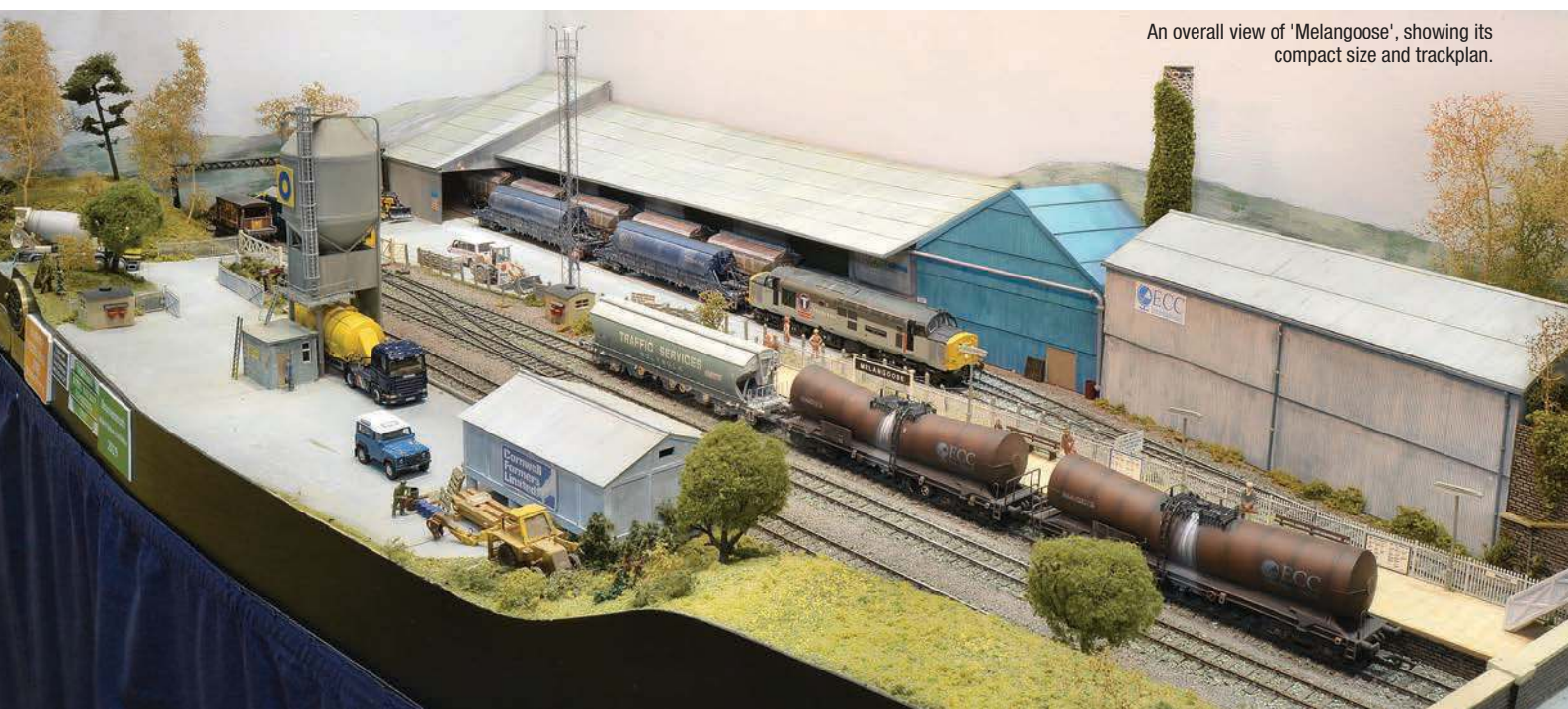
For the next exhibition at Wimborne, I installed a Bachmann EZ Command Controller on the layout that can store 10 locomotives – more than enough, or so I thought. One bugbear of mine, that became clear when operating the layout at the initial exhibitions, was the amount of time taken to remove locomotives off the track to put them on the other end. The single sector plate road was only 42in long. It was time to lengthen the sector plate so I could have a locomotive at each end of a train, to assist with the timely operation of trains on and off the layout. We also decided, that rather have just one road on the sector plate, if we had three, we could run more trains. To add to the DCC side, an NCE Powercab was installed on the layout.

A freight train will arrive and stop in the station, the locomotive is uncoupled and runs around the wagons in the platform. The locomotive is then attached to the

	Make	Livery	Name (if applicable)
08630	Hornby	EWS	
09008	Hornby	BR Grey	
37425	Bachmann	BR Large Logo	
37521	Bachmann	EWS	<i>English China Clay</i>
37670	Bachmann	EWS	<i>St Blazey</i>
37672	Bachmann	Transrail	
37674	Bachmann	Transrail	<i>St Blaise Church 1445 – 1995</i>
47843	Heljan	Freightliner ex-Virgin Trains	<i>Vulcan</i>
66022	Bachmann	EWS	<i>La Farge Charnwood</i>
66065	Bachmann	EWS	
66546	Bachmann	Freightliner	
150265	Bachmann	Wessex Trains	
150270	Bachmann	Wessex Trains ex Regional Railways	
153382	Hornby	Devon & Cornwall	



37670 eases into the headshunt made from the remains of the line to Meledor.



An overall view of 'Melangoose', showing its compact size and trackplan.

other end and takes the train off scene, then reverses the train to the china clay works or cement terminal. The locomotive will again uncouple and couples-up to another set of wagons for the outbound journey.

We normally use the one locomotive in operation method, which includes the shunting, because this was typical of operations on Cornish china clay traffic. To give a welcome break from freight operations, we run a sparse passenger service with Class 150s and 153s.

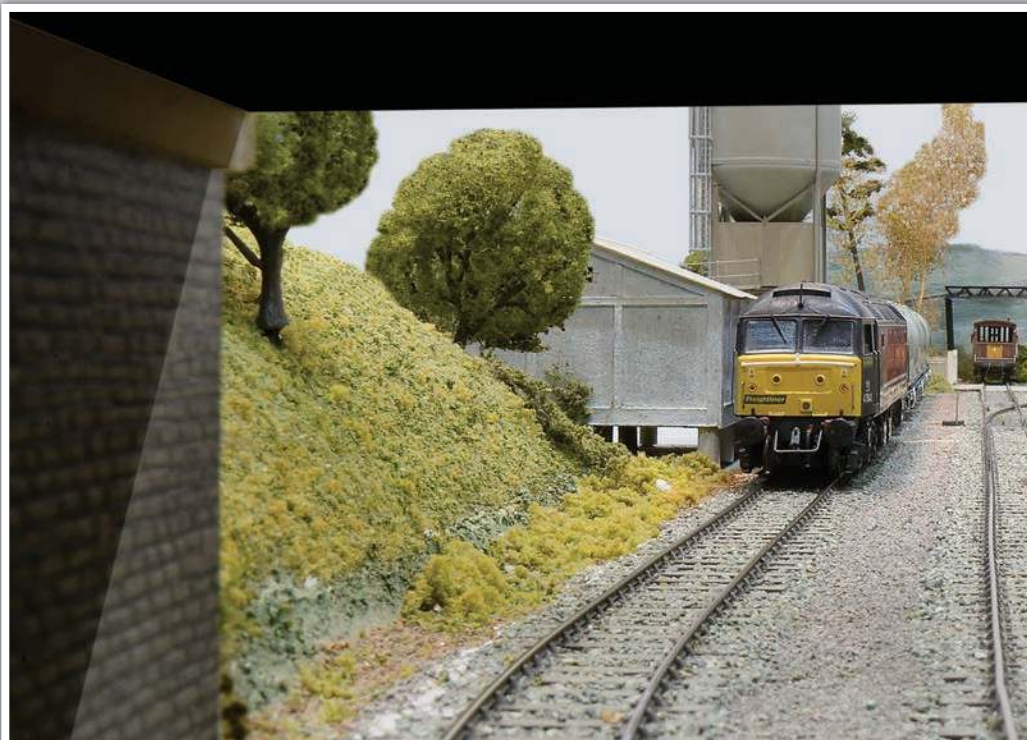
I can't say that I have ever been a huge fan of 'celebrity' diesel locomotives – I prefer to model the everyday 'run of the mill' locomotives with a weathered appearance as opposed to the ex-works look. I have a model of 47843 *Vulcan* in ex-Virgin Trains livery, as this locomotive operated into Cornwall on cement trains to Moorswater (on the Looe branch), for Freightliner in 2003. I have always regarded this as my celebrity locomotive.

During the Stafford exhibition, Geoff Griffiths, one of our guest operators had a model of 37308 in BR blue. I know that this locomotive operated in Cornwall in 2003. It was painted for the Old Oak Common open day in 2000. The model performed faultlessly over the weekend and got plenty of admiring glances, mostly from me.

THE CORNWALL MINERALS RAILWAY

The line from St Dennis Junction to Melangoose Mill opened on June 1, 1874. In 1883, a short narrow-gauge tramway was laid to Meledor Works. This was replaced by a standard gauge siding in 1886. In 1896, the line, along with most of the independent railways in Cornwall, was sold to the Great Western Railway.

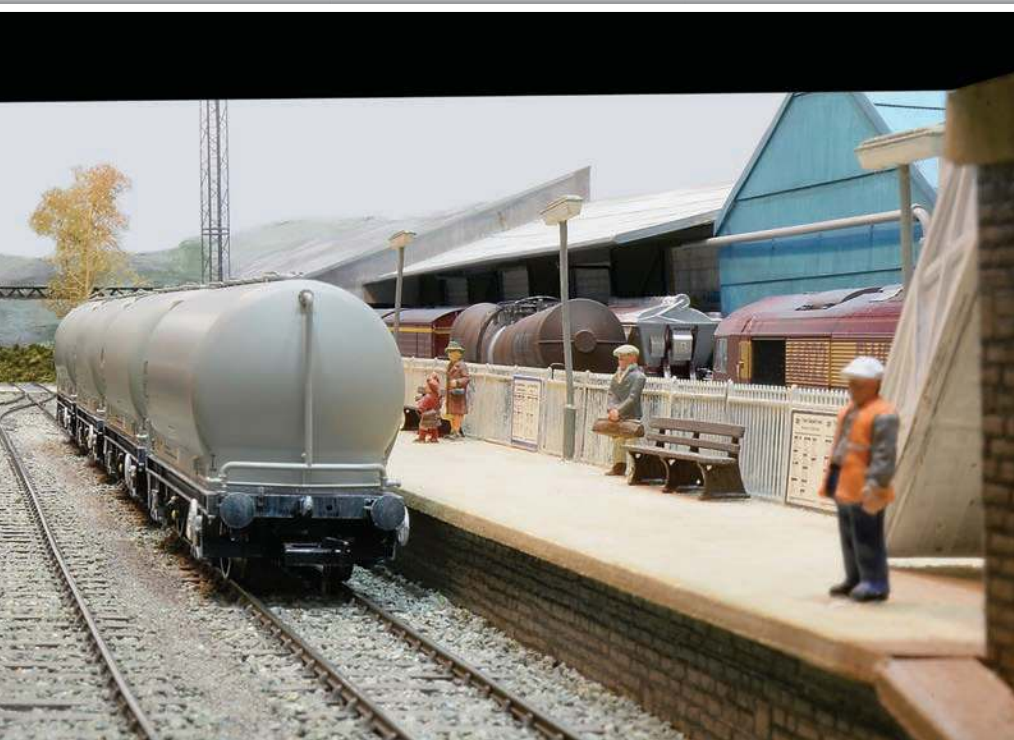
Sadly the line to Melangoose/Melangoose Mill was closed to traffic in June 1973. The site is now owned by Imreys Minerals UK.



66022 is captured in the headshunt as a cement lorry approaches the Cement Terminal.

SPECIAL RECOGNITION

Thanks are due to the following modellers who have helped encourage me and assist at model railway exhibitions: Kevin Broadhurst, Steve Adcock, Nick Gurney, John Humphries, Geoff Griffiths, Ian Blackall, my dad, Dennis Crow (or Jim as he likes to be known) and my uncle, Graham Crow. A special thanks to my wife and family for putting up with me and my imaginary little part of Cornwall, and of course Dave Smart for letting me take over the charming model railway layout that he built.



At the Stafford exhibition my dad purchased a Legomanbiffo sound chip for a Class 37, with the intention of having a locomotive that he could bring to exhibitions and run on 'Melangoose'. It's a change for him wanting to run a steam special whenever our backs are turned. I was pondering what livery to select as we already had two EWS locomotives – one in Transrail livery and a triple grey Trainload subsector example. I recall that EWS finished with Class 37 haulage in Cornwall in 2004, but in May of 2005, Valley Trains loco-hauled 'celebrity' 37425 was put to work in the Duchy, working china clay trains and petroleum TTA tanks before going back to its normal Rhymney Valley passenger trains. So, 37425 was selected in its BR/EWS heritage livery. We chose not to name the locomotive, as it received its *Pride of the Valleys* nameplate after its working holiday in Cornwall. Also, the many pictures on the internet of the locomotive in Cornwall show the weathering on its grey roof and silver Class 60 type buffers fitted. I like a challenge.

47843 *Vulcan* (on hire to Freightliner) collects empty wagons in the 'Melangoose' Mill Cement Terminal. These will be replaced by full wagons waiting in the platform.

153 382 in the decorative Devon and Cornwall livery arrives with a train from Par. Wessex Trains operate a sparse service to 'Melangoose'.



CLASS 158 DMU

An Icon of the 1990s, Reborn for the 21st Century



The Class 158 second generation Diesel Multiple Units (DMU) were introduced in the early 1990s and were the instant success of the Regional Railways fleet - replacing locomotive-hauled passenger trains on secondary expresses, cross-country routes and other regional services.

Announced shortly after the real units entered service and released in the mid-1990s, the Branchline OO scale model was one of the first developed from scratch by Bachmann to standards that eclipsed many of the other models available at the time.

Although excellent for its era, in the 20 years since the model was first released, British modellers have come to expect even higher levels of detail, including variants specific to individual class members, and new technologies such as Digital Command Control - bringing even more functionality and features such as lights and sound. Production methods have also advanced, allowing new techniques and printing processes which enable even the minutest elements of the livery to be replicated.

Confident that we could create a model fit for the 21st century modeller, we set about tackling this '90s icon from scratch once again and the first all-new Bachmann Branchline models were released at the end of 2019.

WORKING IN MULTIPLE

Conductive couplings are fitted between the two cars meaning a single Plux22 decoder is all that's required to convert the model to DCC. The models are fitted with working BSI couplings on the outer ends allowing multiple units to be mechanically coupled together.



The BSI couplings fitted to the outer ends of each model not only look realistic, they're operational too allowing two units to be coupled together.

LET THERE BE LIGHT

The new Branchline model features directional lighting with illuminated destination panels, cab lights, interior lights and door interlock lights. For analogue users, four switches are located on the underframe of Car A which allow the directional lights to be turned off independently at either end - ideal if you're coupling two units together and wish to turn off the lights at one end only, plus a switch to turn the interior lights on/off and another to alternate the directional lights between day and night mode.

For DCC users each of these lighting functions can be controlled using the function buttons on your controller; you can also operate the door interlock lights and the cab light.

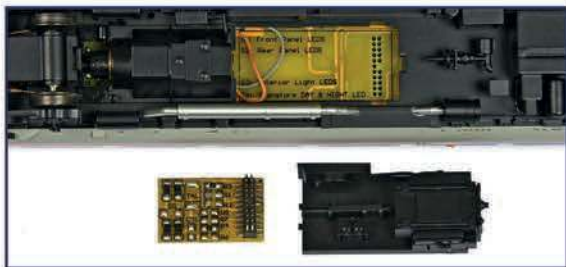
- Directional lighting, including illuminated destination panels and switchable Day/Night mode
- Interior Saloon lighting
- Cab lights
- Door interlock lights



Door interlock lights are controlled by DCC, SOUND FITTED models even have a door opening/closing sequence with realistic sound effects including the distinctive hustle alarm.



Front and rear lights are a given, but the Branchline model also features illuminated destination panels plus the option to illuminate the rear cab - ideal for the guard travelling in the rear of the train as they often do.



AS EASY AS DCC

The model uses the latest DCC decoder technology - Plux22; the socket is located beneath a clip-on chassis section on Car A.

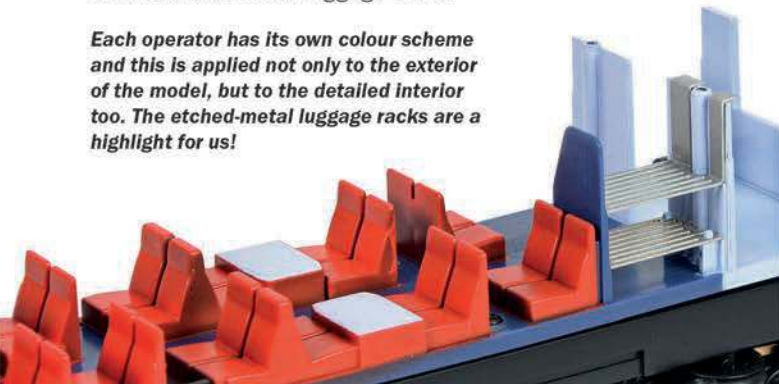


With a speaker pre-fitted to every model, it's never been easier to fit DCC or even better, Sound to your model. **SOUND FITTED** models are pre-fitted and ready for you to enjoy the full array of sound & lighting functions straight from the box.

IT'S ALL IN THE DETAIL

Despite the tinted windows, we've not held back on the saloon detail which really comes alive when the interior lights are turned on. Seats and tables are decorated in corporate colours that coordinate with the exterior livery, whilst etchings are used to capture the characteristic metal luggage racks.

Each operator has its own colour scheme and this is applied not only to the exterior of the model, but to the detailed interior too. The etched-metal luggage racks are a highlight for us!



Externally, it's the livery application which really brings these models to life, every element has been replicated in miniature from the sweeping curves of the East Midlands Trains livery to the GWR logo and lining which in actual fact is made up of many individual lines - some would simply print this as a single block but Bachmann has gone the extra mile to replicate it on our models. Whilst the main colours are spray-painted - for which copper masks are made by highly skilled craftsmen to ensure the crisp lines and sharp transitions between the different colours - the myriad smaller logos, symbols and lettering are applied by 'tampo-printing' and for every application the model or component must be perfectly aligned to the machine - for prints comprising multiple colours this process must be repeated for each colour.



The GWR logo is made up of lots of individual lines, another feature replicated in miniature on the Branchline model.

Like most modern machinery each vehicle, and in particular its solebar and underframe, is liberally decorated with warning stickers and equipment labels and again these are reproduced right down to the last detail. Every print requires a separate application, as does each colour used, so it's easy to see why it takes so long to produce these masterpieces in miniature!



As with most modern trains, there are countless markings, labels and logos on the lower bodysides and underframe - all of which are legible!

AVAILABLE NOW FROM YOUR BACHMANN STOCKIST



**SOUND
FITTED**

31-517DS Class 158 2-Car DMU 158849 BR Regional Railways



**SOUND
FITTED**

31-519DS Class 158 2-Car DMU 158766 GWR Green (FirstGroup)



**SOUND
FITTED**

31-518SF Class 158 2-Car DMU 158773 East Midlands Trains



**SOUND
FITTED**

31-520SF Class 159 3-Car DMU 159013 BR Network SouthEast (Revised)

ALSO AVAILABLE : 31-517 Class 158 2-Car DMU 158849 BR Regional Railways, 31-518 Class 158 2-Car DMU 158773 East Midlands Trains, 31-519 Class 158 2-Car DMU 158766 GWR Green (FirstGroup) & 31-520 Class 159 3-Car DMU 159013 BR Network SouthEast (Revised)



BACHMANN CLASS 158/159

Words & photography: Andy York

I'm certainly not decrying the model, not one bit, but for me, BR's Class 158 introduced from 1989 meant the end of so many locomotive-hauled secondary passenger services with their spread across the whole network at a rapid rate. Of course, they were generally more comfortable, faster and with better facilities (what have the Romans ever done for us?), but they heralded the end of window-hanging and the beat of Type 2 and 3 haulage. Aside from some brake and air-conditioning reliability issues they were the face of a more efficient railway to come.

Criticism has been made in some quarters about the gestation period to bring these models to market, but the important thing to bear in mind is that the original announcement was for a DCC-ready upgrade to the Class 158. During development, a decision was made to completely retool the product, which gives

the benefit of a very good new chassis design with the drive contained within the height of the chassis, allowing clear space through the model, something that benefits not just the Class 158/9, but also the forthcoming first-generation DMU models.

The Class 158 arrived on the market just before Christmas and the three-car Class 159, with its centre trailer vehicle in revised Network SouthEast livery, is arriving imminently. Each of the Regional Railways, East Midlands Trains, GWR and Network SouthEast models are available in DCC-ready and sound-fitted variants.

Unboxing

Out of the box the immediate impression is of typical Bachmann livery application quality; it's faultless. The chassis and underframe details are excellent too, with no ugly compromises around tanks and equipment boxes; link that to the low floor

depth inside the vehicles, which means interesting interior detail such as luggage racks can be modelled, in addition to the seating. It seems inconceivable how all this can be achieved, until I got to completely dismantle the vehicles. I unscrewed the front valance of the car, prised off the bodyshell and then unscrewed the ceiling circuit board with interior LEDs. The seating unit is then unscrewed and removed and the main circuit board can be lifted to reveal the small three-pole motor, concealed within the underframe gearbox with a cardan shaft and small gearbox unit to drive both axles of the inner bogie of the vehicle, as with the real units. Simple when you see it, but a brilliant bit of design. Both driving vehicles are motorised and the performance is quite strong considering the diminutive size of the drive mechanism.

Good design continues with the provision for DCC decoder fitting; a section of the underframe shown in the clear instruction unplugs to reveal the PluX22 blanking plug



and socket. Bachmann's Plux22 decoder is only 3.5mm thick, which further helps to conceal the provision. The use of connective 12-pin couplings between vehicles means that only one decoder is needed per train, giving full control over the day or night directional lighting, passenger saloon lighting, door interlocking lights and back-lit destinations over the driver's windscreen. Analogue users can choose their lighting modes from the four DIP switches under the chassis.

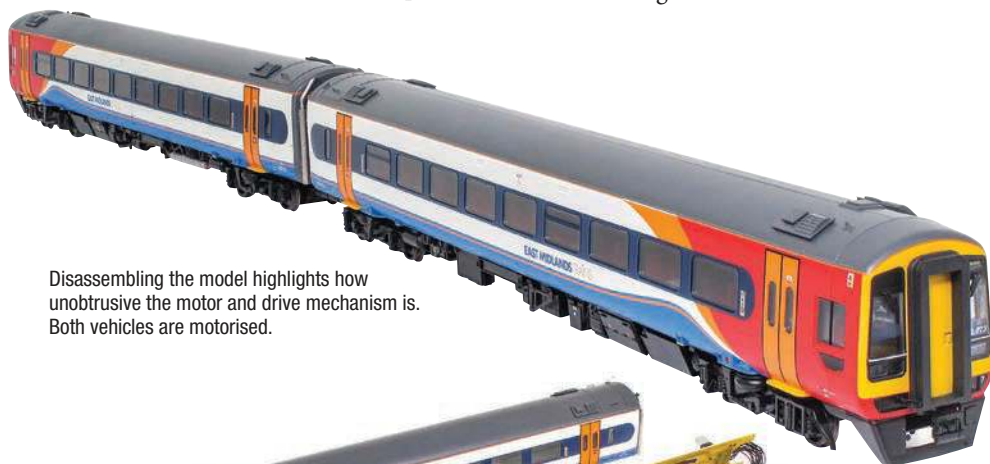
Key Observations

There are no ugly tension-lock couplings on the ends of the model nor NEM pockets,

but a good representation of the prototypical BSI coupling, which enables the modeller to realistically couple two units together.

In operation, the unit is quiet, smooth and doesn't falter. The passenger saloon lighting looks good through the tinted windows and, although subtle, I am impressed with the destination blind. However, both ends of the review model are for 'Nottingham' and I would imagine they would be difficult to replace with an alternative.

Overall, it's an excellent model and well-worth the extended wait. The chassis design excites me as I've seen just how beneficial this will be on the underframe detail of the forthcoming Class 117 and 121 models.



Disassembling the model highlights how unobtrusive the motor and drive mechanism is. Both vehicles are motorised.



Directional lighting with day and night modes is very good but we do like the back-lit destination blinds.



datafile

BASICS

Manufacturer: Bachmann Europe PLC

CATALOGUE REFS:

(31-517) 158849 BR Regional Railways
(31-517DS) 158849 BR Regional Railways (Sound-fitted)
(31-518) 158773 East Midlands Trains
(31-518SF) 158773 East Midlands Trains (Sound-fitted)
(31-519) 158766 GWR Green (FirstGroup)
(31-519SF) 158766 GWR Green (FirstGroup) (Sound-fitted)
(31-520) Three-car DMU 159013 BR Network SouthEast
(31-520SF) Three-car DMU 159013 BR Network SouthEast (Sound-fitted)

RRP: Class 158: £259.95 DC, £349.95 DCC sound-fitted

Class 159 £329.95 DC, £429.95 DCC sound-fitted

Gauge/scale: 16.5mm gauge / 1:76 scale / OO

Era: 1989 - present

Company/Operator: BR to Privatisation

Weight: 445g

Body and chassis: Plastic

Minimum Curve Radius: R2

Wheel Profile: RP25

Couplings: Conductive inter-car coupling and working inter-unit coupling

Accessories: Exhaust and uncoupling tool



HELJAN

CLASS 03 0-6-0

Words: Howard Smith Photography: Tony Wright

The rapid dieselisation of Britain's railways yielded many new designs of motive power, many of which would have to prove themselves in use. The Beeching axe severed many of the country's lines that connected smaller rail-served manufacturers, docks or branch lines to pick-up goods interchanges. Before it did, however, the routes that required locomotives with light axle loadings and short wheelbases to negotiate the tight track radii were operated by a variety of compact locomotives.

All change

British Railways identified a requirement for 230 shunting locomotives to a lighter axle loading than its standard Class 08 shunters. Built between BR's Doncaster and

The finest representation we've seen of the Class 03 in O gauge. So widespread and long-lived was the Class that tempting excuses can be found to justify at least one.

Swindon works, the class of diesel-mechanical locomotives were 19T lighter than the Class 08s and entered service numbered D2000-D2199 and D2370-D2399. The mechanical drive from the 203hp Gardner engine to a five-speed gearbox and final drive unit was less-cumbersome and lighter than a generator and two large traction motors. The class was mostly used on short freight work, though some members would see use as station pilots, often coupled to a runner wagon or 'match truck' to complete track circuits because of their short wheelbase.

Smaller cousin

For the O gauge modeller, kits for this locomotive have been available for some time, and a ready-made – though unpainted – brass model was issued a number of years ago

from Bachmann. Modellers have had access to this prototype in model form in limited numbers, though surely Heljan has made it more accessible with the release of this new model in RTR form.

At 875g, the model has a useful tractive effort. We received a sample of D2011 in BR green with late crest and conical chimney (2051). The locomotive is one of only two from this batch to have the chimney, the other eight have the wider 'flowerpot' chimney. For such a locomotive to be issued with a choice of 10 liveries, from BR green to BR blue, industrial blue/yellow and industrial yellow is great for modellers, though I wonder who will be brave enough to paint a model into First Capital Connect purple as applied to 03179 *Clive* when based at Hornsey depot?



All visible detail is injection-moulded plastic. Cab windows are flush – bar one on our sample that wasn't glued properly, and the brass-painted frames of these are incredibly fine and well-reproduced. Joining the classic panel lines, louvres and rivet detail around the model are the grab handles and handrails. Fitting these in the factory must be a repetitive task given their number, though all are uniform, sticking out to the correct amount.

The front grille mesh looks the part, its finesse more obvious if weathered a lighter shade of dirt than the dark of the shadow behind it. The final drive protection screen is well-executed and of a different pattern than the front grille. Often mangled after many years' service, these screens would act as protection should a shunter slip when riding the steel steps with the locomotive in motion.

Class modifications existed, though most occurred after the locomotives were sold into industry in the late-1960s and 1970s following them being surplus to requirements. Many remaining class members, which remained in active use by BR, were upgraded to dual air/vacuum brakes, these being easily distinguished by the large vertical air tank by the cab on the right-hand running plate and additional air hoses to their buffer beams. At least 17 were so-equipped.

Fine representation

The buffer beams on Heljan's model are convincing and complemented by sprung buffers and draw-hooks. The buffers can only be pressed in by a millimetre or so, a

compromise no doubt of the exposed rear bufferbeam. Underframe detail is good – sanding gear, chunky scale coupling rods and jackshaft drive weights to name but a few of the areas which caught my attention. Air pipes, isolator valves and purge valves are left in injection-moulded plastic, though larger vacuum pipes have been painted white from the sprue. The treadplate which keeps these pieces tidy and mostly hidden from view is an injection-moulded plastic piece that fits over the die-cast metal chassis. When lifting this heavy model from the box, this area is prone to stress and on our sample the running plate had flexed, lifting away from the chassis. Repairing it with superglue and securing it in its correct place with soft grip modelling clamps might be a requirement, or simply handle with care using the MDF base to which it is screwed, provided.

As expected on a Heljan product, cab interior detail doesn't disappoint, with linked power regulators, capstan-style handbrake wheel, gauge cluster, brake controls and gear selectors clearly on show. An interior cab light is included, courtesy of an SMD LED on a circuit board, though I question the positioning of the cables to this, which travel the length of the cab floor in the centre. Perhaps painting these to better blend with the cab floor and fitting a crew will help disguise them. Modellers can access the cab interior to further detail or perform upgrades via the two screws that locate and secure it to the underside.

It's a fine model, though I wonder if the chassis had been made from a one-piece solid die-cast piece it might have alleviated

Outside the cab, the early-style rear lights look the part, though watch out for fragile glazing. Inside, controls and gauges please the eyes.

datafile

BASICS

Manufacturer: Heljan A/S

CATALOGUE REFS:

(HJ2051) BR green, conical chimney, D2011, red buffer beams
(HJ2052) BR green, flowerpot chimney, un-numbered, red buffer beams
(HJ2055) BR green, chevrons, flowerpot chimney, D2033, red buffer beams
(HJ2057) BR green, chevrons, flowerpot chimney, un-numbered, red buffer beams
(HJ2070) BR blue, chevrons, flowerpot chimney, 2096, yellow buffer beams
(HJ2071) BR blue, chevrons, flowerpot chimney, 03107, yellow buffer beams
(HJ2072) BR blue, chevrons, flowerpot chimney, un-numbered, yellow buffer beams
(HJ2073) BR blue, chevrons, conical chimney, un-numbered, black buffer beams
(HJ2074) Industrial yellow, conical chimney, un-numbered
(HJ2075) Industrial blue / yellow, flowerpot chimney, un-numbered

RRP: £395.00

Gauge/scale: 32mm gauge, 1:43.5 scale O

Era: 5-9

Company/Operator: BR / Private industrial / WAGN / First Capital Connect

Weight: 875g

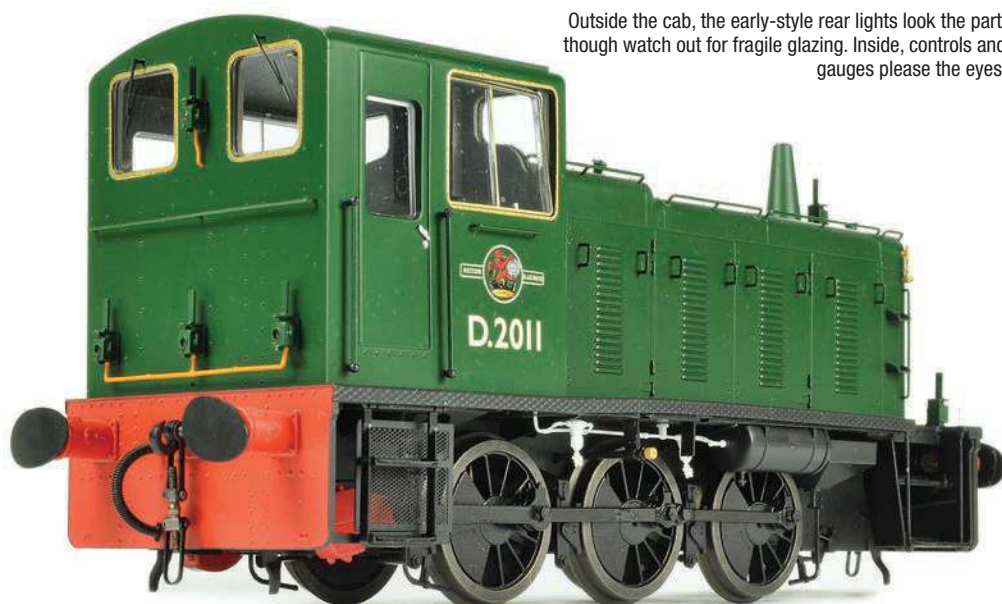
Body: Plastic

Chassis: Die-cast metal

Wheel Profile: GOG finescale standard

Couplings: sprung screw-link

the flexible running plate problem, unless perhaps it is for manufacturing purposes and Heljan intends to produce a Class 04, soon? Time will tell, though for now, these shunters bar a few negligible quirks are excellent for all your shunting needs. More, please!



BRM TV

BRITISH RAILWAY MODELLING

DIGITAL EDITION EXCLUSIVE

HELJAN 03 UNBOXING

Howard Smith casts an eye over the latest RTR locomotive to come from the Danish manufacturer. Does it live up to expectations? Find out in the review with the Spring 2020 issue of BRM, on-sale at www.pocketmags.com/BRM

EXCLUSIVE

WATCH THE VIDEO HERE

(You must have a wifi connection to stream video content)



BACHMANN CLASS 24/1

Words & photography: Andy York

Bachmann's all-new Class 24/1 gives a ready-to-run version of the prototype that we have not yet seen, a Class 24 with headcode indicator boxes on the cab roof and without the lower valance to the body sides. The new tooling gives a level of detail and definition at a much higher level than the previous Class 24/0 model, in addition to addressing concerns on the shape of cab elements. This model certainly looks the part from all angles when compared with photographs.

In my pre-TOPS numbering trainspotting days, we didn't know the locomotives as Class 24, 24/1 or even Class 25, they were

just 'Sulzer Twos'. The early locomotives (D5000 – D5150) had lower-powered engines (1160hp) and became Class 24, while the later numbered locomotives from D5151 onwards utilised 1250hp Sulzer engines and became Class 25. D5000-D5113 (later classified as Class 24/0) were readily identifiable from a distance with their domed cab roof without a headcode box and from that point upwards it became a little more difficult, from a distance, to know what you were seeing. Simplistically a Class 24/1 is a 1160hp Sulzer Type 2 with a headcode indicator box and not just because the TOPS numbering gave it a 24 1xx identification. As the story moves

on with the 1200hp (Class 25) versions, the early numbers, D5151 – D5176, also featured the same style of headcode box, while D5177 onwards had an extended box with the horns on each side of the headcode indicators. I'll end that line of thought there before we get bogged down with Class 25 detail differences, but hopefully it explains some of the differences.

Our review model is 24137 in BR blue, which dates it to post-April 1974, when it was allocated to Stoke and prior to its storage and withdrawal in May 1978. This version illustrates that Bachmann has made a commitment to different tooling slides to accommodate variations that happened within the operational life of the locomotives, with its plated-over boiler room grille and bodyside grab rails and





A detailed side profile, with exquisite bogies. Much to appreciate for the discerning modeller.

steps which led to the steam boiler filler at cant rail height. The grille was plated over to reduce the amount of cool air getting into the steam-heat boiler area and the steps were plated over for safety reasons as the class frequently operated below overhead electric equipment. The other two models released in BR green feature the boiler room grille and steps thanks to a different tooling slide for the bodyside being utilised. One hopes that with Bachmann's approach to producing different slides within the tooling, we may see revised versions of the earlier Class 24 in due course and even revised Class 25s.

The chassis and bogie frames feature excellent relief and detail, comparing very well with the prototype. In this area there were variations in the size and style of the fuel and water tanks around and beneath the batteries, so it's worth consulting photographs if you wish to renumber the model. The same caveat certainly applies when looking at headcode boxes too, as some Class 24/1 locomotives received later-style headcode boxes with side horns after maintenance at one or both ends. The class is certainly a minefield for the unwary and that's before we get into tablet recesses for Inverness allocations – always check the photographs and dates!

An excellent detailing pack is included

with nine items to be fitted to each bufferbeam area and etched makers' plates for the drivers' cab doors. Snowploughs are also included to fit into the NEM pockets, but these can compromise what detail and couplings can be fitted, especially if you expect the model to go around train-set curves.

Moving up to the body, I've checked and every detail permutation looks to be right. The livery application is spot on. The glazing is commendably flush with a lack of prismatic effect that affects other models.

Fitting a 21-pin DCC decoder is straightforward - six screws to remove the body and you are in. The model has a pre-fitted speaker below the fan grille area so fitting sound is easy. Directional lamps and subtle headcode indicator characters can also be configured by the analogue control user via the dipswitches beneath the tanks.

I've always enjoyed the smooth and quiet running of a Bachmann Bo-Bo chassis and the Class 24/1 meets all expectations in that regard, comfortably handling a rake of 12 Mk. 1 coaches; something which would have been a struggle in real life on an excursion from Stoke to Skegness!

There's nothing to fault the model and, just like the real thing, it's a useful and capable maid of all work for a 1970s layout.

datafile

BASICS

Manufacturer: Bachmann Europe PLC

CATALOGUE REFS:

(32-440) Class 24/1 D5135 BR Green (late crest)

(32-441) Class 24/1 D5149 BR Green (small yellow panels)

(32-442) Class 24/1 24137 BR blue

RRP: £149.95

Gauge/scale: 16.5mm gauge / 1:76 scale / 00

Era: 5-7

Company/Operator: BR

Weight: 418g

Body: Plastic **Chassis:** Metal

Minimum Curve Radius: R2

Wheel Profile: RP25

Couplings: NEM-mounted tension-lock

Accessories: Bufferbeam detailing, cosmetic screw-link couplings, snowploughs, etched makers' plates



End appearances appear accurate in every respect - a classic shape to which many enthusiasts have become fond.



Configure the directional lights and headcode indicators to your taste using the dipswitches hidden underneath.

ACCURASCALE PFA CONTAINER WAGONS

Words & Photography: Andy York

Accurascale seems to have a knack of selecting interesting prototypes to add to its range of quality wagons, and they have succeeded again with these small two-axle container wagons.

Manufactured in 1986 and 1987 for Cawoods, a subsidiary of the British Coal Corporation, by the Standard Railway Wagon Company to diagram PF012A to carry open containers of coal, the fleet extended to 172 vehicles. These generally ran as train loads from collieries in the North East, East Midlands and South Wales to ports on the west coast for export of household coal and smokeless fuels to Ireland. The containers were handled at established ports in the same manner as conventional closed containers.

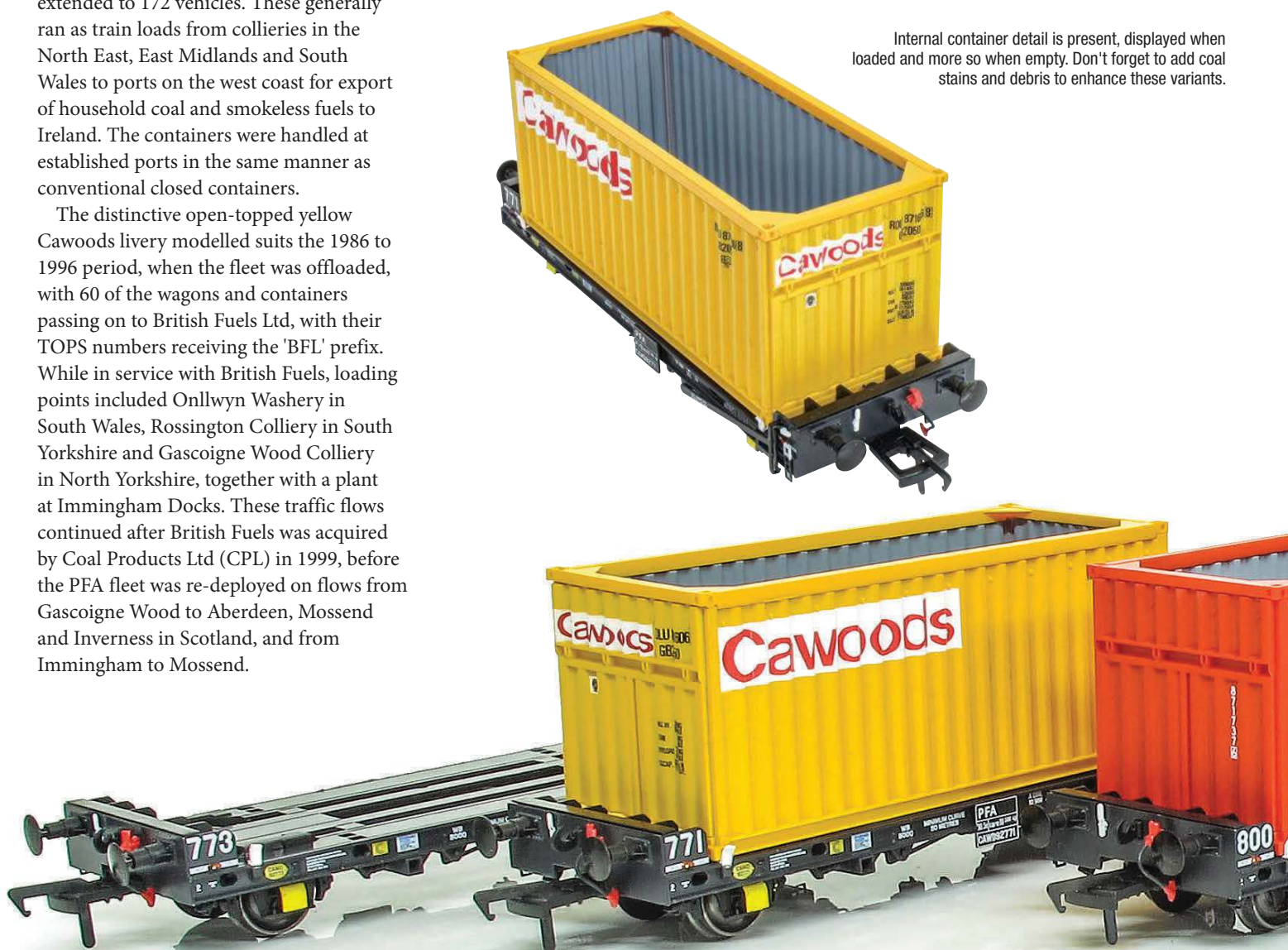
The distinctive open-topped yellow Cawoods livery modelled suits the 1986 to 1996 period, when the fleet was offloaded, with 60 of the wagons and containers passing on to British Fuels Ltd, with their TOPS numbers receiving the 'BFL' prefix. While in service with British Fuels, loading points included Onllwyn Washery in South Wales, Rossington Colliery in South Yorkshire and Gascoigne Wood Colliery in North Yorkshire, together with a plant at Immingham Docks. These traffic flows continued after British Fuels was acquired by Coal Products Ltd (CPL) in 1999, before the PFA fleet was re-deployed on flows from Gascoigne Wood to Aberdeen, Mossend and Inverness in Scotland, and from Immingham to Mossend.

While in CPL ownership, many of the 'coaltainers' retained the British Fuels red livery, although a portion of the fleet was repainted green and received the corporate branding of the new owners. Both types were mixed together in rakes. Accurascale has also produced the green CPL Distribution-liveried models exclusively for Rails of Sheffield.

A number of wagons were acquired by

British Gypsum, with containers adapted with hinged-top closed containers for transporting powdered gypsum and painted in blue and white liveries. The containers handled imported gypsum, from docks, and flue gas desulphurisation (FGD) gypsum, a by-product of coal-fired power stations, which was then transported to plasterboard production sites at East Leake, Mountfield and New Biggin.

Internal container detail is present, displayed when loaded and more so when empty. Don't forget to add coal stains and debris to enhance these variants.



Direct Rail Services Ltd. (DRS) acquired a fleet of 23 for carrying low-level nuclear waste in smaller containers in 2000. These versions of the model proved very popular as they are ideal for creating short-length trains with DRS traction from nuclear facilities to disposal facilities or transhipment overseas. Empty wagons can often be seen as barrier wagons in these consists and also in those of nuclear flask wagons.

Diminutive, for a UK container wagon, the unladen model wagon weighs in at 40g thanks to the die-cast components in the chassis with a wealth of fitted plastic detail parts underneath representing the braking systems. Accurascale has produced the models in a form that allows for easy replacements of the wheelsets for those working to EM and P4 standards. There is a substantial amount of small printing to the sides of the wagon, particularly the DRS version, which is festooned with small notices meticulously printed into the

recesses of the solebars.

Each wagon is individually numbered and, because the wagons are available in sets of three in each of the liveries, it means that there are some 45 separately-numbered chassis within the range. The printing on the containers is exemplary, too. Noteworthy examples of this are the data printing on the ribbed ends of the Cawoods-branded containers, which must be very difficult to achieve with tampo printing, and also the side-print on the half-height nuclear waste containers.

Some versions of the model are already in short supply directly from Accurascale but may still be available through its stockists.

Priced at £69.95 for a set of three wagons, the models represent good value considering the quality, and savings of around 10% are available if you wish to buy four packs of the wagons with the different running numbers in the Cawoods and British Fuels liveries direct from Accurascale.

datafile

BASICS

Manufacturer: Accurascale

CATALOGUE REFS:

Triple Packs:

PFA - Cawoods Coal Containers A-D

PFA - British Fuels Coal Containers E-H

PFA - British Gypsum Container I-M

PFA - DRS LLNW - Nuclear Half Height Container N-O

RRP: £69.95 per triple-pack

Gauge/scale: 16.5mm gauge, 1:76 scale
OO

Era: 8 to 9

Company/Operator: Private Owners

Weight: 53g with container, chassis 40g

Body: Plastic

Chassis: Die-cast and plastic parts

Wheel Profile: RP25

Couplings: NEM mounted tension-lock

Warranty: 12 months – customercare@accurascale.co.uk

Underframe detail is admirable for an RTR wagon. Air reservoirs, brake rods and pipes are exposed through the skeletal underframe.



BSC BOGIE TIPLERS AND COIL CONVERSIONS

Words & Photography: David Ratcliffe

Ahead of Accurascale's new JSA bogie coil wagon models, rolling stock historian, David Ratcliffe, explains the history of these conversions.

In 1988, the British Steel South Wales fleet, which in total numbered 114 wagons, began to be repainted in a blue livery with large white lettering. JUA No. BSSW 26577 seen at Cardiff in August 1990.



Following the renationalisation of the steel industry in 1967, the British Steel Corporation embarked on a plan to concentrate production at or near coastal sites using imported iron ore from Australia, Canada, Sweden and South America. Rail was chosen to move the ore from selected coastal terminals to nearby steelworks, and as the import berths at Immingham, Redcar, Port Talbot, and Hunterston gradually came on stream separate fleets of 102T glw, 77T capacity, bogie tippler wagons were introduced.

Being gravity-discharged, they had no need of doors and were relatively cheap to construct and maintain, while the major drawback of the traditional tippler, the need to uncouple each wagon before unloading, was overcome by adopting Association of American Railroads type F couplers, which were designed to rotate at one end of the wagon. This enabled each vehicle to be tipped while still coupled to its neighbours, reducing the time taken to unload an entire train of 21 wagons to less than an hour.

Built at BREL's Shildon Works in 1971

for the working between Immingham and Scunthorpe, the first batch comprised 107 wagons fitted with FBT bogies, a similar design to the French Y25 bogies, while all the subsequent batches, which were constructed by Redpath Dorman Long (a BSC subsidiary) had BSC primary coil 'Axle-Motion' bogies.

The RDL-built wagons also differed from the first batch in the configuration of their body side strengthening ribs. Furthermore, each end had three horizontal strengthening ribs compared to the two vertical ribs found

on the BREL batch, while the RDL-built wagons had a more substantial top-lip to the body as well as being fitted with a ladder on one side.

The 114 British Steel Teeside tipplers, which were built in 1972 to work from Redcar to the steelworks at Consett and South Bank, included a greater number of 'Outer' wagons within the fleet since the severely-graded route between South Pelaw and Annfield Plain restricted the Consett trains to just nine wagons (see table). The 123-strong British Steel Ravenscraig fleet was also equipped with more 'Outers', for in addition to the 21 wagon iron ore workings from Hunterston, they were also used in shorter formations of up to 11 wagons to carry limestone from the British Steel quarry at Hardendale, near Shap, to Ravenscraig steelworks.

Prior to the opening of Hunterston import terminal in 1980, the BSRV tipplers were used to carry iron ore from the quay at Glasgow General Terminus to Ravenscraig, as well as handling the coking coal movement from Rothesay Dock to the steelworks. In later years they could also be seen delivering dolofines from Thrislington Quarry to Ravenscraig.

Closure of Consett steelworks in 1980 saw the entire BSTE fleet sold to Procor, and by 1982, they had been repainted and refurbished for lease to both Amey Roadstone (ARC) and Foster Yeoman in limestone traffic from the Mendips. They retained their original AAR couplers, although the rotary couplers were all fitted with anti-rotation plates so that they would no longer be able to rotate. The wagons were put to work out of the quarries at Merehead and Whately, serving stone terminals across the south of England, not equipped with hopper discharge that had previously relied on railway owned MSVs.

By the early 1990s the Yeoman wagons could also be found working from the Isle of Grain carrying granite, which had been delivered by coastal ships from a quarry at Glensanda on the west coast of Scotland.

Coil Conversions

Following the ending of steel production at Ravenscraig in 1992, a handful of the BSRV 'Inners' had been transferred to South Wales, where they joined the BSSW fleet working from Port Talbot to Llanwern, but in 1996 some 65 BSRV tipplers were rebuilt as covered coil carriers by Marcroft Engineering at Stoke. In addition to being fitted with coil cradles, the wagons gained

new ends and three-piece telescopic sliding top covers to protect the "bright steel" load (cold reduced coil) from the elements, while standard drawgear was fitted at both ends. Recoded on TOPS as JSAs, they were also renumbered and re-prefixed BSSP (British Steel Strip Products). Operating from the BSC works at Llanwern and Port Talbot, in both block steel trains and in air-braked wagonload services, they supplied steel stockholders in the Midlands, the North West of England, and on Teesside, as well as being used to carry coil from South Wales to the BSC galvanizing and coating plant at Shotton in North Wales.

In 1997, the works at Stoke undertook a further coil conversion when 18 of the bogie tipplers, that had previously been in stone traffic with ARC, were modified for the wagon leasing company CAIB. These were fitted with a more rounded three-piece top cover, and although initially intended for the movement of imported coil from Boston Docks to Wolverhampton, they soon joined the BSSP JSAs working out of South Wales.

The BSSP JSAs were purchased by VTG in 2003, after which they were fitted with new unpainted aluminium top covers, while in 2007 a further 40 JSAs were produced by WH Davis, these being rebuilt from former

Key information

1. BSSC 26095-99/105 are non-rotary coupler Outers; BSSC 26100-104/106 rotary coupler Outers.
2. BSTE 26542-552 were non-rotary coupler Outers; BSTE 26553-563 rotary coupler Outers.
3. BSSW 26666-671 were rotary coupler Outers; BSSW 26672-677 non-rotary coupler Outers.
4. BSRV 26779-789 were rotary coupler Outers; BSRV 26790-800 non-rotary coupler Outers.

BSSW tipplers.

In 2011 the 'Boston Docks' JSAs were withdrawn and by 2016 the popularity of curtain-roofed coil wagons had seen the rest of the JSAs put into store. Consequently, in 2018 it was decided that some 65 would be refurbished and have their top covers removed for use in the hot rolled coil traffic from Port Talbot to Corby, Hartlepool, Llanwern, and Trostre. This work was undertaken at Eastleigh by Arlington Fleet Services with the wagons retaining their JSA code and VTG 40xx or VTG 41xx numbers.

Finally, in something of a surprise development, during 2019 WH Davis converted 16 of the remaining covered JSAs back into bogie box wagons for use in spot-hire aggregates flows, although they also kept their previous VTG prefixed numbers.

Number series	Code	Builder Note
BSSC 26000-106	PTA	BREL Shildon 1971 1
BSTE 26450-563	PTA	Redpath Dorman Long 1972 2
BSSW 26564-677	PTA	Redpath Dorman Long 1974 3
BSRV 26678-800	PTA	Redpath Dorman Long 1977 4
BSSP 4020-76/91-99	JSA	Marcroft Engineering 1996
CAIB 3063-80	JSA	Marcroft Engineering 1997
VTG 4100-39	JSA	WH Davis 2007

In 2004, the former BSSP JSAs were fitted with aluminium sliding top-covers and in May of that year No. VTG 4093 was recorded awaiting unloading at Middlesbrough Goods.





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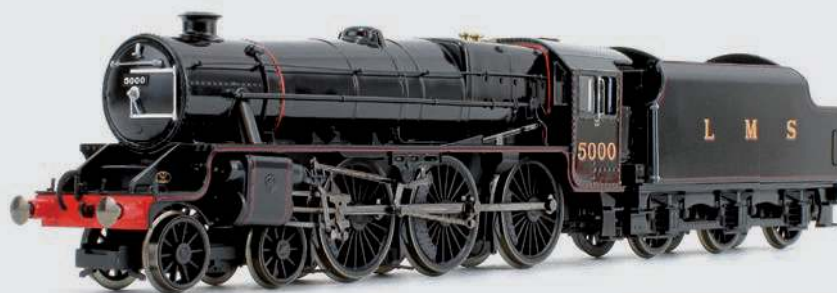


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The Footplate to the Boardroom

Part 1 - The Footplate Years



a lifetime of learning
Burton Coggles

RICHARD MACLENNAN

THE FOOTPLATE TO THE BOARDROOM. PART 1 – THE FOOTPLATE YEARS A LIFETIME OF LEARNING BURTON COGGLES by Richard MacLennan, self-published

As the author writes, 'Not many people in this industry can claim they have gone from the footplate to the boardroom', but he has, and it's a most-engaging and fascinating journey he describes.

The tale begins with the author's train-watching experiences in late-'60s and early-'70s, when, as a child, he recalls being frightened by escaping steam or the roar of a Deltic starting up! He clearly loves the mighty EE Type 5s, and many mentions of them are made during his formative years in the late-70s, towards the end of the Deltics' careers. The story moves on from his joining the railways in the early-'80s in Scotland (where he'd been living in the Highlands since 1972), working in Haymarket Depot. His subsequent career path gives us a professional insight into the running of a railway in a very much 'traditional' way, with a steam-age infrastructure to operate over – a system all but disappeared today. Indeed, an infrastructure the author had a go at demolishing, when he ran into some crossing gates at Lairg! It resulted in an official reprimand on his file, which, as far as is known, is still there to this day. It's well-written, informative and amusing in parts. It's printed on high-quality gloss stock and the numerous photographs have reproduced very well, apart from a couple or so rather gloomy B&W images. Speaking of the illustrations, they'll be of great use to modellers of the Scottish Region in the post-steam era, with so little changing in many ways, at least to begin with. I agree entirely with the author about the HSTs being the greatest passenger train ever built, but I'm puzzled over 'Burton Coggles' on the cover. Burton Coggles is a tiny Lincolnshire village, a few miles from where I live, and,

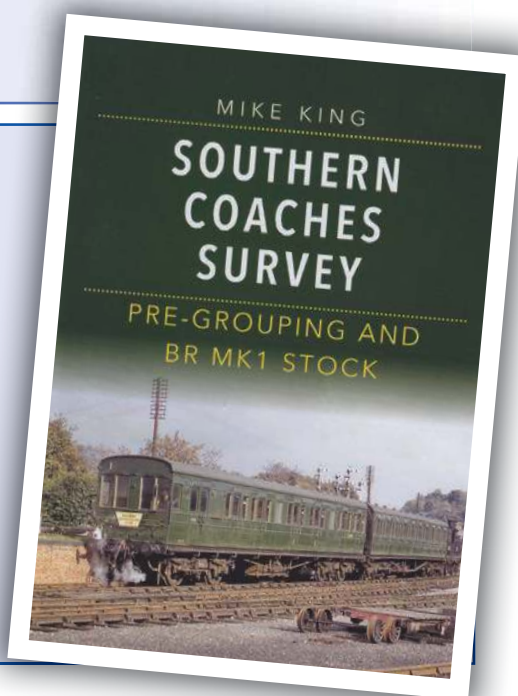
apart from HSTs once racing past it until the end of last year, I cannot see the connection. A worthy read and I look forward to the next instalment.

Price: £17.00 W: www.whitropesiding.co.uk

SOUTHERN COACHES SURVEY PRE-GROUPING AND BR MK1 STOCK by Mike King, OPC/Crecy

This lavish book is a kind of follow-up to the author's definitive *An Illustrated History of Southern Coaches* (OPC, 2003) and is fully up to the same superb standards. Indeed, every now and then a volume appears that really is 'definitive' on the subject inside, and this is just one of those. It should find a home on every railway and model railway enthusiasts' bookshelf, whatever their principal interest or area of study. It is superbly-illustrated and also contains numerous drawings and tables. Passenger-rated vans are also included in the survey. A few of the photographs (out of necessity) illustrate preserved stock, but most show the various vehicles in revenue-earning service for their respective companies. Anyone building models of any of the carriages described will find this wonderful work invaluable. It might seem slightly incongruous to include both the Southern's oldest and newest stock (separated by nearly 30 years!) in the same volume, but it does complete the whole story. Entirely recommended.

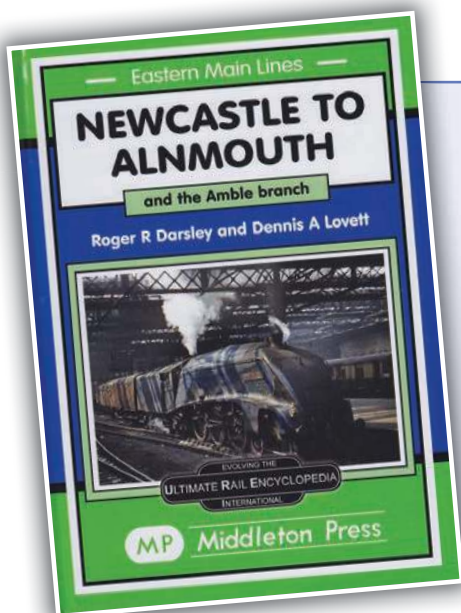
Price: £25.00 W: www.crecy.co.uk



EASTERN MAIN LINES NEWCASTLE TO ALNMOUTH AND THE AMBLE BRANCH by Roger R Darsley and Dennis A Lovett, Middleton Press

This is the latest A5 hardback book in the popular series 'Evolving the Ultimate Rail Encyclopedia' (a grand boast?). It follows the usual format of tracing the route between the settlements in the title, from pre-Grouping times right up to the current scene, all very well-illustrated in B&W. There are the usual useful maps, tickets and timetables included, and the whole thing should be of use to modellers in all scales and gauges. As with all these volumes, speaking personally, I'm left with a profound sense of loss at what has disappeared forever. Not just the inevitable changes in the locomotives/rolling stock down the years, but also the beautiful architecture and infrastructure. What a joy the likes of Widdrington Station was! At least the surviving stations (some of which appear to be little more than just 'bus station' halts) appear to be clean and well-maintained.

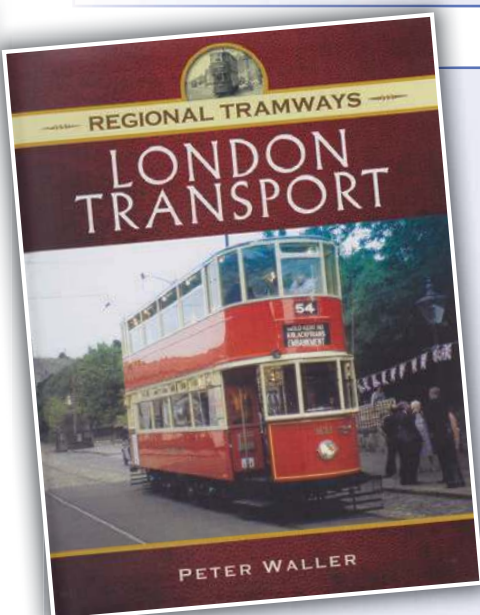
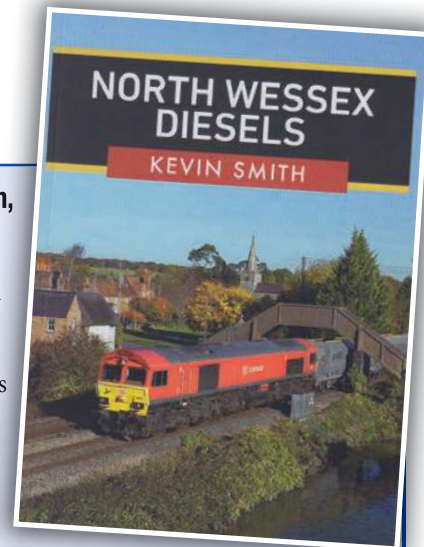
Price: £18.95 W: www.middletonpress.co.uk



CREWE IN THE DAYS OF BR BLUE by Michael Hitchen, NORTH WESSEX DIESELS by Kevin Smith, and RAILWAYS IN THE PEAK DISTRICT by Christopher Nicholson and Peter Barnes, Amberley

I have three new books from Amberley this month, covering quite widely different subjects. Though thought of in rather disparaging terms at the time, the BR corporate blue years are now looked at with a sense of nostalgia. A sense manifesting itself in more BR blue-period layouts being seen at shows nowadays. Crewe was (is?) the largest railway town in the realm, and, up to the virtual halving of the platforms in 1985, must have had one of the largest stations. Even long after steam had gone, there was a vast selection of motive power to be seen, and this variety has been very well-captured in Michael Hitchen's book; all in good-quality colour. The mighty Works are shown, as well as infrastructure items, and the whole book should be of great use to 'modern image' modellers. The Wessex diesels volume is far more up-to-date, illustrating the years from 2004 to 2019. In this respect, it's certainly more-colourful than the Crewe book! Indeed, some of the more 'extreme' colours applied to the locomotives/rolling stock appear entirely incongruous to the sylvan surroundings they're running through, at least to my eyes. It's very well-illustrated in full colour and will be of help to modellers of the period. The volume describing the Peak District is also of great use to modellers (and all railway enthusiasts), and is beautifully-produced. It's basically a short history of the railways in this area of outstanding natural beauty and is an excellent complement to the larger established works on the subject. The photographs, both in B&W and colour, have reproduced exceptionally-well. There are also contemporary advertising images. Though much has been lost, at least some of the routes remain accessible to walkers, ramblers or cyclists.

Price: £14.99 W: www.amberley-books.com



REGIONAL TRAMWAYS LONDON TRANSPORT by Peter Waller, Pen and Sword.

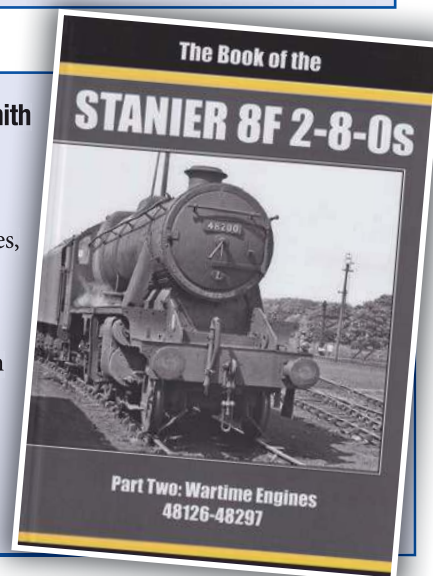
Among the usual varied selection from P&S this month, I've chosen this splendid book describing London's trams. Although they were still operating when I was a boy, I never saw 'traditional' trams in the Capital (the trams I saw were in Sheffield). This well-illustrated volume is a snapshot of what has now long gone – and shows me what I've missed! I say 'long gone', but the current Croydon Tramlink and the Docklands Light Railway, which both qualify (at least to me) as modern tramway systems, are included. Though many of the B&W illustrations are of some antiquity, they've generally reproduced very well, and all will be of great help to modellers. In fact, some of the B&W images are as crisp as some of the more-recent colour shots. Linear maps of the once-extensive systems are also included. Models of tramways are popular at shows, and this book should help those who model London's trams – or the trams in preservation. It's also of great help for those modelling any aspect of the periods depicted, with its wealth of street furniture, advertising and signage. Jolly good.

Price: £25.00 W: www.pen-and-sword.co.uk

THE BOOK OF THE STANIER 8F 2-8-0S PART TWO: WARTIME ENGINES 48126-48297 by Ian Sixsmith and Richard Derry Irwell Press.

This is the second volume describing these highly-successful Stanier-designed 2-8-0s (since the class goes up to 48772, there'll be a few more!). It follows the normal format of introductory notes (the first volume contains the main class history), then a pictorial record of every locomotive in the list. Build dates, modifications, repair dates, boilers, tenders, allocations and withdrawal dates are all included, making a most-comprehensive record. There are at least two pictures of every locomotive, at various times throughout their lives. All these pictures have reproduced very well, printed on very good, high-gloss stock. As with every volume in this famous 'book of' series from Irwell, it'll be invaluable to modellers in all scales and gauges. With a very 'highly-recommended' given to it by me, at this stage I have to express an interest. I helped by writing captions and proof-reading/checking many others. I can't find any errors in the work, but if there are any, then you can blame me! Great value for money.

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NEW & RECENT TITLES FROM LIGHTMOOR PRESS

British Railways The First 25 Years Vol. 9: London

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The ninth in the series of books depicting the first 25 years of British Railways, this volume covers the London Midland Region lines in the London area, from the terminus stations at Euston and St. Pancras out as far as Watford and St. Albans respectively, the North London Line from Broad Street and the former London Tilbury & Southend Railway (LT&SR) lines out of Fenchurch Street. We look at the steam age Euston station and its rebuilding for the West Coast electrification in the 1960s. We visit the four principal motive power depots serving the Western and Midland Divisions, Camden, Willesden, Cricklewood and Kentish Town, and also Devons Road on the North London Line and Ripple Lane, Plaistow, Tilbury and Shoeburyness on the London Tilbury & Southend. A variety of locomotive types are featured, including 'Jubilees', 'Royal Scots' and Stanier Pacifics, English Electric Type '4's, 'Peak' diesel-electrics, Sulzer and BRC&W Co. Type '2's and ex-L&NWR 0-8-0s alongside '4F' 0-6-0s and '8F' 2-8-0s together with Fowler and Stanier 2-6-4Ts. On the North London Line the EMU-worked passenger services from Broad Street to Watford and Richmond contrasted with the variety of steam classes on cross-London freights. The LT&SR was the LM&SRs busiest commuter line into the Capital and there was also a healthy summer traffic of day trippers, primarily to Southend. The 'Tilbury' was almost exclusively a tank engine line with the LT&SR-designed 4-4-2Ts working alongside the LM&SR 2-6-4Ts until electrification in 1962. As with the previous London volumes, we have included detailed maps of the principal stations and depots showing how they fit into the Capital's streets. Platform layouts are also provided for each of the termini.

DUE MID-MARCH 2020. 208 pages, gloss art paper, laminated board covers.

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VOLUME TWO JIM SUMMERS

This volume looks at how the Caledonian planned its operations and how it handled matters when things did not go according to plan. The constraints and possibilities of track layouts are considered, followed by an explanation of the art of compiling timetables and the workings of the rolling stock in order to make the most of that infrastructure. Did a L&NWR Manchester guard really work the Balerno Branch? The methods of dealing with particular traffic and controlling assets, from wagons to sacks, are described. Carlisle Control office is featured and the significance of traffic control in the Quintinshill accident is explored. The operating practices for some important flows of traffic are examined, with a chapter on electrification, for which the Caledonian sought powers. The reader will feel an affinity with the traffic men and understand the issues with which they grappled, while students of other railways will wonder how their companies addressed similar questions.

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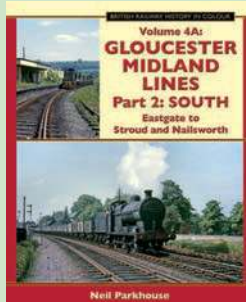
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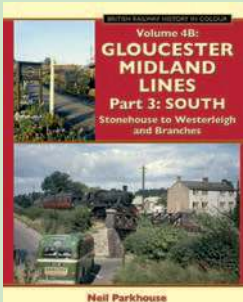
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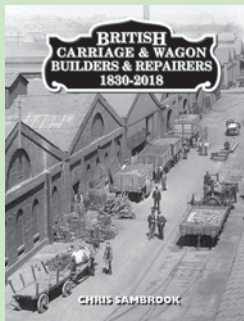
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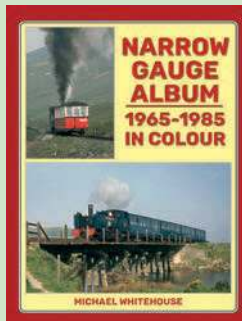


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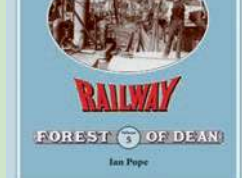


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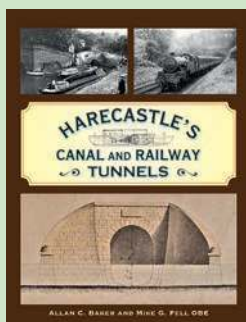
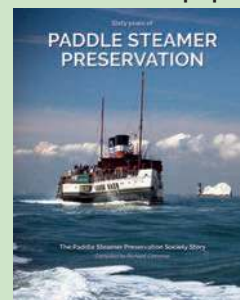
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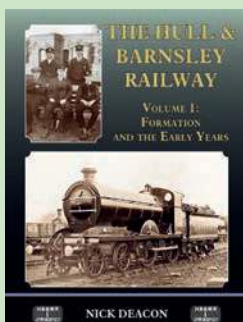
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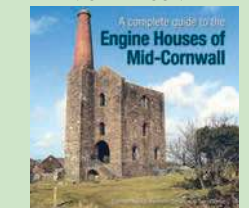


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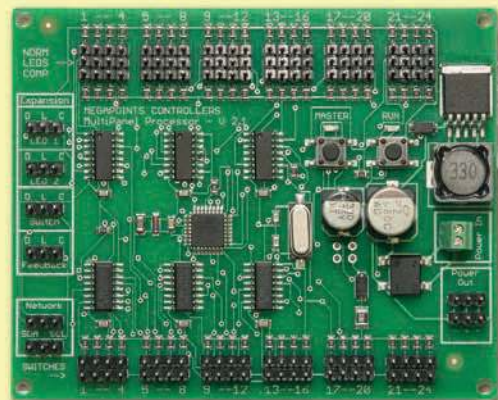
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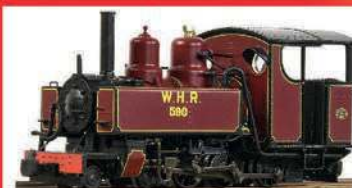
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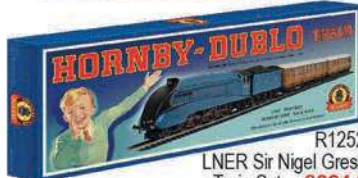
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APRIL 18

Narrow Gauge South 2020 The Barton Peveril College, Chestnut Avenue, Eastleigh, Hants, SO50 5BX
Opening Times: Saturday 1000 - 1630
Prices: Adults £8.00 accompanied under 16s £2.00, Family (2+2) £18.00

W: www.narrowgaugesouth.org.uk

TRAIN & TOY FAIRS

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T: 07504 035955

APRIL 2

Joe Lock Toy & Train Fair - Theydon Bois Village Hall, Coppice Row, Theydon Bois, Epping, Essex CM16 7ER

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APRIL 5

Joe Lock Toy & Train Fairs - Aston Clinton

Aston Clinton, Stablebridge Road, Aston Clinton, Aylesbury, Buckinghamshire HP22 5NE

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Prices: Admission £2.00

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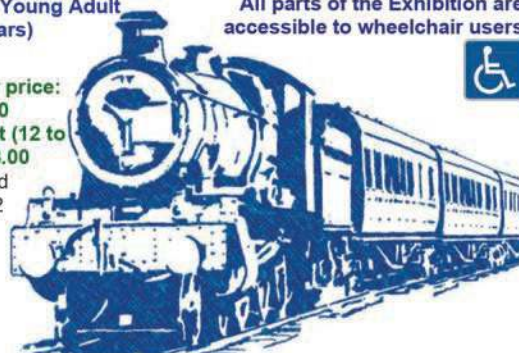
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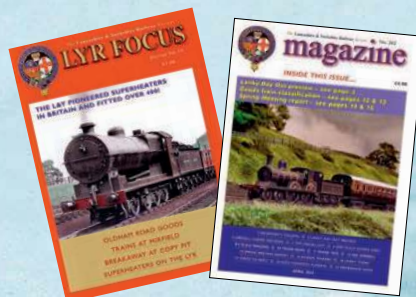
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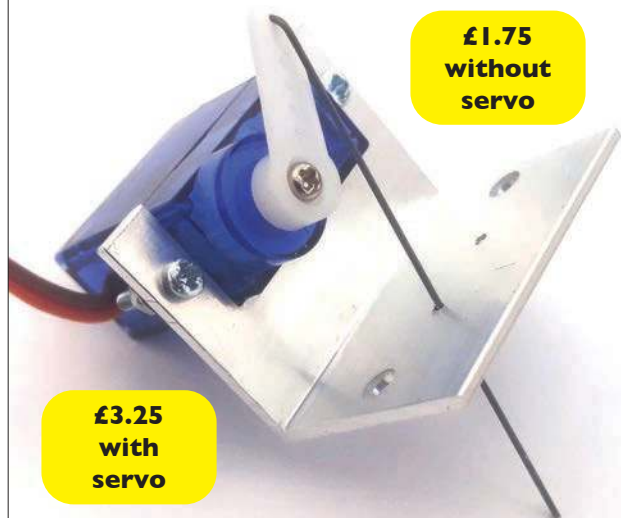
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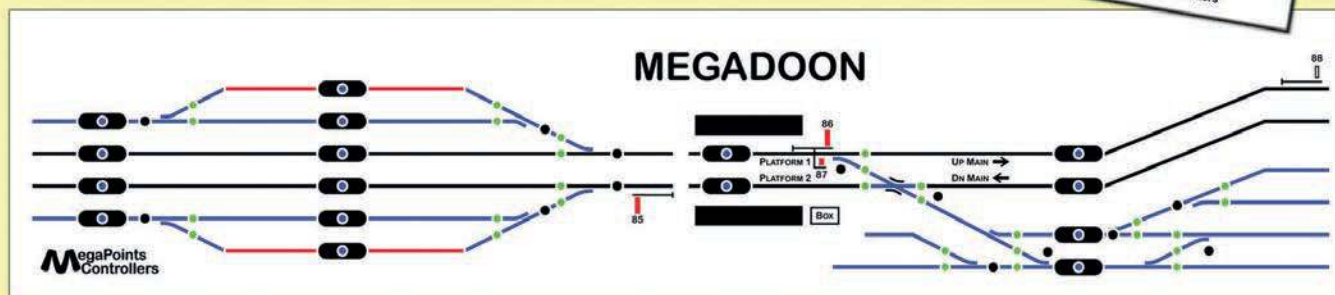
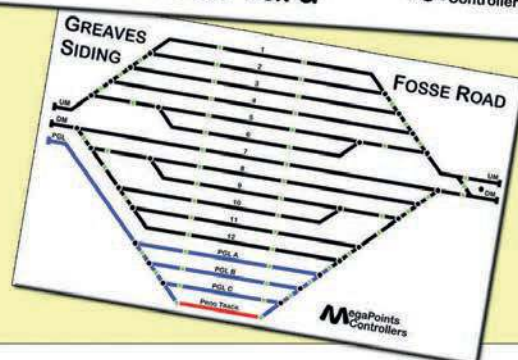
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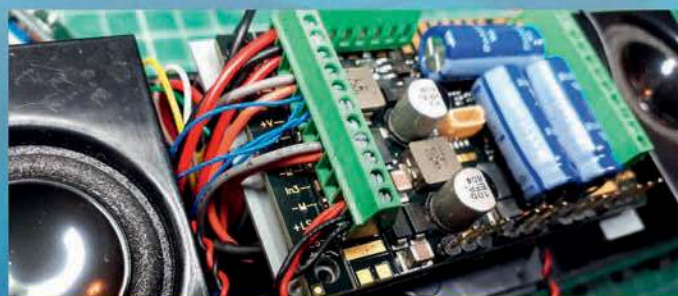
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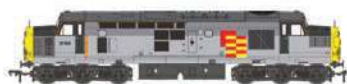
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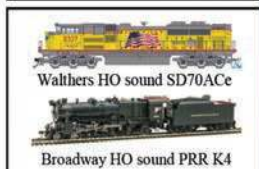
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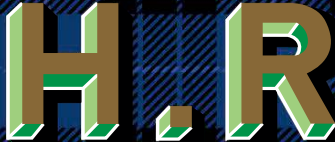
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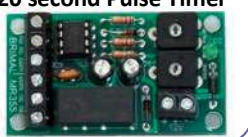
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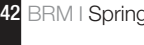
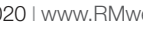












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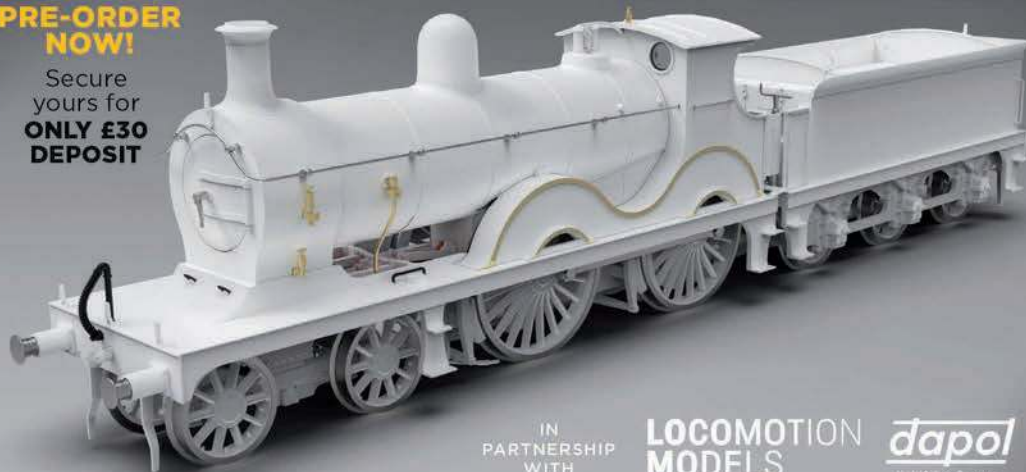
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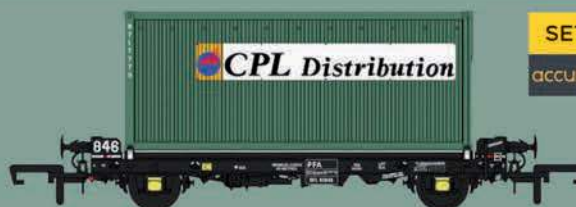


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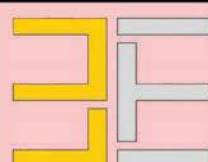


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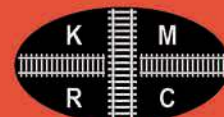


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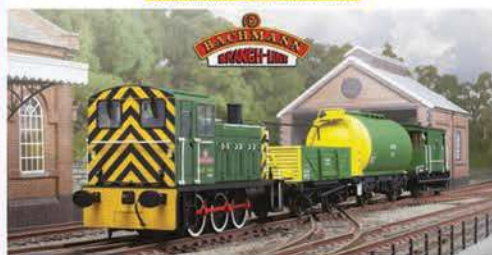


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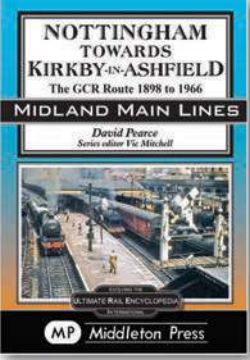
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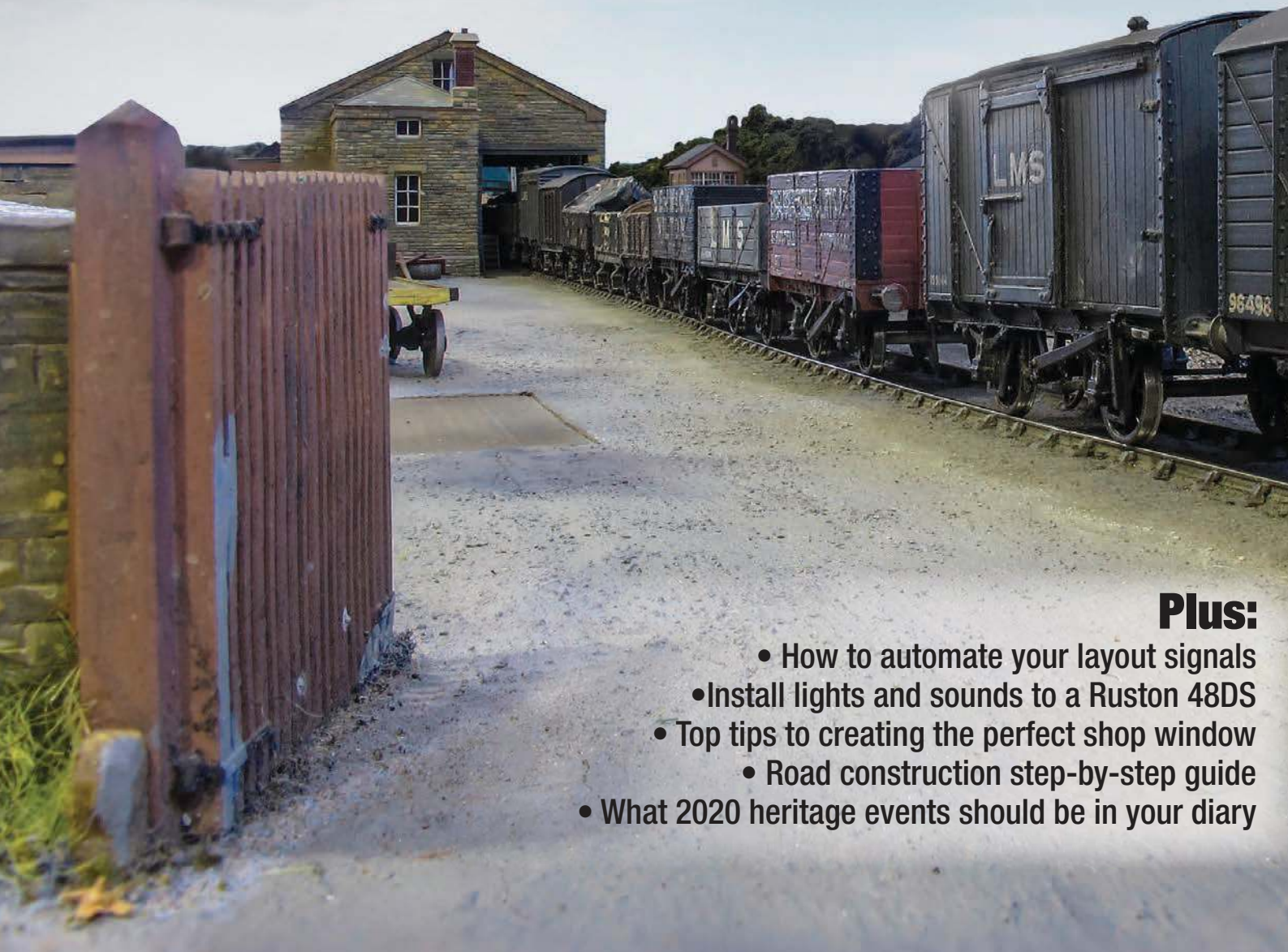
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Buffer Stop

Do children draw diesels?

Poking around the London Toy Fair, we were amazed at the number of train-shaped toys on offer. Weirdly, all of them were steam trains.

If you remember steam on the main line you'll now be well into your sixties. Apart from trips to a heritage railway, and of course a certain little tank engine, children won't have been exposed to a steam locomotive. Despite this, it seems the toy manufacturers, who presumably know what they are up to, think train means steam locomotive. They are probably right. It's often said that if you ask a child to draw a train, you'll see picture of steam locomotive. Is steam just more memorable?

We are often told that people model what they remember, but maybe the future for steam models is brighter than we expect. Will children who play with steam suddenly decide they want to model diesels when they grow up?



Burning Question...

Who is your favourite Thomas the Tank character?

Andy: Daisy. Because she's not a steam locomotive.

Phil: Edward. He was the first locomotive in the book and still the best!

Howard: Have to admit I can't really remember most of them – the fat controller? Keeping track of the rest of the team...

Debbie: My son is called Thomas, so he has to be my favourite.

Ruth: Percy, because he reminds me of Howard.

This month in pictures...



If you leave your camera in the back of the car overnight, the results can be interesting as Andy found out. Fortunately, while he had a cup of tea, it warmed up again and all was fine.



The Toy Fair isn't all about trains, Phil bumped into some full-size characters from the TV series Paw Patrol and was hugged by a Pikachu.



Exactly how did someone move their magnificent 10 1/4 in gauge locomotive into Ally Pally for the London Model Engineering Exhibition, we wonder?



Finally, a buffer stop for this page. We spotted this one at the Tallylyn Railway while photographing for our Thomas coverage.

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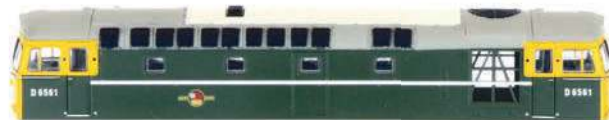
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Weathered

2F-027-012 - Silver Bullet Nacco/ECC 3387 789 B 066-8

Weathered

7F-072-004 - William Duck 1 - 7 Plank (2 Door)

7F-072-004W - William Duck 1 - 7 Plank (2 Door) Weathered

7F-072-005 - F Meaker 4 - 7 Plank (2 Door)

7F-072-005W - F Meaker 4 - 7 Plank (2 Door) Weathered

4F-028-015 - 45 Ft Container Hi Cube P & O Ferry 008462 4/008032 4

4F-028-016 - 45 Ft Container Hi Cube P & O Ferry 008462/008032 Weathered

4F-028-017 - 45 Ft Container Hi Cube Russell 459644 6/459677 0

4F-028-018 - 45 Ft Container Hi Cube Russell 459644 6/459677 0 Weathered

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